



ADA TRANSITION PLAN UPDATE

2024



Providing Equitable Access for all Transportation Users



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The Indiana Department of Transportation (INDOT) Civil Rights Counsel/ADA Coordinator has prepared this transition plan with the assistance of the six districts' ADA Liaisons, various members of the TAC, and contributing divisions of the INDOT that are responsible for ADA compliance within INDOT's programs, policies, facilities, activities, and services.

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INDIANA DEPARTMENT OF TRANSPORTATION

100 North Senate Avenue
Room N758
Indianapolis, Indiana 46204

PHONE: (855) 463-6848

Eric Holcomb, Governor
Michael Smith, Commissioner

Dear Citizens,

The Indiana Department of Transportation (INDOT) is committed to building the safest and most resilient transportation system for all users. Our mission is to serve and provide citizens and visitors of our state meaningful access to our programs, services, and facilities, regardless of ability, in accordance with Section 504 of the Rehabilitation ACT and the Americans with Disabilities Act (ADA).

With this update to the Americans with Disabilities Act (ADA) Transition Plan, INDOT continues our commitment to this effort. Since the previous plan was published in 2021, INDOT has collected additional accessibility data, implemented projects involving accessibility improvements, and developed new statewide plans that complement and influence the ADA program.

INDOT remains fully committed to ensuring and promoting accessibility and nondiscrimination in all our program areas and facilities. This updated ADA Transition Plan demonstrates and effectuates our commitment to ongoing barrier removal, compliance monitoring, public participation, and program evaluation to provide a means to the most inclusive future of transportation.

We, at INDOT, believe collaboration with the public and partnering agencies is key to our success. We welcome all public comments concerning this plan. Public comments may be submitted to accessforall@indot.in.gov or by calling our customer service hotline at 1-855-INDOT4U (855463-6848). Thank you in advance for your participation! Your voice matters.

Sincerely,

Michael Smith, Commissioner
Indiana Department of Transportation



PREFACE:

INDOT FACTS

INDOT is responsible for constructing and maintaining interstate highways, U.S. routes, and state roads in Indiana, including adjacent overpasses, ramps, and traffic control devices, including signs and traffic signals, on these roadways. Local cities, counties, and towns are responsible for Indiana roadways that are not interstate highways, U.S. routes, or state roads.

INDOT also administers federal highway funds provided to cities, towns, and counties, and supports and provides financial assistance to public transit systems, freight and passenger rail, and port facilities. INDOT also encourages and assists in the development of airports, landing fields, and other aviation navigation facilities.

INDOT has six district offices that handle day-to-day operations such as construction and detours, traffic signal operations, permits, and maintenance operations, including repairing potholes and plowing snow, along with various other responsibilities.

- INDOT maintains more than 29,000 lane miles (over 11,200 centerline miles) of highways in Indiana.
- INDOT is responsible for maintaining more than 5,700 bridges across the state.
- INDOT supports approximately 4,500 rail miles and regulates more than 110 public access airports across the state.
- INDOT has approximately 3,600 employees, making it one of the state's largest agencies.
- INDOT's FY 2024 capital program budget was approximately \$2.23 billion.
- INDOT's FY 2024 operating obligations were approximately \$633.8 million.

INDOT Funding

INDOT currently receives funding from several sources, the primary sources are federal and state motor fuel taxes. See: [INDOT Funding listing](#).

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CHAPTER 1

The Indiana Department of Transportation (INDOT) Americans with Disabilities Act (ADA) Transition Plan directs the department's efforts to provide an accessible transportation system within the State of Indiana. The plan is to provide guidance to remove barriers and to enable equitable access to INDOT's programs, facilities, and services for transportation users of all abilities. The plan provides an overview of INDOT's external ADA program, outlines INDOT's mission and ADA policy, and identifies methods to assist INDOT in complying with ADA regulations. The plan is intended to be a living document to reflect the current INDOT ADA program and compliance status. The INDOT ADA Compliance Manager is responsible for implementing the plan. Attachments to the plan outline accomplishments to date and future actions necessary to enable ADA compliance.





1.0 INDOT MISSION AND ADA POLICY STATEMENT

INDOT's mission is to serve and connect people and communities by providing transportation facilities with the safest, most accessible, efficient, and cost-effective methods independently usable by a broad spectrum of users.

INDOT recognizes the importance of providing equitable access to all transportation system users, including those with impairments affecting mobility. Accessible building facilities, right-of-way features, and websites offer critical access to employment, health care, education, and government services. Equitable access for people with disabilities facilitates independence and self-sufficiency, quality of life, participation in the work force, and meaningful social and economic contributions to society. The INDOT ADA Transition Plan is an important step in helping INDOT eliminate barriers and provide equitable opportunities for disabled individuals to use INDOT programs, right-of-way features, and building facilities based on their specific needs.

INDOT is adhered to 28 CFR 35.130. It is INDOT policy that no qualified person with a disability shall, solely by reason of disability, be excluded from participation in, be denied the benefits of, or otherwise be subjected to discrimination under any program or activity that receives federal financial assistance administered by the United States Department of Transportation.

1.1. BACKGROUND

1.1.1 Section 504 of the Rehabilitation Act of 1973. Section 504 of the Rehabilitation Act makes it illegal for the federal government, federal contractors, and state and local governments, receiving federal funds to discriminate on the basis of disability. It requires that state and local governments ensure persons with disabilities have equal access to any programs, services, or activities receiving federal funding. It also requires them to ensure their employment practices do not discriminate on the basis of disability.

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1.1.2 Americans with Disabilities Act of 1990. The ADA expands the requirements of Section 504 of the Rehabilitation Act. It is an important civil rights legislation that prohibits discrimination against people on the basis of disability by public entities irrespective of receipt of federal financial assistance. This act ensures people with disabilities have equal rights and opportunities as able-bodied people. The ADA is comprised of the following five titles that cover different aspects of public life:

- Title I (Employment)
- Title II (State and Local Government)
- Title III (Public Accommodations)
- Title IV (Telecommunications)
- Title V (Miscellaneous Provisions)

INDOT's Transition Plan is made pursuant to Title II of the ADA, which applies to state and local governments and the programs, services, and activities they provide. As a state government, INDOT must ensure its programs, services, and activities provided to the public are accessible to individuals with disabilities regardless of the source of funding.

Under Title II, public entities employing 50 or more people must develop a transition plan outlining the steps necessary to complete any structural changes (i.e., changes to physical assets) to achieve program compliance.

INDOT must meet the following requirements:

- Designate an ADA/Section 504 Coordinator responsible for coordinating INDOT's efforts to comply with and carry out its responsibilities under Title II.
- Make available to applicants, participants, beneficiaries, and other interested parties, information regarding Title II and its applicability to the services, programs, or activities of INDOT (Notice to the Public).
- Complete a self-assessment of services, policies, and practices in compliance with the requirements of Title II that identify physical obstacles in the public entity's facilities that limit the accessibility of its programs or activities to individuals with disabilities.
- Describe the methods that will be used to make the facilities accessible.
- Adopt and publish a grievance procedure for complaints alleging any action that violates Title II.



1.2. REQUIREMENTS

1.2.1 Responsibilities. INDOT's many responsibilities include maintenance of all interstates, U.S. routes, state roads, welcome centers, and rest areas throughout the state, including overpasses and exit/entrance ramps on these roadways. Construction of these roads is also INDOT's responsibility, along with traffic control devices along these roadways, including signs and traffic signals

INDOT is not responsible for the maintenance of county, city, or town roads or sidewalks, except in special circumstances. Local counties, cities, and towns are responsible for all other roadways that are not state roads, interstates, or U.S. routes.

IC 8-23-6-3(d) assigns jurisdiction over the maintenance of county, city, and town roads, sidewalks, and related pedestrian facilities. Specifically, "Upon the completion of a street, the department shall maintain the roadway of the street, including the curbs and gutters, catch basins, and inlets within the limits of the street or highway that form integral parts of the street or highway. The city or town shall maintain the sidewalks, grass plats, and the connecting drainage facilities."





CHAPTER 2 ADMINISTRATION



INDOT is committed to administering, providing oversight responsibilities, and ensuring compliance with all federal and state laws, rules, regulations, and guidance relating to ADA compliance. The Prequalification & Compliance Division ensures INDOT conducts business in an environment free of discrimination, and retaliation on the grounds of protected status, which includes its citizens and visitors with any physical or mental disability, in all state, federal, and federally assisted INDOT programs.

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2.0 ADA/504 PROGRAM ADMINISTRATION

2.1 INDOT's ADA COMPLIANCE PROGRAM is administered under the Central Office Prequalification & Compliance Division. The role is to monitor the administration of programs to ensure compliance with state and federal laws within INDOT's contracting practices, programs, and activities. This division is dedicated to upholding and enforcing all state and federal statutes prohibiting discrimination based on disability.

2.1.2 ADA Coordinator Program Manager. The role of the ADA Coordinator Program Manager is to manage programs and policies related to internal and external compliance with the ADA, Section 504 of the Rehabilitation Act of 1973, Americans with Disabilities Act of 1990, and all other federal and related laws, rules and regulations associated with regulations for persons with disabilities. The ADA Coordinator will act as the primary contact for all public outreach regarding discrimination against people with disabilities.

INDOT's Commissioner delegates authority to ensure the agency's compliance to the ADA Coordinator Program Manager. The ADA Coordinator has direct access to the Commissioner as needed. The ADA Coordinator reports to the Director of Compliance/Prequalification and Ethics who reports to the Chief Legal Counsel. The ADA Coordinator has direct access to INDOT's Chief Legal Counsel, as needed, (who reports directly to the Commissioner).

ADA Coordinator Program Manager:

Taffanee Keys

Civil Rights Counsel

Indiana Department of Transportation
100 N. Senate Avenue – Room N758-PQ
Indianapolis, IN 46204
<mailto:accessforall@indot.in.gov>

Prequalification & Compliance

Michele Steele, J.D., LLM

Director Compliance/Prequalification and Ethics Officer

100 N. Senate Avenue – Room N758-PQ
Indianapolis, IN 46204

Chief Legal Counsel

Kate Shelby

Deputy Commissioner and Chief Legal Counsel

Indiana Department of Transportation
Central Office
100 N. Senate Avenue – Room N758
Indianapolis, IN 46204

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INDOT's ADA Coordinator is responsible for:

- Developing and implementing the agency's ADA Transition Plan;
- Reviewing and recommended changes and additions to agency policies related to accessibility and nondiscrimination;
- Investigating complaints related to accessibility and allegations of discrimination on the basis of disability;
- Participating in the design, construction, maintenance, and inspection of INDOT facilities as the legal advisor and member of the ADA Technical Advisory Committee (see below) to ensure accessibility standards are met;
- Disseminating information to the public and obtaining public participation related to accessibility needs;
- Monitoring subrecipients for compliance with the ADA and other accessibility requirements; and
- Training INDOT staff, and external constituents inclusive of LPAs, MPOs, and construction consultant contractors.

2.2 ADA TECHNICAL ADVISORY COMMITTEE.

The Indiana Department of Transportation (INDOT) is divided into six districts, with district offices in LaPorte, Fort Wayne, Crawfordsville, Greenfield, Vincennes, and Seymour. Each district has a Technical Services Department, with support from INDOT Central Office staff, is responsible for managing INDOT assets and providing technical support to the district (including capital programs, maintenance, construction projects, and others). They also provide input for statewide asset management and capital programs. Each Technical Services Department has assigned asset owners who are responsible for the management of assets such as roadways/pavement, bridges, large culverts, small culverts, traffic/safety, mobility, and (in the case of Crawfordsville) drainage.

INDOT established an Interdisciplinary Team to administer and oversee INDOT's efforts toward compliance with the requirements outlined in Title II and Section 504 to ensure INDOT provides equal access for individuals with disabilities to its services, buildings, and rights of way. The Interdisciplinary Team is comprised of six people from the Technical Services Department, one from each district. These six people are INDOT's district ADA Liaisons. The ADA

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Coordinator and Program Manager actively solicits input and participation directly from team members. The ADA Liaisons are responsible for managing their district's ADA inventory and developing projects based on local needs as well as addressing grievances as required.

INDOT's ADA Technical Advisory Committee, the "TAC", is responsible for reviewing standards, policies, and issues related to the design of accessible facilities. This committee is comprised of staff from INDOT's legal, design, and standards divisions. The TAC receives inquiries that may be as simple as a question from a project manager, designer, or consultant. Inquiries may extend to the scope and nature of the improvement required at a particular location. The TAC also receives and responds to technical infeasibility requests when it is deemed likely that achieving full compliance with accessibility requirements at a given location is not possible and will not ever become possible (such as with the acquisition of additional right-of-way, etc.)

District ADA Liaisons:

Eric Fair

Special Project Engineer – Technical Services
Crawfordsville District
41 W. 300N
Crawfordsville, IN 47933

Robert Gill

Special Projects Engineer-Technical Services
Greenfield District
32 S Broadway St, Greenfield, IN 46140

Dirk Schmidt

Assistant Traffic Engineer
Fort Wayne District
5333 Hatfield Rd
Fort Wayne, IN 46808

Steven Vanes

Asset Management Engineer -Technical Services
Northwest District
315 E. Boyd Blvd
LaPorte, IN 46350

Linnea Wellings

Special Projects Engineer-Technical Services

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Seymour District
185 Agrico Lane
Seymour, IN 47274

Mickey Tosti
Special Projects Engineer -Technical Services
Vincennes District
3650 S. US Highway 41
Vincennes, IN 4791

2.3 COORDINATION WITH LOCAL PUBLIC AGENCIES & DISABILITY COMMUNITY

2.3.1 Local Public Agencies and Municipalities. In recognition of the importance of ensuring the proper implementation of accessibility elements and ongoing maintenance of ADA compliance, INDOT will continue to communicate and coordinate with local public agencies, counties, cities, towns, government agencies, and the Indianapolis Metropolitan Planning Organization to identify partnering opportunities to address needs within public rights-of-way that intersect with the state transportation system. Local agencies are independently responsible for developing and maintaining a transition plan. INDOT will assist local agencies, as needed, in understanding this obligation.

2.3.2 Disability Community. The TAC and the ADA Coordinator & Program Manager work closely with advocacy groups and disabled individuals to identify critical ADA needs and obtain feedback on INDOT's ADA program. INDOT maintains a list of ADA stakeholders which include numerous organizations representing the disability community. INDOT regularly consults with these organizations to coordinate community education and outreach and periodically provides updates on INDOT's ADA policies and procedures. Currently, INDOT is working with Bosma to ensure that we utilize the best methods concerning audible devices to communicate with the visually impaired as well as to comply with the requirements of PROWAG's new rule. To date, INDOT has set up two pilot programs during actual construction to determine the best Audible Information Device (AID) to use for visually impaired citizens within the state of Indiana. AID is used as a method to provide spoken information to visually impaired individuals, primarily in public spaces like sidewalks, crosswalks, and construction zones, by delivering audio

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messages regarding their surroundings, such as warnings about obstacles, directions to navigate, or notifications about temporary path changes, essentially acting as an auditory equivalent to visual signage for people who cannot see signage. The pilot testing of two different devices raised issues such as poor audio quality, loud ambient noise that can drown out the audible cues from the device, inaccurate information given at multiple intersections or complex pedestrian pathways, overlapping sounds if multiple AIDs are activated near each other, and limited range issues. INDOT will continue to work with Bosma with pilots to find the most viable device.

INDOT welcomes feedback from disability community organizations and strives to consider all comments to ensure the disability community is fairly and equitably represented through INDOT's ADA program. Additionally, INDOT personnel may invite disability community representatives to participate in walk audits during project scoping activities as appropriate to better understand ADA needs from the perspective of disabled users.

2.4 COMMUNICATIONS.

Section 35.160(a) of the ADA states, "A public entity shall take appropriate steps to ensure that communications with applicants, participants, and members of the public with disabilities, are as effective as communications with others." INDOT will continue to strive to provide equally effective communication with all persons of the public. These communications, by use of alternative communication formats, include large print, interpreters, teletypewriter (TTY), and audio. These options may be requested at no cost to the individual.

2.4.1 Notice to the Public. INDOT is committed to providing an ADA Notice to the Public (see Appendix A) regarding their rights and INDOT's responsibilities under Title II of ADA. INDOT will continually provide this notice and make it accessible by alternative formats whenever necessary.

2.4.2 Public Outreach. INDOT recognizes that engaging the public early and throughout the transportation decision-making process is key to building trust and credibility for the department and enhancing the awareness and understanding of INDOT's actions and business practices. The INDOT Public Involvement Plan provides guidance to INDOT staff, consultants, and

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subrecipients to offer opportunities for public input at key points in planning and project development. To ensure all individuals have equal opportunity to participate, INDOT requires that a standard ADA notice providing information on accessible alternative accommodations be included on all printed materials and, on its website and social media platforms. Additionally, all videos must be closed captioned, or a corresponding script will be provided.

INDOT's Planning and Public Involvement Plan is found at:

[INDOT Public Involvement Plan](#)

Comments or inquiries are always welcomed and can be shared through the public involvement survey on the Planning and Public Involvement website.

2.4.3 Public Meetings. All public meetings and hearings held by INDOT are required to be accessible to all members of the public. Public meetings, training sessions, or other programs or events, must be held in an accessible location and provide necessary accommodations to all people.

Further guidance regarding meeting accommodations for accessibility can be obtained:

<https://archive.ada.gov/business/accessiblemtg.htm>.

INDOT will provide qualified interpreters upon request and will provide documents in an accessible electronic format or other alternative formats such as large print. We ask that such accommodations, for meetings or events, be made with as much advance notice as possible to allow time for complete accessibility.

Requests for accommodations, at a meeting or event, should include the following information:

- The requestor's name, address, e-mail address, and telephone number (if any).
- A description of the meeting or event, including the location of the meeting or event.
- A brief description of what accommodation is needed (e.g., interpreters, alternative format of materials, etc.).

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Requests for accommodations regarding meetings or events can be made via telephone, teletypewriter, or at: INDOT4U. An email can also be sent to accessforall@indot.IN.gov.

2.5 ADA Grievance Procedure.

28 CFR 35.107(b) requires a public entity that employs more than 50 people to adopt and publish a grievance procedure providing for the prompt and equitable resolution of complaints alleging discrimination on the basis of disability access to any governmental services provided by the entity.

The resolution of any complaint or inquiry will require reviewing the nature of the noncompliance, establishing governing entities, and reviewing options for accommodating remedies to the location of interest. In determining possible solutions to the issue, INDOT will consider the health and safety of others, the feasibility of alterations, the cost of the possible solutions, availability of funding, ease of implementation, and how the solution compares in priority to other proposed ADA projects. The resolution of any one complaint or inquiry by INDOT does not constitute a precedent to which INDOT is bound or upon which others may rely.

2.5.1 Filing a Complaint.

A complainant may preliminarily submit his or her complaint by online form submission, mail, facsimile, or email. However, the complainant must submit a signed, original copy of the complaint by first-class U. S. Postal Mail to the ADA Coordinator & Program Manager to officially begin the complaint process. Any person with a disability may request to file his or her complaint using an alternative format. INDOT does not require a complainant to use the INDOT complaint form when submitting his or her complaint. A copy of INDOT's Complaint is attached as Appendix B, and in Spanish as Appendix C.

Direct all complaints of discrimination under the ADA to:

ADA Coordinator and Program Manager
Indiana Department of Transportation
100 N. Senate, Room N755 Indianapolis, IN 46204
Email address: AccessForAll@indot.in.gov

And/or

Federal Highway Administration Headquarters - Office of Civil Rights 1200 New Jersey Avenue, SE HCR-40, Room E81-101

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Washington, DC 20590
202-366-0693 or Fax: 202-366-1599
TTY: 202-366-5751

Additionally, complaints may be filed with the U.S. Department of Justice at:
Federal Coordination and Compliance Section - NWB Civil Rights Division
U.S. Department of Justice
950 Pennsylvania Avenue, N.W.
Washington, D.C. 20530

2.5.2 Elements of a Complete Complaint.

A complaint must be both written and signed to be complete. Verbal complaints must be reduced to writing and provided to the complainant for confirmation, review, and signature before processing. The complaint form is available online at: [External Complaint Form](#) and for download from the INDOT website at: [Nondiscrimination at INDOT](#).

A complaint must include the following information:

1. The full name and address of the complainant
2. The full name and address of the respondent, which may be the individual, agency, department, or program that allegedly discriminated against the complainant;
3. A description of the alleged discriminatory act(s) that violated Title VI (i.e., an act of intentional discrimination or one that has the effect of discriminating on the basis of race, color, or national origin; and
4. The date of occurrence.

The following items are not acceptable as a complete complaint unless accompanied by a signed cover letter that specifically requests INDOT take action concerning the allegations:

- Anonymous complaints
- Inquiries seeking advice or information
- Newspaper articles
- Courtesy copies of court pleadings
- Courtesy copies of complaints addressed to other agencies
- Courtesy copies of internal grievances
- Oral complaints

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The ADA Coordinator & Program Manager shall notify the complainant in writing if his or her complaint is incomplete and allot 15 calendar days for the Complainant to respond and provide the supplemental information needed to complete the complaint.

2.5.3 Processing Complaints: The ADA Coordinator & Program Manager processes all complaints

2.5.4 Logging Complaints: The ADA Coordinator & Program Manager will note the complaint in the log by sequential case number based on the year, month, and order in which INDOT received the complaint. For example, if INDOT received a complaint on March 4, 2024, the case number would be 2024-03-04.

Acknowledging receipt: The ADA Coordinator & Program Manager will acknowledge receipt of the complaint and inform the Complainant of the action taken or proposed action to be taken to process the complaint by letter. This letter of acknowledgment shall include a restatement of the complaint, a brief statement of INDOT's jurisdiction over the subrecipient, and contact information for the investigator assigned to conduct the investigation.

2.5.2 FHWA Notice. INDOT shall forward the complaint to the FHWA within 10 business days of receipt of the complaint for assignment.

2.5.3 Respondent Notice. If assigned by HCR, INDOT forwards a notice via certified mail to the Respondent informing them of the allegations, requesting a position statement, and providing the name and telephone number of the Program staff person assigned to investigate the complaint.

2.5.4 Complainant Notice. INDOT informs the Complainant that he or she has a right: (1) to have a witness or representative present during any interviews and (2) to submit any documentation he or she perceives as relevant to proving the allegations contained in the complaint.

2.5.5 Opportunity to Respond. INDOT will provide the Respondent with a reasonable opportunity to respond to all aspects of the Complainant's allegations.

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2.5.6 Witnesses. INDOT will determine if witnesses will be contacted and interviewed as part of its investigation.

2.5.7 Additional Information. INDOT may reach out to gather additional information from the parties. Investigative Report: INDOT will draft a confidential investigative report (IR) and forward a copy of the same to the FHWA within 180 calendar days following the receipt of the complaint by INDOT.

The report shall not be disclosed to the Complainant or Respondent. The report typically includes the following:

- A summary of the written complaint
- A brief description of the standard of review/methodology used to investigate the complaint
- Summarized statements taken from witnesses interviewed by INDOT
- Findings of fact and an analysis of the evidence gathered. The analysis should address each allegation in the complaint and the Respondent's position
- A determination, based on the preponderance of evidence presented, of whether the complaint is substantiated or unsubstantiated; and •
- Proposed corrective action for substantiated cases

2.5.8 Letter of Findings. INDOT is responsible for drafting a Letter of Findings (LOF) and mailing the LOF to the FHWA, Respondent, and Complainant within 180 calendar days of the date the complaint was received by INDOT. The LOF may include the following:

- A summary of the written complaint
- A brief description of the standard of review/methodology used to investigate the complaint
- Findings of fact and an analysis of the evidence gathered. The analysis should address each allegation in the complaint and the Respondent's position
- A determination, based on the preponderance of evidence presented, of whether the complaint is substantiated or unsubstantiated
- Proposed corrective action for substantiated cases

In accordance with DOT Order 1000.12, INDOT shall keep all Complainants' identities confidential except to the extent necessary for carrying out an

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investigation. If an investigator determines that it is necessary to disclose the Complainant's identity to the Respondent or a third party, the investigator must first obtain the Complainant's written permission.

INDOT may refer complaints to the appropriate agency or entity without obtaining permission as referral may be required. INDOT will notify the Complainant of the referral at the time the referral is made. Otherwise, INDOT shall obtain a Complainant's written consent before providing a copy of the complaint to the Respondent or a third party. The ADA Coordinator & Program Manager shall maintain all records of an investigation in a confidential area for three (3) years after the completion of the investigation.

2.6 TRACKING NON-GRIEVANCE ADA COMMUNICATION AND INFORMAL INQUIRIES. INDOT offers the opportunity for the public to submit inquiries regarding any ADA-related issue via a comment form on the INDOT4U website. On this site, submit complaints that capture ADA inquiries that do not constitute a formal grievance complaint but involve ADA external policies, decisions, or physical assets. INDOT considers public feedback and inquiries and will respond to each comment.

2.7 SUBRECIPIENT COMPLIANCE MONITORING

49 Code of Federal Regulations (CFR) Parts 27 and 28 require that all recipients of federal-aid highway funds comply with Section 504 of the Rehabilitation Act of 1973 (Section 504). As a recipient, the State of Indiana is responsible for ensuring that its subrecipients comply with Section 504. It states that "no otherwise qualified individual with a disability in the United States shall, solely by reason of his or her disability, be excluded from participation in, be denied the benefits of or be subjected to discrimination under any program or activity receiving federal financial assistance." 28 CFR 35 implements the Americans with Disabilities Act of 1990 (ADA) and Section 504, which extended the prohibition of discrimination on the basis of disability to all local agencies, including those that do not receive Federal financial assistance. Some ADA administrative requirements differ for agencies with 50 or more full-time and part-time employees versus agencies with fewer than 50 full-time and part-time employees. This assessment will address the requirements for both and is a first step for INDOT to determine subrecipient compliance; help

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subrecipients understand their ADA/504 responsibilities; and assist INDOT in planning future training and technical assistance.

INDOT monitors subrecipients of federal funds and state funds/grant recipients for compliance with accessibility requirements. The requirements vary depending on the recipient's organizational type and structure and the size of the organization. For ALL recipients / subrecipients of state and federal funds, INDOT includes nondiscrimination and accessibility compliance language in the contract with these agencies and organizations. The first level of compliance is always certification. If the recipient is a contractor or consultant, certification begins during the prequalification process to be able to do business with INDOT. If the recipient is a local public agency (LPA) as many cities and towns are, or a local metropolitan planning organization (MPO), Rural Planning Organization (RPO), or a transit, airport, or other transportation authority, certification is part of the funding application.

Once any recipient / subrecipient of funds is awarded a grant or a project, the contract language requires the organization to comply with the accessibility requirements and gives INDOT the ability to monitor that recipient's compliance with these requirements. INDOT's monitoring processes are described in detail in its Title VI Implementation Plan and specific actions and activities are described in our annual Goals & Accomplishments Report. Both of these documents can be found at: [Title VI Implementation Plan](#).

2.7.1 Written Assurance Guaranteeing Nondiscrimination As a condition of federal aid under Section 504 of the Rehabilitation Act of 1973 (49 CFR 27.9), INDOT must guarantee non-discrimination and ensure new projects will be ADA-compliant. INDOT includes a non-discrimination agreement on all federally funded projects. The document requires INDOT to comply with federal statutes, policies, and procedures. The document states no person may be excluded from federally funded programs on the grounds of race, color, national origin, sex, age, and handicap/disability. ADA compliance must also be assured on all federally funded projects conducted by sub-recipients of federal funds.

