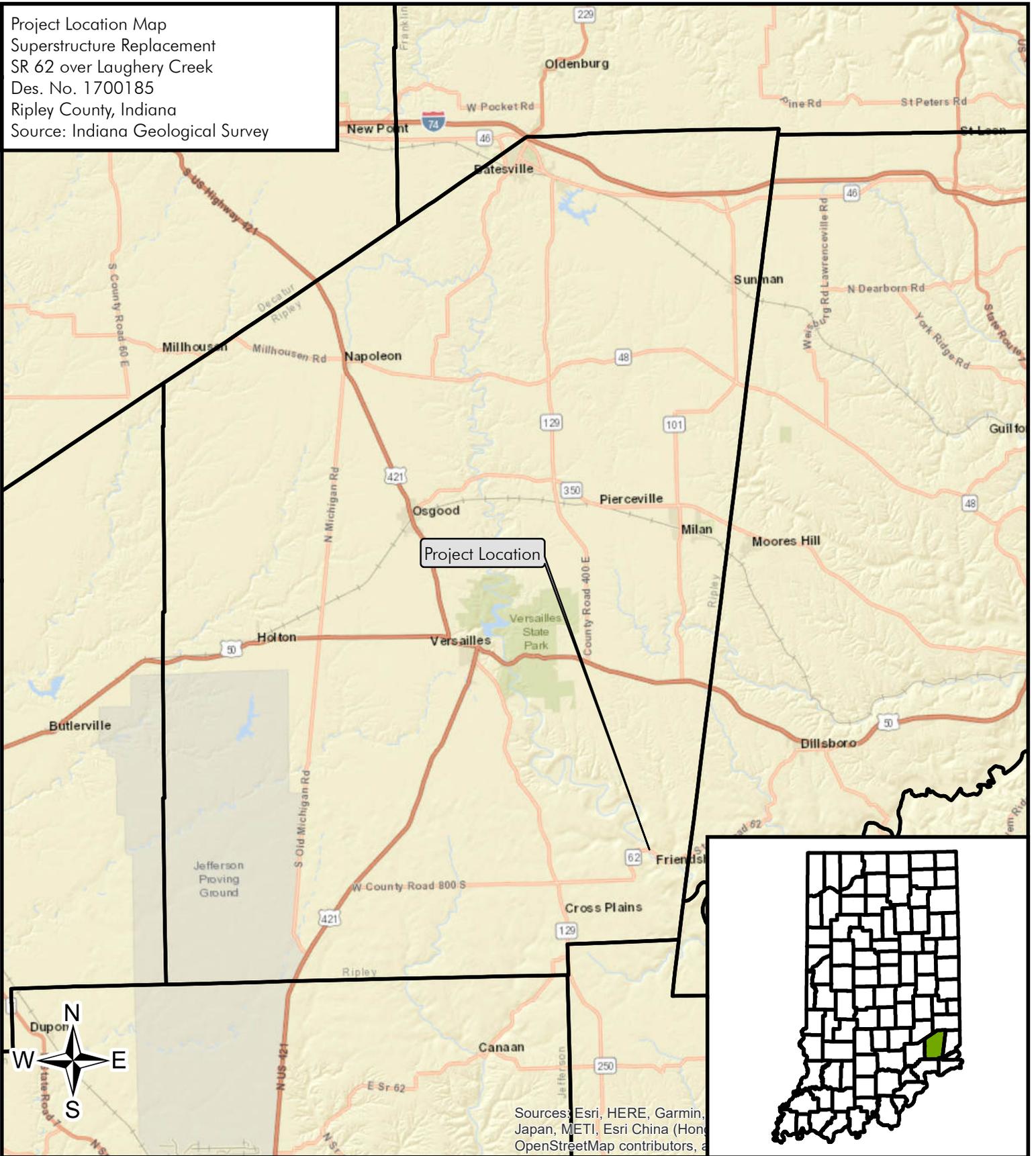


Des 1700185 CE-2
Appendix B
Graphics and Plan Sheets

Project Location Map
 Superstructure Replacement
 SR 62 over Laughery Creek
 Des. No. 1700185
 Ripley County, Indiana
 Source: Indiana Geological Survey



Sources: Esri, HERE, Garmin, Japan, METI, Esri China (Hong Kong), Swatch, Bing, OpenStreetMap contributors, and the GIS User Community



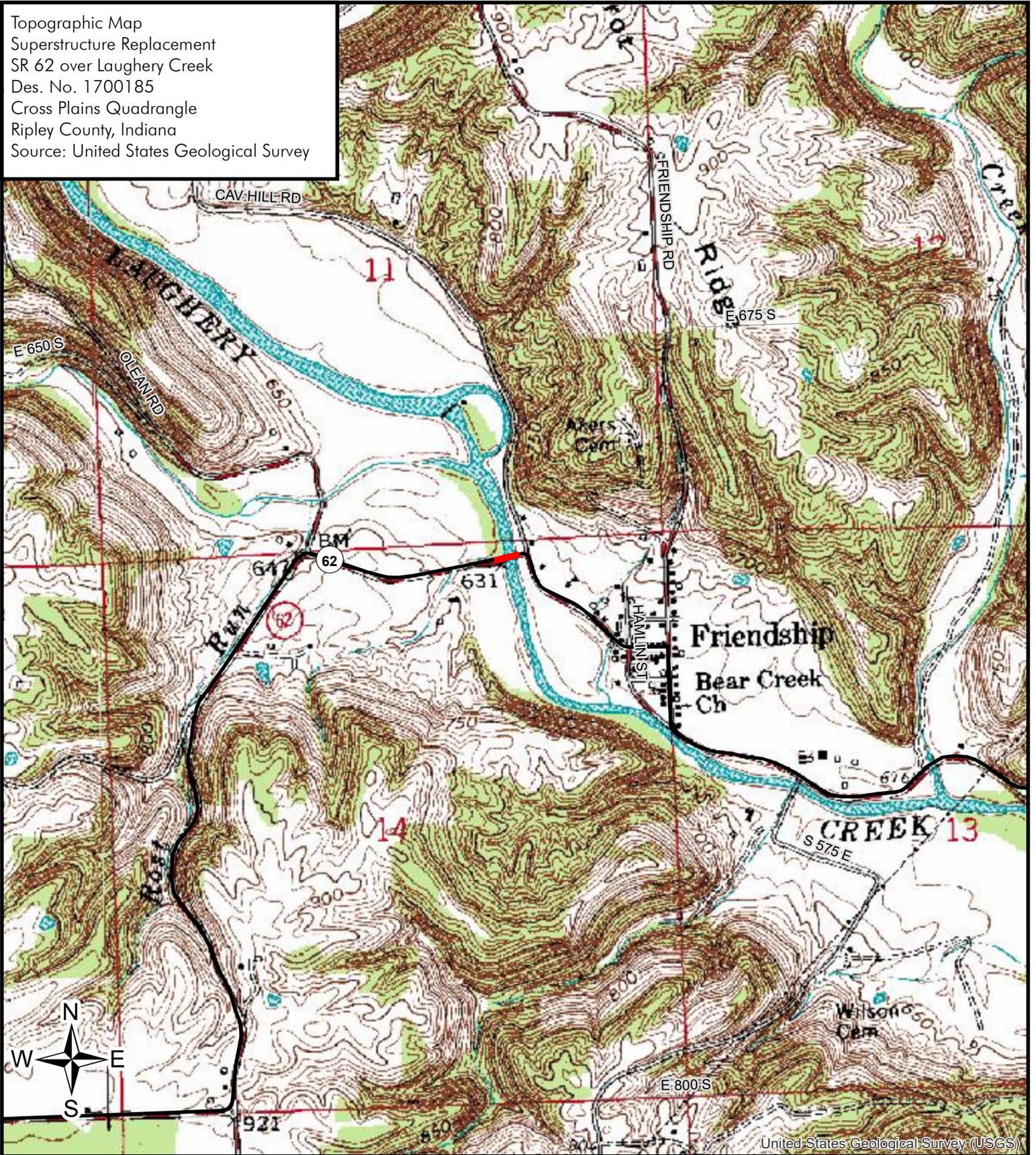
-  County Boundary
-  Project County



B-1

3/29/2019

Topographic Map
 Superstructure Replacement
 SR 62 over Laughery Creek
 Des. No. 1700185
 Cross Plains Quadrangle
 Ripley County, Indiana
 Source: United States Geological Survey



United States Geological Survey (USGS)



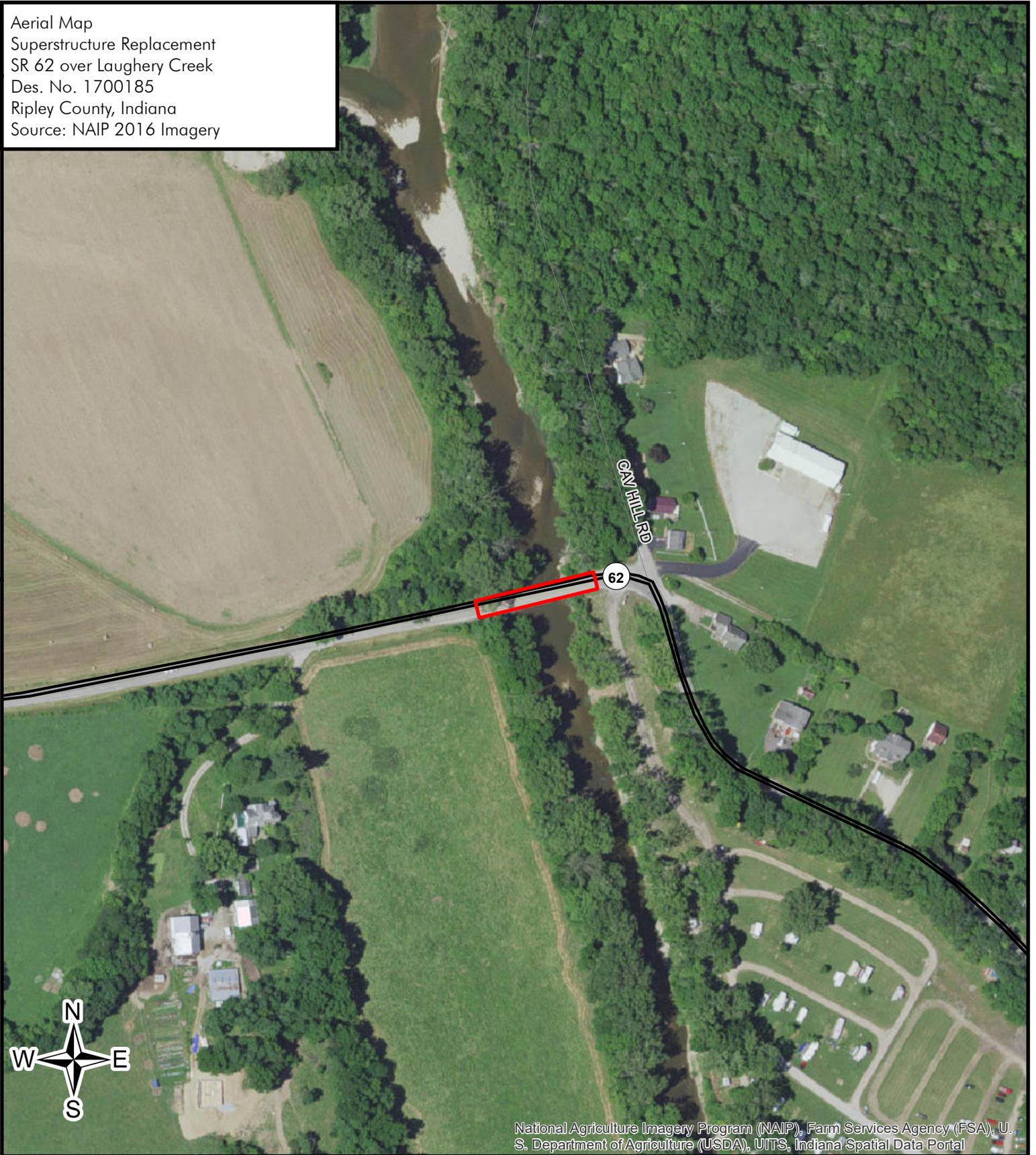
 Project Location



B-2

3/29/2019

Aerial Map
Superstructure Replacement
SR 62 over Laughery Creek
Des. No. 1700185
Ripley County, Indiana
Source: NAIP 2016 Imagery



National Agriculture Imagery Program (NAIP), Farm Services Agency (FSA), U. S. Department of Agriculture (USDA), UITS, Indiana Spatial Data Portal

0 0.025 0.05
Miles

 Project Location

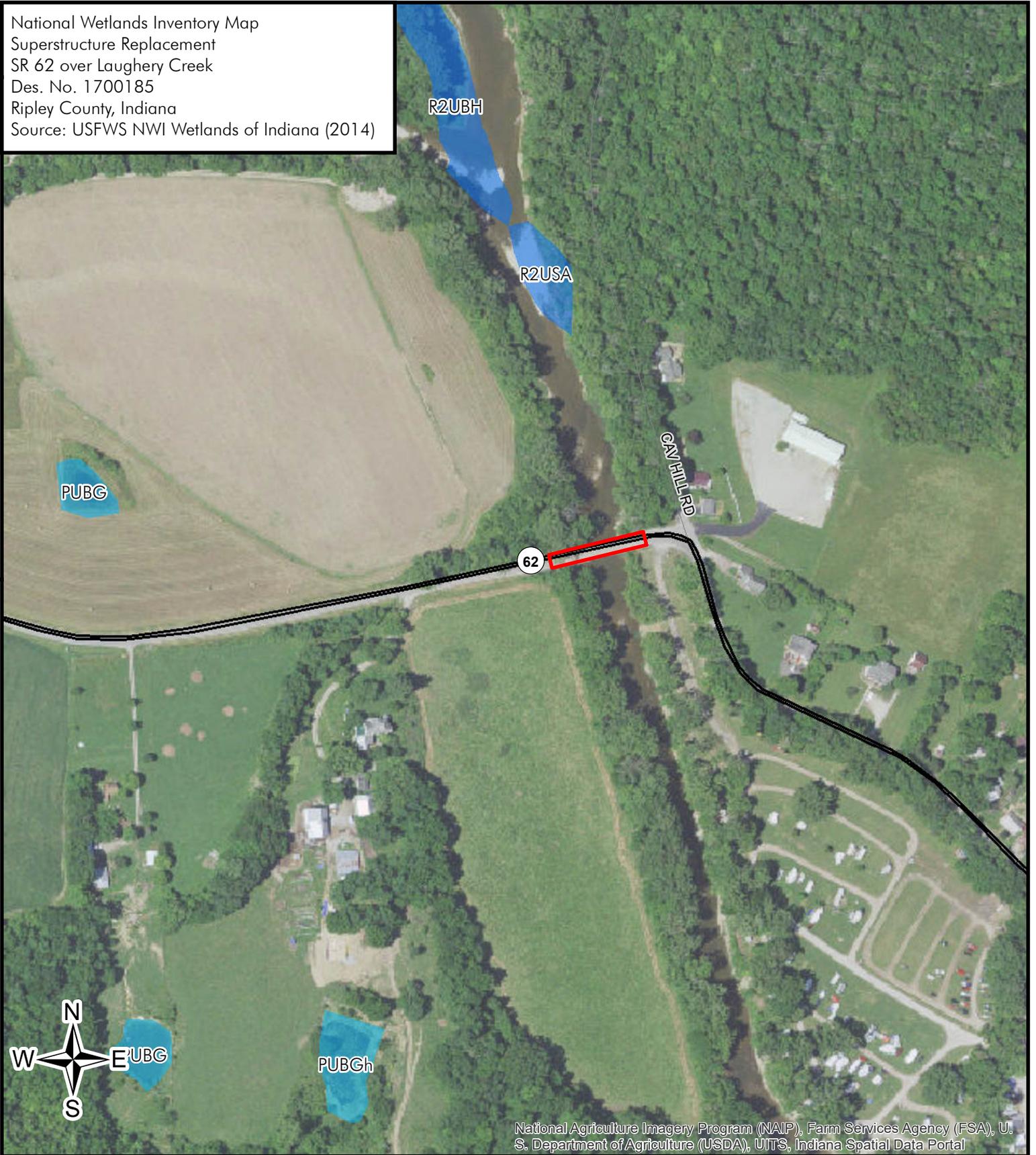
green • 3



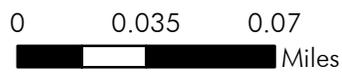
B-3

3/29/2019

National Wetlands Inventory Map
 Superstructure Replacement
 SR 62 over Laughery Creek
 Des. No. 1700185
 Ripley County, Indiana
 Source: USFWS NWI Wetlands of Indiana (2014)



National Agriculture Imagery Program (NAIP), Farm Services Agency (FSA), U. S. Department of Agriculture (USDA), UITS, Indiana Spatial Data Portal



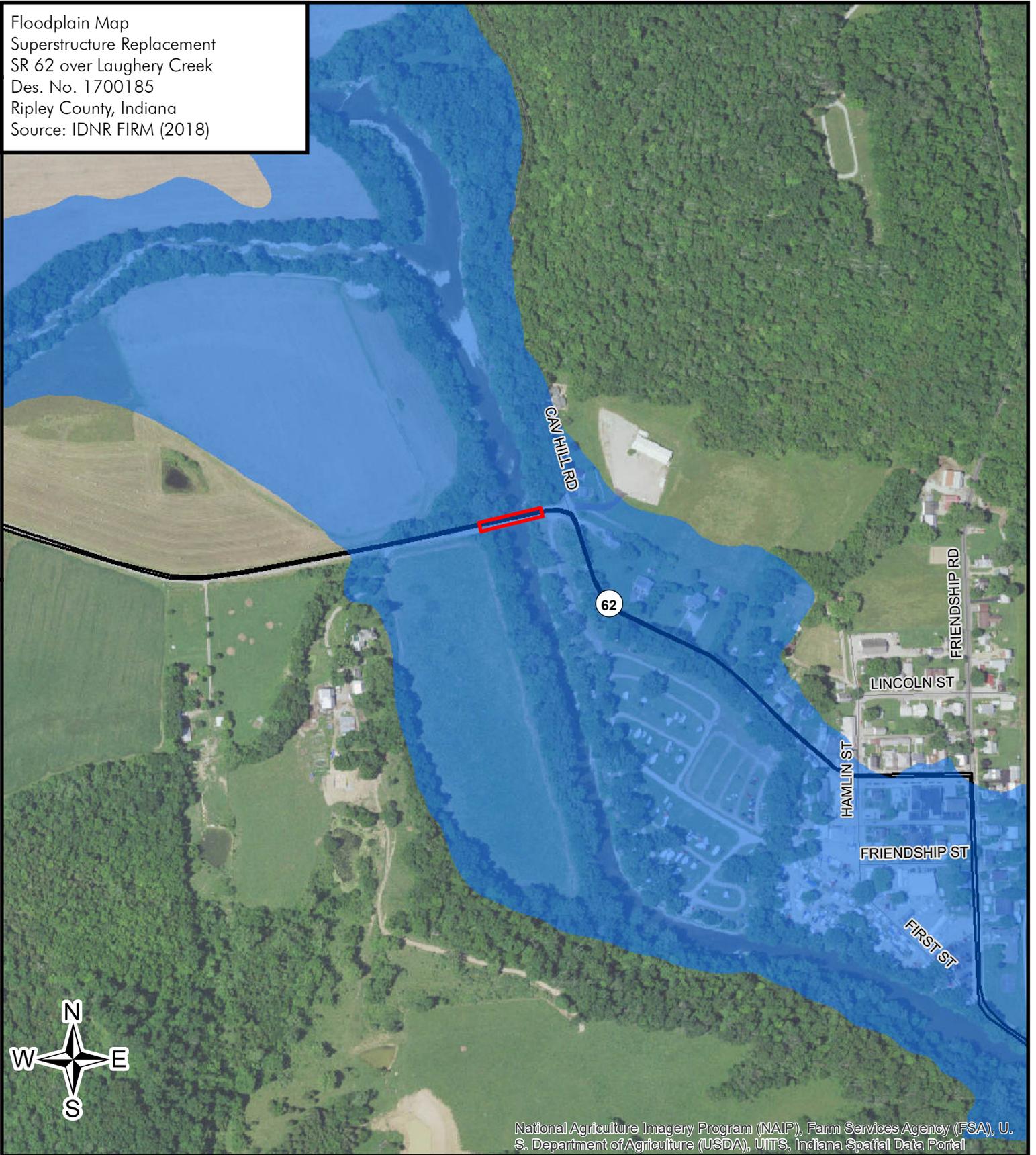
- | | | | |
|---|-----------------------------------|--|----------|
|  | Project Location |  | Lake |
|  | Freshwater Emergent Wetland |  | Riverine |
|  | Freshwater Forested/Shrub Wetland |  | Other |
|  | Freshwater Pond | | |



3/29/2019

B-4

Floodplain Map
 Superstructure Replacement
 SR 62 over Laughery Creek
 Des. No. 1700185
 Ripley County, Indiana
 Source: IDNR FIRM (2018)



