

Indiana Department of Transportation

County Clark

Route U.S. 31

Des. No. 1700111

**FHWA-Indiana Environmental Document
CATEGORICAL EXCLUSION / ENVIRONMENTAL ASSESSMENT FORM
GENERAL PROJECT INFORMATION**

Road No./County:	U.S. 31/Clark County
Designation Number:	1700111
Project Description/Termini:	Road Rehabilitation and Drainage Improvement / U.S. 31 from 1.53 miles N. of State Road (SR) 60 (Foothill Road) to 3.28 Miles North of SR 60

After completing this form, I conclude that this project qualifies for the following type of Categorical Exclusion (FHWA must review/approve if Level 4 CE):

X	Categorical Exclusion, Level 2 – The proposed action meets the criteria for Categorical Exclusion Manual Level 2 - table 1, CE Level Thresholds. Required Signatories: ESM (Environmental Scoping Manager)
	Categorical Exclusion, Level 3 – The proposed action meets the criteria for Categorical Exclusion Manual Level 3 - table 1, CE Level Thresholds. Required Signatories: ESM, ES (Environmental Services Division)
	Categorical Exclusion, Level 4 – The proposed action meets the criteria for Categorical Exclusion Manual Level 4 - table 1, CE Level Thresholds. Required Signatories: ESM, ES, FHWA
	Environmental Assessment (EA) – EAs require a separate FONSI. Additional research and documentation is necessary to determine the effects on the environment. Required Signatories: ES, FHWA

Note: For documents prepared by or for Environmental Services Division, it is not necessary for the ESM of the district in which the project is located to release for public involvement or sign for approval.

Approval

_____	_____	_____	_____
ESM Signature	Date	ES Signature	Date
_____		_____	
FHWA Signature		Date	

Release for Public Involvement

DGD 2021.04.19 12:08:56
-04'00'

_____	_____	_____	_____
ESM Initials	Date	ES Initials	Date

Certification of Public Involvement

_____	_____
Office of Public Involvement	Date

Note: Do not approve until after Section 106 public involvement and all other environmental requirements have been satisfied.

INDOT ES/District Env. Reviewer Signature: _____ Date: _____

Name and Organization of CE/EA Preparer: Elizabet Biggio, Butler, Fairman and Seufert, Inc.

Indiana Department of Transportation

County Clark

Route U.S. 31

Des. No. 1700111

Part I - PUBLIC INVOLVEMENT

Every Federal action requires some level of public involvement, providing for early and continuous opportunities throughout the project development process. The level of public involvement should be commensurate with the proposed action.

Does the project have a historic bridge processed under the Historic Bridges PA*? If No, then: Opportunity for a Public Hearing Required? Yes No X

*A public hearing is required for all historic bridges processed under the Historic Bridges Programmatic Agreement between INDOT, FHWA, SHPO, and the ACHP.

Discuss what public involvement activities (legal notices, letters to affected property owners and residents (i.e. notice of entry), meetings, special purpose meetings, newspaper articles, etc.) have occurred for this project.

Remarks:

Notice of Entry letters were mailed to potentially affected property owners near the project area on December 14, 2017... To meet the public involvement requirements of Section 106... The project meets the minimum requirements described in the current Indiana Department of Transportation (INDOT) Public Involvement Manual... The project received public involvement certification from INDOT on June 14, 2021...

Public Controversy on Environmental Grounds

Will the project involve substantial controversy concerning community and/or natural resource impacts? Yes No X

Remarks:

At this time, there is no substantial public controversy concerning impacts to the community or to natural resources.

Indiana Department of Transportation

County Clark Route U.S. 31 Des. No. 1700111

Part II - General Project Identification, Description, and Design Information

Sponsor of the Project: INDOT INDOT District: Seymour

Local Name of the Facility: Indiana Avenue (U.S. 31)

Funding Source (mark all that apply): Federal [X] State [X] Local [] Other* []

*If other is selected, please identify the funding source:

PURPOSE AND NEED:

Describe the transportation problem that the project will address. The solution to the traffic problem should NOT be discussed in this section. (Refer to the CE Manual, Section IV.B.2. Purpose and Need)

Need

The need for the project is derived from the deteriorated pavement conditions along U.S. 31. Portions of the corridor exhibit significant cracking in the asphalt or concrete surface. Existing curbs are buried and deteriorating. Drainage is insufficient to handle stormwater. The pedestrian facilities are not Americans with Disabilities Act (ADA)-compliant, and in some stretches are nonexistent. According to INDOT data, in the period between 2010 and 2018 there were 463 crashes within the project area, or an average of 56 per year. About 63% were rear-end crashes, which are associated with signalized intersections, congested traffic conditions, and driver inattention. This problem is particularly apparent at the intersection of U.S. 31 and CR 403 (Old SR 403). In addition, the West Clark Community Schools Corp. has expressed safety concerns about the mid-block pedestrian crossing near the entrance to Silver Creek Elementary and Silver Creek Middle Schools.

Purpose

The purpose of this project is to extend the functional life of the existing pavement, improve drainage, provide ADA-compliant pedestrian facilities, and improve safety along U.S. 31 between Foothill Road and the Silver Creek Bridge.

PROJECT DESCRIPTION (PREFERRED ALTERNATIVE):

County: Clark Municipality: Town of Sellersburg

Limits of Proposed Work: U.S. 31 from 1.53 miles N. of State Road (SR) 60 (Foothill Rd.) to 3.28 Miles N. of SR 60 (Silver Creek Bridge)

Total Work Length: 1.82 Mile(s) Total Work Area: 12.3 Acre(s)

Is an Interchange Modification Study / Interchange Justification Study (IMS/IJS) required?

Yes No [X]

If yes, when did the FHWA grant a conditional approval for this project?

Date:

1If an IMS or IJS is required; a copy of the approved CE/EA document must be submitted to the FHWA with a request for final approval of the IMS/IJS.

Indiana Department of Transportation

County Clark

Route U.S. 31

Des. No. 1700111

In the remarks box below, describe existing conditions, provide in detail the scope of work for the project, including the preferred alternative. Include a discussion of logical termini. Discuss any major issues for the project and how the project will improve safety or roadway deficiencies if these are issues.

Location

The proposed undertaking is on U.S. 31 between Foothill Road and the Silver Creek Bridge (Muddy Fork) in Clark County, Indiana. It is within Silver Creek Township, in Clark Military Grants 110, 111, and 130 on the USGS Speed, Indiana Quadrangle (Appendix B, B2-B3).

Existing Conditions

U.S. 31 (referred to locally as Indiana Avenue) consists of two travel lanes and is aligned in a northwest-southeast direction between Foothill Road and County Road (CR) 311 and northeast-southwest from CR 311 to the Silver Creek Bridge (Muddy Fork). U.S. 31 is classified as a Minor Arterial between Foothill Road and CR 403 (Old SR 403) and as a Major Collector from CR 403 to the Silver Creek Bridge. The following additional lanes are present:

- Northbound passing blister at Triangle Drive
- Northbound left-turn lane at CR 311 / Prather Street
- Westbound left and right-turn lanes at CR 311 / Prather Street
- Two-way left-turn lane between CR 311 / Prather Street and Utica Street
- North and southbound left-turn lanes at Utica Street
- Northbound left-turn lane and southbound right-turn lane at Silver Creek Middle and Elementary School entrance
- Northbound right-turn lane and southbound left-turn lane at CR 403
- Northbound left-turn lane at Renz Avenue

The typical existing pavement width is 36 feet from face of curb to face of curb. Where turn lanes are not present, travel lanes are approximately 18 feet wide. Where turn lanes are present, travel lanes and turn lanes are each approximately 12 feet wide. South of Triangle Drive, the existing pavement width is approximately 38 feet to 46 feet and consists of two 12-foot wide travel lanes, a southbound 12-foot wide shoulder, and a northbound 2 to 10-foot wide shoulder.

Concrete sidewalk is intermittently present within the town limits of Sellersburg. There is a 6-foot concrete sidewalk along the west side of U.S. 31 north to Indiana Avenue, approximately 1,100 feet south of the northern project terminus. No sidewalk presently exists south of CR 311 along U.S. 31.

South of approximately Bucheit Street, existing stormwater is handled largely by open roadside ditches. There are some storm inlets present south of CR 311. Between approximately Bucheit Street and CR 403 there is a concrete curb of varying height and shallow stormwater inlets. The 15-18 inch storm sewer trunkline outlets to an unnamed tributary (UNT) to Silver Creek. North of CR 403 there is a concrete curb and stormwater inlets.

The project is in an urban setting. Land use in the area is mixed residential, commercial, religious, and educational. The Sellersburg Cemetery is located on the east side of U.S. 31 near the south end of the project area.

Road Reconstruction (Preferred Alternative)

The project is approximately 1.82 miles long. The project consists of the following:

Segment 1- From Foothill Road to approximately Triangle Drive (Appendix B, B20-B23):

- Mill and repave U.S. 31.

Segment 2- U.S. 31/SR 311/Prather Lane Intersection (Appendix B, B23-B24):

- Full-depth pavement replacement with underdrains;
- Replace non-ADA-compliant curb ramps;
- Replace traffic signal;
- Minor pavement widening to adjust the lane configuration on the southwest-bound approach of U.S. 31 to provide a shared through/right-turn lane;
- Incidental storm drainage improvements.

Indiana Department of Transportation

County Clark

Route U.S. 31

Des. No. 1700111

Segment 3- Approximately Bucheit Street to CR 403 (Appendix B, B25- B31, B37):

- Full-depth pavement replacement with underdrains;
- Install new curb inlets and storm sewer trunkline;
- Install a new stormwater outlet to the UNT to Silver Creek, including riprap. Install in-line detention to release stormwater at the existing rate. The offline stormwater quality units will address water quality requirements. Install an approximately 24-inch pipe in the southeast quadrant of the UNT crossing of U.S. 31;
- Replace non-ADA-compliant curb ramps with ADA-compliant ramps;
- Replace traffic signal at Utica Street intersection;
- Add high-visibility pavement markings and signage and a rapid rectangular flashing beacon (RRFB) or pedestrian hybrid beacon (HAWK) to the crosswalk at the U.S. 31/St. Paul Street intersection;
- Improve visibility of mid-block crosswalk near the Sellersburg Library with high-visibility pavement markings and signage, RRFB, or HAWK;
- Remove mid-block crosswalk near Silver Creek Elementary and Silver Creek Middle School entrances and relocate to U.S. 31/CR 403 intersection with ADA-compliant pushbuttons and pedestrian countdown heads. Add a 6-foot-wide concrete sidewalk on the east side of U.S. 31 between the mid-block crossing and the CR 403 intersection. Extend the right turn lane from southbound CR 403 to northbound U.S. 31 to the L&I Railroad tracks. Designate the existing lane as a left-turn lane. Remove the existing grass median;
- Replace traffic signal at CR 403 intersection;
- Revise pavement markings to provide 12-foot wide two-way left-turn lane (TWLTL) from Utica Street to CR 403.

Segment 4- Approximately CR 403 to the Silver Creek Bridge (Appendix B, B31-B37):

- Mill and repave U.S. 31;
- Replace non-ADA-compliant curb ramps;
- Replace segments of sidewalk disturbed by project;
- Replace inlet castings and adjust to grade;
- Add curb inlets and connect to existing storm sewer;
- Revise pavement markings to provide 12-foot wide two-way left-turn lane (TWLTL) from CR 403 to Silver Creek Elementary and Silver Creek Middle School entrances.

Segments 1 and 4 will be constructed under traffic and segments 2 and 3 will require a detour.

The preferred alternative will meet the purpose and need for the project because it will extend the functional life of the existing pavement, improve drainage, provide ADA-compliant pedestrian facilities, and improve safety along U.S. 31 between Foothill Road and the Silver Creek Bridge. The logical termini are Foothill Road and the Silver Creek Bridge. The project has independent utility because it addresses the project purpose and need within the logical termini without the need for additional transportation improvements in the area.

OTHER ALTERNATIVES CONSIDERED:

Describe all discarded alternatives, including the Do-Nothing Alternative and an explanation of why each discarded alternative was not selected.

Do-Nothing Alternative

This alternative proposes that no construction take place. There would be no cost and no environmental impacts; however, U.S. 31 would continue to have inadequate drainage, poor pavement conditions, and a lack of ADA-compliant pedestrian facilities. Therefore, the Do-Nothing Alternative does not meet the project purpose and need and was dismissed from further consideration.

No Sidewalk Alternative

This alternative proposes to eliminate all sidewalk work from project segments 3 and 4. The cost of this alternative would be approximately \$7,3567,800.00 (compared to the preferred alternative cost of approximately \$7,923,200). This alternative would not meet the purpose and need for the project because it does not provide ADA-compliant pedestrian facilities throughout the project area. Therefore, this alternative is not considered a prudent alternative.

Indiana Department of Transportation

 County Clark

 Route U.S. 31

 Des. No. 1700111

Two Storm Trunk Lines Alternative

This alternative proposes constructing two storm trunk lines, one along each side of U.S. 31, to convey storm drainage within project segment 3. This option would allow the size of the trunklines to be reduced and some storm sewer laterals across U.S. 31 to be eliminated, which would make maintaining one lane of traffic during MOT easier. However, this alternative would require additional length of pipe and two phases of storm sewer construction. The total cost for this alternative would be approximately \$8,049,800. The preferred alternative has a total cost of approximately \$7,923,200. This alternative would meet the purpose and need for the project; however, given the additional project costs without long-term benefits to the storm sewer system, this alternative is not considered the preferred alternative.

The Do Nothing Alternative is not feasible, prudent or practicable because (Mark all that apply):

- It would not correct existing capacity deficiencies;
- It would not correct existing safety hazards;
- It would not correct the existing roadway geometric deficiencies;
- It would not correct existing deteriorated conditions and maintenance problems; or
- It would result in serious impacts to the motoring public and general welfare of the economy.
- Other (Describe): It would not address the lack of ADA-compliant pedestrian facilities or poor storm water drainage within the project area

ROADWAY CHARACTER: U.S. 31 from Foothill Road to approx. Triangle Drive (Segment 1)

Functional Classification: Urban Minor Arterial
 Current AADT: 22,426 VPD (2022) Design Year ADT: 22,476 VPD (2042)
 Design Hour Volume (DHV): 1,961 Truck Percentage (%) 10
 Designed Speed (mph): 30-40 Legal Speed (mph): 30-40

	Existing	Proposed
Number of Lanes:	2 @ 12 ft. 1 @ 0-12 ft.	1 @ 12 ft. 1 @ 12-18.7 ft. 1 @ 0-12 ft.
Type of Lanes:	Through Turn	Through Turn
Pavement Width:	38-46 ft.	36-42.7 ft.
Curb and Gutter:	N/A ft.	N/A ft.
Shoulder	1 @ 2-10 ft. 1 @ 12 ft.	1 @ 2-10 ft. 1 @ 4-15 ft.
Grass Buffer:	N/A ft.	N/A ft.
Sidewalk Width:	N/A ft.	N/A ft.

Setting: Urban Suburban Rural
 Topography: Level Rolling Hilly

ROADWAY CHARACTER: U.S. 31 from approx. Bucheit Street to High Street (part of Segment 3)

Functional Classification: Urban Minor Arterial
 Current AADT: 22,426 VPD (2022) Design Year ADT: 24,476 VPD (2042)
 Design Hour Volume (DHV): 1,961 Truck Percentage (%) 10
 Designed Speed (mph): 30 Legal Speed (mph): 30

	Existing	Proposed
Number of Lanes:	3 @ 12 ft.	3 @ 12 ft.
Type of Lanes:	Through Two-Way-Left-Turn	Through Two-Way-Left-Turn
Pavement Width:	36 ft.	36 ft.

Indiana Department of Transportation

 County Clark

 Route U.S. 31

 Des. No. 1700111

Curb and Gutter:	2 @ 0.5-1	ft.	2 @ 0.58	ft.
Grass Buffer:	1 @ 0 1 @ 0-3	ft.	2 @ 0	ft.
Sidewalk Width:	2 @ 6 (intermittent)	ft.	2 @ 6 (intermittent)	ft.
Setting:	<input checked="" type="checkbox"/> Urban	<input type="checkbox"/> Suburban	<input type="checkbox"/> Rural	
Topography:	<input checked="" type="checkbox"/> Level	<input type="checkbox"/> Rolling	<input type="checkbox"/> Hilly	

ROADWAY CHARACTER: U.S. 31 from High Street to CR 403 (part of Segment 3)

Functional Classification: Urban Minor Arterial
 Current AADT: 22,426 VPD (2022) Design Year ADT: 24,476 VPD (2042)
 Design Hour Volume (DHV): 1,961 Truck Percentage (%): 10
 Designed Speed (mph): 30 Legal Speed (mph): 30

	Existing		Proposed	
Number of Lanes:	2 @ 18 ft.		3 @ 12 ft.	
Type of Lanes:	Through		Through Two-Way-Left-Turn	
Pavement Width:	36	ft.	36	ft.
Curb and Gutter:	2 @ 0.5	ft.	2 @ 0.58	ft.
Grass Buffer:	1 @ 0 1 @ 0-6	ft.	2 @ 2	ft.
Sidewalk Width:	2 @ 6 (intermittent)	ft.	2 @ 6 (intermittent)	ft.

Setting: Urban Suburban Rural
 Topography: Level Rolling Hilly

ROADWAY CHARACTER: U.S. 31 from CR 403 to the Silver Creek Bridge (Segment 4)

Functional Classification: Urban Major Collector
 Current AADT: 11,502 VPD (2022) Design Year ADT: 12,593 VPD (2042)
 Design Hour Volume (DHV): 1,210 Truck Percentage (%): 10
 Designed Speed (mph): 45 Legal Speed (mph): 45

	Existing		Proposed	
Number of Lanes:	1 @ 18-20 ft. 1 @ 18-19 ft.		1 @ 18-20 ft. 1 @ 18-19 ft.	
Type of Lanes:	Through		Through	
Pavement Width:	36-39	ft.	36-39	ft.
Curb and Gutter:	2 @ 0-0.5	ft.	2 @ 0.58	ft.
Grass Buffer:	1 @ 0-7	ft.	1 @ 0-7	ft.
Sidewalk Width:	1 @ 0-6	ft.	1 @ 0-6	ft.

Setting: Urban Suburban Rural
 Topography: Level Rolling Hilly

Indiana Department of Transportation

County Clark

Route U.S. 31

Des. No. 1700111

DESIGN CRITERIA FOR BRIDGES: Two Culverts Only

Structure/NBI Number(s): CV 031-010-08.85 Sufficiency Rating: N/A
CV 031-010-09.80 _____
(Rating, Source of Information)

	Existing	Proposed
Bridge Type:	N/A	N/A
Number of Spans:	N/A	N/A
Weight Restrictions:	N/A	N/A
Height Restrictions:	N/A	N/A
Curb to Curb Width:	N/A	N/A
Outside to Outside Width:	N/A	N/A
Shoulder Width:	N/A	N/A
Length of Channel Work:	N/A	N/A

Describe bridges and structures; provide specific location information for small structures.

Remarks:

There are two culverts within the project area. An approximately 19 x 3 foot concrete box culvert carries U.S. 31 over Camp Run just north of Foothill Drive (CV 031-010-08.85). The culvert has a span of approximately 19 feet and is approximately 50 feet in length. The channel opening is approximately 3 feet tall. No work on this culvert is planned.

An approximately 72 x 96 inch concrete box culvert with wingwalls carries U.S. 31 over a UNT to Silver Creek approximately 0.12 mile north of Hauss Avenue (CV 031-010-09.80). The culvert has a span of approximately 8 feet and is approximately 60 feet in length. The channel opening is approximately 8 feet tall. No work on this culvert is planned.

Yes No N/A

Will the structure be rehabilitated or replaced as part of the project?
If the proposed action has multiple bridges or small structures, this section should be filled out for each structure.

MAINTENANCE OF TRAFFIC (MOT) DURING CONSTRUCTION:

	Yes	No
Is a temporary bridge proposed?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Is a temporary roadway proposed?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Will the project involve the use of a detour or require a ramp closure? (describe in remarks)	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Provisions will be made for access by local traffic and so posted.	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Provisions will be made for through-traffic dependent businesses.	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Provisions will be made to accommodate any local special events or festivals.	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Will the proposed MOT substantially change the environmental consequences of the action?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Is there substantial controversy associated with the proposed method for MOT?	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Remarks:

Segments 1 and 4 will be constructed under traffic. Segments 2 and 3 will require a detour, likely utilizing SR 60, I-65, and Blue Lick Road or SR 160. The detour will add a distance of approximately 0.5 mile for through-travelers. During closure of Segments 2 and 3, access to local residences and businesses will be provided using a phased construction plan which may maintain a single, one-way travel lane between local intersecting streets. Access for property owners and businesses will be maintained at all times. Road construction is expected to last approximately seven months.

The closure will pose a temporary inconvenience to traveling motorists (including school buses and emergency services); however, no significant delays are anticipated and all inconveniences will cease upon project completion. It is the responsibility of the project sponsor to notify school corporations and emergency services at least two weeks prior to any construction that would block or limit access.

Indiana Department of Transportation

County Clark Route U.S. 31 Des. No. 1700111

ESTIMATED PROJECT COST AND SCHEDULE:

Engineering: \$ 480,000 (2018)* Right-of-Way: \$ 240,000 (2021) Construction: \$ 4,987,027.00 (2022)*

Anticipated Start Date of Construction: Spring 2023

Date project incorporated into STIP July 2, 2019

Is the project in an MPO Area? **Yes** **No**

If yes,
 Name of MPO Kentuckiana Regional Planning & Development Agency (KIPDA)

Location of Project in TIP Fiscal Year (FY) 2020-2025 KIDPA Transportation Improvement Plan (TIP) Pg. 132-133

Date of incorporation by reference into the STIP July 2, 2019

*The CE and CN funding amounts do not match in the STIP and TIP. The project designer has been made aware of this discrepancy.

RIGHT OF WAY:

Land Use Impacts	Amount (acres)	
	Permanent	Temporary
Residential	0.36	0.17
Commercial	1.42	0.66
Agricultural	0.00	0.00
Forest	0.00	0.00
Wetlands	0.00	0.00
Other:	0.00	0.00
Other:	0.00	0.00
TOTAL	1.78	0.83

Describe both Permanent and Temporary right-of-way and describe their current use. Typical and Maximum right-of-way widths (existing and proposed) should also be discussed. Any advance acquisition or reacquisition, either known or suspected, and their impacts on the environmental analysis should be discussed.

Remarks:
 The existing typical ROW along U.S. 31 is highly varied. The existing maximum ROW along U.S. 31 is approximately 120 feet, 60 feet on each side of the centerline.

 Approximately 1.78 acres of permanent ROW acquisition is anticipated. Approximately 0.83 acre of temporary ROW acquisition is anticipated for grading and driveway reconstruction. Approximately 20%, or 0.36 acres of permanent and 0.17 acres of temporary ROW acquisition will come from residential properties. The remaining 80%, or approximately 1.42 acres of permanent and 0.66 acre of temporary ROW acquisition will be from commercial properties. If the scope of work or permanent or temporary ROW amounts change, the INDOT Environmental Services Division (ESD) and the INDOT Seymour District Environmental Section will be contacted immediately.

Indiana Department of Transportation

County Clark

Route U.S. 31

Des. No. 1700111

Part III – Identification and Evaluation of Impacts of the Proposed Action

SECTION A – ECOLOGICAL RESOURCES

	Presence	Impacts	
		Yes	No
Streams, Rivers, Watercourses & Jurisdictional Ditches	X	X	
Federal Wild and Scenic Rivers			
State Natural, Scenic or Recreational Rivers			
Nationwide Rivers Inventory (NRI) listed			
Outstanding Rivers List for Indiana			
Navigable Waterways			

Remarks:

Based on a desktop review, a site visit on October 28, 2020 by Butler, Fairman, & Seufert, Inc. (BF&S), the aerial map of the project area (Appendix B, B3), and the water resource map in the Red Flag Investigation (RFI) report (Appendix E, E12), there are 16 stream segments within the 0.5 mile search radius. Three stream segments are located within the project area, including Camp Run, a UNT to Silver Creek, and Muddy Fork.

A *Waters of the U.S. Determination* was approved by the INDOT Ecology and Waterway Permitting Office on December 4, 2020. Please refer to Appendix F for the *Waters of the U.S. Determination Report*. It was determined two waterways, Camp Run and a UNT to Silver Creek, within the project area qualified as "Waters of the United States". It was also determined one non-jurisdictional roadside ditch handling stormwater runoff is present within the project area on the east side of U.S. 31 north of Camp Run. The U.S. Army Corps of Engineers (USACE) makes all final determinations regarding jurisdiction.

There are no State Natural, Scenic, and Recreational Rivers; Outstanding Rivers for Indiana; navigable waterways present in the project area. According to the database administered by the Bureau of Land Management, National Park Service (NPS), U.S. Fish and Wildlife Service (USFWS) and U.S. Forest Service (<http://www.rivers.gov/>), there are no streams in this area of Clark County that are on the list of Wild, Scenic or Recreational Rivers. In addition, according to the database administered by the NPS <https://www.nps.gov/subjects/rivers/nationwide-rivers-inventory.htm>, there are no streams in this area of Clark County that are on the Nationwide Rivers Inventory.

The project will install a new, approximately 24-inch stormwater outlet to the UNT to Silver Creek, including riprap scour protection. The riprap will impact approximately 30 linear feet of the UNT to Silver Creek, which is a "Waters of the United States". In-line detention will be provided to release stormwater at the existing rate. Three offline stormwater quality units will be included to address water quality requirements. Work in this area of the non-jurisdictional ditch (Segment 1) will be limited to a mill and repave of U.S. 31. Therefore, no impacts to this roadside ditch are expected. A Section 404 Regional General Permit (RGP) from the USACE and a Section 401 Water Quality Certification permit from the Indiana Department of Environmental Management (IDEM) are anticipated to be required for the stormwater new outfall.

Early coordination letters were sent on September 16, 2020 (Appendix C, C1-C2). The United States Fish and Wildlife Service (USFWS) replied on September 22, 2020 and made standard recommendations for minimizing impacts by limiting work to streams (Appendix C, C17-C18). This project meets the criteria of the USFWS Interim Policy for the Review of Highway Transportation Projects in Indiana dated May 29, 2013 because it will impact less than 0.5 acre of forested ROW, will impact less than 300 feet of streams, will impact no wetlands, will not occur in an exempted stream or the National Lakeshore, and will not impact a surface karst feature. The Indiana Department of Natural Resources Division of Fish and Wildlife (IDNR-DFW) responded on October 16, 2020 with standard recommendations to minimize impacts to waterways (Appendix C, C20-C21).

All applicable agency recommendations are listed in the Environmental Commitments section of this CE document.

Indiana Department of Transportation

County Clark

Route U.S. 31

Des. No. 1700111

	<u>Presence</u>	<u>Impacts</u>	
		Yes	No
Other Surface Waters			
Reservoirs	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Lakes	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Farm Ponds	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Detention Basins	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Storm Water Management Facilities	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Other: _____	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Remarks:

Based on a desktop review, a site visit on October 28, 2020 by BF&S, the aerial map of the project area (Appendix B, B3), and the water resource map in the RFI report (Appendix E, E12), there are 15 other surface waters within the 0.5 mile search radius. No surface waters are present within or adjacent to the project area.

A *Waters of the U.S. Determination* was approved by the INDOT Ecology and Waterway Permitting Office on December 4, 2020. Please refer to Appendix F for the *Waters of the U.S. Determination Report*. It was determined one roadside ditch handling stormwater runoff is present within the project area on the east side of U.S. 31 north of Camp Run. Work in this area of the project (Segment 1) will be limited to a mill and repave of U.S. 31. Therefore, no impacts to this roadside ditch are expected.

The USFWS replied to early coordination on September 22, 2020 with standard commitments to minimize impacts to surface waters (Appendix C, C17-C18). This project meets the criteria of the USFWS Interim Policy for the Review of Highway Transportation Projects in Indiana dated May 29, 2013. The IDNR-DFW responded on October 16, 2019 with recommendations to minimize impacts to waterways (Appendix C, C20-C21).

All applicable agency recommendations are listed in the Environmental Commitments section of this CE document.

Wetlands	<u>Presence</u>	<u>Impacts</u>	
		Yes	No
	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Total wetland area: 0 acre(s) Total wetland area impacted: 0 acre(s)
 (If a determination has not been made for non-isolated/isolated wetlands, fill in the total wetland area impacted above.)

Wetland No.	Classification	Total Size (Acres)	Impacted Acres	Comments
N/A	N/A	N/A	N/A	N/A

Wetlands (Mark all that apply)	<u>Documentation</u>	<u>ES Approval Dates</u>
	Wetland Determination	<input checked="" type="checkbox"/>
Wetland Delineation	<input type="checkbox"/>	<input type="checkbox"/>
USACE Isolated Waters Determination	<input type="checkbox"/>	<input type="checkbox"/>
Mitigation Plan	<input type="checkbox"/>	<input type="checkbox"/>

Improvements that will not result in any wetland impacts are not practicable because such avoidance would result in (Mark all that apply and explain):

Substantial adverse impacts to adjacent homes, business or other improved properties;	<input type="checkbox"/>
Substantially increased project costs;	<input type="checkbox"/>
Unique engineering, traffic, maintenance, or safety problems;	<input type="checkbox"/>
Substantial adverse social, economic, or environmental impacts, or	<input type="checkbox"/>
The project not meeting the identified needs.	<input type="checkbox"/>

Measures to avoid, minimize, and mitigate wetland impacts need to be discussed in the remarks box.

Remarks: Based on a review of the National Wetlands Inventory (NWI) map online mapper (<https://www.fws.gov/wetlands/data/Mapper.html>; Appendix F, F10), the USGS topographic map (Appendix B,

Indiana Department of Transportation

County Clark

Route U.S. 31

Des. No. 1700111

B2), and the RFI report (Appendix E), 42 wetland polygons are located within the 0.5 mile search radius. The nearest wetlands are mapped on both sides of U.S. 31 north of Muddy Fork and are adjacent to, but outside, the project area. Work in this area of the project will consist of milling and repaving U.S. 31 and will end on the south side of the bridge over Muddy Fork (the Silver Creek Bridge). Therefore, no impacts are expected.

A *Waters of the U.S. Determination* was approved by the INDOT Ecology and Waterway Permitting Office on December 4, 2020. Please refer to Appendix F for the *Waters of the U.S. Determination Report*. No wetlands were located within the project area.

The USFWS replied to early coordination on September 22, 2020 and did not make any comments specific to wetlands (Appendix C, C17-C18). This project meets the criteria of the USFWS Interim Policy for the Review of Highway Transportation Projects in Indiana dated May 29, 2013. The IDNR-DFW responded on October 16, 2020 and did not make any comments specific to wetlands (Appendix C, C20-C21).

All applicable agency recommendations are listed in the Environmental Commitments section of this CE document.

<u>Presence</u>	<u>Impacts</u>	
	<u>Yes</u>	<u>No</u>
<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

Terrestrial Habitat
 Unique or High Quality Habitat

Use the remarks box to identify each type of habitat and the acres impacted (i.e. forested, grassland, farmland, lawn, etc).

Remarks:

Based on a desktop review, a site visit on October 28, 2020 by BF&S (Appendix F, F1-F3), and the aerial map of the project area (Appendix B, B3), there is upland vegetation along the corridor. The dominant species are Eastern cottonwood (*Populus deltoides*), green ash (*Fraxinus pennsylvanica*) and sycamore (*Platanus occidentalis*). Approximately 0.21 acre of this type of habitat will be impacted by the stormwater outlet to the UNT to Silver Creek.

Land use in the vicinity of the project area is mixed residential, commercial, educational, and industrial, with one cemetery present. Many of these properties have maintained lawns and herbs, which provide limited habitat for small mammals, snakes, birds, and insects. This habitat is not considered to be unique or of high quality. Approximately 0.07 acre of this type of habitat will be impacted by the removal of the median at the intersection of U.S. 31 and CR 403 and minor pavement widening on the south side of the intersection of U.S. 31 and CR 311 and on the north side of the intersection of U.S. 31 and Prather Street.

There are also areas of upland forest present around Camp Run and the UNT to Silver Creek. Approximately 0.01 acre of tree clearing in this habitat will occur dur to the new storm sewer outlet to the UNT to Silver Creek. This area is dominated by green ash (*Fraxinus pennsylvanica*), red mulberry (*Morus rubra*), and hackberry (*Celtis occidentalis*).

The USFWS replied to early coordination on September 22, 2020, and provided recommendations to limit tree clearing (Appendix C, C17-C18). This project meets the criteria of the USFWS Interim Policy for the Review of Highway Transportation Projects in Indiana dated May 29, 2013. The IDNR-DFW responded on October 16, 2020 and made recommendations to limit impacts and revegetate disturbed areas (Appendix C, C20-C21). All applicable agency recommendations are listed in the Environmental Commitments section of this CE document.

If there are high incidences of animal movements observed in the project area, or if bridges and other areas appear to be the sole corridor for animal movement, consideration of utilizing wildlife crossings should be taken.

Karst

Is the proposed project located within or adjacent to the potential Karst Area of Indiana?
 Are karst features located within or adjacent to the footprint of the proposed project?
 If yes, will the project impact any of these karst features?

<u>Yes</u>	<u>No</u>
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>

Use the remarks box to identify any karst features within the project area. (Karst investigation must comply with the Karst MOU, dated October 13, 1993

Remarks:

Based on a desktop review, the project is located outside of the designated karst region of Indiana as outlined in the October 13, 1993 Memorandum of Understanding (MOU). According to the topographic map of the

Indiana Department of Transportation

County Clark Route U.S. 31 Des. No. 1700111

project area (Appendix B, B2), and the RFI report (Appendix E), there are no karst features identified within or adjacent to the project area. In the early coordination response, the Indiana Geological Survey (IGS) did not indicate that karst features may exist in the project area (Appendix C, C7-C9). IGS also reported high liquefaction potential and a high potential for bedrock resources within the project area. The response from IGS has been communicated with the designer on September 16, 2020. No impacts are expected.

	<u>Presence</u>	<u>Impacts</u>	
		Yes	No
Threatened or Endangered Species			
Within the known range of any federal species	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Any critical habitat identified within project area	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Federal species found in project area (based upon informal consultation)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
State species found in project area (based upon consultation with IDNR)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Is Section 7 formal consultation required for this action?	<input type="checkbox"/> Yes	<input checked="" type="checkbox"/> No
--	------------------------------	--

Remarks:

Based on a desktop review and the RFI (Appendix E), completed by BF&S and approved on September 4, 2018, the IDNR Clark County Endangered, Threatened and Rare (ETR) Species List has been checked and is included in (Appendix E, E15-E18). The highlighted species on the list reflect the federal and state identified ETR species located within the county. According to the IDNR-DFW early coordination response letter dated October 16, 2020 (Appendix C, C20-C21), the Natural Heritage Program's Database has been checked. The IDNR-DFW reported no endangered, threatened, or rare species have been documented in the project vicinity. As a result of the above information, impacts to listed federal ETR species are not likely.

Indiana Bat and Northern Long-Eared Bat

Project information was submitted through the USFWS's Information for Planning and Consultation (IPaC) portal, and an official species list was generated (Appendix C, C22-C29). This project is within the range of the federally endangered Indiana bat (*Myotis sodalis*) and the federally threatened Northern long-eared bat (NLEB) (*Myotis septentrionalis*). One other species was found within or adjacent to the project area other than the Indiana bat and northern long-eared bat. Refer to paragraph below.

The project qualifies for the *Range-wide Programmatic Informal Consultation for the Indiana bat and northern long-eared bat (NLEB)*, dated May 2016 (revised February 2018), between FHWA, the Federal Railroad Administration (FRA), Federal Transit Authority (FTA), and USFWS. An effect determination was completed on September 23, 2020 and based on the responses provided it was found the project "May Affect-Not Likely to Adversely Affect" the Indiana bat and/or the NLEB. INDOT reviewed and verified the effect finding on September 23, 2020 and requested USFWS's review of the finding (Appendix C, C30-C44). No response was received from USFWS within the 14-day review period; therefore, it was concluded they concur with the finding. Avoidance and Mitigation Measures (AMMs) are included as firm commitments in the Environmental Commitments section of this document.

The official species list generated from IPaC indicated one other species present within the project area: the gray bat (*Myotis grisescens*). The project qualifies for the *USFWS Interim Policy*. Further coordination with USFWS is not required.

This precludes the need for further consultation on this project as required under Section 7 of the Endangered Species Act, as amended. If new information on endangered species at the site becomes available, or if project plans are changed, USFWS will be contacted for consultation. AMMs are included as firm commitments in the Environmental Commitments section of this document.

Indiana Department of Transportation

County Clark

Route U.S. 31

Des. No. 1700111

SECTION B – OTHER RESOURCES

Drinking Water Resources

- Wellhead Protection Area
- Public Water System(s)
- Residential Well(s)
- Source Water Protection Area(s)
- Sole Source Aquifer (SSA)

	<u>Presence</u>	<u>Impacts</u>	
		<u>Yes</u>	<u>No</u>
Wellhead Protection Area			
Public Water System(s)	X	X	
Residential Well(s)			
Source Water Protection Area(s)			
Sole Source Aquifer (SSA)			

If a SSA is present, answer the following:

- Is the Project in the St. Joseph Aquifer System?
- Is the FHWA/EPA SSA MOU Applicable?
- Initial Groundwater Assessment Required?
- Detailed Groundwater Assessment Required?

	<u>Yes</u>	<u>No</u>
Is the Project in the St. Joseph Aquifer System?		
Is the FHWA/EPA SSA MOU Applicable?		
Initial Groundwater Assessment Required?		
Detailed Groundwater Assessment Required?		

Remarks:

The project is located in Clark County, which is not located within the area of the St. Joseph Sole Source Aquifer, the only legally designated sole source aquifer in the state of Indiana. Therefore, the FHWA/EPA Sole Source Aquifer Memorandum of Understanding (MOU) is not applicable to this project. Therefore, a detailed groundwater assessment is not needed, and no impacts are expected.

The IDEM Wellhead Proximity Determinator website (<http://www.in.gov/idem/cleanwater/pages/wellhead/>) was accessed on September 16, 2020 by BF&S. This project is not located within a Wellhead Protection Area or a Source Water Protection Area. No impacts are expected.

The IDNR's Water Well Record Database website (<https://indnr.maps.arcgis.com/apps/webappviewer/index.html?id=4b4f37e1dde744ce865e1be4d157ac93>) was accessed on November 5, 2020 by BF&S. No wells are located near this project. Therefore, no impacts are expected.

Based on a desktop review of the INDOT Municipal Separate Storm Sewer Systems (MS4) website (<https://entapps.indot.in.gov/MS4/>) by BF&S on December 21, 2017 and the RFI report, this project is located within an Urban Area Boundary (UAB). An early coordination letter was sent on September 16, 2020 to the Sellersburg/Clark County MS4 Coordinator. The MS4 coordinator did not respond within the 30-day time frame. Stormwater runoff in the project area is currently handled by partial curbs and runs along the road along U.S. 31. This project will replace curbs, gutters, and inlets on U1 31 north of SR 311 and add new storm sewer between Bucheit Street and CR 403. The project will install a new stormwater outlet to the UNT to Silver Creek, including riprap scour protection. In-line detention will be provided to release stormwater at the existing rate.

Based on a desktop review, a site visit on December 21, 2017 by BF&S, and the aerial map of the project area (Appendix B, B3), this project is located where there is a public water system. The public water system will be affected due to conflicts with the storm sewer depth between Bucheit Street and CR 403. Early coordination letters were sent on December 13, 2017 to the Rural Membership Water Corp. of Clark County. No response was received. The horizontal location of the new sewer minimizes conflicts with the public water system. However, total avoidance alternatives are not practical because the depth of the storm sewer is a function of pipe length at a minimum slope from the outlet point in order to maintain gravity flow. Waterline relocation is expected and coordination with the water utility will be continued through construction.

Indiana Department of Transportation

County Clark

Route U.S. 31

Des. No. 1700111

Flood Plains	Presence	Impacts	
		Yes	No
Longitudinal Encroachment	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Transverse Encroachment	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Project located within a regulated floodplain	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Homes located in floodplain within 1000' up/downstream from project	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Discuss impacts according to classification system described in the "Procedural Manual for Preparing Environmental Studies".

Remarks:

The IDNR Indiana Floodway Information Portal website (<http://dnrmmaps.dnr.in.gov/appsphp/fdms/>) was accessed on November 6, 2020 by BF&S, this project is located in a regulatory floodplain as determined from approved IDNR floodplain maps (Appendix F, F11). An early coordination letter was sent on September 16, 2020, to the local Floodplain Administrator (Sellersburg Planning Commission). The floodplain administrator did not respond within the 30-day time frame. The project will include in-line detention in order to release stormwater at the existing rate. This project qualifies as a Category 2 per the current INDOT CE Manual, which covers work that does not involve the modification of any drainage structures.

This project will not involve the replacement or modification of any existing drainage structures or the addition of any new drainage structures. As a result, this project will not affect flood heights or floodplain limits. This project will not increase flood risks or damage, and it will not adversely affect existing emergency services or emergency routes, therefore, it has been determined that this encroachment is not substantial.

Farmland	Presence	Impacts	
		Yes	No
Agricultural Lands	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Prime Farmland (per NRCS)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Total Points (from Section VII of CPA-106/AD-1006* N/A

**If 160 or greater, see CE Manual for guidance.*

See CE Manual for guidance to determine which NRCS form is appropriate for your project.

Remarks:

Based on a desktop review, a site visit on December 21, 2017 by BF&S, and the aerial map of the project area (Appendix B, B3), there is no land that meets the definition of farmland under the Farmland Protection Policy Act (FPPA) within or adjacent to the project area. The requirements of the FPPA do not apply to this project; therefore, no impacts are expected. An early coordination letter was sent to the Natural Resources Conservation Service (NRCS) on September 16, 2020. The NRCS responded on September 23, 2020 and stated the project will not cause a conversion of prime farmland (Appendix C, C16).

Indiana Department of Transportation

County Clark

Route U.S. 31

Des. No. 1700111

SECTION C – CULTURAL RESOURCES

	Category	Type	INDOT Approval Dates	N/A
Minor Projects PA Clearance				X

Eligible and/or Listed Resource Present

Results of Research

Archaeology	
NRHP Buildings/Site(s)	X
NRHP District(s)	
NRHP Bridge(s)	

Project Effect

No Historic Properties Affected No Adverse Effect Adverse Effect

Documentation Prepared

Documentation (mark all that apply)

		ES/FHWA Approval Date(s)	SHPO Approval Date(s)
Historic Properties Short Report			
Historic Property Report	X	February 24, 2020	March 31, 2020
Archaeological Records Check/ Review			
Archaeological Phase Ia Survey Report	X	February 4, 2020	March 31, 2020
Archaeological Phase Ic Survey Report			
Archaeological Phase II Investigation Report			
Archaeological Phase III Data Recovery			
APE, Eligibility and Effect Determination	X	November 12, 2020	November 30, 2020
800.11 Documentation	X	November 12, 2020	November 30, 2020

MOA Signature Dates (List all signatories)

Memorandum of Agreement (MOA)

Describe all efforts to document cultural resources, including a detailed summary of the Section 106 process, using the categories outlined in the remarks box. The completion of the Section 106 process requires that a Legal Notice be published in local newspapers. Please indicate the publication date, name of paper(s) and the comment period deadline. Likewise include any further Section 106 work which must be completed at a later date, such as mitigation or deep trenching.

Remarks:

Area of Potential Effects (APE):
 The Area of Potential Effects (APE) includes the existing and proposed ROW, immediately adjacent properties, and those areas where a visual differentiation may occur between an existing structure and the project area. The APE consists largely of a corridor surrounding U.S. 31, expanding in less-dense areas (Appendix D, D12).

Coordination with Consulting Parties:
 In addition to the Indiana State Historic Preservation Officer (SHPO), the following individuals/organizations were sent early coordination via email on November 26, 2019 (Appendix D, D14-D20):

Indiana Department of Transportation

County Clark

Route U.S. 31

Des. No. 1700111

Consulting Party	Response
Indiana Landmarks Southern Regional Office	No Response
Clark County Historian	No Response
Jeff-Clark Preservation, Inc.	No Response
Clark's Grant Historical Society	No Response
Kentuckiana Regional Planning & Development Agency	No Response
Sellersburg Clerk-Treasurer	No Response
Sellersburg Town Council	No Response
Sellersburg Streets & Sanitation Department	No Response
Clark County Commissioners	No Response
Clark County Highway Superintendent	No Response
Eastern Shawnee Tribe of Oklahoma	No Response
Miami Tribe of Oklahoma	December 18, 2019
Peoria Tribe of Indians of Oklahoma	No Response
Pokagon Band of Potawatomi Indians	No Response
United Keetoowah Band of Cherokee Indians	No Response
Delaware Tribe of Indians	No Response

The Miami Tribe of Oklahoma responded on December 18, 2019 indicating they wished to be a consulting party and stating they had no objections to the project (Appendix D, D25). The SHPO responded on December 13, 2019 and stated they were not aware of any other consulting parties that should be invited.

No other responses to the November 26, 2019 mailing were received.

Archaeology:

In regard to archaeology, a Phase Ia archaeological reconnaissance was conducted by 106 Consulting, LLC on December 21, 2017. The archaeologist did not locate any archaeological resources within the project area. No further work was recommended in the resulting archaeological short report (January 30, 2020, Appendix D, D2-D31).

INDOT-CRO approved the archaeological report on February 4, 2020 (Appendix D, D32). The letter was referred to the SHPO on February 25, 2020 (Appendix D, D31). The SHPO responded on March 31, 2020, concurring with the opinion of the archaeologist, stating in part, *"we have not identified any currently known archaeological resources listed in or eligible for inclusion in the NRHP within the proposed project area"* (Appendix D, D34-D37).

Historic Properties:

A site visit was conducted by a DHPA-Qualified Professional with BF&S on December 21, 2017. Information from the site visit and research regarding historic resources, including buildings, structures, districts, and objects, was compiled into a Historic Property Report (HPR). The English cottage at 479 N. Indiana Avenue, and the bridge carrying the Southern Indiana Railway over Creek Drive and Muddy Fork were eligible for listing in the National Register of Historic Places (NRHP, BF&S, February 24, 2020). INDOT-CRO approved the HPR on February 24, 2019 (Appendix D, D33). Consulting parties were sent instructions on how to access the Historic Property Report (HPR) in INSCOPE, INDOT's Section 106 Consultation and Outreach Portal Enterprise, on February 25, 2020 (Appendix D, D34-D37).

The SHPO responded on March 31, 2020, disagreeing with the eligibility of the bridge carrying the Southern Indiana Railway over Creek Drive and Muddy Fork. The SHPO further stated, in part, *"[...] aside from the House at 479 N. Indiana Avenue and the Dr. Quincy Robert Hauss House at 227 N. Albany Street, there are no other historic properties listed in or eligible for inclusion in the NRHP in the project's APE."* INDOT elected to defer to the SHPO's determination, therefore only the Dr. Q. Robert Hauss House and 479 N. Indiana Avenue were considered eligible for the NRHP.

No other responses to the February 25, 2020 mailing were received.

Documentation, Findings:

An Effects Letter was approved by INDOT-CRO on June 10, 2020 and sent to consulting parties on June 11, 2020 (Appendix D, D41). The letter recommended the project had "No Adverse Effect" on the Dr. Q. Robert

Indiana Department of Transportation

County Clark

Route U.S. 31

Des. No. 1700111

Hauss House and 479 N. Indiana Avenue due to the lack of physical changes or permanent ROW acquisition from the properties and the limited changes to the setting (Appendix D, D41-D47).

The SHPO responded to the Effects Letter on June 29, 2020, concurring with the "No Adverse Effect" determination, stating in part, "We agree with the opinions expressed in the June 11, 2020 effects letter that the Dr. Q. Robert Hauss House (Indiana Historic Sites and Structures Inventory ["IHSSI"] #019-604-51035) and the house at 479 N. Indiana Avenue (IHSSI #019-604-51008), which are eligible for inclusion in the National Register of Historic Places ("NRHP"), will not be adversely affected by this project" (Appendix D, D48-D49). No other responses to the Effects Letter were received.

INDOT approved an 800.11(d) finding of "No Adverse Effect" on November 12, 2020 (Appendix D, D1-D11, D50). The INDOT-approved finding was forwarded to the SHPO and consulting parties on November 13, 2020. (Appendix D, D51-D52). SHPO concurred with the "No Adverse Effect" finding on November 30, 2020 (Appendix D, D53-D54).

No other responses to the 800.11(d) were received.

Work will take place within 100 feet of the Sellersburg Cemetery, including road repaving, pavement replacement, curb ramp replacement, and storm sewer and inlet installation. A Cemetery Development Plan per IC 14-21-1-26.5 will be required.

Public Involvement:
 A public notice regarding the "No Adverse Effect" finding was published in *The Clark County News and Tribune* on November 17, 2020 (Appendix D, D55). No public comments were received by the established 30-day deadline date of December 17, 2020. Therefore, the Section 106 process has been completed and the FHWA's Section 106 responsibilities have been fulfilled.

SECTION D – SECTION 4(f) RESOURCES/ SECTION 6(f) RESOURCES

Section 4(f) Involvement (mark all that apply)

Parks & Other Recreational Land

- Publicly owned park
- Publicly owned recreation area
- Other (school, state/national forest, bikeway, etc.)

Presence

X

Use

Yes	No
	X

Evaluations

Prepared

- Programmatic Section 4(f)*
- "De minimis" Impact*
- Individual Section 4(f)

FHWA
Approval date

--

Wildlife & Waterfowl Refuges

- National Wildlife Refuge
- National Natural Landmark
- State Wildlife Area
- State Nature Preserve

Presence

Use

Yes	No

Evaluations

Prepared

- Programmatic Section 4(f)*
- "De minimis" Impact*
- Individual Section 4(f)

FHWA
Approval date

--

Indiana Department of Transportation

County Clark

Route U.S. 31

Des. No. 1700111

Historic Properties

Sites eligible and/or listed on the NRHP

Presence

Use

Yes	No
<input type="checkbox"/>	<input checked="" type="checkbox"/>

Evaluations

Prepared

FHWA

Approval date

**FHWA approval of the environmental document also serves as approval of any Section 4f Programmatic and/or De minimis evaluation(s) discussed below.*

Discuss Programmatic Section 4(f) and "de minimis" Section 4(f) impacts in the remarks box below. Individual Section 4(f) documentation must be separate Draft and Final documents. For further discussions on Programmatic, "de minimis" and Individual Section 4(f) evaluations please refer to the "Procedural Manual for the Preparation of Environmental Studies". Discuss proposed alternatives that satisfy the requirements of Section 4(f).

Remarks

Section 4(f) of the U.S. Department of Transportation Act of 1966 prohibits the use of certain public and historic lands for federally funded transportation facilities unless there is no feasible or prudent alternative. The law applies to significant publicly owned parks, recreation areas, wildlife/waterfowl refuges, and NRHP-eligible or listed historic properties. Lands subject to this law are considered Section 4(f) resources.

Based on a desktop review, a site visit on December 21, 2017 by BF&S, the aerial map of the project area (Appendix B, B3), coordination with the SHPO, and the RFI report (Appendix E), there are 12 Section 4(f) resources located within 0.5 mile of the project. There are four Section 4(f) resources located within or adjacent to the project area. Two recreational resources identified in the RFI, Silver Creek schools and St. John Paul II Catholic School, contain playground equipment. The St. John Paul II Catholic Facilities are not owned by or open to the public. The recreational facilities associated with the Silver Creek Schools are located behind (west of) the school buildings and are, therefore, not adjacent to the project area. Access to this property will not be altered. Therefore, no use of these properties is expected.

Speed Park is adjacent to the project area in the community of Speed. However, this property is not publicly owned, and, therefore, does not qualify as a Section 4(f) recreational resource.

The House at 479 N. Indiana Avenue and the Dr. Quincy Robert Hauss House at 227 N. Albany Street, are eligible for listing in the NRHP. Therefore, these resources are subject to evaluation through Section 4(f) of the Transportation Act of 1966. Approximately 0.02 acre of temporary ROW will be acquired from Dr. Quincy Robert Hauss House and less than 0.01 acre of temporary ROW will be acquired from 479 N. Indiana Avenue for private driveway reconstruction. The temporary ROW acquisition and work within the temporary ROW will meet the conditions of a temporary occupancy as listed in 23 CFR 774.13(d):

1. Duration of work must be temporary, i.e., less than the time needed for construction of the project, and there should be no change in ownership of the land;
2. Scope of the work must be minor, i.e., both the nature and the magnitude of the changes to the Section 4(f) property are minimal;
3. There are no anticipated permanent adverse physical impacts, nor will there be interference with the protected activities, features, or other attributes of the property, on either a temporary or permanent basis;
4. The land being used must be fully restored, i.e., the property must be returned to a condition which is at least as good as that which existed prior to the project;
5. There must be documented agreement of the official(s) with jurisdiction over the Section 4(f) resource regarding the above conditions.

The temporary ROW will be used for driveway reconstruction, which will take less time than the total construction of the project. There will be no change in land ownership. The scope of work will be minor, consisting of driveway and landscape work to restore the previous conditions. No permanent adverse impacts to any physical elements of the property are anticipated and the property's use and attributes will not be affected. The land will be fully restored to its pre-existing condition. The SHPO is the Official With Jurisdiction (OWJ) for historic properties. The SHPO concurred with the "No Adverse Effect" finding on November 30, 2020 (Appendix D, D53-D54).

Indiana Department of Transportation

County Clark Route U.S. 31 Des. No. 1700111

The project will not use these resources by taking permanent ROW and will not alter the environment in such a way as to constitute constructive use of this resource. Therefore, no use is expected.

No other potential Section 4(f) resources were identified within or adjacent to the project area.

Section 6(f) Involvement Presence Use

Section 6(f) Property

Discuss proposed alternatives that satisfy the requirements of Section 6(f). Discuss any Section 6(f) involvement.

Remarks:

The U.S. Land and Water Conservation Fund Act of 1965 established the Land and Water Conservation Fund (LWCF), which was created to preserve, develop, and assure accessibility to outdoor recreation resources. Section 6(f) of this Act prohibits conversion of lands purchased with LWCF monies to a non-recreation use.

A review of 6(f) properties on the Land and Water Conservation Fund (LWCF) website at <https://lwcfcoalition.org/tools> revealed a total of 20 properties in Clark County (Appendix I, I1). None of these properties are located within or adjacent to the project area. Therefore, there will be no impacts to 6(f) resources as a result of this project.

SECTION E – Air Quality

Air Quality

Conformity Status of the Project

	Yes	No
Is the project in an air quality non-attainment or maintenance area?	<input checked="" type="checkbox"/>	<input type="checkbox"/>
If YES, then:		
Is the project in the most current MPO TIP?	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Is the project exempt from conformity?	<input checked="" type="checkbox"/>	<input type="checkbox"/>
If the project is NOT exempt from conformity, then:		
Is the project in the Transportation Plan (TP)?	<input type="checkbox"/>	<input type="checkbox"/>
Is a hot spot analysis required (CO/PM)?	<input type="checkbox"/>	<input type="checkbox"/>

Level of MSAT Analysis required?

Level 1a Level 1b Level 2 Level 3 Level 4 Level 5

Remarks:

This project is included in the FY 2020-2025 Kentuckiana Regional Planning & Development Agency (KIPDA) Transportation Improvement Plan (TIP) and the FY 2020-2024 Statewide Transportation Improvement Program (STIP) (Appendix G, G1-G3).

The project is located in Clark County, which is currently a non-attainment area of the 2015 8-hour Ozone Standard according to IDEM's non-attainment map. This project has been identified as being exempt from air quality analysis in accordance with 40 CFR Part 93.126 and this project is not a project of air quality concern (40 CFR Part 93.123). Therefore, the project will have no significant impact on air quality.

This project is of a type qualifying as a categorical exclusion (Group 1) under 23 CFR 771.117(c), or exempt under the Clean Air Act conformity rule under 40 CFR 93.126, and as such, a Mobile Source Air Toxics analysis is not required.

Indiana Department of Transportation

County Clark

Route U.S. 31

Des. No. 1700111

SECTION F - NOISE

Noise **Yes** **No**
 Is a noise analysis required in accordance with FHWA regulations and INDOT's traffic noise policy?

	No	Yes/ Date
ES Review of Noise Analysis	<input type="checkbox"/>	<input type="checkbox"/>

Remarks:

This project is a Type III project. In accordance with 23 CFR 772 and the current *Indiana Department of Transportation Traffic Noise Analysis Procedure*, this action does not require a formal noise analysis.

SECTION G – COMMUNITY IMPACTS

Regional, Community & Neighborhood Factors

	Yes	No
Will the proposed action comply with the local/regional development patterns for the area?	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Will the proposed action result in substantial impacts to community cohesion?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Will the proposed action result in substantial impacts to local tax base or property values?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Will construction activities impact community events (festivals, fairs, etc.)?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Does the community have an approved transition plan?	<input checked="" type="checkbox"/>	<input type="checkbox"/>
If No, are steps being made to advance the community's transition plan?	<input type="checkbox"/>	<input type="checkbox"/>
Does the project comply with the transition plan? (explain in the remarks box)	<input checked="" type="checkbox"/>	<input type="checkbox"/>

Remarks:

The project will be constructed in an urban/suburban environment and will not alter local development patterns near the project area.

It is not anticipated the project will result in substantial impacts to community cohesion, property values, or community events. No increase in local taxes will occur as a result of this project, as all funds will come from the FHWA and established INDOT accounts (Appendix H, H1-H3). The project does not divide a community or impair any areas where the community hosts events. Early coordination letters were sent to the Town of Sellersburg, Sellersburg Town Council, Sellersburg Plan Commission, Sellersburg Parks and Recreation Department, Sellersburg Police Department, Clark County Commissioners, and Clark County Plan Commission on September 16, 2020 (Appendix C, C1-C2). No responses to early coordination were received. No community events with the potential to be impacted by this project were identified as a result of early coordination or a review of the Clark County, Town of Sellersburg, and IDNR websites by BF&S. Road construction is expected to last approximately seven months.

INDOT updated its ADA Transition Plan on May 18, 2015 ([http://www.in.gov/indot/files/2015 Transition Plan.pdf](http://www.in.gov/indot/files/2015%20Transition%20Plan.pdf)). This project will be designed according to INDOT Design Standards, and will therefore comply with INDOT's ADA transition plan.

Based on the above investigations and coordination, no permanent community or economic impacts are anticipated from this project.

Indirect and Cumulative Impacts **Yes** **No**
 Will the proposed action result in substantial indirect or cumulative impacts?

Remarks:

Indirect impacts are effects which are caused by the action and are later in time or farther removed in distance but are still reasonably foreseeable. Indirect effects may include growth inducing effects and other effects related to induced changes in the pattern of land use, population density, or growth rate. Cumulative impacts

Indiana Department of Transportation

County Clark Route U.S. 31 Des. No. 1700111

affect the environment through the incremental impact of the action when added to other past, present, and reasonably foreseeable future actions regardless of what agency or person undertakes such actions.

The project will not change the general land uses in the area. The change to the viewshed will be minimal. As a result, this project is not anticipated to have any substantial indirect or cumulative impacts to the area.

Public Facilities & Services

Will the proposed action result in substantial impacts on health and educational facilities, public and private utilities, emergency services, religious institutions, airports, public transportation or pedestrian and bicycle facilities? *Discuss how the maintenance of traffic will affect public facilities and services.*

Yes	No
<input type="checkbox"/>	<input checked="" type="checkbox"/>

Remarks:

Based on a desktop review, a site visit on December 21, 2017 by BF&S, utility coordination, and the aerial map of the project area (Appendix B, B3), there are 14 public facilities and services (five schools, five utilities, one fire station, one police station, one library, and one park) located within 0.5 mile of the project. Four schools, a library, and a park are located adjacent to the project area. The use of these facilities and services will not change. Access to all properties will be maintained during construction. Therefore, no impacts are expected.

Early coordination letters were sent to to the Town of Sellersburg, Sellersburg Town Council, Sellersburg Plan Commission, Sellersburg Parks and Recreation Department, Sellersburg Police Department, West-Clark School District, Clark County Commissioners, and Clark County Plan Commission on September 16, 2020 (Appendix C, C1-C2). West-Clark School District responded on September 16, 2020 and stated the project would cause school bus transportation problems if it was not completed before the beginning of the school year (Appendix C, C6). Coordination with the West-Clark School District concerning detour routes for buses will be continued to project construction. No other responses to early coordination were received.

The INDOT Office of Aviation responded to early coordination on September 16, 2020, and stated if any object, obstruction, or equipment will exceed 95 feet in height, further coordination will be required (Appendix C, C5). No such object, obstruction, or equipment will be required; therefore, no impact is expected.

It is the responsibility of the project sponsor to notify school corporations and emergency services at least two weeks prior to any construction that would block or limit access.

Environmental Justice (EJ) (Presidential EO 12898)

During the development of the project were EJ issues identified?

Does the project require an EJ analysis?

If YES, then:

Are any EJ populations located within the project area?

Will the project result in adversely high or disproportionate impacts to EJ populations?

Yes	No
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input checked="" type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>

Remarks:

Under FHWA Order 6640.23A, FHWA and the project sponsor, as a recipient of funding from FHWA, are responsible to ensure that their programs, policies, and activities do not have a disproportionately high and adverse effect on minority or low-income populations. Per the current INDOT Categorical Exclusion Manual, an Environmental Justice (EJ) Analysis is required for any project that has two or more relocations or 0.5 acre of additional permanent right-of-way. The project will require 1.78 acres of permanent ROW. Therefore, an EJ Analysis is required.

Potential EJ impacts are detected by locating minority and low-income populations relative to a reference population to determine if populations of EJ concern exists and whether there could be disproportionately high and adverse impacts to them. The reference population may be a county, city or town and is called the community of comparison (COC). In this project, the COC is Clark County, Indiana. The community that overlaps the project area is called the affected community (AC). In this project, the ACs are Census Tract 507.03, Clark County, Indiana (AC-1) and Census Tract 507.4, Clark County, Indiana (AC-2). An AC has a population of concern for EJ if the population is more than 50% minority or low-income or if the low-income or minority population is 125% of the COC. Data from the 2019 American Community Survey 5-Year Estimates was obtained from the US Census Bureau Website <https://data.census.gov/cedsci/> on January 18, 2021 by BF&S. The data collected for minority and low-income populations within the AC are summarized in the below

Indiana Department of Transportation

County Clark

Route U.S. 31

Des. No. 1700111

table.

Table: Minority and Low-Income Data (American Community Survey, 2019)			
	COC – Clark County, Indiana	AC-1 - Census Tract 507.03, Clark County, Indiana	AC-2 - Census Tract 507.04, Clark County, Indiana
Percent Minority	16.8 %	18.9 %	9.1 %
125% of COC	21.0 %	AC < 125% COC	AC < 125% COC
EJ Population of Concern		No	No
Percent Low-Income	10.1 %	6.5 %	6.7 %
125% of COC	12.6 %	AC < 125% COC	AC < 125% COC
EJ Population of Concern		No	No

AC-1, Census Tract 507.03, Clark County, Indiana, has a percent minority of 9.1%, which is below 50% and below the 125% COC threshold. AC-2, Census Tract 507.04, Clark County, Indiana, has a percent minority of 18.9%, which is below 50% and below the 125% COC threshold. Therefore, the ACs do not contain minority populations of EJ concern.

AC-1, Census Tract 507.03, Clark County, Indiana, has a percent low-income of 6.5%, which is below 50% and below the 125% COC threshold. AC-2, Census Tract 507.04, Clark County, Indiana, has a percent low-income of 6.7%, which is below 50% and below the 125% COC threshold. Therefore, the ACs do not contain low-income populations of EJ concern.

The census data sheets, map, and calculations can be found in Appendix I. There are no environmental justice populations of concern and no further environmental justice analysis is warranted.

Relocation of People, Businesses or Farms

Will the proposed action result in the relocation of people, businesses or farms?
 Is a Business Information Survey (BIS) required?
 Is a Conceptual Stage Relocation Study (CSRS) required?
 Has utility relocation coordination been initiated for this project?

Yes	No
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input checked="" type="checkbox"/>	<input type="checkbox"/>

Number of relocations: Residences: 0 Businesses: 0 Farms: 0 Other: 0

If a BIS or CSRS is required, discuss the results in the remarks box.

Remarks: No relocations of people, businesses, or farms will take place as a result of this project.

SECTION H – HAZARDOUS MATERIALS & REGULATED SUBSTANCES

Documentation

Hazardous Materials & Regulated Substances (Mark all that apply)

Red Flag Investigation X
 Phase I Environmental Site Assessment (Phase I ESA)
 Phase II Environmental Site Assessment (Phase II ESA)
 Design/Specifications for Remediation required?

No Yes/ Date

ES Review of Investigations	X / September 4, 2018
------------------------------------	------------------------------

Include a summary of findings for each investigation.

Remarks: Based on a review of GIS and available public records, an RFI was completed by BF&S on July 25, 2018 and approved by INDOT Site Assessment & Management (SAM) on September 4, 2018 (Appendix E, E1-E10). Two RCRA Generator sites, six state cleanup sites, one tire waste site, 12 underground storage tank (UST)

Indiana Department of Transportation

County Clark Route U.S. 31 Des. No. 1700111

sites, one institutional control site, one tire waste site, one National Pollutant Discharge Elimination System (NPDES) facility, 13 NPDES pipe locations, and 13 leaking underground storage tank (LUST) sites are located within 0.5 mile of the project area. One UST site, one institutional control site, four state cleanup sites, and nine LUST sites are located adjacent to or within the project area. One UST site, one institutional control site, and nine LUST sites may impact the project.

The nearest RCRA Generator site is mapped approximately 0.03 mile east of the project area. No impacts are expected.

The nearest state cleanup sites are adjacent to the project area. Based on records in the IDEM Virtual Filing Cabinet, no impacts are expected.

The nearest tire site waste site is mapped approximately 0.10 mile east of the project area. No impact is expected.

The nearest UST site is adjacent to the project area. Low levels of soil and groundwater contamination remain on the Five Star Food site (239 S. Indiana Avenue). If excavation occurs in this area, it is likely that petroleum contamination will be encountered. Proper removal and disposal of soil and/or groundwater may be necessary. Coordination with IDEM will occur.

The nearest institutional control site is adjacent to the project area. If excavation occurs in the area of Hesens Food Mart (492 N. Indiana Avenue), it is likely that petroleum contamination will be encountered. Proper removal and disposal of soil and/or groundwater may be necessary. Coordination with IDEM will occur.

The nearest NPDES facility is located immediately east of the project area. No impacts are expected.

The nearest NPDES pipe locations are adjacent to the project area. No impacts are expected.

Nine LUST sites are mapped within or adjacent to the project area:

- Johnson Oil Bigfoot #042, 604 S. Indiana Avenue. Low levels of soil and groundwater contamination remain on the site. If excavation occurs in this area, it is likely that petroleum contamination will be encountered. Proper removal and disposal of soil and/or groundwater may be necessary.
- Empire Gas Incorporated, 414 Popp Avenue. According to the IDEM VFC, no investigation has ever been conducted on this property. A Phase II Environmental Site Assessment is recommended prior to any subsurface work on this property or ROW acquisition. No subsurface work on or ROW acquisition from this property is anticipated.
- Former Dairy Mart #349, 624 S. Indiana Avenue (northwest intersection of Foothill Road and U.S. 31). Low levels of soil and groundwater contamination remain on the site. If excavation occurs in this area, it is likely that petroleum contamination will be encountered. Proper removal and disposal of soil and/or groundwater may be necessary.
- Dollar General Store, 475 N. Indiana Avenue. According to the IDEM VFC, no investigation has ever been conducted on this property. A Phase II Environmental Site Assessment was recommended, however subsequent coordination with INDOT Site Assessment and Management (SAM) determined proper disposal of any contaminated soil/and of groundwater would be adequate for this site due to the limited scope of work near the property (Appendix E, E19-E22).
- Former Gas Station, 475 Hauss Avenue: If excavation occurs in this area, petroleum contamination may be encountered. If contamination is encountered, before proper removal and disposal of soil and/or groundwater, analysis for lead will be necessary.
- CarMerica, 105 Prather Lane. According to the IDEM VFC, this site has a former diesel UST and used oil UST onsite. A Further Site Investigation (FSI) was completed on March 10, 2015 and indicates the soil contamination is contained to the site. No indication was given that USTs have been removed. If excavation occurs in this area, it is likely that petroleum contamination will be encountered. Proper removal and disposal of soil and/or groundwater may be necessary.
- Sellersburg Motors Inc., 392 S. Indiana Avenue. Low levels of soil and groundwater contamination remain on the site. If excavation occurs in this area, it is likely that petroleum contamination will be encountered. Proper removal and disposal of soil and/or groundwater may be necessary.
- Essroc Materials, Inc., 301 U.S. 31 S. Low levels of soil and groundwater contamination remain on the site. If excavation occurs in this area, it is likely that petroleum contamination will be encountered. Proper removal and disposal of soil and/or groundwater may be necessary. An Environmental Restrictive Covenant (ERC) was placed on the property on August 8, 2016 for restricted land use,

Indiana Department of Transportation

County Clark Route U.S. 31 Des. No. 1700111

groundwater use and soil disturbance. Coordination will be conducted with IDEM before further site activities occur.

- Swifty Oil #141, 254 S. Indiana Avenue. Low levels of soil and groundwater contamination remain on the site. If excavation occurs in this area, it is likely that petroleum contamination will be encountered. Proper removal and disposal of soil and/or groundwater may be necessary.

SECTION I – PERMITS CHECKLIST

Permits (mark all that apply)

Likely Required

Army Corps of Engineers (404/Section10 Permit)

Individual Permit (IP)	<input type="checkbox"/>
Nationwide Permit (NWP)	<input type="checkbox"/>
Regional General Permit (RGP)	X
Pre-Construction Notification (PCN)	<input type="checkbox"/>
Other	<input type="checkbox"/>
Wetland Mitigation required	<input type="checkbox"/>
Stream Mitigation required	<input type="checkbox"/>

IDEM

Section 401 WQC	X
Isolated Wetlands determination	<input type="checkbox"/>
Rule 5	X
Other	<input type="checkbox"/>
Wetland Mitigation required	<input type="checkbox"/>
Stream Mitigation required	<input type="checkbox"/>

IDNR

Construction in a Floodway	<input type="checkbox"/>
Navigable Waterway Permit	<input type="checkbox"/>
Lake Preservation Permit	<input type="checkbox"/>
Other	<input type="checkbox"/>
Mitigation Required	<input type="checkbox"/>

US Coast Guard Section 9 Bridge Permit

Others (Please discuss in the remarks box below)

<input type="checkbox"/>
<input type="checkbox"/>

Remarks:

Since more than 1.0 acre of land will be disturbed, it is anticipated an IDEM Rule 5 permit will be required. A Section 404 Regional General Permit (RGP) from the USACE and a Section 401 Water Quality Certification permit from IDEM are anticipated to be required for the stormwater new outfall. The project area is in an MS4 area regulated by the Town of Sellersburg; local ordinances may apply.

Applicable recommendations provided by IDNR are included in the Environmental Commitments section of this document. If permits are found to be necessary, the conditions of the permit will be requirements of the project and will supersede these recommendations.

It is the responsibility of the project sponsor to identify and obtain all required permits.

Indiana Department of Transportation

County Clark

Route U.S. 31

Des. No. 1700111

SECTION J- ENVIRONMENTAL COMMITMENTS

The following information should be provided below: List all commitments, name of agency/organization requesting the commitment(s) and indicating which are firm and which are for further consideration. The commitments should be numbered.

Remarks:

Firm:

1. If the scope of work or permanent or temporary ROW amounts change, the INDOT Environmental Services Division (ESD) and the INDOT District Environmental Section will be contacted immediately. (INDOT ESD and INDOT-Seymour District)
2. It is the responsibility of the project sponsor to notify school corporations and emergency services at least two weeks prior to any construction that would block or limit access. (INDOT ESD)
3. Any work in a wetland area within right-of-way or in borrow/waste areas is prohibited unless specifically allowed in the U.S. Army Corps of Engineers permit. (INDOT ESD)
4. Coordination with the West-Clark School District concerning detour routes for buses will be continued to project construction. (INDOT)
5. A Cemetery Development Plan for the Sellersburg Cemetery will be approved prior to project construction. (INDOT ESD)
6. (General AMM 1) Ensure all operators, employees, and contractors working in areas of known or presumed bat habitat are aware of all FHWA environmental commitments, including all applicable AMMs. (USFWS)
7. (Lighting AMM 1) Direct temporary lighting away from suitable habitat during the active season. (USFWS)
8. (Lighting AMM 2) When installing new or replacing existing permanent lights, use downward-facing, full cut-off lens lights (with same intensity or less for replacement lighting); or for those transportation agencies using the BUG system developed by the Illuminating Engineering Society, be as close to 0 for all three ratings with a priority of "uplight" of 0 and "backlight" as low as practicable. (USFWS)
9. (Tree Removal AMM 1) Modify all phases/aspects of the project (e.g. temporary work areas, alignments) to avoid tree removal. (USFWS)
10. (Tree Removal AMM 2) Apply time of year restrictions for tree removal when bats are not likely to be present, or limit tree removal to 10 or fewer trees per project at any time of year within 100 feet of existing road/rail surface and **outside of documented** roosting/foraging habitat or travel corridors; visual emergence survey must be conducted with no bats observed. (No tree clearing April 1 through September 30) (USFWS)
11. (Tree Removal AMM 3) Ensure tree removal is limited to that specified in project plans and ensure that contractors understand clearing limits and how they are marked in the field (e.g., install bright colored flagging/fencing prior to any tree clearing to ensure contractors stay within clearing limits). (USFWS)
12. (Tree Removal AMM 4) Do not remove **documented** Indiana bat or NLEB roosts that are still suitable for roosting, or trees within 0.25 miles of roosts, or **documented** foraging habitat any time of year. (USFWS)
13. If excavation occurs near the Five Star Food site (239 S. Indiana Avenue) it is likely that petroleum contamination will be encountered. Proper removal and disposal of soil and/or groundwater may be necessary. Coordination with IDEM will occur. (INDOT SAM)
14. If excavation occurs in the area of Hesens Food Mart site (492 N. Indiana Avenue), it is likely that petroleum contamination will be encountered. Proper removal and disposal of soil and/or groundwater may be necessary. Coordination with IDEM will occur. (INDOT SAM)
15. If excavation occurs near the Johnson Oil Bigfoot #042 site (604 S. Indiana Avenue) it is likely that petroleum contamination will be encountered. Proper removal and disposal of soil and/or groundwater may be necessary. (INDOT SAM)
16. The Former Dairy Mart #349 site, 624 S. Indiana Avenue (northwest intersection of Foothill Road and U.S. 31) has low levels of soil and groundwater contamination. If excavation occurs in this area, it is likely that petroleum contamination will be encountered. Proper removal and disposal of soil and/or groundwater may be necessary. (INDOT SAM)
17. The Dollar General Store site (475 N. Indiana Avenue) has not been subject to site investigations. If excavation occurs in this area, it is likely that petroleum contamination will be encountered. If contamination is encountered, before proper removal and disposal of soil and/or groundwater, analysis for lead will be necessary. (INDOT SAM)

Indiana Department of Transportation

County Clark

Route U.S. 31

Des. No. 1700111

18. If any subsurface work will occur near the Empire Gas Incorporated site (414 Popp Avenue), a Phase II Environmental Site Assessment will be conducted.
19. If excavation occurs near the Former Gas Station site at 475 Hauss Avenue, petroleum contamination may be encountered. If contamination is encountered, before proper removal and disposal of soil and/or groundwater, analysis for lead will be necessary. (INDOT SAM)
20. If excavation occurs near the CarMerica site (105 Prather Lane), it is likely that petroleum contamination will be encountered. Proper removal and disposal of soil and/or groundwater may be necessary. (INDOT SAM)
21. If excavation occurs near the Sellersburg Motors Inc. site (392 S. Indiana Avenue), it is likely that petroleum contamination will be encountered. Proper removal and disposal of soil and/or groundwater may be necessary. (INDOT SAM)
22. If excavation occurs near the Essroc Materials, Inc. site (301 U.S. 31 S.), it is likely that petroleum contamination will be encountered. Proper removal and disposal of soil and/or groundwater may be necessary. Coordination will be conducted with IDEM before further site activities occur. (INDOT SAM)
23. If excavation occurs near the Swifty Oil #141 site (254 S. Indiana Avenue), it is likely that petroleum contamination will be encountered. Proper removal and disposal of soil and/or groundwater may be necessary. (INDOT SAM)

For Further Consideration:

24. Restrict below low-water work in streams to placement of culverts, piers, pilings and/or footings, shaping of the spill slopes around the bridge abutments, and placement of riprap. Culverts should span the active stream channel, should be either embedded or a 3-sided or open-arch culvert, and be installed where practicable on an essentially flat slope. When an open-bottom culvert or arc is used in a stream, which has a good natural bottom substrate, such as gravel, cobbles and boulders, the existing substrate should be left undisturbed beneath the culvert to provide natural habitat for the aquatic community. (USFWS)
25. Avoid all work within the inundated part of the stream channel (in perennial streams and larger intermittent streams) during the fish spawning season (April 1 through June 30), except for work within sealed structures such as caissons or cofferdams that were installed prior to the spawning season. No equipment shall be operated below Ordinary High Water Mark during this time unless the machinery is within the caissons or on the cofferdams. (USFWS)
26. Evaluate wildlife crossings under bridge/culverts projects in appropriate situations. Suitable crossings include flat areas below bridge abutments with suitable ground cover, high water shelves in culverts, amphibian tunnels and diversion fencing. (USFWS)
27. Impacts to non-wetland forest of one (1) acre or more should be mitigated at a minimum 2:1 ratio. If less than one acre of non-wetland forest is removed in a rural setting, replacement should be at a 1:1 ratio based on area. Impacts to non-wetland forest under one (1) acre in an urban setting should be mitigated by planting five trees, at least 2 inches in diameter-at-breast height (dbh), for each tree which is removed that is 10" dbh or greater (5:1 mitigation based on the number of large trees) or by using the 1:1 replacement ratio based on area depending on the type of habitat impacted (individual canopy tree removal in an urban streetscape or park-like environment versus removal of habitat supporting a tree canopy, woody understory, and herbaceous layer). Impacts under 0.10 acre in an urban area may still involve the replacement of large diameter trees but typically do not require any additional mitigation or additional plantings beyond seeding and stabilizing disturbed areas. There are exceptions for high quality habitat sites however. (IDNR)
28. Do not excavate in the waterway and minimize disturbance to bank vegetation and contain disturbance to within the project limits. (IDNR)
29. Do not cut any trees suitable for Indiana bat or Northern long-eared bat roosting (greater than 5 inches dbh, living or dead, with loose-hanging bark, or with cracks, crevices, or cavities) from April 1 through September 30. (IDNR)

Indiana Department of Transportation

County Clark

Route U.S. 31

Des. No. 1700111

SECTION K- EARLY COORDINATION

Please list the date coordination was sent and all agencies that were contacted as a part of the development of this Environmental Study. Also, include the date of their response or indicate that no response was received. INDOT and FHWA are automatically considered early coordination participants and should only be listed if a response is received.