2.7.2 Indiana Communities and Transportation Authorities. INDOT monitors Indiana local public agencies (LPAs), Metropolitan Planning Organizations

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(MPOs), Rural Planning Organizations (RPOs) and Transit authorities for compliance with the ADA. For prequalification, pre-award subrecipient review, Indiana Communities, (LPAs, MPOs, RPOs) are required to self-certify the following. If self-certification cannot be achieved the LPA must submit a voluntary compliance action plan (VCAP Letter) stating that they will cure all deficiencies within 120 days of the date of their submission.

Self-Certification Questions are as follows:

- Do you have an ADA Coordinator?
- Does the subrecipient have a disability nondiscrimination policy that includes the name, title, office address and office telephone number of the ADA/Section 504 Coordinator?
- Does the subrecipient have a grievance policy and procedure that prohibits discrimination based on disability under any of the subrecipient's programs and activities?
- Does the subrecipient have proof of public dissemination of its nondiscrimination policy statement?
- Are the subrecipient's website and telephone services accessible to individuals with sight and hearing impairments?
- Has the subrecipient completed a self-evaluation of current services, policies, and practices to determine necessary modifications to achieve program accessibility?
- If the subrecipient has completed an ADA self-evaluation when was it completed and has it been updated?
- Has the subrecipient developed and implemented an ADA Accessibility Transition Plan or other Accessibility Plan that outlines which structural modifications must be made to those programs and services that are not accessible?
- How often does the subrecipient update its ADA Accessibility Transition Plan or other Accessibility Plan?

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- If applicable, please provide the date of the last update to the subrecipient ADA Accessibility Transition Plan or other Accessibility Plan:
- Please provide a detailed description of how the subrecipient made its self-evaluation and/or ADA Accessibility Transition Plan or other Accessibility Plan available for public inspection or comment.
- Please provide a brief description of how the subrecipient monitors its compliance with the ADA and Section 504
- Does the subrecipient have an ADA nondiscrimination policy that prohibits discrimination based on disability that it makes available to contractors, consultants, beneficiaries, etc.?
- Does the subrecipient have proof of public dissemination of its ADA nondiscrimination policy that it makes available to contractors, consultants, beneficiaries, etc.?
- Does the subrecipient's ADA nondiscrimination policy and its corresponding processes include the subrecipient maintaining a complaint log showing all ADA complaints received for the last three years (i.e. numbers, issues involved, how it was resolved)?
- Has any subrecipient staff received any ADA training (formal or informal) within the past year?
- Does the subrecipient ensure it holds its public meetings, hearings, open houses, etc. in accessible locations?
- Do the subrecipient's public meeting announcements provide notification that auxiliary aids are available upon request?
- Does your entity provide auxiliary aids (sign language, interpreters, readers, Braille, and large print text) upon request to participants with disabilities?

2.7.3 Contractors / Consultants and Other Contractual Accessibility Requirements. During the Prequalification process, contractors /consultants are required to self-certify they take measures to ensure discrimination does

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not occur. Consultants and contractors are subject to random audits to ensure compliance. Audit questions include but are not limited to:

- Does your entity have an ADA/504 Coordinator?
- Has your entity drafted and disseminated to participants, applicants, employees, unions, and contractors / consultants an ADA non-discrimination policy statement

Training

INDOT personnel may participate in walk audits during project scoping activities to understand ADA needs from the perspective of disabled users.



- Has your entity adopted a written grievance procedure?
- Has any subrecipient staff received any ADA training (formal or informal) within the past year?

ADA resources for contractors /consultants are found at [ADA Resources Contractors / Consultants](#).

2.8 TRAINING

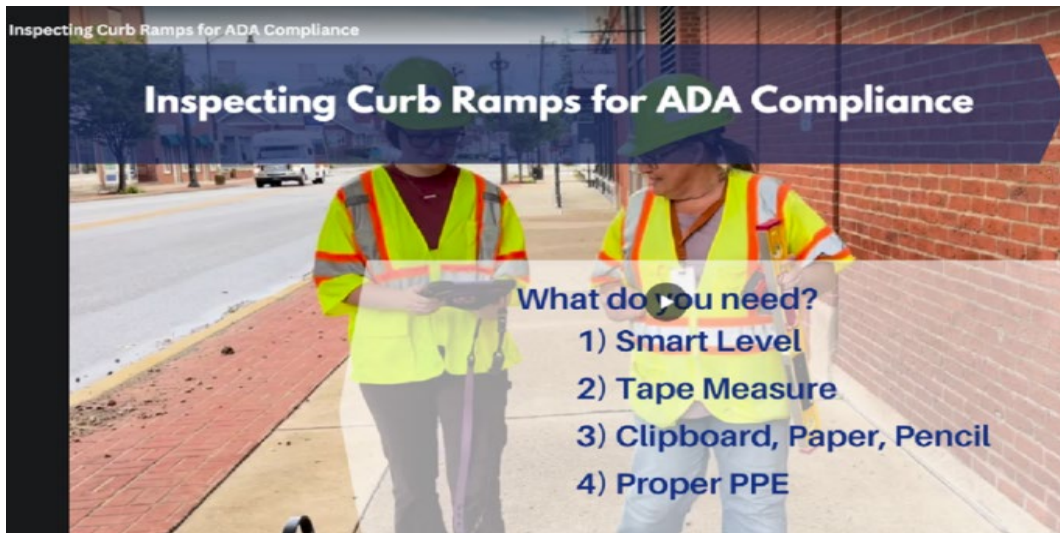
2.8.1 INDOT Personnel INDOT facilitates varying levels of ADA training to employees depending on personnel involvement with ADA. INDOT District and Central Office personnel receive training addressing inventory, design, review, construction, and maintenance of ADA features in support of the ADA policy statement. This training reinforces the importance of ADA regulations and educates personnel on applicable ADA requirements. INDOT personnel may also participate in walk audits during project scoping activities to understand ADA needs from the perspective of disabled users.

2.8.2 Subrecipient Training. INDOT conducts annual live in-person and virtual via Microsoft Teams training to its subrecipients. This platform allows a question-and-answer session for subrecipients to aid in maximum compliance. Additionally, ADA training videos and PowerPoints are on INDOT's ADA Nondiscrimination website. Further, the ADA Coordinator Program Manager offers an external online training module to satisfy the training needs of Subrecipients, Consultants, and Contractors in an on-demand manner. Additionally, the ADA Coordinator

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provides individual (one-on-one) technical training to Subrecipients on an “as-needed basis” and specialized training upon request. INDOT’s Subrecipient Self-Evaluation Training video can be found at: [Subrecipient Self-Evaluation Training](#)





CHAPTER 3

ADA METHODS FOR COMPLIANCE

In support of INDOT's mission and ADA policy, and alignment with other plans and documents relating to ADA, the following sections outline methods INDOT uses to make its programs, rights-of-way, and building facilities accessible to individuals with disabilities in fulfillment of 28 CFR 35.150(d)(3)(ii)

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3.0 LAWS, REGULATIONS, STANDARDS, MANUALS, MEMOS, GUIDELINES, AND PLANS

INDOT must follow all federal laws, regulations, standards, and guidance relating to accessibility. INDOT is committed to upholding the requirements of the ADA and Section 504, including provisions of Title II of the ADA that apply to policies, programs, and services, and to following the ADA Accessibility Standards for facilities. INDOT has also adopted the proposed Public Rights-of-Way Accessibility Guidelines (PROWAG) issued by the U.S. Access Board in 2011 and has adopted the final rule published October 7th, 2023, as discussed in section 3.1.2 herein. Further, INDOT currently adheres to Web Content Accessibility Guidelines WCAG for all new or revised web pages as discussed in section 3.4 herein below.

The Rehabilitation Act of 1973, Section 504 states that “no otherwise qualified individual with a disability shall, solely by reason of his or her disability, be excluded from the participation in, be denied the benefits of, or be subjected to discrimination under any program or activity receiving federal financial assistance.” Other federal laws that provide very similar nondiscrimination language based on disability that apply to INDOT include the Americans with Disabilities Act, and the Architectural Barriers Act. The regulations for the ADA, Title II, and Section 504 include administrative requirements for all government entities employing more than fifty people. These administrative requirements are:

- Completion of a self-evaluation to evaluate programs, services, and activities.
- Development of an ADA complaint procedure.
- Designation of a person who is responsible for overseeing Title II compliance.
- Development of a transition plan is required for any public entity with more than 50 employees.

Administrative Requirements, Locations of Information:

- The ADA can be found at: www.ada.gov/pubs/ada.htm

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- Department of Justice's ADA regulations for Title II, 28 CFR Part 35, can be found at: <http://www.ada.gov/regs2010/ADAREgs2010.htm>
- Section 504 can be found at:
<https://www.hud.gov/programdescription/sec504>
- DOT's Section 504 regulations, 49 CFR Part 27 can be found at:
<https://www.govinfo.gov/content/pkg/CFR-2013-title49-vol1/xml/CFR-2013-title49vol1-part27.xml> and
<https://www.govinfo.gov/content/pkg/CFR-2010-title49-vol1/pdf/CFR-2010-title49-vol1-part27.pdf>

INDOT's ADA/504 Program is governed by other laws, regulations, standards, and guidance, including but not limited to:

- The Architectural Barriers Act of 1968 (ABA), requires that facilities designed, built, or altered with certain federal funds be accessible according to federal standards. The Access Board's updated ADA-ABA Accessibility Guidelines of 2004 have been incorporated into the ABA Accessibility Standards and adopted by the General Services Administration, whose regulation applies to DOT-funded facilities covered by the ABA.
- 1991 ADA Standards for Accessible Design
<https://www.ada.gov/1991standards/adastd94archive.pdf>
- 2010 ADA Standards for Accessible Design
<https://www.ada.gov/regs2010/2010ADASTandards/2010ADASTandards.pdf>
- Final Rule Public Rights-of-Way Accessibility Guidelines (PROWAG) 36 CFR Part 1190,
<https://www.federalregister.gov/documents/2023/08/08/2023-16149/accessibility-guidelines-for-pedestrian-facilities-in-the-public-right-of-way>
- ADA Standards for Transportation Facilities www.access-board.gov/ada-aba/ada-standardsdot.cfm

INDOT has developed a range of statewide plans, operations manuals, design standards and criteria, and communications guidelines which INDOT uses to conduct self-evaluations of its activities, programs, policies, procedures, and services to complete INDOT's ADA Transition Plan. The following documents describe the methods that are used to make INDOT's facilities,

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programs, activities, and services accessible as required by Title II regulations, PROWAG, and the ADA Standards for Accessible Design in 2010. Additionally, the documents in Section 3.5 below describe the planning steps necessary to achieve compliance.

3.1 Accessibility in the Public Rights-of-Way Design Manuals & Memos

Indiana Department of Transportation—2013 Design Manual, Chapter 51, Special Design Elements ([Rev. Oct. 2020](#)). *NOTE: This chapter is being rewritten and its content will be included in Chapter 307.* The Department, along with each local public agency, under ADA Title II, is required to provide ADA-compliant, otherwise known as accessible, facilities within the public right of way. Buildings within the public right of way, sidewalks, curb ramps, transit stops, on-street parking, parking lots, overpasses and underpasses are just a few examples of programs covered by Title II. The U.S. Access Board proposed guidelines specifically for pedestrian facilities in the public right of way - The Public Rights-of-Way Accessibility Guidelines (PROWAG). These guidelines are recommended as best practice by the Federal Highway Administration and are currently being evaluated as part of the federal rulemaking process. Once adopted as a regulation, with or without modifications, the guidelines will be mandatory.

The Department's accessibility criteria meet the requirements of the ADA and seek to ensure that persons with disabilities may access the public right of way without discrimination. Unless otherwise noted, the Department's accessibility criteria are based on the PROWAG, dated July 26, 2011. The applicable sections of the PROWAG are noted in brackets next to each section heading below. If local public agencies or local codes require standards that exceed the PROWAG, the stricter criteria should be used.

Method 3.1.2 PROWAG Public Right-of-Way Accessibility Guidelines

The Access Board published new guidelines on August 8, 2023, under the Americans with Disabilities Act (ADA) and the Architectural Barriers Act (ABA) that address access to sidewalks and streets, crosswalks, curb ramps, pedestrian signals, on-street parking, and other components of the public right-of-way. These guidelines also review shared-use paths which are designed

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primarily for use by bicyclists and pedestrians for transportation and recreation purposes. As of October 7th, 2023, the Public Right-of-Way Accessibility Guidelines (PROWAG) were published & in full effect. [publishes final rule in the Federal Register](#). INDOT continues to use PROWAG as its design standard and is currently updating to ensure full compliance.

Method 3.1.3 INDOT Active Design Memos

Design Memos are published between the first Monday and the subsequent Thursday of each month. All revisions to the *Design Manual* described in these memos have been incorporated into the current [Indiana Design Manual](#) (IDM).

24-05	04/11/24	PROWAG Elements Phased Implementation
-----------------------	----------	---------------------------------------

Design Memo 23-16 was issued in response to the U.S. Access Board issuing its final rule on accessibility guidelines for pedestrian facilities in the public right-of-way (known as PROWAG) in the Federal Register on August 8, 2023. The memo proactively established a contract letting compliance date of July 1, 2024, and highlighted several notable changes. INDOT remains committed to providing accessible pedestrian infrastructure.

For select PROWAG elements, INDOT will now use a phased implementation. This approach is to allow sufficient time to understand the full scope of how to achieve compliance, establish guidance, standards, and training, and to address specific challenges that have been identified by INDOT and industry. Compliance for the select PROWAG elements noted below will be effective with lettings on or after July 1, 2028 (Fiscal Year 2029)

24-05	04/11/24	PROWAG Elements Phased Implementation
-----------------------	----------	---------------------------------------

Design Memo 23-16 was issued in response to the U.S. Access Board issuing its final rule on accessibility guidelines for pedestrian facilities in the public right-of-way (known as PROWAG) in the Federal Register on August 8, 2023.

The memo proactively established a contract letting compliance date of July 1, 2024, and highlighted several notable changes. INDOT remains committed to providing accessible pedestrian infrastructure.

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achieve compliance, establish guidance, standards, and training, and to address specific challenges that have been identified by INDOT and the industry. Compliance for the select PROWAG elements noted below will be effective with lettings on or after July 1, 2028 (Fiscal Year 2029).

23-16	10/16/23	Public Rights-of-Way Accessibility Guidelines (PROWAG)
-----------------------	----------	--

The U.S. Access Board issued its final rule on accessibility guidelines for pedestrian facilities in the public right-of-way (known as PROWAG) in the Federal Register on August 8, 2023. This is the first step to the PROWAG becoming law. The second step, adoption by the US Department of Justice and Department of Transportation with or without modifications, is expected in early 2024. The U.S. Access Board has published notable changes <https://www.access-board.gov/news/2023/09/01/notable-changes-in-public-right-of-way-accessibility-guidelines-final-rule/> and released videos on its YouTube channel that offer a chapter-by-chapter review of the guidelines.

This memo is to provide a high-level summary of changes that may result in design revisions for compliance. It is not a comprehensive list.

Method 3.1.4 Design Standard Drawings

24-07	05/14/24	2024 INDOT Standard Drawings
-----------------------	----------	------------------------------

The 2024 INDOT Standard Drawings are now available from the Standard Drawings webpage at [INDOT Standard Drawings](#). The webpage has been reorganized to contain the current year and prior year's Standard Drawings. All other years have been consolidated to a new page.

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3.1.5 Standard Drawing E 604-SWCR Curb Ramps

Curb Ramps Standard Drawings are attached as Appendix C. Note revisions.

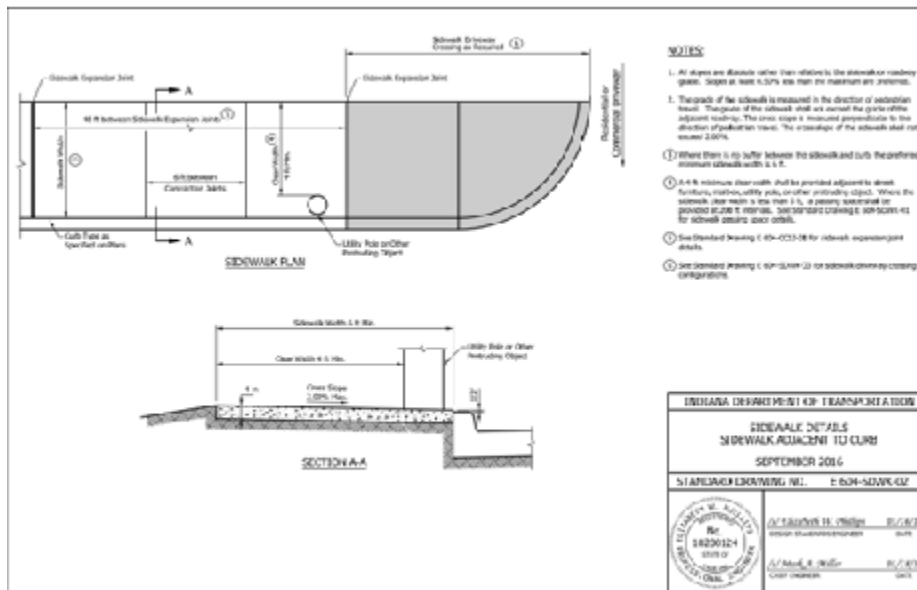
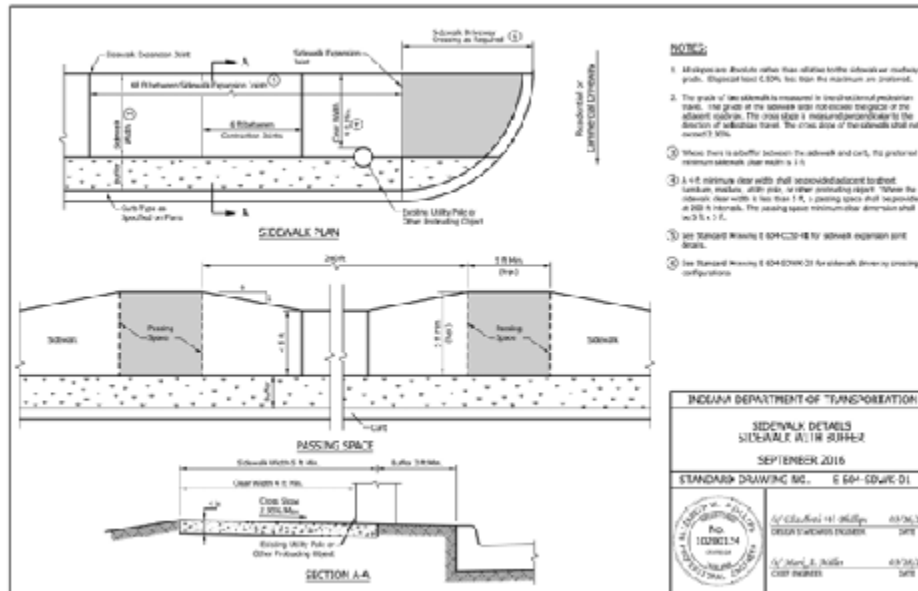
STANDARD DRAWING E 604-SWCR

SHEET	EFF. DATE	BY	REVISION
11	09/01/23	SB	REVISED -Changed 2.00% to 5.00% in Note 3.
13	09/01/23	SB	REVISED -Replaced Note 4, renumbered Notes 5, 6 and 7 to 6, 7 and 8 respectively and added Note 5. Added heading to Median Cut-Through detail and Skewed Railroad Crossing Detail. Approved by Standards Committee February 2023.
03	09/01/19	EWP	EDITORIAL -Removed S in notes as there is only one note.
13	09/01/19	EWP	EDITORIAL -Added line between break points in alternate detectable warning surface placement figure. Removed (typ.) where there was (typ.) and (max.) in the same callout. Added the word length after minimum on the first line and added the last sentence to note 1.
01	09/01/18	EWP	REVISED -Revised note 5, added note 12. Approved by Standards Committee March 2018.
02	09/01/18	EWP	REVISED -Revised note 1. Approved by Standards Committee March 2018.
04, 06, 08, 10, 11, 14	09/01/18	EWP	REVISED -Added concrete depth dimensions to section views. Approved by Standards Committee March 2018.
09	09/01/18	EWP	REVISED -Revised callouts. Approved by Standards Committee March 2018.
12, 13	09/01/18	EWP	REVISED -Revised DWS setback. Approved by Standards Committee March 2018.
01-14	09/01/16	EWP	REVISED -Format update. Drawings changed to reflect PROWAG. Curb letter identification removed. Approved by Standards Committee January 2016.
11	09/01/16	EWP	EDITORIAL -Walkable surface callout updated with note #6.

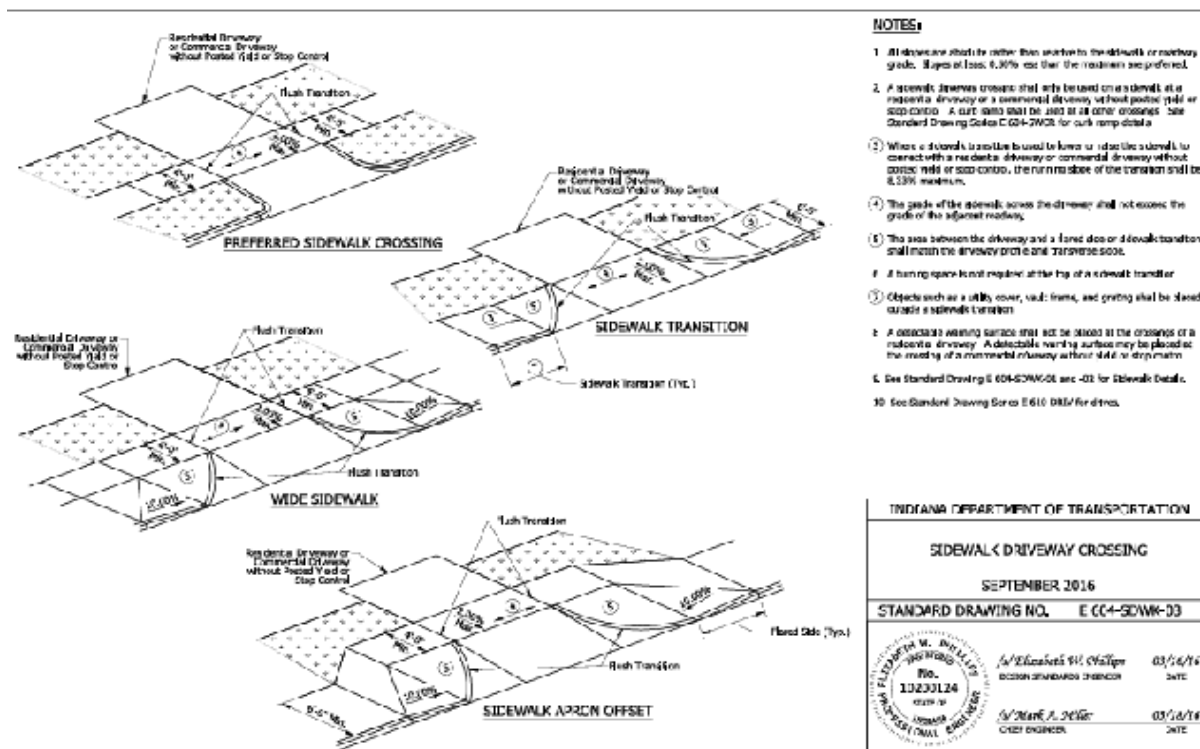
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Method 3.1.6 Standard Drawing E 604-SDWK Sidewalk Details



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3.2 2010 ADA Standards for Accessible Design

INDOT evaluates its public-facing facilities, Rest Areas, and Welcome Centers, located on the interstate freeway system to identify physical obstacles that limit accessibility. INDOT uses the Department of Justice's September 15, 2010, published and revised regulations for Titles II and III of the Americans with Disabilities Act of 1990. These regulations adopted revised, enforceable accessibility standards called the 2010 ADA Standards for Accessible Design 2010 Standards or Standards. The 2010 Standards set minimum requirements – both scoping and technical – for newly designed and constructed or altered State and local government facilities, public accommodations, and commercial facilities to be readily accessible to and usable by individuals with disabilities. The 2010 ADA Standards for Accessible Design can be found at: <https://www.ada.gov/law-and-regs/design-standards/2010-stds/>

3.3 Indiana Manual on Uniform Traffic Control Devices for Streets and Highways (IMUTCD)

According to Indiana Code 9-21-2 -1, INDOT has adopted the IMUTCD by order of the Indiana Department of Transportation Commissioner and by letter of

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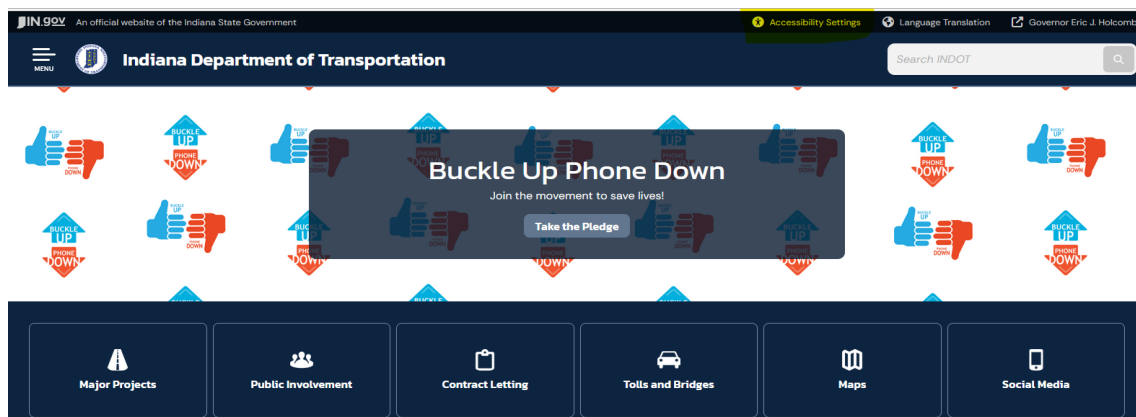
concurrence from the Federal Highway Administration. (FHWA). The IMUTCD establishes minimum standards and provides guidance for the installation and maintenance of traffic control devices, such as signs, signals, and pavement markings.

3.4 Accessibility of Web Information Services of State and Local Government Entities, 28 CFR Part 35

INDOT's website is a key platform for providing information to the public. To ensure accessibility, information provided on the website must be compatible with appropriate software to accommodate specific disabilities. INDOT strives to make its websites accessible to people with disabilities. INDOT currently follows WCAG guidelines which are based on four guiding principles: perceivable, operable, understandable, and robust.

WCAG Requirement: Effective April 26, 2027, a public entity with a total population of less than 50,000 or any public entity that is a special district government shall begin complying with the Web Content Accessibility Guidelines (WCAG) Version 2.1. WCAG 2.1 is the technical standard for state and local governments' web content and mobile apps.

- This rule sets a specific technical standard that state and local governments must follow to meet their existing obligations under Title II of the ADA for web and mobile app accessibility.
- WCAG, the Web Content Accessibility Guidelines, is a set of guidelines that say what is needed for web accessibility, such as requirements for captions for videos. WCAG is developed by the World Wide Web Consortium.





THE SELF-EVALUTATION

Conducted a self-evaluation of its policies, practices, services, facilities and public right-of-way. Our assessment enables INDOT to ensure that INDOT is appropriately addressing the needs of individuals with disabilities and to remove barriers to accessibility.





