

**FHWA-Indiana Environmental Document**  
**CATEGORICAL EXCLUSION / ENVIRONMENTAL ASSESSMENT FORM**  
**GENERAL PROJECT INFORMATION**

<b>Road No./County:</b>	State Road (SR) 62 and Michigan Road (Rd.)/Jefferson County
<b>Designation Number(s):</b>	1902023
<b>Project Description/Termini:</b>	Access Management Project/580 feet (ft.) along SR 62 and 694 ft. along Michigan Rd.

	<b>Categorical Exclusion, Level 2</b> – Required Signatories: INDOT DE and/or INDOT ESD
	<b>Categorical Exclusion, Level 3</b> – Required Signatories: INDOT ESD
<b>X</b>	<b>Categorical Exclusion, Level 4</b> – Required Signatories: INDOT ESD and FHWA
	<b>Environmental Assessment (EA)</b> – Required Signatories: INDOT ESD and FHWA
	<b>Additional Investigation (AI)</b> – The proposed action included a design change from the original approved environmental document. Required Signatories must include the appropriate environmental approval authority

**Approval**

_____	_____
INDOT DE Signature and Date	INDOT ESD Signature and Date
_____	
FHWA Signature and Date	

**Release for Public Involvement**

_____	_____
INDOT DE Initials and Date	ATR 10/20/2022 INDOT ESD Initials and Date

**Certification of Public Involvement**

\_\_\_\_\_

INDOT Consultant Services Signature and Date

**INDOT DE/ESD Reviewer Signature and Date:** \_\_\_\_\_

**Name and Organization of CE/EA Preparer:** \_\_\_\_\_

Susan Castle, Metric Environmental, LLC (Metric)

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County Jefferson Route SR 62 and Michigan Rd. Des. No. 1902023

Note: Refer to the most current INDOT CE Manual, guidance language, and other ESD resources for further guidance regarding any section of this form.

### Part I – Public Involvement

Every Federal action requires some level of public involvement, providing for early and continuous opportunities throughout the project development process. **The level of public involvement should be commensurate with the proposed action.**

Does the project have a historic bridge processed under the Historic Bridges PA*?	Yes <input type="checkbox"/>	No <input checked="" type="checkbox"/>
If No, then: Opportunity for a Public Hearing Required?	<input checked="" type="checkbox"/>	<input type="checkbox"/>

\*A public hearing is required for all historic bridges processed under the Historic Bridges Programmatic Agreement between INDOT, FHWA, SHPO, and the ACHP.

Discuss what public involvement activities (legal notices, letters to affected property owners and residents (i.e. notice of entry), meetings, special purpose meetings, newspaper articles, etc.) have occurred for this project.

Notice of Entry letters were not mailed to property owners near the project area, notifying them about the project, because all activities will take place within existing right-of-way (ROW). Since no ROW will be required, all survey activities were conducted within existing ROW; therefore, a Notice of Entry/Survey letter was not sent to adjacent property owners.

The project does not meet any of the conditions set by the current *Indiana Department of Transportation (INDOT) Project Development Public Involvement Procedures Manual* that require formal public involvement. However, based on the proposed alternative eliminating left turns and not including an alternative access such as a U-turn or J-turn, the Federal Highway Administration (FHWA) and Indiana Department of Transportation Environmental Services Division (INDOT ESD) agree that public involvement is likely an important component of this project. Therefore, a legal notice will appear in a local publication contingent upon the release of this document for public involvement. This document will be revised after the public involvement requirements are fulfilled.

### **Public Controversy on Environmental Grounds**

Discuss public controversy concerning community and/or natural resource impacts, including what is being done during the project to minimize impacts.

At this time, there is no substantial public controversy concerning impacts to the community or to natural resources.

### Part II - General Project Identification, Description, and Design Information

Sponsor of the Project: INDOT INDOT District: Seymour

Local Name of the Facility: Access Management Project/580 ft. along SR 62 and 694 ft. along Michigan Rd.

Funding Source (mark all that apply): Federal  State  Local  Other\*

\*If other is selected, please identify the funding source: \_\_\_\_\_

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### PURPOSE AND NEED:

*The need should describe the specific transportation problem or deficiency that the project will address. The purpose should describe the goal or objective of the project. The solution to the traffic problem should NOT be discussed in this section.*

#### Need:

The need for this project is due to the crash frequency and severity in the project area, as discussed in the *Engineering Assessment*, prepared by the project engineers, Strand and Associates, Inc. (Strand), and signed by INDOT on August 4, 2020 (Appendix H, pages H-1 to H-9). The supporting data for this Purpose and Need statement is provided in Appendix H, pages H-4 and H-9. Crash frequency can be described in terms of Index of Crash Frequency (ICF), which is defined as the difference between expected crash frequency and the actual, reported crash frequency. Index of Crash Cost (ICC) is the difference between expected crash cost and the actual, reported cost. ICF and ICC values of 1.0 would be calculated for roadways with expected crash rate and crash cost based on the type of roadway and volume of traffic. Therefore, an ICF and/or ICC value above 1.0 would indicate a higher-than-expected crash rate and/or crash cost, respectively, for a given roadway.

In the northwest corner of the intersection, the excessively wide turning radius results in vehicles becoming adjacent to one another because the line separating the outside edge of pavement from the shoulder is approximately 13 ft. from the edge of the pavement, which is wider than a lane width. In other words, the radius is large enough to allow for a passenger vehicle to pass and queue alongside a vehicle waiting at the stop bar to turn west. Additionally, driveway crashes have occurred up to approximately 270 ft. south, 200 ft. north, and 500 ft. west of the intersection due to the presence of multiple commercial driveways. There are no access conflicts east of the intersection. With the existing short medians in mind as well, the north/south extent along Michigan Rd. goes as far as the commercial build up at and near the intersection. Beyond 284 ft. south and 300 ft. north, the access conflict points decrease. Additionally, the west extent along SR 62 has multiple commercial driveways experiencing several access points of conflict. Beyond 580 ft. west the access conflict points decrease. Drivers were observed turning left from the gas station driveway despite the presence of a raised concrete median. The existing medians are not long enough to deter left turns. A broken signpost is present on the west end of the median on the west leg of the intersection and signal visibility is impeded at times by the glare from the sun. To summarize, the crash rate is high due to excessively wide turning radius on the northwest corner, the presence of problematic commercial driveways, such as northbound Michigan Rd. from the south entrance of Circle K in the southwest quadrant, southbound Michigan Rd. from Next Generation Hearing Care in the southeast quadrant, northbound Michigan Rd. from the north driveway of Circle K/Mobil in the northwest quadrant, and to southbound Michigan Rd. from Taco Bell in the northeast quadrant, conflicts with the existing short median curb lengths, and impeded signal visibility on SR 62.

Crash data from January 15, 2017 through January 15, 2020 for the intersection of SR 62 with Michigan Rd. was analyzed for crash type and severity. The intersection was analyzed as a signalized intersection of a state highway with a non-state roadway. The Index of Crash Frequency was 2.05 and the Index of Crash Cost was 2.15. The analysis indicates that the intersection experiences a higher-than-expected crash rate and cost. As indicated in the table below, a total of 79 crashes occurred in this period, of which four crashes (5 percent of total) were fatal/incapacitating and nine crashes (11 percent) resulted in injury.

	Crash Severity			Crash Type				
	Fatal/ Incap.	Injury	PDO	Right Angle	Ran Off Road	Rear End	Sides wipe	R/L Turn
2017- 2020	4	9	66	8	4	27	15	25
% Total	5%	11%	84%	10.1%	5.1%	34.2%	19%	31.6%

#### Purpose:

The purpose of this project is to improve safety at this intersection by correcting access conflicts, reducing the crash frequency to as close to 1.0 as possible, and correcting the excessively wide turning radius in the northwest quadrant of the project area.

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## PROJECT DESCRIPTION (PREFERRED ALTERNATIVE):

County: Jefferson Municipality: Madison

Limits of Proposed Work: 580 ft. along SR 62 and 694 ft. along Michigan Rd.

Total Work Length: 0.24 Mile(s) Total Work Area: N/A Acre(s)

Is an Interstate Access Document (IAD)<sup>1</sup> required?  
If yes, when did the FHWA provide a Determination of Engineering and Operational Acceptability?

Yes <sup>1</sup>	No
<input type="checkbox"/>	<input checked="" type="checkbox"/>
Date: <input type="text"/>	

<sup>1</sup>If an IAD is required; a copy of the approved CE/EA document must be submitted to the FHWA with a request for final approval of the IAD.

*Describe location of project including township, range, city, county, roads, etc. Existing conditions should include current conditions, current deficiencies, roadway description, surrounding features, etc. Preferred alternative should include the scope of work, anticipated impacts, and how the project will meet the Purpose and Need. Logical termini and independent utility also need discussed.*

### Location:

This project is located at the intersection of SR 62 and Michigan Rd. in the City of Madison, in the south-central portion of Jefferson County, Indiana (Appendix B, page B-1). Specifically, the project is located in Sections 22 and 23, Township 4 North, Range 10 East of the Clifty Falls, Indiana 7.5-minute USGS topographic quadrangle (Appendix B, page B-2).

### Existing Conditions:

SR 62 is functionally classified as a Principal Arterial, conveys traffic east and west through this area, and has a posted speed limit of 35 mph. A typical cross section of SR 62 includes two 12-ft.-wide travel lanes in each direction. The west half of SR 62 is curbed with storm sewers and the eastern half has shoulders with open ditches (Appendix B, pages B-3 to B-8).

Michigan Rd. is functionally classified as a Minor Arterial roadway, conveys traffic north and south through this area, and has a posted speed limit of 40 mph. The adjacent land use is commercial. The crash rate is high due to excessive turning radius, conflicts with the existing median curb, and impeded signal visibility on SR 62.

This project was originally bundled with another access management project on SR 229 in Batesville, Franklin County, Indiana. The projects were split into two different contracts; therefore, two separate environmental documents are necessary.

### Preferred Alternative:

The INDOT intends to utilize Federal Highway Administration (FHWA) funds for a portion of the costs to construct the preferred alternative, which is to convert the access points to right-in / right-out only, within the project limits, in order to reduce the crash rate and improve safety (Appendix B, page B-9). The design plans are provided in Appendix B, pages B-10 to B-34. Approximately 1,050 square ft. of pavement will be removed in the northwest corner of the intersection in order to reduce the turning radius. A curb turnout and 15 linear ft. of paved side ditch will be created in the northwest corner to match the existing drainage pattern. The approach medians will be replaced and extended by approximately 50 ft. on the north approach, 200 ft. on the south approach, and 130 ft. on the west approach. Full-depth patches will be installed and milling and resurfacing will occur as needed. New LED signal heads with backplates, five section signal head for northbound right turns, and flashing yellow arrows for north and southbound left turns will be installed. The signpost on the west end of the median on the west leg will be replaced. These improvements will eliminate access to northbound Michigan Rd. from the south entrance of Circle K in the southwest quadrant, southbound Michigan Rd. from

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Next Generation Hearing Care in the southeast quadrant, northbound Michigan Rd. from the north driveway of Circle K/Mobil in the northwest quadrant, and to southbound Michigan Rd. from Taco Bell in the northeast quadrant, which is expected to reduce crashes and improve the safety of the intersection. Some utility relocation may be required. No additional permanent or temporary ROW will be required.

**Logical Termini/Independent Utility:**

The project will extend 580 ft. along SR 62 and 694 ft. along Michigan Rd., for a total of 1,274 ft. Beyond 284 ft. south and 300 ft. north along Michigan Rd., the access conflict points decrease. Beyond 580 ft. west along SR 62 the access conflict points decrease. The project has independent utility because it will meet the stated purpose and need of the project without relying on other transportation improvements.

**OTHER ALTERNATIVES CONSIDERED:**

*Provide a header for each alternative. Describe all discarded alternatives, including the No Build Alternative. Explain why each discarded alternative was not selected. Make sure to state how each alternative meets or does not meet the Purpose and Need and why.*

**No Build Alternative**

The no build alternative would retain the existing roadway and driveway configurations at the site. No additional cost would be incurred by maintaining continued use of the site. Crashes would likely continue at their current rates, likely resulting in continued fatalities, injuries, and property damage. This alternative would not meet the stated purpose and need of the project; therefore, it was discarded from further consideration.

**The No Build Alternative is not feasible, prudent or practicable because** (Mark all that apply):

- It would not correct existing capacity deficiencies;
- It would not correct existing safety hazards;
- It would not correct the existing roadway geometric deficiencies;
- It would not correct existing deteriorated conditions and maintenance problems; or
- It would result in serious impacts to the motoring public and general welfare of the economy.
- Other (Describe):

**ROADWAY CHARACTER:**

*If the proposed action includes multiple roadways, complete and duplicate for each roadway.*

Name of Roadway SR 62  
 Functional Classification: Principal Arterial - Other  
 Current ADT: 20,712 VPD (2019) Design Year ADT: 22,705 VPD (2042)  
 Design Hour Volume (DHV): 9.21 Truck Percentage (%) 5  
 Designed Speed (mph): 40 Legal Speed (mph): 40

	Existing		Proposed	
Number of Lanes:	Eight		Eight	
Type of Lanes:	Left-turn and right-turn only lanes and two through lanes in each direction		Left-turn and right-turn only lanes and two through lanes in each direction	
Pavement Width:	72-83	ft.	72-83	ft.
Shoulder Width:	0-11	ft.	1-11	ft.
Median Width:	4	ft.	4	ft.
Sidewalk Width:	N/A	ft.	N/A	ft.

Setting:  Urban  Suburban  Rural  
 Topography:  Level  Rolling  Hilly

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Name of Roadway Michigan Rd.  
 Functional Classification: Minor Arterial  
 Current ADT: 11,264 VPD (2019) Design Year ADT: 14,161 VPD (2042)  
 Design Hour Volume (DHV): 9.0 Truck Percentage (%) 14  
 Designed Speed (mph): 40 Legal Speed (mph): 40

	Existing	Proposed
Number of Lanes:	Six	Six
Type of Lanes:	Left-turn and right-turn only lanes and one through lane in each direction	Left-turn and right-turn only lanes and one through lane in each direction
Pavement Width:	72-82.5 ft.	72-82.5 ft.
Shoulder Width:	0-10.6 ft.	1-11 ft.
Median Width:	4 ft.	4 ft.
Sidewalk Width:	0-5 ft.	0-5 ft.

Setting:  Urban  Suburban  Rural  
 Topography:  Level  Rolling  Hilly

### BRIDGES AND/OR SMALL STRUCTURE(S):

*If the proposed action includes multiple structures, complete and duplicate for each bridge and/or small structure. Include both existing and proposed bridge(s) and/or small structure(s) in this section.*

Structure/NBI Number(s): N/A Sufficiency Rating: N/A  
(Rating, Source of Information)

	Existing	Proposed
Bridge/Structure Type:		
Number of Spans:		
Weight Restrictions:	ton	ton
Height Restrictions:	ft.	ft.
Curb to Curb Width:	ft.	ft.
Outside to Outside Width:	ft.	ft.
Shoulder Width:	ft.	ft.

*Describe impacts and work involving bridge(s), culvert(s), pipe(s), and small structure(s). Provide details for small structure(s): structure number, type, size (length and dia.), location and impacts to water. Use a table if the number of small structures becomes large. If the table exceeds a complete page, put it in the appendix and summarize the information below with a citation to the table.*

No bridges or small structures are located within the project area.

### MAINTENANCE OF TRAFFIC (MOT) DURING CONSTRUCTION:

	Yes	No
Is a temporary bridge proposed?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Is a temporary roadway proposed?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Will the project involve the use of a detour or require a ramp closure? (describe below)	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Provisions will be made for access by local traffic and so posted.	<input type="checkbox"/>	<input type="checkbox"/>
Provisions will be made for through-traffic dependent businesses.	<input type="checkbox"/>	<input type="checkbox"/>
Provisions will be made to accommodate any local special events or festivals.	<input type="checkbox"/>	<input type="checkbox"/>

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	Yes	No
Will the proposed MOT substantially change the environmental consequences of the action?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Is there substantial controversy associated with the proposed method for MOT?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Will the project require a sidewalk, curb ramp, and/or bicycle lane closure? (describe below)	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Provisions will be made for access by pedestrians and/or bicyclist and so posted (describe below).	<input type="checkbox"/>	<input type="checkbox"/>

*Discuss closures, detours, and/or facilities (if any) that will be provided for maintenance of traffic. Any known impacts from these temporary measures should be quantified to the extent possible, particularly with respect to properties such as Section 4(f) resources and wetlands. Discuss any pedestrian/bicycle closures. Any local concerns about access and traffic flow should be detailed as well.*

The MOT for the project will require single lane closures, lane shifting, and reduced speed limits through the work site. The contractor will be responsible for following road closure standards as detailed in the current INDOT *Standard Specifications* and the *Indiana Manual on Uniform Traffic Control Devices*. Access shall be maintained for vehicular and pedestrian use to all businesses and side streets throughout construction.

The closures/lane restrictions will pose a temporary inconvenience to traveling motorists (including school buses and emergency services); however, no significant delays are anticipated, and all inconveniences and delays will cease upon project completion.

### ESTIMATED PROJECT COST AND SCHEDULE:

Engineering: \$ \_\_\_\_\_ Right-of-Way: \$ \_\_\_\_\_ Construction: \$ 282,031.00 (2023)

Anticipated Start Date of Construction: Spring 2023

### RIGHT OF WAY:

Land Use Impacts	Amount (acres)	
	Permanent	Temporary
Residential	0	0
Commercial	0	0
Agricultural	0	0
Forest	0	0
Wetlands	0	0
Other:	0	0
Other:	0	0
TOTAL	0	0

*Describe both Permanent and Temporary right-of-way and describe their current use. Typical and Maximum right-of-way widths (existing and proposed) should also be discussed. Any advance acquisition, reacquisition or easements, either known or suspected, and their impacts on the environmental analysis should be discussed.*

The existing ROW) is approximately 50 ft. from the centerlines of the roadways through the project area. The existing ROW contains concrete, pavement, and roadside grass.

This project will occur within existing ROW. No permanent or temporary ROW will be required for this project.

If the scope of work or permanent or temporary ROW amounts change, the INDOT Environmental Services Division (ESD) and the INDOT District Environmental Section will be contacted immediately.

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**Part III – Identification and Evaluation of Impacts of the Proposed Action**

**SECTION A - EARLY COORDINATION:**

List the date(s) coordination was sent and all resource agencies that were contacted as a part of the development of this Environmental Study. Also, include the date of their response or indicate that no response was received.

Early coordination letters were sent on June 30 and July 8, 2021, Appendix C, pages C-1 to C-3.

<u>Agency</u>	<u>Date Sent</u>	<u>Date Response Received</u>	<u>Appendix C, Page(s)</u>
FHWA	June 30, 2021	None received	N/A
Indiana Geological and Water Survey (IGWS)	June 30, 2021	June 30, 2021	C-4 to C-5
Indiana Department of Natural Resources (IDNR) Division of Fish and Wildlife (DFW)	June 30, 2021	July 30, 2021	C-10
National Parks Service	June 30, 2021	None received	N/A
Indiana Department of Environmental Management (IDEM) Drinking Water Branch, Groundwater Section	June 30, 2021	July 7, 2021	C-7 to C-8
United States Department of Housing and Urban Development	June 30, 2021	None received	N/A
INDOT Seymour District	June 30, 2021	None received	N/A
INDOT Office of Aviation	June 30, 2021	July 6, 2021	Response was for Des. No. 2101170. Removed from this project.
Natural Resources Conservation Service (NRCS)	June 30, 2021	July 15, 2021	C-9
United States Fish and Wildlife Service (USFWS)	June 30, 2021	None received	N/A
Jefferson County Highway Department	June 30, 2021	None received	N/A
Jefferson County Surveyor	June 30, 2021	None received	N/A
Jefferson County Commissioner	June 30, 2021	None received	N/A
Jefferson County Emergency Management Agency	June 30, 2021	June 30, 2021	C-6
City of Madison - Mayor	June 30, 2021	None received	N/A
City of Madison – City Council, Districts 4 and 5	June 30, 2021	None received	N/A
INDOT Utilities and Railroads	June 30, 2021	None received	N/A
Madison Municipal Separate Storm Sewer (MS4) Coordinator	July 8, 2021	None received	N/A

All applicable recommendations are included in the Environmental Commitments section of this CE document.



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### SECTION B – ECOLOGICAL RESOURCES:

**Streams, Rivers, Watercourses & Other Jurisdictional Features**

- Federal Wild and Scenic Rivers
- State Natural, Scenic or Recreational Rivers
- Nationwide Rivers Inventory (NRI) listed
- Outstanding Rivers List for Indiana
- Navigable Waterways

**Presence**


**Impacts**

Yes                  No


Total stream(s) in project area: 0 Linear feet      Total impacted stream(s): 0 Linear feet

Stream Name	Classification	Total Size in Project Area (linear feet)	Impacted linear feet	Comments (i.e. location, flow direction, likely Water of the US, appendix reference)
N/A				

*Describe all streams, rivers, watercourses and other jurisdictional features adjacent or within the project area. Include whether or not impacts (both permanent and temporary) will occur to the features identified. Include if the streams or rivers are listed on any federal or state lists for Indiana. Include if features are likely subject to federal or state jurisdiction. Discuss measures to avoid, minimize, and mitigate if impacts will occur.*

Based on the desktop review, the aerial map of the project area, and the Water Resources Map (Appendix E, page E-3), there are five streams, rivers, watercourse, or other jurisdictional features within the 0.5-mile search radius. There are no streams, rivers, watercourse, or other jurisdictional features within or adjacent to the project area, which was confirmed by the site visit on June 26, 2021 by Metric. Therefore, no impacts are expected.

**Open Water Feature(s)**

- Reservoirs
- Lakes
- Farm Ponds
- Retention/Detention Basin
- Storm Water Management Facilities
- Other: \_\_\_\_\_

**Presence**


**Impacts**

Yes                  No


*Describe all open water feature(s) identified adjacent or within the project area. Include whether or not impacts (both permanent and temporary) will occur to the features identified. Include if features are likely subject to federal or state jurisdiction. Discuss measures to avoid, minimize, and mitigate if impacts will occur.*

Based on the desktop review, the aerial map of the project area, and the Water Resources Map (Appendix E, page E-3) there are five open water feature(s) within the 0.5-mile search radius. There are no open water feature(s) within or adjacent to the project area. That number was confirmed by the site visit on June 26, 2021 by Metric. Therefore, no impacts are expected.

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**Presence**

**Impacts**

Yes      No

**Wetlands**




Total wetland area: N/A Acre(s) Total wetland area impacted: N/A Acre(s)

(If a determination has not been made for non-isolated/isolated wetlands, fill in the total wetland area impacted above.)

Wetland No.	Classification	Total Size (Acres)	Impacted Acres	Comments (i.e. location, likely Water of the US, appendix reference)
N/A				

**Documentation**

**ESD Approval Dates**

**Wetlands (Mark all that apply)**

Wetland Determination

N/A

Wetland Delineation

N/A

USACE Isolated Waters Determination

N/A

**Improvements that will not result in any wetland impacts are not practicable because such avoidance would result in** (Mark all that apply and explain):

- Substantial adverse impacts to adjacent homes, business or other improved properties;
- Substantially increased project costs;
- Unique engineering, traffic, maintenance, or safety problems;
- Substantial adverse social, economic, or environmental impacts, or
- The project not meeting the identified needs.


*Describe all wetlands identified adjacent or within the project area. Include whether or not impacts (both permanent and temporary) will occur to the features identified. Include if features are likely subject to federal or state jurisdiction. Discuss measures to avoid, minimize, and mitigate if impacts will occur.*

Based on the desktop review, the aerial map of the project area, and the Water Resources Map (Appendix E, page E-3) there are three NWI wetland polygons and one NWI wetland point within the 0.5-mile search radius. There are no wetlands within or adjacent to the project area, which was confirmed by the site visit on June 26, 2022 by Metric. Therefore, no impacts are expected.

*location*

**Presence**

**Impacts**

Yes      NO

**Terrestrial Habitat**




Total terrestrial habitat in project area: 0.2 Acre(s) Total tree clearing: N/A Acre(s)

*Describe types of terrestrial habitat (i.e. forested, grassland, farmland, lawn, etc) adjacent or within the project area. Include whether or not impacts will occur to habitat identified. Include total terrestrial habitat impacted and total tree clearing that will occur. Discuss measure to avoid, minimize, and mitigate if impacts will occur.*

Based on a desktop review, a site visit on June 26, 2021 by Metric, and the aerial map of the project area (Appendix B, page B-3) there is grass habitat within the project area. The dominant species present is tall fescue (*Festuca arundinacea*). Up to approximately 0.2 acre of grass will be temporarily disturbed for the contractor to access the project area and install appropriate MOT equipment. All disturbed areas will be reseeded as soon as possible upon completion of the project in accordance with current INDOT *Standard Specifications*. No permanent impacts to terrestrial habitat are anticipated as the project area is primarily pavement and concrete. No trees will be trimmed or removed for this project.

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**Protected Species**

**Federally Listed Bats**

Information for Planning and Consultation (IPaC) determination key completed  
 Section 7 informal consultation completed (IPaC cannot be completed)  
 Section 7 formal consultation Biological Assessment (BA) required

Yes	No
<input checked="" type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>

Determination Received for Listed Bats from USFWS: NE  NLAA  LAA

**Other Species not included in IPaC**

Additional federal species found in project area (based on IPaC species list)  
 State species (not bird) found in project area (based upon consultation with IDNR)

Yes	No
<input checked="" type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>

**Migratory Birds**

Known usage or presence of birds (i.e. nests)  
 State bird species based upon coordination with IDNR

Yes	No
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>

*Discuss IDNR coordination and species identified. Describe USFWS Section 7 consultation and determination received for Indiana bat and northern long-eared bat impacts. Discuss if other federally listed species were identified. If so, include consultation that has occurred and the determination that was received. Discuss if migratory birds have been observed and any impacts.*

Based on a desktop review, the IDNR Jefferson Endangered, Threatened, or Rare (ETR) Species List has been checked (Appendix E, pages E-5 to E-8). According to the IDNR-DFW early coordination response letter dated July 30, 2021 (Appendix C, page C-10), the Natural Heritage Program's Database has been checked and, as of that date, no state- or federally-listed ETR plant or animal species had been reported to occur in the project vicinity.

Project information was submitted through the USFWS Information for Planning and Consultation (IPaC) portal, and an official species list was generated (Appendix C, pages C-11 to C-16). The project is within range of the federally endangered Indiana bat (*Myotis sodalis*) and the federally threatened Northern long-eared bat (NLEB) (*Myotis septentrionalis*). Other species were generated in the IPaC species list along with the Indiana bat and NLEB. Refer to paragraph below.

The official species list generated from IPaC indicated one other species present within the project area. The candidate Monarch Butterfly (*Danaus plexippus*) was found within the project area. The project qualifies for the USFWS Interim Policy. Further coordination with USFWS is not needed.

The project qualifies for the *Range-wide Programmatic Informal Consultation for the Indiana bat and NLEB*, dated May 2016 (revised February 2018), between FHWA, Federal Railroad Administration (FRA), Federal Transit Administration (FTA), and USFWS. An effect determination key was completed on October 19, 2021 and, based on the responses provided, the project was found to "May Affect, Not Likely to Adversely Affect" the Indiana bat and/or the NLEB (Appendix C, pages C-17 to C-25). INDOT reviewed and verified the effect finding on October 19, 2021 and requested USFWS's review of the finding. No response was received from USFWS within the 14-day review period; therefore, it was concluded they concur with the finding. Avoidance and Minimization Measures (AMMs) and/or commitments are included as firm commitments in the Environmental Commitments section of this document

This precludes the need for further consultation on this project as required under Section 7 of the Endangered Species Act, as amended. If new information on endangered species at the site becomes available, or if project plans are changed, USFWS will be contacted for consultation.

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### Geological and Mineral Resources

Project located within the Indiana Karst Region  
 Karst features identified within or adjacent to the project area  
 Oil/gas or exploration/abandoned wells identified in the project area

Yes	No
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>

Date Karst Evaluation reviewed by INDOT EWPO (if applicable): N/A

*Discuss if project is located in the Indiana Karst Region and if any karst features have been identified in the project area (from RFI). Discuss response received from IGWS coordination. Discuss if any mines, oil/gas, or exploration/abandoned wells were identified and if impacts will occur. Include discussion of karst study/report was completed and results. (Karst investigation must comply with the current Protection of Karst Features during Planning and Construction guidance and coordinated and reviewed by INDOT EWPO)*

Based on a desktop review and the Indiana Karst Region map, the project is located outside the designated Indiana Karst Region as outlined in the most current *Protection of Karst Features during Project Development and Construction*. According to the topo map of the project area (Appendix B, page B-2), and the Water Resources Map (Appendix E, page E-3) there are no karst features identified within or adjacent to the project area. In the early coordination response June 30, 2020, the Indiana Geological and Water Survey (IGWS) did not indicate that karst features exist in the project area (Appendix C, pages C-4 to C-5). The IGWS indicated that, within 0.5-mile of the project area, geological hazards include a high liquefaction potential; there is a high potential for bedrock resources and no documented sand and gravel resources; and no active or abandoned mineral resource extraction sites had been documented. The features will not be affected because excavation deeper than 10 ft. below ground surface is not required for the project. Response from the IGWS has been communicated to the designer on November 29, 2021. No impacts are expected.

### SECTION C – OTHER RESOURCES

#### Drinking Water Resources

Wellhead Protection Area(s)  
 Source Water Protection Area(s)  
 Water Well(s)  
 Urbanized Area Boundary  
 Public Water System(s)

Presence	Impacts	
	Yes	No
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Is the project located in the St. Joseph Sole Source Aquifer (SSA):  
 If Yes, is the FHWA/EPA SSA MOU Applicable?  
 If Yes, is a Groundwater Assessment Required?

Yes	No
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>

*Check the appropriate boxes and discuss each topic below. Provide details about impacts and summarize resource-specific coordination responses and any mitigation commitments. Reference responses in the Appendix.*

The project is located in Jefferson County which is not located within the area of the St. Joseph Sole Source Aquifer, the only legally designated sole source aquifer in the state of Indiana. Therefore, the FHWA/EPA Sole Source Aquifer Memorandum of Understanding (MOU) is not applicable to this project, a detailed groundwater assessment is not needed, and no impacts are expected.

The IDEM Wellhead Proximity Determinator website (<http://www.in.gov/idem/cleanwater/pages/wellhead/>) was accessed on June 30, 2021 by Metric. This project is not located within a Wellhead Protection Area or Source Water Area. In an early coordination letter dated July 7, 2021, IDEM stated that the project is not located within a wellhead protection area (Appendix C, pages C-7 to C-8).

The IDNR Water Well Record Database website (<https://www.in.gov/dnr/water/3595.htm>) was accessed on July 16,

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2021 by Metric. No wells are located near this project. Therefore, no impacts are expected.

Based on a desktop review of the INDOT Municipal Separate Storm Sewer System (MS4) website (<https://entapps.indot.in.gov/MS4/>) by Metric on July 8, 2021, this project is located in an Urban Area Boundary (UAB). An early coordination letter was sent on July 8, 2021 to the City of Madison MS4 Coordinator (Appendix C, pages C-1 to C-3). The MS4 coordinator did not respond within the 30-day timeframe. The project will comply with the City of Madison Storm Water Program because the appropriate erosion and sediment control devices and pollution prevention measures outlined in the current INDOT *Standard Specifications* will be implemented during construction and will satisfy all City of Madison requirements.

Based on a desktop review, the aerial map of the project area (Appendix B, page B-3) and a site visit on June 26, 2021 by Metric, this project is located where there is a public water system. The public water system will not be affected because the water lines are not included to be repaired or replaced during this project. Early coordination letter was sent on February 11, 2021 to Madison Utilities Water and Sewer. Madison Utilities indicated that there is an active 10 inch water main crossing SR 62 and an active 8 inch water main crossing Michigan Road. The existing water lines should not be affected by the project (Appendix C, pages C-26 to C-27).

<b>Floodplains</b>	<u>Presence</u>	<u>Impacts</u>	
		Yes	No
Project located within a regulated floodplain	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Longitudinal encroachment	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Transverse encroachment	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Homes located in floodplain within 1000' up/downstream from project	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

If applicable, indicate the Floodplain Level?

Level 1  Level 2  Level 3  Level 4  Level 5

Use the IDNR Floodway Information Portal to help determine potential impacts. Include floodplain map in appendix. Discuss impacts according to the classification system. If encroachment on a flood plain will occur, coordinate with the Local Flood Plain Administrator during design to insure consistency with the local flood plain planning.

The Indiana Department of Natural Resources Indiana Floodway Information Portal website (<https://indnr.maps.arcgis.com/apps/webappviewer/index.html?id=05026dabc2e8461983e196d56a213c1e>) was accessed on June 30, 2021 by Metric. This project is not located in a regulatory floodplain as determined from approved IDNR floodplain maps (Appendix F, page F-4 ). Therefore, it does not fall within the guidelines for the implementation of 23 CFR 650, 23 CFR 771, and 44 CFR. No impacts are expected.

<b>Farmland</b>	<u>Presence</u>	<u>Impacts</u>	
		Yes	No
Agricultural Lands	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Prime Farmland (per NRCS)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Total Points (from Section VII of CPA-106/AD-1006\*) \_\_\_\_\_

\*If 160 or greater, see CE Manual for guidance.

Discuss existing farmland resources in the project area, impacts that will occur to farmland, and mitigation and minimization measures considered.

Based on a desktop review, a site visit on June 26, 2021 by Metric, the aerial map of the project area (Appendix B, page B-3), there is no land that meets the definition of farmland under the Farmland Protection Policy Act (FPPA) within or adjacent to the project area. The requirements of the FPPA do not apply to this project; therefore, no impacts are expected. An early coordination letter was sent on June 30, 2021, to Natural Resources Conservation Service (NRCS). In a response dated July 15, 2021, NRCS stated that the project will not cause a conversion of prime farmland (Appendix C, page C-9). Therefore, no impact is expected.

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### SECTION D – CULTURAL RESOURCES

**Minor Projects PA**      **Category(ies) and Type(s)** Category B, Types 1, 2, and 3      **INDOT Approval Date(s)** June 25, 2021      **N/A**

**Full 106 Effect Finding**

No Historic Properties Affected       No Adverse Effect       Adverse Effect

**Eligible and/or Listed Resources Present**

NRHP Building/Site/District(s)       Archaeology       NRHP Bridge(s)

**Documentation Prepared** (mark all that apply)

APE, Eligibility and Effect Determination   
 800.11 Documentation   
 Historic Properties Report or Short Report   
 Archaeological Records Check and Assessment  **X**  
 Archaeological Phase Ia Survey Report   
 Archaeological Phase Ic Survey Report   
 Other:

**ESD Approval Date(s)**

**SHPO Approval Date(s)**

	June 2, 2021	

Memorandum of Agreement (MOA)

**MOA Signature Dates** (List all signatories)

*If the project falls under the MPPA, describe the category(ies) that the project falls under and any approval dates. If the project requires full Section 106, use the headings provided. The completion of the Section 106 process requires that a Legal Notice be published in local newspapers. Please indicate the publication date, name of the paper(s) and the comment period deadline. Include any further Section 106 work which must be completed at a later date, such as mitigation from a MOA or avoidance commitments.*

INDOT Cultural Resource Office (CRO) reviewed the Indiana State Historic Architectural and Archaeological Research Database (SHAARD) Geographic Information Systems (GIS) mapping program. According to SHAARD GIS, there are no archaeological sites located within or adjacent to the project area. Since no ROW is being acquired and the project will be confined to excavation work in previously disturbed soils, there are no archaeological concerns.

On June 25, 2021, the INDOT CRO determined that this project falls within the guidelines of Category B, Types 1, 2, and 3 under the Minor Projects Programmatic Agreement (Appendix D, pages D-1 to D-5). Category B-1 includes replacement, repair, or installation of curbs, curb ramps, or sidewalks, including when such projects are associated with roadway work such as surface replacement, reconstruction, rehabilitation, or resurfacing projects, including overlays, shoulder treatments, pavement repair, seal coating, pavement grinding, and pavement marking. Category B-2 includes installation of new lighting, signals, signage, and other traffic control devices. Category B-3 includes construction of added travel, turning, or auxiliary lanes (e.g., bicycle, truck climbing, and acceleration and deceleration lanes) and shoulder widening. All three categories require the project to meet certain conditions for Archaeological and Above-Ground Resources. The project met the criterion for Archaeological Resources because work will occur in previously disturbed soils. The project met the criteria for Above-Ground Resources because work will not occur adjacent to or within a National register of Historic Places (NRHP)-listed or NRHP-eligible district or individual above-ground resource. This completes the Section 106 process and the responsibilities of the FHWA under Section 106 have been fulfilled.

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<b>SECTION E – SECTION 4(f) RESOURCES/ SECTION 6(f) RESOURCES</b>
---

	<u>Presence</u>	<u>Use</u>	
		Yes	No
<b>Parks and Other Recreational Land</b>			
Publicly owned park	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Publicly owned recreation area	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Other (school, state/national forest, bikeway, etc.)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
<b>Wildlife and Waterfowl Refuges</b>			
National Wildlife Refuge	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
National Natural Landmark	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
State Wildlife Area	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
State Nature Preserve	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
<b>Historic Properties</b>			
Site eligible and/or listed on the NRHP	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
 <b><u>Evaluations</u></b>			
<b><u>Prepared</u></b>			
Programmatic Section 4(f)	<input type="checkbox"/>		
"De minimis" Impact	<input type="checkbox"/>		
Individual Section 4(f)	<input type="checkbox"/>		
Any exception included in 23 CFR 774.13	<input type="checkbox"/>		

*Discuss Programmatic Section 4(f) and "de minimis" Section 4(f) impacts in the discussion below. Individual Section 4(f) documentation must be included in the appendix and summarized below. Discuss proposed alternatives that satisfy the requirements of Section 4(f). FHWA has identified various exceptions to the requirement for Section 4(f) approval. Refer to 23 CFR § 774.13 - Exceptions.*

Section 4(f) of the U.S. Department of Transportation Act of 1966 prohibits the use of certain public and historic lands for federally funded transportation facilities unless there is no feasible and prudent alternative. The law applies to significant publicly owned parks, recreation areas, wildlife / waterfowl refuges, and NRHP eligible or listed historic properties regardless of ownership. Lands subject to this law are considered Section 4(f) resources.

Based on a desktop review, the aerial map of the project area (Appendix B, page B-3) and the Infrastructure Map (Appendix E, page E-2), there are two potential 4(f) resources located within the 0.5-mile search radius. According to the approved Minor Projects Programmatic Agreement, dated June 25, 2021 (Appendix D, pages D-1 to D-5) and the site visit on June 26, 2021 by Metric, there are no Section 4(f) resources within or adjacent to the project area. Therefore, no use is expected.

