

CATEGORICAL EXCLUSION LEVEL 1 FORM

Date: November 25, 2020

Initial Version

Additional Information to CE Level 1 Dated: _____

Purpose of this document:

CE Level 1 documentation for exempted projects

State-funded categorical exemption documentation

Approval CE Level 1 or State-Funded CE:

Environmental Scoping Manager or
Environmental Policy Manager

Date

Release for Public Involvement

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ESM/ES Initials

Date

Certification of Public Involvement

Office of Public Involvement

Date

PROJECT INFORMATION

County, Route

Decatur County, SR 3 and SR 46/CR 250W

Des Number

1700050

Purpose and Need:

Need:

The need for the project is the above average crash severity at the existing intersection. There are a higher than average number of crashes at the SR 3 and SR 46 northern junction and those crashes are more severe. Per RoadHAT 3.0 analysis (traffic engineering safety modeling software) of 2014-2016 crash data, the most recent data available at the time of this analysis, the index of crash frequency is 0.27 (above average) and the index of crash cost is 1.92 (well above average). These indices compare the crash cost and crash frequency for this intersection to intersections with similar volumes, roadway classification, and control type throughout Indiana. The index of crash cost exceeds the threshold of 1.5 set by INDOT's Office of Traffic Safety, prompting further study. The property damage only (PDO) equivalent crash frequency is a weighted measure of crash severity level, based on a ratio of factors. Based upon the crash analysis, and following methodology set by the INDOT Intersection Decision Guide, the PDO equivalent crash frequency of the intersection is 34 crashes annually.

Crash Data			
Crash Data for SR 3 and SR 46 (2014 thru 2016)			
Severity		Type	
Crash with no Injury	9	Rear End	7
Non-Incapacitating Injury	3	Right Angle	4
Incapacitating Injury	5	Sideswipe	2
Fatal	1	Left Turns/Right Turns	5
		Other	
TOTALS	18	TOTALS	18

Purpose:

The purpose of the project is to reduce the PDO equivalent crash frequency by more than 15 crashes.

Project

The Indiana Department of Transportation (INDOT), with funding from the Federal Highway Administration

Description:	<p>(FHWA), is proposing to proceed with improvements to the State Road (SR) 3 and SR 46 north junction.</p> <p><u>Project Location:</u> The project is located at the north junction of SR 3 and SR 46, west/southwest of Greensburg, in Washington Township, Decatur County, Indiana. The project extends approximately 800 feet (ft) northeast of the intersection to approximately 700 ft southwest of the intersection along SR 3 and approximately 890 ft northwest of the intersection along County Road (CR) 250W (Base Rd) to approximately 555 ft southeast of the intersection along SR 46. Specifically, the project is located within Section 9 and 10, Township 10 North, Range 9 East as shown on the attached 7.5 minute Forest Hill, Indiana, United States Geological (USGS) quadrangle map (Appendix B, Page B-2).</p> <p><u>Existing Conditions:</u> SR 3 is classified as a Principal Arterial road which is designated as part of the National Highway System and the National Truck Network. The road has a posted speed limit of 55 miles per hour (mph). The existing road through the project area is a north-south, four-lane, divided highway. The road consists of 12 ft travel lanes, 10 ft paved shoulders, 11 ft useable shoulders, and a 48 ft grass median. At the intersection, in both directions, SR 3 has a left turn lane, two through lanes, and a right turn lane. A railroad spur, at-grade crossing exists on the SR3/SR 46 northbound approach approximately 700 ft from the intersection. Cantilevered RR signal arms are present at the grade crossing in both directions.</p> <p>SR 46 is classified as a Principal Arterial and has a posted speed limit of 45 mph. The existing road within the project area is an east-west two-lane highway. The road consists of 12 ft travel lanes, 10 ft paved shoulders, and 11 ft usable shoulders. At the intersection, SR 46 has a left turn lane, a through lane, and a right turning roadway.</p> <p>CR 250W is classified as a minor collector road and has a posted speed limit of 45 mph. The existing road within the project area is an east-west two-lane highway. The road consists of 12 ft travel lanes, 4 ft paved shoulders, and 8 ft usable shoulders. At the intersection, CR 250W has a left turn lane, a through lane, and a right turning lane.</p> <p>Land use in the project vicinity consists of agricultural land and commercial land uses. The project is also located near the Greensburg Municipal Airport, Decatur County Fairgrounds, Greensburg City Park, and a combined feeding operation (CFO). A railroad travels east to west, abutting, though not included within, the southern project limits. A number of utilities were noted throughout the project area, including underground fiber optic, electric, and pipeline. Maps and photographs of the area can be found in Appendix B, Pages B-1 to B-12</p> <p><u>Preferred Alternative:</u> The preferred alternative will reconfigure the existing intersection as an unsignalized Median U-turn (MUT) intersection. The reconfiguration will close the median of SR 3 at the intersection and provide left-turn lanes and openings in the median for U-turns along SR 3, 800 ft north of the intersection and 700 ft south of the intersection. The existing left- and right-turn lanes in the eastbound CR 250W and southbound SR 3 approaches will be removed and will be replaced with standard right turn lanes. Cross culverts will be removed where the ramps will be removed and culvert extensions will be placed where the roads are widened. New pavement will be placed for the shoulders and the new turn lanes. All remaining existing pavement will be milled and resurfaced. Curbed islands will be added on SR 46 and CR 250W. Excavation depths will vary but are expected to range from 3-5 feet.</p> <p><u>Maintenance of Traffic (MOT):</u> The Maintenance of Traffic (MOT) will involve lane closures and traffic will be maintained throughout construction. At the start of construction, traffic will be shifted to the inside lanes on SR 3, then shifted to the outside lanes of SR 3 until construction is complete.</p> <p><u>Purpose and Need:</u> Based upon the crash analysis, and following the methodology set by the INDOT Intersection Decision Guide, the PDO equivalent crash reduction of the MUT intersection is 18 and therefore meets the project purpose.</p> <p><u>Independent Utility/Logical Termini:</u> This improvement is warranted by the above average crash severity at the existing intersection. The proposed intersection reconfiguration will independently address the above average crash severity at this intersection without requiring other improvements, which may have additional impacts. The intersection improvement has logical termini, in that each leg is extended only as far as necessary to connect the modified lanes of each</p>
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	<p>approach back to the existing roadways.</p>		
<p>Other Alternatives Considered:</p>	<p><u>Restricted Crossing U-Turn with Left Turn Lanes (RCUT):</u> This alternative would consist of reconfiguring the existing intersection as an RCUT; closing the median along SR 3 to through traffic for SR 46 and CR 250W and provide median u-turns to the north and south of the intersection. The median would allow left turns at the intersection for vehicles along SR 3. The eastbound-southbound (EB-SB) and northbound-eastbound (NB-SB) turn lanes would be removed. The culvert under the eastbound-southbound turning roadway would be removed and reset next to the existing culvert under CR 250W to allow for a widened pavement width for the addition of a right turn lane. Curbed islands would be added on SR 46, CR 250W, and the center turning lanes along SR 3.</p> <p>This alternative would not require any ROW. The overall cost of this alternative is estimated to be \$1,007,525.</p> <p>This alternative would meet the purpose and need, reducing the PDO equivalent crash frequency by more than 15 crashes. However, the RCUT alternative would require design changes that would reduce deceleration speeds in the left turn lane, which would require the project to extend southwest across the railroad tracks. During on-going design, the pavement removal cost for this alternative increased. These pavement costs and additional railroad coordination costs increased the estimated cost of this alternative to be greater than that of the Preferred Alternative. Therefore, the RCUT alternative is not prudent and was dismissed from further consideration.</p> <p><u>T-Intersection with CR 250W Cul-de-Sac:</u> This alternative would consist of reconfiguring the existing intersection as a T-Intersection by converting the CR 250W approach to a cul-de-sac. The EB-SB and NB-EB turn lanes would be removed for this alternative. The culvert under the EB-SB turning roadway would be removed and reset next to the existing culvert under CR 250W to allow for a widened pavement width for the addition of a cul-de-sac. Vehicles that want to access CR 250W would have to divert to either CR 350W or US 421.</p> <p>This alternative would not require any ROW. The overall cost of this alternative is estimated to be \$751,250. A Level 3 design exception would be required for the turn lane length on the northbound SR 3 right turn lane.</p> <p>This alternative is feasible; however, it does not reduce the PDO equivalent crash frequency by more than 15 crashes, and therefore does not meet the purpose and need. For the stated reasons, this alternative was dismissed from further consideration.</p> <p><u>No Build:</u> The “Do Nothing” alternative was considered for the project. This alternative proposed utilization of the existing intersection with no expenditure of capital funds or improvement. However, the “Do Nothing” alternative would not address the purpose and need of the project, which is to reduce the PDO equivalent crash frequency by more than 15 crashes. Therefore, for the stated reasons, the “Do Nothing” alternative was dismissed from further consideration.</p>		
<p>Project Termini:</p>	<p>800 ft. northeast of the intersection to 700 ft. southwest of the intersection along SR 3 and 890 ft. northwest of the intersection along County Road (CR) 250 (Base Rd.) to 555 ft. southeast of the intersection along SR 46.</p>		
<p>Funding Source(s):</p>	<p><input checked="" type="checkbox"/> Federal <input checked="" type="checkbox"/> State <input type="checkbox"/> Local <input type="checkbox"/> Other</p>	<p>Estimated Cost</p>	<p>\$1,449,782.00 (2022)</p>
<p>Project Sponsor:</p>	<p>Indiana Department of Transportation</p>	<p>Project Length</p>	<p>0.32 miles</p>

Name and organization of CE Level 1 Preparer: Mackenzie Knotts, Environmental Scientist, CHA Consulting, Inc.

INDOT ES/District Env. Reviewer Signature: _____ Date: _____

SCOPE OF THE PROPOSED ACTION			
Public Involvement*		No:	Yes: <input checked="" type="checkbox"/>
Comments:	<p>Notice of Survey letters were not required due to the project occurring within existing right-of-way.</p> <p>The project will meet the minimum requirements described in the current <i>Indiana Department of Transportation (INDOT) Public Involvement Manual</i> or other INDOT policy which requires the project sponsor to offer the public an opportunity to submit comment and/or request a public hearing. Therefore, a legal notice will appear in a local publication contingent upon the release of this document for public involvement. This document will be revised after the public involvement requirements are fulfilled.</p>		
Right-of-way (permanent and temporary, in acres)		No: <input checked="" type="checkbox"/>	Yes:
Comments:	<p>The apparent existing ROW for SR 3 varies from 140 to 160 feet on either side from the center line. The apparent existing ROW for SR 46 varies from 125 to 200 feet on either side from the center line. The apparent existing ROW for CR 250W varies from 110 to 200 feet on either side of the center line. The existing ROW consists of roadside drainage, agricultural, and residential use.</p> <p>This project will occur within existing right-of-way (ROW). No permanent or temporary ROW will be required for this project.</p>		
Disruption to public facilities/services (such as schools, emergency service)		No: <input checked="" type="checkbox"/>	Yes:
Comments:	<p>Based on a desktop review, a site visit on October 14, 2019 by CHA Consulting, Inc., the aerial map of the project area (Appendix B, page B-3) and the Red Flag Investigation (RFI) report (Appendix E, pages E-9) there are ten (10) public facilities within the 0.5 mile search radius. There are no public facilities within or adjacent to the project area. Access to all properties will be maintained during construction. Therefore, no impacts are expected.</p> <p>The MOT will occur in five phases with traffic being maintained throughout construction. In the first phase, during the construction of the u-turn lanes and right turn lanes from SR 46 and CR 250W the traffic will be shifted to the inside lanes on SR 3. In the second phase, traffic will be shifted to the outside lanes on SR 3 while the median pavement is being constructed. In the third phase, SR 3 traffic will remain in the outside lanes while construction is finalized on the median at the existing intersection. In the fourth phase, the right turn lane on CR 250W and the island on SR 46 will be constructed. In the fifth phase, the existing right turn lane pavement will be removed along CR 250W. The closures/lane restrictions will pose a temporary inconvenience to traveling motorists (including school buses and emergency services); however no significant delays are anticipated, and all inconveniences will cease upon project completion.</p> <p>Early coordination letters were sent to INDOT Office of Aviation on April 3, 2020. The Office of Aviation responded on April 3, 2020 with recommendations to avoid or minimize impacts to Greensburg Airport (Appendix C, page C-16 to C-17). The Office of Aviation noted that the southwest end of the project overlaps the aircraft approach to the adjacent Greensburg Airport. Therefore, a tall structure permit from the Federal Aviation Administration (FAA) is required for any object, obstruction, or equipment that will exceed 6 feet in height. All applicable Office of Aviation recommendations are included in the Environmental Commitments sections of this CE document.</p> <p>It is the responsibility of the project sponsor to notify school corporations and emergency services at least two weeks prior to any construction that would block or limit access.</p>		
Involvement with existing bridge(s) (Include structure number(s))		No: <input checked="" type="checkbox"/>	Yes:
Comments:	<p>The existing twin 34" x 22" maintenance pipes under the eastbound-southbound (EB-SB) ramp will be removed and reset next to the existing maintenance pipes under CR 250W. Additionally, there are a number of small pipes (36 inches or smaller in diameter) throughout the project area that are used to carry drainage. These will remain in place and no impact will occur. These pipes are identified in the plans found in Appendix B, pages 16-17.</p>		

* Limited public involvement, CE-1 level projects will typically have no public hearing opportunity offered.