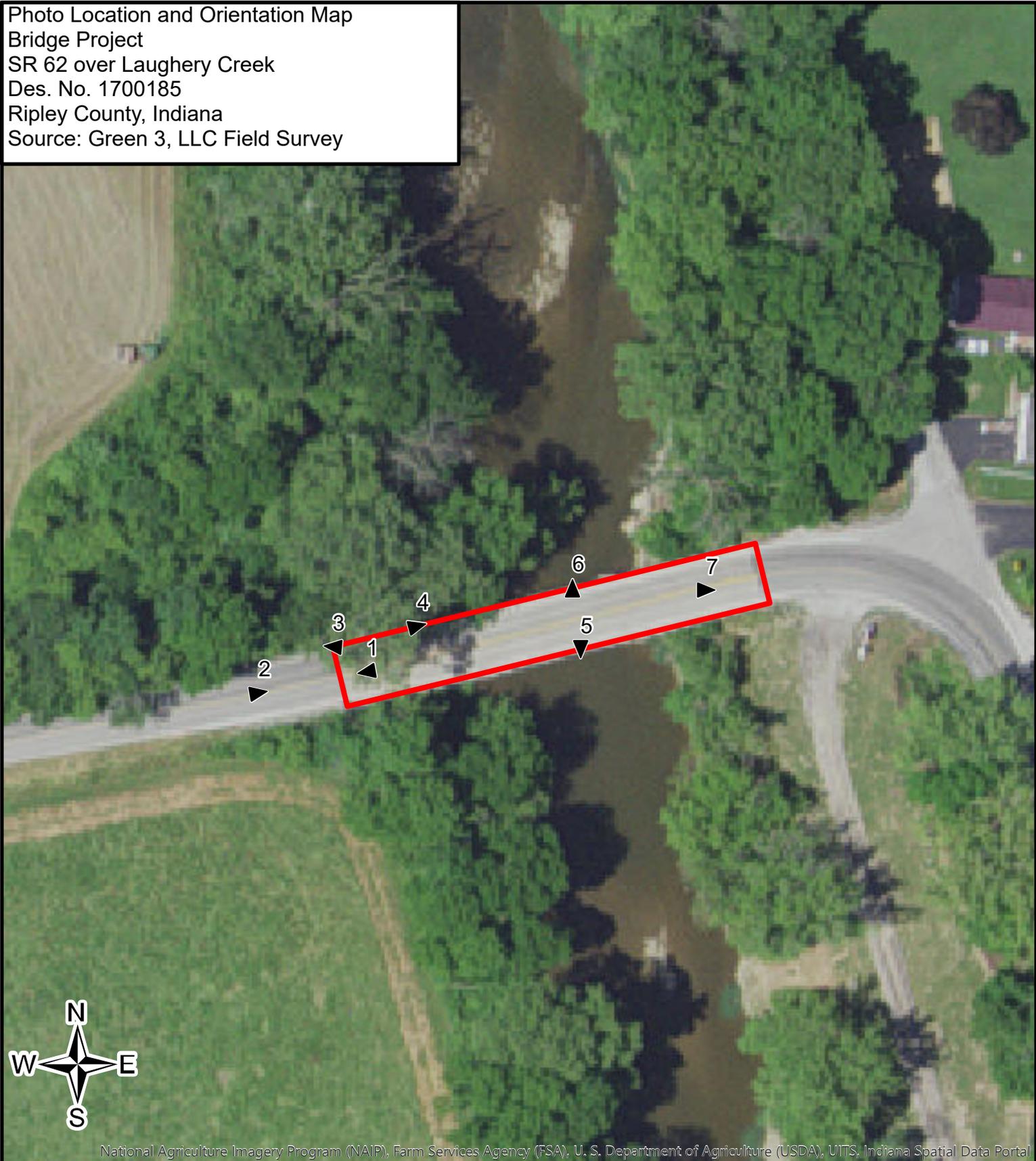
National Agriculture Imagery Program (NAIP), Farm Services Agency (FSA), U.S. Department of Agriculture (USDA), UITS, Indiana Spatial Data Portal

-  Project Location
-  0.2% Annual Chance Flood Hazard
-  0.2% Annual Chance, Protected by Levee
-  1% Annual Chance Flood Hazard

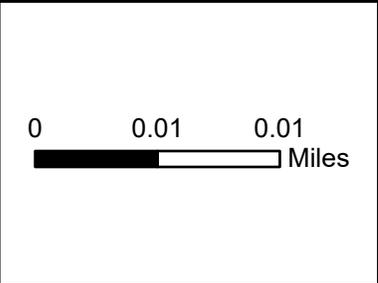
green • 3



Photo Location and Orientation Map
Bridge Project
SR 62 over Laughery Creek
Des. No. 1700185
Ripley County, Indiana
Source: Green 3, LLC Field Survey



National Agriculture Imagery Program (NAIP), Farm Services Agency (FSA), U. S. Department of Agriculture (USDA), UTTS, Indiana Spatial Data Portal



▲ Photo Location
▭ Project Location

B-6

3/12/2020

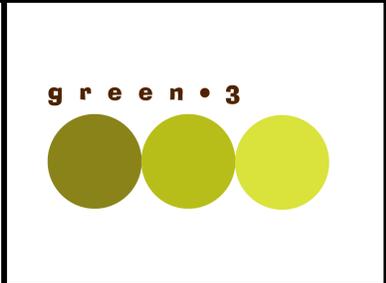




Photo 1: West Bridge Approach Facing West



Photo 2: West Bridge Approach Facing East



Photo 3: Northwest Quadrant Right of Way Facing Northwest



Photo 4: Northwest Quadrant Right of Way Facing Northeast



Photo 5: Facing Southeast From Bridge Towards Laughery Creek



Photo 6: Facing Northwest From Bridge Towards Laughery Creek



Photo 7: Southeast Quadrant of Right of Way Facing Southeast

PROJECT	DESIGNATION NO.
1700185	1700185
CONTRACT	BRIDGE FILE
B-40434	062-69-05860 B

INDIANA DEPARTMENT OF TRANSPORTATION



BRIDGE PRESERVATION PLANS FOR SPANS OVER 20 FEET

ROUTE: SR 62
PROJECT NO.

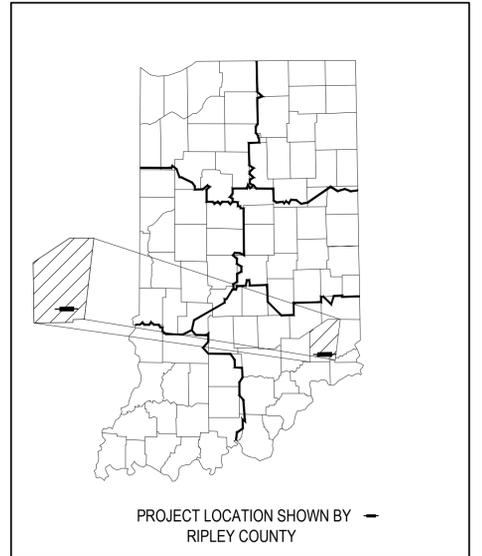
AT: 219+21
1700185 P.E.
1700185 R/W
1700185 CONST.

STRUCTURE INFORMATION			
STRUCTURE	TYPE	SPAN AND SKEW	OVER
062-69-05860 B	Continuous Steel Beam Bridge	3 Spans: 66'-9", 67'-6", 66'-9", Square	Laughery Creek

TRAFFIC DATA			
A.A.D.T. (2022)		404	V.P.D.
A.A.D.T. (2042)		509	V.P.D.
D.H.V. (2042)		47	V.P.H.
DIRECTIONAL DISTRIBUTION		45.5 %	
TRUCKS		21 % D.H.V.	
		23 % A.A.D.T.	

DESIGN DATA	
DESIGN SPEED	45 MPH
POSTED SPEED	45 MPH
PROJECT DESIGN CRITERIA	3R (Non-Freeway)
FUNCTIONAL CLASSIFICATION	Major Collector
RURAL/URBAN	Rural
TERRAIN	Level
ACCESS CONTROL	None

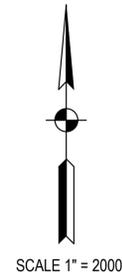
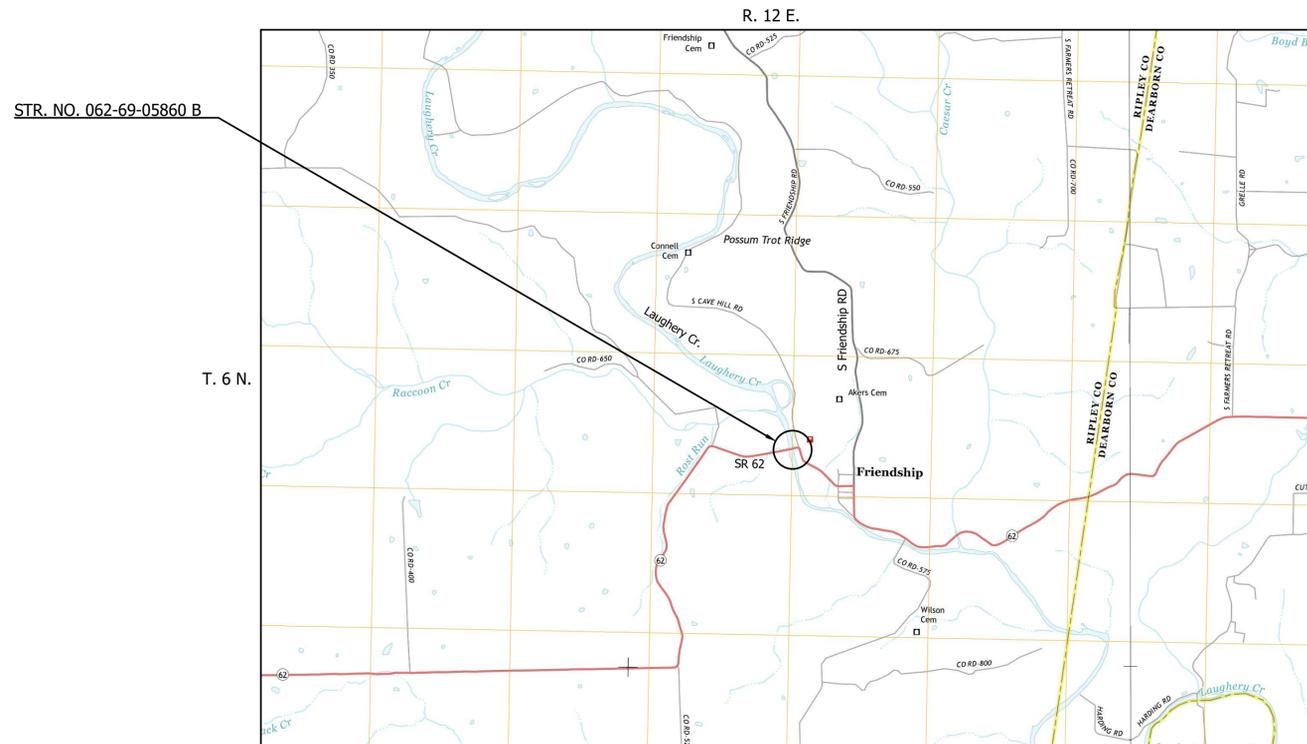
SUPERSTRUCTURE REPLACEMENT ON S.R. 62 OVER LAUGHERY CREEK. THE PROJECT IS LOCATED 3.91 MILES EAST OF SR 129, IN SECTION 14, T-6-N, R-12-E, RIPLEY COUNTY, INDIANA.



LATITUDE: 38°58'20.53" N. LONGITUDE: 85°09'11.19" W.

BRIDGE LENGTH = 0.038 mi.
ROAD LENGTH = 0.001 mi.
TOTAL LENGTH = 0.039 mi.
MAX. GRADE = +1.19%

HUC: 05090203070030



PRELIMINARY PLANS 9-19-19

LOCATION MAP

INDIANA DEPARTMENT OF TRANSPORTATION
STANDARD SPECIFICATIONS DATED 2020
TO BE USED WITH THESE PLANS



PLANS PREPARED BY:	USI Consultants, Inc.	317-544-4996
		PHONE NUMBER
CERTIFIED BY:		DATE
APPROVED FOR LETTING:		DATE
	INDIANA DEPARTMENT OF TRANSPORTATION	

BRIDGE FILE	
062-69-05860B	
DESIGNATION NO.	
1700185	
SHEETS	
1	of 12
PROJECT NO.	
1700185	
CONTRACT	
B-40434	

S:\2019\0318-109 BR SR 62 Dec 17 2019\Preserve Bridge Plans\0318-109 This Sheet.dwg, This 9/17/2019 10:32:57 AM

PHASE 1 CONSTRUCTION SEQUENCE

1. Install temporary signals as shown on plans, and close existing drive as indicated on plans.
2. Activate temporary signals. Remove existing Stop Sign.
3. Place temporary traffic barrier as shown on plan, using eastbound lane for eastbound and westbound traffic. Commence Phase 1 construction.

SIGNALIZATION NOTES:

All temporary signal equipment to remain the property of the contractor.

Temporary wood poles or Portable trailers to be placed in locations to avoid conflict with construction of bridge.

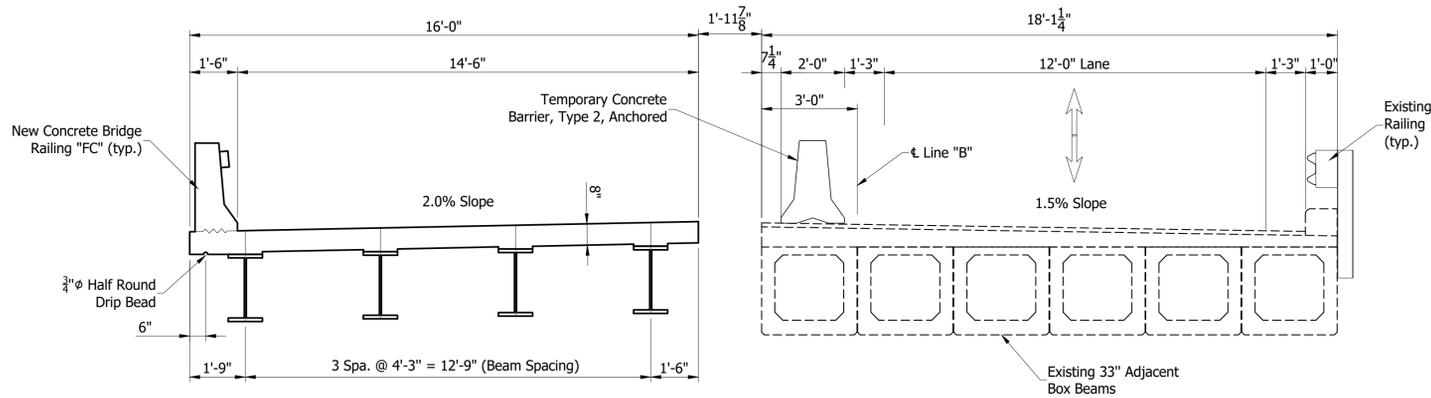
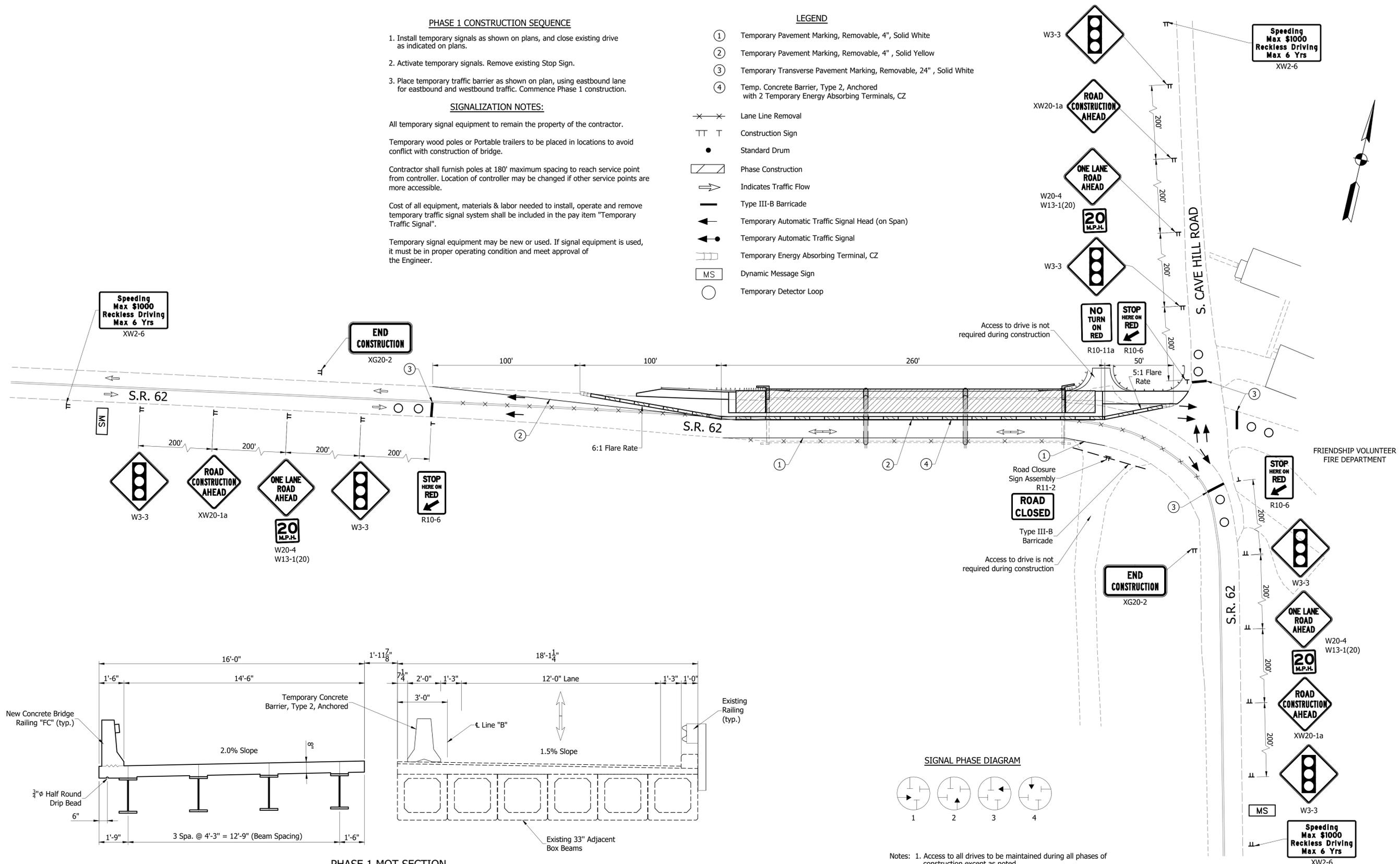
Contractor shall furnish poles at 180' maximum spacing to reach service point from controller. Location of controller may be changed if other service points are more accessible.

Cost of all equipment, materials & labor needed to install, operate and remove temporary traffic signal system shall be included in the pay item "Temporary Traffic Signal".

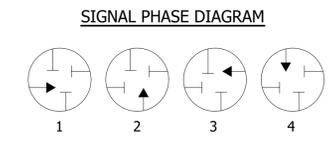
Temporary signal equipment may be new or used. If signal equipment is used, it must be in proper operating condition and meet approval of the Engineer.

LEGEND

- ① Temporary Pavement Marking, Removable, 4", Solid White
- ② Temporary Pavement Marking, Removable, 4", Solid Yellow
- ③ Temporary Transverse Pavement Marking, Removable, 24", Solid White
- ④ Temp. Concrete Barrier, Type 2, Anchored with 2 Temporary Energy Absorbing Terminals, CZ
- x—x— Lane Line Removal
- TT T Construction Sign
- Standard Drum
- ▨ Phase Construction
- ⇒ Indicates Traffic Flow
- Type III-B Barricade
- ← Temporary Automatic Traffic Signal Head (on Span)
- ←● Temporary Automatic Traffic Signal
- ▭ Temporary Energy Absorbing Terminal, CZ
- MS Dynamic Message Sign
- Temporary Detector Loop



PHASE 1 MOT SECTION
Scale: 3/8" = 1'-0"



- Notes:
1. Access to all drives to be maintained during all phases of construction except as noted.
 2. Existing conflicting pavement markings to be removed within project limits.

CONSTRUCTION ZONE DESIGN SPEED: 20 MPH

NOT FOR CONSTRUCTION

DESIGNED: MKT	DRAWN: DWB
CHECKED: MJH	CHECKED: MJH

INDIANA DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC PHASE 1

HORIZONTAL SCALE 1"=30'	BRIDGE FILE 062-69-05860 B
VERTICAL SCALE NA	DESIGNATION 1700185
SURVEY BOOK ---	SHEETS 3 of 12
CONTRACT B-40434	PROJECT 1700185

S:\2018\10\18-109 BR 06 Dec 17\0105\Final\Bridge Plans\0105-109 MOT Phase 1.dwg, 10/17/2018 10:26:46 AM

PHASE 2 CONSTRUCTION SEQUENCE

1. Move temporary traffic barrier as shown on plans utilizing flaggers as necessary.
2. Commence Phase 2 Construction
3. Preform HMA Milling and placing HMA Overlay as shown on plans utilizing flaggers as necessary.