Remarks:

Early Coordination was sent for this project on September 16, 2020 (Appendix C, C1-C2). A list of the resource agencies contacted during Early Coordination is provided below, along with their response date (if applicable).

AGENCY	RESPONSE DATE
U.S. Fish and Wildlife Service	September 22, 2020
Indiana Department of Environmental Management	September 16, 2020
USDA Natural Resources Conservation Service	September 23, 2020
Indiana Geological Survey	September 16, 2020
U.S. Department of Housing and Urban Development	No Response
INDOT Office of Aviation	September 16, 2020
Indiana Department of Natural Resources Division of Fish and Wildlife	October 16, 2020
U.S. Army Corps of Engineers-Louisville District	No Response
INDOT Office of Communications	No Response
National Park Service	No Response
Kentuckiana Regional Planning & Development Agency	No Response
Sellersburg Water Plant and Utilities	No Response
Town of Sellersburg	No Response
Sellersburg Town Council	No Response
Town of Sellersburg MS4 Coordinator	No Response
Sellersburg Parks and Recreation Department	No Response
Sellersburg Plan Commission	No Response
Sellersburg Police Department	No Response
Clark County Highway Superintendent	No Response
Clark County Surveyor	No Response
Clark County Plan Commission	No Response
Clark County Emergency Management	No Response
Clark County Commissioners	No Response
Clark County Sheriff	No Response
West Clark School District	September 16, 2020
St John Paul II Catholic School	No Response
Speed Community Church	No Response
Grace Community Church and School	No Response

APPENDIX TABLE OF CONTENTS
U.S. 31 Small Town Pavement Replacement Project
Town of Sellersburg/Community of Speed, Clark County, Indiana
Des. No. 1700111

Appendix A: INDOT Supporting Documentation	
CE Level Thresholds Chart	A1
Appendix B: Graphics	
Indiana State Map	B1
USGS Speed Quadrangle Map	B2
Aerial Map	B3
Photo Key Map	B4
Photo Reference Pages	B5-B13
Preliminary Plan Sheets	B14-B78
Appendix C: Early Coordination	
Early Coordination Letter/List of Recipients	C1-C4
INDOT Office of Aviation	C5
West-Clark School District Response	C6
Indiana Geological Society Response	C7-C9
IDEM Online Letter	C10-C16
USFWS Response	C17-C18
USDA-NRCS Response	C19
IDNR, Division of Fish and Wildlife Response	C20-C21
USFWS IPaC Species List Letter	C22-C29
USFWS IPaC Concurrence Verification Letter	C30 – C43
INDOT Concurrence with IPaC Determination	C44
Appendix D: Section 106 of the NHPA	
FHWA 800.11(d) Documentation	D1-D11
APE Map	D12
NRHP Boundary Maps	D13-D14
INDOT Approval of ECL	D15
Section 106 ECL	D16-D22
SHPO Response to ECL	D23-D24
Consulting Party Responses	D25
Excerpt from Historic Property Report	D26-D28
Excerpt from Archaeological Short Report	D29-D31
INDOT Approval of Archaeological Report Email	D32
INDOT Approval of HPR Email	D33
Distribution of HPR/ASR to Consulting Parties	D34-D37
SHPO Concurrence with HPR/ASR	D38-D39
INDOT Approval of Effects Letter Email	D40
Effects Letter and Distribution to Consulting Parties	D41-D47
SHPO Response to Effects Letter	D48-D49
INDOT Approval of 800.11 Document Email	D50
Provision of 800.11 Documentation to CPs	D51-D52
SHPO Concurrence with Finding	D53-D54
Section 106 Public Notice and Publisher's Affidavit	D55

Appendix E: Red Flag Investigation	
Red Flag Investigation Report	E1-E14
Clark County ETR Species List	E15-E18
Additional Correspondence	E19-E22
Appendix F: Ecological and Water Resources	
Waters of the US Report	F1-F33
Appendix G: Public Involvement	
Notice of Survey	G1
Notice of Planned Improvement	G2-G3
Public Notice Affidavit	G4-G5
Appendix H: Air Quality	
STIP Excerpt from FY 2020 – 2024	H1
STIP Excerpt from FY 2020 – 2024	H2
Kentuckiana Regional TIP Excerpt	H3-H4
Appendix I: Additional Studies	
List of Section 6(f) Properties in Clark County	I1
Environmental Justice Data	I2-I4

Appendix A

INDOT Supporting Documentation

Categorical Exclusion Level Thresholds

	PCE	Level 1	Level 2	Level 3	Level 4 ¹
Section 106	Falls within guidelines of Minor Projects PA	“No Historic Properties Affected”	“No Adverse Effect”	-	“Adverse Effect” Or Historic Bridge involvement ²
Stream Impacts	No construction in waterways or water bodies	< 300 linear feet of stream impacts	≥ 300 linear feet of stream impacts	-	Individual 404 Permit
Wetland Impacts	No adverse impacts to wetlands	< 0.1 acre	-	< 1 acre	≥ 1 acre
Right-of-way³	Property acquisition for preservation only or none	< 0.5 acre	≥ 0.5 acre	-	-
Relocations	None	-	-	< 5	≥ 5
Threatened/Endangered Species (Species Specific Programmatic for Indiana bat & northern long eared bat)	“No Effect”, “Not likely to Adversely Affect” (Without AMMs ⁴ or with AMMs required for all projects ⁵)	“Not likely to Adversely Affect” (With any other AMMs)	-	“Likely to Adversely Affect”	Project does not fall under Species Specific Programmatic
Threatened/Endangered Species (Any other species)	Falls within guidelines of USFWS 2013 Interim Policy	“No Effect”, “Not likely to Adversely Affect”	-	-	“Likely to Adversely Affect”
Environmental Justice	No disproportionately high and adverse impacts	-	-	-	Potential ⁶
Sole Source Aquifer	Detailed Assessment Not Required	-	-	-	Detailed Assessment
Floodplain	No Substantial Impacts	-	-	-	Substantial Impacts
Coastal Zone Consistency	Consistent	-	-	-	Not Consistent
National Wild and Scenic River	Not Present	-	-	-	Present
New Alignment	None	-	-	-	Any
Section 4(f) Impacts	None	-	-	-	Any
Section 6(f) Impacts	None	-	-	-	Any
Added Through Lane	None	-	-	-	Any
Permanent Traffic Alteration	None	-	-	-	Any
Coast Guard Permit	None	-	-	-	Any
Noise Analysis Required	No	-	-	-	Yes
Air Quality Analysis Required	No	-	-	-	Yes ⁷
Approval Level	Concurrence by INDOT District Environmental or Environmental Services	Yes	Yes	Yes Yes	Yes Yes Yes

¹Coordinate with INDOT Environmental Services. INDOT will then coordinate with the appropriate FHWA Environmental Specialist.

²Any involvement with a bridge processed under the Historic Bridge Programmatic Agreement.

³Permanent and/or temporary right-of-way.

⁴AMMs = Avoidance and Mitigation Measures.

⁵AMMs determined by the IPAC decision key to be needed that are listed in the USFWS *User’s Guide for the Range-wide Programmatic Consultation for Indiana bat and Northern long-eared bat* as “required for all projects”.

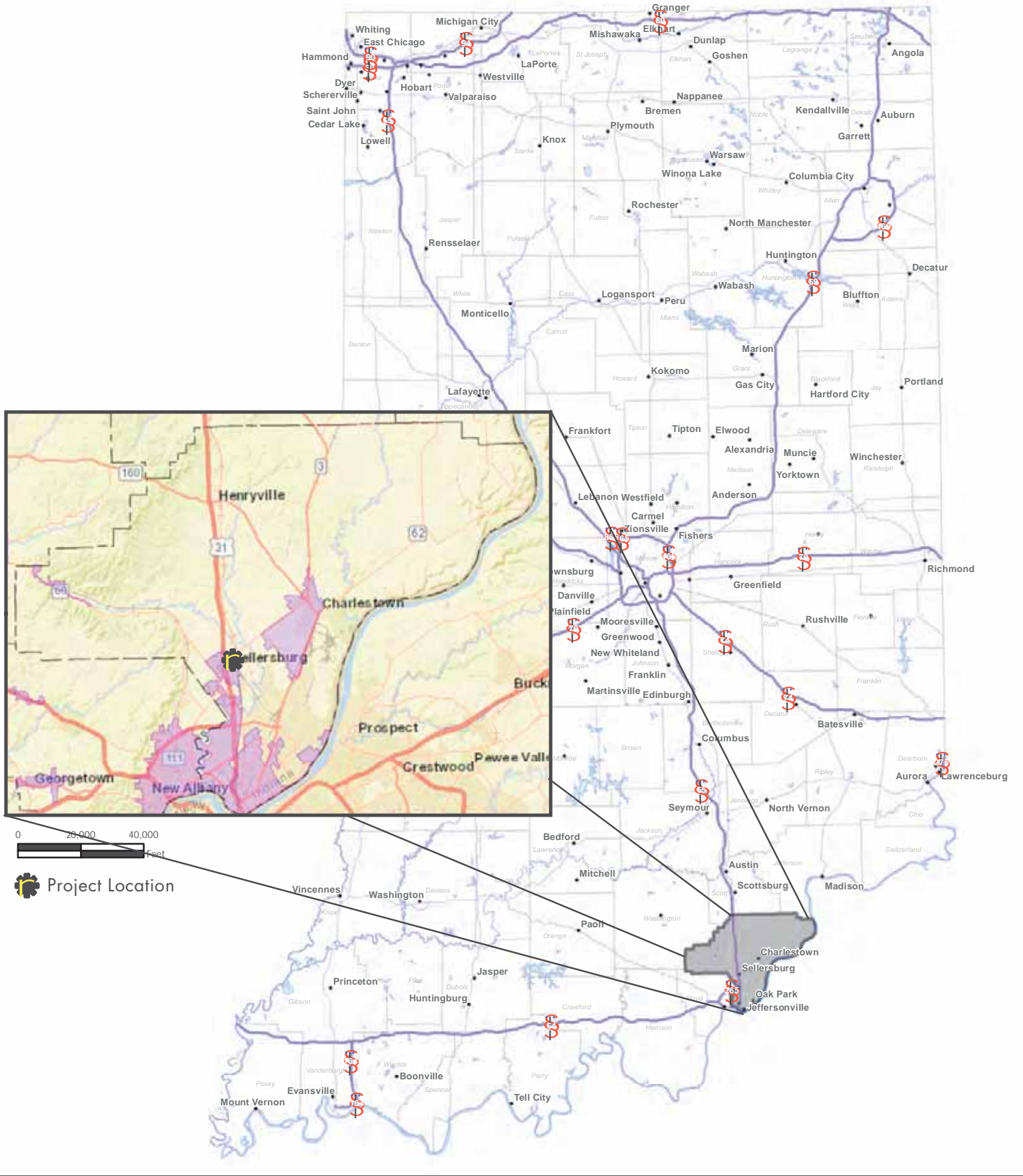
⁶Potential for causing a disproportionately high and adverse impact.


⁷Hot Spot Analysis and/or MSAT Quantitative Emission Analysis.

*Substantial public or agency controversy may require a higher-level NEPA document.

Appendix B

Graphics



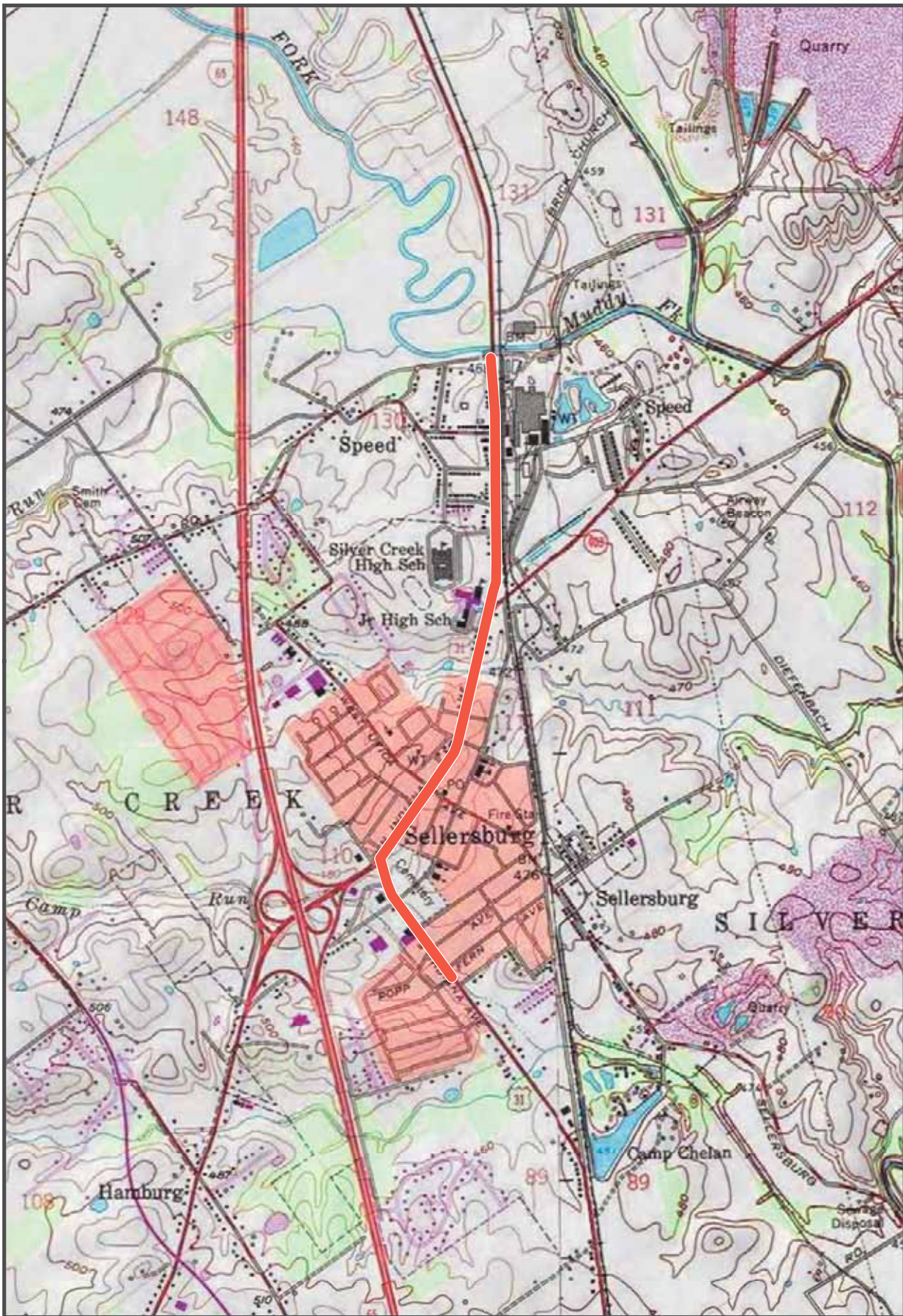
0 20,000 40,000 Feet
 Project Location

0 80,000 160,000 320,000 Feet



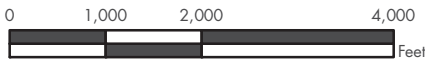
Map Source: Indiana Map

State Map
 US 31 Preventative Pavement Maintenance
 Sellersburg, Clark County, IN
 Section (CMG) 110, 111, 130, Township 1 S, Range 6 E
 Des. No. 1700111



Legend
 Project Area

Map Source: Indiana Geological Survey (IGS), IndianaMap, ArcGIS Online (ESRI) USA Topo Maps



USGS Speed Quadrangle

US 31 Preventative Pavement Maintenance

Sellersburg, Clark County, IN

Section (CMG) 110, 111, 130, Township 1 South, Range 6 East

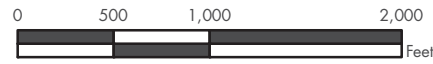
Des. No. 1700111





- Legend**
- Project Area
 - Railroad 100k
 - Streams (NHD)

Map Source: Indiana Geological Survey (IGS), IndianaMap, ArcGIS Online (ESRI) World Imagery.



Aerial Map

US 31 Preventative Pavement Maintenance
 Sellersburg, Clark County, IN
 Des. No. 1700111





Legend

Project Area



###

Photo Number

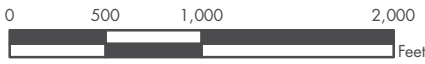


Photo Key

US 31 Preventative Pavement Maintenance
 Sellersburg, Clark County, IN
 Des No. 1700111





Photo 1: Looking north along US 31 from the northern project terminus.



Photo 2: Looking south along US 31 from the northern project terminus. The Louisville Cement Company is at left.



Photo 3: Looking south along US 31 from Maryland Street.



Photo 4: Looking north along US 31 from Renz Avenue.



Photo 5: Looking southwest along CR 403 towards the intersection with US 31.



Photo 6: Looking northwest at CR 403 from the west side of US 31.

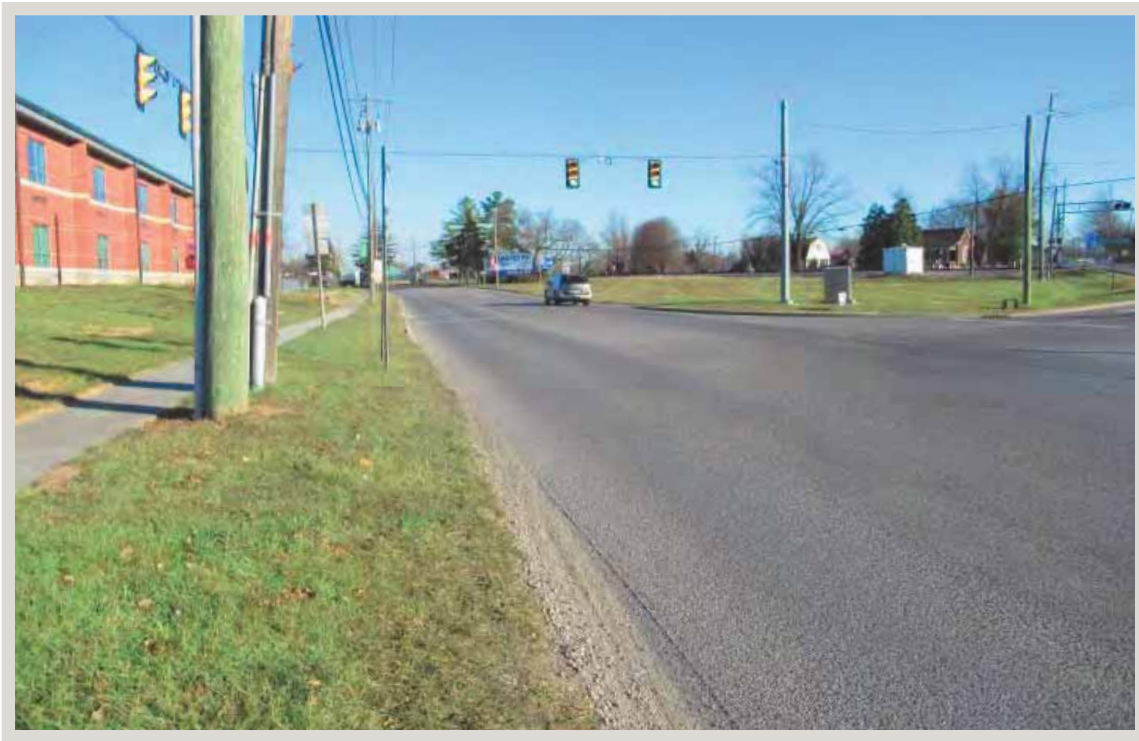


Photo 7: Looking north along US 31 from the intersection with CR 403.



Photo 8: Looking south along US 31 from the Dollar General store.



Photo 9: Looking south along US 31 from the Clark County Public Library.



Photo 10: Looking northeast along US 31 from Utica Street.



Photo 11: Looking southwest along US 31 from Utica Street.



Photo 12: Looking northeast along US 31 from between Allhands Avenue and Buehite Street.



Photo 13: Looking northeast along US 31 from the north side of the intersection with SR 311.



Photo 14: Looking southwest along SR 311 from US 31.



Photo 15: Looking southeast along US 31 from the north side of the intersection with SR 311, across the Sellersburg Cemetery.



Photo 16: Looking southeast along US 31 from S. New Albany Street.



Photo 17: Looking northwest along US 31 from Foothill Drive, the southern project terminus.



Photo 18: Looking east down US 31 from Foothill Drive, the southern project terminus.

PROJECT	DESIGNATION
1700111	1700111
CONTRACT	
R-40412	

INDIANA DEPARTMENT OF TRANSPORTATION



ROAD PLANS

ROUTE: U.S. 31 FROM: RP 8+91 TO: RP 10+74

PROJECT NO. 1700111 P.E.
1700111 R/W
1700111 CONST.

SMALL TOWN PAVEMENT REPLACEMENT ON US 31 IN SELLERSBURG, 1.53 MILES NORTH OF SR 60 TO 3.28 MILES NORTH OF SR 60 IN CLARK MILITARY GRANTS #110, #111, AND #130, SILVER CREEK TOWNSHIP, CLARK COUNTY, INDIANA

TRAFFIC DATA	US 31 - SEGMENTS 1, 2, & 3	US 31 - SEGMENT 4
A.A.D.T. (2022)	22,426	11,502
A.A.D.T. (2042)	28,476	12,593
D.H.V. (2042)	1.961	1.210
DIRECTIONAL DISTRIBUTION	48.49 %	53.58 %
TRUCKS	10.0 % A.A.D.T.	10.0 % A.A.D.T.
	5.0 %D.H.V.	5.0 %D.H.V.
DESIGN DATA	US 31 - SEGMENTS 1, 2, & 3	US 31 - SEGMENT 4
DESIGN SPEED	40, 30, & 30 MPH	45 MPH
PROJECT DESIGN CRITERIA	SR (NON-FREEWAY)	SR (NON-FREEWAY)
FUNCTIONAL CLASSIFICATION	MINOR ARTERIAL (INTERMEDIATE)	MAJOR COLLECTOR (INTERMEDIATE)
RURAL/URBAN	URBAN (SUBURBAN)	URBAN (SUBURBAN)
TERRAIN	LEVEL	LEVEL
ACCESS CONTROL	NONE	NONE

ADDITIONAL RIGHT-OF-WAY REQUIRED FOR THIS PROJECT

END SEGMENT 3
BEGIN SEGMENT 4
STA. 475+50.00 "A"

END SEGMENT 2
BEGIN SEGMENT 3
STA. 436+70.00 "A"

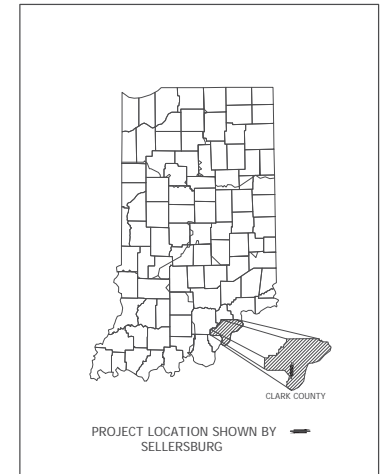
END SEGMENT 1
BEGIN SEGMENT 2
STA. 30+15.00 "B"

BEGIN PROJECT
BEGIN SEGMENT 1
STA. 12+35.00 "B"



VINCINITY MAP
CLARK COUNTY

END PROJECT
END SEGMENT 4
STA. 510+24.85 "A"



LATITUDE: 38° 23' 45" N LONGITUDE: 85° 45' 30" W

GROSS LENGTH: 1.818 MI.
NET LENGTH: 1.818 MI.
MAX. GRADE: 6.05 %

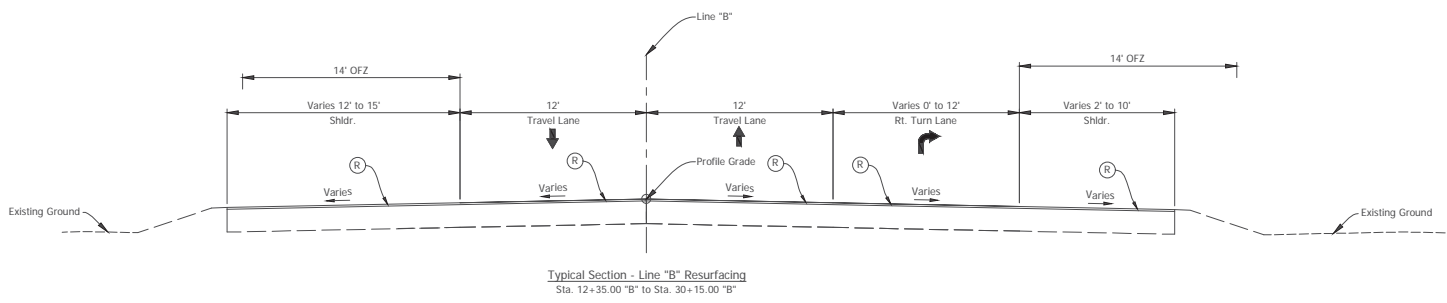
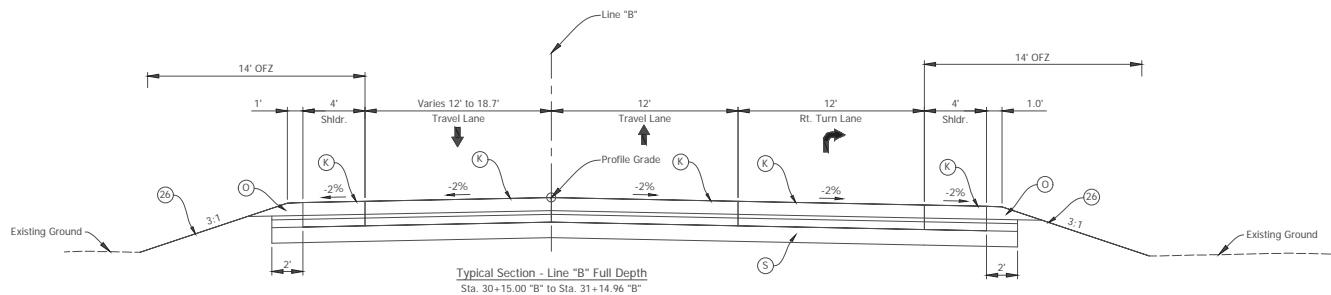
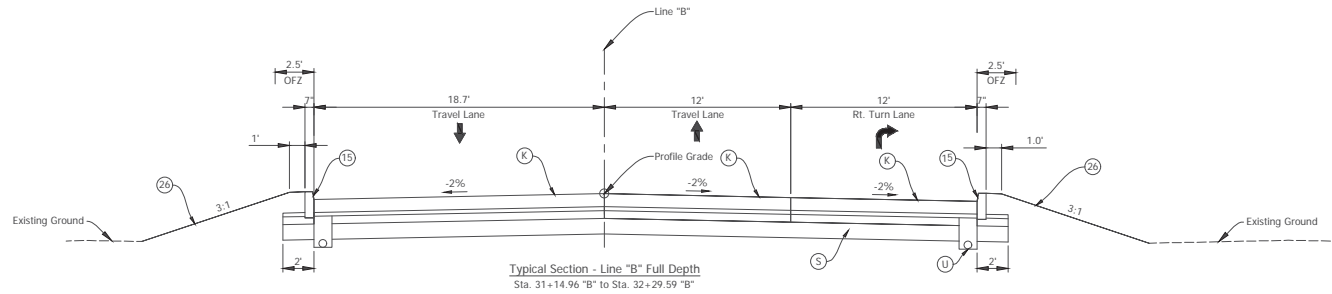
INDIANA DEPARTMENT OF TRANSPORTATION
STANDARD SPECIFICATIONS DATED 2020
TO BE USED WITH THESE PLANS



PLANS PREPARED BY:	SHREWSBERRY & ASSOCIATES, LLC	317-841-4799 PHONE NUMBER
CERTIFIED BY:		DATE
APPROVED FOR LETTING:	INDIANA DEPARTMENT OF TRANSPORTATION	DATE

BRIDGE FILE	
DESIGNATION	
1700111	
SURVEY BOOK	SHEETS
1	of 150
CONTRACT R-40412	PROJECT 1700111

DIRECTORY: S:\Project Files\2017\17-0199\CADD\Design\Sheet Sets
 File Name: Typ Section.dwg
 PLOT SCALE: 1" = 10'-0"
 PLOTTED BY: mvaireen
 DATE: Nov 13, 2020 - 1:22pm



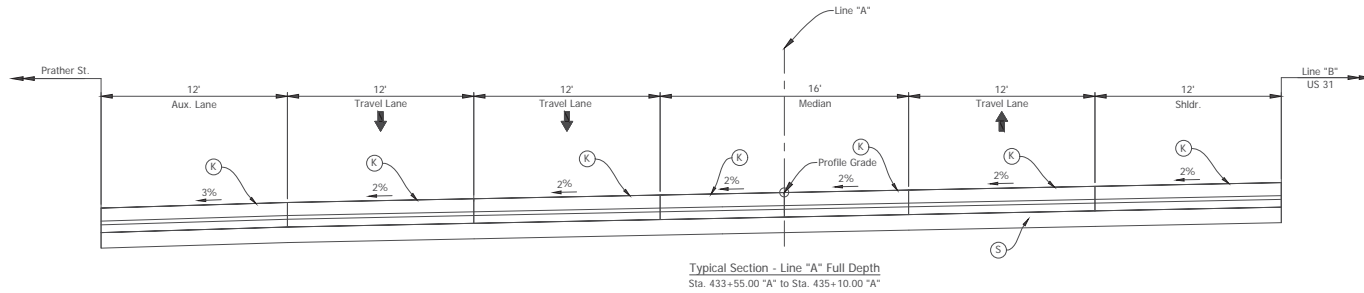
- | | | | | |
|--|---|---|---------------------------------|------------------|
| (D) 6" PCCP for Approaches on Dense Graded Subbase on Subgrade Treatment, Type II | (K) Full Depth Pavement | (R) 165#/SYS QC/OA-HMA, 2, 70, Surface, 9.5mm on Asphalt Milling, 1.5 in. | (DW) Detectable Warning Surface | (15) Curb |
| (D1) 9" PCCP for Approaches on Dense Graded Subbase on Geogrid, Type IB on Subgrade Treatment, Type II | (K1) Full Depth Pavement, PCCP for Approaches | (R1) 165#/SYS HMA For Approaches, Type D on Asphalt Milling, 1.5 in. | (R) Concrete Curb Ramp | (16) Center Curb |
| (F) Sidewalk, Concrete, 4" | OFZ Obstruction-Free Zone | (S) Subgrade Treatment, Type XX | (O) Compacted Aggregate | (2A) Sodding |
| | | (U) Underdrain | | |

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: BWS	DRAWN: MRW	
CHECKED: ACD	CHECKED: BWS	

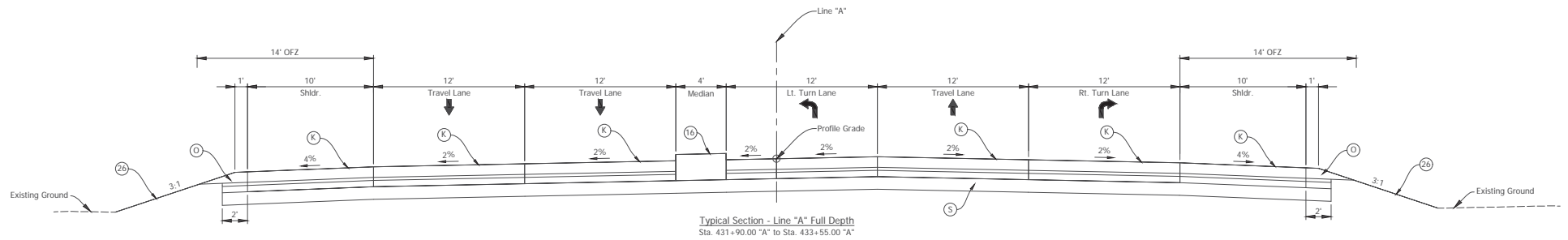
INDIANA
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS
LINE "B"

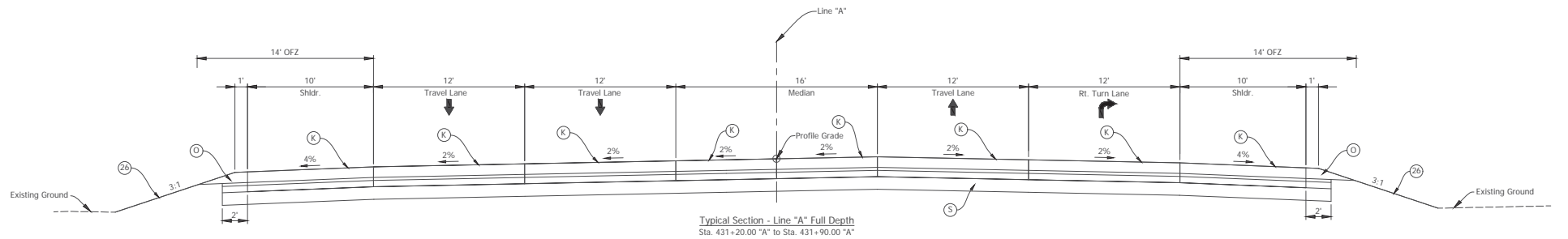
HORIZONTAL SCALE	BRIDGE FILE
1/4" = 1'	N/A
VERTICAL SCALE	DESIGNATION
1/4" = 1'	1700111
SURVEY BOOK	SHEETS
	3 OF 150
CONTRACT	PROJECT
R-40412	1700111



Typical Section - Line "A" Full Depth
Sta. 433+55.00 "A" to Sta. 435+10.00 "A"



Typical Section - Line "A" Full Depth
Sta. 431+90.00 "A" to Sta. 433+55.00 "A"



Typical Section - Line "A" Full Depth
Sta. 431+20.00 "A" to Sta. 431+90.00 "A"

- (D) 6" PCCP for Approaches on Dense Graded Subbase on Subgrade Treatment, Type II
- (K) Full Depth Pavement
- (R) 165#/SYS QC/OA-HMA, 2, 70, Surface, 9.5mm on Asphalt Milling, 1.5 in.
- (DW) Detectable Warning Surface
- (15) Curb
- (D1) 9" PCCP for Approaches on Dense Graded Subbase on Geogrid, Type IB on Subgrade Treatment, Type II
- (K1) Full Depth Pavement, PCCP for Approaches
- (R1) 165#/SYS HMA For Approaches, Type D on Asphalt Milling, 1.5 in.
- (R) Concrete Curb Ramp
- (16) Center Curb
- (F) Sidewalk, Concrete, 4"
- (OFZ) OFZ Obstruction-Free Zone
- (S) Subgrade Treatment, Type XX
- (U) Underdrain
- (O) Compacted Aggregate
- (2A) Sodding

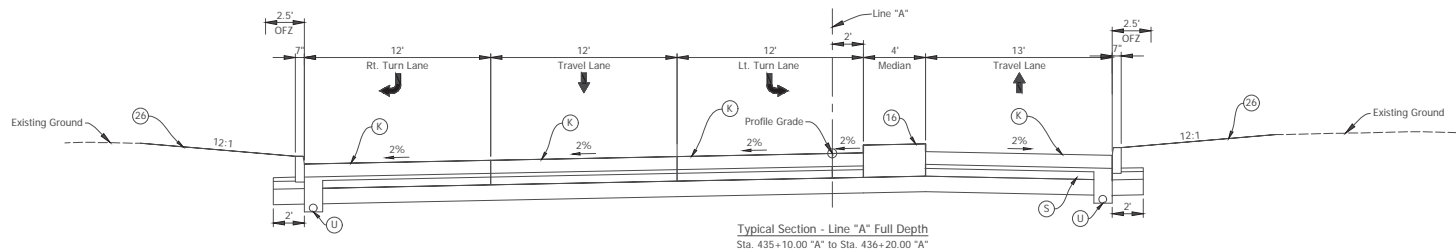
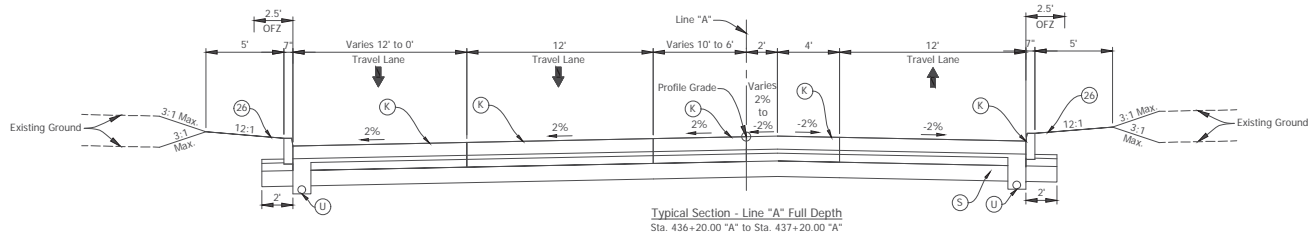
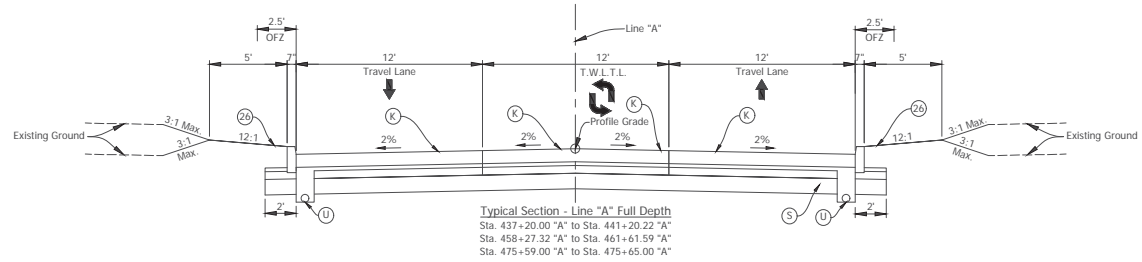
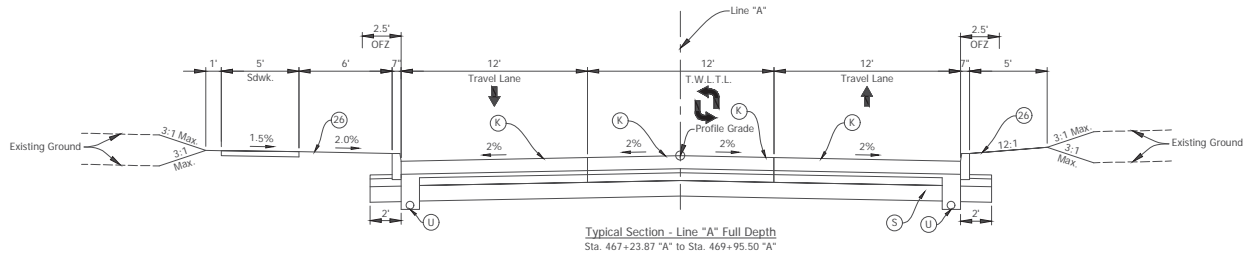
RECOMMENDED FOR APPROVAL		DESIGN ENGINEER	DATE
DESIGNED BY: BWS	DRAWN BY: MRW		
CHECKED BY: ACD	CHECKED BY: BWS		

INDIANA DEPARTMENT OF TRANSPORTATION	
TYPICAL SECTIONS LINE "A"	

HORIZONTAL SCALE 1/4" = 1'	BRIDGE FILE N/A
VERTICAL SCALE 1/4" = 1'	DESIGNATION 1700111
SURVEY BOOK	SHEETS
CONTRACT R-40412	4 OF 150 PROJECT 1700111

DIRECTORY: S:\Project Files\2017\17-0199\CADD\Design\Sheet Sets
 Project: 17-0199 - Typical Section.dwg
 PLOTSCALE: 1" = 100'
 PLOTTED BY: mvaaren
 DATE: Nov. 13, 2020 - 1:23pm

DIRECTOR: S:\Project Files\2017\17-0199\CADD\Design\Sheet Sets
 FILENAME: 17-0199-Subtyp Section.dwg
 DIMSCALE: 1/4" = 1'-0"
 PLOTTED BY: mvaireni
 DATE: Nov 15, 2020 - 12:52pm



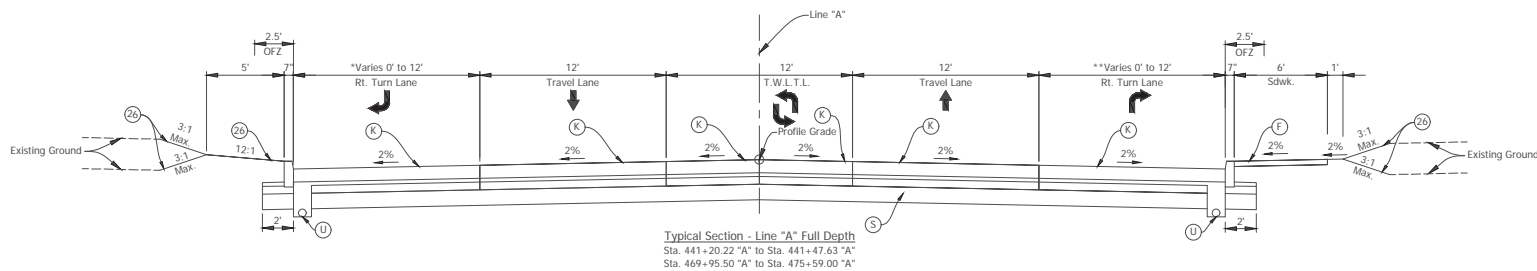
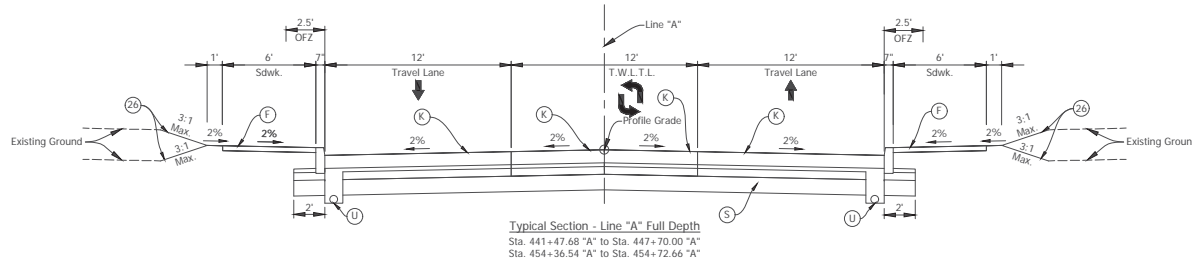
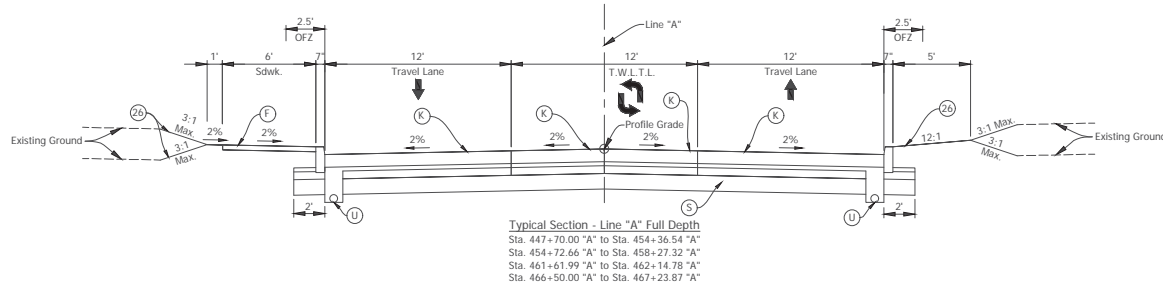
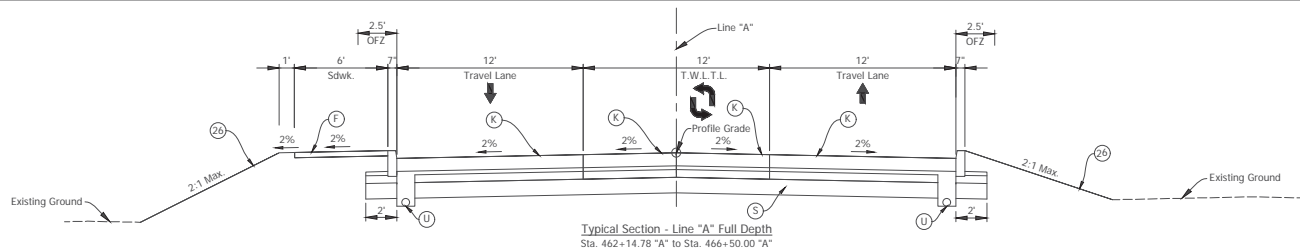
- | | | | |
|--|---|---|---------------------------------|
| (D) 6" PCPP for Approaches on Dense Graded Subbase on Subgrade Treatment, Type II | (K) Full Depth Pavement | (R) 165#/SYS QC/OA-HMA, 2, 70, Surface, 9.5mm on Asphalt Milling, 1.5 in. | (WW) Detectable Warning Surface |
| (D1) 9" PCPP for Approaches on Dense Graded Subbase on Geogrid, Type IB on Subgrade Treatment, Type II | (K1) Full Depth Pavement, PCPP for Approaches | (R1) 165#/SYS HMA For Approaches, Type D on Asphalt Milling, 1.5 in. | (R) Concrete Curb Ramp |
| (F) Sidewalk, Concrete, 4" | OFZ Obstruction-Free Zone | (S) Subgrade Treatment, Type XX | (15) Curb |
| | | (U) Underdrain | (16) Center Curb |
| | | | (26) Sodding |

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: BWS	DRAWN: MRW	
CHECKED: ACD	CHECKED: BWS	

INDIANA DEPARTMENT OF TRANSPORTATION	
TYPICAL SECTIONS LINE "A"	

HORIZONTAL SCALE	BRIDGE FILE
1/4" = 1'	N/A
VERTICAL SCALE	DESIGNATION
1/4" = 1'	1700111
SURVEY BOOK	SHEETS
	5 of 150
CONTRACT	PROJECT
R-40412	1700111

DIRECTORY: S:\Project Files\2017\17-0199\CADD\Design\Sheet Sets
 File Name: Typ Section.dwg
 PLOTTED BY: mvaireni
 DATE: Nov 13, 2020 - 1:24pm



* From Sta. 469+95.50 to 471+26.05
 Width Varies 0' to 12'
 From Sta. 471+26.05 to 471+66.62
 Width = 12'
 From Sta. 471+66.62 to 472+66.84
 Width Varies 12' to 0'

** From Sta. 472+30.00 to 473+00.00
 Width Varies 0' to 12'
 From Sta. 473+00.00 to 473+86.06
 Width = 12'
 From Sta. 473+86.06 to 475+59.00
 Width Varies 12' to 0'

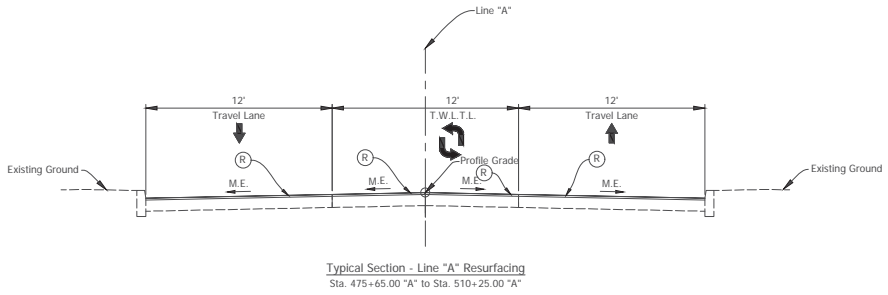
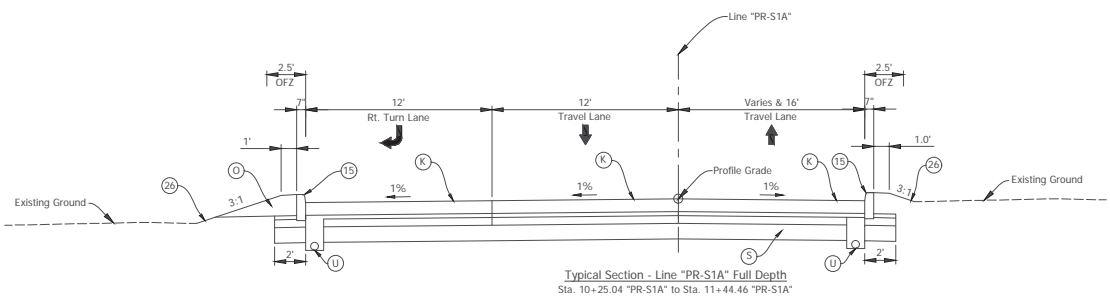
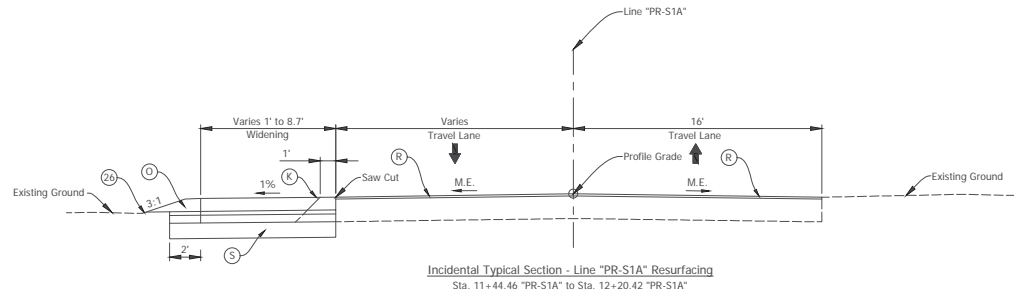
- | | | | | |
|--|---|---|---------------------------------|------------------|
| (D) 6" PCCP for Approaches on Dense Graded Subbase on Subgrade Treatment, Type II | (K) Full Depth Pavement | (R) 165#/SYS QC/QA-HMA, 2, 70, Surface, 9.5mm on Asphalt Milling, 1.5 in. | (DW) Detectable Warning Surface | (15) Curb |
| (D1) 9" PCCP for Approaches on Dense Graded Subbase on Geogrid, Type IB on Subgrade Treatment, Type II | (K1) Full Depth Pavement, PCCP for Approaches | (R1) 165#/SYS HMA for Approaches, Type D on Asphalt Milling, 1.5 in. | (R) Concrete Curb Ramp | (16) Center Curb |
| (F) Sidewalk, Concrete, 4" | (OFZ) OFZ Obstruction-Free Zone | (S) Subgrade Treatment, Type XX | (O) Compacted Aggregate | (2A) Sodding |
| | | (U) Underdrain | | |

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: BWS	DRAWN: MRW	
CHECKED: ACD	CHECKED: BWS	

INDIANA DEPARTMENT OF TRANSPORTATION	
TYPICAL SECTIONS LINE "A"	

HORIZONTAL SCALE	BRIDGE FILE
1/4" = 1'	N/A
VERTICAL SCALE	DESIGNATION
1/4" = 1'	1700111
SURVEY BOOK	SHEETS
	6 OF 150
CONTRACT	PROJECT
R-40412	1700111

DIRECTORY: S:\Project Files\2017\17-0199\CADD\Design\Sheet Sets
 FILENAME: 17-0199-Typ Section.dwg
 PLOTTED BY: mvaireni
 DATE: Nov 13, 2020 - 1:24pm



- | | | | |
|--|---|---|---------------------------------|
| (D) 6" PCCP for Approaches on Dense Graded Subbase on Subgrade Treatment, Type II | (K) Full Depth Pavement | (R) 165#/SYS QC/OA-HMA, 2, 70, Surface, 9.5mm on Asphalt Milling, 1.5 in. | (DW) Detectable Warning Surface |
| (D1) 9" PCCP for Approaches on Dense Graded Subbase on Geogrid, Type IB on Subgrade Treatment, Type II | (K1) Full Depth Pavement, PCCP for Approaches | (R1) 165#/SYS HMA For Approaches, Type D on Asphalt Milling, 1.5 in. | (R) Concrete Curb Ramp |
| (F) Sidewalk, Concrete, 4" | OFZ Obstruction-Free Zone | (S) Subgrade Treatment, Type XX | (15) Curb |
| | | (U) Underdrain | (16) Center Curb |
| | | | (2A) Sodding |

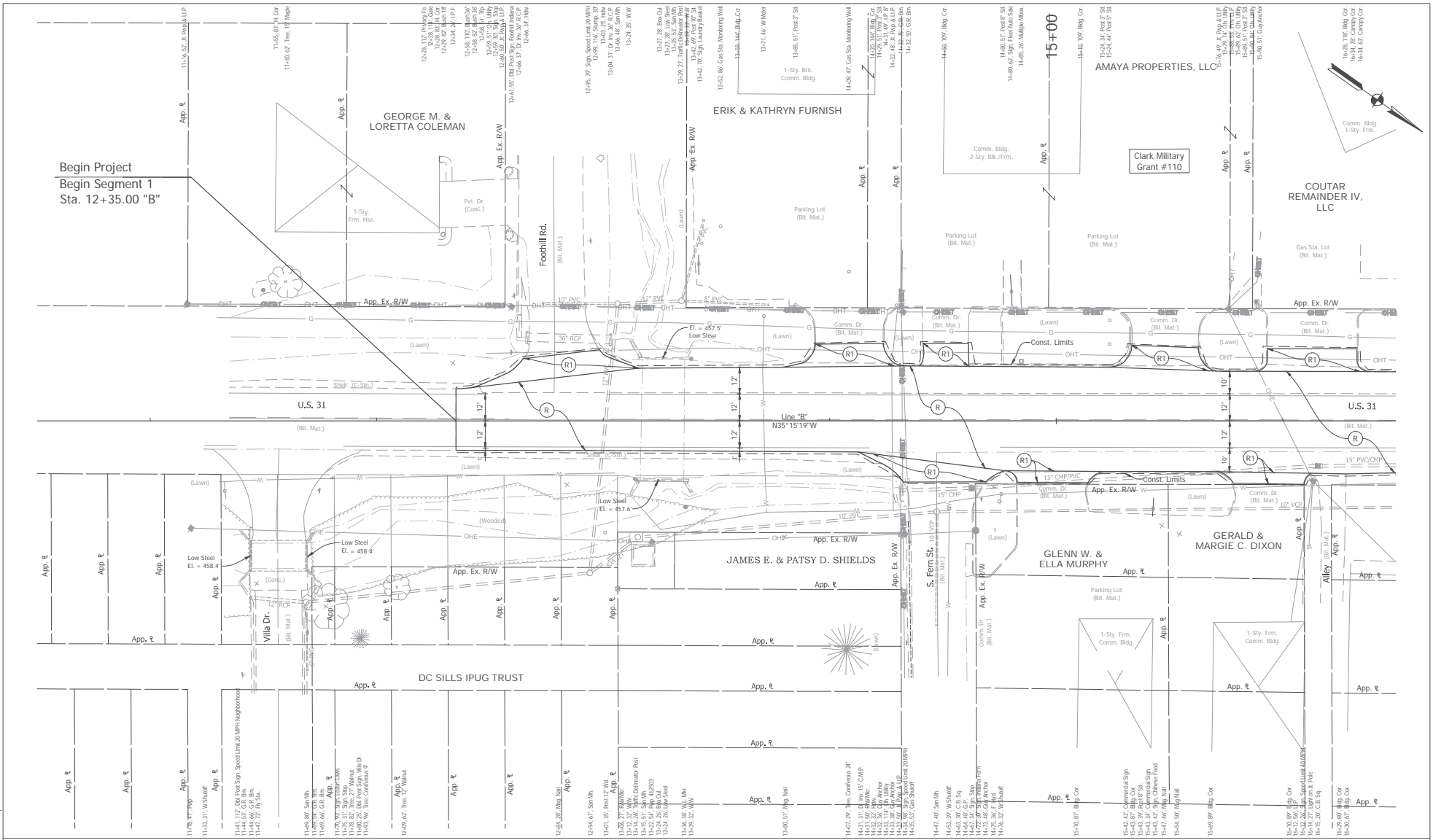
RECOMMENDED FOR APPROVAL		DESIGN ENGINEER		DATE	
DESIGNED: BWS	DRAWN: MRW				
CHECKED: ACD	CHECKED: BWS				

INDIANA
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS
LINE "A" & "PR-S1A"

HORIZONTAL SCALE	BRIDGE FILE
1/4" = 1'	N/A
VERTICAL SCALE	DESIGNATION
1/4" = 1'	1700111
SURVEY BOOK	SHEETS
	7 OF 150
CONTRACT	PROJECT
R-40412	1700111

DIRECTORY: S:\Project Files\2017\17-0199\CADD\Design\Sheet Sets
 FILE: 170199 Plan Sheets.dwg
 PLOTTED BY: mwatman
 DATE: Nov 13, 2020 - 1:33pm



Begin Project
 Begin Segment 1
 Sta. 12+35.00 "B"

AMAYA PROPERTIES, LLC

ERIK & KATHRYN FURNISH

Clark Military
 Grant #110

COUTAR
 REMAINDER IV,
 LLC

JAMES E. & PATSY D. SHIELDS

GLENN W. &
 ELLA MURPHY

GERALD &
 MARGIE C. DIXON

DC SILLS IPUG TRUST

- (D) 6" PCCP for Approaches on Dense Graded Subbase on Subgrade Treatment, Type II
- (K) Full Depth Pavement
- (R) 165#/SYS OC/OA-HMA, 2, 70, Surface, 9.5mm on Asphalt Milling, 1.5 in.
- (DW) Detectable Warning Surface
- (15) Curb
- (16) Center Curb
- (17) 6" PCCP for Approaches on Dense Graded Subbase on Geogrid, Type IB on Subgrade Treatment, Type II
- (S) Subgrade Treatment, Type XX
- (24) Sodding
- (E) Sidewalk, Concrete, 4"
- (U) Underdrain
- (18) Full Depth Pavement, PCCP for Approaches
- (R1) 165#/SYS HMA For Approaches, Type D on Asphalt Milling, 1.5 in.
- (R) Concrete Curb Ramp
- (19) Compacted Aggregate

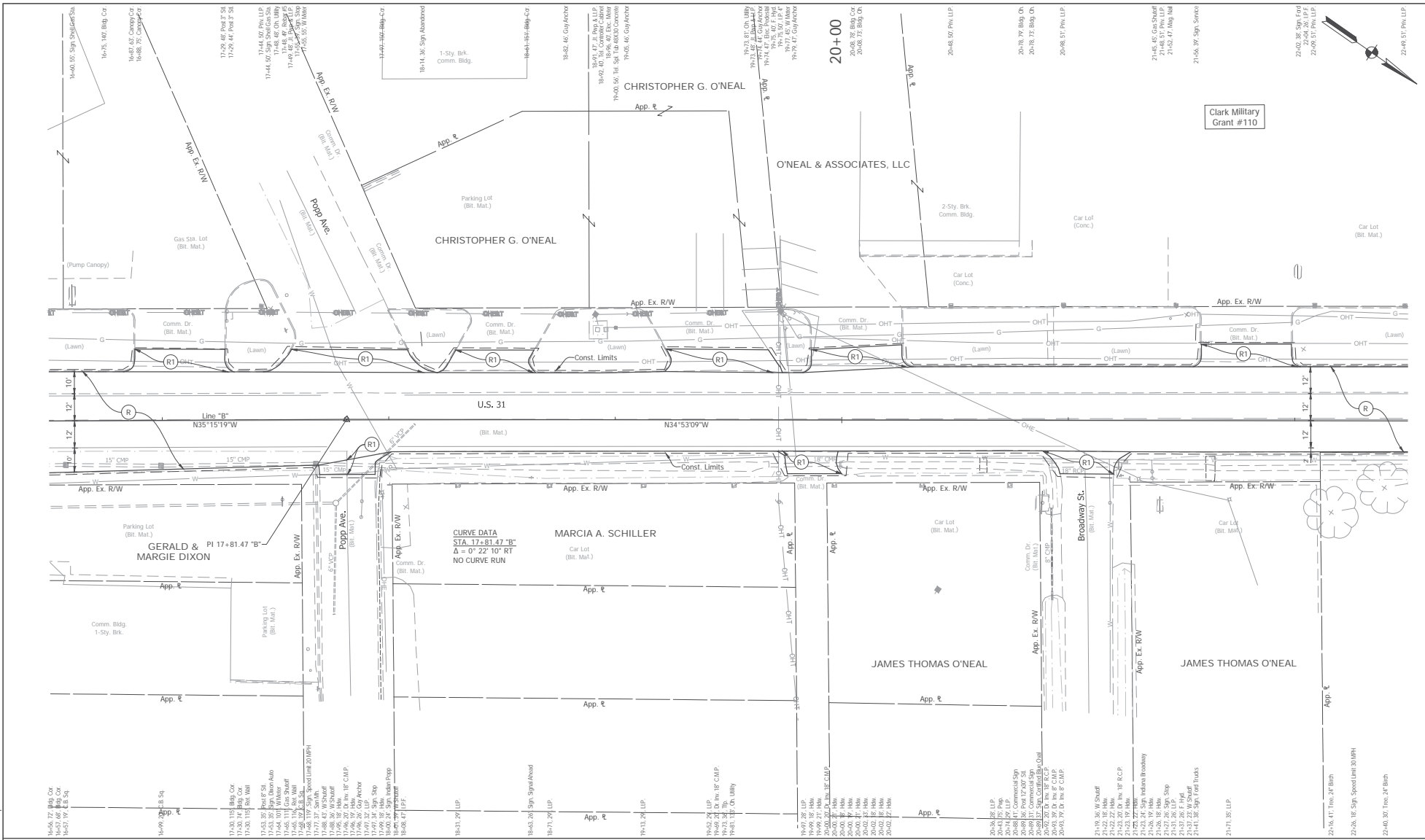
RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: BWS	DRAWN: MRW	CHECKED: ACD
CHECKED: ACD	CHECKED: BWS	

INDIANA
 DEPARTMENT OF TRANSPORTATION

PLAN SHEET
 STA. 10+50 TO STA. 16+50 "B"

HORIZONTAL SCALE	BRIDGE FILE
1"=20'	N/A
VERTICAL SCALE	DESIGNATION
	1700111
SURVEY BOOK	SHEETS
	25 of 150
CONTRACT	PROJECT
R-49412	1700111

DIRECTORY: S:\Project Files\2017\17-0199\CADD\Design\Sheet Sets
 FILE: 170199 Plan Sheets.dwg
 PLOTTED BY: mwatren
 DATE: Nov 13, 2020 - 1:46pm



(D) 6" PCCP for Approaches on Dense Graded Subbase on Subgrade Treatment, Type II	(K) Full Depth Pavement	(R) 165#/SYS OC/QA-HMA, 2, 70, Surface, 9.5mm on Asphalt Milling, 1.5 in.	(DW) Detectable Warning Surface
(D1) 4" PCCP for Approaches on Dense Graded Subbase on Geogrid, Type IB on Subgrade Treatment, Type II	(K1) Full Depth Pavement, PCCP for Approaches	(RT) 165#/SYS HMA For Approaches, Type D on Asphalt Milling, 1.5 in.	(R) Concrete Curb Ramp
(E) Sidewalk, Concrete, 4"	(S) Subgrade Treatment, Type XX	(U) Underdrain	(15) Curb
			(16) Center Curb
			(24) Sodding

16-56, 72, 88, 94, 100, 106, 112, 118, 124, 130, 136, 142, 148, 154, 160, 166, 172, 178, 184, 190, 196, 202, 208, 214, 220, 226, 232, 238, 244, 250, 256, 262, 268, 274, 280, 286, 292, 298, 304, 310, 316, 322, 328, 334, 340, 346, 352, 358, 364, 370, 376, 382, 388, 394, 400, 406, 412, 418, 424, 430, 436, 442, 448, 454, 460, 466, 472, 478, 484, 490, 496, 502, 508, 514, 520, 526, 532, 538, 544, 550, 556, 562, 568, 574, 580, 586, 592, 598, 604, 610, 616, 622, 628, 634, 640, 646, 652, 658, 664, 670, 676, 682, 688, 694, 700, 706, 712, 718, 724, 730, 736, 742, 748, 754, 760, 766, 772, 778, 784, 790, 796, 802, 808, 814, 820, 826, 832, 838, 844, 850, 856, 862, 868, 874, 880, 886, 892, 898, 904, 910, 916, 922, 928, 934, 940, 946, 952, 958, 964, 970, 976, 982, 988, 994, 1000

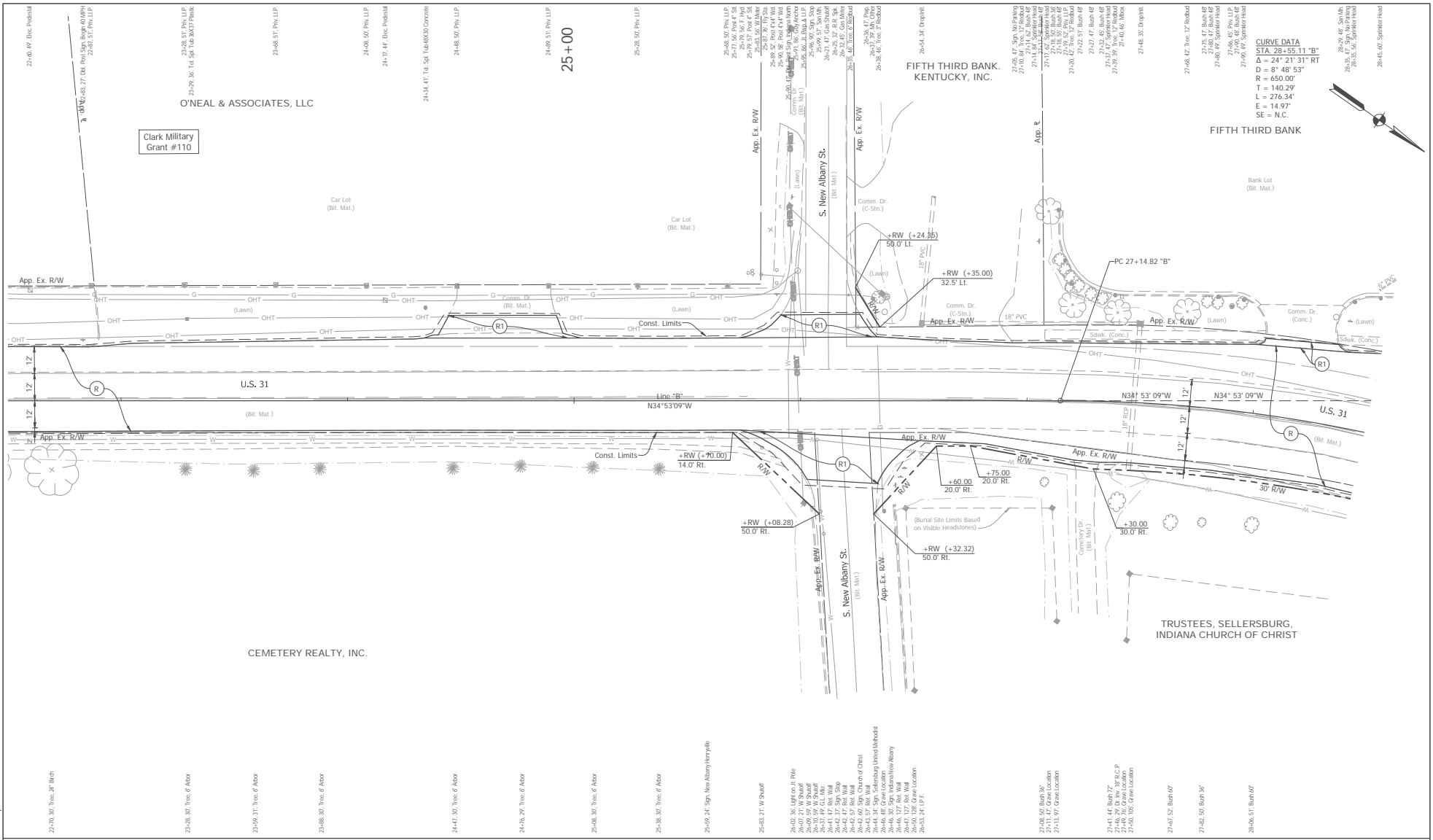
RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: BWS	DRAWN: MRW	CHECKED: BWS
CHECKED: ACD	CHECKED: BWS	

INDIANA
DEPARTMENT OF TRANSPORTATION

PLAN SHEET
STA. 16+50 TO STA. 22+50 "B"

HORIZONTAL SCALE	1"=20'	BRIDGE FILE	N/A
VERTICAL SCALE	1"=20'	DESIGNATION	1700111
SURVEY BOOK	27	SHEETS	of 150
CONTRACT	R-49412	PROJECT	1700111

DIRECTORY: S:\Project Files\2017\17-0199\CADD\Design\Sheet Sets
 FILE: 170199 Plan Sheets.dwg
 PLOTTED BY: mwatren
 DATE: Nov 13, 2020 - 1:48pm

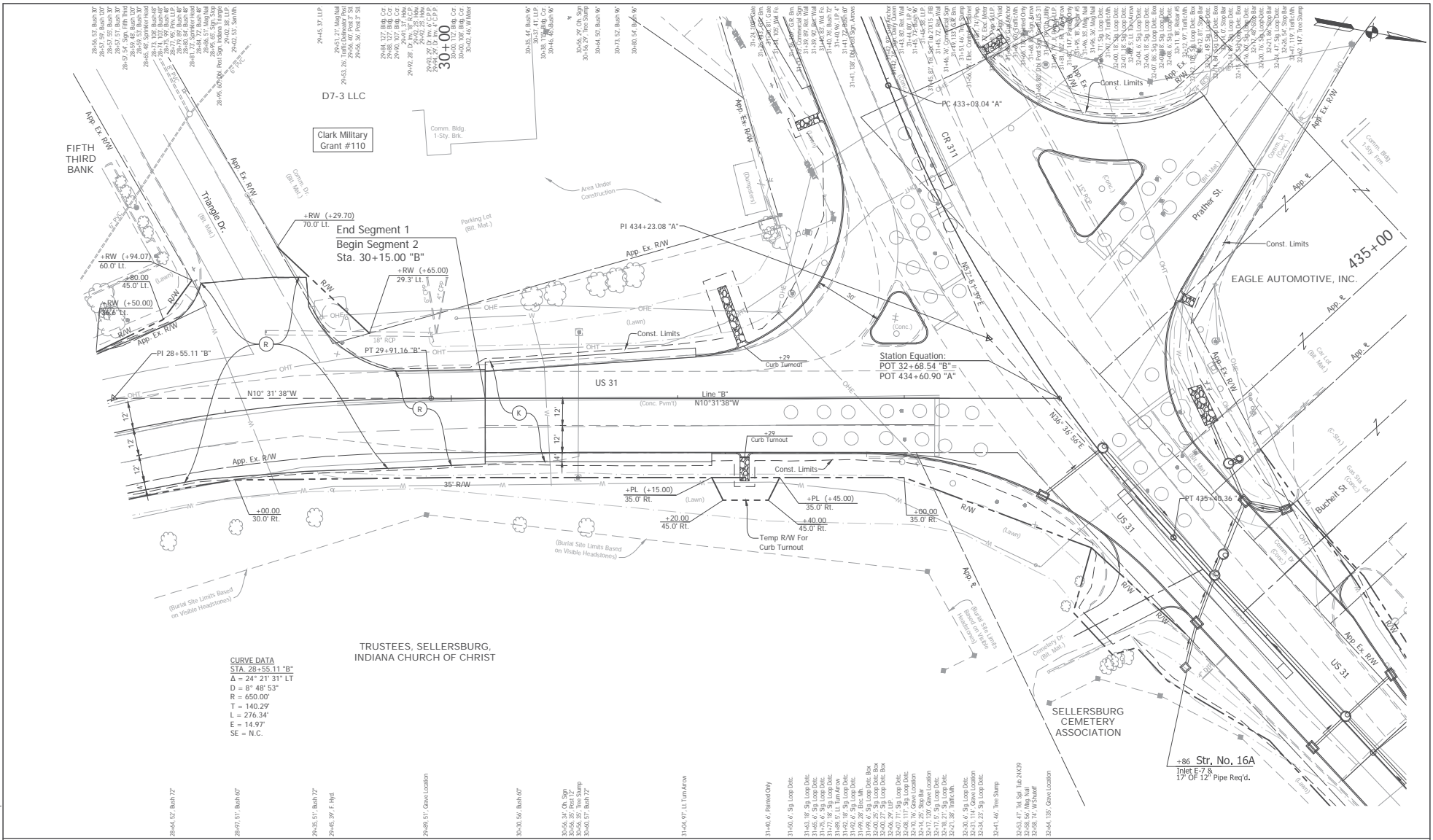


CURVE DATA
 STA. 28+55.11 "B"
 Δ = 24° 21' 31" RT
 D = 8° 48' 53"
 R = 650.00'
 T = 140.29'
 L = 276.34'
 E = 14.97'
 SE = N.C.