INVOLVEMENT WITH RESOURCES			
Streams, Rivers, and Watercourses Impacted (linear feet)	No: <input checked="" type="checkbox"/>	Yes: <input type="checkbox"/>	Possible: <input type="checkbox"/>
<p>Comments:</p> <p>Based on a desktop review, a site visit on October 14, 2020 by CHA Consulting, Inc., the aerial map of the project area (Appendix B, page B-3), and the water resource map in the RFI report (Appendix E, pages E-10) there are ten (10) stream segments located within the 0.5 mile search radius. There is one unnamed tributary present within the project area.</p> <p>The Federal Wild and Scenic Rivers listing, State Natural, Scenic, and Recreational Rivers listing, the Nationwide Rivers Inventory, Outstanding Rivers List for Indiana, and the United States Army Corps of Engineers (USACE) list of Navigable Waters were reviewed by CHA Consulting, Inc. to determine possible presence in the project area. No listed waterways were identified within or adjacent to the project area. Therefore, no impacts are expected.</p> <p>A <i>Waters of the U.S. Determination/Wetland Delineation Report</i> was completed for the project on March 31, 2020. INDOT Ecology and Waterway Permitting office approved the report on April 7, 2020. Please refer to Appendix F, pages F-1 to F-26 for the <i>Waters of the U.S. Determination/Wetland Delineation Report</i>. It was determined that one Unnamed Tributary (UNT 1) was located directly adjacent to the project area. UNT 1 was identified as an ephemeral stream and exhibits an Ordinary High Water Mark (OHWM) of 1 ft. wide and 0.25 ft. deep. UNT 1 is considered a poor quality stream due to surrounding agricultural land use, narrow riparian buffer, and little aquatic habitat. The small ephemeral stream appears to be connected through an impounded pond to the southeast of the project area that outlets to Muddy Fork Sand Creek, a Relatively Permanent Water (RPW) and Waters of the U.S. Due to this connection, UNT 1 would likely be considered jurisdictional. The U.S. Army Corps of Engineers (USACE) makes all final determinations regarding jurisdiction. No impacts to UNT 1 are anticipated.</p> <p>Early coordination letters were sent to the National Park Service (NPS), USACE, Indiana Department of Environmental Management (IDEM) and Indiana Department of Natural Resources, Division of Fish and Wildlife (IDNR) on April 3, 2020 (Appendix C-1 to C-5). Coordination with IDEM was accomplished electronically through the standardized environmental review letter process (http://www.in.gov/idem/5284.htm) on April 3, 2020 (Appendix C, pages C-9 to C-15).</p> <p>The NPS and the USACE did not respond to the early coordination letter. The standard IDEM letter noted the need for Section 401 and 404 permitting for fill or excavation in a regulated water and also made the recommendation to minimize stream and riparian vegetation to only what is absolutely necessary to complete a project (Appendix C, pages C-9 to C-15).</p> <p>The IDNR responded on May 1, 2020 and indicated that the formal approval by the Department of Natural Resources under the regulatory programs administered by the Division of Water is not required for this project. Additionally, the IDNR had recommendations for minimizing impacts to fish, wildlife, and botanical resources (Appendix C, page C-20). These recommendations included temporary erosion control techniques and revegetation techniques.</p> <p>All applicable IDEM and IDNR recommendations are included in the Environmental Commitments section of this CE document.</p>			
Wetlands (acres)	No: <input checked="" type="checkbox"/>	Yes: <input type="checkbox"/>	Possible: <input type="checkbox"/>
<p>Comments:</p> <p>Based on a review of the National Wetlands Inventory (NWI) online mapper (https://www.fws.gov/wetlands/data/Mapper.html), a site visit on October 14, 2019 by CHA Consulting, Inc, the USGS topographic map (Appendix B, page B-2), and the RFI report (Appendix E, page E-10) there are five wetlands located within the 0.5 mile search radius. There is one wetland present within or adjacent to the project area.</p> <p>A <i>Waters of the U.S. Determination / Wetland Delineation Report</i> was completed on March 31, 2020. INDOT Ecology and Waterway Permitting Office approved the report on April 7, 2020. Please refer to Appendix F, pages F-1 to F-26 for the <i>Waters of the U.S. Determination / Wetland Delineation Report</i>. It was determined that one emergent wetland, Wetland A, is adjacent to the project area. The USACE makes all final determinations regarding jurisdiction.</p> <p><u>Wetland A:</u> Wetland A is a small emergent wetland that is 0.145 acre in size. This wetland is east of the culvert under the north side of SR-46 and west of the culvert under SR 3. The wetland is considered poor quality based on the small size, the surrounding agricultural land use, the presence of invasive species, and its proximity to the highway and the utilization as a roadside ditch. This wetland is connected through a UNT to Greensburg City Park Lake eventually connecting to Muddy Fork Sand Creek. Due to this connection, Wetland A would likely be considered a Waters of the U.S. and will</p>			

INVOLVEMENT WITH RESOURCES			
	<p>be under the jurisdiction of the USACE. The project limits do not extend into the boundaries of Wetland A. Therefore, no impacts are expected.</p> <p>As noted in the “Streams, Rivers, Watercourses & Jurisdictional Ditches” section of this document, early coordination packets were submitted to the NPS, IDEM, USACE, and IDNR on April 3, 2020. The USACE and NPS did not respond to early coordination.</p> <p>The IDNR responded on May 1, 2020 with a number of recommendations to help avoid and minimize impacts to fish, wildlife, and botanical resources (Appendix C, page C-20). These recommendations generally include erosion control techniques and revegetation techniques.</p> <p>Coordination with IDEM was accomplished electronically through the standardized environmental review letter process (http://www.in.gov/idem.5284.htm) on April 3, 2020 (Appendix C, pages 33-39). The standard IDEM letter noted the need for Section 401 and 404 permitting for fill or excavation in a regulated water.</p> <p>All applicable IDEM and IDNR recommendations are included in the Environmental Commitments section of this CE document.</p>		
Disturbance of Terrestrial Habitat (acres)		No:	Yes: <input checked="" type="checkbox"/> Possible:
Comments:	<p>Based on a desktop review, a site visit on October 14, 2019 by CHA Consulting, Inc., and the aerial map of the project area (Appendix B, page B-3), there is mowed right-of-way consisting of grasses and forbs adjacent to the project area. No trees or shrubs are located in or adjacent to the project area. The project area mainly consists of existing roadway. The total amount of ground disturbance within the construction limit of the project is anticipated to be 9.87 acres.</p> <p>As noted in the “Streams, Rivers, Watercourses & Jurisdictional Ditches” section of this document, early coordination packets were submitted to the NPS, IDEM, USACE, USFWS, and IDNR on April 3, 2020. These agencies responded providing a number of recommendations to help avoid and minimize impacts to fish, wildlife, and botanical resources. All applicable IDEM and IDNR recommendations are included in the Environmental Commitments section of this CE document.</p>		
Karst Features		No: <input checked="" type="checkbox"/>	Yes: Possible:
Comments:	<p>Based on a desktop review, the project is located outside the designated karst region of Indiana as outlined in the October 13, 1993 Memorandum of Understanding (MOU). According to the topo map of the project area (Appendix B, page B-2) and the RFI report (Appendix E, page E-10) there are no karst features identified within or adjacent to the project area. In the early coordination response, the Indiana Geological Survey (IGS) did not indicate that karst features exist in the project area (Appendix C, pages C-6 to C-8). The IGS identified a moderate liquefaction potential and floodway as geological hazards, a high potential for bedrock resources, and a low potential for sand and gravel resources. As for potential abandoned mineral resources, the IGS noted petroleum exploration wells and abandoned industrial mineral quarries. These features will not be affected because no active or abandoned mineral resources extraction sites were documented and no karst features were identified within the project area. Response from IGS has been communicated with the designer on April 6, 2020. No impacts are expected.</p>		
Threatened and Endangered Species		No:	Yes: <input checked="" type="checkbox"/> Possible:
Comments:	<p>Based on a desktop review and the RFI report (Appendix E), completed by CHA Consulting, Inc., on April 16, 2020, the IDNR Decatur County Endangered, Threatened and Rare (ETR) Species List has been checked and is included in (Appendix E, page E-14). The highlighted species on the list reflect the federal and state identified ETR species located within the county. According to the IDNR-DFW early coordination response letter dated May 1, 2020 (Appendix C, page C-20), the Natural Heritage Program’s Data has been checked and to date, no plant or animal species listed as state or federally threatened, endangered, or rare have been reported to occur in the project vicinity.</p> <p><u>Indiana Bat and Northern Long-Eared Bat:</u> Project information was submitted through the USFWS’s Information for Planning and Consultation (IPaC) portal, and an official species list was generated (Appendix C, pages C-33 to C-38). The project is within range of the federally endangered Indiana bat (<i>Myotis sodalis</i>) and the federally threatened northern long-eared bat (NLEB) (<i>Myotis septentrionalis</i>). No additional species were found within or adjacent to the project area other than the Indiana bat and northern long-eared bat.</p> <p>The project qualifies for the <i>Range-wide Programmatic Informal Consultation for the Indiana bat and northern long-eared bat (NLEB)</i>, dated May 2016 (revised February 2018), between FHWA, Federal Railroad Administration</p>		

INVOLVEMENT WITH RESOURCES			
	<p>(FRA), Federal Transit Administration (FTA), and USFWS. An effect determination key was completed on March 10, 2020, and based on the responses provided, the project was found to may affect, but is not likely to adversely affect the Indiana bat and/or the NLEB. INDOT reviewed and verified the effect finding on March 11, 2020 and requested USFWS's review of the finding (Appendix C, pages C-23 to C-32). No response was received from USFWS within the 14-day review period; therefore, it was concluded they concur with the finding. Avoidance and Mitigation Measures (AMMs) are included as firm commitments in the Environmental Commitments section of this document.</p> <p>This precludes the need for further consultation on this project as required under Section 7 of the Endangered Species Act, as amended. If new information on endangered species at the site becomes available, or if project plans are changed, USFWS will be contacted for consultation.</p>		
Drinking Water Resources		No: <input checked="" type="checkbox"/>	Yes: <input type="checkbox"/>
Comments:	<p><u>Sole Source Aquifer (SSA):</u> The project is located in Decatur County, which is not located within the area of the St. Joseph Sole Source Aquifer, the only legally designated sole source aquifer in the state of Indiana. Therefore, the FHWA/EPA Sole Source Aquifer Memorandum of Understanding (MOU) is not applicable to this project. Therefore, a detailed groundwater assessment is not needed and no impacts are expected.</p> <p><u>Wellhead Protection Area and/or Source Water:</u> The Indiana Department of Environmental Management's Wellhead Proximity Determinator website (http://www.in.gov/idem/cleanwater/pages/wellhead/) was accessed on April 3, 2020 by CHA Consulting, INC. This project is not located within a Wellhead Protection Area or Source Water Area. No impacts are expected.</p> <p><u>Water Well(s):</u> The Indiana Department of Natural Resources Water Well Record Database website (https://www.in.gov/dnr/water/3595.htm) was accessed on April 3, 2020 by CHA Consulting, Inc. Four (4) wells were identified near the boundary of the project area. Two (2) of the wells were located near the eastern portion of the project area and two (2) of the wells were located near the southern portion of the project area. The closest well is located 370 feet east of the southeastern project extent. Survey for this project did not locate residential wells within the construction limits and wells were not identified during the field investigation conducted on October 14, 2019. Therefore, no impacts are expected. Should it be determined during the right-of-way phase that these wells are affected, a cost to cure will likely be included in the appraisal to restore the wells.</p> <p><u>Urban Area Boundary:</u> Based on a desktop review of the INDOT MS4 website (https://entapps.indot.in.gov/MS4/) by CHA Consulting, Inc., on April 16, 2020, and the RFI report; this project is located in an Urban Area Boundary (UAB) location. An early coordination letter was sent on April 24, 2020, to Greensburg UAB MS4. The MS4 coordinator did not respond within the 30-day time frame.</p> <p><u>Public Water System:</u> Based on a desktop review, a site visit on October 14, 2019 by CHA Consulting, Inc. and the aerial map of the project area (Appendix B, page B-3) no public water systems were identified. Therefore, no impacts are expected.</p>		
Flood Plains (note transverse or longitudinal impact)		No: <input checked="" type="checkbox"/>	Yes: <input type="checkbox"/>
Comments:	<p>The Indiana Department of Natural Resources Indiana Floodway Information Portal website (http://dnrmaps.dnr.in.gov/appsphp/fdms/) was accessed on April 16, 2020 by CHA Consulting, Inc. This project is not located in a regulatory floodplain as determined from approved IDNR floodplain maps (Appendix B, page B-6). Therefore, it does not fall within the guidelines for the implementation of 23 CFR 650, 23 CFR 771, and 44 CFR. No impacts are expected.</p>		
Farmland (acres)		No: <input checked="" type="checkbox"/>	Yes: <input type="checkbox"/>
Comments:	<p>Based on a desktop review, a site visit on October 14, 2019 by CHA Consulting, Inc. and the aerial map of the project area (Appendix B, page B-3), there is no land that meets the definition of farmland under the Farmland Protection Policy Act (FPPA) within or adjacent to the project area. The requirements of the FPPA do not apply to this project; therefore, no impacts are expected. An early coordination letter was sent on April 3, 2020, to Natural Resources Conservation Services (NRCS). The NRCS confirmed this by indicating that the project "will not cause a conversion of prime farmland" in their correspondence of April 8, 2020 (Appendix C, page C-19).</p>		