SIGNALIZATION NOTES:

All temporary signal equipment to remain the property of the contractor.

Temporary wood poles or Portable trailers to be placed in locations to avoid conflict with construction of bridge.

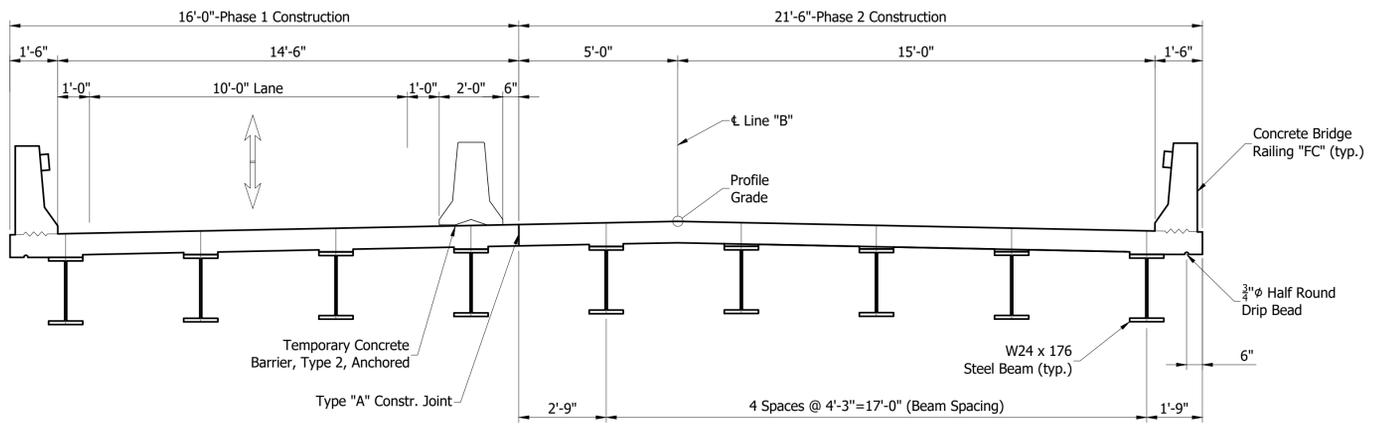
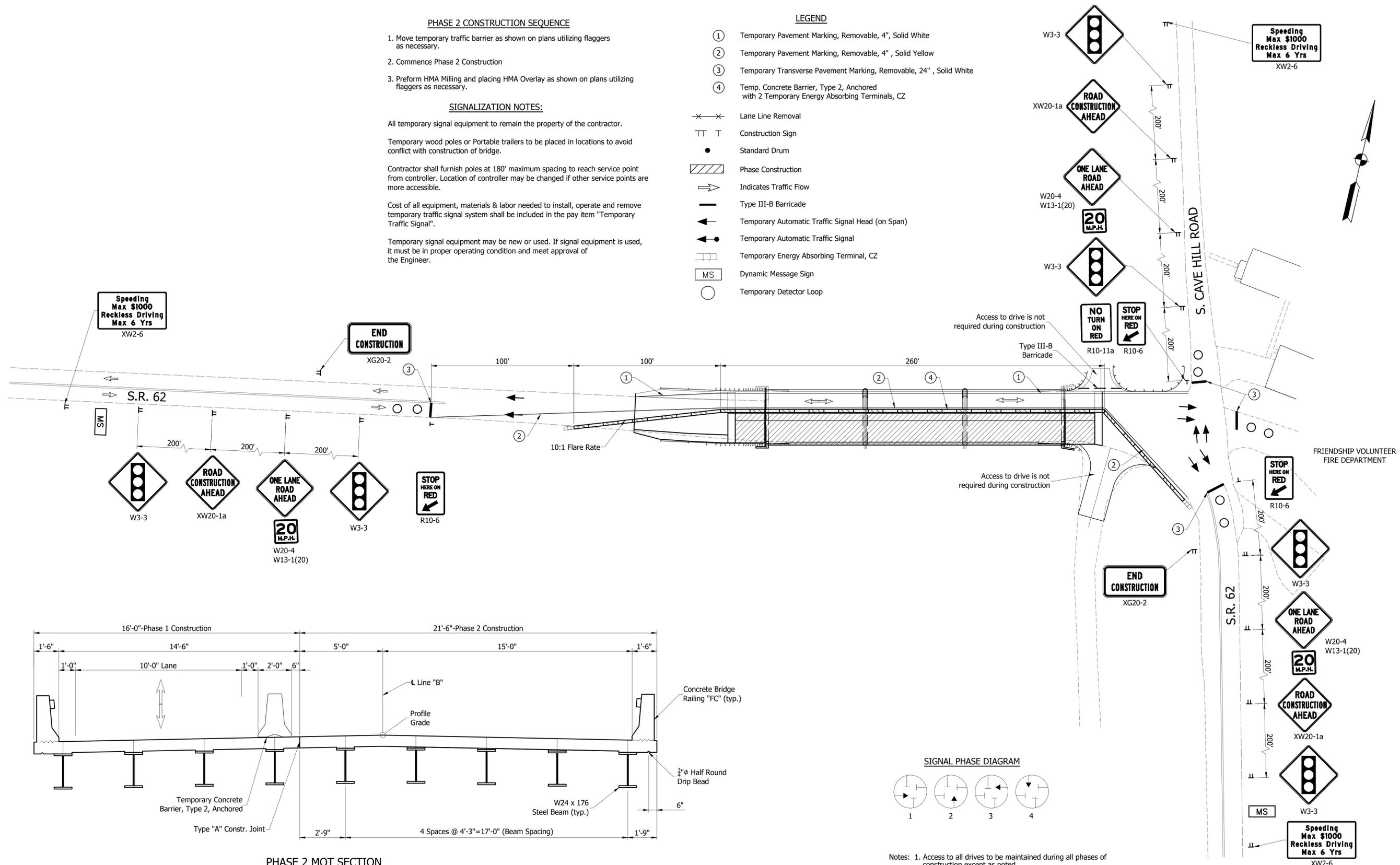
Contractor shall furnish poles at 180' maximum spacing to reach service point from controller. Location of controller may be changed if other service points are more accessible.

Cost of all equipment, materials & labor needed to install, operate and remove temporary traffic signal system shall be included in the pay item "Temporary Traffic Signal".

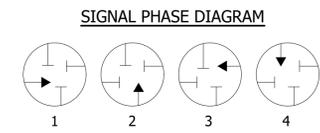
Temporary signal equipment may be new or used. If signal equipment is used, it must be in proper operating condition and meet approval of the Engineer.

LEGEND

- ① Temporary Pavement Marking, Removable, 4", Solid White
- ② Temporary Pavement Marking, Removable, 4", Solid Yellow
- ③ Temporary Transverse Pavement Marking, Removable, 24", Solid White
- ④ Temp. Concrete Barrier, Type 2, Anchored with 2 Temporary Energy Absorbing Terminals, CZ
- X— Lane Line Removal
- TT T Construction Sign
- Standard Drum
- ▨ Phase Construction
- ⇒ Indicates Traffic Flow
- Type III-B Barricade
- ← Temporary Automatic Traffic Signal Head (on Span)
- ← Temporary Automatic Traffic Signal
- Temporary Energy Absorbing Terminal, CZ
- MS Dynamic Message Sign
- Temporary Detector Loop



PHASE 2 MOT SECTION
Scale: 3/8" = 1'-0"



- Notes:
1. Access to all drives to be maintained during all phases of construction except as noted.
 2. Existing conflicting pavement markings to be removed within project limits.

CONSTRUCTION ZONE DESIGN SPEED: 20 MPH

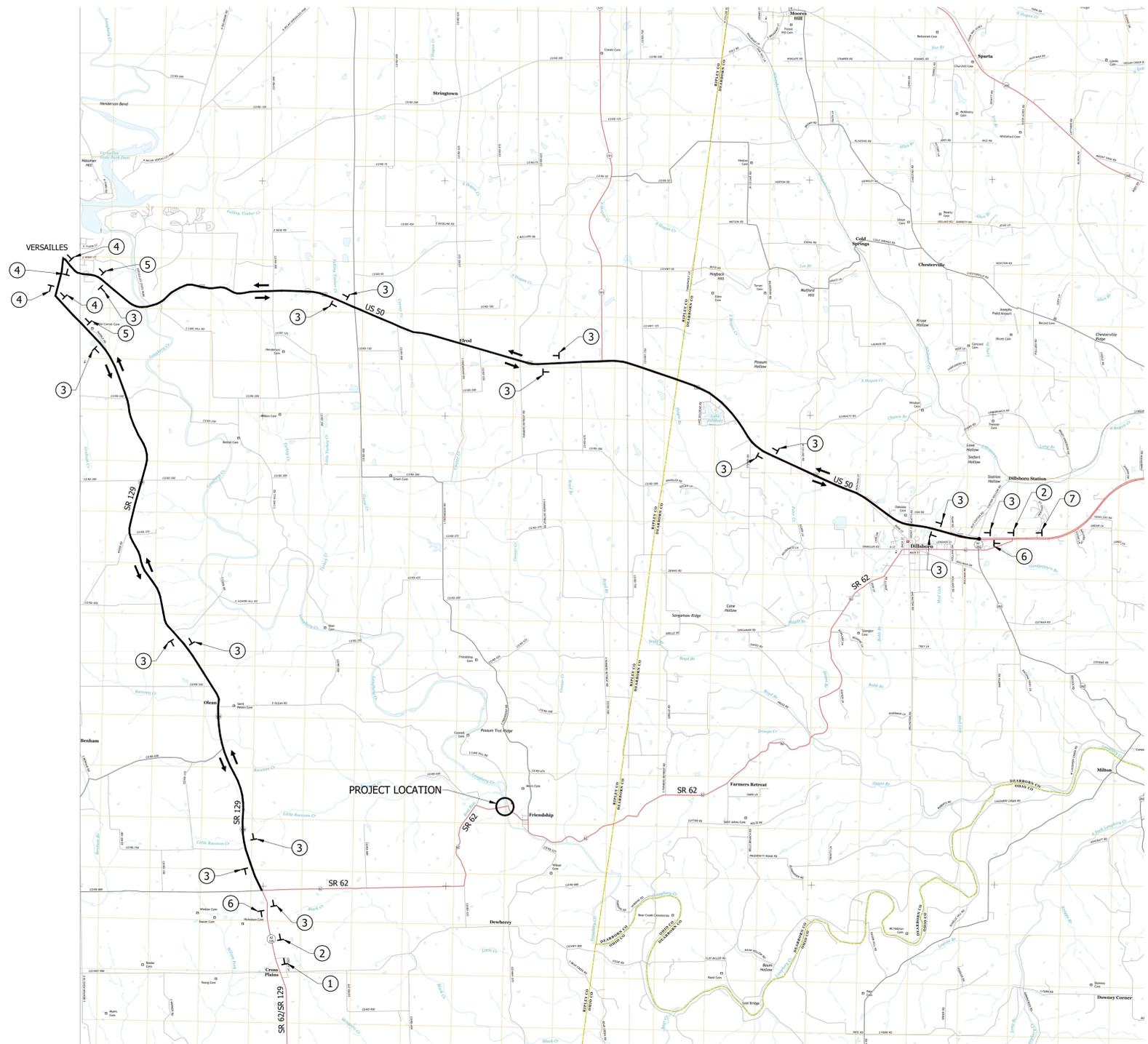
NOT FOR CONSTRUCTION

DESIGNED: MKT	DRAWN: DWB
CHECKED: MJH	CHECKED: MJH

INDIANA DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC PHASE 2

HORIZONTAL SCALE 1"=30'	BRIDGE FILE 062-69-05860 B
VERTICAL SCALE NA	DESIGNATION 1700185
SURVEY BOOK ---	SHEETS 4 of 12
CONTRACT B-40434	PROJECT 1700185



LEGEND	DESCRIPTION	SIZE	TYPE	NO.	
①	ROAD CONSTRUCTION X MILES AHEAD	XW20-2 (4 Miles)	60" X 36"	A	1
②	DETOUR AHEAD	XW20-2	48" X 48"	A	2
	TRUCK ROUTE	R14-1	48" X 48"	A	2
③	TRUCK DETOUR INDIANA 11 WEST EAST	M4-4(S)	24" X 12"	Detour Route Assembly	16
		XM4-8	24" X 12"		
		M1-5 (S.R. 337)	24" X 24"		
		M3-1 or M3-3	24" X 12"		
④	TRUCK DETOUR INDIANA 62 WEST EAST ↑	M4-4(S)	24" X 12"	Detour Route Assembly	4
		XM4-8	24" X 12"		
		M1-5 (S.R. 62)	24" X 24"		
		M3-1 or M3-3	24" X 12"		
⑤	TRUCK DETOUR INDIANA 62 WEST EAST →	M4-4(S)	24" X 12"	Detour Route Assembly	2
		XM4-8	24" X 12"		
		M1-5 (S.R. 62)	24" X 24"		
		M3-1 or M3-3	24" X 12"		
⑥	END DETOUR INDIANA 62 → ↑	M4-4(S)	24" X 12"	Detour Route Assembly	2
		XM4-8	24" X 12"		
		M1-5 (S.R. 62)	24" X 24"		
		M5-1 (L or R) or M6-3	21" X 15"		
⑦	ROAD CONSTRUCTION X MILES AHEAD	XW20-2 (8 Miles)	60" X 36"	A	1

LEGEND
T Construction Sign

NOT FOR CONSTRUCTION

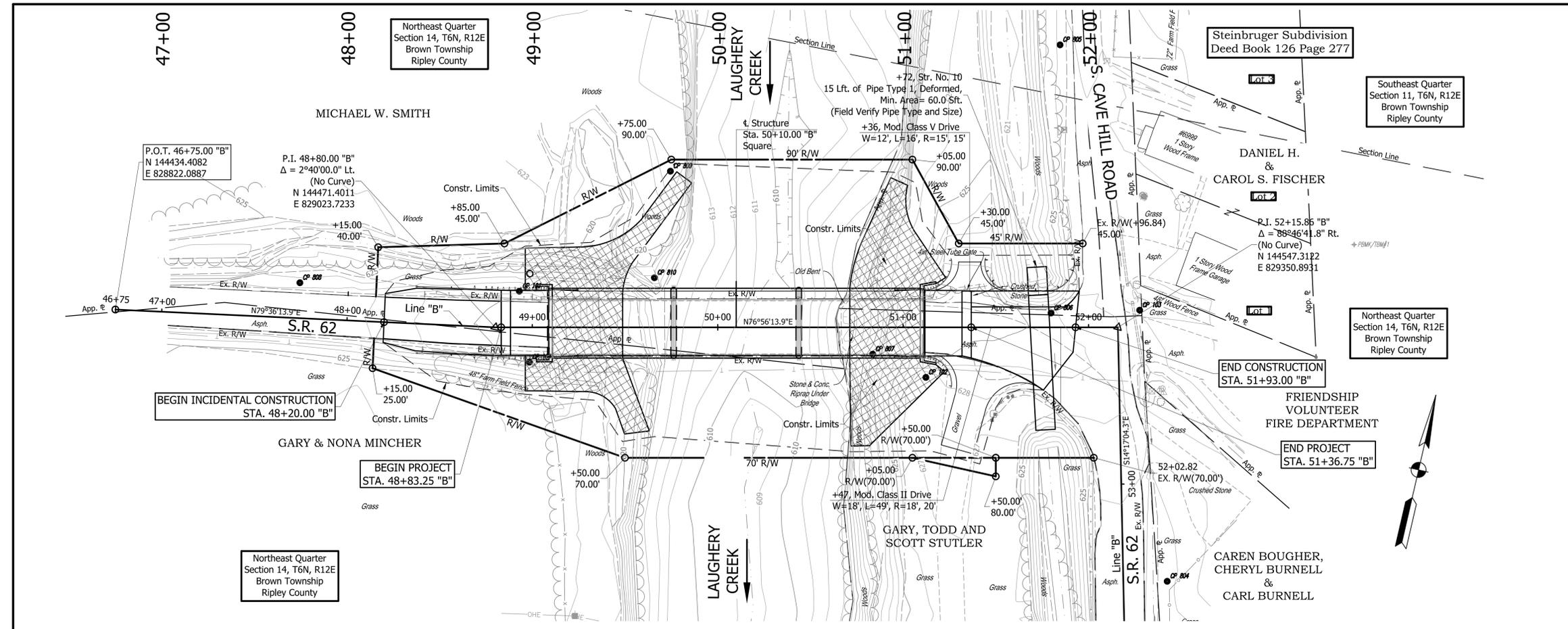
B-12

DESIGNED: MKT DRAWN: DWB
CHECKED: MJH CHECKED: MJH

INDIANA DEPARTMENT OF TRANSPORTATION
MAINTENANCE OF TRAFFIC TRUCK DETOUR

HORIZONTAL SCALE	BRIDGE FILE
1"=4000'	062-69-05860 B
VERTICAL SCALE	DESIGNATION
NA	1700185
SURVEY BOOK	SHEETS
---	6 of 12
CONTRACT	PROJECT
B-40434	1700185

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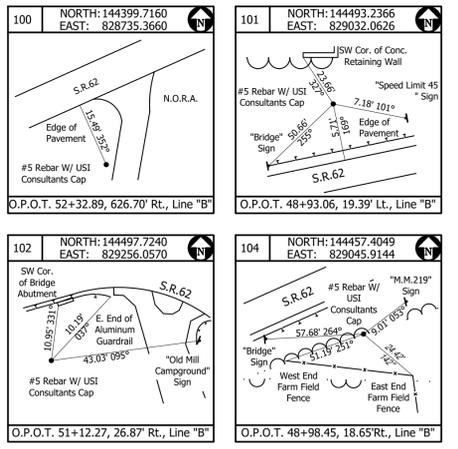


BENCHMARK INFO

TBM#1- CUT "X" IN NW BONNET BOLT OF FIRE HYDRANT THAT IS 125' E OF CAVE HILL DR AND 55' N OF S.R. 62 W EXTENDED. ON FIRE DEPARTMENT PROPERTY. N:144619.3, E:829464.9, ELEV.:634.5426'

TBM#2- RAILROAD SPIKE IN E FACE OF POWER POLE NUMBER LC-106W-50. 205' S OF CL S.R. 62 BRIDGE OVER LAUGHEY CREEK AND 17' W OF CL S.R. 6. N:144336.2, E:829385.7, ELEV.:626.4138'

TBM#3- CHISELED SQUARE IN NE CORNER OF SOUTH HEADWALL FOR CULVERT CROSSING S.R. 62. 325' WEST OF WEST EDGE OF S.R. 62 BRIDGE OVER LAUGHEY CREEK AND 13' SOUTH OF CL OF S.R. 62. 5736 HOUSE NUMBER. JUST WEST OF ENTRY DRIVE TO EASTER RISING FARM, N:144403.6, E:828727.8, ELEV.:630.6779'



HYDRAULIC DATA

SITE DATA:

- Drainage Area: = 200 sq. mile
- Q100 Discharge: = 33,800 cfs
- Q500 Discharge: = 47,320 cfs
- Q100 Elevation: = 628.13 ft.