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**Section 6(f) Involvement**

**Presence**

**Use**

**Section 6(f) Property**

**Yes**

**No**




*Discuss Section 6(f) resources present or not present. Discuss if any conversion would occur as a result of this project. If conversion will occur, discuss the conversion approval.*

The U.S. Land and Water Conservation Fund Act of 1965 established the Land and Water Conservation Fund (LWCF), which was created to preserve, develop, and assure accessibility to outdoor recreation resources. Section 6(f) of this Act prohibits conversion of lands purchased with LWCF monies to a non-recreation use.

A review of 6(f) properties on the INDOT ESD website revealed a total of ten properties in Jefferson County (Appendix H, page H-10). None of these properties are located within or adjacent to the project area. Therefore, there will be no impacts to 6(f) resources.

### SECTION F – Air Quality

**STIP/TIP and Conformity Status of the Project**

- Is the project in the most current STIP/TIP?
- Is the project located in an MPO Area?
- Is the project in an air quality non-attainment or maintenance area?
- If Yes, then:
  - Is the project in the most current MPO TIP?
  - Is the project exempt from conformity?
- If No, then:
  - Is the project in the Transportation Plan (TP)?
  - Is a hot spot analysis required (CO/PM)?

Yes	No
<input checked="" type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>

Location in STIP: Fiscal Year (FY) 2022-2026, dated June 17, 2022

Name of MPO (if applicable): N/A

Location in TIP (if applicable): N/A

Level of MSAT Analysis required?

Level 1a  Level 1b  Level 2  Level 3  Level 4  Level 5

*Describe if the project is listed in the STIP and if it is in a TIP. Describe the attainment status of the county(ies) where the project is located. Indicate whether the project is exempt from a conformity determination. If the project is not exempt, include information about the TP and TIP. Describe if a hot spot analysis is required and the MSAT Level.*

This project is included in the Fiscal Year (FY) 2022 – 2026 Statewide Transportation Improvement Program (STIP) (Appendix G, page G-1).

This project is located in Jefferson County, which is currently in attainment for all criteria pollutants according to (<https://www.in.gov/idem/sips/nonattainment-status-of-counties/>). Therefore, the conformity procedures of 40 CFR Part 93 do not apply.

This project is of a type qualifying as a categorical exclusion (Group 1) under 23 CFR 771.117(c), or exempt under the Clean Air Act conformity rule under 40 CFR 93.126, and as such, a Mobile Source Air Toxics analysis is not required.



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### SECTION G - NOISE

**Noise**

**Yes      No**

Is a noise analysis required in accordance with FHWA regulations and INDOT's traffic noise policy?

Date Noise Analysis was approved/technically sufficient by INDOT ESD: N/A

*Describe if the project is a Type I or Type III project. If it is a Type I project, describe the studies completed to date and if noise impacts were identified. If noise impacts were identified, describe if abatement is feasible and reasonable and include a statement of likelihood.*

This project is a Type III project. In accordance with 23 CFR 772 and the current *Indiana Department of Transportation Traffic Noise Analysis Procedure*, this action does not require a formal noise analysis.

### SECTION H – COMMUNITY IMPACTS

**Regional, Community & Neighborhood Factors**

**Yes      No**

Will the proposed action comply with the local/regional development patterns for the area?	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Will the proposed action result in substantial impacts to community cohesion?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Will the proposed action result in substantial impacts to local tax base or property values?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Will construction activities impact community events (festivals, fairs, etc.)?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Does the community have an approved transition plan?	<input checked="" type="checkbox"/>	<input type="checkbox"/>
If No, are steps being made to advance the community's transition plan?	<input type="checkbox"/>	<input type="checkbox"/>
Does the project comply with the transition plan? (explain in the discussion below)	<input checked="" type="checkbox"/>	<input type="checkbox"/>

*Discuss how the project complies with the area's local/regional development patterns; whether the project will impact community cohesion; and impact community events. Discuss how the project conforms with the ADA Transition Plan.*

Based on a review of the Jefferson County Government website by Metric on November 27, 2021, Jefferson County has an approved Americans with Disabilities Act (ADA) Transition Plan (<https://jeffersoncounty.in.gov/DocumentCenter/View/371/JeffCo-Title-ADA-Transition-Plan?bidId=>). This project will not impact existing pedestrian facilities, as none exist in the project area, and will not create new pedestrian facilities; therefore, the project complies with the transition plan and no impact is expected.

Based on a review of the Indiana Festivals website (<https://indianafestivals.org/>) and the Indiana Association of Fairs and Festivals website (<https://www.indianafairsandfestivals.org/>), no listed fairs or festivals will occur within the project areas during construction. Therefore, no impact to community events is expected.

The project is not expected to result in any permanent adverse impact to local mobility, access, pedestrian or motorist safety, or emergency services. The project is not expected to result in any permanent impacts to community cohesion, local tax base, property values, or community events. During construction, delays may occur due to the lane closures. The project will permanently alter the movement of traffic at this intersection; however, the changes in access to local businesses (right -in and right-out only) should be considered a positive result as the changes are expected to reduce crash frequency and severity in the area. Local access will be maintained during construction.

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### Public Facilities and Services

*Discuss what public facilities and services are present in the project area and impacts (such as MOT) that will occur to them. Include how the impacts have been minimized and what coordination has occurred. Some examples of public facilities and services include health facilities, educational facilities, public and private utilities, emergency services, religious institutions, airports, transportation or public pedestrian and bicycle facilities.*

Based on a desktop review, the aerial map of the project area (Appendix B, page B-3), and the Infrastructure Map (Appendix E, page E-2), there are three religious facilities, one school, one recreational facility, two natural gas pipeline segments, and one railroad segment located within the 0.5-mile search radius and one public use airport located within 3.8 miles of the project area. One of the natural gas pipelines crosses SR 62 within the project area. Excavation will not occur to a depth that would impact the natural gas pipeline; therefore, no impacts are expected. Access to all properties will be maintained during construction.

On August 25, 2022 and August 29, 2022, Metric sent an e-mail to Center Point Energy, owner and operator of the natural gas pipeline that crosses SR 62, adjacent west of the intersection of SR 26 and Michigan Rd. As of September 9, 2022, no response has been received. Impacts to the gas pipeline are not likely; however, if the pipeline is exposed during construction activities, work shall stop, and the contractor shall contact Center Point Energy.

Jefferson County Emergency Management Agency responded on June 30, 2021 indicating they have no concerns regarding the project but requested to be notified of any detours or closures associated with the project in advance (Appendix C, page C-6).

All applicable recommendations are included in the Environmental Commitments section of this CE document.

It is the responsibility of the project sponsor to notify school corporations and emergency services at least two weeks prior to any construction that would block or limit access.

### Environmental Justice (EJ) (Presidential EO 12898)

During the development of the project were EJ issues identified?

Yes	No
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>

Does the project require an EJ analysis?

If YES, then:

Are any EJ populations located within the project area?

<input type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>

Will the project result in adversely high and disproportionate impacts to EJ populations?

*Indicate if EJ issues were identified during project development. If an EJ analysis was not required, discuss why. If an EJ analysis was required, describe how the EJ population was identified. Include if the project has a disproportionately high or adverse effect on EJ populations and explain your reasoning. If yes, describe actions to avoid, minimize and mitigate these effects.*

Under FHWA Order 6640.23A, FHWA and the project sponsor, as a recipient of funding from FHWA, are responsible to ensure that their programs, policies, and activities do not have a disproportionately high and adverse effect on minority or low-income populations. This project will have no relocations and will require less than 0.5 acre of additional permanent ROW; therefore, an EJ analysis is not required per the current INDOT Categorical Exclusion Manual.

### Relocation of People, Businesses or Farms

Will the proposed action result in the relocation of people, businesses or farms?

Yes	No
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>

Is a BIS or CSRS required?

Number of relocations:      Residences:   0        Businesses:   0        Farms:   0        Other:   0  

*Discuss any relocations that will occur due to the project. If a BIS or CSRS is required, discuss the results in the discussion below.*

No relocations of people, businesses, or farms will take place as a result of this project.

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### SECTION I – HAZARDOUS MATERIALS & REGULATED SUBSTANCES

**Hazardous Materials & Regulated Substances** (Mark all that apply)

Documentation

Red Flag Investigation (RFI)	<input type="checkbox"/>
Phase I Environmental Site Assessment (Phase I ESA)	<input type="checkbox"/>
Phase II Environmental Site Assessment (Phase II ESA)	<input type="checkbox"/>
Design/Specifications for Remediation required?	<input type="checkbox"/>

Date RFI concurrence by INDOT SAM (if applicable): N/A

*Include a summary of the potential hazardous material concerns found during review. Discuss in depth sites found within, directly adjacent to, or ones that could impact the project area. Refer to current INDOT SAM guidance. If additional documentation (special provisions, pay quantities, etc.) will be needed, include in discussion. Include applicable commitments.*

On November 18, 2021 a staff member of INDOT Site Assessment & Management (SAM) indicated that they agree that no RFI is warranted for this project due to impacts (from an RFI standpoint) will be minimal (Appendix E, page E-1).

Based on a review of the State of Indiana Geographic Information Office (GIO) library, five Resource Conservation and Recovery Act (RCRA) Generator/Treatment, Storage, and Disposal (TSD) sites, two State Cleanup sites, thirteen underground storage tanks (USTs), six mapped and one unmapped Leaking UST (LUST) sites, three mapped Institutional Controls, one of which is duplicate, one unmapped Institutional Control site, three Brownfield sites, and six National Pollutant Discharge Elimination Sites (NPDES) are located within 0.5-mile of the project area. Two of the LUST sites and three of the UST sites are located adjacent to the project area. The planned depth of excavation is minimal; therefore, none of the hazmat sites identified are anticipated to impact the project. Further investigation for hazardous material concerns is not required at this time.

## Part IV – Permits and Commitments

### PERMITS CHECKLIST

**Permits** (mark all that apply)

Likely Required

**Army Corps of Engineers (404/Section10 Permit)**

Nationwide Permit (NWP)	<input type="checkbox"/>
Regional General Permit (RGP)	<input type="checkbox"/>
Individual Permit (IP)	<input type="checkbox"/>
Other	<input type="checkbox"/>

**IN Department of Environmental Management (401/Rule 5)**

Nationwide Permit (NWP)	<input type="checkbox"/>
Regional General Permit (RGP)	<input type="checkbox"/>
Individual Permit (IP)	<input type="checkbox"/>
Isolated Wetlands	<input type="checkbox"/>
Rule 5	<input type="checkbox"/>
Other	<input type="checkbox"/>

**IN Department of Natural Resources**

Construction in a Floodway	<input type="checkbox"/>
Navigable Waterway Permit	<input type="checkbox"/>
Other	<input type="checkbox"/>

**Mitigation Required**

	<input type="checkbox"/>
--	--------------------------

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Permits (mark all that apply)

Likely Required

**US Coast Guard Section 9 Bridge Permit**  
**Others (Please discuss in the discussion below)**


*List the permits likely required for the project and summarize why the permits are needed, including permits designated as "Other."*

As no impacts to water resources are expected and soil disturbance will be less than 1 acre, no permits are anticipated to be required.

Applicable recommendations provided by resource agencies are included in the Environmental Commitments section of this document. If permits are found to be necessary, the conditions of the permit will be requirements of the project and will supersede these recommendations.

It is the responsibility of the project sponsor to identify and obtain all required permits.

### ENVIRONMENTAL COMMITMENTS

*List all commitments and include the name of agency/organization requesting/requiring the commitment(s). Listed commitments should be numbered.*

**Firm:**

1. If the scope of work or permanent or temporary right-of-way amounts change, the INDOT Environmental Services Division (ESD) and the INDOT District Environmental Section will be contacted immediately. (INDOT ESD and INDOT Seymour District)
2. It is the responsibility of the project sponsor to notify school corporations and emergency services at least two weeks prior to any construction that would block or limit access. (INDOT ESD)
3. Impacts to the gas pipeline are not likely; however, if the pipeline is exposed during construction activities, work shall stop, and the contractor shall contact Center Point Energy. (INDOT ESD)
4. GENERAL AMM 1: Ensure all operators, employees, and contractors working in areas of known or presumed bat habitat are aware of all FHWA/FRA/FTA (Transportation Agencies) environmental commitments, including all applicable AMMs. (USFWS)
5. LIGHTING AMM 1: Direct temporary lighting away from suitable habitat during the active season. (USFWS)
6. LIGHTING AMM 2: When installing new or replacing existing permanent lights, use downward-facing, full cut-off lens lights (with same intensity or less for replacement lighting); or for those transportation agencies using the BUG system developed by the Illuminating Engineering Society, be as close to 0 for all three ratings with a priority of "uplight" of 0 and "backlight" as low as practicable.

**For Further Consideration:**

N/A

# APPENDICES

## APPENDIX A: INDOT Supporting Documentation

- Threshold Chart ..... A-1

## APPENDIX B: Graphics

- Project Location on a Topographic Map ..... B-1
- USGS Topographic Map ..... B-2
- 2016 Aerial Photograph ..... B-3
- Photograph Location Map ..... B-4
- Site Photographs; June 26, 2021 ..... B-5
- Proposed driveways to be converted to right-in / right-out ..... B-9
- Road Plans ..... B-10

## APPENDIX C: Early Coordination

- Sample Early Coordination letter; June 30 and July 8, 2021 ..... C-1
- Early Coordination Recipients ..... C-3
- IGS Response; June 30, 2021 ..... C-4
- Jefferson County Emergency Management Response; June 30, 2021 ..... C-6
- IDEM Groundwater Section Response; July 7, 2021 ..... C-7
- NRCS Response; July 15, 2021 ..... C-9
- IDNR-DFW Response; July 30, 2021 ..... C-10
- USFWS Official Species List; October 19, 2021 ..... C-11
- USFWS Concurrence Verification Letter; October 19, 2021 ..... C-17
- Madison Utilities Water and Sewer Coordination ..... C-26

## APPENDIX D: Section 106 of the National Historic Preservation Act

- MPPA Project Assessment Form; June 25, 2021 ..... D-1

## APPENDIX E: Red Flag and Hazardous Materials

- INDOT ESD SAM Email regarding Red Flag Investigation: November 18, 2021 ..... E-1
- Infrastructure ..... E-2
- Water Resources ..... E-3
- Hazardous Material Concerns ..... E-4
- IDNR Jefferson ETR Species List ..... E-5

## APPENDIX F: Water Resources

- NRCS Soils Map ..... F-1
- USFWS NWI Map ..... F-3
- IDNR Floodplain Portal Map ..... F-4

## APPENDIX G: Air Quality

- Fiscal Year (FY) 2022-2026 STIP ..... G-1

## APPENDIX H: Additional Studies

- Engineering Assessment; Final Signature by INDOT: August 4, 2020 ..... H-1
- LWCF County Property List for Indiana; Jefferson County; September 30, 2021 ..... H-10

# **APPENDIX A: INDOT Supporting Documentation**

## Categorical Exclusion Level Thresholds

	PCE	Level 1	Level 2	Level 3	Level 4 <sup>1</sup>
<b>Section 106</b>	Falls within guidelines of Minor Projects PA	“No Historic Properties Affected”	“No Adverse Effect”	-	“Adverse Effect” Or Historic Bridge involvement <sup>2</sup>
<b>Stream Impacts<sup>3</sup></b>	No construction in waterways or water bodies	< 300 linear feet of stream impacts	≥ 300 linear feet of stream impacts	-	USACE Individual 404 Permit <sup>4</sup>
<b>Wetland Impacts<sup>3</sup></b>	No adverse impacts to wetlands	< 0.1 acre	-	< 1.0 acre	≥ 1.0 acre
<b>Right-of-way<sup>5</sup></b>	Property acquisition for preservation only or none	< 0.5 acre	≥ 0.5 acre	-	-
<b>Relocations</b>	None	-	-	< 5	≥ 5
<b>Threatened/Endangered Species (Species Specific Programmatic for Indiana bat &amp; northern long eared bat)*</b>	“No Effect”, “Not likely to Adversely Affect” (With select AMMs <sup>6</sup> )	“Not likely to Adversely Affect” (With any AMMs or commitments)	-	“Likely to Adversely Affect”	Project does not fall under Species Specific Programmatic <sup>7</sup>
<b>Threatened/Endangered Species (Any other species)*</b>	Falls within guidelines of USFWS 2013 Interim Policy or “No Effect”	“Not likely to Adversely Affect”	-	-	“Likely to Adversely Affect”
<b>Environmental Justice</b>	No disproportionately high and adverse impacts	-	-	-	Potential <sup>8</sup>
<b>Sole Source Aquifer</b>	No Detailed Groundwater Assessment	-	-	-	Detailed Groundwater Assessment
<b>Floodplain</b>	No Substantial Impacts	-	-	-	Substantial Impacts
<b>Section 4(f) Impacts</b>	None	-	-	-	Any <sup>9</sup>
<b>Section 6(f) Impacts</b>	None	-	-	-	Any
<b>Permanent Traffic Alteration</b>	None	-	-	-	Any
<b>Noise Analysis Required</b>	No	-	-	-	Yes
<b>Air Quality Analysis Required</b>	No	-	-	-	Yes <sup>10</sup>
<b>Approval Level</b>					
<ul style="list-style-type: none"> <li>• District Env. (DE)</li> <li>• Env. Serv. Div. (ESD)</li> <li>• FHWA</li> </ul>	Concurrence by DE or ESD	DE or ESD	DE or ESD	DE and/or ESD	DE and/or ESD; and FHWA

<sup>1</sup> Coordinate with INDOT Environmental Services Division. INDOT will then coordinate with the appropriate FHWA Environmental Specialist.

<sup>2</sup> Any involvement with a bridge processed under the Historic Bridge Programmatic Agreement.

<sup>3</sup> Total permanent impacts to streams (linear feet) and wetlands (acres).

<sup>4</sup> US Army Corps of Engineers Individual 404 Permit

<sup>5</sup> Total permanent and temporary right-of-way. This does not include reacquisition of existing apparent right-of-way.

<sup>6</sup> Avoidance and Mitigation Measures (AMMs) determined by the IPAC determination key to be required that are not tree AMMs, bridge AMMs, or structure AMMs.

<sup>7</sup> Projects that do not fall under a Species Specific Programmatic and results in a “Likely to Adversely Affect”. Other findings can be processed as a lower level CE.

<sup>8</sup> Potential for causing a disproportionately high and adverse impact.

<sup>9</sup> Section 4(f) use resulting in an Individual, Programmatic, or *de minimis* evaluation. The only exception is a *de minimis* evaluation for historic properties (Effective January 2, 2020). If a historic property *de minimis* and no other use, mark the *None* column.

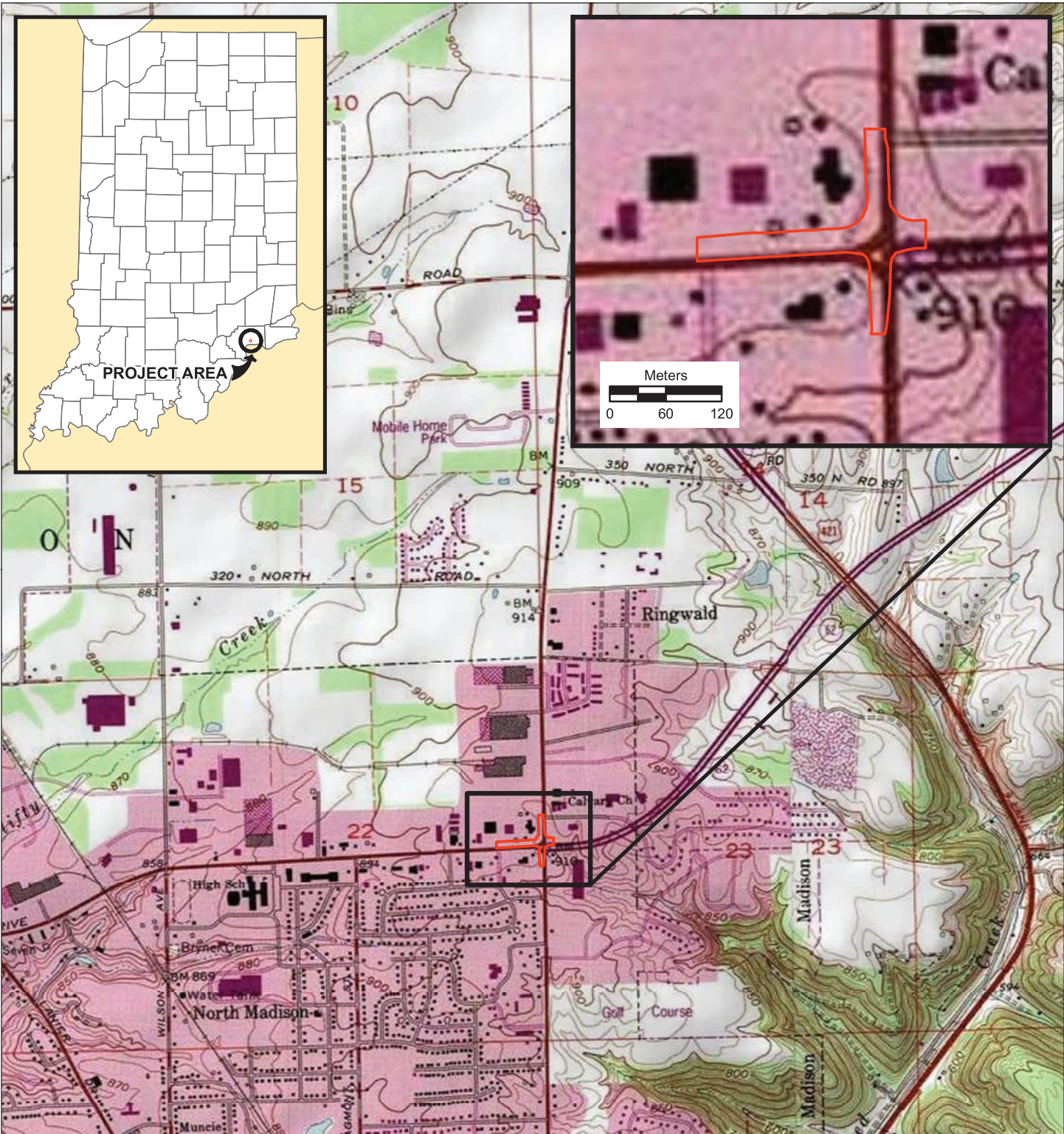
<sup>10</sup> Hot Spot Analysis and/or MSAT Quantitative Emission Analysis.

\* Includes the threatened/endangered species critical habitat

Note: Substantial public or agency controversy may require a higher-level NEPA document.

# **APPENDIX B: Graphics**





 Project Area

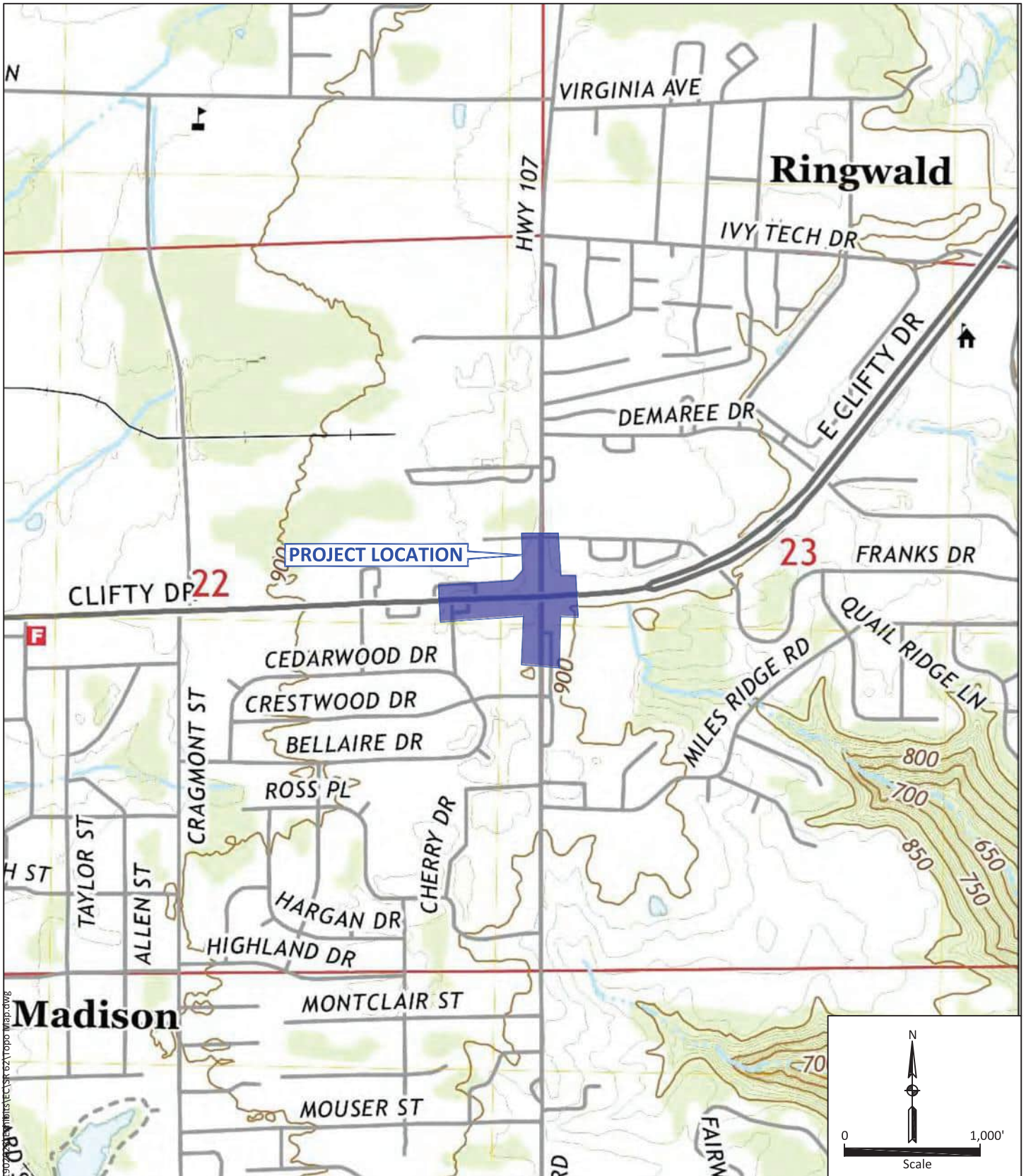
Project Location on a Topographic Map  
 SR 62 and Michigan Road  
 Access Management  
 Madison Township, Jefferson County, Indiana  
 Des. No. 1902023  
 Metric Project No. 18-0070-13  
 Map Date: 10/26/2021

All Locations Approximate



1 cm = 240 m






P:\2018\18-0070 - Strand-INDOT-RFP-0180407-c-Seymour-Dist-PDS\4 - Deliverables\Task 13 - Des.  
 Project\Map\18-0070-Strand-INDOT-RFP-0180407-c-Seymour-Dist-PDS\4 - Deliverables\Task 13 - Des.  
 18-0070-Strand-INDOT-RFP-0180407-c-Seymour-Dist-PDS\4 - Deliverables\Task 13 - Des.

Source: <https://ngmdb.usgs.gov/topoview/viewer/#12/39.5378/-86.2918>

**USGS Topographic Map**  
 Access Management Project  
 SR 62 and Michigan Road  
 Madison, Jefferson County,  
 Indiana  
 Des. No. 1902023  
 Metric Project #18-0070 Task 13

All locations approximate  
 Base map:  
 2019 Clifty Falls, IN  
 7.5 Minute Quadrangle



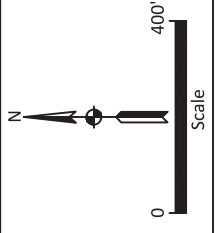
Drawn by: ILJ  
 Checked by: SC  
 Approved by: JRP  
 Date: June, 2021



Source: Google Earth Pro

**2016 Aerial Photograph**  
 Access Management Project  
 SR 62 and Michigan Road  
 Madison, Jefferson County,  
 Indiana  
 Des. No. 1902023  
 Metric Project #18-0070 Task 13

Note: All locations are approximate



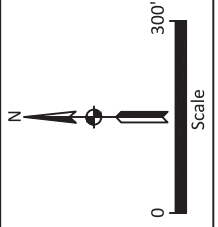
	Drawn by: <u>  JLJ  </u>
	Checked by: <u>  SC  </u>
	Approved by: <u>  JRP  </u>
	Date: <u>  June, 2021  </u>



Source: Google Earth Pro

**Photograph Location Map**  
 Access Management Project  
 SR 62 and Michigan Road  
 Madison, Jefferson County,  
 Indiana  
 Des. No. 1902023  
 Metric Project #18-0070 Task 13

Note: All locations are approximate

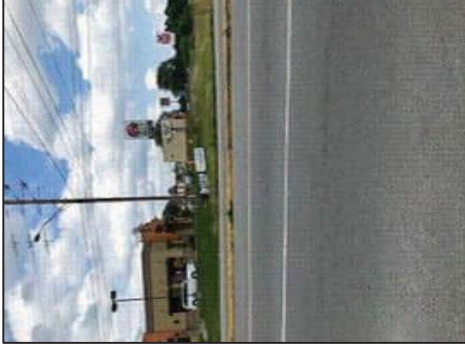


**METRIC ENVIRONMENTAL**

Drawn by: JJJ  
 Checked by: SC  
 Approved by: JRP  
 Date: June 26, 2021



1. View near the intersection of SR 62 and Michigan Road, looking southeast.



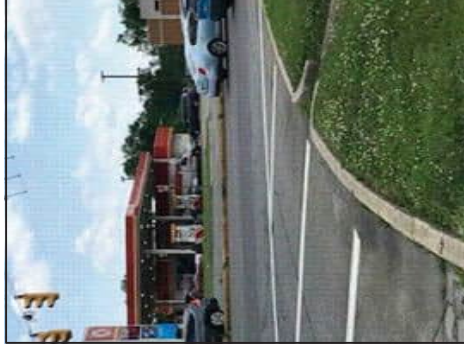
2. View near the intersection of SR 62 and Michigan Road, looking east.



3. View near the intersection of SR 62 and Michigan Road, looking southeast.



4. View along SR 62, looking southeast.



5. View near the intersection of SR 62 and Michigan Road, looking southwest.



6. View near the intersection of SR 62 and Michigan Road, looking west.

**Site Photographs - June 26, 2021**  
Access Management Project  
SR 62 and Michigan Road  
Madison, Jefferson County, Indiana  
Des. No. 1902023  
Metric Project #18-0070 Task 13



Drawn by: ILJ  
Checked by: SC  
Approved by: JRP  
Date: June 26, 2021



7. View of drainage pipe north of the intersection of SR 62 and Michigan Road, looking south



8. View of drainage ditch north of the intersection of SR 62 and Michigan Road, looking south



9. View from the intersection of SR 62 and Michigan Road, looking east.



10. View from the intersection of SR 62 and Michigan Road, looking north.



11. View from the intersection of SR 62 and Michigan Road, looking north.



12. View along Michigan Road, looking south.

**Site Photographs - June 26, 2021**  
 Access Management Project  
 SR 62 and Michigan Road  
 Madison, Jefferson County, Indiana  
 Des. No. 1902023  
 Metric Project #18-0070 Task 13



Drawn by: ILJ  
 Checked by: SC  
 Approved by: JRP  
 Date: June 26, 2021



13. View from Michigan Road, looking northwest.



14. View along Michigan Road, looking northwest.



14. View along SR 62, looking east.



16. View of stormwater drain.



17. View along SR 62, looking west.



18. View from the intersection of SR 62 and Michigan road, looking north.

**Site Photographs - June 26, 2021**  
Access Management Project  
SR 62 and Michigan Road  
Madison, Jefferson County, Indiana  
Des. No. 1902023  
Metric Project #18-0070 Task 13



Drawn by: ILJ  
Checked by: SC  
Approved by: JRP  
Date: June 26, 2021



19. View from the intersection of SR 62 and Michigan Road, looking north.



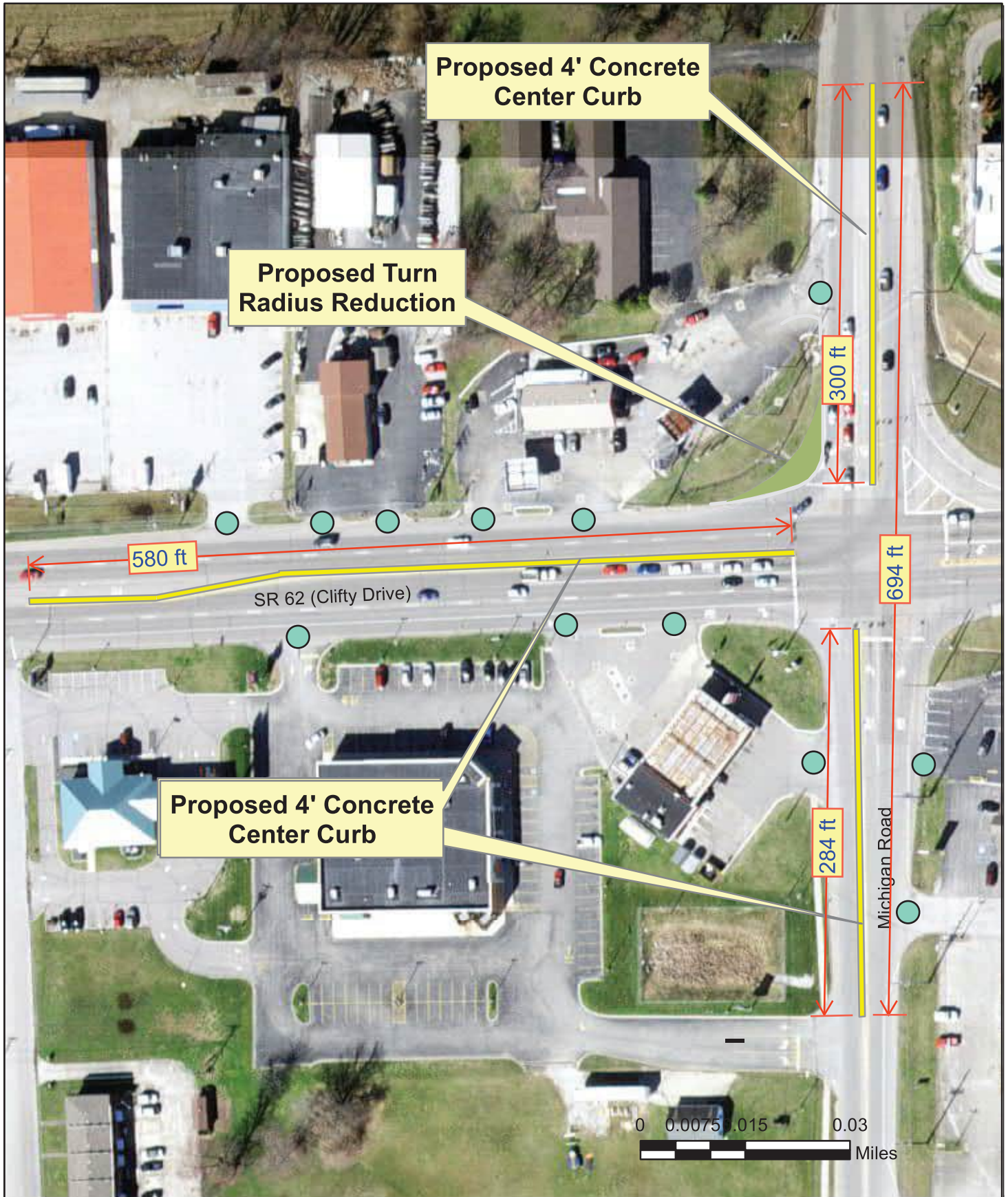
20. View from the intersection of SR 62 and Michigan Road, looking southeast.

