

FHWA-Indiana Environmental Document
CATEGORICAL EXCLUSION / ENVIRONMENTAL ASSESSMENT FORM
GENERAL PROJECT INFORMATION

Road No./County:

U.S. Highway 20 (US 20) / Lake County, IN

Designation Number(s):

1900009

Project

Description/Termini:

Bridge project, located along US 20 from 0.76 to 1.18 miles east of State Road (SR) 152

	Categorical Exclusion, Level 2 – Required Signatories: INDOT DE and/or INDOT ESD
X	Categorical Exclusion, Level 3 – Required Signatories: INDOT ESD
	Categorical Exclusion, Level 4 – Required Signatories: INDOT ESD and FHWA
	Environmental Assessment (EA) – Required Signatories: INDOT ESD and FHWA
	Additional Investigation (AI) – The proposed action included a design change from the original approved environmental document. Required Signatories must include the appropriate environmental approval authority

Approval

INDOT DE Signature and Date

INDOT ESD Signature and Date

FHWA Signature and Date

Release for Public Involvement

INDOT DE Initials and Date

ADWP March 1, 2024

INDOT ESD Initials and Date

Certification of Public Involvement

INDOT Consultant Services Signature and Date

INDOT DE/ESD Reviewer Signature and Date:

Name and Organization of CE/EA Preparer:

Carson Hoogewerf, Lochmueller Group, Inc.

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County LakeRoute US 20Des. No. 1900009

Note: Refer to the most current INDOT CE Manual, guidance language, and other ESD resources for further guidance regarding any section of this form.

Part I – Public Involvement

Every Federal action requires some level of public involvement, providing for early and continuous opportunities throughout the project development process. **The level of public involvement should be commensurate with the proposed action.**

Does the project have a historic bridge processed under the Historic Bridges PA*?	Yes <input type="checkbox"/>	No <input checked="" type="checkbox"/>
If No, then: Opportunity for a Public Hearing Required?	<input checked="" type="checkbox"/>	<input type="checkbox"/>

*A public hearing is required for all historic bridges processed under the Historic Bridges Programmatic Agreement between INDOT, FHWA, SHPO, and the ACHP.

Discuss what public involvement activities (legal notices, letters to affected property owners and residents (i.e. notice of entry), meetings, special purpose meetings, newspaper articles, etc.) have occurred for this project.

Notice of Entry letters were mailed to potentially affected property owners near the project area on November 4, 2021, notifying them about the project and that individuals responsible for land surveying and field activities may be seen in the area. A sample copy of the Notice of Entry letter is included in Appendix G, G1.

To meet the public involvement requirements of Section 106, a legal notice of the Federal Highway Administration's (FHWA) finding of No Adverse Effect was published in *The Times of Northwest Indiana* on November 20, 2023, offering the public an opportunity to submit comment pursuant to 36 Code of Federal Regulations (CFR) 800.2(d), 800.3(e), and 800.6(a)(4). The public comment period closed 30 days later on December 20, 2023. The text of the public notice and the affidavit of publication appear in Appendix D, D80 to D82. No comments were received.

The project will meet the minimum requirements described in the current *Indiana Department of Transportation (INDOT) Project Development Public Involvement Procedures Manual* which requires the project sponsor to offer the public an opportunity to submit comments and/or request a public hearing. Therefore, a legal notice will appear in a local publication contingent upon the release of this document for public involvement. This document will be revised after the public involvement requirements are fulfilled.

Public Controversy on Environmental Grounds

Discuss public controversy concerning community and/or natural resource impacts, including what is being done during the project to minimize impacts.

At this time, there is no substantial public controversy concerning impacts to the community or to natural resources.

Part II - General Project Identification, Description, and Design Information

Sponsor of the Project: INDOT LaPorte District INDOT District: LaPorteLocal Name of the Facility: US 20 (Michigan Street)Funding Source (mark all that apply): Federal ☒ State ☒ Local ☒ Other* ☐*If other is selected, please identify the funding source: N/A

PURPOSE AND NEED:

The need should describe the specific transportation problem or deficiency that the project will address. The purpose should describe the goal or objective of the project. The solution to the traffic problem should NOT be discussed in this section.

Need: The need for the project is evidenced by the deteriorated condition of the existing structure, Bridge No. 020-45-02484 C (National Bridge Inventory (NBI) No. 005200). The most recent *INDOT Bridge Inspection Report* dated July 11, 2022 (Appendix I, I2

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to I14), noted that the superstructure had previous reports of minor chipping on the beam edges in spans. The superstructure was given a condition rating of 6, which is considered to be in "satisfactory" condition and indicates minor deterioration. The substructure was given a condition rating of 5 which is considered "fair" condition and indicated minor section loss, cracking of the wingwalls, and spalling, cracking, and rust staining of the abutment caps and bents. Condition ratings range from 0, which indicates a failing structure, to 9, which indicates a new structure with no deficiencies. In addition, the stopping sight distance between the bridge and the US 20 intersection with the Kennedy Avenue (Ave.) access roads is inadequate according to Level 1 design criteria. Finally, the existing bridge geometry does not provide pedestrian access consistent with regional connectivity plans.

Purpose: The purpose of the project is to improve the condition of the structure to a rating of 7 or greater, which represents a structure in "good" condition; to achieve proper stopping sight distance between the bridge and the US 20 intersection with the Kennedy Ave. access roads; and to facilitate pedestrian access to local trails, including the Marquette Greenway Trail.

PROJECT DESCRIPTION (PREFERRED ALTERNATIVE):

County: Lake Municipality: City of Hammond, City of East Chicago

Limits of Proposed Work: US 20, from 0.76 to 1.18 miles east of SR 152.

Total Work Length: 0.42 Mile(s) Total Work Area: 7.2 Acre(s)

Is an Interstate Access Document (IAD)¹ required?

If yes, when did the FHWA provide a Determination of Engineering and Operational Acceptability?

¹If an IAD is required; a copy of the approved CE/EA document must be submitted to the FHWA with a request for final approval of the IAD.

Yes¹

No

	X
Date:	

Describe location of project including township, range, city, county, roads, etc. Existing conditions should include current conditions, current deficiencies, roadway description, surrounding features, etc. Preferred alternative should include the scope of work, anticipated impacts, and how the project will meet the Purpose and Need. Logical termini and independent utility also need discussed.

The INDOT LaPorte District, with funding from the FHWA, proposes to proceed with the following bridge improvement project. In addition, the Northwestern Indiana Regional Planning Commission (NIRPC) has agreed to fund the improvements necessary to accommodate a trail atop the bridge. The proposed project is located within the City of Hammond and the City of East Chicago in Lake County, Indiana.

Location:

Bridge No. 020-45-02484 C is located 0.99 mile east of SR 152. The proposed project is located along US 20 from 0.76 to 1.18 miles east of SR 152, along the southern limits of the City of East Chicago and the northern limits of the City of Hammond. Specifically, the project is located in Section 4, Township 36 North, Range 9 West and in Section 33, Township 37 North, Range 9 West within North Township, Highland USGS 1:24,000 Topographic Quadrangle (Appendix B, B2).

Existing Conditions:

US 20, also known as Michigan Street (St.), is an east-west route that is functionally classified as principal arterial. The typical cross section of US 20 consists of two 12-foot-wide lanes with 3-foot-wide paved shoulders west of the bridge and 10-foot-wide paved shoulders east of the bridge. The posted speed limit along US 20 within the project area is 45 miles per hour (MPH).

Kennedy Ave. is a north-south route functionally classified as a minor arterial. The typical cross section of Kennedy Ave. currently consists of four 12-foot-wide lanes (two in each direction) with curb and gutter. The posted speed limit along Kennedy Ave. within the project area is 35 MPH. A planned project (Des. No. 1901902) that is scheduled for construction prior to Des. 1900009 would change the typical cross section of Kennedy Ave. at the US 20 bridge to include two 12-foot-wide southbound lanes with a 5-foot-wide bike lane, one 12-foot-wide northbound lane with a 5-foot-wide bike lane, and a sidewalk along the east side of Kennedy Ave.

The subject bridge (Structure No. 020-45-02484 C/NBI No. 005200) carries US 20 over Kennedy Ave. Access to US 20 from Kennedy Ave. occurs by way of two access roads located north and south of US 20 and east of Kennedy Ave. Both access roads support two-way traffic between US 20 and Kennedy Ave. Structure No. 020-45-02484 C is a three-span, continuous prestressed concrete beam bridge that features a clear roadway width, out-to-out width, and the bridge length measuring 52.0 feet, 55.3 feet, and 193.17 feet, respectively. Structure No. 020-45-02484 C also crosses three sets of railroad tracks, owned by Indiana Harbor Belt

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Railroad (IHB RR), which run parallel to the west side of Kennedy Ave. A sidewalk runs parallel to the east side of Kennedy Ave. south of the subject bridge.

Adjacent land use is urban and includes industrial and commercial areas. Six wetlands, Wetland A, B, C, D, E, and F, are located within the project area.

A drainage system consisting of a series of open ditches and underground culverts conveys drainage toward Kennedy Ave. One such culvert is an unnamed 87-foot-long and 12-inch-wide metal pipe that drains Wetland B to Wetland C. A planned trail, Marquette Greenway Trail, is anticipated to be located along the north side of the subject bridge and will extend to the east and west of the structure, tying into new trail construction associated with Des. Nos. 1601146 and 1901900. The proposed trail improvements are described in greater detail in the *Preferred Alternative* section below.

Deficiencies with the existing structure include minor chipping on the beam edges of the spans (superstructure) and minor section loss, cracking of the wingwalls, and spalling, cracking, and rust staining of the abutment caps and bents (substructure). In addition, the stopping sight distance between the bridge and the US 20 intersection with the Kennedy Ave. access roads is inadequate, and the bridge geometry does not provide pedestrian access.

Preferred Alternative:

The preferred alternative involves the replacement of the existing bridge, Structure No. 020-45-02484 C. The replacement bridge will be identified as Structure No. 020-45-02821 and will be a continuous composite welded steel plate girder bridge. The bridge will be a two-span structure with mechanically stabilized earth (MSE) wall abutments and a reinforced concrete pier. The bridge will feature a length of approximately 222.0 feet, a clear roadway width of 53.5 (2x 26.75) feet, and an out-to-out width of approximately 70.3 feet (Note: these dimensions have been updated to match current design and differ slightly from the preliminary project plans). The bridge cross section will include two 12.0-foot-wide travel lanes, one in each direction, separated by a 4.0-foot-wide raised center median. The bridge will have 12.75-foot-wide paved outside shoulders and 2-foot-wide paved inside curb offsets. See Appendix B, B13 to B41 for the preliminary project plans.

An 8- to 10-foot-wide multi-use trail will be included on the north side of the bridge, the north side of US 20, and along the north access road. Atop the bridge, the trail will have a steel and concrete (type PF-1) pedestrian barrier on both sides. East and west of the bridge, the north side of the trail will be protected by a pedestrian fence. The south side of the trail will be protected by guardrail or PF-1 rail. Approximately 1,920 linear feet (LF) of trail will be constructed within the limits of this project in total, including atop the 222.0-foot-long bridge. Beyond the bridge, the trail will extend west along the north side of US 20 (1,014 LF) and extend east along the north side of US 20, turn west, and follow the south side of the north access road (684 LF). At the west end of the project area, the trail along US 20 will connect to the future trail portion to be completed under Des. No. 1601146. The east end of the trail will terminate at Kennedy Ave. at the southwest end of the north access road, where a curb ramp to be completed under Des. No. 1901900 will be located. See more information about the trail below.

A raise in the bridge profile grade of approximately 41.8 inches will occur to provide adequate vertical clearance over the railroad and Kennedy Ave. The US 20 intersection with the Kennedy Ave. access roads will be configured for right-in/right-out only movements with a raised center median along US 20. Additionally, the intersection will be shifted to the east to accommodate the increased bridge profile grade, and the US 20 roadway grade east of the bridge will be raised to achieve proper stopping sight distance at the intersection. The access roads will each feature one 12.0-foot lane in each direction, which will widen at the curves and at the intersections with Kennedy Ave. and US 20. The access roads will feature 2.0-foot paved shoulders at the outside of each lane.

Riprap drainage turnouts will be added to the northwest and southwest quadrants of the bridge and to the northeast bridge approach south of the north access road. Approximately 1,705 LF of open drainage ditches will be added or modified. The ditches will be located between the north access road and US 20 (430 LF), between the south access road and US 20 (452 LF), north of US 20 east of the bridge (210 LF), and south of US 20 east of the bridge (613 LF). One culvert (no Federal ID), an 87-foot-long and 12-inch-wide metal pipe that drains Wetland B to Wetland C, is located southeast of Structure No. 020-45-02484 C. This pipe will be removed and replaced with an open ditch. Seven 12-inch manholes and 19 12-inch pipe inlets will be added and will tie into the existing closed drainage system (Note: only six manholes are currently detailed on the preliminary project plans). Street lighting located along the north and south access ramps will be replaced in-kind and/or relocated due to the work on the ramp. No additional lighting will be added. No work will occur to the sidewalk along Kennedy Ave. Existing guardrail totaling 1,896 feet will be removed, and 1,338.75 feet of new or replaced guardrail will be installed.

The 10-foot-wide multi-use trail will connect to the planned Marquette Greenway Trail. The Marquette Greenway Trail Plan is a 3-State (Illinois, Indiana, and Michigan) initiative to provide a 50-mile continuous trail along Lake Michigan's lakefront. The proposed trail under Des. 1900009 will extend to the west where the project limits coincide with the limits of Des. No. 1601146. The trail

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extends to the east to cut back to Kennedy Ave. and will tie into the sidewalk and bike lanes constructed with Des. No. 1901902.

The preferred alternative will meet the purpose and need of the project by improving the condition rating of the structure to a 7 or greater out of 9, by improving the stopping sight distance between the bridge and the US 20 access roads with Kennedy Ave., and by providing a trail to connect the Marquette Greenway Trail within the project area.

The Maintenance of Traffic (MOT) plan will involve complete closure of US 20 with lane closures and intermittent full closures of Kennedy Ave. See the *Maintenance of Traffic (MOT) During Construction* section for more information.

The project will require approximately 1.13 acres of new permanent right-of-way (ROW) and 0.06 acres of temporary ROW. See the *Right-of-Way (ROW)* section for more information.

The project is anticipated to permanently impact approximately 0.4124 acre of wetland. Mitigation for wetland impacts will likely be required. The project is anticipated to impact approximately 3.92 acres of terrestrial habitat, including 1.30 acres of tree clearing. Environmental impacts will be minimized to the extent possible while achieving the purpose and need of the project. Complete avoidance of habitat impacts is not practicable due to the need to widen the roadway.

Logical Termini/Independent Utility:

The project termini extend along US 20 from 0.76 mile to 1.18 miles east of SR 152. The termini of the project provide the logical beginning and end points necessary to complete the improvements to the bridge and roadway. This project alone will improve the condition of the bridge and enable pedestrian access to the bridge and surrounding roadway. Though the trail portion of this project is expected to tie into future trail segments currently programmed and funded under Des. Nos. 1601146 and 1901900, the project alone would produce a usable bridge, roadway, and trail which would not require any future improvements to be functional. Therefore, the project is independent of any other action and able to be constructed without relying on the completion of any other project.

OTHER ALTERNATIVES CONSIDERED:

Provide a header for each alternative. Describe all discarded alternatives, including the No Build Alternative. Explain why each discarded alternative was not selected. Make sure to state how each alternative meets or does not meet the Purpose and Need and why.

Bridge Deck Overlay with Substructure Repair: This alternative would involve repair or replacement of the bridge substructure while keeping the superstructure and bridge deck intact. This alternative would likely result in fewer environmental impacts to terrestrial habitat and wetlands than the preferred alternative. However, this alternative would not correct the inadequate stopping sight distance and would not facilitate pedestrian connectivity. As such, the alternative would not have met the purpose and need of the project; therefore it was removed from further consideration.

No-Build Alternative: This alternative would involve no improvements to the existing structure. Although this alternative would eliminate project costs and any environmental impacts, this alternative would not meet the purpose and need of the project; therefore, this alternative was removed from further consideration.

The No Build Alternative is not feasible, prudent or practicable because (Mark all that apply)

It would not correct existing capacity deficiencies;

It would not correct existing safety hazards;

It would not correct the existing roadway geometric deficiencies;

It would not correct existing deteriorated conditions and maintenance problems; or

It would result in serious impacts to the motoring public and general welfare of the economy.

Other (Describe):

X
X

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ROADWAY CHARACTER:

If the proposed action includes multiple roadways, complete and duplicate for each roadway.

Name of Roadway	<u>US 20</u>		
Functional Classification:	<u>Principal arterial</u>		
Current ADT:	<u>14,950</u>	<u>VPD (2024)</u>	Design Year ADT: <u>15,307</u> <u>VPD (2044)</u>
Design Hour Volume (DHV):	<u>1,375</u>	<u>Truck Percentage (%)</u>	<u>3.59 DHV</u>
Designed Speed (mph):	<u>45</u>	<u>Legal Speed (mph):</u>	<u>45</u>

Existing		Proposed	
Number of Lanes:	2	2	
Type of Lanes:	Through Travel	Through Travel	
Pavement Width:	52.0	36.0-53.5	ft.
Shoulder Width:	22.0-28.0 total	12.0-25.5 total	ft.
Median Width:	N/A	4.0	ft.
Sidewalk Width:	N/A	8.0-10.0	ft.

Setting:	<input checked="" type="checkbox"/>	Urban	<input type="checkbox"/>	Suburban	<input type="checkbox"/>	Rural
Topography:	<input checked="" type="checkbox"/>	Level	<input type="checkbox"/>	Rolling	<input type="checkbox"/>	Hilly

Name of Roadway	<u>Kennedy Ave.</u>		
Functional Classification:	<u>Minor arterial</u>		
Current ADT:	<u>13,760</u>	<u>VPD (2024)</u>	Design Year ADT: <u>16,790</u> <u>VPD (2044)</u>
Design Hour Volume (DHV):	<u>1,707</u>	<u>Truck Percentage (%)</u>	<u>1.00 DHV</u>
Designed Speed (mph):	<u>35</u>	<u>Legal Speed (mph):</u>	<u>35</u>

Existing		Proposed	
Number of Lanes:	4	4	
Type of Lanes:	Through Travel	Through Travel	
Pavement Width:	48.0	48.0	ft.
Shoulder Width:	0	0	ft.
Median Width:	N/A	N/A	ft.
Sidewalk Width:	4.0	4.0	ft.

Setting:	<input checked="" type="checkbox"/>	Urban	<input type="checkbox"/>	Suburban	<input type="checkbox"/>	Rural
Topography:	<input checked="" type="checkbox"/>	Level	<input type="checkbox"/>	Rolling	<input type="checkbox"/>	Hilly

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BRIDGES AND/OR SMALL STRUCTURE(S):

If the proposed action includes multiple structures, complete and duplicate for each bridge and/or small structure. Include both existing and proposed bridge(s) and/or small structure(s) in this section.

Structure/NBI Number(s): Str. No. 020-45-02484 C/NBI No. 005200 Sufficiency Rating: 83.1, 07/11/2022 Bridge Inspection Report (Appendix I, I9)
(Rating, Source of Information)

	Existing		Proposed	
Bridge/Structure Type:	continuous prestressed concrete beam		continuous composite welded steel plate girder	
Number of Spans:	3		2	
Weight Restrictions:	N/A	ton	N/A	ton
Height Restrictions:	N/A	ft.	N/A	ft.
Curb to Curb Width:	52.0	ft.	57.5	ft.
Outside to Outside Width:	55.3	ft.	70.33	ft.
Shoulder Width:	28.0	ft.	25.5	ft.

Describe impacts and work involving bridge(s), culvert(s), pipe(s), and small structure(s). Provide details for small structure(s): structure number, type, size (length and dia.), location and impacts to water. Use a table if the number of small structures becomes large. If the table exceeds a complete page, put it in the appendix and summarize the information below with a citation to the table.

The subject bridge (Structure No. 020-45-02484 C/NBI No. 005200) carries US 20 over Kennedy Ave. Access to the bridge occurs by way of two access roads located north and south of US 20 and east of Kennedy Ave. Structure No. 020-45-02484 C is a three-span, continuous prestressed concrete beam bridge that features a clear roadway width, out-to-out width, and the bridge length measuring 52.0 feet, 55.3 feet, and 193.17 feet, respectively. Structure No. 020-45-02484 C also crosses three sets of railroad tracks, owned by IHB RR, which run parallel to the west side of Kennedy Ave. A sidewalk runs parallel to the east side of Kennedy Ave. south of the subject bridge.

The preferred alternative involves the replacement of the existing bridge. The replacement bridge will be named Structure No. 020-45-02821 and will be a continuous composite welded steel plate girder bridge. The bridge will be a two-span structure with MSE wall abutments and a reinforced concrete pier. The bridge will feature a length of approximately 222.0 feet, a clear roadway width of 53.5 (2x 26.75) feet, and an out-to-out width of approximately 70.3 feet (Note: these dimensions have been updated to match current design and differ slightly from the preliminary project plans). The bridge cross section will include two 12.0-foot-wide travel lanes, one in each direction, separated by a 4.0-foot-wide raised center median. The bridge will have 12.75-foot-wide paved outside shoulders and 2-foot-wide paved inside curb offsets. In addition, an 8- to 10-foot-wide multi-use trail will be added to the north side of the subject bridge, the north side of US 20, and the north access road. On the bridge the trail will have a steel and concrete (type PF-1) pedestrian barrier on both sides. A raise in the bridge profile grade of approximately 41.8 inches will be necessary to provide adequate vertical clearance over the railroad and Kennedy Ave.

One culvert (no Federal ID), an 87-foot-long and 12-inch-wide metal pipe that drains Wetland B to Wetland C, is located southeast of Structure No. 020-45-02484 C. This pipe will be removed and replaced with an open ditch. All other existing pipes and structures are part of the existing underground stormwater conveyance system.

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MAINTENANCE OF TRAFFIC (MOT) DURING CONSTRUCTION:

	Yes	No
Is a temporary bridge proposed?		X
Is a temporary roadway proposed?		X
Will the project involve the use of a detour or require a ramp closure? (describe below)	X	
Provisions will be made for access by local traffic and so posted.	X	
Provisions will be made for through-traffic dependent businesses.	X	
Provisions will be made to accommodate any local special events or festivals.	X	
Will the proposed MOT substantially change the environmental consequences of the action?		X
Is there substantial controversy associated with the proposed method for MOT?		X
Will the project require a sidewalk, curb ramp, and/or bicycle lane closure? (describe below)	X	
Provisions will be made for access by pedestrians and/or bicyclist and so posted (describe below).		X

Discuss closures, detours, and/or facilities (if any) that will be provided for maintenance of traffic. Any known impacts from these temporary measures should be quantified to the extent possible, particularly with respect to properties such as Section 4(f) resources and wetlands. Discuss any pedestrian/bicycle closures. Any local concerns about access and traffic flow should be detailed as well.

The MOT plan, which is still being finalized, will involve a closure and detour for US 20, lane closures for Kennedy Ave., and potential intermittent closures of Kennedy Ave (Appendix B, B21 to B25). The detour route for US 20 will utilize US 20/Indianapolis Blvd. (one mile west of the bridge), Columbus Dr./US 12, and US 12. The detour route is expected to result in 5.4 additional travel miles and 14 additional minutes of travel time. Lane closures on Kennedy Ave. will take place in two phases. Phase I will involve the closure of the southbound lanes and shift southbound traffic to the left (west) northbound lane. Phase II will involve closure of the northbound lanes and shift northbound traffic to the right (east) southbound lane. One lane of traffic in both directions will be maintained. If required, the detour route for intermittent closure of Kennedy Ave. will utilize E 165th St., US 20/ Indianapolis Blvd., and W Chicago Ave./SR 312. The detour route is expected to result in 1.7 additional travel miles and 5 additional minutes of travel time. The sidewalk located along the east side of Kennedy Ave. will be closed for the duration of the MOT. A pedestrian MOT for the sidewalk closure east of Kennedy Ave. has not been developed at this time, however the need for a pedestrian MOT is still currently being investigated. Should one be required, a pedestrian detour would be in place for the duration of the MOT. The MOT is expected to begin in April of 2026 and be in place for 18 months.

The closures/lane restrictions will pose a temporary inconvenience to traveling motorists (including school buses and emergency services); however, no significant delays are anticipated, and all inconveniences and delays will cease upon project completion.

ESTIMATED PROJECT COST AND SCHEDULE:

Engineering* \$ 735,000 (Pre-2022, Right-of-Way* \$ 0 (N/A) Construction*: \$ 3,854,364 (2024, 2025)

Anticipated Start Date of Construction: April of 2026

*Funding above pulled from the current Fiscal Year (FY) 2022-2026 NIRPC TIP. Funding programmed in the NIRPC TIP is reflective of a different alternative with a reduced scope and no ROW compared to the current preferred alternative. Estimated total project costs now equal approximately \$19,041,000, including Engineering (\$2,766,000), ROW (\$35,000), and Construction (\$16,240,000) (Appendix I, I18).

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Land Use Impacts	Amount (acres)	
	Permanent	Temporary
Residential	0.0	0.0
Commercial	1.00	0.04
Agricultural	0.0	0.0
Forest	0.0	0.0
Wetlands	0.12	0.0
Other: Municipal	0.01	0.02
Other:	0.0	0.0
TOTAL	1.13	0.06

Describe both Permanent and Temporary right-of-way and describe their current use. Typical and Maximum right-of-way widths (existing and proposed) should also be discussed. Any advance acquisition, reacquisition or easements, either known or suspected, and their impacts on the environmental analysis should be discussed.

West of the subject bridge, existing ROW extends approximately 43 to 100 feet north and 37 to 100 feet south of the US 20 centerline. Existing ROW consists of roadway, maintained roadside vegetation, and wetlands.

The project requires approximately 1.13 acres of new permanent ROW. New permanent ROW is located to the northwest, northeast, and southeast of the subject bridge, and consists of commercial property (including private-owned utility, Indiana Toll Road, and industrial business property), City-owned property (National Register of Historic Places (NRHP)-eligible utility property), and wetlands. New permanent ROW will extend approximately 43 to 140 feet north of the US 20 centerline, and 37 to 140 feet south of the US 20 centerline. Activities within permanent ROW areas include expansion and reconstruction of the access roads and drainage work. The project also requires approximately 0.06 acre of temporary ROW. Temporary ROW is located to the northwest, southwest, and southeast of the bridge and consists of commercial property (Indiana Toll Road) and City-owned property (NRHP-eligible utility property). Activities within temporary ROW areas include grading.

If the scope of work or permanent or temporary right-of-way amounts change, the INDOT Environmental Services Division (ESD) and the INDOT District Environmental Section will be contacted immediately. This firm commitment is included in the *Environmental Commitments* of this Categorical Exclusion (CE) document.

Part III – Identification and Evaluation of Impacts of the Proposed Action

SECTION A - EARLY COORDINATION:

List the date(s) coordination was sent and all resource agencies that were contacted as a part of the development of this Environmental Study. Also, include the date of their response or indicate that no response was received.

Early coordination letters were sent on September 7 and 8, 2022, September 13, 2022, and December 5, 2023 (Appendix C, C1 to C4).

<u>Agency</u>	<u>Date Sent</u>	<u>Date Response Received</u>	<u>Appendix C</u>
FHWA – Indiana Division	September 8, 2022	No Response Received	N/A
US Department of Agriculture, Natural Resources Conservation Service	September 8, 2022	September 8, 2022	C7
US Department of Housing and Urban Development, Chicago Regional Office	September 8, 2022	No Response Received	N/A
US Army Corps of Engineers, Chicago District	September 8, 2022	September 16, 2022	C9
National Park Service	September 8, 2022	No Response Received	N/A

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INDOT LaPorte District Office, Environmental Section Manager	September 8, 2022	No Response Received	N/A
INDOT LaPorte District Office, Project Manager	September 8, 2022	No Response Received	N/A
INDOT Environmental Services Division	September 8, 2022	No Response Received	N/A
INDOT Office of Aviation	September 8, 2022	No Response Received	N/A
INDOT Utilities and Railroads	September 8, 2022	No Response Received	N/A
Indiana Dept. of Natural Resources, Division of Fish and Wildlife	September 8, 2022	October 7, 2022	C10 to C12
Indiana Geological and Water Survey	September 7, 2022	September 7, 2022	C5 to C6
Hammond City Engineer	September 8, 2022	No Response Received	N/A
Hammond Fire Department	September 8, 2022	No Response Received	N/A
Hammond Public Works	September 8, 2022	No Response Received	N/A
Hammond Mayor's Office	September 8, 2022	No Response Received	N/A
Hammond Common Council	September 8, 2022	No Response Received	N/A
School City of Hammond	September 8, 2022	No Response Received	N/A
Sanitary District of Hammond (Multiple Separate Storm Sewer System [MS4])	September 8, 2022	No Response Received	N/A
East Chicago City Engineer	September 8, 2022	No Response Received	N/A
East Chicago Public Works	September 8, 2022	No Response Received	N/A
East Chicago Fire Department	September 8, 2022	No Response Received	N/A
East Chicago Mayor's Office	September 8, 2022	No Response Received	N/A
East Chicago Common Council	September 8, 2022	No Response Received	N/A
School City of East Chicago	September 8, 2022	No Response Received	N/A
East Chicago Stormwater Quality Management (MS4)	September 8, 2022	No Response Received	N/A
Lake County Emergency Management Agency	September 8, 2022	No Response Received	N/A
Lake County 911 Department	September 8, 2022	No Response Received	N/A
Lake County Sheriff Department	September 8, 2022	No Response Received	N/A
Lake County Highway Department	September 8, 2022	No Response Received	N/A
Lake County Board of Commissioners	September 8, 2022	No Response Received	N/A
Lake County Council	September 8, 2022	No Response Received	N/A
NIRPC	September 8, 2022	No Response Received	N/A

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East Chicago Parks and Recreation	September 8, 2022	No Response Received	N/A
Hammond Parks and Recreation	September 8, 2022	No Response Received	N/A
East Chicago Water Works Department	September 13, 2022	No Response Received	N/A
Indiana American Water – Northwest	September 13, 2022	September 13, 2022	C8
Hammond Water Works Department	September 13, 2022	No Response Received	N/A
Lake County MS4 Coordinator	December 5, 2023	No Response Received	N/A

All applicable recommendations are included in the *Environmental Commitments* section of this CE document.

SECTION B – ECOLOGICAL RESOURCES:

Streams, Rivers, Watercourses & Other Jurisdictional Features

Federal Wild and Scenic Rivers
State Natural, Scenic or Recreational Rivers
Nationwide Rivers Inventory (NRI) listed
Outstanding Rivers List for Indiana
Navigable Waterways

Presence

Impacts

Yes

No

Total stream(s) in project area: 0.0 Linear feet Total impacted stream(s): 0.0 Linear feet

Describe all streams, rivers, watercourses and other jurisdictional features adjacent or within the project area. Include whether or not impacts (both permanent and temporary) will occur to the features identified. Include if the streams or rivers are listed on any federal or state lists for Indiana. Include if features are likely subject to federal or state jurisdiction. Discuss measures to avoid, minimize, and mitigate if impacts will occur.

Based on the desktop review, the aerial map of the project area (Appendix B, B3), and the Red Flag Investigation (RFI) report (Appendix E, E1 to E15), there are four streams, rivers, watercourses, or other jurisdictional features within the 0.5-mile search radius. There are no streams, rivers, watercourses, or other jurisdictional features within or adjacent to the project area, which was confirmed by the site visit on October 19, 2021 by Lochmueller Group. Therefore, no impacts are expected.

A *Waters of the U.S. Determination / Wetland Delineation Report* was approved by INDOT Ecology and Waterway Permitting Office (EWPO) on August 12, 2022. Please refer to Appendix F, F1 to F19 for the *Waters of the U.S. Determination / Wetland Delineation Report*. It was determined that no jurisdictional streams are present in the project area. The U.S. Army Corps of Engineers (USACE) makes all final determinations regarding jurisdiction.

Open Water Feature(s)

Reservoirs
Lakes
Farm Ponds
Retention/Detention Basin
Storm Water Management Facilities
Other: _____

Presence

Impacts

Yes

No

Describe all open water feature(s) identified adjacent or within the project area. Include whether or not impacts (both permanent and temporary) will occur to the features identified. Include if features are likely subject to federal or state jurisdiction. Discuss measures to avoid, minimize, and mitigate if impacts will occur.

Based on the desktop review, the aerial map of the project area (Appendix B, B3), and the RFI report (Appendix E, E1 to E15), there

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are nine open water features within the 0.5-mile search radius. There are no open water features within or adjacent to the project area, which was confirmed by the site visit on October 19, 2021 by Lochmueller Group. A *Waters of the U.S. Determination / Wetland Delineation Report* was approved by INDOT EWPO on August 12, 2022. Please refer to Appendix F, F1 to F19 for the *Waters of the U.S. Determination / Wetland Delineation Report*. It was determined that no open water features are present in the project area. The USACE makes all final determinations regarding jurisdiction.

Wetlands

Presence

☒ X

Impacts

Yes

☒ X

No

☐

Total wetland area: 2.1 Acre(s) Total wetland area impacted: 0.4124 Acre(s)

(If a determination has not been made for non-isolated/isolated wetlands, fill in the total wetland area impacted above.)

Wetland No.	Classification	Total Size (Acres)	Impacted Acres	Comments (i.e. location, likely Water of the US, appendix reference)
Wetland A	PFO1/EM1F	0.99	0	41.6098274, -87.4558046, Likely WOTUS, Appendix F
Wetland B	PEM1C	0.40	0.278	41.6097802, -87.4591270, Likely WOTUS, Appendix F
Wetland C	PEM1E	0.01	0.0004	41.6096522, -87.4603194, Likely WOTUS, Appendix F
Wetland D	PEM1E	0.28	0	41.6102608, -87.4569263, Likely WOTUS, Appendix F
Wetland E	PEM1E	0.15	0	41.6100638, -87.4550342, Likely WOTUS, Appendix F
Wetland F	PEM1E	0.27	0.134	41.6102759, -87.4630722, Likely WOTUS, Appendix F

Wetlands (Mark all that apply)

Wetland Determination

Wetland Delineation

USACE Isolated Waters Determination

Documentation

☒ X
☒ X
☐

ESD Approval Dates

August 12, 2022
August 12, 2022

Improvements that will not result in any wetland impacts are not practicable because such avoidance would result in (Mark all that apply and explain):

- Substantial adverse impacts to adjacent homes, business or other improved properties;
- Substantially increased project costs;
- Unique engineering, traffic, maintenance, or safety problems;
- Substantial adverse social, economic, or environmental impacts, or
- The project not meeting the identified needs.

☐
☐
☒ X
☐
☒ X

Describe all wetlands identified adjacent or within the project area. Include whether or not impacts (both permanent and temporary) will occur to the features identified. Include if features are likely subject to federal or state jurisdiction. Discuss measures to avoid, minimize, and mitigate if impacts will occur.

Based on the desktop review, the aerial map of the project area (Appendix B, B3), and the RFI report (Appendix E, E1 to E15), there are 33 wetlands within the 0.5-mile search radius. There is one wetland within the project area, which was updated to six wetlands by the site visit on October 19, 2021 by Lochmueller Group.

A *Waters of the U.S. Determination / Wetland Delineation Report* was approved by INDOT EWPO on August 12, 2022. Please refer to Appendix F, F1 to F19 for the *Waters of the U.S. Determination / Wetland Delineation Report*. It was determined that six wetlands, Wetlands A, B, C, D, E, and F, are present within the project area. The USACE makes all final determinations regarding jurisdiction.

Wetland A

Wetland A is a 0.99-acre emergent wetland situated along the south side of US 20 and east of Kennedy Ave. Wetland A is part of a large common reed (*Phragmites australis*) and black willow (*Salix nigra*) dominated wetland/pond complex present within and extending beyond the southeast project area limits. Wetland A would be classified as a palustrine, forested, broad-leaf deciduous /

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emergent, persistent, semi-permanently flooded wetland. Wetland A is connected to underground and surface drainage systems flowing north to Grand Calumet River, a traditional navigable waterway (TNW), within Lake County from the Illinois border to Marquette Park in Gary. As a result, Wetland A possesses a direct hydrologic connection to a TNW and thus would fall under Clean Water Act jurisdiction. Based on a qualitative assessment of Wetland A, this wetland is of low quality; despite its large size and being part of a greater wetland/pond complex, it has a prevalence of invasive species and high disturbance.