EXISTING STRUCTURE

- Q100 Headwall Elevation: = 631.33 ft.
- Existing Backwater: = 2.55 ft.
- Velocity @ Q100: = 11.90 ft./s.
- Existing Waterway Opening below Q100 Elevation (Str.): = 1,748.38 sq. ft.
- Existing Road Overflow Waterway Area: = 1,570.46 sq. ft.
- Existing Low Structure Elevation: = 627.31 ft.
- Skew: = 0 degrees

PROPOSED STRUCTURE

- Q100 Headwall Elevation: = 630.86 ft.
- Backwater: = 2.08 ft.
- Velocity At Q100: = 12.49 ft./sec.
- Gross Waterway Opening Req'd. Below Q100 Elevation (Str.): = 1,793.56 sq. ft.
- Road Overflow Waterway Area: = 1,403.47 sq. ft.
- Low Structure Elevation: = 627.31'
- Skew: = 0 degrees

LOW FLOW SCOUR

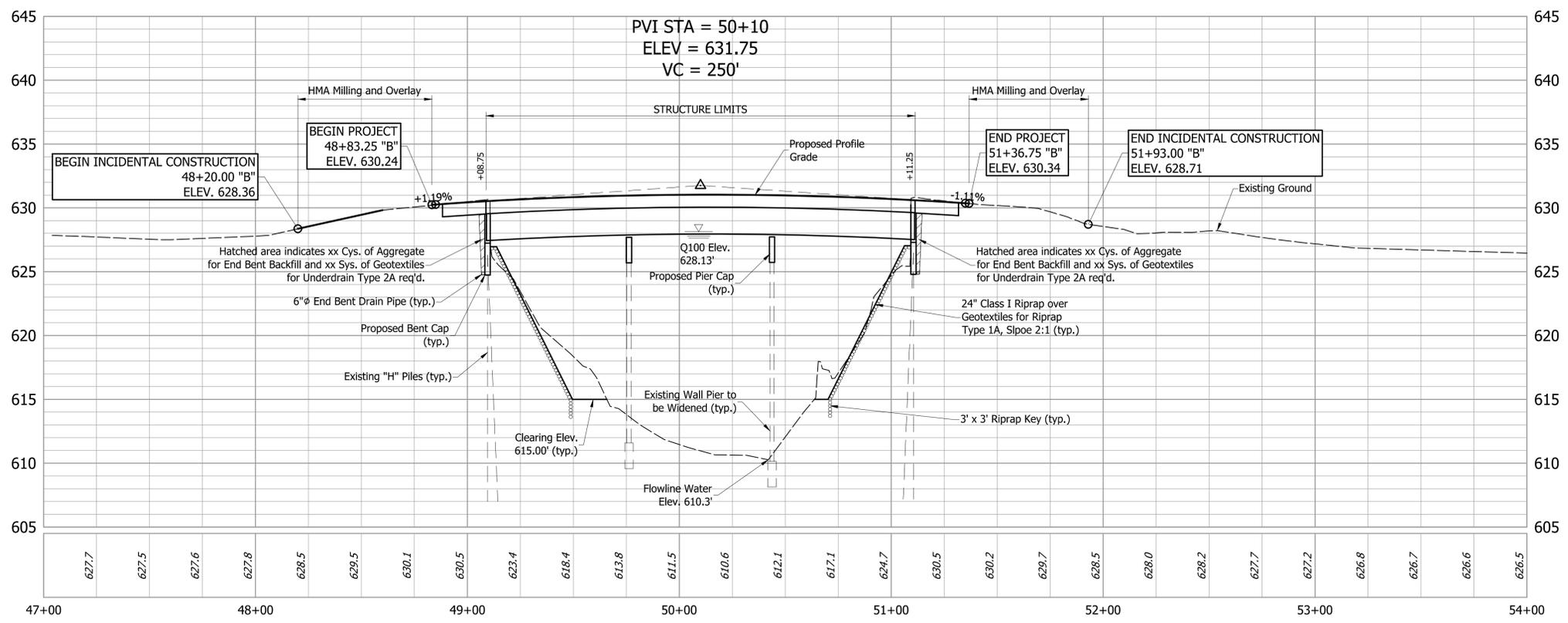
- Contraction Scour Depth: = 6.49 ft.
- Pier Scour Depth: = 4.80 ft.
- Total Scour Depth: = 11.29 ft.
- Low Scour Elevation: = 598.94 ft.
- Max. Velocity: = 14.60 ft./sec.

Q100 SCOUR

- Q100 Contraction Scour: = 4.99 ft.
- Q100 Pier Scour Depth: = 4.80 ft.
- Q100 Total Scour Depth: = 9.79 ft.
- Q100 Low Scour Elevation: = 600.44 ft.
- Max. Velocity @ Q100: = 15.98 ft./sec.

Q500 SCOUR

- Q500 Contraction Scour: = 10.63 ft.
- Q500 Pier Scour Depth: = 4.80 ft.
- Q500 Total Scour Depth: = 15.43 ft.
- Q500 Low Scour Elevation: = 594.80 ft.
- Max. Velocity @ Q500: = 16.76 ft./sec.



CONTINUOUS STEEL BEAM BRIDGE

THREE SPANS @ 66'-9", 67'-6", 66'-9", SQUARE
 34'-6" CLEAR ROADWAY WIDTH
 SR 62 OVER LAUGHEY CREEK
 RIPLEY COUNTY, INDIANA

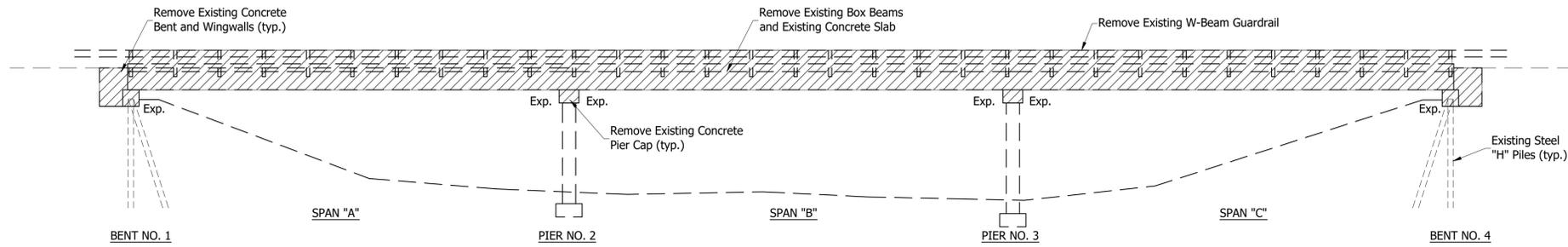
NOT FOR CONSTRUCTION

DESIGNED:	MKT	DRAWN:	DWB
CHECKED:	MJH	CHECKED:	MJH

INDIANA
 DEPARTMENT OF TRANSPORTATION
BRIDGE LAYOUT

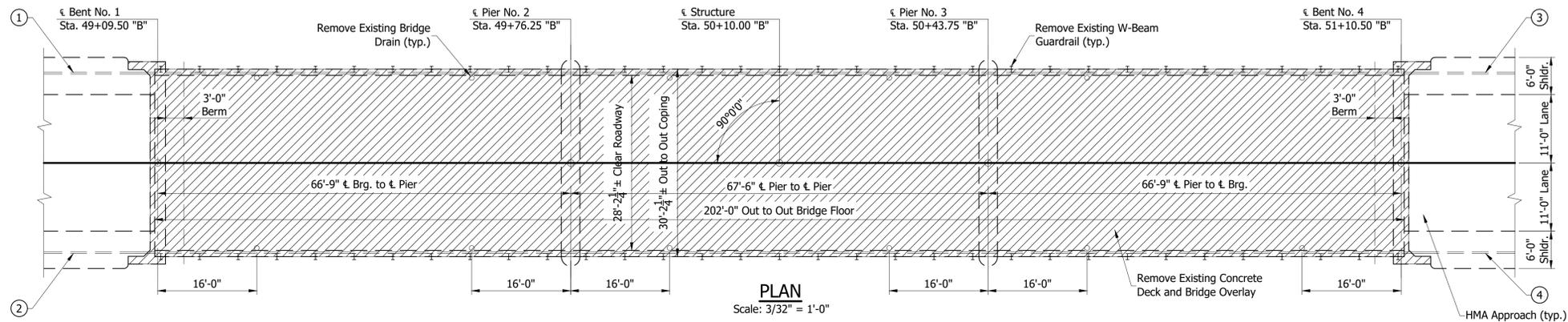
HORIZONTAL SCALE	BRIDGE FILE
1"=30'	062-69-05860 B
VERTICAL SCALE	DESIGNATION
1"=5'	1700185
SURVEY BOOK	SHEETS
----	9 of 12
CONTRACT	PROJECT
B-40434	1700185

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ELEVATION
Scale: 3/32" = 1'-0"

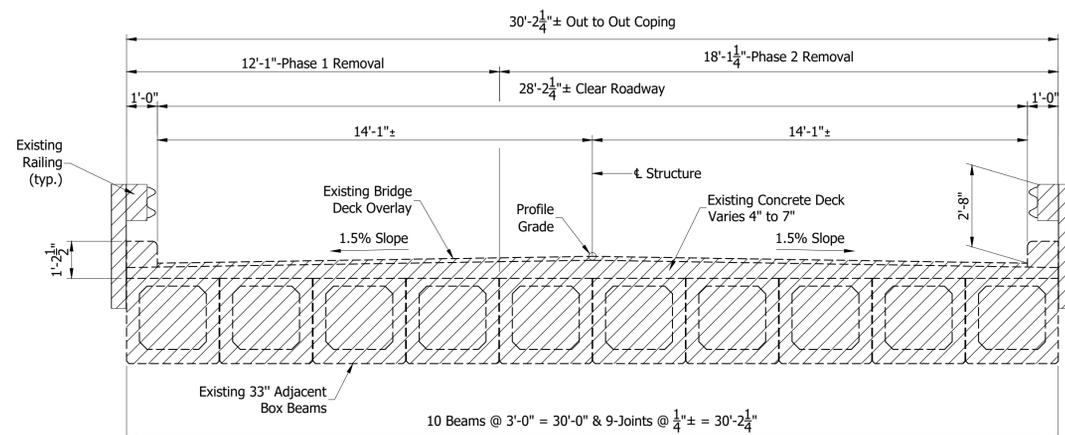
NOTE: Hatched area indicates portions to be removed



PLAN
Scale: 3/32" = 1'-0"

LEGEND

- ① Remove 62.5 Lft. Existing W-Beam Guardrail
- ② Remove 62.5 Lft. Existing W-Beam Guardrail
- ③ Remove 100 Lft. Existing W-Beam Guardrail
- ④ Remove 10 Lft. Existing W-Beam Guardrail



TYPICAL SECTION
Scale: 3/8" = 1'-0"

EXISTING
CONTINUOUS PRESTRESSED
CONCRETE BOX BEAM BRIDGE
THREE SPANS @ 66'-9", 67'-6", 66'-9", SQUARE
28'-2 1/4" CLEAR ROADWAY WIDTH
SR 62 OVER LAUGHERY CREEK
RIPLEY COUNTY, INDIANA

S:\2018\proj\2018-109-RR-62-Drw-1700185\Plan\Bldg_Plan\2018-109-General Plan-Existing.dwg, General Plan-Existing, 9/17/2019 10:50:10 AM

**NOT FOR
CONSTRUCTION**

DESIGNED: MKT DRAWN: DWB
CHECKED: MJH CHECKED: MJH

INDIANA
DEPARTMENT OF TRANSPORTATION
GENERAL PLAN
EXISTING

HORIZONTAL SCALE	BRIDGE FILE
AS NOTED	062-69-05860 B
VERTICAL SCALE	DESIGNATION
AS NOTED	1700185
SURVEY BOOK	SHEETS
----	10 of 12
CONTRACT	PROJECT
B-40434	1700185

GENERAL NOTES:

Plans for the existing structure are on file in the Central Office, Indiana Department of Transportation as Bridge File No. 062-69-05860 and are available upon request.

Portions of present structure shall be removed as noted.

Where new work is to be fitted to old work, the contractor shall check all dimensions and conditions in the field and report any errors or discrepancies to the engineer and assume responsibility for their correctness and the fit of the new part to the old.

Reinforcing steel covering shall be 2 1/2" in top and 1" min. in bottom of floor slabs, 3" in footings except bottom steel which shall be 4", and 2" in all other parts, unless noted.

Concrete in floor slab and concrete barrier railings is to be Class "C".

Surface Seal exposed surfaces of wingwalls, face of deck coping, all faces of concrete barrier railing and approach slabs. Estimated Quantity = xxx Sft.

DESIGN DATA:

Designed for HS20-44 Truck and Alternate Military Loading in accordance with the 2002 AASHTO Specifications and interim specifications.

Designed for actual dead load plus 35 lbs./sft. future wearing surface

Slab designed with 1 1/2" wearing surface.

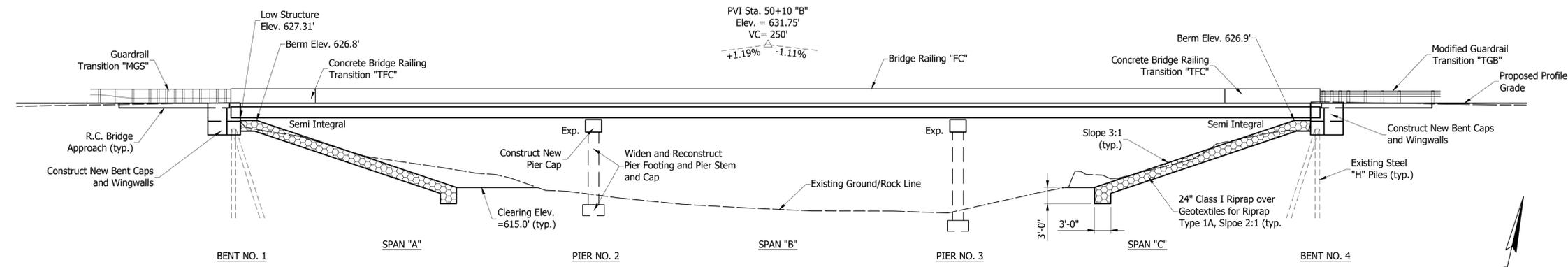
UNIT STRESSES:

Class "C" Concrete $F_c = 4000$ p.s.i.
 Class "A" Concrete $F_c = 3500$ p.s.i.
 Class "B" Concrete $F_c = 3000$ p.s.i.
 Reinforcing Steel (Grade 60) $F_y = 60,000$ p.s.i.

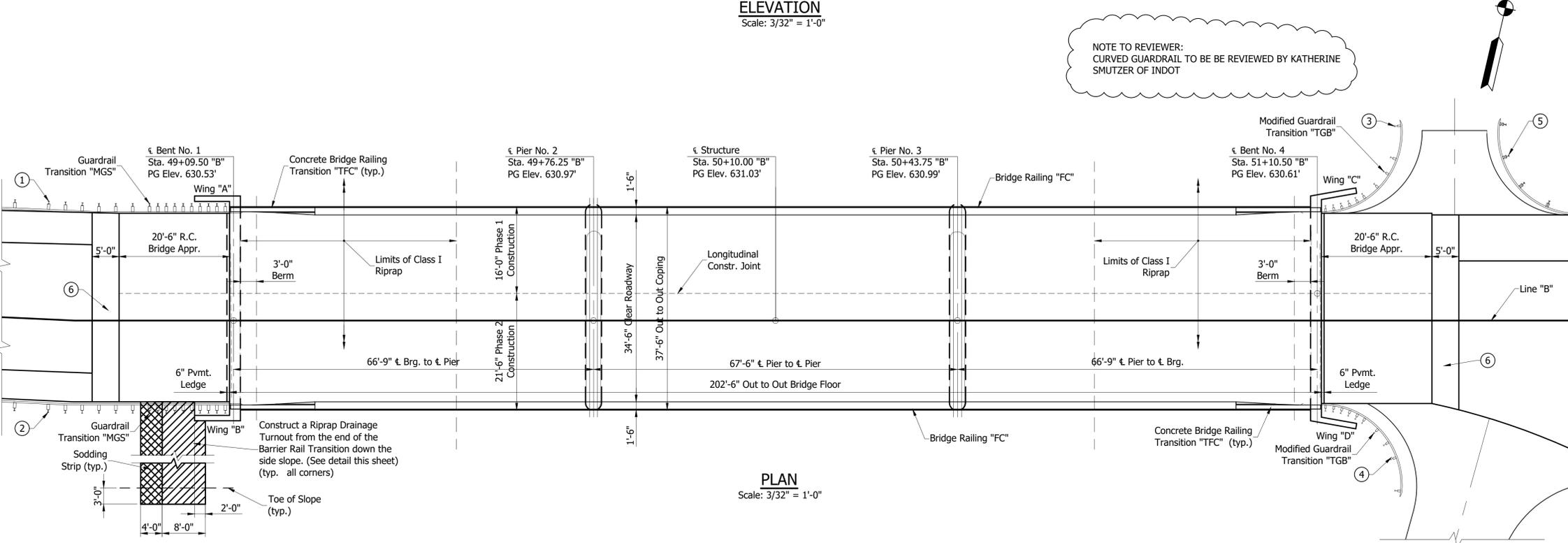
LEGEND

- ① 1-Guardrail Transition MGS, and 1-Guardrail End Treatment type OS req'd.
- ② 1-Guardrail Transition MGS, and 1-Guardrail End Treatment type OS req'd.
- ③ 1-Modified Guardrail Transition TGB (Use Thrie Beam and 15' Radius), and 1-Guardrail Curved Terminal End req'd.
- ④ 1-Modified Guardrail Transition TGB (Use Thrie Beam and 15' Radius), and 1-Guardrail Curved Terminal End req'd.
- ⑤ 1-Guardrail Curved Terminal End, 25 Lft. of Mod. W-Beam Guardrail (15' Radius), 18.75 Lft. of W-Beam Guardrail and 25 Lft. of Mod. W-Beam Guardrail (15' Radius) req'd.
- ⑥ 165 lbs/syd QC/QA HMA, 3, 70, Surface, 9.5 mm on 275 lbs/syd QC/QA HMA, 3, 64, intermediate, 19.0 mm on 330 lbs/syd QC/QA HMA, 3, 64, Base, 19.0 mm on Subgrade Treatment Type "IC"

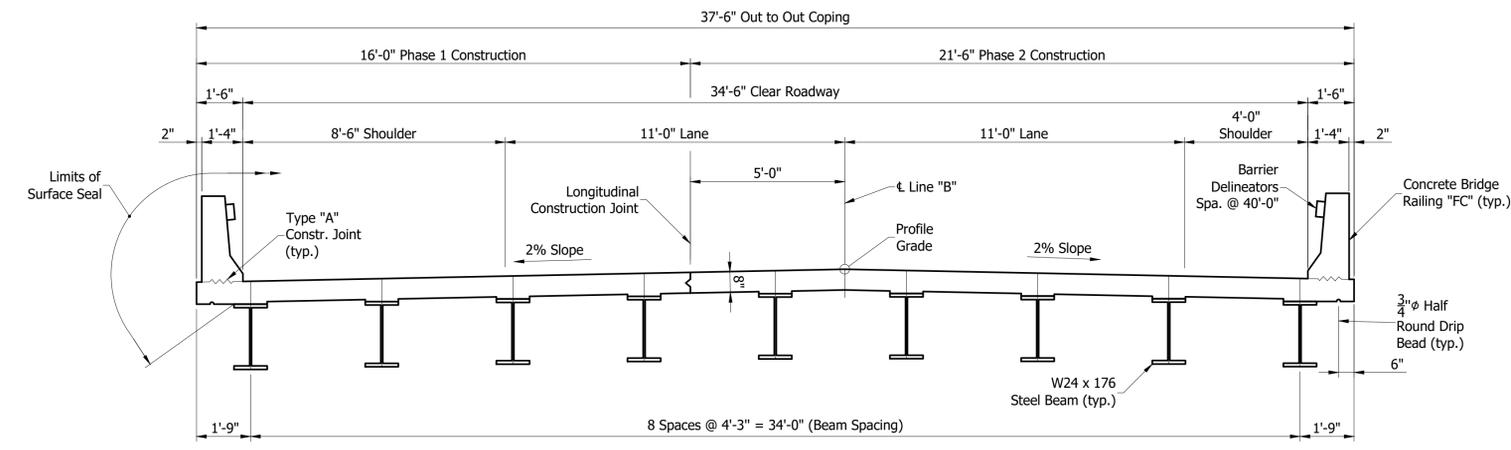
NOTE TO REVIEWER:
 ASSUMED PAVEMENT SECTION, FINAL PAVEMENT DESIGN TO BE PROVIDED BY INDOT



ELEVATION
 Scale: 3/32" = 1'-0"

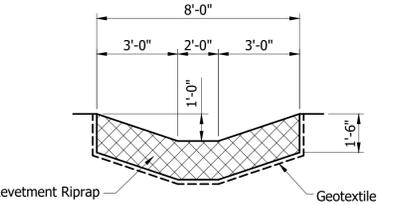


PLAN
 Scale: 3/32" = 1'-0"



TYPICAL SECTION
 Scale: 3/8" = 1'-0"

NOTE TO REVIEWER:
 SUPERSTRUCTURE TYPE CHANGED FROM SCOPING REPORT SUBMITTAL TO SATISFY HYDRAULIC REQUIREMENTS (NO ADDITIONAL BACKWATER CREATED)



RIPRAP DRAINAGE TURNOUT DETAIL
 Not to Scale

BRIDGE PRESERVATION
CONTINUOUS STEEL BEAM BRIDGE
 THREE SPANS @ 66'-9", 67'-6", 66'-9", SQUARE
 34'-6" CLEAR ROADWAY WIDTH
 SR 62 OVER LAUGHERY CREEK
 RIPLEY COUNTY, INDIANA

NOT FOR CONSTRUCTION

DESIGNED: MKT	DRAWN: DWB
CHECKED: MJH	CHECKED: MJH

INDIANA
 DEPARTMENT OF TRANSPORTATION

 GENERAL PLAN
 PROPOSED

HORIZONTAL SCALE	BRIDGE FILE
AS NOTED	062-69-05860 B
VERTICAL SCALE	DESIGNATION
AS NOTED	1700185
SURVEY BOOK	SHEETS
----	11 of 12
CONTRACT	PROJECT
B-40434	1700185

Des 1700185 CE-2

Appendix C

Early Coordination

Sample Early Coordination Letter -
Graphics and photos from Appendix B
were attached.

fun. innovative. smart.

g r e e n • 3



March 13, 2020

Re: Des. No. 1700185, SR 62 over Laughery Creek Bridge Project, Approximately 3.91 Miles East of SR 129

Dear Environmental Reviewer,

The Indiana Department of Transportation (INDOT) and Federal Highway Administration (FHWA) intend to proceed with a bridge rehabilitation project at State Road (SR) 62 over Laughery Creek in Ripley County, Indiana. This letter is part of the early coordination phase of the environmental review process. We are requesting comments from your area of expertise regarding any possible environmental effects associated with this project. Please use the above designation number (Des. No. 1700185) and description in your reply. We will incorporate your comments into a study of the project’s environmental impacts.