**Site Photographs - June 26, 2021**  
Access Management Project  
SR 62 and Michigan Road  
Madison, Jefferson County, Indiana  
Des. No. 1902023  
Metric Project #18-0070 Task 13



Drawn by: ILJ  
Checked by: SC  
Approved by: JRP  
Date: June 26, 2021





**SR 62 PROPOSED IMPROVEMENTS**

● - Driveway to be converted to Right-in, Right-out

**DES. NO. 1902023  
CITY OF MADISON  
JEFFERSON COUNTY, INDIANA**



**FIGURE 5  
4060.554**

PROJECT	DESIGNATION
2101170	2101170
CONTRACT	BRIDGE FILE NO.
R-42558	N/A

# INDIANA DEPARTMENT OF TRANSPORTATION



## ROAD PLANS

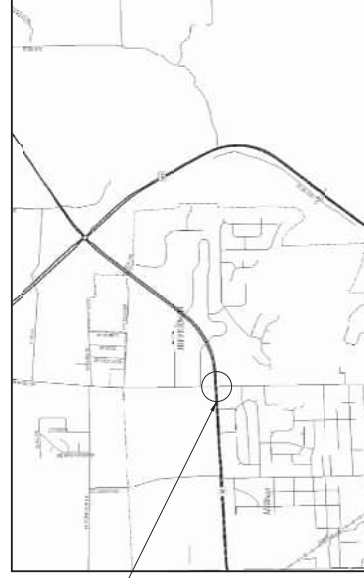
ROUTE: SR 62 AT: RP 194+75

PROJECT NO. 2101170 P.E. 2101170

2101170 CONST.

**Note: Des. No. on plans is incorrect and shall be corrected on Final Design Plans.**

Access Management on SR 62  
In Sections 22 & 23, T-4-N, R-10 E, Jefferson County, Madison, IN



Project Location

### TRAFFIC DATA

SR 62		MICHIGAN RD	
ADPT.	2019	2073 V.S.A.	11,284 V.S.D.
DAV.	2019	23.0%	1.0%
DIRECTIONAL DISTRIBUTION	50 %	50 %	51 %

### DESIGN DATA

DESIGN SPEED:	40 MPH	40 MPH
PROJECT DESIGN CRITERIA:	SR INTERSTATE	SR (NON-INTERSTATE)
DESIGN CLASSIFICATION:	RURAL/URBAN	URBAN (INTERSTATE)
TERRAIN:	URBAN (INTERSTATE)	URBAN (INTERSTATE)
LEVEL:	URBAN (INTERSTATE)	URBAN (INTERSTATE)
ACCESS CONTROL:	NONE	NONE



PROJECT LOCATION SHOWN BY  
FRANKLIN AND JEFFERSON COUNTY

LATITUDE: 38° 46' 31" LONGITUDE: 85° 23' 15"

BRIDGE LENGTH: \_\_\_\_\_ ML  
ROADWAY LENGTH: \_\_\_\_\_ ML  
TOTAL LENGTH: \_\_\_\_\_ ML  
MAX. GRADE: \_\_\_\_\_ %

HUC: 05090203060050



SCALE: 1" = 2500'

INDIANA DEPARTMENT OF TRANSPORTATION  
STANDARD SPECIFICATIONS FOR BRIDGES  
TO BE USED WITH THESE PLANS

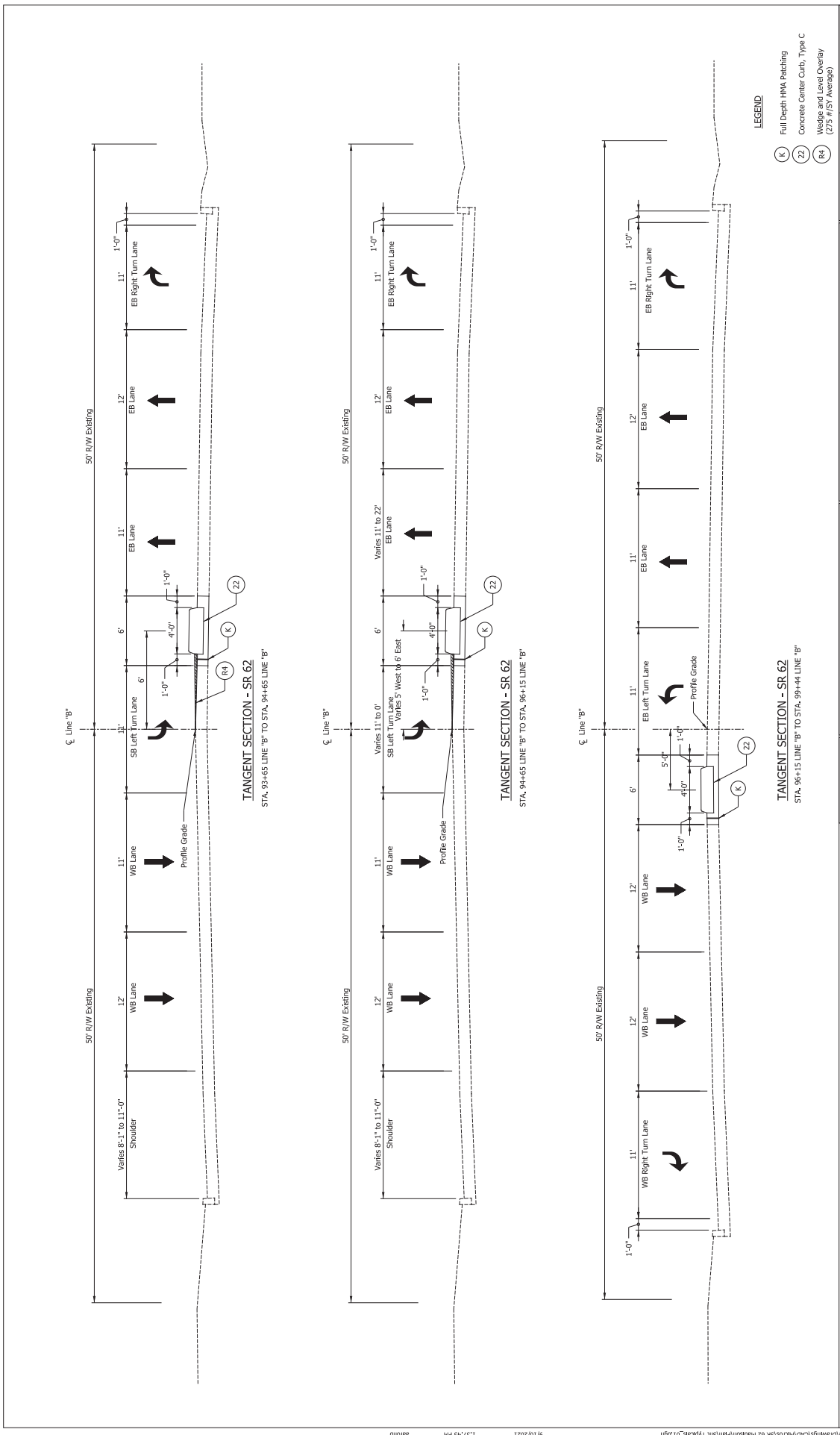
PLANS PREPARED BY:	STRAND ASSOCIATES, INC.	(812)372-5911
	629 WASHINGTON ST., COLUMBUS, IN 47201	PHONE NUMBER
CERTIFIED BY:		DATE
APPROVED FOR LETTING:		DATE



INDIANA DEPARTMENT OF TRANSPORTATION

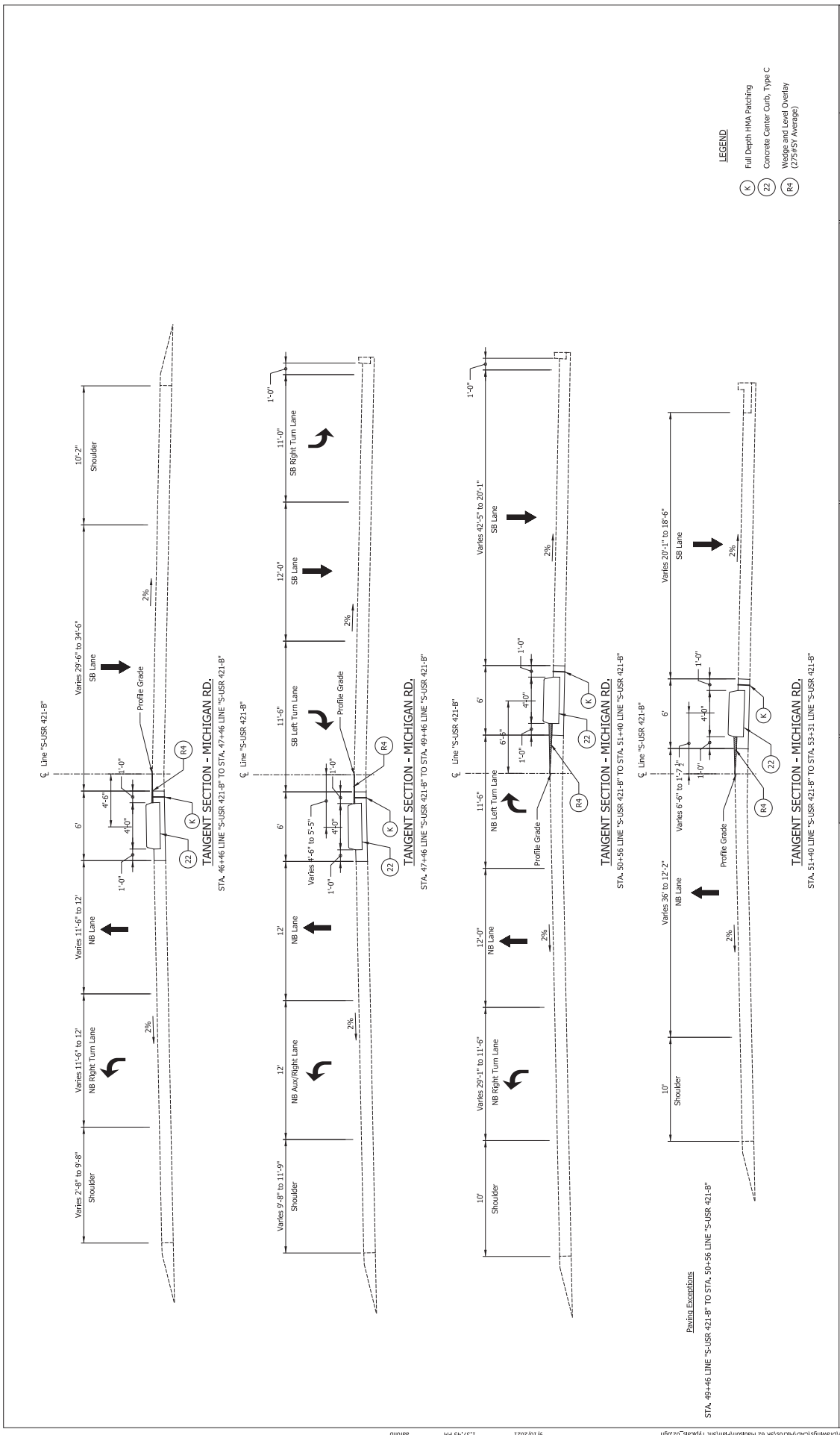
B-10

BRIDGE FILE NO.	N/A
DESIGNATION	2101170
SURVEY BOOK	
SHEETS	1 OF 34
CONTRACT	R-42558
PROJECT	2101170



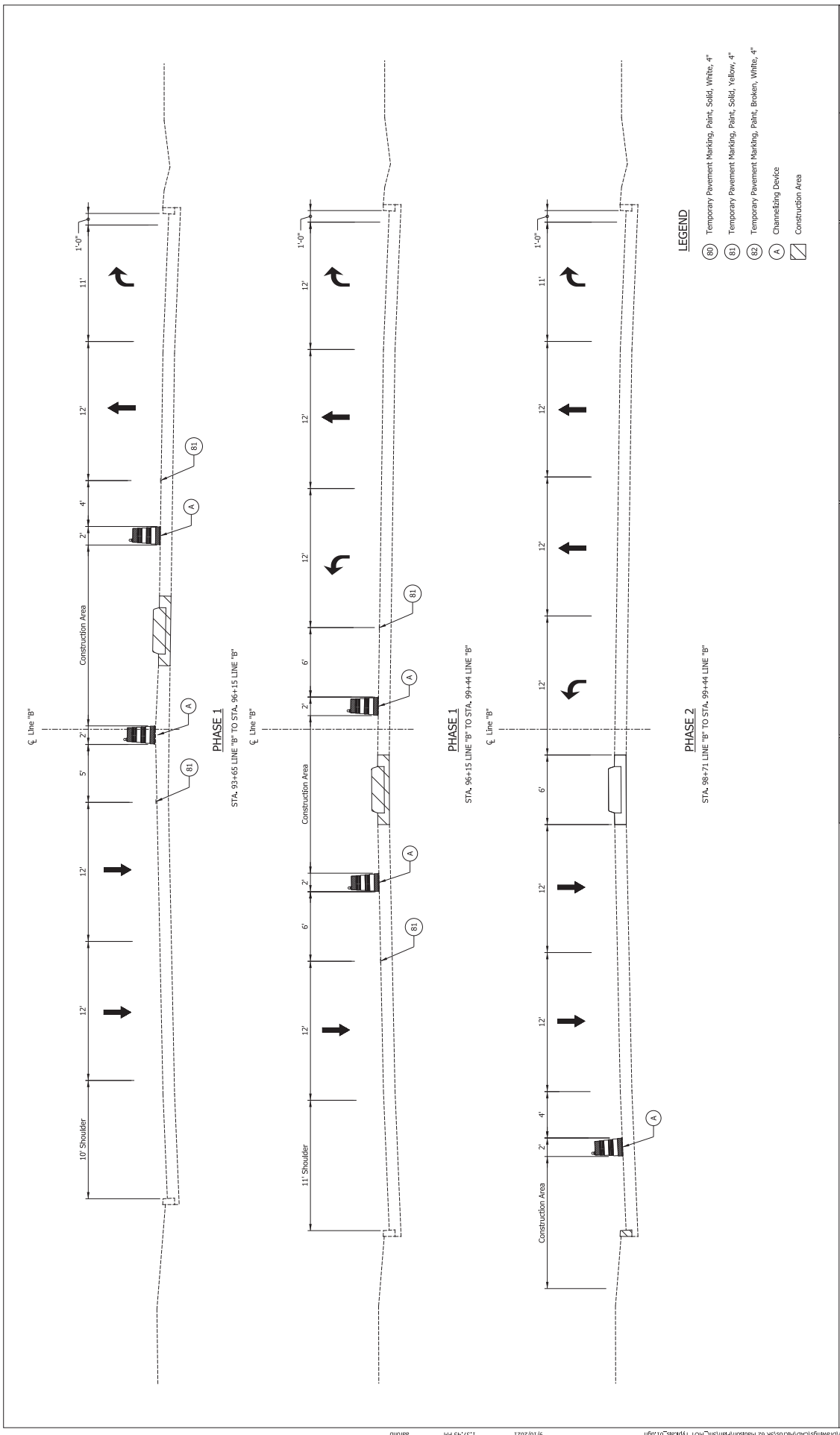
- LEGEND**
- (K) Full Depth HMA Patching
  - (22) Concrete Center Curb, Type C
  - (R4) Wedge and Level Overlay (275 #/SY Average)

RECOMMENDED FOR APPROVAL		DESIGN ENGINEER	DATE
INDIANA DEPARTMENT OF TRANSPORTATION		DESIGNER: JMR	CHECKER: JMR
SR 62 - LINE "B"		DRAWN: ACB	
TYPICAL CROSS SECTIONS		PROJECT NO.	2101170
		CONTRACT NO.	R-42588
		SHEET BOOK NO.	3 OF 34
		DESIGNATION NO.	210170
		VERTICAL SCALE	1" = 1'-0"
		HORIZONTAL SCALE	1" = 1'-0"
		BRIDGE FILE NO.	N/A



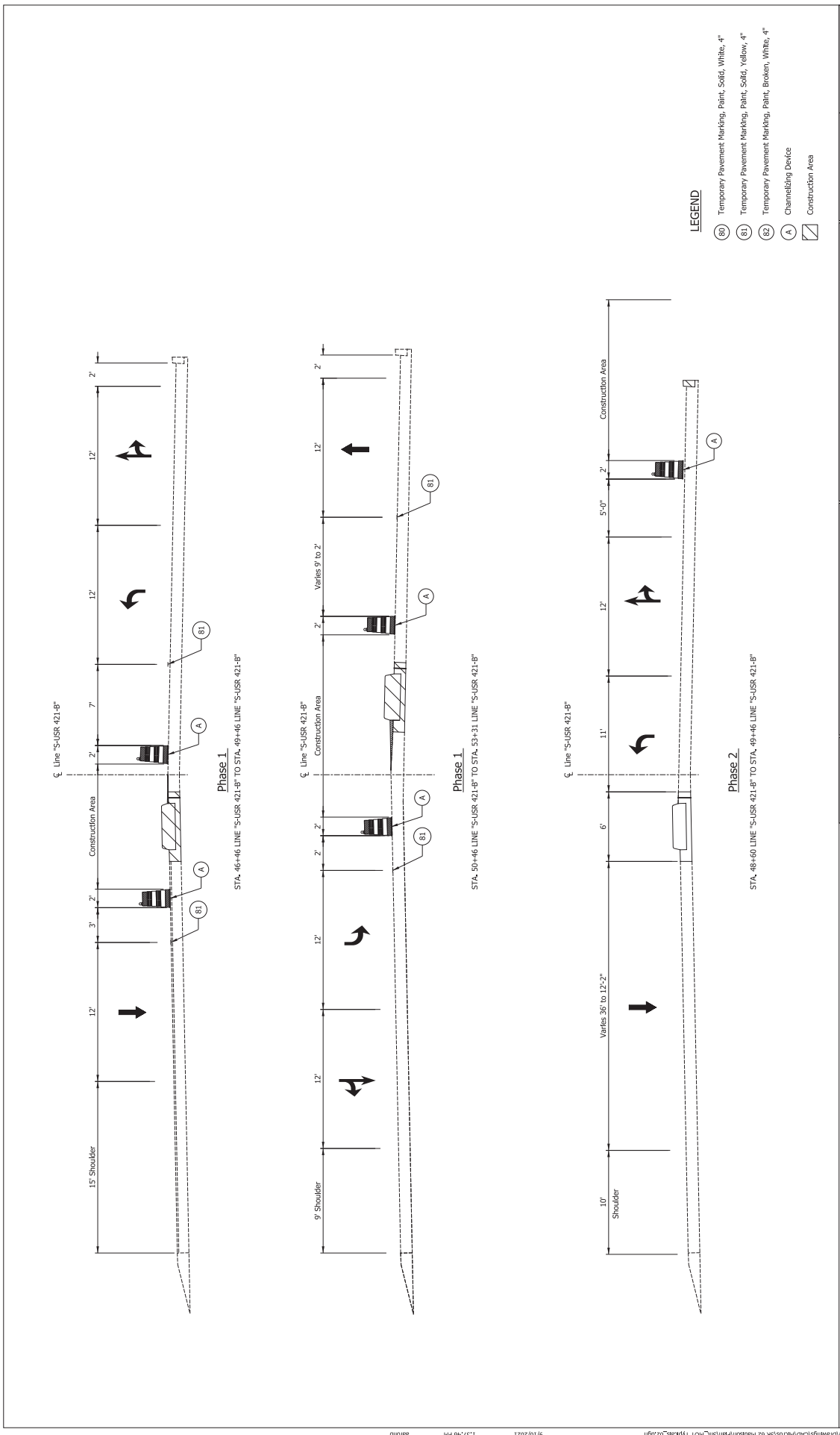
- LEGEND**
- (K) Full Depth HMA Patching
  - (Z) Concrete Center Curb, Type C
  - (R4) Wedge and Level Overlay (275#5Y Average)

RECOMMENDED FOR APPROVAL		INDIANA DEPARTMENT OF TRANSPORTATION	
DESIGN ENGINEER	DATE	TYPICAL CROSS SECTIONS	
DESIGNER: JMR		MICHIGAN RD. - LINE "S-USR 421-B"	
CHECKER: JMR			
BRIDGE FILE NO.		R-42588	
1/4" = 1'-0"		210170	
DESIGNATION NO.		210170	
1/4" = 1'-0"		4	
SHEET BOOK NO.		47	
CONTRACT NO.		PROJECT NO.	
R-42588		210170	



- LEGEND**
- (80) Temporary Pavement Marking, Paint, Solid, White, 4"
  - (81) Temporary Pavement Marking, Paint, Solid, Yellow, 4"
  - (82) Temporary Pavement Marking, Paint, Broken, White, 4"
  - (A) Channelizing Device
  - ▨ Construction Area

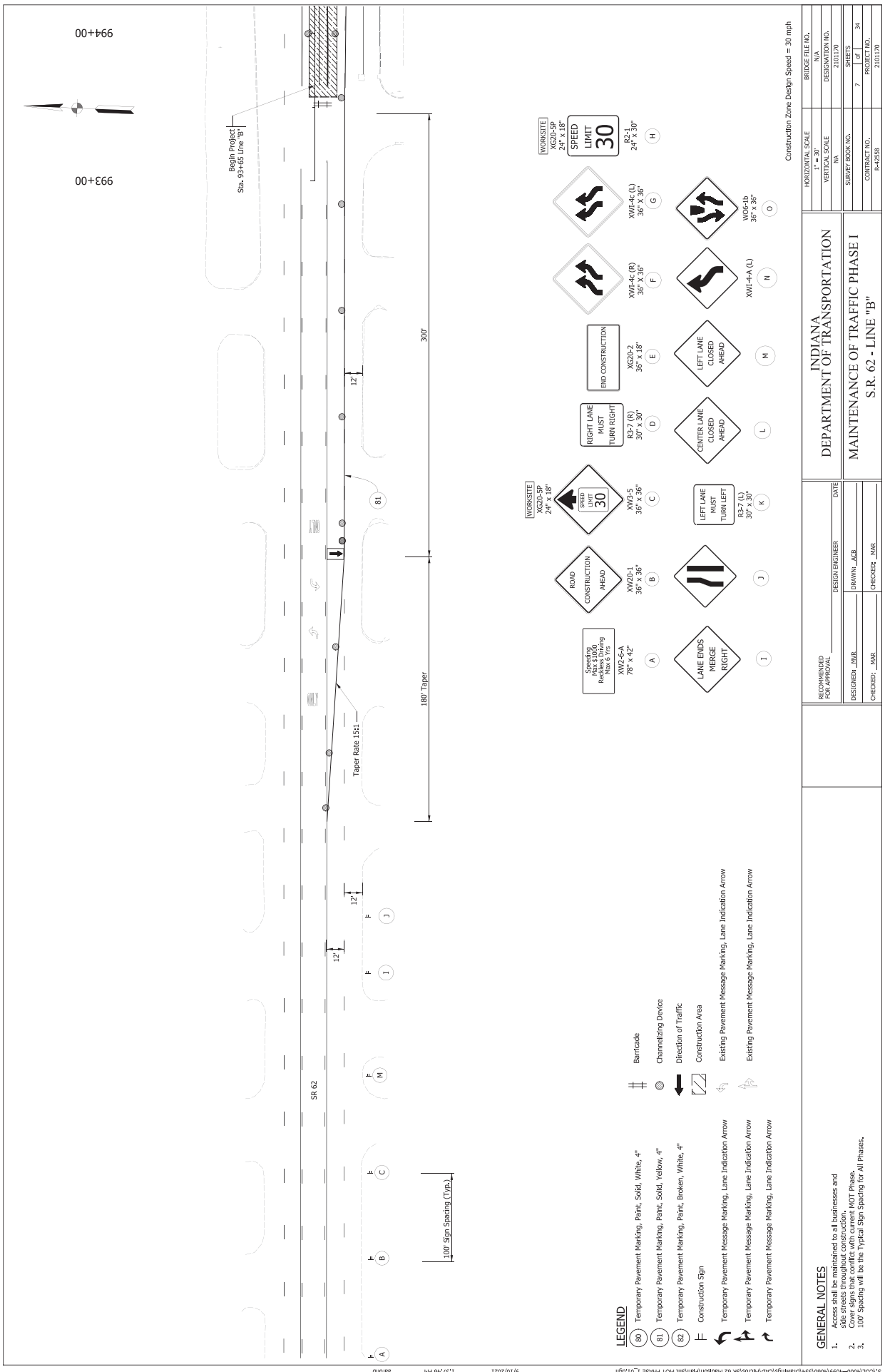
RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE	INDIANA DEPARTMENT OF TRANSPORTATION	HORIZONTAL SCALE	BRIDGE FILE NO.
	DESIGNER: JMR			1" = 1'-0"	N/A
DESIGNER: JMR	DRAWN: JCB		MOT TYPICAL CROSS SECTIONS SR 62 - LINE "B"	VERTICAL SCALE	DESIGNATION NO.
CHECKED: JMS	CHECKER: JMR			1" = 1'-0"	2101170
				SHEET BOOK NO.	PROJECT NO.
				5	47
				CONTRACT NO.	PROJECT NO.
				P-42538	2101170



**LEGEND**

- (80) Temporary Pavement Marking, Paint, Solid, White, 4"
- (81) Temporary Pavement Marking, Paint, Solid, Yellow, 4"
- (82) Temporary Pavement Marking, Paint, Broken, White, 4"
- (A) Channeling Device
- ☐ Construction Area

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE	INDIANA DEPARTMENT OF TRANSPORTATION
	DESIGNER: JMR	CHECKER: JMR	
MOT TYPICAL CROSS SECTIONS			MICHIGAN RD. - LINE "S-USR 421-B"
BRIDGE FILE NO.	N/A		
DESIGNATION NO.	210170		
PROJECT NO.	6		
CONTRACT NO.	R-42588		



**LEGEND**

- 80 Temporary Pavement Marking, Paint, Solid, White, 4"
- 81 Temporary Pavement Marking, Paint, Solid, Yellow, 4"
- 82 Temporary Pavement Marking, Paint, Broken, White, 4"
- Construction Sign
- Temporary Pavement Message Marking, Lane Indication Arrow
- Temporary Pavement Message Marking, Lane Indication Arrow
- Temporary Pavement Message Marking, Lane Indication Arrow

- Barricade
- Channelizing Device
- Direction of Traffic
- Construction Area
- Existing Pavement Message Marking, Lane Indication Arrow
- Existing Pavement Message Marking, Lane Indication Arrow

**GENERAL NOTES**

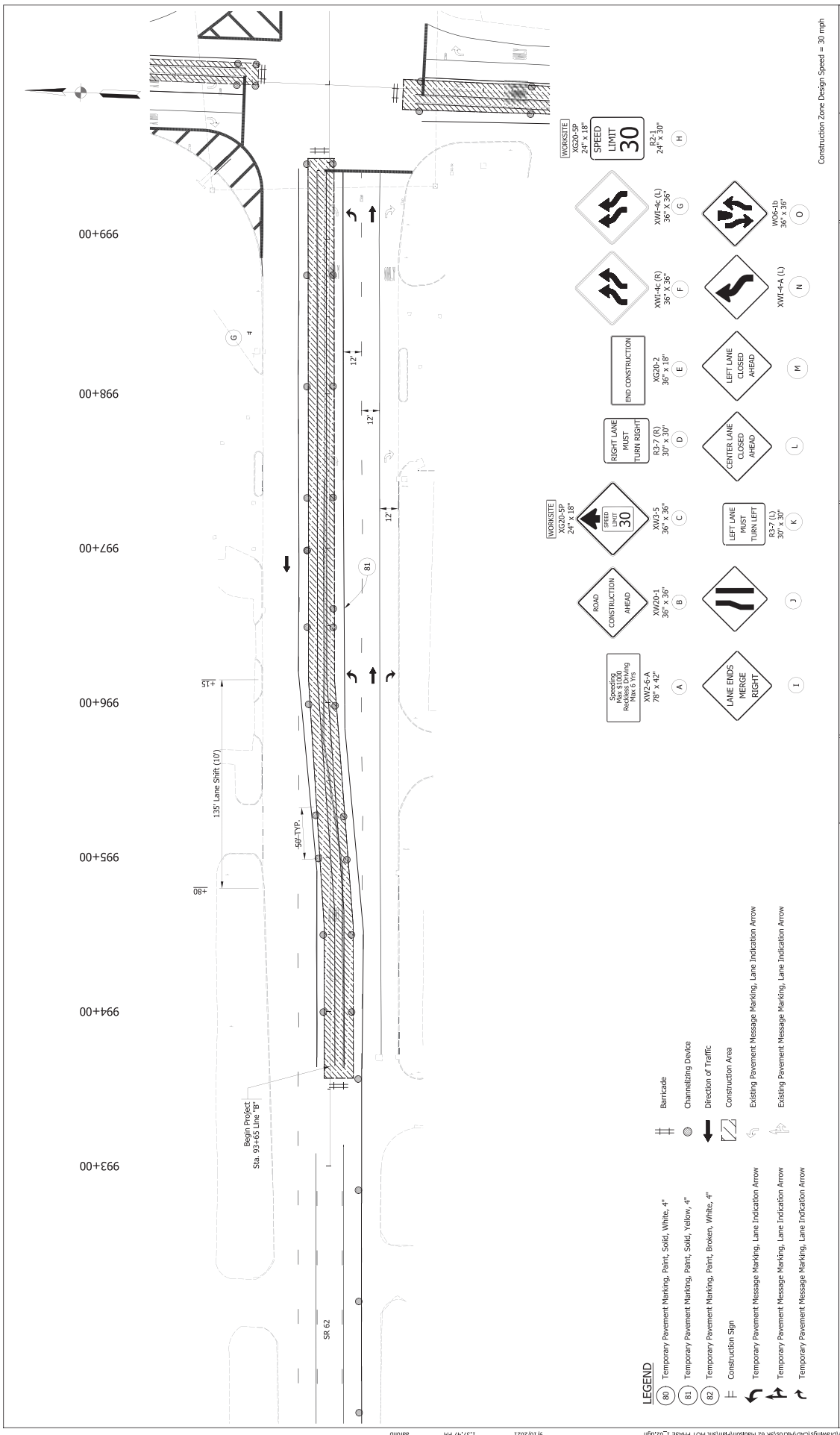
1. Access shall be maintained to all businesses and side streets throughout construction.
2. Cover signs that conflict with current NOT Phase.
3. 100' Spacing will be the Typical Sign Spacing for All Phases.

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNER: <u>JMR</u>	DRAWN: <u>ACB</u>	
CHECKED: <u>JMR</u>	CHECKER: <u>JMR</u>	

INDIANA  
DEPARTMENT OF TRANSPORTATION  
MAINTENANCE OF TRAFFIC PHASE I  
S.R. 62 - LINE "B"

HORIZONTAL SCALE	BRIDGE FILE NO.
1" = 30'	N/A
VERTICAL SCALE	DESIGNATION NO.
1" = 10'	210170
SHEET BOOK NO.	SHEET NO.
7	34
PROJECT NO.	CONTRACT NO.
210170	R-42588

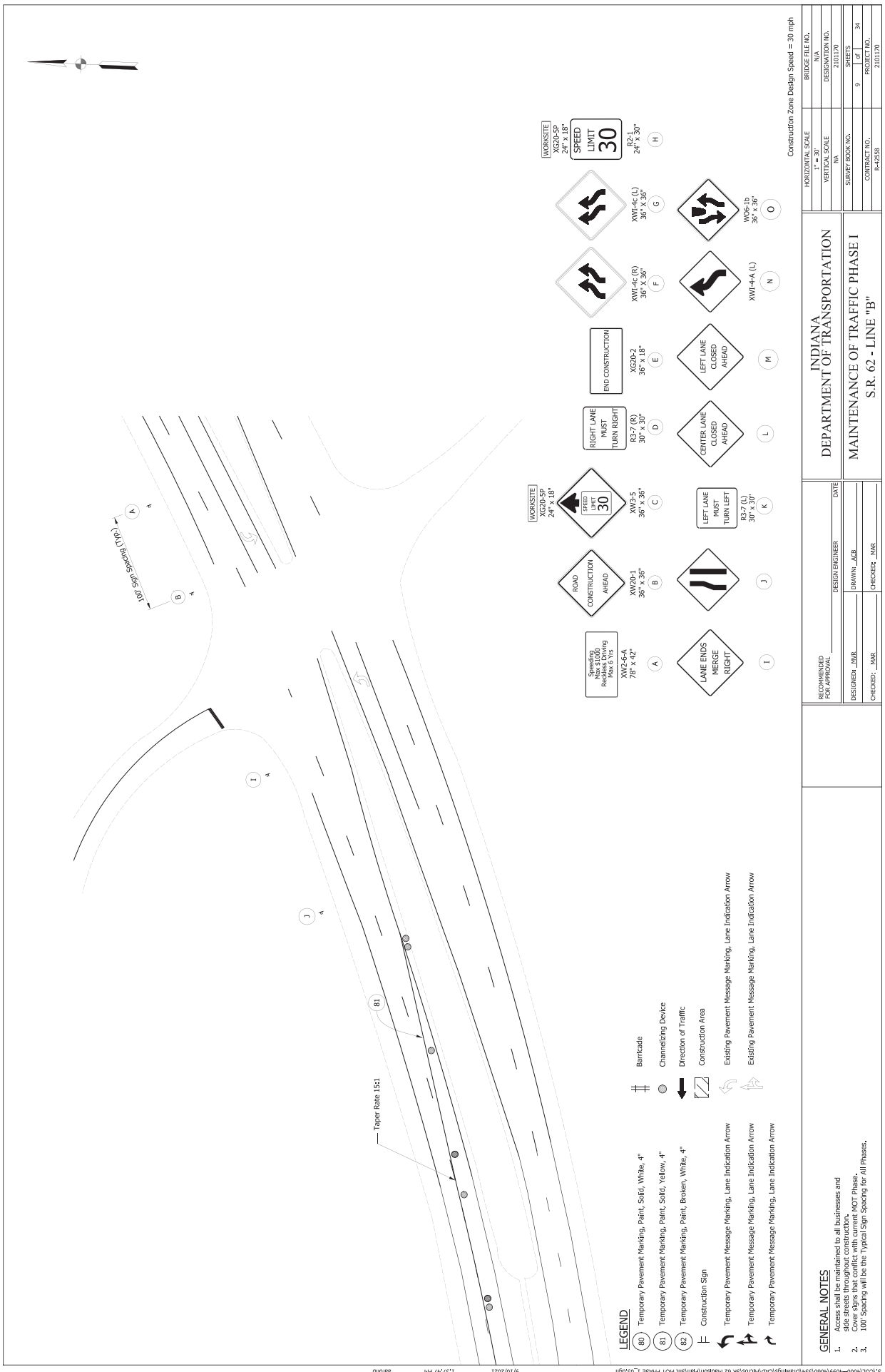
Construction Zone Design Speed = 30 mph



Construction Zone Design Speed = 30 mph

	INDIANA DEPARTMENT OF TRANSPORTATION	
RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNER: JMR	DRAWN: AEB	CHECKED: JMR
CHECKED: JMR	MAINTENANCE OF TRAFFIC PHASE I S.R. 62 - LINE "B"	
HORIZONTAL SCALE 1" = 30'	BRIDGE FILE NO. N/A	DESIGNATION NO. 210170
VERTICAL SCALE 1" = 30'	SHEET BOOK NO. 8	SHEET NO. 34
PROJECT NO. R-42558	PROJECT NO. 210170	PROJECT NO. 210170





**LEGEND**

- (80) Temporary Pavement Marking, Paint, Solid, White, 4"
- (81) Temporary Pavement Marking, Paint, Solid, Yellow, 4"
- (82) Temporary Pavement Marking, Paint, Broken, White, 4"
- Construction Sign
- Temporary Pavement Message Marking, Lane Indication Arrow
- Temporary Pavement Message Marking, Lane Indication Arrow
- Temporary Pavement Message Marking, Lane Indication Arrow

**LEGEND**

- Barricade
- Channelizing Device
- Direction of Traffic
- Construction Area
- Existing Pavement Message Marking, Lane Indication Arrow
- Existing Pavement Message Marking, Lane Indication Arrow

- (A) XW2-5-A  
78" x 42"  
Max 6 Yrs
- (B) XW20-1  
30" x 36"
- (C) XW2-5  
30" x 36"
- (D) R3-7 (R)  
30" x 30"
- (E) XG20-2  
36" x 18"
- (F) XW1-4c (R)  
36" x 36"
- (G) XW1-4c (L)  
36" x 36"
- (H) SPEED LIMIT 30  
24" x 30"
- (I) LANE ENDS MERGE RIGHT
- (J) ROAD CONSTRUCTION AHEAD
- (K) R3-7 (L)  
30" x 30"
- (L) CENTER LANE CLOSED AHEAD
- (M) LEFT LANE CLOSED AHEAD
- (N) XW1-4-A (L)
- (O) W3-5  
36" x 36"

Construction Zone Design Speed = 30 mph

WORKSITE	XG20-SP	24" x 18"	BRIDGE FILE NO.	N/A
SCALE	1" = 30'		DESIGNATION NO.	210170
DESIGNER	JR		SHEET BOOK NO.	17
CHECKER	JMR		PROJECT NO.	9
			CONTRACT NO.	R-42588
				210170

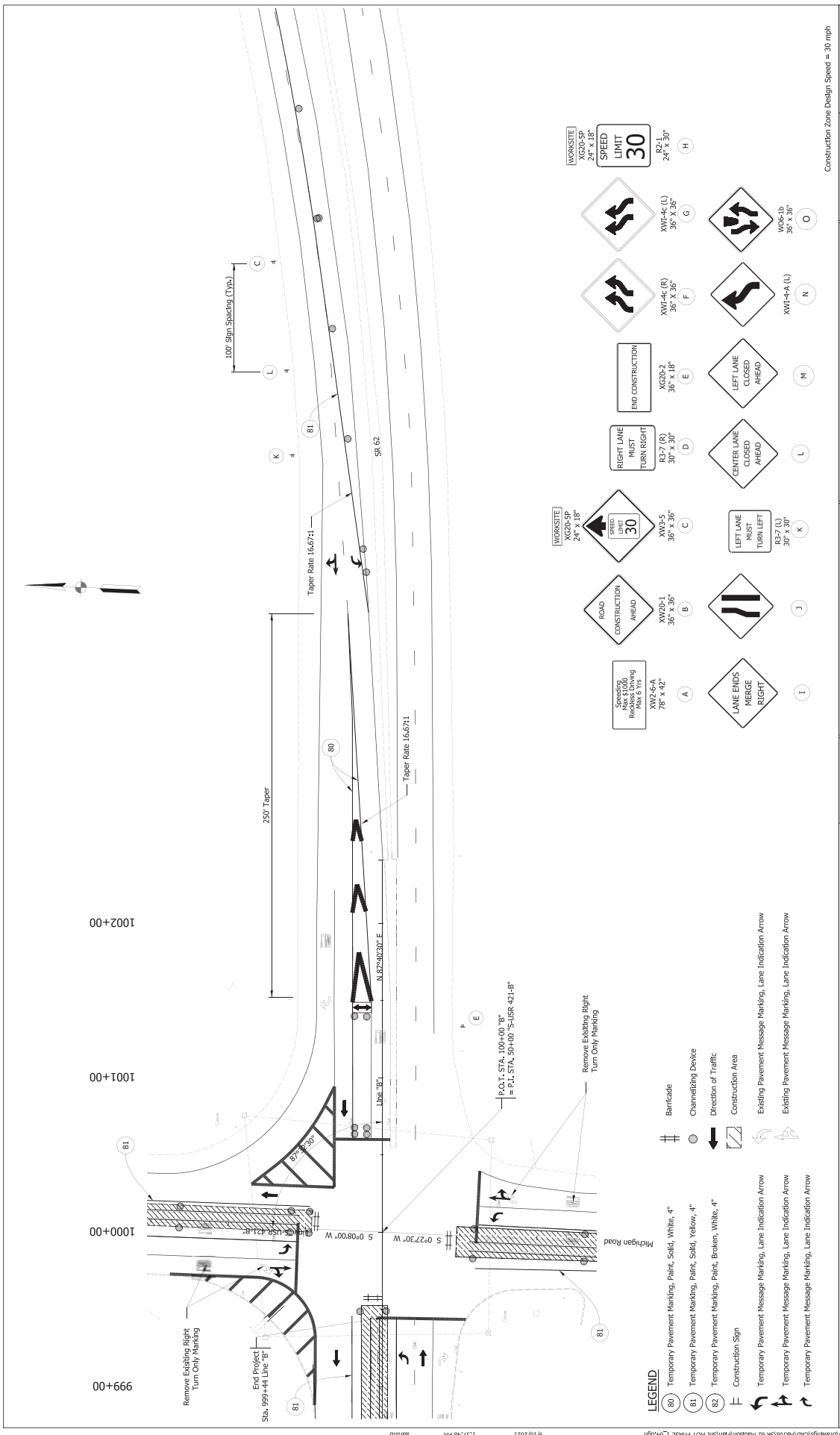
INDIANA  
DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC PHASE I  
S.R. 62 - LINE "B"

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNER: JR	DRAWN: JCB	
CHECKED: JMR	CHECKER: JMR	

**GENERAL NOTES**

- Access shall be maintained to all businesses and residences.
- Cover signs that conflict with current MOT Phase.
- 100' Spacing will be the Typical Sign Spacing for All Phases.