No permanent or temporary impacts to Wetland A are anticipated.

Wetland B

Wetland B is a 0.40-acre emergent wetland located along the south side of US 20 within an excavated concave drainage area. Wetland B serves as a roadside drainage area and is partially mowed. Wetland B would be classified as a palustrine emergent, persistent, seasonally flooded wetland. Wetland B is connected to underground and surface drainage systems flowing north to Grand Calumet River, a TNW. As a result, Wetland B possesses a direct hydrologic connection to a TNW and thus would fall under Clean Water Act jurisdiction. Based on a qualitative assessment of Wetland B, this wetland is of low quality due to its prevalence of invasive species.

Approximately 0.278 acre of permanent impacts to Wetland B are anticipated due to reconstruction of the south approach ramp, work on the culvert abutting the wetland, and ditch excavation for roadside drainage. Temporary impacts to Wetland B are not anticipated.

Wetland C

Wetland C is a 0.01-acre emergent wetland located south of the southern US 20 on/off ramp. Wetland C is a sparsely vegetated concave area subject to ponding with a hydrophyte-dominated fringe and shaded by trees located outside of the wetland. Wetland C would be classified as a palustrine emergent, persistent, seasonally flooded/saturated wetland. Wetland C is located between a culvert outlet leading from Wetland B to the east and a corrugated metal pipe (CMP) culvert inlet to the west which empties outside the project area, presumably following the classified National Hydrologic Dataset (NHD) flowline north to Grand Calumet River. As a result, Wetland C possesses a direct hydrologic connection to a TNW and thus would fall under Clean Water Act jurisdiction. Based on a qualitative assessment of Wetland C, this wetland is of low quality due to its prevalence of invasive species.

Approximately 0.0004 acre of permanent impacts to Wetland C are anticipated due to work on the pipe that abuts it to the east. Temporary impacts to Wetland C are not anticipated.

Wetland D

Wetland D is a 0.28-acre emergent wetland situated on the north side of US 20, east of Kennedy Ave., and west of a field entrance drive. Wetland D would be classified as a palustrine emergent, persistent, seasonally flooded/saturated wetland. No features draining Wetland D were field identified. However, the NHD dataset shows an unclassified drainage flowline originates at Wetland D, converges with other NHD lines, and outlets to Grand Calumet River. Wetland D occurs approximately 1,640 feet southwest of the Grand Calumet River, a TNW, and is separated from Grand Calumet River via roads, embankments, and other manmade structures. Despite presence of barriers, due to an ephemeral connection and proximity to Grand Calumet River, Wetland D would contribute to the significant nexus of a TNW. Thus, Wetland D would fall under Clean Water Act jurisdiction. Based on a qualitative assessment of Wetland D, this wetland is of low quality due to disturbance via mowing and relatively low floristic quality.

No permanent or temporary impacts to Wetland D are anticipated.

Wetland E

Wetland E is a 0.15-acre emergent wetland situated on the north side of US 20, east of Kennedy Ave., and east of the field entrance drive, and continues east outside the project area. Wetland E would be classified as a palustrine emergent, persistent, seasonally flooded/saturated wetland. No features directly draining Wetland E were field identified within the investigation area. However, Wetland E drains to Grand Calumet River outside the investigation area. Wetland E occurs approximately 1,270 feet southwest of the Grand Calumet River, a TNW, and is separated from Grand Calumet River via roads, embankments, and other manmade structures. Despite presence of barriers, due to an ephemeral connection and proximity to Grand Calumet River, Wetland E would contribute to the significant nexus of a TNW. Thus, Wetland E would fall under Clean Water Act jurisdiction. Based on a qualitative assessment of Wetland E, this wetland is of low quality due to severe disturbance via mowing and relatively low floristic quality.

No permanent or temporary impacts to Wetland E are anticipated.

Wetland F

Wetland F is a 0.27-acre emergent wetland situated on the north side of US 20, west of Kennedy Ave. Wetland F would be classified as a palustrine emergent, persistent, seasonally flooded/saturated wetland. Wetland F is connected to Grand Calumet River, a TNW.

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Thus, Wetland F would fall under Clean Water Act jurisdiction. Based on a qualitative assessment of Wetland F, this wetland is of low quality due to severe disturbance via mowing and relatively low floristic quality.

Approximately 0.134 acre of permanent impacts to Wetland F are anticipated due to embankment grading related to raising the profile grade of US 20. Temporary impacts to Wetland F are not anticipated.

In total, 0.4124 acre of permanent impacts to Wetlands B, C, and F are anticipated. Due to these permanent impacts to wetlands, a USACE Section 404 Regional General Permit (RGP) and an Indiana Department of Environmental Management (IDEM) Section 401 Individual Permit (IP) will likely be required. Because the total impact to wetlands is greater than 0.1 acre, mitigation will likely be required and will be determined during permitting.

Wetlands A, D, and E will be identified on the project plans and labeled "Do Not Disturb". This firm commitment is included in the *Environmental Commitments* section of this document.

Impacts to wetlands are required in order to properly grade the replacement bridge and to provide adequate drainage around the bridge. As such there is no practicable alternative to completely avoid impacts to wetlands. The proposed action minimizes and avoids impacts to wetlands to the extent possible while meeting the purpose and need of the project.

The Indiana Department of Natural Resources (IDNR) Division of Fish and Wildlife (DFW) responded on October 7, 2022, with recommendations to avoid and minimize impacts to fish, wildlife, and botanical resources, to coordinate with the appropriate agencies with regard to wetland impacts and permitting, and to mitigate wetland impacts at the appropriate ratio (Appendix C, C10 to C12). All applicable recommendations are included in the *Environmental Commitments* section of this CE document.

	<u>Presence</u>	<u>Impacts</u>	
		<u>Yes</u>	<u>NO</u>
Terrestrial Habitat	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

Total terrestrial habitat in project area: 3.92 Acre(s) Total tree clearing: 1.3 Acre(s)

Describe types of terrestrial habitat (i.e. forested, grassland, farmland, lawn, etc.) adjacent or within the project area. Include whether or not impacts will occur to habitat identified. Include total terrestrial habitat impacted and total tree clearing that will occur. Discuss measure to avoid, minimize, and mitigate if impacts will occur.

Based on a desktop review, a site visit on October 19, 2021, by Lochmueller Group, the aerial map of the project area (Appendix B, B3), there is terrestrial habitat present in the project area consisting of maintained grass fields and wetlands. Dominant species include eastern cottonwood (*Populus deltoides*), Siberian elm (*Ulmus pumila*), black walnut (*Juglans nigra*), common reed, red fescue (*Festuca rubra*), common plantain (*Plantago major*), spotted jewelweed (*Impatiens capensis*), Pennsylvania knotweed (*Persicaria pensylvanica*), shining flatsedge (*Cyperus bipartitus*), path rush (*Juncus tenuis*), and yellow flatsedge (*Cyperus flavescentis*). Impacts to terrestrial habitat total approximately 3.92 acres and will result from bridge, ramps, and roadway widening, regrading, and drainage alterations. Tree clearing will total 1.3 acres. Tree clearing will occur within 100 feet of the roadway, will take place during the inactive season, and will be located around four quadrants of the bridge and along the north side of US 20. Impacts to terrestrial habitat will be minimized and avoided to the greatest extent possible while meeting the purpose and need of the project. Mitigation for terrestrial impacts is not anticipated.

IDNR DFW responded on October 7, 2022, with recommendations to avoid and minimize impacts to fish, wildlife, and botanical resources, to revegetate bare and disturbed areas, to minimize and contain tree and brush clearing, timing restrictions for tree clearing, and to control erosion and sedimentation (Appendix C, C10 to C12). All applicable recommendations are included in the *Environmental Commitments* section of this CE document.

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Protected Species

Federally Listed Bats

Information for Planning and Consultation (IPaC) determination key completed
 Section 7 informal consultation completed (IPaC cannot be completed)
 Section 7 formal consultation Biological Assessment (BA) required

Yes	No
<input checked="" type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>

Determination Received for Listed Bats from USFWS: NE ☐ NLAA ☒ LAA ☐

Other Species not included in IPaC

Additional federal species found in project area (based on IPaC species list)
 State species (not bird) found in project area (based upon consultation with IDNR)

Yes	No
<input checked="" type="checkbox"/>	<input type="checkbox"/>
<input checked="" type="checkbox"/>	<input type="checkbox"/>

Migratory Birds

Known usage or presence of birds (i.e. nests)
 State bird species based upon coordination with IDNR

Yes	No
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input checked="" type="checkbox"/>	<input type="checkbox"/>

Discuss IDNR coordination and species identified. Describe USFWS Section 7 consultation and determination received for Indiana bat and northern long-eared bat impacts. Discuss if other federally listed species were identified. If so, include consultation that has occurred and the determination that was received. Discuss if migratory birds have been observed and any impacts.

Based on a desktop review and the RFI report (Appendix E, E1 to E15), completed by Lochmueller Group on June 7, 2022, the IDNR Lake County Endangered, Threatened, and Rare (ETR) Species List has been checked. According to the IDNR DFW early coordination response letter dated October 7, 2022 (Appendix C, C10 to C12), the Natural Heritage Program's Database has been checked. IDNR DFW noted the presence of several plant, insect, bird, reptile, amphibian, and mammal species, as well as managed lands and natural communities, within 0.5 mile of the project area, listed below. IDNR DFW did not anticipate any impacts to the bird, reptile, amphibian, or mammal species as a result of the project. The IDNR Division of Nature Preserves did not anticipate any impacts to the communities, plant species, or insect species as a result of the project. An INDOT 0.5-mile bat review occurred on October 27, 2021 and did not indicate the presence of endangered bat species in or within 0.5 mile of the project area.

A) MANAGED LANDS:

1. Seidner Dune and Swale Nature Preserve, Shirley Heinze Land Trust
2. Gibson Woods Nature Preserve, Lake County Parks & Rec

B) NATURAL COMMUNITIES:

1. Dry-mesic Sand Prairie
2. Dry-mesic Sand Savanna

C) PLANT:

1. Paper birch (*Betula papyrifera*), state threatened

D) INSECTS:

1. Gemmed cordgrass borer (*Peoria gemmatella*), state threatened
2. Dusted skipper (*Atrytonopsis hianna*), state rare
3. Goat's rue looper (*Digrammia eremiata*), state rare
4. Leonard's skipper (*Hesperia leonardus*), state rare
5. Two-lined cosmoteletix (*Cosmotettix bilineatus*), state rare

E) BIRDS:

1. Black tern (*Chlidonias niger*), state endangered
2. Black-crowned night-heron (*Nycticorax nycticorax*), state endangered
3. Marsh wren (*Cistothorus palustris*), state endangered
4. Virginia rail (*Rallus limicola*), state endangered
5. Great egret (*Ardea alba*), state special concern
6. Bald eagle (*Haliaeetus leucocephalus*)

F) REPTILE & AMPHIBIAN:

1. Blanchard's cricket frog (*Acris blanchardi*), state special concern
2. Western ribbon snake (*Thamnophis proximus proximus*), state special concern

G) MAMMAL:

1. Franklin's ground squirrel (*Poliocitellus franklinii*), state endangered

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Indiana Bat and Northern Long-Eared Bat

Project information was submitted through the U.S. Fish and Wildlife Service's (USFWS) Information for Planning and Consultation (IPaC) portal, and an official species list was generated (Appendix C, C13 to C26). The project is within range of the federally endangered Indiana bat (*Myotis sodalis*) and the federally endangered northern long-eared bat (NLEB) (*Myotis septentrionalis*). Other species were generated in the IPaC species list along with the Indiana bat and NLEB. Refer to paragraph below.

The project qualifies for the *Range-wide Programmatic Informal Consultation for the Indiana bat and northern long-eared bat (NLEB)*, dated May 2016 (revised February 2018), between FHWA, Federal Railroad Administration (FRA), Federal Transit Administration (FTA), and USFWS. Bridge and culvert inspections occurred on October 26, 2023 and no evidence of birds or bats was found (Appendix C, C43 to C44). USFWS Bridge/Structure Assessments are only valid for two years. If construction will begin after October 26, 2025, an inspection of the structures by a qualified individual must be performed. Inspection of the structures should check for presence of bats/bat indicators and/or presence of birds. The results of the inspection must indicate no signs of bats or birds. If signs of bats or birds are documented during this inspection, the INDOT District Environmental Manager must be contacted immediately. This firm commitment is included in the *Environmental Commitments* section of this document.

An effect determination key was completed on October 16, 2023, and based on the responses provided, the project was found to "Not Likely to Adversely Affect" the Indiana bat and/or the NLEB (Appendix C, C27 to C42). INDOT reviewed and verified the effect finding on October 18, 2023, and requested USFWS's review of the finding. No response was received from USFWS within the 14-day review period; therefore, it was concluded they concur with the finding. Avoidance and Minimization Measures (AMMs) listed for the project include General AMM 1, Lighting AMM 1, Lighting AMM 2, and Tree Removal AMMs 1 to 4, which include measures pertaining to bat habitat awareness, use of temporary lighting, installation of permanent lighting, and tree clearing considerations and restrictions. AMMs and/or commitments are included as firm commitments in the *Environmental Commitments* section of this document.

Other Protected Species

The official species list generated from IPaC indicated four other species present within the project area, including Karner blue butterfly (*Lycaeides melissa samuelis*, endangered), tricolored bat (*Perimyotis subflavus*, proposed endangered), whooping crane (*Grus americana*, experimental population, non-essential) and monarch butterfly (*Danaus plexippus*, candidate). The tricolored bat is listed as proposed endangered and was not considered endangered. The whooping crane is listed as endangered wherever found, except where listed as an experimental population according to the Environmental Conservation Online System (<https://ecos.fws.gov/ecp/species/758>). The whooping crane is listed as an experimental population in this location, so it was not considered endangered at the project location. The monarch butterfly is identified as a candidate species, which is not yet listed or proposed for listing. Therefore, these three species were not considered as part of this project. For the Karner blue butterfly, the project was found to have "No Effect" on the species. Because habitat for the Karner blue butterfly is decidedly absent from the project area, the scope of the project would not result in any impacts to the species or habitat suitable for introduction of the species. The project does not qualify for the current INDOT/USFWS agreement due to impacts to wetlands exceeding 0.1 acre.

This precludes the need for further consultation on this project as required under Section 7 of the Endangered Species Act, as amended. If new information on endangered species at the site becomes available, or if project plans are changed, USFWS will be contacted for consultation.

Geological and Mineral Resources

Project located within the Indiana Karst Region
Karst features identified within or adjacent to the project area
Oil/gas or exploration/abandoned wells identified in the project area

Yes

No

X
X
X

Date Karst Evaluation reviewed by INDOT EWPO (if applicable):

N/A

Discuss if project is located in the Indiana Karst Region and if any karst features have been identified in the project area (from RFI). Discuss response received from IGWS coordination. Discuss if any mines, oil/gas, or exploration/abandoned wells were identified and if impacts will occur. Include discussion of karst study/report was completed and results. (Karst investigation must comply with the current Protection of Karst Features during Planning and Construction guidance and coordinated and reviewed by INDOT EWPO)

Based on a desktop review and the Indiana Karst Region map, the project is located outside the designated Indiana Karst Region as outlined in the most current *Protection of Karst Features during Project Development and Construction*. According to the topographic map of the project area (Appendix B, B2) and the RFI report (Appendix E, E1 to E15), there are no karst features identified within or adjacent to the project area. In the early coordination response dated September 7, 2022, the Indiana Geological and Water Survey (IGWS) did not indicate that karst features exist in the project area (Appendix C, C5 to C6). IGWS indicated that the site contained high liquefaction potential, floodway, high potential for bedrock resource, and low potential for sand and gravel resource. No active or

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abandoned mineral resource extraction sites were identified. Response from IGWS has been communicated with the designer on December 14, 2023. No impacts are expected.

SECTION C – OTHER RESOURCES

Drinking Water Resources

Wellhead Protection Area(s)
Source Water Protection Area(s)
Water Well(s)
Urbanized Area Boundary
Public Water System(s)

Presence

X
X
X

Impacts

Yes	No
	X
	X
	X

Is the project located in the St. Joseph Sole Source Aquifer (SSA):

If Yes, is the FHWA/EPA SSA MOU Applicable?

If Yes, is a Groundwater Assessment Required?

Yes	No
	X

Check the appropriate boxes and discuss each topic below. Provide details about impacts and summarize resource-specific coordination responses and any mitigation commitments. Reference responses in the Appendix.

Sole Source Aquifer

The project is located in Lake County, which is not located within the area of the St. Joseph Sole Source Aquifer, the only legally designated sole source aquifer in the state of Indiana. Therefore, the FHWA/Environmental Protection Agency (EPA)/INDOT Sole Source Aquifer Memorandum of Understanding (MOU) is not applicable to this project, a detailed groundwater assessment is not needed, and no impacts are expected.

Wellhead Protection Area and Source Water

The IDEM Wellhead Proximity Determinator website (<http://www.in.gov/idem/cleanwater/pages/wellhead/>) was accessed on September 7, 2022, by Lochmueller Group. This project is not located within a wellhead protection area but is located within a Source Water Area. The Source Water Area Administrator responded to early coordination on September 13, 2022, and indicated that the project was not anticipated to impact the source water area (Appendix C, C8). The features will not be affected because excavation is not likely to encounter groundwater. The project will comply with all required provisions within the Source Water Assessment Program (<https://www.in.gov/idem/cleanwater/information-about/groundwater-monitoring-and-source-water-protection/source-water-assessment-program/>).

Water Wells

The Indiana Department of Natural Resources Water Well Record Database website (<https://www.in.gov/dnr/water/3595.htm>) was accessed on December 5, 2023. No wells are located near this project. Therefore, no impacts are expected.

Urban Area Boundary

Based on a desktop review of the IDEM MS4 Boundaries Map for Indiana (<https://www.in.gov/idem/cleanwater/ms4s-boundaries-map-for-indiana/>) by Lochmueller Group on December 5, 2023, this project is located within three Urban Area Boundaries (UABs). An early coordination letter was sent on September 8, 2022, to the Sanitary District of Hammond and to East Chicago Stormwater Quality Management, and on December 5, 2023, to the Lake County MS4 Coordinator. The MS4 coordinators did not respond within the 30-day time frame. The project will comply with all stormwater management requirements detailed in INDOT standard specifications. Avoidance alternatives are not practicable as work must occur within the urban area boundaries to accomplish the purpose and need of the project.

Public Water System

Based on a desktop review, a site visit on October 19, 2021 by Lochmueller group, the aerial map of the project area (Appendix B, B3), and the project plans (Appendix B, B14), this project is located where there are public water systems. The public water systems include sewer, municipal water, and combined sewer/sanitary sewer. The public water system will not be affected. No pipe relocations or other work requiring stoppage of services will occur. Early coordination letters were sent on September 8, 2022, to Hammond Public Works and East Chicago Public Works. No responses were received.

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Floodplains

Project located within a regulated floodplain

Longitudinal encroachment

Transverse encroachment

Homes located in floodplain within 1000' up/downstream from project

Presence

Impacts

Yes

No

If applicable, indicate the Floodplain Level?

Level 1 ☐

Level 2 ☐

Level 3 ☐

Level 4 ☐

Level 5 ☐

Use the IDNR Floodway Information Portal to help determine potential impacts. Include floodplain map in appendix. Discuss impacts according to the classification system. If encroachment on a flood plain will occur, coordinate with the Local Flood Plain Administrator during design to insure consistency with the local flood plain planning.

The IDNR Indiana Floodway Information Portal website (<https://www.in.gov/dnr/water/surface-water/indiana-floodplain-mapping/indiana-floodplain-information-portal/>) was accessed on May 16, 2022, by Lochmueller Group. This project is not located in a regulatory floodplain as determined from approved IDNR floodplain maps (Appendix F, F20). Therefore, it does not fall within the guidelines for the implementation of 23 CFR 650, 23 CFR 771, and 44 CFR. No impacts are expected.

Farmland

Agricultural Lands

Prime Farmland (per NRCS)

Presence

Impacts

Yes

No

Total Points (from Section VII of CPA-106/AD-1006*) N/A

**If 160 or greater, see CE Manual for guidance.*

Discuss existing farmland resources in the project area, impacts that will occur to farmland, and mitigation and minimization measures considered.

Based on a desktop review, a site visit on October 19, 2021 by Lochmueller Group, and the aerial map of the project area (Appendix B, B3), there is no land that meets the definition of farmland under the Farmland Protection Policy Act (FPPA) within or adjacent to the project area. The requirements of the FPPA do not apply to this project; therefore, no impacts are expected. An early coordination letter was sent on September 8, 2022, to the U.S. Department of Agriculture (USDA) Natural Resources Conservation Service (NRCS). NRCS indicated that the project would not cause a conversion of prime farmland (Appendix C, C7).

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SECTION D – CULTURAL RESOURCES

Minor Projects PA	Category(ies) and Type(s)	INDOT Approval Date(s)	N/A
	<input type="text"/>	<input type="text"/>	X

Full 106 Effect Finding

No Historic Properties Affected ☐ No Adverse Effect ☒ Adverse Effect ☐

Eligible and/or Listed Resources Present

NRHP Building/Site/District(s) ☒ Archaeology ☐ NRHP Bridge(s) ☐

Documentation Prepared (mark all that apply)

APE, Eligibility and Effect Determination ☒
 800.11 Documentation ☒
 Historic Properties Report or Short Report ☒
 Archaeological Records Check and Assessment ☐
 Archaeological Phase Ia Survey Report ☒
 Archaeological Phase Ic Survey Report ☐
 Other:

ESD Approval Date(s)

August 22, 2023
November 13, 2023
February 28, 2022
February 28, 2022

SHPO Approval Date(s)

March 30, 2022
December 12, 2023
March 30, 2022
March 30, 2022

Memorandum of Agreement (MOA) ☐

MOA Signature Dates (List all signatories)

If the project falls under the MPPA, describe the category(ies) that the project falls under and any approval dates. If the project requires full Section 106, use the headings provided. The completion of the Section 106 process requires that a Legal Notice be published in local newspapers. Please indicate the publication date, name of the paper(s) and the comment period deadline. Include any further Section 106 work which must be completed at a later date, such as mitigation from a MOA or avoidance commitments.

Area of Potential Effect (APE):

The APE for this project encompasses all resources immediately adjacent to the project area and those that may not be immediately adjacent but that have a proximate viewshed of the project area. The project area encompasses the area required to support the purpose and need of the project. Along US 20/Michigan St. the APE extends approximately 1,000 feet east and approximately 1,300 feet west of the project area. Along Kennedy Ave. the APE extends approximately 1,200 feet north and approximately 930 feet south of the project area. In other areas, the Indiana Toll Road (I-90) limits viewshed and therefore the APE is located on the outside edge of the Indiana Toll Road north of the project area. Additionally, the southern edge of the APE outside of Kennedy Ave. is limited by petroleum storage tanks and vegetation. The archaeological APE for the undertaking is represented by the 10.3 hectares (ha) (25.5 acres) survey area encompassing portions of US 20, I-94, and Kennedy Ave. examined for the presence of archaeological resources.

Coordination with Consulting Parties:

Consulting parties were identified in accordance with the INDOT *Cultural Resources Manual* and through consultation with INDOT Cultural Resource Office (CRO). Early coordination was initiated on December 10, 2021, with an email to consulting parties (Appendix D, D35 to D36). The email asked consulting parties to review the early coordination letter attached to the email and via IN SCOPE, which is INDOT's Section 106 Consultation and Outreach Portal Enterprise document website (<http://erms.indot.in.gov/Section106Documents/>). A hard copy of these materials was mailed to the State Historic Preservation Officer (SHPO), Lake County Commissioners, and the Lake County Historical Society and Museum that same day.

Section 106 Invited Consulting Parties	Date of Response
SHPO (Automatic Consulting Party)	December 16, 2021; March 30, 2022
Lake County Commissioners	N/A
Lake County Historian	December 14, 2021; February 28, 2022
Hammond Historical Society	N/A
Lake County Historic Society and Museum	N/A
Indiana Landmarks, Northwest Field Office	N/A
Lake County Highway Superintendent	N/A

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Northwestern Indiana Regional Planning Commission	N/A
Eastern Shawnee Tribe of Oklahoma	January 5, 2022; March 7, 2022
Forest County Potawatomi Community	N/A
Miami Tribe of Oklahoma	January 3, 2022
Peoria Tribe of Indians of Oklahoma	December 14, 2021; March 1, 2022
Pokagon Band of Potawatomi Indians	December 22, 2021
Shawnee Tribe	N/A

Bold text indicates accepted consulting party status.

In an email dated December 14, 2021, the Lake County Historian responded to the early coordination email stating, "I have reviewed the submitted documents and feel confident that the necessary fieldwork will identify any historical and archaeological resources that may be adversely affected by this project." See Appendix D, D42 to D43 for a copy of the communication.

In a letter dated December 14, 2021, the Peoria Tribe of Indians of Oklahoma responded to the early coordination letter stating that, "[t]he Peoria Tribe of Indians of Oklahoma is unaware of a direct link to the newly proposed project location ... The Peoria Tribe has no objection at this time to the proposed project." See Appendix D, D44 for a copy of the communication.

In a letter dated December 16, 2021, the SHPO staff responded to the early coordination letter stating they did not know of any other parties that should be invited to participate in the Section 106 consultation process. In that same letter, the SHPO staff asked that property owners be invited as soon as possible if ROW is planned to be taken from adjacent historic properties. See Appendix D, D45 to D46 for a copy of the communication.

In a letter dated December 22, 2021, the Pokagon Band of Potawatomi Indians responded to the early coordination letter stating that, "... all the proposed and alternative project site locations will have No Adverse Effect on any historic, religious, or culturally significant resources to the Pokagon Band of Potawatomi Indians." See Appendix D, D47 for a copy of the communication.

In a letter dated January 3, 2022, the Miami Tribe of Oklahoma responded to the early coordination letter stating that, "[t]he Miami Tribe offers no objection to the above-referenced project at this time, as we are not currently aware of existing documentation directly linking a specific Miami cultural or historic site to the project site." See Appendix D, D48 for a copy of the communication.

In a letter dated January 5, 2022, the Eastern Shawnee Tribe of Oklahoma responded to the early coordination letter stating that, "... the project proposes NO Adverse Effect or endangerment to known sites of interest to the Eastern Shawnee Tribe." See Appendix D, D49 for a copy of the communication.

No additional comments were received after the distribution of the early coordination materials.

Archaeology:

A Phase Ia archaeological reconnaissance survey was conducted by an archaeologist with Cultural Resource Analysts, Inc. who meets the Secretary of the Interior's Professional Qualification Standards. The survey was conducted between November 30 and December 2, 2021 (Appendix D, D73 to D74). The survey area for the proposed project has a total length of approximately 1.1 km (0.7 mi) and encompasses approximately 10.3 ha (25.5 acres) of new, temporary, and existing ROW. The survey area was investigated using methods consisting of systematic screened shovel testing and visual inspection of obviously disturbed areas.

Prior to conducting this survey, an archaeological records review was completed using the Indiana Division of Historic Preservation and Archaeology's State Historic Architectural and Archaeological Research Database (SHAARD). The review indicated that there are no previously recorded sites mapped within the survey area.

As a result of the current survey, two new archaeological sites (12LA736 and 12LA737) were recorded. The newly documented sites are historic artifact scatters that date to the twentieth century. Overall, the portions of these sites that were investigated within the survey area lack the potential to provide important information regarding the history of this region and are recommended not eligible for inclusion in the NRHP. Thus, no further work is recommended for the sites, and archaeological clearance is recommended for the proposed project.

In a letter dated March 30, 2022, the SHPO responded to the Phase Ia Archaeology Report agreeing with the conclusions therein. In addition, the SHPO noted that though the portions of the sites 12LA0736 and 12LA0737 within the survey area lacked sufficient information to determine if the sites were eligible, the portions of the sites outside the survey area need to be avoided by construction activities and those locations clearly marked on plans. If the sites cannot be avoided, the SHPO asked that additional archaeological

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investigations be completed. Please see Appendix D, D58 to D59 for a copy of this communication. The archaeological site locations will be clearly marked on the project plans as "Environmentally Sensitive Area – Do Not Disturb." If avoidance is not feasible, a plan for further archaeological investigations will be submitted to the SHPO for review and comment. This is a firm project commitment.

Historic Properties:

The NRHP, Indiana Register of Historic Sites and Structures (State Register), the Indiana Historic Sites and Structures Inventory (IHSSI), the SHAARD, and the Indiana Historic Buildings, Bridges, and Cemeteries Map (IHBBM) were consulted. No resources currently listed in the NRHP are located within the APE.

The *Indiana Historic Bridge Inventory Volume 2: Listing of Historic and Non-Historic Bridges* (February 2009) by Mead & Hunt was reviewed. No bridges eligible for listing in the NRHP are located within the project area.

A Lochmueller Group qualified historian who meets the Secretary of the Interior's Professional Qualification Standards, performed a site inspection of the project area on December 15, 2021, and documented resources that will be at least 50 years of age at the time of the project letting within the APE. The APE was investigated for the existence of any historic properties, structures, objects, or districts listed in or eligible for listing in the NRHP. The historian walked the APE, taking photographs of all resources meriting a Contributing or higher rating. Non-Contributing resources or those that did not meet the age requirements were noted but not documented other than in general view photographs. No previously surveyed resources were documented and four newly identified aboveground resources with a recommended rating of Contributing or higher were recorded within the APE. No previously surveyed resources were found to be demolished during the field visit. As a result of this field survey, one property is recommended eligible for the NRHP within the APE: Pumping Station (Lochmueller #2, 5721 Kennedy Ave.) (Appendix D, D6).

The Pumping Station at 5721 Kennedy Ave. is a c. 1940 Art Moderne style building. Generally, the building is rectangular with central projections on the façade (west) and east elevation forming a stunted cruciform shape. Features on the building include six glass block windows, multi-light metal windows with a central hopper, smooth limestone square panels around the front entrance, and three horizontal bands of projecting limestone on the front projection. The Pumping Station is eligible for listing in the NRHP under Criterion A for its association with Community Planning and Development and the New Deal as well as under C for its architectural significance (Appendix D, D8).

The proposed undertaking will encroach upon the recommended NRHP boundary for the Pumping Station. Adjacent to the north side of the property, the ramp leading to US 20 will be reconstructed, and as a result grading will take place within the northern section of the recommended NRHP boundary. This work will take place between the existing road and fence surrounding the property. Temporary ROW will be required for this grading. The fence will remain in place during the undertaking. At the northwest corner of the recommended NRHP boundary, below-ground storm sewer work will require both temporary and permanent ROW from the historic property. In total, approximately 0.011 acre of permanent ROW and 0.02 acre of temporary ROW will be acquired from the historic property.

A Historic Property Report (HPR), based on the results of the December 15, 2021, aboveground field survey, was completed, which recommended the Pumping Station (Lochmueller #2) as eligible for listing in the NRHP (Appendix D, D70 to D72). The HPR and Phase Ia Archaeology Report were uploaded to IN SCOPE, and an email was sent to consulting parties notifying them of the availability of the reports online on February 28, 2022. Hard copies of these materials were also mailed to the SHPO on that same day.

In an email dated February 28, 2022, the Lake County Historian responded to the HPR stating, "I have reviewed the documentation sent and feel confident that the Section 106 process has been properly fulfilled and that this project can continue." See Appendix D, D55 for a copy of the correspondence.

In a letter dated March 1, 2022, the Peoria Tribe of Indians of Oklahoma responded to the HPR and Phase Ia Archaeology Report stating that, "[t]he Peoria Tribe has no objection at this time to the proposed project." See Appendix D, D56 for a copy of the correspondence.

In a letter dated March 7, 2022, the Eastern Shawnee Tribe of Oklahoma responded to the HPR and Phase Ia Archaeology Report stating that, "... the project proposes NO Adverse Effect or endangerment to known sites of interest to the Eastern Shawnee Tribe." See Appendix D, D57 for a copy of the correspondence.

In a letter dated March 30, 2022, the SHPO responded to the HPR concurring with most of the conclusions in the report including the eligibility of the Pumping Station (Lochmueller #2) under Criterion C. The SHPO subsequently recommended that the Pumping Station is eligible for the NRHP under Criterion A for its association with Community Planning and Development and the New Deal. In addition, the SHPO responded to the Phase Ia Archaeology Report agreeing with the conclusions therein. In addition, the SHPO

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noted that though the portions of the sites 12LA0736 and 12LA0737 within the survey area lacked sufficient information to determine if the sites were eligible, the portions of the sites outside the survey area need to be avoided by construction activities and those locations clearly marked on plans. If the sites cannot be avoided, the SHPO asked that additional archaeological investigations be completed. Please see Appendix D, D58 to D59 for a copy of this communication. The archaeological site locations will be clearly marked on the project plans as "Environmentally Sensitive Area – Do Not Disturb." If avoidance is not feasible, a plan for further archaeological investigations will be submitted to the SHPO for review and comment. This is a firm project commitment.

Documentation Findings:

This undertaking will convert property from the Pumping Station (Lochmueller #2), a Section 4(f) historic property, to a transportation use; INDOT, acting on FHWA's behalf has determined the appropriate Section 106 finding is "No Adverse Effect"; therefore, FHWA hereby intends to issue a "de minimis" finding for the Pumping Station (Lochmueller #2), pursuant to SAFETEA-LU, thereby satisfying FHWA's responsibilities under Section 4(f) for this historic property (Appendix D, D3 to D4).

A Note to File for the 800.11/Effect Finding documentation was approved by INDOT CRO on January 29, 2024 due to small changes to construction limits and temporary ROW (Appendix D, D83 to D84).

Public Involvement:

To meet the public involvement requirements of Section 106, a legal notice of FHWA's finding of No Adverse Effect was published in *The Times of Northwest Indiana* on November 20, 2023 offering the public an opportunity to submit comment pursuant to 36 CFR 800.2(d), 800.3(e), and 800.6(a)(4). The public comment period closed 30 days later on December 20, 2023. The text of the public notice and the affidavit of publication appear in Appendix D, D80 to D82. No comments were received.

This completes the Section 106 process and the responsibilities of the FHWA under Section 106 have been fulfilled

SECTION E – SECTION 4(f) RESOURCES/ SECTION 6(f) RESOURCES

	<u>Presence</u>	<u>Use</u>	
		Yes	No
Parks and Other Recreational Land			
Publicly owned park	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Publicly owned recreation area	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Other (school, state/national forest, bikeway, etc.)	X	<input type="checkbox"/>	X
Wildlife and Waterfowl Refuges			
National Wildlife Refuge	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
National Natural Landmark	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
State Wildlife Area	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
State Nature Preserve	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Historic Properties			
Site eligible and/or listed on the NRHP	X	X	<input type="checkbox"/>
<u>Evaluations</u>			
<u>Prepared</u>			
Programmatic Section 4(f)	<input type="checkbox"/>		
"De minimis" Impact	X		
Individual Section 4(f)	<input type="checkbox"/>		
Any exception included in 23 CFR 774.13	<input type="checkbox"/>		

Discuss Programmatic Section 4(f) and "de minimis" Section 4(f) impacts in the discussion below. Individual Section 4(f) documentation must be included in the appendix and summarized below. Discuss proposed alternatives that satisfy the requirements of Section 4(f). FHWA has identified various exceptions to the requirement for Section 4(f) approval. Refer to 23 CFR § 774.13 - Exceptions.

Section 4(f) of the U.S. Department of Transportation Act of 1966 prohibits the use of certain public and historic lands for federally funded transportation facilities unless there is no feasible and prudent alternative. The law applies to significant publicly owned parks, recreation areas, wildlife / waterfowl refuges, and NRHP eligible or listed historic properties regardless of ownership. Lands subject to this law are considered Section 4(f) resources.

Based on a desktop review, the aerial map of the project area (Appendix B, B3), and the RFI report (Appendix E, E1 to E15), there are thirteen potential 4(f) resources located within the 0.5-mile search radius, including two recreational facilities, five trails, and six

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managed lands. According to additional research, the Section 106 documentation, and by the site visit on October 19, 2021 by Lochmueller Group, there are three Section 4(f) resources within the project area (Appendix D, D3 to D4). There are two potential trails within the project area.