This project is located at SR 62 over Laughery Creek, approximately 3.91 miles east of SR 129 in Brown Township, Ripley County, Indiana. This section of SR 62 is a two lane Major Collector. The existing bridge (062-69-05860A) is a prestressed concrete continuous box beam with 3 spans. The existing structure is in poor condition due to deterioration, and has a wearing surface with a rating of 3 out of 9, and a superstructure in poor condition with a rating of 4 out of 9 (with advanced deterioration). The intent of this project is to replace the superstructure. The preferred alternative is a bridge rehabilitation, specifically on the superstructure, that will increase the service of the bridge to an additional 20-30 years. Right of way has not been finalized; however, it is anticipated that more than 0.5 acre of right of way will be required. No relocations of residents or businesses will be required for this project. Traffic will be maintained through phased construction for local traffic and a detour route for truck traffic. One side of the bridge will be removed and replaced while the other side is open for traffic. The detour route for truck traffic will utilize US 50 and SR 129 and will be approximately 21 miles in length.

Land use in the vicinity of the project is primarily agricultural, with residential areas to the east and the southwest. Green 3, LLC. will perform waters and wetlands determinations and a biological assessment to identify any ecological resources that may be present. This project qualifies for the application of the USFWS range-wide programmatic informal consultation for the Indiana bat and northern long-eared bat and project information will be submitted through USFWS’s Information for Planning and Consultation (IPaC) separately. Permit coordination will occur with INDOT Ecology and Waterway Permitting specialists to determine permit requirements.

Please respond with comments, questions, and concerns **within thirty (30) calendar days** from the date of this letter; if no response is received, it will be assumed that your agency feels that there are no adverse effects incurred as a result of this proposed project. However, should you find that an extension to the response time is necessary, a reasonable amount may be granted upon request. If you have any questions regarding this matter, please feel free to contact Laney Walstra at Green 3, LLC at laney@green3studio.com or 317-634-4110, or INDOT Project Manager Terry Summers at tsummers@indot.in.gov or 812-524-3749. Thank you in advance for your input on this project.

Sincerely,

Laney Walstra
Green 3 LLC

Enclosures:
Project Maps
Project Area Photographs

**SR 62 over Laughery Creek Bridge Project
DES 1700185, Ripley County
Early Coordination Notice Sent to the Following Agencies:**

Federal Highway Administration
Seymour District
erica.tait@dot.gov

Indiana Geological Survey
On-Line Submission
<https://igs.indiana.edu/eAssessment/>

Christie Stanifer
Environmental Coordinator
IDNR, Div. of Fish & Wildlife
environmentalreview@dnr.in.gov

IDEM Groundwater Section
<https://www.in.gov/idem/cleanwater/pages/wellhead/>

IDEM
On-Line Submission
<http://www.in.gov/idem/5284.htm>

INDOT
Manager, Public Hearings
rclark@indot.in.gov

US Dept. of HUD
Melanie Castillo
melanie.h.castillo@hud.gov

Regional Environmental Coordinator
Midwest Regional Office
National Park Service
601 Riverfront Drive Omaha, Nebraska 6810
Field Supervisor

U.S. Fish and Wildlife Service
Bloomington Office
Robin McWilliams
robin_mcwilliams@fws.gov

State Conservationist NRCS
Indianapolis Office
Rick Neilson
rick.neilson@in.usda.gov

INDOT District Environmental Manager
David Dye
DDye@indot.in.gov

U.S. Army Corps of Engineers Louisville District
Gregory.A.McKay@usace.army.mil

Chief, Bridge Program Section
Eighth Coast Guard District
1222 Spruce St
St. Louis, Missouri 63103-2832

Ripley County Surveyor
surveyor@ripleycounty.com

Ripley Co. Commissioners
Mark Horstman, President
commissionerhorstman@ripleycounty.com

Ripley County Council
Ronald J. Decker, President Pro Tem
councildecker@ripleycounty.com

Floodplain Administrator
tbrinson@ripleycounty.com

Ripley Co. Highway Department
rchwy@comcast.net

Old Mill Campground and Flea Market
oldmillfriendshipfleamarket@gmail.com

Friendship Volunteer Fire Dept.
friendshipfire@seidata.com



Indiana Department of Environmental Management

We Protect Hoosiers and Our Environment.

100 North Senate Avenue - Indianapolis, IN 46204
(800) 451-6027 - (317) 232-8603 - www.idem.IN.gov

INDOT-Seymour District

185 Agrico Lane
Seymour , IN 47274
Date

Green 3, LLC.
Laney Walstra
1104 Prospect Street
Indianapolis , IN 46203

To Engineers and Consultants Proposing Roadway Construction Projects:

RE: The existing bridge (062-69-05860A) is a prestressed concrete continuous box beam with 3 spans. The existing structure is in poor condition due to deterioration, and has a wearing surface with a rating of 3 out of 9, and a superstructure in poor condition with a rating of 4 out of 9 (with advanced deterioration). The preferred alternative is a bridge rehabilitation, specifically on the superstructure, that will increase the service of the bridge to an additional 20-30 years. Right of way has not been finalized; however, it is anticipated that more than 0.5 acre of right of way will be required. Traffic will be maintained through phased construction for local traffic and a detour route for truck traffic. One side of the bridge will be removed and replaced while the other side is open for traffic. The detour route for truck traffic will utilize US 50 and SR 129 and will be approximately 21 miles in length.

This letter from the Indiana Department of Environmental Management (IDEM) serves as a standardized response to enquiries inviting IDEM comments on roadway construction, reconstruction, or other improvement projects within existing roadway corridors when the proposed scope of the project is beneath the threshold requiring a formal National Environmental Policy Act-mandated Environmental Assessment or Environmental Impact Statement. As the letter attempts to address all roadway-related environmental topics of potential concern, it is possible that not every topic addressed in the letter will be applicable to your particular roadway project.

For additional information on specific roadway-related topics of interest, please visit the appropriate Web pages cited below, many of which provide contact information for persons within the various program areas who can answer questions not fully addressed in this letter. Also please be mindful that some environmental requirements may be subject to change and so each person intending to include a copy of this letter in their project documentation packet is advised to download the most recently revised version of the letter; found at: <http://www.in.gov/idem/5283.htm> (<http://www.in.gov/idem/5283.htm>).

To ensure that all environmentally-related issues are adequately addressed, IDEM recommends that you read this letter in its entirety, and consider each of the following issues as you move forward with the planning of your proposed roadway construction, reconstruction, or improvement project:

WATER AND BIOTIC QUALITY

1. Section 404 of the Clean Water Act requires that you obtain a permit from the U.S. Army Corps of Engineers (USACE) before discharging dredged or fill materials into any wetlands or other waters, such as rivers, lakes, streams, and ditches. Other activities regulated include the relocation, channelization, widening, or other such alteration of a stream, and the mechanical clearing (use of heavy construction equipment) of wetlands. Thus, as a project owner or sponsor, it is your responsibility to ensure that no wetlands are disturbed without the proper permit. Although you may initially refer to the U.S. Fish and Wildlife Service National Wetland Inventory maps as a means of identifying potential areas of concern, please be mindful that those maps do

not depict jurisdictional wetlands regulated by the USACE or the Department of Environmental Management. A valid jurisdictional wetlands determination can only be made by the USACE, using the 1987 Wetland Delineation Manual.

USACE recommends that you have a consultant check to determine whether your project will abut, or lie within, a wetland area. To view a list of consultants that have requested to be included on a list posted by the USACE on their Web site, see USACE Permits and Public Notices (<http://www.lrl.usace.army.mil/orf/default.asp>) (<http://www.lrl.usace.army.mil/orf/default.asp>) and then click on "Information" from the menu on the right-hand side of that page. Their "Consultant List" is the fourth entry down on the "Information" page. Please note that the USACE posts all consultants that request to appear on the list, and that inclusion of any particular consultant on the list does not represent an endorsement of that consultant by the USACE, or by IDEM.

Much of northern Indiana (Newton, Lake, Porter, LaPorte, St. Joseph, Elkhart, LaGrange, Steuben, and Dekalb counties; large portions of Jasper, Starke, Marshall, Noble, Allen, and Adams counties; and lesser portions of Benton, White, Pulaski, Kosciusko, and Wells counties) is served by the USACE District Office in Detroit (313-226-6812). The central and southern portions of the state (large portions of Benton, White, Pulaski, Kosciusko, and Wells counties; smaller portions of Jasper, Starke, Marshall, Noble, Allen, and Adams counties; and all other Indiana counties located in north-central, central, and southern Indiana) are served by the USACE Louisville District Office (502-315-6733).

Additional information on contacting these U.S. Army Corps of Engineers (USACE) District Offices, government agencies with jurisdiction over wetlands, and other water quality issues, can be found at <http://www.in.gov/idem/4396.htm> (<http://www.in.gov/idem/4396.htm>). IDEM recommends that impacts to wetlands and other water resources be avoided to the fullest extent.

2. In the event a Section 404 wetlands permit is required from the USACE, you also must obtain a Section 401 Water Quality Certification from the IDEM Office of Water Quality Wetlands Program. To learn more about the Wetlands Program, visit: <http://www.in.gov/idem/4384.htm> (<http://www.in.gov/idem/4384.htm>).
3. If the USACE determines that a wetland or other water body is isolated and not subject to Clean Water Act regulation, it is still regulated by the state of Indiana. A State Isolated Wetland permit from IDEM's Office of Water Quality (OWQ) is required for any activity that results in the discharge of dredged or fill materials into isolated wetlands. To learn more about isolated wetlands, contact the OWQ Wetlands Program at 317-233-8488.
4. If your project will involve over a 0.5 acre of wetland impact, stream relocation, or other large-scale alterations to water bodies such as the creation of a dam or a water diversion, you should seek additional input from the OWQ Wetlands Program staff. Consult the Web at: <http://www.in.gov/idem/4384.htm> (<http://www.in.gov/idem/4384.htm>) for the appropriate staff contact to further discuss your project.
5. Work within the one-hundred year floodway of a given water body is regulated by the Department of Natural Resources, Division of Water. The Division issues permits for activities regulated under the following statutes:
 - o IC 14-26-2 Lakes Preservation Act 312 IAC 11
 - o IC 14-26-5 Lowering of Ten Acre Lakes Act No related code
 - o IC 14-28-1 Flood Control Act 310 IAC 6-1
 - o IC 14-29-1 Navigable Waterways Act 312 IAC 6
 - o IC 14-29-3 Sand and Gravel Permits Act 312 IAC 6
 - o IC 14-29-4 Construction of Channels Act No related code

For information on these Indiana (statutory) Code and Indiana Administrative Code citations, see the DNR Web site at: <http://www.in.gov/dnr/water/9451.htm> (<http://www.in.gov/dnr/water/9451.htm>). Contact the DNR Division of Water at 317-232-4160 for further information.

The physical disturbance of the stream and riparian vegetation, especially large trees overhanging any

affected water bodies should be limited to only that which is absolutely necessary to complete the project. The shade provided by the large overhanging trees helps maintain proper stream temperatures and dissolved oxygen for aquatic life.

6. For projects involving construction activity (which includes clearing, grading, excavation and other land disturbing activities) that result in the disturbance of one (1), or more, acres of total land area, contact the Office of Water Quality – Watershed Planning Branch (317/233-1864) regarding the need for of a Rule 5 Storm Water Runoff Permit. Visit the following Web page
 - o <http://www.in.gov/idem/4902.htm> (<http://www.in.gov/idem/4902.htm>)

To obtain, and operate under, a Rule 5 permit you will first need to develop a Construction Plan (<http://www.in.gov/idem/4917.htm#constreq> (<http://www.in.gov/idem/4917.htm#constreq>)), and as described in 327 IAC 15-5-6.5 (<http://www.in.gov/legislative/iac/T03270/A00150> [PDF] (<http://www.in.gov/legislative/iac/T03270/A00150.PDF>), pages 16 through 19). Before you may apply for a Rule 5 Permit, or begin construction, you must submit your Construction Plan to your county Soil and Water Conservation District (SWCD) (<http://www.in.gov/isda/soil/contacts/map.html> (<http://www.in.gov/isda/soil/contacts/map.html>)).

Upon receipt of the construction plan, personnel of the SWCD or the Indiana Department of Environmental Management will review the plan to determine if it meets the requirements of 327 IAC 15-5. Plans that are deemed deficient will require re-submittal. If the plan is sufficient you will be notified and instructed to submit the verification to IDEM as part of the Rule 5 Notice of Intent (NOI) submittal. Once construction begins, staff of the SWCD or Indiana Department of Environmental Management will perform inspections of activities at the site for compliance with the regulation.

Please be mindful that approximately 149 Municipal Separate Storm Sewer System (MS4) areas are now being established by various local governmental entities throughout the state as part of the implementation of Phase II federal storm water requirements. All of these MS4 areas will eventually take responsibility for Construction Plan review, inspection, and enforcement. As these MS4 areas obtain program approval from IDEM, they will be added to a list of MS4 areas posted on the IDEM Website at: <http://www.in.gov/idem/4900.htm> (<http://www.in.gov/idem/4900.htm>).

If your project is located in an IDEM-approved MS4 area, please contact the local MS4 program about meeting their storm water requirements. Once the MS4 approves the plan, the NOI can be submitted to IDEM.

Regardless of the size of your project, or which agency you work with to meet storm water requirements, IDEM recommends that appropriate structures and techniques be utilized both during the construction phase, and after completion of the project, to minimize the impacts associated with storm water runoff. The use of appropriate planning and site development and appropriate storm water quality measures are recommended to prevent soil from leaving the construction site during active land disturbance and for post construction water quality concerns. Information and assistance regarding storm water related to construction activities are available from the Soil and Water Conservation District (SWCD) offices in each county or from IDEM.

7. For projects involving impacts to fish and botanical resources, contact the Department of Natural Resources - Division of Fish and Wildlife (317/232-4080) for addition project input.
8. For projects involving water main construction, water main extensions, and new public water supplies, contact the Office of Water Quality - Drinking Water Branch (317-308-3299) regarding the need for permits.
9. For projects involving effluent discharges to waters of the State of Indiana , contact the Office of Water Quality - Permits Branch (317-233-0468) regarding the need for a National Pollutant Discharge Elimination System (NPDES) permit.
10. For projects involving the construction of wastewater facilities and sewer lines, contact the Office of Water Quality - Permits Branch (317-232-8675) regarding the need for permits.

AIR QUALITY

The above-noted project should be designed to minimize any impact on ambient air quality in, or near, the project area. The project must comply with all federal and state air pollution regulations. Consideration should be given to the following:

1. Regarding open burning, and disposing of organic debris generated by land clearing activities; some types of open burning are allowed (<http://www.in.gov/idem/4148.htm> (<http://www.in.gov/idem/4148.htm>)) under specific conditions. You also can seek an open burning variance from IDEM.

However, IDEM generally recommends that you take vegetative wastes to a registered yard waste composting facility or that the waste be chipped or shredded with composting on site (you must register with IDEM if more than 2,000 pounds is to be composted; contact 317/232-0066). The finished compost can then be used as a mulch or soil amendment. You also may bury any vegetative wastes (such as leaves, twigs, branches, limbs, tree trunks and stumps) onsite, although burying large quantities of such material can lead to subsidence problems, later on.

Reasonable precautions must be taken to minimize fugitive dust emissions from construction and demolition activities. For example, wetting the area with water, constructing wind barriers, or treating dusty areas with chemical stabilizers (such as calcium chloride or several other commercial products). Dirt tracked onto paved roads from unpaved areas should be minimized.

Additionally, if construction or demolition is conducted in a wooded area where blackbirds have roosted or abandoned buildings or building sections in which pigeons or bats have roosted for 3-5 years precautionary measures should be taken to avoid an outbreak of histoplasmosis. This disease is caused by the fungus *Histoplasma capsulatum*, which stems from bird or bat droppings that have accumulated in one area for 3-5 years. The spores from this fungus become airborne when the area is disturbed and can cause infections over an entire community downwind of the site. The area should be wetted down prior to cleanup or demolition of the project site. For more detailed information on histoplasmosis prevention and control, please contact the Acute Disease Control Division of the Indiana State Department of Health at (317) 233-7272.

2. The U.S. EPA and the Surgeon General recommend that people not have long-term exposure to radon at levels above 4 pCi/L. (For a county-by-county map of predicted radon levels in Indiana, visit: <http://www.in.gov/idem/4145.htm> (<http://www.in.gov/idem/4145.htm>).

The U.S. EPA further recommends that all homes (and apartments within three stories of ground level) be tested for radon. If in-home radon levels are determined to be 4 pCi/L, or higher, EPA recommends a follow-up test. If the second test confirms that radon levels are 4 pCi/L, or higher, EPA recommends the installation of radon-reduction measures. (For a list of qualified radon testers and radon mitigation (or reduction) specialists visit: http://www.in.gov/isdh/regsvcs/radhealth/pdfs/radon_testers_mitigators_list.pdf (http://www.in.gov/isdh/regsvcs/radhealth/pdfs/radon_testers_mitigators_list.pdf)). It also is recommended that radon reduction measures be built into all new homes, particularly in areas like Indiana that have moderate to high predicted radon levels.

To learn more about radon, radon risks, and ways to reduce exposure visit: <http://www.in.gov/isdh/regsvcs/radhealth/radon.htm> (<http://www.in.gov/isdh/regsvcs/radhealth/radon.htm>), <http://www.in.gov/idem/4145.htm> (<http://www.in.gov/idem/4145.htm>), or <http://www.epa.gov/radon/index.html> (<http://www.epa.gov/radon/index.html>).

3. With respect to asbestos removal: all facilities slated for renovation or demolition (except residential buildings that have (4) four or fewer dwelling units and which will not be used for commercial purposes) must be inspected by an Indiana-licensed asbestos inspector prior to the commencement of any renovation or demolition activities. If regulated asbestos-containing material (RACM) that may become airborne is found, any subsequent demolition, renovation, or asbestos removal activities must be performed in accordance with the proper notification and emission control requirements.

If no asbestos is found where a renovation activity will occur, or if the renovation involves removal of less than 260 linear feet of RACM off of pipes, less than 160 square feet of RACM off of other facility components, or less than 35 cubic feet of RACM off of all facility components, the owner or operator of the project does not need to notify IDEM before beginning the renovation activity.

For questions on asbestos demolition and renovation activities, you can also call IDEM's Lead/Asbestos section at 1-888-574-8150.

However, in all cases where a demolition activity will occur (even if no asbestos is found), the owner or operator must still notify IDEM 10 working days prior to the demolition, using the form found at <http://www.in.gov/icpr/webfile/formsdiv/44593.pdf> (<http://www.in.gov/icpr/webfile/formsdiv/44593.pdf>).

Anyone submitting a renovation/demolition notification form will be billed a notification fee based upon the amount of friable asbestos containing material to be removed or demolished. Projects that involve the removal of more than 2,600 linear feet of friable asbestos containing materials on pipes, or 1,600 square feet or 400 cubic feet of friable asbestos containing material on other facility components, will be billed a fee of \$150 per project; projects below these amounts will be billed a fee of \$50 per project. All notification remitters will be billed on a quarterly basis.

For more information about IDEM policy regarding asbestos removal and disposal, visit: <http://www.in.gov/idem/4983.htm> (<http://www.in.gov/idem/4983.htm>).

4. With respect to lead-based paint removal: IDEM encourages all efforts to minimize human exposure to lead-based paint chips and dust. IDEM is particularly concerned that young children exposed to lead can suffer from learning disabilities. Although lead-based paint abatement efforts are not mandatory, any abatement that is conducted within housing built before January 1, 1978, or a child-occupied facility is required to comply with all lead-based paint work practice standards, licensing and notification requirements. For more information about lead-based paint removal visit: <http://www.in.gov/isdh/19131.htm> (<http://www.in.gov/isdh/19131.htm>).
5. Ensure that asphalt paving plants are permitted and operate properly. The use of cutback asphalt, or asphalt emulsion containing more than seven percent (7%) oil distillate, is prohibited during the months April through October. See 326 IAC 8-5-2, Asphalt Paving Rule (<http://www.ai.org/legislative/iac/T03260/A00080.PDF>) (<http://www.ai.org/legislative/iac/T03260/A00080.PDF>)).
6. If your project involves the construction of a new source of air emissions or the modification of an existing source of air emissions or air pollution control equipment, it will need to be reviewed by the IDEM Office of Air Quality (OAQ). A registration or permit may be required under 326 IAC 2 (View at: www.ai.org/legislative/iac/t03260/a00020.pdf (<http://www.ai.org/legislative/iac/t03260/a00020.pdf>)). New sources that use or emit hazardous air pollutants may be subject to Section 112 of the Clean Air Act and corresponding state air regulations governing hazardous air pollutants.
7. For more information on air permits visit: <http://www.in.gov/idem/4223.htm> (<http://www.in.gov/idem/4223.htm>), or to initiate the IDEM air permitting process, please contact the Office of Air Quality Permit Reviewer of the Day at (317) 233-0178 or OAMPROD atdem.state.in.us.

LAND QUALITY

In order to maintain compliance with all applicable laws regarding contamination and/or proper waste disposal, IDEM recommends that:

1. If the site is found to contain any areas used to dispose of solid or hazardous waste, you need to contact the Office of Land Quality (OLQ) at 317-308-3103.
2. All solid wastes generated by the project, or removed from the project site, need to be taken to a properly

permitted solid waste processing or disposal facility. For more information, visit <http://www.in.gov/ideM/4998.htm> (<http://www.in.gov/ideM/4998.htm>).