**WORKSITE**  
 XG20-SP  
 24" x 18"

**SPEED LIMIT**  
 30  
 24" x 30"

**RIGHT LANE MUST TURN RIGHT**  
 R3-7 (R)  
 30" x 30"

**ROAD CONSTRUCTION AHEAD**  
 XW20-1  
 30" x 36"

**LANE ENDS MERGE RIGHT**  
 W2-5-A  
 78" x 42"

**LANE ENDS MERGE LEFT**  
 R3-7 (L)  
 30" x 30"

**LEFT LANE MUST TURN LEFT**  
 R3-7 (L)  
 30" x 30"

**RIGHT LANE MUST TURN RIGHT**  
 R3-7 (R)  
 30" x 30"

**END CONSTRUCTION**  
 XG20-2  
 30" x 18"

**LEFT LANE CLOSED AHEAD**  
 XW1-4-A (L)  
 36" x 36"

**LANE ENDS MERGE RIGHT**  
 W2-5  
 36" x 36"

**LANE ENDS MERGE LEFT**  
 W2-5  
 36" x 36"

**RIGHT LANE CLOSED AHEAD**  
 XW1-4-A (R)  
 36" x 36"

**ROAD CONSTRUCTION AHEAD**  
 XW20-1  
 30" x 36"

**LANE ENDS MERGE RIGHT**  
 W2-5  
 36" x 36"

**LANE ENDS MERGE LEFT**  
 W2-5  
 36" x 36"

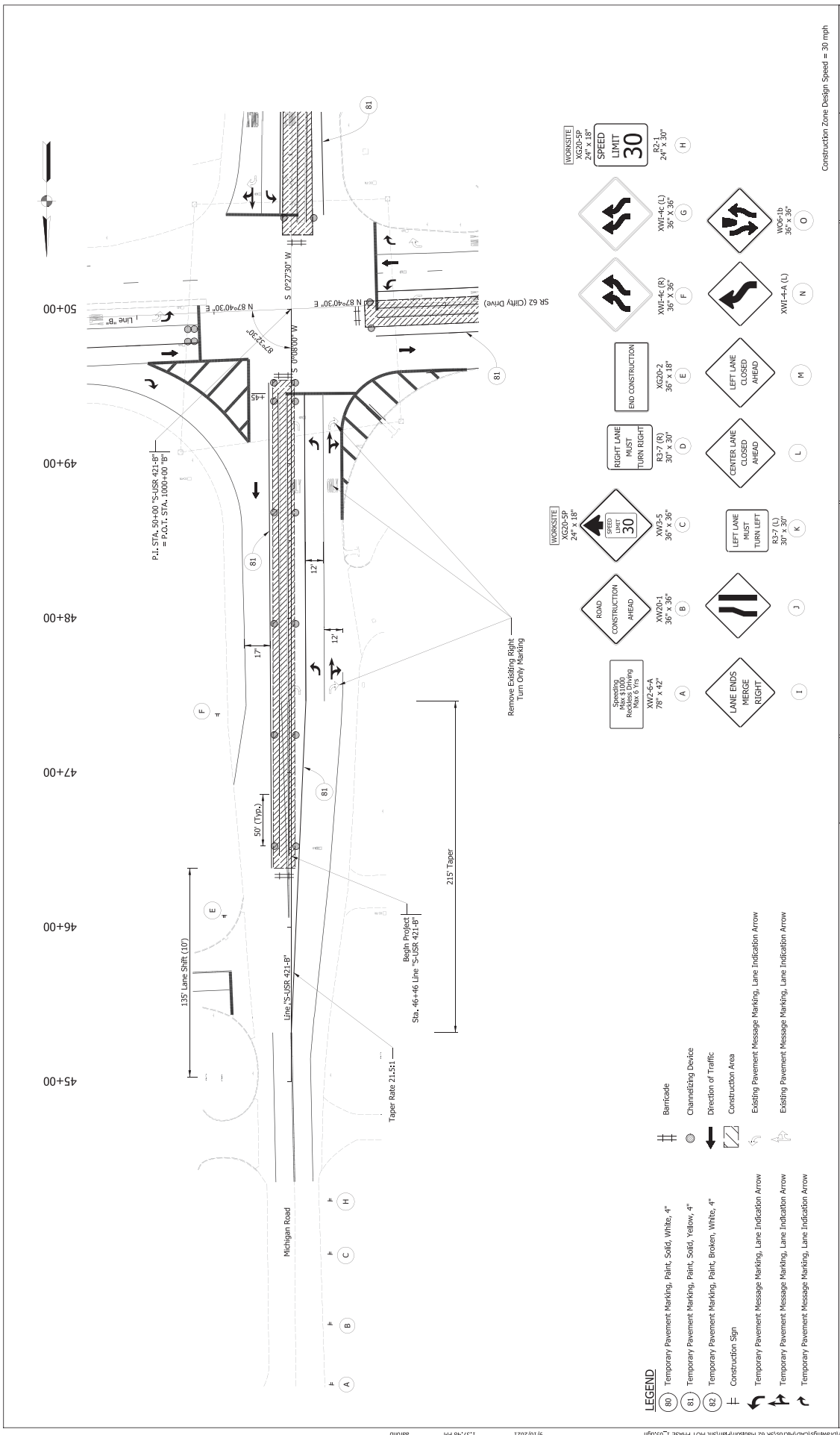
- LEGEND**
- (60) Temporary Pavement Marking, Paint, Solid, White, 4"
  - (61) Temporary Pavement Marking, Paint, Solid, Yellow, 4"
  - (62) Temporary Pavement Marking, Paint, Broken, White, 4"
  - Construction Sign
  - Temporary Pavement Message Marking, Lane Indication Arrow
  - Temporary Pavement Message Marking, Lane Indication Arrow
  - Temporary Pavement Message Marking, Lane Indication Arrow
  - Barricade
  - Channelizing Device
  - Direction of Traffic
  - Construction Area
  - Existing Pavement Message Marking, Lane Indication Arrow
  - Existing Pavement Message Marking, Lane Indication Arrow
  - Remove Existing Right Turn Only Marking

**GENERAL NOTES**

- Access shall be maintained to all businesses and residences.
- Cover signs that conflict with current MOT Phase.
- 100' Spacing will be the Typical Sign Spacing for All Phases.

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
	DRAWN	ACB
CHECKED: -MIR	CHECKED: -MIR	
INDIANA DEPARTMENT OF TRANSPORTATION <b>MAINTENANCE OF TRAFFIC PHASE I</b> <b>S.R. 62 - LINE "B"</b>		
HORIZONTAL SCALE	BRIDGE FILE NO.	
VERTICAL SCALE	DESIGNATION NO.	
	PROJECT NO.	10
	CONTRACT NO.	2101170
	PROJECT NO.	R-42588

S:\C01\4000\991\060155\Drawings\CD\Main\SR 62\Main\Print\MOT Phase I.dgn  
 5/10/2021 1:37:48 PM  
 aaron



Construction Zone Design Speed = 30 mph

WORKSITE	BRIDGE FILE NO.
XS20-5P	N/A
24' x 18'	DESIGNATION NO.
	210170
	DATE
	11/27/21
	PROJECT NO.
	210170
	CONTRACT NO.
	R-42588

INDIANA DEPARTMENT OF TRANSPORTATION  
 MAINTENANCE OF TRAFFIC PHASE 1  
 MICHIGAN RD. - LINE "S"-USR 421-B"

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
CHECKED: -JMS	DRAWN: -JMS	CHECKER: -JMS

GENERAL NOTES

- Access shall be maintained to all businesses and side streets throughout construction.
- Cover signs that conflict with current MOT Phase.
- 100' Spacing will be the Typical Sign Spacing for All Phases.

LEGEND

- 60 Temporary Pavement Marking, Paint, Solid, White, 4"
- 81 Temporary Pavement Marking, Paint, Solid, Yellow, 4"
- 82 Temporary Pavement Marking, Paint, Broken, White, 4"
- Construction Sign
- Temporary Pavement Message Marking, Lane Indication Arrow
- Temporary Pavement Message Marking, Lane Indication Arrow
- Temporary Pavement Message Marking, Lane Indication Arrow

Barricade  
 Channelizing Device  
 Direction of Traffic  
 Construction Area  
 Existing Pavement Message Marking, Lane Indication Arrow  
 Existing Pavement Message Marking, Lane Indication Arrow

WORKSITE  
 XS20-5P  
 24' x 18'

SPEED LIMIT 30  
 R2-1  
 24' x 30"

Yield (L)  
 Y4E-4(L)  
 30" x 36"

Yield (R)  
 Y4E-4(R)  
 30" x 36"

END CONSTRUCTION  
 XS20-3  
 30" x 18"

RIGHT LANE MUST TURN RIGHT  
 R3-7 (R)  
 30" x 30"

ROAD CONSTRUCTION AHEAD  
 XM20-1  
 36" x 36"

Speeding, Reckless Driving, Pass 6 1/5  
 XM2-6A  
 78" x 42"

LEFT LANE MUST TURN LEFT  
 R3-7 (L)  
 30" x 30"

LEFT LANE CLOSED AHEAD  
 W4E-4(L)  
 30" x 36"

LEFT LANE CLOSED AHEAD  
 W4E-4(L)  
 30" x 36"

LANE ENDS MERGE RIGHT  
 J

LANE ENDS MERGE RIGHT  
 I

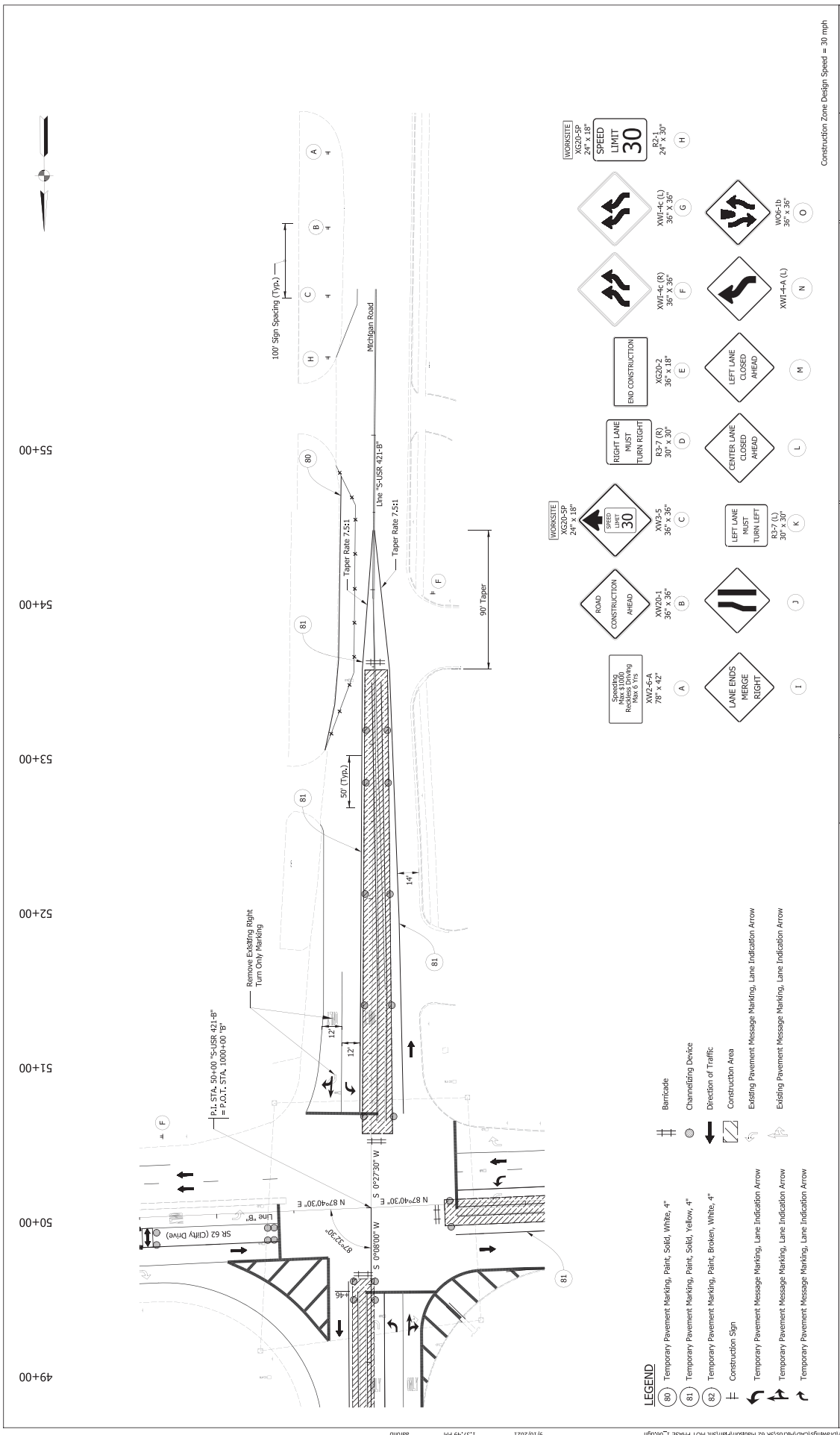
LANE ENDS MERGE RIGHT  
 K

LANE ENDS MERGE RIGHT  
 L

LANE ENDS MERGE RIGHT  
 M

LANE ENDS MERGE RIGHT  
 N

LANE ENDS MERGE RIGHT  
 O



### LEGEND

- 80 Temporary Pavement Marking, Paint, Solid, White, 4"
- 81 Temporary Pavement Marking, Paint, Solid, Yellow, 4"
- 82 Temporary Pavement Marking, Paint, Broken, White, 4"
- Construction Sign
- Temporary Pavement Message Marking, Lane Indication Arrow
- Temporary Pavement Message Marking, Lane Indication Arrow
- Temporary Pavement Message Marking, Lane Indication Arrow
- Barricade
- Channelizing Device
- Direction of Traffic
- Construction Area
- Existing Pavement Message Marking, Lane Indication Arrow
- Existing Pavement Message Marking, Lane Indication Arrow

### WORKSITE

WGS-50-SP	24" x 18"
WGS-50-SP	24" x 18"
WGS-50-SP	24" x 18"

### GENERAL NOTES

- Access shall be maintained to all businesses and side streets throughout construction. Cover signs that conflict with current MOT Phases.
- 100' Spacing will be the Typical Sign Spacing for All Phases.

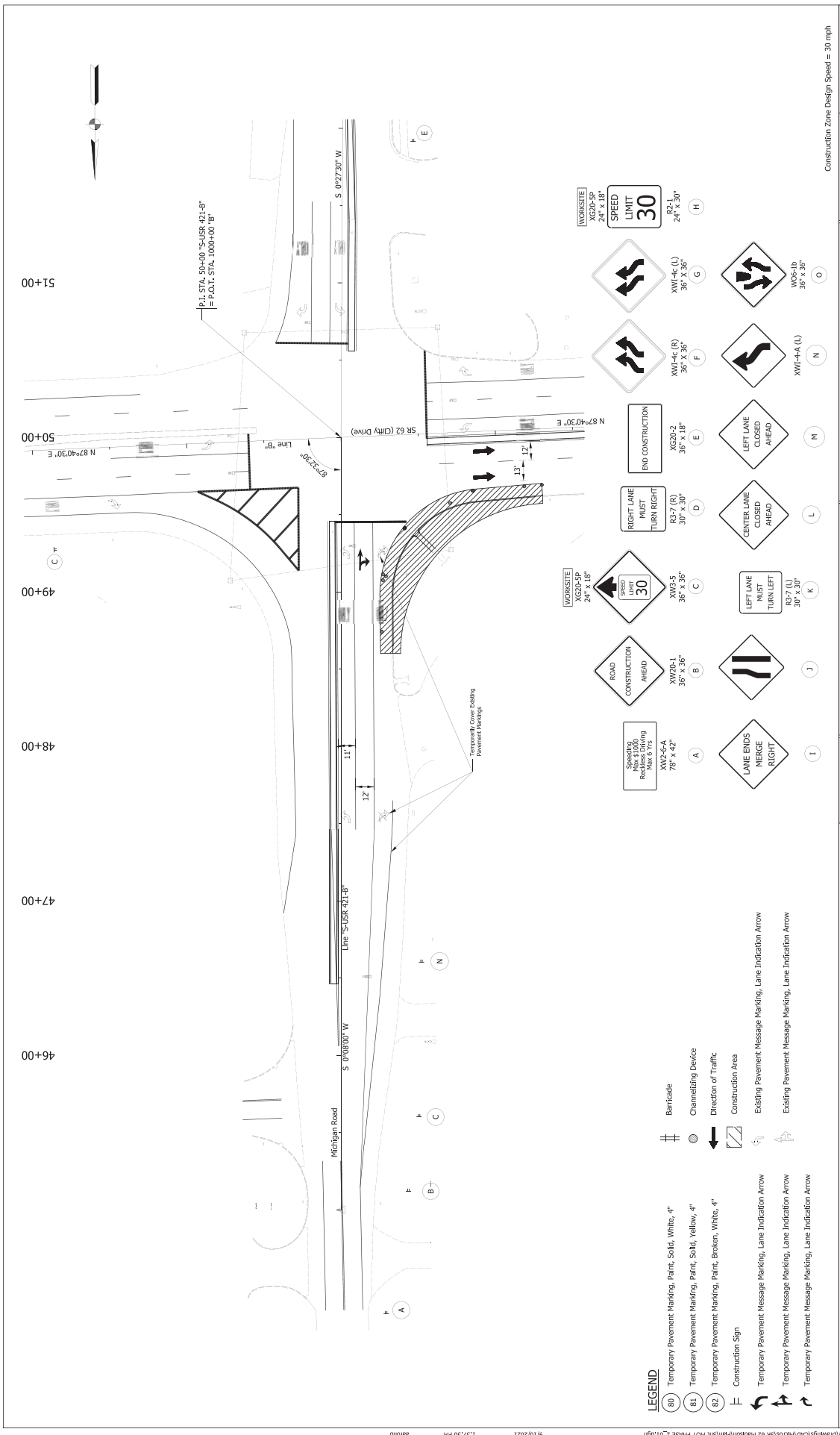
### INDIANA DEPARTMENT OF TRANSPORTATION

**MAINTENANCE OF TRAFFIC PHASE I**  
**MICHIGAN RD. - LINE "S"-USR 421-B"**

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
	DRAWN	DATE
	CHECKER	DATE

HORIZONTAL SCALE	BRIDGE FILE NO.
VERTICAL SCALE	DESIGNATION NO.
SHEET BOOK NO.	PROJECT NO.
CONTRACT NO.	PROJECT NO.

Construction Zone Design Speed = 30 mph



INDIANA DEPARTMENT OF TRANSPORTATION		RECOMMENDED FOR APPROVAL		DESIGNER / DATE	
MAINTENANCE OF TRAFFIC PHASE II		DESIGNER: JMR	DATE:	DESIGNER: JMR	DATE:
MICHIGAN RD. - LINE "S-USR 421-B"		CHECKED: JMR		CHECKED: JMR	
CONTRACT NO. R-42558		PROJECT NO. 2101170		PROJECT NO. 2101170	
BRIDGE FILE NO. N/A		DESIGNATION NO. 2101170		DESIGNATION NO. 2101170	
HORIZONTAL SCALE 1" = 30'		VERTICAL SCALE 1" = 10'		DESIGNER: JMR	
CONSTRUCTION ZONE DESIGN SPEED = 30 MPH		DRAWN: JMR			

**LEGEND**

(80) Temporary Pavement Marking, Paint, Solid, White, 4"

(81) Temporary Pavement Marking, Paint, Solid, Yellow, 4"

(82) Temporary Pavement Marking, Paint, Broken, White, 4"

Construction Sign

Temporary Pavement Message Marking, Lane Indication Arrow

Temporary Pavement Message Marking, Lane Indication Arrow

Temporary Pavement Message Marking, Lane Indication Arrow

Barricade

Channelizing Device

Direction of Traffic

Construction Area

Existing Pavement Message Marking, Lane Indication Arrow

Existing Pavement Message Marking, Lane Indication Arrow

**GENERAL NOTES**

- Access shall be maintained to all businesses and residences.
- Cover signs that conflict with current MOT Phase.
- 10' Spacing will be the Typical Sign Spacing for All Phases.

**WORKSITE**

XG20-SP 24" x 18"

**SPEED LIMIT 30**

R2-1 24" x 30"

XW1-4C (L) 30" x 36"

XW1-4C (R) 30" x 36"

XG20-2 36" x 18"

XS3-7 (R) 30" x 30"

XW20-1 30" x 36"

XW20-5 30" x 36"

XW2-4-A 78" x 42"

XW1-4-A (L) 30" x 36"

W06-1D 30" x 36"

SR 62 (City Drive) N 87°40'30" E

Line "B" S 87°23'30" W

Line "A" S 0°00'00" W

Line "C" N 87°40'30" E

Line "D" S 0°00'00" W

Line "E" N 87°40'30" E

Line "F" S 0°00'00" W

Line "G" N 87°40'30" E

Line "H" S 0°00'00" W

Line "I" N 87°40'30" E

Line "J" S 0°00'00" W

Line "K" N 87°40'30" E

Line "L" S 0°00'00" W

Line "M" N 87°40'30" E

Line "N" S 0°00'00" W

Line "O" N 87°40'30" E

Line "P" S 0°00'00" W

Line "Q" N 87°40'30" E

Line "R" S 0°00'00" W

Line "S" N 87°40'30" E

Line "T" S 0°00'00" W

Line "U" N 87°40'30" E

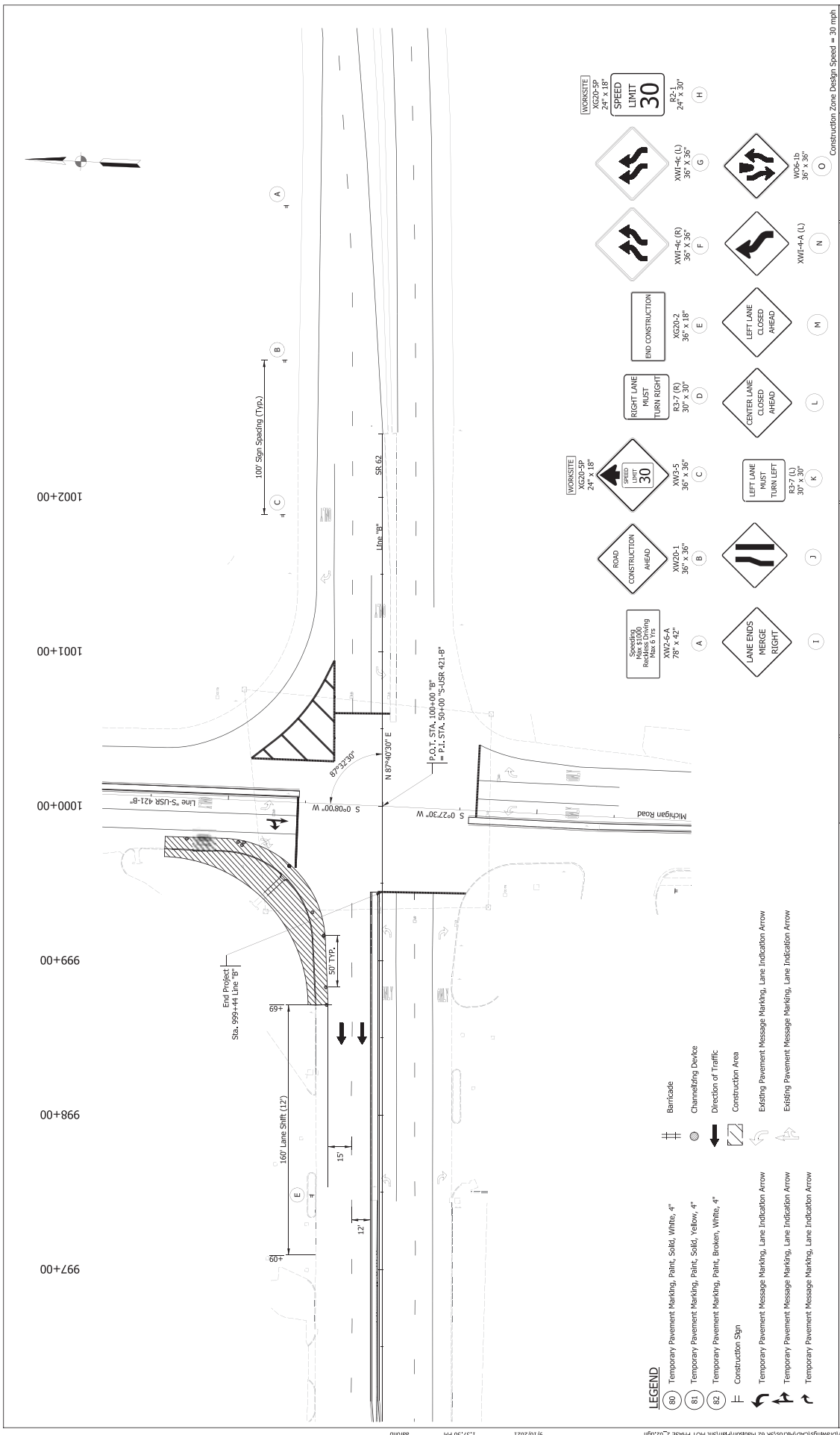
Line "V" S 0°00'00" W

Line "W" N 87°40'30" E

Line "X" S 0°00'00" W

Line "Y" N 87°40'30" E

Line "Z" S 0°00'00" W

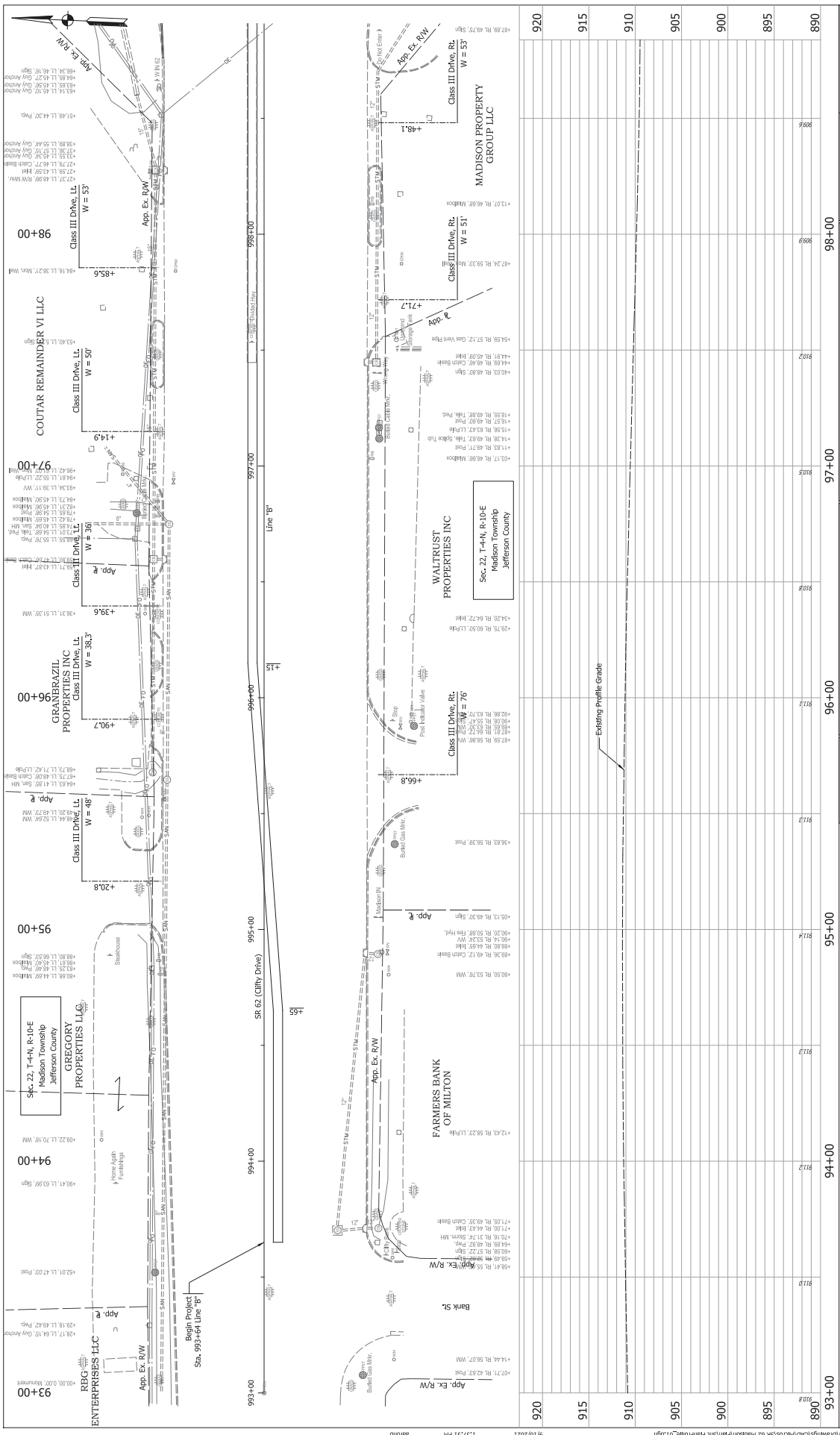


RECOMMENDED FOR APPROVAL		DESIGN ENGINEER		DATE	
DESIGNED BY	CHECKED BY	DESIGNED BY	CHECKED BY	DATE	
INDIANA DEPARTMENT OF TRANSPORTATION		MAINTENANCE OF TRAFFIC PHASE II			
S.R. 62 - LINE "B"		CONTRACT NO. 2101170			
PROJECT NO. R-42588		BRIDGE FILE NO. N/A			
DESIGNER: JMR		DESIGNATION NO. 2101170			
DRAWING: ACB		SHEET BOOK NO. 14			
CHECKED: JMS		PROJECT NO. 2101170			

**GENERAL NOTES**

- Access shall be maintained to all businesses and residences.
- Cover signs that conflict with current MOT Phase.
- 100' Spacing will be the Typical Sign Spacing for All Phases.

Public Road Approach, Rt.  
Sta. 993+38.6, W = 66'

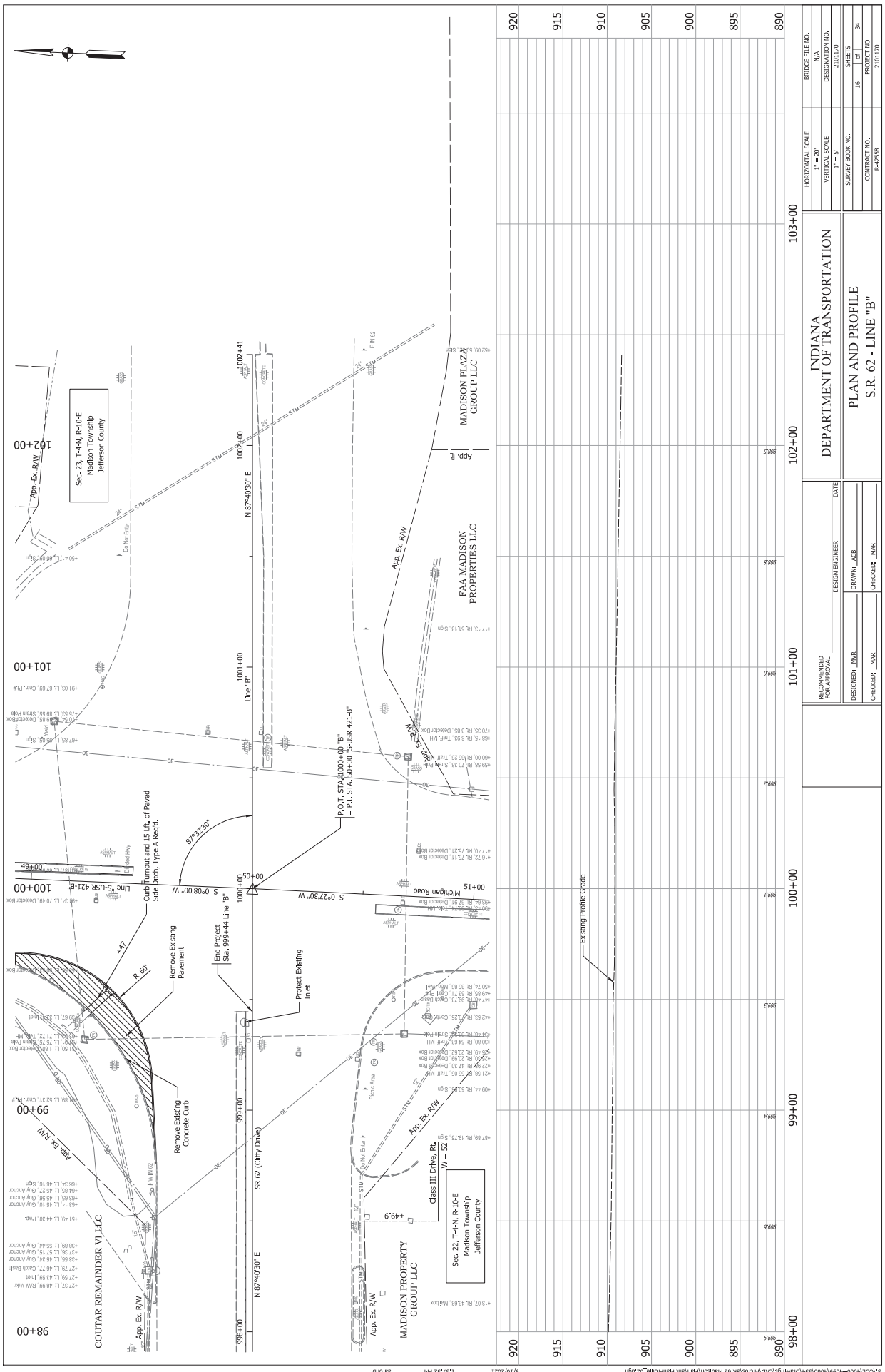


933+00	934+00	935+00	936+00	937+00	938+00	939+00	940+00	941+00	942+00	943+00	944+00	945+00	946+00	947+00	948+00	949+00	950+00	951+00	952+00	953+00	954+00	955+00	956+00	957+00	958+00	959+00	960+00	961+00	962+00	963+00	964+00	965+00	966+00	967+00	968+00	969+00	970+00	971+00	972+00	973+00	974+00	975+00	976+00	977+00	978+00	979+00	980+00	981+00	982+00	983+00	984+00	985+00	986+00	987+00	988+00	989+00	990+00	991+00	992+00	993+00	994+00	995+00	996+00	997+00	998+00
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INDIANA  
DEPARTMENT OF TRANSPORTATION  
PLAN AND PROFILE  
S.R. 62 - LINE "B"

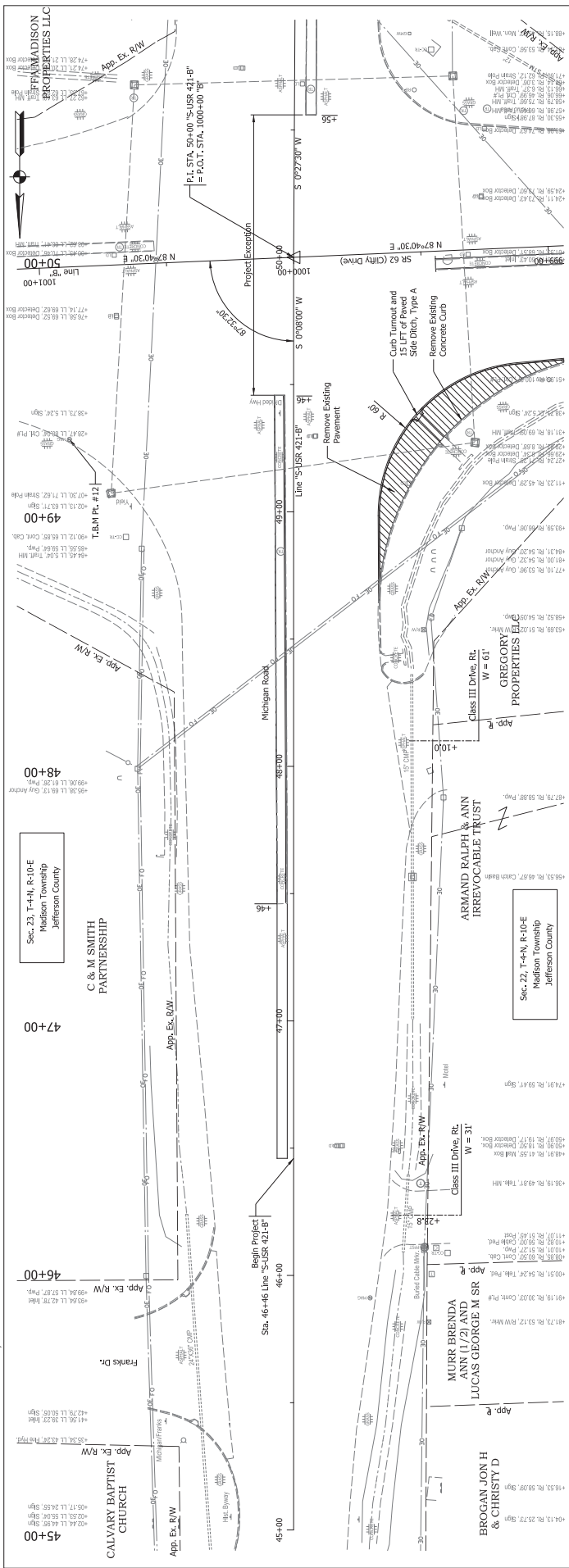
RECOMMENDED FOR APPROVAL  
DESIGN ENGINEER: \_\_\_\_\_ DATE: \_\_\_\_\_  
DESIGNER: JMR DRAWN: JAB  
CHECKED: JMS CHECKER: JMR

BRIDGE FILE NO.: N/A  
VERTICAL SCALE: 1" = 5'  
HORIZONTAL SCALE: 1" = 20'  
SHEET BOOK NO.: 15 OF 34  
PROJECT NO.: 2101170  
CONTRACT NO.: P-42588





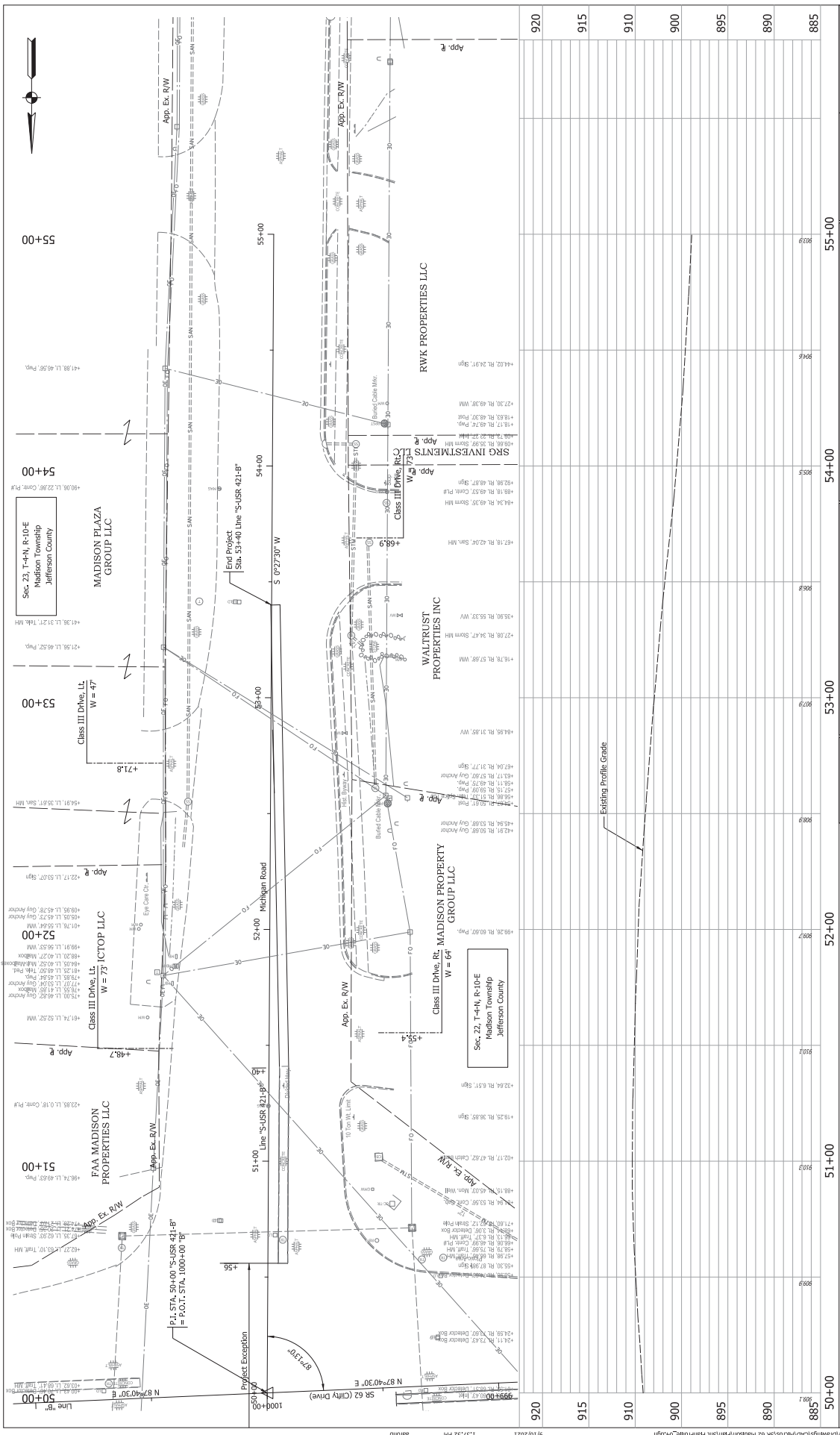
P.I. STA. 50+00.00 "S-USR 421-B"  
 DELTA = 0°19'30"  
 NO CURVE