(D) 6" PCCP for Approaches on Dense Graded Subbase on Subgrade Treatment, Type II	(K) Full Depth Pavement	(R) 165#/SYS OC/QA-HMA, 2, 70, Surface, 9.5mm on Asphalt Milling, 1.5 in.	(DW) Detectable Warning Surface	(15) Curb	RECOMMENDED FOR APPROVAL DESIGN ENGINEER: _____ DATE: _____ DRAWN: _____ MRW CHECKED: _____ BWS	INDIANA DEPARTMENT OF TRANSPORTATION PLAN SHEET STA. 22+50 TO STA. 28+50 "B"	HORIZONTAL SCALE 1"=20'	BRIDGE FILE N/A
(D1) 4" PCCP for Approaches on Dense Graded Subbase on Geogrid, Type IB on Subgrade Treatment, Type II	(K1) Full Depth Pavement, PCCP for Approaches	(R1) 165#/SYS HMA For Approaches, Type D on Asphalt Milling, 1.5 in.	(R) Concrete Curb Ramp	(16) Center Curb			SURVEY BOOK	SHEETS 29 of 150
(E) Sidewalk, Concrete, 4"	(S) Subgrade Treatment, Type XX	(U) Underdrain	(D) Compacted Aggregate	(24) Sodding	CHECKED: _____ ACD	CONTRACT R-40412	PROJECT 1700111	

DIRECTORY: S:\Project Files\2017\17-0199\CADD\Design\Sheet Sets
 FILE: 170199 Plan Sheets.dwg
 PLOTTED BY: mwatren
 DATE: Nov 15, 2020 - 12:55pm



CURVE DATA
 STA. 28+55.11 "B"
 Δ = 24° 21' 31" LT
 D = 8' 48" 53"
 R = 650.00'
 T = 140.29'
 L = 276.34'
 E = 14.97'
 SE = N.C.

- (D) 6" PCCP for Approaches on Dense Graded Subbase or Subgrade Treatment, Type II
- (K) Full Depth Pavement
- (R) 165#/SYS OC/QA-HMA, 2, 70, Surface, 9.5mm on Asphalt Milling, 1.5 in.
- (DW) Detectable Warning Surface
- (15) Curb
- (DT) 4" PCCP for Approaches on Dense Graded Subbase or Geogrid, Type IB on Subgrade Treatment, Type II
- (KL) Full Depth Pavement, PCCP for Approaches
- (RT) 165#/SYS HMA For Approaches, Type D on Asphalt Milling, 1.5 in.
- (R) Concrete Curb Ramp
- (16) Center Curb
- (E) Sidewalk, Concrete, 4"
- (S) Subgrade Treatment, Type XX
- (D) Compacted Aggregate
- (2A) Sodding
- (U) Underdrain

- (15) Curb
- (16) Center Curb
- (2A) Sodding

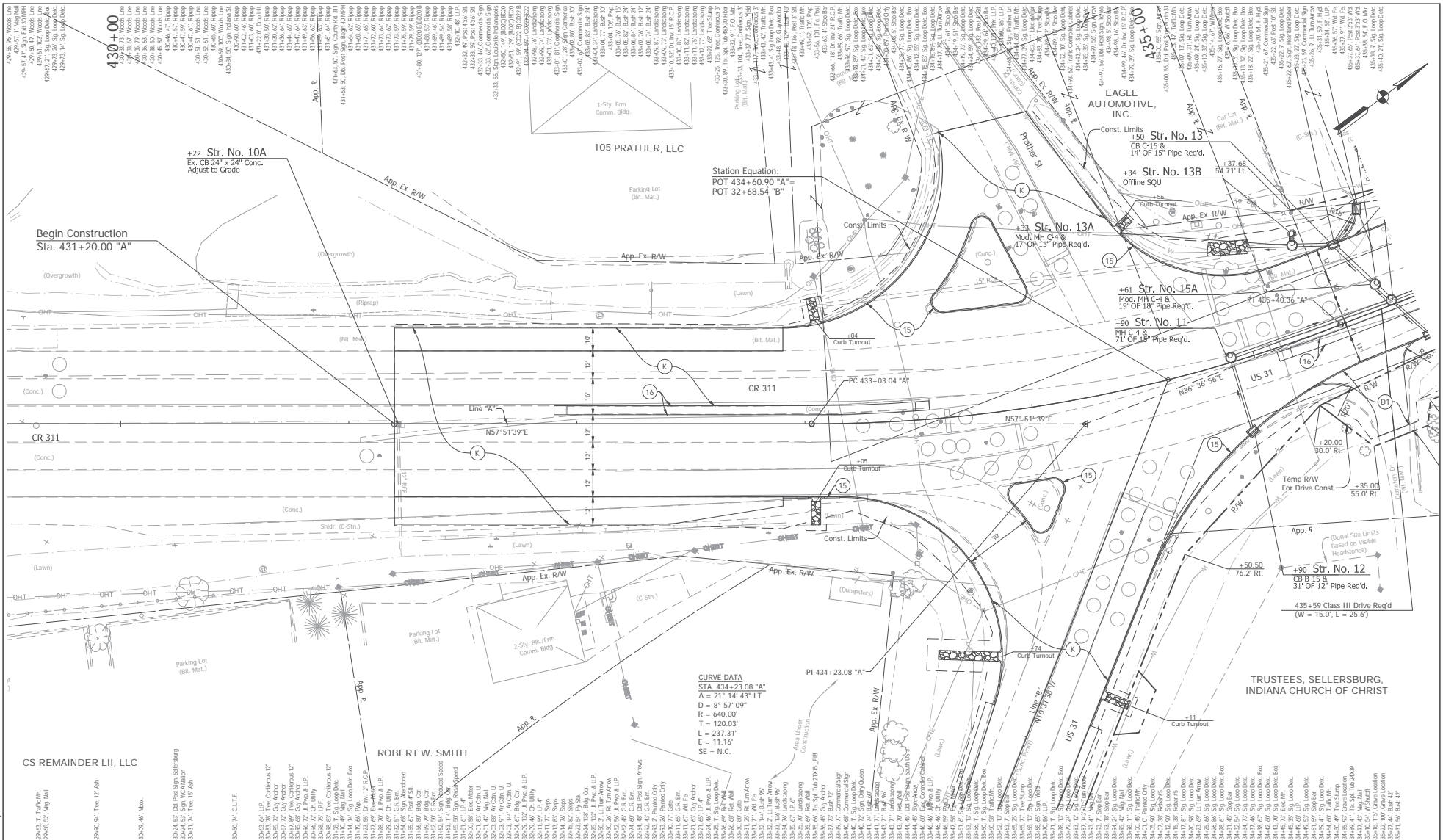
RECOMMENDED FOR APPROVAL		DESIGN ENGINEER		DATE
DESIGNED: BWS	DRAWN: MRW			
CHECKED: ACD	CHECKED: BWS			

INDIANA DEPARTMENT OF TRANSPORTATION

PLAN SHEET
 STA. 28+50 TO STA. 32+68.54 "B"

HORIZONTAL SCALE 1"=20'	BRIDGE FILE N/A
VERTICAL SCALE	DESIGNATION 1700111
SURVEY BOOK	SHEETS 31 of 150
CONTRACT R-40412	PROJECT 1700111

DIRECTORY: S:\Project Files\2017-0199\CADD\Design\Sheet Sets
 FILE: 0199 Plan Sheets.dwg
 DIMSCALE: 1/16"=1'-0"
 PLOTTED BY: mwamren
 DATE: Nov 15, 2020 - 7:43pm



D	6" PCCP for Approaches on Dense Graded Subbase on Subgrade Treatment, Type II	K	Full Depth Pavement	R	165#/SYS Q/OA-HMA, 2, 70, Surface, 9.5mm on Asphalt Milling, 1.5 in.	DW	Detectable Warning Surface
D1	6" PCCP for Approaches on Dense Graded Subbase on Geogrid, Type IB on Subgrade Treatment, Type II	K1	Full Depth Pavement, PCCP for Approaches	R1	165#/SYS HMA For Approaches, Type D on Asphalt Milling, 1.5 in.	R	Concrete Curb Ramp
E	Sidewalk, Concrete, 4"	S	Subgrade Treatment, Type XX	S	Subgrade Treatment, Type XX	D	Compacted Aggregate
		U	Underdrain			15	Curb
						16	Center Curb
						2A	Sodding

CURVE DATA
 STA. 434+23.08 "A"
 $\Delta = 21^\circ 14' 43" \text{LT}$
 $D = 8^\circ 57' 09"$
 $R = 640.00'$
 $T = 120.03'$
 $L = 237.31'$
 $E = 11.16'$
 $SE = \text{N.C.}$

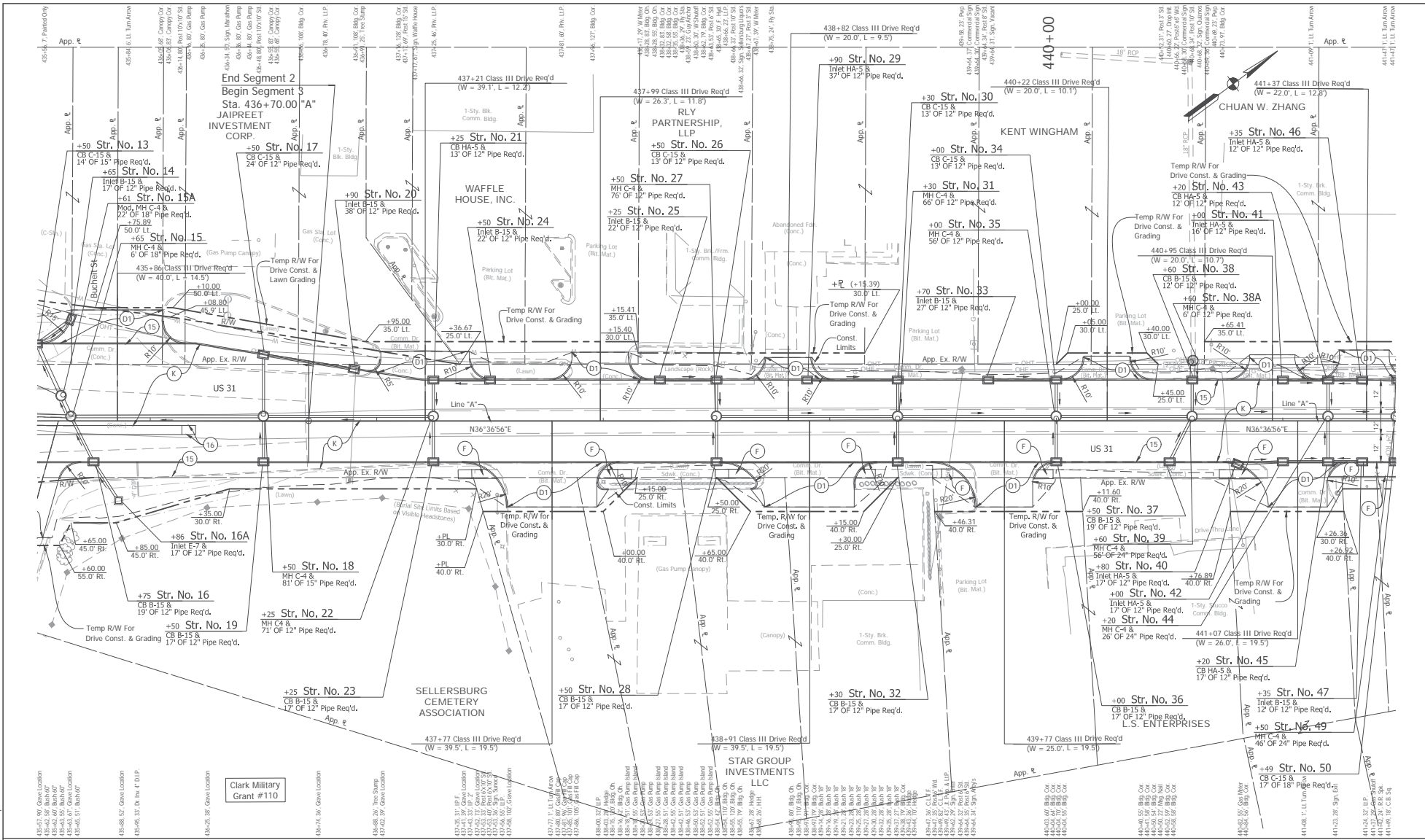
RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: BWS	DRAWN: MRW	
CHECKED: ACD	CHECKED: BWS	

INDIANA
DEPARTMENT OF TRANSPORTATION

PLAN SHEET
 STA. 429+50 TO STA. 435+50 "A"

HORIZONTAL SCALE	1"=20'
VERTICAL SCALE	DESIGNATION
	1700111
SURVEY BOOK	33 of 150
CONTRACT	R-49112
PROJECT	1700111

DIRECTORY: S:\Project Files\2017\17-0199\CADD\Design\Sheet Sets
 FILE: 170199 Plan Sheets.dwg
 PLOTTED BY: mwarmen
 DATE: 11/13/2019 2:11pm



(D)	6" PCCP for Approaches on Dense Graded Subbase on Subgrade Treatment, Type II	(K)	Full Depth Pavement	(R)	165#/SYS OC/QA-HMA, 2, 70, Surface, 9.5mm on Asphalt Milling, 1.5 in.	(DW)	Detectable Warning Surface	(15)	Curb
(D1)	6" PCCP for Approaches on Dense Graded Subbase on Geogrid, Type IB on Subgrade Treatment, Type II	(K1)	Full Depth Pavement, PCCP for Approaches	(R1)	165#/SYS HMA For Approaches, Type D on Asphalt Milling, 1.5 in.	(CR)	Concrete Curb Ramp	(16)	Center Curb
(E)	Sidewalk, Concrete, 4"	(S)	Subgrade Treatment, Type XX	(U)	Underdrain	(D)	Compacted Aggregate	(2A)	Sodding

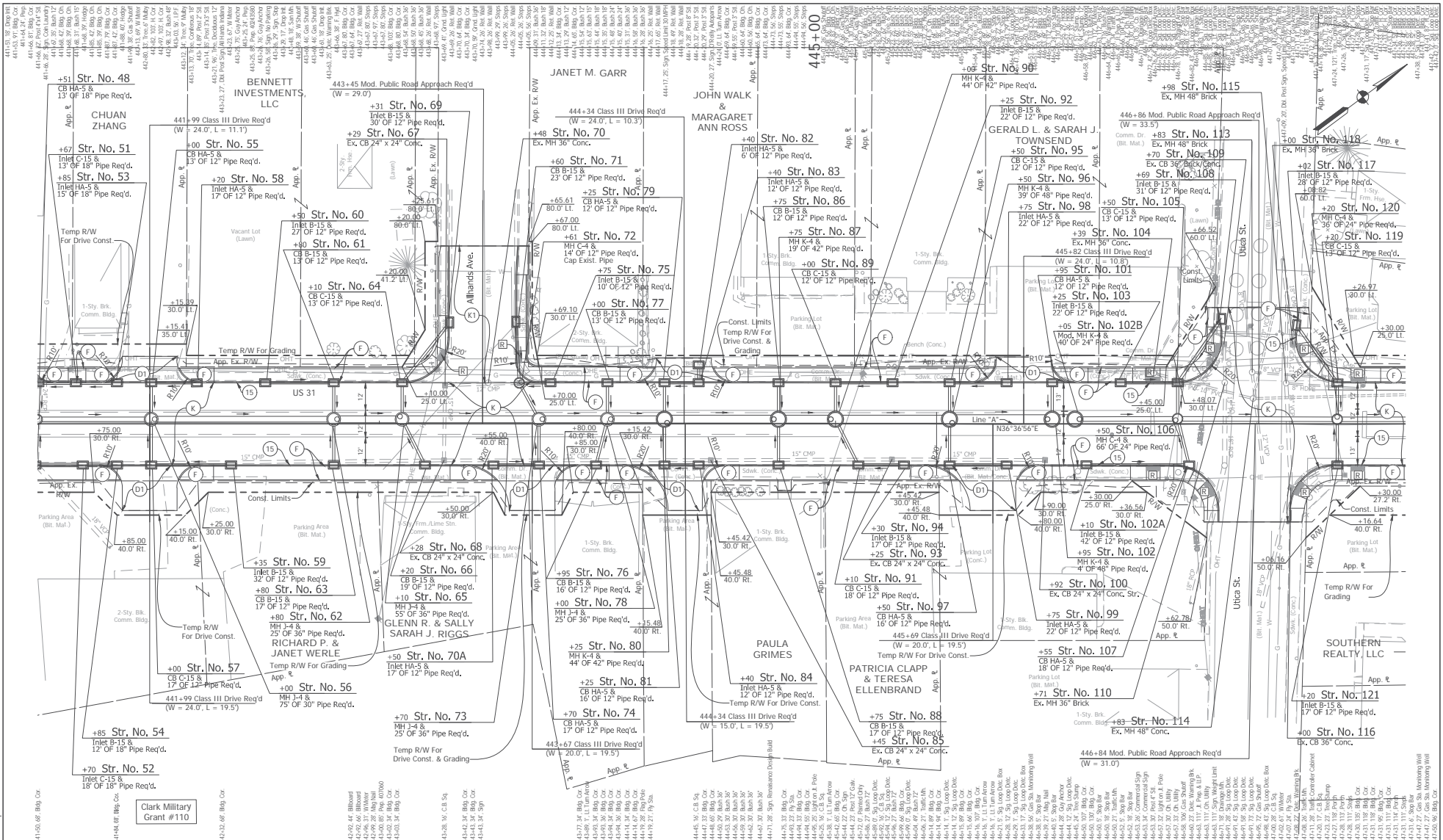
DESIGNED: BWS	DRAWN: MRW	DATE:
CHECKED: ACD	CHECKED: BWS	

INDIANA
DEPARTMENT OF TRANSPORTATION

PLAN SHEET
STA. 435+50 TO STA. 441+50 "A"

HORIZONTAL SCALE 1"=20'	BRIDGE FILE N/A
VERTICAL SCALE DESIGNATION 1700111	SHEETS 35 of 150
SURVEY BOOK	PROJECT R-40112 1700111

DIRECTORY: S:\Project Files\2017\17-019\CADD\Design\Sheet Sets
 FILE: 17019 Plan Sheets.dwg
 PLOTTED BY: mwarmen
 DATE: 11/13/2020 - 2:19pm



- (D) 6" PCCP for Approaches on Dense Graded Subbase on Subgrade Treatment, Type II
- (D1) 4" PCCP for Approaches on Dense Graded Subbase on Geogrid, Type IB on Subgrade Treatment, Type II
- (E) Sidewalk, Concrete, 4"
- (K) Full Depth Pavement
- (K1) Full Depth Pavement, PCCP for Approaches
- (R) 165#/SYS OC/QA-HMA, 2, 70, Surface, 9.5mm on Asphalt Milling, 1.5 in.
- (R1) 165#/SYS HMA for Approaches, Type D on Asphalt Milling, 1.5 in.
- (S) Subgrade Treatment, Type XX
- (U) Underdrain
- (DW) Detectable Warning Surface
- (R) Concrete Curb Ramp
- (D) Compacted Aggregate
- (15) Curb
- (16) Center Curb
- (2A) Sodding

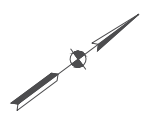
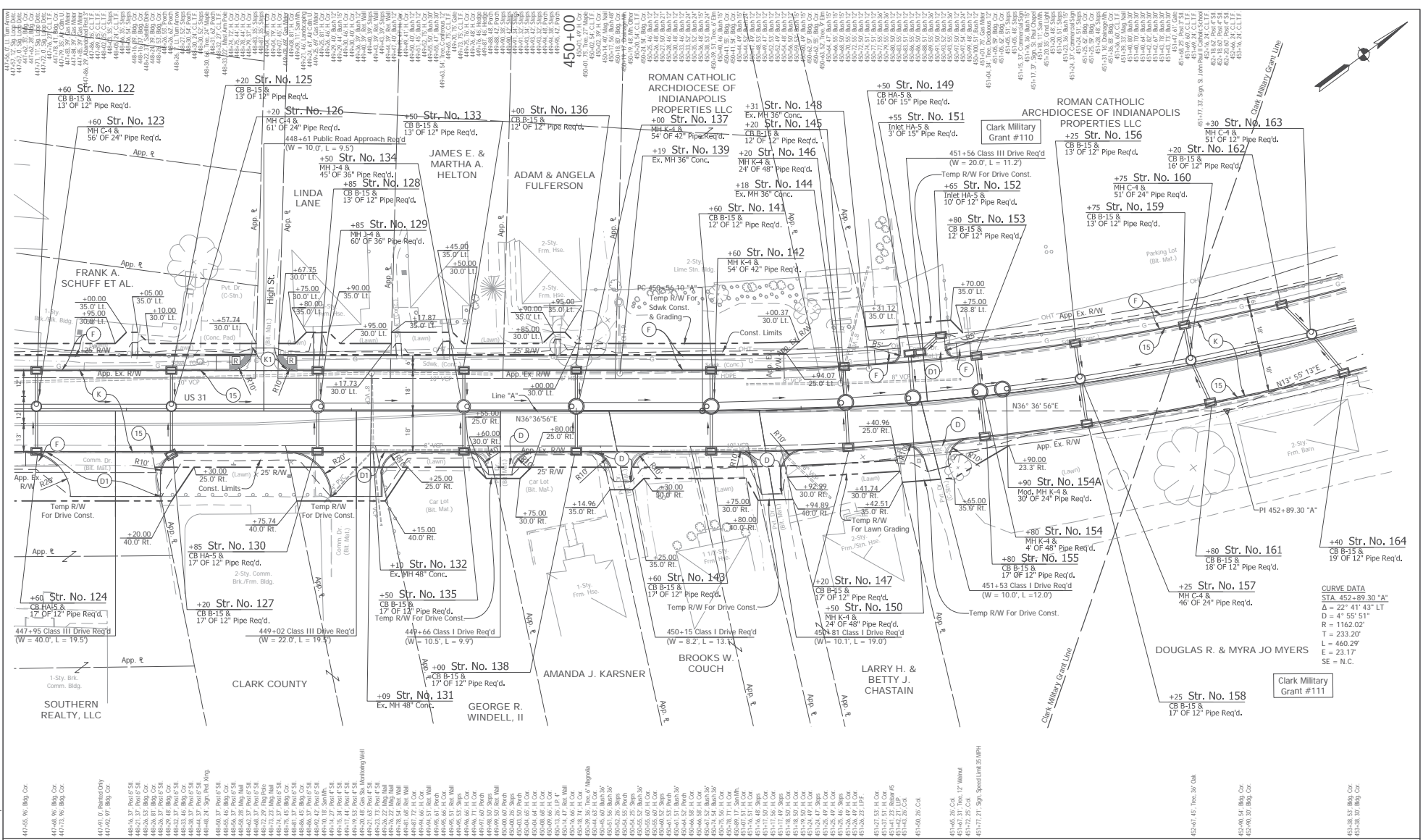
RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: BWS	DRAWN: MRW	
CHECKED: ACD	CHECKED: BWS	

INDIANA
DEPARTMENT OF TRANSPORTATION

PLAN SHEET
STA. 441+50 TO STA. 447+50 "A"

HORIZONTAL SCALE	BRIDGE FILE
1"=20'	N/A
VERTICAL SCALE	DESIGNATION
	1700111
SURVEY BOOK	SHEETS
	37 of 150
CONTRACT	PROJECT
R-49112	1700111

DIRECTORY: S:\Projects\Road\01717-0199\CADD\Design\Sheet Sets
 FILE: 0199 Plan Sheets.dwg
 PLOTTED BY: mwamren
 DATE: Nov 13, 2020 - 2:27pm



(D)	6" PCCP for Approaches on Dense Graded Subbase on Subgrade Treatment, Type II	(K)	Full Depth Pavement	(R)	165#/SYS OC/QA-HMA, 2, 70, Surface, 9.5mm on Asphalt Milling, 1.5 in.	(DW)	Detectable Warning Surface	(15)	Curb
(D1)	4" PCCP for Approaches on Dense Graded Subbase on Geogrid, Type IB on Subgrade Treatment, Type II	(K1)	Full Depth Pavement, PCCP for Approaches	(R1)	165#/SYS HMA for Approaches, Type D on Asphalt Milling, 1.5 in.	(R)	Concrete Curb Ramp	(16)	Center Curb
(E)	Sidewalk, Concrete, 4"	(S)	Subgrade Treatment, Type XX	(U)	Underdrain	(D)	Compacted Aggregate	(2A)	Sodding

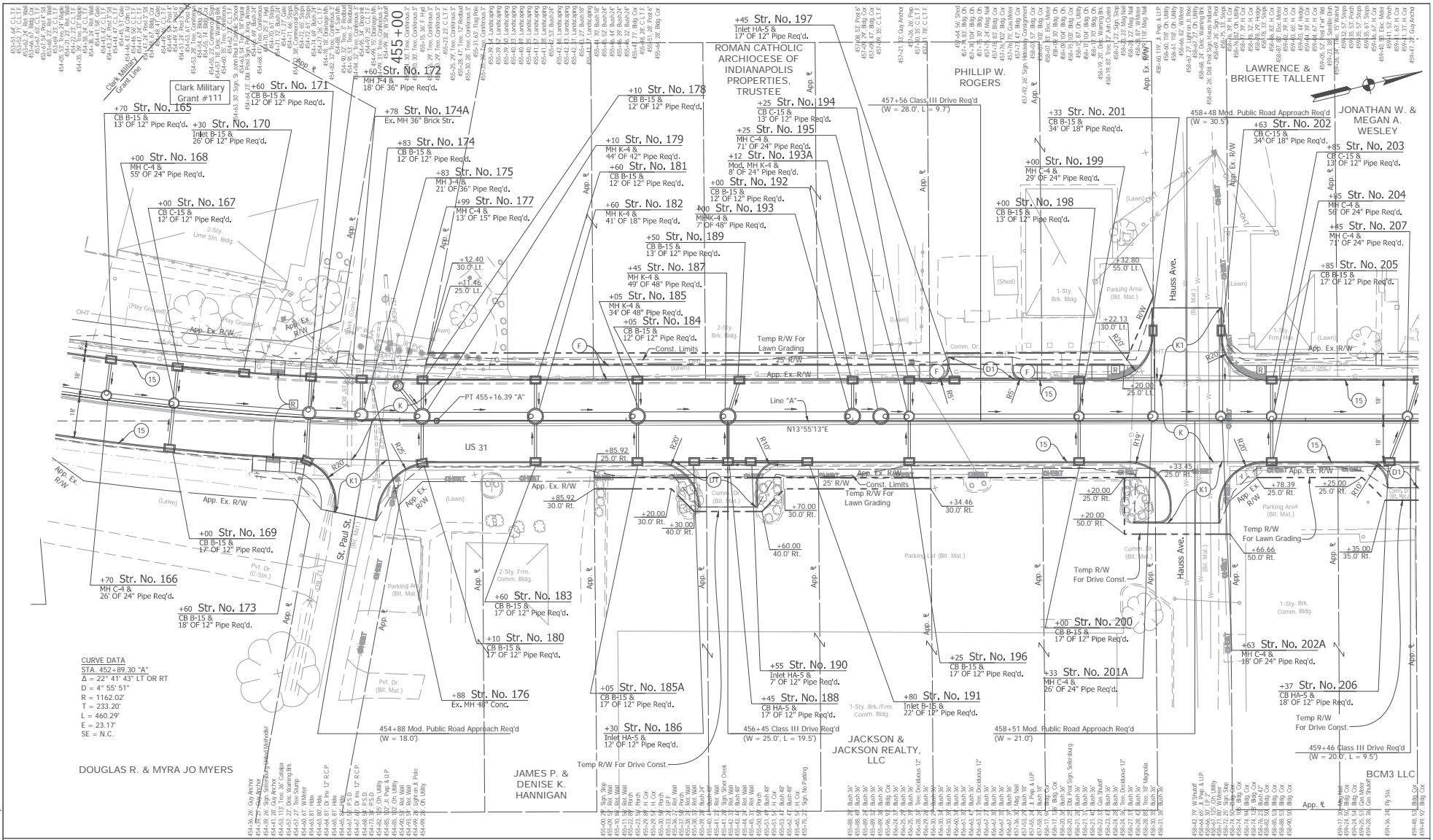
DESIGNED:	BWS	DRAWN:	MRW
CHECKED:	ACD	CHECKED:	BWS

INDIANA
DEPARTMENT OF TRANSPORTATION

PLAN SHEET
 STA. 447+50 TO STA. 453+50 "A"

HORIZONTAL SCALE	1"=20'	BRIDGE FILE	N/A
VERTICAL SCALE	1"=40'	DESIGNATION	1700111
SURVEY BOOK	39	SHEETS	of 150
CONTRACT	R-40412	PROJECT	1700111

DIRECTORY: S:\Project Files\2017\17-0199\CADD\Design\Sheet Sets
 FILE: 170199 Plan Sheets.dwg
 PLOTTED BY: mwarmen
 DATE: 11/13/2020 - 2:35pm



(D)	6" PCCP for Approaches on Dense Graded Subbase on Subgrade Treatment, Type II	(R)	165#/SYS OC/QA-HMA, 2, 70, Surface, 9.5mm on Asphalt Milling, 1.5 in.	(DW)	Detectable Warning Surface	(15)	Curb
(D1)	6" PCCP for Approaches on Dense Graded Subbase on Geogrid, Type IB on Subgrade Treatment, Type II	(K)	Full Depth Pavement	(RT)	165#/SYS HMA for Approaches, Type D on Asphalt Milling, 1.5 in.	(R)	Concrete Curb Ramp
(E)	Sidewalk, Concrete, 4"	(K1)	Full Depth Pavement, PCCP for Approaches	(S)	Subgrade Treatment, Type XX	(16)	Center Curb
		(U)	Underdrain	(D)	Compacted Aggregate	(2A)	Sodding

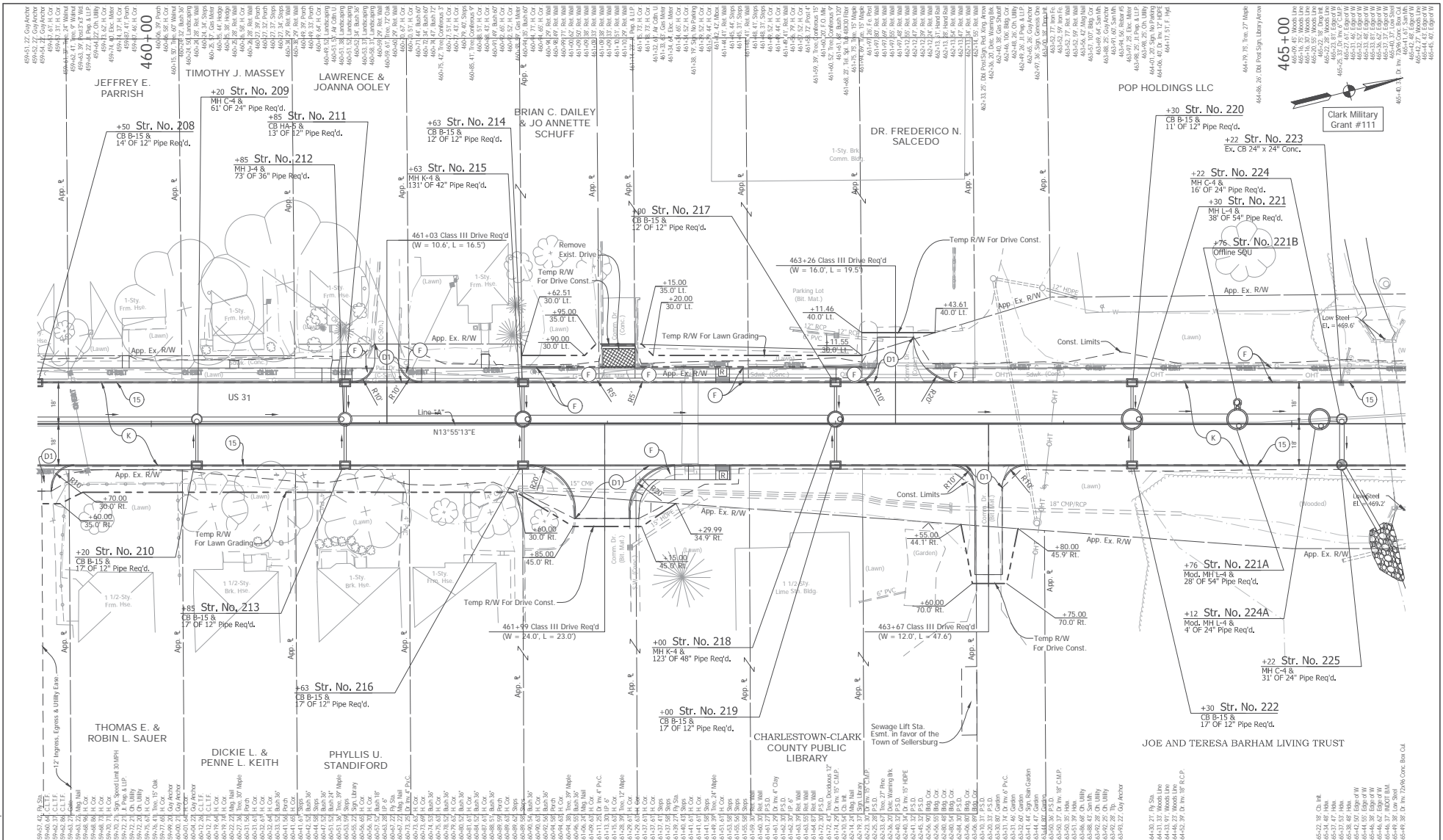
DESIGNED:	BWS	DRAWN:	MRW
CHECKED:	ACD	CHECKED:	BWS

INDIANA
DEPARTMENT OF TRANSPORTATION

PLAN SHEET
 STA. 453+50 TO STA. 459+50 "A"

HORIZONTAL SCALE	1"=20'	BRIDGE FILE	N/A
VERTICAL SCALE	1"=20'	DESIGNATION	1700111
SURVEY BOOK		SHEETS	41 of 150
CONTRACT	R-494112	PROJECT	1700111

DIRECTORY: S:\Project Files\2017\17-0199\CADD\Design\Sheet Sets\1
 FILE: 170199 Plan Sheets.dwg
 PLOTTED BY: mwatren
 DATE: Nov 13, 2020 - 2:42pm



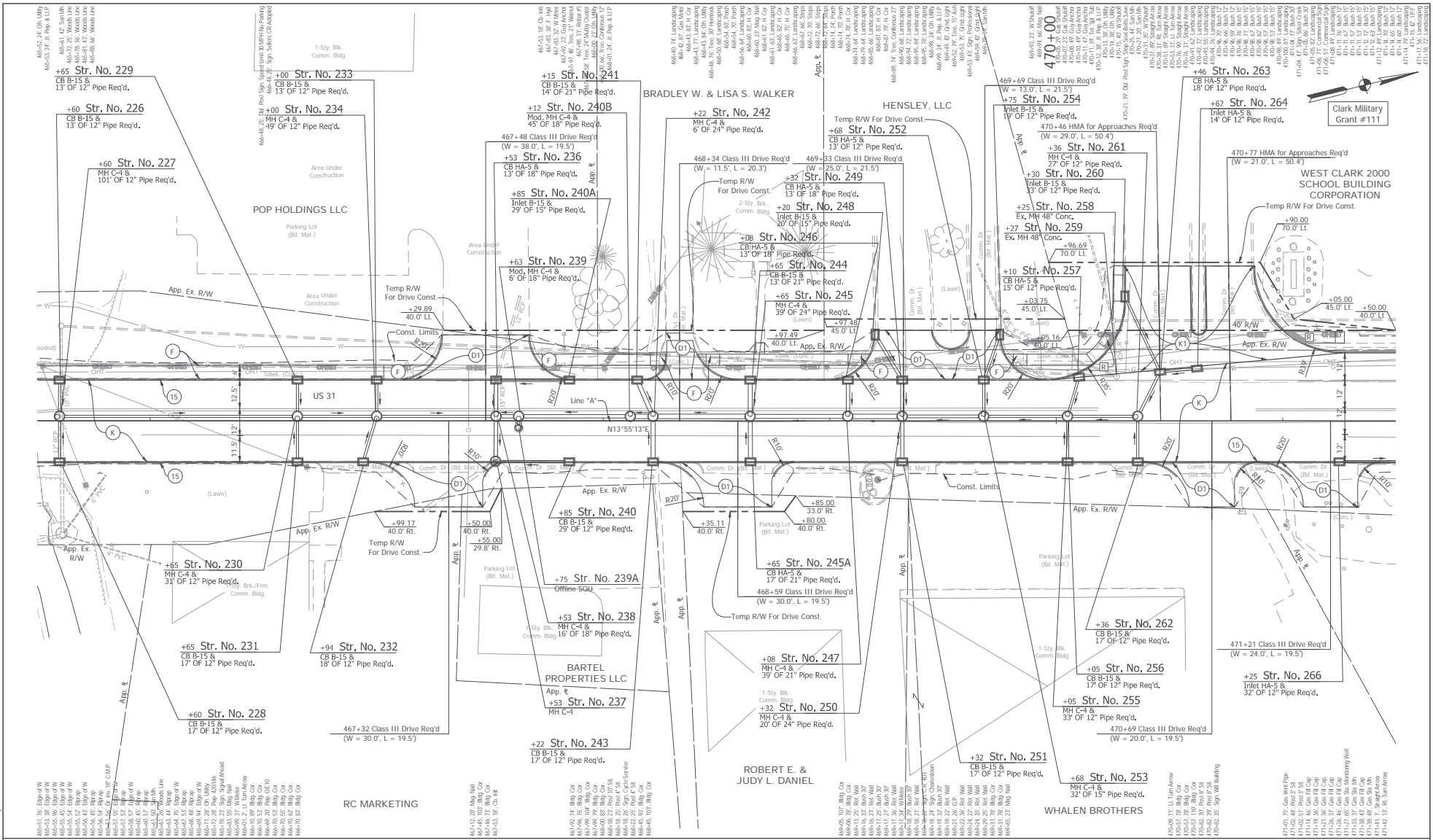
(D)	6" PCCP for Approaches on Dense Graded Subbase on Subgrade Treatment, Type II	(R)	165#/SYS OC/QA-HMA, 2, 70, Surface, 9.5mm on Asphalt Milling, 1.5 in.	(DW)	Detectable Warning Surface	(15)	Curb
(D1)	4" PCCP for Approaches on Dense Graded Subbase on Geogrid, Type IB on Subgrade Treatment, Type II	(K)	Full Depth Pavement	(RT)	165#/SYS HMA for Approaches, Type D on Asphalt Milling, 1.5 in.	(R)	Concrete Curb Ramp
(E)	Sidewalk, Concrete, 4"	(K1)	Full Depth Pavement, PCCP for Approaches	(S)	Subgrade Treatment, Type XX	(D)	Compacted Aggregate
		(U)	Underdrain			(2A)	Sodding

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: BWS	DRAWN: MRW	CHECKED: BWS
CHECKED: ACD	CHECKED: BWS	

INDIANA
 DEPARTMENT OF TRANSPORTATION
 PLAN SHEET
 STA. 459+50 TO STA. 465+50 "A"

HORIZONTAL SCALE	BRIDGE FILE
1"=20'	N/A
VERTICAL SCALE	DESIGNATION
1"=10'	1700111
SURVEY BOOK	SHEETS
43	of 150
CONTRACT	PROJECT
R-40412	1700111

DIRECTORY: S:\Project Files\2017\17-0199\CADD\Design\Sheet Sets
 FILE: 170199 Plan Sheets.dwg
 PLOTTED BY: mwamren
 DATE: Nov 13, 2020 - 2:52pm



D 6" PCCP for Approaches on Dense Graded Subbase on Subgrade Treatment, Type II	K Full Depth Pavement	R 165#/SY'S OC/QA-HMA, 2, 70, Surface, 9.5mm on Asphalt Milling, 1.5 in.	DW Detectable Warning Surface
D1 4" PCCP for Approaches on Dense Graded Subbase on Geogrid, Type IB on Subgrade Treatment, Type II	K1 Full Depth Pavement, PCCP for Approaches	R1 165#/SY'S HMA For Approaches, Type D on Asphalt Milling, 1.5 in.	R Concrete Curb Ramp
E Sidewalk, Concrete, 4"	S Subgrade Treatment, Type XX	S Subgrade Treatment, Type XX	15 Curb
	U Underdrain	20 Sodding	16 Center Curb
		24 Sodding	

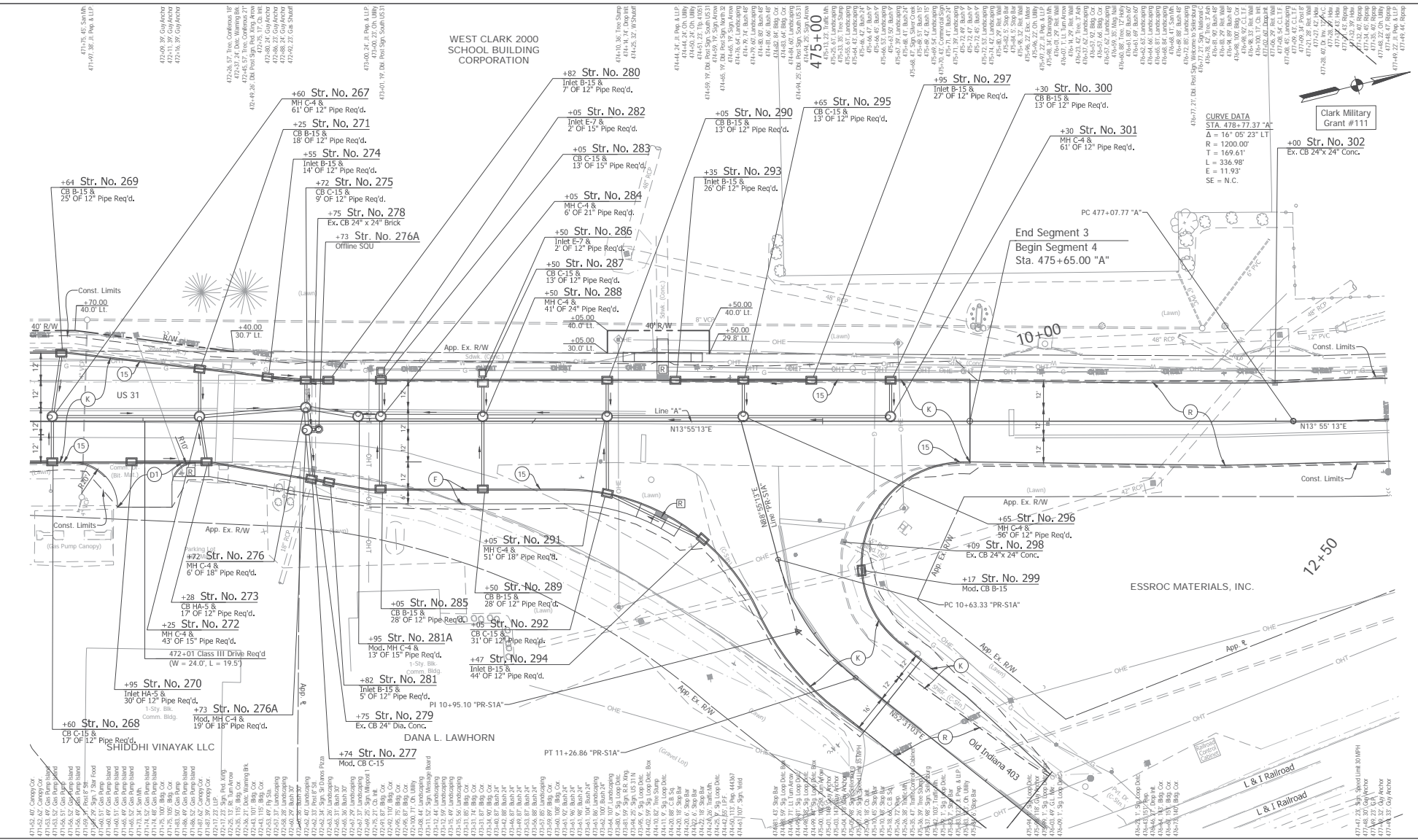
RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: BWS	DRAWN: MRW	
CHECKED: ACD	CHECKED: BWS	

INDIANA
 DEPARTMENT OF TRANSPORTATION

PLAN SHEET
 STA. 465+50 TO STA. 471+50 "A"

HORIZONTAL SCALE	BRIDGE FILE
1"=20'	N/A
VERTICAL SCALE	DESIGNATION
	1700111
SURVEY BOOK	SHEETS
	45 of 150
CONTRACT	PROJECT
R-40412	1700111

DIRECTORY: S:\Project Files\2017\17-0199\CADD\Design\Sheet Sets
 FILE: 170199 Plan Sheets.dwg
 DIMSCALE: 1/8" = 1'-0"
 PLOTTED BY: mwamren
 DATE: 11/15/2019 - 12:59pm



WEST CLARK 2000 SCHOOL BUILDING CORPORATION

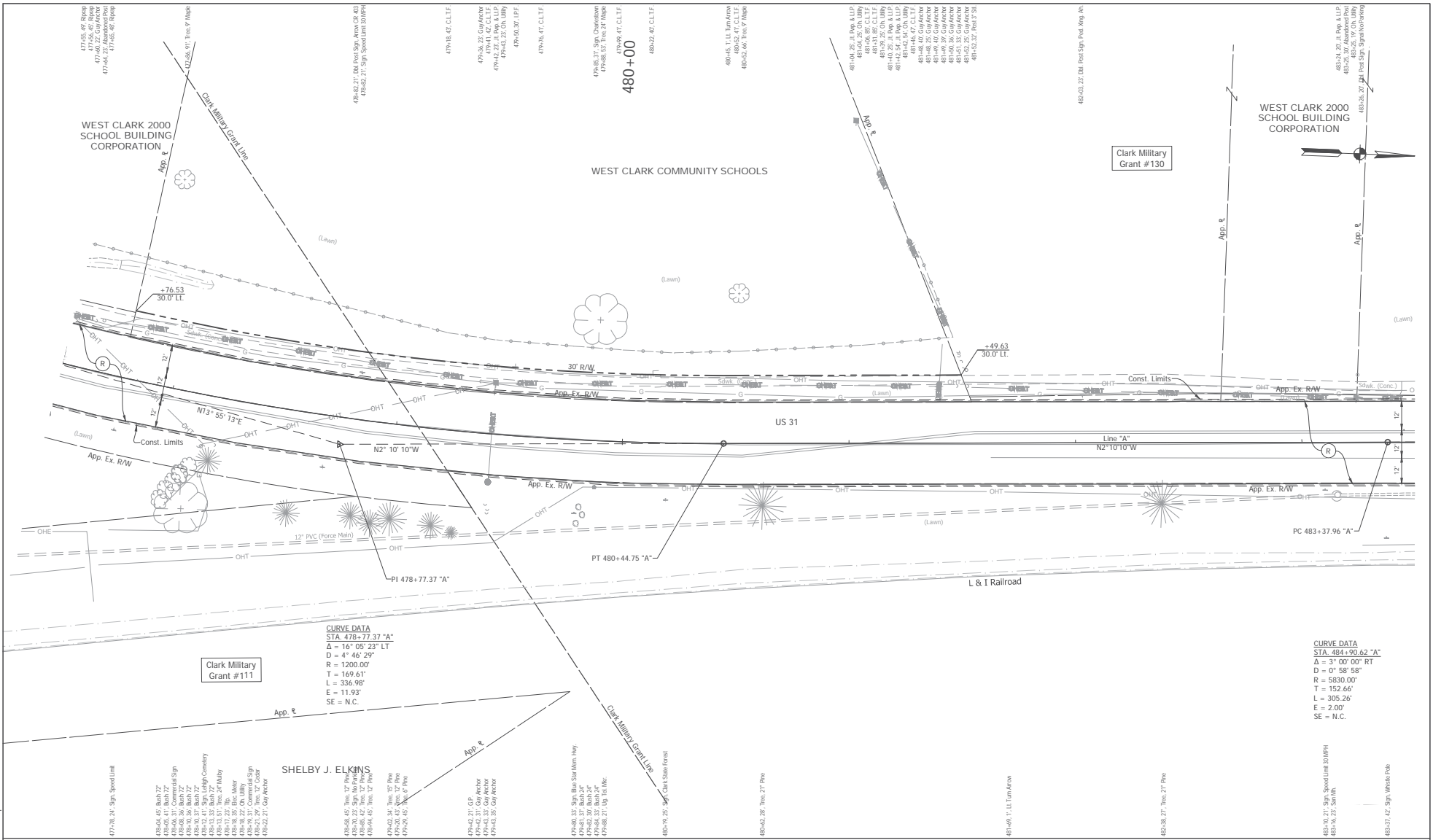


Clark Military Grant #111

CURVE DATA
 STA. 475+77.37 "A"
 Δ = 16° 05' 23" LT
 R = 1200.00'
 T = 169.61'
 L = 336.98'
 E = 11.93'
 SE = N.C.

6" PCCP for Approaches on Dense Graded Subbase on Subgrade Treatment, Type II (K) Full Depth Pavement		15" SY/SYS OC/QA-HMA, 2, 70, Surface, 9.5mm on Asphalt Milling, 1.5 in. (R) 15" SY/SYS HMA For Approaches, Type D on Asphalt Milling, 1.5 in.		(DW) Detectable Warning Surface (C) Concrete Curb Ramp (15) Center Curb (16) Center Curb (20) Sodding		RECOMMENDED FOR APPROVAL _____ DESIGN ENGINEER _____ DATE _____ DESIGNED: BWS DRAWN: MRW CHECKED: ACD CHECKED: BWS	INDIANA DEPARTMENT OF TRANSPORTATION PLAN SHEET STA. 471+50 TO STA. 477+50 "A"	HORIZONTAL SCALE 1"=20' VERTICAL SCALE _____ SURVEY BOOK _____ SHEETS 47 of 150 CONTRACT R-40412 PROJECT 1700111	
6" PCCP for Approaches on Dense Graded Subbase on Geogrid, Type IB on Subgrade Treatment, Type II (D) Sidewalk, Concrete, 4"		(S) Subgrade Treatment, Type XX (U) Underdrain							

DIRECTORY: S:\Project Files\2017\17-0199\CADD\Design\Sheet Sets
 FILE: 170199 Plan Sheets.dwg
 PLOTTED BY: mwatren
 DATE: Nov 15, 2019 - 1:00pm



(D)	6" PCCP for Approaches on Dense Graded Subbase on Subgrade Treatment, Type II	(K)	Full Depth Pavement	(R)	165#/SYS OC/QA-HMA, 2, 70, Surface, 9.5mm on Asphalt Milling, 1.5 in.
(D1)	6" PCCP for Approaches on Dense Graded Subbase on Geogrid, Type IB on Subgrade Treatment, Type II	(K1)	Full Depth Pavement, PCCP for Approaches	(R1)	165#/SYS HMA For Approaches, Type D on Asphalt Milling, 1.5 in.
(E)	Sidewalk, Concrete, 4"	(S)	Subgrade Treatment, Type XX	(D)	Compacted Aggregate
		(U)	Underdrain	(15)	Curb
				(16)	Center Curb
				(24)	Sodding

(DW)	Detectable Warning Surface	(15)	Curb
(R)	Concrete Curb Ramp	(16)	Center Curb
(D)	Compacted Aggregate	(24)	Sodding

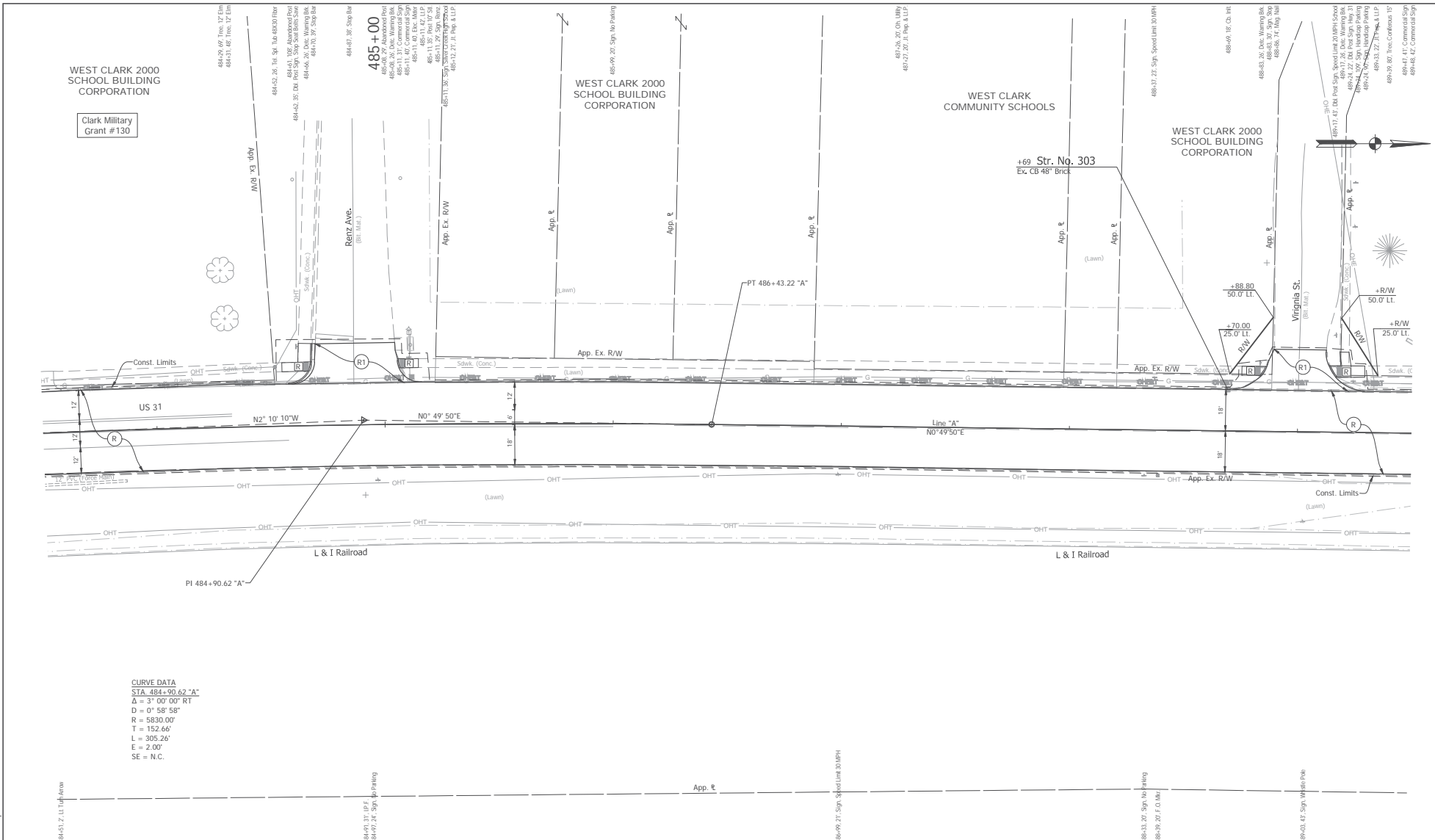
RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: BWS	DRAWN: MRW	
CHECKED: ACD	CHECKED: BWS	

INDIANA
DEPARTMENT OF TRANSPORTATION

PLAN SHEET
STA. 477+50 TO STA. 483+50 "A"

HORIZONTAL SCALE	BRIDGE FILE
1"=20'	N/A
VERTICAL SCALE	DESIGNATION
	1700111
SURVEY BOOK	SHEETS
	49 of 150
CONTRACT	PROJECT
R-40412	1700111

DIRECTORY: S:\Project Files\2017\17-0199\CADD\Design\Sheet Sets
 FILE: 170111 Plan Sheets.dwg
 DIMSCALE: 1/8"=1'-0"
 PLOTTED BY: mwatren
 DATE: Nov 15, 2020 - 1:00pm



CURVE DATA
 STA. 484+90.62 "A"
 Δ = 3° 00' 00" RT
 D = 0° 58' 58"
 R = 5830.00'
 T = 152.66'
 L = 305.26'
 E = 2.00'
 SE = N.C.

- | | | | | |
|--|---|---|---------------------------------|------------------|
| (D) 6" PCCP for Approaches on Dense Graded Subbase on Subgrade Treatment, Type II | (K) Full Depth Pavement | (R) 165#/SYS OC/QA-HMA, 2, 70, Surface, 9.5mm on Asphalt Milling, 1.5 in. | (DW) Detectable Warning Surface | (15) Curb |
| (D1) 4" PCCP for Approaches on Dense Graded Subbase on Geogrid, Type IB on Subgrade Treatment, Type II | (K1) Full Depth Pavement, PCCP for Approaches | (R1) 165#/SYS HMA For Approaches, Type D on Asphalt Milling, 1.5 in. | (R) Concrete Curb Ramp | (16) Center Curb |
| (E) Sidewalk, Concrete, 4" | | (S) Subgrade Treatment, Type XX | (D) Compacted Aggregate | (24) Sodding |
| | | (U) Underdrain | | |

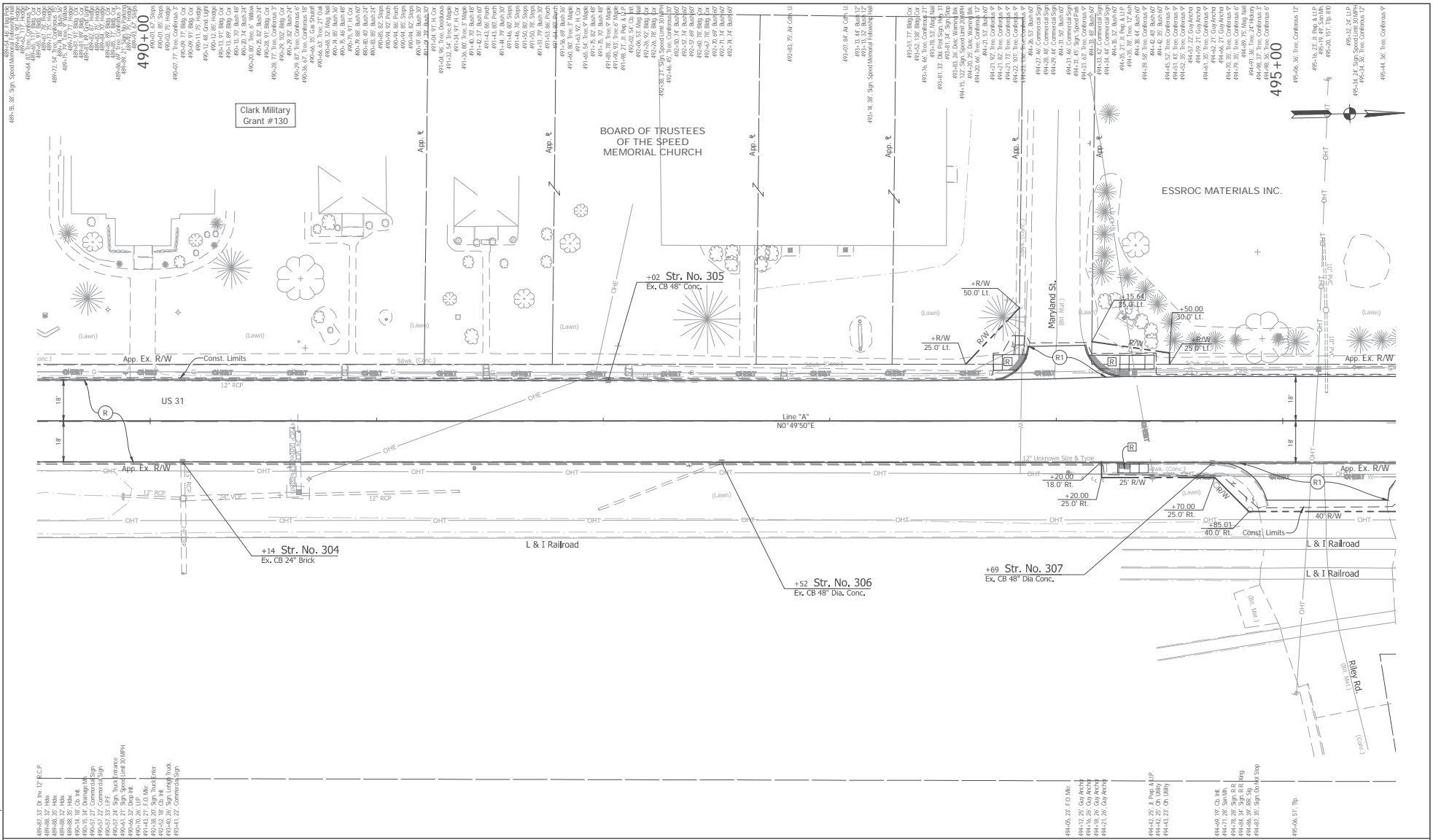
RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: BWS	DRAWN: MRW	
CHECKED: ACD	CHECKED: BWS	

INDIANA
DEPARTMENT OF TRANSPORTATION

PLAN SHEET
 STA. 483+50 TO STA. 489+50 "A"

HORIZONTAL SCALE 1"=20'	BRIDGE FILE N/A
VERTICAL SCALE	DESIGNATION 1700111
SURVEY BOOK	SHEETS 51 of 150
CONTRACT R-40412	PROJECT 1700111

DIRECTORY: S:\Project Files\2017\17-0199\CADD\Design\Sheet Sets
 FILE: 170199 Plan Sheets.dwg
 PLOTTED BY: mwatren
 DATE: Nov 15, 2019 - 1:00pm



- (D) 6" PCCP for Approaches on Dense Graded Subbase on Subgrade Treatment, Type II
- (D1) 4" PCCP for Approaches on Dense Graded Subbase on Geogrid, Type IB on Subgrade Treatment, Type II
- (E) Sidewalk, Concrete, 4"
- (K) Full Depth Pavement
- (K1) Full Depth Pavement, PCCP for Approaches
- (R) 165#/SYS OC/QA-HMA, 2, 70, Surface, 9.5mm on Asphalt Milling, 1.5 in.
- (R1) 165#/SYS HMA For Approaches, Type D on Asphalt Milling, 1.5 in.
- (S) Subgrade Treatment, Type XX
- (U) Underdrain
- (DW) Detectable Warning Surface
- (R) Concrete Curb Ramp
- (D) Compacted Aggregate
- (15) Curb
- (16) Center Curb
- (24) Sodding

- (490+00 to 490+05) 18" x 18" RCP
- (490+05 to 490+10) 18" x 18" RCP
- (490+10 to 490+15) 18" x 18" RCP
- (490+15 to 490+20) 18" x 18" RCP
- (490+20 to 490+25) 18" x 18" RCP
- (490+25 to 490+30) 18" x 18" RCP
- (490+30 to 490+35) 18" x 18" RCP
- (490+35 to 490+40) 18" x 18" RCP
- (490+40 to 490+45) 18" x 18" RCP
- (490+45 to 490+50) 18" x 18" RCP
- (490+50 to 490+55) 18" x 18" RCP
- (490+55 to 490+60) 18" x 18" RCP
- (490+60 to 490+65) 18" x 18" RCP
- (490+65 to 490+70) 18" x 18" RCP
- (490+70 to 490+75) 18" x 18" RCP
- (490+75 to 490+80) 18" x 18" RCP
- (490+80 to 490+85) 18" x 18" RCP
- (490+85 to 490+90) 18" x 18" RCP
- (490+90 to 490+95) 18" x 18" RCP
- (490+95 to 491+00) 18" x 18" RCP
- (491+00 to 491+05) 18" x 18" RCP
- (491+05 to 491+10) 18" x 18" RCP
- (491+10 to 491+15) 18" x 18" RCP
- (491+15 to 491+20) 18" x 18" RCP
- (491+20 to 491+25) 18" x 18" RCP
- (491+25 to 491+30) 18" x 18" RCP
- (491+30 to 491+35) 18" x 18" RCP
- (491+35 to 491+40) 18" x 18" RCP
- (491+40 to 491+45) 18" x 18" RCP
- (491+45 to 491+50) 18" x 18" RCP
- (491+50 to 491+55) 18" x 18" RCP
- (491+55 to 491+60) 18" x 18" RCP
- (491+60 to 491+65) 18" x 18" RCP
- (491+65 to 491+70) 18" x 18" RCP
- (491+70 to 491+75) 18" x 18" RCP
- (491+75 to 491+80) 18" x 18" RCP
- (491+80 to 491+85) 18" x 18" RCP
- (491+85 to 491+90) 18" x 18" RCP
- (491+90 to 491+95) 18" x 18" RCP
- (491+95 to 492+00) 18" x 18" RCP
- (492+00 to 492+05) 18" x 18" RCP
- (492+05 to 492+10) 18" x 18" RCP
- (492+10 to 492+15) 18" x 18" RCP
- (492+15 to 492+20) 18" x 18" RCP
- (492+20 to 492+25) 18" x 18" RCP
- (492+25 to 492+30) 18" x 18" RCP
- (492+30 to 492+35) 18" x 18" RCP
- (492+35 to 492+40) 18" x 18" RCP
- (492+40 to 492+45) 18" x 18" RCP
- (492+45 to 492+50) 18" x 18" RCP
- (492+50 to 492+55) 18" x 18" RCP
- (492+55 to 492+60) 18" x 18" RCP
- (492+60 to 492+65) 18" x 18" RCP
- (492+65 to 492+70) 18" x 18" RCP
- (492+70 to 492+75) 18" x 18" RCP
- (492+75 to 492+80) 18" x 18" RCP
- (492+80 to 492+85) 18" x 18" RCP
- (492+85 to 492+90) 18" x 18" RCP
- (492+90 to 492+95) 18" x 18" RCP
- (492+95 to 493+00) 18" x 18" RCP
- (493+00 to 493+05) 18" x 18" RCP
- (493+05 to 493+10) 18" x 18" RCP
- (493+10 to 493+15) 18" x 18" RCP
- (493+15 to 493+20) 18" x 18" RCP
- (493+20 to 493+25) 18" x 18" RCP
- (493+25 to 493+30) 18" x 18" RCP
- (493+30 to 493+35) 18" x 18" RCP
- (493+35 to 493+40) 18" x 18" RCP
- (493+40 to 493+45) 18" x 18" RCP
- (493+45 to 493+50) 18" x 18" RCP
- (493+50 to 493+55) 18" x 18" RCP
- (493+55 to 493+60) 18" x 18" RCP
- (493+60 to 493+65) 18" x 18" RCP
- (493+65 to 493+70) 18" x 18" RCP
- (493+70 to 493+75) 18" x 18" RCP
- (493+75 to 493+80) 18" x 18" RCP
- (493+80 to 493+85) 18" x 18" RCP
- (493+85 to 493+90) 18" x 18" RCP
- (493+90 to 493+95) 18" x 18" RCP
- (493+95 to 494+00) 18" x 18" RCP
- (494+00 to 494+05) 18" x 18" RCP
- (494+05 to 494+10) 18" x 18" RCP
- (494+10 to 494+15) 18" x 18" RCP
- (494+15 to 494+20) 18" x 18" RCP
- (494+20 to 494+25) 18" x 18" RCP
- (494+25 to 494+30) 18" x 18" RCP
- (494+30 to 494+35) 18" x 18" RCP
- (494+35 to 494+40) 18" x 18" RCP
- (494+40 to 494+45) 18" x 18" RCP
- (494+45 to 494+50) 18" x 18" RCP
- (494+50 to 494+55) 18" x 18" RCP
- (494+55 to 494+60) 18" x 18" RCP
- (494+60 to 494+65) 18" x 18" RCP
- (494+65 to 494+70) 18" x 18" RCP
- (494+70 to 494+75) 18" x 18" RCP
- (494+75 to 494+80) 18" x 18" RCP
- (494+80 to 494+85) 18" x 18" RCP
- (494+85 to 494+90) 18" x 18" RCP
- (494+90 to 494+95) 18" x 18" RCP
- (494+95 to 495+00) 18" x 18" RCP

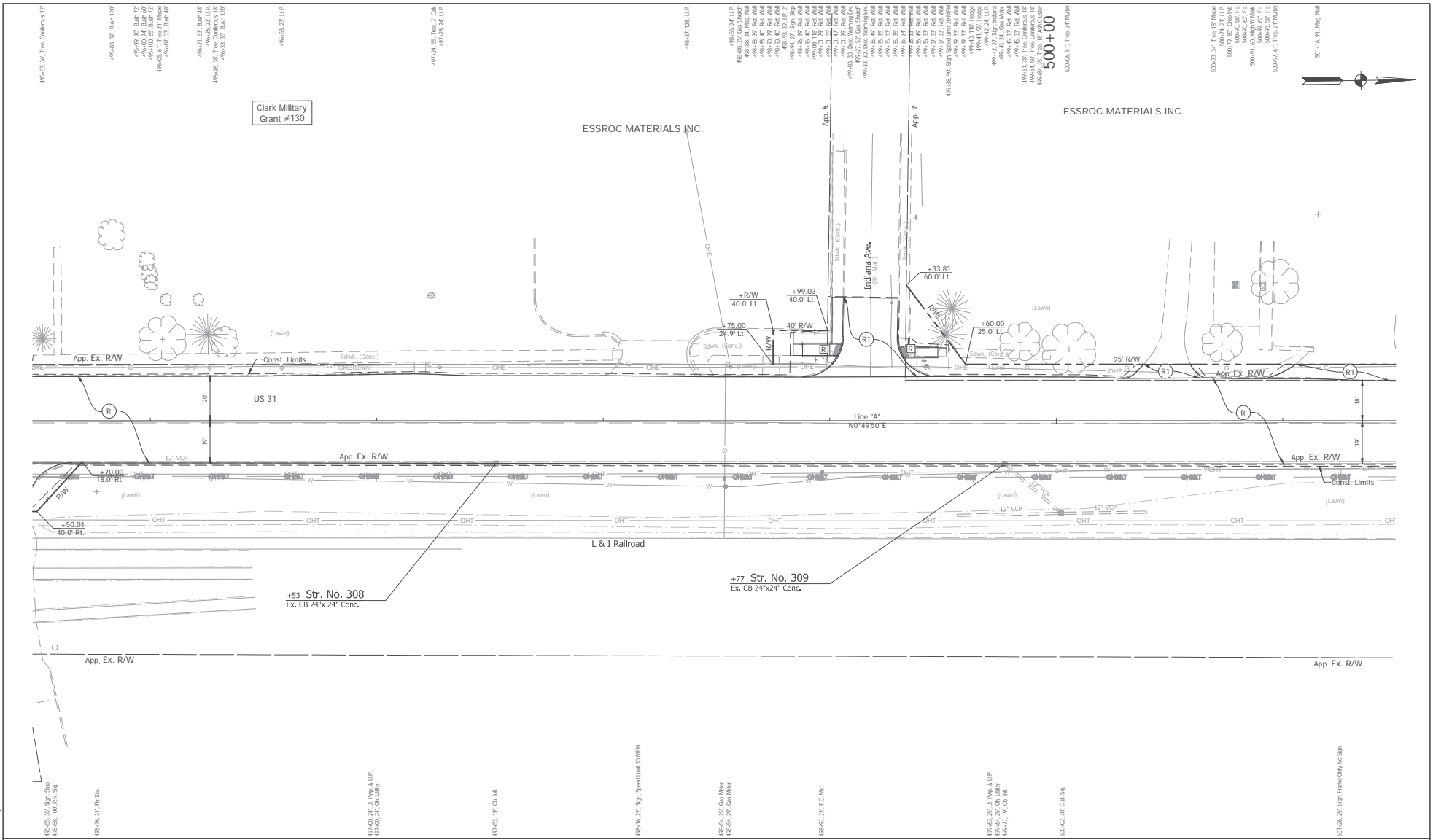
RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: BWS	DRAWN: MRW	
CHECKED: ACD	CHECKED: BWS	

INDIANA
 DEPARTMENT OF TRANSPORTATION

PLAN SHEET
 STA. 489+50 TO STA. 495+50 "A"

HORIZONTAL SCALE	BRIDGE FILE
1"=20'	N/A
VERTICAL SCALE	DESIGNATION
	1700111
SURVEY BOOK	SHEETS
	53 of 150
CONTRACT	PROJECT
R-49412	1700111

DIRECTORY: S:\Project Files\2017\17-0199\CADD\Design\Sheet Sets
 FILE: 170199 Plan Sheets.dwg
 DIMSCALE: 1/16"=1'
 PLOTTED BY: mwatren
 DATE: Nov 13, 2020 - 3:29pm



- (D) 6" PCCP for Approaches on Dense Graded Subbase on Subgrade Treatment, Type II
- (D1) 4" PCCP for Approaches on Dense Graded Subbase on Geogrid, Type IB on Subgrade Treatment, Type II
- (E) Sidewalk, Concrete, 4"
- (K) Full Depth Pavement
- (K1) Full Depth Pavement, PCCP for Approaches
- (R) 165#/SYS OC/QA-HMA, 2, 70, Surface, 9.5mm on Asphalt Milling, 1.5 in.
- (R1) 165#/SYS HMA For Approaches, Type D on Asphalt Milling, 1.5 in.
- (S) Subgrade Treatment, Type XX
- (U) Underdrain
- (DW) Detectable Warning Surface
- (R) Concrete Curb Ramp
- (D) Compacted Aggregate
- (15) Curb
- (16) Center Curb
- (24) Sodding

- 695-05, 35: Sign, Sign
- 695-08, 100: RR, Sign
- 695-16, 31: 19: Sta.
- 697-00, 21: 8: Post, & LIP
- 697-02, 61: 0: Utility
- 697-03, 19: 0: RR
- 698-16, 27: Sign, Speed Limit 30 MPH
- 698-54, 25: 0: Meter
- 698-54, 27: 0: Meter
- 698-67, 23: 1: 0: Meter
- 699-03, 25: 8: Post, & LIP
- 699-07, 19: 0: RR
- 500-02, 30: C.B. Sign
- 501-06, 25: Sign Frame, OHT No Sign