3. If any contaminated soils are discovered during this project, they may be subject to disposal as hazardous waste. Please contact the OLQ at 317-308-3103 to obtain information on proper disposal procedures.
4. If PCBs are found at this site, please contact the Industrial Waste Section of OLQ at 317-308-3103 for information regarding management of any PCB wastes from this site.
5. If there are any asbestos disposal issues related to this site, please contact the Industrial Waste Section of OLQ at 317-308-3103 for information regarding the management of asbestos wastes (Asbestos removal is addressed above, under Air Quality).
6. If the project involves the installation or removal of an underground storage tank, or involves contamination from an underground storage tank, you must contact the IDEM Underground Storage Tank program at 317/308-3039. See: <http://www.in.gov/ideM/4999.htm> (<http://www.in.gov/ideM/4999.htm>).

FINAL REMARKS

Should you need to obtain any environmental permits in association with this proposed project, please be mindful that IC 13-15-8 requires that you notify all adjoining property owners and/or occupants within ten days your submittal of each permit application. However, if you are seeking multiple permits, you can still meet the notification requirement with a single notice if all required permit applications are submitted with the same ten day period.

Should the scope of the proposed project be expanded to the extent that a National Environmental Policy Act Environmental Assessment (EA) or Environmental Impact Statement (EIS) is required, IDEM will actively participate in any early interagency coordination review of the project.

Meanwhile, please note that this letter does not constitute a permit, license, endorsement or any other form of approval on the part of the Indiana Department of Environmental Management regarding any project for which a copy of this letter is used. Also note that is it the responsibility of the project engineer or consultant using this letter to ensure that the most current draft of this document, which is located at <http://www.in.gov/ideM/5284.htm> (<http://www.in.gov/ideM/5284.htm>), is used.

Signature(s) of the Applicant

I acknowledge that the following proposed roadway project will be financed in part, or in whole, by public monies.

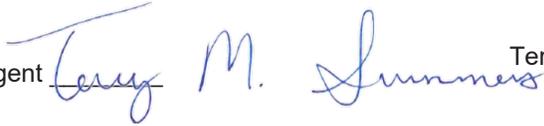
Project Description

The existing bridge (062-69-05860A) is a prestressed concrete continuous box beam with 3 spans. The existing structure is in poor condition due to deterioration, and has a wearing surface with a rating of 3 out of 9, and a superstructure in poor condition with a rating of 4 out of 9 (with advanced deterioration). The preferred alternative is a bridge rehabilitation, specifically on the superstructure, that will increase the service of the bridge to an additional 20-30 years. Right of way has not been finalized; however, it is anticipated that more than 0.5 acre of right of way will be required. Traffic will be maintained through phased construction for local traffic and a detour route for truck traffic. One side of the bridge will be removed and replaced while the other side is open for traffic. The detour route for truck traffic will utilize US 50 and SR 129 and will be approximately 21 miles in length.

With my signature, I do hereby affirm that I have read the letter from the Indiana Department of Environment that appears directly above. In addition, I understand that in order to complete that project in which I am interested, with a minimum of impact to the environment, I must consider all the issues addressed in the aforementioned letter, and further, that I must obtain any required permits.

Date: 9/09/2020

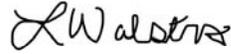
Signature of the INDOT
Project Engineer or Other Responsible Agent



Terry Summers

Date: 5/20/2020

Signature of the
For Hire Consultant



Laney Walstra



Organization and Project Information

Project ID:

Des. ID: 1700185

Project Title: SR 62 over Laughery Creek Bridge Project

Name of Organization: Green 3, LLC.

Requested by: Laney Walstra

Environmental Assessment Report

1. Geological Hazards:

- High liquefaction potential
- 1% Annual Chance Flood Hazard

2. Mineral Resources:

- Bedrock Resource: Low Potential
- Sand and Gravel Resource: Low Potential

3. Active or abandoned mineral resources extraction sites:

- Petroleum Exploration Wells

*All map layers from Indiana Map (maps.indiana.edu)

DISCLAIMER:

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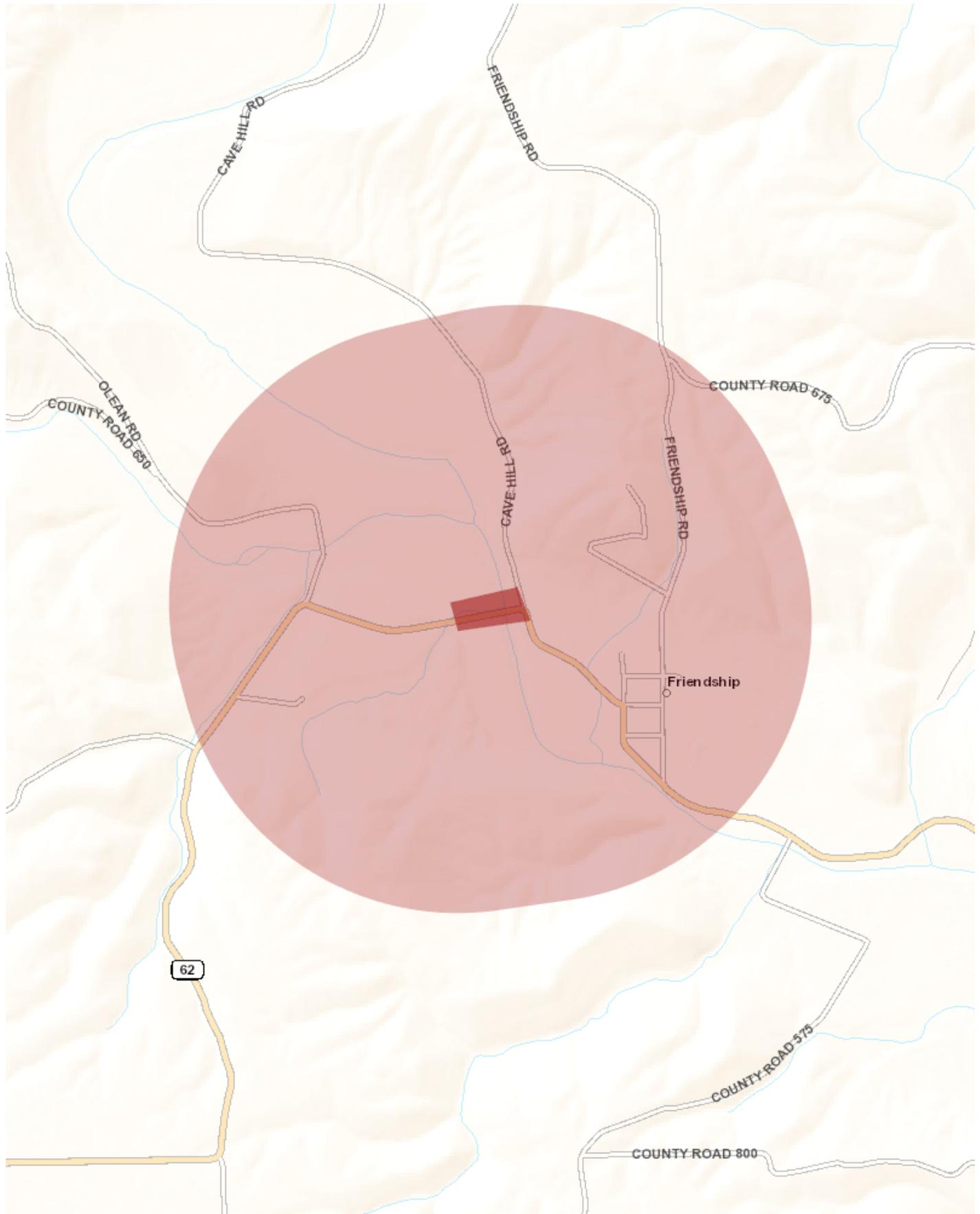
This information was furnished by Indiana Geological Survey

Address: 420 N. Walnut St., Bloomington, IN 47404

Email: IGSEnvir@indiana.edu

Phone: 812 855-7428

Date: March 16, 2020



Metadata:

- https://maps.indiana.edu/metadata/Geology/Petroleum_Wells.html
- https://maps.indiana.edu/metadata/Geology/Seismic_Earthquake_Liquefaction_Potential.html
- https://maps.indiana.edu/metadata/Geology/Industrial_Minerals_Sand_Gravel_Resources.html
- https://maps.indiana.edu/metadata/Hydrology/Floodplains_FIRM.html
- https://maps.indiana.edu/metadata/Geology/Bedrock_Geology.html

Laney Walstra

From: Surveyor <surveyor@ripleycounty.com>
Sent: Monday, March 16, 2020 7:41 AM
To: Laney Walstra
Subject: FW: Early Coordination - SR 62 over Laughery Creek Bridge Project (DES 1700185)

From: Jeff French <jpfrench@frenchsurvey.com>
Sent: Friday, March 13, 2020 5:20 PM
To: laney@green3studio.com
Cc: Surveyor <surveyor@ripleycounty.com>
Subject: RE: Early Coordination - SR 62 over Laughery Creek Bridge Project (DES 1700185)

Hi Laney,

The Ripley County Surveyor's Office does not have any concern or issues with the bridge project.

Thanks,

Jeff

Jeffrey P. French, LS
Ripley County Surveyor
244 North Main Street
Versailles, IN 47042
812.689.5995
jpfrench@frenchsurvey.com

From: Surveyor <surveyor@ripleycounty.com>
Sent: Friday, March 13, 2020 4:44 PM
To: Jeff French <jpfrench@frenchsurvey.com>
Subject: FW: Early Coordination - SR 62 over Laughery Creek Bridge Project (DES 1700185)

From: Laney Walstra <laney@green3studio.com>
Sent: Friday, March 13, 2020 11:17 AM
To: erica.tait@dot.gov; environmentalreview@dnr.in.gov; rclark@indot.in.gov; melanie.h.castillo@hud.gov; robin_mcwilliams@fws.gov; rick.neilson@in.usda.gov; DDye@indot.in.gov; Gregory.A.McKay@usace.army.mil; Surveyor <surveyor@ripleycounty.com>; Mark Horstman <commissionerhorstman@ripleycounty.com>; Ron Decker <councildecker@ripleycounty.com>; Tad Brinson <tbrinson@ripleycounty.com>; rchwy@comcast.net; oldmillfriendshipfleamarket@gmail.com; friendshipfire@seidata.com
Subject: RE: Early Coordination - SR 62 over Laughery Creek Bridge Project (DES 1700185)

All,

March 17, 2020

Laney Walstra
Green 3
1104 Prospect Street
Indianapolis, Indiana 46203

Dear Ms. Walstra:

The proposed project to make bridge improvements along State Road 62 over Laughery Creek in Ripley County, Indiana, (Des No 1700185) as referred to in your letter received March 13, 2020, will not cause a conversion of prime farmland.

The attached packet of information is for your use competing Parts VI and VII of the AD-1006. After completion, the federal funding agency needs to forward one copy to NRCS for our records.

If you need additional information, please contact John Allen at 317-295-5859.

Sincerely,

**RICHARD
NEILSON**

Digitally signed by
RICHARD NEILSON
Date: 2020.03.20
06:49:41 -04'00'

Acting For

JERRY RAYNOR
State Conservationist

Enclosures



FARMLAND CONVERSION IMPACT RATING

PART I (To be completed by Federal Agency)		Date Of Land Evaluation Request			
Name of Project DES1700185 SR62 LaugheryCr		Federal Agency Involved Federal Highway Administration			
Proposed Land Use Right of Way		County and State Ripley County, Indiana			
PART II (To be completed by NRCS)		Date Request Received By NRCS 3/13/2020		Person Completing Form: JRA	
Does the site contain Prime, Unique, Statewide or Local Important Farmland? <i>(If no, the FPPA does not apply - do not complete additional parts of this form)</i>		YES <input checked="" type="checkbox"/> NO <input type="checkbox"/>		Acres Irrigated _____ Average Farm Size 200 ac	
Major Crop(s) Corn		Farmable Land In Govt. Jurisdiction Acres: 249119 % 87		Amount of Farmland As Defined in FPPA Acres: 189989 % 66	
Name of Land Evaluation System Used LESA		Name of State or Local Site Assessment System		Date Land Evaluation Returned by NRCS 3/17/2020	
PART III (To be completed by Federal Agency)		Alternative Site Rating			
		Site A	Site B	Site C	Site D
A. Total Acres To Be Converted Directly					
B. Total Acres To Be Converted Indirectly					
C. Total Acres In Site					
PART IV (To be completed by NRCS) Land Evaluation Information					
A. Total Acres Prime And Unique Farmland		0.30			
B. Total Acres Statewide Important or Local Important Farmland		0.00			
C. Percentage Of Farmland in County Or Local Govt. Unit To Be Converted		<0.001			
D. Percentage Of Farmland in Govt. Jurisdiction With Same Or Higher Relative Value		99			
PART V (To be completed by NRCS) Land Evaluation Criterion Relative Value of Farmland To Be Converted (Scale of 0 to 100 Points)		60			
PART VI (To be completed by Federal Agency) Site Assessment Criteria <i>(Criteria are explained in 7 CFR 658.5 b. For Corridor project use form NRCS-CPA-106)</i>		Maximum Points	Site A	Site B	Site C
1. Area In Non-urban Use		(15)	12		
2. Perimeter In Non-urban Use		(10)	8		
3. Percent Of Site Being Farmed		(20)	0		
4. Protection Provided By State and Local Government		(20)	20		
5. Distance From Urban Built-up Area		(15)	0		
6. Distance To Urban Support Services		(15)	8		
7. Size Of Present Farm Unit Compared To Average		(10)	0		
8. Creation Of Non-farmable Farmland		(10)	0		
9. Availability Of Farm Support Services		(5)	5		
10. On-Farm Investments		(20)	3		
11. Effects Of Conversion On Farm Support Services		(10)	0		
12. Compatibility With Existing Agricultural Use		(10)	0		
TOTAL SITE ASSESSMENT POINTS		160	56	0	0
PART VII (To be completed by Federal Agency)					
Relative Value Of Farmland (From Part V)		100	60	0	0
Total Site Assessment (From Part VI above or local site assessment)		160	56	0	0
TOTAL POINTS (Total of above 2 lines)		260	116	0	0
Site Selected:		Date Of Selection		Was A Local Site Assessment Used? YES <input type="checkbox"/> NO <input checked="" type="checkbox"/>	
Reason For Selection:					
Name of Federal agency representative completing this form: <i>LWalstra</i>					Date: 7/6/2020

(See Instructions on reverse side)

State of Indiana
DEPARTMENT OF NATURAL RESOURCES
Division of Fish and Wildlife
Early Coordination/Environmental Assessment

DNR #: ER-22331

Request Received: March 13, 2020

Requestor: Green 3 LLC
Laney Walstra
1104 Prospect Street
Indianapolis, IN 46203

Project: SR 62 bridge (#062-69-05860A) rehabilitation over Laughery Creek, about 3.91 miles east of SR 129; Des #1700185

County/Site info: Ripley

The Indiana Department of Natural Resources has reviewed the above referenced project per your request. Our agency offers the following comments for your information and in accordance with the National Environmental Policy Act of 1969.

If our agency has regulatory jurisdiction over the project, the recommendations contained in this letter may become requirements of any permit issued. If we do not have permitting authority, all recommendations are voluntary.

Regulatory Assessment: This proposal will require the formal approval of our agency for construction in a floodway, pursuant to the Flood Control Act (IC 14-28-1), unless it qualifies under the INDOT and IDNR Memorandum of Understanding for Maintenance Activity Exemption, dated March 1997. Please include a copy of this letter with the permit application, if required.

Natural Heritage Database: The Natural Heritage Program's data have been checked. To date, no plant or animal species listed as state or federally threatened, endangered, or rare have been reported to occur in the project vicinity.

Fish & Wildlife Comments: Avoid and minimize impacts to fish, wildlife, and botanical resources to the greatest extent possible, and compensate for impacts. The following are recommendations that address potential impacts identified in the proposed project area:

1) Riparian Habitat:

It is unclear if any forested habitat impacts, whether permanent or temporary due to construction access or staging, will occur. We recommend a mitigation plan be developed (and submitted with the permit application, if required) for any unavoidable habitat impacts that will occur. The DNR's Floodway Habitat Mitigation guidelines (and plant lists) can be found online at:

<http://www.in.gov/legislative/iac/20190130-IR-312190041NRA.xml.pdf>.

Impacts to non-wetland forest of one (1) acre or more should be mitigated at a minimum 2:1 ratio. If less than one acre of non-wetland forest is removed in a rural setting, replacement should be at a 1:1 ratio based on area. Impacts to non-wetland forest under one (1) acre in an urban setting should be mitigated by planting five trees, at least 2 inches in diameter-at-breast height (dbh), for each tree which is removed that is 10" dbh or greater (5:1 mitigation based on the number of large trees). Impacts to wetland habitat should be mitigated at the appropriate ratio according to the 1991 INDOT/IDNR/USFWS Memorandum of Understanding.

The mitigation site should be located in the floodway, downstream of the one (1) square mile drainage area of that stream (or another stream within the 8-digit HUC, preferably as close to the impact site as possible) and adjacent to existing forested riparian

State of Indiana
DEPARTMENT OF NATURAL RESOURCES
Division of Fish and Wildlife
Early Coordination/Environmental Assessment

habitat.

2) Wildlife Passage:

The new, replacement, or rehabbed structure, and any bank stabilization under the structure, should not create conditions that are less favorable for wildlife passage under the structure compared to current conditions. Minimize the use of riprap and use alternative erosion protection materials whenever possible. Riprap must not be placed in the active thalweg channel or placed in the streambed in a manner that precludes fish or aquatic organism passage (riprap must not be placed above the existing streambed elevation). Where riprap must be used, we recommend placing only enough riprap to provide stream bank toe protection, such as from the toe of the bank up to the ordinary high water mark (OHWM). The banks above the OHWM should be restored, stabilized, and revegetated using geotextiles and a mixture of grasses, sedges, wildflowers, shrubs, and trees native to the area and specifically for stream bank/floodway stabilization purposes as soon as possible upon completion.

Where hard armoring is needed, wildlife passage can be facilitated by using a smooth-surfaced material instead of riprap, such as articulated concrete block mats, fabric-formed concrete mats or other similar smooth-surfaced materials as these materials will not impair wildlife movement.

Information about bioengineering techniques can be found at <http://www.in.gov/legislative/iac/20120404-IR-312120154NRA.xml.pdf>. Also, the following is a USDA/NRCS document that outlines many different bioengineering and other bank stabilization techniques: <http://directives.sc.egov.usda.gov/17553.wba>.

The additional measures listed below should be implemented to avoid, minimize, or compensate for impacts to fish, wildlife, and botanical resources:

1. Revegetate all bare and disturbed areas in the floodway with a mixture of native grasses, sedges, wildflowers, and also native hardwood trees and shrubs if any woody plants are disturbed during construction as soon as possible upon completion. Do not use any varieties of Tall Fescue or other non-native plants, including prohibited invasive species (see 312 IAC 18-3-25).
2. Minimize and contain within the project limits inchannel disturbance and the clearing of trees and brush.
3. Do not work in the waterway from April 1 through June 30 without the prior written approval of the Division of Fish and Wildlife.
4. Do not cut any trees suitable for Indiana bat or Northern Long-eared bat roosting (greater than 5 inches dbh, living or dead, with loose hanging bark, or with cracks, crevices, or cavities) from April 1 through September 30.
5. Do not construct any temporary runarounds or causeways.
6. Appropriately designed measures for controlling erosion and sediment must be implemented to prevent sediment from entering the stream or leaving the construction site; maintain these measures until construction is complete and all disturbed areas are stabilized.
7. Seed and protect all disturbed streambanks and slopes not protected by other methods that are 3:1 or steeper with erosion control blankets that are heavy-duty, biodegradable, and net free or that use loose-woven / Leno-woven netting to minimize the entrapment and snaring of small-bodied wildlife such as snakes and turtles (follow manufacturer's recommendations for selection and installation); seed and apply mulch on all other disturbed areas.

THIS IS NOT A PERMIT

State of Indiana
DEPARTMENT OF NATURAL RESOURCES
Division of Fish and Wildlife
Early Coordination/Environmental Assessment

Contact Staff:

Christie L. Stanifer, Environ. Coordinator, Fish & Wildlife
Our agency appreciates this opportunity to be of service. Please contact the above staff member at (317) 232-4080 if we can be of further assistance.

Christie L. Stanifer

Date: April 9, 2020

Christie L. Stanifer
Environ. Coordinator
Division of Fish and Wildlife

Christian Radcliff

From: Washburn, Eric CIV <Eric.Washburn@uscg.mil>
Sent: Friday, June 5, 2020 2:17 PM
To: Christian Radcliff
Subject: Ripley County Bridge Project

Good afternoon. We are all set...the CG will not be issuing a permit at that location.

Tks.