Station	Profile	Notes
45+00	885	
46+00	885	
47+00	885	
48+00	885	
49+00	885	
50+00	885	
920	915	910
900	895	890
885		

INDIANA DEPARTMENT OF TRANSPORTATION		BRIDGE FILE NO. N/A	
RECOMMENDED FOR APPROVAL		VERTICAL SCALE 1" = 20'	
DESIGN ENGINEER	DATE	PROJECT NO. 2101170	
DESIGNER: JMR		CONTRACT NO. P-42588	
DRAWN: AEB		SHEET BOOK NO. 17 OF 34	
CHECKED: JMS		PROJECT NO. 2101170	

PLAN AND PROFILE  
 MICHIGAN ROAD - LINE "S-USR 421-B"



50+00	51+00	52+00	53+00	54+00	55+00	885	890	895	900	910	915	920
888	889	890	891	892	893	894	895	896	897	898	899	900
901	902	903	904	905	906	907	908	909	910	911	912	913
914	915	916	917	918	919	920	921	922	923	924	925	926
927	928	929	930	931	932	933	934	935	936	937	938	939
940	941	942	943	944	945	946	947	948	949	950	951	952
953	954	955	956	957	958	959	960	961	962	963	964	965
966	967	968	969	970	971	972	973	974	975	976	977	978
979	980	981	982	983	984	985	986	987	988	989	990	991
992	993	994	995	996	997	998	999	1000	1001	1002	1003	1004

INDIANA  
DEPARTMENT OF TRANSPORTATION

PLAN AND PROFILE  
MICHIGAN ROAD - LINE "S-USR 421-B"

RECOMMENDED FOR APPROVAL

DESIGN ENGINEER: \_\_\_\_\_ DATE: \_\_\_\_\_

DESIGNER: JMR DRAWN: JAB

CHECKED: JMR

HORIZONTAL SCALE  
1" = 20'

VERTICAL SCALE  
1" = 5'

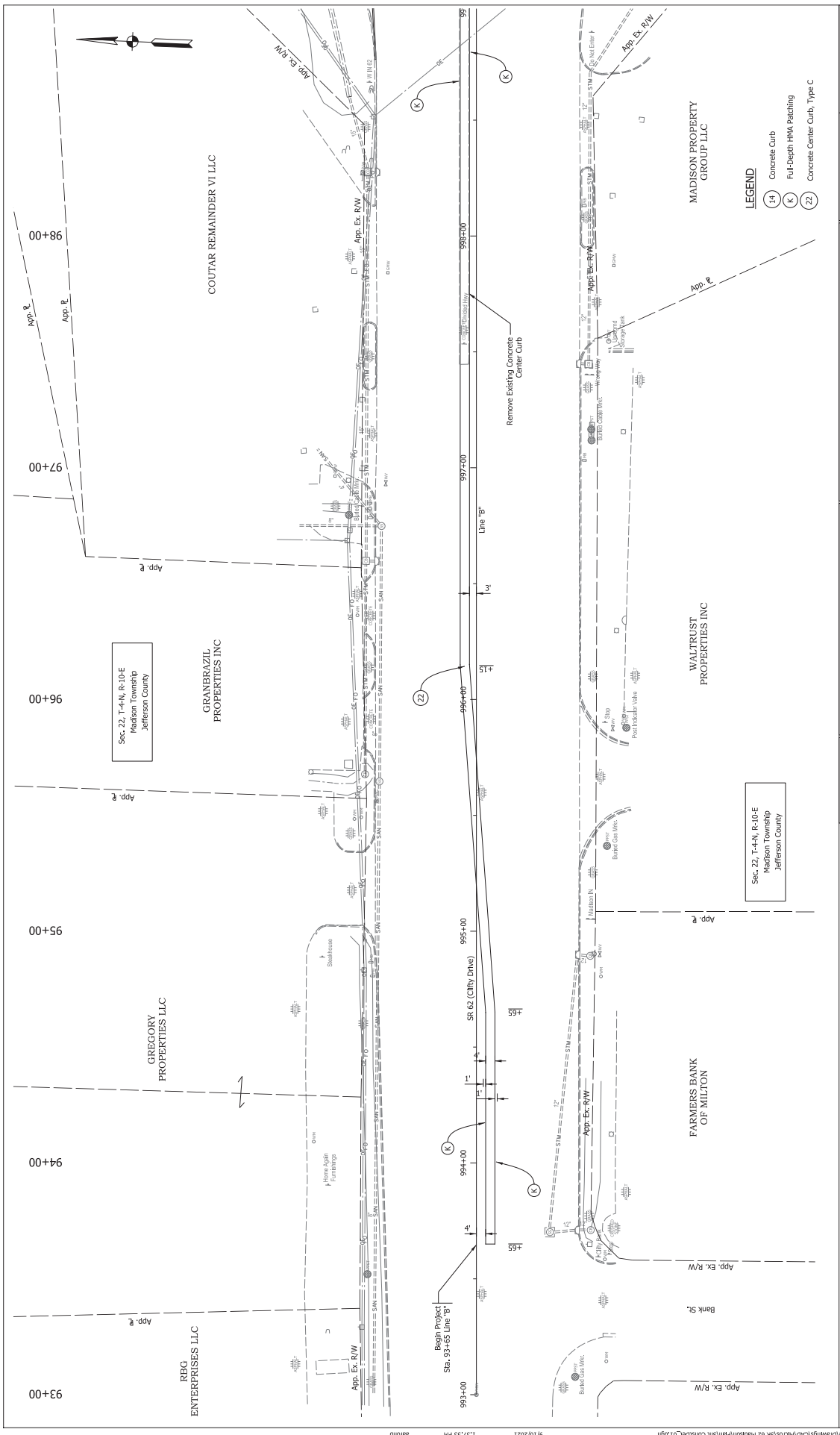
SHEET BOOK NO.  
18 OF 34

PROJECT NO.  
2101170

CONTRACT NO.  
R-42588

BRIDGE FILE NO.  
N/A

DESIGNATION NO.  
2101170



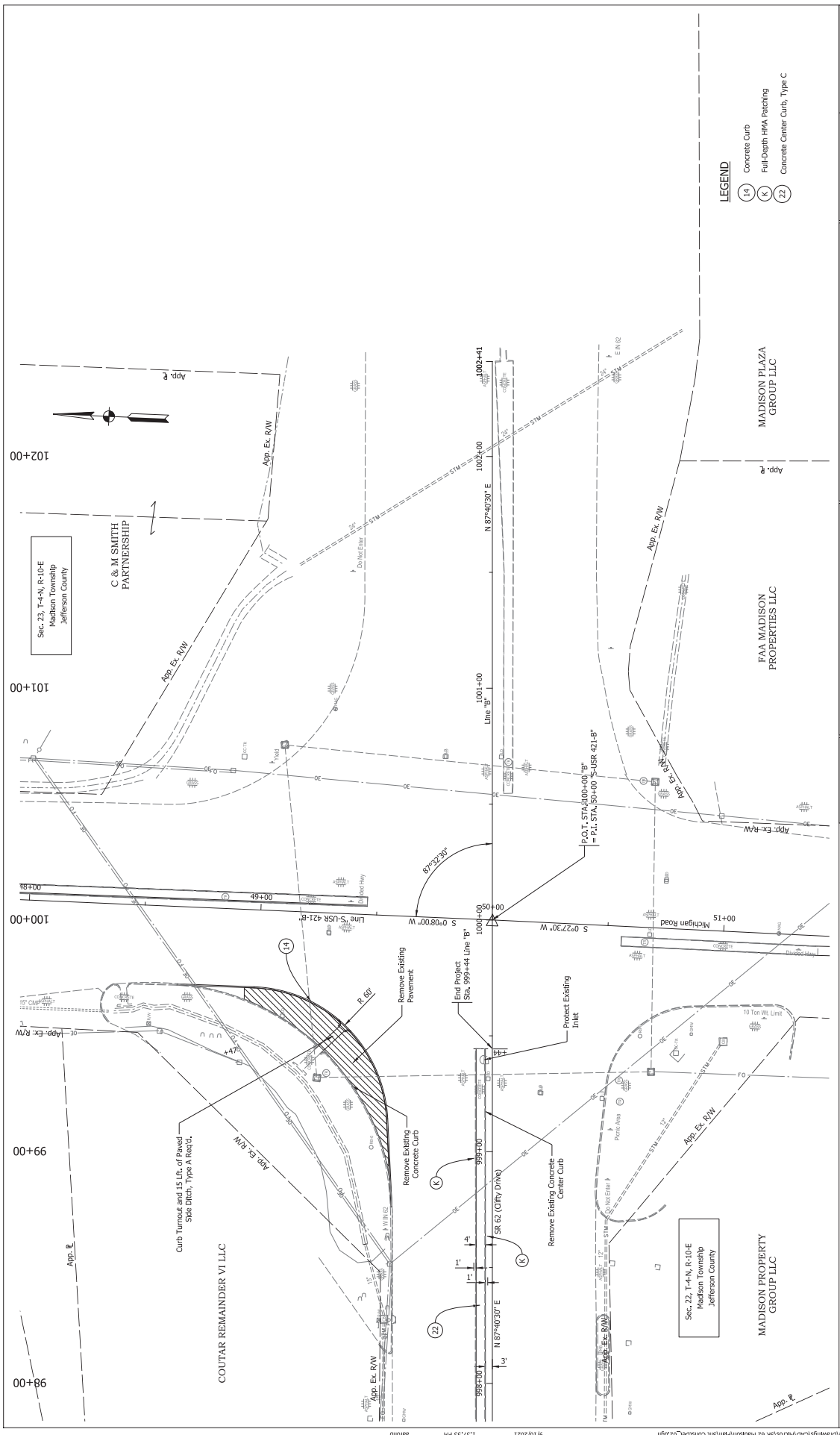
- LEGEND**
- (14) Concrete Curb
  - (16) Full-Depth HMA Patching
  - (22) Concrete Center Curb, Type C

HORIZONTAL SCALE	BRIDGE FILE NO.
1" = 20'	N/A
VERTICAL SCALE	DESIGNATION NO.
1" = 10'	210170
SHEET BOOK NO.	SHEET NO.
19	27
CONTRACT NO.	PROJECT NO.
R-42558	210170

**INDIANA**  
**DEPARTMENT OF TRANSPORTATION**  
**CONSTRUCTION DETAIL**  
**S.R. 62 - LINE "B"**

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
	DR/AMW_ACB	
DESIGNER: JMR	CHECKER: JMR	
CHECKED: JMS		

S:\COL\1000-1099\1060155\Final\Drawings\CD\Midwest\SR 62 Madison\Print\Site Center\01.dgn 5/10/2021 1:32:55 PM aaronb



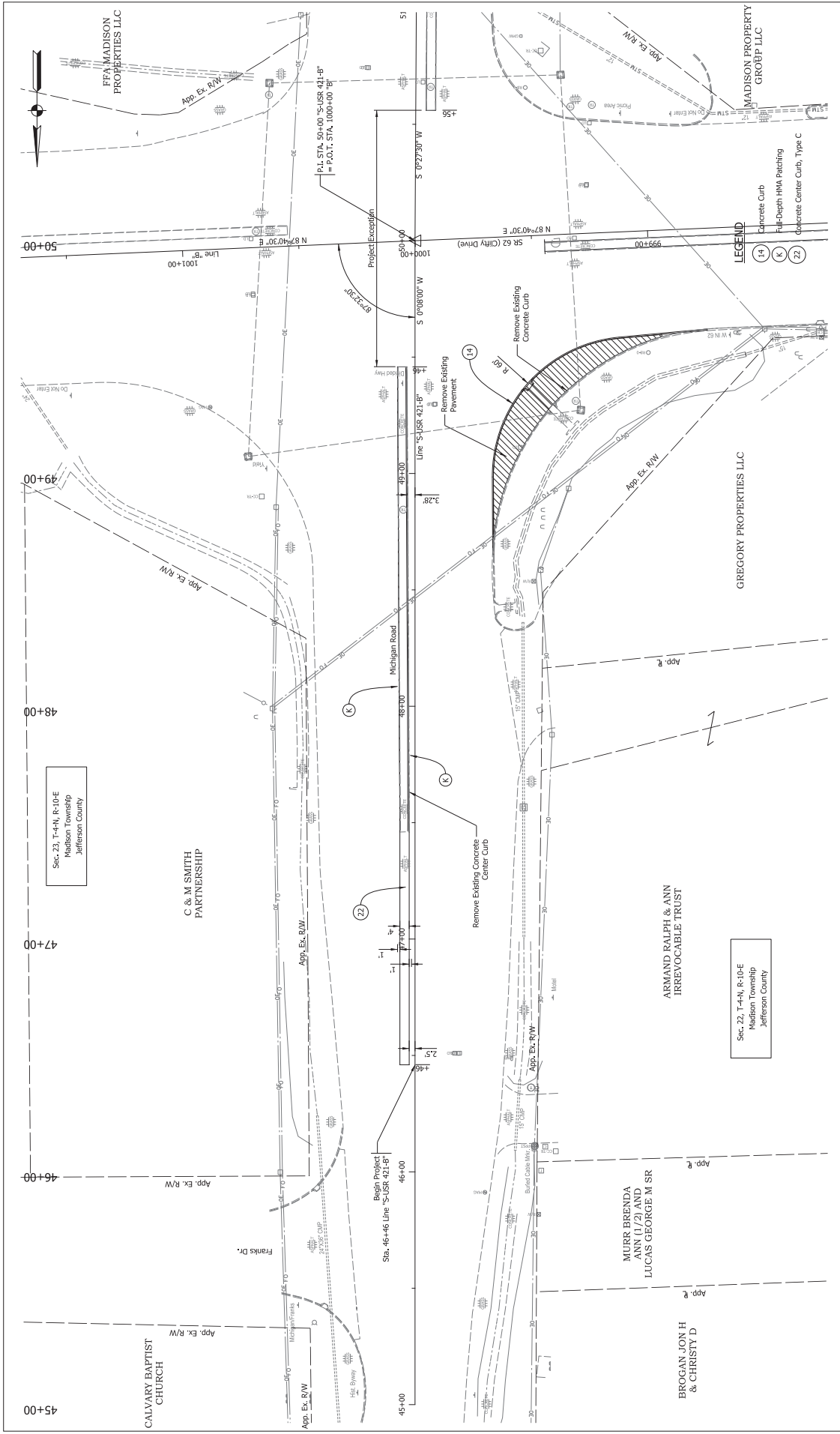
- LEGEND**
- (14) Concrete Curb
  - (K) Full-Depth HMA Patching
  - (22) Concrete Center Curb, Type C

HORIZONTAL SCALE	BRIDGE FILE NO.
1" = 20'	N/A
VERTICAL SCALE	DESIGNATION NO.
1" = 10'	210170
SHEET BOOK NO.	SHEET NO.
20	27
PROJECT NO.	PROJECT NO.
R-42588	210170

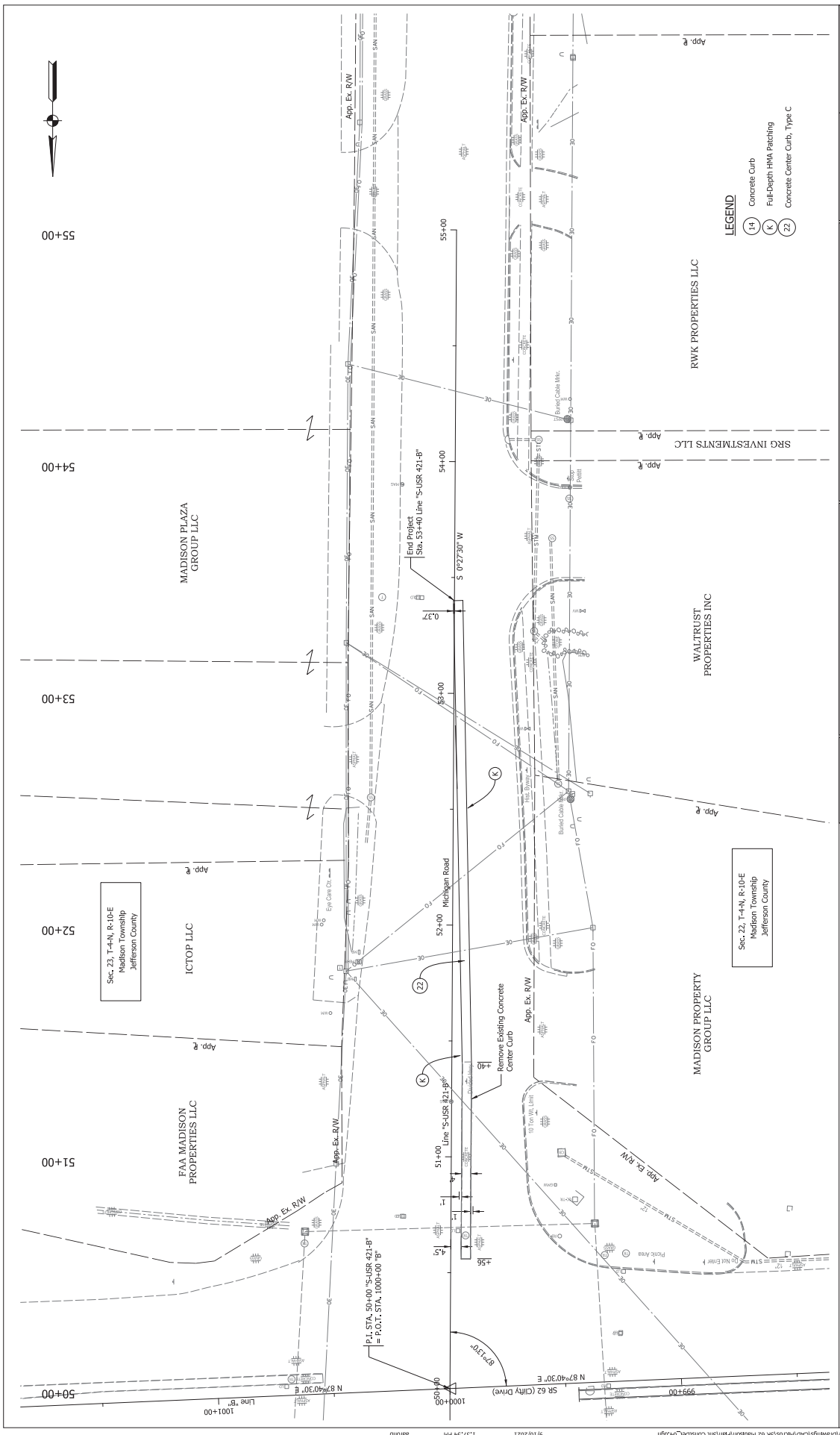
**INDIANA**  
**DEPARTMENT OF TRANSPORTATION**  
**CONSTRUCTION DETAIL**  
**S.R. 62 - LINE "B"**

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNER: JMR	DRAWN: AEB	
CHECKED: JMS	CHECKER: JMR	

Public Road Approach, LL.  
Sta. 45+68.2, W= 82'



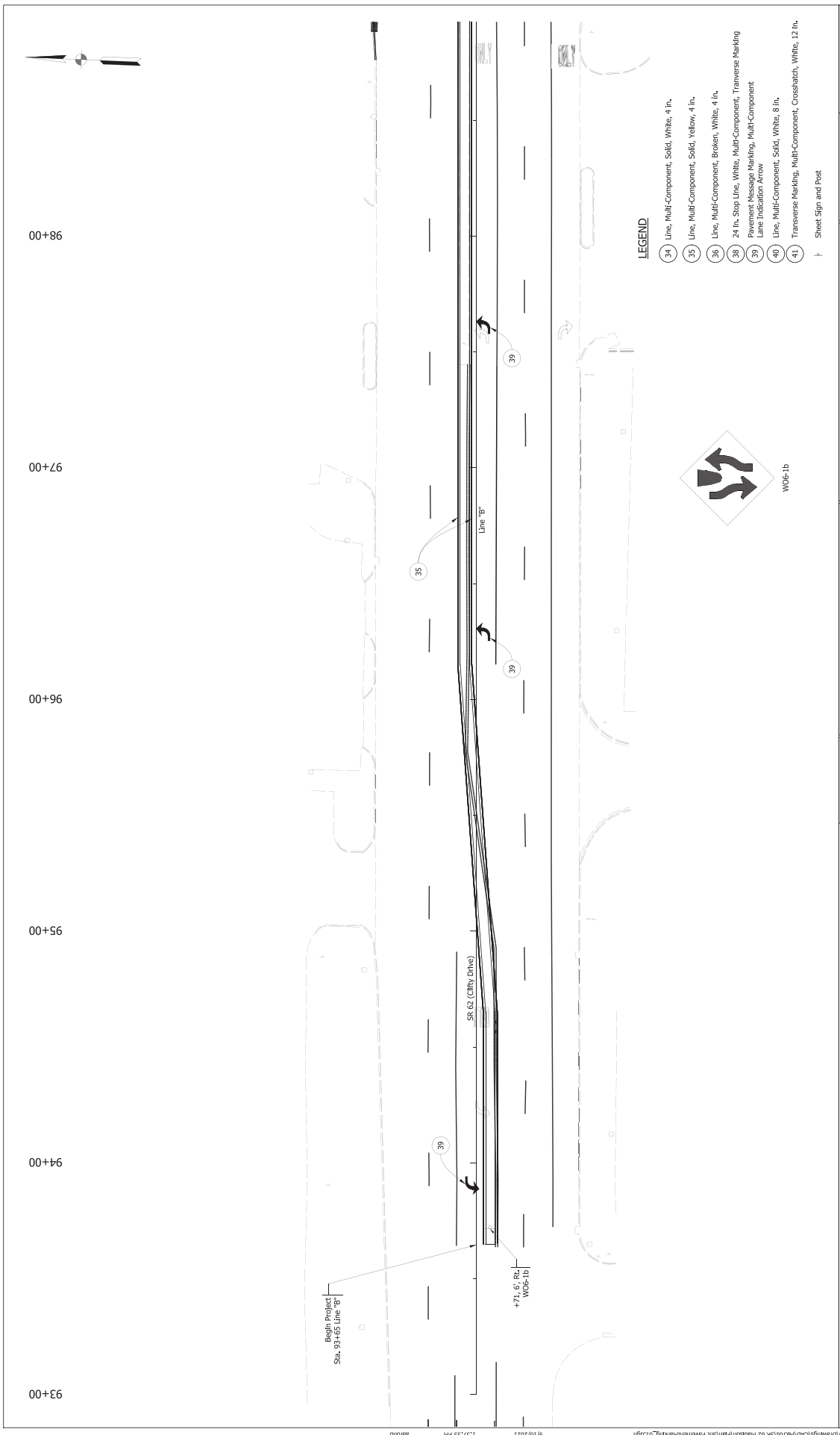
RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE	INDIANA DEPARTMENT OF TRANSPORTATION
	DESIGNER: JMR	CHECKER: JMR	
BRIDGE FILE NO.	N/A		CONSTRUCTION DETAIL
	DESIGNATION NO.		
HORIZONTAL SCALE		1" = 20'	MICHIGAN RD. - LINE "S-USR 421-B"
VERTICAL SCALE		N/A	
BRIDGE BOOK NO.		21	PROJECT NO.
CONTRACT NO.		47	
DRAWN: AFB		21	PROJECT NO.
CHECKED: JMS		47	CONTRACT NO.
		34	R-42558



- LEGEND**
- (14) Concrete Curb
  - (K) Full-Depth HMA Patching
  - (22) Concrete Center Curb, Type C

Sec. 22, T-4N, R-10E  
Madison Township  
Jefferson County

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE	INDIANA	HORIZONTAL SCALE	BRIDGE FILE NO.
	DESIGNER: JMR		DEPARTMENT OF TRANSPORTATION	T = 20'	N/A
	CHECKED: JMS		CONSTRUCTION DETAIL	VERTICAL SCALE	DESIGNATION NO.
			MICHIGAN RD. - LINE "S-USR 421-B"	N/A	210170
				SHEET BOOK NO.	SHEET
				22	34
				CONTRACT NO.	PROJECT NO.
				R-42558	210170



**LEGEND**

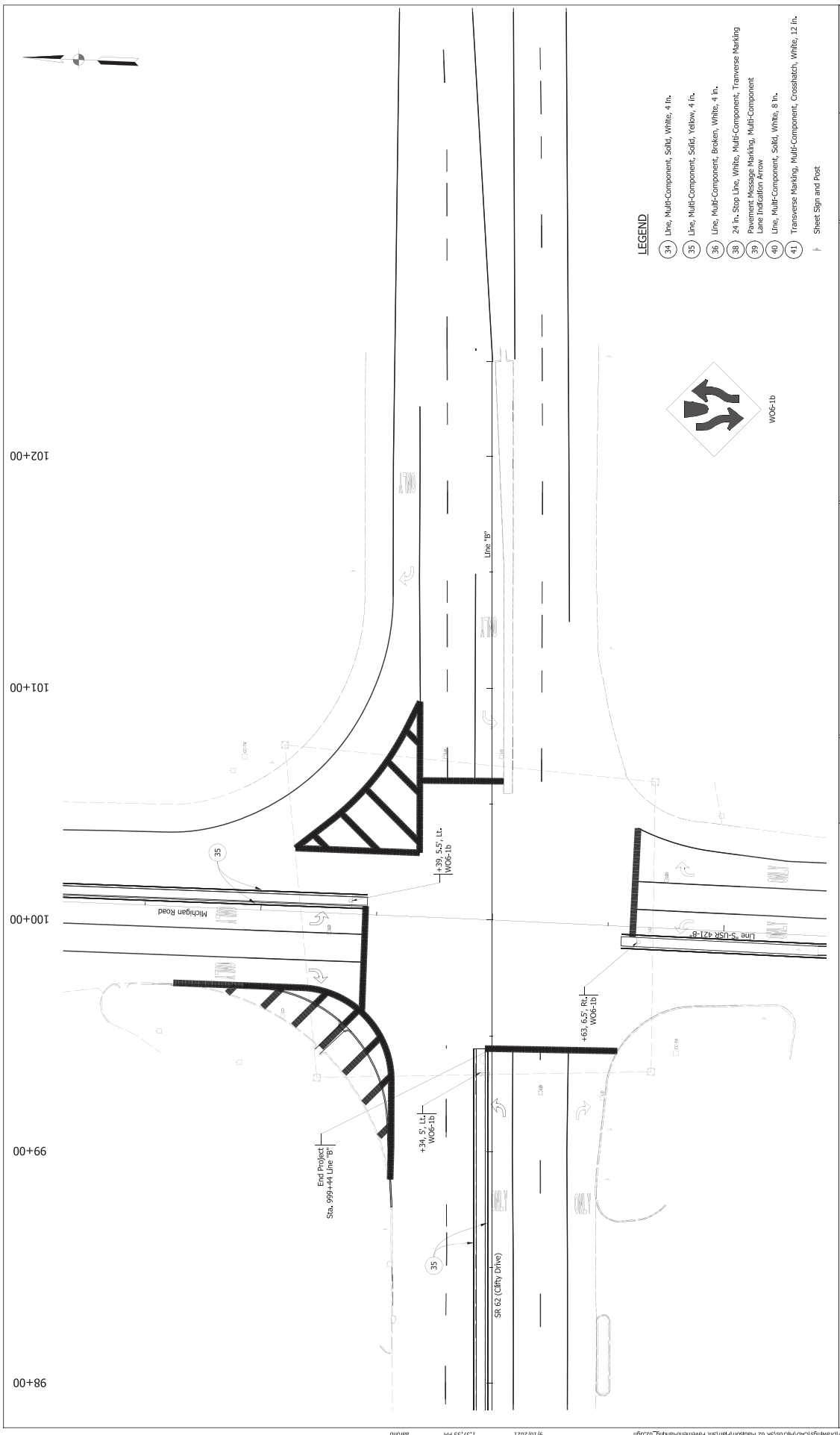
- 34 Line, Multi-Component, Solid, White, 4 in.
- 35 Line, Multi-Component, Solid, Yellow, 4 in.
- 36 Line, Multi-Component, Broken, White, 4 in.
- 38 24 in. Stop Line, White, Multi-Component, Transverse Marking
- 39 Pavement Message Marking, Multi-Component Lane Indication Arrow
- 40 Line, Multi-Component, Solid, White, 8 in.
- 41 Transverse Marking, Multi-Component, Crosshatch, White, 12 in.



W06-1b

Sheet Sign and Post

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE	INDIANA DEPARTMENT OF TRANSPORTATION	
	DESIGNER: JMR		S.R. 62 - LINE "B"	
CHECKED: JMS	DRAWN: JCB		PAVEMENT MARKING DETAIL	
	CHECKER: JMR		S.R. 62 - LINE "B"	
			BRIDGE FILE NO. N/A	
			DESIGNATION NO. 2101170	
			SHEET BOOK NO. 23	
			PROJECT NO. 2101170	
			CONTRACT NO. R-42588	



**LEGEND**

- 34 Line, Multi-Component, Solid, White, 4 in.
- 35 Line, Multi-Component, Solid, Yellow, 4 in.
- 36 Line, Multi-Component, Broken, White, 4 in.
- 38 24 in. Stop Line, White, Multi-Component, Transverse Marking
- 39 Pavement Message Marking, Multi-Component Lane Indication Arrow
- 40 Line, Multi-Component, Solid, White, 8 in.
- 41 Transverse Marking, Multi-Component, Crosshatch, White, 12 in.

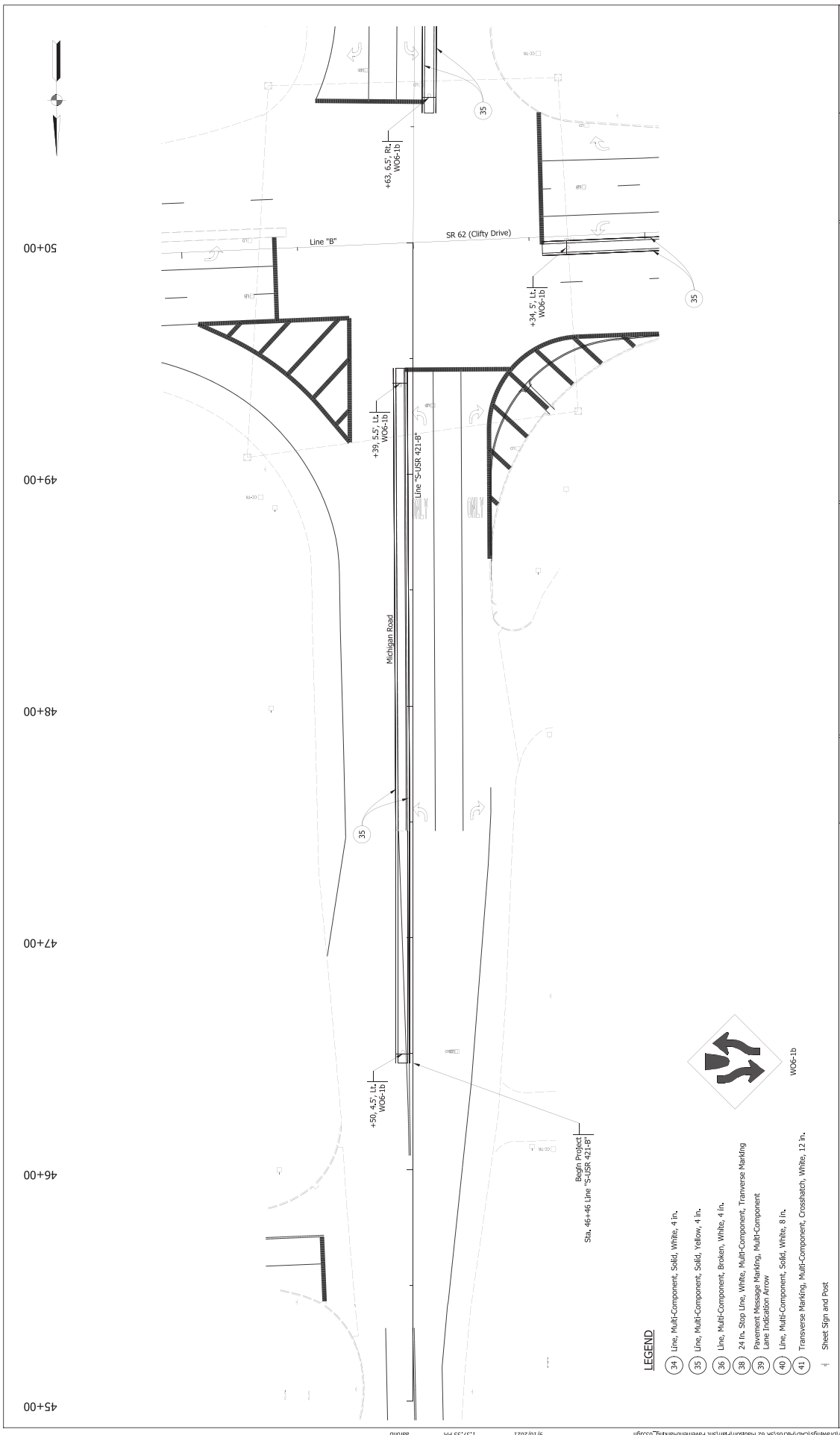


W06-1b

Sheet Sign and Post

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE	INDIANA DEPARTMENT OF TRANSPORTATION
	DESIGNER: JMR		
	DRAWN: ACB		PAVEMENT MARKING DETAIL S.R. 62 - LINE "B"
	CHECKED: JMR		
HORIZONTAL SCALE			BRIDGE FILE NO.
VERTICAL SCALE			N/A
PROJECT NO.			2101170
CONTRACT NO.			R-42588





**LEGEND**

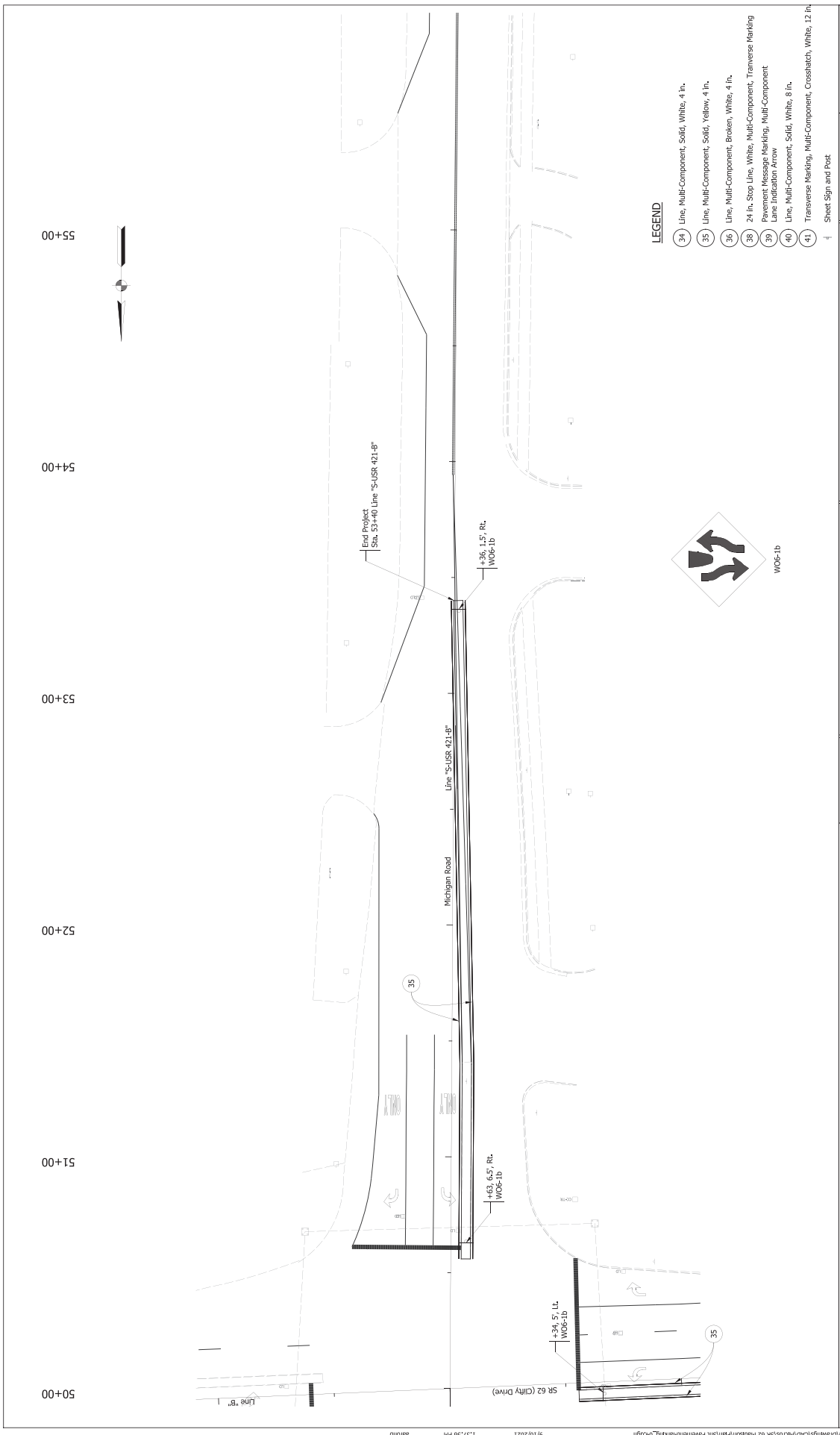
- (34) Line, Multi-Component, Solid, White, 4 in.
- (35) Line, Multi-Component, Solid, Yellow, 4 in.
- (36) Line, Multi-Component, Broken, White, 4 in.
- (38) 24 in. Stop Line, White, Multi-Component, Transverse Marking
- (39) Pavement Message Marking, Multi-Component
- (40) Lane Indication Arrow
- (41) Line, Multi-Component, Solid, White, 8 in.
- (42) Transverse Marking, Multi-Component, Crosshatch, White, 12 in.