INVOLVEMENT WITH RESOURCES				
Cultural Resources		No: <input checked="" type="checkbox"/>	Yes: <input type="checkbox"/>	Possible: <input type="checkbox"/>
Comments:	<p>On January 6, 2020 CHA Consulting, Inc. determined that this project falls within the guidelines of Category A, Types 2, 3, and 4 under the Minor Projects Programmatic Agreement, (Appendix D, pages D-1 to D-3). Category A-2 covers the following types of work: all work within interchanges and within medians of divided highways in previously disturbed soils. Category A-3 covers the following types of work: replacement, repair, lining, or extension of culverts and other drainage structures that do not exhibit wood, stone, or brick structures or parts therein and are in previously disturbed soils. Category A-4 covers the following types of work: roadway work associated with surface replacement, reconstruction, rehabilitation, or resurfacing projects, including overlays, shoulder treatments, pavement repair, seal coating, pavement grinding, and pavement marking within previously disturbed soils where replacement, repair, or installation of curbs, curb ramps or sidewalks will not be required. No further consultation is required. This completes the Section 106 process and the responsibilities of the FHWA under Section 106 have been fulfilled.</p>			
Section 4(f) and Section 6(f) Resources		No: <input checked="" type="checkbox"/>	Yes: <input type="checkbox"/>	Possible: <input type="checkbox"/>
Comments:	<p>Section 4(f): Section 4(f) of the U.S. Department of Transportation Act of 1966 prohibits the use of certain public and historic lands for federally funded transportation facilities unless there is no feasible and prudent alternative. The law applies to significant publicly owned parks, recreation areas, wildlife / waterfowl refuges, and NRHP eligible or listed historic properties regardless of ownership. Lands subject to this law are considered Section 4(f) resources.</p> <p>Based on a desktop review, a site visit on October 14, 2019 by CHA Consulting, Inc., the aerial map of the project area (Appendix B, page B-3) and the RFI report (Appendix E, page E-9) there are six 4(f) resources located within the 0.5 mile search radius. There are no Section 4(f) resources within or adjacent to the project area. Therefore, no use is expected.</p> <p>Section 6(f): The U.S. Land and Water Conservation Fund Act of 1965 established the Land and Water Conservation Fund (LWCF), which was created to preserve, develop, and assure accessibility to outdoor recreation resources. Section 6(f) of this Act prohibits conversion of lands purchased with LWCF monies to a non-recreation use.</p> <p>A review of 6(f) properties on the Land and Water Conservation Fund (LWCF) website at https://www.lwcfcoalition.com/tools revealed a total of one property in Decatur County (Appendix H, page H-1). This property is not located within or adjacent to the project area. Therefore, there will be no impacts to 6(f) resources as a result of this project.</p>			
Air Quality Impacts		No: <input checked="" type="checkbox"/>	Yes: <input type="checkbox"/>	Possible: <input type="checkbox"/>
Comments:	<p>STIP/TIP: This project is included in the Fiscal Year (FY) 2018-2021 and 2020-2024 Statewide Transportation Improvement Program (STIP) (Appendix G, pages G-1 to G-2).</p> <p>Attainment area: This project is located in Decatur County, which is currently in attainment for all criteria pollutants according to IDEM website at https://www.in.gov/idem/airquality/files/nonattainment_areas_map.pdf. Therefore, the conformity procedures of 40 CFR Part 93 do not apply.</p> <p>MSAT Level 1a Analysis: This project is of a type qualifying as a categorical exclusion (Group 1) under 23 CFR 771.117(c), or exempt under the Clean Air Act conformity rule under 40 CFR 93.126, and as such, a Mobile Source Air Toxics analysis is not required.</p>			
Community/Economic Impacts		No: <input checked="" type="checkbox"/>	Yes: <input type="checkbox"/>	Possible: <input type="checkbox"/>
Comments:	<p>Indirect and Cumulative Impacts: Indirect impacts are effects which are caused by the action and are later in time or farther removed in distance, but are still reasonably foreseeable. Indirect effects may include growth inducing effects and other effects related to induced changes in the pattern of land use, population density, or growth rate. Cumulative impacts affect the environment which result from the incremental impact of the action when added to other past, present, and reasonably foreseeable future actions regardless of what agency or person undertakes such actions.</p> <p>No changes in land use or development are anticipated as a result of the project. Therefore, the project is not likely to</p>			

INVOLVEMENT WITH RESOURCES			
	cause substantial indirect or cumulative impacts. Environmental Justice (EJ): Under FHWA Order 6640.23A, FHWA and the project sponsor, as a recipient of funding from FHWA, are responsible to ensure that their programs, policies, and activities do not have a disproportionately high and adverse effect on minority or low-income populations. This project will have no relocations and will require less than 0.5 acre of additional permanent right-of-way; therefore, an EJ analysis is not required per the current INDOT Categorical Exclusion Manual.		
Hazardous Materials		No: <input checked="" type="checkbox"/>	Yes: <input type="checkbox"/>
Comments:	Based on a review of GIS and available public records, a RFI was completed on April 16, 2020 by CHA Consulting, Inc. (Appendix E, Pages E-1 to E-14). One (1) Underground Storage Tank (UST) site, one (1) Voluntary Remediation Program (VRP) site, one (1) Leaking Underground Storage Tank (LUST), and one (1) Confined Feeding Operation (CFO) are located within 0.5 mile of the project area; however, no hazmat sites were identified in or within 0.5 mile of the project area that will impact the project. The nearest UST is 0.32 mile from the project area. The nearest VRP is 0.45 mile from the project area. The nearest LUST is 0.49 mile from the project area. The nearest CFO is 0.22 mile from the project area. No impacts are expected. Further investigation for hazardous material concerns is not required at this time.		
Permits		No: <input type="checkbox"/>	Yes: <input checked="" type="checkbox"/>
Comments:	An IDEM Rule 5 Permit will be required as the project will disturb more than one acre of total land area. The Greensburg Airport is located 0.36 mile south of the project. If any object, obstruction, or equipment will exceed 6 ft. in height, a FAA form 7460 (Notice of Proposed Construction or Alternative) will need to be filed through the FAA to obtain a tall structure permit. For assistance contact Julian Courtade, INDOT Office of Aviation, 317-232-1477. (Appendix C, page C-16 to C-17). Applicable recommendations provided by IDEM, IDNR, and USFWS are included in the Environmental Commitments section of this document. If permits are found to be necessary, the conditions of the permit will be requirements of the project and will supersede these recommendations. It is the responsibility of the project sponsor to identify and obtain all required permits.		

ENVIRONMENTAL COMMITMENTS:
<p>Firm:</p> <ol style="list-style-type: none"> 1. If the scope of work or permanent or temporary right-of-way amounts change, the INDOT Environmental Services Division (ESD) and the INDOT District Environmental Section will be contacted immediately (INDOT ESD and INDOT District). 2. It is the responsibility of the project sponsor to notify school corporations and emergency services at least two weeks prior to any construction that would block or limit access (INDOT ESD). 3. Any work in a wetland area within right-of-way or in borrow/waste areas is prohibited unless specifically allowed in the U.S. Army Corps of Engineers Permit (INDOT ESD). 4. General AMM 1: Ensure all operators, employees, and contractors working in areas of known or presumed bat habitat are aware of all FHWA/FRA/FTA (Transportation Agencies) environmental commitments, including all applicable AMMs (USFWS). 5. Lighting AMM 1: Direct temporary lighting away from suitable habitat during the active season (USFWS). 6. Lighting AMM 2: When installing new or replacing existing permanent lights, use downward-facing, full cut-off lens lights (with same intensity or less for replacement lighting); or for those transportation agencies using the BUG system developed by the Illuminating Engineering Society, be as close to 0 for all three ratings with a priority of “uplight” of 0 and “backlight” as low as practicable (USFWS). <p>For Further Consideration: No additional commitments were identified.</p>

THE CATEGORICAL EXCLUSION CANNOT BE PROCESSED AS A LEVEL ONE IF YES IS SELECTED FOR ANY OF THE FOLLOWING ITEMS*:		
Formal noise analysis required?	No: X	Yes:
Environmental Justice analysis required?	No: X	Yes:
Right-of-Way acquisition greater than 0.5 acre?	No: X	Yes:
Relocation of residences/businesses/etc.?	No: X	Yes:
Added through-traffic lanes?	No: X	Yes:
Facility on new location or realignment?	No: X	Yes:
Permanent alteration of local traffic pattern?	No: X	Yes:
Section 4(f) and Section 6(f) resource impacts?	No: X	Yes:
Sole Source Aquifer Groundwater Assessment required?	No: X	Yes:
Is the project “Likely to Adversely Affect” Threatened and Endangered Species?	No: X	Yes:
Stream impacts greater than 300 linear feet, or work beyond 75 feet from pavement?	No: X	Yes:
Wetland impacts greater than 0.1 acre?	No: X	Yes:
Does the project have historic bridge involvement, or a Section 106 finding of No Adverse Effect / Adverse Effect?	No: X	Yes:

* Please note, this table is not applicable for state funded CE's.