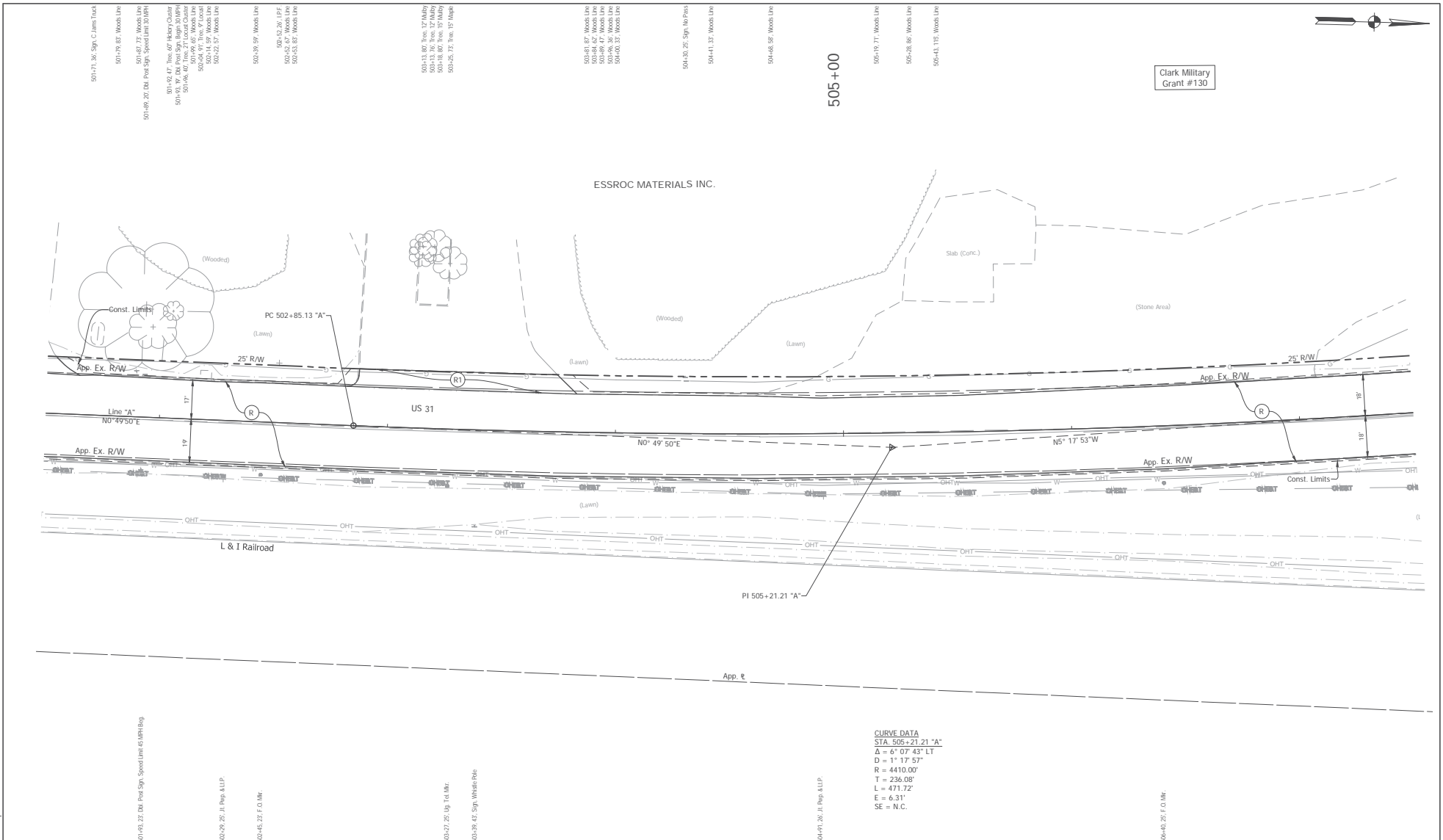
RECOMMENDED FOR APPROVAL _____ DESIGN ENGINEER	_____ DATE
DESIGNED: BWS	DRAWN: MRW
CHECKED: ACD	CHECKED: BWS

INDIANA
DEPARTMENT OF TRANSPORTATION

PLAN SHEET
STA. 495+50 TO STA. 501+50 "A"

HORIZONTAL SCALE 1"=20' VERTICAL SCALE 1"=10'	BRIDGE FILE N/A DESIGNATION 1700111
SURVEY BOOK 55 of 150	SHEETS PROJECT 1700111
CONTRACT R-40412	PROJECT 1700111

DIRECTORY: S:\Project Files\2017\17-0199\CADD\Design\Sheet Sets
 FILE: 170199 Plan Sheets.dwg
 DIMSCALE: 1/8"=1'-0"
 PLOTTED BY: mwatren
 DATE: Nov 13, 2020 - 3:34pm



(D)	6" PCCP for Approaches on Dense Graded Subbase on Subgrade Treatment, Type II	(K)	Full Depth Pavement	(R)	165#/SYS OC/QA-HMA, 2, 70, Surface, 9.5mm on Asphalt Milling, 1.5 in.
(D1)	4" PCCP for Approaches on Dense Graded Subbase on Geogrid, Type IB on Subgrade Treatment, Type II	(K1)	Full Depth Pavement, PCCP for Approaches	(R1)	165#/SYS HMA For Approaches, Type D on Asphalt Milling, 1.5 in.
(E)	Sidewalk, Concrete, 4"	(S)	Subgrade Treatment, Type XX	(U)	Underdrain

(DW)	Detectable Warning Surface	(15)	Curb
(R)	Concrete Curb Ramp	(16)	Center Curb
(D)	Compacted Aggregate	(24)	Sodding

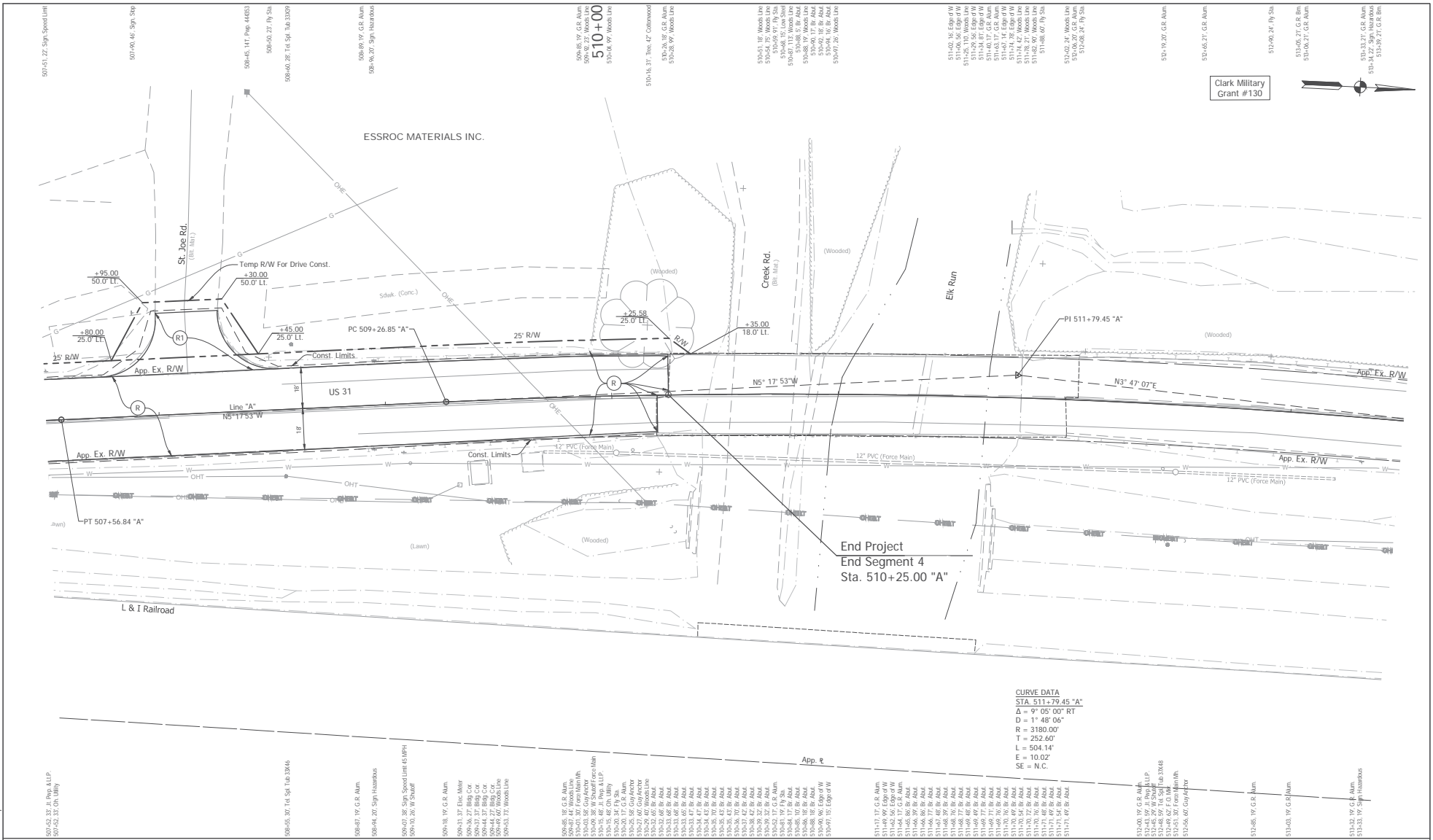
RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: BWS	DRAWN: MRW	
CHECKED: ACD	CHECKED: BWS	

INDIANA
DEPARTMENT OF TRANSPORTATION

PLAN SHEET
 STA. 501+50 TO STA. 507+50 "A"

HORIZONTAL SCALE	BRIDGE FILE
1"=20'	N/A
VERTICAL SCALE	DESIGNATION
	1700111
SURVEY BOOK	SHEETS
	57 of 150
CONTRACT	PROJECT
R-40412	1700111

DIRECTORY: S:\Project Files\2017\17-0199\CADD\Design\Sheet Sets
 FILE: 170199 Plan Sheets.dwg
 PLOTTED BY: mwatren
 DATE: Nov 15, 2020 - 10:10pm



(D) 6" PCCP for Approaches on Dense Graded Subbase on Subgrade Treatment, Type II	(K) Full Depth Pavement	(R) 165#/SYS OC/QA-HMA, 2, 70, Surface, 9.5mm on Asphalt Milling, 1.5 in.	(DW) Detectable Warning Surface
(D1) 4" PCCP for Approaches on Dense Graded Subbase on Geogrid, Type IB on Subgrade Treatment, Type II	(K1) Full Depth Pavement, PCCP for Approaches	(R1) 165#/SYS HMA For Approaches, Type D on Asphalt Milling, 1.5 in.	(R) Concrete Curb Ramp
(E) Sidewalk, Concrete, 4"	(S) Subgrade Treatment, Type XX	(U) Underdrain	(15) Curb
			(16) Center Curb
			(24) Sodding
			(D) Compacted Aggregate

507+52.33: 3' R. Pave. 4" LP	509+18.19: G.R. Alum.	509+48.17: 3" Elec. Meter	509+85.18: G.R. Alum.	511+17.17: G.R. Alum.
507+52.33: 3' Ch. Utility	509+21.37: Elec. Meter	509+36.27: 8" Gas Con.	509+87.44: Wooded Line	511+49.99: 10" Wood Line
	509+27.37: 8" Gas Con.	509+41.27: 8" Gas Con.	509+90.53: G.R. Alum.	511+49.99: 10" Wood Line
	509+41.27: 8" Gas Con.	509+49.40: Wooded Line	509+94.20: Sign Hardware	511+56.86: 8" Ansa.
	509+50.75: Wooded Line	509+55.75: Wooded Line	509+98.15: 48" Ch. Utility	511+66.86: 8" Ansa.
			510+00.24: P. Sign	511+66.86: 8" Ansa.
			510+02.24: P. Sign	511+66.86: 8" Ansa.
			510+04.24: P. Sign	511+66.86: 8" Ansa.
			510+06.24: P. Sign	511+66.86: 8" Ansa.
			510+08.24: P. Sign	511+66.86: 8" Ansa.
			510+10.24: P. Sign	511+66.86: 8" Ansa.
			510+12.24: P. Sign	511+66.86: 8" Ansa.
			510+14.24: P. Sign	511+66.86: 8" Ansa.
			510+16.24: P. Sign	511+66.86: 8" Ansa.
			510+18.24: P. Sign	511+66.86: 8" Ansa.
			510+20.24: P. Sign	511+66.86: 8" Ansa.
			510+22.24: P. Sign	511+66.86: 8" Ansa.
			510+24.24: P. Sign	511+66.86: 8" Ansa.
			510+26.24: P. Sign	511+66.86: 8" Ansa.
			510+28.24: P. Sign	511+66.86: 8" Ansa.
			510+30.24: P. Sign	511+66.86: 8" Ansa.
			510+32.24: P. Sign	511+66.86: 8" Ansa.
			510+34.24: P. Sign	511+66.86: 8" Ansa.
			510+36.24: P. Sign	511+66.86: 8" Ansa.
			510+38.24: P. Sign	511+66.86: 8" Ansa.
			510+40.24: P. Sign	511+66.86: 8" Ansa.
			510+42.24: P. Sign	511+66.86: 8" Ansa.
			510+44.24: P. Sign	511+66.86: 8" Ansa.
			510+46.24: P. Sign	511+66.86: 8" Ansa.
			510+48.24: P. Sign	511+66.86: 8" Ansa.
			510+50.24: P. Sign	511+66.86: 8" Ansa.
			510+52.24: P. Sign	511+66.86: 8" Ansa.
			510+54.24: P. Sign	511+66.86: 8" Ansa.
			510+56.24: P. Sign	511+66.86: 8" Ansa.
			510+58.24: P. Sign	511+66.86: 8" Ansa.
			510+60.24: P. Sign	511+66.86: 8" Ansa.
			510+62.24: P. Sign	511+66.86: 8" Ansa.
			510+64.24: P. Sign	511+66.86: 8" Ansa.
			510+66.24: P. Sign	511+66.86: 8" Ansa.
			510+68.24: P. Sign	511+66.86: 8" Ansa.
			510+70.24: P. Sign	511+66.86: 8" Ansa.
			510+72.24: P. Sign	511+66.86: 8" Ansa.
			510+74.24: P. Sign	511+66.86: 8" Ansa.
			510+76.24: P. Sign	511+66.86: 8" Ansa.
			510+78.24: P. Sign	511+66.86: 8" Ansa.
			510+80.24: P. Sign	511+66.86: 8" Ansa.
			510+82.24: P. Sign	511+66.86: 8" Ansa.
			510+84.24: P. Sign	511+66.86: 8" Ansa.
			510+86.24: P. Sign	511+66.86: 8" Ansa.
			510+88.24: P. Sign	511+66.86: 8" Ansa.
			510+90.24: P. Sign	511+66.86: 8" Ansa.
			510+92.24: P. Sign	511+66.86: 8" Ansa.
			510+94.24: P. Sign	511+66.86: 8" Ansa.
			510+96.24: P. Sign	511+66.86: 8" Ansa.
			510+98.24: P. Sign	511+66.86: 8" Ansa.
			511+00.24: P. Sign	511+66.86: 8" Ansa.
			511+02.24: P. Sign	511+66.86: 8" Ansa.
			511+04.24: P. Sign	511+66.86: 8" Ansa.
			511+06.24: P. Sign	511+66.86: 8" Ansa.
			511+08.24: P. Sign	511+66.86: 8" Ansa.
			511+10.24: P. Sign	511+66.86: 8" Ansa.
			511+12.24: P. Sign	511+66.86: 8" Ansa.
			511+14.24: P. Sign	511+66.86: 8" Ansa.
			511+16.24: P. Sign	511+66.86: 8" Ansa.
			511+18.24: P. Sign	511+66.86: 8" Ansa.
			511+20.24: P. Sign	511+66.86: 8" Ansa.
			511+22.24: P. Sign	511+66.86: 8" Ansa.
			511+24.24: P. Sign	511+66.86: 8" Ansa.
			511+26.24: P. Sign	511+66.86: 8" Ansa.
			511+28.24: P. Sign	511+66.86: 8" Ansa.
			511+30.24: P. Sign	511+66.86: 8" Ansa.
			511+32.24: P. Sign	511+66.86: 8" Ansa.
			511+34.24: P. Sign	511+66.86: 8" Ansa.
			511+36.24: P. Sign	511+66.86: 8" Ansa.
			511+38.24: P. Sign	511+66.86: 8" Ansa.
			511+40.24: P. Sign	511+66.86: 8" Ansa.
			511+42.24: P. Sign	511+66.86: 8" Ansa.
			511+44.24: P. Sign	511+66.86: 8" Ansa.
			511+46.24: P. Sign	511+66.86: 8" Ansa.
			511+48.24: P. Sign	511+66.86: 8" Ansa.
			511+50.24: P. Sign	511+66.86: 8" Ansa.

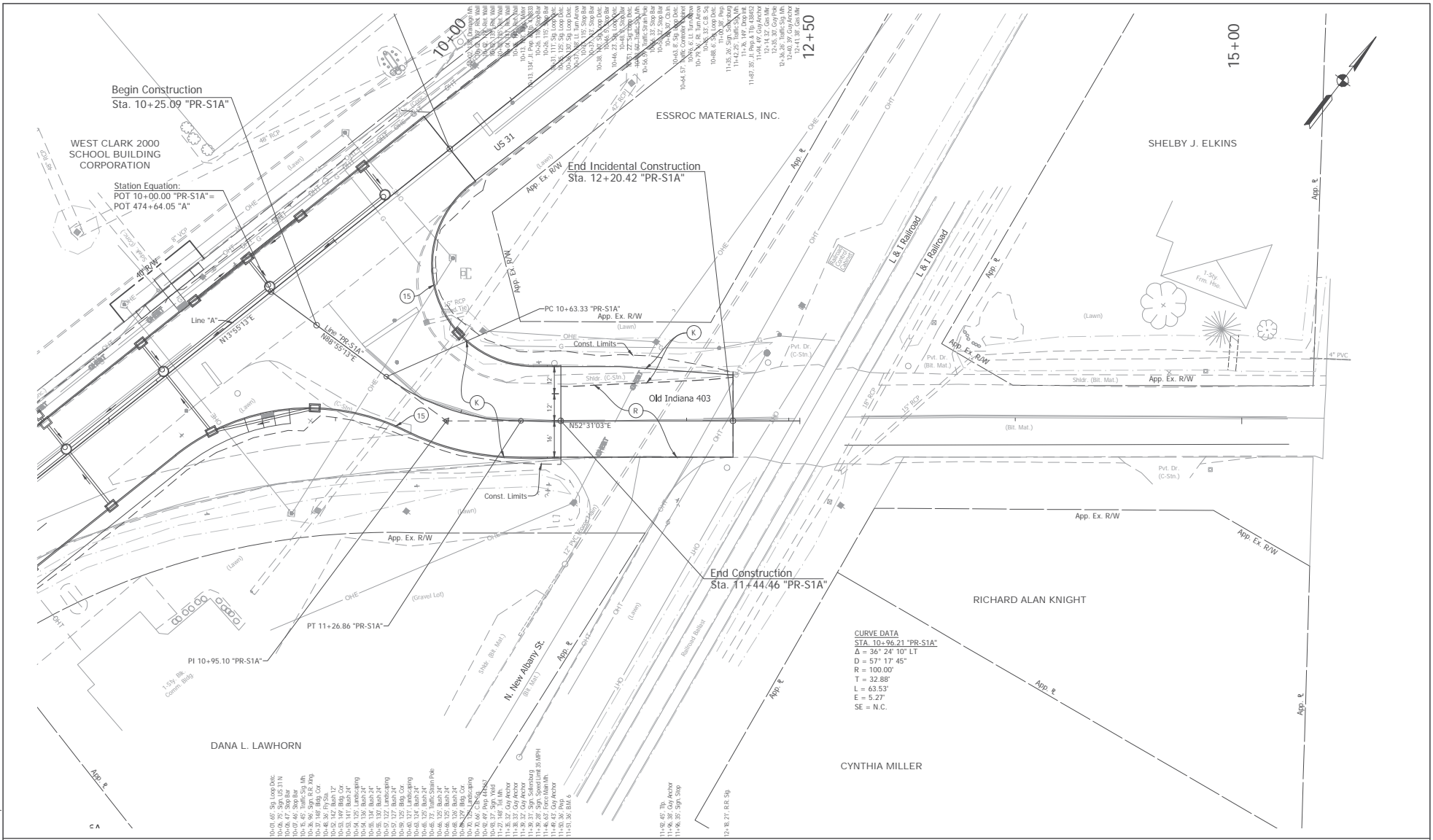
RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: BWS	DRAWN: MRW	
CHECKED: ACD	CHECKED: BWS	

INDIANA
DEPARTMENT OF TRANSPORTATION

PLAN SHEET
STA. 507+50 TO STA. 513+50 "A"

HORIZONTAL SCALE 1"=20'	BRIDGE FILE N/A
VERTICAL SCALE 1"=10.00'	DESIGNATION 1700111
SURVEY BOOK	SHEETS
CONTRACT R-40412	PROJECT 1700111

DIRECTORY: S:\Project Files\2017\17-0199\CADD\Design\Sheet Sets
 FILE: 170199 Plan Sheets.dwg
 PLOTTED BY: mwatren
 DATE: Nov 15, 2020 - 1:01pm



(D) 6" PCCP for Approaches on Dense Graded Subbase on Subgrade Treatment, Type II	(K) Full Depth Pavement	(R) 165#/SYS OC/QA-HMA, 2, 70, Surface, 9.5mm on Asphalt Milling, 1.5 in.	(DW) Detectable Warning Surface	(15) Curb
(D1) 4" PCCP for Approaches on Dense Graded Subbase on Geogrid, Type IB on Subgrade Treatment, Type II	(K1) Full Depth Pavement, PCCP for Approaches	(R1) 165#/SYS HMA For Approaches, Type D on Asphalt Milling, 1.5 in.	(R) Concrete Curb Ramp	(16) Center Curb
(E) Sidewalk, Concrete, 4"	(S) Subgrade Treatment, Type XX	(U) Underdrain	(D) Compacted Aggregate	(24) Sodding

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: BWS	DRAWN: MRW	
CHECKED: ACD	CHECKED: BWS	

INDIANA
DEPARTMENT OF TRANSPORTATION

PLAN SHEET
STA. 10+00 TO STA. 12+50 "PR-S1A"

HORIZONTAL SCALE 1"=20'	BRIDGE FILE N/A
VERTICAL SCALE	DESIGNATION 1700111
SURVEY BOOK	SHEETS 61 of 150
CONTRACT R-40412	PROJECT 1700111

Appendix C

Early Coordination



September 16, 2020

Sample Early Coordination Letter

Re: Des. No. 1700111, Small Town Pavement Replacement, US 31 between Foothill Road to 3.28 miles north of SR 60 (Silver Creek Bridge), Clark County, IN

Dear Interested Agency:

The Indiana Department of Transportation (INDOT) and Federal Highway Administration (FHWA) intends to proceed with a project involving the aforementioned segment of United States Highway (US) 31 in Clark County. This letter is part of the early coordination phase of the environmental review process. We are requesting comments from your area of expertise regarding any possible environmental effects associated with this project. Please use the above designation numbers and description in your reply. We will incorporate your comments into a study of the project's environmental impacts.

This project is located on US 31 between approximately 1.53 miles north of State Road (SR) 60 (Foothill Rd) to 3.28 miles north of SR 60 (Silver Creek Bridge) in Clark County, Indiana. It is within Silver Creek Township in Sections 110, 111, and 130, Township 1 South, Range 6 East on the U.S. Geological Survey (USGS) Speed, Indiana Quadrangle.

This section of US 31 is a two-lane, Urban Minor Collector, consisting of two (2) 12-ft. through lanes adjoined by a 6-ft. paved shoulder on the northbound lane and a 13.5 ft. paved shoulder on the southbound lane. After approximately 0.35 miles northward, US 31 merges with SR 311, and consists of three lanes: a 13 ft. shared left/right center turn lane bordered by a 10 ft. through lane and 2 ft. paved shoulder on either side. At Allhands Avenue, the center turn lane becomes a left turn lane serving at the four-way intersection of US 31 and W. Utica Street. North of the intersection, US 31 narrows back to two (2) 15 ft. through-lanes adjoined by 5 ft. paved shoulders. The project area is an urban area. Land use in the vicinity of the project is mixed use, with residential, commercial, and institutional.

The current proposed project would mill and repave US 31 between Foothill Road to approximately Triangle Drive. Between US 31/SR 311/Prather Lane, a full-depth pavement replacement with underdrain and incidental storm drainage improvements would be undertaken. This would also include replacing non-Americans with Disabilities Act (ADA)-compliant curb ramps, replacing a traffic signal, and minor pavement widening to adjust the lane configuration on the southwest-bound approach of US 31 to provide a shared through/right-turn lane.

From Bucheit Street to County Road (CR) 403 (Old SR 403), improvements would include a full depth pavement replacement with underdrains, installing new curb inlets and storm sewer, as well as replacing non-ADA compliant curb ramps. The traffic signals at Utica Street and CR 403 intersections would also be replaced. In addition, high visibility pavement markings and signage, a rapid rectangular flashing beacon (RRFB), or pedestrian hybrid beacon (HAWK)

would be added to the crosswalk at the US 31/St. Paul Street intersection and the mid-block crosswalks near the Sellersburg Library. The mid-block crosswalk near Silver Creek Elementary and Silver Creek Middle School entrances would be removed and relocated to US 31/CR 403 intersection with ADA-compliant pushbuttons and pedestrian countdown heads.

A 6-foot-wide concrete sidewalk will be added on the east side of US 31 between the mid-block crossing and the CR 403 (Old SR 403) intersection. A right turn lane, from southbound CR 403 (Old SR 403) to northbound US 31, will be added, extending from US 31 to the L&I Railroad tracks. The existing lane would become a left-turn lane. Additionally, the pavement markings from Utica Street to Silver Creek Elementary and Silver Creek Middle School entrances would be revised to provide a 12-ft.-wide two-way left-turn lane.

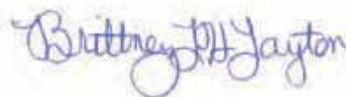
Between CR 403 (Old SR 403) to the Silver Creek Bridge, the proposed project would mill and repave US 31 while replacing non-ADA compliant curb ramps, replacing sidewalk segments disturbed by the project, adding/replacing inlet castings and adjusting to grade, and connecting to existing storm sewer.

The project would require the acquisition of approximately 3.0 acre of permanent right-of-way and 2.0 acres of temporary right-of-way. The project would be approximately 1.75 miles in length. The method of traffic (MOT) maintenance has three phases. The Phase 1 and Phase 3 MOTs, respectively, are the same and would allow the project to be constructed under traffic. The MOTs would utilize flagging, signage, and lane closures. The Phase 2 MOT would require a detour, likely utilizing SR 60, I-65, and Blue Lick Road. During the closures of Phase 2, access to local residences and businesses would be provided using a phased construction plan which maintains a single, one-way travel lane between local intersecting streets. Construction is proposed to occur in four Segments. During Construction Segment 1, Phase 1 MOT will be used. Throughout both Construction Segments 2 and 3, Phase 2 MOT will be implemented. Finally, while Construction Segment 4 is ongoing, Phase 3 MOT will be used. Construction is anticipated to begin in Summer of 2022.

The range-wide programmatic consultation for the Indiana Bat and Northern Long-eared Bat will be completed according to "Using the USFWS's Information for Planning and Consultation (IPaC) System for Listed Bat Consultation for INDOT Projects". Butler, Fairman, & Seufert, Inc. will perform waters and wetlands determinations and a biological assessment to identify any ecological resources that may be present. Butler, Fairman, & Seufert, Inc. will also investigate the areas of additional right-of-way for archaeological and historic resources for Section 106 compliance. The results of this investigation will be forwarded to the State Historic Preservation Officer (SHPO) for review and concurrence.

Should we not receive your response within thirty (30) calendar days from the date of this letter, it will be assumed that your agency feels that there will be no adverse effects incurred as a result of the proposed project. However, should you find that an extension to the response time is necessary, a reasonable amount may be granted upon request. If you have any questions regarding this matter, please feel free to contact Brittney Layton, Environmental Scientist at BLayton@bfsengr.com, or (317) 713-4616, or 8450 Westfield Blvd, Suite 300, Indianapolis, IN 46240. Alternatively, you may contact Terry Summers, INDOT Project Manager, at (812) 524-3749 or tsummers@indot.in.gov. Thank you in advance for your input.

On behalf of INDOT,
Butler, Fairman, & Seufert,



Brittney Layton, M.A.
Environmental Scientist
cc.

Enclosures:

Ecological Evaluation Form
State Map
Aerial Map
USGS Speed Quadrangle Map

Photo Key/Site Photographs
NWI Map
Soil Map with Legends
ETR List Clark County

See Appendix
B and
Appendix F

CC:

Erica Tait
Federal Highway Administration
Room 254, Federal Office Building
575 North Pennsylvania Street
Indianapolis, IN 46204

Bert Frost, Midwest Regional Director
National Park Service, Department of Interior
601 Riverfront Drive
Omaha, NE 68102

David Dye, INDOT Environmental Manager
INDOT Seymour District
185 Agrico Lane
Seymour, IN 47274

Jerry Raynor, State Conservationist
Natural Resources Conservation Service
6013 Lakeside Boulevard
Indianapolis, IN 46204

Christie Stanifer, Environmental Coordinator
Division of Water, Environmental Unit
Indiana Department of Natural Resources
402 West Washington Street, W-264
Indianapolis, IN 46204-2641

Rickie Clark, Hearings Manager
Mary Wright, Hearing Examiner
INDOT Office of Communications
100 North Senate Avenue, Room 642
Indianapolis, IN 46204

Paul Lehmann, Acting Regional Environmental Office
Field Environmental Officer
Department of Housing and Urban Development
Chicago Regional Office
Metcalf Federal Building
77 West Jackson Boulevard, Room 2401
Chicago, IL 60604

Julian Courtade, Chief Airport Inspector
INDOT Office of Aviation
Indiana Government Center, N-955
100 North Senate Avenue
Indianapolis, IN 46204-2891

Sellersburg Water Plant & Utilities
Bill Rigdon, Water Department
Lori Kearney, Sewer Division
103 S. New Albany Street
Sellersburg, IN 47172

Gregory McKay
U.S. Army Corps of Engineers
Louisville District
ATTN: CELRL-RDN
P.O. Box 59
Louisville, KY 40201-0059

Jarrett Haley, Executive Director
Kentuckiana Regional Planning & Development
Agency
11520 Commonwealth Drive
Louisville, Kentucky 40299

Town of Sellersburg and Clark County UABs
Bart Meyer, Sellersburg MS4 Coordinator
316 E. Utica Street
Sellersburg, IN 47172

Clark County Commissioners
24406 Tom Evans Road
Borden, IN 47106

Sellersburg Town Council
316 E. Utica Street
Sellersburg, IN 47172

Town of Sellersburg
316 E. Utica Street
Sellersburg, IN 47172

Sellersburg Parks and Recreation
316 E. Utica Street
Sellersburg, IN 47172

Francis Conroy, Vice President
Sellersburg Plan Commission
316 E. Utica Street
Sellersburg, IN 47172

Sellersburg Police Department
101 S. New Albany Street
Sellersburg, IN 47172

David Blankenbeker, P.L.S.
Clark County Surveyor
501 E. Court Avenue, Room #421
Jeffersonville, IN 47130

Stacia S. Franklin, Executive Director
Clark County Plan Commission
501 East Court Avenue, Room 416
Jeffersonville, IN 47130

Tim Cochran, Highway Superintendent Clark County
501 East Court Avenue, Room 404
Jeffersonville, IN 47130

Clark County Sheriff Department
501 E. Court Avenue, Room #421
Jeffersonville, IN 47130

Clark County Emergency Management
110 North Indiana Avenue
Sellersburg, IN 47172

Speed Memorial Church
328 US 31
Sellersburg, IN 47172

St. John Paul II Catholic Church
216 Schellers Avenue
Sellersburg, IN 47172

Grace Community Church and School
124 S. Indiana Avenue #131
Sellersburg, IN 47172

West-Clark School District
Transportation
Karie Kahafer, Director of Transportation
601 Renz Avenue
Sellersburg, IN 47172

Indiana Geological Survey
{<https://igs.indiana.edu/eAssessment/>}

Indiana Department of Environmental Management
Proposed Roadway Construction Projects Letter
{<http://www.in.gov/idem/5284.htm>}

IDEM Wellhead Proximity Determinator
Electronic Review of Location
{<http://www.in.gov/idem/cleanwater/pages/wellhead>}

From: [Courtade, Julian](#)
To: [Brittney Layton](#)
Subject: RE: Early Coordination Des. No. 1700111, US 31—Small Town Pavement Replacement, Clark County, IN
Date: Wednesday, September 16, 2020 2:05:46 PM
Attachments:

Brittney –

After reviewing the Early Coordination Letter, I have determined that if any object, obstruction, or equipment will exceed 95 ft. in height, further coordination will be required with our office. This is due to the close proximity of Clark Regional Airport and the need for any obstructions within 5 miles to meet a 100:1 glideslope to the nearest runway. Please let me know if you have any questions!

Best,

Julian L. Courtade

Chief Airport Inspector

100 North Senate Ave, N955

Indianapolis, IN 46204

Cell: (317) 954-7385

Email: jcourtade@indot.in.gov



From: [Karie Kahafer](#)
To: [Brittney Layton](#)
Subject: Re: Early Coordination Des. No. 1700111, US 31—Small Town Pavement Replacement, Clark County, IN
Date: Wednesday, September 16, 2020 2:21:55 PM

Thank you for this information, I guess my biggest question would this be completed by the start of the school year- since they say it is a summer project. I strongly feel this will negatively impact bus transportation significantly if done during the school year. All of items look ok to me-

Karie

On Wed, Sep 16, 2020 at 9:55 AM Brittney Layton <BLayton@bfsengr.com> wrote:

Good morning,

Butler, Fairman, & Seufert is conducting Early Coordination as part of the requirements for the environmental process for the proposed small town pavement replacement project on United States Highway 31 in Clark County, Indiana, on behalf of Shrewsberry & Associates.

We respectfully request your review of the attached Early Coordination Packet within 30 days. Feel free to reach out with any questions or concerns.

Thank you,

Brittney Layton, M.A.
Environmental Scientist

Butler, Fairman & Seufert, Inc.
8450 Westfield Blvd., Suite 300 | Indianapolis, IN 46240-8302 |
p 317-713-4615 | f 317-713-4616
BLayton@bfsengr.com | www.BFSEngr.com

Organization and Project Information

Project ID: 6302
Des. ID: 1700111
Project Title: US 31 between 1.53 miles north of State Road (SR) 60 to 3.28 miles north of SR 60
Name of Organization: Butler, Fairman, & Siefert
Requested by: Brittney Layton

Environmental Assessment Report

1. Geological Hazards:

- High liquefaction potential
- Floodway

2. Mineral Resources:

- Bedrock Resource: High Potential
- Sand and Gravel Resource: Low Potential

3. Active or abandoned mineral resources extraction sites:

- Petroleum Exploration Wells
- Active Industrial Minerals Sites (2016) ([Industrial Minerals](#))

*All map layers from Indiana Map (maps.indiana.edu)

DISCLAIMER:

This document was compiled by Indiana University, Indiana Geological Survey, using data believed to be accurate; however, a degree of error is inherent in all data. This product is distributed "AS-IS" without warranties of any kind, either expressed or implied, including but not limited to warranties of suitability to a particular purpose or use. No attempt has been made in either the design or production of these data and document to define the limits or jurisdiction of any federal, state, or local government. The data used to assemble this document are intended for use only at the published scale of the source data or smaller (see the metadata links below) and are for reference purposes only. They are not to be construed as a legal document or survey instrument. A detailed on-the-ground survey and historical analysis of a single site may differ from these data and this document.

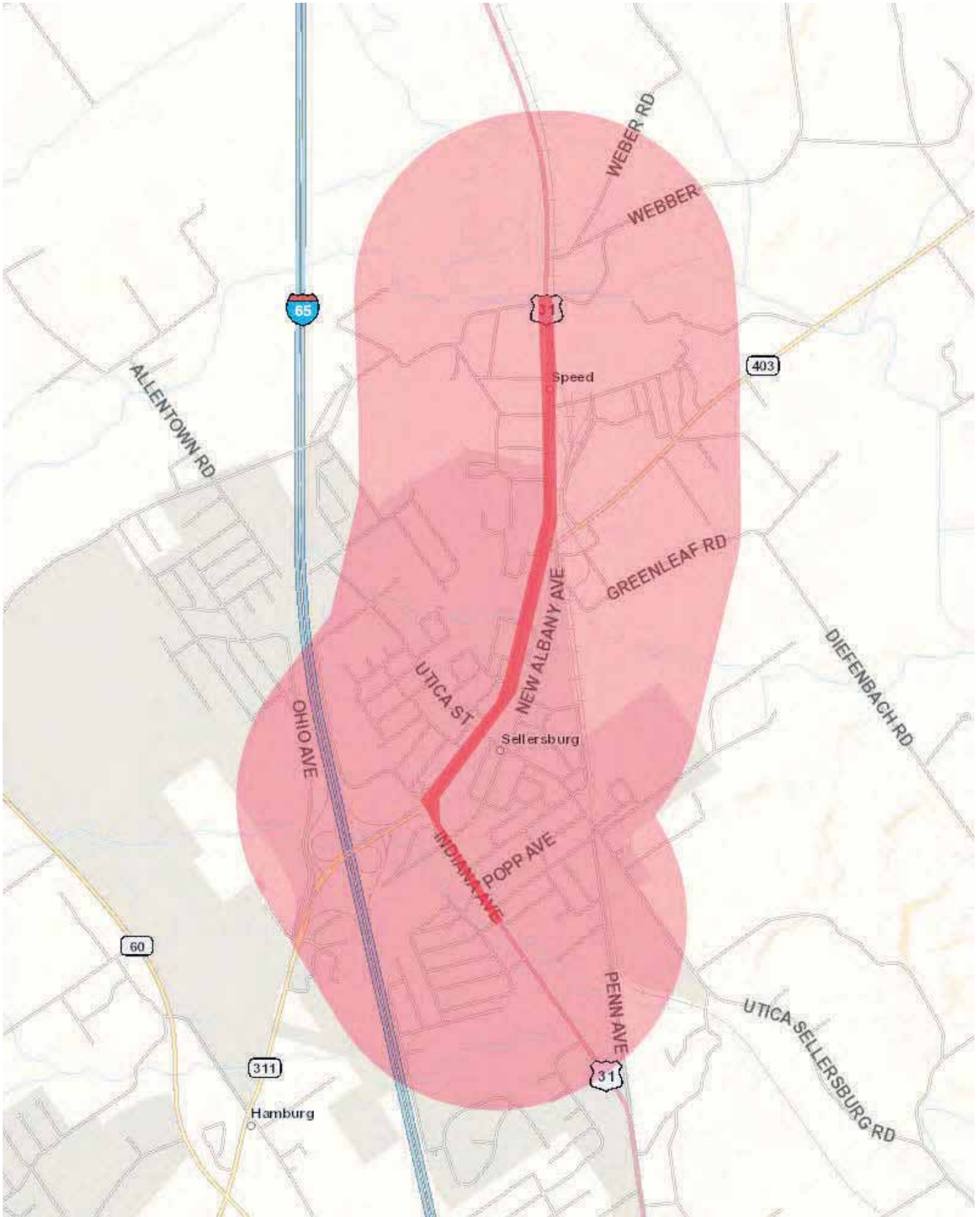
This information was furnished by Indiana Geological Survey

Address: 420 N. Walnut St., Bloomington, IN 47404

Email: IGSEnvir@indiana.edu

Phone: 812 855-7428

Date: September 16, 2020



Metadata:

- https://maps.indiana.edu/metadata/Geology/Petroleum_Wells.html
- https://maps.indiana.edu/metadata/Geology/Industrial_Minerals_Sites_2016.html
- https://maps.indiana.edu/metadata/Geology/Seismic_Earthquake_Liquefaction_Potential.html
- https://maps.indiana.edu/metadata/Geology/Industrial_Minerals_Sand_Gravel_Resources.html
- https://maps.indiana.edu/metadata/Hydrology/Floodplains_FIRM.html
- https://maps.indiana.edu/metadata/Geology/Bedrock_Geology.html



Indiana Department of Environmental Management

We Protect Hoosiers and Our Environment.

100 North Senate Avenue - Indianapolis, IN 46204
(800) 451-6027 - (317) 232-8603 - www.idem.IN.gov

INDOT Seymour District
Terry Summers, Highway Engineer Supervisor
185 Agrico Lane
Seymour , IN 47274

Butler, Fairman, & Siefert
Brittney Layton, Environmental Scientist
8450 Westfield Blvd
Suite 300
Indianapolis , IN 46240

Date

To Engineers and Consultants Proposing Roadway Construction Projects:

RE: Des. No. 1700111, Small Town Pavement Replacement, US 31 between approximately 1.53 miles north of State Road (SR) 60 (Foothill Rd) to 3.28 miles north of SR 60 (Silver Creek Bridge) in Clark County, Indiana
This letter from the Indiana Department of Environmental Management (IDEM) serves as a standardized response to enquiries inviting IDEM comments on roadway construction, reconstruction, or other improvement projects within existing roadway corridors when the proposed scope of the project is beneath the threshold requiring a formal National Environmental Policy Act-mandated Environmental Assessment or Environmental Impact Statement. As the letter attempts to address all roadway-related environmental topics of potential concern, it is possible that not every topic addressed in the letter will be applicable to your particular roadway project.

For additional information on specific roadway-related topics of interest, please visit the appropriate Web pages cited below, many of which provide contact information for persons within the various program areas who can answer questions not fully addressed in this letter. Also please be mindful that some environmental requirements may be subject to change and so each person intending to include a copy of this letter in their project documentation packet is advised to download the most recently revised version of the letter; found at: <http://www.in.gov/idem/5283.htm> (<http://www.in.gov/idem/5283.htm>).

To ensure that all environmentally-related issues are adequately addressed, IDEM recommends that you read this letter in its entirety, and consider each of the following issues as you move forward with the planning of your proposed roadway construction, reconstruction, or improvement project:

WATER AND BIOTIC QUALITY

1. Section 404 of the Clean Water Act requires that you obtain a permit from the U.S. Army Corps of Engineers (USACE) before discharging dredged or fill materials into any wetlands or other waters, such as rivers, lakes, streams, and ditches. Other activities regulated include the relocation, channelization, widening, or other such alteration of a stream, and the mechanical clearing (use of heavy construction equipment) of wetlands. Thus, as a project owner or sponsor, it is your responsibility to ensure that no wetlands are disturbed without the proper permit. Although you may initially refer to the U.S. Fish and Wildlife Service National Wetland Inventory maps as a means of identifying potential areas of concern, please be mindful that those maps do not depict jurisdictional wetlands regulated by the USACE or the Department of

Environmental Management. A valid jurisdictional wetlands determination can only be made by the USACE, using the 1987 Wetland Delineation Manual.

USACE recommends that you have a consultant check to determine whether your project will abut, or lie within, a wetland area. To view a list of consultants that have requested to be included on a list posted by the USACE on their Web site, see USACE Permits and Public Notices (<http://www.lrl.usace.army.mil/orf/default.asp>) (<http://www.lrl.usace.army.mil/orf/default.asp>) and then click on "Information" from the menu on the right-hand side of that page. Their "Consultant List" is the fourth entry down on the "Information" page. Please note that the USACE posts all consultants that request to appear on the list, and that inclusion of any particular consultant on the list does not represent an endorsement of that consultant by the USACE, or by IDEM.

Much of northern Indiana (Newton, Lake, Porter, LaPorte, St. Joseph, Elkhart, LaGrange, Steuben, and Dekalb counties; large portions of Jasper, Starke, Marshall, Noble, Allen, and Adams counties; and lesser portions of Benton, White, Pulaski, Kosciusko, and Wells counties) is served by the USACE District Office in Detroit (313-226-6812). The central and southern portions of the state (large portions of Benton, White, Pulaski, Kosciusko, and Wells counties; smaller portions of Jasper, Starke, Marshall, Noble, Allen, and Adams counties; and all other Indiana counties located in north-central, central, and southern Indiana) are served by the USACE Louisville District Office (502-315-6733).

Additional information on contacting these U.S. Army Corps of Engineers (USACE) District Offices, government agencies with jurisdiction over wetlands, and other water quality issues, can be found at <http://www.in.gov/idem/4396.htm> (<http://www.in.gov/idem/4396.htm>). IDEM recommends that impacts to wetlands and other water resources be avoided to the fullest extent.

2. In the event a Section 404 wetlands permit is required from the USACE, you also must obtain a Section 401 Water Quality Certification from the IDEM Office of Water Quality Wetlands Program. To learn more about the Wetlands Program, visit: <http://www.in.gov/idem/4384.htm> (<http://www.in.gov/idem/4384.htm>).
3. If the USACE determines that a wetland or other water body is isolated and not subject to Clean Water Act regulation, it is still regulated by the state of Indiana. A State Isolated Wetland permit from IDEM's Office of Water Quality (OWQ) is required for any activity that results in the discharge of dredged or fill materials into isolated wetlands. To learn more about isolated wetlands, contact the OWQ Wetlands Program at 317-233-8488.
4. If your project will involve over a 0.5 acre of wetland impact, stream relocation, or other large-scale alterations to water bodies such as the creation of a dam or a water diversion, you should seek additional input from the OWQ Wetlands Program staff. Consult the Web at: <http://www.in.gov/idem/4384.htm> (<http://www.in.gov/idem/4384.htm>) for the appropriate staff contact to further discuss your project.
5. Work within the one-hundred year floodway of a given water body is regulated by the Department of Natural Resources, Division of Water. The Division issues permits for activities regulated under the following statutes:
 - o IC 14-26-2 Lakes Preservation Act 312 IAC 11
 - o IC 14-26-5 Lowering of Ten Acre Lakes Act No related code
 - o IC 14-28-1 Flood Control Act 310 IAC 6-1
 - o IC 14-29-1 Navigable Waterways Act 312 IAC 6
 - o IC 14-29-3 Sand and Gravel Permits Act 312 IAC 6
 - o IC 14-29-4 Construction of Channels Act No related code

For information on these Indiana (statutory) Code and Indiana Administrative Code citations, see the DNR Web site at: <http://www.in.gov/dnr/water/9451.htm> (<http://www.in.gov/dnr/water/9451.htm>) . Contact the DNR Division of Water at 317-232-4160 for further information.

The physical disturbance of the stream and riparian vegetation, especially large trees overhanging any affected water bodies should be limited to only that which is absolutely necessary to complete the project. The shade provided by the large overhanging trees helps maintain proper stream temperatures and dissolved oxygen for aquatic life.

6. For projects involving construction activity (which includes clearing, grading, excavation and other land disturbing activities) that result in the disturbance of one (1), or more, acres of total land area, contact the Office of Water Quality – Watershed Planning Branch (317/233-1864) regarding the need for of a Rule 5 Storm Water Runoff Permit. Visit the following Web page
 - <http://www.in.gov/idem/4902.htm> (<http://www.in.gov/idem/4902.htm>)

To obtain, and operate under, a Rule 5 permit you will first need to develop a Construction Plan (<http://www.in.gov/idem/4917.htm#constreq> (<http://www.in.gov/idem/4917.htm#constreq>)), and as described in 327 IAC 15-5-6.5 (<http://www.in.gov/legislative/iac/T03270/A00150> [PDF] (<http://www.in.gov/legislative/iac/T03270/A00150.PDF>), pages 16 through 19). Before you may apply for a Rule 5 Permit, or begin construction, you must submit your Construction Plan to your county Soil and Water Conservation District (SWCD) (<http://www.in.gov/isda/soil/contacts/map.html> (<http://www.in.gov/isda/soil/contacts/map.html>)).

Upon receipt of the construction plan, personnel of the SWCD or the Indiana Department of Environmental Management will review the plan to determine if it meets the requirements of 327 IAC 15-5. Plans that are deemed deficient will require re-submittal. If the plan is sufficient you will be notified and instructed to submit the verification to IDEM as part of the Rule 5 Notice of Intent (NOI) submittal. Once construction begins, staff of the SWCD or Indiana Department of Environmental Management will perform inspections of activities at the site for compliance with the regulation.

Please be mindful that approximately 149 Municipal Separate Storm Sewer System (MS4) areas are now being established by various local governmental entities throughout the state as part of the implementation of Phase II federal storm water requirements. All of these MS4 areas will eventually take responsibility for Construction Plan review, inspection, and enforcement. As these MS4 areas obtain program approval from IDEM, they will be added to a list of MS4 areas posted on the IDEM Website at: <http://www.in.gov/idem/4900.htm> (<http://www.in.gov/idem/4900.htm>).

If your project is located in an IDEM-approved MS4 area, please contact the local MS4 program about meeting their storm water requirements. Once the MS4 approves the plan, the NOI can be submitted to IDEM.

Regardless of the size of your project, or which agency you work with to meet storm water requirements, IDEM recommends that appropriate structures and techniques be utilized both during the construction phase, and after completion of the project, to minimize the impacts associated with storm water runoff. The use of appropriate planning and site development and appropriate storm water quality measures are recommended to prevent soil from leaving the construction site during active land disturbance and for post construction water quality concerns. Information and assistance regarding storm water related to construction activities are available from the Soil and Water Conservation District (SWCD) offices in each county or from IDEM.

7. For projects involving impacts to fish and botanical resources, contact the Department of Natural Resources - Division of Fish and Wildlife (317/232-4080) for addition project input.
8. For projects involving water main construction, water main extensions, and new public water supplies, contact the Office of Water Quality - Drinking Water Branch (317-308-3299) regarding the need for permits.
9. For projects involving effluent discharges to waters of the State of Indiana , contact the Office of Water Quality - Permits Branch (317-233-0468) regarding the need for a National Pollutant Discharge Elimination System (NPDES) permit.
10. For projects involving the construction of wastewater facilities and sewer lines, contact the Office of Water Quality - Permits Branch (317-232-8675) regarding the need for permits.

AIR QUALITY

The above-noted project should be designed to minimize any impact on ambient air quality in, or near, the project area. The project must comply with all federal and state air pollution regulations. Consideration should be given to the following:

1. Regarding open burning, and disposing of organic debris generated by land clearing activities; some types of open burning are allowed (<http://www.in.gov/idem/4148.htm> (<http://www.in.gov/idem/4148.htm>)) under specific conditions. You also can seek an open burning variance from IDEM.

However, IDEM generally recommends that you take vegetative wastes to a registered yard waste composting facility or that the waste be chipped or shredded with composting on site (you must register with IDEM if more than 2,000 pounds is to be composted; contact 317/232-0066). The finished compost can then be used as a mulch or soil amendment. You also may bury any vegetative wastes (such as leaves, twigs, branches, limbs, tree trunks and stumps) onsite, although burying large quantities of such material can lead to subsidence problems, later on.

Reasonable precautions must be taken to minimize fugitive dust emissions from construction and demolition activities. For example, wetting the area with water, constructing wind barriers, or treating dusty areas with chemical stabilizers (such as calcium chloride or several other commercial products). Dirt tracked onto paved roads from unpaved areas should be minimized.

Additionally, if construction or demolition is conducted in a wooded area where blackbirds have roosted or abandoned buildings or building sections in which pigeons or bats have roosted for 3-5 years precautionary measures should be taken to avoid an outbreak of histoplasmosis. This disease is caused by the fungus *Histoplasma capsulatum*, which stems from bird or bat droppings that have accumulated in one area for 3-5 years. The spores from this fungus become airborne when the area is disturbed and can cause infections over an entire community downwind of the site. The area should be wetted down prior to cleanup or demolition of the project site. For more detailed information on histoplasmosis prevention and control, please contact the Acute Disease Control Division of the Indiana State Department of Health at (317) 233-7272.

2. The U.S. EPA and the Surgeon General recommend that people not have long-term exposure to radon at levels above 4 pCi/L. (For a county-by-county map of predicted radon levels in Indiana, visit: <http://www.in.gov/idem/4145.htm> (<http://www.in.gov/idem/4145.htm>).

The U.S. EPA further recommends that all homes (and apartments within three stories of ground level) be tested for radon. If in-home radon levels are determined to be 4 pCi/L, or higher, EPA recommends a follow-up test. If the second test confirms that radon levels are 4 pCi/L, or higher, EPA recommends the installation

of radon-reduction measures. (For a list of qualified radon testers and radon mitigation (or reduction) specialists visit: http://www.in.gov/isdh/regsvcs/radhealth/pdfs/radon_testers_mitigators_list.pdf (http://www.in.gov/isdh/regsvcs/radhealth/pdfs/radon_testers_mitigators_list.pdf.) It also is recommended that radon reduction measures be built into all new homes, particularly in areas like Indiana that have moderate to high predicted radon levels.

To learn more about radon, radon risks, and ways to reduce exposure visit:

<http://www.in.gov/isdh/regsvcs/radhealth/radon.htm> (<http://www.in.gov/isdh/regsvcs/radhealth/radon.htm>), <http://www.in.gov/idem/4145.htm> (<http://www.in.gov/idem/4145.htm>), or <http://www.epa.gov/radon/index.html> (<http://www.epa.gov/radon/index.html>).

3. With respect to asbestos removal: all facilities slated for renovation or demolition (except residential buildings that have (4) four or fewer dwelling units and which will not be used for commercial purposes) must be inspected by an Indiana-licensed asbestos inspector prior to the commencement of any renovation or demolition activities. If regulated asbestos-containing material (RACM) that may become airborne is found, any subsequent demolition, renovation, or asbestos removal activities must be performed in accordance with the proper notification and emission control requirements.

If no asbestos is found where a renovation activity will occur, or if the renovation involves removal of less than 260 linear feet of RACM off of pipes, less than 160 square feet of RACM off of other facility components, or less than 35 cubic feet of RACM off of all facility components, the owner or operator of the project does not need to notify IDEM before beginning the renovation activity.

For questions on asbestos demolition and renovation activities, you can also call IDEM's Lead/Asbestos section at 1-888-574-8150.

However, in all cases where a demolition activity will occur (even if no asbestos is found), the owner or operator must still notify IDEM 10 working days prior to the demolition, using the form found at <http://www.in.gov/icpr/webfile/formsdiv/44593.pdf> (<http://www.in.gov/icpr/webfile/formsdiv/44593.pdf>).

Anyone submitting a renovation/demolition notification form will be billed a notification fee based upon the amount of friable asbestos containing material to be removed or demolished. Projects that involve the removal of more than 2,600 linear feet of friable asbestos containing materials on pipes, or 1,600 square feet or 400 cubic feet of friable asbestos containing material on other facility components, will be billed a fee of \$150 per project; projects below these amounts will be billed a fee of \$50 per project. All notification remitters will be billed on a quarterly basis.

For more information about IDEM policy regarding asbestos removal and disposal, visit:

<http://www.in.gov/idem/4983.htm> (<http://www.in.gov/idem/4983.htm>).

4. With respect to lead-based paint removal: IDEM encourages all efforts to minimize human exposure to lead-based paint chips and dust. IDEM is particularly concerned that young children exposed to lead can suffer from learning disabilities. Although lead-based paint abatement efforts are not mandatory, any abatement that is conducted within housing built before January 1, 1978, or a child-occupied facility is required to comply with all lead-based paint work practice standards, licensing and notification requirements. For more information about lead-based paint removal visit: <http://www.in.gov/isdh/19131.htm> (<http://www.in.gov/isdh/19131.htm>).
5. Ensure that asphalt paving plants are permitted and operate properly. The use of cutback asphalt, or asphalt emulsion containing more than seven percent (7%) oil distillate, is prohibited during the months April through October. See 326 IAC 8-5-2, Asphalt Paving Rule

(<http://www.ai.org/legislative/iac/T03260/A00080.PDF>
(<http://www.ai.org/legislative/iac/T03260/A00080.PDF>)).

6. If your project involves the construction of a new source of air emissions or the modification of an existing source of air emissions or air pollution control equipment, it will need to be reviewed by the IDEM Office of Air Quality (OAQ). A registration or permit may be required under 326 IAC 2 (View at: www.ai.org/legislative/iac/t03260/a00020.pdf (<http://www.ai.org/legislative/iac/t03260/a00020.pdf>)). New sources that use or emit hazardous air pollutants may be subject to Section 112 of the Clean Air Act and corresponding state air regulations governing hazardous air pollutants.
7. For more information on air permits visit: <http://www.in.gov/idem/4223.htm> (<http://www.in.gov/idem/4223.htm>), or to initiate the IDEM air permitting process, please contact the Office of Air Quality Permit Reviewer of the Day at (317) 233-0178 or OAMPROD atdem.state.in.us.

LAND QUALITY

In order to maintain compliance with all applicable laws regarding contamination and/or proper waste disposal, IDEM recommends that:

1. If the site is found to contain any areas used to dispose of solid or hazardous waste, you need to contact the Office of Land Quality (OLQ) at 317-308-3103.
2. All solid wastes generated by the project, or removed from the project site, need to be taken to a properly permitted solid waste processing or disposal facility. For more information, visit <http://www.in.gov/idem/4998.htm> (<http://www.in.gov/idem/4998.htm>).
3. If any contaminated soils are discovered during this project, they may be subject to disposal as hazardous waste. Please contact the OLQ at 317-308-3103 to obtain information on proper disposal procedures.
4. If PCBs are found at this site, please contact the Industrial Waste Section of OLQ at 317-308-3103 for information regarding management of any PCB wastes from this site.
5. If there are any asbestos disposal issues related to this site, please contact the Industrial Waste Section of OLQ at 317-308-3103 for information regarding the management of asbestos wastes (Asbestos removal is addressed above, under Air Quality).
6. If the project involves the installation or removal of an underground storage tank, or involves contamination from an underground storage tank, you must contact the IDEM Underground Storage Tank program at 317/308-3039. See: <http://www.in.gov/idem/4999.htm> (<http://www.in.gov/idem/4999.htm>).

FINAL REMARKS

Should you need to obtain any environmental permits in association with this proposed project, please be mindful that IC 13-15-8 requires that you notify all adjoining property owners and/or occupants within ten days your submittal of each permit application. However, if you are seeking multiple permits, you can still meet the notification requirement with a single notice if all required permit applications are submitted with the same ten day period.

Should the scope of the proposed project be expanded to the extent that a National Environmental Policy Act Environmental Assessment (EA) or Environmental Impact Statement (EIS) is required, IDEM will actively participate in any early interagency coordination review of the project.

Meanwhile, please note that this letter does not constitute a permit, license, endorsement or any other form of approval on the part of the Indiana Department of Environmental Management regarding any project for which a copy of this letter is used. Also note that it is the responsibility of the project engineer or consultant using this letter to ensure that the most current draft of this document, which is located at <http://www.in.gov/idem/5284.htm> (<http://www.in.gov/idem/5284.htm>), is used.

Signature(s) of the Applicant

I acknowledge that the following proposed roadway project will be financed in part, or in whole, by public monies.

Project Description

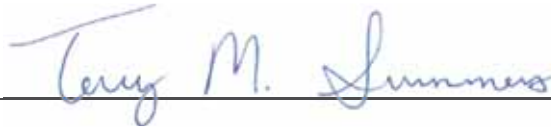
Des. No. 1700111, Small Town Pavement Replacement, US 31 between approximately 1.53 miles north of State Road (SR) 60 (Foothill Rd) to 3.28 miles north of SR 60 (Silver Creek Bridge) in Clark County, Indiana

With my signature, I do hereby affirm that I have read the letter from the Indiana Department of Environment that appears directly above. In addition, I understand that in order to complete that project in which I am interested, with a minimum of impact to the environment, I must consider all the issues addressed in the aforementioned letter, and further, that I must obtain any required permits.

Date: September 17, 2020

Signature of the INDOT

Project Engineer or Other Responsible Agent



Terry Summers, Highway Engineer Supervisor

Date: September 16, 2020

Signature of the

For Hire Consultant



Brittney Layton, Environmental Scientist

Brittney Layton

From: McWilliams, Robin <robin_mcwilliams@fws.gov>
Sent: Tuesday, September 22, 2020 10:45 AM
To: Brittney Layton
Subject: Re: [EXTERNAL] Early Coordination Des. No. 1700111, US 31—Small Town Pavement Replacement, Clark County, IN

Dear Brittney,

This responds to your recent letter requesting our comments on the aforementioned project.

These comments have been prepared under the authority of the Fish and Wildlife Coordination Act (16 U.S.C. 661 et. seq.) and are consistent with the intent of the National Environmental Policy Act of 1969, the Endangered Species Act of 1973, and the U. S. Fish and Wildlife Service's Mitigation Policy.

The project is within the range of the Indiana bat (*Myotis sodalis*) and northern long-eared bat (*Myotis septentrionalis*) and should follow the new Indiana bat/northern long-eared bat programmatic consultation process, if applicable (i.e. a federal transportation nexus is established). The Service has 14 days after a "Not Likely to Adversely Affect" determination letter is generated to review the project and provide additional comments or request additional information; if you do not receive a response from us within 14 days, we have no additional comments.

The project is also within the range of the gray bat (*Myotis grisescens*). Gray bats are year-round cave obligates, roosting in caves both during hibernation and summer maternity season; they may also occasionally use structures for roosting. Foraging habitat of gray bats is generally correlated with rivers, streams, lakes or reservoirs and associated shorelines and riparian areas. They use forested corridors and tree cover to travel between caves and foraging areas. There does not appear to be gray bat habitat impacts as a result of this project.

Based on a review of the information you provided, the U.S. Fish and Wildlife Service has no other comments on the project as currently proposed. However, should new information arise pertaining to project plans or a revised species list be published, it will be necessary for the Federal agency to reinitiate consultation. Standard recommendations are provided below.

We appreciate the opportunity to comment at this early stage of project planning. If you have any questions about our recommendations, please call (812) 334-4261 x. 207.

Sincerely,
Robin McWilliams Munson

Standard Recommendations:

1. Do not clear trees or understory vegetation outside the construction zone boundaries. **(This restriction is not related to the "tree clearing" restriction for potential Indiana Bat habitat.)**
2. Restrict below low-water work in streams to placement of culverts, piers, pilings and/or footings, shaping of the spill slopes around the bridge abutments, and placement of riprap.

Culverts should span the active stream channel, should be either embedded or a 3-sided or open-arch culvert, and be installed where practicable on an essentially flat slope. When an open-bottom culvert or arch is used in a stream, which has a good natural bottom substrate, such as gravel, cobbles and boulders, the existing substrate should be left undisturbed beneath the culvert to provide natural habitat for the aquatic community.

3. Restrict channel work and vegetation clearing to the minimum necessary for installation of the stream crossing structure.
4. Minimize the extent of hard armor (riprap) in bank stabilization by using bioengineering techniques whenever possible. If riprap is utilized for bank stabilization, extend it below low-water elevation to provide aquatic habitat.
5. Implement temporary erosion and sediment control methods within areas of disturbed soil. All disturbed soil areas upon project completion will be vegetated following INDOT's standard specifications.
6. Avoid all work within the inundated part of the stream channel (in perennial streams and larger intermittent streams) during the fish spawning season (April 1 through June 30), except for work within sealed structures such as caissons or cofferdams that were installed prior to the spawning season. No equipment shall be operated below Ordinary High Water Mark during this time unless the machinery is within the caissons or on the cofferdams.
7. Evaluate wildlife crossings under bridge/culverts projects in appropriate situations. Suitable crossings include flat areas below bridge abutments with suitable ground cover, high water shelves in culverts, amphibian tunnels and diversion fencing

Robin McWilliams Munson
Fish and Wildlife Biologist
U.S. Fish and Wildlife Service
620 South Walker Street
Bloomington, IN 46142
812-334-4261

Mon-Tues 8-3:30p
Wed-Thurs 8:30-3p Telework

September 23, 2020

Brittney Layton
Butler, Fairman & Seufert
8450 Westfield Boulevard, Suite 300
Indianapolis, Indiana 46240

Dear Ms. Layton:

The proposed project to replace the pavement along US 31 between Foothill Road to north of State Road 60 in Clark County, Indiana, (Des No 1700111) as referred to in your letter received September 16, 2020, will not cause a conversion of prime farmland.

If you need additional information, please contact John Allen at 317-295-5859.

Sincerely,

RICK NEILSON
State Soil Scientist



State of Indiana
DEPARTMENT OF NATURAL RESOURCES
Division of Fish and Wildlife
Early Coordination/Environmental Assessment

DNR #: ER-23070

Request Received: September 16, 2020

Requestor: Butler, Fairman & Seufert Inc
Brittney Layton
8450 Westfield Boulevard, Suite 300
Indianapolis, IN 46240

Project: US 31 roadway improvements between Foothill Road to 3.28 miles north of SR 60 (Silver Creek Bridge), Sellersburg; Des #1700111

County/Site info: Clark

The Indiana Department of Natural Resources has reviewed the above referenced project per your request. Our agency offers the following comments for your information and in accordance with the National Environmental Policy Act of 1969.

If our agency has regulatory jurisdiction over the project, the recommendations contained in this letter may become requirements of any permit issued. If we do not have permitting authority, all recommendations are voluntary.

Regulatory Assessment: This proposal may require the formal approval of our agency pursuant to the Flood Control Act (IC 14-28-1) for any proposal to construct, excavate, or fill in or on the floodway of Muddy Fork (at the north end of the project area), unless it qualifies under the INDOT and IDNR Memorandum of Understanding for Maintenance Activity Exemption, dated March 1997. Please include a copy of this letter with the permit application, if required.

Natural Heritage Database: The Natural Heritage Program's data have been checked. To date, no plant or animal species listed as state or federally threatened, endangered, or rare have been reported to occur in the project vicinity.

Fish & Wildlife Comments: We recommend a mitigation plan be developed (and submitted with the permit application, if required) for any unavoidable riparian or forest habitat impacts that will occur. The DNR's Habitat Mitigation guidelines (and plant lists) can be found online at: <http://iac.iga.in.gov/iac/20200527-IR-312200284NRA.xml.pdf>.

Impacts to non-wetland forest of one (1) acre or more should be mitigated at a minimum 2:1 ratio. If less than one acre of non-wetland forest is removed in a rural setting, replacement should be at a 1:1 ratio based on area. Impacts to non-wetland forest under one (1) acre in an urban setting should be mitigated by planting five trees, at least 2 inches in diameter-at-breast height (dbh), for each tree which is removed that is 10" dbh or greater (5:1 mitigation based on the number of large trees) or by using the 1:1 replacement ratio based on area depending on the type of habitat impacted (individual canopy tree removal in an urban streetscape or park-like environment versus removal of habitat supporting a tree canopy, woody understory, and herbaceous layer). Impacts under 0.10 acre in an urban area may still involve the replacement of large diameter trees but typically do not require any additional mitigation or additional plantings beyond seeding and stabilizing disturbed areas. There are exceptions for high quality habitat sites however.

The measures below should be implemented to avoid, minimize, or compensate for impacts to fish, wildlife, and botanical resources:

1. Revegetate all bare and disturbed areas with a mixture of grasses (excluding all varieties of tall fescue) and legumes as soon as possible upon completion; low endophyte tall fescue may be used in the ditch bottom and side slopes only.

THIS IS NOT A PERMIT

State of Indiana
DEPARTMENT OF NATURAL RESOURCES
Division of Fish and Wildlife
Early Coordination/Environmental Assessment

2. Minimize and contain within the project limits all tree and brush clearing.
3. Do not excavate in the waterway and minimize disturbance to bank vegetation and contain disturbance to within the project limits.
4. Do not cut any trees suitable for Indiana bat or Northern Long-eared bat roosting (greater than 5 inches dbh, living or dead, with loose hanging bark, or with cracks, crevices, or cavities) from April 1 through September 30.
5. Appropriately designed measures for controlling erosion and sediment must be implemented to prevent sediment from entering the stream or leaving the construction site; maintain these measures until construction is complete and all disturbed areas are stabilized.

Contact Staff:

Christie L. Stanifer, Environ. Coordinator, Fish & Wildlife
Our agency appreciates this opportunity to be of service. Please contact the above staff member at (317) 232-4080 if we can be of further assistance.



Date: October 16, 2020

Christie L. Stanifer
Environ. Coordinator
Division of Fish and Wildlife



United States Department of the Interior



FISH AND WILDLIFE SERVICE

Indiana Ecological Services Field Office

620 South Walker Street

Bloomington, IN 47403-2121

Phone: (812) 334-4261 Fax: (812) 334-4273

<http://www.fws.gov/midwest/Endangered/section7/s7process/step1.html>

In Reply Refer To:

September 22, 2020

Consultation Code: 03E12000-2020-SLI-2639

Event Code: 03E12000-2020-E-10727

Project Name: Des No. 1700111, US 31 between Foothill Road and Silver Creek Bridge, Clark County, IN

Subject: Updated list of threatened and endangered species that may occur in your proposed project location, and/or may be affected by your proposed project

To Whom It May Concern:

The attached species list identifies any federally threatened, endangered, proposed and candidate species that may occur within the boundary of your proposed project or may be affected by your proposed project. The list also includes designated critical habitat if present within your proposed project area or affected by your project. This list is provided to you as the initial step of the consultation process required under section 7(c) of the Endangered Species Act, also referred to as Section 7 Consultation.

Section 7 of the Endangered Species Act of 1973 requires that actions authorized, funded, or carried out by Federal agencies not jeopardize federally threatened or endangered species or adversely modify designated critical habitat. To fulfill this mandate, Federal agencies (or their designated non-federal representative) must consult with the Service if they determine their project “may affect” listed species or critical habitat.

Under 50 CFR 402.12(e) (the regulations that implement Section 7 of the Endangered Species Act) the accuracy of this species list should be verified after 90 days. This verification can be completed formally or informally. You may verify the list by visiting the ECOS-IPaC website <http://ecos.fws.gov/ipac/> at regular intervals during project planning and implementation and completing the same process you used to receive the attached list. As an alternative, you may contact this Ecological Services Field Office for updates.

Please use the species list provided and visit the U.S. Fish and Wildlife Service's Region 3 Section 7 Technical Assistance website at - <http://www.fws.gov/midwest/endangered/section7/s7process/index.html>. This website contains step-by-step instructions which will help you

determine if your project will have an adverse effect on listed species and will help lead you through the Section 7 process.

For all **wind energy projects** and **projects that include installing towers that use guy wires or are over 200 feet in height**, please contact this field office directly for assistance, even if no federally listed plants, animals or critical habitat are present within your proposed project or may be affected by your proposed project.

Although no longer protected under the Endangered Species Act, be aware that bald eagles are protected under the Bald and Golden Eagle Protection Act (16 U.S.C. 668 *et seq.*) and Migratory Bird Treaty Act (16 U.S.C. 703 *et seq.*), as are golden eagles. Projects affecting these species may require measures to avoid harming eagles or may require a permit. If your project is near an eagle nest or winter roost area, see our Eagle Permits website at <http://www.fws.gov/midwest/midwestbird/EaglePermits/index.html> to help you determine if you can avoid impacting eagles or if a permit may be necessary.

We appreciate your concern for threatened and endangered species. Please include the Consultation Tracking Number in the header of this letter with any request for consultation or correspondence about your project that you submit to our office.

Attachment(s):

- Official Species List

Official Species List

This list is provided pursuant to Section 7 of the Endangered Species Act, and fulfills the requirement for Federal agencies to "request of the Secretary of the Interior information whether any species which is listed or proposed to be listed may be present in the area of a proposed action".

This species list is provided by:

Indiana Ecological Services Field Office

620 South Walker Street

Bloomington, IN 47403-2121

(812) 334-4261

Project Summary

Consultation Code: 03E12000-2020-SLI-2639

Event Code: 03E12000-2020-E-10727

Project Name: Des No. 1700111, US 31 between Foothill Road and Silver Creek Bridge, Clark County, IN

Project Type: TRANSPORTATION

Project Description: INDOT Seymour District, with funding from Federal Highway Administration, intends to proceed with a Road Pavement Replacement project of US 31, between approximately 1.53 miles north of State Road (SR) 60 (Foothill Rd) to 3.28 miles north of SR 60 (Silver Creek Bridge) in Clark County, Indiana.

The current proposed project would mill and repave US 31 between Foothill Road to approximately Triangle Drive. Between US 31/SR 311/Prather Lane, a full-depth pavement replacement with underdrain and incidental storm drainage improvements would be undertaken. This would also include replacing non-ADA compliant curb ramps, replacing a traffic signal, and minor pavement widening to adjust the lane configuration on the southwest-bound approach of US 31 to provide a shared through/right-turn lane.

From Bucheit Street to County Road (CR) 403 (Old SR 403), improvements would include a full depth pavement replacement with underdrains, installing new curb inlets and storm sewer, as well as replacing non-ADA compliant curb ramps. The traffic signals at Utica Street and CR 403 intersections would also be replaced. In addition, high visibility pavement markings and signage, a rapid rectangular flashing beacon (RRFB), or pedestrian hybrid beacon (HAWK) would be added to the crosswalk at the US 31/St. Paul Street intersection and the mid-block crosswalks near the Sellersburg Library. The mid-block crosswalk near Silver Creek Elementary and Silver Creek Middle School entrances would be removed and relocated to US 31/CR 403 intersection with ADA-compliant pushbuttons and pedestrian countdown heads. The 6-ft.-wide concrete sidewalk on the east side of US 31 would be extended north to the CR 403 intersection in order to add a right turn lane from southbound DR 403 to northbound US 31 and extend from US 31 to the L & I Railroad tracks. The existing lane would become a left-turn lane. Additionally, the pavement markings from Utica Street to Silver Creek Elementary and Silver Creek Middle School entrances would be revised to provide a 12-ft.-wide two-way left-turn lane.

Between CR 403 (Old SR 403) to the Silver Creek Bridge, the proposed project would mill and repave US 31 while replacing non-ADA compliant curb ramps, replacing sidewalk segments disturbed by the project, adding/replacing inlet castings and adjusting to grade, and connecting to existing storm sewer.

It is anticipated that the project will require the acquisition of approximately 3 acres of permanent right-of-way acquisition and 2 acres of temporary right-of-way acquisition. The method of traffic (MOT) maintenance has three phases. Phases 1 and 3, respectively, would take place during Construction Segments 1 and 4, where the project would be constructed under traffic, utilizing flagging, signage, and lane closures. Phase 2 would occur during Construction Segments 2 & 3 would require a detour, likely utilizing State Road 60, I-65, and Blue Lick Road. Access to local residences and business would be provided using a phased construction plan, maintaining a single, one-way travel lane between local intersecting streets. No permanent lighting will be installed; however, existing lighting on poles may be relocated if in conflict with construction. Temporary lighting may be used. Suitable summer habitat is located in the project vicinity. Approximately 0.21 acre of trees is going to be removed. Trees to be removed may include eastern cottonwood (*Populus deltoides*), green ash (*Fraxinus pennsylvanica*), and/or sycamore (*Platanus occidentalis*). During Butler, Fairman, & Seufert's field investigation on July 2, 2018, no presence of endangered bats was identified. The letting date for this project is scheduled to be February 9, 2022 with construction anticipated to occur spring of 2022. A review of the USFWS database on September 9, 2020 did not indicate the presence of endangered bat species in or within 0.5 mile of the project area.

Project Location:

Approximate location of the project can be viewed in Google Maps: <https://www.google.com/maps/place/38.4036211609573N85.75365021143986W>



Counties: Clark, IN

Endangered Species Act Species

There is a total of 3 threatened, endangered, or candidate species on this species list.

Species on this list should be considered in an effects analysis for your project and could include species that exist in another geographic area. For example, certain fish may appear on the species list because a project could affect downstream species. Note that 1 of these species should be considered only under certain conditions.

IPaC does not display listed species or critical habitats under the sole jurisdiction of NOAA Fisheries¹, as USFWS does not have the authority to speak on behalf of NOAA and the Department of Commerce.

See the "Critical habitats" section below for those critical habitats that lie wholly or partially within your project area under this office's jurisdiction. Please contact the designated FWS office if you have questions.

-
1. [NOAA Fisheries](#), also known as the National Marine Fisheries Service (NMFS), is an office of the National Oceanic and Atmospheric Administration within the Department of Commerce.

Mammals

NAME	STATUS
Gray Bat <i>Myotis grisescens</i> No critical habitat has been designated for this species. Species profile: https://ecos.fws.gov/ecp/species/6329	Endangered
Indiana Bat <i>Myotis sodalis</i> There is final critical habitat for this species. Your location is outside the critical habitat. Species profile: https://ecos.fws.gov/ecp/species/5949 Species survey guidelines: https://ecos.fws.gov/ipac/guideline/survey/population/1/office/31440.pdf	Endangered
Northern Long-eared Bat <i>Myotis septentrionalis</i> No critical habitat has been designated for this species. This species only needs to be considered under the following conditions: <ul style="list-style-type: none"> Incidental take of the NLEB is not prohibited here. Federal agencies may consult using the 4(d) rule streamlined process. Transportation projects may consult using the programmatic process. See www.fws.gov/midwest/endangered/mammals/nleb/index.html Species profile: https://ecos.fws.gov/ecp/species/9045	Threatened

Critical habitats

THERE ARE NO CRITICAL HABITATS WITHIN YOUR PROJECT AREA UNDER THIS OFFICE'S JURISDICTION.