W06-1b

↓ Sheet Sign and Post

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE	INDIANA DEPARTMENT OF TRANSPORTATION	HORIZONTAL SCALE	BRIDGE FILE NO.
	DESIGNER: JMR			VERTICAL SCALE	N/A
DESIGNER: JMR	DRAWN: JCB		PAVEMENT MARKING DETAIL MICHIGAN RD. - LINE "S-USR 421-B"	VERTICAL SCALE	DESIGNATION NO.
CHECKED: JMS	CHECKER: JMR			DATE	2/10/20
				SHEET BOOK NO.	25
				CONTRACT NO.	R-42558
				PROJECT NO.	2101170



**LEGEND**

- 34 Line, Multi-Component, Solid, White, 4 in.
- 35 Line, Multi-Component, Solid, Yellow, 4 in.
- 36 Line, Multi-Component, Broken, White, 4 in.
- 38 24 in. Stop Line, White, Multi-Component, Transverse Marking
- 39 Pavement Message Marking, Multi-Component Lane Indication Arrow
- 40 Line, Multi-Component, Solid, White, 8 in.
- 41 Transverse Marking, Multi-Component, Crosshatch, White, 12 in.



W06-1b

1 Sheet Sign and Post

INDIANA DEPARTMENT OF TRANSPORTATION		RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
PAVEMENT MARKING DETAIL MICHIGAN RD. - LINE "S-USR 421-B"		DESIGNER: JMR	DRAWN: JCB	CHECKED: JMR
HORIZONTAL SCALE	BRIDGE FILE NO.			
1" = 20'	N/A			
VERTICAL SCALE	DESIGNATION NO.			
N/A	210170			
SHEET BOOK NO.	SHEET NO.			
35	27			
CONTRACT NO.	PROJECT NO.			
R-42588	210170			

# **APPENDIX C: Early Coordination**



# INDIANA DEPARTMENT OF TRANSPORTATION

100 North Senate Avenue  
Room N642  
Indianapolis, Indiana 46204

Eric J. Holcomb, Governor  
Joe McGuinness, Commissioner

## SAMPLE EARLY COORDINATION LETTER

July 8, 2021

{See Attached List}

Note: Project was initially bundled with another Access Management Project. Projects were split and SR 62 and Michigan Road kept Des. No. 1902023; and the other location was assigned a new Des. No. 2101170. Des. No. 2101170 is covered under a separate CE document. Only highlighted portions pertain to this project.

Re: Early Coordination Designation Number (Des. No.) 1902023, Access Management Project  
State Road (SR) 229 from Grayson Street (St.) to north Kroger driveway; SR 62 and Michigan Road (Rd.)  
Franklin and Jefferson Counties, Indiana

Dear Agency:

The Indiana Department of Transportation (INDOT) and Federal Highway Administration (FHWA) intend to proceed with a project involving the aforementioned Access Management project in Franklin and Jefferson Counties. This letter is part of the early coordination phase of the environmental review process. We are requesting comments from your area of expertise regarding any possible environmental effects associated with this project. Please use the above designation number and description in your reply. We will incorporate your comments into a study of the project's environmental impacts.

The project involves two sites. Site 1 includes the corridor of SR 229 beginning at Grayson Street and terminating 0.14 mile northeast at the north Kroger driveway in Batesville, in the southwest portion of Franklin County. Specifically, Site 1 is located in Section 17, Township 10 North, Range 12 East of the Batesville, Indiana 7.5-minute United States Geological Survey (USGS) topographic quadrangle. SR 229 is functionally classified as a Minor Arterial roadway, conveys traffic northeast and southwest through this area, and has a posted speed limit of 40 miles per hour (mph). A typical cross section of SR 229 includes two 12-ft.-wide travel lanes in each direction and a 16-ft.-wide two-way left-turn lane (TWLTL). The roadway is bounded by curb-and-gutter and drains via surface inlets and enclosed storm sewers. No shoulders, sidewalks, or guardrail are present. The adjacent land use is primarily commercial. Full-access commercial driveways are provided on both sides of the road. Left turning vehicles queue past Grayson Street, which creates a safety concern in this area.

Site 2 includes the intersection of SR 62 and Michigan Rd. in Madison, in the south-central portion of Jefferson County, Indiana. Specifically, Site 2 is located in Sections 22 and 23, Township 4 North, Range 10 East of the Clifty Falls, Indiana 7.5-minute USGS topographic quadrangle. SR 62 is functionally classified as a Principal Arterial – Other roadway, conveys traffic east and west through this area, and has a posted speed limit of 35 mph. A typical cross section of SR 62 includes two 12-ft.-wide travel lanes in each direction. The west half of SR 62 is curbed with storm sewers and the eastern half has shoulders with open ditches. Michigan Rd. is functionally classified as a Minor Arterial roadway, conveys traffic north and south through this area, and has a posted speed limit of 40 mph. The adjacent land use is commercial. The crash rate is high due to excessive turning radius, conflicts with the existing median curb, and impeded signal visibility on SR 62.

The draft need for this project is due to the crash frequency and severity in these areas. The draft purpose of this project is to reduce the crash frequency by correcting access conflicts at the intersections.

The proposed improvements to Site 1 include installing a 2-ft.-wide median curb along the full corridor, adjusting the signal heads at Northside Drive to align with the new northbound lane locations, resetting left-turn-only signs, converting the two-way left-turn lane to a left-turn lane, shifting/applying lane striping, applying full-depth asphalt patches to widen the roadway, milling and resurfacing, and installing drainage inlets. Approximately 0.015 acre of temporary right-of-way is anticipated to be required at the northern entrance of Kroger. The proposed improvements to Site 2 include pavement removal in the northwest corner, replacing and extending the approach

[www.in.gov/dot/](http://www.in.gov/dot/)

An Equal Opportunity Employer

C-1

NextLevel  
INDIANA

medians, installing full-depth patches, milling and resurfacing, reducing the radius of the northwestern corner, modifying traffic signalization, and installing a curb turnout to match the existing drainage pattern. Some utility relocation may be required for each site.

Traffic will be maintained during construction with single lane closures. To maintain a southbound lane during construction of the Site 1 improvements, the fourth lane southbound will be extended to Northside Drive. This will require the curb to be removed and relocated, as well as paving the new lane and adding the appropriate striping.

This project qualifies for the application of the *Range-wide Programmatic Informal Consultation for the Indiana Bat and Northern Long-eared Bat between the FHWA, Federal Railroad Administration, Federal Transit Administration, and United States Fish and Wildlife Service (USFWS)* dated May 2016 (revised February 2018). Project information will be submitted through the USFWS Information for Planning and Consultation (IPaC) separately. The project also qualifies for programmatic coordination as outlined in the *USFWS Interim Policy for the Review of Highway Transportation Projects in the State of Indiana* (2013).

This project appears to fall under the current *Programmatic Agreement among the FHWA, the INDOT, the Advisory Council on Historic Preservation, and the Indiana State Historic Preservation Officer Regarding the Implementation of the Federal Aid Highway Program in the State of Indiana* (MPPA). Consultation with the INDOT Cultural Resources Office will be conducted as necessary.

As no mapped Waters of the U.S. are within or adjacent to the project areas, and ground disturbance will be very minor, a Waters of the U.S. report and Clean Water Act permits are not anticipated to be required.

If we do not receive your response within thirty (30) calendar days from the date of this letter, it will be assumed that your agency believes that there will be no adverse effects incurred as a result of the proposed project. However, if you find that an extension to the response time is necessary, a reasonable amount may be granted upon request. If you have any questions regarding this matter, please feel free to contact Jessica Peterson, Project Manager, Metric Environmental, at JessicaP@metricenv.com, 812.325.2809, or 6958 Hillside Ct., Indianapolis, IN 46250 or Will Fortson, Project Manager, INDOT Seymour District, at WFortson@indot.in.gov, 812.524.3745, or 5701 Highway 31 E, Clarksville, IN 47129. Thank you in advance for your input.

Sincerely,



Jessica Peterson, MS  
Project Manager  
Metric Environmental, LLC

cc: File No. 18-0070-13  
Marc Rape, PE; Strand Associates, Inc.  
Will Fortson, Project Manager, INDOT Seymour District

Attachments: Recipient List, Location Map, USGS Topographic Maps, 2016 Aerial Photographs, Photograph Location Maps, Site Photographs, NRCS Soils Maps, NRCS Soils Map Legends, National Wetlands Inventory Maps, and Flood Insurance Rate Maps



# INDIANA DEPARTMENT OF TRANSPORTATION

100 North Senate Avenue  
Room N642  
Indianapolis, Indiana 46204

**Eric J. Holcomb, Governor**  
**Joe McGuinness, Commissioner**

## **Early Coordination Recipients** **June 30, 2021**

Federal Highway Administration  
Seymour District  
{erica.tait@dot.gov}

Indiana Geological and Water Survey  
{<https://igws.indiana.edu/eAssessment>}

Indiana Department of Natural Resources  
Division of Fish and Wildlife  
{environmentalreview@dnr.in.gov}

National Parks Service  
Midwest Regional Office  
{Mwro\_Compliance@nps.gov}

Indiana Department of Environmental Management  
Drinking Water Branch, Groundwater Section  
{ATurnbow@IDEM.IN.gov}

Indiana Department of Environmental Management  
Proposed Roadway Construction Projects  
{<http://www.in.gov/idem/5284.htm>}

US Department of Housing & Urban Development  
Chicago Regional Office  
{Melanie.H.Castillo@hud.gov}

INDOT – Seymour District  
{DDye@indot.in.gov}

INDOT - Office of Aviation  
{JCourtade@indot.in.gov}

Natural Resources Conservation Service  
{rick.neilson@in.usda.gov}

United States Fish and Wildlife Service  
{robin\_mcwilliams@fws.gov}

Jefferson County Highway Department  
{robert.phillips@jeffersoncounty.in.gov}

Jefferson County Surveyor  
{mike.pittman@jeffersoncounty.in.gov}

Jefferson County Commissioner  
Middle District  
{robert.little@jeffersoncounty.in.gov}

Jefferson County Emergency Management  
Agency {troy.morgan@jeffersoncounty.in.gov}

City of Madison – Mayor  
{mayor@madison-in.gov}

City of Madison – City Council Districts 4 and 5  
{krampy@madison-in.gov}  
{cchatham@madison-in.gov}

INDOT - Utilities and Railroads  
{mjett@indot.in.gov}

## **July 8, 2021**

Madison MS4 Coordinator  
{ms4@madison-in.gov}

## Organization and Project Information

**Project ID:** 18-0070-13 - Site 2  
**Des. ID:** 1902023 - Site 2  
**Project Title:** Access Management  
**Name of Organization:** Metric Environmental  
**Requested by:** Jessica Peterson

## Environmental Assessment Report

1. Geological Hazards:
  - High liquefaction potential
2. Mineral Resources:
  - Bedrock Resource: High Potential
  - Sand and Gravel Resource: None documented in the area
3. Active or abandoned mineral resources extraction sites:
  - None documented in the area

\*All map layers from Indiana Map ([maps.indiana.edu](http://maps.indiana.edu))

### **DISCLAIMER:**

This document was compiled by Indiana University, Indiana Geological Survey, using data believed to be accurate; however, a degree of error is inherent in all data. This product is distributed "AS-IS" without warranties of any kind, either expressed or implied, including but not limited to warranties of suitability to a particular purpose or use. No attempt has been made in either the design or production of these data and document to define the limits or jurisdiction of any federal, state, or local government. The data used to assemble this document are intended for use only at the published scale of the source data or smaller (see the metadata links below) and are for reference purposes only. They are not to be construed as a legal document or survey instrument. A detailed on-the-ground survey and historical analysis of a single site may differ from these data and this document.

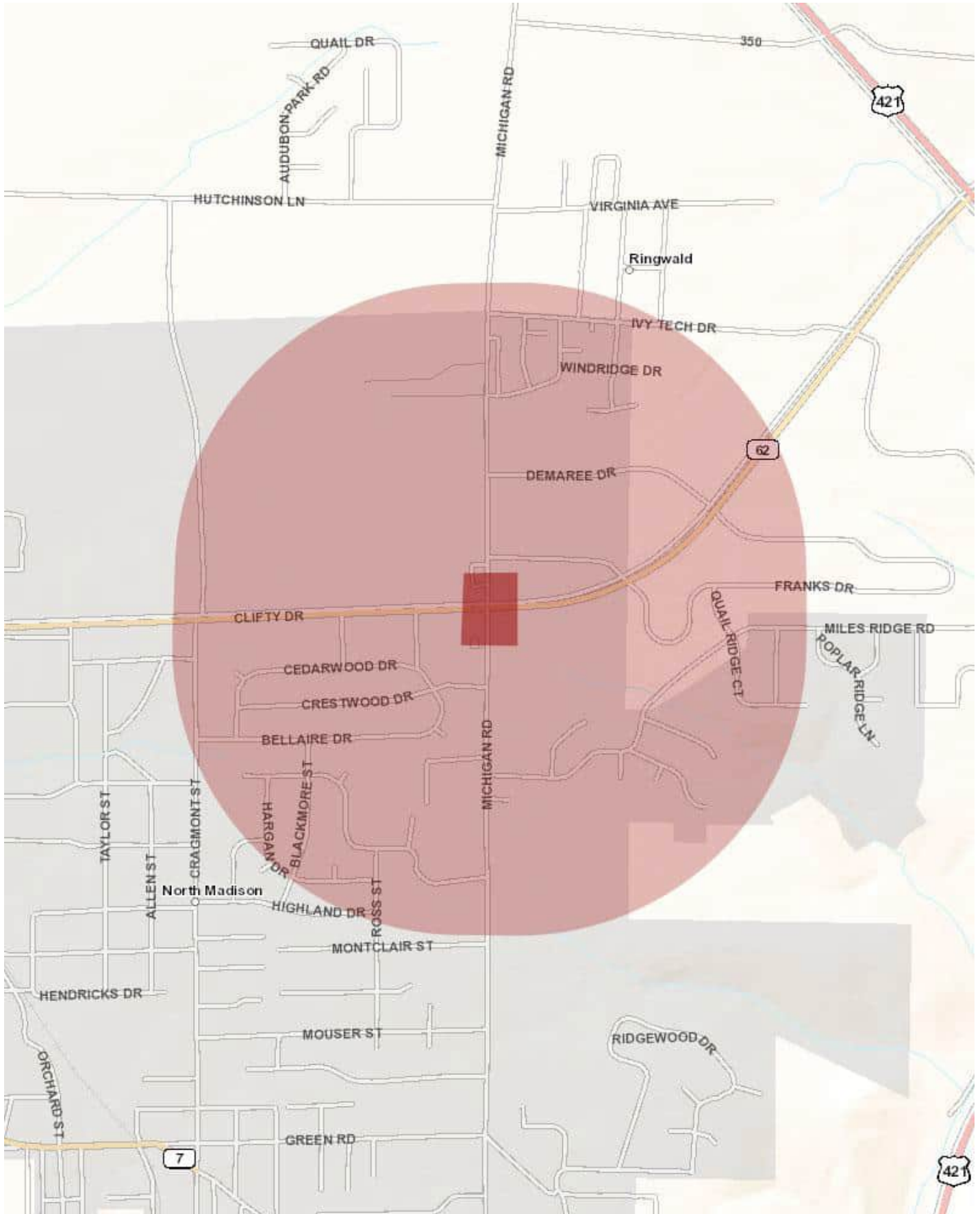
This information was furnished by Indiana Geological Survey

Address: 1001 E. 10th St., Bloomington, IN 47405

Email: [IGSEnvir@indiana.edu](mailto:IGSEnvir@indiana.edu)

Phone: 812 855-7428

Date: June 30, 2021





Note: Project was initially bundled with another Access Management Project. Projects were split and SR 62 and Michigan Road kept Des. No. 1902023; and the other location was assigned a new Des. No. 2101170. Des. No. 2101170 is covered under a separate CE document. This response is for SR 62 and Michigan Rd. only.



## Jefferson County Emergency Management Agency

Troy D. Morgan, Director

REQUEST DATE: 30 June 2021

RE: Early Coordination Designation Number (Des. No.) 1902023, Access Management Project  
~~State Road (SR) 229 from Grayson Street (St.) to north Kroger driveway~~; SR 62 and Michigan Road (Rd.)  
Franklin and Jefferson Counties, Indiana

The Jefferson County Emergency Management Agency (Agency) has reviewed the material you supplied. The Agency has no concerns regarding this project. Please notify this office of any detours or closures associated with this project in advance, to aid us in maintaining continuity of Emergency Services in the affected area.

### CONTACT AUTHORITY:

Jefferson County Emergency Management Agency  
620 Green Road  
Madison, IN 47250  
812.265.7616 (Office)  
812.265.8955 (Fax)

Should you have questions or need additional information, please don't hesitate to contact me.

Best Regards,

A handwritten signature in red ink, appearing to read "TDM", written over a white background.

Troy D. Morgan  
Director

TDM



# Indiana Department of Environmental Management

*We Protect Hoosiers and Our Environment.*

100 N. Senate Avenue • Indianapolis, IN 46204

(800) 451-6027 • (317) 232-8603 • [www.idem.IN.gov](http://www.idem.IN.gov)

Eric J. Holcomb  
Governor

Bruno Pigott  
Commissioner

July 7, 2021

66-33  
Metric Environmental  
Attention: Jessica Peterson  
6971 Hillside Court  
Indianapolis, Indiana 46250

Note: Project was initially bundled with another Access Management Project. Projects were split and SR 62 and Michigan Road kept Des. No. 1902023; and the other location was assigned a new Des. No. 2101170. Des. No. 2101170 is covered under a separate CE document. Only highlighted portions pertain to this project.

Dear Jessica Peterson,

RE: Wellhead Protection Area  
Proximity Determination  
Des No 1902023  
Access Management Projects

Site 1 - SR 229 from Grayson Street  
to North Kroger Driveway  
Batesville, Franklin County, Indiana

Site 2 - SR 62 and Michigan Road  
Madison, Jefferson County, Indiana

Upon review of the above referenced project site, it has been determined that the proposed project areas are not located within a Wellhead Protection Area. The information is accurate to the best of our knowledge; however, there are in some cases a few factors that could impact the accuracy of this determination. Some Wellhead Protection Area Delineations have not been submitted, and many have not been approved by this office. In these cases we use a 3,000 foot fixed radius buffer to make the proximity determination. To find the status of a Public Water Supply System's (PWSS's) Wellhead Protection Area Delineation please visit our tracking database at <http://www.in.gov/idem/cleanwater/2456.htm> and scroll to the bottom of the page.

The Site 1 project area is located within a Source Water Assessment Area for a PWSS's surface water intake. The Site 2 project area is located not within a Source Water Assessment Area for a PWSS's surface water intake. The Source Water Assessment Area relates to the surface water drainage area that water could potentially flow and influence water quality for a PWSS's source of drinking water. The PWSS that could be impacted by the Site 1 project area is Osgood Water Department. A contact person for Osgood Water Department is Anthony J. Wood, and they could be reached via e-mail and/or phone at: [woodwwtp@yahoo.com](mailto:woodwwtp@yahoo.com) and (812) 621-8770. The contact information is provided as a courtesy and reference for you if any issues arise that could potentially impact the water quality for the PWSS during the course of the project. It is not a requirement of IDEM that you contact the system regarding the project.

Note: the Drinking Water Branch has a self service feature which allows one to determine wellhead proximity without submitting the application form. Use the following instructions:

1. Go to <https://www.in.gov/idem/cleanwater/pages/wellhead/>
2. Use the search tool located in the upper left hand corner of the application to zoom to your site of interest by way of city, county, or address; or use the mouse to click on the site of interest displayed on the map.
3. Once the site of interest has been located and selected, use the print tool to create a .pdf of a wellhead protection area proximity determination response.

In the future please consider using this self service feature if it suits your needs.



Please Reduce, Reuse, Recycle

If you have any additional questions please feel free to contact me at the address above or at (317) 233-9158 and [aturnbow@idem.in.gov](mailto:aturnbow@idem.in.gov).

Sincerely,

A handwritten signature in black ink that reads "Alisha Turnbow". The signature is written in a cursive, flowing style.

Alisha Turnbow,  
Environmental Manager  
Ground Water Section  
Drinking Water Branch  
Office of Water Quality

e-cc: Osgood Water Department

July 15, 2021

Jessica Peterson, MS  
Metric Environmental  
6971 Hillside Court  
Indianapolis, Indiana 46250

Note: Project was initially bundled with another Access Management Project. Projects were split and SR 62 and Michigan Road kept Des. No. 1902023; and the other location was assigned a new Des. No. 2101170. Des. No. 2101170 is covered under a separate CE document. This response is for SR 62 and Michigan Rd. only.

Dear Ms. Peterson:

The proposed project to proceed with access management along ~~State Road 229 from Crayson Street to north Kroger driveway~~, State Road 62 and Michigan Road in ~~Franklin and Jefferson~~ Counties, Indiana (Des No 1902023), as referred to in your letter received June 30, 2021, will not cause a conversion of prime farmland.

If you need additional information, please contact John Allen at 317-295-5859.

Sincerely,

**RICHARD**  
**NEILSON**

Digitally signed by  
RICHARD NEILSON  
Date: 2021.07.15  
15:47:48 -04'00'

RICK NEILSON  
State Soil Scientist



State of Indiana  
DEPARTMENT OF NATURAL RESOURCES  
Division of Fish and Wildlife  
Early Coordination/Environmental Assessment

DNR #: ER-23852

Request Received: June 30, 2021

Requestor: Metric Environmental  
Jessica Peterson  
6971 Hillsdale Court  
Indianapolis, IN 46250

Note: Project was initially bundled with another Access Management Project. Projects were split and SR 62 and Michigan Road kept Des. No. 1902023; and the other location was assigned a new Des. No. 2101170. Des. No. 2101170 is covered under a separate CE document. This response is for SR 62 and Michigan Rd. only.

Project: Access management project; Des #1902023:  
~~Site 1) SR 229 from Grayson St. to the north Kroger driveway, Batesville, Franklin Co.~~  
Site 2) SR 62 and Michigan Rd. intersection, Madison; Jefferson Co.

County/Site info: ~~Franklin~~ Jefferson

The Indiana Department of Natural Resources has reviewed the above referenced project per your request. Our agency offers the following comments for your information and in accordance with the National Environmental Policy Act of 1969.

If our agency has regulatory jurisdiction over the project, the recommendations contained in this letter may become requirements of any permit issued. If we do not have permitting authority, all recommendations are voluntary.

Regulatory Assessment: Formal approval by the Department of Natural Resources under the regulatory programs administered by the Division of Water is not required for this project.

Natural Heritage Database: The Natural Heritage Program's data have been checked. To date, no plant or animal species listed as state or federally threatened, endangered, or rare have been reported to occur in the project vicinity.

Fish & Wildlife Comments: The measures below should be implemented to avoid, minimize, or compensate for impacts to fish, wildlife, and botanical resources:  
1. Revegetate all bare and disturbed areas with a mixture of grasses (excluding all varieties of tall fescue) and legumes as soon as possible upon completion; low endophyte tall fescue may be used in the ditch bottom and side slopes only.  
2. Appropriately designed measures for controlling erosion and sediment must be implemented to prevent sediment from entering the stream or leaving the construction site; maintain these measures until construction is complete and all disturbed areas are stabilized.

Contact Staff: Christie L. Stanifer, Environ. Coordinator, Fish & Wildlife  
Our agency appreciates this opportunity to be of service. Please contact the above staff member at (317) 232-4080 if we can be of further assistance.

*Christie L. Stanifer*

Date: July 30, 2021

Christie L. Stanifer  
Environ. Coordinator  
Division of Fish and Wildlife



## United States Department of the Interior



### FISH AND WILDLIFE SERVICE

Indiana Ecological Services Field Office

620 South Walker Street

Bloomington, IN 47403-2121

Phone: (812) 334-4261 Fax: (812) 334-4273

<http://www.fws.gov/midwest/Endangered/section7/s7process/step1.html>

In Reply Refer To:

October 19, 2021

Consultation Code: 03E12000-2022-SLI-0142

Event Code: 03E12000-2022-E-00549

Project Name: Des. No. 1902023, Access Management, SR 62 and Michigan Rd, Madison, Jefferson Co.

Subject: List of threatened and endangered species that may occur in your proposed project location or may be affected by your proposed project

To Whom It May Concern:

The attached species list identifies any federally threatened, endangered, proposed and candidate species that may occur within the boundary of your proposed project or may be affected by your proposed project. The list also includes designated critical habitat if present within your proposed project area or affected by your project. This list is provided to you as the initial step of the consultation process required under section 7(c) of the Endangered Species Act, also referred to as Section 7 Consultation.

Section 7 of the Endangered Species Act of 1973 requires that actions authorized, funded, or carried out by Federal agencies not jeopardize federally threatened or endangered species or adversely modify designated critical habitat. To fulfill this mandate, Federal agencies (or their designated non-federal representative) must consult with the Service if they determine their project “may affect” listed species or critical habitat.

Under 50 CFR 402.12(e) (the regulations that implement Section 7 of the Endangered Species Act) the accuracy of this species list should be verified after 90 days. This verification can be completed formally or informally. You may verify the list by visiting the ECOS-IPaC website <http://ecos.fws.gov/ipac/> at regular intervals during project planning and implementation and completing the same process you used to receive the attached list. As an alternative, you may contact this Ecological Services Field Office for updates.

Please use the species list provided and visit the U.S. Fish and Wildlife Service’s Region 3 Section 7 Technical Assistance website at - <http://www.fws.gov/midwest/endangered/section7/s7process/index.html>. This website contains step-by-step instructions which will help you

determine if your project will have an adverse effect on listed species and will help lead you through the Section 7 process.

For all **wind energy projects** and **projects that include installing towers that use guy wires or are over 200 feet in height**, please contact this field office directly for assistance, even if no federally listed plants, animals or critical habitat are present within your proposed project or may be affected by your proposed project.

Although no longer protected under the Endangered Species Act, be aware that bald eagles are protected under the Bald and Golden Eagle Protection Act (16 U.S.C. 668 *et seq.*) and Migratory Bird Treaty Act (16 U.S.C. 703 *et seq.*), as are golden eagles. Projects affecting these species may require measures to avoid harming eagles or may require a permit. If your project is near an eagle nest or winter roost area, see our Eagle Permits website at <http://www.fws.gov/midwest/midwestbird/EaglePermits/index.html> to help you determine if you can avoid impacting eagles or if a permit may be necessary.

We appreciate your concern for threatened and endangered species. Please include the Consultation Tracking Number in the header of this letter with any request for consultation or correspondence about your project that you submit to our office.

Attachment(s):

- Official Species List

## **Official Species List**

This list is provided pursuant to Section 7 of the Endangered Species Act, and fulfills the requirement for Federal agencies to "request of the Secretary of the Interior information whether any species which is listed or proposed to be listed may be present in the area of a proposed action".

This species list is provided by:

### **Indiana Ecological Services Field Office**

620 South Walker Street  
Bloomington, IN 47403-2121  
(812) 334-4261



## Project Summary

Consultation Code: 03E12000-2022-SLI-0142  
Event Code: Some(03E12000-2022-E-00549)  
Project Name: Des. No. 1902023, Access Management, SR 62 and Michigan Rd,  
Madison, Jefferson Co.  
Project Type: TRANSPORTATION  
Project Description: The Indiana Department of Transportation intends to utilize Federal Highway Administration funds to proceed with an Access Management Project, Des. No. 1902023.

This project is located at the intersection of SR 62 and Michigan Rd. in Madison, in the south-central portion of Jefferson County, Indiana. A typical cross section of SR 62 includes two 12-ft.-wide travel lanes in each direction. The west half of SR 62 is curbed with storm sewers and the eastern half has shoulders with open ditches. The adjacent land use is commercial. The crash rate of the intersection is high due to excessive turning radius, conflicts with the existing median curb, and impeded signal visibility on SR 62.

The draft need for this project is due to the crash frequency and severity in the project area. The draft purpose of this project is to reduce the crash frequency by correcting access conflicts in the project area.

The proposed improvements include pavement removal in the northwest corner, replacing and extending the approach medians, installing full-depth patches, milling and resurfacing, reducing the radius of the northwestern corner, modifying traffic signalization (i.e., installing new LED signal heads, a 5-section signal head, adding yellow flashing arrows, and replacing the signs), and installing a curb turnout to match the existing drainage pattern. Some utility relocation(s) may be required. Traffic will be maintained during construction with single lane closures.

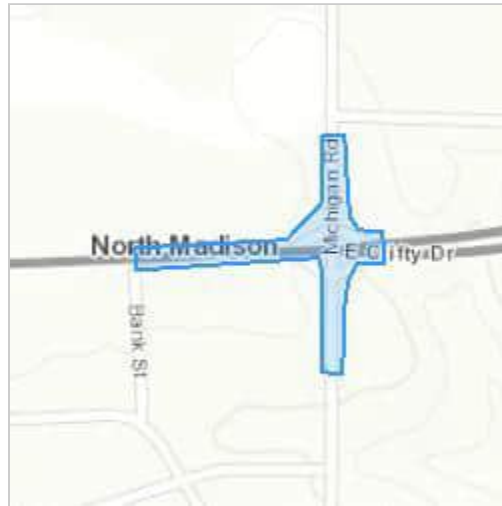
Suitable summer habitat exists within 1,000 ft. of the project areas however, none is present within the project area. No trees will be trimmed or removed for this project.

Traffic signalization (i.e., permanent lighting) will be modified for this project as described above. Temporary lighting may be utilized during construction. Construction is anticipated to begin in March 2023 and will last for approximately 8-9 months.

A review of the USFWS database by INDOT on July 13, 2021 did not indicate the presence of endangered bat species or known roost or hibernacula in or within 0.5 mile of the project area.

Project Location:

Approximate location of the project can be viewed in Google Maps: <https://www.google.com/maps/@38.7751506,-85.38817969423626,14z>



Counties: Jefferson County, Indiana

## Endangered Species Act Species

There is a total of 3 threatened, endangered, or candidate species on this species list.

Species on this list should be considered in an effects analysis for your project and could include species that exist in another geographic area. For example, certain fish may appear on the species list because a project could affect downstream species. Note that 1 of these species should be considered only under certain conditions.

IPaC does not display listed species or critical habitats under the sole jurisdiction of NOAA Fisheries<sup>1</sup>, as USFWS does not have the authority to speak on behalf of NOAA and the Department of Commerce.

See the "Critical habitats" section below for those critical habitats that lie wholly or partially within your project area under this office's jurisdiction. Please contact the designated FWS office if you have questions.

- 
1. [NOAA Fisheries](#), also known as the National Marine Fisheries Service (NMFS), is an office of the National Oceanic and Atmospheric Administration within the Department of Commerce.

## Mammals

NAME	STATUS
Indiana Bat <i>Myotis sodalis</i> There is <b>final</b> critical habitat for this species. The location of the critical habitat is not available. Species profile: <a href="https://ecos.fws.gov/ecp/species/5949">https://ecos.fws.gov/ecp/species/5949</a>	Endangered
Northern Long-eared Bat <i>Myotis septentrionalis</i> No critical habitat has been designated for this species. This species only needs to be considered under the following conditions: <ul style="list-style-type: none"><li>▪ Incidental take of the NLEB is not prohibited here. Federal agencies may consult using the 4(d) rule streamlined process. Transportation projects may consult using the programmatic process. See <a href="http://www.fws.gov/midwest/endangered/mammals/nleb/index.html">www.fws.gov/midwest/endangered/mammals/nleb/index.html</a></li></ul> Species profile: <a href="https://ecos.fws.gov/ecp/species/9045">https://ecos.fws.gov/ecp/species/9045</a>	Threatened

## Insects

NAME	STATUS
Monarch Butterfly <i>Danaus plexippus</i> No critical habitat has been designated for this species. Species profile: <a href="https://ecos.fws.gov/ecp/species/9743">https://ecos.fws.gov/ecp/species/9743</a>	Candidate

## Critical habitats

THERE ARE NO CRITICAL HABITATS WITHIN YOUR PROJECT AREA UNDER THIS OFFICE'S JURISDICTION.



## United States Department of the Interior



### FISH AND WILDLIFE SERVICE

Indiana Ecological Services Field Office

620 South Walker Street

Bloomington, IN 47403-2121

Phone: (812) 334-4261 Fax: (812) 334-4273

<http://www.fws.gov/midwest/Endangered/section7/s7process/step1.html>

In Reply Refer To:

October 19, 2021

Consultation code: 03E12000-2022-I-0142

Event Code: 03E12000-2022-E-00558

Project Name: Des. No. 1902023, Access Management, SR 62 and Michigan Rd, Madison, Jefferson Co.

Subject: Concurrence verification letter for the 'Des. No. 1902023, Access Management, SR 62 and Michigan Rd, Madison, Jefferson Co.' project under the revised February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion for Transportation Projects within the Range of the Indiana Bat and Northern Long-eared Bat.

To whom it may concern:

The U.S. Fish and Wildlife Service (Service) has received your request to verify that the **Des. No. 1902023, Access Management, SR 62 and Michigan Rd, Madison, Jefferson Co.** (Proposed Action) may rely on the concurrence provided in the February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion for Transportation Projects within the Range of the Indiana Bat and Northern Long-eared Bat (PBO) to satisfy requirements under Section 7(a)(2) of the Endangered Species Act of 1973 (ESA) (87 Stat. 884, as amended; 16 U.S.C 1531 *et seq.*).

Based on the information you provided (Project Description shown below), you have determined that the Proposed Action is within the scope and adheres to the criteria of the PBO, including the adoption of applicable avoidance and minimization measures, and may affect, but is not likely to adversely affect (NLAA) the endangered Indiana bat (*Myotis sodalis*) and/or the threatened Northern long-eared bat (*Myotis septentrionalis*).

The Service has 14 calendar days to notify the lead Federal action agency or designated non-federal representative if we determine that the Proposed Action does not meet the criteria for a NLAA determination under the PBO. If we do not notify the lead Federal action agency or designated non-federal representative within that timeframe, you may proceed with the Proposed Action under the terms of the NLAA concurrence provided in the PBO. This verification period allows Service Field Offices to apply local knowledge to implementation of the PBO, as we may identify a small subset of actions having impacts that were unanticipated. In such instances,

Service Field Offices may request additional information that is necessary to verify inclusion of the proposed action under the PBO.

**For Proposed Actions that include bridge/structure removal, replacement, and/or maintenance activities:** If your initial bridge/structure assessments failed to detect Indiana bats, but you later detect bats during construction, please submit the Post Assessment Discovery of Bats at Bridge/Structure Form (User Guide Appendix E) to this Service Office. In these instances, potential incidental take of Indiana bats may be exempted provided that the take is reported to the Service.

If the Proposed Action is modified, or new information reveals that it may affect the Indiana bat and/or Northern long-eared bat in a manner or to an extent not considered in the PBO, further review to conclude the requirements of ESA Section 7(a)(2) may be required. If the Proposed Action may affect any other federally-listed or proposed species, and/or any designated critical habitat, additional consultation between the lead Federal action agency and this Service Office is required. If the proposed action has the potential to take bald or golden eagles, additional coordination with the Service under the Bald and Golden Eagle Protection Act may also be required. In either of these circumstances, please contact this Service Office.

The following species may occur in your project area and **are not** covered by this determination:

- Monarch Butterfly *Danaus plexippus* Candidate

## **Project Description**

The following project name and description was collected in IPaC as part of the endangered species review process.

### ***Name***

Des. No. 1902023, Access Management, SR 62 and Michigan Rd, Madison, Jefferson Co.

### ***Description***

The Indiana Department of Transportation intends to utilize Federal Highway Administration funds to proceed with an Access Management Project, Des. No. 1902023.

This project is located at the intersection of SR 62 and Michigan Rd. in Madison, in the south-central portion of Jefferson County, Indiana. A typical cross section of SR 62 includes two 12-ft.-wide travel lanes in each direction. The west half of SR 62 is curbed with storm sewers and the eastern half has shoulders with open ditches. The adjacent land use is commercial. The crash rate of the intersection is high due to excessive turning radius, conflicts with the existing median curb, and impeded signal visibility on SR 62.

The proposed improvements include pavement removal in the northwest corner, replacing and extending the approach medians, installing full-depth patches, milling and resurfacing, reducing the radius of the northwestern corner, modifying traffic signalization (i.e., installing new LED signal heads, a 5-section signal head, adding yellow flashing arrows, and replacing the signs), and installing a curb turnout to match the existing drainage pattern. Some utility relocation(s) may be required. Traffic will be maintained during construction with single lane closures.

Suitable summer habitat exists within 1,000 ft. of the project areas however, none is present within the project area. No trees will be trimmed or removed for this project.

Traffic signalization (i.e., permanent lighting) will be modified for this project as described above. Temporary lighting may be utilized during construction. Construction is anticipated to begin in March 2023 and will last for approximately 8-9 months.

A review of the USFWS database by INDOT on July 13, 2021 did not indicate the presence of endangered bat species or known roost or hibernacula in or within 0.5 mile of the project area.

# Determination Key Result

Based on your answers provided, this project(s) may affect, but is not likely to adversely affect the endangered Indiana bat and/or the threatened Northern long-eared bat, therefore, consultation with the U.S. Fish and Wildlife Service pursuant to Section 7(a)(2) of the Endangered Species Act of 1973 (ESA) (87 Stat. 884, as amended 16 U.S.C. 1531 *et seq.*) is required. However, also based on your answers provided, this project may rely on the concurrence provided in the revised February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion for Transportation Projects within the Range of the Indiana Bat and Northern Long-eared Bat.

## Qualification Interview

1. Is the project within the range of the Indiana bat<sup>[1]</sup>?

[1] See [Indiana bat species profile](#)

**Automatically answered**

Yes

2. Is the project within the range of the Northern long-eared bat<sup>[1]</sup>?

[1] See [Northern long-eared bat species profile](#)

**Automatically answered**

Yes

3. Which Federal Agency is the lead for the action?

*A) Federal Highway Administration (FHWA)*

4. Are *all* project activities limited to non-construction<sup>[1]</sup> activities only? (examples of non-construction activities include: bridge/abandoned structure assessments, surveys, planning and technical studies, property inspections, and property sales)

[1] Construction refers to activities involving ground disturbance, percussive noise, and/or lighting.

No

5. Does the project include *any* activities that are **greater than** 300 feet from existing road/rail surfaces<sup>[1]</sup>?

[1] Road surface is defined as the actively used [e.g. motorized vehicles] driving surface and shoulders [may be pavement, gravel, etc.] and rail surface is defined as the edge of the actively used rail ballast.