Table of Appendices

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Threshold Chart A-1

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Appendix C: Early Coordination

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Appendix A

Categorical Exclusion Threshold Table

Item	Appendix Page
Categorical Exclusion Threshold Chart	A-1

Categorical Exclusion Level Thresholds

	PCE	Level 1	Level 2	Level 3	Level 4 ¹
Section 106	Falls within guidelines of Minor Projects PA	“No Historic Properties Affected”	“No Adverse Effect”	-	“Adverse Effect” Or Historic Bridge involvement ²
Stream Impacts	No construction in waterways or water bodies	< 300 linear feet of stream impacts	≥ 300 linear feet of stream impacts	-	Individual 404 Permit
Wetland Impacts	No adverse impacts to wetlands	< 0.1 acre	-	< 1 acre	≥ 1 acre
Right-of-way³	Property acquisition for preservation only or none	< 0.5 acre	≥ 0.5 acre	-	-
Relocations	None	-	-	< 5	≥ 5
Threatened/Endangered Species (Species Specific Programmatic for Indiana bat & northern long eared bat)	“No Effect”, “Not likely to Adversely Affect” (Without AMMs ⁴ or with AMMs required for all projects ⁵)	“Not likely to Adversely Affect” (With any other AMMs)	-	“Likely to Adversely Affect”	Project does not fall under Species Specific Programmatic
Threatened/Endangered Species (Any other species)	Falls within guidelines of USFWS 2013 Interim Policy	“No Effect”, “Not likely to Adversely Affect”	-	-	“Likely to Adversely Affect”
Environmental Justice	No disproportionately high and adverse impacts	-	-	-	Potential ⁶
Sole Source Aquifer	Detailed Assessment Not Required	-	-	-	Detailed Assessment
Floodplain	No Substantial Impacts	-	-	-	Substantial Impacts
Coastal Zone Consistency	Consistent	-	-	-	Not Consistent
National Wild and Scenic River	Not Present	-	-	-	Present
New Alignment	None	-	-	-	Any
Section 4(f) Impacts	None	-	-	-	Any
Section 6(f) Impacts	None	-	-	-	Any
Added Through Lane	None	-	-	-	Any
Permanent Traffic Alteration	None	-	-	-	Any
Coast Guard Permit	None	-	-	-	Any
Noise Analysis Required	No	-	-	-	Yes
Air Quality Analysis Required	No	-	-	-	Yes ⁷
Approval Level <ul style="list-style-type: none"> • District Env. Supervisor • Env. Services Division • FHWA 	Concurrence by INDOT District Environmental or Environmental Services	Yes	Yes	Yes Yes	Yes Yes Yes

¹Coordinate with INDOT Environmental Services. INDOT will then coordinate with the appropriate FHWA Environmental Specialist.

²Any involvement with a bridge processed under the Historic Bridge Programmatic Agreement.

³Permanent and/or temporary right-of-way.

⁴AMMs = Avoidance and Mitigation Measures.

⁵AMMs determined by the IPAC decision key to be needed that are listed in the USFWS *User’s Guide for the Range-wide Programmatic Consultation for Indiana bat and Northern long-eared bat* as “required for all projects”.

⁶Potential for causing a disproportionately high and adverse impact.

⁷Hot Spot Analysis and/or MSAT Quantitative Emission Analysis.

*Substantial public or agency controversy may require a higher-level NEPA document.

Appendix B

Graphics

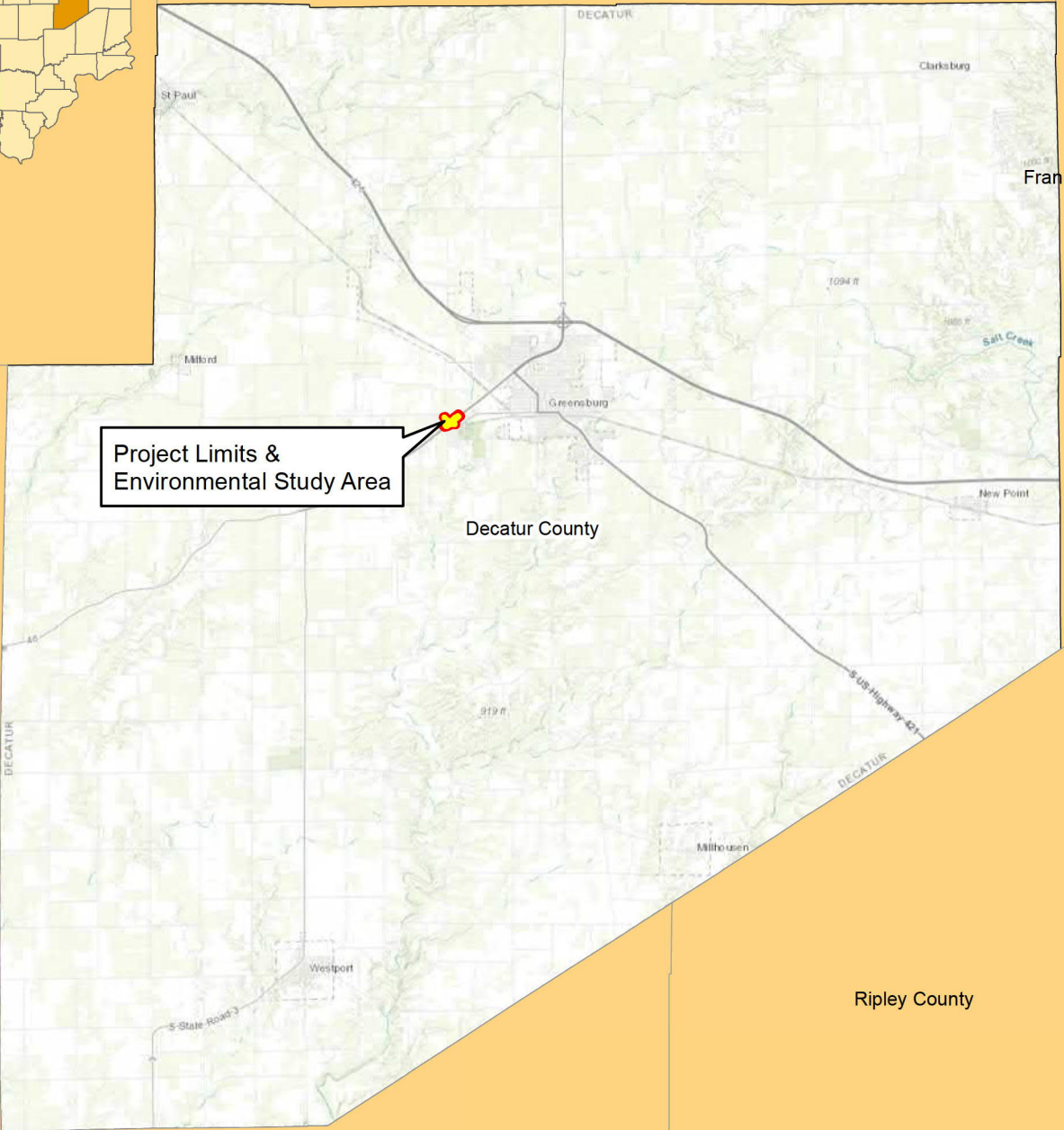
Item	Appendix Page
Project Maps	B-1 to B-6
Photo Location Map	B-7
Photographs of the Project Area	B-8 to B-12
Project Plans	B-13 to B-17

Fayette County

Rush County



Franklin County



Project Limits & Environmental Study Area

Decatur County

Bartholomew County

Ripley County

Jennings County

Date Saved: 10/1/2019 • Author: D.Bargovic

Indiana Department of Transportation



State Location Map

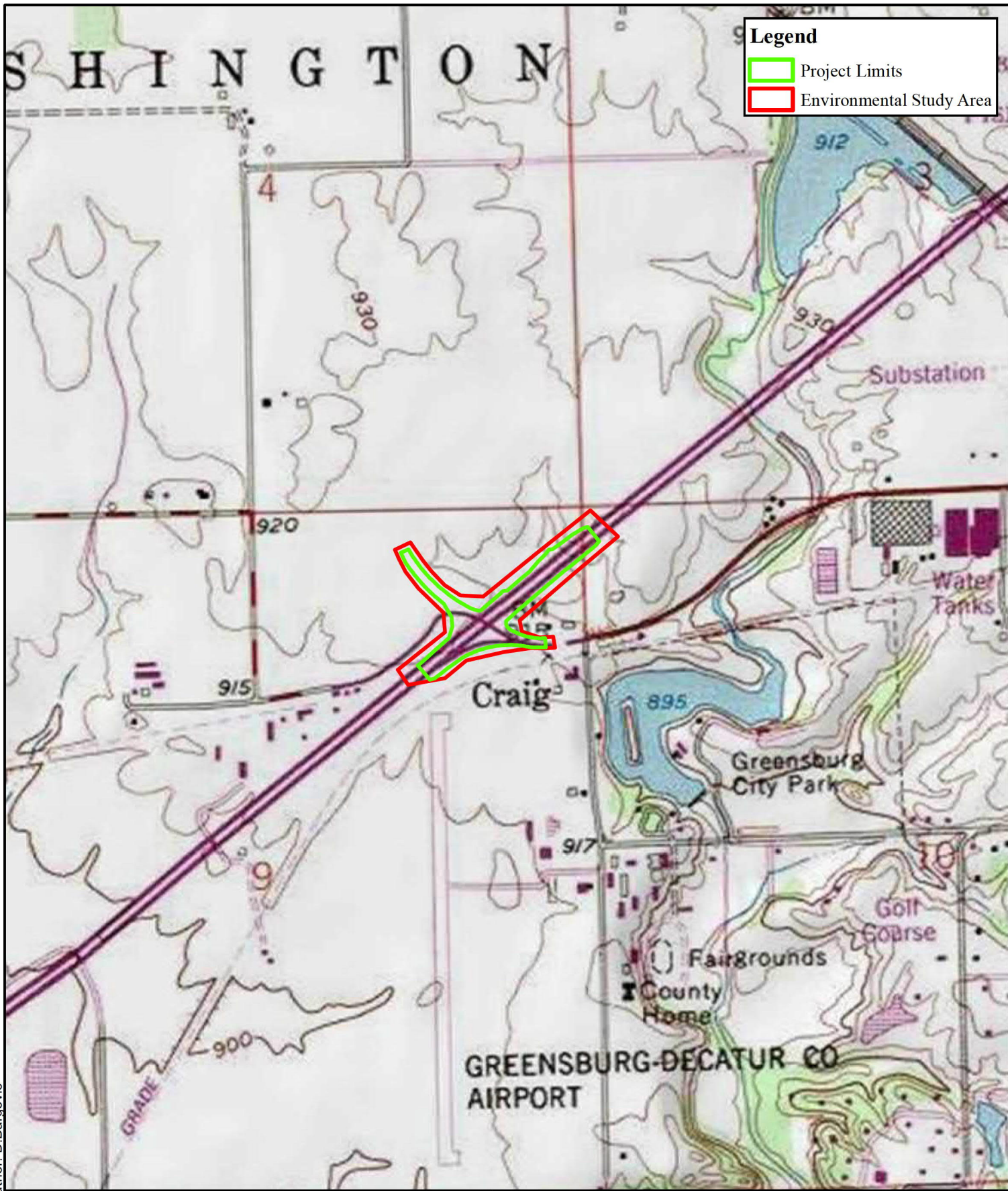
SR 46 and SR 3 Intersection Improvement
Decatur County, Indiana



Scale 1" = 18,000'

DES 1700050

County boundaries and transportation network
courtesy of the Indiana Spatial Data Portal



Legend

- Project Limits
- Environmental Study Area

Date Saved: 10/17/2019 • Author: D. Bargovic



Indiana Department of Transportation

USGS Project Location Map

SR 46 and SR 3 Intersection Improvement
Decatur County, Indiana

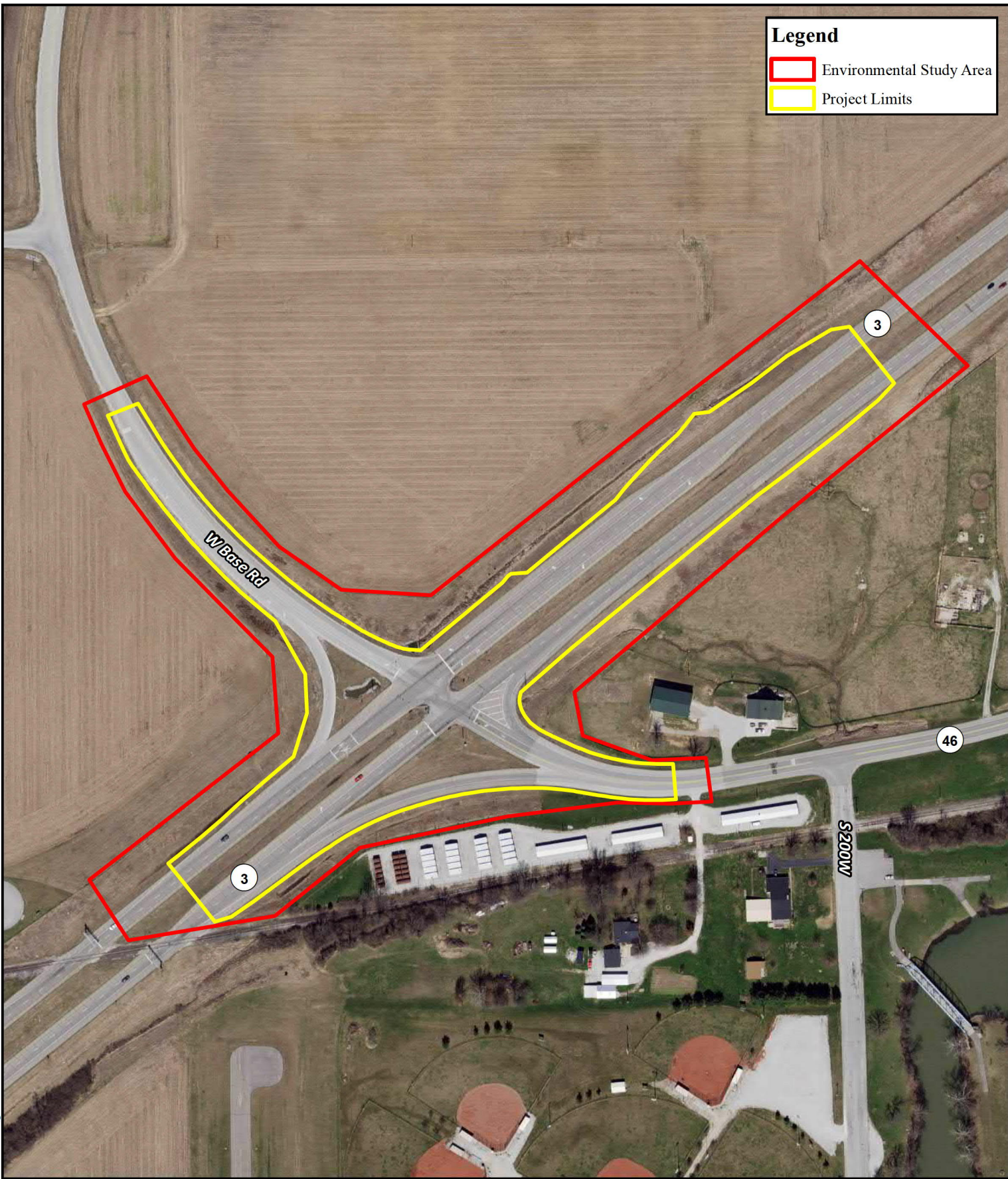
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DES 1700050