Eric

Christian Radcliff

From: McWilliams, Robin <robin_mcwilliams@fws.gov>
Sent: Tuesday, June 9, 2020 2:29 PM
To: Christian Radcliff
Subject: Re: [EXTERNAL] Des 1700185 SR 62 over Laughery Creek Early Coordination Letter

Dear Christian,

Sorry for the delay. I did see this and then it got lost in my emails!

This responds to your recent letter requesting our comments on the aforementioned project.

These comments have been prepared under the authority of the Fish and Wildlife Coordination Act (16 U.S.C. 661 et. seq.) and are consistent with the intent of the National Environmental Policy Act of 1969, the Endangered Species Act of 1973, and the U. S. Fish and Wildlife Service's Mitigation Policy.

The project is within the range of the Indiana bat (*Myotis sodalis*) and northern long-eared bat (*Myotis septentrionalis*) and should follow the new Indiana bat/northern long-eared bat programmatic consultation process, if applicable (i.e. a federal transportation nexus is established). The Service has 14 days after a "Not Likely to Adversely Affect" determination letter is generated to review the project and provide additional comments or request additional information; if you do not receive a response from us within 14 days, we have no additional comments.

Based on a review of the information you provided, the U.S. Fish and Wildlife Service has no other comments on the project as currently proposed. However, should new information arise pertaining to project plans or a revised species list be published, it will be necessary for the Federal agency to reinitiate consultation. Standard recommendations are provided below.

We appreciate the opportunity to comment at this early stage of project planning. If you have any questions about our recommendations, please call (812) 334-4261 x. 207.

Sincerely,
Robin McWilliams Munson

Standard Recommendations:

1. Do not clear trees or understory vegetation outside the construction zone boundaries. **(This restriction is not related to the "tree clearing" restriction for potential Indiana Bat habitat.)**
2. Restrict below low-water work in streams to placement of culverts, piers, pilings and/or footings, shaping of the spill slopes around the bridge abutments, and placement of riprap. Culverts should span the active stream channel, should be either embedded or a 3-sided or open-arch culvert, and be installed where practicable on an essentially flat slope. When an open-bottom culvert or arch is used in a stream, which has a good natural bottom substrate, such as gravel, cobbles and boulders, the existing substrate should be left undisturbed beneath the culvert to provide natural habitat for the aquatic community.
3. Restrict channel work and vegetation clearing to the minimum necessary for installation of the stream crossing structure.

4. Minimize the extent of hard armor (riprap) in bank stabilization by using bioengineering techniques whenever possible. If riprap is utilized for bank stabilization, extend it below low-water elevation to provide aquatic habitat.
5. Implement temporary erosion and sediment control methods within areas of disturbed soil. All disturbed soil areas upon project completion will be vegetated following INDOT's standard specifications.
6. Avoid all work within the inundated part of the stream channel (in perennial streams and larger intermittent streams) during the fish spawning season (April 1 through June 30), except for work within sealed structures such as caissons or cofferdams that were installed prior to the spawning season. No equipment shall be operated below Ordinary High Water Mark during this time unless the machinery is within the caissons or on the cofferdams.
7. Evaluate wildlife crossings under bridge/culverts projects in appropriate situations. Suitable crossings include flat areas below bridge abutments with suitable ground cover, high water shelves in culverts, amphibian tunnels and diversion fencing

Robin McWilliams Munson
Fish and Wildlife Biologist
U.S. Fish and Wildlife Service
620 South Walker Street
Bloomington, IN 46142
812-334-4261

Mon-Tues 8-3:30p
Wed-Thurs 8:30-3p Telework

From: Christian Radcliff <christian@green3studio.com>
Sent: Tuesday, June 9, 2020 1:38 PM
To: McWilliams, Robin <robin_mcwilliams@fws.gov>
Subject: [EXTERNAL] Des 1700185 SR 62 over Laughery Creek Early Coordination Letter

Good afternoon Robin,

I am preparing the CE for the above referenced project. One of my coworkers, Laney Walstra (laney@green3studio.com) should have sent the attached early coordination letter to you for your review and comment. She says that she did not receive a response from you; however, I know that you are usually very diligent to respond. I wanted to give you the opportunity to issue a comment for the project if you wished to do so. If you have no comment on the project that is fine as well! I'm hoping to submit the environmental document on Friday this week, so if you could respond by then it would be greatly appreciated. If you already issued a response to Laney and she somehow has lost it, could you please forward that to me?

Thanks!

Christian Radcliff
Ecologist

christian@green3studio.com



United States Department of the Interior



FISH AND WILDLIFE SERVICE

Indiana Ecological Services Field Office

620 South Walker Street

Bloomington, IN 47403-2121

Phone: (812) 334-4261 Fax: (812) 334-4273

<http://www.fws.gov/midwest/Endangered/section7/s7process/step1.html>

In Reply Refer To:

April 06, 2020

Consultation Code: 03E12000-2020-I-1219

Event Code: 03E12000-2020-E-05483

Project Name: Des 1700185 SR 62 over Laughery Creek

Subject: Concurrence verification letter for the 'Des 1700185 SR 62 over Laughery Creek' project under the revised February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion for Transportation Projects within the Range of the Indiana Bat and Northern Long-eared Bat.

To whom it may concern:

The U.S. Fish and Wildlife Service (Service) has received your request to verify that the **Des 1700185 SR 62 over Laughery Creek** (Proposed Action) may rely on the concurrence provided in the February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion for Transportation Projects within the Range of the Indiana Bat and Northern Long-eared Bat (PBO) to satisfy requirements under Section 7(a)(2) of the Endangered Species Act of 1973 (ESA) (87 Stat. 884, as amended; 16 U.S.C 1531 *et seq.*).

Based on the information you provided (Project Description shown below), you have determined that the Proposed Action is within the scope and adheres to the criteria of the PBO, including the adoption of applicable avoidance and minimization measures, and may affect, but is not likely to adversely affect (NLAA) the endangered Indiana bat (*Myotis sodalis*) and/or the threatened Northern long-eared bat (*Myotis septentrionalis*).

The Service has 14 calendar days to notify the lead Federal action agency or designated non-federal representative if we determine that the Proposed Action does not meet the criteria for a NLAA determination under the PBO. If we do not notify the lead Federal action agency or designated non-federal representative within that timeframe, you may proceed with the Proposed Action under the terms of the NLAA concurrence provided in the PBO. This verification period allows Service Field Offices to apply local knowledge to implementation of the PBO, as we may identify a small subset of actions having impacts that were unanticipated. In such instances, Service Field Offices may request additional information that is necessary to verify inclusion of the proposed action under the PBO.

For Proposed Actions that include bridge/structure removal, replacement, and/or maintenance activities: If your initial bridge/structure assessments failed to detect Indiana bats, but you later detect bats during construction, please submit the Post Assessment Discovery of Bats at Bridge/Structure Form (User Guide Appendix E) to this Service Office. In these instances, potential incidental take of Indiana bats may be exempted provided that the take is reported to the Service.

If the Proposed Action is modified, or new information reveals that it may affect the Indiana bat and/or Northern long-eared bat in a manner or to an extent not considered in the PBO, further review to conclude the requirements of ESA Section 7(a)(2) may be required. If the Proposed Action may affect any other federally-listed or proposed species, and/or any designated critical habitat, additional consultation between the lead Federal action agency and this Service Office is required. If the proposed action has the potential to take bald or golden eagles, additional coordination with the Service under the Bald and Golden Eagle Protection Act may also be required. In either of these circumstances, please contact this Service Office.

Project Description

The following project name and description was collected in IPaC as part of the endangered species review process.

Name

Des 1700185 SR 62 over Laughery Creek

Description

Des 1700185 involves the replacement of the superstructure of the bridge carrying SR 62 over Laughery Creek in Ripley County, Indiana. The existing bridge is a three-span bridge with poor condition of the wearing surface and superstructure. The project proposes to replace the superstructure to extend the usable life of the bridge. The existing pipe culvert east of the existing bridge will be replaced. Suitable habitat in the vicinity of the project area includes stands of trees in the riparian corridor of Laughery Creek and individual trees within 1,000 feet of the stands of trees. Tree removal of approximately 0.20 acres will be necessary to complete the project. Tree removal will occur outside of the active season for bats and within 100 feet of the existing road surface. The dominant trees to be removed are Sugar Maple (*Acer saccharum*) and Sycamore (*Platanus occidentalis*). Temporary lighting may be utilized to complete construction. No permanent lighting is anticipated for this project. Construction is anticipated to occur in Winter 2021 and Spring 2022.

The Red Flag Investigation dated March 20, 2020 indicated that a search into the USFWS bat database did not indicate the presence of bats within 0.5 mile of the project area. An inspection on the bridge and culvert on February 25, 2020 did not indicate the presence of bats on the structures.

Determination Key Result

Based on your answers provided, this project(s) may affect, but is not likely to adversely affect the endangered Indiana bat and/or the threatened Northern long-eared bat, therefore, consultation with the U.S. Fish and Wildlife Service pursuant to Section 7(a)(2) of the Endangered Species Act of 1973 (ESA) (87 Stat. 884, as amended 16 U.S.C. 1531 *et seq.*) is required. However, also based on your answers provided, this project may rely on the concurrence provided in the revised February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion for Transportation Projects within the Range of the Indiana Bat and Northern Long-eared Bat.

Qualification Interview

1. Is the project within the range of the Indiana bat^[1]?

[1] See [Indiana bat species profile](#)

Automatically answered

Yes

2. Is the project within the range of the Northern long-eared bat^[1]?

[1] See [Northern long-eared bat species profile](#)

Automatically answered

Yes

3. Which Federal Agency is the lead for the action?

A) *Federal Highway Administration (FHWA)*

4. Are *all* project activities limited to non-construction^[1] activities only? (examples of non-construction activities include: bridge/abandoned structure assessments, surveys, planning and technical studies, property inspections, and property sales)

[1] Construction refers to activities involving ground disturbance, percussive noise, and/or lighting.

No

5. Does the project include *any* activities that are **greater than** 300 feet from existing road/rail surfaces^[1]?

[1] Road surface is defined as the actively used [e.g. motorized vehicles] driving surface and shoulders [may be pavement, gravel, etc.] and rail surface is defined as the edge of the actively used rail ballast.

No

6. Does the project include *any* activities **within** 0.5 miles of a known Indiana bat and/or NLEB hibernaculum^[1]?

[1] For the purpose of this consultation, a hibernaculum is a site, most often a cave or mine, where bats hibernate during the winter (see suitable habitat), but could also include bridges and structures if bats are found to be hibernating there during the winter.

No

7. Is the project located **within** a karst area?

No

8. Is there *any* suitable^[1] summer habitat for Indiana Bat or NLEB **within** the project action area^[2]? (includes any trees suitable for maternity, roosting, foraging, or travelling habitat)

[1] See the Service's [summer survey guidance](#) for our current definitions of suitable habitat.

[2] The action area is defined as all areas to be affected directly or indirectly by the Federal action and not merely the immediate area involved in the action (50 CFR Section 402.02). Further clarification is provided by the [national consultation FAQs](#).

Yes

9. Will the project remove *any* suitable summer habitat^[1] and/or remove/trim any existing trees **within** suitable summer habitat?

[1] See the Service's [summer survey guidance](#) for our current definitions of suitable habitat.

Yes

10. Will the project clear more than 20 acres of suitable habitat per 5-mile section of road/rail?

No

11. Have presence/probable absence (P/A) summer surveys^{[1][2]} been conducted^{[3][4]} **within** the suitable habitat located within your project action area?

[1] See the Service's [summer survey guidance](#) for our current definitions of suitable habitat.

[2] Presence/probable absence summer surveys conducted within the fall swarming/spring emergence home range of a documented Indiana bat hibernaculum (contact local Service Field Office for appropriate distance from hibernacula) that result in a negative finding requires additional consultation with the local Service Field Office to determine if clearing of forested habitat is appropriate and/or if seasonal clearing restrictions are needed to avoid and minimize potential adverse effects on fall swarming and spring emerging Indiana bats.

[3] For projects within the range of either the Indiana bat or NLEB in which suitable habitat is present, and no bat surveys have been conducted, the transportation agency will assume presence of the appropriate species. This assumption of presence should be based upon the presence of suitable habitat and the capability of bats to occupy it because of their mobility.

[4] Negative presence/probable absence survey results obtained using the [summer survey guidance](#) are valid for a minimum of two years from the completion of the survey unless new information (e.g., other nearby surveys) suggest otherwise.

No

12. Does the project include activities **within documented Indiana bat habitat**^{[1][2]}?

[1] Documented roosting or foraging habitat – for the purposes of this consultation, we are considering documented habitat as that where Indiana bats and/or NLEB have actually been captured and tracked using (1) radio telemetry to roosts; (2) radio telemetry triangulation/triangulation to estimate foraging areas; or (3) foraging areas with repeated use documented using acoustics. Documented roosting habitat is also considered as suitable summer habitat within 0.25 miles of documented roosts.)

[2] For the purposes of this key, we are considering documented corridors as that where Indiana bats and/or NLEB have actually been captured and tracked to using (1) radio telemetry; or (2) treed corridors located directly between documented roosting and foraging habitat.

No

13. Will the removal or trimming of habitat or trees occur **within** suitable but **undocumented Indiana bat** roosting/foraging habitat or travel corridors?

Yes

14. What time of year will the removal or trimming of habitat or trees **within** suitable but **undocumented Indiana bat** roosting/foraging habitat or travel corridors occur^[1]?

[1] Coordinate with the local Service Field Office for appropriate dates.

B) During the inactive season

15. Does the project include activities **within documented NLEB habitat**^{[1][2]}?

[1] Documented roosting or foraging habitat – for the purposes of this consultation, we are considering documented habitat as that where Indiana bats and/or NLEB have actually been captured and tracked using (1) radio telemetry to roosts; (2) radio telemetry biangulation/triangulation to estimate foraging areas; or (3) foraging areas with repeated use documented using acoustics. Documented roosting habitat is also considered as suitable summer habitat within 0.25 miles of documented roosts.)

[2] For the purposes of this key, we are considering documented corridors as that where Indiana bats and/or NLEB have actually been captured and tracked to using (1) radio telemetry; or (2) treed corridors located directly between documented roosting and foraging habitat.

No

16. Will the removal or trimming of habitat or trees occur **within** suitable but **undocumented NLEB** roosting/foraging habitat or travel corridors?

Yes

17. What time of year will the removal or trimming of habitat or trees **within** suitable but **undocumented NLEB** roosting/foraging habitat or travel corridors occur?

B) During the inactive season

18. Will *any* tree trimming or removal occur **within** 100 feet of existing road/rail surfaces?

Yes

19. Will *any* tree trimming or removal occur **between** 100-300 feet of existing road/rail surfaces?

No

20. Are *all* trees that are being removed clearly demarcated?

Yes

21. Will the removal of habitat or the removal/trimming of trees include installing new or replacing existing **permanent** lighting?

No

22. Does the project include wetland or stream protection activities associated with compensatory wetland mitigation?

No

23. Does the project include slash pile burning?

No

24. Does the project include *any* bridge removal, replacement, and/or maintenance activities (e.g., any bridge repair, retrofit, maintenance, and/or rehabilitation work)?

Yes

25. Is there *any* suitable habitat^[1] for Indiana bat or NLEB **within** 1,000 feet of the bridge? (includes any trees suitable for maternity, roosting, foraging, or travelling habitat)

[1] See the Service's current [summer survey guidance](#) for our current definitions of suitable habitat.

Yes

26. Has a bridge assessment^[1] been conducted **within** the last 24 months^[2] to determine if the bridge is being used by bats?

[1] See [User Guide Appendix D](#) for bridge/structure assessment guidance

[2] Assessments must be completed no more than 2 years prior to conducting any work below the deck surface on all bridges that meet the physical characteristics described in the Programmatic Consultation, regardless of whether assessments have been conducted in the past. Due to the transitory nature of bat use, a negative result in one year does not guarantee that bats will not use that bridge/structure in subsequent years.

Yes

SUBMITTED DOCUMENTS

- *Des 1700185 Bat Inspection.pdf* <https://ecos.fws.gov/ipac/project/RDR7GBPJVJVEWRPWBCMBMSEJZJE/projectDocuments/21134826>

27. Did the bridge assessment detect *any* signs of Indiana bats and/or NLEBs roosting in/under the bridge (bats, guano, etc.)^[1]?

[1] If bridge assessment detects signs of *any* species of bats, coordination with the local FWS office is needed to identify potential threatened or endangered bat species. Additional studies may be undertaken to try to identify which bat species may be utilizing the bridge prior to allowing *any* work to proceed.

Note: There is a small chance bridge assessments for bat occupancy do not detect bats. Should a small number of bats be observed roosting on a bridge just prior to or during construction, such that take is likely to occur or does occur in the form of harassment, injury or death, the PBO requires the action agency to report the take. Report all unanticipated take within 2 working days of the incident to the USFWS. Construction activities may continue without delay provided the take is reported to the USFWS and is limited to 5 bats per project.

No

28. Will the bridge removal, replacement, and/or maintenance activities include installing new or replacing existing **permanent** lighting?

No

29. Does the project include the removal, replacement, and/or maintenance of *any* structure other than a bridge? (e.g., rest areas, offices, sheds, outbuildings, barns, parking garages, etc.)

No

30. Will the project involve the use of **temporary** lighting *during* the active season?

Yes

31. Is there *any* suitable habitat **within** 1,000 feet of the location(s) where **temporary** lighting will be used?

Yes

32. Will the project install new or replace existing **permanent** lighting?

No

33. Does the project include percussives or other activities (**not including tree removal/trimming or bridge/structure work**) that will increase noise levels above existing traffic/background levels?

No

34. Are *all* project activities that are **not associated with** habitat removal, tree removal/trimming, bridge and/or structure activities, temporary or permanent lighting, or use of percussives, limited to actions that DO NOT cause any additional stressors to the bat species?

Examples: lining roadways, unlighted signage , rail road crossing signals, signal lighting, and minor road repair such as asphalt fill of potholes, etc.

Yes

35. Will the project raise the road profile **above the tree canopy**?

No

36. Are the project activities that are not associated with habitat removal, tree removal/trimming, bridge and/or structure activities, temporary or permanent lighting, or use of percussives consistent with a No Effect determination in this key?

Automatically answered

Yes, other project activities are limited to actions that DO NOT cause any additional stressors to the bat species as described in the BA/BO

37. Is the habitat removal portion of this project consistent with a Not Likely to Adversely Affect determination in this key?

Automatically answered

Yes, because the tree removal/trimming that occurs outside of the Indiana bat's active season occurs greater than 0.5 miles from the nearest hibernaculum, is less than 100 feet from the existing road/rail surface, includes clear demarcation of the trees that are to be removed, and does not alter documented roosts and/or surrounding summer habitat within 0.25 miles of a documented roost.

38. Is the habitat removal portion of this project consistent with a Not Likely to Adversely Affect determination in this key?

Automatically answered

Yes, because the tree removal/trimming that occurs outside of the NLEB's active season occurs greater than 0.5 miles from the nearest hibernaculum, is less than 100 feet from the existing road/rail surface, includes clear demarcation of the trees that are to be removed, and does not alter documented roosts and/or surrounding summer habitat within 0.25 miles of a documented roost.

39. Is the bridge removal, replacement, or maintenance activities portion of this project consistent with a No Effect determination in this key?

Automatically answered

Yes, because the bridge has been assessed using the criteria documented in the BA and no signs of bats were detected

40. General AMM 1

Will the project ensure *all* operators, employees, and contractors working in areas of known or presumed bat habitat are aware of *all* FHWA/FRA/FTA (Transportation Agencies) environmental commitments, including all applicable Avoidance and Minimization Measures?

Yes

41. Tree Removal AMM 1

Can *all* phases/aspects of the project (e.g., temporary work areas, alignments) be modified, to the extent practicable, to avoid tree removal^[1] in excess of what is required to implement the project safely?

Note: Tree Removal AMM 1 is a minimization measure, the full implementation of which may not always be practicable. Projects may still be NLAA as long as Tree Removal AMMs 2, 3, and 4 are implemented and LAA as long as Tree Removal AMMs 3, 5, 6, and 7 are implemented.

[1] The word “trees” as used in the AMMs refers to trees that are suitable habitat for each species within their range. See the USFWS’ current summer survey guidance for our latest definitions of suitable habitat.

Yes

42. Tree Removal AMM 3

Can tree removal be limited to that specified in project plans and ensure that contractors understand clearing limits and how they are marked in the field (e.g., install bright colored flagging/fencing prior to any tree clearing to ensure contractors stay within clearing limits)?

Yes

43. Tree Removal AMM 4

Can the project avoid cutting down/removal of *all* (1) **documented**^[1] Indiana bat or NLEB roosts^[2] (that are still suitable for roosting), (2) trees **within** 0.25 miles of roosts, and (3) documented foraging habitat any time of year?

[1] The word documented means habitat where bats have actually been captured and/or tracked.

[2] Documented roosting or foraging habitat – for the purposes of this consultation, we are considering documented habitat as that where Indiana bats and/or NLEB have actually been captured and tracked using (1) radio telemetry to roosts; (2) radio telemetry biangulation/triangulation to estimate foraging areas; or (3) foraging areas with repeated use documented using acoustics. Documented roosting habitat is also considered as suitable summer habitat within 0.25 miles of documented roosts.)