The Pumping Station at 5721 Kennedy Ave. is a Section 4(f) resource present within the project area. The Pumping Station is considered a Section 4(f) resource because it is a historic building that is eligible for listing in the NRHP. The project will convert property from the Pumping Station to a transportation use; INDOT, acting on FHWA's behalf has determined the appropriate Section 106 finding is "No Adverse Effect"; therefore, FHWA hereby intends to issue a "*de minimis*" finding for the Pumping Station, pursuant to SAFETEA-LU, thereby satisfying FHWA's responsibilities under Section 4(f) for this historic property (Appendix D, D3 to D4).

Two potential trails are located within the project area, the Grand Calumet River Trail owned by East Chicago Parks and Recreation Department and the Twin Rivers Trail owned by Hammond Parks and Recreation Department. Both trails are planned and documented in the Statewide Transportation Improvement Plan (STIP) and are therefore considered Section 4(f) resources. No physical trail exists within the project area for either trail segment and no public easements have been set aside for their construction. Both planned trail segments follow, in part, the proposed footprint of the Marquette Greenway Trail, a portion of which will be constructed as part of the current project. Consequently, impacts to these planned trails will occur due to construction of the segment of the Marquette Greenway Trail. A Section 4(f) use would not occur to a trail occupying the right-of-way of a highway if the adjustments to the highway or the trail would not substantially impair the continuity of the trail. Because Des. No. 1900009 will construct part of the trail and will not restrict future trails from being constructed in the future, a Section 4(f) use of either planned trail will not occur. Should any part of these trails be constructed before the completion of the Des. 1900009, the work would not be considered a Section 4(f) use of the trails since the project would serve to improve the connectivity of the other trails.

Section 6(f) Involvement

Presence

Use

Yes

No

Section 6(f) Property

☐
☐
☐

Discuss Section 6(f) resources present or not present. Discuss if any conversion would occur as a result of this project. If conversion will occur, discuss the conversion approval.

The U.S. Land and Water Conservation Fund Act of 1965 established the Land and Water Conservation Fund (LWCF), which was created to preserve, develop, and assure accessibility to outdoor recreation resources. Section 6(f) of this Act prohibits conversion of lands purchased with LWCF monies to a non-recreation use.

A review of 6(f) properties on the INDOT ESD website revealed a total of 57 properties in Lake County (Appendix I, I1). None of these properties are located within or adjacent to the project area. Therefore, there will be no impact to 6(f) resources.

SECTION F – Air Quality

STIP/TIP and Conformity Status of the Project

Is the project in the most current STIP/TIP?

Is the project located in an MPO Area?

Is the project in an air quality non-attainment or maintenance area?

If Yes, then:

Is the project in the most current MPO TIP?

Is the project exempt from conformity?

If No, then:

Is the project in the Transportation Plan (TP)?

Is a hot spot analysis required (CO/PM)?

Yes

No

☒

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Location in STIP:

By Reference to FY 2022-2026 NIRPC TIP

Name of MPO (if applicable):

NIRPC

Location in TIP (if applicable):

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Level of MSAT Analysis required?

Level 1a

☒

Level 1b

☐

Level 2

☐

Level 3

☐

Level 4

☐

Level 5

☐

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Describe if the project is listed in the STIP and if it is in a TIP. Describe the attainment status of the county(ies) where the project is located. Indicate whether the project is exempt from a conformity determination. If the project is not exempt, include information about the TP and TIP. Describe if a hot spot analysis is required and the MSAT Level.

STIP/TIP

This project is included in the Fiscal Year (FY) 2022-2026 NIRPC Transportation Improvement Program (TIP), which was directly incorporated by reference into the INDOT FY 2024-2028 STIP (Appendix H, H1 to H5). The funding currently programmed in the approved TIP/STIP does not include funds for the ROW phase. Funding is expected to be updated in a future TIP amendment. The CE-3 document cannot be approved until funds for the ROW phase are included in the TIP and STIP.

Attainment Status

This project is located in Lake County, which is currently a maintenance area for Ozone, under the 1997 Ozone 8-hour standard which was revoked in 2015 but is being evaluated for conformity due to the February 16, 2018, South Coast Air Quality Management District V. Environmental Protection Agency, Et. Al. Decision. This project has been identified as being exempt from air quality analysis in accordance with 40 CFR Part 93.126 and this project is not a project of air quality concern (40 CFR Part 93.123). Therefore, the project will have no significant impact on air quality.

MSAT

This project is of a type qualifying as a categorical exclusion (Group 1) under 23 CFR 771.117(c), or exempt under the Clean Air Act conformity rule under 40 CFR 93.126, and as such, a Mobile Source Air Toxics analysis is not required.

SECTION G - NOISE**Noise****Yes****No**Is a noise analysis required in accordance with FHWA regulations and INDOT's traffic noise policy? ☐ ☒Date Noise Analysis was approved/technically sufficient by INDOT ESD: N/A

Describe if the project is a Type I or Type III project. If it is a Type I project, describe the studies completed to date and if noise impacts were identified. If noise impacts were identified, describe if abatement is feasible and reasonable and include a statement of likelihood.

This project is a Type III project. In accordance with 23 CFR 772 and the current INDOT Traffic Noise Analysis Procedure, this action does not require a formal noise analysis

SECTION H – COMMUNITY IMPACTS**Regional, Community & Neighborhood Factors**

Will the proposed action comply with the local/regional development patterns for the area?

Yes**No**☒☐

Will the proposed action result in substantial impacts to community cohesion?

☐☒

Will the proposed action result in substantial impacts to local tax base or property values?

☐☒

Will construction activities impact community events (festivals, fairs, etc.)?

☐☒

Does the community have an approved transition plan?

☒☐

If No, are steps being made to advance the community's transition plan?

☐☐

Does the project comply with the transition plan? (explain in the discussion below)

☒☐

Discuss how the project complies with the area's local/regional development patterns; whether the project will impact community cohesion; and impact community events. Discuss how the project conforms with the ADA Transition Plan.

The project complies with local and regional development patterns and has the potential to increase community cohesion and community events. The bridge replacement will maintain sufficient travel conditions and access between Kennedy Ave. and US 20. The addition of the trail will allow more modes of travel in the area and greater intra- and intercommunity connection. ROW acquisition along the property frontage will not substantially diminish the affected properties' existing use.

According to the Indiana Festivals website (<https://indianafestivals.org/>), accessed on December 14, 2023 by Lochmueller Group, no festivals or other major events occur in the vicinity of the project during the time of construction.

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The project complies with the U.S. Access Board Public Right-of-Way Accessibility Guidelines (PROWAG) and the Lake County Americans with Disabilities Act (ADA) Transition Plan. Though the temporary sidewalk closure along Kennedy Ave. may provide temporary impacts to pedestrians during project construction, the project will not diminish, and will likely enhance, people with disabilities' ability to participate or enjoy community programs, services, or activities by constructing a portion of the ADA-compliant, multi-use Marquette Greenway Trail.

Public Facilities and Services

Discuss what public facilities and services are present in the project area and impacts (such as MOT) that will occur to them. Include how the impacts have been minimized and what coordination has occurred. Some examples of public facilities and services include health facilities, educational facilities, public and private utilities, emergency services, religious institutions, airports, transportation or public pedestrian and bicycle facilities.

Based on a desktop review, the aerial map of the project area (Appendix B, B3), the RFI report (Appendix E, E1 to E15), and the Section 106 documentation (Appendix D, D1 to D80), there is one public use airport located within 3.8 miles of the project. There are two recreational facilities located within 0.5 mile of the project. There are five trail segments located within 0.5 mile of the project. There are six managed lands located within 0.5 mile of the project. One existing public utility facility, the Pumping Station at 5721 Kennedy Ave., is located within the project area. Additionally, there are two planned trail segments located within the project area. Those numbers were confirmed by the site visit on October 19, 2021 by Lochmueller Group.

A portion of permanent and temporary ROW will be acquired from the Pumping Station property. The project was found to have No Adverse Effect on the Pumping Station. See *Section D – Cultural Resources* and *Section E – Section 4(f) Resources/ Section 6(f) Resources* above for more information.

The planned trail segments identified by the RFI report include the Grand Calumet River Trail, owned by East Chicago Parks and Recreation Department, and Twin Rivers Trail, owned by Hammond Parks and Recreation Department. Both planned trail segments follow, in part, the proposed footprint of the Marquette Greenway Trail, a portion of which will be constructed as part of the current project. Consequently, impacts to these planned trails will occur due to construction of the segment of the Marquette Greenway Trail.

Access to all properties will be maintained during construction.

Early coordination letters were sent to INDOT Office of Aviation, Hammond Parks and Recreation Department, and East Chicago Parks and Recreation Department on September 8, 2022. No responses were received.

It is the responsibility of the project sponsor to notify school corporations and emergency services at least two weeks prior to any construction that would block or limit access.

Environmental Justice (EJ) (Presidential EO 12898)

During the development of the project were EJ issues identified?

Does the project require an EJ analysis?

If YES, then:

Are any EJ populations located within the project area?

Will the project result in adversely high and disproportionate impacts to EJ populations?

Yes	No
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input checked="" type="checkbox"/>	<input type="checkbox"/>
<input checked="" type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>

Indicate if EJ issues were identified during project development. If an EJ analysis was not required, discuss why. If an EJ analysis was required, describe how the EJ population was identified. Include if the project has a disproportionately high or adverse effect on EJ populations and explain your reasoning. If yes, describe actions to avoid, minimize and mitigate these effects.

Under FHWA Order 6640.23A, FHWA and the project sponsor, as a recipient of funding from FHWA, are responsible to ensure that their programs, policies, and activities do not have a disproportionately high and adverse effect on minority or low-income populations. Per the current INDOT CE Manual, an EJ analysis is required for any project that has two or more relocations or 0.5 acre of additional permanent right-of-way. The project will require 1.13 acres of new permanent ROW and no relocations. Therefore, an EJ Analysis is required.

Potential EJ impacts are detected by locating minority and low-income populations relative to a reference population to determine if populations of EJ concern exists and whether there could be disproportionately high and adverse impacts to them. The reference population may be a county, city, or town and is called the community of comparison (COC). In this project, the COC is North Township in Lake County, IN. The community that overlaps the project area is called the affected community (AC). In this project there are three ACs, Census Tract 209, Census Tract 210, and Census Tract 306. An AC has a population of concern for EJ if the population is more than 50% minority or low-income or if the low-income or minority population is 125% of the COC. Data from the 2021 American Community Survey 5-Year Estimate Detailed Tables were obtained from the U.S. Census Bureau's webpage

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(<https://data.census.gov/cedsci/>) on November 20, 2023, by Lochmueller Group (Appendix I, I29 to I39). The data collected for minority and low-income populations within the ACs is summarized in the below table.

Low-Income and Minority 5-Year Census Data. 2021.

	COC	AC1	AC2	AC3
	North Township, Lake County, IN	Census Tract 209, Lake County, IN	Census Tract 210, Lake County, IN	Census Tract 306, Lake County, IN
LOW-INCOME POPULATION				
Total Population for Whom Poverty Status is Determined	154,348	3,545	6,481	4,177
Total Population Below Poverty Level	28,068	833	1,262	925
Percent Low-Income	18.2%	23.5%	19.5%	22.1%
125 Percent of COC	22.7%			
AC Percent Low-Income Greater Than 125 Percent of COC?		YES	NO	NO
AC Percent Low-Income Greater Than 50 Percent?		NO	NO	NO
Population of EJ Concern?		YES	NO	NO
MINORITY POPULATION				
Total Population	155,976	4,140	6,481	4,231
Minority Population	90,360	2,479	3,368	3,592
Percent Minority	57.9%	59.9%	52.0%	84.9%
125 Percent of COC	72.4%			
AC Percent Minority Greater Than 125 Percent of COC?		NO	NO	YES
AC Percent Minority Greater Than 50 Percent?		YES	YES	YES
Population of EJ Concern?		YES	YES	YES

AC1, Census Tract 209, has a percent low-income of 23.5%, which is below 50% and is above the 125% COC threshold of 22.7%. Therefore, AC1 contains a low-income population of EJ concern. AC2, Census Tract 210, has a percent low-income of 19.5%, which is below 50% and is below the 125% COC threshold of 22.7%. Therefore, AC2 does not contain low-income populations of EJ concern. AC3, Census Tract 306, has a percent low-income of 22.1%, which is below 50% and is below the 125% COC threshold of 22.7%. Therefore, AC3 does not contain low-income populations of EJ concern.

AC1, Census Tract 209, has a percent minority of 59.9%, which is above 50% and is below the 125% COC threshold of 72.4%. Therefore, AC1 contains a minority population of EJ concern. AC2, Census Tract 210, has a percent minority of 52.0%, which is above 50% and is below the 125% COC threshold of 72.4%. Therefore, AC2 contains a minority population of EJ concern. AC3, Census Tract 306, has a percent minority of 84.9%, which is above 50% and is above the 125% COC threshold of 72.4%. Therefore, AC3 contains a minority population of EJ concern.

Conclusion

The proposed project is expected to require the acquisition of approximately 1.13 acres of permanent ROW. This ROW will be acquired from the immediate property frontage nearest US 20 and the access roads east of Kennedy Avenue. This ROW will be acquired from within ACs containing populations of EJ concern. Permanent ROW will be acquired from an industrial parcel, a Hammond municipally-owned parcel used for public utility distribution, and parcels used for private utility distribution. Permanent ROW will not be acquired from residential parcels. Moreover, the small amounts of ROW being acquired from these parcels' roadway frontages will not affect their use or function. Land use within the proposed permanent ROW consists of maintained open fields, wetlands, an industrial equipment storage area, and a lawn area surrounding a municipal building.

The negative impacts to property owners within the project area will be minimal and consist primarily of short-term construction impacts and the loss of portions of ROW along US 20. No relocations are anticipated. The ROW to be acquired will not substantially diminish the remaining properties' existing use or function. The MOT plan, which is still being finalized, will involve a closure and detour for US 20, lane closures for Kennedy Ave., and potential intermittent closures of Kennedy Ave. See the *Maintenance of Traffic (MOT) During Construction* section for more information.

Based upon the analysis provided (Appendix I, I29 to I39), INDOT-ESD would not consider the impacts associated with this project as causing a disproportionately high and adverse effect on minority and low-income populations of EJ concern relative to non-EJ

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populations in accordance with the provisions of Executive Order 12898 and FHWA Order 6640.23a (Appendix I, I40). No further EJ Analysis is required.

Relocation of People, Businesses or Farms

Will the proposed action result in the relocation of people, businesses or farms?

Is a BIS or CSRS required?

Yes	No
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>

Number of relocations: Residences: 0 Businesses: 0 Farms: 0 Other: 0

Discuss any relocations that will occur due to the project. If a BIS or CSRS is required, discuss the results in the discussion below.

No relocations of people, businesses, or farms will take place as a result of this project.

SECTION I – HAZARDOUS MATERIALS & REGULATED SUBSTANCES

Hazardous Materials & Regulated Substances (Mark all that apply)

Red Flag Investigation (RFI)

Phase I Environmental Site Assessment (Phase I ESA)

Phase II Environmental Site Assessment (Phase II ESA)

Design/Specifications for Remediation required?

Documentation

<input checked="" type="checkbox"/>
<input type="checkbox"/>
<input checked="" type="checkbox"/>
<input type="checkbox"/>

Date RFI concurrence by INDOT SAM (if applicable): June 7, 2022

Include a summary of the potential hazardous material concerns found during review. Discuss in depth sites found within, directly adjacent to, or ones that could impact the project area. Refer to current INDOT SAM guidance. If additional documentation (special provisions, pay quantities, etc.) will be needed, include in discussion. Include applicable commitments.

Based on a review of geographic information systems (GIS) and available public records, the RFI was completed on June 7, 2022, by Lochmueller Group and INDOT Site Assessment and Management (SAM) provided their concurrence on June 7, 2022 (Appendix E, E1 to E15). One superfund site is located within 0.5 mile of the project area. Five Resource Conservation and Recovery Act (RCRA)/Generator/Treatment, Storage, and Disposal (TSD) sites are located within 0.5 mile of the project area. Four State Cleanup sites are located within 0.5 mile of the project area. One Underground Storage Tank (UST) site is located within 0.5 mile of the project area. Two Voluntary Remediation Program (VRP) sites are located within 0.5 mile of the project area. One Solid Waste Landfill is located within 0.5 mile of the project area. Five Leaking Underground Storage Tank (LUST) sites are located within 0.5 mile of the project area. Two Brownfields are located within 0.5 mile of the project area. Ten Institutional Controls are located within 0.5 mile of the project area. Ten National Pollution Discharge Elimination System (NPDES) Facilities are located within 0.5 mile of the project area. Seventeen NPDES Pipe Locations are located within 0.5 mile of the project area.

One Superfund site is located 0.19 mile north of the project area and could affect the project area. Two RCRA Generator/TSD sites are located within or adjacent to the project area. One State Cleanup site is located within the project area. One UST site is located within the project area. One VRP site is located within the project area. One LUST site is located adjacent to the project area. Two Institutional Controls are located within or adjacent to the project area. Five NPDES Facilities are located within or adjacent to the project area.

Superfund: One Superfund site, USS Lead Refinery Inc, located at 5300 Kennedy Ave, Agency Interest ID (AID) 20151, although not mapped, is located 0.19 mile north of the project area, on the north bank of Grand Calumet River. Heavy metal concentrations are elevated in the soil and groundwater beneath the site as a result of deposition from air and various waste piles. Due to the history of heavy metal deposition and groundwater contamination near the project area, a Phase II Environmental Site Assessment (ESA) is recommended to occur before the Ready for Contracts (RFC) date. INDOT SAM is currently managing the preparation of this Phase II ESA.

Voluntary Remediation Program: One VRP Site, Buckeye Terminals LLC Hammond, located at 2400 Michigan St., AID 11991, is located within the project area. The site is an active petroleum storage terminal and contains ASTs, truck loading racks, and rail car loading racks. Multiple releases of petroleum products have occurred at the site with resulting remediation actions. According to the IDEM Covenant Not to Sue letter dated June 30, 2020, the requirements for the VRP have been fulfilled and a Certificate of Completion was issued February 20, 2020; however, contamination remains on the property and has migrated off-site. If excavation occurs in this area, it is possible that petroleum contamination will be encountered. Proper handling, removal, and disposal of soil and/or groundwater may be necessary. Refer to Appendix G of the SAM Manual for the recommended procedure to manage and

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report contamination.

If groundwater monitoring wells are encountered in the project area, they should be maintained in place. If they cannot be maintained, then the contractor must contact the INDOT Project Manager who will notify the INDOT Permits Group. The INDOT Permits Group will notify the permit holder that the well must be removed prior to construction. The permit holder is responsible for coordination with IDEM and the INDOT Permit Group for replacement or relocation of the well. If a property owner cannot be found in connection with the monitoring well, then well abandonment will be included in the project contract. All well abandonment activities must be completed by an Indiana Licensed Well Driller in accordance with IAC 312-13-10. Regardless of whether the well is abandoned by the contractor or the property owner, a record of well abandonment, including the well driller's license number, must be provided to the INDOT Project manager once the well has been abandoned.

Brownfield: One Brownfield, Trident Machine Technologies Incorporated, located at 2300 & 2320 Michigan St., AID 25669, is located 0.17 mile southwest of the project area. According to the most recent IDEM site status letter dated December 22, 2021, soil sampling showed mercury, lead, and chromium contamination and groundwater samples showed benzene contamination. Because contamination exists at the site, if excavation occurs in this area, proper handling, removal, and disposal of soil and/or groundwater may be necessary. Refer to Appendix G of the SAM Manual for the recommended procedure to manage and report contamination.

Institutional Controls: Two Institutional Control sites are located within or adjacent to the project area.

- Buckeye Terminals LLC Hammond, located at 2400 Michigan St., AID 11991, is located within the project area. The property is located both north of the Indiana Toll Road and south of US 20 within the project area. Two institutional control polygons representing this site extend into the project area. An Environmental Restrictive Covenant (ERC) was instituted for this site on December 5, 2019. The ERC dictates the following: the site shall not be used for residential purposes, extraction of groundwater, or agriculture; soil disturbed as a result of excavation and construction activities shall be restored; written evidence including sampling data showing the excavated and restored area affected by the excavation shall be provided; disposal of soils must be done in accordance with all applicable federal and state laws; the Gundle Barrier (located approximately 0.33 mile northwest of the project area) shall not be removed; finally IDEM shall be notified in writing at least 15 days in advance of conducting any construction or excavation work that may impact an engineered control. Volatile organic compounds (VOCs) exist in the soil at this site. If excavation occurs in this area, it is possible that petroleum contamination will be encountered. Proper handling, removal, and disposal of soil and/or groundwater may be necessary. Coordination will be conducted with the IDEM Institutional Controls section (institutionalcontrols@idem.IN.gov) before RFC. Refer to Appendix G of the SAM Manual for the recommended procedure to manage and report contamination.
- Dana Transport, located at 5723 Kennedy Ave, AID 17690, is located 0.01 mile south of the project area. Contaminants of Concern (COC) exist in the soil and groundwater at the site including benzene, toluene, ethylbenzene, and xylenes. The ERC forbids the site from being used as residential property, extraction of groundwater, and agriculture. It forbids use of the property for occupancy of a dwelling or workspace unless vapor mitigation is performed. Finally, it dictates that soil disturbed as a result of excavation and construction activities shall be restored in a manner that the remaining contaminant concentrations do not present a threat to human health or the environment, and that excavation and disposal of contaminated soils must be managed in accordance with all applicable federal and state laws. The ERC only applies to the parcel immediately adjacent to Kennedy Ave. (Parcel 201-003) and does not apply to the scrap/storage yard to the east that extends into the project area. If excavation occurs in this area, it is possible that petroleum contamination will be encountered. Proper handling, removal, and disposal of soil and/or groundwater may be necessary. Coordination will be conducted with the IDEM Institutional Controls section (institutionalcontrols@idem.IN.gov) before RFC. Refer to Appendix G of the SAM Manual for the recommended procedure to manage and report contamination.

NPDES Facility: One NPDES Facility, Buckeye Terminals Hammond Terminal ING340066, located at 2400 Michigan St, AID 11991, is located within the project area, and the three outfall pipes associated with the site are located 0.15 mile north of the project area. The master NPDES general permit was renewed on November 1, 2020, effective May 1, 2021, and does not specify an end date. Coordination with the facility and/or permit owner should occur.

Part IV – Permits and Commitments

PERMITS CHECKLIST

Permits (mark all that apply)

Likely Required

Army Corps of Engineers (404/Section10 Permit)
Nationwide Permit (NWP)

☒

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Permits (mark all that apply)

Likely Required

Regional General Permit (RGP)
Individual Permit (IP)
Other

**IN Department of Environmental Management
(401/Rule 5)**

Nationwide Permit (NWP)
Regional General Permit (RGP)
Individual Permit (IP)
Isolated Wetlands
CSGP (Rule 5)
Other

X
X

IN Department of Natural Resources

Construction in a Floodway
Navigable Waterway Permit
Other

Mitigation Required

US Coast Guard Section 9 Bridge Permit

Others (Please discuss in the discussion below)

X

List the permits likely required for the project and summarize why the permits are needed, including permits designated as "Other."

Due to the 0.4124 acre of permanent impacts to wetlands, a USACE Section 404 RGP and an IDEM Section 401 IP will likely be required. Because the total impact to wetlands is greater than 0.1 acre, mitigation will likely be required and will be determined during permitting.

Because ground disturbance will exceed one acre, an IDEM Construction Stormwater General Permit (CSGP) will likely be required.

Applicable recommendations provided by resource agencies are included in the *Environmental Commitments* section of this document. If permits are found to be necessary, the conditions of the permit will be requirements of the project and will supersede these recommendations.

It is the responsibility of the project sponsor to identify and obtain all required permits.

ENVIRONMENTAL COMMITMENTS

List all commitments and include the name of agency/organization requesting/requiring the commitment(s). Listed commitments should be numbered.

Firm:

- 1) If the scope of work or permanent or temporary right-of-way amounts change, the INDOT Environmental Services Division (ESD) and the INDOT District Environmental Section will be contacted immediately. (INDOT ESD and INDOT LaPorte District)
- 2) It is the responsibility of the project sponsor to notify school corporations and emergency services at least two weeks prior to any construction that would block or limit access. (INDOT ESD)
- 3) Any work in a wetland area within right-of-way or in borrow/waste areas is prohibited unless specifically allowed in the U.S. Army Corps of Engineers permit. (INDOT EWPO)
- 4) Wetlands A, D, and E will be identified on the project plans and labeled "Do Not Disturb." (INDOT ESD)
- 5) USFWS Bridge/Structure Assessment are only valid for two years. If construction will begin after October 26, 2025, an inspection of the structures by a qualified individual must be performed. Inspection of the structures should check for presence of bats/bat indicators and/or presence of birds. The results of the inspection must indicate no signs of bats or birds. If signs of bats or birds are documented during this inspection, the INDOT District Environmental Manager must be contacted immediately. (INDOT ESD)
- 6) General AMM 1: Ensure all operators, employees, and contractors working in areas of known or presumed bat habitat are aware of all FHWA/FRA/FTA (Transportation Agencies) environmental commitments, including all applicable AMMs. (USFWS)

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- 7) Lighting AMM 1: Direct temporary lighting away from suitable habitat during the active season. (USFWS)
- 8) Lighting AMM 2: When installing new or replacing existing permanent lights, use downward-facing, full cut-off lens lights (with same intensity or less for replacement lighting); or for those transportation agencies using the BUG system developed by the Illuminating Engineering Society, be as close to 0 for all three ratings with a priority of "uplight" of 0 and "backlight" as low as practicable. (USFWS)
- 9) Tree Removal AMM 1: Modify all phases/aspects of the project (e.g., temporary work areas, alignments) to avoid tree removal. (USFWS)
- 10) Tree Removal AMM 2: Apply time of year restrictions for tree removal when bats are not likely to be present, or limit tree removal to 10 or fewer trees per project at any time of year within 100 feet of existing road/rail surface and outside of documented roosting/foraging habitat or travel corridors; visual emergence survey must be conducted with no bats observed. (USFWS & IDNR DFW)
- 11) Tree Removal AMM 3: Ensure tree removal is limited to that specified in project plans and ensure that contractors understand clearing limits and how they are marked in the field (e.g., install bright colored flagging/fencing prior to any tree clearing to ensure contractors stay within clearing limits). (USFWS)
- 12) Tree Removal AMM 4: Do not remove documented Indiana bat or NLEB roosts that are still suitable for roosting, or trees within 0.25 miles of roosts, or documented foraging habitat any time of year. (USFWS)
- 13) The archaeological site locations will be clearly marked on the project plans as "Environmentally Sensitive Area – Do Not Disturb." If avoidance is not feasible, a plan for further archaeological investigations will be submitted to the SHPO for review and comment (IDNR SHPO).
- 14) One (1) Superfund site, USS Lead Refinery Inc, located at 5300 Kennedy Ave, AID 20151, although not mapped, is located 0.19 mile north of the project area, on the north bank of Grand Calumet River. Heavy metal concentrations are elevated in the soil and groundwater beneath the site as a result of deposition from air and various waste piles. Due to the history of heavy metal deposition and groundwater contamination nearby the project area, a Phase II Environmental Site Assessment is recommended to occur before RFC. Prior to any investigation activities, an SOW plan will be prepared and submitted to INDOT SAM for review and approval. (INDOT SAM)
- 15) One (1) VRP Site, Buckeye Terminals LLC Hammond, located at 2400 Michigan St., AID 11991, is located within the project area. The site is an active petroleum storage terminal and contains ASTs, truck loading racks, and rail car loading racks. Multiple releases of petroleum products have occurred at the site with resulting remediation actions. According to the IDEM Covenant Not to Sue letter dated June 30, 2020, the requirements for the VRP have been fulfilled and a Certificate of Completion was issued February 20, 2020; however, contamination remains on the property and has migrated off-site. If excavation occurs in this area, it is possible that petroleum contamination will be encountered. Proper handling, removal, and disposal of soil and/or groundwater may be necessary. Refer to Appendix G of the SAM Manual for the recommended procedure to manage and report contamination. (INDOT SAM)
- 16) If groundwater monitoring wells are encountered in the project area, they should be maintained in place. If they cannot be maintained, then the contractor must contact the INDOT Project Manager who will notify the INDOT Permits Group. The INDOT Permits Group will notify the permit holder that the well must be removed prior to construction. The permit holder is responsible for coordination with IDEM and the INDOT Permit Group for replacement or relocation of the well. If a property owner cannot be found in connection with the monitoring well, then well abandonment will be included in the project contract. All well abandonment activities must be completed by an Indiana Licensed Well Driller in accordance with IAC 312-13-10. Regardless of whether the well is abandoned by the contractor or the property owner, a record of well abandonment, including the well driller's license number, must be provided to the INDOT Project manager once the well has been abandoned. (INDOT SAM)
- 17) One (1) Brownfield, Trident Machine Technologies Incorporated, located at 2300 & 2320 Michigan St., AID 25669, is located 0.17 mile southwest of the project area. According to the most recent IDEM site status letter dated December 22, 2021, soil sampling showed mercury, lead, and chromium contamination and groundwater samples showed benzene contamination. Because contamination exists at the site, if excavation occurs in this area, proper handling, removal, and disposal of soil and/or groundwater may be necessary. Refer to Appendix G of the SAM Manual for the recommended procedure to manage and report contamination. (INDOT SAM)
- 18) Institutional Control Site: Buckeye Terminals LLC Hammond, located at 2400 Michigan St., AID 11991, is located within the project area. The property is located both north of the Indiana Toll Road and south of US 20 within the project area. Two institutional control polygons representing this site extend into the project area. An ERC was instituted for this site on December 5, 2019. The ERC dictates the following: the site shall not be used for residential purposes, extraction of groundwater, or agriculture; soil disturbed as a result of excavation and construction activities shall be restored; written evidence including sampling data showing the excavated and restored area affected by the excavation shall be provided; disposal of soils must be done in accordance with all applicable federal and state laws; the Gundie Barrier (located approximately 0.33 mile northwest of the project area) shall not be removed; finally IDEM shall be notified in writing at least 15 days in advance of conducting any construction or excavation work that may impact an engineered control. Volatile organic compounds (VOCs) exist in the soil at this site. If excavation occurs in this area, it is possible that petroleum contamination will be encountered. Proper handling, removal, and disposal of soil and/or groundwater may be necessary.

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Coordination will be conducted with the IDEM Institutional Controls section () before RFC. Refer to Appendix G of the SAM Manual for the recommended procedure to manage and report contamination. (INDOT SAM)

- 19) Institutional Control Site: Dana Transport, located at 5723 Kennedy Ave, AID 17690, is located 0.01 mile south of the project area. COCs exist in the soil and groundwater at the site including benzene, toluene, ethylbenzene, and xylenes. The ERC forbids the site from being used as residential property, extraction of groundwater, and agriculture. It forbids use of the property for occupancy of a dwelling or workspace unless vapor mitigation is performed. Finally, it dictates that soil disturbed as a result of excavation and construction activities shall be restored in a manner that the remaining contaminant concentrations do not present a threat to human health or the environment, and that excavation and disposal of contaminated soils must be managed in accordance with all applicable federal and state laws. The ERC only applies the parcel immediately adjacent to Kennedy Ave. (Parcel 201-003) and does not apply to the scrap/storage yard to the east that extends into the project area. If excavation occurs in this area, it is possible that petroleum contamination will be encountered. Proper handling, removal, and disposal of soil and/or groundwater may be necessary. Coordination will be conducted with the IDEM Institutional Controls section (institutionalcontrols@idem.IN.gov) before RFC. Refer to Appendix G of the SAM Manual for the recommended procedure to manage and report contamination. (INDOT SAM)
- 20) One (1) NPDES Facility, Buckeye Terminals Hammond Terminal ING340066, located at 2400 Michigan St, AID 11991, is located within the project area, and the three (3) outfall pipes associated with the site are located 0.15 mile north of the project area. The master NPDES general permit was renewed on November 1, 2020, effective May 1, 2021, and does not specify an end date. Coordination with the facility and/or permit owner should occur. (INDOT SAM)

For Further Consideration:

- 21) The rehabilitated crossing structure, and any bank stabilization under or around the structure, must not create conditions that are less favorable for wildlife passage when compared to existing conditions. (IDNR DFW)
- 22) Upgrading wildlife passage for rehabilitated structures is recommended whenever possible to improve wildlife/vehicle safety. Passage areas must be maintained or restored to allow for wildlife passage where it is currently possible. (IDNR DFW)
- 23) All wildlife passage designs must include a smooth level pathway a minimum of 1-3 feet in width composed of natural substrate (soil, sand, gravel, etc.) or compacted aggregate fill over riprap (#2, #53, #73, etc.) tied into existing elevations. The location of the wildlife pathway is dependent on the wildlife species using the area. (IDNR DFW)
- 24) The Division of Fish & Wildlife recommends avoiding removing trees to the greatest extent possible and replacing trees that must be removed. (IDNR DFW)

Appendix A: INDOT Supporting Documentation

Categorical Exclusion Threshold Chart.....	A1
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Appendix B: Graphics

General Location Map.....	B1
USGS Topographic Map	B2
Aerial Map (2020).....	B3
Photo Location Map	B4
Site Photographs.....	B5-B12
Stage 2 Preliminary Plans.....	B13-B41

Appendix C: Early Coordination

Sample Early Coordination Letter (September 8, 2022)	C1-C4
Indiana Geological and Water Survey Automated Response Letter (September 7, 2022).....	C5-C6
US Department of Agriculture, Natural Resources Conservation Service Response Letter (September 8, 2022)	C7
Indiana American Water Response Email (September 13, 2022)	C8
U.S. Army Corps of Engineers, Chicago District Response Email (September 16, 2022)	C9
Indiana Department of Natural Resources, Division of Fish and Wildlife Response Letter (October 7, 2022)	C10-C12
U.S. Fish and Wildlife Service Official Species List (January 12, 2024)	C13-C26
NLAA Concurrence Verification Letter (October 18, 2023).....	C27-C42
Bridge/Structure Bat Assessment Forms (October 26, 2023).....	C43-C44

Appendix D: Section 106 of the National Historic Preservation Act (NHPA)

Effect Finding/800.11 Document	D1-D11
Appendix A - Maps.....	D12-D16
Appendix B – General Photographs.....	D17-D31
Appendix C – Consulting Parties List.....	D32-D33
Appendix D – Consulting Parties Correspondence.....	D34-D68
Appendix E – Historic Property/Archeology/Effects Report Summaries	D69-D77
DHPA Finding Approval Email (December 12, 2023)	D78-D79
Section 106 Public Notice Text	D80
Section 106 Public Notice Affidavit of Publication	D81-D82
800.11 Note to File Correspondence	D83-D84

Appendix E: Red Flag Investigation and Hazardous Materials

Red Flag Investigation (Approved June 7, 2022)	E1-E15
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Appendix F: Water Resources

Waters of the US Report (Approved August 12, 2022).....	F1-F10
USDA SSURGO Soils Map.....	F11
USFWS NWI Map.....	F12
StreamStats Watershed Map	F13
Best Available Flood Hazard Map.....	F14
Water Resources Map	F15
Preliminary Jurisdictional Determination	F16-F19
IDNR Best Available Flood Hazard Map (From FARA Report)	F20

Appendix G: Public Involvement

Sample Notice of Survey Letter (November 4, 2021)	G1
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Appendix H: Air Quality

Relevant page from the FY 2022-2026 NIRPC TIP	H1
INDOT TIP Incorporation Letter (August 28, 2023)	H2-H3
USDOT STIP Approval Letter (September 1, 2023)	H4-H5

Appendix I: Other Information

Land and Water Conservation Fund Grants: Lake County	I1
Bridge Inspection Report (July 11, 2022)	I2-I14
Addendum No. 1 to Abbreviated Engineer's Report (DRAFT)	I15-I20
Abbreviated Engineer's Report	I21-I28
Environmental Justice Analysis (Approved February 21, 2024)	I29-I39
Approval Email (February 21, 2024)	I40

Categorical Exclusion

Appendix A

INDOT Supporting Documentation

Categorical Exclusion Level Thresholds

	PCE	Level 1	Level 2	Level 3	Level 4 ¹
Section 106	Falls within guidelines of Minor Projects PA	“No Historic Properties Affected”	“No Adverse Effect”	-	“Adverse Effect” Or Historic Bridge involvement ²
Stream Impacts³	No construction in waterways or water bodies	< 300 linear feet of stream impacts	≥ 300 linear feet of stream impacts	-	USACE Individual 404 Permit ⁴
Wetland Impacts³	No adverse impacts to wetlands	< 0.1 acre	-	< 1.0 acre	≥ 1.0 acre
Right-of-way⁵	Property acquisition for preservation only or none	< 0.5 acre	≥ 0.5 acre	-	-
Relocations⁶	None	-	-	< 5	≥ 5
Threatened/Endangered Species (Species Specific Programmatic for Indiana bat & northern long eared bat)*	“No Effect”, “Not likely to Adversely Affect” (With select AMMs ⁷)	“Not likely to Adversely Affect” (With any AMMs or commitments)	-	“Likely to Adversely Affect”	Project does not fall under Species Specific Programmatic ⁸
Threatened/Endangered Species (Any other species)*	Falls within guidelines of USFWS 2013 Interim Policy or “No Effect”	“Not likely to Adversely Affect”	-	-	“Likely to Adversely Affect”
Environmental Justice	No disproportionately high and adverse impacts	-	-	-	Potential ⁹
Sole Source Aquifer	No Detailed Groundwater Assessment	-	-	-	Detailed Groundwater Assessment
Floodplain	No Substantial Impacts	-	-	-	Substantial Impacts
Section 4(f) Impacts	None	-	-	-	Any ¹⁰
Section 6(f) Impacts	None	-	-	-	Any
Permanent Traffic Alteration	None	-	-	-	Any
Noise Analysis Required	No	-	-	-	Yes
Air Quality Analysis Required	No	-	-	-	Yes ¹¹
Approval Level					
<ul style="list-style-type: none"> District Env. (DE) Env. Serv. Div. (ESD) FHWA 	Concurrence by DE or ESD	DE or ESD	DE or ESD	DE and/or ESD	DE and/or ESD; and FHWA

¹ Coordinate with INDOT Environmental Services Division. INDOT will then coordinate with the appropriate FHWA Environmental Specialist.