Service Layer Credits:
Copyright: © 2013 National Geographic Society, I-cubed
Forest Hill USGS Quadrangle • Date: 1993

Legend

- Environmental Study Area
- Project Limits



Date Saved: 1/9/2020 • Author: D.Bargovic

Indiana Department of Transportation



Aerial Location Map

SR 46 and SR 3 Intersection Improvement
Decatur County, Indiana



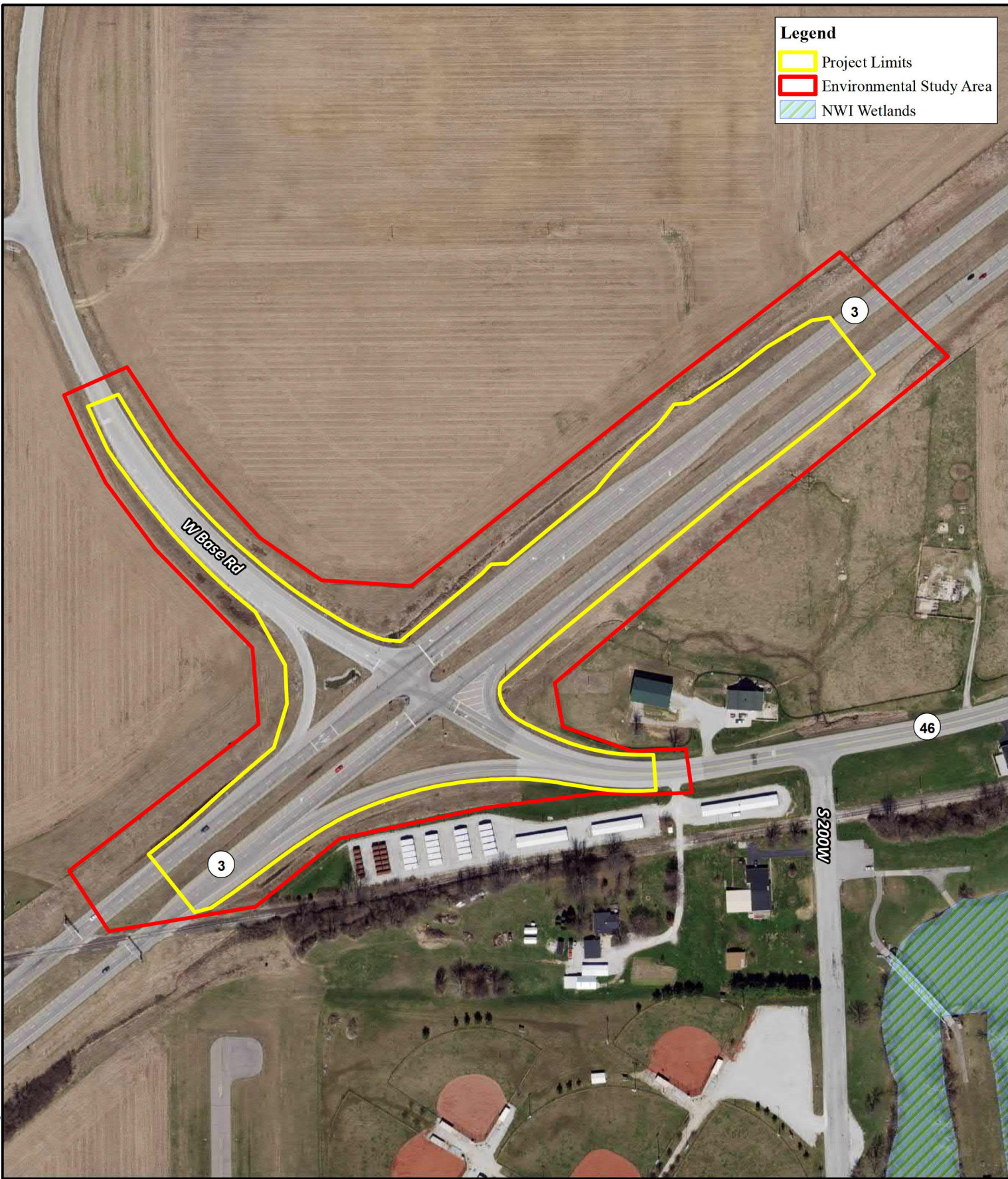
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DES 1700050

Image Courtesy of the IndianaMap
Photo Date: 2017

Legend

- Project Limits
- Environmental Study Area
- NWI Wetlands



Date Saved: 1/9/2020 • Author: D.Bargovic

Indiana Department of Transportation

CHIA

NWI Wetlands Map

SR 46 and SR 3 Intersection Improvement
Decatur County, Indiana

Scale 1" = 250'

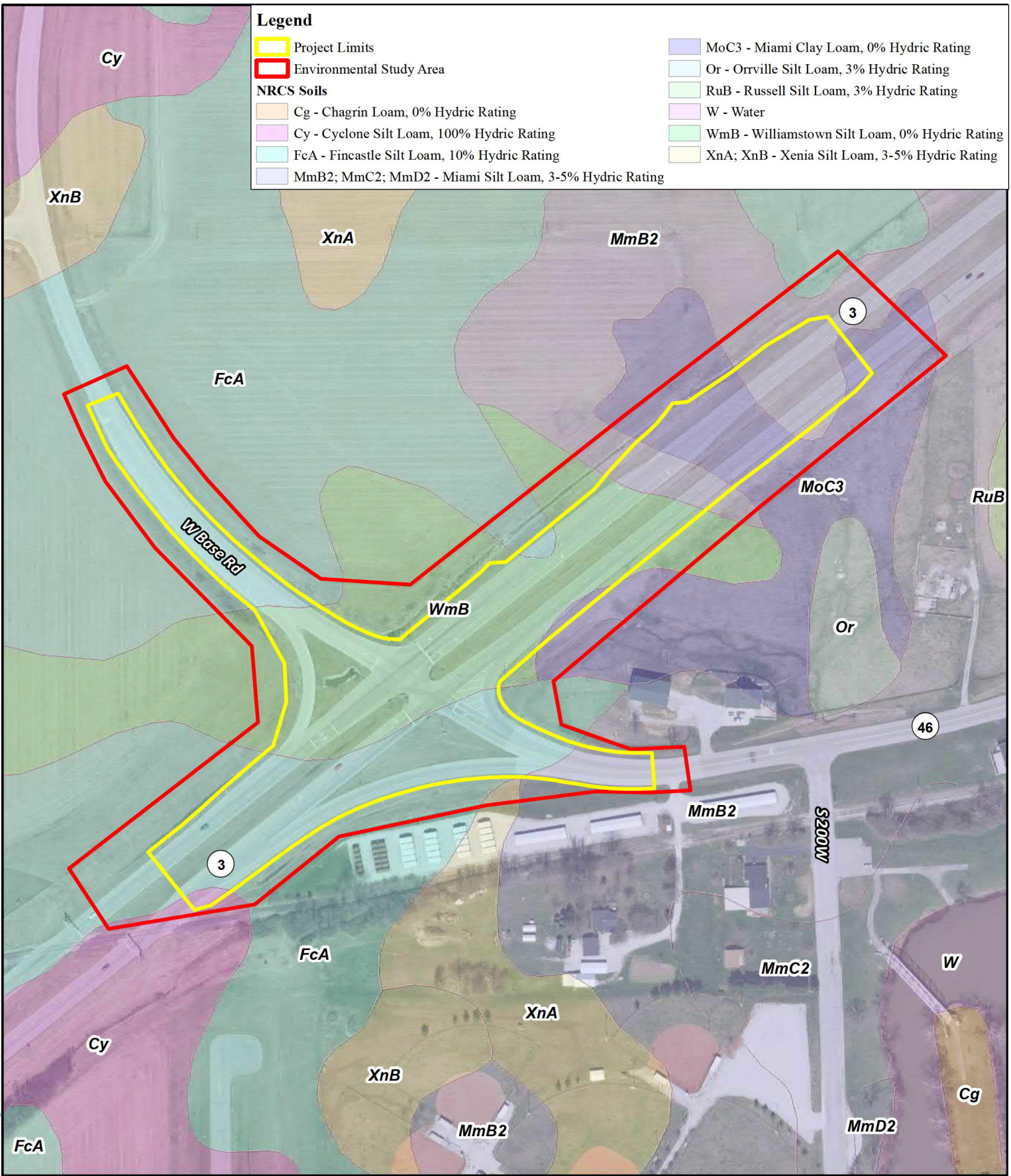
DES 1700050

Image Courtesy of the IndianaMap - Photo Date: 2017
NWI Wetland data courtesy of the
National Wetlands Inventory produced by the U.S. Fish and Wildlife Service



Legend

- Project Limits
- Environmental Study Area
- NRCS Soils**
- Cg - Chagrin Loam, 0% Hydric Rating
- Cy - Cyclone Silt Loam, 100% Hydric Rating
- FcA - Fincastle Silt Loam, 10% Hydric Rating
- MmB2; MmC2; MmD2 - Miami Silt Loam, 3-5% Hydric Rating
- MoC3 - Miami Clay Loam, 0% Hydric Rating
- Or - Orrville Silt Loam, 3% Hydric Rating
- RuB - Russell Silt Loam, 3% Hydric Rating
- W - Water
- WmB - Williamstown Silt Loam, 0% Hydric Rating
- XnA; XnB - Xenia Silt Loam, 3-5% Hydric Rating