Yes

44. Lighting AMM 1

Will *all* **temporary** lighting be directed away from suitable habitat during the active season?

Yes

Project Questionnaire

1. Have you made a No Effect determination for *all* other species indicated on the FWS IPaC generated species list?

Yes

2. Have you made a May Affect determination for *any* other species on the FWS IPaC generated species list?

No

3. How many acres^[1] of trees are proposed for removal between 0-100 feet of the existing road/rail surface?

[1] If described as number of trees, multiply by 0.09 to convert to acreage and enter that number.

0.2

4. Please describe the proposed bridge work:

The existing bridge superstructure will be replaced and the culvert east of the existing bridge will be replaced. The entire superstructure will be removed and replaced with a wider superstructure. The piers will be widened to accommodate the new superstructure.

5. Please state the timing of all proposed bridge work:

Spring and Summer 2022.

6. Please enter the date of the bridge assessment:

February 25, 2020

Avoidance And Minimization Measures (AMMs)

This determination key result includes the commitment to implement the following Avoidance and Minimization Measures (AMMs):

GENERAL AMM 1

Ensure all operators, employees, and contractors working in areas of known or presumed bat habitat are aware of all FHWA/FRA/FTA (Transportation Agencies) environmental commitments, including all applicable AMMs.

LIGHTING AMM 1

Direct temporary lighting away from suitable habitat during the active season.

TREE REMOVAL AMM 1

Modify all phases/aspects of the project (e.g., temporary work areas, alignments) to avoid tree removal.

TREE REMOVAL AMM 2

Apply time of year restrictions for tree removal when bats are not likely to be present, or limit tree removal to 10 or fewer trees per project at any time of year within 100 feet of existing road/rail surface and **outside of documented** roosting/foraging habitat or travel corridors; visual emergence survey must be conducted with no bats observed.

TREE REMOVAL AMM 3

Ensure tree removal is limited to that specified in project plans and ensure that contractors understand clearing limits and how they are marked in the field (e.g., install bright colored flagging/fencing prior to any tree clearing to ensure contractors stay within clearing limits).

TREE REMOVAL AMM 4

Do not remove **documented** Indiana bat or NLEB roosts that are still suitable for roosting, or trees within 0.25 miles of roosts, or **documented** foraging habitat any time of year.

Determination Key Description: FHWA, FRA, FTA Programmatic Consultation For Transportation Projects Affecting NLEB Or Indiana Bat

This key was last updated in IPaC on December 02, 2019. Keys are subject to periodic revision.

This decision key is intended for projects/activities funded or authorized by the Federal Highway Administration (FHWA), Federal Railroad Administration (FRA), and/or Federal Transit Administration (FTA), which may require consultation with the U.S. Fish and Wildlife Service (Service) under Section 7 of the Endangered Species Act (ESA) for the endangered **Indiana bat** (*Myotis sodalis*) and the threatened **Northern long-eared bat** (NLEB) (*Myotis septentrionalis*).

This decision key should only be used to verify project applicability with the Service's [February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion for Transportation Projects](#). The programmatic biological opinion covers limited transportation activities that may affect either bat species, and addresses situations that are both likely and not likely to adversely affect either bat species. This decision key will assist in identifying the effect of a specific project/activity and applicability of the programmatic consultation. The programmatic biological opinion is not intended to cover all types of transportation actions. Activities outside the scope of the programmatic biological opinion, or that may affect ESA-listed species other than the Indiana bat or NLEB, or any designated critical habitat, may require additional ESA Section 7 consultation.



United States Department of the Interior



FISH AND WILDLIFE SERVICE

Indiana Ecological Services Field Office

620 South Walker Street

Bloomington, IN 47403-2121

Phone: (812) 334-4261 Fax: (812) 334-4273

<http://www.fws.gov/midwest/Endangered/section7/s7process/step1.html>

In Reply Refer To:

April 06, 2020

Consultation Code: 03E12000-2020-SLI-1219

Event Code: 03E12000-2020-E-05473

Project Name: Des 1700185 SR 62 over Laughery Creek

Subject: List of threatened and endangered species that may occur in your proposed project location, and/or may be affected by your proposed project

To Whom It May Concern:

The attached species list identifies any federally threatened, endangered, proposed and candidate species that may occur within the boundary of your proposed project or may be affected by your proposed project. The list also includes designated critical habitat if present within your proposed project area or affected by your project. This list is provided to you as the initial step of the consultation process required under section 7(c) of the Endangered Species Act, also referred to as Section 7 Consultation.

Section 7 of the Endangered Species Act of 1973 requires that actions authorized, funded, or carried out by Federal agencies not jeopardize federally threatened or endangered species or adversely modify designated critical habitat. To fulfill this mandate, Federal agencies (or their designated non-federal representative) must consult with the Service if they determine their project “may affect” listed species or critical habitat.

Under 50 CFR 402.12(e) (the regulations that implement Section 7 of the Endangered Species Act) the accuracy of this species list should be verified after 90 days. This verification can be completed formally or informally. You may verify the list by visiting the ECOS-IPaC website <http://ecos.fws.gov/ipac/> at regular intervals during project planning and implementation and completing the same process you used to receive the attached list. As an alternative, you may contact this Ecological Services Field Office for updates.

Please use the species list provided and visit the U.S. Fish and Wildlife Service's Region 3 Section 7 Technical Assistance website at - <http://www.fws.gov/midwest/endangered/section7/s7process/index.html>. This website contains step-by-step instructions which will help you

determine if your project will have an adverse effect on listed species and will help lead you through the Section 7 process.

For all **wind energy projects** and **projects that include installing towers that use guy wires or are over 200 feet in height**, please contact this field office directly for assistance, even if no federally listed plants, animals or critical habitat are present within your proposed project or may be affected by your proposed project.

Although no longer protected under the Endangered Species Act, be aware that bald eagles are protected under the Bald and Golden Eagle Protection Act (16 U.S.C. 668 *et seq.*) and Migratory Bird Treaty Act (16 U.S.C. 703 *et seq.*), as are golden eagles. Projects affecting these species may require measures to avoid harming eagles or may require a permit. If your project is near an eagle nest or winter roost area, see our Eagle Permits website at <http://www.fws.gov/midwest/midwestbird/EaglePermits/index.html> to help you determine if you can avoid impacting eagles or if a permit may be necessary.

We appreciate your concern for threatened and endangered species. Please include the Consultation Tracking Number in the header of this letter with any request for consultation or correspondence about your project that you submit to our office.

Attachment(s):

- Official Species List

Official Species List

This list is provided pursuant to Section 7 of the Endangered Species Act, and fulfills the requirement for Federal agencies to "request of the Secretary of the Interior information whether any species which is listed or proposed to be listed may be present in the area of a proposed action".

This species list is provided by:

Indiana Ecological Services Field Office

620 South Walker Street

Bloomington, IN 47403-2121

(812) 334-4261

Project Summary

Consultation Code: 03E12000-2020-SLI-1219

Event Code: 03E12000-2020-E-05473

Project Name: Des 1700185 SR 62 over Laughery Creek

Project Type: TRANSPORTATION

Project Description: Des 1700185 involves the replacement of the superstructure of the bridge carrying SR 62 over Laughery Creek in Ripley County, Indiana. The existing bridge is a three-span bridge with poor condition of the wearing surface and superstructure. The project proposes to replace the superstructure to extend the usable life of the bridge. The existing pipe culvert east of the existing bridge will be replaced. Suitable habitat in the vicinity of the project area includes stands of trees in the riparian corridor of Laughery Creek and individual trees within 1,000 feet of the stands of trees. Tree removal of approximately 0.20 acres will be necessary to complete the project. Tree removal will occur outside of the active season for bats and within 100 feet of the existing road surface. The dominant trees to be removed are Sugar Maple (*Acer saccharum*) and Sycamore (*Platanus occidentalis*). Temporary lighting may be utilized to complete construction. No permanent lighting is anticipated for this project. Construction is anticipated to occur in Winter 2021 and Spring 2022.

The Red Flag Investigation dated March 20, 2020 indicated that a search into the USFWS bat database did not indicate the presence of bats within 0.5 mile of the project area. An inspection on the bridge and culvert on February 25, 2020 did not indicate the presence of bats on the structures.

Project Location:

Approximate location of the project can be viewed in Google Maps: <https://www.google.com/maps/place/38.97238877061976N85.1532819698543W>



Counties: Ripley, IN

Endangered Species Act Species

There is a total of 2 threatened, endangered, or candidate species on this species list.

Species on this list should be considered in an effects analysis for your project and could include species that exist in another geographic area. For example, certain fish may appear on the species list because a project could affect downstream species. Note that 1 of these species should be considered only under certain conditions.

IPaC does not display listed species or critical habitats under the sole jurisdiction of NOAA Fisheries¹, as USFWS does not have the authority to speak on behalf of NOAA and the Department of Commerce.

See the "Critical habitats" section below for those critical habitats that lie wholly or partially within your project area under this office's jurisdiction. Please contact the designated FWS office if you have questions.

-
1. [NOAA Fisheries](#), also known as the National Marine Fisheries Service (NMFS), is an office of the National Oceanic and Atmospheric Administration within the Department of Commerce.

Mammals

NAME	STATUS
Indiana Bat <i>Myotis sodalis</i> There is final critical habitat for this species. Your location is outside the critical habitat. Species profile: https://ecos.fws.gov/ecp/species/5949 Species survey guidelines: https://ecos.fws.gov/ipac/guideline/survey/population/1/office/31440.pdf	Endangered
Northern Long-eared Bat <i>Myotis septentrionalis</i> No critical habitat has been designated for this species. This species only needs to be considered under the following conditions: <ul style="list-style-type: none"> ▪ Incidental take of the NLEB is not prohibited here. Federal agencies may consult using the 4(d) rule streamlined process. Transportation projects may consult using the programmatic process. See www.fws.gov/midwest/endangered/mammals/nleb/index.html Species profile: https://ecos.fws.gov/ecp/species/9045	Threatened

Critical habitats

THERE ARE NO CRITICAL HABITATS WITHIN YOUR PROJECT AREA UNDER THIS OFFICE'S JURISDICTION.

Des 1700185 CE-2

Appendix D

Section 106 of the NHPA

Minor Projects PA Project Assessment Form– Category B Projects with Archaeology Work

Date: 7/2/2020

Project Designation Number: 1700185

Route Number: State Road (SR) 62

Project Description: SR 62 over Laughery Creek Bridge Rehabilitation

This project involves the rehabilitation of the bridge carrying SR 62 over Laughery Creek, approximately 3.91 miles east of SR 129 in Ripley County, Indiana. The existing bridge (062-69-05860A; NBI 22530), c. 1969, is a prestressed concrete continuous box beam with 3 spans. The need for this project is due to the existing structure's poor condition. The bridge superstructure was rated 4 out of 9 in the December 12, 2019 bridge inspection report. The bridge deck and wearing surface are showing signs of advanced deterioration with cracking and spalling present throughout the superstructure. This is allowing water to leak through the beams of the bridge and to cause additional damage to the superstructure. The purpose of this project is to improve the rating of the bridge superstructure to at least an 8 out of 9.

It is proposed that the superstructure of the bridge that is approximately 30 feet 2 inches in width be removed and replaced, including the bridge deck, beams, and guardrails. The new bridge superstructure will consist of semi integral end bents, concrete bridge railing, and guardrail transitions and will be approximately 37 feet 6 inches in width. Riprap will be placed in the channel for bank stabilization and the piers will be widened to accommodate the additional bridge deck width. Approximately 0.891 acre of new right-of-way will be acquired for this project.

Feature crossed (if applicable): Laughery Creek

Township: Brown Township

City/County: Ripley County

Information reviewed (please check all that apply):

- General project location map USGS map Aerial photograph Interim Report
- Written description of project area General project area photos Soil survey data
- Previously completed historic property reports Previously completed archaeology reports
- Bridge Inspection Information SHAARD SHAARD GIS Streetview Imagery

Other (please specify): Bridge Inspection Application System (BIAS); Indiana Historic Bridge Inventory; Indiana State Historic Architectural and Archaeological Research Database (SHAARD); Indiana Historic Buildings, Bridges, and Cemeteries Map (IHBBCM) website; *Ripley County Interim Report*; Arc Map GIS; Ripley County GIS (accessed via <https://ripleyin.wthgis.com/>); online street-view imagery; MPPA application (including maps and photographs) sent by Green3 dated June 10th, 2020 and on file at INDOT-CRO.

Jackson, Christopher

2020 A Phase Ia Archaeological Records Check and Reconnaissance Survey for the Proposed Rehabilitation of the SR 62 Bridge over Laughery Creek (Des 1700185), approximately 3.91 miles east of SR 129, Brown Township, Ripley County, Indiana. Green 3, Indianapolis.

Kelley, Lisa J.

2018 A Phase Ia Archaeological Reconnaissance for the Proposed Replacement of the Small Structure Carrying State Road 62 over an Unnamed Tributary of Laughery Creek in Ripley County, Indiana (INDOT Des. No. 1600684). Publication Series 18-428, Cultural Resource Analysts, Evansville.

Results of the Records Review for Above-Ground Resources:

With regard to above-ground resources, an INDOT Cultural Resources historian who meets the Secretary of the Interior's Professional Qualification Standards as per 36 CFR Part 61 performed a desktop review, checking the Indiana Register of Historic Sites and Structures (State Register) and National Register of Historic Places (National Register) lists for Ripley County. No listed resources are located within 0.25 mile of the project area, a distance that serves as an adequate area of potential effects.

The Indiana Historic Sites and Structures Inventory (IHSSI) and National Register information for Ripley County is available in the Indiana State Historic Architectural and Archaeological Research Database (SHAARD) and the Indiana Historic Buildings, Bridges, and Cemeteries Map (IHBBCM). The *Ripley County Interim Report* (1986; Brown Township) of the Indiana Historic Sites and Structures Inventory (IHSSI) was also consulted.

According to the IHSSI rating system, generally properties rated "contributing" do not possess the level of historical or architectural significance necessary to be considered individually National Register-eligible, although they would contribute to a historic district. If they retain material integrity, properties rated "notable" might possess the necessary level of significance after further research. Properties rated "outstanding" usually possess the necessary level of significance to be considered National Register eligible if they retain material integrity.

An INDOT-CRO historian reviewed the SHAARD online map and checked it against the interim report hard-copy maps. Two (2) resources rated higher than "contributing" are located within 0.25 mile of the project area.

- IHSSI# 137-140-50006, Paul House, 5859 SR 62,1840, Federal, "Notable"
- IHSSI# 137-140-50005, Akers Farm, 7027 S. Friendship Rd., c. 1850, Greek Revival, "Notable"

The INDOT-CRO historian reviewed structures adjacent to the project area utilizing online aerial, street-view photography, and the Ripley County GIS website. The project area is located on a two (2)-lane highway in a rural small-town setting; the adjacent building stock primarily ranges from early to late-twentieth century residential and commercial buildings. The above-listed resources, with survey ratings higher than "contributing," are within 0.25 mile of the project area. The Paul House (IHSSI# 137-140-50006) is located approximately 0.15 mile southeast and the Akers Farm (IHSSI# 137-140-50005) is located approximately 0.22 mile east of the project area. The area is heavily wooded and road alignment in is curved. The above noted resources are outside the viewshed of the project area. Due to the distance from the project area, the alignment of the road, and vegetation in the area, the resources are not considered adjacent.

The most-recent inspection report (S. Hurst; 12/12/2019), referenced via the Bridge Inspection Application System (BIAS), was consulted to review the bridge. The subject structure (Bridge No. 062-69-05860A /NBI No. 22530) carries SR 62 over Laughery Creek and is a 3-span pre-stressed concrete box beam bridge constructed in 1969 and reconstructed in 1982. During the survey of bridges for the

Indiana Historic Bridge Inventory, structures built after 1965 were not included in data-gathering; therefore, since the bridge was reconstructed in 1982, it was not evaluated as part of the inventory.

The Program Comment Issued for Streamlining Section 106 Review for Actions Affecting Post-1945 Concrete and Steel Bridges went into effect in 2012 and exempts common concrete and/or steel bridges and culverts built after 1945 from requirements under Section 106 of the National Historic Preservation Act. The *Program Comment* applies for Bridge # 062-69-05860A because it has not been previously listed in or determined eligible for listing in the National Register of Historic Places and it is not located in or adjacent to a historic district (Section IV.A of the *Program Comment*). As an example of a pre-stressed concrete box beam bridge structure, the bridge is also not one of the types exempted from the *Program Comment* (arch bridges, truss bridges, bridges with movable spans, suspension bridges, cable-stayed bridges, or covered bridges [Section IV.B]). Additionally, this bridge has not been identified as having exceptional significance for association with a person or event, being a very early or particularly important example of its type in the state or the nation, having distinctive engineering or architectural features that depart from standard designs, or displaying other elements that were engineered to respond to a unique environmental context (Section IV.C). This bridge also has not been identified as having some exceptional quality. Based on consultation between FHWA, INDOT, SHPO and interested parties, no bridges with exceptional significance were identified in Indiana (Section IV.C). Because the above criteria from the *Program Comment* have been met, no individual consideration under Section 106 is required for Bridge # 062-69-05860A.

Based on the available information, as summarized above, no above-ground concerns exist.

Archaeology Report Author/Date:

Christopher Jackson/June 24, 2020

Summary of Archaeology Investigation Results:

An archaeological records check and Phase Ia reconnaissance survey were conducted by Green 3 (Jackson 2020). The records check found that the western end of the project area had been previously examined for archaeological resources (Kelley 2018), but that that no archaeological sites have been previously recorded within or adjacent to the current survey area. A 2.4-acre survey area was examined through the excavation of 11 shovel probes and visual inspection of disturbed areas. Portions of the survey area were found to be severely eroded or disturbed during shovel probing, and no cultural materials were present in any of the shovel probes. One archaeological site, consisting of two sections of dry-laid cut limestone wall (one section mostly collapsed), was newly recorded as a result of the survey. This site was determined to be ineligible for the National Register of Historic Places and no further work was recommended. The report was reviewed by INDOT Cultural Resources personnel who meet the Secretary of the Interior's Professional Qualification Standards as per 36 CFR Part 61. It is our opinion that the report is acceptable, and we concur with the evaluations and recommendations made by Green 3 (Jackson 2020). Therefore, there are no archaeological concerns.

Does the project appear to fall under the Minor Projects PA? yes no

If yes, please specify category and number (applicable conditions are highlighted**):**

A-9. Installation, repair, or replacement of erosion control measures along roadways, waterways and bridge piers within previously disturbed soils.

B-12. Replacement, widening, or raising the elevation of the superstructure on existing bridges, and bridge replacement projects (when both the superstructure and substructure are removed), under the following conditions [**BOTH Condition A, which pertains to Archaeological Resources, and Condition B, which pertains to Above-Ground Resources, must be satisfied**]:

Condition A (Archaeological Resources)

One of the two conditions listed below must be met (*EITHER Condition i or Condition ii must be satisfied*):

- i. Work occurs in previously disturbed soils; *OR*
- ii. Work occurs in undisturbed soils and an archaeological investigation conducted by the applicant and reviewed by INDOT Cultural Resources Office determines that no National Register-listed or potentially National Register-eligible archaeological resources are present within the project area. If the archaeological investigation locates National Register-listed or potentially National Register-eligible archaeological resources, then full Section 106 review will be required. Copies of any archaeological reports prepared for the project will be provided to the DHPA and any archaeological site form information will be entered directly into the SHAARD by the applicant. The archaeological reports will also be available for viewing (by Tribes only) on INSCOPE.

Condition B (Above-Ground Resources)

The conditions listed below must be met (*BOTH Condition i and Condition ii must be satisfied*)

- i. Work does not occur adjacent to or within a National Register-listed or National Register-eligible district or individual above-ground resource; *AND*
- ii. With regard to the subject bridge, at least one of the conditions listed below is satisfied (*AT LEAST one of the conditions a, b or c, must be fulfilled*):
 - a. The latest Historic Bridge Inventory identified the bridge as non-historic (see <https://www.in.gov/indot/2531.htm>);
 - b. The bridge was built after 1945, and is a common type as defined in Section V. of the Program Comment Issued for Streamlining Section 106 Review for Actions Affecting Post-1945 Concrete and Steel Bridges issued by the Advisory Council on Historic Preservation on November 2, 2012 for so long as that Program Comment remains in effect *AND* the considerations listed in Section IV of the Program Comment do not apply;
 - c. The bridge is part of the Interstate system and was determined not eligible for the National Register under the Section 106 Exemption Regarding Effects to the Interstate Highway System adopted by the Advisory Council on Historic Preservation on March 10, 2005, for so long as that Exemption remains in effect.

If no, please explain:

Additional comments: If any archaeological artifacts or human remains are uncovered during construction, demolition, or earthmoving activities, construction in the immediate area of the find will be stopped and the INDOT Cultural Resources Office and the Division of Historic Preservation and Archaeology will be notified immediately.

INDOT Cultural Resources staff reviewer(s): Clint Kelly and Matt Coon

****Be sure to attach this form to the National Environmental Policy Act documentation for this project. Also, the NEPA documentation shall reference and include the description of the specific stipulation in the PA that qualifies the project as exempt from further Section 106 review.*