United States Department of the Interior



FISH AND WILDLIFE SERVICE

Indiana Ecological Services Field Office

620 South Walker Street

Bloomington, IN 47403-2121

Phone: (812) 334-4261 Fax: (812) 334-4273

<http://www.fws.gov/midwest/Endangered/section7/s7process/step1.html>

In Reply Refer To:

September 23, 2020

Consultation Code: 03E12000-2020-I-2639

Event Code: 03E12000-2020-E-10764

Project Name: Des No. 1700111, US 31 between Foothill Road and Silver Creek Bridge, Clark County, IN

Subject: Concurrence verification letter for the 'Des No. 1700111, US 31 between Foothill Road and Silver Creek Bridge, Clark County, IN' project under the revised February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion for Transportation Projects within the Range of the Indiana Bat and Northern Long-eared Bat.

To whom it may concern:

The U.S. Fish and Wildlife Service (Service) has received your request to verify that the **Des No. 1700111, US 31 between Foothill Road and Silver Creek Bridge, Clark County, IN** (Proposed Action) may rely on the concurrence provided in the February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion for Transportation Projects within the Range of the Indiana Bat and Northern Long-eared Bat (PBO) to satisfy requirements under Section 7(a)(2) of the Endangered Species Act of 1973 (ESA) (87 Stat. 884, as amended; 16 U.S.C 1531 *et seq.*).

Based on the information you provided (Project Description shown below), you have determined that the Proposed Action is within the scope and adheres to the criteria of the PBO, including the adoption of applicable avoidance and minimization measures, and may affect, but is not likely to adversely affect (NLAA) the endangered Indiana bat (*Myotis sodalis*) and/or the threatened Northern long-eared bat (*Myotis septentrionalis*).

The Service has 14 calendar days to notify the lead Federal action agency or designated non-federal representative if we determine that the Proposed Action does not meet the criteria for a NLAA determination under the PBO. If we do not notify the lead Federal action agency or designated non-federal representative within that timeframe, you may proceed with the Proposed Action under the terms of the NLAA concurrence provided in the PBO. This verification period allows Service Field Offices to apply local knowledge to implementation of the PBO, as we may identify a small subset of actions having impacts that were unanticipated. In such instances,

Service Field Offices may request additional information that is necessary to verify inclusion of the proposed action under the PBO.

For Proposed Actions that include bridge/structure removal, replacement, and/or maintenance activities: If your initial bridge/structure assessments failed to detect Indiana bats, but you later detect bats during construction, please submit the Post Assessment Discovery of Bats at Bridge/Structure Form (User Guide Appendix E) to this Service Office. In these instances, potential incidental take of Indiana bats may be exempted provided that the take is reported to the Service.

If the Proposed Action is modified, or new information reveals that it may affect the Indiana bat and/or Northern long-eared bat in a manner or to an extent not considered in the PBO, further review to conclude the requirements of ESA Section 7(a)(2) may be required. If the Proposed Action may affect any other federally-listed or proposed species, and/or any designated critical habitat, additional consultation between the lead Federal action agency and this Service Office is required. If the proposed action has the potential to take bald or golden eagles, additional coordination with the Service under the Bald and Golden Eagle Protection Act may also be required. In either of these circumstances, please contact this Service Office.

The following species may occur in your project area and **are not** covered by this determination:

- Gray Bat, *Myotis grisescens* (Endangered)

Project Description

The following project name and description was collected in IPaC as part of the endangered species review process.

Name

Des No. 1700111, US 31 between Foothill Road and Silver Creek Bridge, Clark County, IN

Description

INDOT Seymour District, with funding from Federal Highway Administration, intends to proceed with a Road Pavement Replacement project of US 31, between approximately 1.53 miles north of State Road (SR) 60 (Foothill Rd) to 3.28 miles north of SR 60 (Silver Creek Bridge) in Clark County, Indiana.

The current proposed project would mill and repave US 31 between Foothill Road to approximately Triangle Drive. Between US 31/SR 311/Prather Lane, a full-depth pavement replacement with underdrain and incidental storm drainage improvements would be undertaken. This would also include replacing non-ADA compliant curb ramps, replacing a traffic signal, and minor pavement widening to adjust the lane configuration on the southwest-bound approach of US 31 to provide a shared through/right-turn lane.

From Bucheit Street to County Road (CR) 403 (Old SR 403), improvements would include a full depth pavement replacement with underdrains, installing new curb inlets and storm sewer, as well as replacing non-ADA compliant curb ramps. The traffic signals at Utica Street and CR 403 intersections would also be replaced. In addition, high visibility pavement markings and signage, a rapid rectangular flashing beacon (RRFB), or pedestrian hybrid beacon (HAWK) would be added to the crosswalk at the US 31/St. Paul Street intersection and the mid-block crosswalks near the Sellersburg Library. The mid-block crosswalk near Silver Creek Elementary and Silver Creek Middle School entrances would be removed and relocated to US 31/CR 403 intersection with ADA-compliant pushbuttons and pedestrian countdown heads. The 6-ft.-wide concrete sidewalk on the east side of US 31 would be extended north to the CR 403 intersection in order to add a right turn lane from southbound CR 403 to northbound US 31 and extend from US 31 to the L & I Railroad tracks. The existing lane would become a left-turn lane. Additionally, the pavement markings from Utica Street to Silver Creek Elementary and Silver Creek Middle School entrances would be revised to provide a 12-ft.-wide two-way left-turn lane.

Between CR 403 (Old SR 403) to the Silver Creek Bridge, the proposed project would mill and repave US 31 while replacing non-ADA compliant curb ramps, replacing sidewalk segments disturbed by the project, adding/replacing inlet castings and adjusting to grade, and connecting to existing storm sewer.

It is anticipated that the project will require the acquisition of approximately 3 acres of

permanent right-of-way acquisition and 2 acres of temporary right-of-way acquisition. The method of traffic (MOT) maintenance has three phases. Phases 1 and 3, respectively, would take place during Construction Segments 1 and 4, where the project would be constructed under traffic, utilizing flagging, signage, and lane closures. Phase 2 would occur during Construction Segments 2 & 3 would require a detour, likely utilizing State Road 60, I-65, and Blue Lick Road. Access to local residences and business would be provided using a phased construction plan, maintaining a single, one-way travel lane between local intersecting streets. No permanent lighting will be installed; however, existing lighting on poles may be relocated if in conflict with construction. Temporary lighting may be used. Suitable summer habitat is located in the project vicinity. Approximately 0.21 acre of trees is going to be removed. Trees to be removed may include eastern cottonwood (*Populus deltoides*), green ash (*Fraxinus pennsylvanica*), and/or sycamore (*Platanus occidentalis*). During Butler, Fairman, & Seufert's field investigation on July 2, 2018, no presence of endangered bats was identified. The letting date for this project is scheduled to be February 9, 2022 with construction anticipated to occur spring of 2022. A review of the USFWS database on September 9, 2020 did not indicate the presence of endangered bat species in or within 0.5 mile of the project area.

Determination Key Result

Based on your answers provided, this project(s) may affect, but is not likely to adversely affect the endangered Indiana bat and/or the threatened Northern long-eared bat, therefore, consultation with the U.S. Fish and Wildlife Service pursuant to Section 7(a)(2) of the Endangered Species Act of 1973 (ESA) (87 Stat. 884, as amended 16 U.S.C. 1531 *et seq.*) is required. However, also based on your answers provided, this project may rely on the concurrence provided in the revised February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion for Transportation Projects within the Range of the Indiana Bat and Northern Long-eared Bat.

Qualification Interview

1. Is the project within the range of the Indiana bat^[1]?

[1] See [Indiana bat species profile](#)

Automatically answered

Yes

2. Is the project within the range of the Northern long-eared bat^[1]?

[1] See [Northern long-eared bat species profile](#)

Automatically answered

Yes

3. Which Federal Agency is the lead for the action?

A) Federal Highway Administration (FHWA)

4. Are *all* project activities limited to non-construction^[1] activities only? (examples of non-construction activities include: bridge/abandoned structure assessments, surveys, planning and technical studies, property inspections, and property sales)

[1] Construction refers to activities involving ground disturbance, percussive noise, and/or lighting.

No

5. Does the project include *any* activities that are **greater than** 300 feet from existing road/rail surfaces^[1]?

[1] Road surface is defined as the actively used [e.g. motorized vehicles] driving surface and shoulders [may be pavement, gravel, etc.] and rail surface is defined as the edge of the actively used rail ballast.

No

6. Does the project include *any* activities **within** 0.5 miles of a known Indiana bat and/or NLEB hibernaculum^[1]?

[1] For the purpose of this consultation, a hibernaculum is a site, most often a cave or mine, where bats hibernate during the winter (see suitable habitat), but could also include bridges and structures if bats are found to be hibernating there during the winter.

No

7. Is the project located **within** a karst area?

No

8. Is there *any* suitable^[1] summer habitat for Indiana Bat or NLEB **within** the project action area^[2]? (includes any trees suitable for maternity, roosting, foraging, or travelling habitat)

[1] See the Service's [summer survey guidance](#) for our current definitions of suitable habitat.

[2] The action area is defined as all areas to be affected directly or indirectly by the Federal action and not merely the immediate area involved in the action (50 CFR Section 402.02). Further clarification is provided by the [national consultation FAQs](#).

Yes

9. Will the project remove *any* suitable summer habitat^[1] and/or remove/trim any existing trees **within** suitable summer habitat?

[1] See the Service's [summer survey guidance](#) for our current definitions of suitable habitat.

Yes

10. Will the project clear more than 20 acres of suitable habitat per 5-mile section of road/rail?

No

11. Have presence/probable absence (P/A) summer surveys^{[1][2]} been conducted^{[3][4]} **within** the suitable habitat located within your project action area?

[1] See the Service's [summer survey guidance](#) for our current definitions of suitable habitat.

[2] Presence/probable absence summer surveys conducted within the fall swarming/spring emergence home range of a documented Indiana bat hibernaculum (contact local Service Field Office for appropriate distance from hibernacula) that result in a negative finding requires additional consultation with the local Service Field Office to determine if clearing of forested habitat is appropriate and/or if seasonal clearing restrictions are needed to avoid and minimize potential adverse effects on fall swarming and spring emerging Indiana bats.

[3] For projects within the range of either the Indiana bat or NLEB in which suitable habitat is present, and no bat surveys have been conducted, the transportation agency will assume presence of the appropriate species. This assumption of presence should be based upon the presence of suitable habitat and the capability of bats to occupy it because of their mobility.

[4] Negative presence/probable absence survey results obtained using the [summer survey guidance](#) are valid for a minimum of two years from the completion of the survey unless new information (e.g., other nearby surveys) suggest otherwise.

No

12. Does the project include activities **within documented Indiana bat habitat**^{[1][2]}?

[1] Documented roosting or foraging habitat – for the purposes of this consultation, we are considering documented habitat as that where Indiana bats and/or NLEB have actually been captured and tracked using (1) radio telemetry to roosts; (2) radio telemetry biangulation/triangulation to estimate foraging areas; or (3) foraging areas with repeated use documented using acoustics. Documented roosting habitat is also considered as suitable summer habitat within 0.25 miles of documented roosts.)

[2] For the purposes of this key, we are considering documented corridors as that where Indiana bats and/or NLEB have actually been captured and tracked to using (1) radio telemetry; or (2) treed corridors located directly between documented roosting and foraging habitat.

No

13. Will the removal or trimming of habitat or trees occur **within** suitable but **undocumented Indiana bat** roosting/foraging habitat or travel corridors?

Yes

14. What time of year will the removal or trimming of habitat or trees **within** suitable but **undocumented Indiana bat** roosting/foraging habitat or travel corridors occur^[1]?

[1] Coordinate with the local Service Field Office for appropriate dates.

B) During the inactive season

15. Does the project include activities **within documented NLEB habitat**^{[1][2]}?

[1] Documented roosting or foraging habitat – for the purposes of this consultation, we are considering documented habitat as that where Indiana bats and/or NLEB have actually been captured and tracked using (1) radio telemetry to roosts; (2) radio telemetry biangulation/triangulation to estimate foraging areas; or (3) foraging areas with repeated use documented using acoustics. Documented roosting habitat is also considered as suitable summer habitat within 0.25 miles of documented roosts.)

[2] For the purposes of this key, we are considering documented corridors as that where Indiana bats and/or NLEB have actually been captured and tracked to using (1) radio telemetry; or (2) treed corridors located directly between documented roosting and foraging habitat.

No

16. Will the removal or trimming of habitat or trees occur **within** suitable but **undocumented NLEB** roosting/foraging habitat or travel corridors?

Yes

17. What time of year will the removal or trimming of habitat or trees **within** suitable but **undocumented NLEB** roosting/foraging habitat or travel corridors occur?

B) During the inactive season

18. Will *any* tree trimming or removal occur **within** 100 feet of existing road/rail surfaces?

Yes

19. Will *any* tree trimming or removal occur **between** 100-300 feet of existing road/rail surfaces?

No

20. Are *all* trees that are being removed clearly demarcated?

Yes

21. Will the removal of habitat or the removal/trimming of trees include installing new or replacing existing **permanent** lighting?

Yes

22. Does the project include wetland or stream protection activities associated with compensatory wetland mitigation?

No

23. Does the project include slash pile burning?

No

24. Does the project include *any* bridge removal, replacement, and/or maintenance activities (e.g., any bridge repair, retrofit, maintenance, and/or rehabilitation work)?

No

25. Does the project include the removal, replacement, and/or maintenance of *any* structure other than a bridge? (e.g., rest areas, offices, sheds, outbuildings, barns, parking garages, etc.)

No

26. Will the project involve the use of **temporary** lighting *during* the active season?

No

27. Will the project install *any* new or replace any existing **permanent** lighting in addition to the lighting already indicated for habitat removal (including the removal or trimming of trees) or bridge/structure removal, replacement or maintenance activities?

No

28. Does the project include percussives or other activities (**not including tree removal/trimming or bridge/structure work**) that will increase noise levels above existing traffic/background levels?

No

29. Are *all* project activities that are **not associated with** habitat removal, tree removal/trimming, bridge and/or structure activities, temporary or permanent lighting, or use of percussives, limited to actions that DO NOT cause any additional stressors to the bat species?

Examples: lining roadways, unlighted signage, rail road crossing signals, signal lighting, and minor road repair such as asphalt fill of potholes, etc.

Yes

30. Will the project raise the road profile **above the tree canopy**?

No

31. Are the project activities that are not associated with habitat removal, tree removal/trimming, bridge and/or structure activities, temporary or permanent lighting, or use of percussives consistent with a No Effect determination in this key?

Automatically answered

Yes, other project activities are limited to actions that DO NOT cause any additional stressors to the bat species as described in the BA/BO

32. Is the habitat removal portion of this project consistent with a Not Likely to Adversely Affect determination in this key?

Automatically answered

Yes, because the tree removal/trimming that occurs outside of the Indiana bat's active season occurs greater than 0.5 miles from the nearest hibernaculum, is less than 100 feet from the existing road/rail surface, includes clear demarcation of the trees that are to be removed, and does not alter documented roosts and/or surrounding summer habitat within 0.25 miles of a documented roost.

33. Is the habitat removal portion of this project consistent with a Not Likely to Adversely Affect determination in this key?

Automatically answered

Yes, because the tree removal/trimming that occurs outside of the NLEB's active season occurs greater than 0.5 miles from the nearest hibernaculum, is less than 100 feet from the existing road/rail surface, includes clear demarcation of the trees that are to be removed, and does not alter documented roosts and/or surrounding summer habitat within 0.25 miles of a documented roost.

34. **General AMM 1**

Will the project ensure *all* operators, employees, and contractors working in areas of known or presumed bat habitat are aware of *all* FHWA/FRA/FTA (Transportation Agencies) environmental commitments, including all applicable Avoidance and Minimization Measures?

Yes

35. Tree Removal AMM 1

Can *all* phases/aspects of the project (e.g., temporary work areas, alignments) be modified, to the extent practicable, to avoid tree removal^[1] in excess of what is required to implement the project safely?

Note: Tree Removal AMM 1 is a minimization measure, the full implementation of which may not always be practicable. Projects may still be NLAA as long as Tree Removal AMMs 2, 3, and 4 are implemented and LAA as long as Tree Removal AMMs 3, 5, 6, and 7 are implemented.

[1] The word “trees” as used in the AMMs refers to trees that are suitable habitat for each species within their range. See the USFWS’ current summer survey guidance for our latest definitions of suitable habitat.

Yes

36. Tree Removal AMM 3

Can tree removal be limited to that specified in project plans and ensure that contractors understand clearing limits and how they are marked in the field (e.g., install bright colored flagging/fencing prior to any tree clearing to ensure contractors stay within clearing limits)?

Yes

37. Tree Removal AMM 4

Can the project avoid cutting down/removal of *all* (1) **documented**^[1] Indiana bat or NLEB roosts^[2] (that are still suitable for roosting), (2) trees **within** 0.25 miles of roosts, and (3) documented foraging habitat any time of year?

[1] The word documented means habitat where bats have actually been captured and/or tracked.

[2] Documented roosting or foraging habitat – for the purposes of this consultation, we are considering documented habitat as that where Indiana bats and/or NLEB have actually been captured and tracked using (1) radio telemetry to roosts; (2) radio telemetry biangulation/triangulation to estimate foraging areas; or (3) foraging areas with repeated use documented using acoustics. Documented roosting habitat is also considered as suitable summer habitat within 0.25 miles of documented roosts.)

Yes

38. Lighting AMM 2

Does the lead agency use the BUG (Backlight, Uplight, and Glare) system developed by the Illuminating Engineering Society^{[1][2]} to rate the amount of light emitted in unwanted directions?

[1] Refer to [Fundamentals of Lighting - BUG Ratings](#)

[2] Refer to [The BUG System—A New Way To Control Stray Light](#)

Yes

39. Lighting AMM 2

Will the **permanent** lighting used during removal of suitable habitat and/or the removal/trimming of trees within suitable habitat be designed to be as close to 0 for all three BUG ratings as possible, with a priority of "uplight" of 0 and "backlight" as low as practicable?

Yes

Project Questionnaire

1. Have you made a No Effect determination for *all* other species indicated on the FWS IPaC generated species list?

Yes

2. Have you made a May Affect determination for *any* other species on the FWS IPaC generated species list?

No

3. How many acres^[1] of trees are proposed for removal between 0-100 feet of the existing road/rail surface?

[1] If described as number of trees, multiply by 0.09 to convert to acreage and enter that number.

0.21

Avoidance And Minimization Measures (AMMs)

This determination key result includes the committment to implement the following Avoidance and Minimization Measures (AMMs):

GENERAL AMM 1

Ensure all operators, employees, and contractors working in areas of known or presumed bat habitat are aware of all FHWA/FRA/FTA (Transportation Agencies) environmental commitments, including all applicable AMMs.

LIGHTING AMM 2

When installing new or replacing existing permanent lights, use downward-facing, full cut-off lens lights (with same intensity or less for replacement lighting); or for those transportation agencies using the BUG system developed by the Illuminating Engineering Society, be as close to 0 for all three ratings with a priority of "uplight" of 0 and "backlight" as low as practicable.

TREE REMOVAL AMM 1

Modify all phases/aspects of the project (e.g., temporary work areas, alignments) to avoid tree removal.

TREE REMOVAL AMM 2

Apply time of year restrictions for tree removal when bats are not likely to be present, or limit tree removal to 10 or fewer trees per project at any time of year within 100 feet of existing road/rail surface and **outside of documented** roosting/foraging habitat or travel corridors; visual emergence survey must be conducted with no bats observed.

TREE REMOVAL AMM 3

Ensure tree removal is limited to that specified in project plans and ensure that contractors understand clearing limits and how they are marked in the field (e.g., install bright colored flagging/fencing prior to any tree clearing to ensure contractors stay within clearing limits).

TREE REMOVAL AMM 4

Do not remove **documented** Indiana bat or NLEB roosts that are still suitable for roosting, or trees within 0.25 miles of roosts, or **documented** foraging habitat any time of year.

Determination Key Description: FHWA, FRA, FTA Programmatic Consultation For Transportation Projects Affecting NLEB Or Indiana Bat

This key was last updated in IPaC on December 02, 2019. Keys are subject to periodic revision.

This decision key is intended for projects/activities funded or authorized by the Federal Highway Administration (FHWA), Federal Railroad Administration (FRA), and/or Federal Transit Administration (FTA), which may require consultation with the U.S. Fish and Wildlife Service (Service) under Section 7 of the Endangered Species Act (ESA) for the endangered **Indiana bat** (*Myotis sodalis*) and the threatened **Northern long-eared bat** (NLEB) (*Myotis septentrionalis*).

This decision key should only be used to verify project applicability with the Service's [February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion for Transportation Projects](#). The programmatic biological opinion covers limited transportation activities that may affect either bat species, and addresses situations that are both likely and not likely to adversely affect either bat species. This decision key will assist in identifying the effect of a specific project/activity and applicability of the programmatic consultation. The programmatic biological opinion is not intended to cover all types of transportation actions. Activities outside the scope of the programmatic biological opinion, or that may affect ESA-listed species other than the Indiana bat or NLEB, or any designated critical habitat, may require additional ESA Section 7 consultation.

Appendix D

Section 106 of the National Historic Preservation Act (NHPA)

**FEDERAL HIGHWAY ADMINISTRATION'S
SECTION 4(F) COMPLIANCE REQUIREMENTS (for historic properties) AND
SECTION 106 FINDINGS AND DETERMINATIONS
AREA OF POTENTIAL EFFECT
ELIGIBILITY DETERMINATIONS
EFFECT FINDING
US 31 SMALL TOWN PAVEMENT REPLACEMENT
DES. NO.: 1700111**

**AREA OF POTENTIAL EFFECTS
(Pursuant to 36 CFR Section 800.4(a)(1))**

The project is located along US 31 between Foothill Road and the Silver Creek Bridge in the Town of Sellersburg and the unincorporated community of Speed in Silver Creek Township, Clark County, Indiana. The Area of Potential Effects (APE) consists largely of a corridor surrounding US 31, expanding in less-dense areas (Appendix B, B4).

**ELIGIBILITY DETERMINATIONS
(Pursuant to 36 CFR 800.4(c)(2))**

The following properties are eligible for listing in the National Register of Historic Places (NRHP):

Dr. Q. Robert Hauss House/IHSSI #019-604-51035 (227 N. New Albany Street): c 1885, Italianate; eligible under Criterion C for significance in Architecture

IHSSI #019-604-51008 (479 N. Indiana Avenue): c. 1930, English cottage; eligible under Criterion C for significance in Architecture

EFFECT FINDING

Dr. Q. Robert Hauss House/ IHSSI #019-604-51035: "No Adverse Effect"

IHSSI #019-604-51008 (479 N. Indiana Avenue): "No Adverse Effect"

The Indiana Department of Transportation (INDOT), acting on the Federal Highway Administration's (FHWA) behalf, has determined a "No Adverse Effect" finding is appropriate for this undertaking. INDOT respectfully requests the Indiana State Historic Preservation Officer provide written concurrence with the Section 106 determination of effect.

SECTION 4(F) COMPLIANCE REQUIREMENTS (for historic properties)

Dr. Q. Robert Hauss House/ IHSSI #019-604-51035 - This undertaking will temporarily occupy land from the Dr. Q. Robert Hauss House/IHSSI #019-604-51035, a Section 4(f) historic property. INDOT, acting on FHWA's behalf, has determined the appropriate Section 106 finding is "No Adverse Effect". FHWA believes that the temporary occupancy will not constitute a Section 4(f) use because all of the conditions listed in 23 CFR 774.13(d) are satisfied:

1. Duration must be temporary, i.e., less than the time needed for construction of the project, and there should be no change in ownership of the land;
2. Scope of the work must be minor, i.e., both the nature and the magnitude of the changes to the Section 4(f) property are minimal;
3. There are no anticipated permanent adverse physical impacts, nor will there be interference with the protected activities, features, or attributes of the property, on either a temporary or permanent basis;
4. The land being used must be fully restored, i.e., the property must be returned to a condition which is at least as good as that which existed prior to the project; and
5. There must be documented agreement of the official(s) with jurisdiction over the Section 4(f) resource regarding the above conditions.

The fulfillment of conditions 1-4 are detailed in Section 4 of the attached documentation, "Describe the Undertaking's Effects on Historic Properties." With regard to condition 5, FHWA respectfully requests the Indiana State Historic Preservation Officer provide written concurrence that they are in agreement that the above criteria have been met and that the impacts to the Dr. Q. Robert Hauss House/IHSSI #019-604-51035, constitute a temporary occupancy.

IHSSI #019-604-51008 (479 N. Indiana Avenue) - This undertaking will temporarily occupy land from IHSSI #019-604-51008 (479 N. Indiana Avenue), a Section 4(f) historic property. INDOT, acting on FHWA's behalf, has determined the appropriate Section 106 finding is "No Adverse Effect". FHWA believes that the temporary occupancy will not constitute a Section 4(f) use because all of the conditions listed in 23 CFR 774.13(d) are satisfied:

1. Duration must be temporary, i.e., less than the time needed for construction of the project, and there should be no change in ownership of the land;
2. Scope of the work must be minor, i.e., both the nature and the magnitude of the changes to the Section 4(f) property are minimal;
3. There are no anticipated permanent adverse physical impacts, nor will there be interference with the protected activities, features, or attributes of the property, on either a temporary or permanent basis;
4. The land being used must be fully restored, i.e., the property must be returned to a condition which is at least as good as that which existed prior to the project; and
5. There must be documented agreement of the official(s) with jurisdiction over the Section 4(f) resource regarding the above conditions.

The fulfillment of conditions 1-4 are detailed in Section 4 of the attached documentation, "Describe the Undertaking's Effects on Historic Properties." With regard to condition 5, FHWA respectfully requests the Indiana State Historic Preservation Officer provide written concurrence that they are in agreement that the above criteria have been met and that the impacts to IHSSI #019-604-51008 (479 N. Indiana Avenue), constitute a temporary occupancy.

Anuradha V. Kumar

Anuradha V. Kumar, for FHWA
Manager
INDOT Cultural Resources

11/12/2020

Approved Date

**FEDERAL HIGHWAY ADMINISTRATION
DOCUMENTATION OF SECTION 106 FINDING OF
NO ADVERSE EFFECT
SUBMITTED TO THE STATE HISTORIC PRESERVATION OFFICER
PURSUANT TO 36 CFR Section 800.5(c)
US 31 SMALL TOWN PAVEMENT REPLACEMENT, 1.53 miles N. of SR 60 (Foothill Rd)
to 3.28 MILES N. of SR 60
DES. NO.: 1700111**

1. DESCRIPTION OF THE UNDERTAKING

The Indiana Department of Transportation (INDOT)- Seymour District, with funding from the Federal Highway Administration (FHWA) proposes to proceed with improvements to US 31 in the Town of Sellersburg and the unincorporated community of Speed in Clark County Indiana. Section 106 of the National Historic Preservation Act requires Federal agencies to take into account the effects of their undertakings on historic properties. The federal involvement in the project is funding received from the FHWA.

The proposed undertaking is on US 31 between Foothill Road and the Silver Creek Bridge in Clark County, Indiana. The proposed project is approximately 1.75 miles long. It is within Silver Creek Township, in Sections 110, 111, and 130, Township 1 South, Range 6 East on the USGS Speed, Indiana Quadrangle (Appendix B, B2). The total project length is approximately 1.75 miles.

The need for the project is derived from the deteriorated conditions along US 31. Portions of the corridor exhibit significant cracking in the asphalt or concrete surface. Existing curbs are buried and deteriorating. Drainage is insufficient to handle stormwater. The pedestrian facilities are not Americans with Disabilities Act (ADA)-compliant, and in some stretches are nonexistent. According to INDOT data, in the period between 2010 and 2018 there were 463 crashes within the project area, or an average of 56 per year. About 63% were rear-end crashes, which are associated with signalized intersections, congested traffic conditions, and driver inattention. This problem is particularly apparent at the intersection of US 31 and CR 403 (Old SR 403). Finally, the West Clark Community Schools Corp. has expressed safety concerns about the mid-block pedestrian crossing near the entrance to Silver Creek Elementary and Silver Creek Middle Schools.

The purpose of this project is to extend the functional life of the existing pavement, improve drainage, provide ADA-compliant pedestrian facilities, and improve safety along US 31 between Foothill Road and the Silver Creek Bridge by 2022.

The project proposes the following:

Segment 1- From Foothill Road to approximately Triangle Drive:

- Mill and repave US 31.

Segment 2- US 31/SR 311/Prather Lane Intersection:

- Full-depth pavement replacement with underdrains;
- Replace non-ADA-compliant curb ramps;
- Replace traffic signal;

- Minor pavement widening to adjust the lane configuration on the southwest-bound approach of US 31 to provide a shared through/right-turn lane;
- Incidental storm drainage improvements.

Segment 3- Approximately Bucheit Street to CR 403 (Old SR 403):

- Full-depth pavement replacement with underdrains;
- Install new curb inlets and storm sewer;
- Replace non-ADA-compliant curb ramps;
- Replace traffic signal at Utica Street intersection;
- Add high-visibility pavement markings and signage and a rapid rectangular flashing beacon (RRFB) or pedestrian hybrid beacon (HAWK) to the crosswalk at the US 31/St. Paul Street intersection;
- Improve visibility of mid-block crosswalk near Sellersburg Library with high-visibility pavement markings and signage, RRFB, or HAWK;
- Remove mid-block crosswalk near Silver Creek Elementary and Silver Creek Middle School entrances and relocate to US 31/CR 403 (Old SR 403) intersection with ADA-compliant pushbuttons and pedestrian countdown heads. A 6-foot-wide concrete sidewalk will be added on the east side of US 31 between the mid-block crossing and the CR 403 (Old SR 403) intersection. A right turn lane from southbound CR 403 (Old SR 403) to northbound US 31, extending from US 31 to the L&I Railroad tracks. The existing lane will become a left-turn lane;
- Replace traffic signal at CR 403 (Old SR 403) intersection;
- Revise pavement markings to provide 12-foot wide two-way left-turn lane (TWLTL) from Utica Street to Silver Creek Elementary and Silver Creek Middle School entrances.

Segment 4- Approximately CR 403 (Old SR 403) to the Silver Creek Bridge:

- Mill and repave US 31;
- Replace non-compliant curb ramps;
- Replace segments of sidewalk disturbed by project;
- Replace inlet castings and adjust to grade;
- Add curb inlets and connect to existing storm sewer.

Approximately 1.78 acres of permanent right-of-way (ROW) acquisition for road construction, traffic signal modification, and curb and sidewalk replacement is expected. Approximately 0.83 acre of temporary ROW acquisition is anticipated. Segments 1 and 4 will be constructed under traffic. Segments 2 and 3 will require a detour, likely utilizing State Road 60, I-65, and Blue Lick Road. During closure of Segments 2 and 3, access to local residences and businesses would be provided using a phased construction plan which maintains a single, one-way travel lane between local intersecting streets.

The Area of Potential Effects (APE) includes the existing and proposed ROW, immediately adjacent properties, and those areas where a visual differentiation may occur between an existing structure and the project area. The APE consists largely of a corridor surrounding US 31, expanding in less-dense areas (Appendix B, B4).

2. EFFORTS TO IDENTIFY HISTORIC PROPERTIES

The National Register of Historic Places (NRHP) and the Indiana Register of Historic Sites and Structures (State Register) were consulted by Butler, Fairman, & Seufert, Inc. (BF&S). No listed properties are located within the APE.

The 1988 *Clark County Interim Report* and the 2010 resurvey of Clark County available Indiana State Historic Architectural and Archaeological Research Database (SHAARD)/Indiana Buildings, Bridges, and Cemeteries Map (IBBCM) were checked by BF&S on July 17, 2018. Sixteen (16) previously surveyed resources were located within the APE. BF&S conducted a site visit on December 21, 2017. Information from the site visit and research regarding historic resources were compiled into a Historic Property Report (HPR; BF&S, February 24, 2020, Appendix C, C1-C3). The HPR recommended the following three properties eligible for the NRHP:

1. the Dr. Q. Robert Hauss House/IHSSI #019-604-51035 (227 N. New Albany Street);
2. IHSSI #019-604-51008 (479 N. Indiana Avenue);
3. IHSSI #019-604-52011 (Bridge carrying Southern Indiana Railway over Creek Drive and Muddy Fork).

The Indiana State Historic Preservation Officer (SHPO) is considered an automatic consulting party, and an early coordination letter was sent to the SHPO on November 26, 2019. In addition, the following individuals and organizations were sent an early coordination letter via email on November 26, 2019 (Appendix E, E1-E7):

Indiana Landmarks Southern Regional Office
Clark County Historian
Jeff-Clark Preservation, Inc.
Clark's Grant Historical Society
Kentuckiana Regional Planning & Development Agency
Sellersburg Clerk-Treasurer
Sellersburg Town Council
Sellersburg Streets & Sanitation Department
Clark County Commissioners
Clark County Highway Superintendent
Eastern Shawnee Tribe of Oklahoma
Miami Tribe of Oklahoma
Peoria Tribe of Indians of Oklahoma
Pokagon Band of Potawatomi Indians
United Keetoowah Band of Cherokee Indians
Delaware Tribe of Indians

The Miami Tribe of Oklahoma responded on December 18, 2019, indicating they wished to be a consulting party. The letter stated they had no objections to the proposed project, but noted the project area is within the aboriginal homelands of the Miami Tribe (Appendix E, E10).

The SHPO responded on December 13, 2019 stating they were not aware of any other parties who should be invited to consulting party consultation (DHPA #24727; Appendix E, E8-E9).

No other responses to the November 26, 2019 early coordination letter were received.

The HPR was approved by the INDOT-Cultural Resources Office (CRO) on February 24, 2020. The HPR was distributed to SHPO and consulting parties on February 25, 2020 (Appendix E, E11-E14).

The SHPO responded on March 31, 2020 stating in part, “...we agree with the conclusions in the HPR regarding the National Register of Historic Places (“NRHP”) eligibility of the house at 479 N. Indiana Avenue (Indiana Historic Sites and Structures Inventory [“IHSSI”] no. 019-604-51008) and 227 N. Albany Street (IHSSI no. 019-604-51035). We believe both meet Criterion C for their architecture. Regarding the Southern Indiana Railroad Bridge over Creek Drive and Muddy Fork (IHSSI no. 019-604-52011), we respectfully disagree with the conclusions in the HPR. [...] It does not appear to be a significant example of bridge design nor does it serve a locally significant role in transportation.” The SHPO also concluded no other properties within the APE were eligible for the National Register (Appendix E, E15-E16). Therefore, IHSSI #019-604-52011 was not considered eligible for the National Register for the purposes of Section 106 review.

No other responses to the HPR were received.

In regard to archaeology, a Phase Ia archaeological reconnaissance was conducted by 106 Consulting, LLC on December 21, 2017. The archaeologist did not locate any archaeological resources within the project area. No further work was recommended in the resulting archaeological short report (ASR; Appendix D, D1-D3). INDOT-CRO approved the ASR on February 4, 2020, and the ASR was sent to consulting parties on February 25, 2020 (Appendix E, E11-E14).

The SHPO concurred with the ASR on March 31, 2020, stating in part, “in terms of archaeological resources, based on the submitted information and the documentation available to the staff of the Indiana SHPO, we have not identified any currently known archaeological resources listed in or eligible for inclusion in the NRHP within the proposed project area; and we concur with the opinion of the archeologist [...] that no further archaeological investigation appear necessary...” (Appendix E, E15-E16).

No other responses to the ASR were received.

3. DESCRIBE AFFECTED HISTORIC PROPERTIES

Dr. Q. Robert Hauss House/IHSSI #019-604-51035: The Dr. Q. Robert Hauss House is roughly bounded by US 31/N. Indiana Avenue, St. Paul Street, N. New Albany Street, and the southwest parcel line (Appendix B, B10). The boundaries include the c. 1885/c. 1925 frame Italianate/Prairie style house with a non-contributing detached garage and pole barn. The house sits roughly at the center of a large lot with many mature trees. The Dr. Q. Robert Hauss is eligible for the NRHP under Criterion C for significance in Architecture.

IHSSI #019-604-51008 (479 N. Indiana Avenue): IHSSI #019-604-51008 (479 N. Indiana Avenue) is a c. 1930 brick English cottage. The boundaries include the front yard of the house up to the parcel line (Appendix B, B9). The house sits close to US 31 and has a concrete walk leading to the sidewalk. IHSSI #019-604-51008 is eligible for the NRHP under Criterion C for significance in Architecture.

4. DESCRIBE THE UNDERTAKING'S EFFECTS ON HISTORIC PROPERTIES

Dr. Q. Robert Hauss House/IHSSI #019-604-51035: A full-depth replacement of US 31 at its existing width will take place adjacent to the property. New underdrains will be installed under the street. The adjacent concrete curb will be replaced. Pavement markings will be revised to provide a 12-foot wide TWLTL on US 31 adjacent to the property. The project will replace the curb ramps on the south side of the intersection of US 31 and St. Paul Street, adjacent to the north corner of the property, within the existing ROW. High-visibility pavement markings and signage and a RRFB HAWK will be added to the intersection. The 6-foot wide concrete sidewalk on the west side of US 31, across from the Dr. Q. Robert Hauss House, will be replaced (Appendix B, B44-B45). MOT will require closure of US 31 adjacent to the property, but access will be maintained.

Approximately 0.02 acre of temporary ROW acquisition from the Dr. Q. Robert Hauss House/IHSSI #019-604-51035 will be required for reconstruction of the driveway at the south end of the property. No permanent ROW will be acquired from this property. The temporary ROW acquisition and work within the temporary ROW will meet the conditions of a temporary occupancy as listed in 23 CFR 774.13(d):

1. Duration of work must be temporary, i.e., less than the time needed for construction of the project, and there should be no change in ownership of the land;
2. Scope of the work must be minor, i.e., both the nature and the magnitude of the changes to the Section 4(f) property are minimal;
3. There are no anticipated permanent adverse physical impacts, nor will there be interference with the protected activities, features, or other attributes of the property, on either a temporary or permanent basis; and
4. The land being used must be fully restored, i.e., the property must be returned to a condition which is at least as good as that which existed prior to the project.

The temporary ROW will be used for driveway reconstruction, which will take less time than the total construction of the project. There will be no change in land ownership. The scope of work will be minor, consisting of restoration of an existing driveway. No permanent adverse impacts to any physical elements of the property are anticipated and the property's use and attributes will not be affected. The land will be fully restored to its preexisting condition.

IHSSI #019-604-51008 (479 N. Indiana Avenue): A full-depth replacement of US 31 at its existing width will take place adjacent to the property. New underdrains will be installed under the street. The adjacent concrete curb will be replaced. Pavement markings will be revised to provide a 12-foot wide TWLTL on US 31 adjacent to the property. The 4-foot wide concrete curb ramps adjacent to the southeast side of the property will be replaced within their existing footprint. The curb ramps on the commercial drive north of IHSSI #019-604-51008 will also be replaced (Appendix B, B47). MOT will require closure of US 31 adjacent to the property, but access will be maintained.

Less than 0.01 acre of temporary ROW acquisition from 479 N. Indiana Avenue will be required for reconstruction of the driveway adjacent to the property. No permanent ROW will be acquired from this property. The temporary ROW acquisition and work within the temporary ROW will meet the conditions of a temporary occupancy as listed in 23 CFR 774.13(d):

1. Duration of work must be temporary, i.e., less than the time needed for construction of the project, and there should be no change in ownership of the land;
2. Scope of the work must be minor, i.e., both the nature and the magnitude of the changes to the Section 4(f) property are minimal;
3. There are no anticipated permanent adverse physical impacts, nor will there be interference with the protected activities, features, or other attributes of the property, on either a temporary or permanent basis; and
4. The land being used must be fully restored, i.e., the property must be returned to a condition which is at least as good as that which existed prior to the project.

The temporary ROW will be used for driveway reconstruction, which will take less time than the total construction of the project. There will be no change in land ownership. The scope of work will be minor, consisting of restoration of an existing driveway. No permanent adverse impacts to any physical elements of the property are anticipated and the property's use and attributes will not be affected. The land will be fully restored to its preexisting condition.

5. EXPLAIN APPLICATION OF CRITERIA OF ADVERSE EFFECT -- INCLUDE CONDITIONS OR FUTURE ACTIONS TO AVOID, MINIMIZE OR MITIGATE ADVERSE EFFECTS

A finding of "*No Adverse Effect*" is appropriate for this project because the US 31 Small Town Pavement Replacement will not alter any of the characteristics for which **the Dr. Q. Robert Hauss House/IHSSI #019-604-51035 and IHSSI #019-604-51008 (479 N. Indiana Avenue)** are eligible for the NRHP in a manner which would diminish their integrity.

Part 36 CFR § 800.5(a)(2), examples of adverse effect include but are not limited to:

- (i) Physical destruction of or damage to all or part of a property;
- (ii) Alteration of a property, including restoration, rehabilitation, repair, maintenance, stabilization, hazardous material remediation, and provision of handicapped access not consistent with the Secretary's Standard for the Treatment of Historic Properties (36 CFR part 68) and applicable guidelines;
- (iii) Removal of the property from its historic location;
- (iv) Change of the character of the property's use or physical features within the property's setting that contribute to its historic significance;
- (v) Introduction of visual, atmospheric, or audible elements that diminish the integrity of the property's significant historic features;
- (vi) Neglect of a property which causes its deterioration, except where such neglect and deterioration are recognized qualities of a property of religious and cultural significance to an Indian tribe or Native Hawaiian organization; and
- (vii) Transfer, lease, or sale of a property out of Federal ownership or control without adequate and legally enforceable restrictions or conditions to ensure long-term preservation of the property's historic significance.

Application of the criteria of adverse effect defined in 36 CFR § 800.5(a)(1), finds the proposed project's potential effects are not likely to be described by the examples in 36 CFR § 800.5(a)(2)(i), (ii), (iii), (vi), or (vii). The application of adverse effect, focusing on 36 CFR § 800.5(a)(2)(iv) and (v) follows:

Dr. Q. Robert Hauss House/IHSSI #019-604-51035: Application of the criteria of adverse effect, as defined per 36 CFR § 800.5(a)(1), finds the proposed project's potential effects upon the Dr. Q. Robert Hauss House/IHSSI #019-604-51035 are most similar to those described in 36 CFR § 800.5(a)(2)(iv) and (v).

In regard to 36 CFR § 800.5(a)(2)(iv), "Change of the character of the property's use or physical features within the property's setting that contribute to its historic significance," the project will not alter the use of the property and will not alter physical features within the property's setting that contribute to its historic significance. Approximately 0.02 acre of temporary ROW will be acquired to reconstruct the existing driveway on the south end of the property. No permanent ROW will be acquired from the property.

In regard to 36 CFR § 800.5(a)(2)(v), "Introduction of visual, atmospheric or audible elements that diminish the integrity of the property's significant historic features," the project will change the property's setting by adding high-visibility pavement markings and signage and an RRFB/HAWK signal to the crosswalk at the US 31/St. Paul Street intersection. However, the existing character of the surrounding area is developed, and there are existing streetlights. These systems are unlikely to add significantly to light pollution to the area. The existing curb ramp on the southeast corner with St. Paul Street will be replaced. The total pavement width of US 31 will not change. Therefore, these additions and changes will not diminish the integrity of the property's significant historic features.

IHSSI #019-604-51008 (479 N. Indiana Avenue): Application of the criteria of adverse effect, as defined per 36 CFR § 800.5(a)(1), finds the proposed project's potential effects upon IHSSI #019-604-51008 are most similar to those described in 36 CFR § 800.5(a)(2)(iv) and (v).

In regard to 36 CFR § 800.5(a)(2)(iv), "Change of the character of the property's use or physical features within the property's setting that contribute to its historic significance," the project will not alter the use of the property and will not alter physical features within the property's setting that contribute to its historic significance. Less than 0.01 acre of temporary ROW will be acquired to reconstruct the existing driveway adjacent to the property. No permanent ROW will be acquired from the property.

In regard to 36 CFR § 800.5(a)(2)(v), "Introduction of visual, atmospheric or audible elements that diminish the integrity of the property's significant historic features," streetscape features adjacent to the property will be replaced in-kind. The total pavement width of US 31 will not change. These minor changes will not diminish the integrity of the property's significant historic features.

The US 31 Small Town Pavement Replacement will not diminish the historical associations, historically significant features, or architectural integrity for which the Dr. Q. Robert Hauss House/IHSSI #019-604-51035 and IHSSI #019-604-51008 (479 N. Indiana Avenue) are eligible for the NRHP under Criterion C. Therefore, the project will not introduce negative effects as defined by 36 CFR § 800.5 and will have "*No Adverse Effect*" on the Dr. Q. Robert Hauss House/IHSSI #019-604-51035 and IHSSI #019-604-51008 (479 N. Indiana Avenue).

6. SUMMARY OF CONSULTING PARTIES AND PUBLIC VIEWS

The Miami Tribe of Oklahoma responded to the Section 106 ECL Letter on December 18, 2019, indicating they wished to be a consulting party. The letter stated they had no objections to the proposed project but noted the project area is within the aboriginal homelands of the Miami Tribe (Appendix E, E10).

The SHPO responded on December 13, 2019 stating they were not aware of any other parties who should be invited to consulting party consultation (DHPA #24727; Appendix E, E8-E9).

No other responses to the early coordination letter were received.

The SHPO responded to the HPR and archaeological report on March 31, 2020 stating in part, “...we agree with the conclusions in the HPR regarding the National Register of Historic Places (“NRHP”) eligibility of the house at 479 N. Indiana Avenue (Indiana Historic Sites and Structures Inventory [“IHSSI”] no. 019-604-51008) and 227 N. Albany Street (IHSSI no. 019-604-51035). We believe both meet Criterion C for their architecture. Regarding the Southern Indiana Railroad Bridge over Creek Drive and Muddy Fork (IHSSI no. 019-604-52011), we respectfully disagree with the conclusions in the HPR. [...] It does not appear to be a significant example of bridge design nor does it serve a locally significant role in transportation.” The SHPO also concluded no other properties within the APE were eligible for the National Register (Appendix E, E15-E16). Therefore, IHSSI #019-604-52011 was not considered eligible for the National Register for the purposes of Section 106 review. The SHPO also concurred with the ASR, stating in part, “we have not identified any currently known archaeological resources listed in or eligible for inclusion in the NRHP within the proposed project area; and we concur with the opinion of the archeologist [...] that no further archaeological investigation appear necessary...” (Appendix E, E15-E16).

No other responses to the HPR or archaeological report were received.

An Effects Letter was approved by INDOT-CRO on June 10, 2020 and sent to consulting parties on June 11, 2020. The letter recommended the project had “*No Adverse Effect*” on the Dr. Q. Robert Hauss House/IHSSI #019-604-51035 and IHSSI #019-604-51008 (479 N. Indiana Avenue) due to the lack of physical changes or permanent ROW acquisition from the properties and the limited changes to the setting (Appendix E, E17-E23).

The SHPO responded to the Effects Letter on June 29, 2020, concurring with the “*No Adverse Effect*” determination, stating in part, “We agree with the opinions expressed in the June 11, 2020 effects letter that the Dr. Q. Robert Hauss House (Indiana Historic Sites and Structures Inventory [“IHSSI”] #019-604-51035) and the house at 479 N. Indiana Avenue (IHSSI #019-604-51008), which are eligible for inclusion in the National Register of Historic Places (“NRHP”), will not be adversely affected by this project.” (Appendix E, E24-E25).

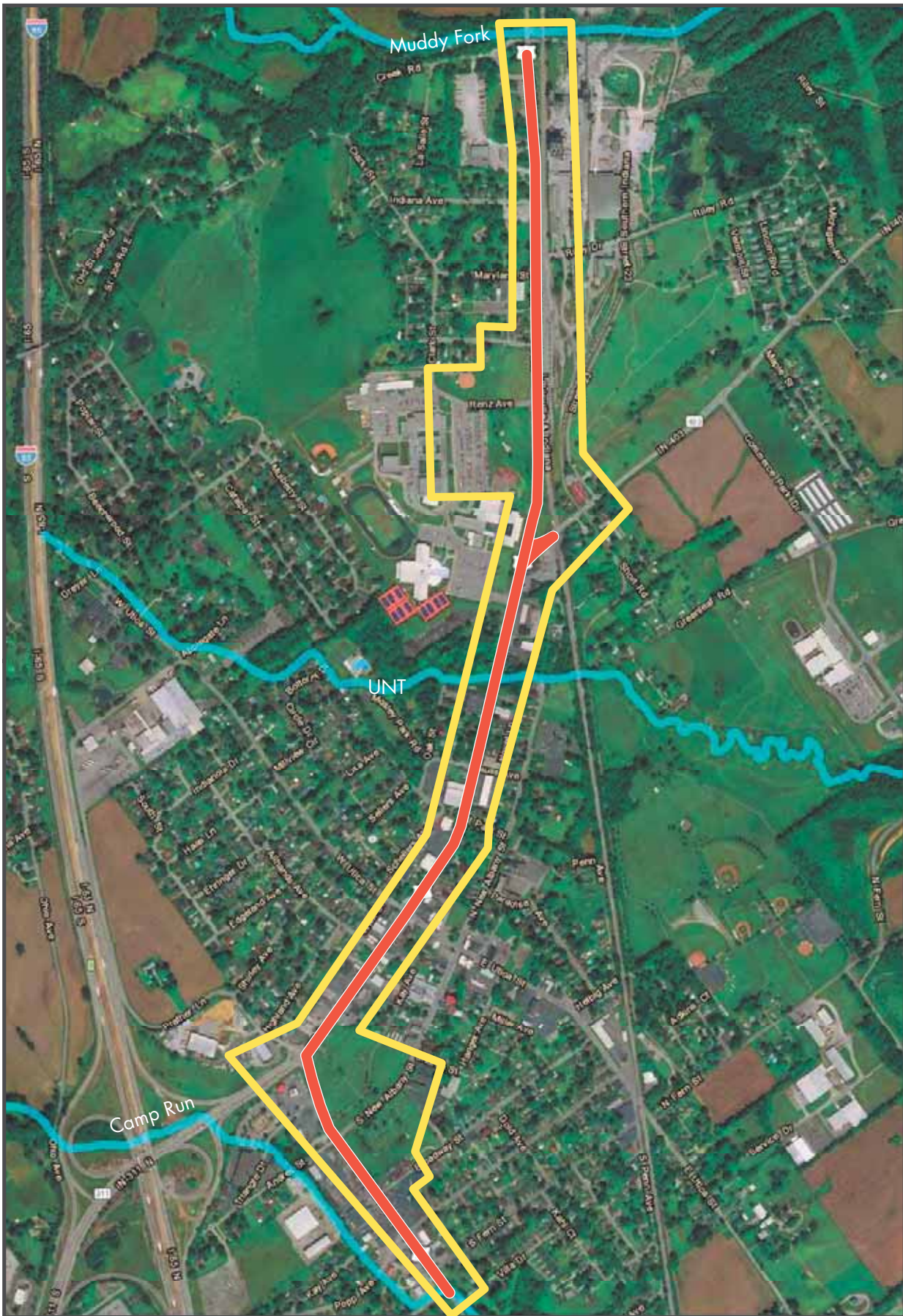
No other responses to the Effects Letter were received.

A finding of “*No Adverse Effect*” is appropriate for this project because the project will not alter characteristics for which the Dr. Q. Robert Hauss House/IHSSI #019-604-51035 and IHSSI #019-604-51008 (479 N. Indiana Avenue) are eligible for the NRHP in a manner that would diminish their integrity.

A Section 106 public notice will be published in the *Clark County News and Tribune* offering the public the opportunity to make comments on INDOT's "No Adverse Effect" finding. A thirty (30) day comment period will be given. The document will be revised, if necessary, after the public notice to reflect any comments received.

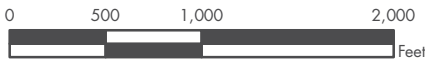
APPENDIX

- Appendix A: Invited Section 106 Consulting Parties
- Appendix B: Graphics
- Appendix C: Excerpt from the Historic Property Short Report
- Appendix D: Excerpt from the ASR
- Appendix E: Correspondence



- Legend**
- Project Area
 - Area of Potential Effects (APE)

Map Source: Indiana Geological Survey (IGS), IndianaMap, ArcGIS Online (ESRI) World Imagery.




APE Map

US 31 Preventative Pavement Maintenance
 Sellersburg, Clark County, IN
 Des No. 1700111



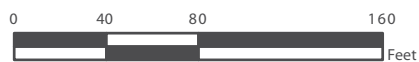


Legend

 Proposed NRHP Boundaries



Map Source: IndianaMap



IHSSI #019-604-51008
(479 N. Indiana Avenue)
NRHP Boundaries
US 31 Preventative Pavement Maintenance
Sellersburg, Clark County, IN
Des No. 1700111



Legend

Proposed NRHP Boundaries



Esri, HERE, Garmin, (c)
OpenStreetMap contributors,
Source: Esri, DigitalGlobe,
GeoEye, Earthstar

Map Source: Indiana Geological
Survey (IGS), IndianaMap,
ArcGIS Online (ESRI) World Imagery.



**Dr. Q. Robert Hauss House/
IHSSI #019-604-51035
NRHP Boundaries**

US 31 Preventative Pavement Maintenance
Sellersburg, Clark County, IN
Des No. 1700111



Elizabet Biggio

From: Alexander, Kelyn <KAlexander3@indot.IN.gov>
Sent: Monday, November 25, 2019 9:28 AM
To: Elizabet Biggio
Cc: Branigin, Susan; Miller, Shaun (INDOT); Kumar, Anuradha
Subject: RE: US 31 Sellersburg-Des1700111-Section 106 ECL
Attachments: US 31 Sellersburg_Des1700111_Sec 106 ECL Email_2019-11-25_INDOTcomments.doc

Elizabet,

Thank you for sending us the email. I had some additional consulting parties to add to the list. Please include them on the ECL list of consulting parties as well. Once those edits are made, the ECL should be ready to be "checked-in" to INSCOPE.

Thank you,

Kelyn Alexander

Historian

Cultural Resources Office

Environmental Services

100 N. Senate Ave., Room N642

Indianapolis, IN 46204

Office: (317) 234-4147

Email: kalexander3@indot.in.gov



**** Historic Property Report (HPR) guidelines can be found [here](#)**

From: Elizabet Biggio [mailto:EBiggio@bfsengr.com]
Sent: Monday, November 25, 2019 8:36 AM
To: Alexander, Kelyn
Cc: Branigin, Susan ; Miller, Shaun (INDOT) ; Kumar, Anuradha
Subject: RE: US 31 Sellersburg-Des1700111-Section 106 ECL

**** This is an EXTERNAL email. Exercise caution. DO NOT open attachments or click links from unknown senders or unexpected email. ****

Kelyn,

Thanks, the email is attached.

Elizabet Biggio

Architectural Historian II

Butler, Fairman & Seufert, Inc.

8450 Westfield Blvd., Suite 300 | Indianapolis, IN 46240-8302

p (317) 713-4615 | f (317) 713-4616

EBiggio@bfsengr.com | www.BFSEngr.com



INDIANA DEPARTMENT OF TRANSPORTATION

100 North Senate Avenue
Room N642
Indianapolis, Indiana 46204

PHONE: (317) 234-5168

Eric Holcomb, Governor
Joe McGuinness, Commissioner

November 25, 2019

This letter was sent to the listed parties.

RE: US 31 Small Town Pavement Replacement, 1.53 miles N of SR 60 (Foothill Rd) to 3.28 miles N of SR 60 (Des. No. 1700111); Clark County, IN

Dear Consulting Party (see attached list),

The Indiana Department of Transportation-Seymour District (INDOT), with funding from the Federal Highway Administration, proposes to proceed with improvement to US 31 in the Town of Sellersburg and the unincorporated community of Speed in Clark County, Des. No. 1700111. Butler, Fairman, & Seufert is under contract with INDOT to advance the environmental documentation for the referenced project.

This letter is part of the early coordination phase of the environmental review process requesting comments associated with this project. We are requesting comments from your area of expertise regarding any possible environmental effects associated with this project. Please use the above Des. Number and project description in your reply and your comments will be incorporated into the formal environmental study.

The proposed undertaking is on US 31 between Foothill Road and the Silver Creek Bridge in Clark County, Indiana. The proposed project is approximately 1.75 miles long. It is within Silver Creek Township, USGS Speed, Indiana Quadrangle, in Sections 110, 111, and 130, Township 99, Range 99.

The need for the project is derived from the deteriorated conditions along US 31. Portions of the corridor exhibit significant cracking in the asphalt or concrete surface. Existing curbs are buried and deteriorating. Drainage is insufficient to handle stormwater. The pedestrian facilities are not Americans with Disabilities Act (ADA)-compliant, and in some stretches are nonexistent. According to INDOT data, in the period between 2010 and 2018 there were 463 crashes within the project area, or an average of 56 per year. About 63% were rear-end crashes, which are associated with signalized intersections, congested traffic conditions, and driver inattention. This problem is particularly apparent at the intersection of US 31 and CR 403 (formerly SR 403). Finally, the West Clark Community Schools Corp. has expressed safety concerns about the mid-block pedestrian crossing near the entrance to Silver Creek Elementary and Silver Creek Middle Schools.

The purpose of this project is to extend the functional life of the existing pavement, improve drainage, provide ADA-compliant pedestrian facilities, and improve safety along US 31 between Foothill Road and the Silver Creek Bridge by 2022.

The project proposes to mill and repave US 31, using full-depth replacement with underdrains where necessary. Non ADA-compliant curb ramps would be replaced; all sidewalks and curb ramps may be replaced. A new stormwater drainage trunk line or lines and inlets would be installed between approximately 250 feet northeast of Prather Street to 50 feet north of CR 403. Improvement at the intersection with CR 403 may include, pending further study:

- Relocating the mid-block pedestrian crosswalk to the signalized intersection with ADA-pushbuttons and pedestrian countdown heads
- Removing the US 31 northbound bypass lane and adding a signal-controlled right turn lane
- Adding turn lanes to southbound CR 403 extending from US 31 to New Albany Street

Section 106 of the National Historic Preservation Act requires federal agencies to take into account the effects of their undertakings on historic and archaeological properties. In accordance with 36 CFR 800.2 (c), you are hereby requested to be a consulting party to participate in the Section 106 process. Entities that have been invited to participate in the Section 106 consultation process for this project are identified in the attached list. Per 36 CFR 800.3(f), we hereby request that the Indiana State Historic Preservation Officer (SHPO) notify this office if the SHPO staff is aware of any other parties that may be entitled to be consulting parties or should be contacted as potential consulting parties for the project.

The Section 106 process involves efforts to identify historic properties potentially affected by the undertaking, assess its effects and seek ways to avoid, minimize or mitigate any adverse effects on historic properties. For more information regarding the protection of historic resources, please see the Advisory Council on Historic Preservation's guide: *Protecting Historic Properties: A Citizen's Guide to Section 106 Review* available online at <https://www.achp.gov/sites/default/files/documents/2017-01/CitizenGuide.pdf> .

The Area of Potential Effects (APE) is the area in which the proposed project may cause alterations in the character or use of historic resources. At this time, no cultural resource investigations have occurred; however, the results of cultural resource identification and evaluation efforts, both above-ground and archaeological, will be forthcoming. Consulting parties will receive notification when these reports are completed.

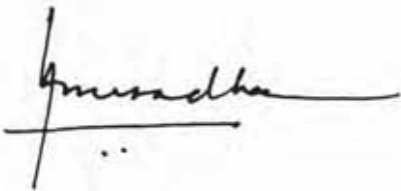
Please review the information and comment within thirty (30) calendar days of receipt. If you indicate that you do not desire to be a consulting party, or if you do not respond, you will not be included on the list of consulting parties for this project. If we do not receive your response in the time allotted, the project will proceed consistent with the proposed design and you will not receive further information about the project unless the design changes.

All future responses regarding the proposed project should be forwarded to Butler, Fairman, & Seufert at the following address:

Elizabeth Biggio
Architectural Historian II
Butler, Fairman, & Seufert, Inc.
8450 Westfield Boulevard, Suite 300
Indianapolis, IN 46240
ebiggio@bfsengr.com

Tribal contacts may contact Shaun Miller at smiller@indot.in.gov or 317-233-6795 or Michelle Allen at FHWA at michelle.allen@dot.gov or 317-226-7344.

Sincerely,



Anuradha V. Kumar, Manager
Cultural Resources Office
Environmental Services

Enclosures:

Topographic Map

See Appendix B

Distribution List:

Indiana State Historic Preservation Officer
Indiana Landmarks Southern Regional Office
Clark County Historian
Jeff-Clark Preservation, Inc.
Clark's Grant Historical Society
Kentuckiana Regional Planning & Development Agency
Sellersburg Clerk-Treasurer
Sellersburg Town Council
Sellersburg Streets & Sanitation Department
Clark County Commissioners
Clark County Highway Superintendent
Eastern Shawnee Tribe of Oklahoma
Miami Tribe of Oklahoma
Peoria Tribe of Indians of Oklahoma
Pokagon Band of Potawatomi Indians
United Keetoowah Band of Cherokee Indians
Delaware Tribe of Indians

Elizabeth Biggio

From: Elizabeth Biggio
Sent: Tuesday, November 26, 2019 7:21 AM
To: Slider, Chad (DNR); 'south@indianalandmarks.org'; 'jeanne_b@hotmail.com'; 'preservation.station@yahoo.com'; 'Tfaith@Unix.Adept.Net'; 'larry.chaney@kipda.org'; 'sellersburgclerk@gmail.com'; 'lkearney@sellersburg.org'; 'sdaniel@co.clark.in.us'; 'jcoffman@co.clark.in.us'; 'csellers@co.clark.in.us'; 'bglover@co.clark.in.us'
Cc: Alexander, Kelyn
Subject: FHWA Project: Des. No. 1700111; US 31 Small Town Pavement Replacement, 1.53 miles N of SR 60 (Foothill Rd) to 3.28 miles N of SR 60, Clark County, Indiana
Categories: Filed by Newforma

Des. No.: 1700111

Project Description: Pavement Replacement, Drainage Improvements

Location: City of Sellersburg and Community of Speed, Clark County, IN

The Indiana Department of Transportation, with funding from the Federal Highway Administration, proposes to proceed with the US 31 Pavement Replacement project, including drainage and pedestrian safety improvements (Des. No. 1700111).

Section 106 of the National Historic Preservation Act requires federal agencies to take into account the effects of their undertakings on historic properties. The following agencies/individuals are being invited to become consulting parties:

Indiana State Historic Preservation Officer
Indiana Landmarks Southern Regional Office
Clark County Historian
Jeff-Clark Preservation, Inc.
Clark's Grant Historical Society
Kentuckiana Regional Planning & Development Agency
Sellersburg Clerk-Treasurer
Sellersburg Town Council
Sellersburg Streets & Sanitation Department
Clark County Commissioners
Clark County Highway Superintendent
Eastern Shawnee Tribe of Oklahoma
Miami Tribe of Oklahoma
Peoria Tribe of Indians of Oklahoma
Pokagon Band of Potawatomi Indians
United Keetoowah Band of Cherokee Indians
Delaware Tribe of Indians

This letter is part of the early coordination phase of the environmental review process requesting comments associated with this project. We are requesting comments from your area of expertise regarding any possible environmental effects associated with this project. Please use the above Des. Number and project description in your reply and your comments will be incorporated into the formal environmental study.

Please review the attached letter, which is also located in IN SCOPE at <http://erms.indot.in.gov/Section106Documents/> (the Des. No. is the most efficient search term, once in IN SCOPE), and respond with your comments on any historic

resource impacts incurred as a result of this project so that an environmental report can be completed. We also welcome your related opinions and other input to be considered in the preparation of the environmental document. If a hard copy of the materials is needed, please respond to this email with your request within seven (7) days.

Consulting parties have thirty (30) calendar days from receipt of this information to review and provide comments. If we do not receive a response from an invited consulting party within the time allotted, the project will proceed consistent with the proposed design. **Therefore, if we do not receive a response within thirty (30) days, your agency or organization will not receive any further information on the project unless the scope of work changes.**

Tribal contacts may contact Shaun Miller at smiller@indot.in.gov or 317-233-6795 or Michelle Allen at FHWA at michelle.allen@dot.gov or 317-226-7344.

Thank you in advance for your input,

Elizabet Biggio
Architectural Historian II

Butler, Fairman & Seufert, Inc.
8450 Westfield Blvd., Suite 300 | Indianapolis, IN 46240-8302
p (317) 713-4615 | f (317) 713-4616
EBiggio@bfsengr.com | www.BFSEngr.com

CONFIDENTIALITY NOTICE: This Email and any attachments are confidential and may be protected by legal privilege. If you are not the intended recipient, be aware that any disclosure, copying, distribution, or use of this Email or any attachment is prohibited. If you have received this Email in error, please notify us immediately by returning it to the sender and delete this copy from your system. Thank you. Butler, Fairman & Seufert, Inc.

Elizabeth Biggio

From: Alexander, Kelyn <KAlexander3@indot.IN.gov>
Sent: Tuesday, November 26, 2019 7:48 AM
To: thpo@estoo.net; dhunter@miamination.com; lpappenfort@peoriatribe.com; Matthew.Bussler@pokagonband-nsn.gov; cwolf@ukb-nsn.gov; lheady@delawaretribe.org
Cc: Miller, Shaun (INDOT); michelle.allen@dot.gov; Branigin, Susan; Elizabet Biggio
Subject: FHWA Project: Des. No. 1700111; US 31 Small Town Pavement Replacement, 1.53 miles N of SR 60 (Foothill Rd) to 3.28 miles N of SR 60, Clark County, Indiana
Attachments: US 31 Sellersburg_Des1700111_Sec 106 ECL_2019-11-25.pdf

Des. No.: 1700111

Project Description: Pavement Replacement, Drainage Improvements

Location: City of Sellersburg and Community of Speed, Clark County, IN

The Indiana Department of Transportation, with funding from the Federal Highway Administration, proposes to proceed with the US 31 Pavement Replacement project, including drainage and pedestrian safety improvements (Des. No. 1700111).

Section 106 of the National Historic Preservation Act requires federal agencies to take into account the effects of their undertakings on historic properties. The following agencies/individuals are being invited to become consulting parties:

Indiana State Historic Preservation Officer
Indiana Landmarks Southern Regional Office
Clark County Historian
Jeff-Clark Preservation, Inc.
Clark's Grant Historical Society
Kentuckiana Regional Planning & Development Agency
Sellersburg Clerk-Treasurer
Sellersburg Town Council
Sellersburg Streets & Sanitation Department
Clark County Commissioners
Clark County Highway Superintendent
Eastern Shawnee Tribe of Oklahoma
Miami Tribe of Oklahoma
Peoria Tribe of Indians of Oklahoma
Pokagon Band of Potawatomi Indians
United Keetoowah Band of Cherokee Indians
Delaware Tribe of Indians

This letter is part of the early coordination phase of the environmental review process requesting comments associated with this project. We are requesting comments from your area of expertise regarding any possible environmental effects associated with this project. Please use the above Des. Number and project description in your reply and your comments will be incorporated into the formal environmental study.

Please review the attached letter, which is also located in IN SCOPE at <http://erms.indot.in.gov/Section106Documents/> (the Des. No. is the most efficient search term, once in IN SCOPE), and respond with your comments on any historic resource impacts incurred as a result of this project so that an environmental report can be completed. We also

welcome your related opinions and other input to be considered in the preparation of the environmental document. If a hard copy of the materials is needed, please respond to this email with your request within seven (7) days.

Consulting parties have thirty (30) calendar days from receipt of this information to review and provide comments. If we do not receive a response from an invited consulting party within the time allotted, the project will proceed consistent with the proposed design. **Therefore, if we do not receive a response within thirty (30) days, your agency or organization will not receive any further information on the project unless the scope of work changes.**

Tribal contacts may contact Shaun Miller at smiller@indot.in.gov or 317-233-6795 or Michelle Allen at FHWA at michelle.allen@dot.gov or 317-226-7344.

Thank you in advance for your input,

Kelyn Alexander

Historian

Cultural Resources Office

Environmental Services

100 N. Senate Ave., Room N642

Indianapolis, IN 46204

Office: (317) 234-4147

Email: kalexander3@indot.in.gov



**** Historic Property Report (HPR) guidelines can be found [here](#)**



Division of Historic Preservation & Archaeology · 402 W. Washington Street, W274 · Indianapolis, IN 46204-2739
Phone 317-232-1646 · Fax 317-232-0693 · dhpa@dnr.IN.gov · www.IN.gov/dnr/historic



December 13, 2019

Elizabet Biggio
Butler, Fairman, & Seufert, Inc.
8450 Westfield Boulevard, Suite 300
Indianapolis, Indiana 46240

Federal Agency: Indiana Department of Transportation (“INDOT”),
on behalf of Federal Highway Administration, Indiana Division (“FHWA”)

Re: Early coordination letter for US 31 pavement replacement between Foothill Road and Silver
Creek Bridge in Silver Creek Township, Clark County, Indiana (Des. No. 1700111; DHPA
No. 24727)

Dear Ms. Biggio:

Pursuant to Section 106 of the National Historic Preservation Act of 1966, as amended (54 U.S.C. § 306108), 36 C.F.R. Part 800, and the “Programmatic Agreement (PA) Among the Federal Highway Administration, the Indiana Department of Transportation, the Advisory Council on Historic Preservation and the Indiana State Historic Preservation Officer Regarding the Implementation of the Federal Aid Highway Program In the State of Indiana,” the staff of the Indiana State Historic Preservation Officer (“Indiana SHPO” or “INDNR-DHPA”) has reviewed your November 25, 2019 review request submittal form, which enclosed INDOT’s early coordination letter, which we received on December 2, 2019 for the aforementioned project.

We are not aware of any parties who should be invited to participate in the Section 106 consultation on this federal undertaking, beyond those whom INDOT already has invited. However, if right-of-way is likely to be taken from a potentially historic property, it might be advisable to invite the owner of that property as soon as possible. In your next regular correspondence on this project, please advise us as to which of the invited consulting parties has accepted the invitation.

We look forward to reviewing the proposed area of potential effects and the reports on investigations of above-ground cultural resources and archaeological resources that the early coordination letter indicated will be forthcoming.

The Indiana SHPO staff’s archaeological reviewer for this project is Wade T. Tharp, and the structures reviewer is Danielle Kauffmann. However, if you have a question about the Section 106 process, please contact initially the INDOT Cultural Resources staff members who are assigned to this project.

In all future correspondence about US 31 pavement replacement between Foothill Road and Silver Creek Bridge in Silver Creek Township, Clark County, Indiana (Des. No. 1700111), please refer to DHPA No. 24727.

Very truly yours,



Beth K. McCord
Deputy State Historic Preservation Officer

BKM:DMK:dmk

emc: Michelle Allen, FHWA
Erica Tait, FHWA
Anuradha Kumar, INDOT
Shaun Miller, INDOT
Susan Branigin, INDOT
Shirley Clark, INDOT
Anthony Ross, INDOT
Elizabet Biggio, Butler, Fairman, & Seufert, Inc.
Wade T. Tharp, INDNR-DHPA
Danielle Kauffmann, INDNR-DHPA



Miami Tribe of Oklahoma

3410 P St. NW, Miami, OK 74354 • P.O. Box 1326, Miami, OK 74355
Ph: (918) 541-1300 • Fax: (918) 542-7260
www.miamination.com



Via email: smiller@indot.IN.gov

December 18, 2019

Shaun Miller
Archaeological Team Lead
Cultural Resources Office, Indiana DOT
575 North Pennsylvania Street
Indianapolis, IN 46204

Re: Des. No. 1700111; US 31 Small Town Pavement Replacement, 1.53 miles N of SR 60 (Foothill Rd) to 3.28 miles N of SR 60, Clark County, Indiana – Comments of the Miami Tribe of Oklahoma

Dear Mr. Miller:

Aya, kikwehsitoole – I show you respect. My name is Diane Hunter, and I am the Tribal Historic Preservation Officer for the Federally Recognized Miami Tribe of Oklahoma. In this capacity, I am the Miami Tribe's point of contact for all Section 106 issues.

The Miami Tribe offers no objection to the above-mentioned project at this time, as we are not currently aware of existing documentation directly linking a specific Miami cultural or historic site to the project site. However, as this project is within the aboriginal homelands of the Miami Tribe, if any human remains or Native American cultural items falling under the Native American Graves Protection and Repatriation Act (NAGPRA) or archaeological evidence is discovered during any phase of this project, the Miami Tribe requests immediate consultation with the entity of jurisdiction for the location of discovery. In such a case, please contact me at 918-541-8966 or by email at dhunter@miamination.com to initiate consultation.

The Miami Tribe accepts the invitation to serve as a consulting party to the proposed project. In my capacity as Tribal Historic Preservation Officer I am the point of contact for consultation.

Respectfully,

Diane Hunter
Tribal Historic Preservation Officer

HISTORIC PROPERTY REPORT

US 31 Small Town Pavement Replacement
Silver Creek Township, Clark County, Indiana
Des. No.: 1700111



Elizabet Biggio
Butler, Fairman & Seufert, Inc.
8450 Westfield Blvd., Suite 300
Indianapolis, IN 46240
(317) 713-4615
ebiggio@bfsengr.com
February 24, 2020

Executive Summary

This Historic Property Report (HPR) documents the identification and evaluation efforts for properties included in the Area of Potential Effects (APE) for a project involving US 31 in the Town of Sellersburg and the community of Speed, Silver Creek Township, Clark County, Indiana. Above-ground resources located within the APE were identified and evaluated according to Section 106, National Historic Preservation Act (NHPA) of 1966, as amended, 36 CFR Part 800 (revised January 2001), the Final Rule on Revision of Current Regulations, dated December 12, 2000, and incorporating amendments effective August 5, 2004. As a result of the NHPA, as amended, and CFR part 800, federal agencies are required to take into account the impact of federal undertakings upon historic properties in the area of the undertaking. Historic properties include buildings, structures, sites, objects, and/or districts included in or eligible for inclusion in the National Register of Historic Places (NRHP). The project will utilize funds from the Federal Highway Administration (FHWA), which makes it an undertaking subject to the Section 106 process.

Of the 104 properties within the APE, none are currently listed in the NRHP. Within the APE, 72 properties will meet the 50-year age criteria at time of project letting (2022). Three properties identified in the APE are recommended eligible for listing in the NRHP:

IHSSI #019-604-51008 (479 N. Indiana Avenue)

Dr. Q. Robert Hauss House/ IHSSI #019-604-51035 (227 N. New Albany Street)

IHSSI #019-604-52011 (Bridge carrying Southern Indiana Railway over Creek Drive and Muddy Fork)

the bridge in 1954 by the Central States Bridge Company, including replacing the substructure, replacing timber beams, and adding thru and girder span reinforcements.¹⁸

The rail line is currently owned by the Louisville and Indiana Railroad and formerly belonged to numerous railroad lines, including the Jeffersonville, Madison and Indianapolis Railroad, the Pittsburgh, Cincinnati, Chicago and St. Louis Railroad, the Indiana Southern Railway, and the Pennsylvania Railroad. The first line, the Jeffersonville Railroad, was opened in 1850.¹⁹ The bridge is adjacent to the Louisville Cement Company mill, which kept its own railroad pump and water tank for the line serving the plant.²⁰

The bridge is associated with growth and development of transportation in Clark County, such as the introduction and prominence of the railroad in the late-nineteenth and early-twentieth centuries and its ties to local industry. However, this property does not reveal any additional or unique aspects of railroad impacts. Since this property lacks sufficient significance, it is not

recommended eligible under Criterion A. Research conducted for this document was unable to establish a connection between the bridge and the lives of historically significant persons. Therefore, the property is not recommended NRHP-eligible under Criterion B. This property is a notable example of a steel girder railroad bridge, an important precursor to modern long-span beam bridges. Therefore, the property is recommended NRHP-eligible under Criterion C. This property has not yielded, and is not likely to yield, information important in prehistory or history and is not recommended NRHP-eligible under Criterion D. IHSSI #019-604-52011 is recommended eligible for listing in the NRHP, with the boundaries being the extent of the bridge (Appendix A, A11).