4.0 SELF-EVALUATION, POLICIES, SERVICES, FACILITIES, and PUBLIC RIGHTS-OF-WAY

A self-evaluation is a public entity's assessment of everything, including its programs, services, activities; facilities; and current policies, practices, and procedures. The self-evaluation identifies and corrects barriers to access that are inconsistent with its Title II requirements. The self-evaluation focus is on program access to ensure that programs, such as websites, pedestrian facilities, public rights-of-way, and or other various facilities available to the public are accessible when viewed in their entirety.

During the ADA self-evaluation process, INDOT assessed policies and procedures, (discussed in Chapter 3 above) public rights-of-way, facilities, and select websites. Each of these assets is governed by different federal standards and guidelines. The method of data collection is unique to each asset type, and the data is evaluated against different ADA regulations and standards, which are discussed in Chapter 3 of this report. This section of the report describes the approach taken in collecting and assessing each unique type of data.

4.1 Self-Evaluation, Website

ADA website accessibility is measured by how usable the site is for people with disabilities. The ADA requires that public agencies build websites in a way that is independently usable for individuals with disabilities along with any assistive technology they might use. This provides equal access to the places, goods, and services of public accommodations.

To determine the accessibility of INDOT's websites, testing of four public-facing websites was conducted. The four public-facing website pages that were identified for review, and carefully evaluated were based on the degrees of use and traffic to sites. INDOT used the most practical approach to conduct accessibility testing which is to limit testing to a fixed set of web pages and evaluate the findings across the entire site by focusing on the common errors found. Four public-facing and high-traffic websites were selected to gain clearer insight into the true accessibility of web content; INDOT's Home page,

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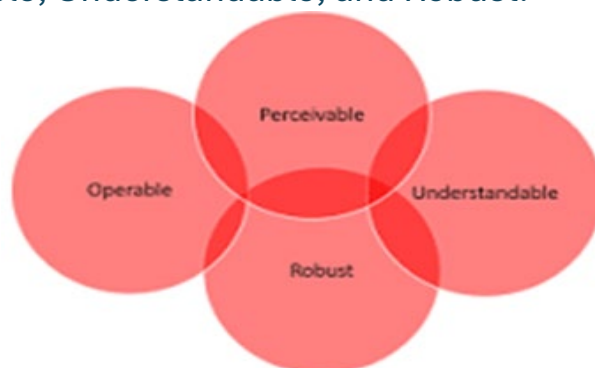


INDOT's Restrictions and Closing Page, INDOT's Contract Letting Page, and INDOT's Travel Information page.

Methodology:

INDOT's Web Content Accessibility Guidelines (WCAG) were used to evaluate INDOT's websites. The Web Content Accessibility Guidelines (WCAG) are technical standards that help make the digital world accessible to people with disabilities. To comply with the Americans with Disabilities Act (ADA) INDOT follows WCAG standards to remove barriers to access for people with disabilities in digital environments.

There are four main guiding principles of accessibility upon which the WCAG has been built. These four principles are known by the acronym POUR for Perceivable, Operable, Understandable, and Robust.



Perceivable – Information and user interface components must be presentable to users in ways they can perceive. This means that users must be able to understand the information being presented; it can't be invisible to any of their senses.

Operable – User interface components and navigation must be operable. This means that users must be able to use the interface, and it cannot require interaction that a user cannot perform.

Understandable – Information and the operation of user interfaces must be understandable. This means that users must be able to understand the information as well as the operation of the user interface (the content or operation cannot be beyond their understanding)

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Robust – Content must be robust enough that it can be interpreted reliably by a wide variety of user agents, including assistive technologies. This means as technologies and user agents advance and evolve, content should remain accessible.¹

INDOT works in tandem with the State of Indiana Office of Technology (IOT) as well as an independent company, Tyler Technologies, in its goal to achieve website accessibility for citizens with disabilities. INDOT Communications directs what falls within the categories of perceivable, operable and understandable. The category of robust falls within IOT and Tyler Technologies unless INDOT manually edits the HTML of the page (very rare).

INDOT's self-assessment revealed the following information regarding its website accessibility.

1. INDOT's Home page.

Perceivable:

- No pictures on the page are vital to the information and missing a text alternative.
- Contrast ratio: All pertinent information, including links and calls to action, are displayed as an easily decipherable white font on blue background.
- Headings are properly nested intentionally and automatically by the Content Management System (CMS)
- Font size is appropriate and relative to users' default size and can be enlarged 200% without losing content
- Button prompts and labels for navigation elements are applicable to the content

Operable:

- Buttons have text alternatives
- Links have text alternatives
- Focus indicators are implemented, clear, and operating
- No videos or audio included to pause or stop

Understandable:

¹ Object: <https://ialabs.ie/understanding-the-pour-principles-of-accessibility/>, content: <https://www.w3.org/TR/UNDERSTANDING-WCAG20/intro.html>

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- Language used is understandable and easily readable
- Consistent navigation is used on this page and is used throughout the site on redesigned/ refreshed pages
- Predictable interactions – clicking buttons leads users to new pages or new content

Robust:

- Standard HTML is used along with cascading style sheets (CSS) set by IOT
- Screen reader accessibility options are compatible with screen readers such as JAWS², NVDA,³ VoiceOver,⁴ and TalkBack
 - Prompted automatically to screen-readers
- Content is easily accessible and fully functions on mobile devices

After running through SiteImprove, the accessibility areas to still improve on this page are:

- Text not included in Accessible Rich Internet Applications (ARIA landmark)
- Interactive element does not meet enhanced size (portion of site controlled by IOT)

2. INDOT's Restrictions/Closings page

Perceivable:

- Contrast ratio: All pertinent information is displayed in black text on a white background
- Headings are properly nested
- Font size is appropriate and relative to users' default size and can be enlarged 200% without losing content

Operable:

- Focus indicators are implemented, clear, and operating
- No videos or audio included to pause or stop

Understandable:

- Language used is understandable and easily readable

² JAWS, Job Access With Speech, is the world's most popular screen reader, developed for computer users whose vision loss prevents them from seeing screen content.

³ NonVisual Desktop Access is a free and open-source, portable screen reader for Microsoft Windows

⁴ VoiceOver is a built-in screen reader that describes aloud what appears on your computer screen: it speaks the text that's in documents and windows.

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- Consistent headers and layout used on this page as non-refreshed pages

Robust:

- Standard HTML is used along with CSS set by IOT
- Screen reader accessibility options are compatible with screen readers such as JAWS, NVDA, VoiceOver, and TalkBack⁵
 - Prompted automatically to screen-readers
- Content is easily accessible and fully functions on mobile devices

After running through SiteImprove,⁶ the accessibility areas to still improve on this page are:

- Text not included in an ARIA landmark
- Interactive element does not meet enhanced size (portion of site controlled by IOT)
- May be unclear which page element has focus from keyboard with screen-reader
- Link sizes should be enhanced and/or made into buttons

3. INDOT's **Contracts: Home** page

Perceivable:

- Contrast ratio: All pertinent information is displayed in black text on a white background
- Headings are properly nested
- Font size is appropriate and relative to users' default size
- Content can be enlarged 200% without losing content and content containers adapt to screen size
- Color contrast meets requirements*

Operable:

- Focus indicators are implemented, clear, and operating
- No videos or audio included to pause or stop
- Button labels align with the content that it navigates to

Understandable:

- Language used is understandable and easily readable

⁵ TalkBack is the Google™ screen reader that gives spoken feedback for what you tap, select, and activate on the screen,

⁶ Siteimprove runs periodic scans of your website and evaluates content for potential accessibility-related issues. These issues are sorted into categories such as Quality Assurance, Accessibility, and Search Engine Optimization.

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- Consistent headers, content containers, and layout used on this page as refreshed pages

Robust:

- Standard HTML is used along with CSS set by IOT
- Screen reader accessibility options are compatible with screen readers such as JAWS, NVDA, VoiceOver, and TalkBack
 - Prompted automatically to screen-readers
- Content is easily accessible and fully functions on mobile devices

After running through SiteImprove, the accessibility areas to still improve on this page are:

- Text not included in an ARIA landmark
- Interactive element does not meet enhanced size (portion of site controlled by IOT)
- May be unclear which page element has focus from the keyboard with screen-reader
- Link sizes should be enhanced and/or made into buttons
- *Colors on tabs should be swapped (color specifically requested in mid-2024)
- “Skip to main content” link could be added

4. INDOT’s **Travel Information** page

Perceivable:

- Contrast ratio: All pertinent information is displayed in black text on a white background
- Headings are properly nested
- Font size is appropriate and relative to users’ default size
- Content can be enlarged by 200% without losing content and content containers adapt to screen size
- Color contrast meets requirements*

Operable:

- Focus indicators are implemented, clear, and operating
- No videos or audio included to pause or stop
- Photos have alternative text

Understandable:

- Language used is understandable and easily readable

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Robust:

- Standard HTML is used along with CSS set by IOT
- Screen reader accessibility options are compatible with screen readers such as JAWS, NVDA, VoiceOver, and TalkBack
 - Prompted automatically to screen-readers
- Content is easily accessible and fully functions on mobile devices

After running through SiteImprove, the accessibility areas to still improve on this page are:

- Text not included in an ARIA landmark
- Interactive element does not meet enhanced size (portion of site controlled by IOT)
- May be unclear which page element has focus from the keyboard with the screen reader
- Hyperlink sizes should be enhanced and/or made into buttons

INDOT's self-evaluation revealed the elements on the top and bottom of the webpages, (ex: INDOT logo, State of Indiana logo) were not in compliance. These elements are controlled by IOT. IOT is in the process of updating these items to ensure INDOT is meeting accessibility requirements.

Accessibility is one of INDOT's E-Communications Division's biggest focuses going into 2025. INDOT continues to receive more information from IOT and monthly In.gov meetings. The Division will continue to conduct individual meetings with the IT Director of In.gov and the Director of Creative Services from Tyler Technologies to develop a plan to best go about INDOT's pre-existing content, and ensure all new content meets the requirements.

4.2 Self-Evaluation, Rest Areas & Welcome Centers

FFY 2022 INDOT began executing a 10-year plan to improve its interstate rest areas and welcome centers statewide to modernize facilities, construct new buildings, improve parking, and convert some to semitrailer parking facilities. INDOT plan calls for an investment of more than \$600 million in improvements to 21 rest areas, welcome centers, and truck parking facilities across Indiana by the end of FY 2034.

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INDOT's Rest Area & Welcome Center Improvement Plan represents INDOT's comprehensive statewide vision for the INDOT Rest Area Program in the context of challenges such as safety, aging infrastructure, high rest area demand, and visibility. INDOT recognizes the value of rest areas and the role they play in combating drowsy and distracted driving, providing shelter during weather events and road closures and supporting the tourism and trucking industries.

INDOT has strategically chosen to focus on the reconstruction and investment of Welcome Center Properties. These are seen as gateways to Indiana for the traveling public and provide an opportunity to make a positive first impression on visitors. INDOT's new standard of Welcome Center seeks to provide design elements that match the region in which it is located. It is expected that these buildings and properties will be larger and provide a higher standard of design features

INDOT operates 26 rest areas or welcome center facilities on Indiana's interstate highways in 17 areas across the state. Nine locations have facilities on both sides of the interstate, and eight sites are located on only one side of the highway.

Each INDOT rest area or welcome center is designed to provide a relaxing, informative oasis along the nearby interstate.

Each rest area facility includes various buildings, restroom facilities, water fountains, picnic areas, vending services, numerous other amenities, driving information, and a variety of other tourism and traveler-related services.

Interstate rest areas and welcome centers are regulated by the Federal Highway Administration (FHWA). Since FHWA funding is used to build rest areas, federal laws govern the use of rest areas.

INDOT is dedicated to making every facility accessible to all citizens traveling within its state. INDOT conducted a self-evaluation of its facilities which included but was not limited to the following:

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- 1) Access Aisles
- 2) Accessible Routes
- 3) Baby Changing Stations
- 4) Curb Ramps
- 5) Designation Signage
- 6) Drinking Fountains
- 7) Fire Alarms
- 8) Grab Bars
- 9) Handrails
- 10) Mirrors
- 13) Operable Parts
- 14) Parking Spaces
- 15) Picnic Tables
- 16) Toilet Compartments
- 17) Vending Machines*

*Vending services for INDOT rest areas and welcome centers are provided by the Indiana Family and Social Services Administration division of Blind and Visually Impaired Services in accordance with the Indiana Code.

INDOT evaluates its facilities according to ADA compliance for buildings and facilities, 2010 ADA Accessibility Guidelines (ADAAG)

<https://www.access-board.gov/guidelines-and-standards/buildings-and-sites/about-the-ada-standards>;

as well as PROWAG

<https://www.federalregister.gov/documents/2023/08/08/2023-16149/accessibility-guidelines-for-pedestrian-facilities-in-the-public-right-of-way>.

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INDOT strives to ensure its facilities follow the requirements of the 2010 Standards, including both the Title II regulations at 28 CFR 35.151; and the 2004 ADAAG at 36 CFR part 1191, appendices B and D. Each facility or part of a facility constructed by, on behalf of, or for the use of public is designed and constructed in such manner that the facility or part of the facility is readily accessible to and usable by individuals with disabilities. Each altered facility is altered in such a manner that the altered portion of the facility is readily accessible to and usable by individuals with disabilities.

To monitor progress and share information on current and future accessibility improvements, INDOT developed a geographic information system (GIS) mapping network. This interactive map displays detailed reports on the status of Rest Areas and Welcome Centers as the reports become available and are migrated to the map. Current projects and closures are:

Lebanon Rest Areas – I-65 NB/SB

- I-65 northbound closed in June 2023 for conversion to truck parking facilities
- I-65 southbound closed in June 2023 for conversion to truck parking facilities

Clear Creek Welcome Center – I-70 EB

- Closed in June 2023 for Welcome Center reconstruction

Black River Welcome Center – I-64 EB

- Closed in June 2023 for Welcome Center reconstruction

Wolcott Rest Areas – I-65 NB/SB

- Permanent closure as of November 2023

Welcome Center and Rest Area Flier, including planned renovation, construction, and closures can be accessed at :[Welcome Centers and Rest Areas](#)

4.3 Self-Evaluation, Public Right-of-way

INDOT is required to comply with the requirements of the Americans with Disabilities Act of 1990 (ADA) and has

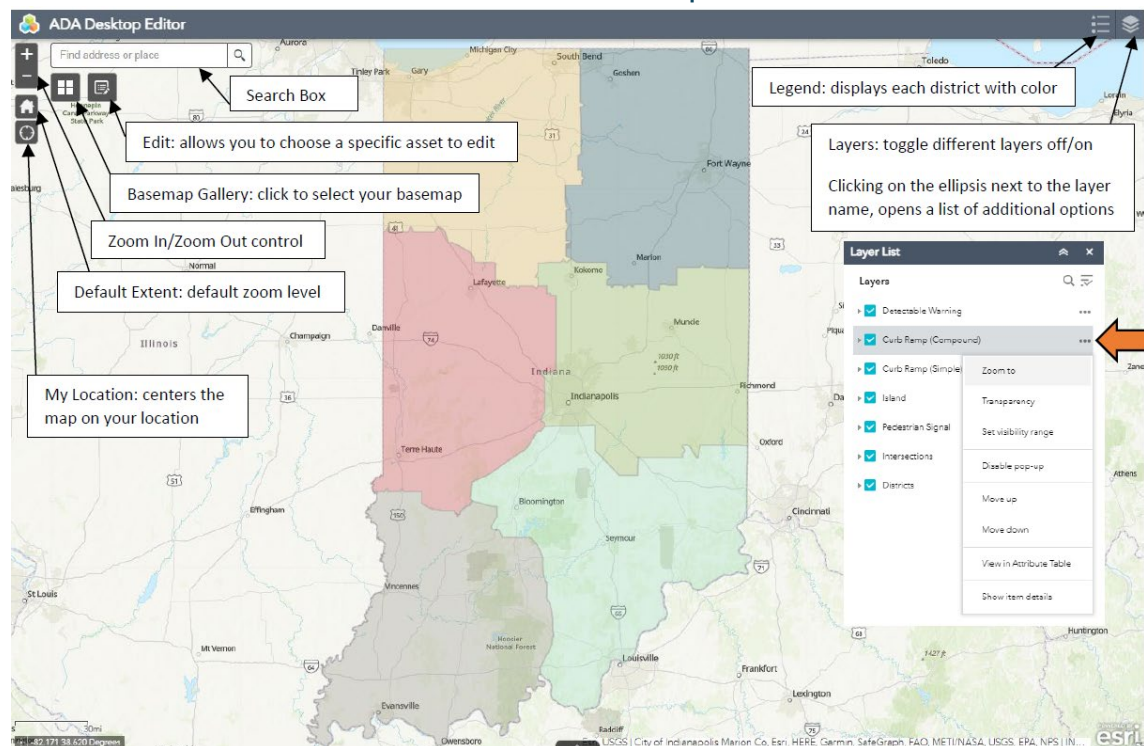
2024 UPDATE



adopted an ADA Transition Plan as part of its ADA compliance. Part of an ADA Transition Plan includes development and maintenance of a complete inventory of all owned pedestrian facilities including curb ramps, sidewalks, islands and pedestrian signals.

To achieve compliance, INDOT licenses GIS products from Esri who owns the ArcGIS brand. INDOT developed an ArcGIS mapping program and a mobile collection program application on APP, called ADA Editor and ADA Viewer to conduct district data collection and analysis. The GIS can be used to visualize and analyze data to determine relationships and answer questions that viewing raw data as a spreadsheet is not capable of providing. Such as,

- How close are two assets?
- How many ADA assets are located in an intersection or near an intersection?
- What is the most recent contract under which work was performed and resulted in a complete alteration to an asset?
- What is the most recent DES number under which work was performed and resulted in a complete alteration to an asset?
- What contracts do we have within an MPO's boundaries?
- Which assets do we have that are in poor condition?



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In each of INDOT six districts, the Technical Services Director (ADA Liaison) is responsible for maintaining the district's ADA asset inventory, which includes:

- Designating district roles and personnel as application users and authorizing their user logins to the Asset Editor application.
- Ensuring processes are in place within their district to collect updated measurements as required by the completion of a project, investigation of a complaint, a recent field audit, or other conditions that may require the asset to be updated.
- Collaborating with the Construction Director to ensure that data is collected once a project has been completed and entered into the inventory by construction inspectors or other designated individuals.
- Collaborating with Capital Program Management staff in the district to ensure the inventory is properly available to and utilized by INDOT staff as part of project selection, especially as it relates to ADA Specific Projects.
- Periodically checking to ensure the data accuracy is improving and collaborating with the TAC to audit data entries and projects to ensure overall compliance and data integrity.
- Submitting annually to the INDOT ADA Coordinator Program Manager any required updates to the district's ADA Asset Management Update, which should detail:
 - Who currently has access to the ADA Asset Inventory in their district?
 - Who currently is responsible for entering data collected in the field following project completion?
 - What audits, if any, have been conducted during the current Fiscal Year by the TSD, TAC Committee, FHWA or other entity? A summary of the results of these audits or where the information may be obtained.
 - Who currently is responsible for verifying the accuracy of data entered into the ADA Asset Database and what verification steps have been performed this year?
 - What specific ADA training does the district currently require (if any) and to whom should it be provided?

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- What comments, questions or feedback does the district currently have?

Each district conducted a self-evaluation of its ADA Assets for the 2023/2024 fiscal year. INDOT customized fields for the geodatabase include location, directions, size, features, and obstruction size. The data structure was pre-programmed for data collection. Data was then logged into the ADA Asset Editor database and analyzed for compliance by each inventory team for their respective areas of collection.

INDOT's pedestrian rights-of-way data provides staff geographic data with:

- Positional information: the digital representation of a barrier conforms to the location found in the field.
- Attribute information: the digital representation of a barrier is represented in a manner that best represents the conditions found in the field (% running slope, % cross-slope, inches of vertical separation, etc.).
- Adherence to collection guidance on what features to inventory within the public rights-of-way. The Application creates organized attribute information and improves the quality assurance of the data. Once the field data collection and validity checks were performed, the raw data for each district was processed for analysis and reporting by the respective district collection team.

The results of the self-evaluation are listed below by each district.

Crawfordsville District



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Eric Fair, ADA Liaison

Special Project Engineer – Technical Services

Crawfordsville District

41 W. 300N, Crawfordsville, IN 47933

[Crawfordsville District Map](#)

2024 UPDATE



DIST NAME	RECOMMENDATIONS	FREQUENCY
CRAWFORDSVILLE	Curb Ramp Simple	1,615
CRAWFORDSVILLE	No Recommendation made*	227
CRAWFORDSVILLE	No Action Required	1,090
CRAWFORDSVILLE	Monitor	10
CRAWFORDSVILLE	Construct	16
CRAWFORDSVILLE	Repair	2
CRAWFORDSVILLE	Reconstruct	215
CRAWFORDSVILLE	Maintenance (ex. Paint)	52
CRAWFORDSVILLE	Other	3

DIST_NAME	RECOMMENDATIONS	FREQUENCY
CRAWFORDSVILLE	Curb Ramp compound	1,178
CRAWFORDSVILLE	No Recommendation made*	152
CRAWFORDSVILLE	No Action Required	672
CRAWFORDSVILLE	Monitor	11
CRAWFORDSVILLE	Construct	46
CRAWFORDSVILLE	Repair	3
CRAWFORDSVILLE	Reconstruct	273
CRAWFORDSVILLE	Maintenance (ex. Paint)	11
CRAWFORDSVILLE	Other	10

DIST_NAME	RECOMMENDATIONS	FREQUENCY
CRAWFORDSVILLE	Detectable Warning Systems	2,751
CRAWFORDSVILLE	No Recommendation made*	302
CRAWFORDSVILLE	No Action Required	1864
CRAWFORDSVILLE	Monitor	34
CRAWFORDSVILLE	Construct	48
CRAWFORDSVILLE	Repair	15
CRAWFORDSVILLE	Reconstruct	472
CRAWFORDSVILLE	Maintenance (ex. Paint)	7
CRAWFORDSVILLE	Other	9

2024 UPDATE



DIST_NAME	RECOMMENDATIONS	FREQUENCY
CRAWFORDSVILLE	Islands	320
CRAWFORDSVILLE	No Recommendation made*	56
CRAWFORDSVILLE	No Action Required	132
CRAWFORDSVILLE	Monitor	6
CRAWFORDSVILLE	Construct	28
CRAWFORDSVILLE	Reconstruct	98

DIST_NAME	RECOMMENDATIONS	FREQUENCY
CRAWFORDSVILLE	Pedestrian Signal	1,085
CRAWFORDSVILLE	No Recommendation made*	168
CRAWFORDSVILLE	No Action Required	576
CRAWFORDSVILLE	Monitor	18
CRAWFORDSVILLE	Construct	31
CRAWFORDSVILLE	Repair	10
CRAWFORDSVILLE	Reconstruct	282

Fort Wayne District

**Todd Johnson, P.E., Deputy
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Dirk Schmidt
Assistant Traffic Engineer

Fort Wayne District

5333 Hatfield Rd

Fort Wayne, IN 46808

2024 UPDATE



Fort Wayne District Map

DIST_NAME	RECOMMENDATIONS	FREQUENCY
FORT WAYNE	Curb Ramp Simple	3,116
FORT WAYNE	No Recommendation made*	748
FORT WAYNE	No Action Required	1,783
FORT WAYNE	Monitor	238
FORT WAYNE	Construct	58
FORT WAYNE	Repair	58
FORT WAYNE	Reconstruct	90
FORT WAYNE	Maintenance (ex. Paint)	41
FORT WAYNE	Other	100

DIST_NAME	RECOMMENDATIONS	FREQUENCY
FORT WAYNE	Curb Ramp compound	1,795
FORT WAYNE	No Recommendation made*	589
FORT WAYNE	No Action Required	784
FORT WAYNE	Monitor	33
FORT WAYNE	Construct	184
FORT WAYNE	Repair	17
FORT WAYNE	Reconstruct	81
FORT WAYNE	Maintenance (ex. Paint)	80
FORT WAYNE	Other	27

DIST_NAME	RECOMMENDATIONS	FREQUENCY
FORT WAYNE	Detectable Warning Systems	4,889
FORT WAYNE	No Recommendation made*	3551
FORT WAYNE	No Action Required	1260
FORT WAYNE	Monitor	11
FORT WAYNE	Construct	6
FORT WAYNE	Reconstruct	61

DIST_NAME	RECOMMENDATIONS	FREQUENCY
FORT WAYNE	Islands	650

2024 UPDATE



FORT WAYNE	No Recommendation made*	581
FORT WAYNE	No Action Required	52
FORT WAYNE	Monitor	12
FORT WAYNE	Construct	5

DIST_NAME	RECOMMENDATIONS	FREQUENCY
FORT WAYNE	Pedestrian Signal	1,709
FORT WAYNE	No Recommendation made*	1360
FORT WAYNE	No Action Required	319
FORT WAYNE	Monitor	10
FORT WAYNE	Construct	8
FORT WAYNE	Reconstruct	1
FORT WAYNE	Maintenance (ex. Paint)	11

Greenfield District



Valerie Cockrum, District Deputy Commissioner

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Robert Gill

Special Projects Engineer-Technical Services

Greenfield District
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Greenfield District Map

DIST_NAME	RECOMMENDATIONS	FREQUENCY
GREENFIELD	Curb Ramp Simple	3,097
GREENFIELD	No Recommendation made*	326
GREENFIELD	No Action Required	2222
GREENFIELD	Monitor	52

2024 UPDATE



GREENFIELD	Construct	7
GREENFIELD	Repair	179
GREENFIELD	Reconstruct	301
GREENFIELD	Maintenance (ex. Paint)	5
GREENFIELD	Other	12

DIST_NAME	RECOMMENDATIONS	FREQUENCY
GREENFIELD	Curb Ramp compound	1,585
GREENFIELD	No Recommendation made*	187
GREENFIELD	No Action Required	953
GREENFIELD	Monitor	40
GREENFIELD	Construct	1
GREENFIELD	Repair	102
GREENFIELD	Reconstruct	287
GREENFIELD	Maintenance (ex. Paint)	4
GREENFIELD	Other	11

DIST_NAME	RECOMMENDATIONS	FREQUENCY
GREENFIELD	Detectable Warning Systems	4,674
GREENFIELD	No Recommendation made*	325
GREENFIELD	No Action Required	3,401
GREENFIELD	Monitor	91
GREENFIELD	Construct	12
GREENFIELD	Repair	291
GREENFIELD	Reconstruct	521
GREENFIELD	Maintenance	11
GREENFIELD	Other	22

DIST_NAME	RECOMMENDATIONS	FREQUENCY
GREENFIELD	Islands	83
GREENFIELD	No Recommendation made*	39
GREENFIELD	No Action Required	41
GREENFIELD	Repair	1
GREENFIELD	Reconstruct	1

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GREENFIELD	Maintenance	1
DIST_NAME	RECOMMENDATIONS	FREQUENCY
GREENFIELD	Pedestrian Signal	1,197
GREENFIELD	No Recommendation made*	204
GREENFIELD	No Action Required	387
FORT WAYNE	Monitor	15
FORT WAYNE	Repair	245
FORT WAYNE	Reconstruct	327
FORT WAYNE	Other	19

LaPorte District



Matt Deitchley, Deputy Commissioner

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LaPorte, IN 46350

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Steven Vanes

Asset Management Engineer -Technical Services

Northwest District

315 E. Boyd Blvd

LaPorte, IN 46350

[LaPorte District Map](#)