Date Saved: 1/9/2020 • Author: D.Bargovic

Indiana Department of Transportation



NRCS Soils Map

SR 46 and SR 3 Intersection Improvement
Decatur County, Indiana



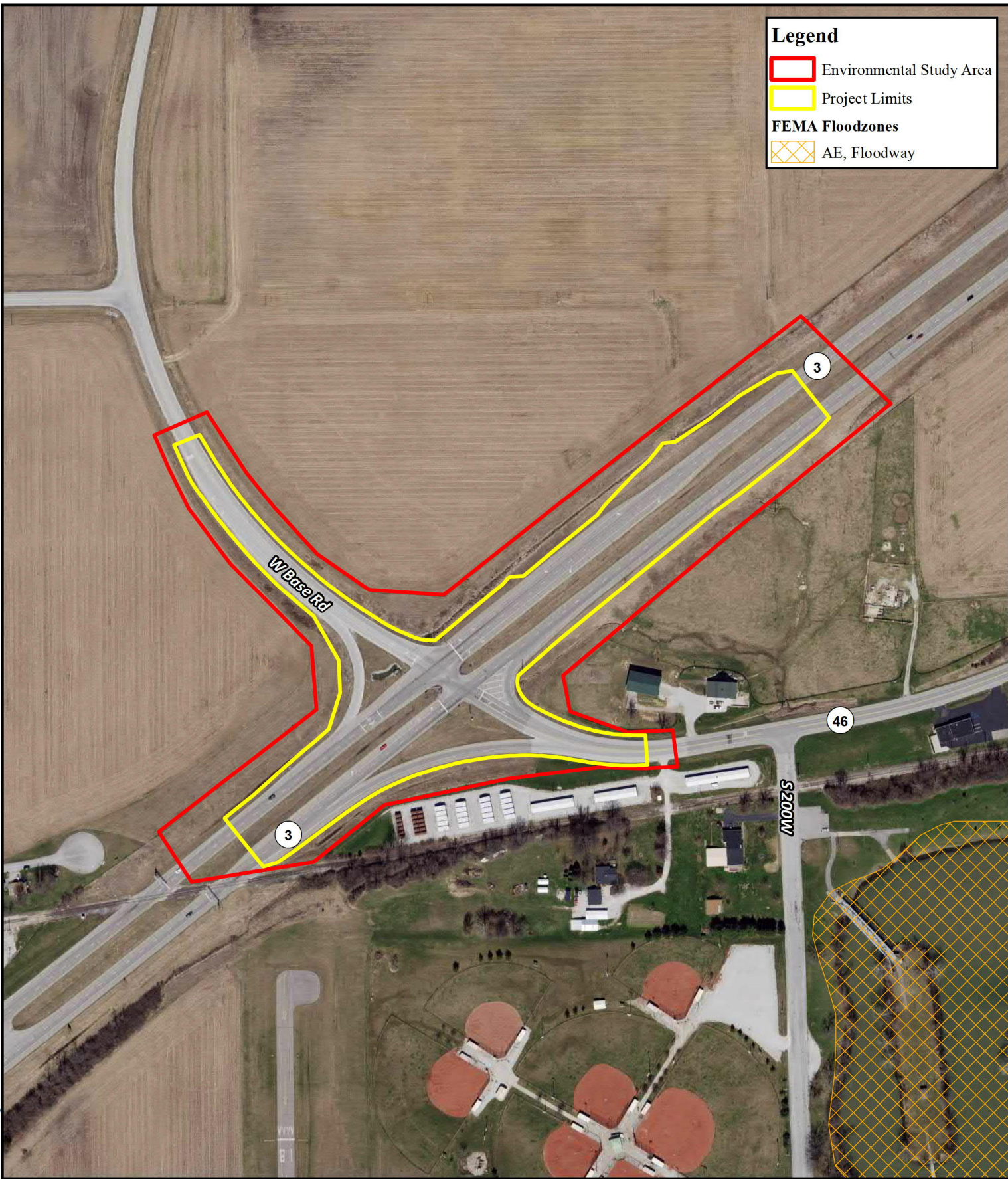
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DES 1700050

Image Courtesy of the Indiana Map - Photo Date: 2017
Soil Data Courtesy of the Natural Resource Conservation Service

Legend

- Environmental Study Area
- Project Limits
- FEMA Floodzones**
- AE, Floodway



Date Saved: 1/10/2020 • Author: D. Bargovic

Indiana Department of Transportation



FEMA Floodzones Map

**SR 46 and SR 3 Intersection Improvement
Decatur County, Indiana**

*Image Courtesy of the IndianaMap
Photo Date: 2017*

Floodzones Courtesy of the Federal Emergency Management Agency

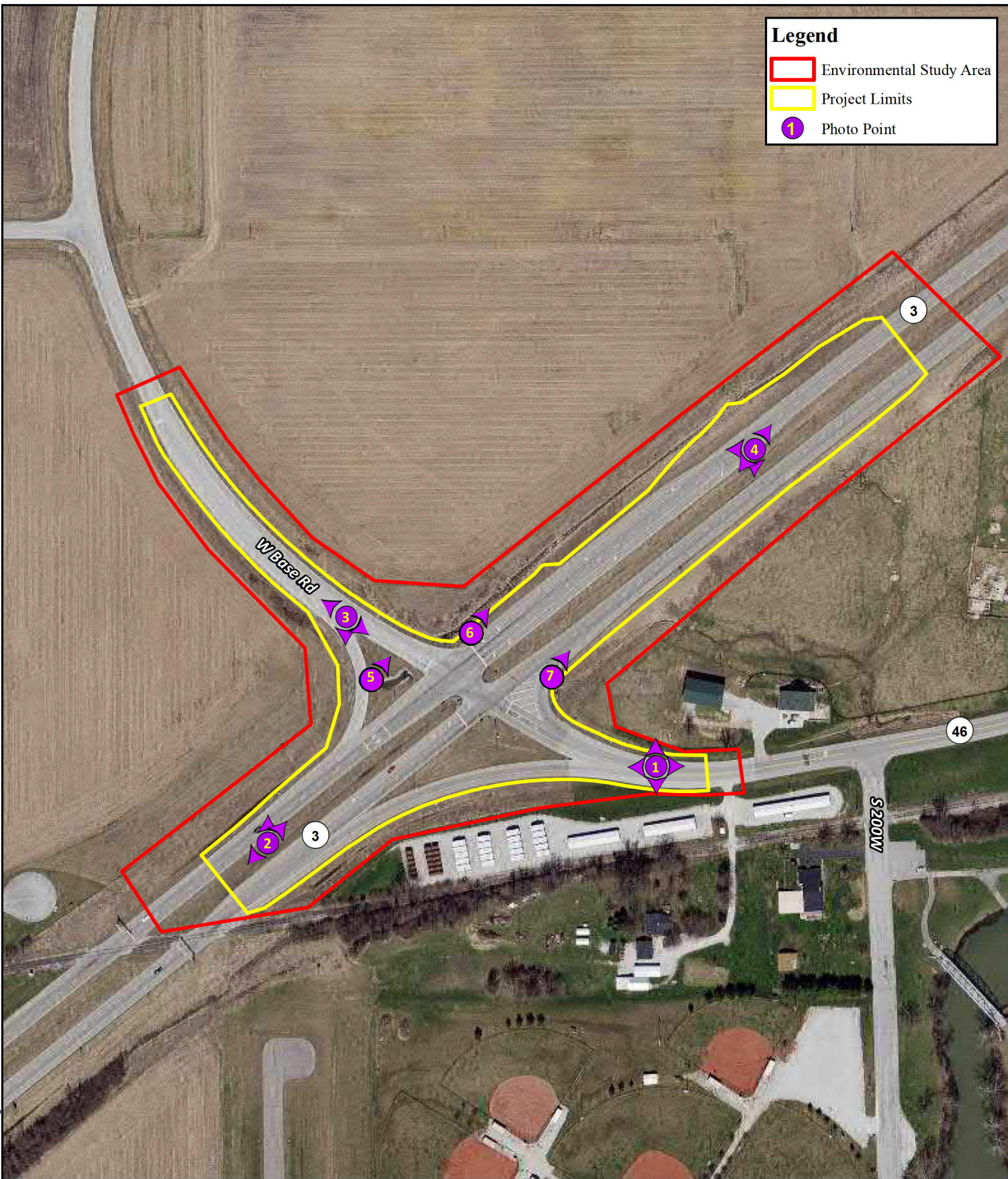


Scale 1" = 300'

DES 1700050

Legend

- Environmental Study Area
- Project Limits
- ① Photo Point



Date Saved: 1/24/2020 • Author: D. Bargovic

Indiana Department of Transportation



Scale 1" = 250'

DES 1700050

Photo Location Map

SR 46 and SR 3 Intersection Improvement
Decatur County, Indiana

Image Courtesy of the IndianaMap
Photo Date: 2017

State Road 3 and State Road 46 Intersection Improvements

INDOT DES 1700050



Photo Point 1: Looking east at SR 46 from south of the intersection



Photo Point 1: Looking north-northeast at agricultural land, from SR 46, south of the intersection



Photo Point 1: Looking south at storage business, from SR 46, south of the intersection



Photo Point 1: Looking west at the intersection from SR 46, from the southeast leg of the intersection

Photos taken October 14, 2019



State Road 3 and State Road 46 Intersection Improvements

INDOT DES 1700050



Photo Point 2: Looking north at the surrounding agricultural land from the median of SR 3 southwest of the intersection



Photo Point 2: Looking northeast toward the intersection from the median of SR 3 southwest of the intersection



Photo Point 2: Looking southwest at the railroad and silos/ag business from the median of SR 3 southwest of the intersection



Photo Point 3: Looking northwest at the CR 250W approach to SR 3 and surrounding agricultural land

Photos taken October 14, 2019



State Road 3 and State Road 46 Intersection Improvements

INDOT DES 1700050



Photo Point 3: Looking south at the south(east) bound CR 250W to SR 3 ramp



Photo Point 3: Looking southeast at the intersection from CR 250W



Photo Point 4: Looking northeast from the median of SR 3 in the northeast leg of the project area



Photo Point 4: Looking south at the residence/agricultural land from the median of SR 3 in northeast leg

Photos taken October 14, 2019



State Road 3 and State Road 46 Intersection Improvements

INDOT DES 1700050



Photo Point 4: Looking southwest toward the main intersection from the northeast median of SR 3



Photo Point 4: Looking west from the northeast median of SR 3

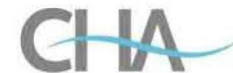


Photo Point 5: Looking northeast at the drainage feature between the south(east)bound CR 250W to SR 3 ramp and the intersection



Photo Point 6: Looking northeast at the roadside ditch parallel to SR 3

Photos taken October 14, 2019



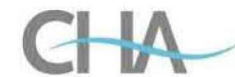
State Road 3 and State Road 46 Intersection Improvements

INDOT DES 1700050



Photo Point 7: Looking northeast at the water feature to the northeast of the right turn lane of SR 46

Photos taken October 14, 2019



PROJECT	DESIGNATION
1700050	1700050
CONTRACT	BRIDGE FILE
T-40427	N/A

KIN DESIGNATION NUMBERS	
DES. NO.	DESCRIPTION

INDIANA DEPARTMENT OF TRANSPORTATION



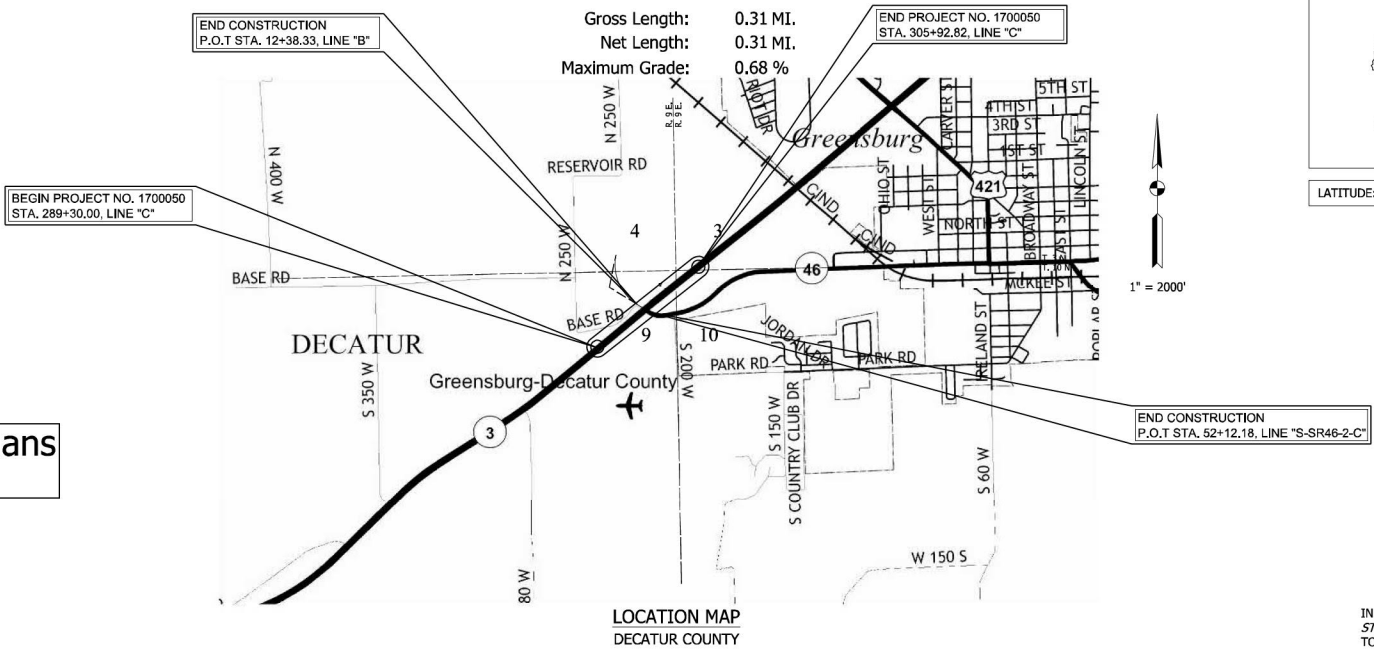
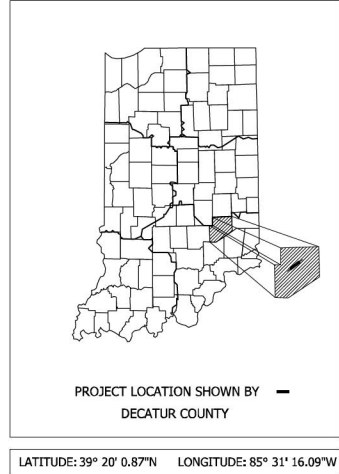
ROAD PLANS

PROJECT NO. 1700050 P.E.
 PROJECT NO. 1700050 R/W
 PROJECT NO. 1700050 CONST.