Conclusions

The APE does not contain any properties currently listed in the NRHP. As a result of identification and evaluation efforts for this project, three properties, **IHSSI #019-604-51008 (479 N. Indiana Avenue)**, the **Dr. Q. Robert Hauss House/IHSSI #019-604-51035 (227 N. New Albany Street)**, and **IHSSI #019-604-52011 (Southern Indiana Railway over Creek Drive and Muddy Fork)** are recommended eligible for listing in the NRHP under Criterion C.

¹⁸ Dr. James Cooper, *Iron Monuments to Distant Posterity: Indiana's Metal Bridges, 1870-1930*, (Greencastle, IN: DePauw University, 1987.), 120-121.

¹⁹ Baird, 107.

²⁰ Sanborn Map Company, *Jeffersonville-Clark County Sanborn Fire Insurance Maps*, (New York City: Sanborn Map & Publishing Co., 1898), 26.

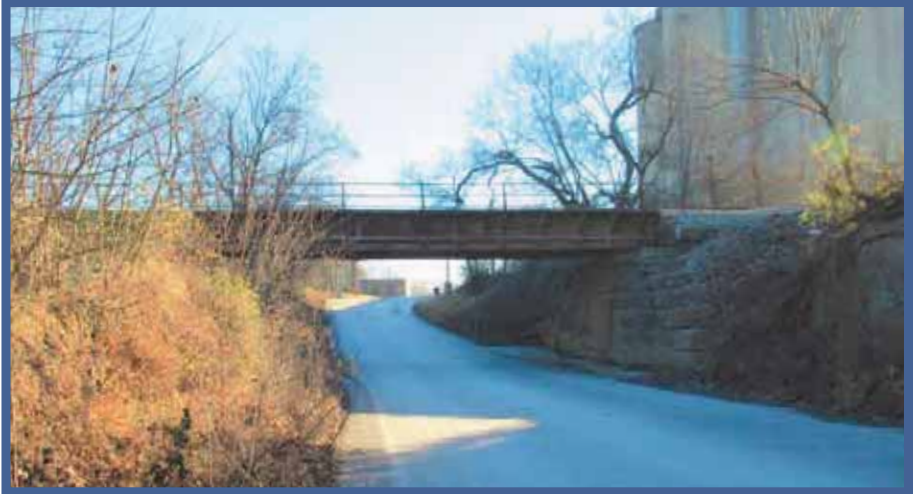


Photo 9: Looking east along Creek Road at IHSSI #019-604-52011

Photo 10: Looking northeast across the Muddy Fork of Silver Creek at IHSSI #019-604-52011



Phase Ia Field Reconnaissance for
Improvements Proposed to US-31 (Des. 1700111) in
Sellersburg, Silver Creek Township, Clark County, Indiana

Prepared by:

Louis Bubb, MA

Submitted By:

Louis Bubb, MA
Principal Investigator
106 Consulting LLC
4425 Redmont Avenue
Deer Park, Ohio 45236
(513) 620-6770
LouisBubb@Gmail.com

Submitted To:

Mr. Ryan L. Scott
Butler, Fairman & Seufert, Inc.
8450 Westfield Blvd., Suite 300
Indianapolis, IN 46240-8302
(317) 713-4615

Lead Agency:

Indiana Department of Transportation

January 30, 2020

A handwritten signature in black ink that reads "Louis Bubb". The signature is written in a cursive, flowing style with a large initial "L" and "B".

Louis Bubb, MA, Principal Investigator
Project #106C – 0313



INDIANA ARCHAEOLOGICAL SHORT REPORT

State Form 54566 (1-11)

INDIANA DEPARTMENT OF NATURAL RESOURCES DIVISION OF HISTORIC PRESERVATION AND ARCHAEOLOGY

402 West Washington Street, Room W274
Indianapolis, Indiana 46204-2739
Telephone Number: (317) 232-1646
Fax Number: (317) 232-0693
E-mail: dhp@dnr.IN.gov

Where applicable, the use of this form is recommended but not required by the Division of Historic Preservation and Archaeology.

Author: Louis Bubb, MA

Date (month, day, year): January 30, 2020

Project Title: Phase Ia Field Reconnaissance for Improvements Proposed to US-31 (Des. 1700111) in Sellersburg, Silver Creek Township, Clark County, Indiana

PROJECT OVERVIEW

Project Description:

The project would involve the resurfacing of US-31 from just north of St. Joe Road south through Sellersburg to New Albany Street (see Attachment B: Figure 1). Its intersections with Old SR-311 and Old SR-403 would also be restructured. In addition, the upgrade and repair of the existing storm sewers, curbs, gutters and sidewalks is also proposed.

A survey area extending out 3.0 m (10 feet) beyond the existing roadway or sidewalk pavement was considered. In addition, a c. 0.8 acre (0.3 ha) parcel located just north of the US-31 and Old SR-403 intersection was also investigated (see Attachment B: Figure 1).

INDOT Designation Number/ Contract Number: 1700111 Project Number:

DHPA Number: Approved DHPA Plan Number:

Prepared For: Butler, Fairman & Seufert, Inc.

Contact Person: Mr. Ryan L. Scott

Address: 8450 Westfield Blvd., Suite 300

City: Indianapolis State: IN ZIP Code: 46240-8302

Telephone Number: 317.713.4615 Email Address:

Principal Investigator: Louis Bubb, MA

Signature: 

Company/Institution: 106 Consulting, LLC

Address: 4425 Redmont Avenue

City: Deer Park State: OH ZIP Code: 45236

Telephone Number: 513.620.6770 Email Address: louisbubb@gmail.com

All fieldwork was conducted in accordance to the DHPA Guidebook for Indiana Historic Sites and Structures Inventory (2008).

The majority of the proposed project area is located within 3.0 m (10 ft) meters of existing roadways and sidewalks (see Attachment C: Figure 1). Consistent with Soil Survey Staff (2020b) – which indicated that the vast majority of the proposed project area contained cut and filled Udorthents (Uaa) and Urban land-Udarents (UngB) – extensive prior soil disturbance was noted through visual inspection (see Attachment C: Figure 2). Disturbance was verified through both 1-inch Oakfield soil cores and screened shovel test probes (STPs) excavated at 15 m (49 ft) interval (see Attachment B: Figure 4).

Screened shovel test probes (STPs) were excavated at a 15 m (49 ft) interval across the c. 0.8 acre (0.3 ha) parcel located north of the US-31 and Old SR-403 intersection (see Attachment C: Figure 3). The majority of the soils in this area were also disturbed, though to varying degrees.

Describe Methods:

Attach photographs documenting disturbances below

Describe Disturbances:

Culverts, ditches and buried utilities adjacent to existing roads and driveways.

Comments:

Results

- Archaeological records check has determined that the project area does not have the potential to contain archaeological resources.
- Archaeological records check has determined that the project area has the potential to contain archaeological resources.
- Phase Ia reconnaissance has located no archaeological resources in the project area.
- Phase Ia reconnaissance has identified landforms conducive to buried archaeological deposits.

Actual Area Surveyed hectares:

02.1

acres:

05.2

Comments:

The vast majority of the proposed project area has been disturbed by prior land use. No cultural materials were encountered.

Recommendation

- The archaeological records check has determined that the project area has the potential to contain archaeological resources and a Phase Ia archaeological reconnaissance is recommended.
- The archaeological records check has determined that the project area does not have the potential to contain archaeological resources and no further work is recommended before the project is allowed to proceed.
- The Phase Ia archaeological reconnaissance has located no archaeological sites within the project area and it is recommended that the project be allowed to proceed as planned.
- The Phase Ia archaeological reconnaissance has determined that the project area includes landforms which have the potential to contain buried archaeological deposits. It is recommended that Phase Ic archaeological subsurface reconnaissance be conducted before the project is allowed to proceed.
- The Phase Ia archaeological reconnaissance has determined that the project area is within 100 feet of a cemetery and a Cemetery Development Plan is required per IC-14-21-1-26.5.

Cemetery Name:

Sellersburg Cemetery (CR-10-118)

Other Recommendations/Commitments:

The Sellersburg Cemetery (CR-10-118) – located on the southeast corner of the US-31 / Old SR-311 intersection– lies within 30.5 m (100 feet) of the proposed project area (see Attachment A: Figure 4). It is estimated that approximately 0.04 acres of new ROW would be required in vicinity of the cemetery.

From: [Moffatt, Charles D](#)
To: [Elizabet Biggio](#)
Cc: [Alexander, Kelyn](#); [Miller, Shaun \(INDOT\)](#); [louis bubb](#); [Branigin, Susan](#)
Subject: RE: File Transfer: US 31 Sellersburg-Des1700111-HPR and ASR - 17-0199 US 31 PAVEMENT SELLERSBURG
Date: Tuesday, February 4, 2020 12:29:19 PM

Elizabet,

The above referenced archaeological report was reviewed by INDOT Cultural Resources personnel who meet the Secretary of the Interior's Professional Qualification Standards as per 36 CFR Part 61. It is our opinion that the report is acceptable, and we concur with the evaluations and recommendations made by Bubb (January 30, 2020). Please submit one copy of the archaeology report to SHPO for review and concurrence. In addition, we ask that a copy of the SHPO submittal form be sent to INDOT, CRO care of David Moffatt at cmoffatt@indot.in.gov during the time of submission and that the approved report be posted to INSCOPE. If there are any questions or concerns regarding this project, please contact me.

David Moffatt
Archaeologist
Environmental Services
Cultural Resources Office
Indiana Department of Transportation
317-233-3703

From: Elizabet Biggio [mailto:EBiggio@bfsengr.com]
Sent: Tuesday, February 04, 2020 11:17 AM
To: Moffatt, Charles D <CMoffatt@indot.IN.gov>
Cc: Alexander, Kelyn <KAlexander3@indot.IN.gov>; Miller, Shaun (INDOT) <smiller@indot.IN.gov>; louis bubb <louisbubb@gmail.com>
Subject: RE: File Transfer: US 31 Sellersburg-Des1700111-HPR and ASR - 17-0199 US 31 PAVEMENT SELLERSBURG

****** This is an EXTERNAL email. Exercise caution. DO NOT open attachments or click links from unknown senders or unexpected email. ******

David,

Thank you. The revised report is attached.

Elizabet Biggio
Architectural Historian

Butler, Fairman & Seufert, Inc.
8450 Westfield Blvd., Suite 300 | Indianapolis, IN 46240-8302 |
p 317-713-4615 | f 317-713-4616
EBiggio@bfsengr.com | www.BFSEngr.com

Elizabeth Biggio

From: Alexander, Kelyn <KAlexander3@indot.IN.gov>
Sent: Monday, February 24, 2020 10:45 AM
To: Elizabeth Biggio
Cc: Branigin, Susan; Moffatt, Charles D; Kumar, Anuradha
Subject: RE: File Transfer: US 31 Sellersburg-Des1700111-HPR
Attachments: US 31 Sellersburg_Des1700111_Section 106 Report Distribution Letter_202....doc

Elizabeth,

Thank you for the submission of the revised HPR and distribution letter. We had a few minor comments for both documents, but once those changes are made, the documents will be ready to check into IN SCOPE. I believe the ASR has already been reviewed and returned by the archaeology reviewer. Please be sure to upload the ASR at the same time as well. Please ensure that the IN SCOPE comment deadline allows for the mandatory 30-day period. This period begins with the email to consulting parties, so INDOT-CRO recommends allowing for a few days grace period in case of delays between "check-in" on IN SCOPE and the email notification to consulting parties.

You can view "US 31 Sellersburg_Des1700111_HPR_2020-02-04_INDOTcomments.pdf" at:
<https://documentcloud.adobe.com/link/track?uri=urn%3Aaaid%3Ascds%3AUS%3A6a0bddb2-3006-4a9e-ad80-41772e2eb9ef>

Once all the documents are released, please send a hard-copy to the SHPO and email the non-tribal consulting parties to notify them that the documents are ready for their review using the email we previously approved. Please cc me on that email as well. When we receive that email, we will notify the Tribes.

When the email notification to consulting parties is sent, please check IN SCOPE and verify that the documents are available for public viewing and that all of the document information is correct. In particular, ensure that the project name is consistent with other project documents and that the comment deadline allows for a 30-day comment period. If any information needs to be corrected, consultants should contact INDOT-CRO.

Thanks again, and if you have any questions or comments, don't hesitate to contact me.

Best regards,

Kelyn Alexander
Historian
Cultural Resources Office
Environmental Services
100 N. Senate Ave., Room N642
Indianapolis, IN 46204
Office: (317) 234-4147
Email: kalexander3@indot.in.gov

**** Historic Property Report (HPR) guidelines can be found [here](#)**

From: Elizabeth Biggio [mailto:EBiggio@bfsengr.com]
Sent: Tuesday, February 04, 2020 11:15 AM
To: Alexander, Kelyn <KAlexander3@indot.IN.gov>



INDIANA DEPARTMENT OF TRANSPORTATION

100 North Senate Avenue
Room N642
Indianapolis, Indiana 46204

PHONE: (317) 234-5168

Eric Holcomb, Governor
Joe McGuinness, Commissioner

February 24, 2020

This letter was sent to the listed parties.

RE: US 31 Small Town Pavement Replacement, 1.53 miles N of SR 60 (Foothill Rd) to 3.28 miles N of SR 60 (Des. No. 1700111);
Clark County, IN

Dear Consulting Party (see attached list),

The Indiana Department of Transportation-Seymour District (INDOT), with funding from the Federal Highway Administration, proposes to proceed with improvement to US 31 in the Town of Sellersburg and the unincorporated community of Speed in Clark County, Des. No. 1700111.

This letter is part of the Section 106 review process for this project. Section 106 of the National Historic Preservation Act requires federal agencies to take into account the effects of their undertakings on historic and archaeological properties. We are requesting comments from you regarding the possible effects of this project. Please use the above Des. Number and project description in your reply and your comments will be incorporated into the formal environmental study. A Section 106 early coordination letter was distributed on November 26, 2019.

The proposed undertaking is on US 31 between Foothill Road and the Silver Creek Bridge in Clark County, Indiana. The proposed project is approximately 1.75 miles long. It is within Silver Creek Township, USGS Speed, Indiana Quadrangle, in Sections 110, 111, and 130.

The need for the project is derived from the deteriorated conditions along US 31. Portions of the corridor exhibit significant cracking in the asphalt or concrete surface. Existing curbs are buried and deteriorating. Drainage is insufficient to handle stormwater. The pedestrian facilities are not Americans with Disabilities Act (ADA)-compliant, and in some stretches are nonexistent. According to INDOT data, in the period between 2010 and 2018 there were 463 crashes within the project area, or an average of 56 per year. About 63% were rear-end crashes, which are associated with signalized intersections, congested traffic conditions, and driver inattention. This problem is particularly apparent at the intersection of US 31 and CR 403 (Old SR 403). Finally, the West Clark Community Schools Corp. has expressed safety concerns about the mid-block pedestrian crossing near the entrance to Silver Creek Elementary and Silver Creek Middle Schools.

The purpose of this project is to extend the functional life of the existing pavement, improve drainage, provide ADA-compliant pedestrian facilities, and improve safety along US 31 between Foothill Road and the Silver Creek Bridge by 2022.

The project proposes to mill and repave US 31, using full-depth replacement with underdrains where necessary. Non ADA-compliant curb ramps would be replaced; all sidewalks and curb ramps may be replaced. A new stormwater drainage trunk line or lines and inlets would be installed between approximately 250 feet northeast of Prather Street to 50 feet north of CR 403. Improvement at the intersection with CR 403 may include, pending further study:

- Relocating the mid-block pedestrian crosswalk to the signalized intersection with ADA-pushbuttons and pedestrian countdown heads
- Removing the US 31 northbound bypass lane and adding a signal-controlled right turn lane
- Adding turn lanes to southbound CR 403 extending from US 31 to New Albany Street

Butler, Fairman, & Seufert is under contract with INDOT to advance the environmental documentation for the referenced project. In accordance with 36 CFR 800.2 (c), you were invited to become a consulting party as part of the Section 106 process, or you are hereby invited to become a consulting party as part of the Section 106 process. Entities that have previously accepted consulting party status--as well as additional entities that are currently being invited to become consulting parties--are identified in the attached list.

The Section 106 process involves efforts to identify historic properties potentially affected by the undertaking, assess its effects and seek ways to avoid, minimize or mitigate any adverse effects on historic properties. For more information regarding the protection of historic resources, please see the Advisory Council on Historic Preservation's guide: *Protecting Historic Properties: A Citizen's Guide to Section 106 Review* available online at <https://www.achp.gov/sites/default/files/documents/2017-01/CitizenGuide.pdf>.

The Area of Potential Effects (APE) is the area in which the proposed project may cause alterations in the character or use of historic resources. The APE contains no resources listed in the National Register of Historic Places (NRHP).

A historian who meets the Secretary of the Interior's Professional Qualification Standards identified and evaluated above-ground resources within the APE for potential eligibility for the NRHP. As a result of the historic property identification and evaluation efforts, IHSSI #019-604-51008 (479 N. Indiana Avenue), the Dr. Q. Robert Hauss House/ IHSSI #019-604-51035 (227 N. New Albany Street), and IHSSI #019-604-52011 (Bridge carrying Southern Indiana Railway over Creek Drive/Muddy Fork) are recommended as eligible for listing in the NRHP.

With regards to archaeological resources, an archaeologist who meets the Secretary of the Interior's Professional Qualification Standards identified no sites within the project area. As a result of these efforts, no further work is recommended. A cemetery development plan was recommended for the Sellersburg Cemetery (CR-10-118).

The Historic Property Report and Archaeology Report (Tribes only) are available for review in IN SCOPE at <http://erms.indot.in.gov/Section106Documents/> (the Des. No. is the most efficient search term, once in IN SCOPE). You are invited to review these documents and respond with comments on any historic resource impacts incurred as a result of this project so that an environmental report can be completed. We also welcome your related opinions and other input to be considered in the preparation of the environmental document. If you prefer a hard copy of this material, please respond to this email with your request within seven (7) days.

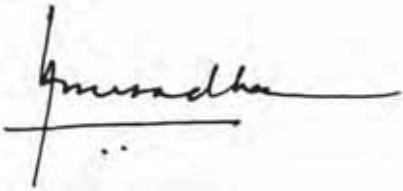
Please review the information and comment within thirty (30) calendar days of receipt. If you indicate that you do not desire to be a consulting party or if you have not previously accepted consulting party status and you do not respond to this letter, you will not be included on the list of consulting parties for this project and will not receive further information about the project unless the design changes.

For questions concerning specific project details, you may contact Elizabet Biggio of Butler, Fairman, & Seufert, Inc. at 317-713-4615 or ebiggio@bfsengr.com. All future responses regarding the proposed project should be forwarded to Butler, Fairman, & Seufert at the following address:

Elizabet Biggio
Architectural Historian II
Butler, Fairman, & Seufert, Inc.
8450 Westfield Boulevard, Suite 300
Indianapolis, IN 46240
ebiggio@bfsengr.com

Tribal contacts may contact Shaun Miller at smiller@indot.in.gov or 317-233-6795 or Michelle Allen at FHWA at michelle.allen@dot.gov or 317-226-7344.

Sincerely,



Anuradha V. Kumar, Manager
Cultural Resources Office
Environmental Services

Distribution List:

Indiana State Historic Preservation Officer
Miami Tribe of Oklahoma

Elizabeth Biggio

From: Elizabeth Biggio
Sent: Tuesday, February 25, 2020 7:57 AM
To: Kauffmann, Danielle; 'WTharp1@dnr.IN.gov'
Cc: Alexander, Kelyn; Dave Moffatt; 'SBranigin@indot.IN.gov'; Miller, Shaun (INDOT); Anuradha Kumar
Subject: FHWA Project: Des. No. 1700111; US 31 Small Town Pavement Replacement, 1.53 miles N of SR 60 (Foothill Rd) to 3.28 miles N of SR 60, Clark County, Indiana
Attachments: US 31 Sellersburg_Des1700111__Section 106 Report Distribution Letter_2020-02-24.pdf

Des. No.: 1700111

Project Description: Pavement Replacement, Drainage Improvements

Location: City of Sellersburg and community of Speed, Clark County, IN

The Indiana Department of Transportation, with funding from the Federal Highway Administration, proposes to proceed with the US 31 Pavement Replacement project, including drainage and pedestrian safety improvements (Des. No. 1700111). The Section 106 Early Coordination Letter for this project was originally distributed on November 26, 2019.

As part of Section 106 of the National Historic Preservation Act, an Archaeology Report (ASR) and a Historic Property Report (HPR) have been prepared and are ready for review and comment by consulting parties.

Please review the HPR and ASR (Tribes only) in IN SCOPE at <http://erms.indot.in.gov/Section106Documents/> (the Des. No. is the most efficient search term, once in IN SCOPE), and respond with any comments that you may have. If a hard copy of the materials is needed, please respond to this email with your request within seven (7) days.

Consulting parties have thirty (30) calendar days from receipt of this information to review and provide comment. Tribal contacts may contact Shaun Miller at smiller@indot.in.gov or 317-233-6795 or Michelle Allen at FHWA at michelle.allen@dot.gov or 317-226-7344.

Thank you in advance for your input,

Elizabeth Biggio
Architectural Historian

Butler, Fairman & Seufert, Inc.
8450 Westfield Blvd., Suite 300 | Indianapolis, IN 46240-8302 |
p 317-713-4615 | f 317-713-4616
EBiggio@bfsengr.com | www.BFSEngr.com



CONFIDENTIALITY NOTICE: This Email and any attachments are confidential and may be protected by legal privilege. If you are not the intended recipient, be aware that any disclosure, copying, distribution, or use of this Email or any attachment is prohibited. If you have received this Email in error, please notify us immediately by returning it to the sender and delete this copy from your system. Thank you. Butler, Fairman & Seufert, Inc.

Elizabeth Biggio

From: Alexander, Kelyn <KAlexander3@indot.IN.gov>
Sent: Tuesday, February 25, 2020 8:09 AM
To: dhunter@miamination.com
Cc: Miller, Shaun (INDOT); michelle.allen@dot.gov; Elizabeth Biggio; Kumar, Anuradha; Branigin, Susan
Subject: FHWA Project: Des. No. 1700111; US 31 Small Town Pavement Replacement, 1.53 miles N of SR 60 (Foothill Rd) to 3.28 miles N of SR 60, Clark County, Indiana
Attachments: US 31 Sellersburg_Des1700111__Section 106 Report Distribution Letter_2020-02-24.pdf

Des. No.: 1700111

Project Description: Pavement Replacement, Drainage Improvements

Location: City of Sellersburg and community of Speed, Clark County, IN

The Indiana Department of Transportation, with funding from the Federal Highway Administration, proposes to proceed with the US 31 Pavement Replacement project, including drainage and pedestrian safety improvements (Des. No. 1700111). The Section 106 Early Coordination Letter for this project was originally distributed on November 26, 2019.

As part of Section 106 of the National Historic Preservation Act, an Archaeology Report (ASR) and a Historic Property Report (HPR) have been prepared and are ready for review and comment by consulting parties.

Please review the HPR and ASR (Tribes only) in IN SCOPE at <http://erms.indot.in.gov/Section106Documents/> (the Des. No. is the most efficient search term, once in IN SCOPE), and respond with any comments that you may have. If a hard copy of the materials is needed, please respond to this email with your request within seven (7) days.

Consulting parties have thirty (30) calendar days from receipt of this information to review and provide comment. Tribal contacts may contact Shaun Miller at smiller@indot.in.gov or 317-233-6795 or Michelle Allen at FHWA at michelle.allen@dot.gov or 317-226-7344.

Thank you in advance for your input,

Kelyn Alexander

Historian

Cultural Resources Office

Environmental Services

100 N. Senate Ave., Room N642

Indianapolis, IN 46204

Office: (317) 234-4147

Email: kalexander3@indot.in.gov

**** Historic Property Report (HPR) guidelines can be found [here](#)**



Division of Historic Preservation & Archaeology · 402 W. Washington Street, W274 · Indianapolis, IN 46204-2739
Phone 317-232-1646 · Fax 317-232-0693 · dhpa@dnr.IN.gov · www.IN.gov/dnr/historic



March 31, 2020

Elizabet Biggio
Butler, Fairman, & Seufert, Inc.
8450 Westfield Boulevard, Suite 300
Indianapolis, Indiana 46240

Federal Agency: Indiana Department of Transportation (“INDOT”),
on behalf of Federal Highway Administration, Indiana Division (“FHWA”)

Re: Historic property report (Biggio, 2/24/2020), and Indiana archaeological short report (Bubb;
01/11/2018, rev. 01/13/2020), for US 31 pavement replacement project between Foothill Road and
Silver Creek Bridge, in Silver Creek Township, Clark County, Indiana (Des. No. 1700111; DHPA No.
24727)

Dear Ms. Biggio:

Pursuant to Section 106 of the National Historic Preservation Act of 1966, as amended (54 U.S.C. § 306108), 36 C.F.R. Part 800, and the “Programmatic Agreement (PA) Among the Federal Highway Administration, the Indiana Department of Transportation, the Advisory Council on Historic Preservation and the Indiana State Historic Preservation Officer Regarding the Implementation of the Federal Aid Highway Program In the State of Indiana,” the staff of the Indiana State Historic Preservation Officer (“Indiana SHPO” or “INDNR-DHPA”) has reviewed your February 24, 2020, review request submittal form, which enclosed the aforementioned historic property report (Biggio, 2/24/2020; “HPR”), and the Indiana archaeological short report (Bubb; 01/11/2018, rev. 01/13/2020), all of which we received on February 27, 2020, for the aforementioned project. We received additional information and clarification regarding the archaeological report via March 30 and 31, 2020, e-mail messages from Elizabet Biggio (Butler, Fairman & Seufert, Inc.), Shaun Miller (INDOT), and Louis Bubb (106 Consulting, LLC); to Wade Tharp (INDNR-DHPA).

The proposed area of potential effects (“APE”) appears to be of adequate size to encompass the geographic area in which direct and indirect effects of a project of this nature could occur.

Regarding buildings and structures, for the purposes of the Section 106 review of this federal undertaking, we agree with the conclusions in the HPR regarding the National Register of Historic Places (“NRHP”) eligibility of the house at 479 N. Indiana Avenue (Indiana Historic Sites and Structures Inventory [“IHSSI”] no. 019-604-51008) and 227 N. Albany Street (IHSSI no. 019-604-51035). We believe both meet Criterion C for their architecture. Regarding the Southern Indiana Railway Bridge over Creek Drive and Muddy Fork (IHSSI no. 019-604-52011), we respectfully disagree with the conclusions in the HPR. We do not believe that this girder-plate bridge is eligible for the State or National Registers. It does not appear to be a significant example of bridge design nor does it serve a locally significant role in transportation.

Accordingly, we agree that aside from the House at 479 N. Indiana Avenue and the Dr. Quincy Robert Hauss House at 227 N. Albany Street, there are no other historic properties listed in or eligible for inclusion in the NRHP in the project’s APE.

Additionally, in terms of archaeological resources, based on the submitted information and the documentation available to the staff of the Indiana SHPO, we have not identified any currently known archaeological resources listed in or eligible for inclusion

in the NRHP within the proposed project area; and we concur with the opinion of the archaeologist, as expressed in the Indiana archaeological short report (Bubb; 01//11/2018, rev. 01/13/2020), that no further archaeological investigations appear necessary at the proposed project area.


Furthermore, because portions of the proposed project area lie 100 feet of portions of Sellersburg Cemetery (CR-10-118 in the INDNR-DHPA SHAARD system database), a cemetery development plan will be necessary under IC 14-21-1-26.5. The aforementioned cemetery must be avoided by all project activities, and provisions of relevant state statutes regarding cemeteries (including IC 14-21-1 and IC 23-14) must be adhered to. Please also be aware of Indiana Code 23-14-44-1 and Indiana Code 23-14-44-2, regarding restrictions on roads and utility construction in cemeteries.

If any prehistoric or historic archaeological artifacts or human remains are uncovered during construction, demolition, or earthmoving activities, state law (Indiana Code 14-21-1-27 and Indiana Code 14-21-1-29) requires that the discovery be reported to INDNR-DHPA within two (2) business days. In that event, please call (317) 232-1646. Be advised that adherence to Indiana Code 14-21-1-27 and Indiana Code 14-21-1-29 does not obviate the need to adhere to applicable federal statutes and regulations, including but not limited to 36 C.F.R. Part 800.

The Indiana SHPO staff's archaeological reviewer for this project is Wade T. Tharp, and the structures reviewer is Danielle Kauffmann. However, if you have a question about the Section 106 process, please contact initially the INDOT Cultural Resources staff members who are assigned to this project.

In all future correspondence about US 31 pavement replacement between Foothill Road and Silver Creek Bridge in Silver Creek Township, Clark County, Indiana (Des. No. 1700111), please refer to DHPA No. 24727.

Very truly yours,



Beth K. McCord
Deputy State Historic Preservation Officer

BKM:DMK:WTT:wtt

emc: Erica Tait, FHWA
Anuradha Kumar, INDOT
Shaun Miller, INDOT
Susan Branigin, INDOT
Shirley Clark, INDOT
Anthony Ross, INDOT
Elizabet Biggio, Butler, Fairman, & Seufert, Inc.
Louis Bubb, 106 Consulting LLC
Diane Hunter, Miami Tribe of Oklahoma
Danielle Kauffmann, INDNR-DHPA
Wade T. Tharp, INDNR-DHPA

Elizabeth Biggio

From: Alexander, Kelyn <KAlexander3@indot.IN.gov>
Sent: Wednesday, June 10, 2020 3:46 PM
To: Elizabeth Biggio
Cc: Moffatt, Charles D; Branigin, Susan; Summers, Terry
Subject: RE: US 31 Sellersburg-Des1700111- Effects Letter- INDOT Project
Attachments: US 31 Sellersburg_Des1700111_EffectsLetter_2020-05-26_INDOTcomments.doc; US 31 Sellersburg_Des1700111_EffectsEmail_2020-05-26_INDOTcomment.docx

Elizabeth,

Thank you for your submission of a revised Effects Letter and email. We had a few additional comments for both documents, but once those changes are made, the Effects Letter can be "checked-in" to IN SCOPE. Please ensure that the IN SCOPE comment deadline allows for the mandatory 30-day period. This period begins with the email to consulting parties, so INDOT-CRO recommends allowing for a few days grace period in case of delays between "check-in" on IN SCOPE and the email notification to consulting parties.

Once the documents are released, please send a hard-copy to the SHPO** and email the non-tribal consulting parties to notify them that the Effects Letter has been posted and is ready for their review using the email we previously approved. Please cc me on that email as well. When we receive that email, we will notify the Tribes.

**In addition to the hard copies SHPO requires, they have also requested during this time for PDF copies of all documents along with the Review Request Submittal form be sent to Miriam Burkett at mburkett@dnr.in.gov. They still require hard copies to be submitted via mail as usual.

When the email notification to consulting parties is sent, please check IN SCOPE and verify that the letter is available for public viewing and that all of the document information is correct. In particular, ensure that the project name is consistent with other project documents and that the comment deadline allows for a 30-day comment period. If any information needs to be corrected, consultants should contact INDOT-CRO.

Thanks again, and if you have any questions or comments, don't hesitate to contact me.

Best regards,

Kelyn Alexander
Historian
Cultural Resources Office
Environmental Services
100 N. Senate Ave., Room N642-ES
Indianapolis, IN 46204
Office: (317) 234-4147
Email: kalexander3@indot.in.gov

** [Historic Property Report \(HPR\) guidelines can be found here](#)

From: Elizabeth Biggio <EBiggio@bfsengr.com>
Sent: Tuesday, May 26, 2020 10:31 AM
To: Alexander, Kelyn <KAlexander3@indot.IN.gov>



INDIANA DEPARTMENT OF TRANSPORTATION

100 North Senate Avenue
Room N642
Indianapolis, Indiana 46204

PHONE: (317) 234-5168

Eric Holcomb, Governor
Joe McGuinness, Commissioner

June 11, 2020

This letter was sent to the listed parties.

RE: US 31 Small Town Pavement Replacement, 1.53 miles N of SR 60 (Foothill Rd) to 3.28 miles N of SR 60 (Des. No. 1700111, DHPA No. 24727); Clark County, IN

Dear Consulting Party,

The Indiana Department of Transportation (INDOT)-Seymour District, with funding from the Federal Highway Administration, proposes to proceed with improvement to US 31 in the Town of Sellersburg and the unincorporated community of Speed in Clark County, Des. No. 1700111.

This letter is part of the Section 106 review process for this project. Section 106 of the National Historic Preservation Act requires federal agencies to take into account the effects of their undertakings on historic and archaeological properties. We are requesting comments from you regarding the possible effects of this project. Please use the above Des. Number and project description in your reply and your comments will be incorporated into the formal environmental study.

A Section 106 early coordination letter was distributed on November 26, 2019. In addition, a letter distributed on February 25, 2020 notified consulting parties that a historic property report and archaeology report were available for review and comment.

The proposed undertaking is on US 31 between Foothill Road and the Silver Creek Bridge in Clark County, Indiana. The proposed project is approximately 1.75 miles long. It is within Silver Creek Township, USGS Speed, Indiana Quadrangle, in Sections 110, 111, and 130.

The need for the project is derived from the deteriorated conditions along US 31. Portions of the corridor exhibit significant cracking in the asphalt or concrete surface. Existing curbs are buried and deteriorating. Drainage is insufficient to handle stormwater. The pedestrian facilities are not Americans with Disabilities Act (ADA)-compliant, and in some stretches are nonexistent. According to INDOT data, in the period between 2010 and 2018 there were 463 crashes within the project area, or an average of 56 per year. About 63% were rear-end crashes, which are associated with signalized intersections, congested traffic conditions, and driver inattention. This problem is particularly apparent at the intersection of US 31 and CR 403 (Old SR 403). Finally, the West Clark Community Schools Corp. has expressed safety concerns about the mid-block pedestrian crossing near the entrance to Silver Creek Elementary and Silver Creek Middle Schools.

The purpose of this project is to extend the functional life of the existing pavement, improve drainage, provide ADA-compliant pedestrian facilities, and improve safety along US 31 between Foothill Road and the Silver Creek Bridge by 2022.

The project proposes the following:

From Foothill Road to approximately Triangle Drive:

- Mill and repave US 31.

US 31/SR 311/Prather Lane Intersection:

- Full-depth pavement replacement with underdrains;
- Replace non-ADA-compliant curb ramps;
- Replace traffic signal;
- Minor pavement widening to adjust the lane configuration on the southwest-bound approach of US 31 to provide a shared through/right-turn lane;
- Incidental storm drainage improvements.

Approximately Bucheit Street to CR 403 (Old SR 403):

- Full-depth pavement replacement with underdrains;
- Install new curb inlets and storm sewer;
- Replace non-ADA-compliant curb ramps;
- Replace traffic signal at Utica Street intersection;
- Add high visibility pavement markings and signage and a rapid rectangular flashing beacon (RRFB) or pedestrian hybrid beacon (HAWK) to the crosswalk at the US 31/St. Paul Street intersection;
- Improve visibility of mid-block crosswalk near Sellersburg Library with high visibility pavement markings and signage, RRFB, or HAWK;
- Remove mid-block crosswalk near Silver Creek Elementary and Silver Creek Middle School entrances and relocate to US 31/CR 403 (Old SR 403) intersection with ADA-compliant pushbuttons and pedestrian countdown heads. The 6 foot-wide concrete sidewalk on the east side of US 31 would be extended north to the CR 403 (Old SR 403) intersection in order to add a right turn lane from southbound CR 403 (Old SR 403) to northbound US 31 and extending from US 31 to the L&I Railroad tracks. The existing lane would become a left-turn lane;
- Replace traffic signal at CR 403 (Old SR 403) intersection;
- Revise pavement markings to provide 12-foot wide two-way left-turn lane (TWLTL) from Utica Street to Silver Creek Elementary and Silver Creek Middle School entrances.

Approximately CR 403 (Old SR 403) to the Silver Creek Bridge:

- Mill and repave US 31;
- Replace non-compliant curb ramps;
- Replace segments of sidewalk disturbed by project;
- Replace inlet castings and adjust to grade;
- Add curb inlets and connect to existing storm sewer.

Approximately 3.0 acres of permanent right-of-way (ROW) acquisition for road construction, traffic signal modification, and curb and sidewalk replacement is expected. Approximately 2.0 acres of temporary ROW acquisition would be anticipated. Segments 1 and 4 would be constructed under traffic. Segments 2 and 3 would require and a detour, likely utilizing State Road 60, I-65, and Blue Lick Road. During closure of Segments 2 and 3, access to local residences and businesses would be provided using a phased construction plan which maintains a single, one-way travel lane between local intersecting streets.

Shrewsberry & Associates is under contract with INDOT to advance the environmental documentation for the referenced project. Butler, Fairman, & Seufert, Inc. (BF&S) has been subcontracted to complete the environmental documentation, including the Section 106 documentation, for the project.

In accordance with 36 CFR 800.2 (c), you were invited to become a consulting party as part of the Section 106 process, or you are hereby invited to become a consulting party as part of the Section 106 process. Entities that have previously accepted consulting party status are identified in the attached list.

The Section 106 process involves efforts to identify historic properties potentially affected by the undertaking, to assess the undertaking's effects and to seek ways to avoid, minimize, or mitigate any adverse effects on historic properties. For more information regarding the protection of historic resources, please see the Advisory Council on Historic Preservation's guide: *Protecting Historic Properties: A Citizen's Guide to Section 106 Review* available online at <https://www.achp.gov/sites/default/files/documents/2017-01/CitizenGuide.pdf>.

The Area of Potential Effects (APE) is the area in which the proposed project may cause alterations in the character or use of historic resources. The APE contains no resources listed in the National Register of Historic Places (NRHP).

The Indiana State Historic Preservation Officer (SHPO) responded to the historic property report and archaeological report in a letter dated March 31, 2020. The SHPO concurred with the results of the archaeological report. The SHPO also objected to the recommended NRHP-eligibility of the bridge carrying the Southern Indiana Railway over Creek Drive and Muddy Fork (IHSSI #019-604-52011), stating, in part, "*It does not appear to be a significant example of bridge design nor does it serve a locally significant role in transportation.*" INDOT has elected to defer to the SHPO's determinations.

The APE includes two NRHP-eligible properties, the **Dr. Q. Robert Hauss House/ IHSSI #019-604-51035 (227 N. New Albany Street)** and **IHSSI #019-604-51008 (479 N. Indiana Avenue)**. These properties are eligible for the National Register under Criterion C for architectural significance. The US 31 Small Town Pavement Replacement Project would have "No Adverse Effect" on these properties.

According to 36 CFR § 800.5(a)(1), an adverse effect is found in the Section 106 process "when an undertaking may alter, directly or indirectly, any of the characteristics of a historic property that qualify the property for inclusion in the National Register in a manner that would diminish the integrity of the property's location, design, setting, materials, workmanship, feeling, or association. Adverse effects may include reasonably foreseeable effects caused by the undertaking that may occur later in time, be farther removed in distance or be cumulative." Per 36 CFR § 800.5(a)(2), examples of adverse effect include but are not limited to:

- (i) Physical destruction of or damage to all or part of a property;
- (ii) Alteration of a property, including restoration, rehabilitation, repair, maintenance, stabilization, hazardous material remediation, and provision of handicapped access not consistent with the Secretary's Standards for the Treatment of Historic Properties (36 CFR part 68) and applicable guidelines;
- (iii) Removal of the property from its historic location;
- (iv) Change of the character of the property's use or physical features within the property's setting that contribute to its historic significance;
- (v) Introduction of visual, atmospheric, or audible elements that diminish the integrity of the property's significant historic features;
- (vi) Neglect of a property which causes its deterioration, except where such neglect and deterioration are recognized qualities of a property of religious and cultural significance to an Indian tribe or Native Hawaiian organization; and
- (vii) Transfer, lease, or sale of a property out of Federal ownership or control without adequate and legally enforceable restrictions or conditions to ensure long-term preservation of the property's historic significance.

The project would include a full-depth replacement of US 31 at its existing width adjacent to the Dr. Q. Robert Hauss House/IHSSI #019-604-51035 and IHSSI #019-604-51008. New underdrains would be installed. The concrete curb would be replaced. Pavement markings would be revised to provide a 12-foot wide TWLTL on US 31 adjacent to the properties. Additional project elements in the vicinity of each property are:

- Dr. Q. Robert Hauss House/IHSSI #019-604-51035: The curb ramps on the south side of the intersection with St. Paul Street would be replaced within the existing ROW. The 6-foot wide concrete sidewalk on the west side of US 31, across from the Dr. Q. Robert Hauss House/IHSSI #019-604-51035, would be replaced. High visibility pavement markings and signage and an RRFB HAWK would be added to the crosswalk at the US 31/St. Paul Street intersection. Temporary ROW acquisition from the Dr. Q. Robert Hauss House/IHSSI #019-604-51035 would be required for reconstruction of the driveway at the south end of the property (Sheets 30-31).
- IHSSI #019-604-51008: In front of the property, a TWLTL would be added to US 31 within the existing street footprint. The 4-foot wide concrete curb ramps adjacent to the southeast side of the property would be replaced within their existing footprint. The curb ramps on the commercial drive north of IHSSI #019-604-51008 would also be replaced (Sheet 36).

Maintenance of traffic (MOT) would require closure of US 31 adjacent to these properties but would maintain access and would not result in further effects. The project would not change of the character of the properties' use or physical features within the properties' settings that contribute to their historic significance. The project area is in an urban setting with existing street lighting. The project would not introduce atmospheric, or audible elements that diminish the integrity of the properties' significant historic features. Therefore, this project is not anticipated to diminish the integrity of the Dr. Q. Robert Hauss House/IHSSI #019-604-51035 or IHSSI #019-604-51008.

The Effects Letter and attachments are also available for review in IN SCOPE at <http://erms.indot.in.gov/Section106Documents/> (the Des. No. is the most efficient search term, once in IN SCOPE). You are invited to review these documents and to respond with comments on any historic resource impacts incurred as a result of this project so that an environmental report can be completed. We also welcome your related opinions and other input to be considered in the preparation of the environmental document. If you prefer a hard-copy of this material, please respond to this email with your request within seven (7) days.

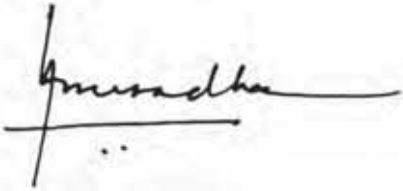
Please review the information and comment within thirty (30) calendar days of receipt. If we do not receive your response in the time allotted, the project will proceed consistent with the proposed design.

All future responses regarding the proposed project should be forwarded to BF&S at the following address:

Elizabeth Biggio
 Architectural Historian II
 Butler, Fairman, & Seufert, Inc.
 8450 Westfield Boulevard, Suite 300
 Indianapolis, IN 46240
 ebiggio@bfsengr.com

Tribal contacts may contact Shaun Miller at smiller@indot.in.gov or 317-233-6795 or Michelle Allen at FHWA at michelle.allen@dot.gov or 317-226-7344.

Sincerely,



Anuradha V. Kumar, Manager
 Cultural Resources Office
 Environmental Services

Enclosures:

Project Area Map
Preliminary plans

See Appendix B

Distribution List:

Indiana State Historic Preservation Officer
Miami Tribe of Oklahoma
Eastern Shawnee Tribe of Oklahoma
Peoria Tribe of Indians of Oklahoma
Pokagon Band of Potawatomi Indians
United Keetoowah Band of Cherokee Indians
Delaware Tribe of Indians

Elizabet Biggio

From: Elizabet Biggio
Sent: Thursday, June 11, 2020 9:44 AM
To: Kauffmann, Danielle; 'Tharp, Wade'
Cc: Alexander, Kelyn; Branigin, Susan; Anuradha Kumar; Miller, Shaun (INDOT); 'Angela DeWees'
Subject: FHWA Project: Des. No. 1700111; US 31 Small Town Pavement Replacement, 1.53 miles N of SR 60 (Foothill Rd) to 3.28 miles N of SR 60, Clark County, Indiana

Categories: Filed by Newforma

Des. No.: 1700111

Project Description: Pavement Replacement, Drainage Improvements

Location: City of Sellersburg and community of Speed, Clark County, IN

The Indiana Department of Transportation, with funding from the Federal Highway Administration, proposes to proceed with the US 31 Pavement Replacement project, including drainage and pedestrian safety improvements (Des. No. 1700111). The early coordination letter for this project was originally sent on November 26, 2019. Additionally, a letter distributed on February 25, 2020 notified consulting parties that a historic property report and archaeology report were available for review and comment. As part of Section 106 of the National Historic Preservation Act, an Effects Letter has been prepared and is ready for review and comment by consulting parties.

Please review this documentation located in IN SCOPE at <http://erms.indot.in.gov/Section106Documents/> (the Des. No. is the most efficient search term, once in IN SCOPE), and respond with any comments that you may have. If a hard copy of the materials is needed, please respond to this email with your request as soon as you can.

Consulting parties have thirty (30) calendar days from receipt of this information to review and provide comment. Tribal consulting parties may enter the process at any time and are encouraged to respond to this notification with any comments or concerns at their earliest convenience. Tribal contacts may contact Shaun Miller at smiller@indot.in.gov or 317-233-6795 or Michelle Allen at FHWA at michelle.allen@dot.gov or 317-226-7344.

Thank you in advance for your input,

Elizabet Biggio
Architectural Historian

Butler, Fairman & Seufert, Inc.
8450 Westfield Blvd., Suite 300 | Indianapolis, IN 46240-8302 |
p 317-713-4615 | f 317-713-4616
EBiggio@bfsengr.com | www.BFSEngr.com



CONFIDENTIALITY NOTICE: This Email and any attachments are confidential and may be protected by legal privilege. If you are not the intended recipient, be aware that any disclosure, copying, distribution, or use of this Email or any attachment is prohibited. If you have received this Email in error, please notify us immediately by returning it to the sender and delete this copy from your system. Thank you. Butler, Fairman & Seufert, Inc.

Elizabeth Biggio

From: Alexander, Kelyn <KAlexander3@indot.IN.gov>
Sent: Thursday, June 11, 2020 9:53 AM
To: thpo@estoo.net; Diane Hunter; lpappenfort@peoriatribe.com;
Matthew.Bussler@pokagonband-nsn.gov; wwarrior@ukb-nsn.gov;
lheady@delawaretribe.org
Cc: Elizabeth Biggio; Miller, Shaun (INDOT); Allen, Michelle (FHWA); Summers, Terry
Subject: FHWA Project: Des. No. 1700111; US 31 Small Town Pavement Replacement, 1.53 miles N of SR 60 (Foothill Rd) to 3.28 miles N of SR 60, Clark County, Indiana

Des. No.: 1700111

Project Description: Pavement Replacement, Drainage Improvements
Location: City of Sellersburg and community of Speed, Clark County, IN

The Indiana Department of Transportation, with funding from the Federal Highway Administration, proposes to proceed with the US 31 Pavement Replacement project, including drainage and pedestrian safety improvements (Des. No. 1700111). The early coordination letter for this project was originally sent on November 26, 2019. Additionally, a letter distributed on February 25, 2020 notified consulting parties that a historic property report and archaeology report were available for review and comment. As part of Section 106 of the National Historic Preservation Act, an Effects Letter has been prepared and is ready for review and comment by consulting parties.

Please review this documentation located in IN SCOPE at <http://erms.indot.in.gov/Section106Documents/> (the Des. No. is the most efficient search term, once in IN SCOPE), and respond with any comments that you may have. If a hard copy of the materials is needed, please respond to this email with your request as soon as you can.

Consulting parties have thirty (30) calendar days from receipt of this information to review and provide comment. Tribal consulting parties may enter the process at any time and are encouraged to respond to this notification with any comments or concerns at their earliest convenience. Tribal contacts may contact Shaun Miller at smiller@indot.in.gov or 317-233-6795 or Michelle Allen at FHWA at michelle.allen@dot.gov or 317-226-7344.

Thank you in advance for your input,

Kelyn Alexander

Historian

Cultural Resources Office

Environmental Services

100 N. Senate Ave., Room N642-ES

Indianapolis, IN 46204

Office: (317) 234-4147

Email: kalexander3@indot.in.gov

**** Historic Property Report (HPR) guidelines can be found [here](#)**

Division of Historic Preservation & Archaeology · 402 W. Washington Street, W274 · Indianapolis, IN 46204-2739
Phone 317-232-1646 · Fax 317-232-0693 · dhpa@dnr.IN.gov · www.IN.gov/dnr/historic



June 29, 2020

Elizabet Biggio
Butler, Fairman, & Seufert, Inc.
8450 Westfield Boulevard, Suite 300
Indianapolis, Indiana 46240

Federal Agency: Indiana Department of Transportation (“INDOT”),
on behalf of Federal Highway Administration, Indiana Division (“FHWA”)

Re: Effects letter for the US 31 pavement replacement project between Foothill Road and the Silver
Creek Bridge in Sellersburg, Silver Creek Township, Clark County, Indiana (Des. No. 1700111;
DHPA No. 24727)

Dear Ms. Biggio:

Pursuant to Section 106 of the National Historic Preservation Act of 1966, as amended (54 U.S.C. § 306108), implementing regulations at 36 C.F.R. Part 800, and the “Programmatic Agreement (PA) Among the Federal Highway Administration, the Indiana Department of Transportation, the Advisory Council on Historic Preservation and the Indiana State Historic Preservation Officer Regarding the Implementation of the Federal Aid Highway Program in the State of Indiana,” the staff of the Indiana State Historic Preservation Officer (“Indiana SHPO” or “INDNR-DHPA”) has reviewed the above-referenced effects letter, dated June 11, 2020, and received by our office the same day.

We agree with the opinions expressed in the June 11 effects letter that the Dr. Q. Robert Hauss House (Indiana Historic Sites and Structures Inventory [“IHSSI”] #019-604-51035) and the house at 479 N. Indiana Avenue (IHSSI #019-604-51008), which are eligible for inclusion in the National Register of Historic Places (“NRHP”), will not be adversely affected by this project.

As previously indicated, based on the submitted information and the documentation available to the staff of the Indiana SHPO, we have no identified any currently known archaeological resources listed in or eligible for inclusion in the NRHP within the proposed project area; and we concur with the opinion of the archaeologist that no further archaeological investigations appears necessary at the proposed project area.

Furthermore, because portions of the proposed project area lie 100 feet within portions of Sellersburg Cemetery (CR-10-118), a cemetery development plan will be necessary under IC 14-21-1-26.5. The aforementioned cemetery must be avoided by all project activities, and provisions of relevant state statutes regarding cemeteries (including IC 14-21-1 and IC 23-14) must be adhered to. Please also be aware of Indiana Code 23-13-33-1 and Indiana Code 23-14-44-2, regarding restrictions on roads and utility construction in cemeteries.

If any prehistoric or historic archaeological artifacts or human remains are uncovered during construction, demolition, or earthmoving activities, state law (Indiana Code 14-21-1-27 and -29) requires that the discovery be reported to the INDNR-DHPA within two (2) business days. In that event, please call (317) 232-1646. Be advised that adherence to Indiana Code 14-21-1-27 and -29 does not obviate the need to adhere to applicable federal statutes and regulations, including but not limited to 36 C.F.R. Part 800.

Unless another consulting party expresses a different opinion about this project’s effects, it might now be appropriate to ask INDOT for a finding.

The archaeological reviewer for this project on the Indiana SHPO staff is Wade T. Tharp and the structures reviewer is Danielle Kauffmann. However, if you have questions about our comments or about the review process, please contact initially the INDOT Cultural Resource Office staff members assigned to this project.

In all future correspondence regarding the US 31 pavement replacement project between Foothill Road and the Silver Creek Bridge in Sellersburg, Silver Creek Township, Clark County, (Des. No. 1700111), please continue to refer to DHPA No. 24727.

Very truly yours,



Beth K. McCord
Deputy State Historic Preservation Officer

BKM:DMK:dmk

emc: Erica Tait, FHWA
Anuradha Kumar, INDOT
Susan Branigin, INDOT
Shaun Miller, INDOT
Shirley Clark, INDOT
Elizabet Biggio, Butler, Fairman, & Seufert, Inc.
Miami Tribe of Oklahoma
Eastern Shawnee Tribe of Oklahoma
Peoria Tribe of Indiana of Oklahoma
Pokagon Band of Potawatomi Indians
United Keetoowah Band of Cherokee Indians
Delaware Tribe of Indians
Wade Tharp, INDNR-DHPA
Danielle Kauffmann, INDNR-DHPA

Elizabeth Biggio

From: Alexander, Kelyn <KAlexander3@indot.IN.gov>
Sent: Thursday, November 12, 2020 2:40 PM
To: Elizabeth Biggio
Cc: Branigin, Susan; Kumar, Anuradha; Miller, Shaun (INDOT); Moffatt, Charles D; Summers, Terry
Subject: RE: US 31 Sellersburg-Des1700111- INDOT Project - 800.11 - SIGNED

Elizabeth,

Thank you for sending in the revised 800.11. The document has been signed and can be "checked-in" to IN SCOPE. Please ensure that the IN SCOPE comment deadline allows for the mandatory 30-days. This period begins with the email to consulting parties, so INDOT-CRO recommends allowing for a few days grace period in case of delays between the document's "check-in" on IN SCOPE and the email notification to consulting parties.

I'm using Adobe Acrobat.

You can view "US 31 Sellersburg_Des1700111_800.11finding_2020-11-12signed.pdf" at:

<https://documentcloud.adobe.com/link/track?uri=urn:aaid:scds:US:ba7f58e4-851a-48bf-bb90-744981d8c2e1>

Once the documents are released, please send a hard-copy to the SHPO** and email the non-tribal consulting parties to notify them that the Finding has been posted and is ready for their review using the Finding/800.11 consultation email template we previously approved. Please cc me on that email as well. When we receive that email, we will notify the Tribes.

**In addition to the hard copies SHPO requires, they have also requested during this time for PDF copies of all documents along with the Review Request Submittal form be sent to Miriam Burkett at mburkett@dnr.in.gov. They still require hard copies to be submitted via mail as usual.

When the email notification to consulting parties is sent, please check IN SCOPE and verify that the document is available for public viewing and that all of the information is correct. In particular, ensure that the project name is consistent with other project documents. If any information needs to be corrected, consultants should contact INDOT-CRO.

Thanks again, and if you have any questions or comments, don't hesitate to contact me.

Kelyn Alexander

Historian

Cultural Resources Office

Environmental Services

100 N. Senate Ave., Room N642-ES

Indianapolis, IN 46204

Office: (317) 519-7759

Email: kalexander3@indot.in.gov

**** Historic Property Report (HPR) guidelines can be found [here](#)**

From: Alexander, Kelyn

Sent: Thursday, November 12, 2020 1:15 PM

Elizabeth Biggio

From: Elizabeth Biggio
Sent: Friday, November 13, 2020 8:37 AM
To: Kauffmann, Danielle; 'Tharp, Wade'
Cc: Alexander, Kelyn; SBranchin@indot.IN.gov; Anuradha Kumar; Miller, Shaun (INDOT); 'Angela DeWees'
Subject: FHWA Project: Des. No. 1700111; US 31 Small Town Pavement Replacement, 1.53 miles N of SR 60 (Foothill Rd) to 3.28 miles N of SR 60, Clark County, Indiana

Categories: Filed by Newforma

Des. No.: 1700111

Project Description: Pavement Replacement, Drainage Improvements

Location: City of Sellersburg and community of Speed, Clark County, IN

The Indiana Department of Transportation, with funding from the Federal Highway Administration, proposes to proceed with the US 31 Pavement Replacement project, including drainage and pedestrian safety improvements (Des. No. 1700111). The early coordination letter for this project was originally sent on November 26, 2019. Additionally, a letter distributed on February 25, 2020 notified consulting parties that a historic property report and archaeology report were available for review and comment. An email notification of the effects letter was sent on June 11, 2020.

INDOT, on behalf of FHWA has signed a determination of "No Adverse Effect" for this Section 106 undertaking. In accordance with 36 CFR 800.4(d), you and the other consulting parties that responded to the early coordination letter are being provided the documentation for this finding. You can view the determination of "No Adverse Effect" electronically by accessing INDOT's Section 106 document posting website IN SCOPE at <http://erms.indot.in.gov/Section106Documents/> (the Des. No. is the most efficient search term, once in IN SCOPE). If a hard copy of the materials is needed, please respond to this email with your request as soon as you can.

Consulting parties have thirty (30) calendar days from receipt of this information to review and provide comment. Tribal consulting parties may enter the process at any time and are encouraged to respond to this notification with any comments or concerns at their earliest convenience. Tribal contacts may contact Shaun Miller at smiller@indot.in.gov or 317-233-6795 or Michelle Allen at FHWA at michelle.allen@dot.gov or 317-226-7344.

Thank you in advance for your input,

Elizabeth Biggio
Architectural Historian

Butler, Fairman & Seufert, Inc.
8450 Westfield Blvd., Suite 300 | Indianapolis, IN 46240-8302 |
p 317-713-4615 | f 317-713-4616
EBiggio@bfsengr.com | www.BFSEngr.com



CONFIDENTIALITY NOTICE: This Email and any attachments are confidential and may be protected by legal privilege. If you are not the intended recipient,

Elizabeth Biggio

From: Miller, Shaun (INDOT) <smiller@indot.IN.gov>
Sent: Friday, November 13, 2020 8:43 AM
To: Diane Hunter
Cc: Alexander, Kelyn; Elizabeth Biggio; Allen, Michelle (FHWA)
Subject: FW: FHWA Project: Des. No. 1700111; US 31 Small Town Pavement Replacement, 1.53 miles N of SR 60 (Foothill Rd) to 3.28 miles N of SR 60, Clark County, Indiana

Des. No.: 1700111

Project Description: Pavement Replacement, Drainage Improvements

Location: City of Sellersburg and community of Speed, Clark County, IN

The Indiana Department of Transportation, with funding from the Federal Highway Administration, proposes to proceed with the US 31 Pavement Replacement project, including drainage and pedestrian safety improvements (Des. No. 1700111). The early coordination letter for this project was originally sent on November 26, 2019. Additionally, a letter distributed on February 25, 2020 notified consulting parties that a historic property report and archaeology report were available for review and comment. An email notification of the effects letter was sent on June 11, 2020.

INDOT, on behalf of FHWA has signed a determination of "No Adverse Effect" for this Section 106 undertaking. In accordance with 36 CFR 800.4(d), you and the other consulting parties that responded to the early coordination letter are being provided the documentation for this finding. You can view the determination of "No Adverse Effect" electronically by accessing INDOT's Section 106 document posting website IN SCOPE at <http://erms.indot.in.gov/Section106Documents/> (the Des. No. is the most efficient search term, once in IN SCOPE). If a hard copy of the materials is needed, please respond to this email with your request as soon as you can.

Consulting parties have thirty (30) calendar days from receipt of this information to review and provide comment. Tribal consulting parties may enter the process at any time and are encouraged to respond to this notification with any comments or concerns at their earliest convenience. Tribal contacts may contact Shaun Miller at smiller@indot.in.gov or 317-233-6795 or Michelle Allen at FHWA at michelle.allen@dot.gov or 317-226-7344.

Thank you in advance for your input,

Shaun Miller
INDOT, Cultural Resources Office
Archaeology Team Lead
(317)416-0876

Division of Historic Preservation & Archaeology · 402 W. Washington Street, W274 · Indianapolis, IN 46204-2739
Phone 317-232-1646 · Fax 317-232-0693 · dhpa@dnr.IN.gov · www.IN.gov/dnr/historic



November 30, 2020

Elizabet Biggio
Butler, Fairman, & Seufert, Inc.
8450 Westfield Boulevard, Suite 300
Indianapolis, Indiana 46240

Federal Agency: Indiana Department of Transportation (“INDOT”),
on behalf of Federal Highway Administration, Indiana Division (“FHWA”)

Re: Indiana Department of Transportation’s finding of “no adverse effect” on behalf of the Federal Highway Administration for the US 31 pavement replacement between Foothill Road and the Silver Creek Bridge, Silver Creek Township, Clark County, Indiana (Des. No. 1700111; DHPA No. 24727)

Dear Ms. Biggio:

Pursuant to Section 106 of the National Historic Preservation Act of 1966, as amended (54 U.S.C. § 306108), implementing regulations at 36 C.F.R. Part 800, and the “Programmatic Agreement (PA) Among the Federal Highway Administration, the Indiana Department of Transportation, the Advisory Council on Historic Preservation and the Indiana State Historic Preservation Officer Regarding the Implementation of the Federal Aid Highway Program in the State of Indiana,” the staff of the Indiana State Historic Preservation Officer (“Indiana SHPO”) has reviewed your November 13, 2020 submission which enclosed INDOT’s finding and documentation for the aforementioned project, received by our office the same day.

As previously indicated, we agree that the Dr. Q. Robert Hauss House (Indiana Historic Sites and Structures Inventory [“IHSSI”] #019-604-51035) and the House at 479 N. Indiana Avenue (IHSSI #019-604-51008) are both eligible for inclusion in the National Register of Historic Places (“NRHP”) and the only historic properties located within the project’s area of potential effects. We also agree that the proposed project will not adversely affect these historic properties and that the proposed scope of work constitutes a temporary occupancy for each and does not necessitate a Section 4(f) use.

Also as previously indicated, based on the submitted information and the documentation available to the staff of the Indiana SHPO, we have no identified any currently known archaeological resources listed in or eligible for inclusion in the NRHP within the proposed project area; and we concur with the opinion of the archaeologist that no further archaeological investigations appear necessary at the proposed project area.

Furthermore, because portions of the proposed project area lie 100 feet within portions of Sellersburg Cemetery (CR-10-118), a cemetery development plan will be necessary under IC 14-21-1-26.5. the aforementioned cemetery must be avoided by all project activities, and provisions of relevant state statutes regarding cemeteries (including IC 14-21-1 and IC 23-14) must be adhered to. Please also be aware of Indiana Code 23-13-33-1 and Indiana Code 23-14-44-2, regarding restrictions on roads and utility construction in cemeteries.

Accordingly, we concur with INDOT’s November 12, 2020, Section 106 finding of “No Adverse Effect” on behalf of FHWA for this federal undertaking.

If any prehistoric or historic archaeological artifacts or human remains are uncovered during construction, demolition, or earthmoving activities, state law (Indiana Code 14-21-1-27 and -29) requires that the discovery be reported to the DNR-DHPA within two (2) business days. In that event, please call (317) 232-1646. Be advised that adherence to Indiana Code 14-21-1-27

and -29 does not obviate the need to adhere to applicable federal statutes and regulations, including but not limited to 36 C.F.R. Part 800.

The archaeological reviewer for this project on the Indiana SHPO staff is Wade T. Tharp and the structures reviewer is Danielle Kauffmann. However, if you have questions about our comments or about the review process, please contact initially the INDOT Cultural Resource Office staff members assigned to this project.

In any future correspondence regarding the US 31 pavement replacement project between Foothill Road and the Silver Creek in Silver Creek Township, Clark County, (Des. No. 1700111), please continue to refer to DHPA No. 24727.

Very truly yours,



Beth K. McCord
Deputy State Historic Preservation Officer

BKM:DMK:dmk

emc: Erica Tait, FHWA
Anuradha Kumar, INDOT
Susan Branigin, INDOT
Shaun Miller, INDOT
Elizabet Biggio, Butler, Fairman, & Seufert, Inc.
Diane Hunter, Miami Tribe of Oklahoma
Wade T. Tharp, DNR-DHPA
Danielle Kauffmann, DNR-DHPA

Proof of Publication

Public Notice
Des. No. 1700111

The Indiana Department of Transportation (INDOT) is planning to undertake a road improvement project funded in part by the Federal Highway Administration (FHWA). The project is located along US 31 between Foothill Road and the Silver Creek Bridge in the Town of Sellersburg and the community of Speed, Clark County, Indiana.

Under the preferred alternative, the proposed project would involve milling and repaving US 31 or a full-depth replacement of US 31 with underdrains throughout the project area. Non-complaint Americans with Disabilities-Act (ADA) curb ramps will be replaced. The following elements will be included in some segments of the project: installation of a new storm sewer; traffic signal replacement; installation of pavement markings and signage for crosswalk visibility; crosswalk relocation; and the revision of pavement markings for two-way-left-turn lanes. Approximately 3.0 acres of permanent and 2.0 acres of temporary right-of-way (ROW) acquisition for road construction, traffic signal modification, and curb and sidewalk replacement is expected.

Properties listed in or eligible for the National Register of Historic Places (NRHP) located within the Area of Potential Effects (APE) include the Dr. Q. Robert Hauss House and 479 N. Indiana Avenue in Sellersburg. The proposed action impacts properties listed in or eligible for the NRHP. The INDOT, on behalf of the FHWA, has issued a "No Adverse Effect" finding for the project because the project will not diminish the integrity of the characteristics that qualify the historic properties within the APE for inclusion in the NRHP. In accordance with the National Historic Preservation Act, the views of the public are being sought regarding the effect of the proposed project on the historic elements as per 36 CFR 800.2(d), 800.3(e) and 800.6(a)(4). Pursuant to 36 CFR 800.4(d)(2), the documentation specified in 36 CFR 800.11(e) is available for inspection in the office of Butler, Fairman, & Seufert, Inc. Additionally, this documentation can be viewed electronically by accessing INDOT's Section 106 document posting website IN SCOPE at <http://erms.indot.in.gov/Section106Documents>. This documentation serves as the basis for the "No Adverse Effect" finding. The views of the public on this effect finding are being sought. Please reply with any comments to Elizabeth Biggio, Butler, Fairman, & Seufert, Inc., 9450 Westfield Blvd, Suite 300, Indianapolis, IN 46240, 317-713-4615, ebiggio@bfsengr.com no later than December 17, 2020.

In accordance with the "Americans with Disabilities Act", if you have a disability for which INDOT needs to provide accessibility to the document(s) such as interpreters or readers, please contact Terry Summers at 812-524-3749 or tsummers@indot.in.gov hspaxlp

STATE OF INDIANA COUNTY OF CLARK -SS

Theresa Wheatbrook on oath says that she is bookkeeper of NEWS AND TRIBUNE and in the employ of the publisher of

NEWS AND TRIBUNE,

a daily newspaper of general circulation printed and published in the city of Jeffersonville, Clark County, State of Indiana, and further says that the annexed advertisement was published in said paper for # (1) time(s) to-wit: In issue of said NEWS AND TRIBUNE

Dated: 11-17 2020

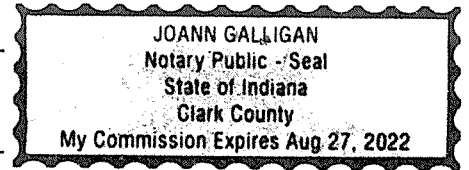
(X) Theresa Wheatbrook

STATE OF INDIANA COUNTY OF CLARK

Subscribed and sworn to before me this

19th day of November 2020

(X) Joann Galligan
Joann Galligan



Notary Public, Clark County, Indiana
(My Commission Expires August 27, 2022
Commission Number 655965

ID # 04-3314494

Appendix E

Red Flag Investigation



INDIANA DEPARTMENT OF TRANSPORTATION

Driving Indiana's Economic Growth

100 North Senate Avenue
Room N642
Indianapolis, Indiana 46204-2216 (317) 232-5348 FAX: (317) 233-4929

Eric Holcomb, Governor
Joe McGuinness, Commissioner

Date: July 25, 2018

To: Site Assessment and Management Unit
Environmental Services
Indiana Department of Transportation
100 N Senate Avenue, Room N642
Indianapolis, IN 46204

From: Ryan L. Scott
Butler, Fairman and Seufert, Inc.
8450 Westfield Boulevard, Suite 300
Indianapolis, IN 46240
rscott@bfsengr.com

Re: RED FLAG INVESTIGATION
DES # 1700111, State Project
US 31 Preventative Pavement Maintenance
1.53 miles North of SR 60 (Foothill Road) to 3.28 miles North of SR 60
Town of Sellersburg, Clark County, Indiana

PROJECT DESCRIPTION

Brief Description of Project: The project is a preventative pavement maintenance project located along US 31 between points 1.53 miles north of SR 60 (Foothill Road) and 3.28 miles north of SR 60. The project is also located in Section (CMG) 110, 111 and 130, Township 99, Range 99 on the USGS Speed, Indiana Quadrangle, Silver Creek Township, Clark County. The purpose of this project is to address the structural condition of the pavement on US 31. The need derives from the poor condition of the pavement. The pavement is experiencing functional and structural cracking.

The project proposes a variable depth milling and hot mix asphalt (HMA) overlay between Foothill Road and New Albany Street, reconstruction from New Albany Street to the north side of the intersection with Old State Road 311, improved intersection alignment north of the US 31/Old State Road 311 intersection, a 4-inch functional overlay from the end of the reconstruction to the intersection with Old State Road 403, and variable depth milling and HMA overlay from Old State Road 403 to Saint Joe Road. Additional project elements would include upgrade and repair of the existing stormwater facilities, casting replacement, and the installation of a new curb and gutter. Existing sidewalks would be upgraded. A left turn lane from southbound US 31 to Old State Road 403 would be added.

Bridge and/or Culvert Project: Yes No **Structure #**

If this is a bridge project, is the bridge Historical? Yes No , Select Non-Select

(Note: If the project involves a historical bridge, please include the bridge information in the Recommendations Section of the report).

Proposed right of way: Temporary # Acres N/A Permanent # Acres N/A

Type of excavation: Excavation will be required north of the US 31 / Old State Road 311 intersection for the purposes of constructing an improved intersection alignment. Depth of excavation is anticipated to range between 3-5 feet in this

area. Existing sidewalks along the project will be upgraded, which will involve excavation between 0.5 - 1 feet. Existing stormwater drainage pipes along the roadway will be repaired, and will require 3-5 feet of excavation.

Maintenance of traffic: It is anticipated that the project will be phased and constructed under traffic. It may be necessary to close traffic lanes periodically during construction, and utilize flag men and women to direct traffic around the construction area.

Work in waterway: Yes No **Above ordinary high water mark:** Yes No

State Project: **LPA:**

Any other factors influencing recommendations: The project description is subject to additional changes as preliminary design progresses.

INFRASTRUCTURE TABLE AND SUMMARY

Infrastructure			
Indicate the number of items of concern found within the 0.5 mile search radius. If there are no items, please indicate N/A:			
Religious Facilities	7*	Recreational Facilities	9
Airports ¹	1	Pipelines	1
Cemeteries	2	Railroads	12
Hospitals	N/A	Trails	1
Schools	5	Managed Lands	N/A

¹In order to complete the required airport review, a review of public airports within 3.8 miles (20,000 feet) is required.

Explanation:

Religious Facilities: There are five (5) religious facilities mapped, and two (2) religious facilities unmapped, within the 0.5 mile search radius. The nearest facilities, Speed Memorial Church, St. Paul’s Catholic Church and Grace Community Church, are adjacent to the project area. Traffic will be maintained throughout construction using lane closures, allowing for continued access. No impact is expected. Coordination with Speed Memorial Church, St. Paul’s Catholic Church and Grace Community Church will occur.

Recreational Facilities: There are nine (9) recreational facilities located within the 0.5 mile search radius. Two (2) recreational facilities are located immediately adjacent to the project area, one of which is St. Paul’s Catholic Elementary School, and the other is associated with the grouping of Silver Creek High School, Silver Creek Junior High, and Stout Elementary. Traffic will be maintained throughout construction using lane closures, allowing for continued access. Coordination with St. Paul Elementary School, Silver Creek High School, Silver Creek Junior High School and Stout Elementary School will occur.

Airports: Although not located within the 0.5 mile search radius, one (1) public airport, Clark Regional Airport, is located within 3.8 miles (20,000 feet) of the project area; therefore, early coordination with INDOT Aviation will occur.

Pipelines: There is one (1) pipeline segment located within the 0.5 mile search radius. The natural gas pipeline, associated with Indiana Gas Company, Inc., crosses the project area approximately 0.3 mile north of Old State Road 403. Coordination with INDOT Utilities and Railroads should occur.

Cemeteries: There are two (2) cemeteries located within the 0.5 mile search radius. The nearest cemetery, Sellersburg Cemetery, is adjacent to the project area. A Cemetery Development Plan may be required since this project is within 100 feet of the cemetery. Coordination with INDOT Cultural Resources is recommended.

Railroads: There are 12 railroad segments located within the 0.5 mile search radius. One railroad segment is located immediately east of the project area between roughly Old State Road 403 and the north project terminus. Coordination with INDOT Utilities and Railroads should occur.

Trails: There is one (1) trail segment located within the 0.5 mile search radius. Sellersburg Park Trail is located approximately 0.2 mile east of the project at its closest point. No impacts are expected.

Schools: There are five (5) schools located within the 0.5 mile search radius. St. Paul Elementary School is not mapped correctly and, like the other mapped schools, is actually located immediately west of US 31 near the center point of the overall project. Traffic will be maintained throughout construction using lane closures, allowing for continued access. Coordination with Silver Creek Primary School, St. Paul Elementary School, Silver Creek High School, Silver Creek Junior High School and Stout Elementary School will occur.

WATER RESOURCES TABLE AND SUMMARY

Water Resources			
Indicate the number of items of concern found within the 0.5 mile search radius. If there are no items, please indicate N/A:			
NWI - Points	N/A	Canal Routes - Historic	N/A
Karst Springs	N/A	NWI - Wetlands	42
Canal Structures – Historic	N/A	Lakes	14
NPS NRI Listed	N/A	Floodplain - DFIRM	5
NWI-Lines	44	Cave Entrance Density	N/A
IDEM 303d Listed Streams and Lakes (Impaired)	4	Sinkhole Areas	N/A
Rivers and Streams	16	Sinking-Stream Basins	N/A

Explanation:

NWI – Wetlands: There are 42 wetland polygons located within the 0.5 mile search radius. Wetlands are located on both sides of US 31 on the north side of Muddy Fork and are adjacent to the project area. A Waters of the US Report will be prepared and coordination with INDOT Ecology and Waterway Permitting will occur.

Lakes: There are 14 lake polygons located within the 0.5 mile search radius. The closest lake is located approximately 0.1 mile west of the project area. No impact is expected.

Floodplain – DFIRM: There are five (5) floodplain polygons located within the 0.5 mile search radius. The project area overlaps floodplain polygons at three (3) different locations. Coordination with INDOT Ecology and Waterway Permitting will occur.

NWI – Lines: There are 44 NWI line segments located within the 0.5 mile search radius. Three (3) NWI line segments are mapped within the project area. A Waters of the US Report will be prepared and coordination with INDOT Ecology and Waterway Permitting will occur.

IDEM 303d Listed Streams and Lakes (Impaired): There are a total of four (4) impaired stream segments, and no impaired lake polygons, located within the 0.5 search radius. The closest impaired stream segment is located near the north terminus of the project and is associated with Muddy Fork. Muddy Fork is listed for E. coli and Impaired Biotic Communities (IBC). No impact is expected.

Rivers and Streams: There are a total of sixteen (16) stream segments located within the 0.5 mile search radius. Three (3) stream segments are located within the project area, including Camp Run, an unnamed tributary to Silver Creek, and Muddy Fork. A Waters of the US Report will be prepared and coordination with INDOT Ecology and Waterway Permitting will occur.

URBANIZED AREA BOUNDARY SUMMARY

Urbanized Area Boundary (UAB): This project lies within the Louisville, KY-IN UAB. Post construction Storm Water Quality Best Management Practices (BMPs) may need to be considered. An early coordination letter with topographic and aerial maps showing the project area should be sent to the Town of Sellersburg MS4 Coordinator at 316 East Utica Street, Sellersburg, Indiana 47172.

MINING AND MINERAL EXPLORATION TABLE AND SUMMARY

Mining/Mineral Exploration			
Indicate the number of items of concern found within the 0.5 mile search radius. If there are no items, please indicate N/A:			
Petroleum Wells	1	Mineral Resources	3*
Mines – Surface	N/A	Mines – Underground	N/A

Explanation:

Petroleum Wells: There is one (1) petroleum well located within the 0.5 mile search radius. The petroleum well is located approximately 0.25 mile east of the project area. No impact is expected.