DIST_NAME	RECOMMENDATIONS	FREQUENCY
LA PORTE	Curb Ramp Simple	3,773
LA PORTE	No Recommendation made*	1162
LA PORTE	No Action Required	1473
LA PORTE	Monitor	658
LA PORTE	Construct	83
LA PORTE	Repair	28
LA PORTE	Reconstruct	313
LA PORTE	Maintenance (ex. Paint)	25
LA PORTE	Other	31

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DIST_NAME	RECOMMENDATIONS	FREQUENCY
LA PORTE	Curb Ramp compound	1,885
LA PORTE	No Recommendation made*	472
LA PORTE	No Action Required	756
LA PORTE	Monitor	309
LA PORTE	Construct	136
LA PORTE	Repair	60
LA PORTE	Reconstruct	111
LA PORTE	Maintenance (ex. Paint)	11
LA PORTE	Other	30

DIST_NAME	RECOMMENDATIONS	FREQUENCY
LA PORTE	Detectable Warning Systems	5,647
LA PORTE	No Recommendation made*	1373
LA PORTE	No Action Required	2449
LA PORTE	Monitor	1135
LA PORTE	Construct	298
LA PORTE	Repair	52
LA PORTE	Reconstruct	233
LA PORTE	Maintenance (ex. Paint)	44
LA PORTE	Other	63

DIST_NAME	RECOMMENDATIONS	FREQUENCY
LA PORTE	Islands	621
LA PORTE	No Recommendation made*	26
LA PORTE	No Action Required	66
LA PORTE	Monitor	385
LA PORTE	Construct	50
LA PORTE	Repair	36
LA PORTE	Reconstruct	32
LA PORTE	Maintenance (ex. Paint)	8
LA PORTE	Other	18

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DIST_NAME	RECOMMENDATIONS	FREQUENCY
LA PORTE	Pedestrian Signal	2,848
LA PORTE	No Recommendation made*	740
LA PORTE	No Action Required	622
LA PORTE	Monitor	1022
LA PORTE	Construct	333
LA PORTE	Repair	137
LA PORTE	Reconstruct	247
LA PORTE	Maintenance (ex. Paint)	27
LA PORTE	Other	40

Seymour District



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Linnea Wellings

Special Projects Engineer-Technical Services

Seymour District
185 Agrico Lane
Seymour, IN 47274
[Seymour District Map](#)

DIST_NAME	RECOMMENDATIONS	FREQUENCY
SEYMOUR	Curb Ramp Simple	1,608
SEYMOUR	No Recommendation made*	8
SEYMOUR	No Action Required	1189
SEYMOUR	Construct	18
SEYMOUR	Repair	5
SEYMOUR	Reconstruct	383
SEYMOUR	Maintenance (ex. Paint)	5

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DIST_NAME	RECOMMENDATIONS	FREQUENCY
SEYMOUR	Curb Ramp compound	804
SEYMOUR	No Recommendation made*	8
SEYMOUR	No Action Required	593
SEYMOUR	Monitor	4
SEYMOUR	Construct	1
SEYMOUR	Repair	7
SEYMOUR	Reconstruct	187
SEYMOUR	Maintenance (ex. Paint)	4

DIST_NAME	RECOMMENDATIONS	FREQUENCY
SEYMOUR	Detectable Warning Systems	2,377
SEYMOUR	No Recommendation made*	4
SEYMOUR	No Action Required	2040
SEYMOUR	Monitor	3
SEYMOUR	Construct	12
SEYMOUR	Repair	8
SEYMOUR	Reconstruct	315
SEYMOUR	Maintenance (ex. Paint)	3

DIST_NAME	RECOMMENDATIONS	FREQUENCY
SEYMOUR	Islands	78
SEYMOUR	No Action Required	64
SEYMOUR	Reconstruct	14

DIST_NAME	RECOMMENDATIONS	FREQUENCY
SEYMOUR	Pedestrian Signal	872
SEYMOUR	No Recommendation made*	21
SEYMOUR	No Action Required	536
SEYMOUR	Monitor	3
SEYMOUR	Construct	6
SEYMOUR	Repair	11
SEYMOUR	Reconstruct	295

2024 UPDATE



Vincennes District

Rusty Fowler, Deputy Commissioner

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Vincennes District Map

Mickey Tosti

Special Projects Engineer -Technical Services

Vincennes District

3650 S. US Highway 41

Vincennes, IN 4791

DIST_NAME	RECOMMENDATIONS	FREQUENCY
VINCENNES	Curb Ramp Simple	1,403
VINCENNES	No Recommendation made*	491
VINCENNES	No Action Required	327
VINCENNES	Monitor	521
VINCENNES	Construct	10
VINCENNES	Reconstruct	21
VINCENNES	Maintenance (ex. Paint)	28
VINCENNES	Other	5

DIST_NAME	RECOMMENDATIONS	FREQUENCY
VINCENNES	Curb Ramp compound	1,016
VINCENNES	No Recommendation made*	129
VINCENNES	No Action Required	230
VINCENNES	Monitor	616
VINCENNES	Construct	19
VINCENNES	Reconstruct	19
VINCENNES	Maintenance (ex. Paint)	3

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DIST_NAME	RECOMMENDATIONS	FREQUENCY
VINCENNES	Detectable Warning Systems	2,412
VINCENNES	No Recommendation made*	329
VINCENNES	No Action Required	699
VINCENNES	Monitor	1298
VINCENNES	Construct	46
VINCENNES	Reconstruct	28
VINCENNES	Maintenance (ex. Paint)	11
VINCENNES	Other	1

DIST_NAME	RECOMMENDATIONS	FREQUENCY
VINCENNES	Islands	222
VINCENNES	No Recommendation made*	42
VINCENNES	No Action Required	14
VINCENNES	Monitor	158
VINCENNES	Construct	5
VINCENNES	Reconstruct	3

DIST_NAME	RECOMMENDATIONS	FREQUENCY
VINCENNES	Pedestrian Signal	737
VINCENNES	No Recommendation made*	139
VINCENNES	No Action Required	103
VINCENNES	Monitor	460
VINCENNES	Construct	24
VINCENNES	Repair	4
VINCENNES	Reconstruct	3
VINCENNES	Maintenance (ex. Paint)	4

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INDOT's self-evaluation of its public rights-of-ways revealed a need to improve INDOT's ArcGIS ADA Editor application to make it easier for inspectors and end-users to input/view data. INDOT will review the current application options under "Recommendations" for necessary remediation or barrier removal. Part of the review process will remove the inspectors/data collector's ability to choose not to make a recommendation. This choice has led to inaccuracy regarding the condition of some of INDOT's ADA Assets.

The challenges uncovered in its public rights-of-way self-evaluation further devote INDOT to stand firm on its commitment to promoting safety and accessibility for all users of the state of Indiana transportation system. The self-evaluation provides information regarding the State's assets per district in real time as inputted into the system by the inspectors/data collector. INDOT realizes that conditions are constantly evolving, directly impacting the built infrastructure including environmental parameters, construction, population changes, public requests, and technological advancement. INDOT maintains a commitment to the public with the understanding that every implementation schedule of each district requires constant evaluation, will be treated as a living document, and will undergo revisions as INDOT strives to deliver projects that achieve improved accessibility and meet performance goals. Please direct all questions regarding the Self-Evaluation process to:

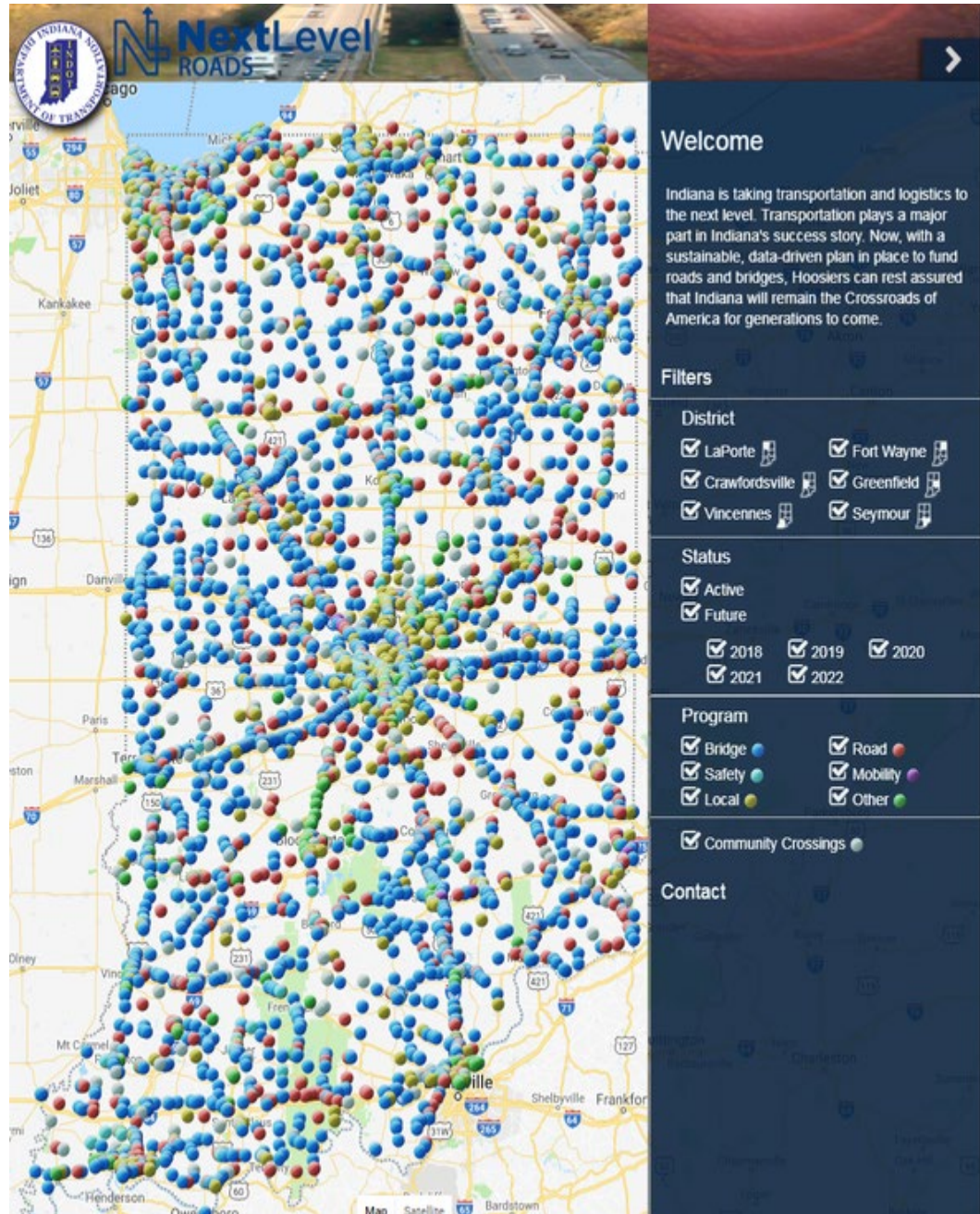
accessforall@indot.in.gov.

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CHAPTER 5 PLAN AND SCHEDULE

INDOT is committed to eliminating accessibility barriers for individuals with disabilities within its programs, rights-of-way, and building facilities. As required under 28 CFR 35.150(d)(3), INDOT must specify a schedule for taking necessary steps to achieve ADA compliance. An implementation plan and schedule provide a timeline for addressing deficiencies in policies and procedures, augmenting missing or incomplete inventory data, and remediating physical barriers identified through inventory.





5.0 Scheduled Steps Necessary to Achieve Compliance.

The transition plan must include a schedule for eliminating barriers where required and prioritizing certain types of facilities, such as those that serve state and local government offices and transportation services. The self-evaluation process provided the necessary, detailed data to determine compliance and barriers of pedestrian facilities, buildings, and websites to identify corrective measures. There are varied approaches to identifying the highest priorities in planning. An advanced method of prioritization is to consider not only the severity of a barrier but the level of use of that asset (such as a ramp, sidewalk, facility, restroom, etc.) by members of the disability community and citizens at large. Knowing that a feature does not meet the ADA criteria is not enough. When many areas need to be addressed, the agency must prioritize what to fix first.

FHWA requires the following:

Schedule – Show a strong commitment toward upgrading ADA elements identified in the inventory of barriers in the short term (planned capital improvement projects) and a strong commitment over time toward prioritizing curb ramps at walkways serving entities covered by the ADA. 28 CFR 35.150(d)(2) This would also include planning and investments directed at eliminating other identified barriers over time. 28 CFR 35.150(d)(3)

The following describes INDOT's methods for barrier removal and describes the schedule and funding.

5.01 Resurfacing Projects. As stated in the Preface, INDOT maintains more than 29,000 lane miles (over 11,200 centerline miles) of highways in Indiana. These roadways serve various purposes, ranging from interstates to US Highways and State Roads. Per the August 2013 DOJ/DOT Joint Technical Assistance memo, INDOT removes barriers to the public right-of-way and provides compliant curb ramps when alterations are made. These alterations include but are not limited to reconstruction, rehabilitation, overlay, mill and

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Fill treatments. All resurfacing projects are included in the STIP described in section 5.02 below.

5.02 INDOT State Transportation Improvement Program (STIP).

The Statewide Transportation Improvement Program (STIP) is Indiana's four-five planning and construction document that lists all projects, and project phases, expected to be funded within 5 years with federal funds as well as state-funded projects that have been deemed Regionally Significant. It is prepared in cooperation with local government entities throughout Indiana, including Transportation Planning Regions (TPRs), Metropolitan Planning Organizations (MPOs), and Regional Planning Organizations (RPOs). The STIP identifies the funding and the scheduling of transportation projects and programs by fiscal year (July 1 through June 30). It includes all state and local transportation projects funded with federal highway and/or federal transit funding along with 100 percent state-funded transportation projects (including highway, passenger rail, freight, public transit, bicycle, and pedestrian) and projects in the national parks).

Indiana has both a decentralized and centralized programming process. Projects are submitted by the six INDOT districts and selected through an agency-wide, statewide asset management process in coordination with MPOs, local public agencies, and transit coordinators. Stakeholder and public input is collected by the MPOs, RPOs, and INDOT at various points within the STIP development process. The STIP is approved by the Federal Highway Administration (FHWA) and the Federal Transit Administration (FTA). The STIP is developed on a two-year cycle and amended monthly. *INDOT's current STIP* can be found [at: STIP FY 2024-2028](#) (Current). Additionally, see the FY 2024-2028 Rural Project Listing was updated on November 1, 2024, and can be found at: [Updated FY 2024-2028 Rural Project Listing](#). Note: *ALL ADA-scheduled projects are included in the STIP. A listing of ADA projects for non-MPO areas in the 24-26 STIP is attached as Appendix E. Additionally, where additional ADA projects are needed, INDOT has provided funding to the districts for stand-alone projects. See section 5.10 below.*

The Indiana Department of Transportation (INDOT) is a critical partner in making improvements to our regional partners' transportation network. INDOT

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receives all federal aid from the USDOT (through the FHWA) and is the steward of all ‘highway’ funds for roadway, bicycle, and pedestrian projects. Bike and Pedestrian ADA Projects included in the Transportation Improvement Program (TIP) to include/revise projects are attached as *Exhibit F*.

INDOT is responsible for all interstate roadways, as well as critical corridors linking the Indiana communities to the state and national transportation system. INDOT identifies and prioritizes investments for the transportation assets INDOT controls as a state agency. MPO ADA projects included in the TIP for funding between 2024 and 2028 are attached as *Appendix G*. Indiana state projects included in the TIP are attached as *Appendix H*.

Additionally, INDOT distributes approximately one-third of all federal aid (highway funds) to Indiana’s fourteen MPOs, and local governments through various competitive programs that INDOT oversees. NIRPC, as the MPO for NWI, works in close partnership with INDOT and the other thirteen MPOs through the MPO Council to understand and discuss important transportation issues across the state. NIRPC ADA Projects are attached as *Appendix I*. Local NIRPC ADA Projects are attached as *Appendix J*.

5.03 Transportation Asset Management Plan. INDOT is responsible for managing the state’s transportation assets, including bridges and roads. The Transportation Asset Management Plan (TAMP) is an essential 10-year management tool that brings together all related business processes, as well as internal and external stakeholders, to achieve a common understanding and commitment to improving the state’s bridge and pavement performance on designated national highway system routes. According to the Federal Highway Administration (FHWA), the TAMP is a tactical-level document that focuses its analysis, options development, programs, delivery mechanisms, and reporting mechanisms on ensuring that strategic objectives are achieved. The TAMP must be risk-based and should contain the following elements:

- A summary listing of the pavement and bridge assets and conditions on the National Highway System (NHS)
- Asset management objectives and measures
- Performance gap identification
- Lifecycle cost and risk management analysis

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- A financial plan and Investment Strategies

Established under 23 Code of Federal Regulations (CFR) Part 490, the TAMP describes existing conditions of the transportation network and provides a 10-year plan for managing the state highway system, including goals, performance targets, funding levels, and investment strategies.

The FHWA has reviewed INDOT's TAMP and certified that our process and TAMP development methodology meet applicable regulatory requirements.

INDOT will routinely update the TAMP document, our development process, and targets every two years, and prepare a new TAMP every four years. *INDOT's current TAMP* is found: [2022 TAMP](#).

5.04 Complete Streets Program

Complete Streets is an approach to road planning and design that considers and balances the needs of all transportation users. The program is about the basics: improving the transportation system's safety and functionality for all users regardless of age, ability, or mode of travel (car, truck, walking, biking, or transit). Its main premise simply involves getting people around safely and efficiently from point A to point B, using whatever mode of travel they choose. INDOT has adopted a [Complete Streets Guideline & Policy](#) that builds upon multiple efforts and promotes an integrated multimodal transportation system that sustains land use developments. INDOT Activities:

- Sidewalks/Crosswalks
- Paved, wide shoulders to accommodate bicycles
- Access Management Strategies
- Roundabouts
- Traffic Signal Timing
- Pedestrian Crossing Signals
- ADA Transition Plans

5.05 Bicycle and Pedestrian Programs.

INDOT works to remove unnecessary barriers to pedestrian and bicycle travel. INDOT funds bicycle and pedestrian improvements in communities

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throughout the state through its federally-funded competitive programs. The program uses a variety of funding sources to provide funding assistance to communities to improve the quality of the community environment. [current-programs/trails-and-greenways.](#)

5.1 ADA Stand-alone Project Funding.

In 2024, INDOT has set aside \$5,000,000.00 annually to address ADA-specific concerns. This flexible fund allows INDOT to move quickly to address ADA concerns that arise and concerns from the public. The funding can be used to address projects that may not have been scoped to include ADA upgrades on pedestrian facilities, or where pedestrian facilities were just outside the scope of a project. The \$5,000,000.00 is divided among the six districts. INDOT has

ADDITIONALLY, INDOT
COMPREHENSIVELY
ADDRESSES ACCESSIBILITY
NEEDS WITHIN THE LIMITS
OF STAND-ALONE ADA
PROJECTS.

given the District ADA Liaisons discretion in funding decisions. Examples include but are not limited to installing curb cuts and ramps, creating accessible pedestrian access routes, providing accessible parking spaces, constructing or modifying shared-use paths, crosswalks,

sidewalks, and signal improvements. Below are the Districts that have scheduled ADA standalone projects and place-holder projects for this ADA three-year Transition Plan reporting period.



2024 UPDATE



Crawfordsville

District

Fiscal Year	DES	Contract	Work Type	Location	County	Total	Comments
FY 2024	2001657	R-43102	ADA Sidewalk Ramp Construction	Frankfort	Clinton	\$1,663,186.00	Crawfordsville fiscal Year Funding, \$416,000.00; Funds from Greenfield, \$416,000.00; FY24 District Discretionary, \$415,186.00; FY25 Funds /Cash Flow, \$416,000.00
FY 2024 Total						\$1,663,186.00	
FY 2025	2001657	R43102	ADA Sidewalk Ramp Construction	Frankfort	Clinton	\$416,000.00	ow to FY24 Frankfort Project, PM Kaitlyn McDaniel; Let 3/13/2024;
FY 2025 Total						\$416,000.00	
FY2026	2101218	R-44039	ADA Sidewalk Ramp Construction	US 136 Waynetown-27 Ramps	Montgomery	\$416,000.00	IN-House Design; Let 07/2025
FY 2026 Total						\$416,000.00	
FY 2027	2300126 2101221 2300128	R-44828 R-43422 R-44829	ADA Sidewalk Ramp Construction	SR 18 Fowler - 8 Curb Corners, SR71 St. Bernice, SR 55 Newtown -15 Curb Ramps	Benton, Vermillion, Fountain	\$832,000.00	IN-House Design, Let 07/2026; IN-House Design, Let 09/2026; IN-House Design, Let 07/2027
FY 2027 Total						\$832,000.00	

Fort Wayne District

Fiscal Year	DES	Contract	Work Type	Location	County	Total	Comments
FY 2025	2001520	R-42969	ADA Sidewalk Ramp Construction	SR 1 in Butler, US 6 in Butler, SR 127 in Angola, SR 120 in Orland	DeKalb, Steuben	\$439,166.74	RFC Estimate, Letting 12/11/2024
FY 2025 Total						\$439,166.74	
FY 2026	2201292	R-44771	ADA Sidewalk Ramp Construction	SR 15 in Leesburg, SR 13 in Pierceton, SR 5 in Cromwell, SR 5 in Ligonier	Kosciusko, Noble	\$440,000.00	Stage 2 Estimate, Letting 11/13/2025
FY 2026 Total						\$440,000.00	
FY 2027	2201293	R-44772	ADA Sidewalk Ramp Construction	SR 205 in Columbia City, US 33 in Churubusco, SR 5 in Larwill, SR 1 in Bluffton, SR 1 in Wells County, US 224 in Markle, SR 3 in Markle	Whitley, Huntington, Wells	\$800,000.00	Programmed Amount, Letting 11/05/2026
FY 2027 Total						\$800,000.00	
FY 2028	2201294	R-44773	ADA Sidewalk Ramp Construction	SR 218 in Berne	Adams	\$800,000.00	Programmed Amount, Letting 11/10/2027
FY 2028 Total						\$800,000.00	

Greenfield District

Des #	Contract #	Work Type	Location	CN Amount From Progra	Funding Program Fis	County	Contract #	Letting Date
2400121	R-42230	ADA Sidewalk Ramp Construction	SR 28 SR 28 from SR 19 E junct to ECL of Tipton	\$ 130,217.00	2025	Tipton	42230	2/12/2025
2400536	B-42319	ADA Sidewalk Ramp Construction	SP 421X Hunter Road Bridge Over US 421X NB/SB 00.26 N I-465	\$ 88,000.00	2026	Marion	42319	10/8/2025
2400593	R-45629	ADA Sidewalk Ramp Construction	VA 1041 Various locations in the Greenfield District	\$ 434,826.00	2025, 2026	Various	45629	2/11/2026
2400694	R-43094	ADA Sidewalk Ramp Construction	SR 213 From Dewitt St to Race St	\$ 676,320.00	2026	Tipton	43094	2/11/2026
2400594	T-45201	ADA Sidewalk Ramp Construction	VA 1041 Various locations in the Greenfield District	\$ 892,389.00	2025, 2027	Various	45201	1/13/2027
2400595	T-45729	ADA Sidewalk Ramp Construction	VA 1041 Various locations in Greenfield District	\$ 914,159.00	2025, 2028	Various	45729	1/12/2028

2024 UPDATE



LaPorte District

Fiscal Year	DES	Contract	Work Type	Location	County	Total	Comments
FY 2024 Total						\$0.00	
FY 2025	2301578	NA	ADA Sidewalk Ramp Construction	NA	NA	\$0.00	Placeholder; no project was performed
FY 2025 Total Estimate						\$0.00	
FY 2026	2301579	R-45477	ADA Sidewalk Ramp Construction	SR 106 in Bremen	Marshall	\$726,250.00	Current Estimate Only; Final Cost will change
FY 2026 Total Estimate						\$726,250.00	
FY 2027	2400065	R-45594	ADA Sidewalk Ramp Construction	US 231 in Rensselaer	Jasper	\$950,300.00	Current Estimate Only; Final Cost will change
FY 2027 Total Estimate						\$950,300.00	
FY 2028	2400066	R-45601	ADA Sidewalk Ramp Construction	SR 14 in Rochester	Fulton	\$756,869.00	Current Estimate Only; Final Cost will change
FY 2028	2400600	R-45602	ADA Sidewalk Ramp Construction	US 231 in Hebron	Porter	\$360,000.00	Current Estimate Only; Final Cost will change
FY 2028 Total Estimate						\$1,116,869.00	
FY 2029	2101654	R-45604	ADA Sidewalk Ramp Construction	SR 2 in LaPorte	LaPorte	\$1,857,000.00	Current Estimate Only; Final Cost will change
FY 2029 Total Estimate						\$1,857,000.00	

Seymour District

Fiscal Year	DES	Contract	Work Type	Location	County	Total	Comments
FY 2025	2201203	R-44714	ADA Sidewalk Ramp Construction	SR 262 and Henrietta and Mulberry Streets, Rising Sun & SR 46 at Ivy Tech, Batesville	Ohio, Ripley	\$343,700	Contract awarded 10/18/24
FY 2026	2201075	R-43745	ADA Curb Ramp Construction	US 231 in Freedom	Owen	\$702,014	Sidewalk and ramp work separated from road project, let date 12/10/2025, stage 2 design complete, cost lisetd is estimate only
FY 2027	2201202	R-44713	ADA Sidewalk Ramp Construction	SR 64 at Merry Way, Georgetown & US 31 and County Line Road, Greenwood	Floyd, Johnson	\$446,000	Let date 07/08/2026, designer selected for SR 64 portion, cost lisetd is estimate only; US 31 to be combined with Greenfield District project
FY 2028	2400106	T-45521	ADA Sidewalk Ramp Construction	SR 45 and Liberty Drive, Bloomington	Monroe	\$832,000	Let date 07/14/2027, cost lisetd is estimate only
FY 2029	2400098	T-45519	ADA Sidewalk Ramp Construction	US 31 and Fry Road, Greenwood at Greenwood Park Mall	Johnson	\$550,000	Let date 07/12/2028, cost lisetd is estimate only

Vincennes District

Des #	Contract #	Work Type	Location	CN Estimate & Inflated Amount	Work Category	Letting Date	Priority Year
2301241	R-45295	Bike/Pedestrian Facilities	SR 66 From 1.81 mi E of US 41 to 1.91 E of US 41 (EB)	\$ 292,175.00	Other Project Type	9/15/2027	2028

2024 UPDATE



5.2 Locally Administered Projects (LAPs)

Local Project Administration through INDOT enables cities, towns, and nonprofit agencies to make transportation improvements with federal and state money. Municipalities and other local agencies use locally administered projects to make intersections safer and develop sidewalks and shared-use paths. INDOT has committed \$50 million in [Highway Safety Improvement Program \(HSIP\)](#) funds to support safety improvement projects on local road networks across Indiana. Examples include new or updated warning devices, signs or pavement markings, raised medians, curb extensions, pedestrian/crosswalk improvements, guardrail additions or improvements, traffic signal enhancements or adjustments, and improvements at rail crossings, among others.

5.3 Requests for Accommodations.

When INDOT receives a request or complaint from the public regarding accessibility INDOT will do our utmost to improve our ADA accommodations. These accommodations will be reviewed and will be addressed to the maximum extent allowable given ADA requirements, safety standards, infrastructure restrictions, and policies. It is important to note that not all accommodations can be made because of safety and infrastructure, still others may need engineering and design to complete and may require time to produce.

At the end of each year, INDOT completes a program review and an annual update report summarizing completed actions and describing any changed conditions. The report enables INDOT to track progress in removing accessibility barriers and achieving ADA compliance.