² Any involvement with a bridge processed under the Historic Bridge Programmatic Agreement.

³ Total permanent impacts to streams (linear feet) and wetlands (acres).

⁴ US Army Corps of Engineers Individual 404 Permit

⁵ Total permanent and temporary right-of-way. This does not include reacquisition of existing apparent right-of-way.

⁶ If any relocations are within an area with a known or suspected Environmental Justice (EJ) or disadvantaged population, or has greater than 5 relocations, a conversation with FHWA, through INDOT ESD, is needed to confirm NEPA classification and outreach plan for the project.

⁷ Avoidance and Mitigation Measures (AMMs) determined by the IPAC determination key to be required that are not tree AMMs, bridge AMMs, or structure AMMs.

⁸ Projects that do not fall under a Species Specific Programmatic and results in a “Likely to Adversely Affect”. Other findings can be processed as a lower-level CE.

⁹ Potential for causing a disproportionately high and adverse impact.

¹⁰ Section 4(f) use resulting in an Individual, Programmatic, or *de minimis* evaluation. The only exception is a *de minimis* evaluation for historic properties (Effective January 2, 2020). If a historic property *de minimis* and no other use, mark the *None* column.

¹¹ Hot Spot Analysis and/or MSAT Quantitative Emission Analysis.

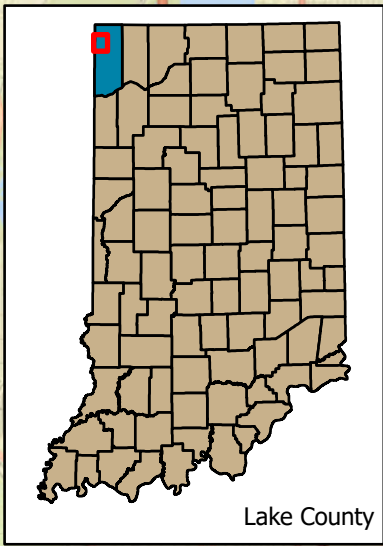
* Includes the threatened/endangered species critical habitat

Note: Substantial public or agency controversy may require a higher-level NEPA document.

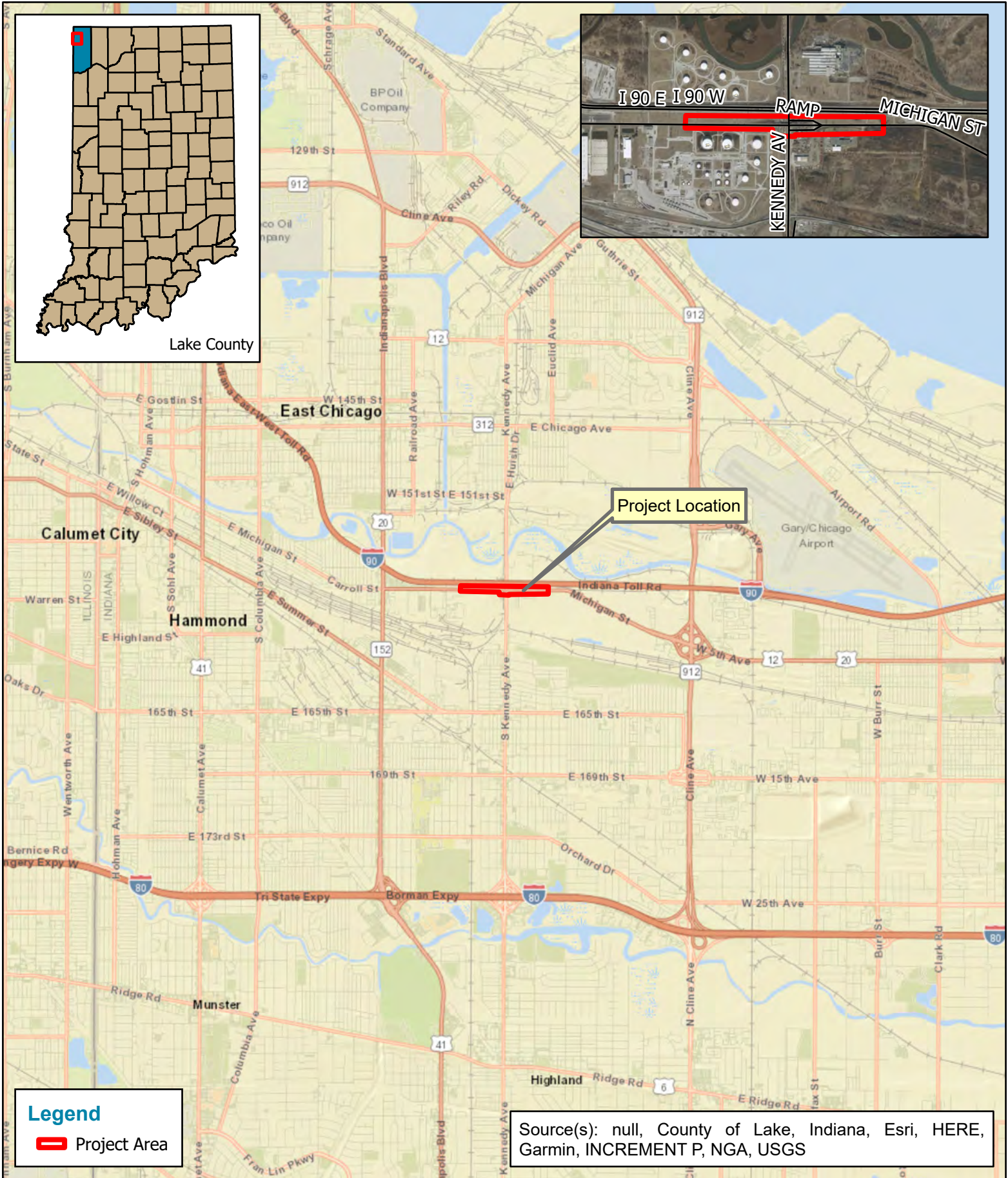
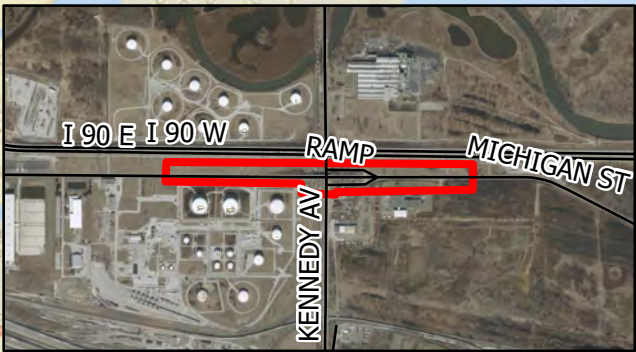
Categorical Exclusion

Appendix B

Graphics



Lake County



Legend

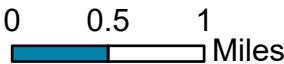
Project Area

Source(s): null, County of Lake, Indiana, Esri, HERE, Garmin, INCREMENT P, NGA, USGS



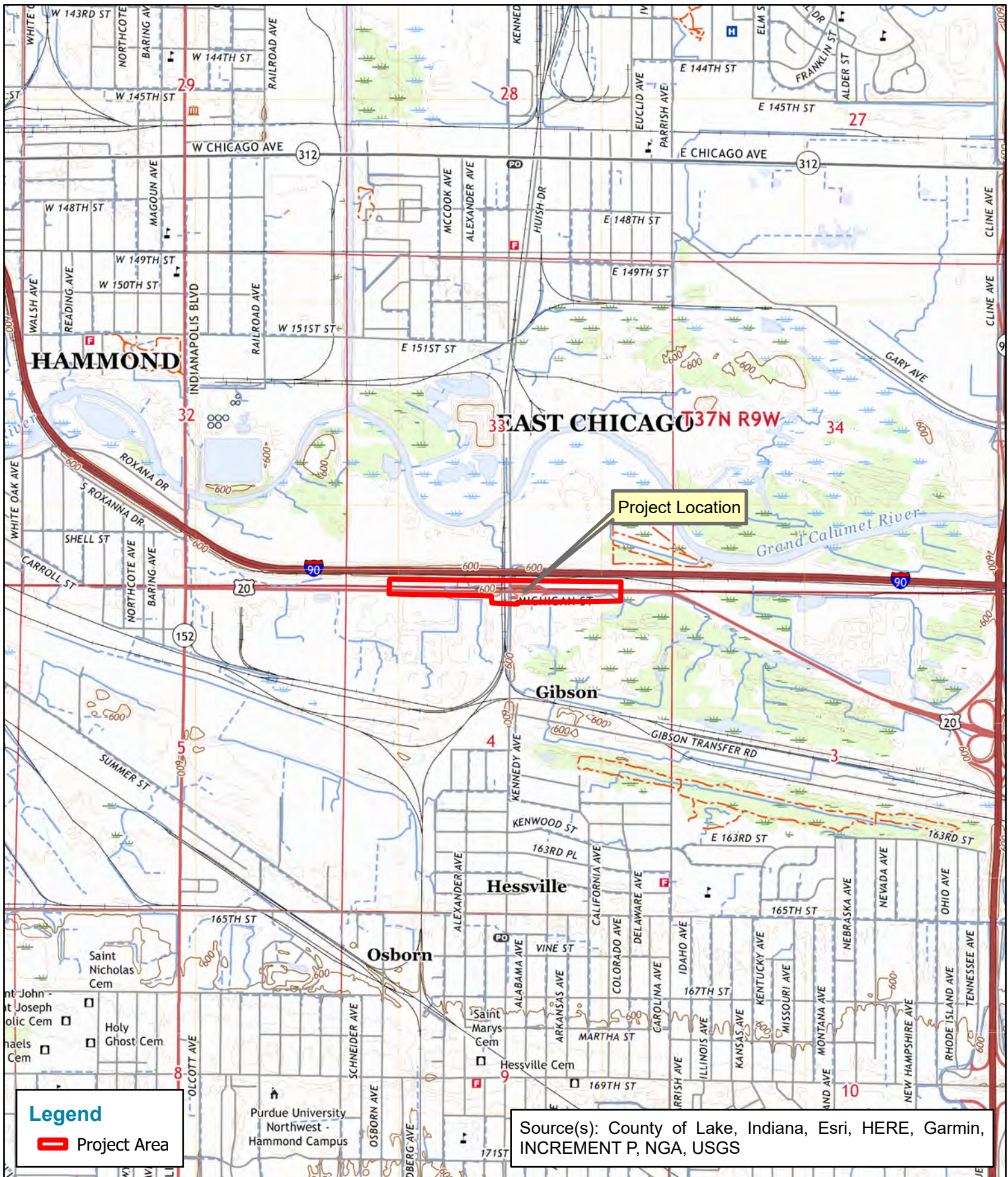
112 West Jefferson Blvd., Suite 500
South Bend, IN 46601
Phone: (574) 334-5464

General Location Map
Des. No. 1900009



County: Lake
Township: North
State: Indiana

US 20 over Kennedy and IHB RR
Bridge Replacement Project
Created: 1/2/2024, CHoogewerf



112 West Jefferson Blvd., Suite 500
South Bend, IN 46601
Phone: (574) 334-5464

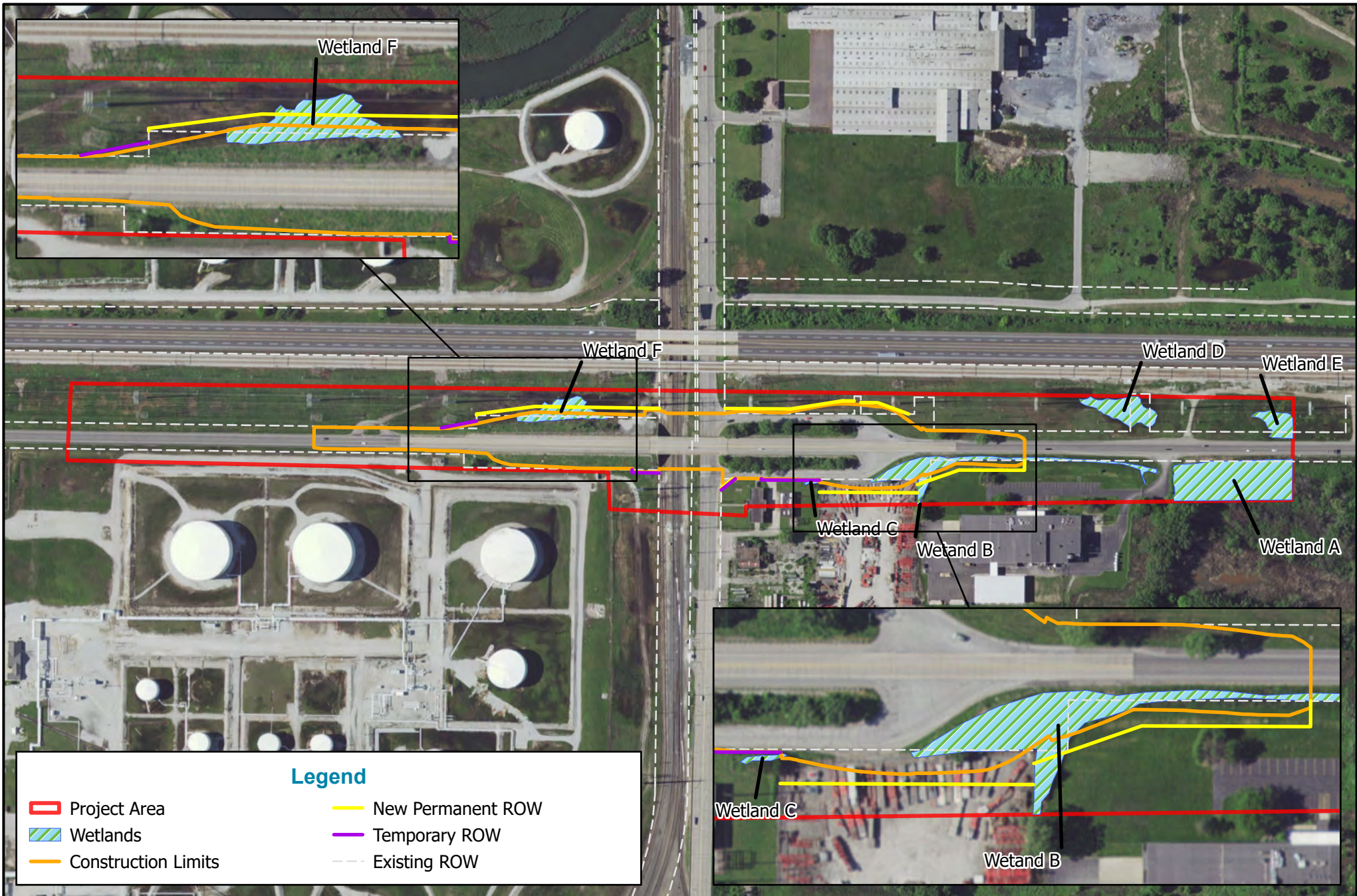
USGS Topographic Map
Highland Quadrangle 1:24,000
Des. No. 1900009

0 1,000 2,000
Feet



County: Lake
Township: North
State: Indiana

US 20 over Kennedy and IHB RR
Bridge Replacement Project
Created: 1/2/2024, CHoogewerf



112 West Jefferson Blvd., Suite 500
South Bend, IN 46601
Phone: (574) 334-5460

Aerial Map (2020)
Des. No. 1900009

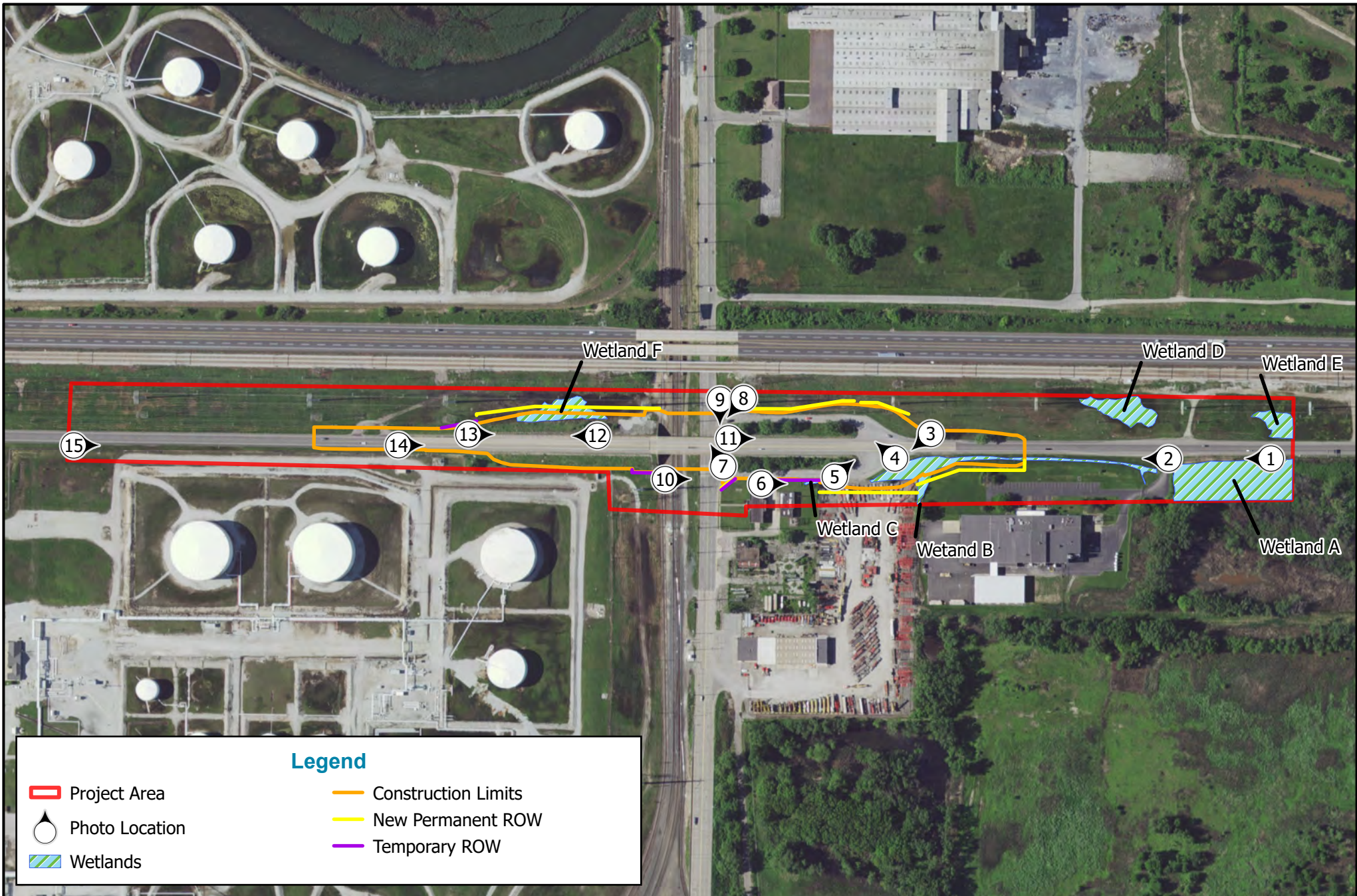
0 225 450
Feet



County: Lake
Township: North
State: Indiana

US 20 over Kennedy and IHB RR
Bridge Replacement Project
Created: 1/18/2024, CHoogewerf

Aerial Source: 2020 Indiana Orthophotography
Map Source(s): Esri, USDA Farm Service
Agency, Microsoft



112 West Jefferson Blvd., Suite 500
South Bend, IN 46601
Phone: (574) 334-5460

Photo Location Map Des. No. 1900009

0 225 450
Feet



County: Lake
Township: North
State: Indiana

US 20 over Kennedy and IHB RR
Bridge Replacement Project
Created: 1/18/2024, CHoogewerf

Aerial Source: 2020 Indiana Orthophotography
Map Source(s): Esri, USDA Farm Service
Agency, Microsoft



1. South side of US 20, looking west from east end of project area



2. North side of US 20, looking west toward bridge



3. North side of US 20, looking southwest toward entry/exit ramps and bridge



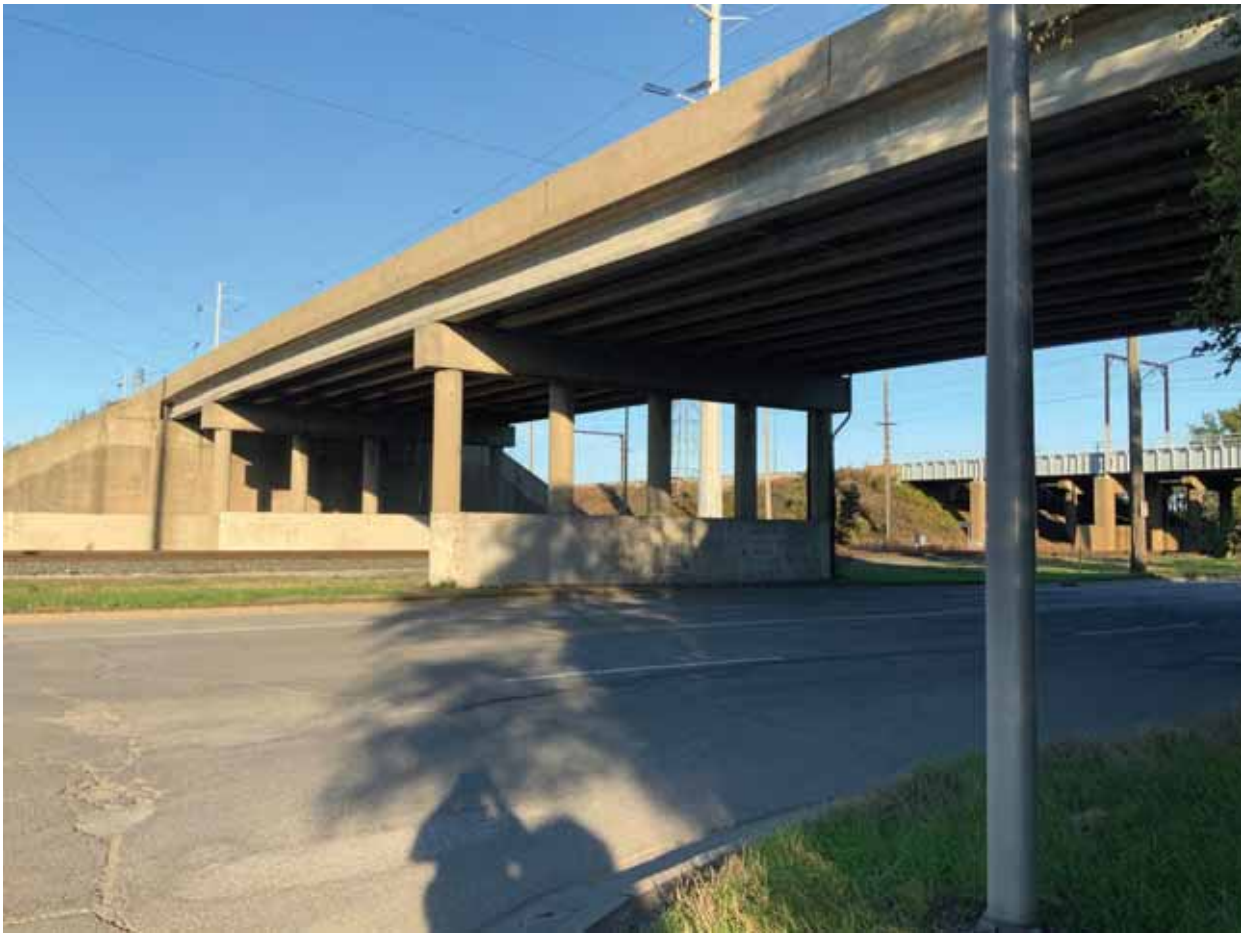
4. South side of US 20, looking northwest across intersection with entry/exit ramps



5. South side of US 20, looking northeast toward the east end of southern access road



6. Looking east along southern entry/exit ramp



7. Looking northwest at underside of bridge, across Kennedy Avenue



8. Looking southwest at bridge from west end of northern entry/exit ramp



9. Looking south along Kennedy Avenue towards bridge



10. West side of Kennedy Avenue, looking east towards south access ramp



11. Looking east along US 20, east of bridge



12. Looking west along US 20, west of bridge



13. Looking east toward bridge from western extent of guard rail



14. South side of US 20, looking east toward bridge



15. South side of US 20 at western edge of project area, looking east toward bridge

PROJECT	DESIGNATION
1900009	1900009
CONTRACT	BRIDGE FILE
B-43636	020-45-02821

STRUCTURE INFORMATION				
STRUCTURE	TYPE	SPAN AND SKEW	OVER	STATION
020-45-02821	Continuous Composite Steel Plate Girder Bridge	2 Spans: 135'-0", 85'-0" Skew: 0°	Kennedy Avenue & Indiana Harbor Belt Railroad	552+19.71 Line "A"

NOTE TO REVIEWER:
The project scope has been updated to include a multi-use path, that will be part of the Marquette Greenway Trail, and will be coordinated with other trail projects at the west end (Des. 1601146) and east end (Des. 1901900), as well as with the Kennedy Avenue Road Diet project (Des. 1901902). Additionally, in coordination with the District, the bridge clear roadway width has been increased to improve MOT for future bridge maintenance projects. Refer to the correspondence file for information regarding these changes. A Draft Addendum to the approved Engineering Assessment, covering these changes, has been included with this submittal and will be submitted to the District for review and approval.

INDIANA DEPARTMENT OF TRANSPORTATION

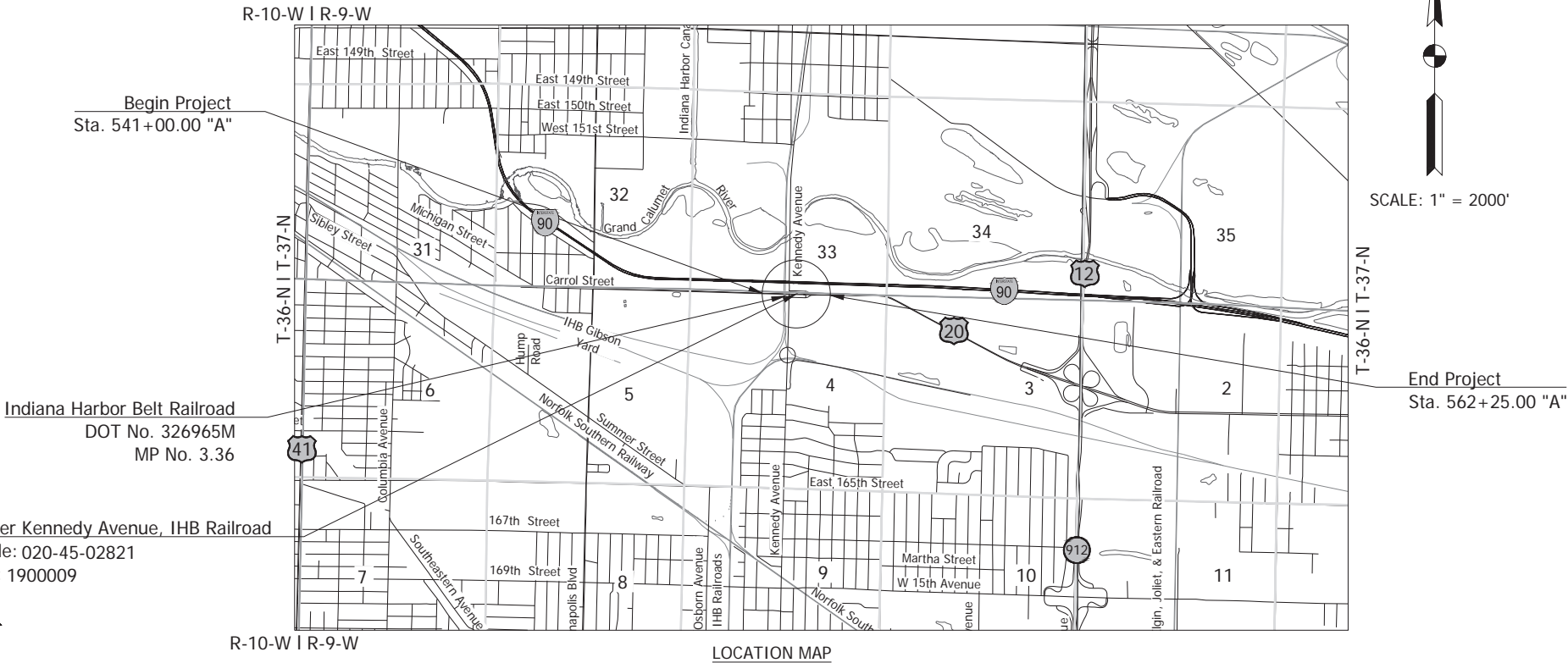


BRIDGE PLANS FOR SPANS OVER 20 FEET

ROUTE: US 20 AT: RP 8+17


PROJECT NO. 1900009 P.E.
 1900009 R/W
 1900009 CONST.

Bridge Replacement on US 20 over Kennedy Avenue & IHB Railroad
Located 0.99 miles East of SR 152
Section 4, T-36-N & Section 33, T-37-N, R-9-W, North Township, Lake County



UTILITIES

<p><u>COMMUNICATIONS:</u></p> <p>AT&T 302 S EAST ST. CROWN POINT, IN 46307 (219) 662-4418 CONTACT: MATT SMITH MS2827@ATT.COM</p> <p>WINDSTREAM 7998 CENTERPOINT DR. STE 300 FISHERS, IN 46256 (720) 341-7845 CONTACT: VICTOR KOTH VICTOR.KOTH@WINDSTREAM.COM</p> <p>MCi ASG.INVESTIGATIONSTEAM@ASGINC.COM</p> <p><u>CABLE:</u></p> <p>COMCAST 16 W 84TH ST. MERRILLVILLE, IN 46410 (574) 320-8203 CONTACT: LARRY SMITH LARRY_SMITH3@COMCAST.COM</p> <p>WIDE OPEN WEST (WOW) 1674 FRONTENAC RD NAPERVILLE, IL 60653 CONTACT: MARVIN HAZLEY (630) 631-2594 MARVIN.HAZLEY@WOWINC.COM</p> <p><u>FIBER OPTICS:</u></p> <p>LUMEN LOCAL RELOCATIONS@LUMEN.COM</p> <p>LUMEN NATIONAL RELOCATIONS@LUMEN.COM</p> <p><u>SEWER:</u></p> <p>HAMMOND SANITARY DISTRICT 5143 COLUMBIA AVE. HAMMOND, IN 46327 (219) 853-6413 CONTACT: JEFF MASSEY JMASSEY@HAMMONDSD.COM</p>	<p><u>WATER:</u></p> <p>HAMMOND WATER WORKS 6505 COLUMBIA AVE. HAMMOND, IN 46320 (317) 853-6421 CONTACT: BRUCE LONG LONGBO@GOHAMMOND.COM</p> <p><u>ELECTRIC:</u></p> <p>NIPSCO (ELECTRIC DISTRIBUTION) 801 E 86TH AVE. MERRILLVILLE, IN 46410 (219) 647-6260 CONTACT: DAVE SCHAAFSMA UTILITYCOORDINATION@NISOURCE.COM</p> <p>NIPSCO (ELECTRIC TRANSMISSION) 801 E 86TH AVE. MERRILLVILLE, IN 46410 (219) 647-6260 CONTACT: DAVE SCHAAFSMA UTILITYCOORDINATION@NISOURCE.COM</p> <p><u>GAS:</u></p> <p>NIPSCO (GAS DISTRIBUTION) 801 E 86TH AVE. MERRILLVILLE, IN 46410 (219) 647-6260 CONTACT: DAVE SCHAAFSMA UTILITYCOORDINATION@NISOURCE.COM</p> <p>NIPSCO (GAS TRANSMISSION) 801 E 86TH AVE. MERRILLVILLE, IN 46410 (219) 647-6260 CONTACT: DAVE SCHAAFSMA UTILITYCOORDINATION@NISOURCE.COM</p> <p><u>PIPELINE:</u></p> <p>BP PIPELINE 30 S WACKER DR., Suite 900 CHICAGO, IL 60606 (312) 809-4708 CONTACT: KEITH BOYLE KEITH.BOYLE@BP.COM</p>	<p>BUCKEYE PIPELINE 5521 W LINCOLN HWY, STE 305 CROWN POINT, IN 46307 (304) 695-9841 CONTACT: SHAWN VAILLANCOURT VAILLANCOURT@BUCKEYE.COM</p> <p>EXPLORER PIPELINE COMPANY P.O. BOX 2650 TULSA, OK 74136 (918) 493-5153 CONTACT: ALTON RYALS ARYALS@EXPL.COM</p> <p>WEST SHORE PIPELINE COMPANY 9999 HAMILTON BLVD. BREINIGSVILLE, PA 18031 (610) 904-4475 CONTACT: TRACI MCCLERNON TMCCLERNON@BUCKEYE.COM</p> <p>MARATHON 1900 W AVENUE H GRIFFITH, IN 46319 (419) 957-7792 CONTACT: LANDON MORRIS LRMORIS@MARATHONPETROLEUM.COM</p>
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Know what's below.
Call before you dig.

RAILROAD CONTACT

RAILROAD:

IHB RAILROAD 2721 161st ST. HAMMOND, IN 46323 (219) 989-4910 CONTACT: EDUARDO GARCIA MANAGER OF PUBLIC WORKS EDUARDO.GARCIA@IHBR.COM	NORFOLK SOUTHERN RAILWAY RAILPROS 11819 MIRACLE HILLS DR., SUITE 102 OMAHA, NE 68154 NS.PERMITTING@RAILPROS.COM
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[illegible][illegible]

Know what's below.
Call before you dig.