No

6. Does the project include *any* activities **within** 0.5 miles of a known Indiana bat and/or NLEB hibernaculum<sup>[1]</sup>?

[1] For the purpose of this consultation, a hibernaculum is a site, most often a cave or mine, where bats hibernate during the winter (see suitable habitat), but could also include bridges and structures if bats are found to be hibernating there during the winter.

No

7. Is the project located **within** a karst area?

No

8. Is there *any* suitable<sup>[1]</sup> summer habitat for Indiana Bat or NLEB **within** the project action area<sup>[2]</sup>? (includes any trees suitable for maternity, roosting, foraging, or travelling habitat)

[1] See the Service's [summer survey guidance](#) for our current definitions of suitable habitat.

[2] The action area is defined as all areas to be affected directly or indirectly by the Federal action and not merely the immediate area involved in the action (50 CFR Section 402.02). Further clarification is provided by the [national consultation FAQs](#).

*Yes*

9. Will the project remove *any* suitable summer habitat<sup>[1]</sup> and/or remove/trim any existing trees **within** suitable summer habitat?

[1] See the Service's [summer survey guidance](#) for our current definitions of suitable habitat.

*No*

10. Have presence/probable absence (P/A) summer surveys<sup>[1][2]</sup> been conducted<sup>[3][4]</sup> **within** the suitable habitat located within your project action area?

[1] See the Service's [summer survey guidance](#) for our current definitions of suitable habitat.

[2] Presence/probable absence summer surveys conducted within the fall swarming/spring emergence home range of a documented Indiana bat hibernaculum (contact local Service Field Office for appropriate distance from hibernacula) that result in a negative finding requires additional consultation with the local Service Field Office to determine if clearing of forested habitat is appropriate and/or if seasonal clearing restrictions are needed to avoid and minimize potential adverse effects on fall swarming and spring emerging Indiana bats.

[3] For projects within the range of either the Indiana bat or NLEB in which suitable habitat is present, and no bat surveys have been conducted, the transportation agency will assume presence of the appropriate species. This assumption of presence should be based upon the presence of suitable habitat and the capability of bats to occupy it because of their mobility.

[4] Negative presence/probable absence survey results obtained using the [summer survey guidance](#) are valid for a minimum of two years from the completion of the survey unless new information (e.g., other nearby surveys) suggest otherwise.

*No*

11. Does the project include activities **within documented Indiana bat habitat**<sup>[1][2]</sup>?

[1] Documented roosting or foraging habitat – for the purposes of this consultation, we are considering documented habitat as that where Indiana bats and/or NLEB have actually been captured and tracked using (1) radio telemetry to roosts; (2) radio telemetry triangulation/triangulation to estimate foraging areas; or (3) foraging areas with repeated use documented using acoustics. Documented roosting habitat is also considered as suitable summer habitat within 0.25 miles of documented roosts.)

[2] For the purposes of this key, we are considering documented corridors as that where Indiana bats and/or NLEB have actually been captured and tracked to using (1) radio telemetry; or (2) treed corridors located directly between documented roosting and foraging habitat.

*No*



12. Does the project include activities **within documented NLEB habitat**<sup>[1][2]</sup>?

[1] Documented roosting or foraging habitat – for the purposes of this consultation, we are considering documented habitat as that where Indiana bats and/or NLEB have actually been captured and tracked using (1) radio telemetry to roosts; (2) radio telemetry triangulation/triangulation to estimate foraging areas; or (3) foraging areas with repeated use documented using acoustics. Documented roosting habitat is also considered as suitable summer habitat within 0.25 miles of documented roosts.)

[2] For the purposes of this key, we are considering documented corridors as that where Indiana bats and/or NLEB have actually been captured and tracked to using (1) radio telemetry; or (2) treed corridors located directly between documented roosting and foraging habitat.

*No*

13. Does the project include wetland or stream protection activities associated with compensatory wetland mitigation?

*No*

14. Does the project include slash pile burning?

*No*

15. Does the project include *any* bridge removal, replacement, and/or maintenance activities (e.g., any bridge repair, retrofit, maintenance, and/or rehabilitation work)?

*No*

16. Does the project include the removal, replacement, and/or maintenance of *any* structure other than a bridge? (e.g., rest areas, offices, sheds, outbuildings, barns, parking garages, etc.)

*No*

17. Will the project involve the use of **temporary** lighting *during* the active season?

*Yes*

18. Is there *any* suitable habitat **within** 1,000 feet of the location(s) where **temporary** lighting will be used?

*Yes*

19. Will the project install new or replace existing **permanent** lighting?

*Yes*

20. Is there *any* suitable habitat **within** 1,000 feet of the location(s) where **permanent** lighting will be installed or replaced?

*Yes*

21. Does the project include percussives or other activities (**not including tree removal/trimming or bridge/structure work**) that will increase noise levels above existing traffic/background levels?

*Yes*

22. Will the activities that use percussives (**not including tree removal/trimming or bridge/structure work**) and/or increase noise levels above existing traffic/background levels be conducted *during* the active season<sup>[1]</sup>?

[1] Coordinate with the local Service Field Office for appropriate dates.

Yes

23. Will *any* activities that use percussives (**not including tree removal/trimming or bridge/structure work**) and/or increase noise levels above existing traffic/background levels be conducted *during* the inactive season<sup>[1]</sup>?

[1] Coordinate with the local Service Field Office for appropriate dates.

Yes

24. Are *all* project activities that are **not associated with** habitat removal, tree removal/trimming, bridge and/or structure activities, temporary or permanent lighting, or use of percussives, limited to actions that DO NOT cause any additional stressors to the bat species?

Examples: lining roadways, unlighted signage , rail road crossing signals, signal lighting, and minor road repair such as asphalt fill of potholes, etc.

Yes

25. Will the project raise the road profile **above the tree canopy**?

No

26. Are the project activities that use percussives (not including tree removal/trimming or bridge/structure work) consistent with a Not Likely to Adversely Affect determination in this key?

**Automatically answered**

*Yes, because the activities are within 300 feet of the existing road/rail surface, greater than 0.5 miles from a hibernacula, and conducted during the active season within undocumented habitat.*

27. Are the project activities that use percussives (not including tree removal/trimming or bridge/structure work) and/or increase noise levels above existing traffic/background levels consistent with a No Effect determination in this key?

**Automatically answered**

*Yes, because the activities are within 300 feet of the existing road/rail surface, greater than 0.5 miles from a hibernacula, and conducted during the inactive season*

28. **General AMM 1**

Will the project ensure *all* operators, employees, and contractors working in areas of known or presumed bat habitat are aware of *all* FHWA/FRA/FTA (Transportation Agencies) environmental commitments, including all applicable Avoidance and Minimization Measures?

Yes

29. **Lighting AMM 1**

Will *all* **temporary** lighting be directed away from suitable habitat during the active season?

Yes

30. **Lighting AMM 2**

Does the lead agency use the BUG (Backlight, Uplight, and Glare) system developed by the Illuminating Engineering Society<sup>[1][2]</sup> to rate the amount of light emitted in unwanted directions?

[1] Refer to [Fundamentals of Lighting - BUG Ratings](#)

[2] Refer to [The BUG System—A New Way To Control Stray Light](#)

Yes

31. **Lighting AMM 2**

Will the **permanent** lighting be designed to be as close to 0 for all three BUG ratings as possible, with a priority of "uplight" of 0 and "backlight" as low as practicable?

Yes

## Project Questionnaire

1. Have you made a No Effect determination for *all* other species indicated on the FWS IPaC generated species list?

N/A

2. Have you made a May Affect determination for *any* other species on the FWS IPaC generated species list?

N/A

## Avoidance And Minimization Measures (AMMs)

This determination key result includes the commitment to implement the following Avoidance and Minimization Measures (AMMs):

### LIGHTING AMM 1

Direct temporary lighting away from suitable habitat during the active season.

### LIGHTING AMM 2

When installing new or replacing existing permanent lights, use downward-facing, full cut-off lens lights (with same intensity or less for replacement lighting); or for those transportation agencies using the BUG system developed by the Illuminating Engineering Society, be as close to 0 for all three ratings with a priority of "uplight" of 0 and "backlight" as low as practicable.

### GENERAL AMM 1

Ensure all operators, employees, and contractors working in areas of known or presumed bat habitat are aware of all FHWA/FRA/FTA (Transportation Agencies) environmental commitments, including all applicable AMMs.

## **Determination Key Description: FHWA, FRA, FTA Programmatic Consultation For Transportation Projects Affecting NLEB Or Indiana Bat**

This key was last updated in IPaC on April 22, 2021. Keys are subject to periodic revision.

This decision key is intended for projects/activities funded or authorized by the Federal Highway Administration (FHWA), Federal Railroad Administration (FRA), and/or Federal Transit Administration (FTA), which may require consultation with the U.S. Fish and Wildlife Service (Service) under Section 7 of the Endangered Species Act (ESA) for the endangered **Indiana bat** (*Myotis sodalis*) and the threatened **Northern long-eared bat** (NLEB) (*Myotis septentrionalis*).

This decision key should only be used to verify project applicability with the Service's [February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion for Transportation Projects](#). The programmatic biological opinion covers limited transportation activities that may affect either bat species, and addresses situations that are both likely and not likely to adversely affect either bat species. This decision key will assist in identifying the effect of a specific project/activity and applicability of the programmatic consultation. The programmatic biological opinion is not intended to cover all types of transportation actions. Activities outside the scope of the programmatic biological opinion, or that may affect ESA-listed species other than the Indiana bat or NLEB, or any designated critical habitat, may require additional ESA Section 7 consultation.

SR 62 & Michigan Road Des. No. 1902023

Date: February 11, 2021

**Subject:**

Utility Relocation Work Plan for:	MADISON UTILITIES
Facility Type:	WATER & SEWER

**Section 1: General Information**

A. INDOT/LPA Project Information

1. DES NO.:	1902023
2. Route Number:	SR 62 & Michigan Road
3. Location:	SR 62 & Michigan Road
4. Work Type:	Access Improvement Project
5. Letting Date:	August 3, 2022
6. Date Work Plan Needed	October 1, 2021
7. Target Date for Utility to be out of conflict with INDOT Project	
Intermediate Phase	
Intermediate Phase	

B. Utility Designated Contact – Information

1. Designated Contact Name:	BRIAN JACKSON
2. Office telephone:	812-265-8326
3. Mobile telephone:	812-701-4094
4. Email address:	<a href="mailto:utilitymanager@madison-in.gov">utilitymanager@madison-in.gov</a>
5. Agency name	City of Madison Utilities
6. Address:	101 W Main Street
7. City, State, Zip Code:	Madison, IN 47250
8. Construction Emergency Contact:	
Name:	BRIAN JACKSON
Number:	812-701-4094

C. By signing here, the Utility has determined to the best of their ability that they do not have facilities within the project area:

\_\_\_\_\_  
Signature of Utility Representative

\_\_\_\_\_  
Print Name

\_\_\_\_\_  
Date

**Note:** A signature by the utility representative at item "(C)" fulfills the requirement to complete the rest of this form and affirms their contact information above is correct

D. INDOT/LPA Utility Coordinator Contact Information

1.	Utility Coordinator Name:	Jason Hoy
2.	Office Telephone:	812-372-9911
3.	Mobile Telephone:	
4.	Email Address:	<a href="mailto:Jason.hoy@strand.com">Jason.hoy@strand.com</a>
5.	Agency Name:	Strand Associates Inc.
6.	Address:	629 Washington Street
7.	City, State, Zip Code	Columbus, IN, 47201

**Section 2:** A narrative description of existing facilities within the project limits and any facility relocation that will be required. [IAC 13-3-3(c)]

- A. Describe what types of existing active and inactive facilities are present.  
**There is an active 10" Water Main crossing State Road #62.**  
**There is an active 8" Water Main crossing Michigan Road.**
- B. Describe the location of existing active and inactive facilities.  
**The 10" Water Main crossing State Road #62 is located at approximately Station 996+80.**  
**The 8" Water Main crossing Michigan Road is located at approximately Station 52+85.**
- C. Describe what will be done with existing active and inactive facilities.  
**Nothing will be done with the existing water lines because they should not be affected by the project.**
- D. Describe the details of the proposed new facilities.  
**N/A**
- E. Describe the proposed location of the new facilities.  
**N/A**
- F. By signing here, the Utility has determined to the best of their ability that they have facilities within the project area and the facilities are not in conflict with the project based upon the plans received on <February 26, 2021>

  
 Signature of Utility Representative

**BRIAN R. JACKSON** 3/2/21  
 Print Name Date

**Note:** A signature by the utility representative at item "(F)" fulfills the requirement to complete the rest of this form and affirms their contact information above is correct.

# **APPENDIX D: Section 106 of the National Historic Preservation Act**

## Minor Projects PA Project Assessment Form

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**Date:** 6/25/2021

**Note: Des. No. 2101170/SR 229, Batesville is no longer part of the scope of this project and INDOT CRO has been informed of this.**

**Project Designation Number:** 1902023 and 2101170

**Route Number:** State Road (SR) 229 & SR 62

**Project Description:** SR 229 and SR 62 Access Improvements Project

The proposed project consists of two roadway access improvements. The first is located on SR 229 between Grayson Street and Northside Drive, a distance of approximately 850 feet, within the city of Batesville in Franklin County. The second is located at the intersection of SR 62 and Michigan Road within the city of Madison in Jefferson County. The need for this project is based on the crash frequency and severity at these locations. In Batesville, there are commercial driveways through SR 229 that cause vehicles to queue past Grayson Street and create safety issues. In Madison, the intersection has experienced a high crash rate due to an excessive turning radius on SR 62 and impeded signal visibility, as well as due to vehicles ignoring or evading the existing median curb. The purpose of the project is to reduce the crash frequency at both locations.

Currently SR 229 includes two (2) 12-foot-wide lanes in each direction with exclusive left-turn lanes northbound and southbound at the Grayson Street intersection, a northbound right-turn-only lane that ends at a commercial entrance 570 feet north of Grayson Street, and a left-turn lane at the Northside Drive intersection. Through the project area there is full access to commercial driveways along SR 229 with no restrictions. Current roadway design on SR 62 in Madison includes a left-turn lane, two through-lanes, and one right-turn lane in both the east- and west-bound lanes. The northbound and southbound approaches of Michigan Road have a left-turn lane, a through-lane, and a right-turn lane.

The undertaking would reduce access points at both locations. On SR 229 the project consists of installation of a two-foot-wide median curb beginning at SR 229 and Grayson Street to Northside Drive, where signals currently exist at both intersections. The median curb would force all left turns to occur at these intersections through the project area. The signal heads at Northside Drive would be adjusted to align with the new northbound lane locations and the left-only signs will be reset at the entrance. The two-way left-turn lanes will be converted to left-turn-only lanes in each direction. The northbound lane striping would need to be shifted on SR 229 to maintain the current lane width after the median has been installed. For the median installation, the existing pavement would be cut one foot on each side for construction, and the area would be patched with full-depth asphalt. One lane width would be milled and resurfaced on each side of the new median. Solid white striping would be added where the outside northbound lane becomes an exclusive right turn lane and then is dropped at the first driveway south of Northside Drive.

Since the existing medians at the intersection of SR 62 and Michigan Road are not currently long enough to deter left turns, the project proposes to extend both the current north and south approach medians. The median north of SR 62 would be extended approximately 100 feet to the first drive on the west; the existing median south of SR 62 would be extended approximately 200 feet south until the first drive on the west. On SR 62 the median west of Michigan Road would be extended through the taper to the west, extending the existing median approximately 130 feet. In addition to the extension of the center medians, the existing medians will be replaced.

For installation of the new medians, the existing pavement will be cut 1 foot on each side for construction, and the area would be patched with full-depth asphalt. Additional project activities at this intersection include reducing the radius at the northwestern corner, as well as reducing the driveway width on the west side of Michigan Road to north of SR 62. Improvements to the current traffic signals include new LED signal heads, 5-section signal head for northbound right turns, yellow flashing arrows for north and southbound left turns, and replacing the signpost on the west end of the median on the west leg. Construction of the medians would be done under single lane closures.



## Minor Projects PA Project Assessment Form

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There will be modification of the curb radius at the south corner of the T-intersection of SR 229 and Northside Drive in Batesville, as well as at the northwest corner of SR 62 and Michigan Road in Madison to channelize traffic. Both areas would require new curbing.

**Feature crossed (if applicable):** N/A

**City/Township:** Batesville, Laughery Township & Madison, Madison Township

**County:** Franklin County & Jefferson County

**Information reviewed (please check all that apply):**

- General project location map     USGS map     Aerial photograph     Interim Report  
 Written description of project area     General project area photos     Soil survey data  
 Previously completed historic property reports     Previously completed archaeology reports  
 Bridge Inspection Information     SHAARD     SHAARD GIS     Streetview Imagery

**Other (please specify):** Indiana State Historic Architectural and Archaeological Research Database (SHAARD); Indiana Buildings, Bridges, and Cemeteries Map website; *Franklin County Interim Report*; *Jefferson County Interim Report*; Arc Map GIS; Franklin County GIS (accessed via <https://franklinin.wthgis.com>); Jefferson County GIS (accessed via <https://jeffersonin.wthgis.com>) online street-view imagery; MPPA application (including maps and photographs) sent by Metric Environmental, dated June 2<sup>nd</sup>, 2021 and on file at Cultural Resources Office (CRO).

**Please specify all applicable categories and condition(s) (conditions that are applicable are highlighted):**

**B-1.** Replacement, repair, or installation of curbs, curb ramps, or sidewalks, including when such projects are associated with roadway work such as surface replacement, reconstruction, rehabilitation, or resurfacing projects, including overlays, shoulder treatments, pavement repair, seal coating, pavement grinding, and pavement marking, under the following conditions [***BOTH Condition A, which pertains to Archaeological Resources, and Condition B, which pertains to Above-Ground Resources, must be satisfied***]:

**Condition A (Archaeological Resources)**

One of the two conditions listed below must be satisfied (*EITHER Condition i or Condition ii must be satisfied*):

- i. Work occurs in previously disturbed soils; OR
- ii. Work occurs in undisturbed soils and an archaeological investigation conducted by the applicant and reviewed by INDOT Cultural Resources Office determines that no National Register-listed or potentially National Register-eligible archaeological resources are present within the project area. If the archaeological investigation locates National Register-listed or potentially National Register-eligible archaeological resources, then full Section 106 review will be required. Copies of any archaeological reports prepared for the project will be provided to the Division of Historic Preservation and Archaeology (DHPA) and any archaeological site form information will be entered directly into the State Historic Architectural and Archaeological Database (SHAARD) by the applicant. The archaeological reports will also be available for viewing (by Tribes only) on INSCOPE.

**Condition B (Above-Ground Resources)**

One of the two conditions listed below must be satisfied (*EITHER Condition i or Condition ii must be satisfied*):

- i. Work does not occur adjacent to or within a National Register-listed or National Register-eligible district or individual above-ground resource; OR

## Minor Projects PA Project Assessment Form

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- ii. Work occurs adjacent to or within a National Register-listed or National Register-eligible district or individual above-ground resource under one of the two additional conditions listed below (*EITHER Condition a OR Condition b must be met, and field work and documentation must be completed as described below*):
  - a. No unusual features, including but not limited to historic brick or stone sidewalks, curbs or curb ramps, stepped or elevated sidewalks and historic brick or stone retaining walls are present in the project area adjacent to or within a National Register-listed or National Register-eligible district or individual above-ground resource; *OR*
  - b. Unusual features, including but not limited to historic brick or stone sidewalks, curbs or curb ramps, stepped or elevated sidewalks and historic brick or stone retaining walls are present in the project area adjacent to or within a National Register-listed or National Register-eligible individual above-ground resource or district and ANY ONE of the conditions (1, 2, or 3) listed below must be fulfilled:
    - 1. Unusual features described above will not be impacted by the project. Firm commitments regarding the avoidance of these features must be listed in the MPPA determination form and the NEPA document and must be entered into the INDOT Project Commitments Database. These projects will also be flagged for quality assurance reviews by INDOT Cultural Resources Office during/after project construction.
    - 2. Unusual features described above have been determined not to contribute to the significance of the historic resource by INDOT Cultural Resources Office in consultation with the SHPO based on an analysis and justification prepared by their staff or review of such information from other qualified professional historians.
    - 3. Impacts to unusual features described above have been determined by INDOT Cultural Resources Office to be so minimal that they do not diminish any of the characteristics that contribute to the significance of the historic resource, based on an analysis and justification prepared by their staff or review of such information from other qualified

**B-2.** Installation of new lighting, signals, signage and other traffic control devices under the following conditions [*BOTH Condition A, which pertains to Archaeological Resources, and Condition B, which pertains to Above-Ground Resources, must be satisfied*]:

**Condition A (Archaeological Resources)**

One of the two conditions listed below must be met (*EITHER Condition i or Condition ii must be satisfied*):

- i. Work occurs in previously disturbed soils; *OR*
- ii. Work occurs in undisturbed soils and an archaeological investigation conducted by the applicant and reviewed by INDOT Cultural Resources Office determines that no National Register-listed or potentially National Register-eligible archaeological resources are present within the project area. If the archaeological investigation locates National Register-listed or potentially National Register-eligible archaeological resources, then full Section 106 review will be required. Copies of any archaeological reports prepared for the project will be provided to the DHPA and any archaeological site form information will be entered directly into the SHAARD by the applicant. The archaeological reports will also be available for viewing (by Tribes only) on INSCOPE.

**Condition B (Above-Ground Resources)**

Work does not occur adjacent to or within a National Register-listed or National Register-eligible district or individual above-ground resource.

**B-3.** Construction of added travel, turning, or auxiliary lanes (e.g., bicycle, truck climbing, acceleration and deceleration lanes) and shoulder widening under the following conditions [*BOTH Condition A, which pertains to Archaeological Resources, and Condition B, which pertains to Above-Ground Resources, must be satisfied*]:

**Condition A (Archaeological Resources)**

One of the two conditions listed below must be met (*EITHER Condition i or Condition ii must be satisfied*):

- i. Work occurs in previously disturbed soils; *OR*

## Minor Projects PA Project Assessment Form

- ii. Work occurs in undisturbed soils and an archaeological investigation conducted by the applicant and reviewed by INDOT Cultural Resources Office determines that no National Register-listed or potentially National Register-eligible archaeological resources are present within the project area. If the archaeological investigation locates National Register-listed or potentially National Register-eligible archaeological resources, then full Section 106 review will be required. Copies of any archaeological reports prepared for the project will be provided to the DHPA and any archaeological site form information will be entered directly into the SHAARD by the applicant. The archaeological reports will also be available for viewing (by Tribes only) on INSCOPE.

### **Condition B (Above-Ground Resources)**

Work does not occur adjacent to or within a National Register-listed or National Register-eligible district or individual above-ground resource.

Are there any commitments associated with this project? If yes, please explain and include in the Additional Comments Section below.      yes       no

Does the project result in a de minimis impact to a Section 4(f) protected historic resource? If yes, please explain in the Additional Comments Section below.      yes       no

Additional Comments:

### Above-ground Resources

An INDOT Cultural Resources historian who meets the Secretary of the Interior's Professional Qualification Standards as per 36 CFR Part 61 performed a desktop review, checking the Indiana Register of Historic Sites and Structures (State Register) and National Register of Historic Places (National Register) lists for Franklin and Jefferson County. No listed resources are located immediately adjacent to the project area, a distance that serves as an adequate area of potential effects given the project scope and setting.

The Indiana Historic Sites and Structures Inventory (IHSSI) and National Register information for Franklin and Jefferson County are available in the Indiana State Historic Architectural and Archaeological Research Database (SHAARD) and the Indiana Historic Buildings, Bridges, and Cemeteries Map (IHBBCM). The *Franklin County Interim Report* (2011; Ray Township) and *Jefferson County Interim Report* (1989; Madison Township) were also consulted. All sites were reviewed through the IHBBCM, which contains the most recently updated SHAARD information. No IHSSI documented resources are located immediately adjacent to the project area.

According to the IHSSI rating system, generally properties rated "contributing" do not possess the level of historical or architectural significance necessary to be considered individually National Register-eligible, although they would contribute to a historic district. If they retain material integrity, properties rated "notable" might possess the necessary level of significance after further research. Properties rated "outstanding" usually possess the necessary level of significance to be considered National Register eligible if they retain material integrity.

Because the location of the project is not adjacent to a National Register-listed or eligible resource, a field visit by a Qualified Professional historian is not required to review the curb replacement along the project area. The project area was reviewed by an INDOT-CRO historian through aerial photography, online street-view imagery, and the Franklin and Jefferson County GIS websites. The project area consists of two separate locations. The first is along SR 229 between Northside Drive and Grayson Street in Batesville. The second is at the intersection of SR 62, locally known as Clifty Drive, and Michigan Road in Madison. Both sites are located in a suburban setting. The adjacent above-ground resources in both locations primarily consist of mid-twentieth to early twenty-first century commercial buildings. New curbing would be required SR 229 and Northside Drive in Batesville, as well as the northwest corner of SR 62 and Michigan Road in Madison. None of the resources immediately adjacent to the project area appear to possess the significance or integrity required to be considered NRHP-eligible. Therefore, Category B-1, Condition B-i is applicable for the proposed work at this intersection.

**Based on the available information, as summarized above, no above-ground concerns exist.**

## Minor Projects PA Project Assessment Form

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### Archaeological Resources

An INDOT Cultural Resources Office (CRO) archaeologist, who meets the Secretary of the Interior's Professional Qualification Standards as per 36 CFR Part 61, reviewed the MPPA request submitted by Metric Environmental, LLC. dated 6/2/2021 and conducted a desktop review of the project area and completed an archaeological assessment.

With regard to archaeological resources, the proposed projects on SR 229 in Franklin County and SR 62 in Jefferson County are limited to the installation of curbs and medians, traffic signal installation, turning lane adjustments/conversion, and the replacement of a signpost within the project limits. All work will occur in soils disturbed by the above-mentioned state roads, turn lanes, ditches, storm sewers, medians, traffic signals, lights, signs and utilities. The area immediately surrounding the ROW has been disturbed by commercial development. According to SHAARD GIS, there are no archaeological sites located within or adjacent to the project areas. Since no right-of-way is being acquired and the project will be confined to excavation work in previously disturbed soils, there are no archaeological concerns.

**Accidental Discovery:** If any archaeological artifacts or human remains are uncovered during construction, demolition, or earth moving activities, construction within 100 feet of the discovery will be stopped, and the INDOT Cultural Resources Office and the Division of Historic Preservation and Archaeology will be notified immediately.

**INDOT Cultural Resources staff reviewer(s):** Patricia Jo Korzeniewski and Clint Kelly

*\*\*\*Be sure to attach this form to the National Environmental Policy Act documentation for this project. Also, the NEPA documentation shall reference and include the description of the specific stipulation in the PA that qualifies the project as exempt from further Section 106 review.*

# **APPENDIX E: Red Flag and Hazardous Materials**

**From:** Foheybreting, Nicole K <NFoheyBreting@indot.IN.gov>  
**Sent:** Thursday, November 18, 2021 12:49 PM  
**To:** Jessica Peterson <jessicap@metricenv.com>  
**Subject:** RE: Des. No. 1902023 RFI

Greetings Jessica –

I discussed this project with Marlene and Ron Bales. They agree that no RFI is warranted. Although the listing for altering traffic is not specifically called out, the proposed scope of work would address access issues; however, the impact (from an RFI standpoint) is minimal.

Thank you and I hope you have a great rest of the week.

Sincerely,  
Nicole

**Nicole Fohey-Breting**

Major Projects / LPA Review Liaison  
100 North Senate Avenue N758-ES  
Indianapolis, Indiana 46204  
**Office:** (317) 416-7084  
**Email:** [NFoheyBreting@indot.in.gov](mailto:NFoheyBreting@indot.in.gov)  
**Office Hours:** 8 to 4 PM



The Site Assessment and Management (SAM) Manual can be found at  
<https://www.in.gov/indot/engineering/environmental-services/environmental-policy/site-assessment-and-management/>

Be sure to refer to the updated information in the SAM Manual for document preparation and submission.

# Infrastructure SR 62 and Michigan Rd. Des. No. 190223, Access Management Jefferson County, Indiana

**Note: Maps provided for illustrative purposes only. No concurrence by INDOT ESD SAM required.**



**Sources:** 0.15 0.075 0 0.15 Miles  
**Non Orthophotography Data** - Obtained from the State of Indiana Geographical Information Office Library  
**Orthophotography** - Obtained from Indiana Map Framework Data ([www.indianamap.org](http://www.indianamap.org))  
**Map Projection:** UTM Zone 16 N **Map Datum:** NAD83  
**This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.**

	Religious Facility		Recreation Facility		Project Area
	Airport		Pipeline		Half Mile Radius
	Cemeteries		Railroad		Toll
	Hospital		Trails		Interstate
	School		Managed Lands		State Route
			County Boundary		US Route
					Local Road

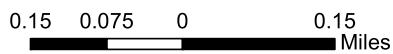
Water Resources  
 SR 62 and Michigan Rd.  
 Des. No. 1902023, Access Management  
 Jefferson County, Indiana

Note: Maps provided for illustrative purposes only. No concurrence by INDOT ESD SAM required.



Sources:  
 Non Orthophotography Data - Obtained from the State of Indiana Geographical Information Office Library  
 Orthophotography - Obtained from Indiana Map Framework Data ([www.indianamap.org](http://www.indianamap.org))  
 Map Projection: UTM Zone 16 N Map Datum: NAD83

This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.

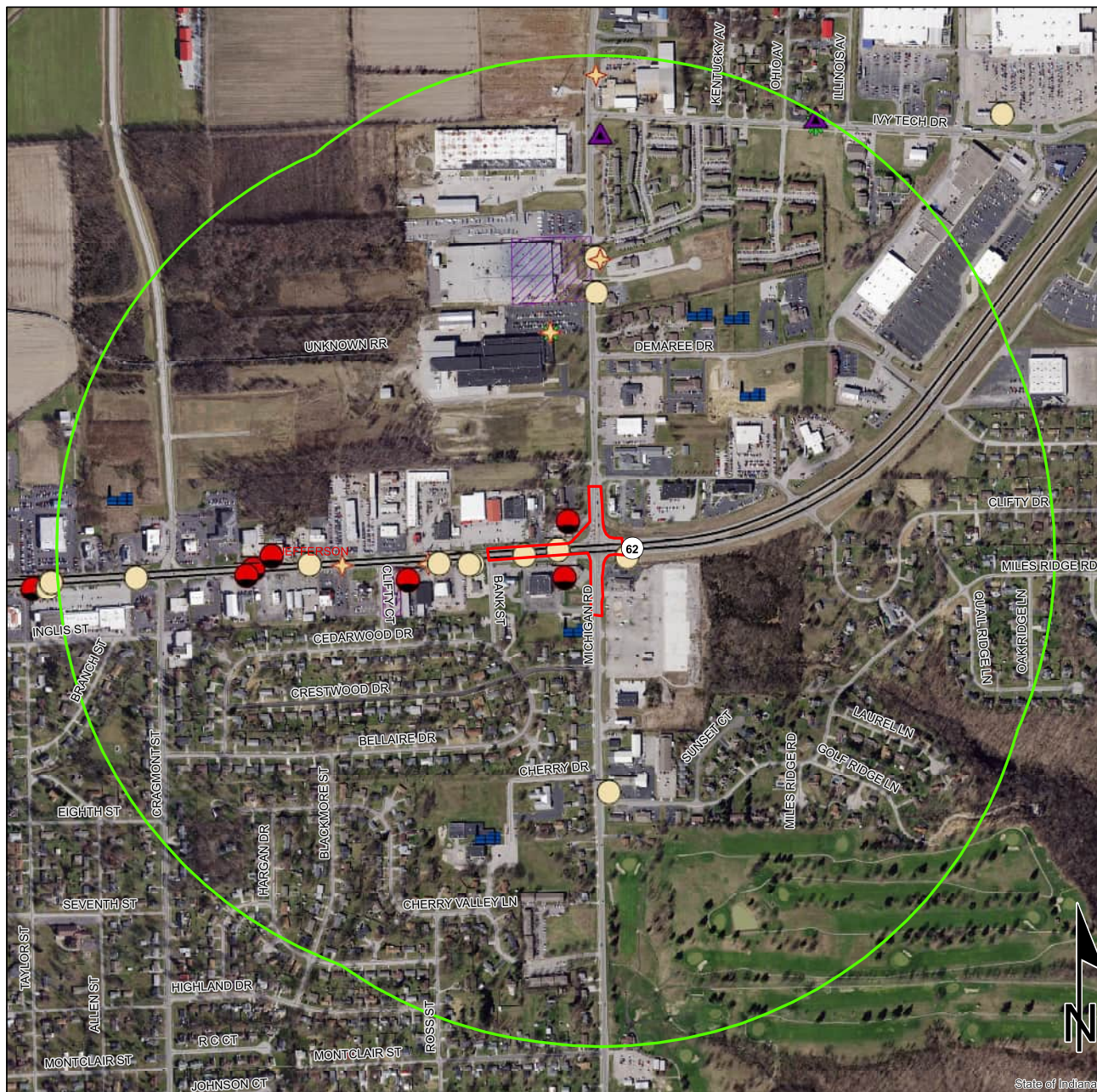


NWI - Point	Wetlands	Project Area
Karst Spring	Lake	Half Mile Radius
NWI - Line	Floodplain - DFIRM	Toll
Impaired_Stream_Lake	Cave Entrance Density	Interstate
NPS NRI listed	Sinkhole Area	State Route
River	Sinking-Stream Basin	US Route
Canal Structure - Historic	County Boundary	Local Road
Canal Route - Historic		

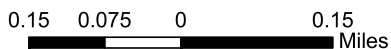


# Hazardous Material Concerns SR 62 and Michigan Rd. Des. No. 190223, Access Management Jefferson County, Indiana

**Note: Maps provided for illustrative purposes only. No concurrence by INDOT ESD SAM required.**



	Brownfield		RCRA Generator/TSD		Institutional Controls
	RCRA Corrective Action Sites		Restricted Waste Site		County Boundary
	Confined Feeding Operation		Septage Waste Site		Project Area
	Notice_of_Contamination		Solid Waste Landfill		Half Mile Radius
	Construction/Demolition Site		State Cleanup Site		Toll
	Infectious/Medical Waste Site		Superfund		Interstate
	Leaking Underground Storage Tank		Tire Waste Site		State Route
	Manufactured Gas Plant		Underground Storage Tank		US Route
	NPDES Facilities		Voluntary Remediation Program		Local Road
	NPDES Pipe Locations		Waste Transfer Station		
	Open Dump Waste Site				



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**Sources:**  
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 Data - Obtained from the State of Indiana Geographical Information Office Library  
**Orthophotography** - Obtained from Indiana Map Framework Data ([www.indianamap.org](http://www.indianamap.org))  
**Map Projection:** UTM Zone 16 N **Map Datum:** NAD83

Indiana County Endangered, Threatened and Rare Species List

County: Jefferson



Species Name	Common Name	FED	STATE	GRANK	SRANK
<b>Platyhelminthes (Flatworms)</b>					
<i>Sphalloplana weingartneri</i>	Weingartner's Cave Flatworm		WL	G4	S3
<b>Crustacean: Malacostraca</b>					
<i>Caecidotea rotunda</i>	Northeastern Cave Isopod		SR	G2G4	S3
<i>Crangonyx lewisi</i>	Lewis Cave Amphipod		SR	G2	S2
<i>Crangonyx packardi</i>	Packard's Cave Amphipod		WL	G4	S3
<b>Crustacean: Copepoda</b>					
<i>Diacyclops indianensis</i>	Indiana Groundwater Copepod		SE	G2	S1
<i>Diacyclops lewisi</i>	Lewis' Groundwater Copepod		SE	G1	S1
<b>Mollusk: Bivalvia (Mussels)</b>					
<i>Epioblasma triquetra</i>	Snuffbox	LE	SE	G3	S1
<i>Lampsilis ovata</i>	Pocketbook		SSC	G5	S2
<i>Ligumia recta</i>	Black Sandshell		SSC	G4G5	S2
<i>Obovaria subrotunda</i>	Round Hickorynut	C	SE	G4	S1
<i>Plethobasus cyphus</i>	Sheepnose	LE	SE	G3	S1
<i>Pleurobema cordatum</i>	Ohio Pigtoe		SSC	G4	S2
<i>Ptychobranthus fasciolaris</i>	Kidneyshell		SSC	G4G5	S2
<i>Simpsonaias ambigua</i>	Salamander Mussel	C	SSC	G3	S2
<i>Toxolasma lividus</i>	Purple Lilliput	C	SSC	G3Q	S2
<i>Villosa lienosa</i>	Little Spectaclecase		SSC	G5	S3
<b>Ellipluran: Collembola</b>					
<i>Pseudosinella fonsa</i>	Fountain Cave Springtail		ST	G3G4	S2
<i>Sminthurides hypogramme</i>	springtail		WL	GNR	S1
<b>Insect: Coleoptera (Beetles)</b>					
<i>Atheta troglaphila</i>			SR	G4	S2
<i>Pseudanophthalmus chthonius</i>	Cave Ground Beetle		SR	G3	S3
<b>Insect: Odonata (Dragonflies &amp; Damselflies)</b>					
<i>Archilestes grandis</i>	Great Spreadwing		WL	G5	S3
<b>Arachnida</b>					
<i>Calymmaria cavicola</i>	Cave Funnel-web Spider			GNR	S1
<b>Amphibian</b>					
<i>Acris blanchardi</i>	Blanchard's Cricket Frog		SSC	G5	S4
<i>Ambystoma barbouri</i>	Streamside Salamander	C	SSC	G4	S3
<i>Cryptobranchus alleganiensis alleganiensis</i>	Eastern Hellbender	C	SE	G3T2	S1
<i>Hemidactylium scutatum</i>	Four-toed Salamander		SSC	G5	S2
<i>Lithobates areolatus circulosus</i>	Northern Crawfish Frog		SE	G4T4	S2
<b>Reptile</b>					
<i>Clonophis kirtlandii</i>	Kirtland's Snake		SE	G2	S2
<i>Opheodrys aestivus</i>	Rough Green Snake		SSC	G5	S3

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Indiana County Endangered, Threatened and Rare Species List