Every three years, INDOT reviews the entire INDOT ADA Transition Plan to ensure it accurately reflects INDOT's ADA program methodology and procedures and updates the Plan as programs, facilities, services, data, and technology change.

2024 UPDATE



ACRONYMS AND ABBREVIATIONS

ADA	Americans with Disabilities Act
ADAAG	ADA Accessibility Guidelines
APS	Accessible Pedestrian Signals
CFR	Code of Federal Regulations
DOJ	United States Department of Justice
DWS	Detectable Warning Systems
FHWA	Federal Highway Administration
LAP	Locally Administered Projects
LPA	Local Public Agency
MPO	Metropolitan Planning Organization
PROWAG	Public Rights-of-Way Accessibility Guidelines
STIP	Statewide Transportation Improvement Program
TAC	Technical Advisory Committee
TAMP	Transportation Asset Management Plan
WCAG	Web Content Accessibility Guidelines

2024 UPDATE



ADA TRANSITION PLAN UPDATE

2024



APPENDICES

2024 UPDATE



Notice of Nondiscrimination

APPENDIX A



INDIANA DEPARTMENT OF TRANSPORTATION

100 North Senate Avenue
Room N758
Indianapolis, Indiana 46204

PHONE: (855) 463-6848

Eric Holcomb, Governor
Michael Smith, Commissioner

**NOTICE OF NONDISCRIMINATION UNDER
THE AMERICANS WITH DISABILITIES ACT OF 1990
and**

SECTION 504 OF THE REHABILITATION ACT OF 1973

In accordance with the requirements of Title II of the Americans with Disabilities Act of 1990 as amended, 42 U.S.C §§12101 et seq. ("ADA"), and Section 504 of the Rehabilitation Act of 1973, as amended (Section 504) (29 U.S.C. §794) and implementing regulations found in 28 CFR 35 and 49 CFR 27, the Indiana Department of Transportation (INDOT) will not discriminate against qualified individuals with disabilities on the basis of disability in its services, programs or activities. INDOT does not discriminate on the basis of disability in its hiring or employment practices and complies with all regulations promulgated by the U.S. Equal Employment Opportunity Commission under Title I of the ADA.

INDOT will, upon request, provide appropriate aids and services leading to effective communication for qualified persons with disabilities so they can participate equally in INDOT's programs, services, and activities, including qualified sign language interpreters, documents in braille, and other ways of making information and communications accessible to people who have speech, hearing, or vision impairments.

INDOT will make all reasonable modifications to policies and programs to ensure that people with disabilities have an equal opportunity to enjoy all of its programs, services, and activities. For example, individuals with service animals are welcomed in INDOT offices, even where pets are generally prohibited. Anyone who requires auxiliary aids or services for effective communication, or a modification of policies and procedures to participate in a program, service or activity of INDOT, should contact INDOT's ADA/504 Coordinator as soon as possible so that the appropriate accommodation can be made. The Americans with Disabilities Act does not require INDOT to take any action that would fundamentally alter the nature of its programs or services or impose an undue financial or administrative burden.

Complaints that a program, service, or activity of INDOT is not accessible to persons with disabilities should be directed to Taffanee Keys, ADA/504 Coordinator Program Manager at Indiana Department of Transportation 100 N. Senate Avenue – Room N758-PQ, Indianapolis, IN 46204, tkeys@indot.in.gov, 317-941-4512. INDOT will not place a surcharge on qualified individuals with disabilities to cover the cost of providing auxiliary aids, services, or reasonable modifications of policies.

Michael Smith, Commissioner

12/20/2024

Date

www.in.gov/dot/

An Equal Opportunity Employer





External Complaint of Discrimination **APPENDIX B**

2024 UPDATE



EXTERNAL COMPLAINT OF DISCRIMINATION

 INDIANA DEPARTMENT OF TRANSPORTATION
 PREQUALIFICATION & COMPLIANCE DIVISION

Taffanee Keys, ADA/Title VI Program Manager
INDIANA DEPARTMENT OF TRANSPORTATION
Prequalification & Compliance Division

100 N Senate Ave. Room N758-PQ

Indianapolis, IN 46204

Telephone number: (317) 941-4512

E-mail address: AccessForAll@INDOT.in.gov**INSTRUCTIONS:**

The purpose of this form is to help any person interested in filing a discrimination complaint with the Indiana Department of Transportation (INDOT). If the complaint is against INDOT, INDOT's Title VI/ADA Program Manager will forward it to the appropriate federal agency for investigation.

You are not required to use this form. You may write a letter with the same information, sign it, and return it to the address printed above.

All items in bold must be completed for your complaint to be investigated. Failure to provide complete information may impair the investigation of your complaint.

Title VI of the Civil Rights Act of 1964, as amended, and its related statutes and regulations (Title VI) prohibit discrimination on the basis of race, color, and national origin in connection with programs or activities receiving federal financial assistance from the United States Department of Transportation, Federal Highway Administration and/or Federal Transit Administration. These prohibitions extend to INDOT as a direct recipient of federal financial assistance and to its sub-recipients, consultants, and contractors, whether federally funded or not.

INDOT is also required to implement measures to ensure that persons with limited English proficiency and persons with disabilities have meaningful access to the services, benefits, and information of all its programs and activities under Executive Order 13166, Section 504 of the Rehabilitation Act of 1973, and the Americans with Disabilities Act of 1990, as amended.

Upon request, assistance will be provided if you are an individual with a disability or have limited English proficiency. Complaints may also be filed using alternative formats, such as computer disk, audiotape, or Braille. For TTY customers, dial 711 to reach the Indiana Relay Service.

You also have the right to file a complaint with other state or federal agencies that provide federal financial assistance to INDOT. Additionally, you have a right to seek private counsel.

INDOT and its sub-recipients, consultants, and contractors are prohibited from retaliating against any individual because he or she opposed an unlawful policy or practice, filed charges, testified, or participated in any complaint action under Title VI or other nondiscrimination authorities.

Please make a copy of your complaint form for your records. Do not send your original documents as they will not be returned. Mail the original complaint form along with any copies of documents or records relevant to your complaint to the address above.

Complaints of discrimination must be filed within 180 days of the date of the alleged discriminatory act. If the alleged act of discrimination occurred more than 180 days ago, please explain your delay in filing this complaint.

****Your complaint cannot be processed without your signature.**

COMPLAINANT INFORMATION

Name (first, middle, and last)

Address (number and street, city, state, and ZIP code)

Home telephone number

() -

Work telephone number

() -

Cellular telephone number

() -

Available in an alternative format upon request.

2024 UPDATE



Email Address:

	Date (month, day, year)
--	-------------------------

WHO DO YOU ALLEGE WAS THE VICTIM OF DISCRIMINATION☐ You☐ A Third-Party Individual☐ A Class of Persons**PERSON / AGENCY YOU BELIEVE ENGAGED IN DISCRIMINATED**

Name (first, middle, and last)

Title

Name of company

Address (number and street, city, state, and ZIP code)

Home telephone number

() -

Work telephone number

() -

Cellular telephone number

() -

When was the last alleged discriminatory act? (month, day, year)

Do you consent to the investigator sharing your name and other personal information with other parties to this matter when doing so will assist in investigating and resolving your complaint? ☐ Yes ☐ No

Complaints of discrimination must be filed within 180 days of the date of the alleged discriminatory act. If the alleged act of discrimination occurred more than 180 days ago, please explain your delay in filing this complaint:

The alleged discrimination was based on:☐ Race☐ Color☐ National Origin☐ Disability☐ RetaliationHave you filed this complaint with any other agency (Federal, State, or Local)? ☐ Yes ☐ No

If yes, please identify: _____

Have you filed a lawsuit regarding this complaint? ☐ Yes ☐ No If yes, please provide a copy of the complaint.

2024 UPDATE



Describe the alleged act(s) of discrimination. *You should include specific details such as names, dates, times, witnesses, and any other information that would assist us in our investigation of your allegations. Please include any other documentation that is relevant to this complaint. (You may attach additional pages to explain your complaint.)*

Signature: _____

Date: _____

Mail form to: Title VI Manager, Civil Rights Counsel
INDOT Prequalification & Compliance
100 N Senate Ave. Room N758-PQ
Indianapolis, IN 46204

Email form to: AccessForAll@INDOT.in.gov

Available in an alternative format upon request.



Denuncia externa por discriminación

APPENDIX C



CONSENTIMIENTO / EXONERACIÓN DE RECLAMANTE

State Form 54514 (1-11)
INDIANA DEPARTMENT OF TRANSPORTATION
ECONOMIC OPPORTUNITY DIVISION

Taffanee Keys, Title VI / ADA Program Manager INDIANA DEPARTMENT OF TRANSPORTATION

100 N Senate RM N758-PQ
Indianapolis, IN 46204
Telephone number : (317) 941-4512
Fax number : (317) 233-0891
E-mail address: tkeys@indot.in.gov
www.in.gov/dot/

Nombre (primer, segundo, y apellido)	Numero de teléfono () -
Dirección (numero y calle, ciudad, estado and código de ZIP)	
Número(s) de caso (si conocido)	
<p>He leído el Aviso de Reclamante y las Derechas del Entrevistado del Título VI proporcionados por el Departamento del Transporte de Indiana (INDOT). Como reclamante, entiendo que durante una investigación se puede llegar a ser necesario que INDOT revele mi identidad a los individuos fuera de INDOT en el curso de verificar la información o de recolectar hechos y evidencia para desarrollar una base para hacer una determinación de la conformidad de las derechas civiles. Entiendo que puede ser necesario que INDOT comparta la información, incluyendo los detalles personales recogidos como parte de su investigación de la queja. Además, entiendo que como un reclamante yo estoy protegida por Title VI del Acto de las Derechas Civiles de 1964, como enmendado, y sus estatutos y regulaciones relacionados que prohíben la intimidación o la venganza por tomar la acción o participar en una acción para asegurar las derechas protegidas por las estatutos hechas cumplir por INDOT que prohíben la discriminación.</p>	
<p>Lea por favor ambos párrafos abajo, compruebe su opción del CONSENTIMIENTO o CONSENTIMIENTO NEGADO y muestra abajo. (Marque por favor uno.)</p> <p><input type="checkbox"/> Consentimiento</p> <p>He leído y entiendo la información antedicha y autorizo INDOT para divulgar mi identidad a los individuos según lo necesitado durante el curso de la investigación para el propósito de verificar la información o de recolectar hechos y para evidenciar relevante a la investigación de mi queja. Autorizo INDOT para recibir, para repasar y para discutir el material y la información sobre mí relevante a la investigación de mi queja. Entiendo que el material y la información serán utilizados para las actividades autorizadas de la conformidad y de la aplicación de las derechas civiles. Además, entiendo que me no requieren autorizar a este exoneración y ofrezco a hacer esto.</p> <p><input type="checkbox"/> Consentimiento Negado</p> <p>He leído y entiendo la información antedicha y no quisiera que INDOT divulgara mi identidad a ningún individuo durante el curso de la investigación. Entiendo que esta opción podría retrasar la investigación de mi queja y que puede, en algunas circunstancias, da lugar a un encierro administrativo de la investigación de mi queja sin que hiciera INDOT una determinación en mi caso.</p>	
Firma	Fecha (mes, día, año)



Standard Design Curb Ramps

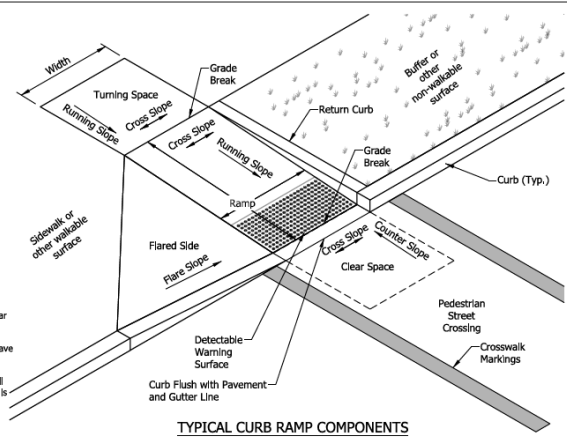
APPENDIX D



SHEET NO.	INDEX
1	Curb Ramp Drawing Index and General Notes
2-3	Perpendicular Curb Ramp Typical Placement
4	Perpendicular Curb Ramp Component Details
5	One-Way-Directional Perpendicular Curb Ramp Typical Placement
6	One-Way-Directional Perpendicular Curb Ramp Component Details
7	Parallel Curb Ramps Typical Placement
8	Parallel Curb Ramp Component Details
9	Blended Transition Curb Ramp, Depressed Curb Ramp and Diagonal Curb Ramp Typical Placement
10	Blended Transition Curb Ramp Component Details
11	Median Cut-Through and Median Perpendicular Curb Ramp Typical Placement
12-13	Detectable Warning Surface Placement and Configuration
14	Detectable Warning Surface Details

GENERAL NOTES:

- All slopes are absolute rather than relative to the sidewalk or roadway grade. Slopes at least 0.50% less than the maximum are preferred.
- Ramp or Blended Transition. A ramp or blended transition shall be used to lower or raise the sidewalk to connect with the street or highway.
- Turning Space. A turning space shall be provided at the top of a perpendicular ramp, bottom of a parallel ramp, or where the pedestrian travel requires a change in direction. A common turning space may be shared by adjacent ramps. The turning space shall have a minimum clear dimension of 4 ft x 4 ft. Where the turning space is constrained at the back of the sidewalk by a curb, retaining wall, building, or feature over 2 inches in height, the minimum clear dimension shall be 4 ft x 5 ft, with the 5-ft dimension in the direction of the ramp running slope.
- Flared Side. A flared side shall be used adjacent to a walkable surface. A flared side may be used adjacent to a non-walkable surface. A flared side shall have a maximum slope of 10.00% measured parallel to the back of the curb.
- Return Curb. A return curb is placed perpendicular to the roadway curb. A return curb may be used adjacent to a non-walkable surface. A return curb shall not be used adjacent to a walkable surface. The return curb may be omitted where the non-walkable surface is flared and the curb adjacent to the roadway is tapered to meet the flush curb at the bottom of the ramp.
- Clear Space. A clear space shall be provided beyond the bottom grade break of a curb ramp wholly contained within the crosswalk and wholly outside the parallel vehicular travel path. The clear space shall have a minimum clear dimension of 4 ft x 4 ft.
- Detectable Warning Surface. A detectable warning surface shall consist of truncated domes and be placed at each street, highway, or railroad crossing. The detectable warning surface shall extend a minimum of 2 ft in the direction of pedestrian travel and be placed the entire width of a ramp, blended transition, or turning space.
- Running Slope. The running slope of a ramp, blended transition, or turning space shall be measured parallel to the direction of pedestrian travel.
 - A running slope of 2.00% or less is considered level.
 - A ramp shall have a maximum running slope of 8.33% but shall not require a ramp length to exceed 15 ft.
 - A blended transition shall have a maximum running slope of 5.00%.
 - A turning space shall have a maximum running slope of 2.00%.
- Width. Unless otherwise noted, minimum width of a ramp, blended transition, or turning space, excluding flared sides or return curb, shall be 4 ft.
- Grade Break. A grade break at the top and bottom of a ramp, blended transition, or turning space shall be perpendicular to the running slope. Grade breaks shall not be within the ramp, blended transition, turning space, or detectable warning surface. Grade breaks shall be flush. Vertical discontinuities shall not be greater than 1/2 in. Where a discontinuity is greater than 1/4 in, the surface shall be beveled with a slope not steeper than 1V:2H.
- Cross Slope Exceptions. The cross slope of a ramp, blended transition, or turning space shall be measured perpendicular to the direction of pedestrian travel.
 - The maximum cross slope at a pedestrian street crossing without posted yield or stop control shall be 5.00%.
 - The maximum cross slope at a pedestrian street crossing with posted yield or stop control shall be 2.00%.
 - The maximum cross slope at a midblock crossing shall be the established grade of the adjacent roadway.
- Counter Slope. A counter slope is the cross slope of the gutter or street adjacent the running slope of the ramp, blended transition, or turning space. See Standard Drawing E 604-SWCR-14 for counter slope details.
- Objects such as a utility cover, vault frame, and grating shall be placed outside the curb ramp.
- Curb ramps shall be placed within the marked crosswalk area.
- Drainage Inlets should be located uphill from a curb ramp to prevent ponding in the path of pedestrian travel.



INDIANA DEPARTMENT OF TRANSPORTATION

CURB RAMP DRAWING INDEX
AND GENERAL NOTES

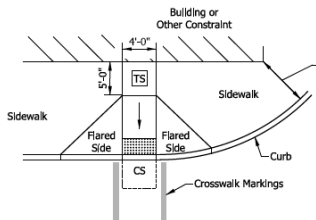
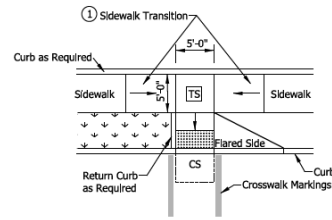
SEPTEMBER 2018

STANDARD DRAWING NO. E 604-SWCR-01

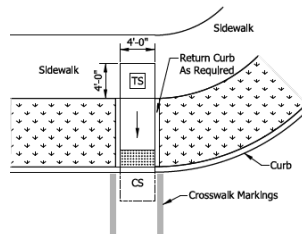
/s/ Elizabeth W. Phillips 03/20/18
DESIGN STANDARDS ENGINEER DATE/s/ John Leckie 04/25/18
CHIEF ENGINEER DATE

NOTES:

- Where insufficient width between the curb and back of sidewalk prevent a standard perpendicular curb ramp running slope, a sidewalk transition may be used to lower the sidewalk grade. The sidewalk transition running slope shall not exceed 8.33%. See Standard Drawing Series E 604-SWCR for sidewalk details.
- The turning space shall have a minimum clear dimension of 4 ft x 4 ft and a running slope of 2.00% maximum. Where the turning space is constrained at the back of the sidewalk, the minimum clear dimension shall be 4 ft x 5 ft, with the 5-ft dimension in the direction of the ramp running slope.

PERPENDICULAR CURB RAMP
ADJACENT WALKABLE SURFACE

TIERED PERPENDICULAR CURB RAMP

PERPENDICULAR CURB RAMP
ADJACENT NON-WALKABLE SURFACE

LEGEND:

- Buffer or Other Non-Walkable Surface
- Ramp
- Detectable Warning Surface
- Turning Space
- Clear Space

INDIANA DEPARTMENT OF TRANSPORTATION

PERPENDICULAR CURB RAMP
TYPICAL PLACEMENT

SEPTEMBER 2018

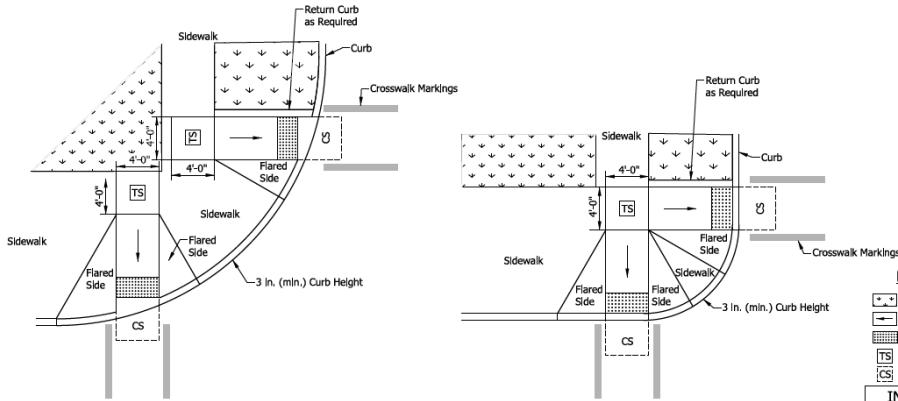
STANDARD DRAWING NO. E 604-SWCR-02

/s/ Elizabeth W. Phillips 03/29/18
DESIGN STANDARDS ENGINEER DATE/s/ John Leckie 04/25/18
CHIEF ENGINEER DATE



NOTE:

- The turning space shall have a minimum clear dimension of 4 ft x 4 ft and a running slope of 2.00% maximum. Where the turning space is constrained at the back of the sidewalk, the minimum clear dimension shall be 4 ft x 5 ft, with the 5-ft dimension in the direction of the ramp running slope.



PAIRED PERPENDICULAR
CURB RAMP AT LARGE RADIUS

PAIRED PERPENDICULAR
CURB RAMP AT SMALL RADIUS

LEGEND:

- Buffer or Other Non-Walkable Surface
- Ramp
- Detectable Warning Surface
- Turning Space
- Clear Space

INDIANA DEPARTMENT OF TRANSPORTATION

PAIRED PERPENDICULAR CURB RAMP
TYPICAL PLACEMENT

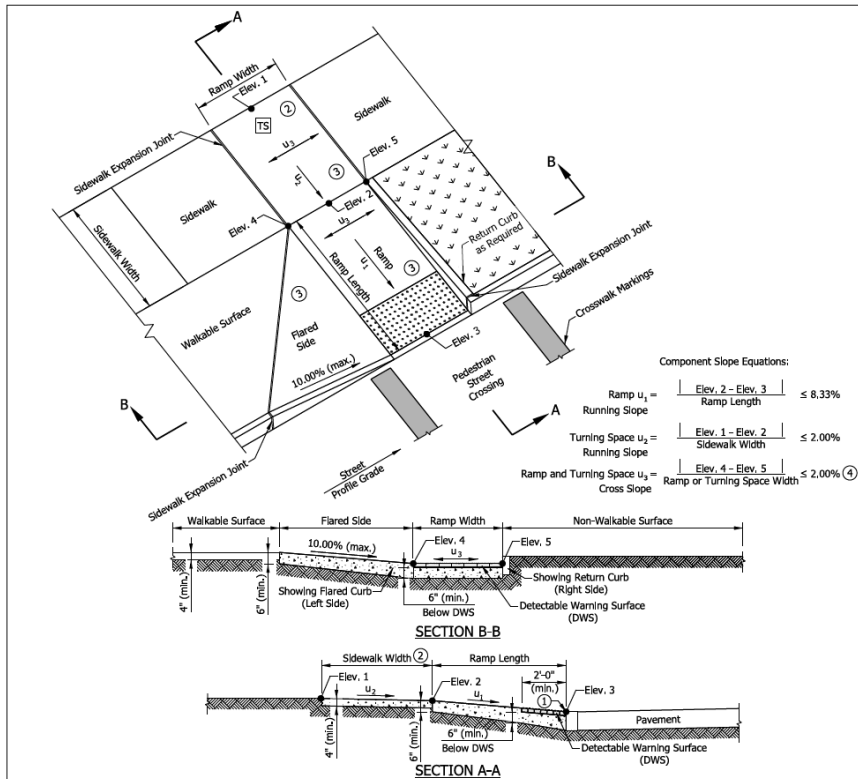
SEPTEMBER 2016

STANDARD DRAWING NO. E 604-SWCR-03



/s/ Elizabeth W. Phillips 03/15/16
DESIGN STANDARDS ENGINEER DATE

/s/ Mark A. Miller 03/18/16
CHIEF ENGINEER DATE



NOTES:

- The bottom edge of the ramp and top of curb shall be flush with the edge of adjacent pavement and gutter line.
- The turning space shall have a minimum clear dimension of 4 ft x 4 ft. Where the turning space is constrained at the back of the sidewalk, the minimum clear dimension shall be 4 ft x 5 ft, with the 5-ft dimension in the direction of the ramp running slope. Where a tiered perpendicular curb ramp is used, a constrained turning space shall have a minimum clear dimension of 5 ft x 5 ft.
- Curb ramp surface shall be coarse broomed transverse to the running slope.
- See Standard Drawing E 604-SWCR-01 for cross slope exceptions.
- See Standard Drawing E 604-SWCR-12, -13, and -14 for Detectable Warning Surface placement, configuration, and details.
- See Standard Drawing E 604-CCS3-01 for sidewalk expansion joint details.