Date: Nov 21, 2023, 5:15pm User Name: Randy.Kilbase
File: X:\Production\Files\2019\219-0350\CAD\Plans\Sit Index.dwg

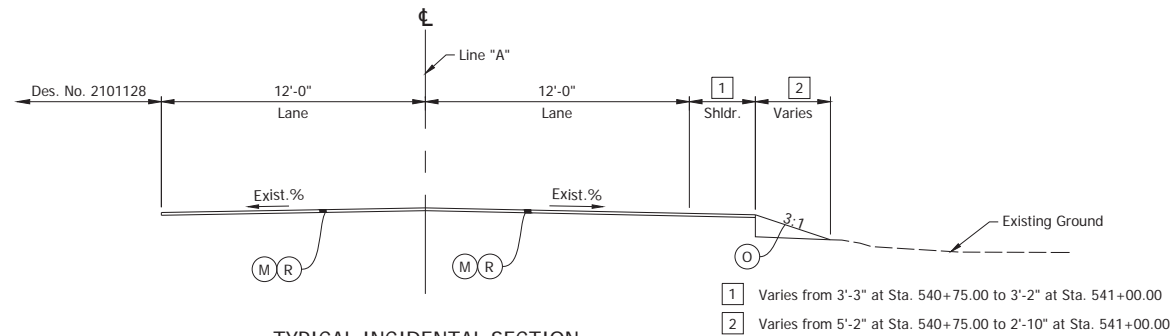
PRELIMINARY
NOT FOR CONSTRUCTION
DATE: 11/21/2023

RECOMMENDED FOR APPROVAL	11/21/2023	
DESIGN ENGINEER		DATE
DESIGNED: SCC	DRAWN: SCC	
CHECKED: BSS	CHECKED: BJS	

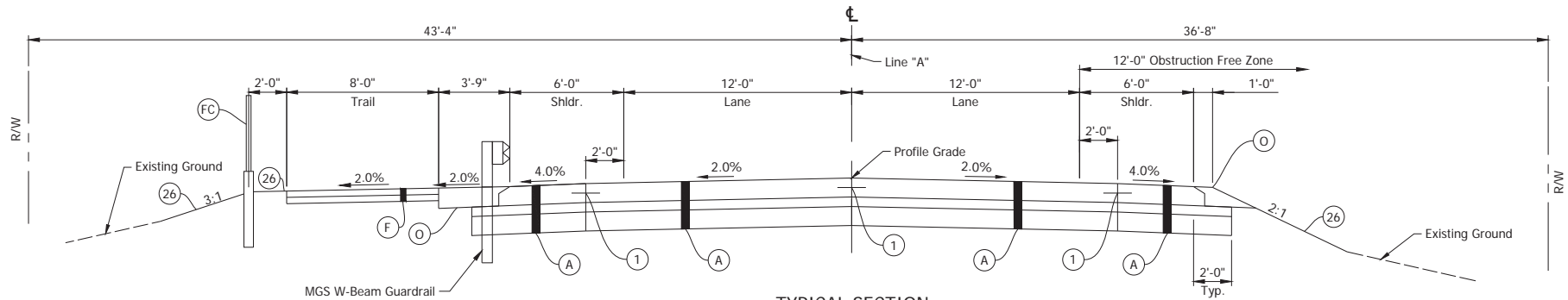
INDEX SHEET

HORIZONTAL SCALE		BRIDGE FILE	
N/A		020-45-02821	
VERTICAL SCALE		DESIGNATION	
N/A		1900009	
SURVEY BOOK		SHEETS	
ELECTRONIC	2	of	76
CONTRACT	PROJECT		
B-43636	1900009		

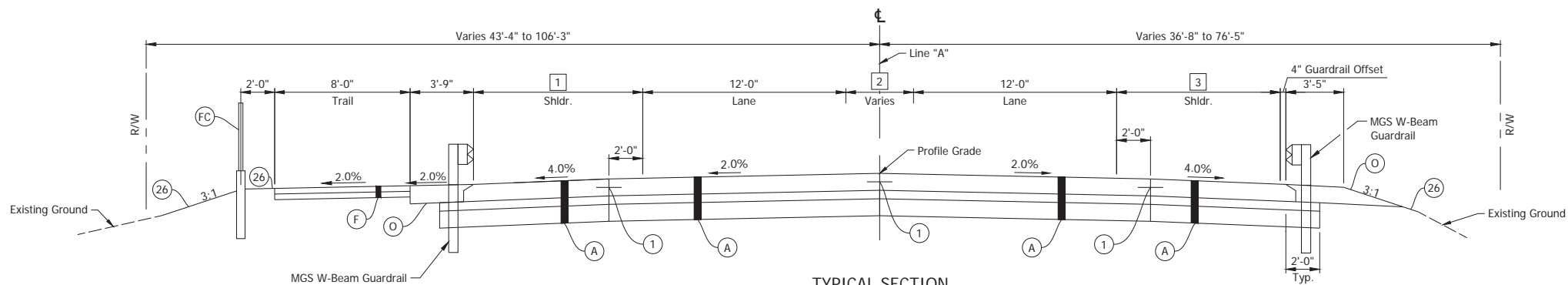
Date: Nov 21, 2023, 5:15pm User Name: Randy Allbase
File: X:\ProductionFiles\2019\215-0350\CAD\WSDMG\2190350 Typical Sections.dwg



TYPICAL INCIDENTAL SECTION
Sta. 540+75.00 Line "A" To Sta. 541+00.00 Line "A"



TYPICAL SECTION
Sta. 541+00.00 Line "A" To Sta. 544+22.48 Line "A"

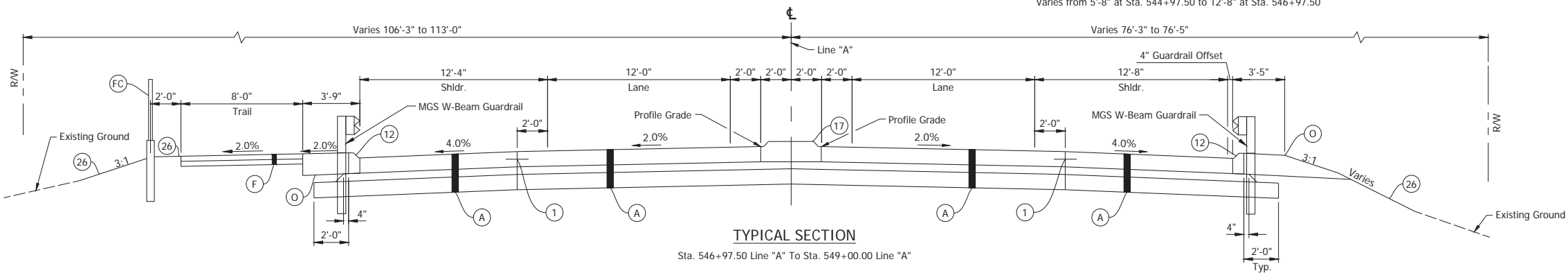


6'-0" at Sta. 544+22.48 to Sta. 544+97.50 1
Varies from 6'-0" at Sta. 544+97.00 to 12'-4" at Sta. 546+97.50

TYPICAL SECTION
Sta. 544+22.48 Line "A" To Sta. 546+97.50 Line "A"

2 0'-0" at Sta. 544+22.48 to Sta. 544+97.50
Varies from 0'-0" at Sta. 544+97.50 to 8'-0" at Sta. 546+97.50

3 5'-8" at Sta. 544+22.48 to Sta. 544+97.50
Varies from 5'-8" at Sta. 544+97.50 to 12'-8" at Sta. 546+97.50



TYPICAL SECTION
Sta. 546+97.50 Line "A" To Sta. 549+00.00 Line "A"

NOTE:
See Sheet 6 for curb and median details.

- LEGEND:
- (A) 12 in. QC/QA PCCP on Dense Graded Subbase, 6 in., on Subgrade Treatment, Type IC
 - (F) Trail, Consisting of: PCCP, 4 in. on Compacted Aggregate No. 53 (4 in.) on Subgrade Treatment, Type III
 - (FC) Fence, Chain Link, PVC Coated, 48 in.
 - (M) Milling, Asphalt, 1.5 in.
 - (O) Compacted Aggregate, No. 53
 - (R) 165 lbs/syd QC/QA-HMA, 3, 70, Surface, 9.5 mm
 - (1) Longitudinal Construction Joint
 - (12) Curb, Type B, Integral Concrete
 - (17) Center Curb, Type B, Concrete, Modified
 - (26) Sodding, Nursery

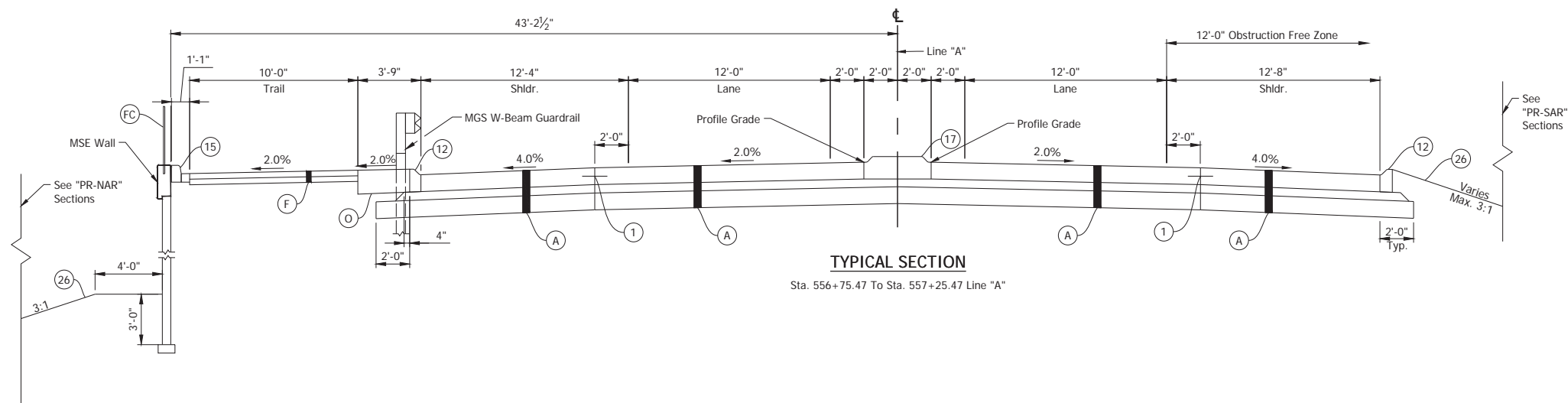
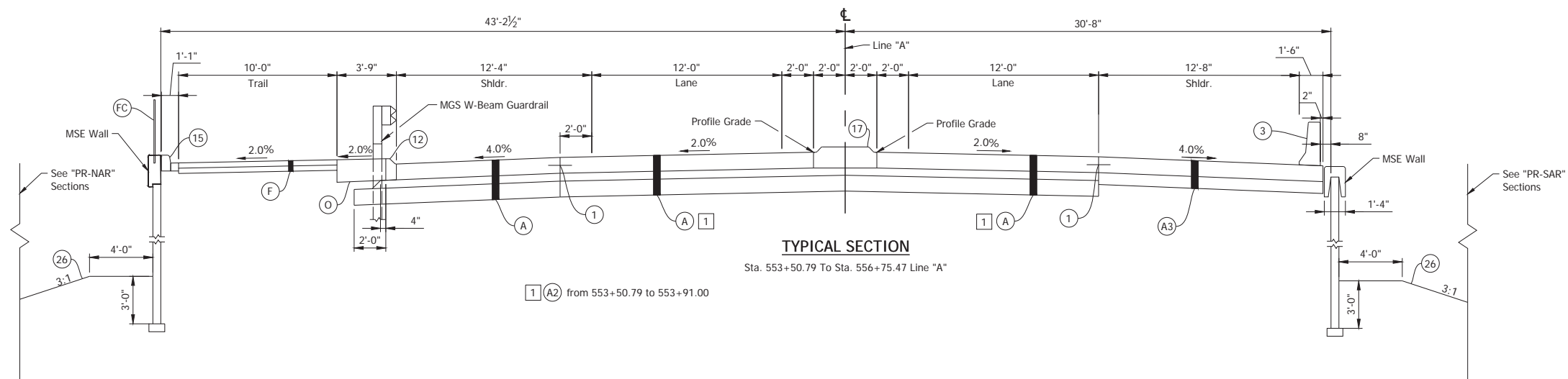
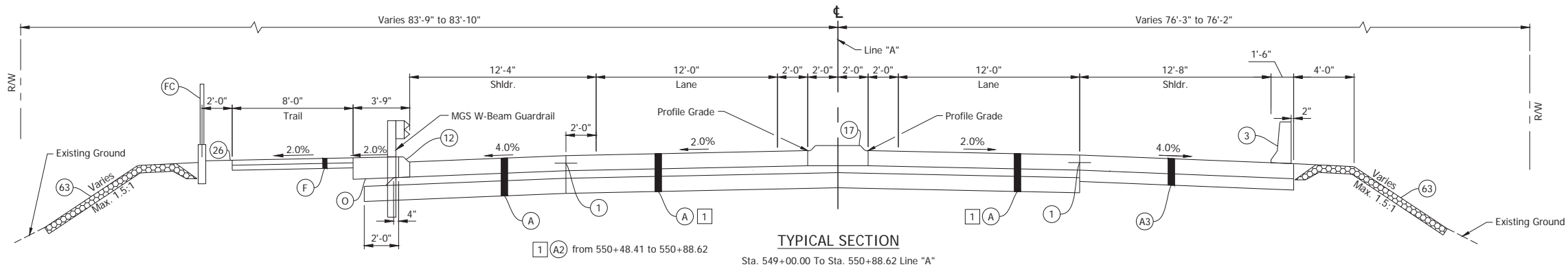
PRELIMINARY
NOT FOR CONSTRUCTION
DATE: 11/21/2023

RECOMMENDED FOR APPROVAL		11/21/2023	
DESIGN ENGINEER		DATE	
DESIGNED: DJM	DRAWN: DJM		
CHECKED: BSS	CHECKED: BJS		

INDIANA
DEPARTMENT OF TRANSPORTATION

TYPICAL CROSS SECTIONS

HORIZONTAL SCALE		BRIDGE FILE	
1/4" = 1'-0"		020-45-02821	
VERTICAL SCALE		DESIGNATION	
1/4" = 1'-0"		1900009	
SURVEY BOOK		SHEETS	
ELECTRONIC		3 of 76	
CONTRACT		PROJECT	
B-43636		1900009	



NOTE:
See Sheet 6 for curb and median details.

- LEGEND:**
- (A) 12 in. OC/OA PCCP on Dense Graded Subbase, 6 in., on Subgrade Treatment, Type IC
 - (A1) 12 in. PCCP on 6 in. Dense Graded Subbase on Subgrade Treatment, Type IC
 - (A2) Jointed Reinforced Concrete Pavement 12 in. on 9" Subbase for PCCP on Subgrade Treatment, Type IC
 - (A3) 12 in. Reinforced Concrete Moment Slab on 9 in. Compacted Aggregate No. 8
 - (F) Trail, Consisting of: PCCP, 4 in. on Compacted Aggregate No. 53 (4 in.) on Subgrade Treatment, Type III
 - (FC) Fence, Chain Link, PVC Coated, 48 in.
 - (1) Longitudinal Construction Joint
 - (3) FC Concrete Railing
 - (12) Curb, Type B, Integral Concrete
 - (15) Curb, Integral Concrete
 - (17) Center Curb, Type B, Concrete, Modified
 - (63) 6 in. Geocell on Geotextile for Subgrade, Type 2B

Date: Nov 21, 2023, 5:15pm User Name: Randy_Villase
File: X:\ProductionFiles\2023\1215-0350\CAD\misc\DWG\2190350 Typical Sections.dwg

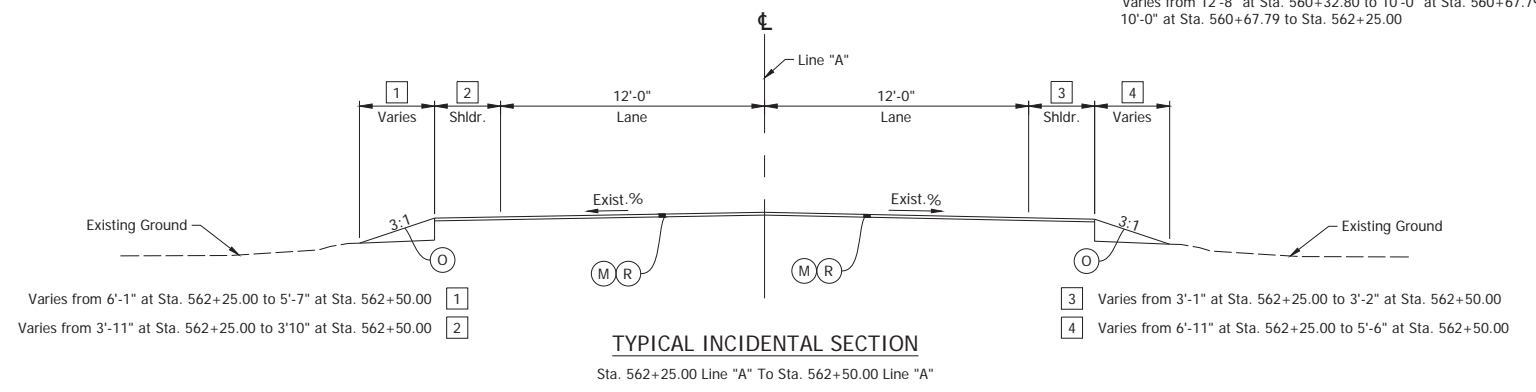
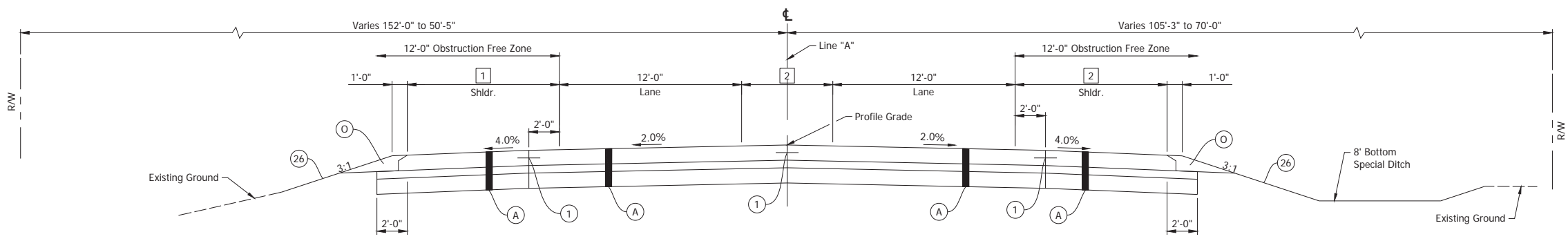
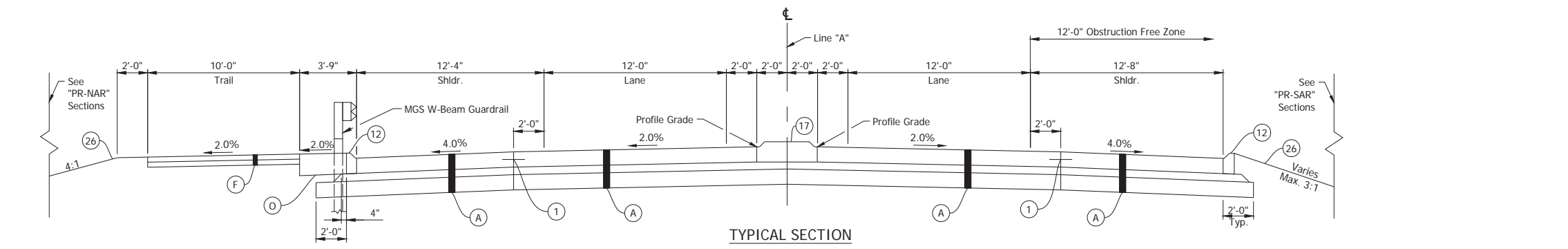
PRELIMINARY
NOT FOR CONSTRUCTION
DATE: 11/21/2023

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	11/21/2023	DATE
DESIGNED: DJM	DRAWN: DJM		
CHECKED: BSS	CHECKED: BJS		

INDIANA
DEPARTMENT OF TRANSPORTATION

TYPICAL CROSS SECTIONS

HORIZONTAL SCALE	BRIDGE FILE
1/4" = 1'-0"	020-45-02821
VERTICAL SCALE	DESIGNATION
1/4" = 1'-0"	1900009
SURVEY BOOK	SHEETS
ELECTRONIC	4 of 76
CONTRACT	PROJECT
B-43636	1900009

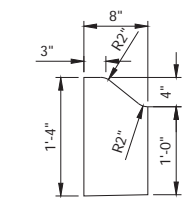
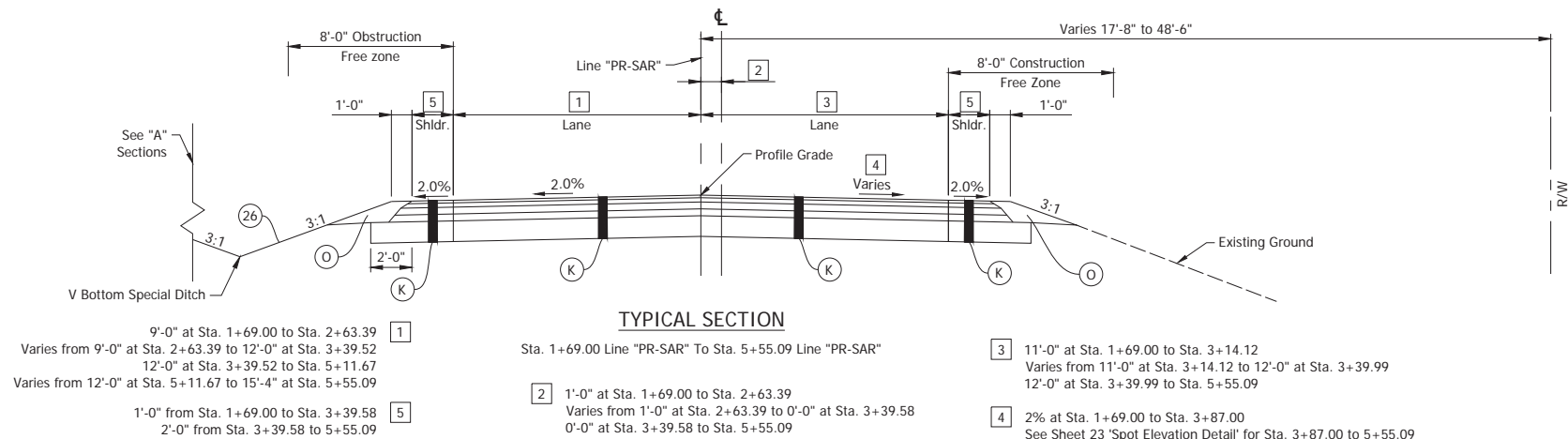
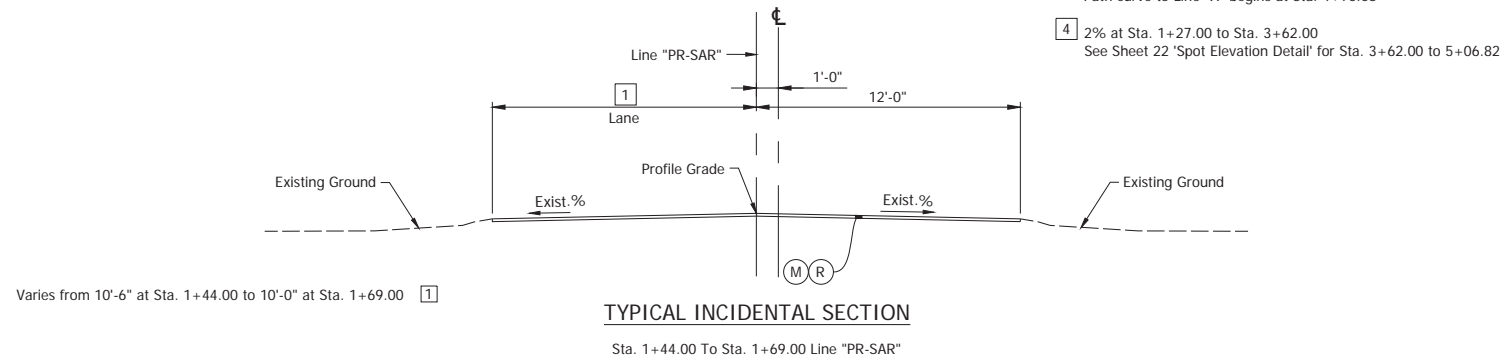
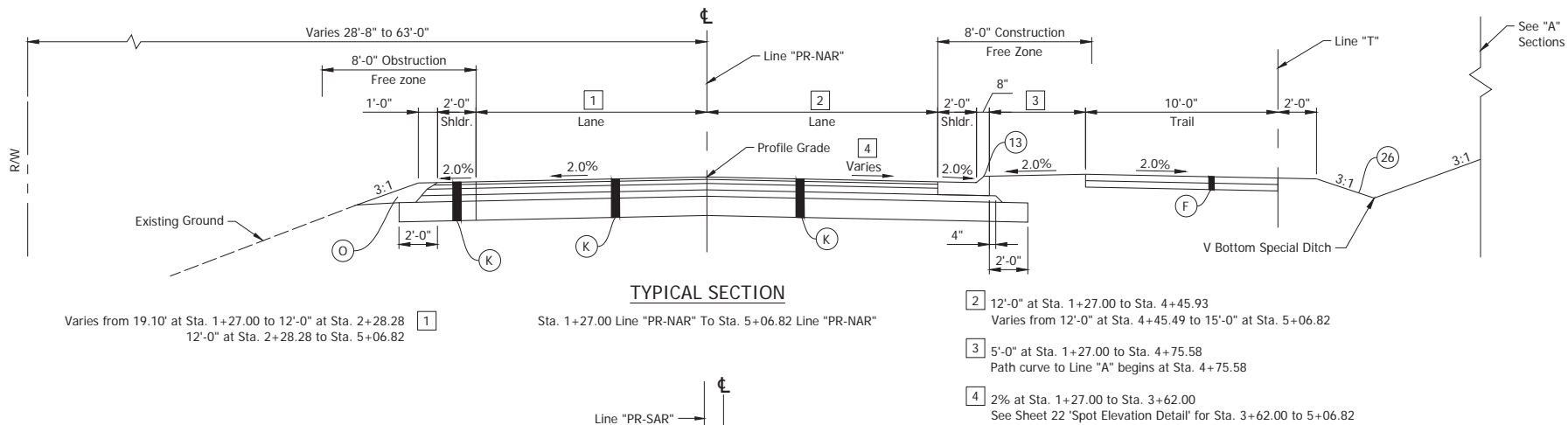
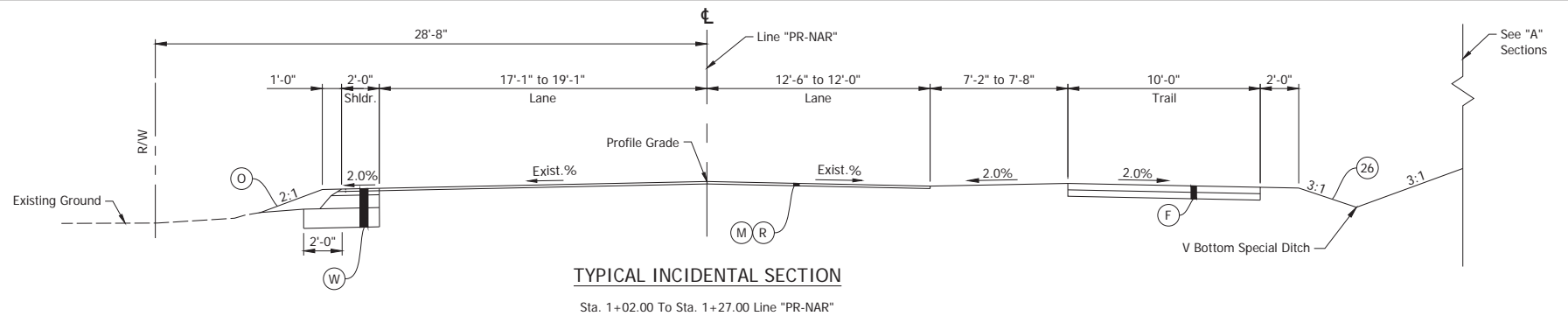


- NOTE:**
See Sheet 6 for curb and median details.
- LEGEND:**
- (A) 12 in. QC/QA PCCP on Dense Graded Subbase, 6 in., on Subgrade Treatment, Type IC
 - (F) Trail, Consisting of: PCCP, 4 in. on Compacted Aggregate No. 53 (4 in.) on Subgrade Treatment, Type III
 - (M) Milling, Asphalt, 1.5 in.
 - (O) Compacted Aggregate, No. 53
 - (R) 165 lbs/syd QC/QA-HMA, 3, 70, Surface, 9.5 mm
 - (1) Longitudinal Construction Joint
 - (12) Curb, Type B, Integral Concrete
 - (17) Center Curb, Type B, Concrete, Modified

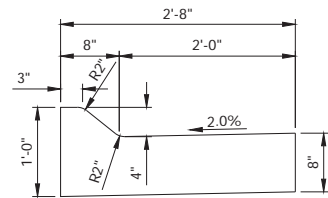
Date: Nov 21, 2023, 5:15pm User Name: Randy Villase
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PRELIMINARY NOT FOR CONSTRUCTION DATE: 11/21/2023	RECOMMENDED FOR APPROVAL _____ 11/21/2023 DESIGN ENGINEER DATE		INDIANA DEPARTMENT OF TRANSPORTATION		HORIZONTAL SCALE		BRIDGE FILE	
					1/4" = 1'-0"		020-45-02821	
					VERTICAL SCALE		DESIGNATION	
					1/4" = 1'-0"		1900009	
	DESIGNED: _____ DJM DRAWN: _____ DJM				SURVEY BOOK		SHEETS	
				ELECTRONIC		5 of 76		
CHECKED: _____ BSS CHECKED: _____ BJS				CONTRACT		PROJECT		
				B-43636		1900009		

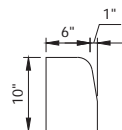
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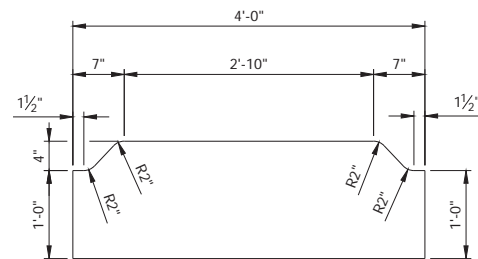
12 CURB DETAIL
Integral Concrete Curb, Type B
(4" Curb Reveal)
Scale: 1" = 1'



13 CURB DETAIL
Concrete Curb and Gutter, Type B
(8" Depth)
Scale: 1" = 1'



15 CURB DETAIL
Integral Concrete Curb
(6" Reveal)
Scale: 1" = 1'



17 CURB DETAIL
Center Curb, Type B, Concrete, Modified
(1' Depth)
Scale: 1" = 1'

LEGEND:

- (F) Trail, Consisting of:
PCCP, 4 in. on
Compacted Aggregate No. 53 (4 in.) on
Subgrade Treatment, Type III
- (K) 165 lbs/syd QC/QA HMA, 3, 70, Surface, 9.5mm on
275 lbs/syd QC/QA HMA, 3, 70, Intermediate, 19mm on
880 lbs/syd QC/QA HMA, 3, 64, Base, 25mm on
Subgrade Treatment, Type IC
- (M) Milling, Asphalt, 1.5 in.
- (O) Compacted Aggregate, No. 53
- (R) 165 lbs/syd QC/QA-HMA, 3, 70, Surface, 9.5 mm
- (W) 165 lbs/syd QC/QA HMA, 3, 70, Surface, 9.5 mm on
Widening with HMA, Type D, consisting of:
275 lbs/syd QC/QA HMA, 3, 70, Intermediate, 19mm on
880 lbs/syd QC/QA HMA, 3, 64, Base, 25mm on
Subgrade Treatment, Type IC
- 13 Concrete Curb and Gutter,
Type B

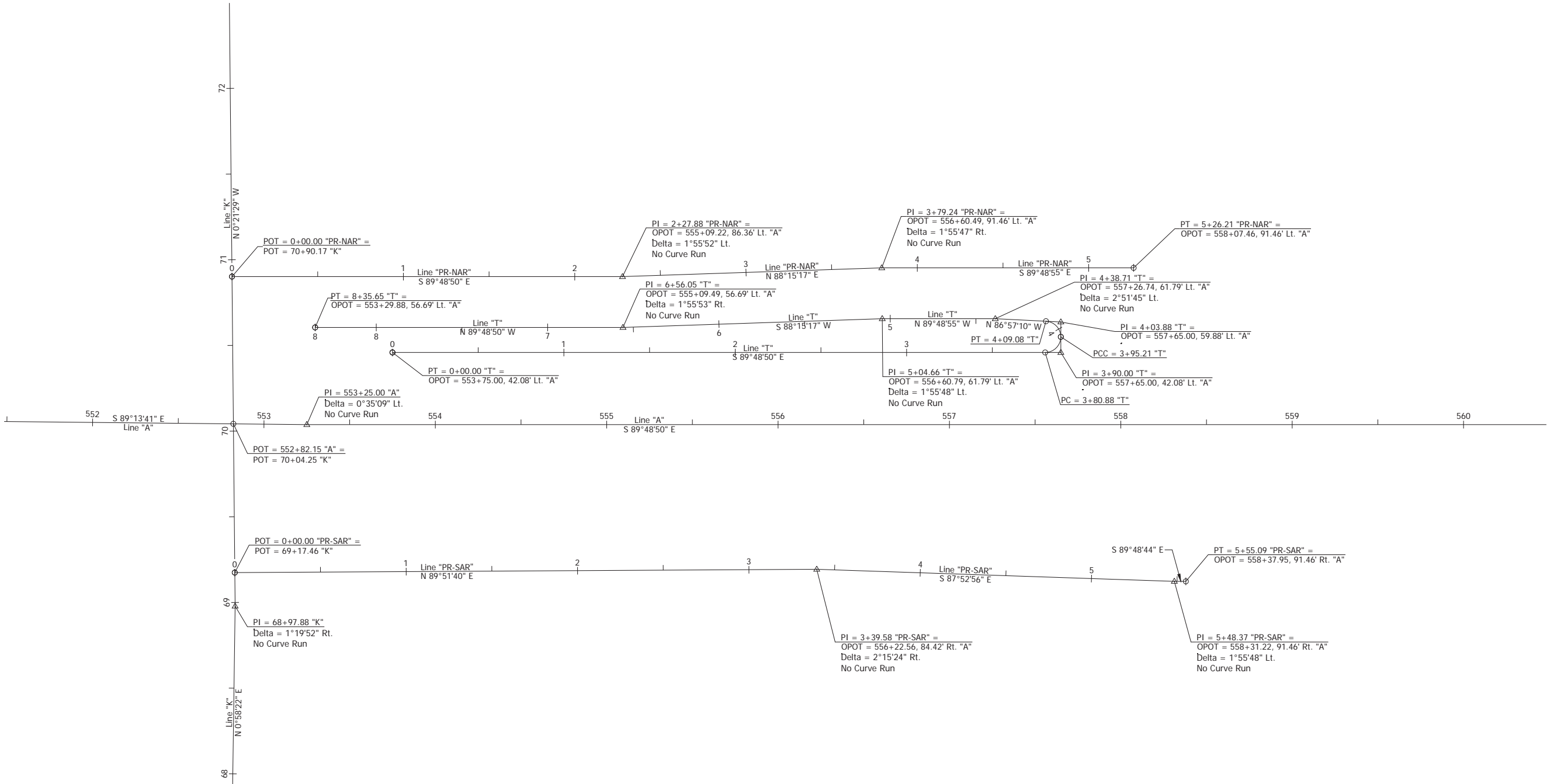
RECOMMENDED
FOR APPROVAL _____ 11/21/2023
DESIGN ENGINEER DATE