Intersection improvement at SR 3 and SR 46/ CR 250W in
 Section 9 of Township 10 North, Range 9 East, Washington Township,
 Decatur County, Indiana

TRAFFIC DATA	SR. 3 (LINE "C")
A.A.D.T. (2017)	11,002 V.P.D.
A.A.D.T. (2012)	14,242 V.P.D.
DAILY (2017)	1,100 V.P.H.
DIRECTIONAL DISTRIBUTION (2017)	52.0% (WB)
TRUCKS (2014)	13.0% A.A.D.T. 13.0% D.H.V.
DESIGN DATA	
DESIGN SPEED	55 M.P.H.
PROJECT DESIGN CRITERIA	3R (NON-FREEWAY)
FUNCTIONAL CLASSIFICATION	PRINCIPAL ARTERIAL
RURAL/URBAN	RURAL
TERRAIN	LEVEL
ACCESS CONTROL	PARTIAL

* SEE INDEX SHEET FOR S-LINE TRAFFIC AND DESIGN DATA.



Stage 2 Plans
5/27/2020

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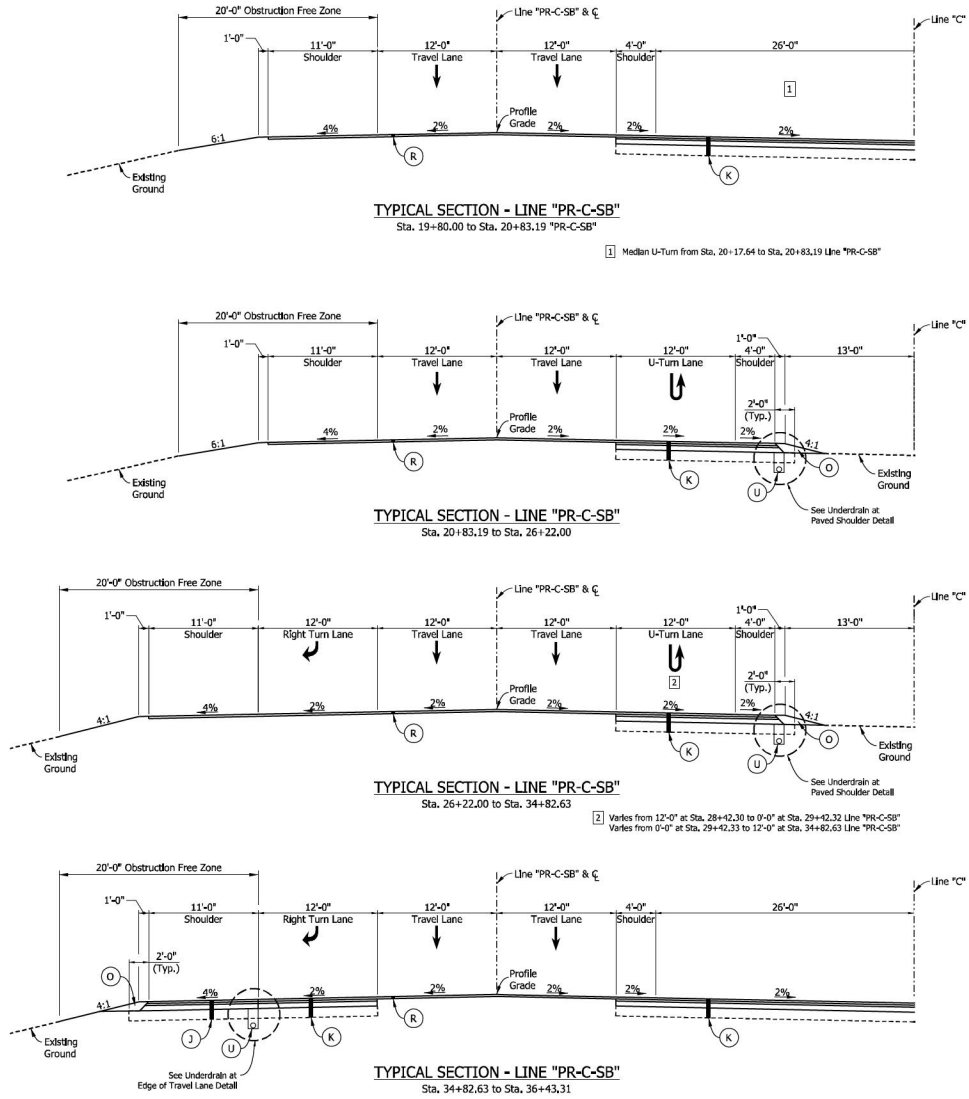


PLANS PREPARED BY: CHA Consulting, Inc.	317.786.0461 PHONE NUMBER
CERTIFIED BY: _____	DATE _____
APPROVED FOR LETTING: _____	DATE _____

INDIANA DEPARTMENT OF TRANSPORTATION
 STANDARD SPECIFICATIONS DATED 2020
 TO BE USED WITH THESE PLANS

SURVEY BOOK	SHEETS	
	1	of 76
CONTRACT	PROJECT	
T-40427	1700050	

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(I) Median U-Turn from Sta. 20+17.64 to Sta. 20+83.19 Line "PR-C-SB"

(Z) Varies from 12'-0" at Sta. 28+42.30 to 0'-0" at Sta. 29+42.32 Line "PR-C-SB"
 Varies from 0'-0" at Sta. 29+42.33 to 12'-0" at Sta. 34+82.63 Line "PR-C-SB"

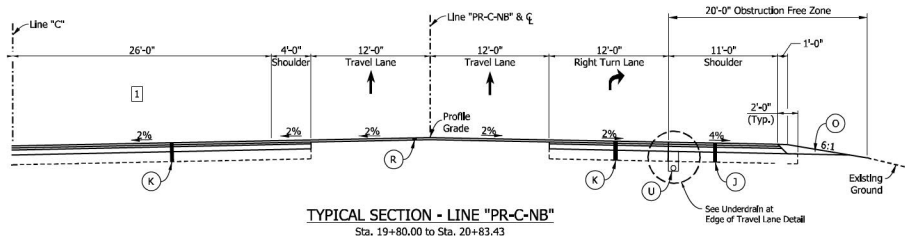
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(R)	Milling, Asphalt, 2", with 220 lb/yd QC/QA-HMA, 2, 64, Surface, 12.5 mm
(O)	275 lb/yd QC/QA-HMA, 3, 70, Intermediate, 19 mm
(U)	275 lb/yd QC/QA-HMA, 2, 64, Base, 25 mm
(J)	Subgrade Treatment, Type I.C.
(I)	220 lb/yd QC/QA-HMA, 3, 70, Surface, 12.5 mm
(Z)	275 lb/yd QC/QA-HMA, 3, 70, Intermediate, 19 mm
(K)	275 lb/yd QC/QA-HMA, 2, 64, Base, 25 mm
(U)	Compacted Aggregate No. 53
(P)	Pipe, Type 4, Circular, 6"
(J)	Subgrade Treatment, Type I.C.

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
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CHECKED: PAW	CHECKED: PAW	

INDIANA
DEPARTMENT OF TRANSPORTATION

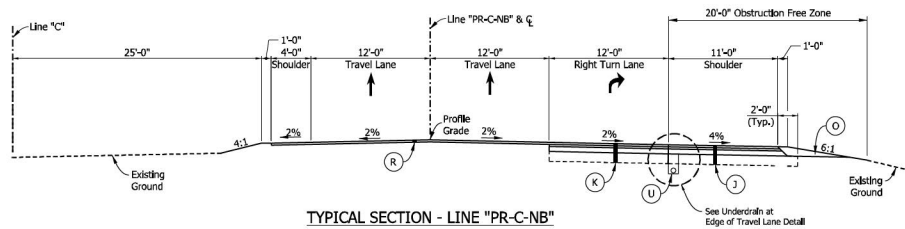
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SURVEY BOOK	SHEETS
	3 of 76
CONTRACT	PROJECT
T-46427	170050

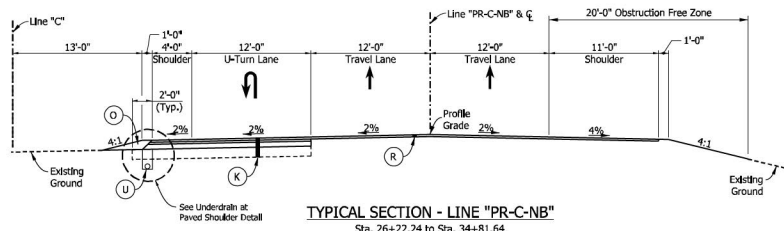


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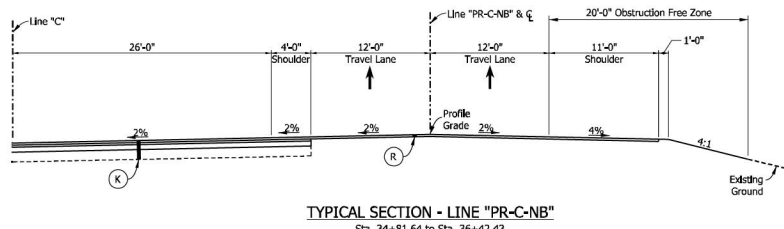
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TYPICAL SECTION - LINE "PR-C-NB"
Sta. 20+83.43 to Sta. 26+22.24