LEGEND:

- Buffer or Other Non-Walkable Surface
- Ramp
- Detectable Warning Surface
- Turning Space

INDIANA DEPARTMENT OF TRANSPORTATION

PERPENDICULAR CURB RAMP
COMPONENT DETAILS

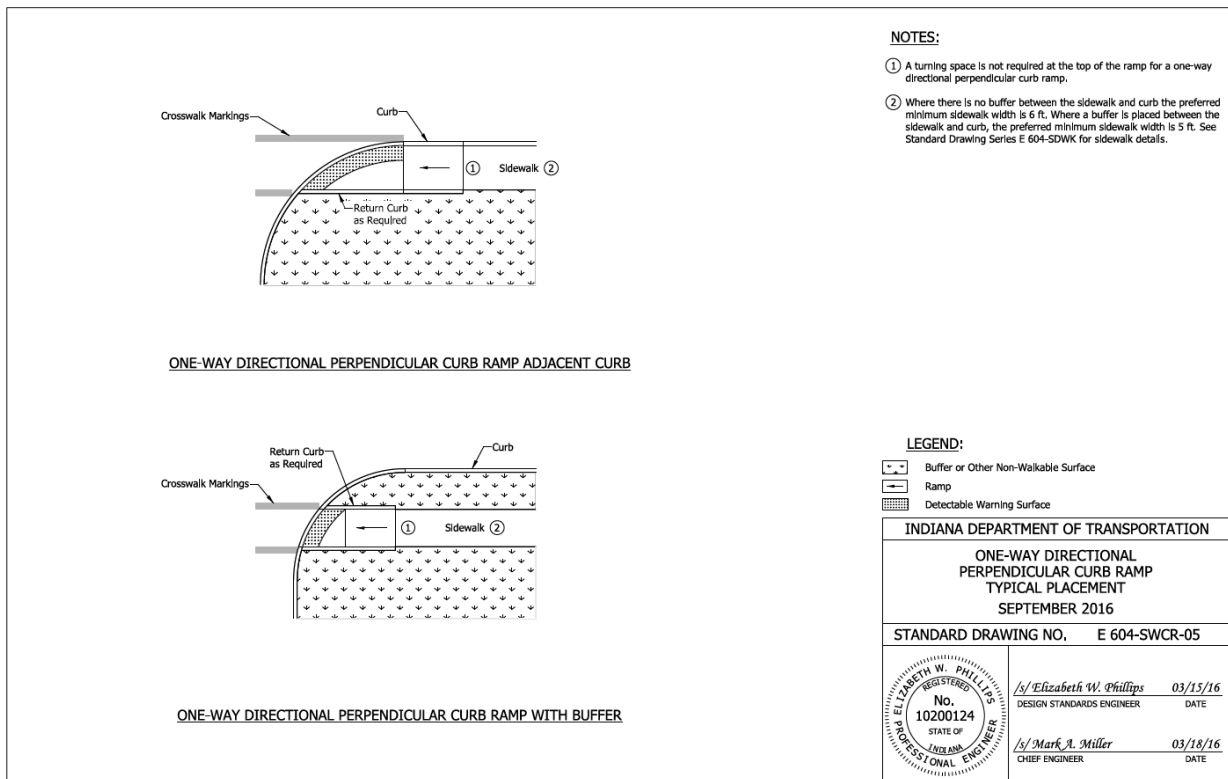
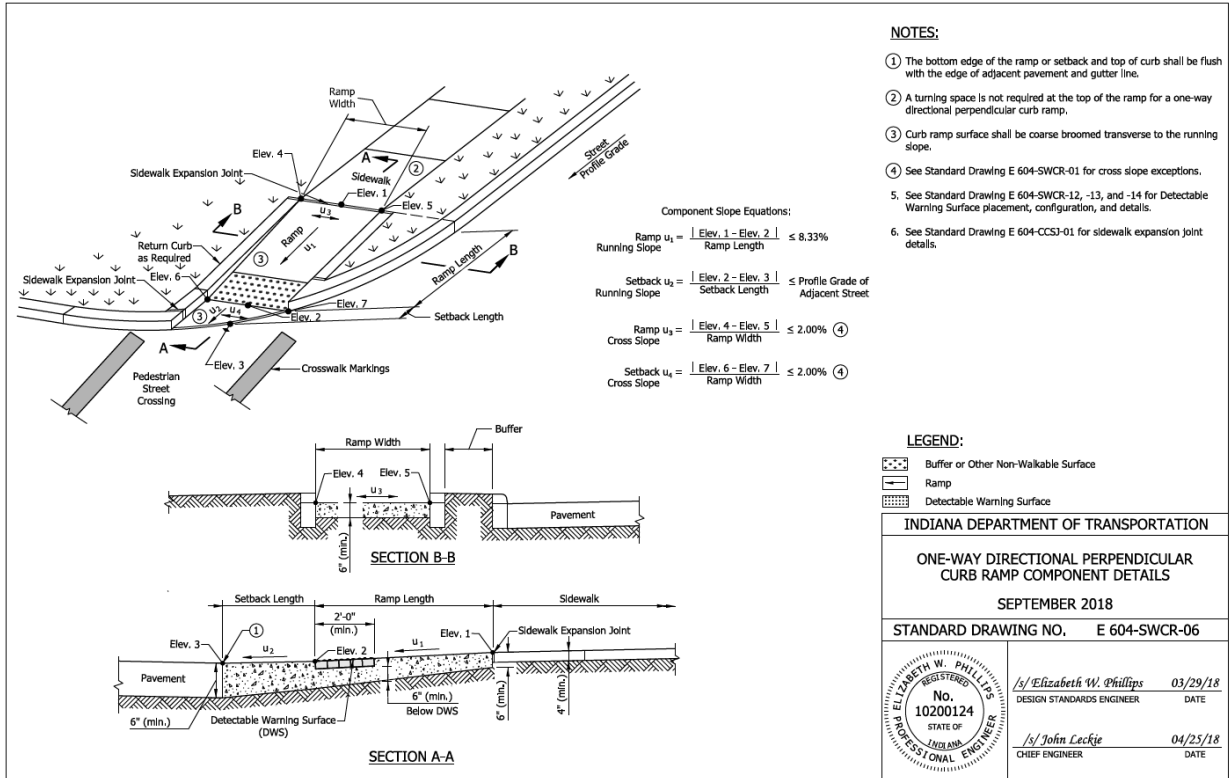
SEPTEMBER 2018

STANDARD DRAWING NO. E 604-SWCR-04

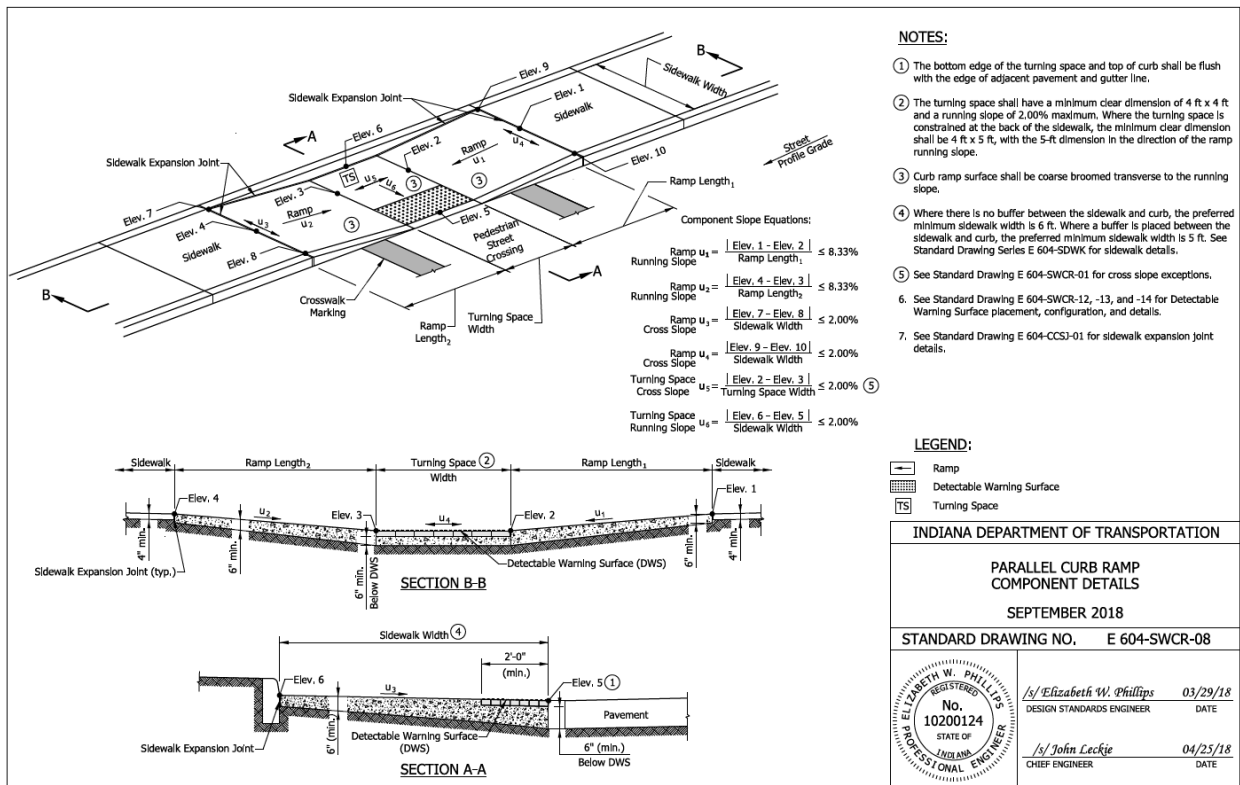
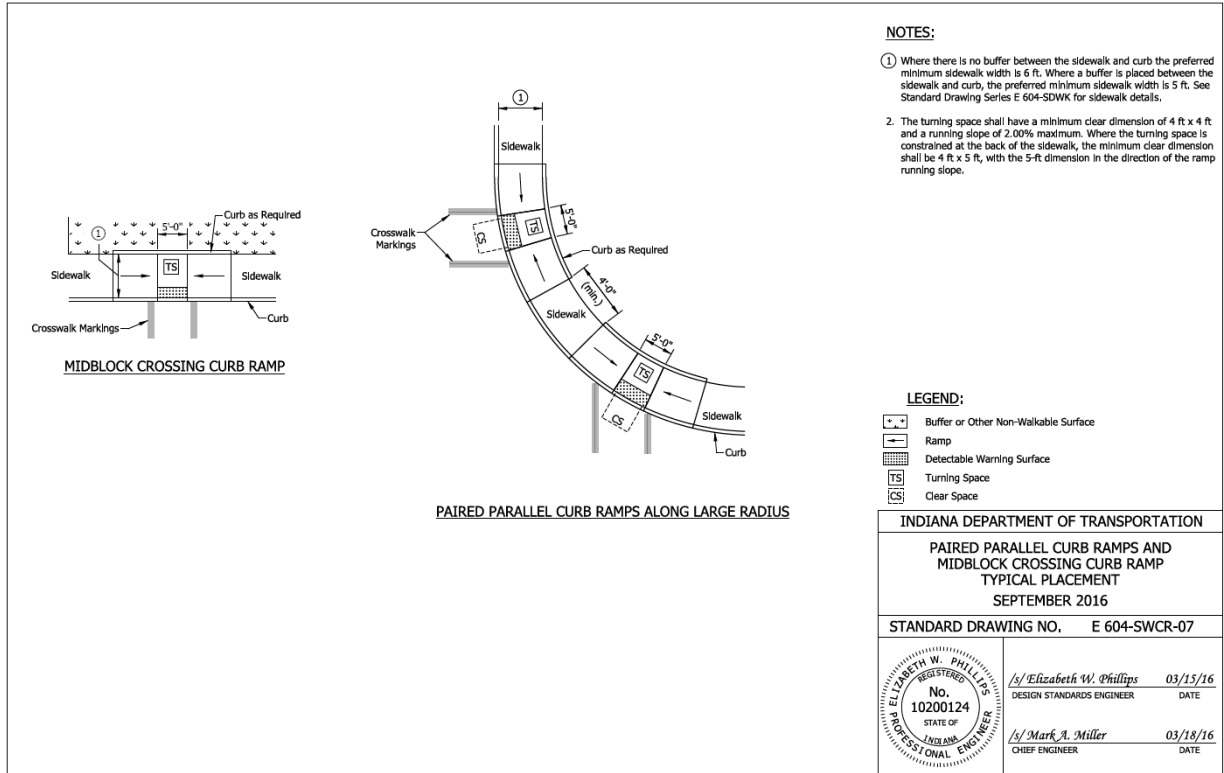


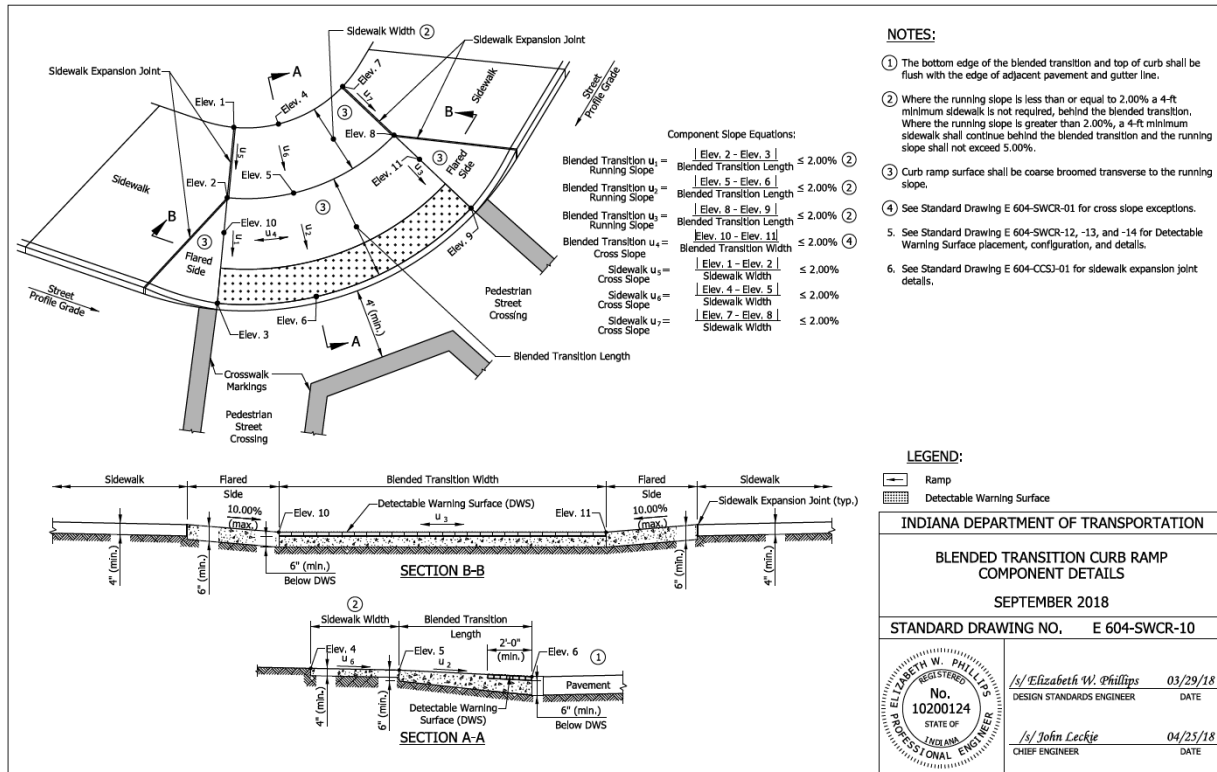
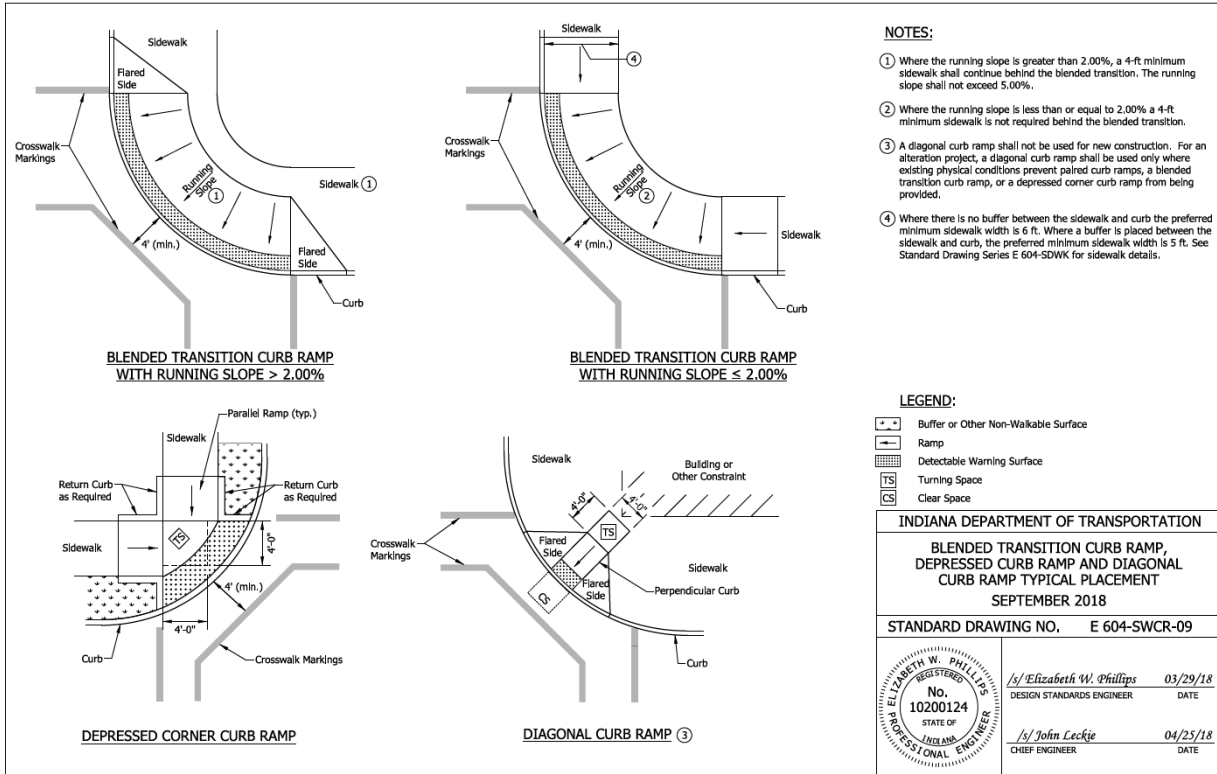
/s/ Elizabeth W. Phillips 03/29/18
DESIGN STANDARDS ENGINEER DATE

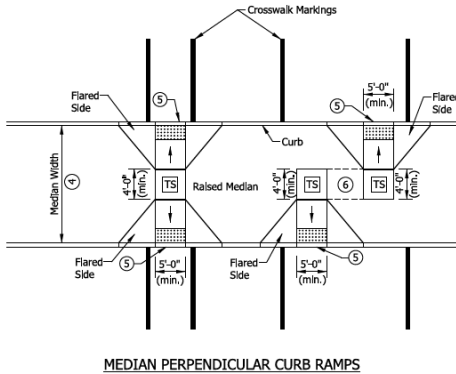
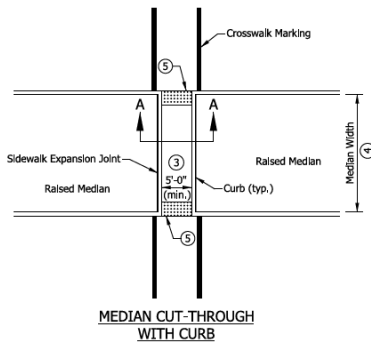
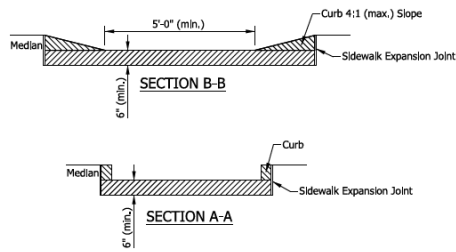
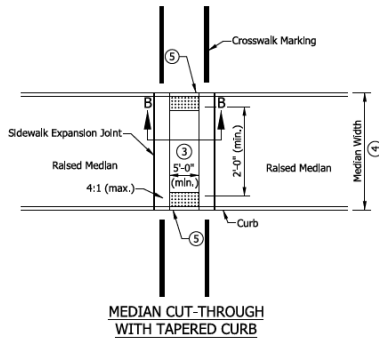
/s/ John Leckie 04/25/18
CHIEF ENGINEER DATE



2024 UPDATE







NOTES:

1. The minimum width of a median cut-through and median perpendicular curb ramp shall be 5 ft.
2. Where in-line or offset perpendicular curb ramps are used with a median, the turning space shall have a minimum clear dimension of 4 ft x 5 ft.
3. Where a median cut through is used the running slope shall be 5.00% maximum.
4. Where median width is less than 6 ft, detectable warning surfaces shall not be placed.
5. The bottom edge of the median cut-through or median perpendicular curb ramp and the top of curb shall be flush with the edge of adjacent pavement gutter line.
6. See Standard Drawing E 604-SWCR-01 for cross slope exceptions.
7. See Standard Drawing E 604-SWCR-12, -13, and -14 for Detectable Warning Surface placement, configuration, and details.
8. See Standard Drawing E 604-CCS-01 for sidewalk expansion joint details.

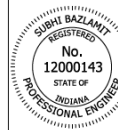
LEGEND:

-  Ramp
 Detectable Warning Surface
 Turning Space

INDIANA DEPARTMENT OF TRANSPORTATION

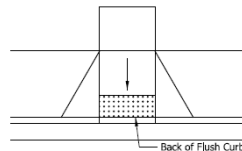
MEDIAN CUT-THROUGH AND
MEDIAN PERPENDICULAR CURB RAMP
TYPICAL PLACEMENT
SEPTEMBER 2023

STANDARD DRAWING NO.	E 604-SWCR-11
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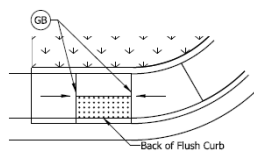


Sudhir Bhandari 04/28/2023
DESIGN STANDARDS ENGINEER DATE

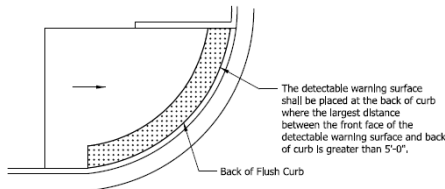
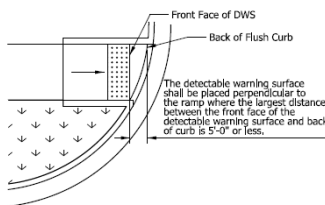
[Signature] 05/12/2023
CHIEF ENGINEER DATE



PERPENDICULAR CURB RAMP



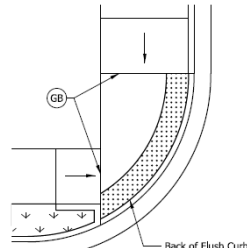
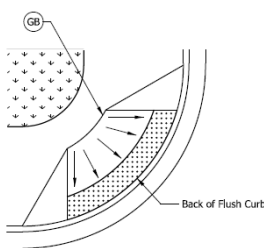
PARALLEL CURB RAMP④







NOTES:

1. A detectable warning surface shall be placed at each street, highway, or railroad crossing. See Standard Drawing E 604-SDWK-03 for a detectable warning surface placement at a sidewalk driveway crossing.
2. The detectable warning surface shall extend a minimum of 2 ft in the direction of pedestrian travel and extend the full width as shown. The detectable warning surface shall not be placed across a grade break.
3. Where the distance from the face of the detectable warning surface is 5 ft or less from the back of curb, the detectable warning surface shall be perpendicular to the curb. Where the distance from the face of the detectable warning surface is more than 5 ft from the back of curb, the detectable warning surface shall be placed at the back of curb in an alternate parallel configuration. See Standard Drawing E 604-SWDC-13 for alternate detectable warning surface placement.
4. The detectable warning surface on a parallel curb ramp shall be placed on the turning space at the flush transition between the street and turning space at the back of curb.
5. The detectable warning surface on a blended transition or depressed corner shall be placed at the back of curb as shown or in an alternate curb configuration. See Standard Drawing E 604-SWDC-13 for alternate detectable warning surface placement.
6. See Standard Drawing E 604-SWRC-14 for detectable warning surface details.

ONE-WAY DIRECTIONAL PERPENDICULAR CURB RAMPS ON A RADIUS ③



LEGEND:

-  Buffer or Other Non-Walkable Surface
-  Detectable Warning Surface (DWS)
-  Ramp
-  Grade Break

INDIANA DEPARTMENT OF TRANSPORTATION

DETECTABLE WARNING SURFACE
PLACEMENT AND CONFIGURATION
SEPTEMBER 2018

STANDARD DRAWING NO.	E 604-SWCR-12
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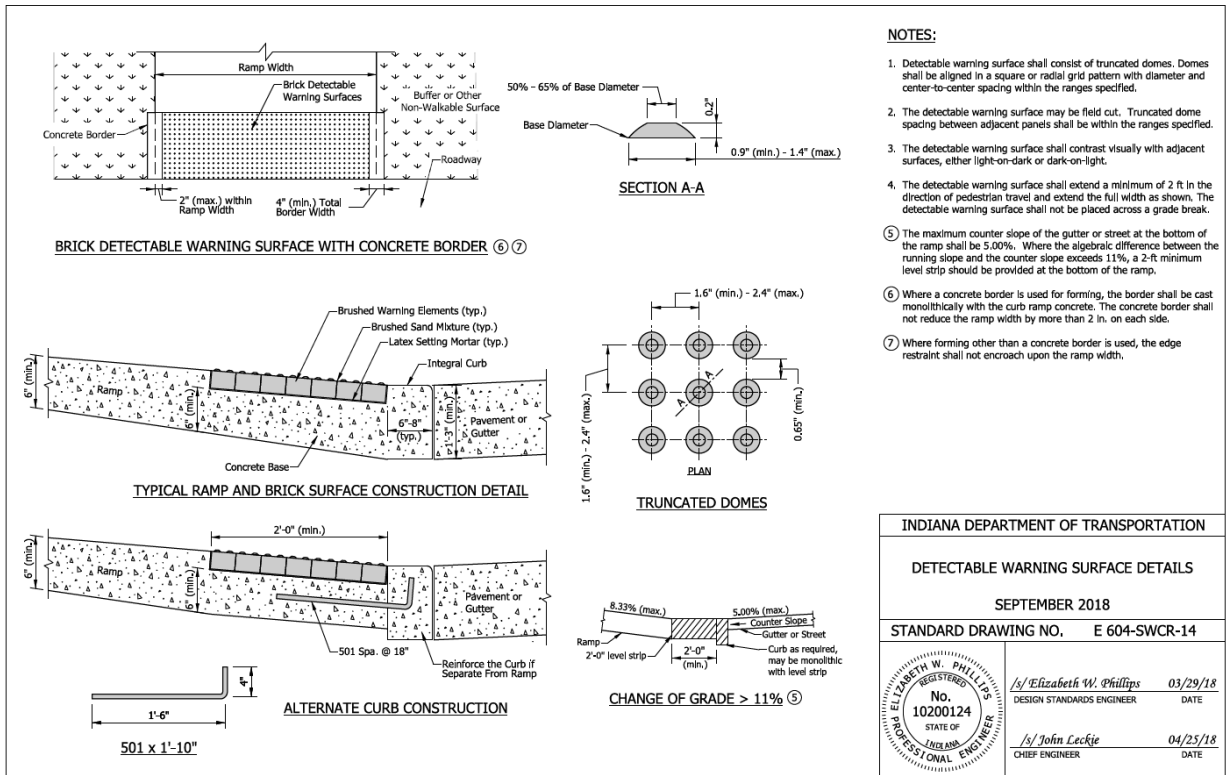
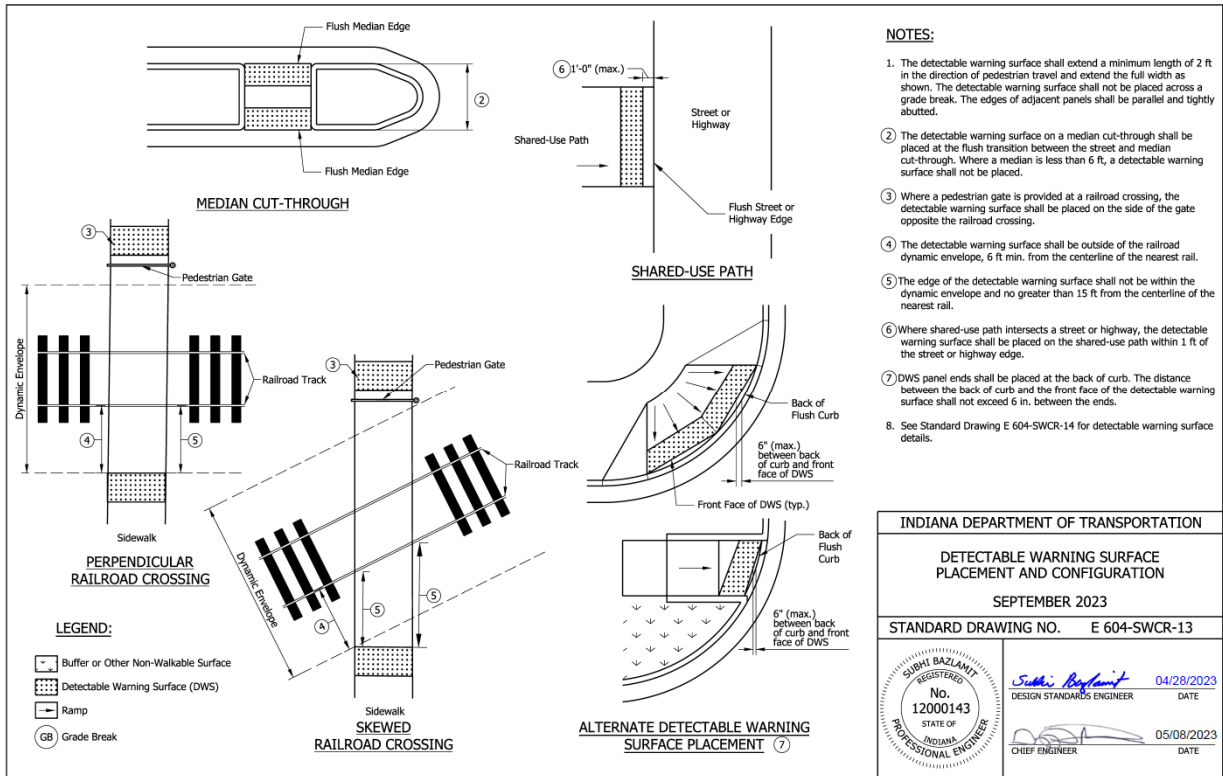


/s/ Elizabeth W. Phillips 03/29/18
DESIGN STANDARDS ENGINEER DATE

/s/ John Leckie 04/25/18
CHIEF ENGINEER DATE

BLENDING TRANSITION CURB RAMP ⑤

DEPRESSED CORNER CURB RAMP (5) (7)