DESIGNED: _____ DJM DRAWN: _____ DJM
CHECKED: _____ BSS CHECKED: _____ BJS

INDIANA
DEPARTMENT OF TRANSPORTATION

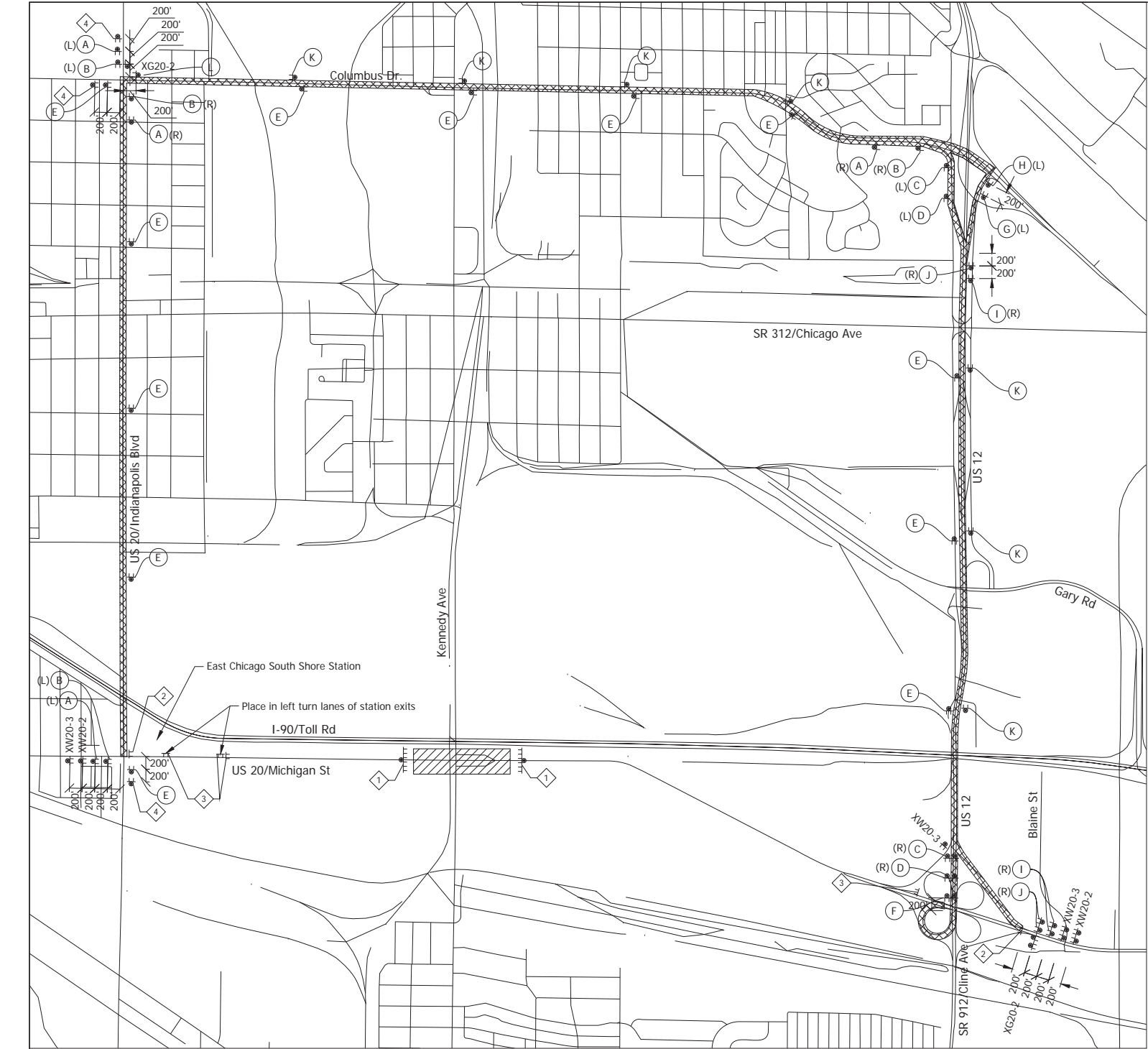
TYPICAL CROSS SECTIONS

HORIZONTAL SCALE	BRIDGE FILE
1/4" = 1'-0"	020-45-02821
VERTICAL SCALE	DESIGNATION
1/4" = 1'-0"	1900009
SURVEY BOOK	SHEETS
ELECTRONIC	6 of 76
CONTRACT	PROJECT
B-43636	1900009



Date: Nov 21, 2023 5:16pm User Name: Randy Kibase
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PRELIMINARY NOT FOR CONSTRUCTION DATE: 11/21/2023	RECOMMENDED FOR APPROVAL _____ 11/21/2023 DESIGN ENGINEER DATE		INDIANA DEPARTMENT OF TRANSPORTATION		HORIZONTAL SCALE 1" = 30'	BRIDGE FILE 020-45-02821
	DESIGNED: _____ MJO CHECKED: _____ BSS		DRAWN: _____ BJS CHECKED: _____ BJS		VERTICAL SCALE N/A	DESIGNATION 1900009
	GEOMETRIC TIE-UP SHEET				SURVEY BOOK	SHEETS
					ELECTRONIC CONTRACT B-43636	8 of 76 PROJECT 1900009



Detour Route
Scale: 1" = 1000'

LEGEND

- CONSTRUCTION ZONE
- DETOUR ROUTE
- ROAD CLOSURE SIGN ASSEMBLY W/ TYPE III-A BARRICADE (60 LFT.) AND R11-2 SIGN
- ROAD CLOSURE SIGN ASSEMBLY W/ TYPE III-B BARRICADE (12 LFT.) AND R11-4 SIGN AND XM4-10 (L OR R)
- ROAD CLOSURE SIGN ASSEMBLY W/ TYPE III-B BARRICADE (12 LFT.) AND R11-4 SIGN

6'-0"

**US 20 CLOSED
AT KENNEDY AVE
USE ALTERNATE ROUTE**

3'-0"

SPE-1
LETTER SERIES: D, LETTER SIZE: 4"
BLACK LEGEND & BORDER W/ ORANGE BACKGROUND

DETOUR
EAST
20

XM4-8
M3-2
M1-4
M5-1 (R or L)

DETOUR
EAST
20

XM4-8
M3-2
M1-4
M6-1 (R or L)

DETOUR
EAST
20

XM4-8
M3-2
M1-4
M5-2 (R or L)

DETOUR
EAST
20

XM4-8
M3-2
M1-4
M6-2 (R or L)

DETOUR
EAST
20

XM4-8
M3-2
M1-4
M6-3 (R or L)

END
DETOUR
EAST
20

XM4-8a
M3-4
M1-4
M6-1 (R)

DETOUR
WEST
20

XM4-8
M3-4
M1-4
M5-1 (R or L)

DETOUR
WEST
20

XM4-8
M3-4
M1-4
M6-1 (R or L)

DETOUR
WEST
20

XM4-8
M3-4
M1-4
M5-2 (R or L)

DETOUR
WEST
20

XM4-8
M3-4
M1-4
M6-2 (R or L)

DETOUR
WEST
20

XM4-8
M3-4
M1-4
M6-3 (R or L)

END
DETOUR
WEST
20

XM4-8a
M3-4
M1-4
M5-2 (R)

ROAD
CLOSED

R11-2*

ROAD CLOSED
TO
THRU TRAFFIC

R11-4*

DETOUR
AHEAD

XW20-2

ROAD
CLOSED
AHEAD

XW20-3

END
CONSTRUCTION

XG20-2

GENERAL NOTES

- All maintenance of traffic devices, signs and pavement markings shall conform to the latest edition of the Indiana MUTCD.
- See INDOT Std. Drwg. 801-TCDT-01 for sign spacing requirements and additional notes.
- For detour route marker assemblies, see INDOT Std. Drwg. 801-TCDT-03.
- See INDOT Std. Drwg. 801-TCLG-01 for standard notes.
- Type B construction warning lights shall be used with all signs located on barricades. Type A construction warning lights shall be used on all other construction signs.
- Road closures to be warned at least one week in advance utilizing XG20-5 sign, reading "US 20 CLOSED AFTER MM/DD"

CONSTRUCTION SIGN SCHEDULE				
SIGN NO.	DESCRIPTION	SIZE (IN)	TYPE	QUANTITY
XW20-2	"DETOUR AHEAD" SIGN	36 x 36	A	3
XW20-3	"ROAD CLOSED AHEAD" SIGN	36 x 36	A	3
XG20-2	"END CONSTRUCTION"	60 x 24	A	2
XG20-5	"US 20 CLOSED AFTER MM/DD"	60 x 36	A	4
SPE-1	"US 20 CLOSED AT KENNEDY AVE USE ALTERNATE ROUTE" SIGN	36 x 72	C	3
			TOTAL TYPE "A" SIGNS	12
			TOTAL TYPE "C" SIGNS	3

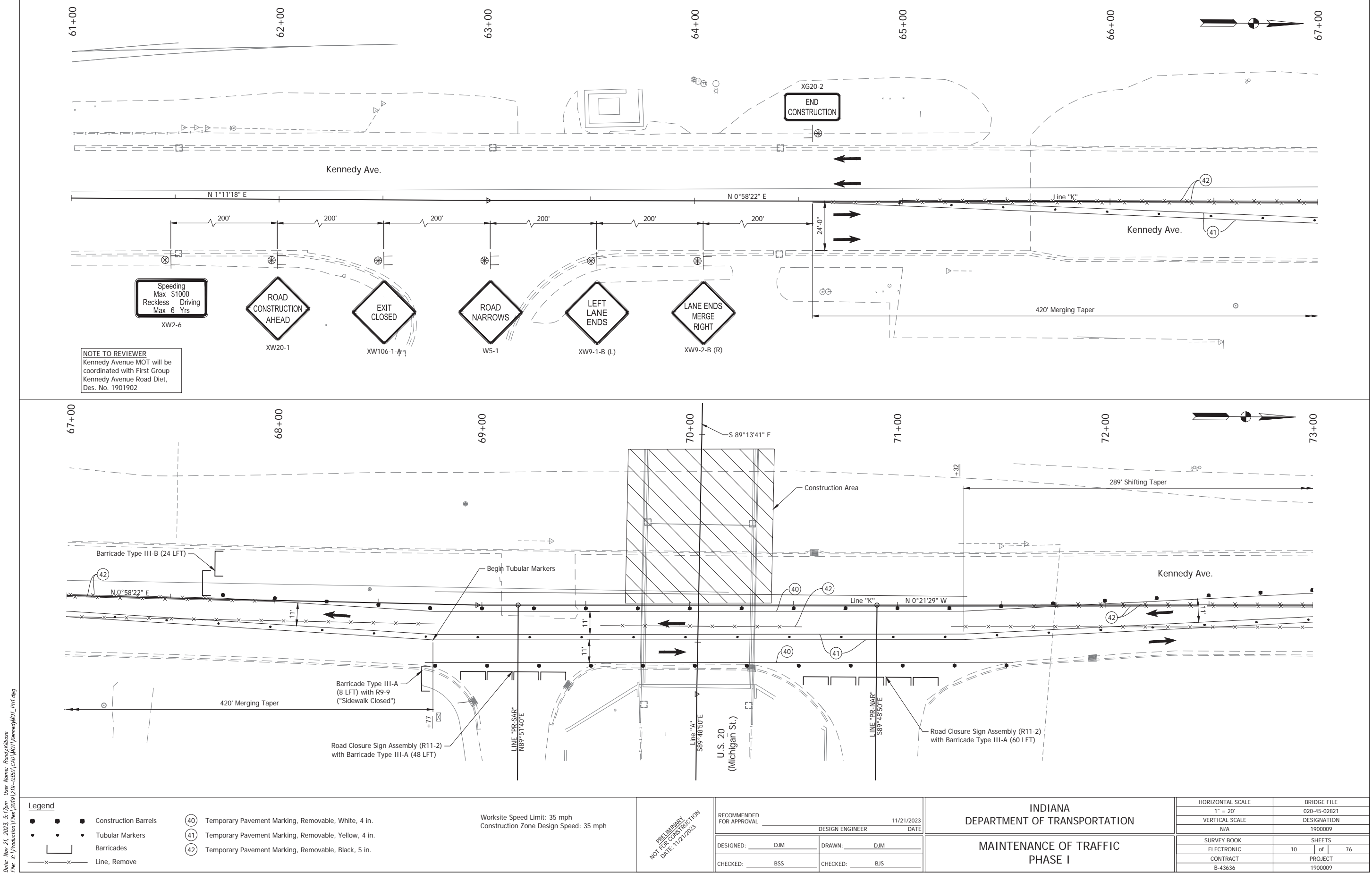
Detour Route Marker Assemblies: 43 Req'd
Road Closure Sign Assemblies: 8 Req'd
Type III-A Barricades: 120 Lft.
Type III-B Barricades: 72 Lft.

*Included with road closure sign assembly.

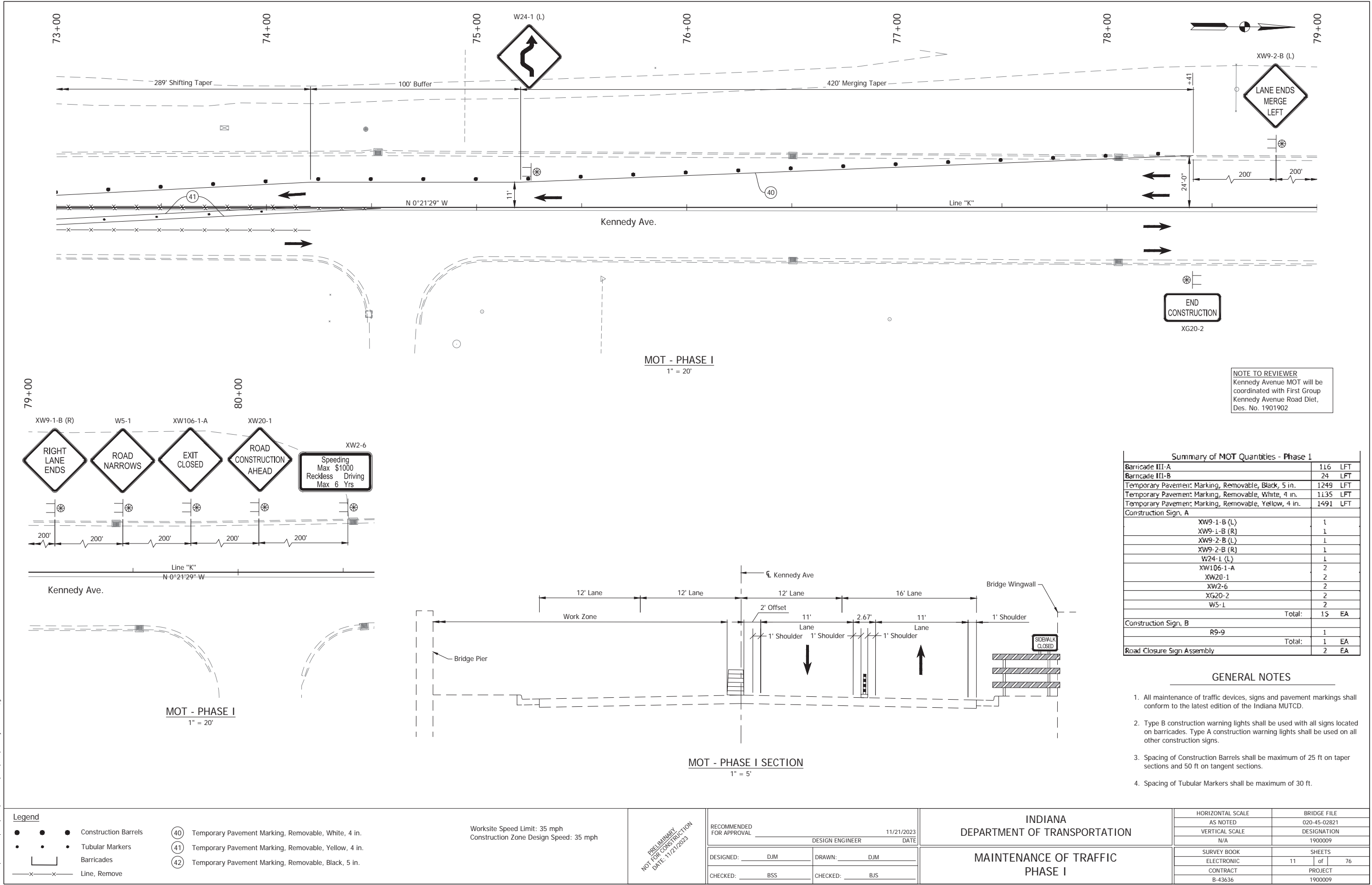
Date: Nov 21, 2023, 5:17pm User Name: Randy Villase
File: X:\ProductionFiles\2019\219-0350\CD\INDOT\Detour.dwg

PRELIMINARY
NOT FOR CONSTRUCTION
DATE: 11/21/2023

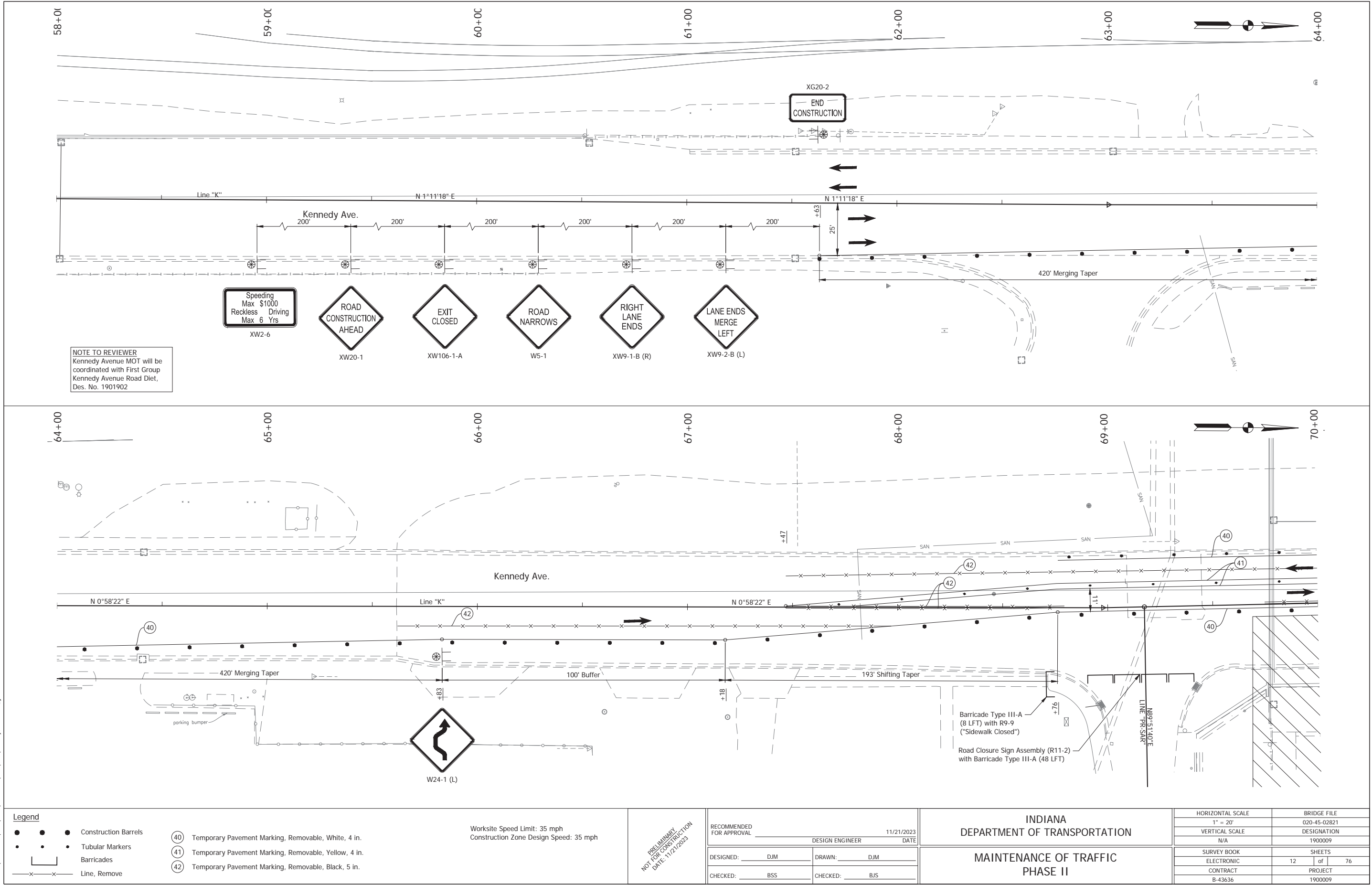
RECOMMENDED FOR APPROVAL _____ DESIGN ENGINEER _____ DATE 11/21/2023		INDIANA DEPARTMENT OF TRANSPORTATION		HORIZONTAL SCALE		BRIDGE FILE	
				AS NOTED		020-45-02821	
				VERTICAL SCALE		DESIGNATION	
				N/A		1900009	
				SURVEY BOOK		SHEETS	
DESIGNED: _____ DJM _____		DRAWN: _____ DJM _____		ELECTRONIC		9 _____ of _____ 76	
CHECKED: _____ BSS _____		CHECKED: _____ BJS _____		CONTRACT		PROJECT	
				B-43636		1900009	



Date: Nov 21, 2023, 5:17pm User Name: RandyKibase
File: X:\Production Files\2019\19-0350\1901\Kennedy\MOT_PHI.dwg



Date: Nov 21, 2023, 5:17pm User Name: RandyKibase
File: K:\Production Files 2019\1719-0350\040\MOI\Kennedy\MOI_P12.dwg



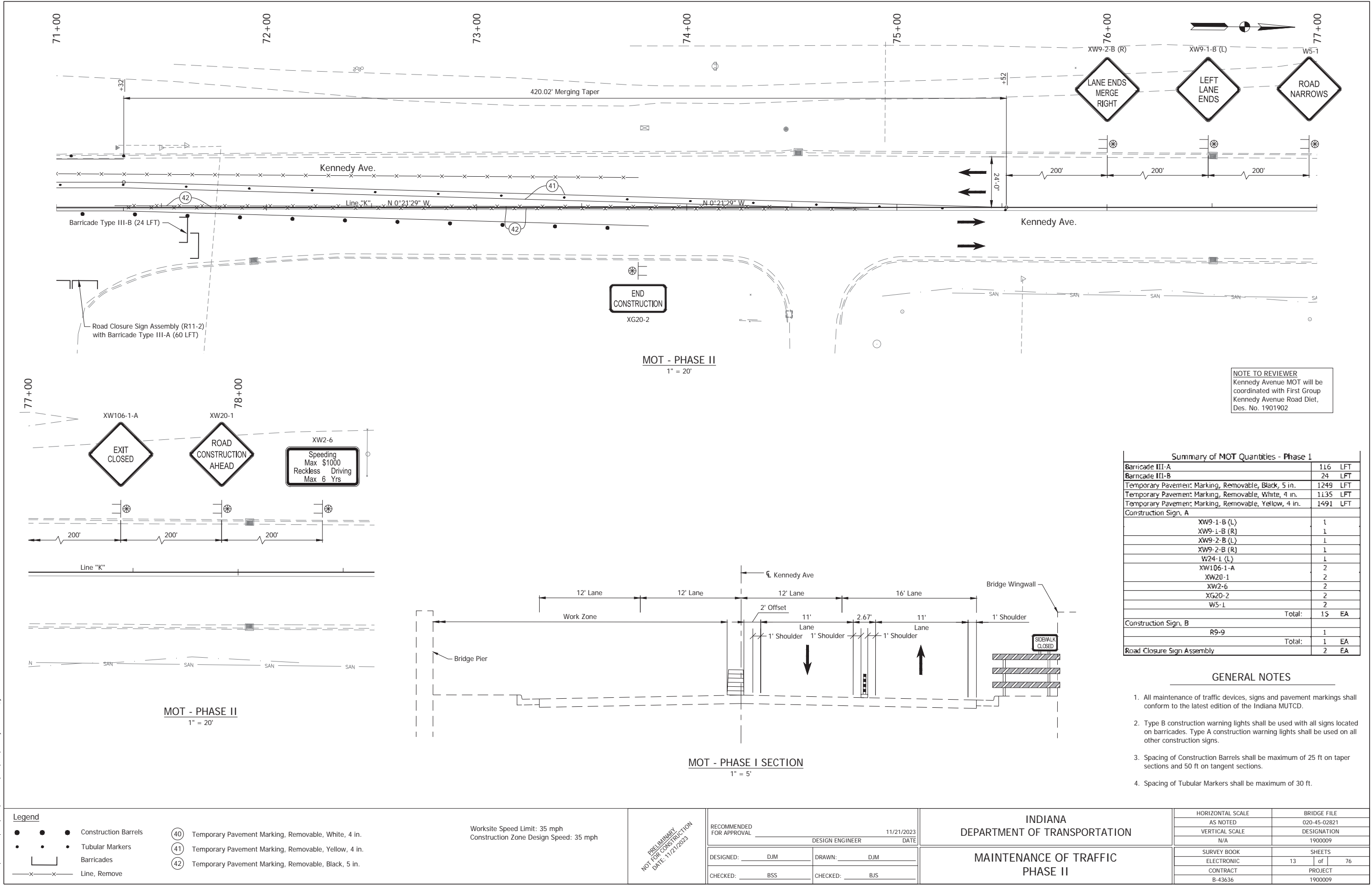
Legend

● ● ●	Construction Barrels	④	Temporary Pavement Marking, Removable, White, 4 in.
● ● ●	Tubular Markers	④	Temporary Pavement Marking, Removable, Yellow, 4 in.
—x—x—	Barricades	④	Temporary Pavement Marking, Removable, Black, 5 in.

Worksite Speed Limit: 35 mph
Construction Zone Design Speed: 35 mph

<div>PRELIMINARY NOT FOR CONSTRUCTION DATE: 11/21/2023</div>	RECOMMENDED FOR APPROVAL _____ 11/21/2023 DESIGN ENGINEER DATE		INDIANA DEPARTMENT OF TRANSPORTATION		HORIZONTAL SCALE 1" = 20'	BRIDGE FILE 020-45-02821
	DESIGNED: _____ DJM	DRAWN: _____ DJM	MAINTENANCE OF TRAFFIC PHASE II		VERTICAL SCALE N/A	DESIGNATION 1900009
	CHECKED: _____ BSS	CHECKED: _____ BJS			SURVEY BOOK ELECTRONIC	SHEETS 12 of 76
					CONTRACT B-43636	PROJECT 1900009

Date: Nov 21, 2023, 5:17pm User Name: Randy Kibase
File: X:\Production Files\2019\1915-0350\CD\MOI\Kennedy\MOT_Ph2.dwg



Summary of MOT Quantities - Phase 1		
Barricade III-A	116	LFT
Barricade III-B	24	LFT
Temporary Pavement Marking, Removable, Black, 5 in.	1249	LFT
Temporary Pavement Marking, Removable, White, 4 in.	1135	LFT
Temporary Pavement Marking, Removable, Yellow, 4 in.	1491	LFT
Construction Sign, A		
XW9-1-B (L)	1	
XW9-1-B (R)	1	
XW9-2-B (L)	1	
XW9-2-B (R)	1	
W24-1 (L)	1	
XW106-1-A	2	
XW20-1	2	
XW2-6	2	
XG20-2	2	
WS-1	2	
Total:	15	EA
Construction Sign, B		
R9-9	1	
Total:	1	EA
Road Closure Sign Assembly	2	EA

- GENERAL NOTES**
- All maintenance of traffic devices, signs and pavement markings shall conform to the latest edition of the Indiana MUTCD.
 - Type B construction warning lights shall be used with all signs located on barricades. Type A construction warning lights shall be used on all other construction signs.
 - Spacing of Construction Barrels shall be maximum of 25 ft on taper sections and 50 ft on tangent sections.
 - Spacing of Tubular Markers shall be maximum of 30 ft.

Legend

- Construction Barrels
- Tubular Markers
- Barricades
- Line, Remove
- 40 Temporary Pavement Marking, Removable, White, 4 in.
- 41 Temporary Pavement Marking, Removable, Yellow, 4 in.
- 42 Temporary Pavement Marking, Removable, Black, 5 in.

Worksite Speed Limit: 35 mph
Construction Zone Design Speed: 35 mph

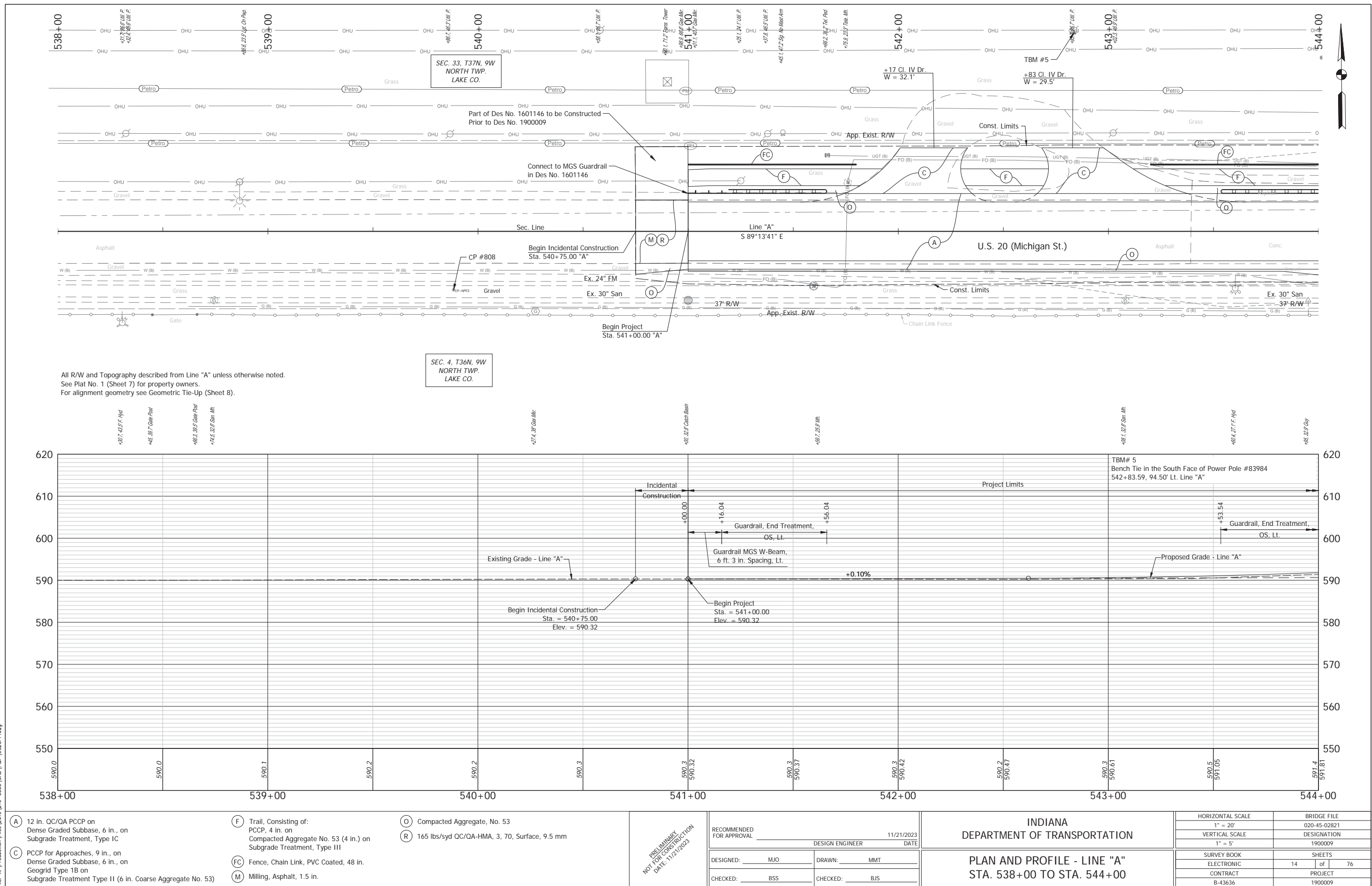
PRELIMINARY
NOT FOR CONSTRUCTION
DATE: 11/21/2023

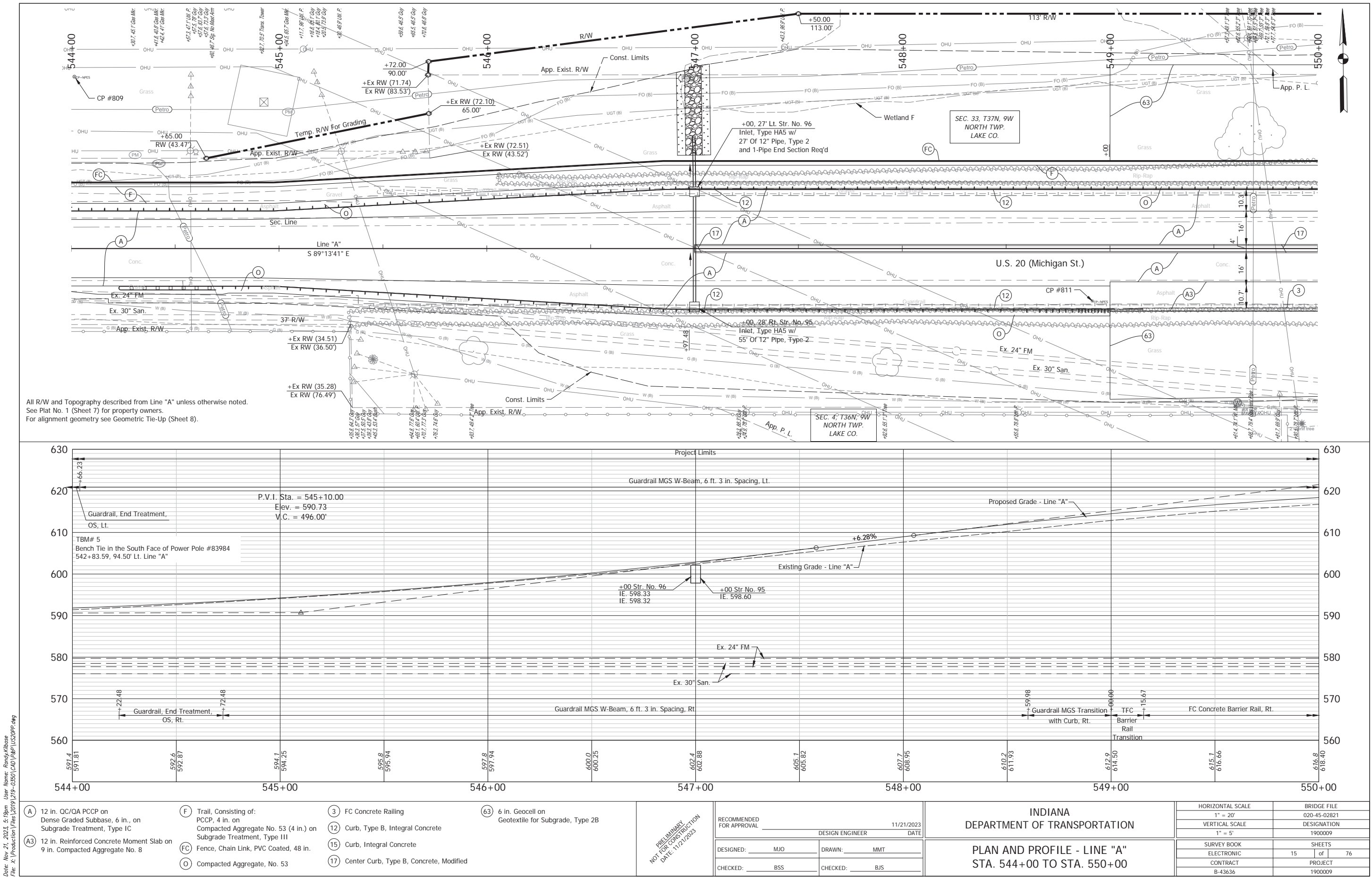
RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: DJM	DRAWN: DJM	
CHECKED: BSS	CHECKED: BJS	