County: Jefferson



Species Name	Common Name	FED	STATE	GRANK	SRANK
<i>Terrapene carolina carolina</i>	Eastern Box Turtle		SSC	G5T5	S3
<b>Bird</b>					
<i>Aimophila aestivalis</i>	Bachman's Sparrow			G3	SXB
<i>Ammodramus henslowii</i>	Henslow's Sparrow		SE	G4	S3B
<i>Buteo platypterus</i>	Broad-winged Hawk		SSC	G5	S3B
<i>Cistothorus platensis</i>	Sedge Wren		SE	G5	S3B
<i>Falco peregrinus</i>	Peregrine Falcon		SSC	G4	S2B
<i>Haliaeetus leucocephalus</i>	Bald Eagle		SSC	G5	S2
<i>Lanius ludovicianus</i>	Loggerhead Shrike		SE	G4	S3B
<i>Setophaga cerulea</i>	Cerulean Warbler		SE	G4	S3B
<i>Setophaga citrina</i>	Hooded Warbler		SSC	G5	S3B
<i>Tyto alba</i>	Barn Owl		SE	G5	S2
<b>Mammal</b>					
<i>Myotis lucifugus</i>	Little Brown Bat	C	SE	G3	S2
<i>Myotis septentrionalis</i>	Northern Long Eared Bat	LT	SE	G1G2	S2S3
<i>Myotis sodalis</i>	Indiana Bat	LE	SE	G2	S1
<i>Perimyotis subflavus</i>	Tricolored Bat		SE	G2G3	S2S3
<i>Taxidea taxus</i>	American Badger		SSC	G5	S2
<b>Vascular Plant</b>					
<i>Asplenium ruta-muraria</i>	wallrue spleenwort		ST	G5	S3
<i>Baptisia australis</i>	wild false indigo		ST	G5	S3
<i>Cardamine dissecta</i>	divided toothwort		SE	G4?	S1
<i>Carex eburnea</i>	ebony sedge		ST	G5	S3
<i>Carex pedunculata</i>	longstalk sedge		WL	G5	S3
<i>Carex seorsa</i>	weak stellate sedge		ST	G5	S3
<i>Carex straminea</i>	straw sedge		ST	G5	S2
<i>Chaerophyllum shortii</i>	wild chervil		ST	G5T3T4Q	S2
<i>Chimaphila maculata</i>	spotted wintergreen		WL	G5	S3
<i>Clinopodium vulgare</i>	American wild basil		WL	G5	S3
<i>Cornus amomum ssp. amomum</i>	silky dogwood		SE	G5	S1
<i>Cyperus pseudovegetus</i>	green flatsedge		ST	G5	S3
<i>Dendrolycopodium obscurum</i>	tree clubmoss		ST	G5	S3
<i>Dichanthelium scoparium</i>	broom panic-grass		SE	G5	S1
<i>Eleocharis wolfii</i>	Wolf's spikerush		ST	G3G5	S2
<i>Helianthus angustifolius</i>	swamp sunflower		SE	G5	S1
<i>Hydrocotyle americana</i>	American water-pennywort		SE	G5	S1
<i>Hypericum frondosum</i>	golden St. John's-wort		SX	G4	SX
<i>Hypopitys monotropa</i>	American pinesap		WL	G5	S3
<i>Isotria verticillata</i>	large whorled pogonia		WL	G5	S3
<i>Juglans cinerea</i>	butternut		ST	G3	S2

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Indiana County Endangered, Threatened and Rare Species List

County: Jefferson



Species Name	Common Name	FED	STATE	GRANK	SRANK
<i>Juniperus communis var. depressa</i>	ground juniper		ST	G5T5	S3
<i>Lilium canadense</i>	Canada lily		ST	G5	S3
<i>Linum striatum</i>	ridged yellow flax		WL	G5	S3
<i>Ludwigia decurrens</i>	primrose willow		WL	G5	S3
<i>Lygodium palmatum</i>	climbing fern		SE	G4	S1
<i>Matelea obliqua</i>	angle pod		ST	G4?	S3
<i>Oenothera perennis</i>	small sundrops		ST	G5	S3
<i>Oenothera triloba</i>	stemless evening-primrose		SX	G4	SX
<i>Orobancha riparia</i>	bottomland broomrape		SE	G4?	S1
<i>Panax quinquefolius</i>	American ginseng		WL	G3G4	S3
<i>Panax trifolius</i>	dwarf ginseng		WL	G5	S3
<i>Patis racemosa</i>	black-fruit mountain-ricegrass		ST	G5	S3
<i>Phlox amplifolia</i>	large-leaved phlox		ST	G3G5	S3
<i>Platanthera peramoena</i>	purple fringeless orchid		WL	G5	S3
<i>Poa alsodes</i>	grove meadow grass		ST	G4G5	S3
<i>Ranunculus pusillus</i>	Pursh's buttercup		SE	G5	S1
<i>Rhexia mariana var. mariana</i>	Maryland meadow beauty		ST	G5T5	S1
<i>Sagittaria australis</i>	longbeak arrowhead		ST	G5	S3
<i>Schoenoplectiella purshiana</i>	weakstalk bulrush		ST	G4G5	S3
<i>Sida hermaphrodita</i>	Virginia mallow		SE	G3	S1
<i>Strophostyles leiosperma</i>	slick-seed wild-bean		WL	G5	S3
<i>Sullivantia sullivantii</i>	Sullivantia		ST	G4	S2
<i>Symphotrichum oblongifolium</i>	aromatic aster		ST	G5	S3
<i>Thalictrum pubescens</i>	tall meadowrue		ST	G5	S3
<i>Tragia cordata</i>	heart-leaved noseburn		WL	G4	S3
<i>Triadenum walteri</i>	Walter's St. John's-wort		WL	G5	S3
<i>Tsuga canadensis</i>	eastern hemlock		WL	G5	S3
<i>Valerianella chenopodiifolia</i>	goose-foot corn-salad		WL	G4	S3
<i>Viburnum molle</i>	softleaf arrow-wood		ST	G5	S3
<i>Wisteria frutescens</i>	American wisteria		ST	G5	S3
<i>Woodwardia areolata</i>	netted chainfern		ST	G5	S3
<b>High Quality Natural Community</b>					
<i>Forest - flatwoods bluegrass till plain</i>	Bluegrass Till Plain Flatwoods		SG	G3	S2
<i>Forest - upland dry Bluegrass</i>	Bluegrass Dry Upland Forest		SG	GNR	S1
<i>Forest - upland dry-mesic Bluegrass</i>	Bluegrass Dry-mesic Upland Forest		SG	GNR	S1
<i>Forest - upland mesic Bluegrass</i>	Bluegrass Mesic Upland Forest		SG	GNR	S3
<i>Primary - cliff limestone</i>	Limestone Cliff		SG	GU	S1
<b>Other Significant Feature</b>					
<i>Freshwater Mussel Concentration Area</i>	Mussel Bed		SG	G3	SNR

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County: Jefferson

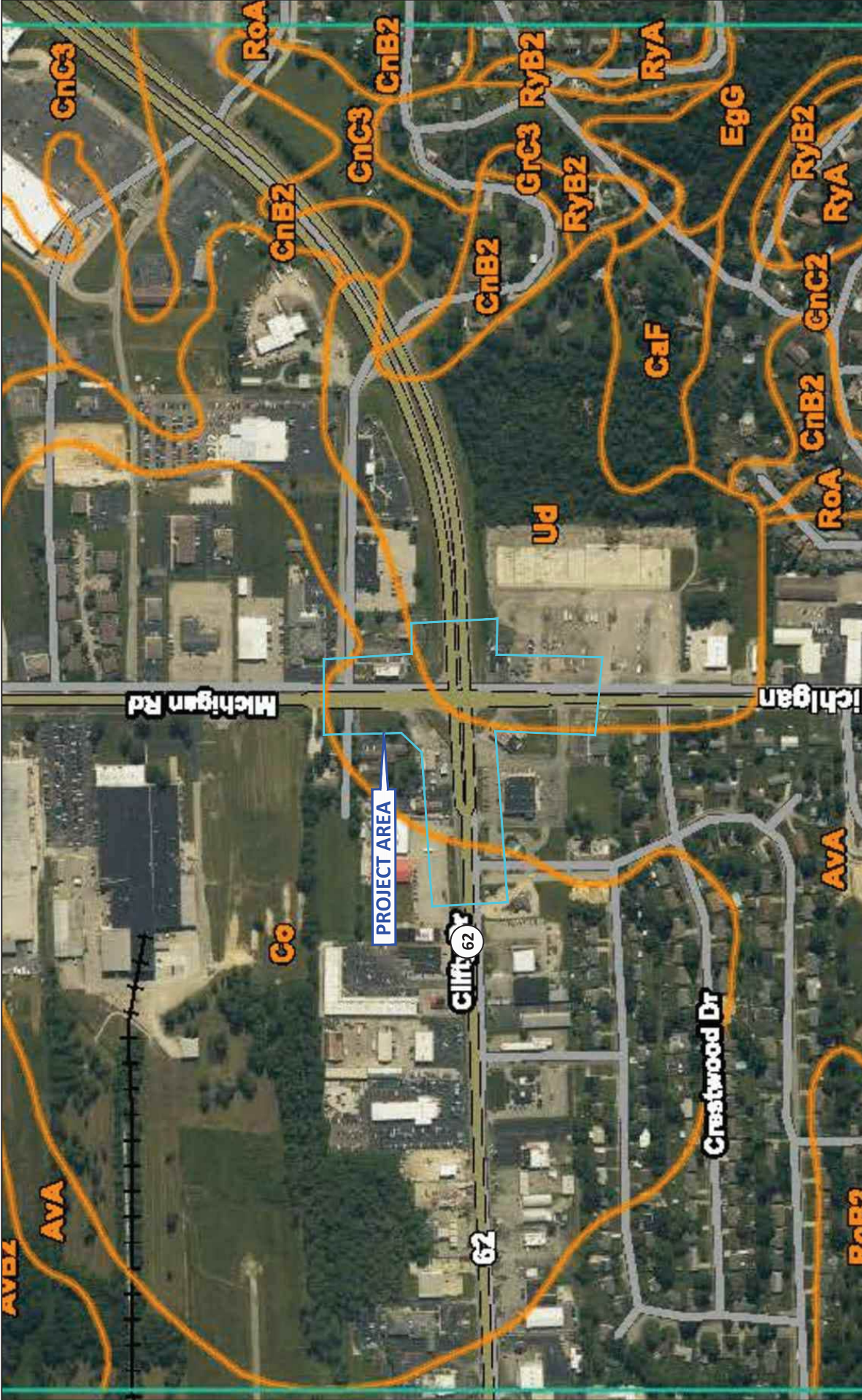


Species Name	Common Name	FED	STATE	GRANK	SRANK
<i>Geomorphic - Nonglacial Erosional Feature - Water Fall and Cascade</i>	Water Fall and Cascade			GNR	SNR

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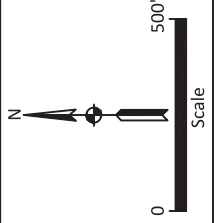
# **APPENDIX F: Water Resources**



Source: <https://websoilsurvey.sc.egov.usda.gov/App/WebSoilsSurvey.aspx>

**NRCS Soils Map**  
 Access Management Project  
 Site SR 62 and Michigan Road  
 Madison, Jefferson County,  
 Indiana  
 Des. No. 1902023  
 Metric Project #18-0070 Task 13

Note: All locations are approximate



Drawn by: JJJ
Checked by: SC
Approved by: JRP
Date: June, 2021

## MAP LEGEND

<b>Area of Interest (AOI)</b>		<b>Soil Area</b>	
Area of Interest (AOI)		Stony Spot	
<b>Soils</b>		Very Stony Spot	
Soil Map Unit Polygons		Wet Spot	
Soil Map Unit Lines		Other	
Soil Map Unit Points		Special Line Features	
<b>Special Point Features</b>		<b>Water Features</b>	
Blowout		Streams and Canals	
Borrow Pit		<b>Transportation</b>	
Borrow Pit		Rails	
Clay Spot		Interstate Highways	
Clay Spot		US Routes	
Closed Depression		Major Roads	
Closed Depression		Local Roads	
Gravel Pit		<b>Background</b>	
Gravel Pit		Aerial Photography	
Gravelly Spot			
Gravelly Spot			
Landfill			
Landfill			
Lava Flow			
Lava Flow			
Marsh or swamp			
Marsh or swamp			
Mine or Quarry			
Mine or Quarry			
Miscellaneous Water			
Miscellaneous Water			
Perennial Water			
Perennial Water			
Rock Outcrop			
Rock Outcrop			
Saline Spot			
Saline Spot			
Sandy Spot			
Sandy Spot			
Severely Eroded Spot			
Severely Eroded Spot			
Sinkhole			
Sinkhole			
Slide or Slip			
Slide or Slip			
Sodic Spot			
Sodic Spot			

## MAP INFORMATION

The soil surveys that comprise your AOI were mapped at 1:15,800.

Please rely on the bar scale on each map sheet for map measurements.

Source of Map: Natural Resources Conservation Service Web Soil Survey URL: \_\_\_\_\_

Coordinate System: Web Mercator (EPSG:3857)

Maps from the Web Soil Survey are based on the Web Mercator projection, which preserves direction and shape but distorts distance and area. A projection that preserves area, such as the Albers equal-area conic projection, should be used if more accurate calculations of distance or area are required.

This product is generated from the USDA-NRCS certified data as of the version date(s) listed below.

Soil Survey Area: Jefferson County, Indiana

Survey Area Data: Version 22, Jun 4, 2020

Soil map units are labeled (as space allows) for map scales 1:50,000 or larger.

Date(s) aerial images were photographed: Apr 17, 2019—Jun 28, 2019

The orthophoto or other base map on which the soil lines were compiled and digitized probably differs from the background imagery displayed on these maps. As a result, some minor shifting of map unit boundaries may be evident.

**NRCS Soils Map Legend**  
 Access Management Project  
 SR 62 and Michigan Road  
 Madison, Jefferson County,  
 Indiana  
 Des. No. 1902023  
 Metric Project #18-0070 Task 13

Source: <https://websoilssurvey.sc.egov.usda.gov/App/WebSoilSurvey.aspx>



Drawn by: ILJ \_\_\_\_\_  
 Checked by: SC \_\_\_\_\_  
 Approved by: JRP \_\_\_\_\_  
 Date: June, 2021





Source: <https://www.fws.gov/wetlands/Data/Mapper.html>

**USFWS NWI Map**  
 Access Management Project  
 SR 62 and Michigan Road  
 Madison, Jefferson County, Indiana  
 Des. No. 1902023  
 Metric Project #18-0070 Task 13

Note: All locations are approximate

Wetlands	
	Freshwater Emergent Wetland
	Freshwater Forest/Shrub Wetland
	Estuarine and Marine Wetland
	Estuarine and Marine Wetland
	Other
	Other
	Freshwater Pond
	River/Stream
	Water

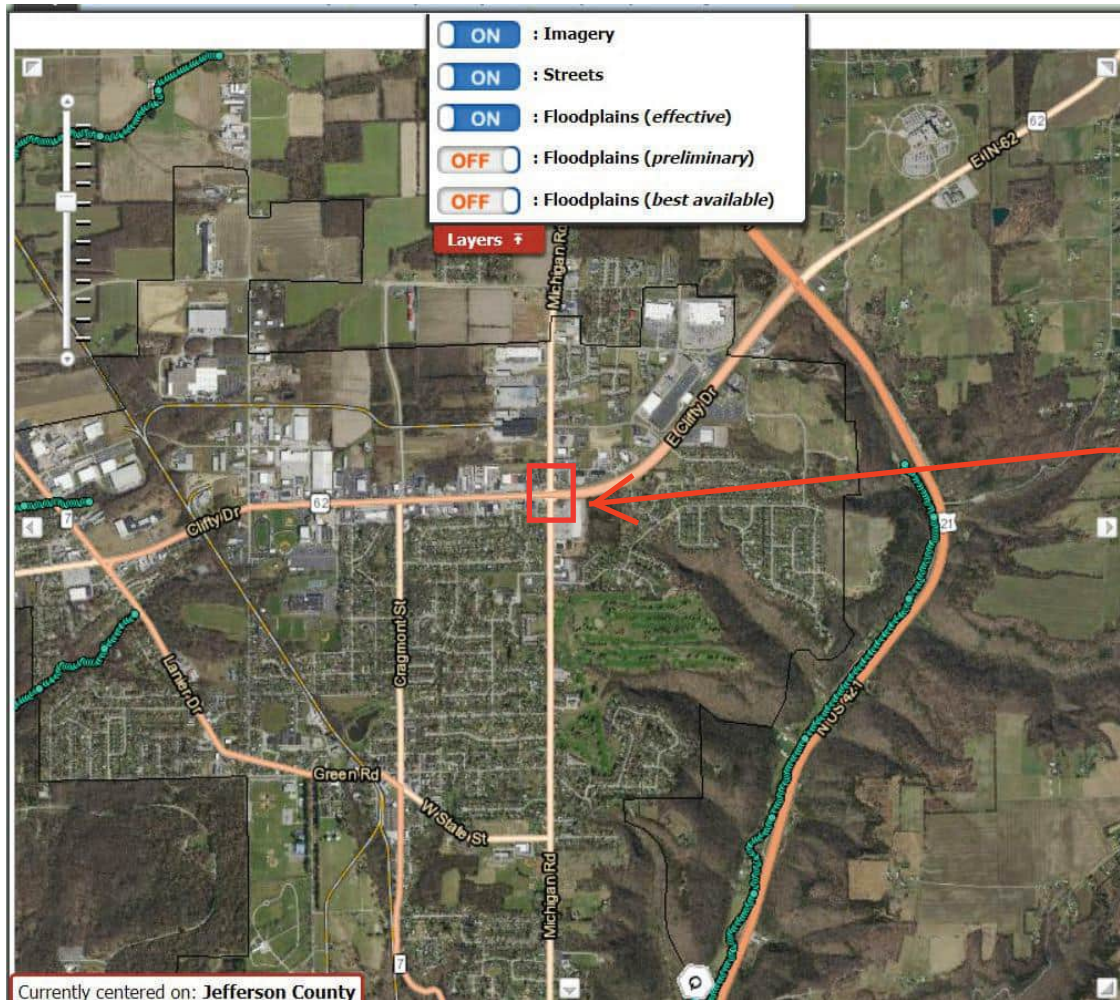


**METRIC ENVIRONMENTAL**








Drawn by: ILJ  
 Checked by: SC  
 Approved by: JRP  
 Date: June, 2021

## IDNR Floodplain Map

Retrieved from the IDNR Indiana Floodplain Portal (<https://dnrmaps.dnr.in.gov/appsphp/fdms/>) on June 30, 2021



Approximate Project Area

-  0.2% Annual Chance Flood Hazard
-  1% Annual Chance Flood Hazard - Zone A (Approximate Study)
-  1% Annual Chance Flood Hazard - Zone AE (Detailed Study)
-  1% Annual Chance Flood Hazard - Floodway
-  1% Annual Chance Flood Hazard - Zone AH
-  1% Annual Chance Flood Hazard - Zone AO
-  Zone X - Protected by Levee

# **APPENDIX G: Air Quality**

Indiana Department of Transportation (INDOT)  
State Preservation and Local Initiated Projects FY 2022 - 2026

SPONSOR	CONTR ACT # / LEAD DES	STIP NAME	ROUTE	WORK TYPE	LOCATION	DISTRICT	MILES	FEDERAL CATEGORY	Total Cost of Project*	PROGRAM	PHASE	FEDERAL	MATCH	2022	2023	2024	2025	2026	
Indiana Department of Transportation	42489 / 1902038	Init.	VA VARI	Traffic Signals Modernization	Uninterruptible Power Supplies (UPS) at Various Locations within the Fort Wayne District.	Fort Wayne	0	ISTBG	\$273,474.00	Safety Construction	CN	\$200,000.00	\$50,000.00	\$250,000.00					
Performance Measure Impacted: Safety																			
Comments:Include DES 1902038																			
Indiana Department of Transportation	42490 / 1902040	Init.	VA VARI	Traffic Signals Modernization	Various Locations within the Fort Wayne District.	Fort Wayne	0	ISTBG	\$1,519,140.00	Safety Construction	CN	\$1,380,000.00	\$345,000.00	\$1,725,000.00					
Performance Measure Impacted: Safety																			
Comments:Include DES 1902040																			
Indiana Department of Transportation	42512 / 1901967	Init.	VA VARI	Guardrail/Alternators, New Or Modernize	Guardrail/End Treatments in Dearborn County on US 50	Seymour	0	ISTBG	\$471,742.00	Safety Construction	CN	\$377,393.60	\$94,348.40	\$471,742.00					
Performance Measure Impacted: Safety																			
Comments:Include DES 1901967																			
Indiana Department of Transportation	42513 / 1901968	Init.	VA VARI	Install New Cable Rail Barriers	Install new cable rail barriers on I-74 and SR 285.	Seymour	0	ISTBG	\$3,624,434.50	Safety Construction	CN	\$80,000.00	\$20,000.00	\$100,000.00					
Performance Measure Impacted: Safety																			
Comments:Include DES 1901968																			
Indiana Department of Transportation	42516 / 1901971	Init.	VA VARI	Lighting	I-65 and SR 252 in Shelby County and I-65 and SR 250 in Jackson County.	Seymour	0	ISTBG	\$440,000.00	Safety Construction	CN	\$600,000.00	\$150,000.00	\$750,000.00					
Performance Measure Impacted: Safety																			
Comments:Include DES 1901971																			
Indiana Department of Transportation	42518 / 1902018	Init.	VA VARI	Raised Pavement Markings, Refurbished	Various Interstate Locations in the Seymour District.	Seymour	0	ISTBG	\$450,000.00	Safety Construction	CN	\$360,000.00	\$90,000.00	\$450,000.00					
Performance Measure Impacted: Safety																			
Indiana Department of Transportation	42558 / 1902023	Init.	VA VARI	Other Type Project (Miscellaneous)	Access management (raised medians/islands) in Jefferson Co.	Seymour	0	ISTBG	\$461,846.00	Safety Construction	CN	\$225,624.80	\$56,406.20	\$282,031.00					
Performance Measure Impacted: Safety																			
Comments:Include DES 1902023																			
Indiana Department of Transportation	42605 / 1902025	Init.	VA VARI	Traffic Signals Modernization	Various Intersections in Jefferson County	Seymour	0	ISTBG	\$1,000,000.00	Safety Construction	CN	\$600,000.00	\$200,000.00	\$1,000,000.00					
Performance Measure Impacted: Safety																			
Comments:Include DES 1902025																			
Indiana Department of Transportation	42607 / 1701466	Init.	VA VARI	Raised Pavement Markings, Refurbished	Various Locations within the Vincennes District - FY 2022 RPMs	Vincennes	0	ISTBG	\$225,000.00	Safety Construction	CN	\$180,000.00	\$45,000.00	\$225,000.00					
Performance Measure Impacted: Safety																			
Comments:Include DES 1701466																			
Indiana Department of Transportation	42607 / 1902185	Init.	VA VARI	Raised Pavement Markings, Refurbished	Various RPM Locations within the Vincennes District	Vincennes	0	ISTBG	\$225,000.00	Safety Construction	CN	\$180,000.00	\$45,000.00	\$225,000.00					
Performance Measure Impacted: Safety																			
Comments:Include DES 1902185																			

\*Estimated Costs left to Complete Project column is for costs that may extend beyond the four years of a STIP. This column is not fiscally constrained and is for information purposes.

# **APPENDIX H: Additional Studies**

## ENGINEERING ASSESSMENT

### Access Management

SR 229 - RP 12+57 to RP 12+71, Franklin County

SR 62 - RP 194+75, Jefferson County

Des. No. 1902023

## I. PURPOSE OF REPORT

The purpose of the report is to document the engineering assessment phase of project development, including the coordination that has been completed in preparation for the median construction. This document outlines the proposal and is intended to serve as a guide for subsequent survey, design, environmental, right-of-way (R/W), and other project tasks leading to construction. The preferred alternative identified in this document is considered pre-decisional, pending the outcome of environmental studies.

## II. PROJECT LOCATION

There are two locations proposed to be improved by this project. The first location is State Road (SR) 229 from Grayson Street to Northside Drive, approximately 0.14 miles northeast of Grayson Street in Batesville, Indiana. The second proposed improvement location occurs at the intersection of SR 62 and Michigan Road in Madison, Indiana. Both locations are within the Indiana Department of Transportation's (INDOT) Seymour District. The project location maps are provided in Appendix A.

## III. PROJECT PURPOSE AND NEED

The need for this project is based on the crash frequency and severity at these intersections. In Batesville, there are commercial driveways through SR 229. Vehicles queue past Grayson Street, creating safety issues. In Madison, the intersection has experienced a high rate of crashes due to multiple issues on SR 62 such as an excessive turning radius, vehicles ignoring or evading the existing median curb, and impeded signal visibility. The purpose of this project will be to reduce the crash frequency by reducing access conflicts at the intersections.

## IV. EXISTING CONDITIONS

SR 229: Between the intersections of Grayson Street and Northside Drive, SR 229 features full-access commercial driveways on both sides of the road. There are two through lanes northbound and southbound as well as exclusive left turn lanes northbound and southbound at the Grayson Street intersection. There is a northbound right turn only lane that ends at a commercial entrance approximately 570 feet north of Grayson Street. At the intersection with Northside Drive, SR 229 has a left turn lane and one through lane in each direction. There are existing traffic signals at Northside Drive. Between these two intersections, there is a continuous two-way left turn lane (TWLTL) that allows access to the various drives. Vehicles waiting to make left turns cause traffic to queue past Grayson Street, which creates a safety concern.

This section of SR 229 has a functional classification of Minor Arterial with a posted speed limit of 40-miles per hour (mph). The typical cross section includes two lanes in each direction, 12 feet wide and a TWLTL that is 16 feet wide. The roadway is bounded by curbs and gutter and drains via surface inlets and enclosed storm sewers. The adjacent land use is primarily commercial.

SR 62: Both directions of SR 62 feature a dedicated left turn lane, two through lanes, and one dedicated right turn lane. Both approaches of Michigan Road feature one left turn lane, a through lane, and a right turn lane. The excessive turning radius on the northwest corner has created the opportunity for turning vehicles to be adjacent to one another due to the edge line being approximately 13' from the edge of the pavement. Driveway crashes have occurred on the south and west legs due to the Walgreens and gas station driveways. During another consultant's field visit motorists were observed turning left from the gas station driveway even though a raised concrete median is present. A broken signpost is present on the west end of the median on the west leg, and signal visibility is impeded by bright sun conditions. There is a horizontal curve along SR 62 to the east of Michigan Road as well.

There is currently a Hot Mix Asphalt (HMA) Overlay project being designed for SR 62 that will bid at approximately the same time. At the scoping meeting, it was discussed that the mill and overlay of SR 62 may be included with the HMA Overlay project. The overlay project number is Des. No. 1801106.

This section of SR 62 has a functional classification of Principal Arterial - Other with a 35-mph posted speed limit. The typical cross section of the roadway includes two lanes in each direction, 12 feet wide. The west half of SR 62 is curbed with storm sewers and the eastern half has shoulders with open ditches. The adjacent land use is commercial.

This section of Michigan Road has a functional classification of Minor Arterial with a 40-mph posted speed limit. The typical cross section of the roadway includes one lane in each direction, 12 feet wide. The northbound lane is bound by shoulders with open ditches and the southbound lane is primarily curbed with storm sewers. The adjacent land use is commercial.

Proposed improvements are discussed in Section IX.

## **V. FIELD INVESTIGATION**

A Virtual Field Check for both locations was held at the INDOT Seymour District office on March 6, 2020. The minutes of that meeting are included as Appendix B.

## **VI. TRAFFIC DATA ANALYSIS**

The Indiana Department of Transportation (INDOT) Traffic Count Database System was utilized to determine the traffic counts. The count station utilized for SR 229 is located just west of Grayson Street (Location ID 240420). The count station utilized for SR 62 is located approximately 580 feet west of Michigan Road (Location ID 390440). Current and projected traffic for the roadways are shown in Table 1 and Table 2 below.

The annual growth rate used for SR 229 was 0.60 percent. The annual growth rate used for SR 62 was 0.40 percent.

<b>SR 229</b>	
Annual Average Daily Traffic (AADT) (2019)	18,211 vehicles per day (VPD)
AADT (2042)	20,897 VPD
Design Hourly Volume (DHV)	9.07%
Directional Distribution	50%

**Table 1 - Traffic Data for SR 229**

<b>SR 62</b>	
Annual Average Daily Traffic (AADT) (2019)	20,713 vehicles per day (VPD)
AADT (2042)	22,705 VPD
Design Hourly Volume (DHV)	9.21%
Directional Distribution	neg 51.4%

**Table 2 - Traffic Data for SR 62**

## VII. CRASH DATA AND ANALYSIS

Crash Data for this segment of SR 229 from April 15, 2017 through April 15, 2020 was analyzed for crash type and severity. This segment of SR 229 was analyzed as a multi-lane urban arterial with a length of 0.33 mile and an intersection density of 9 per mile. The Index of Crash Frequency was 1.73 and the Index of Crash Cost was 0.44. The analysis indicates that the segment features a higher-than-expected crash rate. Table 3 summarizes the crash history over the latest 3-year period. The Crash Data can be found in Appendix D.

	Crash Severity			Crash Type				
	Fatal/Incap.	Injury	PDO	Right Angle	Ran Off Road	Rear End	Sideswipe	R/L Turn
2017 - 2020	0	5	28	7	3	10	6	7
% Total	0%	15%	85%	21.2%	9.1%	30.3%	18.2%	21.2%

**Table 3 - Crash Analysis for SR 229**



Crash Data from January 15, 2017 through January 15, 2020 for the intersection of SR 62 with Michigan Road was analyzed for crash type and severity. The intersection was analyzed as a signalized intersection of a state highway with a non-state roadway. The Index of Crash Frequency was 2.05 and the Index of Crash Cost was 2.15. The analysis indicates that the intersection experiences a higher-than-expected crash rate and crash cost. Table 4 summarizes the crash history over the latest 3-year period. The Crash Data can be found in Appendix D.

	Crash Severity			Crash Type				
	Fatal/Incap.	Injury	PDO	Right Angle	Ran Off Road	Rear End	Sideswipe	R/L Turn
2017 - 2020	4	9	66	8	4	27	15	25
% Total	0% **	16% ***	84%	10.1%	5.1%	34.2%	19.0%	31.6%

**Table 4 - Crash Analysis for SR 62**

**\*\*Note: Calculation error. Should be 5%.**

**\*\*\* Note: Calculation error. Should be 11%**

## VIII. DESIGN CRITERIA

Project Design Criteria	3R Reconstruction (IDM Fig. 53-6)
Functional Classification	Urban Arterial
Design Speed	40 mph
Posted Speed	40 mph
Access Control	None
Number of Lanes and Width	4 lanes @ 12 ft (match existing)
Shoulder Width	N/A (curb and gutter)
Obstruction-Free Zone	10 ft
Horizontal Alignment	Maintain Existing Alignment
Vertical Alignment	Maintain Existing Alignment

**Table 5 - Design Criteria for SR 229**

Project Design Criteria	3R Reconstruction (IDM. Fig. 53-6)
Functional Classification	Urban Arterial
Design Speed	40 mph
Posted Speed	35 mph
Access Control	Partial Limited Access
Number of Lanes and Width	4 lanes @ 12 ft (match existing)
Shoulder Width	N/A (Curb and Gutter)
Obstruction-Free Zone	12 ft
Horizontal Alignment	Maintain Existing Alignment
Vertical Alignment	Maintain Existing Alignment

**Table 6 - Design Criteria for SR 62**

## IX. PROPOSED IMPROVEMENTS

In order to reduce the crash rates of these intersections, reducing the access points via concrete median construction is recommended. Conceptual sketches of the proposed improvements are in Appendix E.

### A. SR 229

A two-foot wide median curb will be installed from the intersection of SR 229 and Grayson St. to Northside Drive. Both are signalized intersections, and the proposed center curb median will force all the left turns to occur at the signals. The signal heads at Northside Drive will be adjusted to align with the new northbound lane locations and the left only signs will be reset at the entrance. The TWLTL will be converted to left turn only lanes in each direction. The northbound lane striping will have to be shifted on SR 229 in order to maintain the current lane width after the median has been installed.

For the installation of the new median, the existing pavement will be cut 1 foot on each side for construction. This area will be patched with full-depth asphalt. One lane width will be milled and resurfaced on each side of the new median. Solid white striping will be added where the outside northbound lane becomes an exclusive right turn lane and then is dropped at the first driveway south of Northside Drive.

### B. SR 62

Since the existing medians at the intersection of SR 62 with Michigan Road are not long enough to deter left turns, the existing north and south approach medians will be extended. The existing median north of SR 62 will be extended approximately 100 feet north until the first drive on the west. The existing median south of SR 62 will be extended approximately 200 feet south until the first drive on the west. On SR 62, the median west of Michigan Road will be extended through the taper to the west, which is approximately 130 feet longer than the existing median. The west entrance to Gran Brazil and Walgreens will remain open. In addition to the extension of the center medians, the existing medians will be replaced.

For the installation of the new medians, the existing pavement will be cut 1 foot on each side for construction. This area will be patched with full-depth asphalt. There is currently a resurface project being designed for SR 62 (Des. No. 1801106), but for the sake of this assessment the milling and resurfacing work was included in the numbers in Table 8. Since Michigan Road is a city street, coordination with City of Madison officials will be necessary.

The radius of the northwestern corner will be reduced, as well as the driveway width on the west side of Michigan Rd to north of SR 62. Improvements to the current traffic signaling include:

- new LED signal heads with backplates
- 5 section signal head for northbound right turns
- yellow flashing arrows for north and southbound left turns
- replacing the signpost on the west end of the median on the west leg

## X. ESTIMATED COSTS SUMMARY

Summaries of the Opinion of Probable Construction Cost (OPCC) for the preferred alternatives are shown in Table 7 and Table 8. A printout of the probable cost breakdowns are in Appendix F.

Cost Item	Total Price
Estimated Construction Cost Subtotal	\$ 200,084
Contingency (20%)	\$ 40,000
<b>Estimated Construction Cost (2020)</b>	<b>\$ 240,084</b>
Utility Relocation	\$ 20,000
<b>Total Estimated Project Cost (2020)</b>	<b>\$ 260,084</b>

**Table 7 - Probable Construction Cost for SR 229**

Cost Item	Total Price
Estimated Construction Cost Subtotal	\$ 282,548
Contingency (20%)	\$ 56,500
<b>Estimated Construction Cost (2020)</b>	<b>\$ 339,048</b>
Utility Relocation	\$ 20,000
<b>Total Estimated Project Cost (2020)</b>	<b>\$ 359,048</b>

**Table 8 Probable Construction Cost for SR 62**

**XI. MAINTENANCE OF TRAFFIC**

Since the existing roadways feature multiple lanes of through traffic, the construction of these medians will be done under single lane closures. In order to maintain a southbound lane during construction of the SR 229 improvements, the fourth lane southbound will be extended to Northside Drive. This will require the curb to be removed and relocated, as well as paving the new lane and adding the appropriate striping.

The contractor will be responsible for following road closure standards as detailed in the INDOT Standard Drawings and the Indiana Manual on Uniform Traffic Control Devices. Coordination with the INDOT District Traffic will take place during design. The final maintenance of traffic plan will be determined during the design phase in coordination with the District's traffic and construction division.

**XII. R/W REQUIREMENTS**

Right of way acquisition is not anticipated for any of the proposed improvements. Existing right-of-way will be verified and documented as part of the design phase.

**XIII. POTENTIAL ENVIRONMENTAL ISSUES**

Because all proposed improvements are to occur on previously disturbed and paved areas, environmental impacts are anticipated to be minimal. All environmental issues will be addressed in greater detail in the environmental phase.

**XIV. RAILROAD & UTILITY IMPACTS**

There is no existing railroad near the project location. Railroad impacts are not anticipated for this project.

Utility conflicts are expected to be minimal for this project. The INDOT Utility Coordination Procedure will be followed during the design phase.

**XV. CHANGES TO PROPOSAL**

The Seymour District Technical Services and Capital Program Management shall be consulted if deviation from the proposal is determined to be necessary during a later phase of project development. The person initiating the changes shall route a memorandum detailing the changes including justification for the change and the estimated cost difference to the Seymour District Scoping Manager and Project Manager for concurrence.

Prepared by:  July 13, 2020  
Marc Rape, P.E. Date  
Strand Associates, Inc.®

Concur: Robert F. Tally Jr. July 30, 2020  
Robert F. Tally Jr., P.E. Date  
System Asset Manager

Zachary Hicks 8/4/20  
Zachary Hicks Date  
Project Manager

**APPENDIX:**

- A-PROJECT LOCATION MAPS
- B-SCOPING MEETING MINUTES
- C-CRASH ANALYSIS
- D-PROPOSED IMPROVEMENTS
- E-OPINION OF PROBABLE COST

**Note: All Appendices except Crash Analysis removed for space conservation.**

<b>Index of Crash Frequency and Cost - Form F1</b>		Page 1/2
Location	SR 62 at Michigan Road	
GIS		
Post		
Analyst		
Date	4/20/2020	
<b>INPUT</b>		
Road Facility Type	Signalized Urban State-Local Intersection	
Major Road AADT (veh/day)	20713	
T-intersection Indicator (1 if present, 0 otherwise)	0	
Arterial Indicator for Crossing Local Road (1 if present, 0 otherwise)	1	
First Year with Crash Data (yyyy)	2017	
Last Year with Crash Data (yyyy)	2020	
Number of Crashes (crash/period)		
Fatal and Incapacitating Injury Crashes	4	
Non-Incapacitating and Possible Injury Crashes	9	
Property Damage Only Crashes	66	
Route or Road Type	Signalized Urban State-Local Intersection	
Average Crash Costs (\$)		
Fatal and Incapacitating Injury Crashes	219500	
Non-Incapacitating and Possible Injury Crashes	31300	
Property Damage Only Crashes	7100	
Crash Cost Year (yyyy)	2013	
<b>OUTPUT</b>		
Expected Crash Frequency (crash/year)		
Fatal and Incapacitating Injury Crashes	0.115	
Non-Incapacitating and Possible Injury Crashes	1.63	
Property Damage Only Crashes	6.62	
All Crashes	8.36	
Index of Crash Frequency	<b>2.05</b>	
Index of Crash Cost	<b>2.15</b>	

<b>Index of Crash Frequency and Cost - Form F1</b>		Page 2/2
Location	SR 62 at Michigan Road	
GIS		
Post		
Analyst		
Date	4/20/2020	
<b>Comments:</b>		

**Land and Water Conservation Fund (LWCF) County Property List for Indiana (Last Updated July 2020)**

ProjectNumber	SubProjectCode	County	Property
1800161	1800161H	Jefferson	Clifty Falls State Park
1800171	1800171C	Jefferson	Clifty Falls State Park
1800177	1800177A	Jefferson	Clifty Falls State Park
1800183	1800183	Jefferson	Clifty Falls State Park & Clifty Canyon Nature Preserve
1800218	1800218	Jefferson	Clifty Falls State Park & Clifty Canyon Nature Preserve
1800305	1800305D	Jefferson	Clifty Falls State Park
1800312	1800312C	Jefferson	Clifty Falls State Park
1800363	1800363F	Jefferson	Clifty Falls State Park
1800409	1800409	Jefferson	Clifty Falls State Park & Clifty Canyon Nature Preserve
1800413	1800413K	Jefferson	Clifty Falls State Park

\*Park names may have changed. If acquisition of publically owned land or impacts to publically owned land is anticipated, coordination with IDNR, Division of Outdoor Recreation, should occur.

Retrieved from the **INDOT Environmental Policy website** (<https://www.in.gov/indot/engineering/files/IN-LWCF-sites-by-county.xlsx>) on September 30, 2021 by Metric.