ADA Projects Non-MPO

APPENDIX E

Version N	Des #	Contract I	Route	Work Category	Work Type	Managed B	Contract Let	Contract Descrip	District	Est. Cost	Left I	Funding Prog.	Total	Federal %	Federal Funding	Match %	Match Amount	2024	2025	2026	2027	2028	Funds Authoriz	Status	
STIP 2024	2201681	44311	VA 3828	Local Transportation Alternatives	ADA Sidewalk Ramp Construction	Local	2/10/2027	Sidewalks around	LaPorte	1,854,000	Local Funds	240,000	0	\$	-	100	\$240,000.00				240,000		Approved		
STIP 2024	2201292	44771	VA VARI	District Pavement Project (Non-I)	ADA Sidewalk Ramp Construction	Ft. Wayne D	11/13/2025	At Various Locati	Fort Wayn	645,000	Safety ROW	25,000	80	\$	20,000.00	20	\$ 5,000.00		25,000					Approved	
STIP 2024	2003029	44062	VA 3894	Local Transportation Alternatives	ADA Sidewalk Ramp Construction	Local	3/11/2026	New sidewalks al	LaPorte	951,000	Local Transp	2,000	100	\$	2,000.00	0	\$ -	2,000					2/5/2024	Approved	
24-06	2201203	44714	VA VARI	Other Project Type	ADA Sidewalk Ramp Construction	Seymour Di	10/9/2024	SR 262 in Rising S	Seymour	416,000	Safety Constr	242,000	80	\$	193,600.00	20	\$ 48,400.00		242,000				9/17/2024	Approved	
STIP 2024	1802923	41971	ST 2355	Local Transportation Alternatives	ADA Sidewalk Ramp Construction	Local	10/12/2023	ADA ramp and sic	LaPorte	1,002,000	Local Transp	634,000	100	\$	634,000.00	0	\$ -	634,000					9/5/2023	Approved	
STIP 2024	2003064	43626	IR 1184	Local Transportation Alternatives	ADA Sidewalk Ramp Construction	Local	1/14/2026	Subdivisions of Li	Fort Wayn	3,133,000	Local Funds	551,000	0	\$	-	100	\$551,000.00			551,000			Approved		
STIP 2024	2011695	44306	VA 3894	Local Transportation Alternatives	ADA Sidewalk Ramp Construction	Local	2/10/2027	New sidewalks in	LaPorte	924,000	Local Transp	527,000	100	\$	527,000.00	0	\$ -				527,000		Approved		
STIP 2024	2201293	44772	VA VARI	District Pavement Project (Non-I)	ADA Sidewalk Ramp Construction	Ft. Wayne D	11/5/2026	Var. locations wit	Fort Wayn	645,000	Safety Constr	420,000	80	\$	336,000.00	20	\$ 84,000.00				20,000	400,000	Approved		
STIP 2024	2009025	44103	VA 3828	Local Transportation Alternatives	ADA Sidewalk Ramp Construction	Local	12/10/2025	Sidewalks around	LaPorte	1,645,000	Local Funds	244,000	0	\$	-	100	\$244,000.00						244,000	Approved	
STIP 2024	2300128	44829	SR 55	District Pavement Project (Non-I)	ADA Sidewalk Ramp Construction	Crawfordsvi	7/8/2026	From 53 mi S of I	Crawfords	330,000	Safety Constr	330,000	80	\$	264,000.00	20	\$ 66,000.00						330,000	Approved	
24-11	2400596	45631	VA 1041	District Pavement Project (Non-I)	ADA Sidewalk Ramp Construction	Greenfield I	7/12/2028	Various locations	Greenfield	1,170,344	Safety Consu	235,000	80	\$	188,000.00	20	\$ 47,000.00						235,000	Approved	
STIP 2024	2011695	44306	VA 3894	Local Transportation Alternatives	ADA Sidewalk Ramp Construction	Local	2/10/2027	New sidewalks in	LaPorte	924,000	Local Transp	18,000	100	\$	18,000.00	0	\$ -					18,000	Approved		
STIP 2024	2201292	44771	VA VARI	District Pavement Project (Non-I)	ADA Sidewalk Ramp Construction	Ft. Wayne D	11/13/2025	At Various Locati	Fort Wayn	645,000	Safety Constr	420,000	80	\$	336,000.00	20	\$ 84,000.00				20,000	400,000	Approved		
STIP 2024	2011695	44306	VA 3894	Local Transportation Alternatives	ADA Sidewalk Ramp Construction	Local	2/10/2027	New sidewalks in	LaPorte	924,000	Local Funds	4,000	0	\$	-	100	\$ 4,000.00					4,000	Approved		
STIP 2024	2001520	42969	VA VARI	District Pavement Project (Non-I)	ADA Sidewalk Ramp Construction	Ft. Wayne D	2/12/2025	ADA RAMPs vari	Fort Wayn	932,000	Safety ROW	25,000	80	\$	20,000.00	20	\$ 5,000.00		25,000				Approved		
STIP 2024	2003064	43626	IR 1184	Local Transportation Alternatives	ADA Sidewalk Ramp Construction	Local	1/14/2026	Subdivisions of Li	Fort Wayn	3,133,000	Local Transp	2,204,000	100	\$	2,204,000.00	0	\$ -			#####			Approved		
24-13	2001519	42968	VA VARI	District Pavement Project (Non-I)	ADA Sidewalk Ramp Construction	Ft. Wayne D	3/13/2024	ADA RAMPs vari	Fort Wayn	902,543	Safety Constr	140,000	80	\$	112,000.00	20	\$ 28,000.00				140,000		2/6/2024	Approved	
STIP 2024	2003025	44103	VA 3828	Local Transportation Alternatives	ADA Sidewalk Ramp Construction	Local	12/10/2025	Sidewalks around	LaPorte	1,645,000	Local Funds	12,000	0	\$	-	100	\$ 12,000.00						12,000	Approved	
STIP 2024	2201189	44237	VA VARI	Other Project Type	ADA Sidewalk Ramp Construction	Seymour Di	9/13/2023	Various locations	Seymour	587,000	Safety Constr	437,000	80	\$	349,600.00	20	\$ 87,400.00						437,000	8/4/2023	Approved
STIP 2024	2011218	44039	US 136	District Pavement Project (Non-I)	ADA Sidewalk Ramp Construction	Crawfordsvi	7/9/2025	From 0.4 mi W of	Crawfords	416,000	Safety Constr	416,000	80	\$	332,000.00	20	\$ 83,200.00						416,000	Approved	
STIP 2024	2011695	44306	VA 3894	Local Transportation Alternatives	ADA Sidewalk Ramp Construction	Local	2/10/2027	New sidewalks in	LaPorte	924,000	Local Funds	132,000	0	\$	-	100	\$132,000.00						132,000	Approved	
STIP 2024	2201293	44772	VA VARI	District Pavement Project (Non-I)	ADA Sidewalk Ramp Construction	Ft. Wayne D	11/5/2026	Var. locations wit	Fort Wayn	645,000	Safety Consu	100,000	80	\$	80,000.00	20	\$ 20,000.00		100,000					4/15/2024	Approved
STIP 2024	2003029	44062	VA 3894	Local Transportation Alternatives	ADA Sidewalk Ramp Construction	Local	3/11/2026	New sidewalks al	LaPorte	951,000	Local Funds	145,000	0	\$	-	100	\$145,000.00						145,000	Approved	
STIP 2024	2300126	44828	SR 18	District Pavement Project (Non-I)	ADA Sidewalk Ramp Construction	Crawfordsvi	7/8/2026	From 0.16 mi W c	Crawfords	135,000	Safety Constr	135,000	80	\$	108,000.00	20	\$ 27,000.00						135,000	Approved	
STIP 2024	2201681	44311	VA 3828	Local Transportation Alternatives	ADA Sidewalk Ramp Construction	Local	2/10/2027	Sidewalks around	LaPorte	1,854,000	Local Transp	961,000	100	\$	961,000.00	0	\$ -						961,000	Approved	
24-89	2201075	43745	VA VARI	District Pavement Project (Non-I)	ADA Sidewalk Ramp Construction	Seymour Di	12/10/2025	8.32 miles S of W	Seymour	441,000	Safety ROW	0	80	\$	-	20	\$ -			0				Approved	
24-89	2201075	43745	VA VARI	District Pavement Project (Non-I)	ADA Sidewalk Ramp Construction	Seymour Di	12/10/2025	8.32 miles S of W	Seymour	441,000	Safety Constr	0	80	\$	-	20	\$ -					0		Approved	
STIP 2024	2001657	43102	VA VARI	District Pavement Project (Non-I)	ADA Sidewalk Ramp Construction	Crawfordsvi	3/13/2024	From 1.09 mi S of	Crawfords	1,236,000	Safety Constr	832,000	80	\$	665,600.00	20	\$ 166,400.00		832,000					3/8/2024	Approved
STIP 2024	2003025	44103	VA 3828	Local Transportation Alternatives	ADA Sidewalk Ramp Construction	Local	12/10/2025	Sidewalks around	LaPorte	1,645,000	Local Transp	50,000	100	\$	50,000.00	0	\$ -						50,000	Approved	
STIP 2024	2003029	44062	VA 3894	Local Transportation Alternatives	ADA Sidewalk Ramp Construction	Local	3/11/2026	New sidewalks al	LaPorte	951,000	Local Transp	581,000	100	\$	581,000.00	0	\$ -						581,000	Approved	
STIP 2024	2201294	44773	VA VARI	District Pavement Project (Non-I)	ADA Sidewalk Ramp Construction	Ft. Wayne D	11/10/2027	At various locatio	Fort Wayn	645,000	Safety Constr	420,000	80	\$	336,000.00	20	\$ 84,000.00					20,000	400,000	Approved	
STIP 2024	2201681	44311	VA 3828	Local Transportation Alternatives	ADA Sidewalk Ramp Construction	Local	2/10/2027	Sidewalks around	LaPorte	1,854,000	Local Transp	108,000	100	\$	108,000.00	0	\$ -						108,000	Approved	
STIP 2024	2001657	43102	VA VARI	District Pavement Project (Non-I)	ADA Sidewalk Ramp Construction	Crawfordsvi	3/13/2024	From 1.09 mi S of	Crawfords	1,236,000	Safety ROW	20,000	80	\$	16,000.00	20	\$ 4,000.00				20,000			Approved	
STIP 2024	2201294	44773	VA VARI	District Pavement Project (Non-I)	ADA Sidewalk Ramp Construction	Ft. Wayne D	11/10/2027	At various locatio	Fort Wayn	645,000	Safety ROW	25,000	80	\$	20,000.00	20	\$ 5,000.00						25,000	Approved	
STIP 2024	1802923	41971	ST 2355	Local Transportation Alternatives	ADA Sidewalk Ramp Construction	Local	10/12/2023	ADA ramp and sic	LaPorte	1,002,000	Local Funds	158,000	0	\$	-	100	\$158,000.00						158,000	9/5/2023	Approved
STIP 2024	2001519	42968	VA VARI	District Pavement Project (Non-I)	ADA Sidewalk Ramp Construction	Ft. Wayne D	3/13/2024	ADA RAMPs vari	Fort Wayn	735,000	Safety Constr	457,000	80	\$	365,600.00	20	\$ 91,400.00						457,000	2/6/2024	Approved
STIP 2024	2001657	43102	VA VARI	District Pavement Project (Non-I)	ADA Sidewalk Ramp Construction	Crawfordsvi	3/13/2024	From 1.09 mi S of	Crawfords	1,236,000	District Other	384,000	80	\$	307,200.00	20	\$ 76,800.00						384,000	3/8/2024	Approved
STIP 2024	2201293	44772	VA VARI	District Pavement Project (Non-I)	ADA Sidewalk Ramp Construction	Ft. Wayne D	11/5/2026	Var. locations wit	Fort Wayn	645,000	Safety ROW	25,000	80	\$	20,000.00	20	\$ 5,000.00						25,000	Approved	
STIP 2024	2003029	44062	VA 3894	Local Transportation Alternatives	ADA Sidewalk Ramp Construction	Local	3/11/2026	New sidewalks al	LaPorte	951,000	Local Funds	1,000	0	\$	-	100	\$ 1,000.00						1,000	2/5/2024	Approved
STIP 2024	2201292	44771	VA VARI	District Pavement Project (Non-I)	ADA Sidewalk Ramp Construction	Ft. Wayne D	11/13/2025	At Various Locati	Fort Wayn	645,000	Safety Consu	100,000	80	\$	80,000.00	20	\$ 20,000.00						100,000	Approved	
STIP 2024	2201203	44714	VA VARI	Other Project Type	ADA Sidewalk Ramp Construction	Seymour Di	10/9/2024	SR 262 in Rising S	Seymour	165,000	Safety Constr	165,000	80	\$	132,000.00	20	\$ 33,000.00						165,000	9/17/2024	Approved
STIP 2024	2201202	44713	SR 64	Other Project Type	ADA Sidewalk Ramp Construction	Seymour Di	3/12/2025	Near Roy Street I	Seymour	450,000	Safety Constr	250,000	80	\$	200,000.00	20	\$ 50,000.00						250,000	Approved	
24-28	1901957	42335	ST 1001	Local Other Type Project	ADA Sidewalk Ramp Construction	Local	2/15/2024	Pedestrian Crash	Greenfield	2,283,864	Local Funds	75,000	0	\$	-	100	\$ 75,000.00						75,000	1/16/2024	Approved
24-28	1901957	42335	ST 1001	Local Other Type Project	ADA Sidewalk Ramp Construction	Local	2/15/2024	Pedestrian Crash	Greenfield	2,283,864	Indianapolis I	301,000	100	\$	301,000.00	0	\$ -						301,000	1/16/2024	Approved
24-09	2201075	43745	VA VARI	District Pavement Project (Non-I)	ADA Sidewalk Ramp Construction	Seymour Di	12/10/2025	8.32 miles S of W	Seymour	416,000	Safety Constr	416,000	80	\$	332,800.00	20	\$ 83,200.00						416,000	Approved	
24-09	2400099	45520	SR 60	Other Project Type	ADA Sidewalk Ramp Construction	Seymour Di	7/11/2029	SR 60 at Sycamor	Seymour	720,000	Safety ROW	15,000	80	\$	12,000.00	20	\$ 3,000.00						15,000	Approved	
STIP 2024	2001520	42969	VA VARI	District Pavement Project (Non-I)	ADA Sidewalk Ramp Construction	Ft. Wayne D	2/12/2025	ADA RAMPs vari	Fort Wayn	932,000	Safety Constr	465,000	80	\$	372,000.00	20	\$ 93,000.00						465,000	11/6/2024	Approved
STIP 2024	2201294	44773	VA VARI	District Pavement Project (Non-I)	ADA Sidewalk Ramp Construction	Ft. Wayne D	11/10/2027	At various locatio	Fort Wayn	645,000	Safety Consu	100,000	80	\$	80,000.00	20	\$ 20,000.00						100,000	4/29/2024	Approved
STIP 2024	2003025	44103	VA 3828	Local Transportation Alternatives	ADA Sidewalk Ramp Construction	Local	12/10/2025	Sidewalks around	LaPorte	1,645,000	Local Transp	976,000	100	\$	976,000.00	0	\$ -						976,000	Approved	
STIP 2024	2201681	44311	VA 3828	Local Transportation Alternatives	ADA Sidewalk Ramp Construction	Local	2/10/2027	Sidewalks around	LaPorte	1,854,000	Local Funds	27,000	0	\$	-	100	\$ 27,000.00						27,000	Approved	
24-81	2201128	44039	US 136	District Pavement Project (Non-I)	ADA Sidewalk Ramp Construction	Crawfordsvi	10/8/2025	ADA Sidewalk Rai	Crawfords	491,000	Safety Constr	75,000	80	\$	60,000.00	20	\$ 15,000.00						75,000	Approved	
24-16	2301089	US 231	District Pavement Project (Non-I)	ADA Sidewalk Ramp Construction	Crawfordsville District	From 5.1 mi S of I	Crawfords	0	Safety Consu	-26,000	80	\$	(20,800.00)	20	\$ (5,200.00)								-26,000	Internal Draft App	
24-16	2301089	US 231	District Pavement Project (Non-I)	ADA Sidewalk Ramp Construction	Crawfordsville District	From 5.1 mi S of I	Crawfords	0	Bridge ROW	-128,000	80	\$	(102,400.00)	20	\$ (25,600.00)								-128,000	Internal Draft App	
24-63	2300126	44828	SR 18	District Pavement Project (Non-I)	ADA Sidewalk Ramp Construction	Crawfordsvi	7/8/2026	From 0.16 mi W c	Crawfords	188,000	District Other	0	80	\$	-	20	\$ -						-10,000	10,000	Approved



Bike & Pedestrian Projects TIP

APPENDIX F

2024 UPDATE



TIP ID	TIP	LEAD	TITLE	PROJECT TYPE	TRANSIT SYSTEM	TOTAL COST	FED FUNDS	LETTING DATE
2200132	24-11.1	Beech Grove	Hornet Greenway	Pedestrian Enhancement	Local	\$0		12/13/2023
1601001	24-00	Indianapolis DPW	Safe Routes to Transit - North	Pedestrian Enhancement	Local	\$0	FEDERAL - HSIP < BR>	11/10/2020
2400602	24-07.56	Indianapolis DPW	Nickel Plate Trail Pedestrian Bridge over Keystone	Pedestrian Enhancement	Local	\$0		7/8/2026
2307FFE	24-00	Indianapolis DPW	86th St Pedestrian Improvements from Meridian Street to Al	Pedestrian Enhancement	Local	\$0		10/30/2025
1901955	24-06.3	Indianapolis DPW	Pedestrian Crash Focus Area 14B	Pedestrian Enhancement	Local	\$0	FEDERAL - HSIP < BR>	1/18/2024
1901957	24-08.3	Indianapolis DPW	Pedestrian Crash Focus Area 14A	Pedestrian Enhancement	Local	\$0	FEDERAL - HSIP < BR>	2/14/2024
1700936	24-00	Indianapolis DPW	Safe Routes to Transit - South	Pedestrian Enhancement	Local	\$0	FEDERAL - HSIP < BR>	9/15/2021
1700903	24-02.2	Indianapolis DPW	Pedestrian Crash Focus Area 3	Pedestrian Enhancement	Local	\$0	FEDERAL - HSIP < BR>	11/17/2021
1801446	24-07.3	Indianapolis DPW	Pedestrian Crash Focus Area 11C	Pedestrian Enhancement	Local	\$0	FEDERAL - HSIP < BR>	12/7/2022
1801437	24-00	Noblesville	Stony Creek Multi-Use Path Under SR 37	Pedestrian Enhancement	State	\$0	FEDERAL - TAP < BR>	9/14/2022
2312FFE	24-00	Noblesville	Midland Trace Trail - Phase 2 (10th Street to 19th Street)	Pedestrian Enhancement	Local	\$0		9/26/2023
2215FFE	24-00	Noblesville	Midland Trace Trail - Phase 1 (River Road to 10th Street)	Pedestrian Enhancement	Local	\$0		9/13/2022
2218FFE	24-02.1	Speedway	Crawfordsville Road Access Improvements	Pedestrian Enhancement	Local	\$0		12/19/2024
2200152	24-06.3	Carmel	96th St path from Ditch Rd to Spring Mill	Bicycle Enhancement	Local	\$0	FEDERAL - STBG < BR>	1/18/2024
2400598	24-07.56	Carmel	116th Street Path from Towne Road to Spring Mill	Bicycle Enhancement	Local	\$0		3/10/2027
1801423	24-07.3	Greenfield	Riley Literary Trail	Bicycle Enhancement	Local	\$0	FEDERAL - TAP < BR>	2/14/2024
2301657	24-10.1	Hancock County	Pennsy Trail from CR300W to CR150W	Bicycle Enhancement	Local	\$0		
1400944	24-02.2	Indianapolis DPW	Monon Bicycle/Pedestrian Bridge over 38th Street	Bicycle Enhancement	Local	\$0	FEDERAL - CMAQ < BR>	1/15/2020
2309FFE	24-09.3	Indianapolis DPW	Eagle Creek Greenway - Phase 2C	Bicycle Enhancement	Local	\$0		9/10/2025
1601003	24-07.3	Indianapolis DPW	Lower Fall Creek Trail - Phase 2	Bicycle Enhancement	Local	\$0	FEDERAL - STBG < BR>	9/15/2021
2200141	24-08.3	Indianapolis DPW	Eagle Creek Greenway - Phase B1	Bicycle Enhancement	Local	\$0	FEDERAL - STBG < BR>	5/9/2024
2308FFE	24-09.3	Indianapolis DPW	Eagle Creek Greenway - Phase 2B	Bicycle Enhancement	Local	\$0		9/10/2025
1901946	24-00	Indianapolis DPW	Bike Share Extension	Bicycle Enhancement	Local	\$0	FEDERAL - CMAQ < BR>	7/6/2023



MPO ADA projects TIP funding between 2024 and 2028 APPENDIX G

2024 UPDATE



Bloomington MPO: ADA Projects from TIP

Des Numbr	Contract N	Work Type	Location	Sponsor	Planning Area	District	Letting Date	Total Cost	Federal Amount	Match Amc	Federal %	Match %	Phase	Funding So	Status
2101681	44311	ADA Sidewalk Ramp Construction	Sidewalks around 2 elementary schools, Junior High School	Logansport	Statewide	LaPorte	2/10/2027	1854000	961000	240000	100	0	CN	Local Trans	Approved
2201292	44771	ADA Sidewalk Ramp Construction	At Various Locations within Leesburg, Piercetown, Cromwell	INDOT	Statewide	Fort Wayne	11/13/2025	645000	336000	84000	80	20	CN	Safety Con	Approved
2003029	44062	ADA Sidewalk Ramp Construction	New sidewalks along Campbell Ave, Weninger St, and Geor	North Judson	Statewide	LaPorte	3/11/2026	951000	581000	145000	100	0	CN	Local Trans	Approved
2201203	44714	ADA Sidewalk Ramp Construction	SR 262 in Rising Sun and SR 46 in Batesville	INDOT	Statewide	Seymour	10/9/2024	416000	193600	48400	80	20	CN	Safety Con	Approved
1802923	41971	ADA Sidewalk Ramp Construction	ADA ramp and sidewalk upgrade in NW and SE neighborhoc	Rochester	Statewide	LaPorte	10/12/2023	1002000	634000	158000	100	0	CN	Local Trans	Approved
2003064	43626	ADA Sidewalk Ramp Construction	Subdivisions of Lincoln Pointe, Donatello's Village and Bel S	Whitley County	Statewide	Fort Wayne	1/14/2026	3133000	2204000	551000	100	0	CN	Local Trans	Approved
2101695	44306	ADA Sidewalk Ramp Construction	New sidewalks in the Hubeny Neighborhood and along Carr	North Judson	Statewide	LaPorte	2/10/2027	924000	527000	132000	100	0	CN	Local Trans	Approved
2400128	R-42379	ADA Sidewalk Ramp Construction	US 20 from 2.13 miles W of SR 13 to SR 13	INDOT	Not Applicable	Elkhart	12/13/2025	791198	633158	158040	80	20	CN	Safety	Active
2400102	R-45285	ADA Sidewalk Ramp Construction	ADA Sidewalks on SR 120 and SR 15 in Bristol	INDOT	Not Applicable	Elkhart	10/9/2024	791300	633040	158260	80	20	CN	ST STBG	Active
2301579	R-45477	ADA Sidewalk Ramp Construction	SR 106 from West Jct US 6 to East Jct US 6	INDOT	Not Applicable	Marshall	3/11/2026	816000	652800	163200	80	20	CN	STP	Active
2001520	42969	ADA Sidewalk Ramp Construction	ADA RAMPS various locations within Elkhart, Fort Wayne, &	INDOT	Statewide	Fort Wayne	2/12/2025	932000	372000	93000	80	20	CN	Safety Con	Approved
2201294	44773	ADA Sidewalk Ramp Construction	At various locations within Bern (FY 2028 ADA)	INDOT	Statewide	Fort Wayne	11/10/2027	645000	336000	84000	80	20	CN	Safety Con	Approved
2301089	NULL	ADA Sidewalk Ramp Construction	From 5.1 mi S of SR 28 to 4.27 mi S of SR 28 (Linden)	INDOT	Statewide	Crawfords	NULL	0	-184136.8	-46034.2	80	20	CN	Safety Con	Internal Draft Approved
2400593	45629	ADA Sidewalk Ramp Construction	ADA Ramps for 2026	INDOT	Statewide	Greenfield	2/11/2026	734826	348000	87000	80	20	CN	Safety Con	Approved
2400066	45601	ADA Sidewalk Ramp Construction	SR 14 from US 31 to 1.33 Mi E of SR 25	INDOT	Statewide	LaPorte	7/14/2027	460000	216000	54000	80	20	CN	Safety Con	Approved
2400594	45630	ADA Sidewalk Ramp Construction	Various locations in the Greenfield District	INDOT	Statewide	Greenfield	7/8/2026	1117389	713911	178478	80	20	CN	Safety Con	Approved
2400595	45626	ADA Sidewalk Ramp Construction	ADA Ramps 2028	INDOT	Statewide	Greenfield	1/12/2028	1114159	160000	40000	80	20	PE	Safety Con	Approved
2400596	45631	ADA Sidewalk Ramp Construction	Various locations in Greenfield District	INDOT	Statewide	Greenfield	7/12/2028	1170344	188000	47000	80	20	PE	Safety Con	Approved
2301622	45504	ADA Sidewalk Ramp Construction	Richland ADA Pedestrian Improvements along Walnut, Lino	Richland	Statewide	Vincennes	7/8/2026	921000	189600	47400	100	0	PE	Local Trans	Approved
2301302	NULL	ADA Sidewalk Ramp Construction	Henryville Sidewalks - ADA improvements and extensions	Henryville	KIPDA	NULL	NULL	NULL	NULL	NULL	NULL	NULL	NULL	NULL	Active
1901957	42335	ADA Sidewalk Ramp Construction	Pedestrian Crash Focus Area 14 A	Indianapolis	IMPO	Greenfield	2/15/2024	2283864	301000	75000	100	0	CN	Indianapol	Approved
1901955	42333	ADA Sidewalk Ramp Construction	Pedestrian Crash Focus Area 14B	Indianapolis	IMPO	Greenfield	1/18/2024	1716744	-32404	-8101	100	0	CN	Indianapol	Approved

Evansville MPO: ADA Projects from TIP

Des #	Contract #	Work Type	Location	Project Status	Planning Area	Contract #	Work Category	Letting Date
2400672	R-45152	ADA Sidewalk Ramp Construction	SR 62 SR 62 from 3.92 mi W of S Jct US 41 Ingle Ave to 2.72 mi W of JCT US 41 (Wabash Ave).	Active	EMPO	45152	District Pavement Project (Non-I)	7/9/2025
2400696	R-44500	ADA Sidewalk Ramp Construction	SR 57 From 3.36 mi S of I 69 (N Green River Road) Intersection	Active	EMPO	44500	Intersection Improvement Project	9/15/2027

Lafayette MPO: ADA Projects from TIP

Des #	Contract #	Work Type	Location	Project Status	Planning Area	Contract #	Work Category	Letting Date
2301198	R-45054	ADA Sidewalk Ramp Construction	SP 52 From 1.43 miles E to 3.73 miles E (Wabash River) of US 52	Active	APCTC	45054	District Intersection Improvement Project	8/11/2027