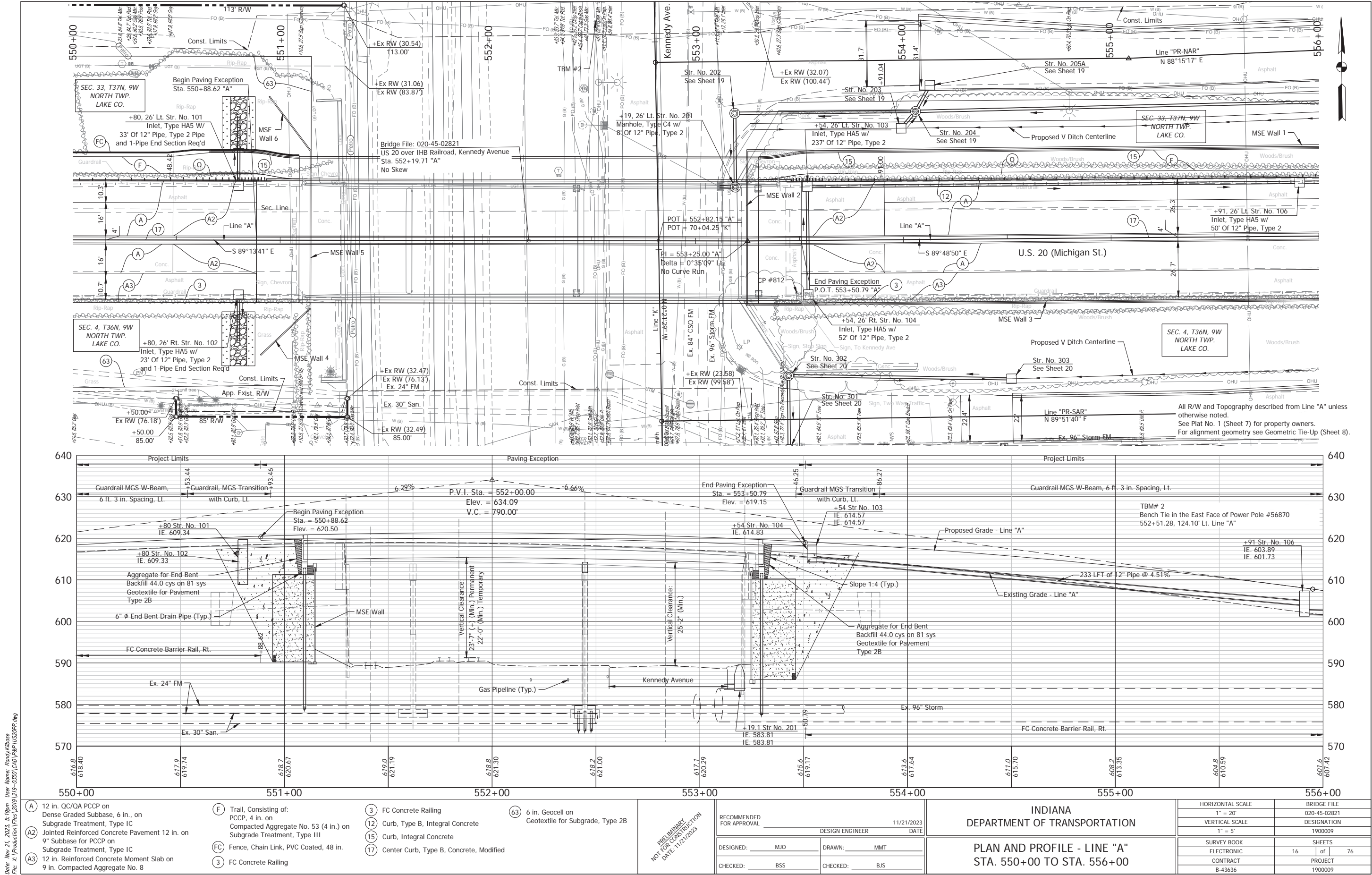
INDIANA
DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC
PHASE II

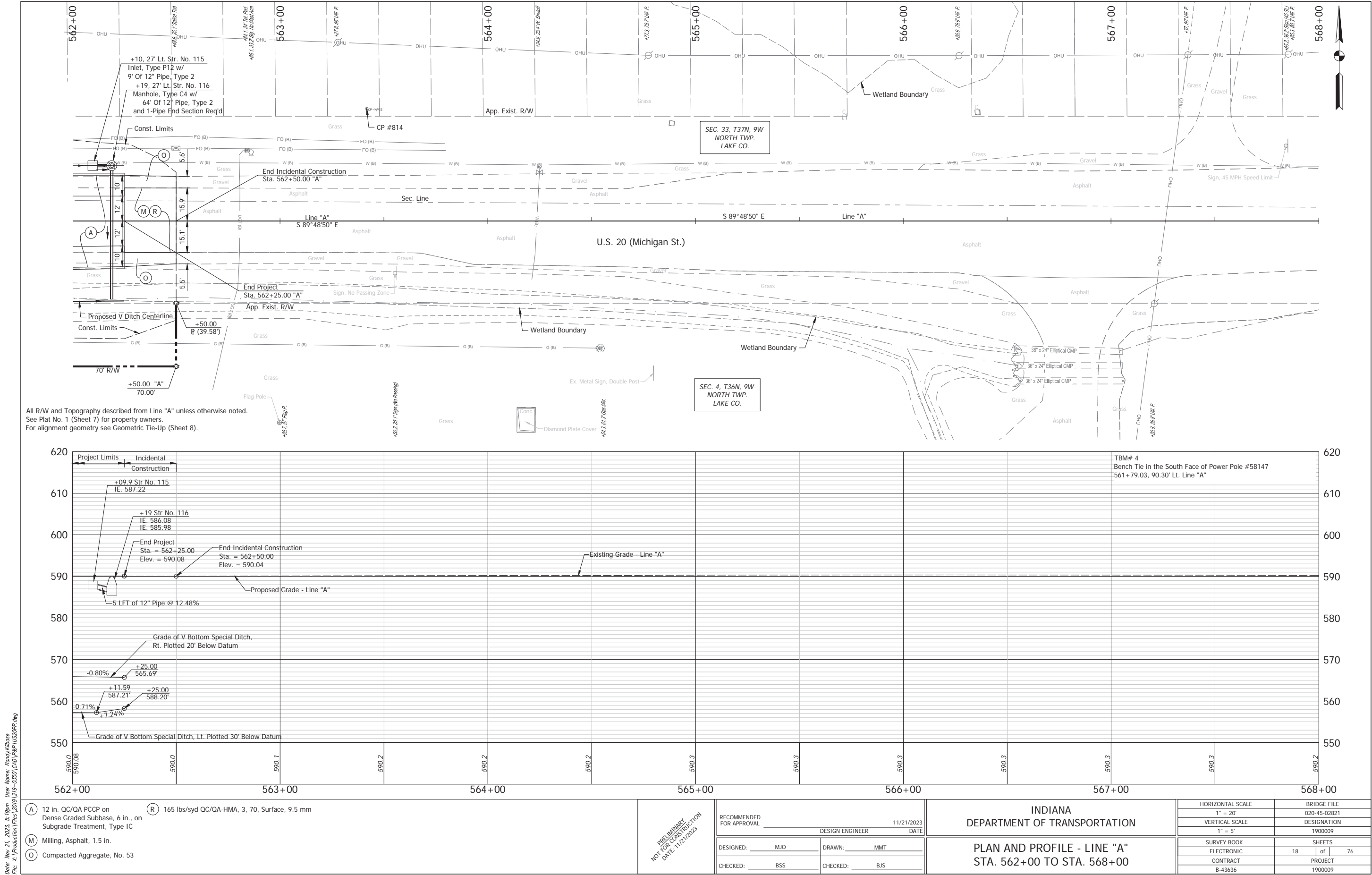
HORIZONTAL SCALE	BRIDGE FILE
AS NOTED	020-45-02821
VERTICAL SCALE	DESIGNATION
N/A	1900009
SURVEY BOOK	SHEETS
ELECTRONIC	13 of 76
CONTRACT	PROJECT
B-43636	1900009



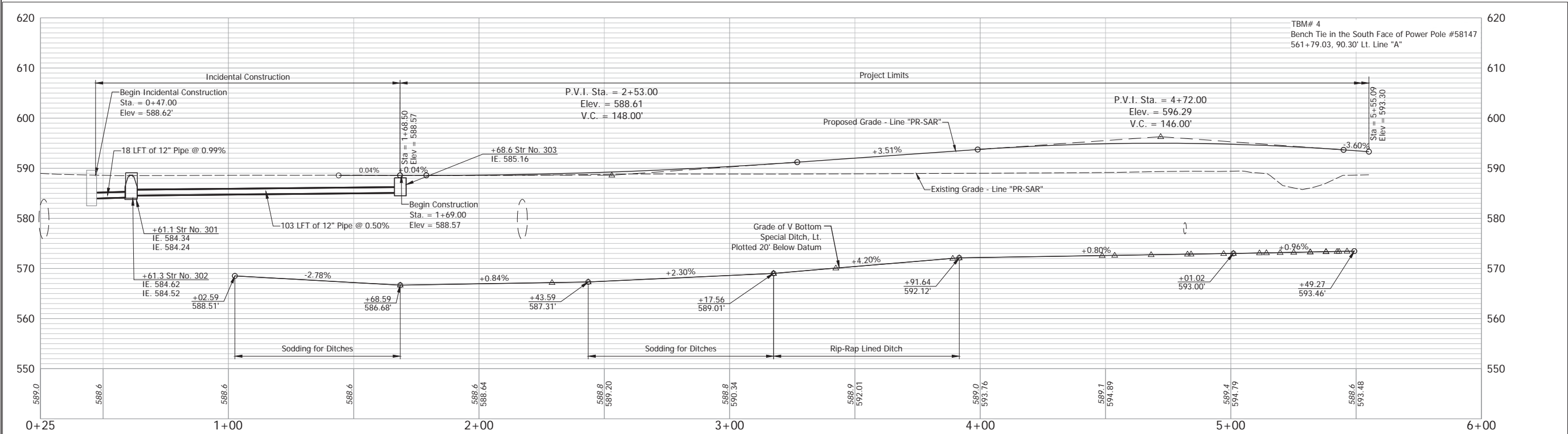
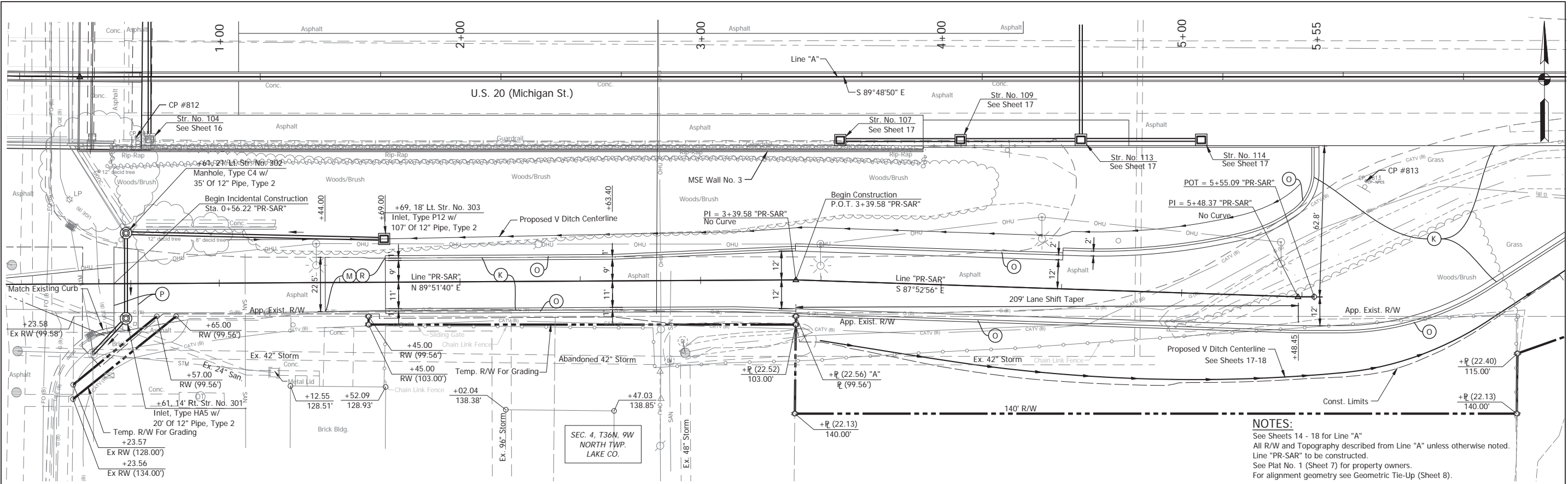




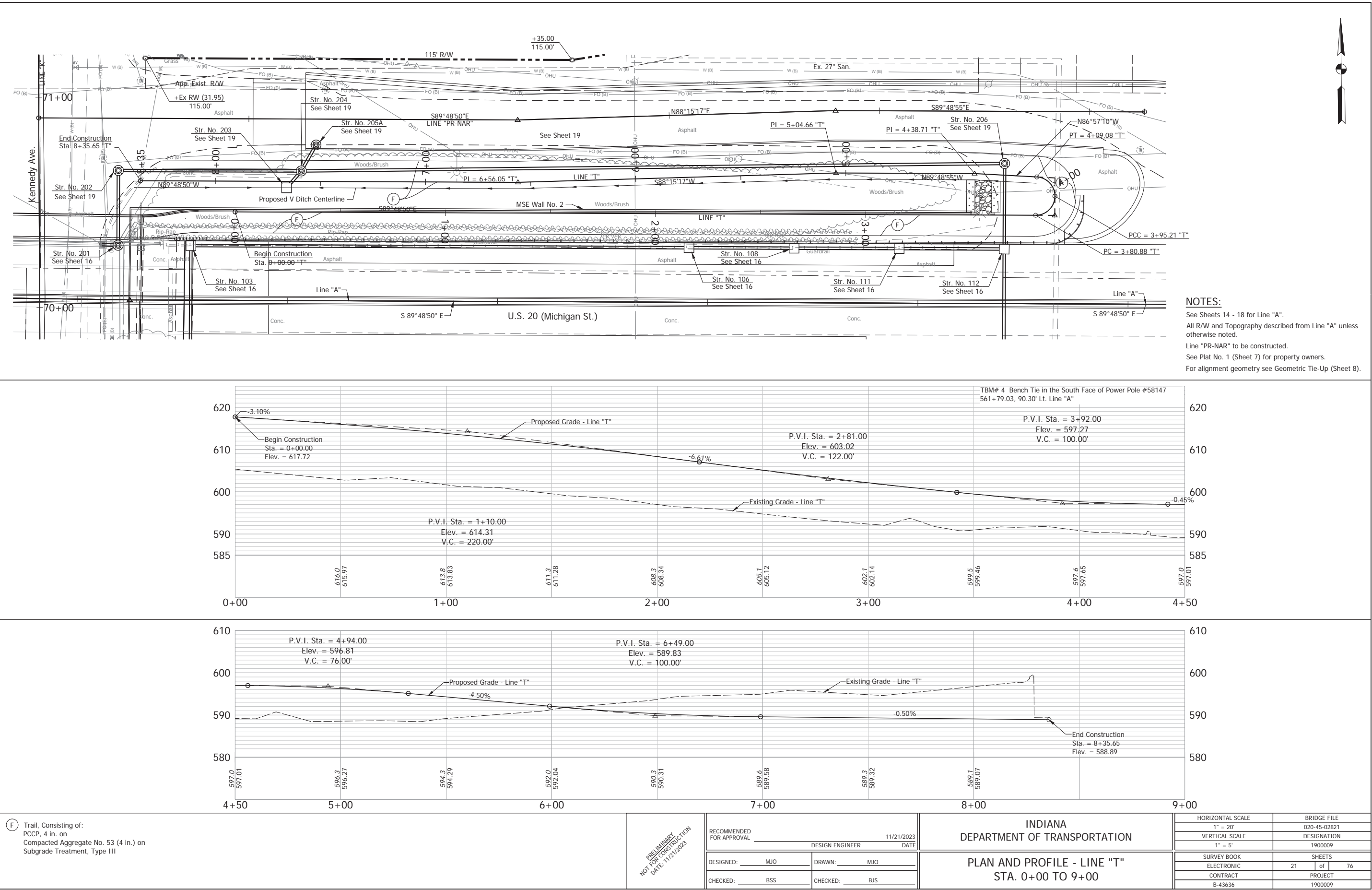
Date: Nov 21, 2023, 5:19pm
User Name: RandyKbase
File: K:\Production Files\2019\19-0350\CAD\1900009.dwg



Date: Nov 21, 2023, 5:19pm User Name: RandyKibase
File: K:\Production Files\2019\19-0350\CAD\18P\1020P.dwg



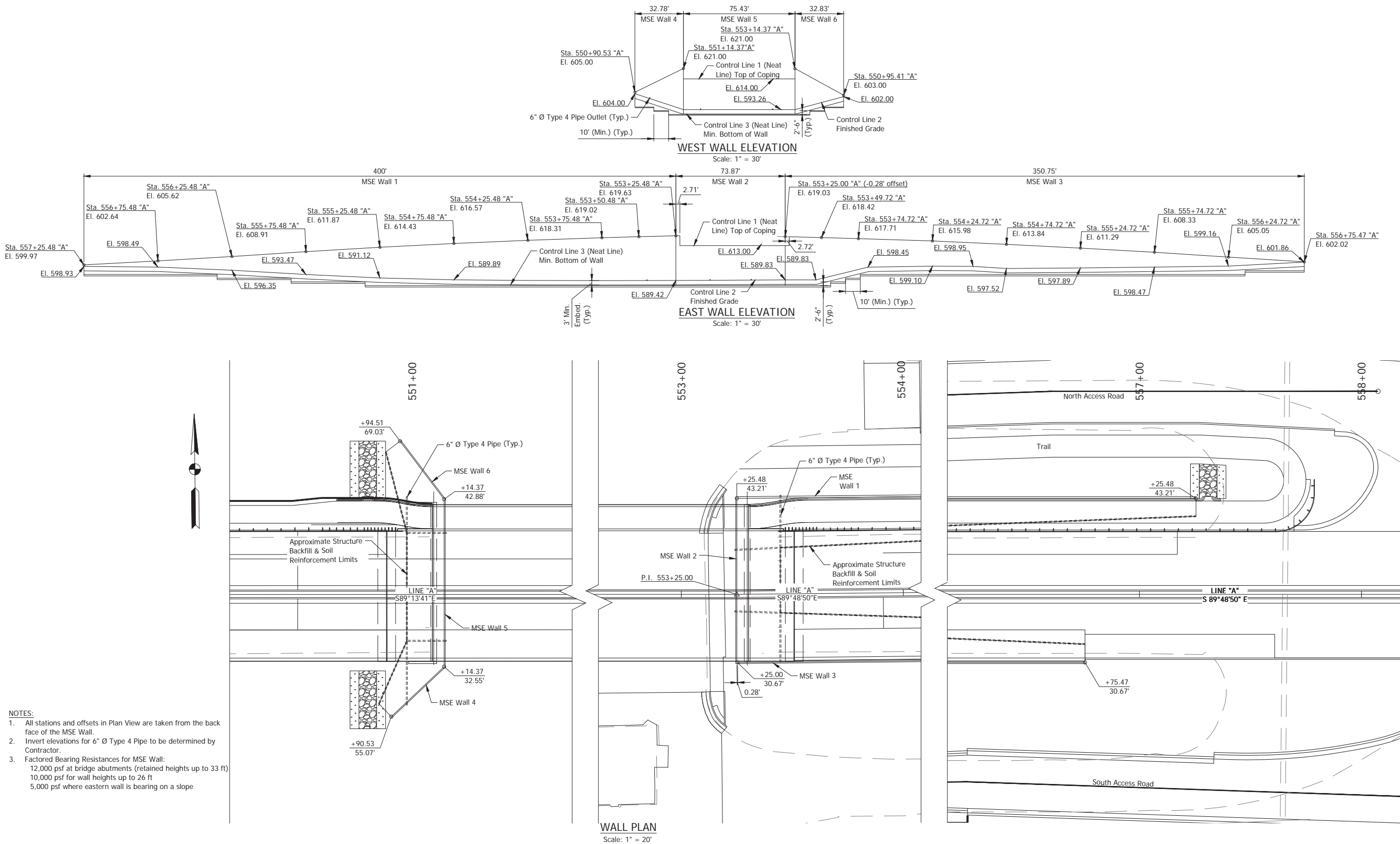
<div><div><div>(K) 165 lbs/syd QC/QA HMA, 3, 70, Surface, 9.5mm on 275 lbs/syd QC/QA HMA, 3, 70, Intermediate, 19mm on 880 lbs/syd QC/QA HMA, 3, 64, Base, 25mm on Subgrade Treatment, Type IC</div><div>(M) Milling, Asphalt, 1.5 in.</div><div>(O) Compacted Aggregate, No. 53</div></div><div><div>(P) HMA Patching, Full Depth, Type C, to be: 165 lbs/syd HMA Surface, Type C, on 330 lbs/syd HMA Intermediate, Type C, on 605 lbs/syd HMA Base, Type C, on Subgrade Treatment, Type IC</div><div>(R) 165 lbs/syd QC/QA-HMA, 3, 70, Surface, 9.5 mm</div></div></div>		<div>PRELIMINARY NOT FOR CONSTRUCTION DATE: 11/21/2023</div>	<div>RECOMMENDED FOR APPROVAL _____ 11/21/2023 DESIGN ENGINEER DATE</div>	<div>INDIANA DEPARTMENT OF TRANSPORTATION</div>	<div>HORIZONTAL SCALE 1" = 20'</div> <div>VERTICAL SCALE 1" = 10'</div>	<div>BRIDGE FILE 020-45-02821</div> <div>DESIGNATION 1900009</div>	
			<div>DESIGNED: _____ MJO</div> <div>DRAWN: _____ MMT</div>		<div>PLAN AND PROFILE - LINE "PR-SAR" STA. 1+00 TO STA. 6+00</div>	<div>SURVEY BOOK</div> <div>ELECTRONIC 20 of 76</div>	<div>SHEETS</div>
			<div>CHECKED: _____ BSS</div> <div>CHECKED: _____ BJS</div>			<div>CONTRACT B-43636</div>	<div>PROJECT 1900009</div>



Date: Nov 21, 2023, 5:25pm User Name: Randy Allbase
File: X:\ProductionFiles\2019\219-0350\CD\Bases\2190350GradingWork.dwg

NOTES:

1. All stations and offsets in Plan View are taken from the back face of the MSE Wall.
2. Invert elevations for 6" Ø Type 4 Pipe to be determined by Contractor.
3. Factored Bearing Resistances for MSE Wall:
12,000 psf at bridge abutments (retained heights up to 33 ft)
10,000 psf for wall heights up to 26 ft
5,000 psf where eastern wall is bearing on a slope

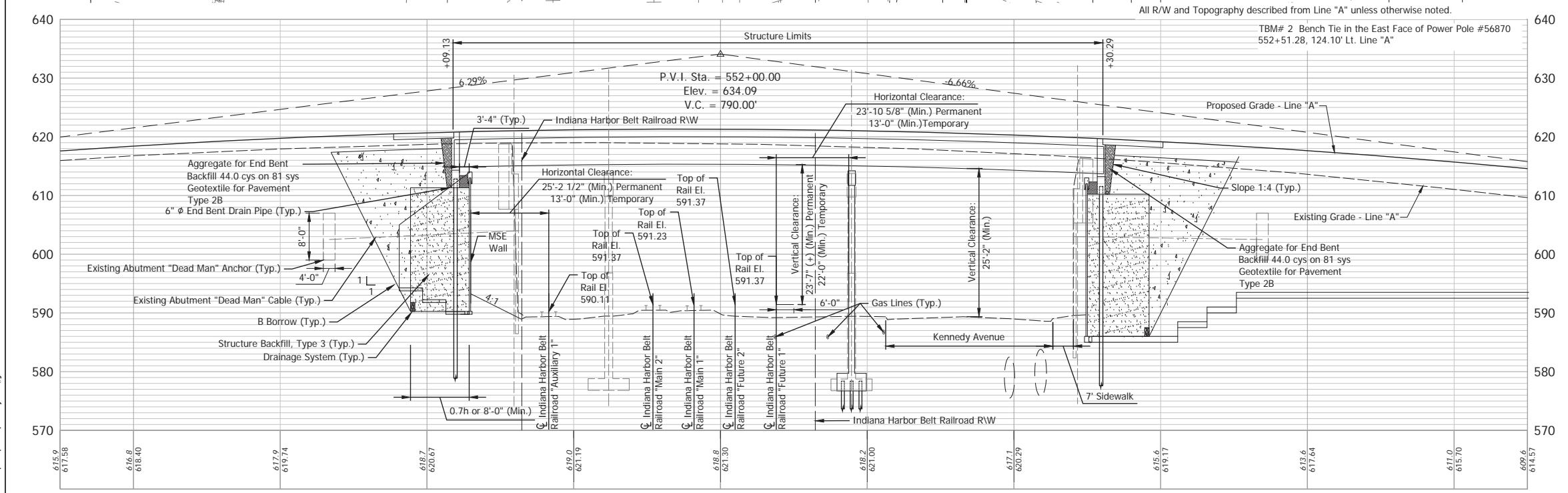
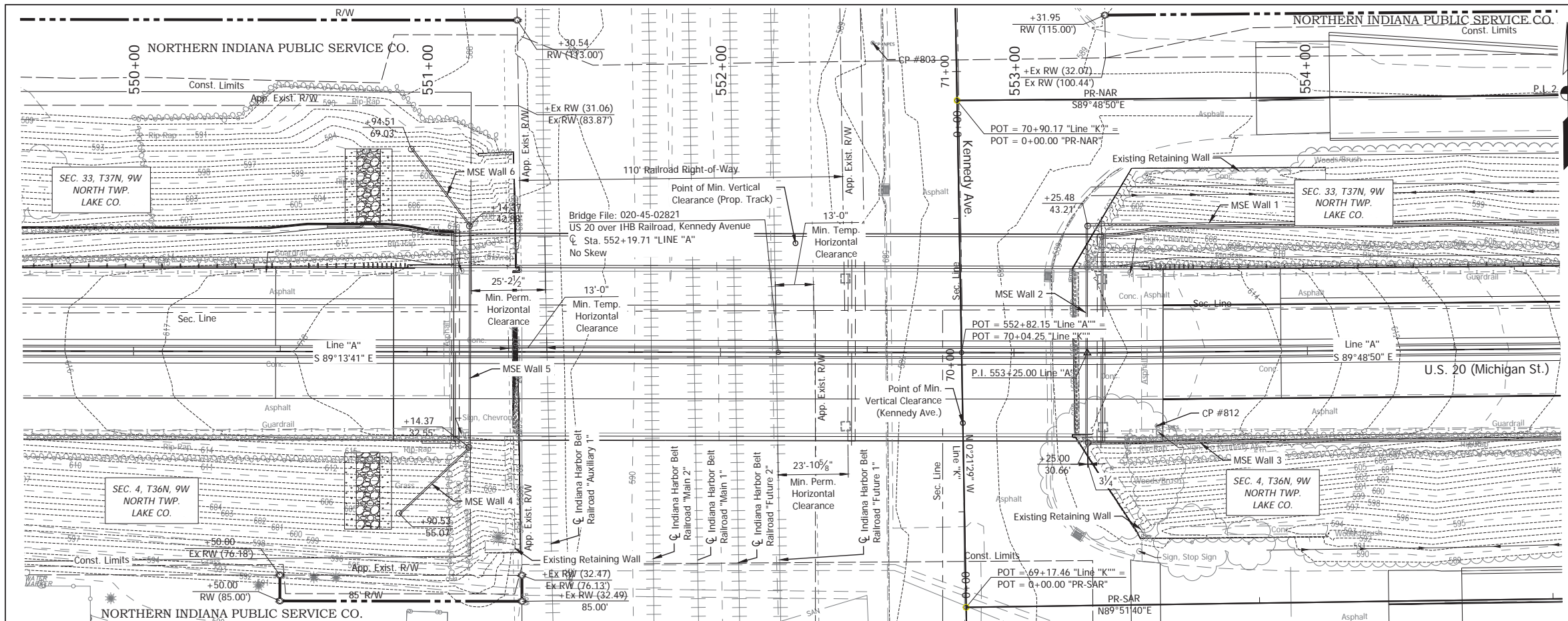


RECOMMENDED FOR APPROVAL		11/21/2023
DESIGN ENGINEER		DATE
DESIGNED: SCC	DRAWN: SCC	
CHECKED: JMH	CHECKED: JMH	

INDIANA
DEPARTMENT OF TRANSPORTATION

MSE WALL DETAILS

HORIZONTAL SCALE	BRIDGE FILE	
AS NOTED	020-45-02821	
VERTICAL SCALE	DESIGNATION	
AS NOTED	1900009	
SURVEY BOOK	SHEETS	
ELECTRONIC	26	of 76
CONTRACT	PROJECT	
B-43636	1900009	



POINT NUMBER/NAME: 803 5/8" Rebar w/ NPES Traverse Cap NORTHING: 2,317,576.279 EASTING: 2,849,321.575 1. Mag Nail, South Face, PP #56870, 1" AG 2. Back of Curb, 38.94', 177" Az 3. Mag Nail, East Face, PP #58154, 1" AG	POINT NUMBER/NAME: 812 Mag Nail w/ NPES Traverse Washer NORTHING: 2,317,424.605 EASTING: 2,849,416.834 1. SE Corner Bridge Apron, 50.91', 355" Az 2. NE Corner Bridge Apron, 50.91', 355" Az 3. Mag Nail, 1" South of Edge Conc., 21.84', 64" Az	POINT NUMBER/NAME: 813 5/8" Rebar w/ NPES Traverse Cap NORTHING: 2,317,424.746 EASTING: 2,849,927.910 1. Mag Nail, North Face, PP #83782, 43.33', 235" Az 2. Mag Nail, 1" South of Edge Conc., 26.85', 0" Az 3. Mag Nail, 1" South of Edge Conc., 39.00', 45" Az	POINT NUMBER/NAME: 816 5/8" Rebar w/ NPES Traverse Cap NORTHING: 2,317,584.785 EASTING: 2,849,906.191 1. "X" Cut, SW Edge, 6.5' Conc. PP Base, 29.10', 58" Az 2. Mag Nail, West Face, PP #58149, 1" AG, 49.04', 104" Az 3. Mag Nail, North Face, 59.94', 180" Az
BRIDGE APRON US 20 Edge of Concrete Edge of Asphalt	BRIDGE APRON US 20 Edge of Concrete Edge of Asphalt	BRIDGE APRON US 20 Edge of Concrete Edge of Asphalt	BRIDGE APRON US 20 Edge of Concrete Edge of Asphalt

EXISTING STRUCTURE

The existing structure (Str. No. 020-45-02484 B) is a three-span AASHTO Type III Prestressed I-Beam bridge built between 1937 and 1944 and reconstructed in 1978. The existing structure has spans of 32'-3", 82'-9", and 77'-0" and a clear roadway width of 52'-0". Existing structure to be removed.

EARTHWORK TABULATION	
FILL	--- CYS
FILL + 20%	--- CYS
COMMON EXCAVATION	--- CYS
USABLE WATERWAY EXCAVATION	--- CYS
BORROW	--- CYS
TOTAL WATERWAY EXCAVATION	--- CYS
EXCAVATION FOUNDATION UNCLASSIFIED	--- CYS
BENCHING (Estimated)	--- CYS
No direct payment for Benching. Benching will not be paid for as Common Excavation.	

CONTINUOUS COMPOSITE STEEL PLATE GIRDER BRIDGE

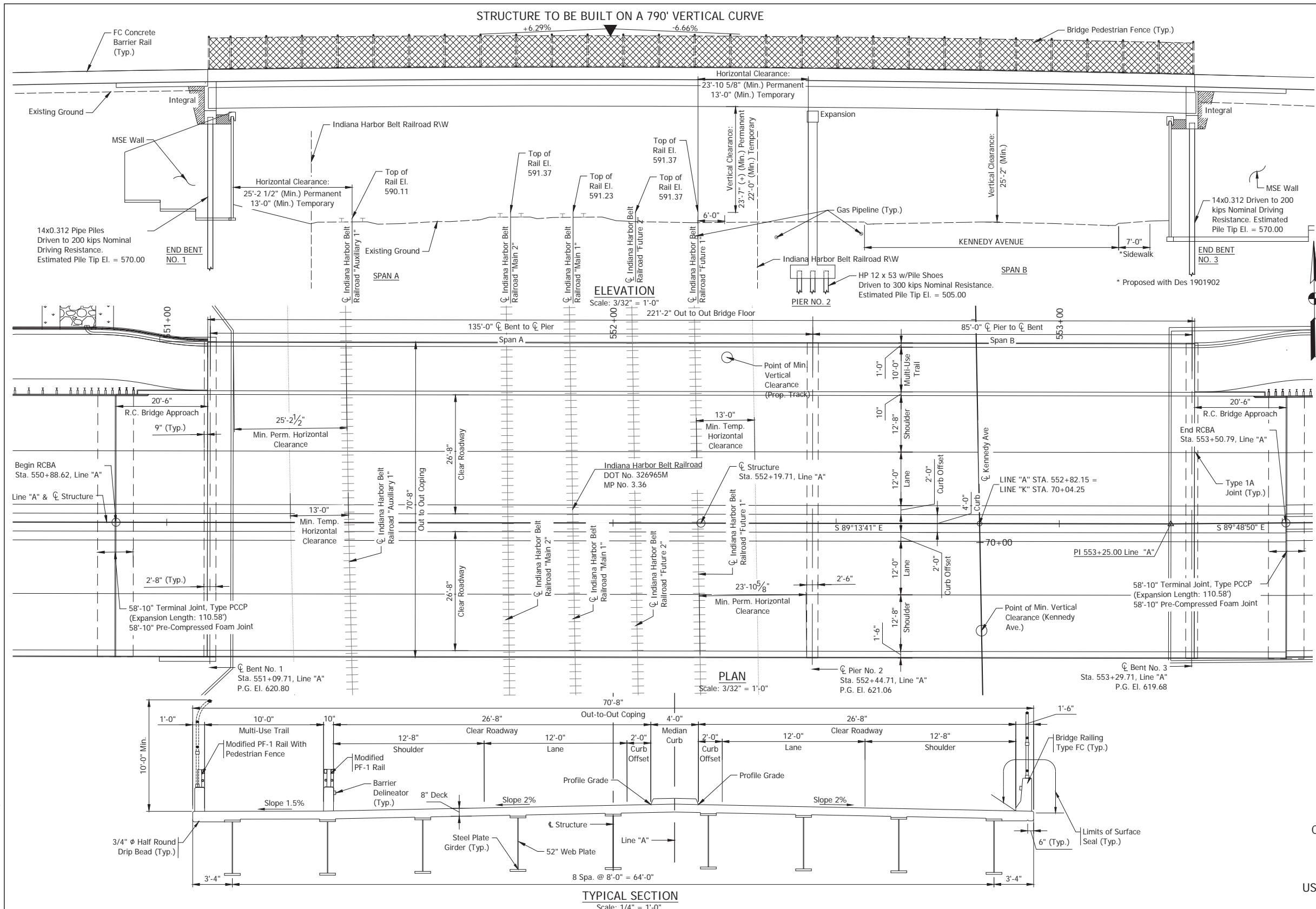
2 SPANS: 135'-0", 85'-0"

2 - 26'-8" CLEAR ROADWAY; NO SKEW

US 20 OVER IHB RAILROAD, KENNEDY AVE.

LAKE COUNTY

Date: Nov 21, 2023, 5:25pm User Name: Randy Wilson
File: K:\Production Files\19-0350\CD\Plans\01 Layout.dwg



GENERAL NOTES

Reinforcing steel cover shall be 2 1/2" in top and 1" minimum in bottom of floor slab, 3" in footings, except bottom steel which shall be 4", and 2" in all other parts, unless noted.

DESIGN DATA

Superstructure and substructure designed for HL-93 loading, in accordance with the AASHTO LRFD Design Specifications, Ninth Edition, 2020 and Subsequent Interims & Toll Road Live Load per INDOT Design Manual.

DEAD LOAD

Actual weight plus 35 lb/ft² for future wearing surface and 15 lb/ft² for permanent metal deck forms.

FLOOR SLAB

Designed with a 7 1/2" structural depth plus 1/2" sacrificial wearing surface.

DESIGN STRESSES

CONCRETE

Class C f'c = 4000 psi
Class B f'c = 3000 psi
Class A f'c = 3500 psi

REINFORCING STEEL

Grade 60 f'y = 60,000 psi

CONSTRUCTION LOADING

The exterior girder has been checked for strength, deflection, and overturning using the construction loads shown below. Cantilever overhang brackets were assumed for support of the deck overhang past the edge of the exterior girder. The finishing machine was assumed to be supported 6 in. outside the vertical coping form. The top overhang brackets were assumed to be located 6 in. past the edge of the vertical coping form. The bottom overhang brackets were assumed to be braced against the intersection of the girder bottom flange and web.