Mineral Resources: There is one (1) mineral resource facility mapped within the 0.5 mile search radius; however, there are two (2) other mineral resource sites that have property overlapping the 0.5 mile search radius: Hanson Aggregates Midwest, Inc., and Irving Materials, Inc. Essroc Cement Corp. is the closest facility, and is located immediately east of US 31 near the north project terminus. Due to the proposed MOT, which is anticipated to include lane closures, coordination with all three (3) facilities will occur.

HAZARDOUS MATERIAL CONCERNS TABLE AND SUMMARY

Hazardous Material Concerns			
Indicate the number of items of concern found within the 0.5 mile search radius. If there are no items, please indicate N/A:			
Superfund	N/A	Manufactured Gas Plant Sites	N/A
RCRA Generator/ TSD	2	Open Dump Waste Sites	N/A
RCRA Corrective Action Sites	N/A	Restricted Waste Sites	N/A
State Cleanup Sites	6	Waste Transfer Stations	N/A
Septage Waste Sites	N/A	Tire Waste Sites	1
Underground Storage Tank (UST) Sites	12	Confined Feeding Operations (CFO)	N/A
Voluntary Remediation Program	N/A	Brownfields	N/A
Construction Demolition Waste	N/A	Institutional Controls	1
Solid Waste Landfill	N/A	NPDES Facilities	1
Infectious/Medical Waste Sites	N/A	NPDES Pipe Locations	13
Leaking Underground Storage (LUST) Sites	13*	Notice of Contamination Sites	N/A

Explanation:

RCRA Generator / TSD: There are two (2) RCRA sites located within the 0.5 mile search radius.

- McKinley Cleaners, 113 E. Utica Street, AI# 890, according to the IDEM Virtual File Cabinet (VFC) the site was listed as a Small Quantity Generator in 2002. No impact is expected.

State Cleanup Sites: There are six (6) cleanup sites located within the 0.5 mile search radius. Four (4) of these sites are located within or immediately adjacent to the project area.

- Sellersburg Auto, 834 South Indiana, Incident # 9902068, according to the IDEM VFC, is a recorded cleanup site of resulting from waste oil tank removal in 1999. IDEM issued a No Further Action letter on November 1, 1999. No impact is expected.
- McKinley Cleaners, 113 E. Utica Street, AI# 890 was formerly the site of a dry cleaners, and is located approximately 0.02 mile east of the project area. According to the IDEM Virtual File Cabinet (VFC) McKinley Cleaners operated as a dry cleaners since at least 1955. Remediation and testing for groundwater impacts (tetrachloroethene and its degradation products) began in 2006 at the site and is ongoing. Although groundwater flow appears to be toward the southeast (away from the project area), there appear to be several utilities that run toward the project area along Utica Street which have the potential to impact the project area. Coordination with IDEM will occur.
- West Clark Community Schools, 495 Indiana Avenue, State Cleanup Site # 2002-04108, Agency ID No. 1920, was issued a Completion of Independent Closure Process from IDEM on October 10, 2003 for the removal of two (2) underground heating oil tanks, and removal and disposal of approximately 3,560.03 tons of impacted soil in 2002. All residual contamination is considered to be removed from the site. No impact is expected.
- Clark Community School Elementary, 206 N. Albany Street, AI#7134, according to the IDEM VFC, is a recorded state cleanup site related to a historically used heating oil UST. IDEM issued a No Further Action letter on January 23, 2007. No impact is expected.

Tire Waste Sites: There is one (1) tire waste site located within the 0.5 mile search radius.

- Sanders Booher Tire Center, 131 S. New Albany Street, Registration # 10-T-00129. According to the IDEM VFC, this site was registered with IDEM as a Waste Tire Transporter on February 7, 1995. No impact is expected.

UST Sites: There are 12 UST sites located within the 0.5 mile search radius. There is one (1) site that is located within or immediately adjacent to the project area.

- Five Star Food, 239 S. Indiana Avenue, AI# 3050. IDEM issued a No Further Action Approval Determination Pursuant to Remediation Closure Guide on August 18, 2014. Low levels of soil and groundwater contamination remain on the site. If excavation occurs in this area, it is likely that petroleum contamination will be encountered. Proper removal and disposal of soil and/or groundwater may be necessary.

Institutional Controls: There is one (1) Institutional Control site located within the 0.5 mile search radius.

Hesens Food Mart, 492 N. Indiana Avenue, AI #1773 was formerly the site of a gas station and is currently the site of Five Star Food, AI# 3050. According to the No Further Action (NFA) Determination Pursuant to Risk Integrated System of Closure (RISC) issued by IDEM on February 17, 2016, contamination remains in the area surrounding the site and exists in the current public ROW. No new ROW acquisition from this site is anticipated. If excavation occurs in this area, proper

removal and disposal of soil and/or groundwater will be necessary. An Environmental Restrictive Covenant (ERC) was placed on the property on October 31, 2014. Coordination will be conducted with IDEM before further site activities occur.

NPDES Facilities: There is one (1) NPDES Facility located within the 0.5 mile search radius. Essroc Cement Corp., 301 US 31, NPDES ID # IN0002071, is located immediately east of the project area. No impact is expected.

NPDES Pipe Locations: There are 13 NPDES pipes located within the 0.5 mile search radius. There are three (3) NPDES pipes located within or immediately adjacent to the project area. No impact is expected.

- A manhole cover servicing a Sellersburg Waste Water Treatment Plant (WWTP) pipe is located in the northwest quadrant of the US 31 / Foothill Road intersection. No impacts are expected.
- A manhole cover servicing a Sellersburg WWTP pipe is located in the southwest quadrant of the US 31 / Allhands Avenue intersection. No impacts are expected.
- A manhole cover servicing a Sellersburg WWTP pipe is located in the northeast quadrant of the US 31 / Spring Street intersection. No impacts are expected.

LUST Sites: There are 12 LUST sites located within the 0.5 mile search radius on the GIS layer, and one (1) additional site that was not mapped, but identified on the IDEM VFC layer. Nine (9) LUST sites are located either within or immediately adjacent to the project site.

- Johnson Oil Bigfoot #042, 604 S. Indiana Avenue, AI# 2525. IDEM issued a No Further Action Approval Determination Pursuant to Remediation Closure Guide on December 29, 2003. Low levels of soil and groundwater contamination remain on the site. If excavation occurs in this area, it is likely that petroleum contamination will be encountered. Proper removal and disposal of soil and/or groundwater may be necessary.
- Empire Gas Incorporated, 414 Popp Avenue, AI# 3751. This site was not mapped on the GIS layer, but was identified on the IDEM VFC layer. According to the IDEM VFC, no investigation has ever been conducted on this property. A Phase II Environmental Site Assessment is recommended prior to any subsurface work on this property or right-of-way acquisition. Coordination with IDEM will occur.
- Former Dairy Mart #349, 624 S. Indiana Avenue, AI# 2835 located on the northwest intersection of Foothill Road and US 31 (the icon is not mapped correctly). IDEM issued a No Further Action Approval Determination on April 21, 1998. Low levels of soil and groundwater contamination remain on the site. If excavation occurs in this area, it is likely that petroleum contamination will be encountered. Proper removal and disposal of soil and/or groundwater may be necessary.
- Dollar General Store, 475 N. Indiana Avenue, AI# 119573. According to the IDEM VFC, no investigation has ever been conducted on this property. A Phase II Environmental Site Assessment is recommended prior to any subsurface work on this property or right-of-way acquisition. Coordination with IDEM will occur.
- CarMerica, 105 Prather Lane, Facility ID 24667. According to the IDEM VFC, this site has a former diesel UST and used oil UST onsite. A Further Site Investigation (FSI) was completed on March 10, 2015, and indicates the soil contamination is contained to the site. No indication was given that USTs have been removed. If excavation occurs in this area, it is likely that petroleum contamination will be encountered. Proper removal and disposal of soil and/or groundwater may be necessary.

- Sellersburg Motors Inc., 392 S. Indiana Avenue, AI# 4996. IDEM issued a No Further Action Approval Determination Pursuant to Risk Integrated System of Closure on October 10, 2012. Low levels of soil and groundwater contamination remain on the site. If excavation occurs in this area, it is likely that petroleum contamination will be encountered. Proper removal and disposal of soil and/or groundwater may be necessary.
- Essroc Materials, Inc., 301 US 31 S., AI# 7158. IDEM issued a No Further Action Approval Determination Pursuant to Remediation Closure Guide on September 21, 2016. Low levels of soil and groundwater contamination remain on the site. If excavation occurs in this area, it is likely that petroleum contamination will be encountered. Proper removal and disposal of soil and/or groundwater may be necessary. An Environmental Restrictive Covenant (ERC) was placed on the property on August 8, 2016 for restricted land use, groundwater use and soil disturbance. Coordination will be conducted with IDEM before further site activities occur.
- Short Stop Food Mart #9, 492 S. Indiana Avenue, AI# 1773. See Institutional Controls Section’s discussion of Hesens Food Mart.
- Swifty Oil #141, 254 S. Indiana Avenue, AI# 15092. IDEM issued a No Further Action Approval Determination Pursuant to 2013 Remediation Closure Guide on September 9, 2014. Low levels of soil and groundwater contamination remain on the site. If excavation occurs in this area, it is likely that petroleum contamination will be encountered. Proper removal and disposal of soil and/or groundwater may be necessary.

ECOLOGICAL INFORMATION SUMMARY

The Clark County listing of the Indiana Natural Heritage Data Center information on endangered, threatened, or rare (ETR) species and high quality natural communities is attached with ETR species highlighted. A preliminary review of the Indiana Natural Heritage Database by INDOT Environmental Services did indicate the presence of endangered species. Coordination with USFWS and IDNR will occur.

A review of the USFWS database did not indicate the presence of endangered bat species in or within 0.5 mile of the project area. The range-wide programmatic consultation for the Indiana Bat and Northern Long-eared Bat will be completed according to “Using the USFWS’s IPaC System for Listed Bat Consultation for INDOT Projects”.

An inquiry using the USFWS Information for Planning and Consultation (IPaC) website did not indicate the presence of the federally endangered species, the Rusty Patched Bumble Bee, in or within 0.5 mile of the project area. No impact is expected.

RECOMMENDATIONS SECTION

INFRASTRUCTURE:

Schools: Traffic will be maintained throughout construction using lane closures, allowing for continued access. Coordination with Speed Memorial Church, St. Paul’s Catholic Church and Grace Community Church, St. Paul Elementary School, Silver Creek High School, Silver Creek Junior High School and Stout Elementary School will occur.

Airports: Clark Regional Airport is located within 3.8 miles (20,000 feet) of the project area; therefore, early coordination with INDOT Aviation will occur.

Pipelines: The natural gas pipeline associated with Indiana Gas Company, Inc. crosses the project area approximately 0.3 mile north of Old State Road 403. Coordination with INDOT Utilities and Railroads should occur.

Cemeteries: Sellersburg Cemetery is adjacent to the project area. A Cemetery Development Plan may be required since this project is within 100 feet of the cemetery. Coordination with INDOT Cultural Resources is recommended.

Railroads: One railroad segment is located immediately east of the project area between roughly Old State Road 403 and the north project terminus. Coordination with INDOT Utilities and Railroads should occur.

WATER RESOURCES: The presence of the following water resources will require the preparation of a Waters of the US Report and coordination with INDOT ES Ecology and Waterway Permitting:

Two (2) wetland polygons are adjacent to the project area.

The project is located within a floodplain.

Three (3) NWI –Lines are mapped within the project area.

Three (3) stream segments are located within the project area, including Camp Run, an unnamed tributary to Silver Creek, and Muddy Fork.

Muddy Fork is listed for E. coli and Impaired Biotic Communities (IBC). Workers who are working in or near water with E. coli should take care to wear appropriate PPE, observe proper hygiene procedures, including regular hand washing, and limit personal exposure.

URBANIZED AREA BOUNDARY:

This project lies within the Louisville, KY-IN UAB. Post construction Storm Water Quality Best Management Practices (BMPs) may need to be considered. An early coordination letter with topographic and aerial maps showing the project area should be sent to the Town of Sellersburg MS4 Coordinator at 316 East Utica Street, Sellersburg, Indiana 47172.

MINING/MINERAL EXPLORATION: N/A

HAZMAT CONCERNS:

UST Sites: One (1) UST site (Five Star Food, 239 S. Indiana Avenue, AI # 3050) is located adjacent to the project area. If excavation occurs in this area, it is likely that petroleum contamination will be encountered. Proper removal and disposal of soil and/or groundwater may be necessary. Coordination with IDEM will occur.

Institutional Controls: One (1) Institutional Control site (Hesens Food Mart, 492 N. Indiana Avenue, AI #1773) is located adjacent to the project area. If excavation occurs in this area, it is likely that petroleum contamination will be encountered. Proper removal and disposal of soil and/or groundwater may be necessary. Coordination with IDEM will occur.

LUST Sites:

- Johnson Oil Bigfoot #042, 604 S. Indiana Avenue, AI# 2525. IDEM issued a No Further Action Approval Determination Pursuant to Remediation Closure Guide on December 29, 2003. Low levels of soil and groundwater contamination remain on the site. If excavation occurs in this area, it is likely that petroleum contamination will be encountered. Proper removal and disposal of soil and/or groundwater may be necessary.
- Empire Gas Incorporated, 414 Popp Avenue, AI# 3751. According to the IDEM VFC, no investigation has ever been conducted on this property. A Phase II Environmental Site Assessment is recommended prior to any subsurface work on this property or right-of-way acquisition. Coordination with IDEM will occur.
- Former Dairy Mart #349, 624 S. Indiana Avenue, AI# 2835 Is located on the northwest intersection of Foothill Road and US 31 (the icon is not mapped correctly). IDEM issued a No Further Action Approval Determination on April 21, 1998. Low levels of soil and groundwater contamination remain on the site. If excavation occurs in this area, it is likely that petroleum contamination will be encountered. Proper removal and disposal of soil and/or groundwater may be necessary.

- Dollar General Store, 475 N. Indiana Avenue, AI# 119573. According to the IDEM VFC, no investigation has ever been conducted on this property. A Phase II Environmental Site Assessment is recommended prior to any subsurface work on this property or right-of-way acquisition. Coordination with IDEM will occur.
- CarMerica, 105 Prather Lane, Facility ID 24667. According to the IDEM VFC, this site has a former diesel UST and used oil UST onsite. A Further Site Investigation (FSI) was completed on March 10, 2015, and indicates the soil contamination is contained to the site. No indication was given that USTs have been removed. If excavation occurs in this area, it is likely that petroleum contamination will be encountered. Proper removal and disposal of soil and/or groundwater may be necessary.
- Sellersburg Motors Inc., 392 S. Indiana Avenue, AI# 4996. IDEM issued a No Further Action Approval Determination Pursuant to Risk Integrated System of Closure on October 10, 2012. Low levels of soil and groundwater contamination remain on the site. If excavation occurs in this area, it is likely that petroleum contamination will be encountered. Proper removal and disposal of soil and/or groundwater may be necessary.
- Essroc Materials, Inc., 301 US 31 S., AI# 7158. IDEM issued a No Further Action Approval Determination Pursuant to Remediation Closure Guide on September 21, 2016. Low levels of soil and groundwater contamination remain on the site. If excavation occurs in this area, it is likely that petroleum contamination will be encountered. Proper removal and disposal of soil and/or groundwater may be necessary. An Environmental Restrictive Covenant (ERC) was placed on the property on August 8, 2016 for restricted land use, groundwater use and soil disturbance. Coordination will be conducted with IDEM before further site activities occur.
- Short Stop Food Mart #9, 492 S. Indiana Avenue, AI# 1773. See Institutional Controls Section’s discussion of Hesens Food Mart.
- Swifty Oil #141, 254 S. Indiana Avenue, AI# 15092. IDEM issued a No Further Action Approval Determination Pursuant to 2013 Remediation Closure Guide on September 9, 2014. Low levels of soil and groundwater contamination remain on the site. If excavation occurs in this area, it is likely that petroleum contamination will be encountered. Proper removal and disposal of soil and/or groundwater may be necessary.

ECOLOGICAL INFORMATION: Coordination with USFWS and IDNR will occur. The range-wide programmatic consultation for the Indiana Bat and Northern Long-eared Bat will be completed according to “Using the USFWS’s IPaC System for Listed Bat Consultation for INDOT Projects”.

September 4, 2018

INDOT Environmental Services concurrence: Nicole Foley-Breting (Signature)

Prepared by:

Ryan L. Scott
 Director of Environmental Services
 Butler, Fairman and Seufert, Inc.

Graphics:

A map for each report section with a 0.5 mile search radius buffer around all project area(s) showing all items identified as possible items of concern is attached. If there is not a section map included, please change the YES to N/A:

SITE LOCATION: YES

INFRASTRUCTURE: YES

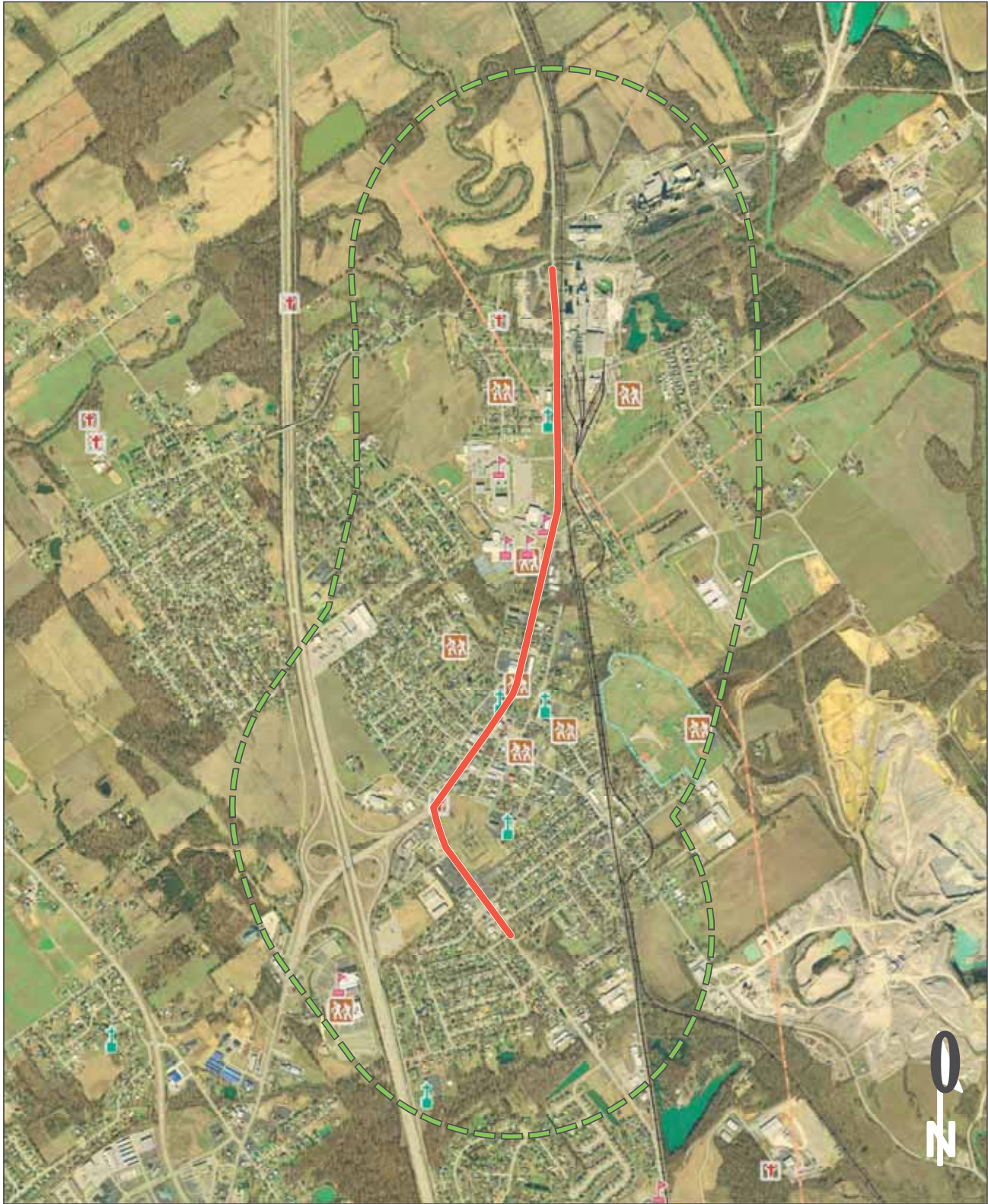
WATER RESOURCES: YES

URBANIZED AREA BOUNDARY: YES

MINING/MINERAL EXPLORATION: YES

HAZMAT CONCERNS: YES

Red Flag Investigation - Infrastructure
 US 31 Preventative Pavement Maintenance
 Des. No. 1700111
 Sellersburg, Clark County, IN

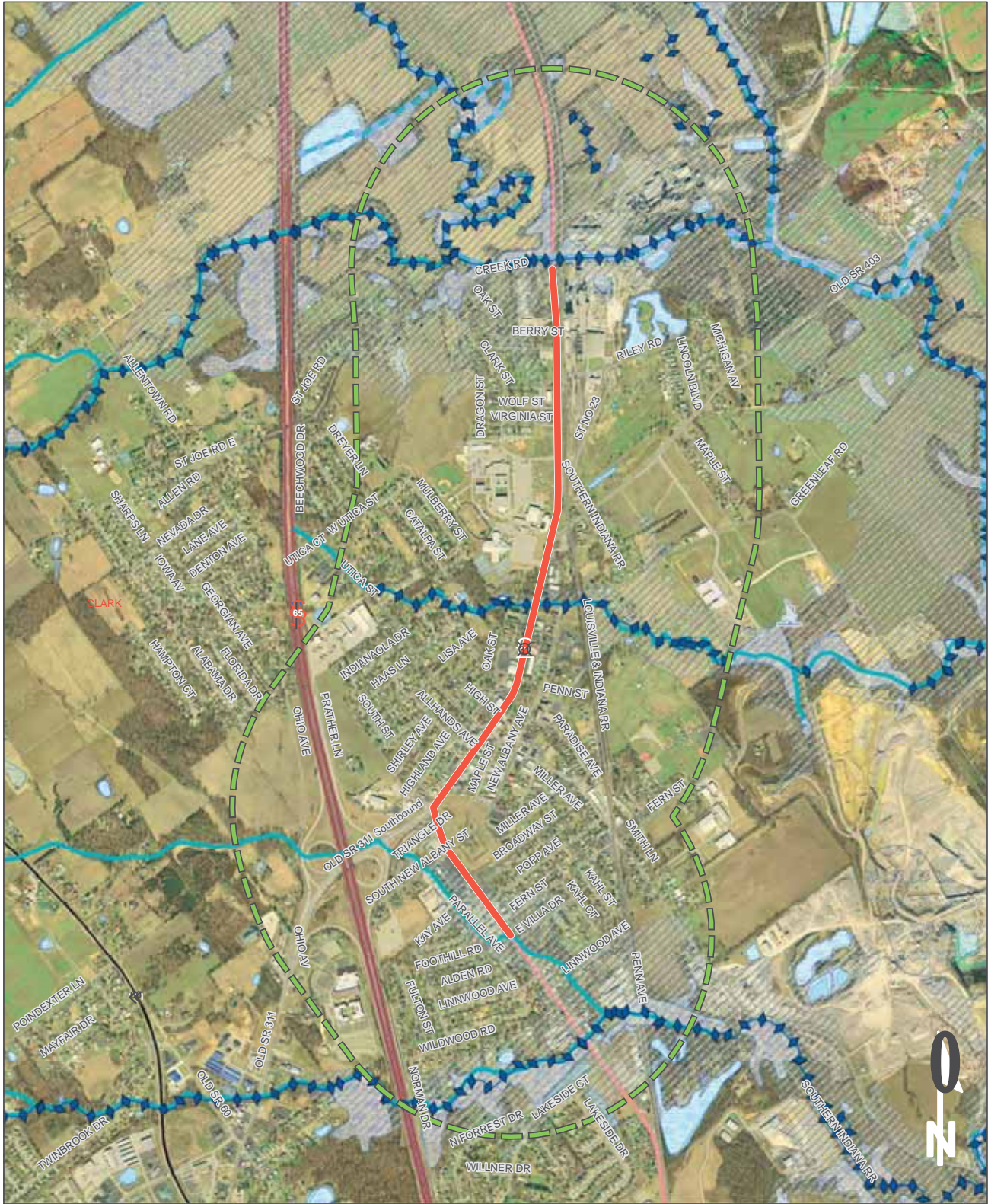


Sources:
Non Orthophotography
Data - Obtained from the State of Indiana Geographical Information Office Library
Orthophotography - Obtained from Indiana Map Framework Data (www.indianamap.org)
Map Projection: UTM Zone 16 N **Map Datum:** NAD83

This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.

	Religious Facility		Recreation Facility		Project Area
	Airport		Pipeline		Half Mile Radius
	Cemeteries		Railroad		Toll
	Hospital		Trails		Interstate
	School		Managed Lands		State Route
			County Boundary		US Route
					Local Road

Red Flag Investigation - Water Resources
 US 31 Preventative Pavement Maintenance
 Des. No. 1700111
 Sellersburg, Clark County, IN



Sources:
Non Orthophotography Data - Obtained from the State of Indiana Geographical Information Office Library
Orthophotography - Obtained from Indiana Map Framework Data (www.indianamap.org)
Map Projection: UTM Zone 16 N **Map Datum:** NAD83

This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.

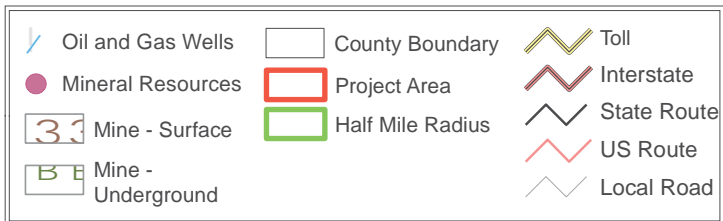




Red Flag Investigation - Mining/Mineral Exploration
 US 31 Preventative Pavement Maintenance
 Des. No. 1700111
 Sellersburg, Clark County, IN



Sources:
 Non Orthophotography
 Data - Obtained from the State of Indiana Geographical Information Office Library
 Orthophotography - Obtained from Indiana Map Framework Data (www.indianamap.org)
 Map Projection: UTM Zone 16 N Map Datum: NAD83
 This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.



Red Flag Investigation - Haz Mat Concerns
 US 31 Preventative Pavement Maintenance
 Des. No. 1700111
 Sellersburg, Clark County, IN



	Brownfield		RCRA Generator/TSD		Institutional Controls
	RCRA Corrective Action Sites		Restricted Waste Site		County Boundary
	Confined Feeding Operation		Septage Waste Site		Project Area
	Construction/Demolition Site		Solid Waste Landfill		Half Mile Radius
	Infectious/Medical Waste Site		State Cleanup Site		Toll
	Leaking Underground Storage Tank		Superfund		Interstate
	Manufactured Gas Plant		Tire Waste Site		State Route
	NPDES Facilities		Underground Storage Tank		US Route
	NPDES Pipe Locations		Voluntary Remediation Program		Local Road
	Open Dump Waste Site		Waste Transfer Station		



This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.

Sources:
Non Orthophotography
 Data - Obtained from the State of Indiana Geographical Information Office Library
Orthophotography - Obtained from Indiana Map Framework Data (www.indianamap.org)
 Map Projection: UTM Zone 16 N Map Datum: NAD83 E14

Indiana County Endangered, Threatened and Rare Species List

County: Clark

Species Name	Common Name	FED	STATE	GRANK	SRANK
Platyhelminthes (Flatworms)					
Sphalloplana weingartneri	Weingartner's Cave Flatworm		WL	G4	S3
Diplopoda					
Pseudotremia nefanda	Clark Cave Millepede		SE	G3G4	S2
Dipluran					
Campodea plusiochaeta	A Dipluran		SE	GNR	S1
Crustacean: Malacostraca					
Caecidotea jordani	Jordan's groundwater isopod		SE	G2G3	S1
Crangonyx ohioensis	An Amphipod			G1G2	S1
Crangonyx packardi	Packard's Cave Amphipod		WL	G4	S3
Gammarus bousfieldi	Bousfield's spring amphipod		SE	G1	S1
Stygobromus mackini	Mackin's cave amphipod		SE	G5	S1
Synurella dentata	Dentate amphipod		WL	GNR	S4
Crustacean: Copepoda					
Diacyclops jeanneli	Jeannel's Cave Copepod		ST	G3G4	S2
Mollusk: Bivalvia (Mussels)					
Fusconaia subrotunda	Longsolid	C	SE	G3	SX
Lampsilis fasciola	Wavyrayed Lampmussel		SSC	G5	S3
Potamilus capax	Fat Pocketbook	LE	SE	G2	S1
Villosa lienosa	Little Spectaclecase		SSC	G5	S3
Mollusk: Gastropoda					
Fontigens cryptica	Hidden Springs Snail		SE	G1	S1
Ellipluran: Collembola					
Pseudosinella fonsa	Fountain Cave Springtail		ST	G3G4	S2
Sinella alata	Springtail		WL	G5	S4
Sinella cavernarum	A Springtail		WL	G5	S3
Insect: Coleoptera (Beetles)					
Aleochara lucifuga	Rove beetle		WL	GNR	S4
Atheta annexa	Rove beetle		WL	G4	S4
Batrisodes krekeri	Krekeler's cave ant beetle		SE	G1	S1
Dryobius sexnotatus	Six-banded Longhorn Beetle		ST	GNR	S2
Pseudanophthalmus barri	Cave Beetle		SE	G1G2	S1
Insect: Lepidoptera (Butterflies & Moths)					
Artogeia virgininiensis	West Virginia White		SR	G3?	S3
Celastrina nigra	Dusky Azure		ST	G4	S2
Arachnida					
Dolomedes scriptus	Lined Nursery Web Spider			G5	S1?
Fish					
Acipenser fulvescens	Lake Sturgeon		SE	G3G4	S1

Indiana Natural Heritage Data Center
Division of Nature Preserves
Indiana Department of Natural Resources
This data is not the result of comprehensive county surveys.

Fed: LE = Endangered; LT = Threatened; C = candidate; PDL = proposed for delisting
State: SE = state endangered; ST = state threatened; SR = state rare; SSC = state species of special concern; SX = state extirpated; SG = state significant; WL = watch list
GRANK: Global Heritage Rank: G1 = critically imperiled globally; G2 = imperiled globally; G3 = rare or uncommon globally; G4 = widespread and abundant globally but with long term concerns; G5 = widespread and abundant globally; G? = unranked; GX = extinct; Q = uncertain rank; T = taxonomic subunit rank
SRANK: State Heritage Rank: S1 = critically imperiled in state; S2 = imperiled in state; S3 = rare or uncommon in state; G4 = widespread and abundant in state but with long term concern; SG = state significant; SH = historical in state; SX = state extirpated; B = breeding status; S? = unranked; SNR = unranked; SNA = nonbreeding status unranked

Indiana County Endangered, Threatened and Rare Species List

County: Clark

Species Name	Common Name	FED	STATE	GRANK	SRANK
<i>Esox masquinongy</i>	Ohio River Muskellunge		SSC	G5	S1
<i>Etheostoma variatum</i>	Variegated Darter		SE	G5	S1
Amphibian					
<i>Acris blanchardi</i>	Northern Cricket Frog		SSC	G5	S4
<i>Cryptobranchus alleganiensis alleganiensis</i>	Eastern Hellbender	C	SE	G3G4T3T4	S1
Reptile					
<i>Clonophis kirtlandii</i>	Kirtland's Snake	C	SE	G2	S2
<i>Crotalus horridus</i>	Timber Rattlesnake		SE	G4	S2
<i>Opheodrys aestivus</i>	Rough Green Snake		SSC	G5	S3
<i>Tantilla coronata</i>	Southeastern Crowned Snake		SE	G5	S1
<i>Terrapene carolina carolina</i>	Eastern Box Turtle		SSC	G5T5	S3
Bird					
<i>Aimophila aestivalis</i>	Bachman's Sparrow			G3	SXB
<i>Ammodramus henslowii</i>	Henslow's Sparrow		SE	G4	S3B
<i>Coragyps atratus</i>	Black Vulture			G5	S1N,S2B
<i>Haliaeetus leucocephalus</i>	Bald Eagle		SSC	G5	S2
<i>Helmitheros vermivorus</i>	Worm-eating Warbler		SSC	G5	S3B
<i>Lanius ludovicianus</i>	Loggerhead Shrike		SE	G4	S3B
<i>Nycticorax nycticorax</i>	Black-crowned Night-heron		SE	G5	S1B
<i>Pandion haliaetus</i>	Osprey		SE	G5	S1B
<i>Setophaga cerulea</i>	Cerulean Warbler		SE	G4	S3B
<i>Tyto alba</i>	Barn Owl		SE	G5	S2
Mammal					
<i>Mustela nivalis</i>	Least Weasel		SSC	G5	S2?
<i>Myotis grisescens</i>	Gray Bat	LE	SE	G4	S1
<i>Myotis sodalis</i>	Indiana Bat or Social Myotis	LE	SE	G2	S1
<i>Sorex hoyi</i>	Pygmy Shrew		SSC	G5	S2
<i>Taxidea taxus</i>	American Badger		SSC	G5	S2
Vascular Plant					
<i>Acalypha deamii</i>	Mercury		SR	G4?	S2
<i>Asclepias viridis</i>	Green Milkweed		SE	G4G5	S1
<i>Asplenium resiliens</i>	Black-stem Spleenwort		SE	G5	S1
<i>Asplenium ruta-muraria</i>	Wallrue Spleenwort		SR	G5	S2
<i>Aster schreberi</i>	Schreber Aster		SE	G4	S1
<i>Azolla caroliniana</i>	Carolina Mosquito-fern		ST	G5	S2
<i>Calamagrostis porteri</i> ssp. <i>insperata</i>	Reed Bent Grass		ST	G4T3	S1
<i>Carex eburnea</i>	Ebony Sedge		SR	G5	S2
<i>Carex straminea</i>	Straw Sedge		ST	G5	S2
<i>Chaerophyllum procumbens</i> var. <i>shortii</i>	Wild Chervil		ST	G5T3T4Q	S1

Indiana Natural Heritage Data Center
Division of Nature Preserves
Indiana Department of Natural Resources
This data is not the result of comprehensive county surveys.

Fed: LE = Endangered; LT = Threatened; C = candidate; PDL = proposed for delisting
State: SE = state endangered; ST = state threatened; SR = state rare; SSC = state species of special concern; SX = state extirpated; SG = state significant; WL = watch list
GRANK: Global Heritage Rank: G1 = critically imperiled globally; G2 = imperiled globally; G3 = rare or uncommon globally; G4 = widespread and abundant globally but with long term concerns; G5 = widespread and abundant globally; G? = unranked; GX = extinct; Q = uncertain rank; T = taxonomic subunit rank
SRANK: State Heritage Rank: S1 = critically imperiled in state; S2 = imperiled in state; S3 = rare or uncommon in state; G4 = widespread and abundant in state but with long term concern; SG = state significant; SH = historical in state; SX = state extirpated; B = breeding status; S? = unranked; SNR = unranked; SNA = nonbreeding status unranked

Indiana County Endangered, Threatened and Rare Species List

County: Clark

Species Name	Common Name	FED	STATE	GRANK	SRANK
<i>Cirsium carolinianum</i>	Carolina Thistle		SR	G5	S2
<i>Cornus amomum</i> ssp. <i>amomum</i>	Silky Dogwood		SE	G5T5	S1
<i>Cuscuta indecora</i>	Pretty Dodder		SE	G5	S1
<i>Eleocharis bifida</i>	Glades spikerush		SE	G3G4	S1
<i>Euphorbia obtusata</i>	Bluntleaf Spurge		SE	G5	S1
<i>Heliotropium tenellum</i>	Slender Heliotrope		ST	G5	S2
<i>Hexalectris spicata</i>	Crested Coralroot		SR	G5	S2
<i>Hottonia inflata</i>	Featherfoil		ST	G4	S2
<i>Iresine rhizomatosa</i>	Eastern Bloodleaf		SR	G5	S2
<i>Isoetes engelmannii</i>	Appalachian Quillwort		SE	G4	S1
<i>Lathyrus venosus</i>	Smooth Veiny Pea		ST	G5	S2
<i>Leavenworthia uniflora</i>	Michaux Leavenworthia		SE	G4	S1
<i>Lechea racemulosa</i>	Illinois Pinweed		SE	G5	S1
<i>Linum sulcatum</i>	Grooved Yellow Flax		SR	G5	S2
<i>Ludwigia decurrens</i>	Primrose Willow		WL	G5	S2
<i>Magnolia acuminata</i>	Cucumber Magnolia		SE	G5	S1
<i>Matelea obliqua</i>	Angle Pod		SR	G4?	S2
<i>Melica nitens</i>	Three-flower Melic Grass		ST	G5	S2
<i>Melothria pendula</i>	Creeping Cucumber		SE	G5?	S1
<i>Ophioglossum engelmannii</i>	Limestone Adder's-tongue		SR	G5	S2
<i>Oxalis illinoensis</i>	Illinois Woodsorrel		WL	G4Q	S2
<i>Panicum bicknellii</i>	A Panic-grass		SE	G4?Q	S1
<i>Passiflora incarnata</i>	Purple Passion-flower		SR	G5	S2
<i>Penstemon deamii</i>	Deam Beardtongue		SR	G1	S1
<i>Phlox amplifolia</i>	Large-leaved Phlox		SR	G3G5	S2
<i>Pleopeltis polypodioides</i>	Resurrection Fern		SR	G5	S2
<i>Rhexia mariana</i> var. <i>mariana</i>	Maryland Meadow Beauty		ST	G5T5	S1
<i>Rubus centralis</i>	Illinois Blackberry		SE	G2?Q	S1
<i>Satureja glabella</i> var. <i>angustifolia</i>	Calamint		SE	G5	S1
<i>Scutellaria parvula</i> var. <i>australis</i>	Southern Skullcap		WL	G4T4?	S2
<i>Sedum telephioides</i>	Allegheny Stonecrop		SR	G4	S2
<i>Solidago squarrosa</i>	Stout-ragged Goldenrod		SE	G4G5	S1
<i>Spiranthes magnicamporum</i>	Great Plains Ladies'-tresses		SE	G3G4	S1
<i>Stachys clingmanii</i>	Clingman Hedge-nettle		SE	G2	S1
<i>Strophostyles leiosperma</i>	Slick-seed Wild-bean		ST	G5	S2
<i>Sullivantia sullivantii</i>	Sullivantia		ST	G4	S2
<i>Thalictrum pubescens</i>	Tall Meadowrue		ST	G5	S2
<i>Tragia cordata</i>	Heart-leaved Noseburn		WL	G4	S2
<i>Trifolium reflexum</i> var. <i>glabrum</i>	Buffalo Clover		SE	G5T2T4Q	S1
<i>Trifolium stoloniferum</i>	Running Buffalo Clover	LE	SE	G3	S1

Indiana Natural Heritage Data Center
Division of Nature Preserves
Indiana Department of Natural Resources
This data is not the result of comprehensive county surveys.

Fed: LE = Endangered; LT = Threatened; C = candidate; PDL = proposed for delisting
State: SE = state endangered; ST = state threatened; SR = state rare; SSC = state species of special concern; SX = state extirpated; SG = state significant; WL = watch list
GRANK: Global Heritage Rank: G1 = critically imperiled globally; G2 = imperiled globally; G3 = rare or uncommon globally; G4 = widespread and abundant globally but with long term concerns; G5 = widespread and abundant globally; G? = unranked; GX = extinct; Q = uncertain rank; T = taxonomic subunit rank
SRANK: State Heritage Rank: S1 = critically imperiled in state; S2 = imperiled in state; S3 = rare or uncommon in state; G4 = widespread and abundant in state but with long term concern; SG = state significant; SH = historical in state; SX = state extirpated; B = breeding status; S? = unranked; SNR = unranked; SNA = nonbreeding status unranked

Indiana County Endangered, Threatened and Rare Species List

County: Clark

Species Name	Common Name	FED	STATE	GRANK	SRANK
Valerianella chenopodiifolia	Goose-foot Corn-salad		SE	G4	S1
Viburnum molle	Softleaf Arrow-wood		SR	G5	S2
Viola hirsutula	Southern Wood Violet		SE	G4	S1
High Quality Natural Community					
Barrens - bedrock limestone	Limestone Glade		SG	G4	S2S3
Barrens - bedrock siltstone	Siltstone Glade		SG	G2	S2
Forest - upland dry Highland Rim	Highland Rim Dry Upland Forest			GNR	S3
Forest - upland dry-mesic Bluegrass	Bluegrass Dry-mesic Upland Forest			GNR	S1
Forest - upland dry-mesic Highland Rim	Highland Rim Dry-mesic Upland Forest			GNR	S3
Forest - upland mesic Bluegrass	Bluegrass Mesic Upland Forest			GNR	S3
Forest - upland mesic Highland Rim	Highland Rim Mesic Upland Forest			GNR	S3
Other Significant Feature					
Geomorphic - Nonglacial Erosional Feature - Water Fall and Cascade	Water Fall and Cascade			GNR	SNR

Indiana Natural Heritage Data Center
Division of Nature Preserves
Indiana Department of Natural Resources
This data is not the result of comprehensive county surveys.

Fed: LE = Endangered; LT = Threatened; C = candidate; PDL = proposed for delisting
State: SE = state endangered; ST = state threatened; SR = state rare; SSC = state species of special concern; SX = state extirpated; SG = state significant; WL = watch list
GRANK: Global Heritage Rank: G1 = critically imperiled globally; G2 = imperiled globally; G3 = rare or uncommon globally; G4 = widespread and abundant globally but with long term concerns; G5 = widespread and abundant globally; G? = unranked; GX = extinct; Q = uncertain rank; T = taxonomic subunit rank
SRANK: State Heritage Rank: S1 = critically imperiled in state; S2 = imperiled in state; S3 = rare or uncommon in state; G4 = widespread and abundant in state but with long term concern; SG = state significant; SH = historical in state; SX = state extirpated; B = breeding status; S? = unranked; SNR = unranked; SNA = nonbreeding status unranked

Elizabet Biggio

From: Brittney Layton
Sent: Tuesday, November 3, 2020 9:12 AM
To: Elizabet Biggio
Subject: FW: Question about HazMat Concern, Des. 1700111 US 31, Sellersburg, Clark County, IN

Good morning Elizabet,
I received this back from INDOT SAM:

Based on the information provided, we can use the following commitment:

- If excavation occurs in this area, petroleum contamination may be encountered. If contamination is encountered, before proper removal and disposal of soil and/or groundwater, analysis for lead will be necessary.

Brittney Layton, M.A.
Environmental Scientist

Butler, Fairman & Seufert, Inc.
8450 Westfield Blvd., Suite 300 | Indianapolis, IN 46240-8302 |
p 317-713-4615 | f 317-713-4616 | c 434-390-8813
BLayton@bfsengr.com | www.BFSEngr.com



CONFIDENTIALITY NOTICE: This Email and any attachments are confidential and may be protected by legal privilege. If you are not the intended recipient, be aware that any disclosure, copying, distribution, or use of this Email or any attachment is prohibited. If you have received this Email in error, please notify us immediately by returning it to the sender and delete this copy from your system. Thank you. Butler, Fairman & Seufert, Inc.

From: Foheybreting, Nicole K <NFoheyBreting@indot.IN.gov>
Sent: Monday, November 2, 2020 7:09 PM
To: Brittney Layton <BLayton@bfsengr.com>
Cc: Mathas, Marlene <MMathas@indot.IN.gov>
Subject: RE: Question about HazMat Concern, Des. 1700111 US 31, Sellersburg, Clark County, IN

Hi Brittney –

Based on the scope of work near this intersection, I think we can use the recommendation:

- If excavation occurs in this area, petroleum contamination may be encountered. If contamination is encountered, before proper removal and disposal of soil and/or groundwater, analysis for lead will be necessary.

Please let me know if there are any questions or concerns.
Thanks!
Nicole

Nicole Fohey-Breting

Site Assessment & Management (SAM) Specialist

100 North Senate Avenue RM N642

Indianapolis, Indiana 46204

Office: ** (317) 416-7084 **

Email: NFoheyBreting@indot.in.gov



Please note my new phone number going forward is (317) 416-7084. Thank you!

The Site Assessment and Management (SAM) Manual can be found at <http://www.in.gov/indot/2523.htm>

Be sure to refer to the updated information in the SAM Manual for document preparation and submission.

From: Brittney Layton <BLayton@bfsengr.com>

Sent: Monday, November 02, 2020 8:41 AM

To: Foheybreting, Nicole K <NFoheyBreting@indot.IN.gov>

Cc: Mathas, Marlene <MMathas@indot.IN.gov>

Subject: RE: Question about HazMat Concern, Des. 1700111 US 31, Sellersburg, Clark County, IN

****** This is an EXTERNAL email. Exercise caution. DO NOT open attachments or click links from unknown senders or unexpected email. ******

Good morning Nicole,

I hope this answers your questions. If you need more info, please let me know and I'll consult with the designers.

1. how much of the property is being acquired (full acquisition or just a sliver?) **It's a strip about 6 feet wide to bring the existing sidewalk into the ROW. Total 0.02 acre.**
2. what type of excavation activities (if any) are occurring in the vicinity of the site? **The sidewalk will be replaced at the existing width. The curb ramp at Hauss Street will be replaced. US 31 will have a full depth replacement with curbs and underdrains in that area.**

I have attached the plan page for Hauss Street, which shows the approximate ROW. (Hauss Ave is on the right side of the first page and the left side of the second page, for your convenience.)

Brittney Layton, M.A.
Environmental Scientist

Butler, Fairman & Seufert, Inc.
8450 Westfield Blvd., Suite 300 | Indianapolis, IN 46240-8302 |
p 317-713-4615 | f 317-713-4616 | c 434-390-8813
BLayton@bfsengr.com | www.BFSEngr.com

CONFIDENTIALITY NOTICE: This Email and any attachments are confidential

and may be protected by legal privilege. If you are not the intended recipient, be aware that any disclosure, copying, distribution, or use of this Email or any attachment is prohibited. If you have received this Email in error, please notify us immediately by returning it to the sender and delete this copy from your system. Thank you. Butler, Fairman & Seufert, Inc.

From: Foheybreting, Nicole K <NFoheyBreting@indot.IN.gov>
Sent: Friday, October 30, 2020 9:16 AM
To: Brittney Layton <BLayton@bfsengr.com>
Cc: Mathas, Marlene <MMathas@indot.IN.gov>
Subject: RE: Question about HazMat Concern, Des. 1700111 US 31, Sellersburg, Clark County, IN

Hi Brittney –

Thank you for reaching out. I think the main information that we need is 1) how much of the property is being acquired (full acquisition or just a sliver?) and 2) what type of excavation activities (if any) are occurring in the vicinity of the site? This will help make a determination on whether or not a Phase II ESA is warranted for this project.

Thank you!
Nicole

Nicole Fohey-Breting
Site Assessment & Management (SAM) Specialist
100 North Senate Avenue RM N642
Indianapolis, Indiana 46204
Office: ** (317) 416-7084 **
Email: NFoheyBreting@indot.in.gov



Please note my new phone number going forward is (317) 416-7084. Thank you!

The Site Assessment and Management (SAM) Manual can be found at <http://www.in.gov/indot/2523.htm>

Be sure to refer to the updated information in the SAM Manual for document preparation and submission.

From: Brittney Layton <BLayton@bfsengr.com>
Sent: Sunday, October 25, 2020 1:45 PM
To: INDOT esd.sam <esd.sam@indot.IN.gov>
Cc: Elizabet Biggio <EBiggio@bfsengr.com>
Subject: Question about HazMat Concern, Des. 1700111 US 31, Sellersburg, Clark County, IN

**** This is an EXTERNAL email. Exercise caution. DO NOT open attachments or click links from unknown senders or unexpected email. ****

Good afternoon Nicole & Marlene,
I hope you have had a wonderful weekend! I'm sorry to reach out again regarding the Sellersburg Project; however, your expertise and guidance is required in this matter pertaining to the necessity of a Phase I or II ESA:

During the field check, the property located in the southwest corner of US 31 & Hauss Avenue (475 Hauss Avenue, Sellersburg, IN) was identified as possibly a former gas station. The RFI did not identify any hazmat feature at this location. I conducted a review of Google Earth historical maps as well as researched IDEM's VFC. Currently, there is no

documentation on any hazardous material concerns on that property within the VFC. I followed this up by reaching out to a contact at IDEM: Jeff Scull, an Environmental Manager in the Petroleum Division—Office of Land Quality at IDEM. He researched multiple databases and did a brief records search. He couldn't find any documentation on the property, either. He did state, though, that the building appears suspicious due to the overhang and the concrete dividers that are similar to the turn stalls that hold gas dispensers.

Permanent right of way is required from this site to complete the project activities (HMA Overlay). I've attached the Early Coordination Letter as it has a good overview of the project scope, in case that helps. **My question: Would a Phase I or II be recommended or required?** I wasn't sure if a Phase I would be recommended for a records search to confirm or deny as we don't even know for sure if it was a gas station. Or, if a Phase I is done and it is confirmed, then a Phase II would be recommended since there is no documentation as it didn't show on the RFI, or on Google Earth (per the commitment in the SAM Manual when no records are available). If we do a Phase I and it is positive, then is putting the following commitment in the CE acceptable?: A Phase II Environmental Site Assessment is recommended. Prior to any investigation activities, a scope of work plan will be prepared and submitted to INDOT SAM for review and approval.

Additionally, there is a second hazmat property located adjacent to the project area, but there will not be any excavation taking place at the location, just an overlay being applied. **Would a Phase II be recommended or required for this second site, also?**



Please let me know if I can answer or clarify anything. I appreciate your time and consideration.

Thank you for your guidance.

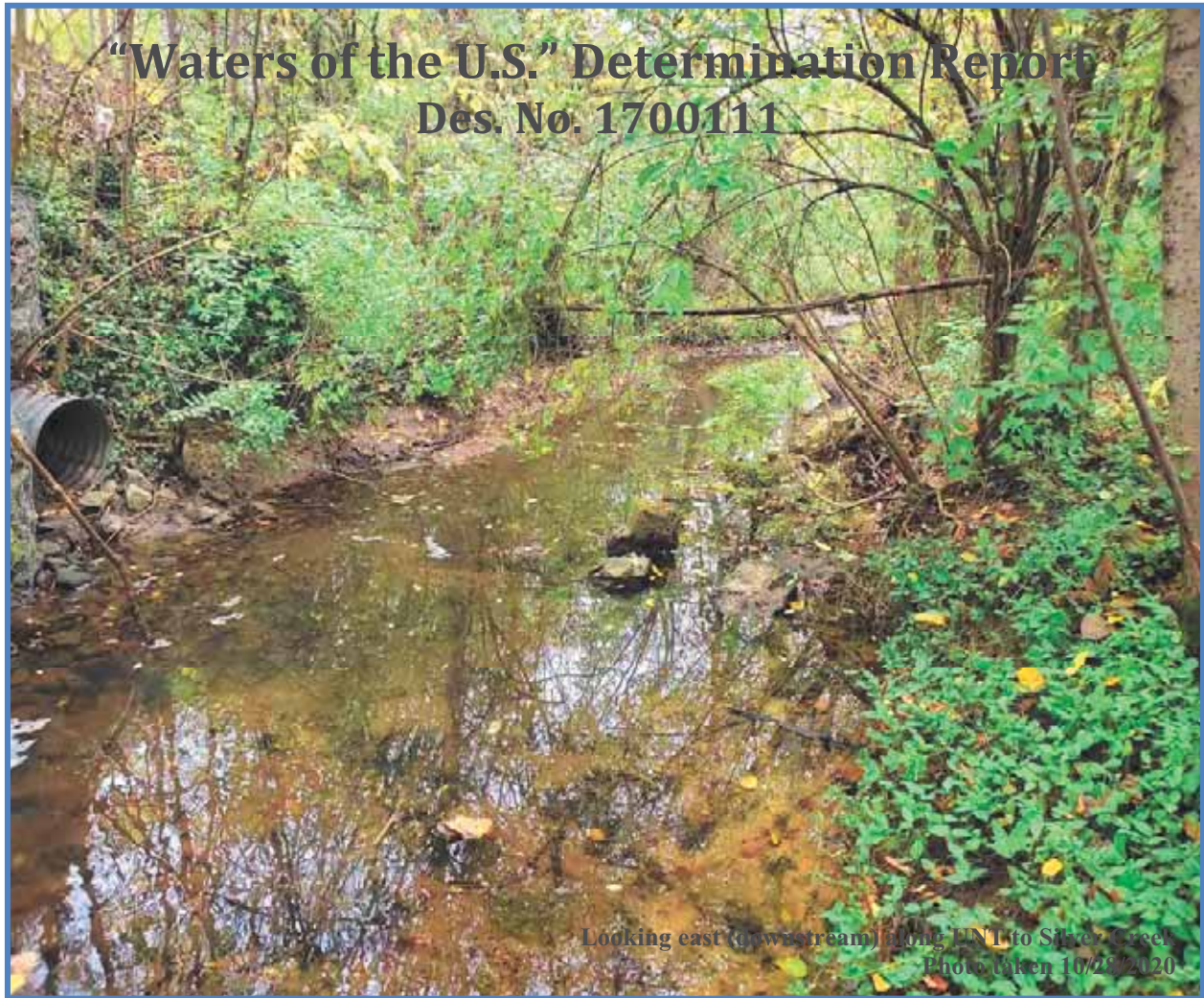
Brittney Layton, M.A.
Environmental Scientist

Butler, Fairman & Seufert, Inc.
8450 Westfield Blvd., Suite 300 | Indianapolis, IN 46240-8302 |
p 317-713-4615 | f 317-713-4616
BLayton@bfsenr.com | www.BFSEngr.com

Appendix F

Ecological and Water Resources

LiKang 12/4/2020



**Butler, Fairman &
Seufert, Inc.**
8450 Westfield Blvd.,
Suite 300
Indianapolis, IN 46240
(317) 713-4615
www.bfsengr.com
November 6, 2020
(Revised November 24, 2020)



Prepared By: Ryan L. Scott
INVESTIGATION FOR INDIANA DEPARTMENT
OF TRANSPORTATION (INDOT)

US 31 PAVEMENT MAINTENANCE
1.53 MILES N. of STATE ROAD (SR) 60
(FOOTHILL ROAD) to 3.28 MILES N. of SR 60

TOWN OF SELLERSBURG
CLARK COUNTY, INDIANA

“WATERS OF THE U.S.” DETERMINATION REPORT
US 31 PAVEMENT MAINTENANCE
TOWN OF SELLERSBURG, CLARK COUNTY, INDIANA
DES. NO. 1700111

Prepared By: Ryan L. Scott
rscott@bfsengr.com / 317-713-4615
Butler, Fairman & Seufert, Inc.

Completed Date: November 6, 2020; Revision Date: November 24, 2020

Date of Field Investigations: June 2, 2020 and October 28, 2020

Project Location: The project is located within the Town of Sellersburg, Clark County, Indiana. More specifically, the project is located along US 31 from 1.53 miles N. of SR 60 (Foothill Road) to 3.28 miles N. of SR 60. The project is also located in Sections 110, 111, and 130, Township 1 South, Range 6 East on the USGS Speed, Indiana Quadrangle (see Attachment 2).

LAT 38.40344 N; LONG -85.75364 W (Investigation Area 1)

LAT 38.39147 N; LONG -85.75525 W (Investigation Area 2)

USGS 8-digit hydrologic unit code (HUC) 05140101, 12-digit HUC 051401010805

Project Description:

The proposed project is approximately 1.75 miles long. No work to existing bridges/culverts/pipes is proposed. The project proposes the following (see Attachment 3 for map showing project segments):

Segment 1- Foothill Road to Triangle Drive:

- Mill and repave US 31.

Segment 2- US 31/SR 311/Prather Lane Intersection:

- Full-depth pavement replacement with underdrains;
- Replace non-ADA-compliant curb ramps.

Segment 3- Bucheit Street to CR 403 (Old SR 403):

- Full-depth pavement replacement with underdrains;
- Install new curb inlets and storm sewer;
- Replace non-ADA-compliant curb ramps;
- Replace traffic signal at Utica Street and CR 403 (Old SR 403) intersections;
- Remove mid-block crosswalk near Silver Creek Elementary and Silver Creek Middle School entrances and relocate to US 31/CR 403 (Old SR 403) intersection with ADA-compliant pushbuttons and pedestrian countdown heads;
- Revise pavement markings from Utica Street to Silver Creek Elementary and Silver Creek Middle School entrances.

Segment 4- CR 403 (Old SR 403) to St. Joe Road (just south of Muddy Fork):

- Mill and repave US 31;
- Replace non-compliant curb ramps;
- Replace segments of sidewalk disturbed by project;
- Replace inlet castings and adjust to grade;
- Add curb inlets and connect to existing storm sewer.

Due to the linear nature of this pavement maintenance project, the INDOT Ecology and Waterway Permitting Office (EWPO) was contacted by Butler, Fairman and Seufert, Inc. (BF&S Inc.) on October 27, 2020 in order to determine the appropriate approach for this investigation. The majority of the project is located within a previously disturbed urban setting with either no roadside drainage features present, or existing curb and gutter present (see photographs 1-6 on Attachments 14-16). Therefore, it was determined that two (2) specific areas along the project should be the focus of this investigation.

Investigation Area 1 is located near the central portion the overall project where US 31 crosses an unnamed tributary (UNT) to Silver Creek. The footprint of Investigation Area 1 consisted of the area that has the potential to be impacted based on all possible design scenarios, specifically the proposed construction of a new storm water outlet in the southeast quadrant of the crossing and associated riprap placement for scour protection. The area of investigation was evaluated for the presence or absence of wetlands and streams. Approximately 0.17 acre was investigated. The study limits extend a total of 150 linear feet along US 31, starting at the stream crossing and continuing south, and extending 50 feet east from the edge of pavement of US 31. This area was investigated by walking transects north to south within the study limits for the project and looking for any visual evidence of stream or wetland characteristics.

Investigation Area 2 is located near the south terminus of the overall project where US 31 crosses Camp Run. The footprint of Investigation Area 2 consisted of the area along US 31 between Foothill Road and Fern Street (approximately 150 linear feet along US 31) and the existing 90-foot wide right-of-way width around the crossing, which extends from the roadway centerline 40 feet to the east and 50 feet to the west.

All areas mapped as wetlands on the U.S. Fish and Wildlife Service's (USFWS) National Wetlands Inventory (NWI) map were investigated and sampling points (also referred to as data points) were taken where wetland characteristics were observed and in any potential problem areas. Any drainage features that displayed a defined channel and ordinary high water mark (OHWM) were considered potentially jurisdictional streams. Any water features that did not meet these criteria were not considered as streams.

Site Background:

Prior to the field investigation, several reference materials were consulted to gain information about the site. The USGS Speed, IN quadrangle map was used to determine contours of the site and locate any water bodies in the area, as well as to provide a legal description of the area (see Attachment 2). The Natural Resources Conservation Service (NRCS) Web Soil Survey (<https://websoilsurvey.nrcs.usda.gov/app/>) was consulted to determine if the project area contained any soils listed in either the *Hydric Soils of the United States* manual or the state list of hydric soils publication, along with a description of characteristics displayed by the mapped soil types of the area (see Attachment 6-8). The USFWS NWI map was used to find and classify any previously catalogued wetlands in the project area (see Attachment 4). The Indiana Department of Natural Resources' (IDNR) floodplain map was consulted to gain an understanding of historic flood locations and frequency. The project is located within a mapped floodplain (see Attachment 5). All of this information provided a background for the hydrologic regime of the area.

Attached Documentation:

Summary tables of the waterways known to be present in the project area.

Maps of the project area.

Photographs of the project area.

Data Sheets.

National Wetlands Inventory (NWI) Map

The following is a list of mapped wetlands located either within or near the proposed project limits (see Attachment 4).

- An intermittent stream is mapped, classified by Cowardin et. al.¹ as a riverine, intermittent, streambed, seasonally flooded (R4SBC) wetland, as crossing US 31 near the central portion of the overall project. This stream is an unnamed tributary (UNT) to Silver Creek.
- An intermittent stream is mapped, classified by Cowardin et. al.¹ as a riverine, intermittent, streambed, seasonally flooded (R4SBC) wetland, as crossing US 31 near the southern terminus of the project. This stream is Camp Run.

Soil Map Data

According to the NRCS Web Soil Survey website¹ for Clark County, Indiana (see Attachments 6 – 8); the following soil types are located within the proposed project limits.

<u>Soil Name</u>	<u>Map Abbreviation</u>	<u>Hydric Range</u>
Haymond silt loam, 0-2 percent slopes, frequently flooded, very brief duration	HcgAV	Not Hydric 0% Hydric Inclusions
Udorthents, cut and filled	Uaa	Not Hydric 0% Hydric Inclusions
Urban land-Udarents, fragipan Substratum, complex, till plain 0 to 12 percent slopes	UngB	Not Hydric 0% Hydric Inclusions
Wilbur silt loam, 0 to 2 percent slopes occasionally flooded, very brief duration	WokAW	Not Hydric 0% Hydric Inclusions

The results of the soil mapping indicate that none of the mapped soil types are considered to have any hydric soil potential.

Summary of Findings:

Streams:

There are two (2) mapped streams located within the study area. The first waterway is known as UNT to Silver Creek, which flows east under US 31 and is illustrated as a dashed blue line on the Speed, Indiana quadrangle map (see Attachment 2). This stream has a drainage area upstream of the study limits of approximately 0.52 square mile (as calculated using the web-

¹ <https://websoilsurvey.nrcs.usda.gov/app/WebSoilSurvey.aspx>

tools on the USGS *Indiana StreamStats* website²). This stream falls within the larger Lower Ohio Watershed identified by the USGS 8-HUC 05140101. UNT to Silver Creek is classified as a riverine, intermittent, streambed, seasonally flooded (R4SBC) waterway. It is of average quality due to the presence of a mostly intact riparian corridor and moderate sinuosity. No riffle-pool complexes were noted in the stream near the project area. The substrate is primarily silt and various sizes of gravel. The stream has an approximate average 22-foot bankfull width and approximate average 3.5-foot bankfull depth. The OHWM depth is approximately 2.5 feet and width is approximately 18.0 feet. UNT to Silver Creek should be considered a “Waters of the United States”.

The second mapped stream within the study area is Camp Run, which flows east under US 31 near the southern project terminus and is illustrated as a dashed blue line on the Speed, Indiana quadrangle map (see Attachment 2). This stream has a drainage area upstream of the study limits of approximately 0.97 square mile (as calculated using the web-tools on the USGS *Indiana StreamStats* website³). This stream falls within the larger Lower Ohio Watershed identified by the USGS 8-HUC 05120207. Camp Run is classified as a riverine, intermittent, streambed, seasonally flooded (R4SBC) waterway. It is of poor quality due to the lack of a riparian corridor and absence of riffle-pool complexes. The substrate is primarily silt. The stream has an approximate average 18-foot bankfull width and approximate average 4.5-foot bankfull depth. The OHWM depth is approximately 1.3 feet and width is approximately 6.0 feet. Camp Run should be considered a “Waters of the United States”.

Roadside Ditches:

Roadside ditch 1 (RSD1) was observed within the project limits. This feature is located in Investigation Area 2. Specifically, this feature parallels the east side of US 31 and conveys stormwater runoff south to Camp Run. This feature has a defined channel but does not contain an ordinary high water mark. During the site visit conducted on October 28, 2020, RSD1 did not contain any flowing water. Therefore, it should not be considered a jurisdictional feature.

All measurements were determined in the field and compared to the results that were generated in *Indiana StreamStats*. No surveyed cross sections were available to the investigator for comparison. As a result, UNT to Silver Creek and Camp Run should be the only streams considered “Waters of the United States” within the study areas.

Wetlands:

There are no mapped wetlands in the study area; however, the southeast quadrant of the US 31 crossing of UNT to Silver Creek (Investigation Area 1) was investigated for potential wetland conditions due to proposed outfall work in this area. One (1) sampling point was taken in the field during the site visit on October 28, 2020 (see Attachments 10 and 11). The sampling point (Sample Point 1) was evaluated for all three (3) criteria to be considered wetland as described in the *1987 Corps of Engineers Wetland Delineation Manual ('87 Manual)* and as currently applied in the *Midwest Regional Supplement* manual.

Sample Point 1 was taken in a relatively flat floodplain area in the immediate southeast quadrant of the US 31 crossing of UNT to Silver Creek (see Attachment 5). The sample point is located east of what is sloped US 31 roadway fill. The general topography of the larger area

² <https://streamstats.usgs.gov/ss/>

³ <https://streamstats.usgs.gov/ss/>

containing the sample point is a low-lying, somewhat bowl-shaped floodplain that is bordered to the west by US 31 roadway fill, to the south and east by forested high ground, and to the north by UNT to Silver Creek.

Sample Point 1 is located within the 1% Annual Chance Floodplain of UNT to Silver Creek (see Attachment 5). The soil type mapped in this area is moderately well drained with a water table 18 to 30 inches below ground surface, no ponding, and no restrictive layer within the upper 80 cm or approximately 32 inches. Transects were walked throughout the study area and no indications of prolonged hydrology (ponded water for greater than 14 consecutive days) were observed.

Sample Point 1 was found to be dominated by mostly upland vegetation. Documented dominant species in this area include common hackberry (*Celtis occidentalis*, FAC), red mulberry (*Morus rubra*, FACU), bush honeysuckle (*Lonicera maackii*, UPL) and wintercreeper (*Euonymus fortunei*, UPL).

Open Water:

No open water areas were observed in the investigated area.

Conclusion and Recommendations:

Table 1. Stream Summary Table

Stream Name	Photo Numbers	Latitude/ Longitude (UTM NAD 83)	OHWL width/depth (feet)	USGS ID	Presence of Riffles/Pools	Channel Substrate	Functional Quality	Likely Water of the U.S.	Linear Ft. in Study Area
UNT to Silver Creek	7-10	38.40344, -85.75364	18.0/2.5	Dashed blue line (intermittent)	No	Silt and various gravel sizes	Average	Yes	50
Camp Run	13-16	38.39147, -85.75525	6.0/1.3	Dashed blue line (intermittent)	No	Silt	Average	Yes	90

Table 2. Roadside Ditch Summary Table

Stream Name	Photo Numbers	Latitude/ Longitude (UTM NAD 83)	OHWL width/depth (ft.)	USGS ID	Presence of Riffles/Pools	Channel Substrate	Functional Quality	Likely Water of the U.S.	Linear Ft. in Study Area
RSD1	17-18	38.39160, -85.75525	No OHWL present	Not mapped	No	Veg/Silt	Poor	No	90

Table 3: Data Point Summary Table

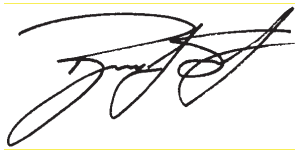
Data Point ID	Photo Numbers	Latitude/ Longitude (UTM NAD 83)	Hydrophytic Vegetation Present	Hydric Soil Present	Wetland Hydrology Present	Is the Sampled Area within a Wetland?
RSD1	11-12	38.40340, -85.75364	No	No	No	No

Field observations revealed two (2) streams (UNT to Silver Creek and Camp Run) within the right-of-way that exhibited a defined channel and OHWM characteristics, and no wetland areas within the study limits of the project area. All identified streams and wetlands are considered jurisdictional features. Every effort should be taken to avoid and minimize impacts to these features. If impacts are necessary, then mitigation may be required. INDOT Environmental Services should be contacted immediately if impacts occur.

The U.S. Army Corps of Engineers (USACE) has regulatory authority over all “waters of the United States”. In addition, the Indiana Department of Environmental Management (IDEM) has jurisdiction over all “waters of the State of Indiana” including isolated wetlands. It is recommended that coordination with the USACE and IDEM occur prior to any construction, tree clearing or any other disturbance causing activity that is performed within the project area in order to verify the findings of this report. The final determination of jurisdictional waters is ultimately made by the USACE. This report is our best judgement based on the guidelines set forth by the Corps.

This waters determination has been prepared based on the best available information, interpreted in the light of the investigator’s training, experience and professional judgement in conformance with the 1987 Corps of Engineers Wetlands Delineation Manual, the appropriate regional supplement, the USACE Jurisdictional Determination Form Instructional Guidebook, and other appropriate agency guidelines.

Ryan L. Scott



Director of Environmental Services
Butler, Fairman and Seufert, Inc.

References:

Chadde, Steven W. 2002. A Great Lakes Wetland Flora: A Complete Guide to the Aquatic and Wetland Plants of the Upper Midwest. 2nd ed. Laurium, MI. Pocketflora Press.

Cowardin, Lewis M., et al. 1979. Classifications of Wetlands and Deepwater Habitats of the United States. United States Fish and Wildlife Service. Office of Biological Services. Washington D.C. Publ. No. FWS/OBS-79/31.

Environmental Laboratory. 1987. Corps of Engineers Wetland Delineation Manual. U.S. Army Engineers Waterways Experiment Station. Vicksburg, MS. Tech Rpt. Y-87-1.

Indiana Natural Resources Commission. Division of Hearings. 2007. “Roster of Indiana Waters Declared Navigable or Nonnavigable”.

Indiana Department of Transportation. Office of Environmental Services. Waterway Permits Unit. Indiana Waterway Permits Manual.

Jackson, Marion T. 2004. 101 Trees of Indiana: A Field Guide. Bloomington, IN. Indiana University Press

U.S. Army Corps of Engineers 2018. National Wetland Plant List, version 3.4. <http://wetland-plants.usace.army.mil/> .U.S. Army Corps of Engineers. Engineer Research and Development Center Cold Regions Research and Engineering Laboratory, Hanover, NH

Mohlenbrock, Robert H. 2005. Aquatic and Standing Water Plants of the Central Midwest: Cyperaceae/Sedges. Carbondale, IL. Southern Illinois University Press.

Munsell Color. 2000. "Munsell Soil Color Charts". New Winsor, NY. Gretag-Macbeth Corporation.

Royer, France and Richard Dickinson. 1999. Weeds of the Northern U.S. and Canada. Edmonton, Alberta. University of Alberta Press.

Soil Survey Staff, Natural Resources Conservation Service, United States Department of Agriculture. Web Soil Survey. Available online at <http://websoilsurvey.nrcs.usda.gov/> accessed [October 28, 2020].

Swink, Floyd and Gerould Wilhelm. 1994. Plants of the Chicago Region. 4th ed. Indianapolis. Indiana Academy of Science.

United States Army Corps of Engineers. 2010. Regional Supplement to the Corps of Engineers Wetland Delineation Manual: Midwest Region (Version 2.0), ed. J.S. Wakeley, R.W. Lichvar, and C.V. Noble. ERDC/EL TR-10-16. Vicksburg, MS: U.S. Army Engineer Research and Development Center.

United States Army Corps of Engineers Regulatory Guidance Letter No. 08-02. "Jurisdictional Determinations". June 26, 2008.

United States Department of Agriculture, Natural Resources Conservation Service. 2018. Field Indicators of Hydric Soils in the United States, Version 8.2. L.M. Vasilas, G.W. Hurt, and J.F. Berowitz (eds.). USDA, NRCS, in cooperation with the National Technical Committee for Hydric Soils.

United States Department of Agriculture, Soil Conservation Service. 1991. Hydric Soils of the United States. Washington D.C. National Bulletin No. 1491.

United States Fish and Wildlife Service. Branch of Resource and Mapping Support "National Wetland Inventory of Clark County, Indiana" [map]. Visual Scale. U.S. Fish and Wildlife Service Wetlands Mapper. Available online at <http://www.fws.gov/wetlands/Data/Mapper.html/> accessed October 28, 2020.

United States Geological Survey. "Speed quadrangle, Indiana". 1:24,000. 7.5 Minute Series. Washington D.C.: USGS.

United States Geological Survey. 2012. The StreamStats program for Indiana. Available online at http://streamstatsags.cr.usgs.gov/v3_beta/viewer.htm?stabbr=IN accessed October 28, 2020.



Legend

Project Area



SEGMENT 1:

Mill and Repave US 31

SEGMENT 2:

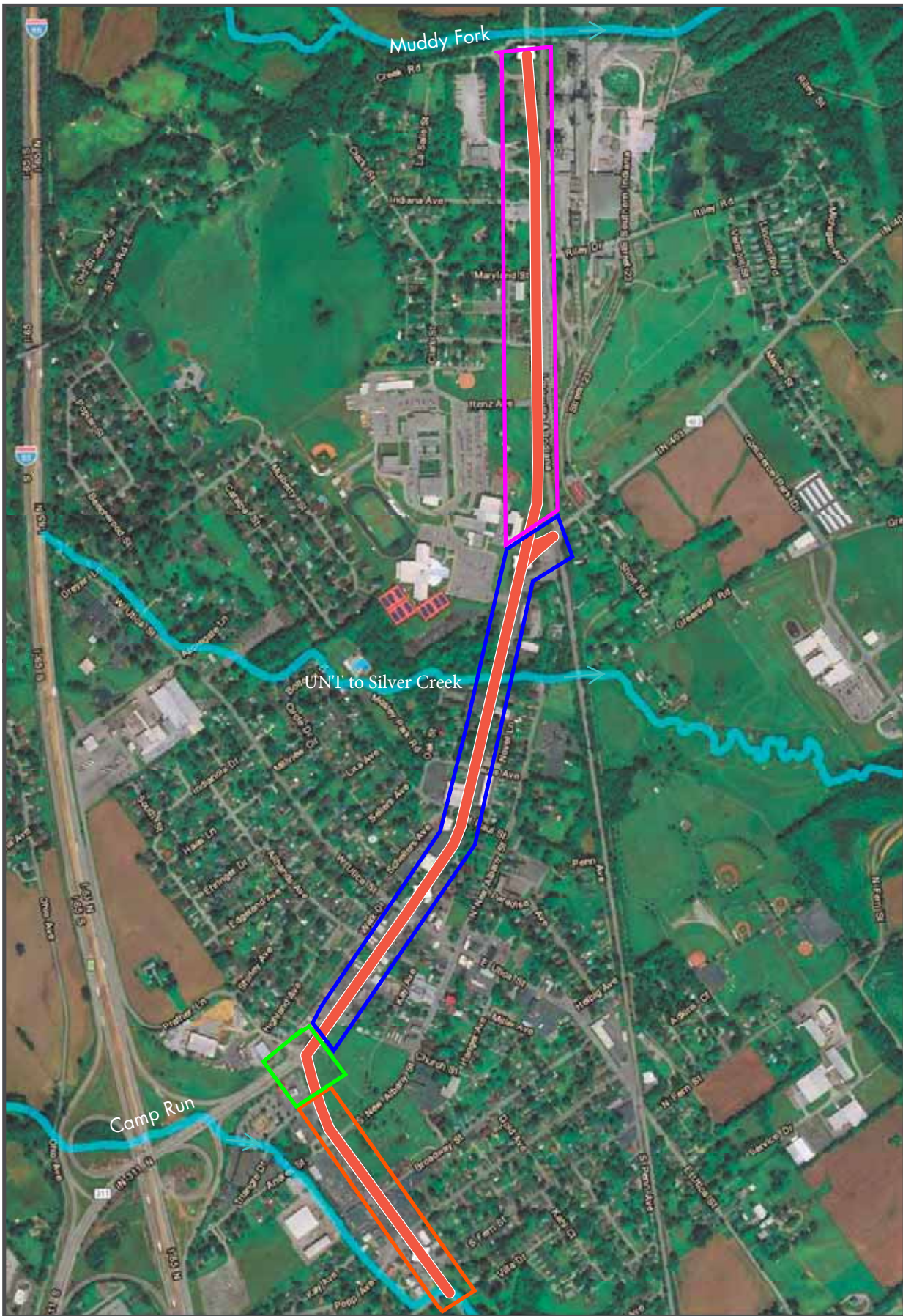
Full-depth pavement replacement and underdrain work; curb ramp replacement

SEGMENT 3:

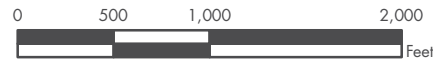
Full-depth pavement replacement; new curb inlets and storm sewer; traffic signal replacement; crosswalk improvements; pavement marking improvements

SEGMENT 4:

Mill and Repave US 31; Curb ramp improvements; Add curb inlets and connect to existing storm sewer



Map Source: Indiana Geological Survey (IGS), IndianaMap, ArcGIS Online (ESRI) World Imagery.