2024 UPDATE



Indianapolis MPO: ADA Projects in Current IMPO TIP

TIP ID	TIP	LEAD	TITLE	PROJECT TYPE	TRANSIT SYSTEM	TOTAL COST	FED FUNDS	PROJECT MANAGER	LETTING DATE
2200132	24-11.1	Beech Grove	Hornet Greenway	Pedestrian Enhancement	Local	\$0		Brad Meriwether	12/13/2023
1601001	24-00	Indianapolis DPW	Safe Routes to Transit - North	Pedestrian Enhancement	Local	\$0	FEDERAL - HSIP < BR>	Ben Weber	11/10/2020
2400602	24-07.56	Indianapolis DPW	Nickel Plate Trail Pedestrian Bridge over K	Pedestrian Enhancement	Local	\$0		Rob Zolcak	7/8/2026
2307FFE	24-00	Indianapolis DPW	86th St Pedestrian Improvements from Me	Pedestrian Enhancement	Local	\$0		Jordan Williams	10/30/2025
1901955	24-06.3	Indianapolis DPW	Pedestrian Crash Focus Area 14B	Pedestrian Enhancement	Local	\$0	FEDERAL - HSIP < BR>	Ben Weber	1/18/2024
1901957	24-08.3	Indianapolis DPW	Pedestrian Crash Focus Area 14A	Pedestrian Enhancement	Local	\$0	FEDERAL - HSIP < BR>	Ben Weber	2/14/2024
1700936	24-00	Indianapolis DPW	Safe Routes to Transit - South	Pedestrian Enhancement	Local	\$0	FEDERAL - HSIP < BR>	Ben Weber	9/15/2021
1700903	24-02.2	Indianapolis DPW	Pedestrian Crash Focus Area 3	Pedestrian Enhancement	Local	\$0	FEDERAL - HSIP < BR>	Bob Zieles	11/17/2021
1801446	24-07.3	Indianapolis DPW	Pedestrian Crash Focus Area 11C	Pedestrian Enhancement	Local	\$0	FEDERAL - HSIP < BR>	Ben Weber	12/7/2022
1801437	24-00	Noblesville	Stony Creek Multi-Use Path Under SR 37	Pedestrian Enhancement	State	\$0	FEDERAL - TAP < BR>	Jim Hellmann	9/14/2022
2312FFE	24-00	Noblesville	Midland Trace Trail - Phase 2 (10th Street t	Pedestrian Enhancement	Local	\$0		Alison Krupski	9/26/2023
2215FFE	24-00	Noblesville	Midland Trace Trail - Phase 1 (River Road t	Pedestrian Enhancement	Local	\$0		Alison Krupski	9/13/2022
2218FFE	24-02.1	Speedway	Crawfordsville Road Access Improvements	Pedestrian Enhancement	Local	\$0		Robert Wetnight	12/19/2024
2200152	24-06.3	Carmel	96th St path from Ditch Rd to Spring Mill	Bicycle Enhancement	Local	\$0	FEDERAL - STBG < BR>	Littlejohn, David	1/18/2024
2400598	24-07.56	Carmel	116th Street Path from Towne Road to Spr	Bicycle Enhancement	Local	\$0		Littlejohn, David	3/10/2027
1801423	24-07.3	Greenfield	Riley Literary Trail	Bicycle Enhancement	Local	\$0	FEDERAL - TAP < BR>	Jason Koch	2/14/2024
2301657	24-10.1	Hancock County	Pennsy Trail from CR300W to CR150W	Bicycle Enhancement	Local	\$0		Gary Pool	
1400944	24-02.2	Indianapolis DPW	Monon Bicycle/Pedestrian Bridge over 38t	Bicycle Enhancement	Local	\$0	FEDERAL - CMAQ < BR>	Bob Zieles	1/15/2020
2309FFE	24-09.3	Indianapolis DPW	Eagle Creek Greenway - Phase 2C	Bicycle Enhancement	Local	\$0		Bill Kincius	9/10/2025
1601003	24-07.3	Indianapolis DPW	Lower Fall Creek Trail - Phase 2	Bicycle Enhancement	Local	\$0	FEDERAL - STBG < BR>	Bob Zieles	9/15/2021
2200141	24-08.3	Indianapolis DPW	Eagle Creek Greenway - Phase B1	Bicycle Enhancement	Local	\$0	FEDERAL - STBG < BR>	Ben Weber	5/9/2024
2308FFE	24-09.3	Indianapolis DPW	Eagle Creek Greenway - Phase 2B	Bicycle Enhancement	Local	\$0		Bill Kincius	9/10/2025
1901946	24-00	Indianapolis DPW	Bike Share Extension	Bicycle Enhancement	Local	\$0	FEDERAL - CMAQ < BR>	Zortman, Gretchen	7/6/2023



Terre Haute MPO: ADA Projects from TIP

Des #	Route	Sponsor	Work Category	Work Type	Contract Description	ADA Component
1810982	injoin Arteial (13th Street) and Major Collector (8th Ave.)	Terre Haute	Major Bridge Project	New Bridge Construction	Grade separation of 13th Street and 8th Avenue over CSX RR	Includes the reconstruction of sidewalks with ADA crossing and curb ramps at intersection along 13th street from 6th Avenue to Plum Avenue on east and west side of 13th Street. This includes grade appropriate ramps up to the bridge deck and round-about intersections. Reconstruction of sidewalks along 8th Avenue with ADA crossing and curb ramps at intersections from Garfield Street to 14th Street on south and north side of 8th Avenue. This includes grade appropriate ramps up to the bridge deck and round-about intersections. ADA crossing and curb ramps at the round-about intersections and sidewalks along both sides of the bridge deck
1901781	Primary Arteial (Clinton Road)	Vigo County	Local Road Project	Road Rehabilitation (3R/4R Standards)	Clinton Road from Park Avenue to Hasselburger Avenue	Includes the installation of sidewalk (8' to 10' wide sidepath) along east side with ADA crossing and curb ramps at the intersection of Shabur Avenue, Grant Avenue, Avalon Avenue, Emerald Avenue, and Hasselburger Avenue. Curb Ramps will also be required at all driveway cuts (approximatley 30).
2500593	Minor Arteial (Magaret Dr.)	Terre Haute	Local Road Project	Auxiliary Lanes, Two- way Left Turn Lanes	Margaret Drive from Harding Street to Prairieton Road	Includes the installation of sidewalks along the north side from Harding Street to Thompson Street with ADA crossing and curb ramps at 1st Street, Arleth Street, and Dilman Street; Reconstruction of sidewalk from Thompson Street to Jackson Street with ADA crossing and curb ramps at Thompson Street James Drive, Jefferson Street, Krumbhaar Street, and Jackson Street; sidewalk installation from Jackson Street to Prairieton Road; the north side includes 7 drive/alley cuts and a bus stop shelter which be installed per ADA regulations. the South side will include sidewalk installation from Harding Street to the US Postal Office with ADA crossing and curb ramps at 1st Street and across all drive cuts (approx. 4).
N/A	Primary Arteial (Lafayette Ave)	Terre Haute	Local Road Project	Road Rehabilitation (3R/4R Standards)	Lafayette Avenue from Fort Harrison to Lost Creek Bridge	Includes the installation of sidewalks along both sides of the roadways with ADA ramps and crossing at Kennedy Avenue, Prairie Avenue, 19th Street, Lee Avenue, 17th Street, Seeburger Avenue, Elizabeth Avenue, 16th Street, Carl Avenue, 15 1/2 Street, and Florida Avenue. Currently no DES# assigned as this is a 100% local funded PE phase with RW and CN anticipated to be 2030 or beyond (scope will likely change during PE phase).
1800193	US150	INDOT	District Pavement (Non-I)	Pavement Replacement	US 150 from 0.60 mi w of US 41 to US 41 (Cherry and Ohio Street in Terre Haute)	Reconstruction of sidewalks from Wabash River Bridges to US 41 with curb ramps and ADA crossing at 1st Street.



Indiana state ADA projects in TIP 2024-2028

Appendix H

2024 UPDATE

MPA State Projects
Sponsor

Des #	Project Name (limits/ITS)	Project Description	Funding Type & Total Cost	Funding Source INDOT / Federal	INDOT / Match	2026	2027	2028	INFO ONLY
2002415	INDOT	At various locations in Greenfield District	Traffic signals Modernization	STBG \$3,216,000	\$2,021,343	\$505,336			\$2,527,000
2200995	INDOT	District Wide Pedestrian Crossings	Safety Revisions	STBG \$1,615,000	\$771,628	\$192,907	\$965,000		
2201210	INDOT	Various Locations throughout Greenfield District	Signs, Lighting, Signals And Markings	STBG \$1,530,000	\$1,223,783	\$305,946	\$1,530,000		
2201247	INDOT	Statewide High Mast Tower Lighting Replacement at various intercha	Repair Or Replace Lighting	STBG \$4,620,000	\$3,113,272	\$778,318	\$3,892,000		
2200003	INDOT	Various locations in the Greenfield District	Signing Installation / Repair	STBG \$9,810,000	\$5,365,198	\$1,341,300	\$6,706,000		
2201707	INDOT	Greenfield & Crawfordsville District ITS & Signal Maintenance Contra	ITS Devices Maintenance Contracts	STBG \$532,000	\$425,451	\$106,363	\$532,000		
2301262	INDOT	Various locations in the Greenfield District	Traffic Signals, New Or Modernized	STBG \$1,806,000	\$1,204,936	\$301,234	\$1,506,000		
2301231	INDOT	Various Signal Locations in the Greenfield District	Traffic Signals Modernization	STBG \$5,337,000	\$4,269,940	\$1,067,485		\$5,337,000	
2301233	INDOT	HSIP Systemic Projects in the Greenfield District	Other Type Project (Miscellaneous)	STBG \$2,872,000	\$2,297,600	\$574,400		\$2,872,000	
2400079	INDOT	Various locations in the Greenfield District	Guardrail, Maintenance Or Repair	STBG \$1,000,000	\$800,000	\$200,000	#####		
2400080	INDOT	Various locations in the Greenfield District	Repair Or Replace Barrier Wall	STBG \$400,000	\$320,000	\$80,000	\$400,000		
2400601	INDOT	High Mast Tower and conventional Lighting Replacement at various l	Lighting Installation / Maintenance	STBG \$3,417,000	\$2,717,767	\$679,442		\$3,397,000	
2400625	INDOT	Various locations in the Greenfield District	Tree Removal/Trimming	STBG \$300,000	\$240,000	\$60,000	\$300,000		
2400605	INDOT	NEVI BP applicant sites	Other Type Project (Miscellaneous)	STBG \$6,000,000	\$2,400,000	\$600,000	#####		
2400610	INDOT	NEVI PILOT Locations	Other Type Project (Miscellaneous)	STBG \$4,500,000	\$1,800,000	\$450,000	#####		
2400612	INDOT	NEVI Tesla Locations	Other Type Project (Miscellaneous)	STBG \$4,160,370	\$1,635,750	\$408,938	#####		
2400593	INDOT	ADA Ramps for 2026	ADA Sidewalk Ramp Construction	STBG \$735,000	\$347,861	\$86,965	\$435,000		
2400594	INDOT	Various locations in the Greenfield District	ADA Sidewalk Ramp Construction	STBG \$4,224,000	\$713,911	\$178,478	\$892,000		
2400812	INDOT	Greenfield & Crawfordsville District ITS & Signal Maintenance Contra	ITS Devices Maintenance Contracts	STBG \$838,000	\$670,606	\$167,652	\$838,000		
2400826	INDOT	Greenfield & Crawfordsville District ITS & Signal Maintenance Contra	ITS Devices Maintenance Contracts	STBG \$1,071,000	\$856,579	\$214,145		\$1,071,000	
2001788	INDOT	Geotechnical on call - multiple locations throughout the state	Other Type Project (Miscellaneous)	STBG \$30,983,000	\$2,400,000	\$600,000	#####		
2002952	INDOT	Software License for Statewide ATMS for FY 26	ITS Program Contracted Services	NHPP \$500,000	\$450,000	\$50,000	\$500,000		
2002953	INDOT	Statewide TMC Dispatcher Operations Contract for FY 26	ITS Program Contracted Services	NHPP \$1,800,000	\$1,620,000	\$180,000	#####		
2002955	INDOT	Statewide O&M fee for CARS (Condition Acquisition & Reporting Syst	ITS Program Contracted Services	STBG \$500,000	\$400,000	\$100,000	\$500,000		
2002956	INDOT	Statewide INRIX Traffic Data for FY 26	ITS Program Contracted Services	NHPP \$1,200,000	\$1,080,000	\$120,000	#####		
2101642	INDOT	Post-Construction BMP Program Implementation / MS4 MCM5 - Vari	Other Type Project (Miscellaneous)	STBG \$3,318,000	\$885,840	\$221,460	#####		
2201179	INDOT	Statewide Cell Service for Communications for Signals and ITS Devic	ITS Operations And Maintenance Co	STBG \$1,250,000	\$1,000,000	\$250,000	#####		
2201180	INDOT	Statewide ITS Field Device Cell Hardware (Modem) Upgrades for FY 2	ITS Program Equipment	STBG \$350,000	\$280,000	\$70,000	\$350,000		
2300076	INDOT	Statewide Underwater Bridge Inspection FY-24 through FY-27	Bridge Inspections	STBG \$2,000,000	\$640,000	\$160,000	\$400,000	\$400,000	
2300077	INDOT	Statewide Vertical Clearance measuring over/under bridges. FY-202	Bridge Inspections	STBG \$900,000	\$160,000	\$40,000	\$100,000	\$100,000	
2301248	INDOT	Various locations within the Greenfield District	Traffic Signals Modernization	STBG \$4,811,000	\$3,848,969	\$962,242	\$4,811,000		
2400077	INDOT	HELPERS Program for Local Roads and Streets - Statewide LPA Traini	Other Type Project (Miscellaneous)	STBG \$1,335,000	\$806,794	\$201,699	\$328,000	\$336,000	\$344,000
2400095	INDOT	Noise Analysis Technical Review Support - Small Purchase Contract	Other Intersection Improvement	STBG \$500,000	\$200,000	\$50,000	\$250,000		
2400543	INDOT	Various bridges around the state.	Bridge Inspections	STBG \$500,000	\$400,000	\$100,000	\$500,000		
2400803	INDOT	Statewide ATMS Camera / Communications / Detection / DMS Repla	ITS Traffic Management Systems	NHPP \$300,000	\$270,000	\$30,000	\$300,000		
2400804	INDOT	Software License for Statewide ATMS for FY 27	ITS Program Contracted Services	NHPP \$500,000	\$450,000	\$50,000	\$500,000		
2400806	INDOT	Statewide TMC Dispatcher Operations & Engineering Support Contra	ITS Program Contracted Services	NHPP \$1,800,000	\$1,620,000	\$180,000	\$1,800,000		
2400807	INDOT	Statewide O&M fee for CARS (Condition Acquisition & Reporting Syst	ITS Program Contracted Services	STBG \$500,000	\$400,000	\$100,000	\$500,000		
2400808	INDOT	Statewide INRIX Traffic Data for FY 27	ITS Program Contracted Services	NHPP \$1,200,000	\$1,080,000	\$120,000	\$1,200,000		
2400809	INDOT	Statewide Cell Service for Communications for Signals and ITS Devic	ITS Operations And Maintenance Co	STBG \$1,250,000	\$1,000,000	\$250,000	\$1,250,000		
2400810	INDOT	Statewide ITS Field Device Cell Hardware (Modem) Upgrades for FY 2	ITS Program Equipment	STBG \$350,000	\$280,000	\$70,000	\$350,000		
2400818	INDOT	Statewide INRIX Traffic Data for FY 28	ITS Program Contracted Services	NHPP \$1,200,000	\$1,080,000	\$120,000		\$1,200,000	
2400819	INDOT	Software License for Statewide ATMS for FY 28	ITS Program Contracted Services	NHPP \$750,000	\$675,000	\$75,000		\$750,000	
2400820	INDOT	Statewide O&M fee for CARS (Condition Acquisition & Reporting Syst	ITS Program Contracted Services	STBG \$500,000	\$400,000	\$100,000		\$500,000	



NIRPC ADA Projects TIP 2024-2028

Appendix I

2024 UPDATE



Des #	Contract #	Work Type	Location	Sponsor	Project Sta	Contract #	Work Category	Letting Date	Comments			
1500421	R-39012	ADA Sidewalk Ramp Const	SP BIKE Northview elementary side	Valparaiso	Historical	39012	Local Trans	4/11/2018	in 2018-2022 TIP			
1500422	R-38913	ADA Sidewalk Ramp Const	IR 1023 Handley Elementary Safe R	La Porte	Historical	38913	Local Trans	5/9/2019	in 2018-2022 TIP			
1800173	R-40828	ADA Sidewalk Ramp Const	VA VARI ADA Curb Ramps Along US	INDOT	eliminated	40828	Other Proj	3/9/2022 3/9/2021	in 2020-2024 TIP Letting date 2021, (not 2022)			
1902005	R-42418	ADA Sidewalk Ramp Const	ST 1001 ADA improvements to vario	La Porte	Active	42418	Local Other	5/10/2023	deleted in Amend 22-10 (2022-2026 TIP)			
1902007		ADA Sidewalk Ramp Const	ST 1001 ADA sidewalks in various lo	La Porte	Active		Local Other Type Project		added into 1902008			
2101654	R-45604	ADA Sidewalk Ramp Const	VA VARI SR 2 from W JCT of SR39 to	INDOT	Active	45604	Other Proj	8/9/2028				
2400600	R-45602	ADA Sidewalk Ramp Const	US 231 From N. Jct of SR 2 to I65	INDOT	Active	45602	Other Proj	7/14/2027				
2101110	R-43999	Complete Strteets/Sidewa	Sidewalks on 133rd a& Morris	Cedar Lak	Active	43999	Local Trans	3/11/2028				
2101118	R-44001	Pavement Reconstruction	Michigan Ave Reconstruction Ph 1	East Chica	Active	44001	Local Road	3/15/2026				
2300726	R-45020	Pavement Reconstruction	Michigan Ave Reconstruction Ph 2	East Chica	Active	45020	Local Road	11/15/2027				
1902830	R-42650	Signal- Other	5th & Bigger	Gary	Active	42650	Signal Proj	11/15/2025				
1901902	R-42312	Pavement Reconstruction	Kennedy Ave from Toll Rd to 164th P	Hammond	Active	42312	Local Road	12/15/2024				
2002586	R-43589	Pavement Reconstruction	Hohman Ave between Russell St & C	Hammond	Active	43589	Local Road	9/14/2024				
2300727	R-45014	Pavement Reconstruction	Columbia Ave from 164th to River D	Hammond		45014	Local Road	11/15/2027				
1902008	R-42421	Resurfacing	Throughout the city	La Porte	Active	42421	Local Road	1/10/2024				
1902818	R-42836	Bike/Pedestrian Facilities	Sidewalks on Hamilton & Water St	La Porte C	Active	42836	Local Trans	1/14/2027				
1901951	R-42316	Pedestrian Enhancement	Sidewalks around Pierce Middle Sch	Merrillville	Active	42316	Local Trans	12/10/2027				
1902691	R-42613	Bike/Pedestrian Facilities	Washington Park Blvd; US 12 to Lak	Michigan C	Active	42613	Local Trans	12/10/2027				
2101132	R-44007	Pavement Reconstruction	Main St from Columbia Ave to Hart C	Munster	Active	44007	Local Road	12/10/2026				
1173430	R-45022	Roadway Reconstruction/I	Central Ave from Willowdale to Swa	Portage	Active	45022	Local Road	10/9/2024				
2100131	R-44057	Roadway Reconstruction/I	Central Ave from County Line Rd to I	Portage	Active	44057	Local Road	11/15/2027				
2300657	R-45015	Bike/Pedestrian Facilities	Indiana Ave from Lafayette St & Mor	Valparaiso	Active	45015	Local Trans	11/15/2027				
1901947	R-42314	Pedeastian Enhancement	Randolph St from elementary schoo	Winfield	Active	42314	Local Trans	3/15/2026				



Local NIRPC ADA Projects TIP 2024-2028

Appendix J

2024 UPDATE



Project Information	DES #	Estimated Cost Fiscal Year						Other Year	Federal Funds	Local Funds	Contract # Funding Letting	TIP Date ICG Date AQ Finding
		2024	2025	2026	2027	2028*						
*Amstutz Road	1801426							2019	\$200,000	\$50,000	R-41299	6/30/2023
Hosler Rd to Leo Rd/SR 1	1801426							2020	\$291,500	\$72,875	STBG & CRRSA	
	1801426							2023	\$396,000	\$99,000		
(1901912 Nettlehorst Ditch Bridge)	1801426	\$5,865,742							\$4,679,873	\$1,185,869		
(1901913 Ped Bridge)	1901912	\$3,482							\$2,786	\$696		
	1901913	\$298,751							\$239,001	\$59,750		
Road Reconstruction: 2 lane roadway w/CTL as needed with	1801426	\$562,500							\$450,000	\$112,500	12/13/2023	exempt
Total cost for project:	\$7,839,850	\$6,730,475	\$0	\$0	\$0	\$0			\$6,259,160	\$1,580,690		
*Ardmore Avenue									\$0	\$0		6/30/2023
Covington Rd to Lower Huntington Rd			\$6,973,055						\$5,578,444	\$1,394,611		
						\$437,500			\$350,000	\$87,500		
Phase 1: Covington Rd to s/o Engle Rd	2300603							2029	\$2,200,000	\$550,000	STBG	
Phase 2: s/o Engle Rd to s/o Sand Point Rd (south)								2032	\$5,500,000	\$1,375,000		
Phase 3: s/o Sand Point Rd (south) to Lower								2032	\$100,000	\$25,000		
Huntington Rd								3034	\$5,500,000	\$1,375,000		
Road Reconstruction and Widening: from two								3034	\$100,000	\$25,000	10/8/2031	non-exempt
lanes to four/five lane roadway with pedestrian								3036	\$5,500,000	\$1,375,000		
facilities								3036	\$100,000	\$25,000		
Total cost for project:	#####	\$0	\$6,973,055	\$0	\$0	\$437,500			\$24,928,444	\$6,232,111		
Carroll Road	1801749							2019	\$200,000	\$50,000		6/30/2023
SR 3/Lima Rd to Coral Springs & Shearwater	1801749							2023	\$321,493	\$80,373		
Run (east section) #1801749	1801748							2023	\$261,611	\$65,403		
e/o Bethel Rd to Millstone Dr (west section)	1801749	\$2,087,500							\$1,670,000	\$417,500	STBG & CRRSA	
#1801748	1801748	\$1,187,500							\$950,000	\$237,500		
Road Reconstruction: 2 lane roadway	1801748 & 18	\$312,500							\$250,000	\$62,500	5/10/2023	exempt
w/CTL as needed, with pedestrian facilities												
Total cost for project:	\$4,566,380	\$3,587,500	\$0	\$0	\$0	\$0			\$3,653,104	\$913,276		
*Clinton Street		\$5,022,500							\$4,018,000	\$1,004,500	R-42160	6/30/2023
Auburn Rd to Mayhew Rd				\$1,393,750					\$1,115,000	\$278,750		
					\$2,162,500				\$1,730,000	\$432,500		
Phase 1: Riveroak Dr to Diebold Rd								2029	\$9,440,000	\$2,360,000	STBG & CRRSA	
Phase 2: Auburn Rd to Riveroak Dr	1901703							2029	\$944,000	\$236,000		
								2030	\$9,440,000	\$2,360,000		
Phase 3: Diebold Rd to Mayhew Rd								2030	\$100,000	\$25,000		
								2031	\$844,000	\$211,000	11/15/2028	non-exempt
Added Travel Lanes: expanding to 4 lane								2032	\$7,600,000	\$1,900,000		
roadway with pedestrian facilities								2032	\$100,000	\$25,000		
Total cost for project:	#####	\$5,022,500	\$0	\$1,393,750	\$2,162,500	\$0			\$35,331,000	\$8,832,750		
*Grabill Road Bridge over St. Joe River (#1901712)	1901712							2022	\$0	\$500,000	R-42165	6/30/2023
	1901712							2023	\$150,000	\$37,500		
and SR 1/Leo Rd at Clay St (#2200137)	1901712	187,500							\$150,000	\$37,500		
1=Group 1 STBG	2200137		329,408						\$2,635,261	\$658,823	1=Group 1 STBG	

2024 UPDATE



2=INDOT STBG	2200137		329,408				\$2,635,262	\$658,822	2=INDOT STBG
3=Allen County STBG & TA	2200137		31,500				\$252,001	\$63,003	3=Allen County
	2200137		31,500				\$252,002	\$63,002	STBG & TA
Bridge Reconstruction: existing 2 lane bridge	1901712		6,140,000				\$4,912,000	\$1,228,000	8/14/2024 exempt
with pedestrian facilities (#1901712)	1901712		700,000				\$560,000	\$140,000	
Intersection Improvement, Signalized (#2200137)	1901712		657,500				\$526,000	\$131,500	
Total cost for project:	\$9,094,316	\$187,500	\$8,219,316	\$0	\$0	\$0	\$6,875,452	\$2,218,864	
Gump Road and State Road 3/Lima Road					\$250,000		\$200,000	\$50,000	6/30/2023
Pedestrian bridge over SR 3/Lima Road	2300620					\$437,500	\$350,000	\$87,500	STBG & CRRSA
							2029	\$500,000	\$125,000
							2031	\$8,000,000	\$2,000,000
							2031	\$800,000	\$200,000
									10/9/2030 exempt
Pedestrian Bridge							\$9,850,000	\$2,462,500	
Total cost for project:	#####	\$0	\$0	\$0	\$250,000	\$437,500			
*Hillegas Road	1901705						2022	\$1,720,000	\$430,000
State Blvd to Coliseum Blvd	1901705	\$1,875,000						\$1,500,000	\$375,000
Phase 1: State Blvd to approx 400' n/o Butler Rd	2201692			\$9,400,000				\$7,520,000	\$1,880,000
Phase 2: approx 400' n/o Butler Rd to Coliseum Blvd	2201692			\$550,000				\$440,000	\$110,000
Added Travel Lanes: expanding to 4 lane	1901705				\$9,500,000			\$7,600,000	\$1,900,000
roadway with pedestrian facilities	1901705				\$550,000			\$440,000	\$110,000
Total cost for project:	#####	\$1,875,000	\$0	\$9,950,000	\$10,050,000	\$0		\$19,220,000	\$4,805,000
*Illinois Road Trail							2023	\$0	\$131,595
A 1.97 mile trail along Illinois Road from Getz Rd		\$115,000						\$0	\$115,000
to W. Jefferson Blvd/Willowdale Dr			\$4,447,210					\$1,000,000	\$3,447,210
New Trail									
Total cost for project:	\$4,693,805	\$115,000	\$4,447,210	\$0	\$0	\$0		\$1,000,000	\$3,693,805
*Kell Road Bridge #46							2021	\$106,814	\$26,704
Bridge over Willow Creek							2023	\$24,000	\$6,000
(INDOT funded project)	1902837		\$1,020,927					\$640,000	\$380,927
Bridge Deck Overlay & Widening for Pedestrian Facilities			\$120,000					\$96,000	\$24,000
Total cost for project:	\$1,304,445	\$0	\$1,140,927	\$0	\$0	\$0		\$866,814	\$437,631
*Pufferbelly Trail Bridge							2023	\$40,000	\$10,000
Pedestrian bridge over Coliseum Blvd/SR 930		\$700,000						\$0	\$700,000
(approx. 1,150' e/o Lima Rd/US 27)	2201280		\$500,000					\$0	\$500,000
				\$7,000,000				\$0	\$7,000,000
New Pedestrian Bridge				\$750,000				\$0	\$750,000
Total cost for project:	\$9,000,000	\$700,000	\$500,000	\$7,750,000	\$0	\$0		\$40,000	\$8,960,000