TYPICAL SECTION - LINE "PR-C-NB"
Sta. 26+22.24 to Sta. 34+81.64



TYPICAL SECTION - LINE "PR-C-NB"
Sta. 34+81.64 to Sta. 36+42.43

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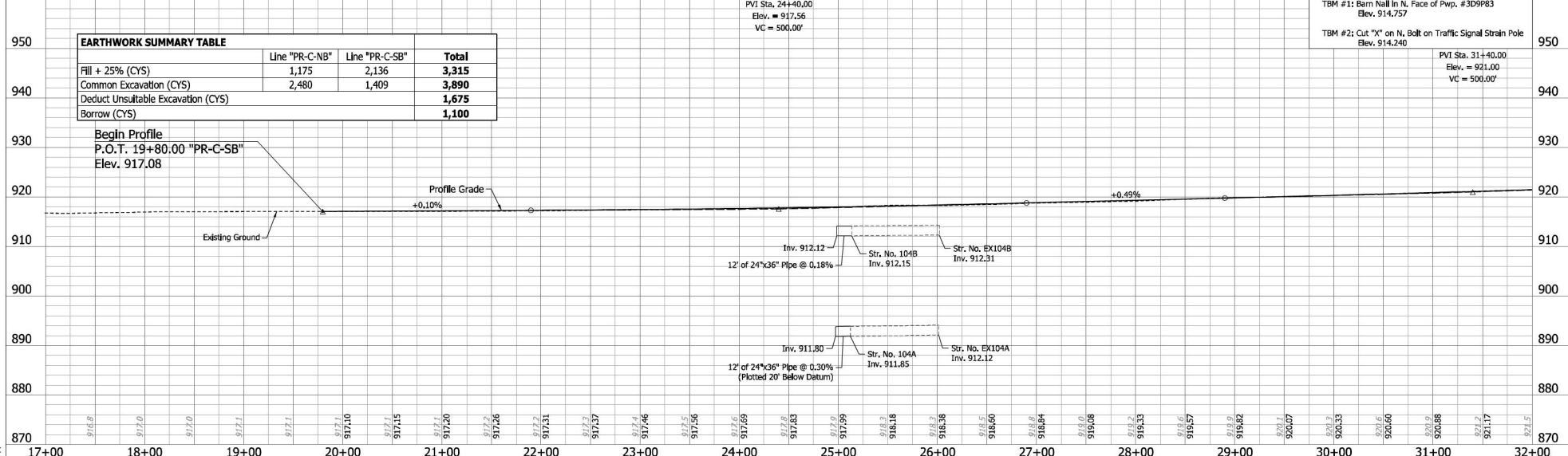
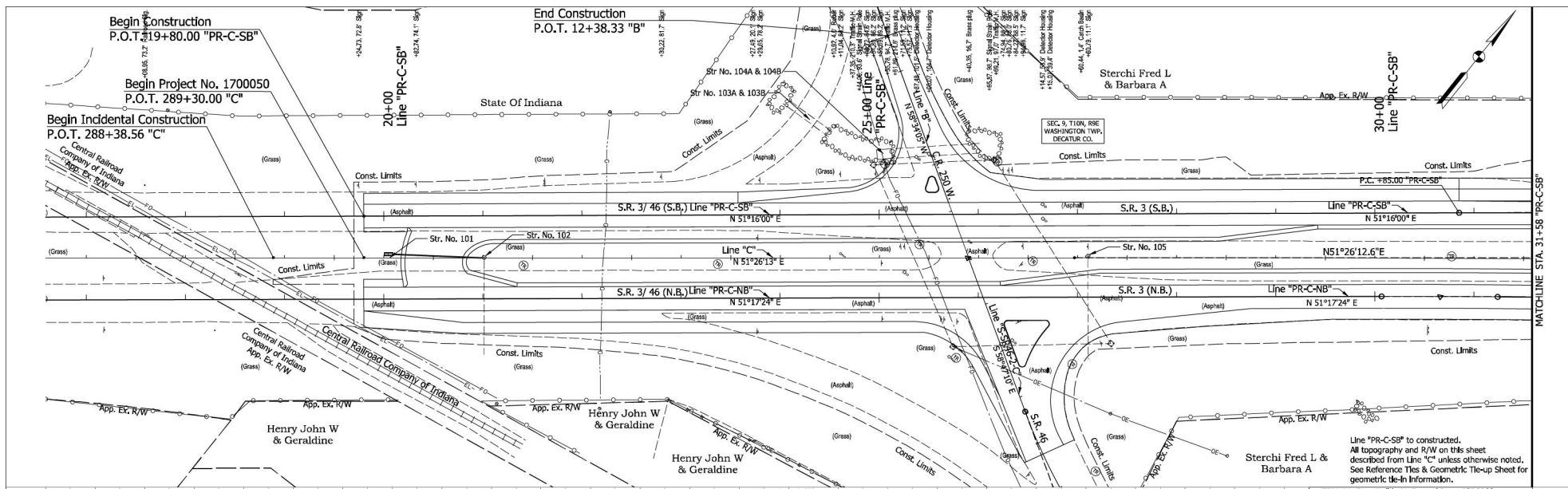
LEGEND	
(K)	230 lb/yd QC/QA-HMA, 3, 70, Surface, 12.5 mm
(R)	275 lb/yd QC/QA-HMA, 3, 70, Intermediate, 19 mm
(O)	275 lb/yd QC/QA-HMA, 2, 64, Base, 25 mm
(J)	230 lb/yd QC/QA-HMA, 3, 70, Surface, 12.5 mm
(U)	275 lb/yd QC/QA-HMA, 3, 70, Intermediate, 19 mm
	275 lb/yd QC/QA-HMA, 2, 64, Base, 25 mm
	Compacted Aggregate No. 53
	Pipe, Type 4, Circular, 6"
	Milling, Asphalt, 2", with 220 lb/yd QC/QA-HMA, 2, 64, Surface, 12.5 mm
	Subgrade Treatment, Type I, C

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: CJC	DRAWN: ELM	
CHECKED: PAW	CHECKED: PAW	

INDIANA DEPARTMENT OF TRANSPORTATION	
TYPICAL CROSS SECTIONS SR 3 & SR 46 - LINE "PR-C-NB"	

HORIZONTAL SCALE	BRIDGE FILE
1/4" = 1'-0"	N/A
VERTICAL SCALE	DESIGNATION
	1700050
SURVEY BOOK	SHEETS
	4 of 76
CONTRACT	PROJECT
T-40427	170050

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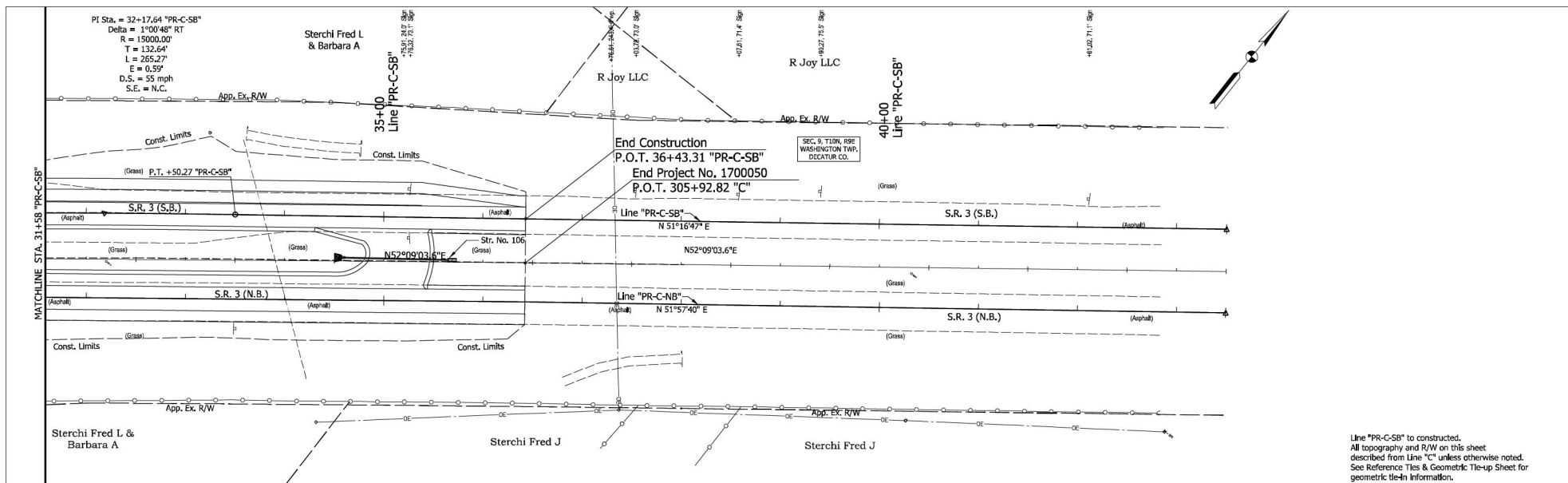


EARTHWORK SUMMARY TABLE			
	Line "PR-C-NB"	Line "PR-C-SB"	Total
Fill + 25% (CYS)	1,175	2,136	3,315
Common Excavation (CYS)	2,480	1,409	3,890
Deduct Unsuitable Excavation (CYS)			1,675
Borrow (CYS)			1,100

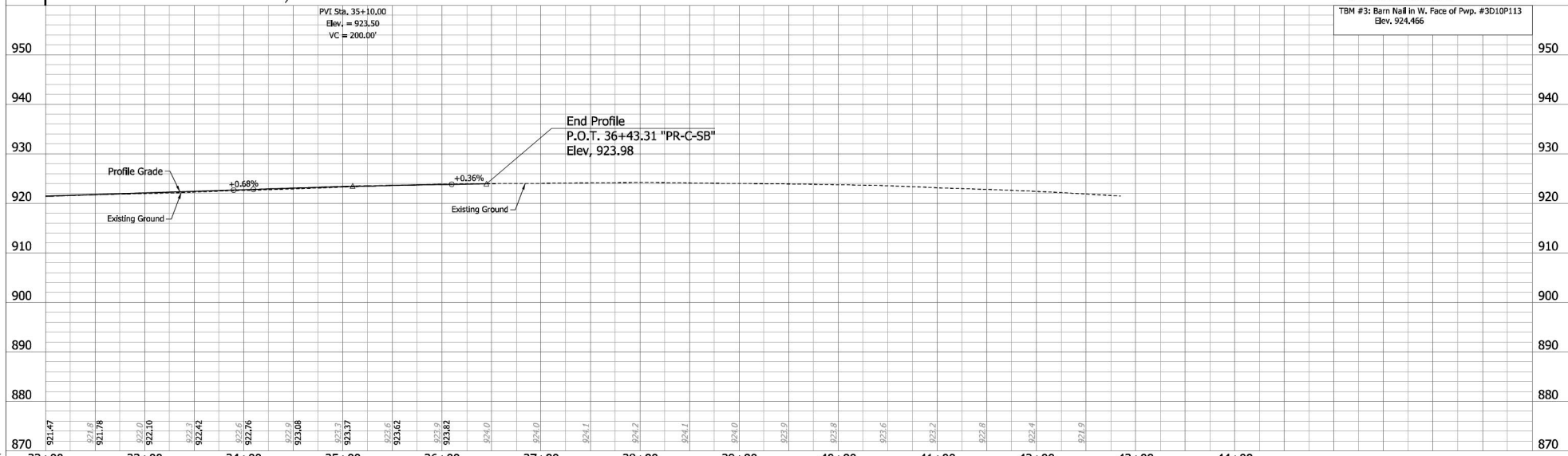
See Geometric Tie-Up Sheet for Control Points

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE	INDIANA DEPARTMENT OF TRANSPORTATION PLAN & PROFILE STATE ROAD 3 & STATE ROAD 46 - LINE "PR-C-SB"	HORIZONTAL SCALE	BRIDGE FILE
DESIGNED: CC	DRAWN: ELM			1" = 50'	N/A
CHECKED: PAW	CHECKED: PAW			VERTICAL SCALE	DESIGNATION
				1" = 10'	1700050
				SURVEY BOOK	SHEETS
				30	of 76
				CONTRACT	PROJECT
				T-4047	1700050

PATH AND FILENAME: \\P:\Projects\1700050\3234\CAD\JUSTIN\Sheet Drawings\Site Plan\PR-C-SB_03.dgn
 MODEL NAME: Site Plan\PR-C-SB_03.dwg
 DATE PLOTTED: 5/18/2020
 TIME PLOTTED: 1:37:28 PM



Line "PR-C-SB" to be constructed.
 All topography and R/W on this sheet described from Line "C" unless otherwise noted.
 See Reference Ties & Geometric Tie-Up Sheet for geometric tie-in information.



See Geometric Tie-Up Sheet for Control Points	<table border="1"> <tr> <td>RECOMMENDED FOR APPROVAL</td> <td>DESIGN ENGINEER</td> <td>DATE</td> </tr> <tr> <td>DESIGNED: CJC</td> <td>DRAWN: ELM</td> <td></td> </tr> <tr> <td>CHECKED: PAW</td> <td>CHECKED: PAW</td> <td></td> </tr> </table>	RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE	DESIGNED: CJC	DRAWN: ELM		CHECKED: PAW	CHECKED: PAW		<p style="text-align: center;">INDIANA DEPARTMENT OF TRANSPORTATION</p> <p style="text-align: center;">PLAN & PROFILE STATE ROAD 3 & STATE ROAD 46 - LINE "PR-C-SB"</p>	<table border="1"> <tr> <td>HORIZONTAL SCALE</td> <td>BRIDGE FILE</td> </tr> <tr> <td>1" = 50'</td> <td>N/A</td> </tr> <tr> <td>VERTICAL SCALE</td> <td>DESIGNATION</td> </tr> <tr> <td>1" = 10'</td> <td>1700050</td> </tr> <tr> <td>SURVEY BOOK</td> <td>SHEETS</td> </tr> <tr> <td></td> <td>31 of 76</td> </tr> <tr> <td>CONTRACT</td> <td>PROJECT</td> </tr> <tr> <td>T-40427</td> <td>1700050</td> </tr> </table>	HORIZONTAL SCALE	BRIDGE FILE	1" = 50'	N/A	VERTICAL SCALE	DESIGNATION	1" = 10'	1700050	SURVEY BOOK	SHEETS		31 of 76	CONTRACT	PROJECT	T-40427	1700050
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