PROJECT SEGMENT LOCATION Aerial Map










US 31 Preventative Pavement Maintenance
Sellersburg, Clark County, IN
Des No. 1700111



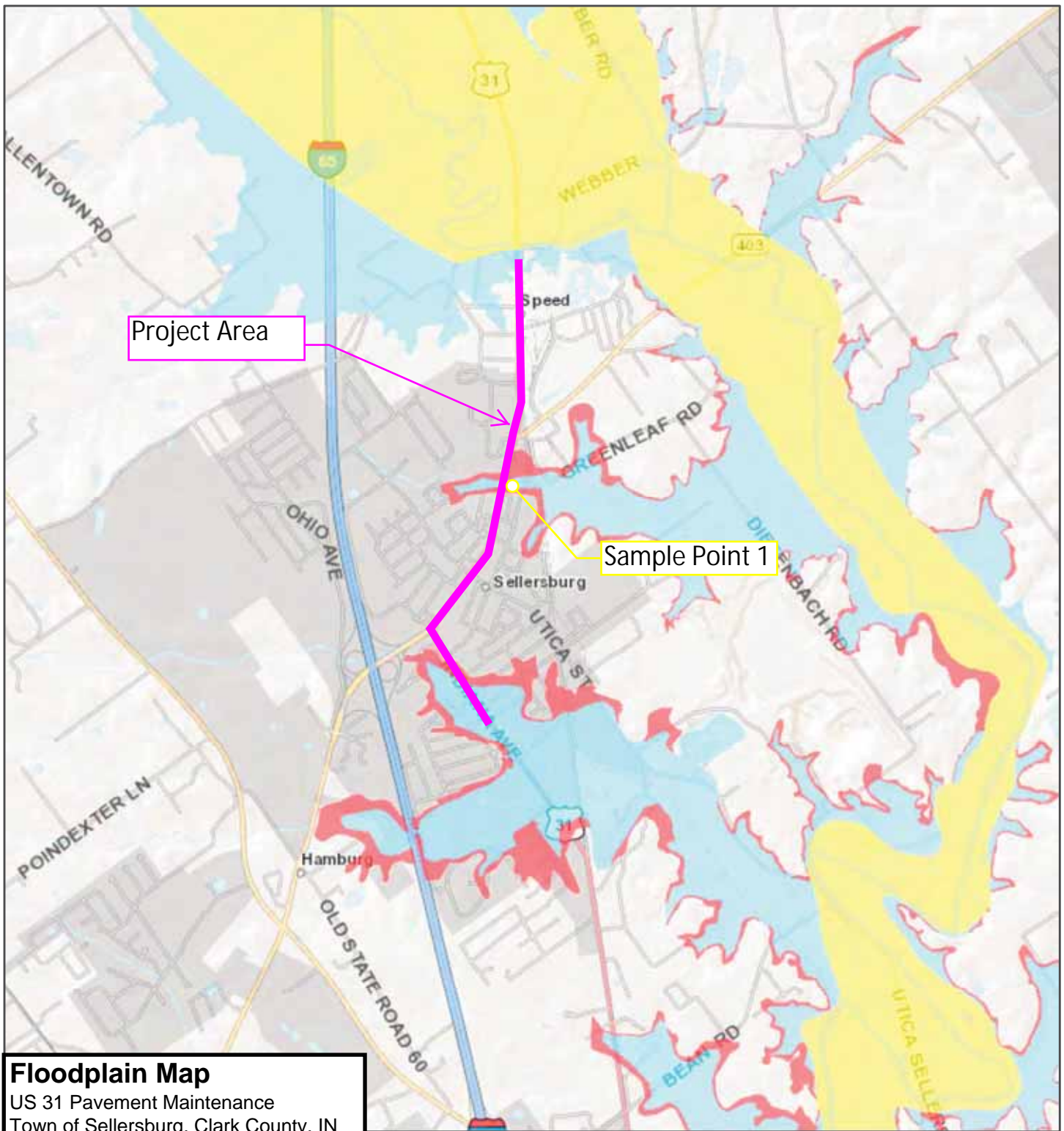


November 20, 2019

Wetlands

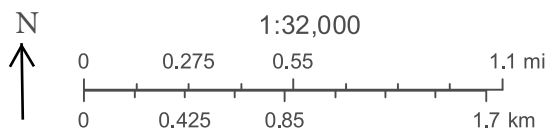
-  Estuarine and Marine Deepwater
-  Estuarine and Marine Wetland
-  Project Area
-  Freshwater Emergent Wetland
-  Freshwater Forested/Shrub Wetland
-  Freshwater Pond
-  Lake
-  Other
-  Riverine

This map is for general reference only. The US Fish and Wildlife Service is not responsible for the accuracy or currentness of the base data shown on this map. All wetlands related data should be used in accordance with the layer metadata found on the Wetlands Mapper web site.



Floodplain Map
 US 31 Pavement Maintenance
 Town of Sellersburg, Clark County, IN
 Des. No. 1700111

- Floodplains - FIRM (Mar 2020)**
- Floodway
 - 1% Annual Chance Flood Hazard
 - 0.2% Annual Chance, Protected by Levee
 - 0.2% Annual Chance Flood Hazard



Indiana Department of Transportation (INDOT), U.S. Census Bureau (USCB), Indiana Geographic Information Council (IGIC), UITS, Indiana Spatial Data Portal, Federal Emergency Management Agency (FEMA), Indiana Department of Natural Resources (IDNR)

US 31 Preventative Pavement Maintenance Des. 1700111, Clark County, IN

Soil Map—Clark County, Indiana
(US 31 Pavement Maintenance- Sellersburg)



Map Scale: 1:16,300 if printed on A portrait (8.5" x 11") sheet.




Map projection: Web Mercator Corner coordinates: WGS84 Edge tics: UTM Zone 16N WGS84

 Project Area

MAP LEGEND

Area of Interest (AOI)

 Area of Interest (AOI)

Soils

 Soil Map Unit Polygons

 Soil Map Unit Lines

 Soil Map Unit Points

Special Point Features



Blowout



Borrow Pit



Clay Spot



Closed Depression



Gravel Pit



Gravelly Spot



Landfill



Lava Flow



Marsh or swamp



Mine or Quarry



Miscellaneous Water



Perennial Water



Rock Outcrop



Saline Spot



Sandy Spot



Severely Eroded Spot



Sinkhole



Slide or Slip



Sodic Spot



Spoil Area



Stony Spot



Very Stony Spot



Wet Spot



Other



Special Line Features

Water Features



Streams and Canals

Transportation



Rails



Interstate Highways



US Routes



Major Roads



Local Roads

Background



Aerial Photography

MAP INFORMATION

The soil surveys that comprise your AOI were mapped at 1:12,000.

Please rely on the bar scale on each map sheet for map measurements.

Source of Map: Natural Resources Conservation Service

Web Soil Survey URL:

Coordinate System: Web Mercator (EPSG:3857)

Maps from the Web Soil Survey are based on the Web Mercator projection, which preserves direction and shape but distorts distance and area. A projection that preserves area, such as the Albers equal-area conic projection, should be used if more accurate calculations of distance or area are required.

This product is generated from the USDA-NRCS certified data as of the version date(s) listed below.

Soil Survey Area: Clark County, Indiana

Survey Area Data: Version 22, Sep 16, 2019

Soil map units are labeled (as space allows) for map scales 1:50,000 or larger.

Date(s) aerial images were photographed: Feb 12, 2012—Jul 5, 2014

The orthophoto or other base map on which the soil lines were compiled and digitized probably differs from the background imagery displayed on these maps. As a result, some minor shifting of map unit boundaries may be evident.

Map Unit Legend

Map Unit Symbol	Map Unit Name	Acres in AOI	Percent of AOI
HcgAV	Haymond silt loam, 0 to 2 percent slopes, frequently flooded, very brief duration	0.0	0.1%
Uaa	Udorthents, cut and filled	19.9	28.9%
UngB	Urban land-Udarents, fragipan substratum, complex, till plain, 0 to 12 percent slopes	47.7	69.5%
WokAW	Wilbur silt loam, 0 to 2 percent slopes, occasionally flooded, very brief duration	1.1	1.6%
Totals for Area of Interest		68.7	100.0%



Legend

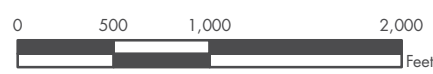
Project Area



Photo Location



Map Source: Indiana Geological Survey (IGS), IndianaMap, ArcGIS Online (ESRI) World Imagery.



Aerial Map

US 31 Preventative Pavement Maintenance
Sellersburg, Clark County, IN
Des No. 1700111

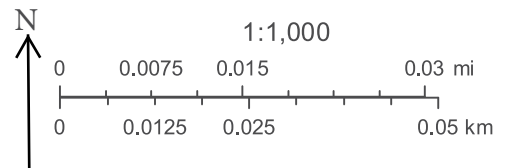
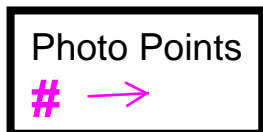




INVESTIGATION AREA 1

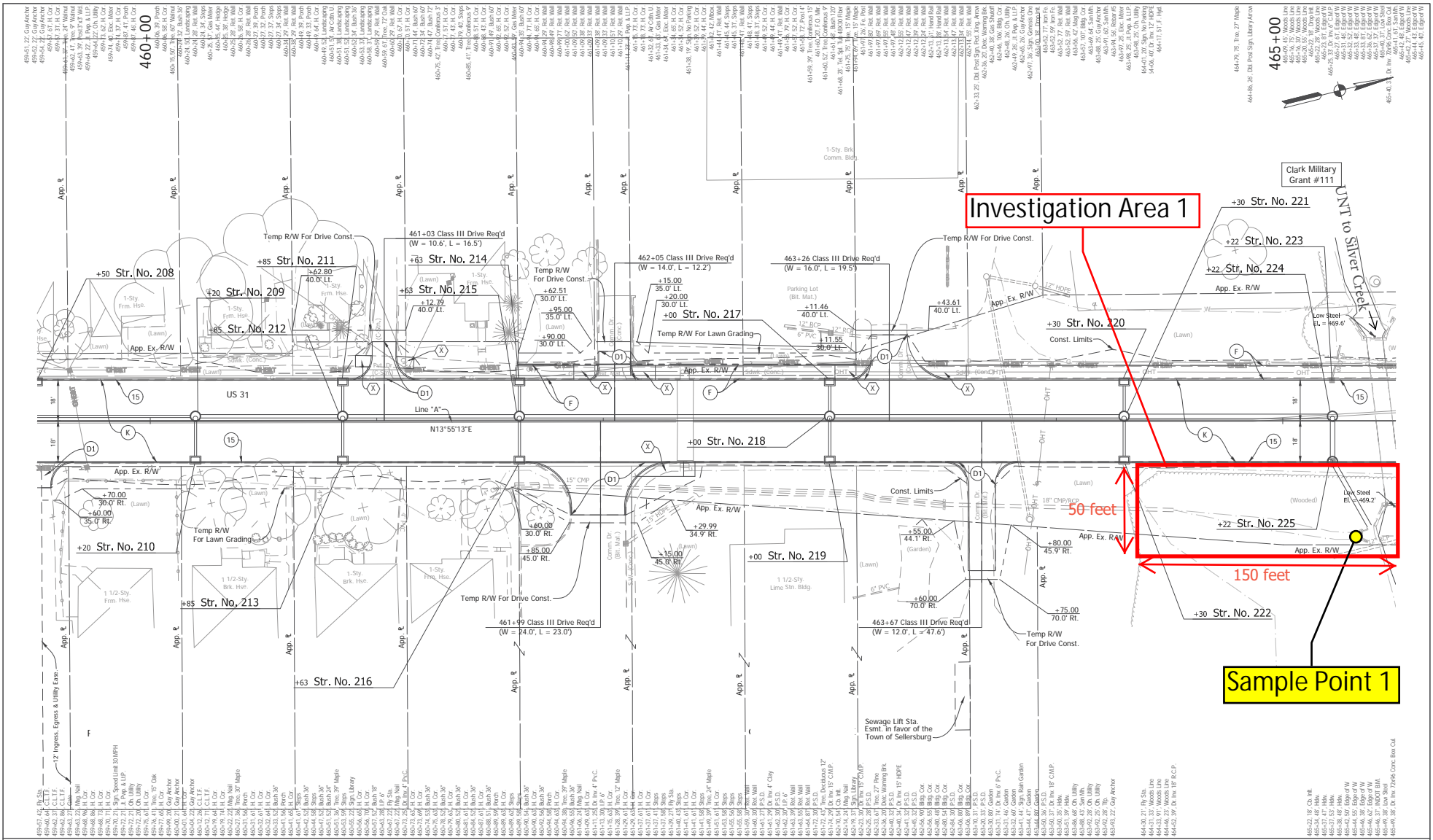


US 31 Preventative Pavement Maintenance
 Town of Sellersburg, Clark County, IN
 Des. No. 1700111



Indiana Office of Information Technology, Indiana University Spatial Data Portal, UITS, Woolpert Inc.
 Indiana Department of Transportation (INDOT), U.S. Census Bureau (USCB), Indiana Geographic Information Council (IGIC), UITS, Indiana Spatial Data Portal

DIRECTORY: S:\Project Files\2017-0199\CADD\Design\Sheet Sets
 FILE: 170199 Plan Sheets.dwg
 PLOTTED BY: mwarmen
 DATE: Sep 14, 2020 - 6:37am



(D)	6" PCBP on Dense Graded Subbase on Subgrade Treatment, Type II	(K)	Full Depth Pavement	(R)	Resurfacing	(DW)	Detectable Warning Surface	(15)	Curb
(D1)	PCBP on Dense Graded Subbase on Subgrade Treatment, Type II	(K1)	Full Depth Pavement, HMA for Approaches	(S)	Subgrade Treatment, Type XX	(X)	Concrete Curb Ramp	(16)	Center Curb
(F)	Sidewalk, Concrete, 4"	(U)	Underdrain	(O)	Compacted Aggregate	(26)	Sodding		

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: BWS	DRAWN: MRW	
CHECKED: ACD	CHECKED: BWS	

INDIANA
 DEPARTMENT OF TRANSPORTATION
 PLAN SHEET
 STA. 459+50 TO STA. 465+50 "A"

HORIZONTAL SCALE	BRIDGE FILE
1"=20'	N/A
VERTICAL SCALE	DESIGNATION
	1700111
SURVEY BOOK	SHEETS
	43 of 128
CONTRACT	PROJECT
R-40412	1700111



INVESTIGATION AREA 2



US 31 Preventative Pavement Maintenance
 Town of Sellersburg, Clark County, IN
 Des. No. 1700111

1:1,000

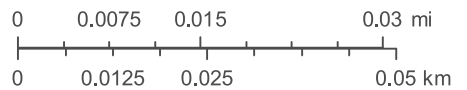
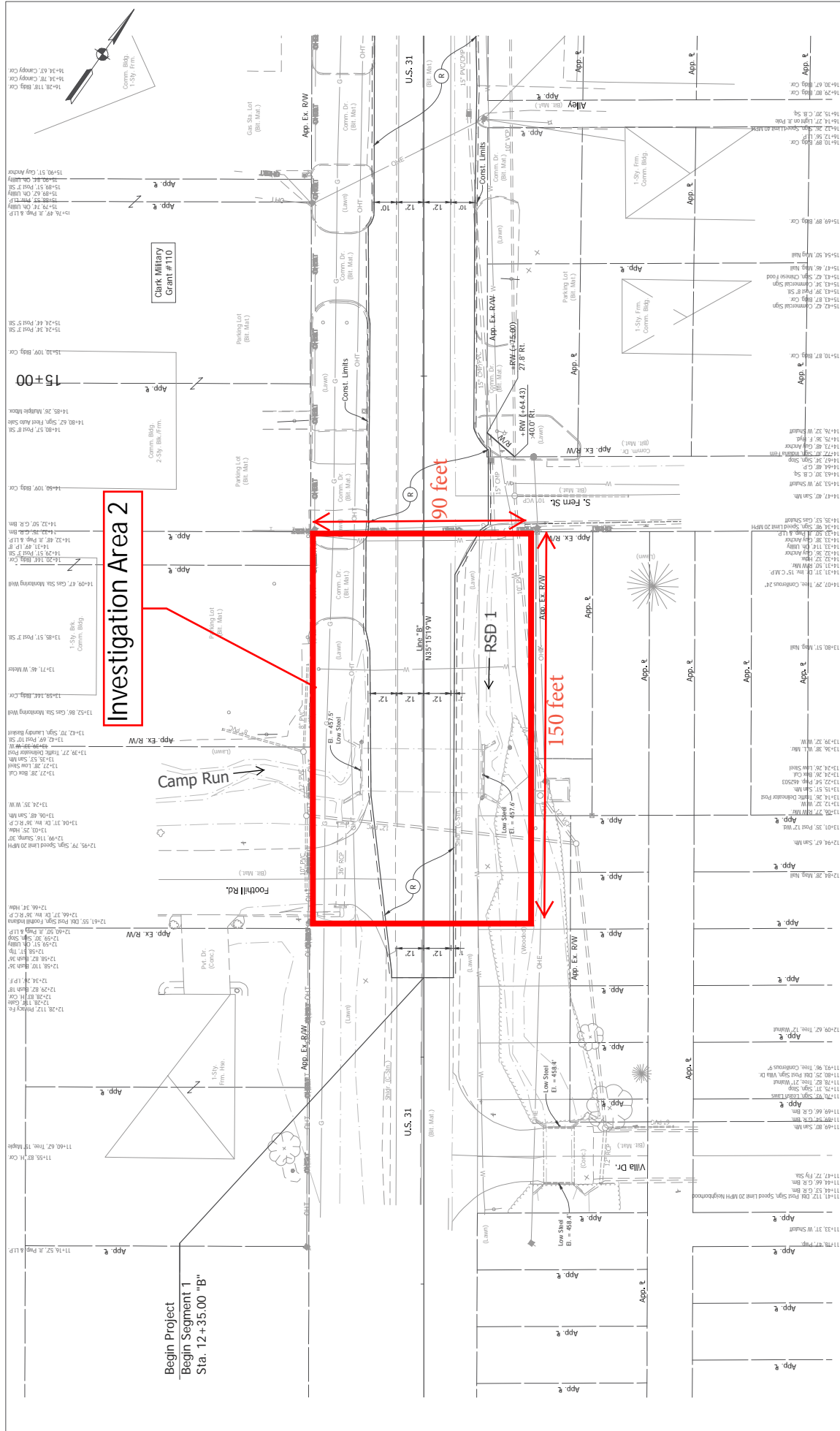


Photo Points

→

Indiana Office of Information Technology, Indiana University Spatial Data Portal, UITS, Woolpert Inc.
 Indiana Department of Transportation (INDOT), U.S. Census Bureau (USCB), Indiana Geographic Information Council (IGIC), UITS, Indiana Spatial Data Portal



INDIANA DEPARTMENT OF TRANSPORTATION		PLAN SHEET STA. 10+50 TO STA. 16+50 "B"	
HORIZONTAL SCALE: 1"=20' VERTICAL SCALE: 1"=10' SHEETS: 25 of 128 CONTRACT: 64002		BRIDGE FILE: N/A DESIGNATION: 170011 SURVEY BOOK: 25 CONTRACT: 64002	
RECOMMENDED FOR APPROVAL: _____ DESIGNER: BWS CHECKED: ACD		DATE: _____ DESIGN ENGINEER: _____ DRAWN: MVR CHECKED: BWS	
6" PCRP on Subgrade Treatment, Type II RCC Paved Subbase on Geogrid, Type IB on Subgrade Treatment, Type II Sidewalk, Concrete, 4"	DW Detectable Warning Surface X Concrete Curb Ramp O Compacted Aggregate	15 Curb 16 Center Curb 20 Sodding	R Resurfacing S Subgrade Treatment, Type XX U Underdrain
K Full Depth Pavement, HMA for Approaches K1	Full Depth Pavement, HMA for Approaches K1	15 Curb 16 Center Curb 20 Sodding	R Resurfacing S Subgrade Treatment, Type XX U Underdrain



Photo 1: General Project Corridor View; looking north along US 31 near the south terminus of the overall project; no roadside drainage features noted



Photo 2: General Project Corridor View; looking north along US 31 towards the SR 311 / Charlestowns Road Junction; no roadside drainage features noted



Photo 3: General Project Corridor View; looking northeast along US 31 between the SR 331/Charlestown Road Junction and Utica Street; storm water is managed by existing curb and gutter in this area



Photo 4: General Project Corridor View; looking northeast along US 31 near Hauss Avenue; storm water is managed by existing curb and gutter in this area



Photo 5: General Project Corridor View; looking north along US 31 at the intersection of Old SR 403; no roadside drainage features noted



Photo 6: General Project Corridor View; looking north along US 31 near the north project terminus; no roadside drainage features noted



Photo 7: Investigation Area 1; looking north along the east side of US 31; culvert shown on the left carries US 31 over UNT to Silver Creek (downstream side pictured); no culvert work is proposed



Photo 8: Investigation Area 1; looking east (downstream) along UNT to Silver Creek from the outlet of the US 31 culvert

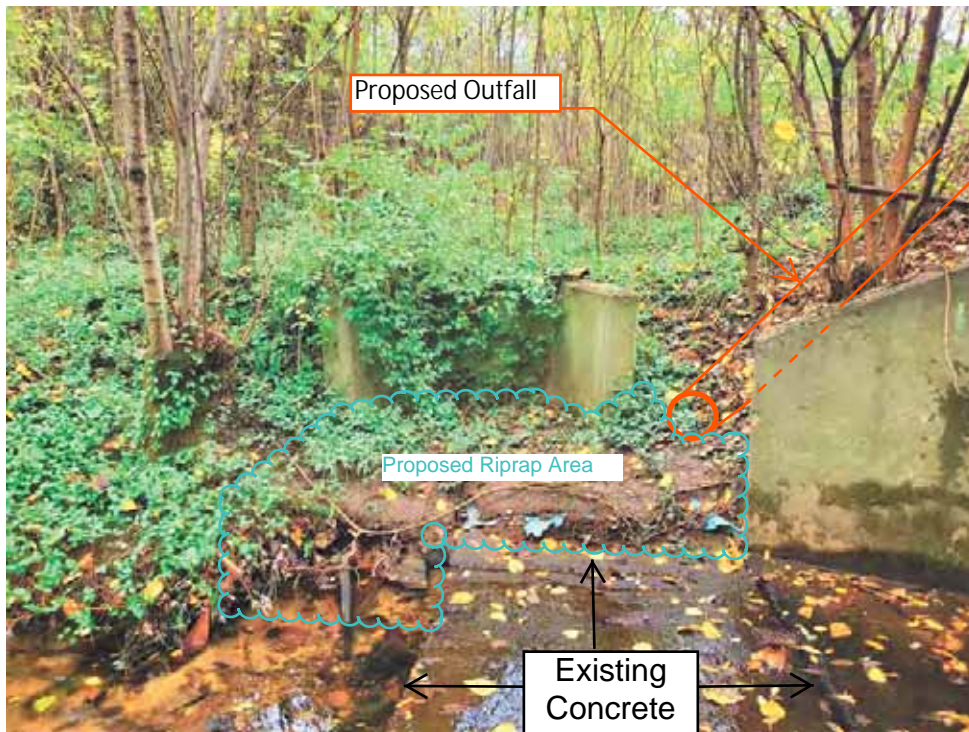


Photo 9: Investigation Area 1; view of the southeast quadrant of the US 31 crossing of UNT to Silver Creek; proposed new storm water outfall and riprap area noted



Photo 10: Investigation Area 1; view of the south bank of UNT to Silver Creek east (downstream) of the proposed outfall/riprap location

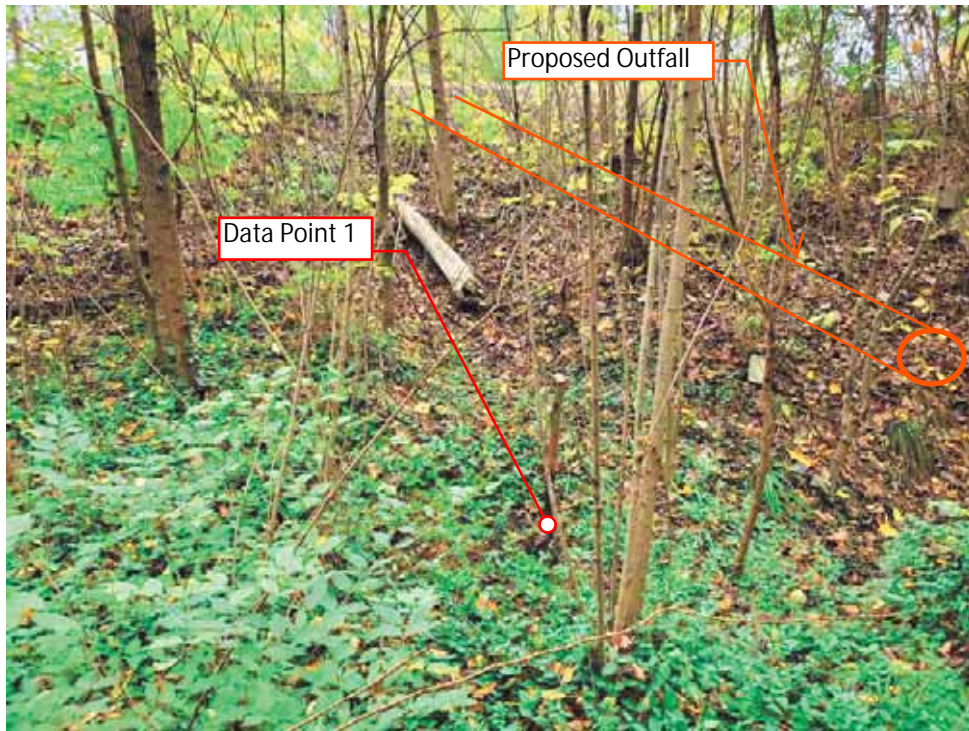


Photo 11: Investigation Area 1; Data Point 1 (non-wetland); looking west towards US 31 and the proposed new storm water outfall structure



Photo 12: Investigation Area 1; Data Point 1 (non-wetland); view of sample location



Photo 13: Investigation Area 2; Looking east (upstream) along Camp Run; US 31 is carried by the culvert shown in the background



Photo 14: Investigation Area 2; Looking west (downstream) along Camp Run from the US 31 crossing



Photo 15: Investigation Area 2; looking east (upstream) along Camp Run from US 31



Photo 16: Investigation Area 2; looking south along the east side of US 31 towards the inlet of Camp Run



Photo 17: Investigation Area 2; looking north at a roadside ditch (RSD 1) located along the east side of US 31 approximately 50 feet north of Camp Run



Photo 18: Investigation Area 2; looking south from Fern Street at a roadside ditch (RSD1) long the east side of US 31; culvert carrying US 31 over Camp Run shown in the background

WETLAND DETERMINATION DATA FORM – Midwest Region

Project/Site: US 31 Pavement Maintenance City/County: Town of Sellersburg/Clark County Sampling Date: 10/28/2020
 Applicant/Owner: Indiana Department of Transportation State: IN Sampling Point: 1
 Investigator(s): Ryan Scott (BF&S Inc.) Section, Township, Range: Section (CMG) 110, 111, 130, Township 1 S, Range 6 E
 Landform (hillslope, terrace, etc.): depression Local relief (concave, convex, none): none
 Slope (%): <1% Lat: 38.40340 Long: -85.75364 Datum: NAD83
 Soil Map Unit Name: Wilbur silt loam, 0 to 2 percent slopes, occasionally flooded, very brief duration NWI classification: None

Are climatic / hydrologic conditions on the site typical for this time of year? Yes No (If no, explain in Remarks.)
 Are Vegetation N, Soil N, or Hydrology N significantly disturbed? Are "Normal Circumstances" present? Yes No
 Are Vegetation N, Soil N, or Hydrology N naturally problematic? (If needed, explain any answers in Remarks.)

SUMMARY OF FINDINGS – Attach site map showing sampling point locations, transects, important features, etc.

Hydrophytic Vegetation Present? Yes <input type="checkbox"/> No <input checked="" type="checkbox"/> Hydric Soil Present? Yes <input type="checkbox"/> No <input checked="" type="checkbox"/> Wetland Hydrology Present? Yes <input type="checkbox"/> No <input checked="" type="checkbox"/>	Is the Sampled Area within a Wetland? Yes <input type="checkbox"/> No <input checked="" type="checkbox"/>
Remarks:	

VEGETATION – Use scientific names of plants.

Tree Stratum (Plot size: <u>30-ft radius</u>)	Absolute % Cover	Dominant Species?	Indicator Status	
1. <u>Celtis occidentalis</u>	<u>30</u>	<u>Y</u>	<u>FAC</u>	Dominance Test worksheet: Number of Dominant Species That Are OBL, FACW, or FAC: <u>1</u> (A) Total Number of Dominant Species Across All Strata: <u>4</u> (B) Percent of Dominant Species That Are OBL, FACW, or FAC: <u>25%</u> (A/B)
2. <u>Morus rubra</u>	<u>30</u>	<u>Y</u>	<u>FACU</u>	
3. <u>Ulmus americana</u>	<u>10</u>	<u>N</u>	<u>FACW</u>	
4. _____				
5. _____				
	<u>70</u>			
Sapling/Shrub Stratum (Plot size: <u>15-ft radius</u>)				
1. <u>Lonicera maackii</u>	<u>20</u>	<u>Y</u>	<u>UPL</u>	Prevalence Index worksheet: Total % Cover of: _____ Multiply by: _____ OBL species _____ x 1 = _____ FACW species <u>10</u> x 2 = <u>20</u> FAC species <u>35</u> x 3 = <u>105</u> FACU species <u>30</u> x 4 = <u>120</u> UPL species <u>90</u> x 5 = <u>450</u> Column Totals: <u>165</u> (A) <u>695</u> (B) Prevalence Index = B/A = <u>4.21</u>
2. <u>Acer negundo</u>	<u>5</u>	<u>N</u>	<u>FAC</u>	
3. _____				
4. _____				
5. _____				
	<u>25</u>			
Herb Stratum (Plot size: <u>5-ft radius</u>)				
1. <u>Euonymus fortunei</u>	<u>70</u>	<u>Y</u>	<u>UPL</u>	Hydrophytic Vegetation Indicators: ___ 1 - Rapid Test for Hydrophytic Vegetation ___ 2 - Dominance Test is >50% ___ 3 - Prevalence Index is ≤3.0 ¹ ___ 4 - Morphological Adaptations ¹ (Provide supporting data in Remarks or on a separate sheet) ___ Problematic Hydrophytic Vegetation ¹ (Explain) ¹ Indicators of hydric soil and wetland hydrology must be present, unless disturbed or problematic.
2. _____				
3. _____				
4. _____				
5. _____				
6. _____				
7. _____				
8. _____				
9. _____				
10. _____				
	<u>70</u>			
Woody Vine Stratum (Plot size: <u>15-ft radius</u>)				
1. <u>None observed</u>				Hydrophytic Vegetation Present? Yes <input type="checkbox"/> No <input checked="" type="checkbox"/>
2. _____				

Remarks: (Include photo numbers here or on a separate sheet.)

Photos 11 and 12

SOIL

Sampling Point: 1

Profile Description: (Describe to the depth needed to document the indicator or confirm the absence of indicators.)								
Depth (inches)	Matrix		Redox Features				Texture	Remarks
	Color (moist)	%	Color (moist)	%	Type ¹	Loc ²		
0-8	10 YR 4/3	100					Silt loam	<1/2 inch ribbon
8-20	10YR 5/3	100					Silt loam	
¹ Type: C=Concentration, D=Depletion, RM=Reduced Matrix, MS=Masked Sand Grains.					² Location: PL=Pore Lining, M=Matrix.			
Hydric Soil Indicators:						Indicators for Problematic Hydric Soils³:		
<input type="checkbox"/> Histosol (A1) <input type="checkbox"/> Histic Epipedon (A2) <input type="checkbox"/> Black Histic (A3) <input type="checkbox"/> Hydrogen Sulfide (A4) <input type="checkbox"/> Stratified Layers (A5) <input type="checkbox"/> 2 cm Muck (A10) <input type="checkbox"/> Depleted Below Dark Surface (A11) <input type="checkbox"/> Thick Dark Surface (A12) <input type="checkbox"/> Sandy Mucky Mineral (S1) <input type="checkbox"/> 5 cm Mucky Peat or Peat (S3)			<input type="checkbox"/> Sandy Gleyed Matrix (S4) <input type="checkbox"/> Sandy Redox (S5) <input type="checkbox"/> Stripped Matrix (S6) <input type="checkbox"/> Loamy Mucky Mineral (F1) <input type="checkbox"/> Loamy Gleyed Matrix (F2) <input type="checkbox"/> Depleted Matrix (F3) <input type="checkbox"/> Redox Dark Surface (F6) <input type="checkbox"/> Depleted Dark Surface (F7) <input type="checkbox"/> Redox Depressions (F8)			<input type="checkbox"/> Coast Prairie Redox (A16) <input type="checkbox"/> Dark Surface (S7) <input type="checkbox"/> Iron-Manganese Masses (F12) <input type="checkbox"/> Very Shallow Dark Surface (TF12) <input type="checkbox"/> Other (Explain in Remarks)		
Restrictive Layer (if observed):								
Type: _____								
Depth (Inches): _____						Hydric Soil Present? Yes _____ No <input checked="" type="checkbox"/>		
Remarks:								

HYDROLOGY

Wetland Hydrology Indicators:		
Primary Indicators (minimum of one is required; check all that apply)	Secondary Indicators (minimum of two required)	
<input type="checkbox"/> Surface Water (A1) <input type="checkbox"/> High Water Table (A2) <input type="checkbox"/> Saturation (A3) <input type="checkbox"/> Water Marks (B1) <input type="checkbox"/> Sediment Deposits (B2) <input type="checkbox"/> Drift Deposits (B3) <input type="checkbox"/> Algal Mat or Crust (B4) <input type="checkbox"/> Iron Deposits (B5) <input type="checkbox"/> Inundation Visible on Aerial Imagery (B7) <input type="checkbox"/> Sparsely Vegetated Concave Surface (B8)	<input type="checkbox"/> Water-Stained Leaves (B9) <input type="checkbox"/> Aquatic Fauna (B13) <input type="checkbox"/> True Aquatic Plants (B14) <input type="checkbox"/> Hydrogen Sulfide Odor (C1) <input type="checkbox"/> Oxidized Rhizospheres on Living Roots (C3) <input type="checkbox"/> Presence of Reduced Iron (C4) <input type="checkbox"/> Recent Iron Reduction in Tilled Soils (C6) <input type="checkbox"/> Thin Muck Surface (C7) <input type="checkbox"/> Gauge or Well Data (D9) <input type="checkbox"/> Other (Explain in Remarks)	<input type="checkbox"/> Surface Soil Cracks (B6) <input type="checkbox"/> Drainage Patterns (B10) <input type="checkbox"/> Dry-Season Water Table (C2) <input type="checkbox"/> Crayfish Burrows (C8) <input type="checkbox"/> Saturation Visible on Aerial Imagery (C9) <input type="checkbox"/> Stunted or Stressed Plants (D1) <input checked="" type="checkbox"/> Geomorphic Position (D2) <input type="checkbox"/> FAC-Neutral Test (D5) No; 0:2
Field Observations:		
Surface Water Present? Yes _____ No <input checked="" type="checkbox"/>	Depth (inches): _____	
Water Table Present? Yes _____ No <input checked="" type="checkbox"/>	Depth (inches): _____	
Saturation Present? (includes capillary fringe) Yes _____ No <input checked="" type="checkbox"/>	Depth (inches): _____	
Wetland Hydrology Present? Yes _____ No <input checked="" type="checkbox"/>		
Describe Recorded Data (stream gauge, monitoring well, aerial photos, previous inspections), if available:		
Remarks:		

Appendix 2 - PRELIMINARY JURISDICTIONAL DETERMINATION (PJD) FORM

BACKGROUND INFORMATION

A. REPORT COMPLETION DATE FOR PJD: October 28, 2020

B. NAME AND ADDRESS OF PERSON REQUESTING PJD: Ryan Scott, BF&S, 8450 Westfield Blvd., Indianapolis, IN 46240/317-713-4615/rscott@bfsengr.com

C. DISTRICT OFFICE, FILE NAME, AND NUMBER:

D. PROJECT LOCATION(S) AND BACKGROUND INFORMATION: Des. No. 1700111;US 31 Pavement Maintenance

State: **IN** County/parish/borough: **Clark** City: **near Jeffersonville**

Center coordinates of site (lat/long in degree decimal format):

Lat.: **38.40344** Long.: **-85.75364**

Universal Transverse Mercator: UTM 17 84830.15 E; 4261291.51 N

Name of nearest waterbody: **UNT to Silver Creek**

E. REVIEW PERFORMED FOR SITE EVALUATION (CHECK ALL THAT APPLY):

Office (Desk) Determination. Date:

Field Determination. Date(s):

TABLE OF AQUATIC RESOURCES IN REVIEW AREA WHICH "MAY BE" SUBJECT TO REGULATORY JURISDICTION.

Site number	Latitude (decimal degrees)	Longitude (decimal degrees)	Estimated amount of aquatic resource in review area (acreage and linear feet, if applicable)	Type of aquatic resource (i.e., wetland vs. non-wetland waters)	Geographic authority to which the aquatic resource "may be" subject (i.e., Section 404 or Section 10/404)
UNT to Silver Ck.	38.40344	-85.75364	50 linear feet	non-wetland waters	Section 404
Camp Run	38.39147	-85.75525	90 linear feet	non-wetland waters	Section 404

- 1) The Corps of Engineers believes that there may be jurisdictional aquatic resources in the review area, and the requestor of this PJD is hereby advised of his or her option to request and obtain an approved JD (AJD) for that review area based on an informed decision after having discussed the various types of JDs and their characteristics and circumstances when they may be appropriate.
- 2) In any circumstance where a permit applicant obtains an individual permit, or a Nationwide General Permit (NWP) or other general permit verification requiring "pre-construction notification" (PCN), or requests verification for a non-reporting NWP or other general permit, and the permit applicant has not requested an AJD for the activity, the permit applicant is hereby made aware that: (1) the permit applicant has elected to seek a permit authorization based on a PJD, which does not make an official determination of jurisdictional aquatic resources; (2) the applicant has the option to request an AJD before accepting the terms and conditions of the permit authorization, and that basing a permit authorization on an AJD could possibly result in less compensatory mitigation being required or different special conditions; (3) the applicant has the right to request an individual permit rather than accepting the terms and conditions of the NWP or other general permit authorization; (4) the applicant can accept a permit authorization and thereby agree to comply with all the terms and conditions of that permit, including whatever mitigation requirements the Corps has determined to be necessary; (5) undertaking any activity in reliance upon the subject permit authorization without requesting an AJD constitutes the applicant's acceptance of the use of the PJD; (6) accepting a permit authorization (e.g., signing a proffered individual permit) or undertaking any activity in reliance on any form of Corps permit authorization based on a PJD constitutes agreement that all aquatic resources in the review area affected in any way by that activity will be treated as jurisdictional, and waives any challenge to such jurisdiction in any administrative or judicial compliance or enforcement action, or in any administrative appeal or in any Federal court; and (7) whether the applicant elects to use either an AJD or a PJD, the JD will be processed as soon as practicable. Further, an AJD, a proffered individual permit (and all terms and conditions contained therein), or individual permit denial can be administratively appealed pursuant to 33 C.F.R. Part 331. If, during an administrative appeal, it becomes appropriate to make an official determination whether geographic jurisdiction exists over aquatic resources in the review area, or to provide an official delineation of jurisdictional aquatic resources in the review area, the Corps will provide an AJD to accomplish that result, as soon as is practicable. This PJD finds that there "*may be*" waters of the U.S. and/or that there "*may be*" navigable waters of the U.S. on the subject review area, and identifies all aquatic features in the review area that could be affected by the proposed activity, based on the following information:


SUPPORTING DATA. Data reviewed for PJD (check all that apply)

Checked items should be included in subject file. Appropriately reference sources below where indicated for all checked items:

- Maps, plans, plots or plat submitted by or on behalf of the PJD requestor.
Map: State, Quad, Aerial, Plans
- Data sheets prepared/submitted by or on behalf of the PJD requestor.
 - Office concurs with data sheets/delineation report.
 - Office does not concur with data sheets/delineation report. Rationale: _____
- Data sheets prepared by the Corps: _____
- Corps navigable waters' study: _____
- U.S. Geological Survey Hydrologic Atlas: _____
 - USGS NHD data.
 - USGS 8 and 12 digit HUC maps.
- U.S. Geological Survey map(s). Cite scale & quad name: Speed, Indiana (scale as noted)
- Natural Resources Conservation Service Soil Survey. Citation: Clark County Soil Survey
- National wetlands inventory map(s). Cite name: Sellersburg, Indiana
- State/local wetland inventory map(s): _____
- FEMA/FIRM maps: IDNR Floodplain Map
- 100-year Floodplain Elevation is: _____ (National Geodetic Vertical Datum of 1929)
- Photographs: Aerial (Name & Date): 2016 Orthophotography (leaves on)
or Other (Name & Date): Site Photos 7/2/2020 and 10/28/2020
- Previous determination(s). File no. and date of response letter: _____
- Other information (please specify): _____

IMPORTANT NOTE: The information recorded on this form has not necessarily been verified by the Corps and should not be relied upon for later jurisdictional determinations.

Signature and date of
Regulatory staff member
completing PJD

 11-24-20
Signature and date of
person requesting PJD
(REQUIRED, unless obtaining
the signature is impracticable)¹

¹ Districts may establish timeframes for requestor to return signed PJD forms. If the requestor does not respond within the established time frame, the district may presume concurrence and no additional follow up is necessary prior to finalizing an action.

Appendix G

Public Involvement

December 14, 2017

NOTICE OF SURVEY

Sample Notice of
Survey

RE: Topographic Survey for Pavement Replacement on U.S. 31 from 1.53 Miles North of S.R. 60 (Foothill Rd.) to 3.28 Miles North of S.R. 60, Sellersburg, Clark County, Indiana, Des. No. 1700111

Dear Property Owner(s):

The Indiana Department of Transportation has selected Butler, Fairman and Seufert, Inc., to survey and design the referenced project. Courthouse records show that you are a property owner within the limits of the area where data will be collected for the project survey. It may be necessary for our employees to enter your property to complete this work. This is permitted by law per Indiana Code IC 8-23-7-6. If you have sold this property, or it is occupied by someone else, please let us know the name and address of the new owner or current occupant so we can contact them about the survey.

At this stage, we generally do not know what effect, if any, our project can eventually have on your property. If we determine later that your property is involved, we will contact you with additional information.

The survey work will include mapping the location of features such as trees, buildings, fences and drives, and obtaining ground elevations. The survey is needed for the proper planning and design of this highway project. Please be assured of our sincere desire to cause you as little inconvenience as possible during this survey. If problems do occur, please contact our field crew or contact me at the telephone number or address shown above or the included e-mail address.

Sincerely,

BUTLER, FAIRMAN and SEUFERT, INC.

*Mark W. Neal, P.S.
mneal@bfsengr.com*

MWN:lm



LEGAL NOTICE OF PLANNED IMPROVEMENT

The Indiana Department of Transportation (INDOT) is developing plans for proposed Road Rehabilitation and Drainage Improvements to US 31 in the Town of Sellersburg and the Community of Speed in Clark County.

The need for the project is derived from the deteriorated conditions along US 31. Portions of the corridor exhibit significant cracking in the asphalt or concrete surface. Existing curbs are buried and deteriorating. Drainage is insufficient to handle stormwater. In the period between 2010 and 2018 there were 463 crashes within the project area. About 63% were rear-end crashes, which are associated with signalized intersections, congested traffic conditions, and driver inattention. The purpose of this project is to extend the functional life of the existing pavement, improve drainage, provide ADA-compliant pedestrian facilities, and improve safety along US 31 between Foothill Road and the Silver Creek Bridge.

The project proposes to mill and repave US 31 from Foothill Road to Triangle Drive. At the US 31/SR 311/Prather Lane intersection there will be a full-depth pavement replacement with underdrains; curb ramp and traffic signal replacements; and minor pavement widening to the southwest-bound approach of US 31 to provide a shared through/right-turn lane. From Bucheit Street to CR 403, work will consist of a full-depth pavement replacement with underdrains; new curb inlets and storm sewer trunkline; a new stormwater outlet to the tributary to Silver Creek, including in-line detention to release stormwater at the existing rate; and curb ramp and traffic signal replacement. High-visibility pavement markings and signage will be added to the crosswalk at the St. Paul Street intersection and the Sellersburg Library. The mid-block crosswalk near Silver Creek Elementary and Silver Creek Middle School will be relocated to the US 31/CR 403 intersection. A 6-foot-wide concrete sidewalk will be installed on the east side of US 31 between the mid-block crossing and CR 403. The right turn lane from southbound CR 403 to northbound US 31 will be extended to the L&I Railroad tracks. Pavement markings from Utica Street to CR 403 to create a 12-foot wide two-way left-turn lane (TWLTL). From CR 403 to the Silver Creek Bridge, work will consist of a mill and repave of US 31; replacement of curb ramps, sidewalks, and inlet castings; and the addition of new curb inlets and connect to existing storm sewer. Pavement markings would be revised to provide 12-foot wide TWLTL from CR 403 to Silver Creek Elementary and Silver Creek Middle School entrances.

The proposed construction of this project will require approximately 1.78 acres of new permanent right-of-way and 0.83 acre of temporary right-of-way. The project is approximately 1.75 miles long.

The maintenance of traffic (MOT) plan proposes road closure of US 31 between SR 311 and CR 403. The remainder of the project will be constructed under traffic. An official state route detour utilizing SR 60, I-65, and Blue Lick Road or SR 160 will be used during construction. Local roads may be used by local traffic. Access to all properties will be maintained throughout construction. Project stakeholders including local school corporations and emergency services will be notified of closures prior to construction. The proposed start of construction is in the Spring of 2023.

The cost associated with this project is approximately \$9.5 million which includes preliminary engineering, right-of-way, and construction with both federal and state funds anticipated to be used. The Federal Highway Administration (FHWA) and INDOT have agreed this project falls within the guidelines of a Categorical Exclusion (CE) Level 2 environmental document. Preliminary design plans along with the CE are available for review at the following locations:

1. Indiana Department of Transportation- Seymour District, 185 Agrico Lane, Seymour, IN 4727, (855) INDOT4U (463-6848)
2. Sellersburg Library, 430 N Indiana Avenue, Sellersburg, IN 47172, (812) 246-4493
3. Online at: <https://www.in.gov/indot/4288.htm>

All interested persons may request a public hearing be held, express their concerns by submitting comments, or request project documents be mailed by contacting Elizabet Biggio, Architectural Historian II, Butler, Fairman & Seufert, Inc., 8450 Westfield Blvd., Suite 300, Indianapolis, IN 46240-8302, 317- 713-4615, ebiggio@bfsengr.com, on or before June 4, 2021.

In accordance with the Americans with Disabilities Act (ADA), persons and/or groups requiring project information be made available in alternative formats are encouraged to contact INDOT for the arrangement and coordination of services. Please contact Terry Summers, Project Manager, 185 Agrico Lane, Seymour, IN 47274, (812) 524-3749, tsummers@indot.in.gov. In accordance with Title VI of the Civil Rights Act of 1964, persons and/or groups requiring project information be made available in another language are encouraged to contact INDOT.

This notice is published in compliance with: 1) Code of Federal Regulations, Title 23, Section 771 (CFR 771.111(h)(1) stating, "Each State must have procedures approved by the FHWA to carry out a public involvement/public hearing program."; 2) 23 CFR 450.210(a)(1)(ix) stating, "Provide for the periodic review of the effectiveness of the public involvement process to ensure that the process provides full and open access to all interested parties and revise the process, as appropriate."; and 3) The INDOT *Public Involvement Policies and Procedures* approved by the Federal Highway Administration on August 16, 2012.

**Proof of
Publication**

**STATE OF INDIANA
COUNTY OF CLARK -SS**

Theresa Wheatbrook on oath says that she is
bookkeeper of NEWS AND TRIBUNE and in
the employ of the publisher of

NEWS AND TRIBUNE,

a daily newspaper of general circulation printed and
published in the city of Jeffersonville, Clark County,
State of Indiana, and further says that the annexed
advertisement was published in said paper for
#(2) time(s) to-wit: In issue of said NEWS AND TRIBUNE

Dated: 5/20, 5/27 2021

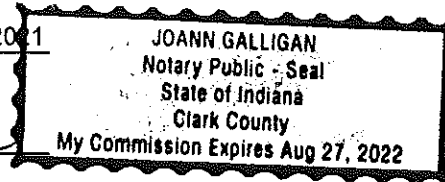
(X) Theresa Wheatbrook

**STATE OF INDIANA
COUNTY OF CLARK**

Subscribed and sworn to before me this

4TH day of June 2021

(X) Joann Galligan
Joann Galligan



Notary Public, Clark County, Indiana
(My Commission Expires August 27, 2022
Commission Number 655965

Publication
Fee \$ 554.80

ID # 04-3314494

1711492

ORDER CONFIRMATION (CONTINUED)

Salesperson: LEGALS

Printed at 06/04/21 10:17 by jgall

Acct #: 15579

Ad #: 1711492

Status: Expired

DES. # 1700111

LEGAL NOTICE OF PLANNED IMPROVEMENT

The Indiana Department of Transportation (INDOT) is developing plans for proposed Road Rehabilitation and Drainage Improvements to US 31 in the Town of Sellersburg and the Community of Speed in Clark County.

The need for the project is derived from the deteriorated conditions along US 31. Portions of the corridor exhibit significant cracking in the asphalt or concrete surface. Existing curbs are buried and deteriorating. Drainage is insufficient to handle stormwater. In the period between 2010 and 2018 there were 463 crashes within the project area. About 63% were rear-end crashes, which are associated with signalized intersections, congested traffic conditions, and driver inattention. The purpose of this project is to extend the functional life of the existing pavement, improve drainage, provide ADA-compliant pedestrian facilities, and improve safety along US 31 between Foothill Road and the Silver Creek Bridge.

The project proposes to mill and repave US 31 from Foothill Road to Triangle Drive. At the US 31/SR 311/Prather Lane intersection there will be a full-depth pavement replacement with underdrains; curb ramp and traffic signal replacements; and minor pavement widening to the southwest-bound approach of US 31 to provide a shared through/right-turn lane. From Buchelt Street to CR 403, work will consist of a full-depth pavement replacement with underdrains; new curb inlets and storm sewer trunkline; a new stormwater outlet to the tributary to Silver Creek, including in-line detention to release stormwater at the existing rate; and curb ramp and traffic signal replacement. High-visibility pavement markings and signage will be added to the crosswalk at the St. Paul Street intersection and the Sellersburg Library. The mid-block crosswalk near Silver Creek Elementary and Silver Creek Middle School will be relocated to the US 31/CR 403 intersection. A 6-foot-wide concrete sidewalk will be installed on the east side of US 31 between the mid-block crossing and CR 403. The right turn lane from southbound CR 403 to northbound US 31 will be extended to the L&I Railroad tracks. Pavement markings from Utica Street to CR 403 to create a 12-foot wide two-way left-turn lane (TWLTL). From CR 403 to the Silver Creek Bridge, work will consist of a mill and repave of US 31; replacement of curb ramps, sidewalks, and inlet castings; and the addition of new curb inlets and connect to existing storm sewer. Pavement markings would be revised to provide 12-foot wide TWLTL from CR 403 to Silver Creek Elementary and Silver Creek Middle School entrances.

The proposed construction of this project will require approximately 1.78 acres of new permanent right-of-way and 0.83 acre of temporary right-of-way. The project is approximately 1.75 miles long.

The maintenance of traffic (MOT) plan proposes road closure of US 31 between SR 311 and CR 403. The remainder of the project will be constructed under traffic. An official state route detour utilizing SR 60, I-65, and Blue Lick Road or SR 160 will be used during construction. Local roads may be used by local traffic. Access to all properties will be maintained throughout construction. Project stakeholders including local school corporations and emergency services will be notified of closures prior to construction. The proposed start of construction is in the Spring of 2023.

The cost associated with this project is approximately \$9.5 million which includes preliminary engineering, right-of-way, and construction with both federal and state funds anticipated to be used. The Federal Highway Administration (FHWA) and INDOT have agreed this project falls within the guidelines of a Categorical Exclusion (CE) Level 2 environmental document. Preliminary design plans along with the CE are available for review at the following locations:

1. Indiana Department of Transportation- Seymour District, 185 Agrico Lane, Seymour, IN 4727, (855) INDOT4U (463-6848)
2. Sellersburg Library, 430 N Indiana Avenue, Sellersburg, IN 47172, (812) 246-4493
3. Online at: <https://www.in.gov/indot/4288.htm>

All interested persons may request a public hearing be held, express their concerns by submitting comments, or request project documents be mailed by contacting Elizabeth Biggio, Architectural Historian II, Butler, Fairman & Seufert, Inc., 8450 Westfield Blvd., Suite 300, Indianapolis, IN 46240-8302, 317- 713-4615, ebiggio@bfsengr.com, on or before June 4, 2021.

In accordance with the Americans with Disabilities Act (ADA), persons and/or groups requiring project information be made available in alternative formats are encouraged to contact INDOT for the arrangement and coordination of services. Please contact Terry Summers, Project Manager, 185 Agrico Lane, Seymour, IN 47274, (812) 524-3749, tsummers@indot.in.gov. In accordance with Title VI of the Civil Rights Act of 1964, persons and/or groups requiring project information be made available in another language are encouraged to contact INDOT.

This notice is published in compliance with: 1) Code of Federal Regulations, Title 23, Section 771 (CFR 771.111(h)(1) stating, "Each State must have procedures approved by the FHWA to carry out a public involvement/public hearing program."; 2) 23 CFR 450.210(a)(1)(ix) stating, "Provide for the periodic review of the effectiveness of the public involvement process to ensure that the process provides full and open access to all interested parties and revise the process, as appropriate."; and 3) The INDOT Public Involvement Policies and Procedures approved by the Federal Highway Administration on August 16, 2012. hspaxlp

Appendix H

Air Quality

Indiana Department of Transportation (INDOT)
 State Preservation and Local Initiated Projects FY 2020 - 2024

SPONSOR	CONTRACT # / LEAD DES	STIP NAME	ROUTE	WORK TYPE	LOCATION	DISTRICT	MILES	FEDERAL CATEGORY	Estimated Cost left to Complete Project*	PROGRAM	PHASE	FEDERAL	MATCH	2020	2021	2022	2023	2024
Clarksville	40289 / 1700725	Init.	VA VARI	Access Control	Riverside Drive from the Town limits to Ashland Park	Seymour	0	STPBG		Local Funds	RW	\$0.00	\$410,880.00		\$410,880.00			
										Louisville MPO	RW	\$1,643,520.00	\$0.00		\$1,643,520.00			
Clarksville	40289 / 1700725	M 07	VA VARI	Access Control	Riverside Drive from the Town limits to Ashland Park	Seymour	0	STBG	\$6,780,302.00	Local Funds	RW	\$0.00	\$166,712.00		\$166,712.00			
										Louisville MPO	RW	\$666,846.00	\$0.00		\$666,846.00			
Comments:Adding RW funds to FY 2021 in the amount of \$833,558 per the TIP dated 2/27/2020. AQC Exempt 3/3/2020																		
Indiana Department of Transportation	40387 / 1592995	Init.	SR 62	HMA Overlay, Preventive Maintenance	From SR 265 to 0.15 miles N of SR 3	Seymour	6.441	STPBG		Road Construction	CN	\$3,511,996.00	\$877,999.00	\$4,389,995.00				
Indiana Department of Transportation	40412 / 1700111	Init.	US 31	Pavement Replacement, Small Town	1.53 miles N of SR 60 (Foothill Rd) to 3.28 miles N of SR 60	Seymour	1.84	STPBG		Road Construction	CN	\$3,989,621.60	\$997,405.40			\$4,987,027.00		
										Road ROW	RW	\$240,000.00	\$60,000.00		\$300,000.00			
Indiana Department of Transportation	40799 / 1600679	Init.	SR 60	Small Structure Pipe Lining	0.87 mile E of SR 111	Seymour	0	STPBG		Bridge Construction	CN	\$434,276.80	\$108,569.20		\$542,846.00			
										Bridge ROW	RW	\$24,000.00	\$6,000.00	\$30,000.00				
Clark County	40812 / 1702787	Init.	IR 1024	Road Rehabilitation (3 R/4R Standards)	Bethany Road Phase 2- 1.33 miles Northwest of SR62 to Old SR403 in Clark County	Seymour	1.46	STPBG		Group IV Program	CN	\$5,680,000.00	\$0.00	\$5,680,000.00				
										Local Funds	CN	\$0.00	\$1,110,115.18	\$1,110,115.18				
										Route Transfer/rel inquisition	CN	\$309,884.82	\$0.00	\$309,884.82				
Clark County	40812 / 1702787	M 02	IR 1024	Road Rehabilitation (3 R/4R Standards)	Bethany Road Phase 2- 1.33 miles Northwest of SR62 to Old SR403 in Clark County	Seymour	1.46	STBG	\$7,100,000.00	Group IV Program	CN	-\$1,680,000.00	\$0.00	(\$1,680,000.00)				
										Local Funds	CN	\$0.00	-\$420,000.00	(\$420,000.00)				
Comments:CN Phase for -(\$2,100,000) FY 2020. No MPO																		
Clark County	40812 / 1702787	M 03	IR 1024	Road Rehabilitation (3 R/4R Standards)	Bethany Road Phase 2- 1.33 miles Northwest of SR62 to Old SR403 in Clark County	Seymour	1.46	STBG	\$7,100,000.00	Local Funds	CN	\$0.00	\$154,942.41	\$154,942.41				
										Group IV Program	CN	\$1,680,000.00	\$0.00	\$1,680,000.00				
Comments:Adding CN Phase \$1,834,942.41 FY 2020. Per Kipda TIP dated 9/5/2019																		
Clark County	40812 / 1702787	M 04	IR 1024	Road Rehabilitation (3 R/4R Standards)	Bethany Road Phase 2- 1.33 miles Northwest of SR62 to Old SR403 in Clark County	Seymour	1.46	STBG	\$7,100,000.00	Local Funds	CN	\$0.00	\$419,681.67	\$419,681.67				
Comments:Adding CE local funds for FY 2020 in the amount of \$419,681.67. No MPO																		

*Estimated Costs left to Complete Project column is for costs that may extend beyond the four years of a STIP. This column is not fiscally constrained and is for information purposes.

Indiana Department of Transportation (INDOT)
 State Preservation and Local Initiated Projects FY 2018 - 2021

SPONSOR	CONTRACT # / LEAD DES	STIP NAME	ROUTE	WORK TYPE	LOCATION	DISTRICT	MILES	FEDERAL CATEGORY	Estimated Cost left to Complete Project*	PROGRAM	PHASE	FEDERAL	MATCH	2018	2019	2020	2021
Comments:Increase in CN phase for FY 2019 per KIPDA Administrative Modification 6 dated 11/21/17.																	
Indiana Department of Transportation	40346 / 1701094	A 08	I 65	Bridge Deck Replacement	I-265 EB Ramp over I-65	Seymour	0	NHPP	\$1,384,278.00	Bridge Construction	CN	\$1,088,350.20	\$120,927.80			\$1,209,278.00	
										Bridge Consulting	PE	\$157,500.00	\$17,500.00	\$175,000.00			
Comments:Amend PE phase in FY 2018 and CN in FY 2020 to current STIP per KIPDA Administrative Modification 4 dated 9/28/17.																	
Indiana Department of Transportation	40387 / 1592995	A 37	SR 62	HMA Overlay, Preventive Maintenance	From SR 265 to 0.15 miles N of SR 3	Seymour	6.441	STPBG	\$4,389,995.00	Road Construction	CN	\$3,511,996.00	\$877,999.00			\$4,389,995.00	
Comments:Amend CN phase in FY 2020 to current STIP. Amended to KIPDA's TIP per Administrative Modification 26 dated 1/24/19.																	
Indiana Department of Transportation	40387 / 1592995	A 08	SR 62	HMA Overlay, Preventive Maintenance	From SR 265 to 0.15 miles N of SR 3	Seymour	6.441	STP	\$4,261,215.00	Road Consulting	PE	\$156,000.00	\$39,000.00	\$195,000.00			
Comments:Amend PE phase to the current STIP in FY 2018 per KIPDA Administrative Modification 4 dated 9/28/17.																	
Indiana Department of Transportation	40412 / 1700111	A 08	US 31	Pavement Replacement, Small Town	1.53 miles N of SR 60 (Foothill Rd) to 3.28 miles N of SR 60	Seymour	1.74	STP	\$5,525,836.00	Road Consulting	PE	\$384,000.00	\$96,000.00	\$480,000.00			
										Road ROW	RW	\$240,000.00	\$60,000.00				\$300,000.00
Comments:Amend PE phase in FY 2018 and RW in 2021 to current STIP per KIPDA Administrative Modification 4 dated 9/28/17.																	
Indiana Department of Transportation	40413 / 1600744	M 14	I 65	Replace Superstructure	2.68 miles S of SR 160 over Blue Lick Creek NBL	Seymour	0	NHPP	\$4,131,768.00	Bridge Consulting	PE	\$180,000.00	\$20,000.00	(\$300,000.00)	\$500,000.00		
Comments:Move PE phase from FY 2018 to FY 2019. Modified in KIPDA's TIP per Administrative Modification 9 dated 3/22/18 on page 2.																	
Indiana Department of Transportation	40413 / 1600744	A 08	I 65	Replace Superstructure	2.68 miles S of SR 160 over Blue Lick Creek NBL	Seymour	0	NHPP	\$1,965,884.00	Bridge Consulting	PE	\$135,000.00	\$15,000.00	\$150,000.00			
Comments:Amend PE phase in FY 2018 to current STIP per KIPDA Administrative Modification 4 dated 9/28/17.																	
Indiana Department of Transportation	40413 / 1600750	A 08	I 65	Replace Superstructure	2.68 miles S of SR 160 over Blue Lick Creek SB	Seymour	0	NHPP	\$1,965,884.00	Bridge Consulting	PE	\$135,000.00	\$15,000.00	\$150,000.00			
Comments:Amend PE phase in FY 2018 to current STIP per KIPDA Administrative Modification 4 dated 9/28/17.																	
Indiana Department of Transportation	40453 / 1701449	A 08	SR 60	Bridge Replacement, Concrete	04.21 miles W of SR 111 at Moneys Branch	Seymour	0	STP	\$873,050.00	Bridge Consulting	PE	\$120,000.00	\$30,000.00	\$150,000.00			
										Bridge ROW	RW	\$8,000.00	\$2,000.00				\$10,000.00
Comments:Amend PE phase in FY 2018 and RW in FY 2021 to current STIP per KIPDA Administrative Modification 4 dated 9/28/17.																	
Indiana Department of Transportation	40766 / 1702419	A 11	IR 1001	Railroad Protection	CR 160 at LIRC RR DOT # 535372W and Main St DOT# 535371P in Henryville	Seymour	0	Safety	\$600,000.00	Local Safety Program - 130	CN	\$600,000.00	\$0.00	\$600,000.00			
										Local Safety Program - 130	PE	\$20,000.00	\$0.00	\$20,000.00			
Comments:PE Phase for \$20,000.00 in FY 2018 amended into the 18-11 STIP. CN phase in FY 2018 for \$600,000 amended into the 18-11 STIP. KIPDA Administrative Modification 6 to the 2018-2021 TIP. Dated November 21, 2017.																	
Indiana Department of Transportation	40976 / 1800811	A 21	I 65	Bridge Deck Overlay	01.12 mile N of SR 311 under St Joe Road	Seymour	0	NHPP	\$740,076.00	Bridge Construction	CN	\$558,068.40	\$62,007.60				\$620,076.00

*Estimated Costs left to Complete Project column is for costs that may extend beyond the four years of a STIP. This column is not fiscally constrained and is for information purposes.

PROJECT LISTINGS

Project Name	Secondary Identifier	Description	Project Purpose	Primary Contact Agency	
TARC Cross River Connectors		Implementation of 2 routes to improve cross river mobility over the Kennedy/Lincoln bridges and the Lewis and Clark Bridge to provide access to jobs between Louisville Metro and River Ridge Commerce Center in Southern Indiana. Funding for service begins in FY 2019.	To provide transit service to major destination points from western Louisville to River Ridge Commerce Center and from eastern Jefferson County to River Ridge Commerce Center.	TARC	
Traffic Signals on US 31		Traffic signal modernization on US 31 at IN 60/ Bean Road near Sellersburg.	Traffic Signal Modernization on US 31 at IN 60/Bean Road near Sellersburg which will be coordinated with a highway rail safety project to upgrade the railroad crossing on Bean Road just east of the intersection of US 31 and IN 60/ Bean Road.	INDOT	
US 31		Bridge rehabilitation project of bridge deck overlay on US 31, 0.68 mile north of IN 403 over Muddy Fork and Country Road.	Bridge deck overlay	INDOT	
US 31		Construct a bridge deck overlay on US 31, 1.94 miles south of IN 160 over Caney Fork.	Bridge deck overlay.	INDOT	
US 31		Pavement replacement on US 31, 1.53 miles north of IN 60 (Foothill Road) to 3.28 miles north of IN 60.	Pavement replacement.	INDOT	
US 31 at Wolf Run		Bridge replacement on US 31, 0.20 mile north of IN 160 at Wolf Run.	Bridge replacement, concrete.	INDOT	
US 31 Intersection Improvement		There is a pattern of rear-end crashes with a railroad running parallel to US 31. When a train is crossing Bud Prather Rd (east approach), there is not a large amount of room to store vehicles and a southbound vehicle may not have a safe storage place. Project length is 0.08 miles.	The intent of this project is to improve the safety of the intersection and reduce the frequency and severity of crashes that occur by constructing left-turn lanes on US 31.	INDOT	
US 31 Replace Superstructure		Bridge superstructure replacement on US 31, 0.89 miles south of IN 160, US 31 @ Blue Lick Creek.	Bridge rehabilitation on US 31, superstructure replacement.	INDOT	
Various Interstates Raised Pavement Markings*		Raised Pavement markings in various locations in Seymour District. These locations could include but are not limited to I-65, I-265, I-64, US-31 (Clark and Floyd Counties) and also I-74, I-275, and if funding remains, IN 67.	Seymour District Safety Project which is refurbishing Raised pavement markings in various locations through the Seymour District.	INDOT	

KIPDA ID #	State ID #	Parent Project	Group ID	Phase	Year	Federal	Other	Total	Federal Funding Category	Estimated Completion
2408	1801625			Oper	2020	\$800,000	\$200,000	\$1,000,000	CMAQ-MPO	2020
				Oper	2021	\$800,000	\$200,000	\$1,000,000	CMAQ-MPO	
						\$1,600,000	\$400,000	\$2,000,000		
2716	1902011		2678	C	2020	\$103,500	\$11,500	\$115,000	Rail Safety	2020
						\$103,500	\$11,500	\$115,000		
2285	1593200		2676	C	2020	\$496,800	\$124,200	\$621,000	NHS	2020
						\$496,800	\$124,200	\$621,000		
2314	1593192		2676	C	2021	\$582,437	\$145,609	\$728,046	STBG-ST	2021
						\$582,437	\$145,609	\$728,046		
2487	1700111		2676	PE	2020	\$133,600	\$33,400	\$167,000	STBG-ST	2022
				ROW	2021	\$240,000	\$60,000	\$300,000	STBG-ST	
				PE	2022	\$8,000	\$2,000	\$10,000	STBG-ST	
				U	2022	\$160,000	\$40,000	\$200,000	STBG-ST	
				C	2022	\$3,821,622	\$955,405	\$4,777,027	STBG-ST	
						\$4,363,222	\$1,090,805	\$5,454,027		
2722	1900343		2676	ROW	2022	\$40,000	\$10,000	\$50,000	STBG-ST	2024
				PE	2024	\$88,000	\$22,000	\$110,000	STBG-ST	
				C	2024	\$1,316,470	\$329,117	\$1,645,587	STBG-ST	
						\$1,444,470	\$361,117	\$1,805,587		
2618	1800375			ROW	2021	\$40,000	\$10,000	\$50,000	HSIP-ST	2023
				C	2022	\$240,000	\$60,000	\$300,000	HSIP-ST	
				C	2023	\$689,375	\$172,344	\$861,719	HSIP-ST	
						\$969,375	\$242,344	\$1,211,719		
2719	1802996		2676	PE	2020	\$280,000	\$70,000	\$350,000	STBG-ST	2024
				ROW	2022	\$40,000	\$10,000	\$50,000	STBG-ST	
				PE	2024	\$80,000	\$20,000	\$100,000	STBG-ST	
				C	2024	\$950,570	\$237,642	\$1,188,212	STBG-ST	
						\$1,350,570	\$337,642	\$1,688,212		
2507	1700313		2680	C	2020	\$270,000	\$30,000	\$300,000	HSIP-ST	2020
						\$270,000	\$30,000	\$300,000		

Appendix I

Additional Studies

Section 6(f) Properties in Clark County

Project Number	SubProjectCode	County	Property
1800014	1800014	Clark	Henry Lansden Park
1800029	1800029	Clark	Northaven Park (Connie Sellmer)
1800029.1	1800029.1	Clark	Highland Park
1800041	1800041	Clark	Moser Park
1800053	1800053	Clark	Vissing Park
1800075	1800075	Clark	Henry Lansden Park
1800123	1800123	Clark	Deam Lake State Recreation Area
1800124	1800124	Clark	Lapping Park, Wooded View Golf Course
1800154	1800154	Clark	Deam Lake State Recreation Area
1800166	1800166	Clark	Deam Lake State Recreation Area
1800171	1800171AA	Clark	Deam Lake State Recreation Area
1800205	1800205	Clark	Lapping Park, Wooded View Golf Course
1800216	1800216	Clark	Vissing Park
1800248	1800248	Clark	Henry Lansden Park
1800305	1800305B	Clark	Deam Lake State Recreation Area
1800342	1800342	Clark	Lapping Park, Wooded View Golf Course
1800363	1800363E	Clark	Clark State Forest
1800363	1800363G	Clark	Deam Lake State Recreation Area
1800446	1800446	Clark	Clark State Forest
1800616	1800616	Clark	Borden Community Park

*Source: INDOT LWCF List: <https://www.in.gov/indot/2523.htm>

Environmental Justice Data Analysis

Des. No. 1700111: US 31 Small Town Pavement Replacement

Source: U.S. Census Bureau 2019 ACS 5-year Estimates

		COC1	AC1	AC2
		Clark County, Indiana	Census Tract 507.03, Clark County, Indiana	Census Tract 507.04, Clark County, Indiana
B17001	LOW-INCOME			
	Population for whom poverty status is determined: Total	114,572	5,569	6,681
	Income in the past 12 months below poverty level	11,544	362	446
	Percent Low-income	10.1%	6.5%	6.7%
	125 Percent of COC	12.6%	AC < 125% COC	AC < 125% COC
	Potential Population of EJ Concern?		No	No
B03002	MINORITY			
	Total population: Total	116,507	5,703	6,749
	Total population: Not Hispanic or Latino	110,243	5,385	6,442
	Total population: Not Hispanic or Latino; White alone	96,942	4,624	6,136
	Total population: Not Hispanic or Latino; Black or African American alone	7,794	292	62
	Total population: Not Hispanic or Latino; American Indian and Alaska Native alone	107	0	0
	Total population: Not Hispanic or Latino; Asian alone	1,213	0	184
	Total population: Not Hispanic or Latino; Native Hawaiian and Other Pacific Islander alone	13	0	0
	Total population: Not Hispanic or Latino; Some other race alone	121	0	1
	Total population: Not Hispanic or Latino; Two or more races	4,053	469	59
	Total population: Hispanic or Latino	6,664	318	307
	Total population: Hispanic or Latino; White alone	5,060	318	36
	Total population: Hispanic or Latino; Black or African American alone	60	0	2
	Total population: Hispanic or Latino; American Indian and Alaska Native alone	108	0	0
	Total population: Hispanic or Latino; Asian alone	50	0	15
	Total population: Hispanic or Latino; Native Hawaiian and Other Pacific Islander alone	0	0	0
	Total population: Hispanic or Latino; Some other race alone	943	0	254
	Total population: Hispanic or Latino; Two or more races	43	0	0
	Number Non-white/minority	19,565	1,079	613
	Percent Non-white/Minority	16.8%	18.9%	9.1%
	125 Percent of COC	21.0%	AC <125% COC	AC < 125% COC
	Potential Population of EJ Concern?		No	No

HISPANIC OR LATINO ORIGIN BY RACE

Survey/Program: American Community Survey
TableID: B03002

Product: 2019 ACS 5-Year Estimates Detailed Tables
Universe: Total population

CUSTOMIZE TABLE

Label	Clark County, Indiana		Census Tract 507.03, Clark County, Indiana		Census Tract 507.04, Clark County, Indiana	
	Estimate	Margin of Error	Estimate	Margin of Error	Estimate	Margin of Error
▼ Total:	116,507	*****	5,703	±360	6,749	±370
▼ Not Hispanic or Latino:	110,243	*****	5,385	±396	6,442	±489
White alone	96,942	±126	4,624	±502	6,136	±546
Black or African American alone	7,794	±534	292	±182	62	±85
American Indian and Alaska Native alone	107	±77	0	±17	0	±17
Asian alone	1,213	±184	0	±17	184	±186
Native Hawaiian and Other Pacific Islander alone	13	±22	0	±17	0	±17
Some other race alone	121	±125	0	±17	1	±2
▶ Two or more races:	4,053	±511	469	±350	59	±72
▼ Hispanic or Latino:	6,264	*****	318	±252	307	±339
White alone	3,060	±498	318	±252	36	±88
Black or African American alone	80	±73	0	±17	2	±4
American Indian and Alaska Native alone	108	±132	0	±17	0	±17
Asian alone	50	±55	0	±17	15	±28
Native Hawaiian and Other Pacific Islander alone	0	±28	0	±17	0	±17
Some other race alone	943	±484	0	±17	254	±337
▶ Two or more races:	43	±61	0	±17	0	±17

POVERTY STATUS IN THE PAST 12 MONTHS BY SEX BY AGE

Survey/Program: American Community Survey
TableID: B17001

Product: 2019 ACS 5-Year Estimates Detailed Tables
Universe: Population for whom poverty status is determined

CUSTOMIZE TABLE

Label	Clark County, Indiana		Census Tract 507.03, Clark County, Indiana		Census Tract 507.04, Clark County, Indiana	
	Estimate	Margin of Error	Estimate	Margin of Error	Estimate	Margin of Error
▼ Total:	114,572	±382	5,569	±367	6,681	±380
▼ Income in the past 12 months below poverty level:	11,544	±1,249	362	±282	466	±424
▶ Male:	5,146	±668	82	±44	338	±294
▶ Female:	6,398	±769	310	±254	128	±136
▼ Income in the past 12 months at or above poverty level:	103,028	±1,280	5,207	±447	6,215	±558
▶ Male:	50,472	±736	2,566	±390	3,188	±357
▶ Female:	52,556	±793	2,641	±448	3,027	±390