DECK FALSEWORK LOADS

Designed for 15 lb/ft² for permanent metal stay-in-place deck forms, removable deck forms, and 2-ft exterior walkway.

CONSTRUCTION LIVE LOAD

Designed for 20 lb/ft² extending 2-ft past the edge of coping and 75 lb/ft vertical force applied at a distance of 6 in. outside the face of coping over a 30-ft length of the deck centered with the finishing machine.

FINISHING-MACHINE LOAD

4500 lb distributed over 10 ft along the coping.

WIND LOAD

Designed for 70 mph horizontal wind loading in accordance with LRFD 3.8.1.

SEISMIC DESIGN DATA

Seismic Design Category: A
Acceleration Coefficient: 0.090
Seismic Soil Profile Type: Class D

CONTINUOUS COMPOSITE STEEL PLATE GIRDER BRIDGE
2 SPANS: 135'-0", 85'-0"
2 - 26'-8" CLEAR ROADWAY; NO SKEW
US 20 OVER IHB RAILROAD, KENNEDY AVE.
LAKE COUNTY

Date: Nov 21, 2023, 5:26pm User Name: Randy Allbase
File: X:\Production\Files\2015\1215-0350\CD\Trans\SR General Plan.dwg

DATE	REVISION	PRELIMINARY NOT FOR CONSTRUCTION DATE: 11/21/2023	RECOMMENDED FOR APPROVAL DESIGN ENGINEER DATE: 11/21/2023	INDIANA DEPARTMENT OF TRANSPORTATION GENERAL PLAN	HORIZONTAL SCALE	BRIDGE FILE
					AS NOTED	020-45-02821
					VERTICAL SCALE	DESIGNATION
					AS NOTED	1900009
					SURVEY BOOK	SHEETS
					ELECTRONIC	28 of 76
					CONTRACT	PROJECT
					B-43636	1900009

[illegible]

PRELIMINARY
NOT FOR CONSTRUCTION
DATE: 11/21/2023

APPROACH TABLE

B38

LOCATION	LINE PAINT		MULTI-COMPONENT				THERMOPLASTIC				TRANSVERSE MARKINGS THERMOPLASTIC STOP LINE		MULTI-COMPONENT TRANSVERSE MARKINGS CROSSHATCH LINE		THERMOPLASTIC TRANSVERSE MARKINGS CROSSHATCH LINE		PAVEMENT MESSAGE THERMOPLASTIC LANE INDICATION ARROW	PAVEMENT MESSAGE THERMOPLASTIC WORD "ONLY"	SNOWPLOWABLE RAISED PAVEMENT MARKERS
	SOLID WHITE 6 in	SOLID YELLOW 6 in	SOLID WHITE 6 in	SOLID YELLOW 6 in	SOLID WHITE 8 in	SOLID YELLOW 8 in	SOLID WHITE 6 in	BROKEN YELLOW 6 in	BROKEN WHITE 8 in	BROKEN YELLOW 8 in	SOLID WHITE 12 in	SOLID WHITE 24 in	SOLID YELLOW 8 in	SOLID YELLOW 12 in	SOLID WHITE 4 in	SOLID WHITE 8 in			
	ft	ft	ft	ft	ft	ft	ft	ft	ft	ft	ft	ft	ft	ft	ft	ft			
Line "A"																	EACH	EACH	EACH
540+75.00 to 544+97.42			845	845															
544+97.42 to 546+97.48			405	405											227				
546+97.48 to 550+88.62			782	777															
550+88.62 to 553+50.82			524	524															
553+50.82 to 559+50.00			1,262	1,198								16							
559+50.00 to 560+94.00			494	288								12		243					
560+94.00 to 562+50.00			293	312										66					
Line "NAR"																			
1+02.00 to 5.26.21								846											
Ramp WB							171												
Ramp EB							223												
Line "SAR"																			
1+44.00 to 5+55.09								844											
Ramp WB							271												
Ramp EB							208												
Total			4,606	4,350			872	1,690				28	536						

[illegible][illegible][illegible]

NOTE TO REVIEWER
Incomplete Summary Tables will be filled in for later submittals.

Date: Nov 21, 2023, 5:26pm User Name: Randy.Kilbase
File: X:\Production\Files\2019\219-0350\CAD\Plans\Sht Road Summary.dwg

<div>PRELIMINARY</div> <div>NOT FOR CONSTRUCTION</div> <div>DATE: 11/21/2023</div>	<div>RECOMMENDED FOR APPROVAL</div> <div>11/21/2023</div> <div>DESIGN ENGINEER</div> <div>DATE</div>		<div>INDIANA</div> <div>DEPARTMENT OF TRANSPORTATION</div>		<div>HORIZONTAL SCALE</div> <div>BRIDGE FILE</div> <div>N/A</div> <div>020-45-02821</div> <div>VERTICAL SCALE</div> <div>DESIGNATION</div> <div>N/A</div> <div>1900009</div>	
	<div>DESIGNED: JRM</div> <div>DRAWN: JRM</div>		<div>ROAD SUMMARY</div>		<div>SURVEY BOOK</div> <div>SHEETS</div> <div>ELECTRONIC</div> <div>32 of 76</div> <div>CONTRACT</div> <div>PROJECT</div> <div>B-43636</div> <div>1900009</div>	
	<div>CHECKED: BSS</div> <div>CHECKED: BJS</div>					

[illegible]

Date: Nov 21, 2023, 5:27pm User Name: Randy.Kilbase
File: X:\Production\Files\2019\219-0350\CAD\Plans\Sst Road Summary.dwg

PRELIMINARY NOT FOR CONSTRUCTION DATE: 11/21/2023	RECOMMENDED FOR APPROVAL _____ 11/21/2023 _____ DESIGN ENGINEER _____ DATE		INDIANA DEPARTMENT OF TRANSPORTATION		HORIZONTAL SCALE N/A		BRIDGE FILE 020-45-02821	
					VERTICAL SCALE N/A		DESIGNATION 1900009	
	DESIGNED: _____ JRM _____		DRAWN: _____ JRM _____		SURVEY BOOK ELECTRONIC		SHEETS 33 of 76	
	CHECKED: _____ BSS _____		CHECKED: _____ BJS _____		CONTRACT B-43636		PROJECT 1900009	

Date: Nov 21, 2023, 5:27pm User Name: Randy Albase
File: X:\Production\Files\2019\219-0350\CD\Plans\SR Road Summary.dwg

PIPE MATERIALS																									
STRUCTURE NUMBER		95	96	101	102	103	104	106	107	108	109	111	112	113	114	115	116	201	202	203	204	205A	206	301	302
PIPE TYPE / SHAPE (CIR or DEF)		2 / Circular	2 / Circular	2 / Circular	2 / Circular	2 / Circular	2 / Circular	2 / Circular	2 / Circular	2 / Circular	2 / Circular	2 / Circular	2 / Circular	2 / Circular	2 / Circular	2 / Circular	2 / Circular	2 / Circular	2 / Circular	2 / Circular	2 / Circular	2 / Circular	2 / Circular	2 / Circular	2 / Circular
SMOOTH PIPE SIZE		12"	12"	12"	12"	12"	12"	12"	12"	12"	12"	12"	12"	12"	12"	12"	12"	12"	12"	12"	12"	12"	12"	12"	12"
CORRUGATED PIPE SIZE																									
SEMI-SMOOTH PIPE SIZE																									
RCP/RCHPE (S)	CLASS	II	II	II	II	II	II	II	II	II	II	II	II	II	II	II	II	II	II	II	II	II	II	II	II
	D _{0.01} RATING	1000	1000	1000	1000	1000	1000	1000	1000	1000	1000	1000	1000	1000	1000	1000	1000	1000	1000	1000	1000	1000	1000	1000	1000
NON-REINFORCED CONCRETE PIPE, CLASS 3 (S)																									
CORRUGATED PE PIPE, TYPE S (S)*		X	X	X	X	X	X	X	X	X	X	X	X	X	X		X	X	X		X	X	X		X
PROFILE WALL (RIBBED) PE PIPE (S)*																									
PROFILE WALL (CLOSED) PE PIPE (S)*																									
SMOOTH WALL PE PIPE (S)* / MAXIMUM DR		X / 26	X / 26	X / 26	X / 26	X / 26	X / 26	X / 26	X / 26	X / 26	X / 26	X / 26	X / 26	X / 26	X / 26		X / 26	X / 26	X / 26		X / 26	X / 26	X / 26		X / 26
CORRUGATED PP PIPE (S)		X	X	X	X	X	X	X	X	X	X	X	X	X	X		X	X	X		X	X	X		X
PROFILE WALL PVC PIPE (S)		X	X	X	X	X	X	X	X	X	X	X	X	X	X		X	X	X		X	X	X		X
SMOOTH WALL PVC PIPE (S)*		X	X	X	X	X	X	X	X	X	X	X	X	X	X		X	X	X		X	X	X		X
VITRIFIED CLAY PIPE, EXTRA STRENGTH (S)																									
FULLY BIT. PAVED & LINED (S)	CORR. PROFILE THICKNESS																								
ZINC COATED (C)	CORR. PROFILE THICKNESS																								
ZINC COATED W/ BPI (C)	CORR. PROFILE THICKNESS																								
ALUM. COATED TYPE 2 (C)	CORR. PROFILE THICKNESS																								
POLYMER PRECOATED GALVANIZED (C)	CORR. PROFILE THICKNESS																								
POLYMER PRECOATED GALVANIZED	CORR. PROFILE THICKNESS																								
CORRUGATED STEEL PIPE TYPE 1A (S)	CORR. PROFILE THICKNESS																								
CORRUGATED ALUM. ALLOY (C)	CORR. PROFILE THICKNESS																								
CORRUGATED ALUM. ALLOY W/ BPI (C)	CORR. PROFILE THICKNESS																								
ZINC COATED (SS)	RIB PROFILE THICKNESS																								
ZINC COATED W/ BPI (SS)	RIB PROFILE THICKNESS																								
ALUM. COATED TYPE 2 (SS)	RIB PROFILE THICKNESS																								
POLYMER PRECOATED GALVANIZED (SS)	RIB PROFILE THICKNESS																								
STR. PLATE ALUMINUM ALLOY (C)	CORR. PROFILE THICKNESS																								
STR. PLATE ALUMINUM ALLOY W/ CFP (C)	CORR. PROFILE THICKNESS																								
STR. PLATE STEEL (C)	CORR. PROFILE THICKNESS **																								
STR. PLATE STEEL W/ CFP (C)	CORR. PROFILE THICKNESS **																								

LEGEND

PIPE MATERIAL

- RCP Reinforced Concrete Pipe
- RCHEP Reinforc
- PE Polyethyl
- DR Dimension Ratio
- PVC Polyvinyl Chloride
- PP Polypropylene
- CORR Corrugation
- ALUM Aluminum
- STR Structural
- (LS) Lock Seam Pipe Required

PIPE PROTECTION

- BPI Bituminous Paved Invert
- CFP Concrete Field Paving
- BIT Bituminous

SHAPE

- CIR Circular Pipe
- DEF Deformed Pipe

INTERIOR DESIGNATION

- (S) Smooth Pipe Material
- (C) Corrugated Pipe Material
- (SS) Semi-Smooth Pipe Material

PIPE SIZE

- Circular pipe is shown as diameter in inches
- Deformed pipe is shown as area in square feet

PRELIMINARY
NOT FOR CONSTRUCTION
DATE: 11/21/2023

RECOMMENDED FOR APPROVAL _____ 11/21/2023
DESIGN ENGINEER DATE

DESIGNED: _____ JRM DRAWN: _____ JRM

CHECKED: _____ BSS CHECKED: _____ BJS

INDIANA
DEPARTMENT OF TRANSPORTATION

PIPE MATERIAL TABLE

HORIZONTAL SCALE _____ BRIDGE FILE
NA 020-45-02821
VERTICAL SCALE _____ DESIGNATION
NA 1900009

SURVEY BOOK _____ SHEETS
ELECTRONIC 34 of 76
CONTRACT _____ PROJECT
B-43636 1900009

Categorical Exclusion

Appendix C

Early Coordination



INDIANA DEPARTMENT OF TRANSPORTATION

100 North Senate Avenue
Room N758-ES
Indianapolis, Indiana 46204

PHONE: (855) 463-6848

Eric Holcomb, Governor
Michael Smith, Commissioner

Date: September 8, 2022

SAMPLE LETTER

Re: Early Coordination Letter, Des. No. 1900009, Bridge Project over Kennedy Avenue on US 20, 0.99 mi E of SR 152, Lake County, Indiana

To whom it may concern:

The Indiana Department of Transportation (INDOT) LaPorte District, with funding from the Federal Highway Administration, intends to proceed with the following bridge project along US 20 in Lake County, Indiana (Des. No. 1900009).

This letter is part of the early coordination phase of the environmental review. At this time, we are requesting comments from your area of expertise regarding any possible environmental effects (social and natural) associated with this project. **Please use the above Des. No. and project description in your reply.** Your comments will be incorporated into the formal environmental study. Your cooperation in this endeavor is appreciated.

Project Location and Existing Conditions

The proposed project is located along US 20, 0.99 mi. E of SR 152, along the southern limits of the city of East Chicago and the northern limits of the city of Hammond. Specifically, the project is located in Section 4, Township 36 North, Range 9 West; and Section 33, Township 37 North, Range 9 West in North Township as depicted on the Highland Quadrangle of the U.S. Geological Survey 1:24,000 scale map. Adjacent land use is urban and includes industrial areas.

US 20 is functionally classified as a Principal Arterial. The typical cross section of US 20 consists of two 12-foot lanes with 14-foot paved shoulders on either side. The posted speed limit along US 20 within the project area is 45 miles per hour (MPH). Kennedy Avenue is functionally classified as a Minor Arterial. The typical cross section of Kennedy Avenue consists of four 12-foot minimum lanes (two in each direction) surrounded by curb and gutter. The posted speed limit along Kennedy Avenue within the project area is 35 MPH. US 20 is connected to Kennedy Avenue by way of entry/exit ramps that are present north and south of US 20, east of Kennedy Avenue.

The existing bridge, Structure No. 020-45-02484 B, conveys US 20 over Kennedy Avenue and the Indiana Harbor Belt Railroad (IHB RR). The existing bridge is a prestressed concrete continuous beam bridge with a concrete cast-in-place deck built in 1934 and most recently reconstructed in 1978. The bridge is three spans totaling 192 feet in length with a clear roadway width of 52 feet.

Purpose and Need

The need for the project is evidenced by the deteriorated condition of the existing structure. The most recent INDOT Bridge Inspection Report from January 2021, noted that the abutment wall and northeast wing wall are exhibiting cracking. The superstructure was noted to have minor chipping on the beam edges in spans. The superstructure was given a condition rating of 6, which is considered to be in "satisfactory" condition and indicates minor deterioration. The substructure was given a condition rating

3502 Woodview Trace, Suite 150
Indianapolis, Indiana 46268
PHONE: 317.222.3878 • TOLL FREE: 800.423.7422

of 5 which is considered “fair” condition and indicates minor section loss. Condition ratings range from 0, which indicates a failing structure, to 9, which indicates a new structure with no deficiencies. The purpose of the project is to improve the condition of the structure to a rating of 8 or greater, which represents a structure in “very good” condition.

Proposed Project

The preferred alternative involves replacement of the existing bridge with a two-span continuous composite steel plate girder structure with MSE wall abutments and a reinforced concrete pier. The new bridge will have an out-to-out length of approximately 222 feet and a clear roadway width of 48 feet. A 4-foot-wide curbed center median will be installed between the eastbound and westbound traffic lanes along US 20. A raise in the profile grade will be necessary to provide adequate vertical clearance over the railroad and adequate stopping sight distance on US 20. Additionally, the US 20 roadway grade east of the bridge will require modification to achieve proper stopping sight distance at the intersection with the entry/exit ramps accessing Kennedy Avenue.

The maintenance of traffic (MOT) plan is expected to involve complete closure of US 20 with a detour. The proposed detour route utilizes US 20 (one mile west of the bridge) and US 12. Access to local businesses shall be maintained at all times. Kennedy Avenue will remain open during construction but will be reduced to two lanes (one in each direction). The MOT will be implemented per the *Indiana Design Manual* guidelines.

Right-of-Way (ROW)

It is anticipated that additional permanent right-of-way (ROW) may need to be acquired to accommodate the proposed improvements. Most of the new ROW, if necessary, would be acquired east of Kennedy Avenue and would accommodate any necessary realignments or grade adjustments for the US 20 entry/exit ramps. New permanent ROW amounts are not likely to exceed 0.5 acre. No relocations are anticipated. Tree clearing will likely be necessary.

Environmental Resources

A Red Flag Investigation (RFI) was performed within a 0.5-mile radius around the project area. Several “Red Flag” resources of concern were identified; however, not all will be impacted by the proposed project. These resources will be monitored throughout project development.

Lochmueller Group conducted a field investigation of the project area on October 19, 2021. During the investigation, six wetlands were identified. Preparation of a Waters of the U.S. Determination Report will be required for this project. Design will seek to avoid or minimize impacts to the identified wetland resources.

Cultural Resources

Coordination will occur with INDOT Cultural Resources Office (CRO) to investigate the project area for archaeological and historic resources and for Section 106 compliance. The results of this investigation will be forwarded to the State Historic Preservation Officer (SHPO) for review and concurrence as appropriate.

Range-wide Informal Programmatic Consultation

Lake County is within the range of the federally endangered Indiana bat (*Myotis sodalis*) and the federally threatened northern long-eared bat (NLEB) (*Myotis septentrionalis*). The project is anticipated to qualify

for the U.S. Fish and Wildlife Service (USFWS) Range-wide Programmatic Informal Consultation for the Indiana bat and the NLEB.

Early Coordination

This letter is part of the early coordination review process. You are asked to review this information and provide any comments you may have relative to anticipated impacts of the project on areas in which you have jurisdiction or special expertise. We will incorporate your comments into a study of the project's environmental impacts. To facilitate the development of this project, you are asked to reply within 30 calendar days of receipt of this letter. However, should you find that an extension to the response time is necessary, a reasonable amount may be granted upon request.

If you have any questions regarding this project, please feel free to contact me at (574) 334-5464 or at CCunningham@lochgroup.com. Additionally, should you want to contact the sponsor of this project, INDOT LaPorte District, please contact Andrew Borden at (219) 851-4481 or at aborden1@indot.in.gov.

Thank you in advance for your input.

Sincerely,



C.J. Cunningham
Environmental Project Manager
Lochmueller Group, Inc.

Attachments:

• Location Map	
• USGS Quad Map	<i>Removed to avoid duplication, refer to Appendix B</i>
• Aerial Map	
• Red Flag Investigation Maps	<i>Removed to avoid duplication, refer to Appendix E</i>
• Photo Location Map and Project Photographs	<i>Removed to avoid duplication, refer to Appendix B</i>

Distribution List

- FHWA – Indiana Division
- Natural Resources Conservation Service, Indianapolis Office
- U.S. Housing and Urban Development
- U.S. Army Corps of Engineers, Chicago District
- National Park Service
- INDOT, LaPorte District, Project Manager
- INDOT, LaPorte District, Environmental Section Manager
- INDOT, Environmental Service Division
- INDOT Aviation
- INDOT Utilities and Railroads
- Indiana Department of Natural Resources, Division of Fish and Wildlife
- Hammond City Engineer
- Hammond Fire Department
- Hammond Public Works

- Hammond Mayor's Office
- Hammond Common Council
- School City of Hammond
- Sanitary District of Hammond (MS4)
- East Chicago City Engineer
- East Chicago Public Works
- East Chicago Fire Department
- East Chicago Mayor's Office
- East Chicago Common Council
- School City of East Chicago
- East Chicago Stormwater Quality Management (MS4)
- Lake County Emergency Management Agency
- Lake County 911 Department
- Lake County Sheriff Department
- Lake County Highway Department
- Lake County Board of Commissioners
- Lake County Council
- Northwestern Indiana Regional Planning Commission
- East Chicago Parks and Recreation
- Hammond Parks and Recreation
- Indiana Geological and Water Survey
- Lake County MS4 Coordinator



Organization and Project Information

Project ID:

Des. ID:

Des. No. 1900009

Project Title:

US 20 over Kennedy Ave., Bridge Project

Name of Organization:

Lochmueller Group

Requested by:

C.J. Cunningham

Environmental Assessment Report

1. Geological Hazards:

- High liquefaction potential
- Floodway

2. Mineral Resources:

- Bedrock Resource: High Potential
- Sand and Gravel Resource: Low Potential

3. Active or abandoned mineral resources extraction sites:

- None documented in the area

*All map layers from Indiana Map (maps.indiana.edu)

DISCLAIMER:

This document was compiled by Indiana University, Indiana Geological Survey, using data believed to be accurate; however, a degree of error is inherent in all data. This product is distributed "AS-IS" without warranties of any kind, either expressed or implied, including but not limited to warranties of suitability to a particular purpose or use. No attempt has been made in either the design or production of these data and document to define the limits or jurisdiction of any federal, state, or local government. The data used to assemble this document are intended for use only at the published scale of the source data or smaller (see the metadata links below) and are for reference purposes only. They are not to be construed as a legal document or survey instrument. A detailed on-the-ground survey and historical analysis of a single site may differ from these data and this document.

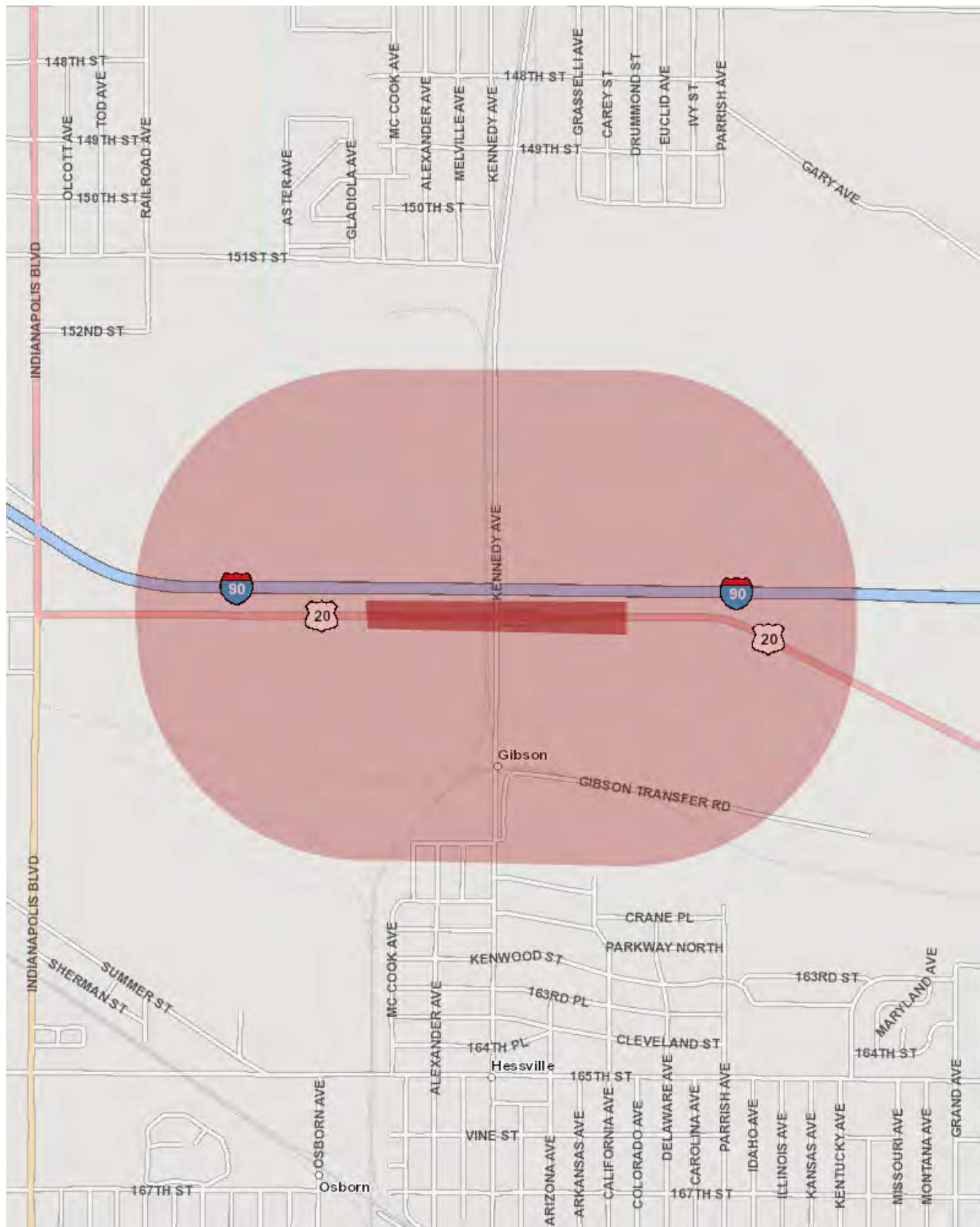
This information was furnished by Indiana Geological Survey

Address: 420 N. Walnut St., Bloomington, IN 47404

Email: IGSEnvir@indiana.edu

Phone: 812 855-7428

Date: September 07, 2022





United States
Department of
Agriculture

Farm
Production
and
Conservation

Natural
Resources
Conservation
Service

Indiana State Office
6013 Lakeside Boulevard
Indianapolis, Indiana 46278
317-295-5800

September 8, 2022

CJ Cunningham
Lochmueller Group
112 W Jefferson Blvd, Suite 500
South Bend, Indiana 46601

Dear Mr. Cunningham:

The proposed Bridge Project in Lake County, Indiana (Des. No. 1900009), as referred to in your letter received September 8, 2022, will not cause a conversion of prime farmland.

If you need additional information, please contact John Allen at 317-295-5859 or john.allen@usda.gov.

Sincerely,

JOHN ALLEN

Digitally signed by JOHN ALLEN
Date: 2022.09.08 10:31:17 -04'00'

JOHN ALLEN
State Soil Scientist

Enclosures

C.J. Cunningham

From: Brian J Marciniak <Brian.Marciniak@amwater.com>
Sent: Tuesday, September 13, 2022 9:20 AM
To: C.J. Cunningham
Subject: FW: US 20 over Kennedy Ave. Bridge Project (Des. No. 1900009) - Early Coordination

Follow Up Flag: Follow up
Flag Status: Flagged

EXTERNAL

Good morning,

It is anticipated that this project will NOT impact Indiana American Water.

Thank you,
Brian

Brian J Marciniak
Supervisor – Water Quality and Environmental Compliance
Indiana American Water

From: Christina L Gosnell <Christina.Gosnell@amwater.com>
Sent: Tuesday, September 13, 2022 8:02 AM
To: Brian J Marciniak <Brian.Marciniak@amwater.com>
Subject: FW: US 20 over Kennedy Ave. Bridge Project (Des. No. 1900009) - Early Coordination

Thanks.

Christina Gosnell
Senior Manager Water Quality and Environmental Compliance
Indiana American Water Company
153 N. Emerson Ave
Greenwood, IN 46143
Tel: 317-885-2408
Cell: 812-344-8436
E-mail: Christina.Gosnell@amwater.com

C.J. Cunningham

From: Brown, Anastasia F CIV USARMY CELRC (USA) <stasi.f.brown@usace.army.mil>
Sent: Friday, September 16, 2022 11:32 AM
To: C.J. Cunningham; Jason Hickle; aborden1@indot.in.gov; SMichels@indot.IN.gov
Subject: FW: USACE Request Assigned - LRC-2022-597 - US 20 over Kennedy Ave. Bridge Project (Des. No. 1900009) - Early Coordination

Follow Up Flag: Follow up
Flag Status: Flagged

EXTERNAL

Good morning,

Thank you for including our agency on the early coordination email. It appears that aquatic resources exist on the proposed project site. A jurisdictional determination and/or permit authorizing impacts to Waters of the US may be needed before proceeding with the project. Please contact our agency for a pre-application meeting if you have further questions.

Stasi Brown (she/her/hers)
Project Manager
Regulatory Branch - U.S. Army Corps of Engineers

231 S. LaSalle St, Suite 1500
Chicago, Illinois 60604
(312) 846-5544

SHARP Victim Advocate
USACE SHARP 24/7 Helpline: 1-800-281-6224
DOD Safe Helpline 24/7 - 1-877-995-5247
Website and Online Chat: <http://www.safehelpline.org>

State of Indiana
DEPARTMENT OF NATURAL RESOURCES
Division of Fish and Wildlife
Early Coordination/Environmental Assessment

DNR #: ER-24986

Request Received: September 8, 2022

Requestor: Lochmueller Group
CJ Cunningham
112 West Jefferson Boulevard, Suite 500
South Bend, IN 46601

Project: US 20 bridge replacement over Kennedy Avenue, 0.99 mile east of SR 152; Des #1900009

County/Site info: Lake

The Indiana Department of Natural Resources has reviewed the above referenced project per your request. Our agency offers the following comments for your information and in accordance with the National Environmental Policy Act of 1969.

If our agency has regulatory jurisdiction over the project, the recommendations contained in this letter may become requirements of any permit issued. If we do not have permitting authority, all recommendations are voluntary.

Regulatory Assessment: Formal approval by the Department of Natural Resources under the regulatory programs administered by the Division of Water is not required for this project.

Natural Heritage Database: The Natural Heritage Program's data have been checked. The managed lands, high quality natural communities, and species below have been documented within 1/2 mile of the project area. The Division of Nature Preserves does not anticipate any impacts to the communities, plant, or insects as a result of this project.

A) MANAGED LANDS:

1. Seidner Dune and Swale Nature Preserve, Shirley Heinze Land Trust
2. Gibson Woods Nature Preserve, Lake County Parks & Rec

B) NATURAL COMMUNITIES:

1. Dry-mesic Sand Prairie
2. Dry-mesic Sand Savanna

C) PLANT: Paper birch (*Betula papyrifera*), state threatened

D) INSECTS:

1. gemmed cordgrass borer (*Peoria gemmatella*), state threatened
2. Dusted Skipper (*Atrytonopsis hianna*), state rare
3. Goat's rue looper (*Digrammia eremiata*), state rare
4. Leonard's Skipper (*Hesperia leonardus*), state rare
5. Two-lined cosmoteletix (*Cosmotettix bilineatus*), state rare

E) BIRDS:

1. Black Tern (*Chlidonias niger*), state endangered
2. Black-crowned Night-heron (*Nycticorax nycticorax*), state endangered
3. Marsh wren (*Cistothorus palustris*), state endangered
4. Virginia Rail (*Rallus limicola*), state endangered
5. Great Egret (*Ardea alba*), state special concern
6. Great Egret (*Ardea alba*), state special concern
7. Bald eagle (*Haliaeetus leucocephalus*)

F) REPTILE & AMPHIBIAN:

1. Blanchard's cricket frog (*Acris blanchardi*), state special concern
2. Western ribbon snake (*Thamnophis proximus proximus*), state special concern

G) MAMMAL: Franklin's ground squirrel (*Poliocitellus franklinii*), state endangered

State of Indiana
DEPARTMENT OF NATURAL RESOURCES
Division of Fish and Wildlife
Early Coordination/Environmental Assessment

Fish & Wildlife Comments: We do not foresee any impacts to the birds, reptile, amphibian, or mammal species above as a result of this project.

Avoid and minimize impacts to fish, wildlife, and botanical resources to the greatest extent possible, and compensate for impacts. The following are recommendations that address potential impacts identified in the proposed project area:

1) Wildlife Passage:

Maintaining or improving wildlife passage along Indiana roadways is a priority for the Division of Fish & Wildlife. Based on site level photos submitted for review, wildlife passage over Kennedy Avenue on the existing US 20 bridge does not appear to be possible but is likely possible across Kennedy Avenue at grade. Wildlife passage parallel to Kennedy Avenue is also likely possible.

The rehabilitated crossing structure, and any bank stabilization under or around the structure, must not create conditions that are less favorable for wildlife passage when compared to existing conditions. Upgrading wildlife passage for rehabilitated structures is recommended whenever possible to improve wildlife/vehicle safety. Passage areas must be maintained or restored to allow for wildlife passage where it is currently possible. All wildlife passage designs must include a smooth level pathway a minimum of 1-3 feet in width composed of natural substrate (soil, sand, gravel, etc.) or compacted aggregate fill over riprap (#2, #53, #73, etc.) tied into existing elevations. The location of the wildlife pathway is dependent on the wildlife species using the area. There are a number of techniques and materials for incorporating wildlife passage into the design of a crossing structure if maintaining or restoring passage areas is not possible. The following links are good resources to consider in the design of stream crossing structures to maintain fish and wildlife passage:

<https://www.fs.usda.gov/wildlifecrossings/library/index.php>,

https://www.fhwa.dot.gov/clas/ctip/wildlife_crossing_structures/.

2) Tree Clearing:

The Division of Fish & Wildlife recommends avoiding removing trees to the greatest extent possible and replacing trees that must be removed. Trees are important to fish and wildlife resources in urban and rural areas. Trees also provide millions of dollars of tangible benefits to Indiana communities by their presence in the environment. Their shade and beauty contribute to the quality of life. They provide significant increases in real estate values, create attractive settings for commercial businesses, and improve community appeal. Trees decrease energy consumption by providing shade and acting as windbreaks. They reduce water treatment costs and impede soil erosion by slowing the runoff of stormwater. Trees also cool the air temperature, cleanse pollutants from the air, and produce oxygen while absorbing carbon dioxide. Trees are an integral component of the urban and rural environment. Proactively managing and maintaining a tree population will ultimately maximize the benefits afforded by their aesthetic and ecological functions. The following links give a good overview of the benefits of a tree program and how to select the right species to avoid the negative impacts of non-native invasive species such as the common and popular Bradford pear:

<https://www.in.gov/dnr/forestry/forestry-publications-and-presentations/> (scroll down to the Community & Urban Forestry section).

3) Wetland Habitat:

Due to the presence or potential presence of wetland habitat on site, we recommend contacting and coordinating with the Indiana Department of Environmental Management (IDEM) 401 program and also the US Army Corps of Engineers (USACE) 404 program. Impacts to wetland habitat should be mitigated at the appropriate ratio according to the 1991 INDOT/IDNR/USFWS Memorandum of Understanding.

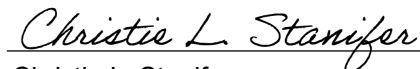
State of Indiana
DEPARTMENT OF NATURAL RESOURCES
Division of Fish and Wildlife
Early Coordination/Environmental Assessment

The additional measures listed below should be implemented to avoid, minimize, or compensate for impacts to fish, wildlife, and botanical resources:

1. Revegetate all bare and disturbed areas that are not currently mowed and maintained with a mixture of grasses, sedges, and wildflowers native to Central Indiana as soon as possible upon completion; turf-type grasses (including low-endophyte, friendly endophyte, and endophyte free tall fescue but excluding all other varieties of tall fescue) may be used in currently mowed areas only. A native herbaceous seed mixture must include at least 5 species of grasses and sedges and 5 species of wildflowers.
2. Minimize and contain within the project limits all tree and brush clearing.
3. Do not cut any trees suitable for Indiana bat or Northern Long-eared bat roosting (greater than 5 inches dbh, living or dead, with loose hanging bark, or with cracks, crevices, or cavities) from April 1 through September 30.
4. Appropriately designed measures for controlling erosion and sediment must be implemented to prevent sediment from entering the waterbody or leaving the construction site; maintain these measures until construction is complete and all disturbed areas are stabilized.
5. Seed and protect all disturbed streambanks and slopes not protected by other methods that are 3:1 or steeper with erosion control blankets that are heavy-duty, biodegradable, and net free or that use loose-woven / Leno-woven netting to minimize the entrapment and snaring of small-bodied wildlife such as snakes and turtles (follow manufacturer's recommendations for selection and installation); seed and apply mulch on all other disturbed areas.
6. Do not excavate or place fill in any riparian wetland.

Contact Staff:

Christie L. Stanifer, Environ. Coordinator, Fish & Wildlife
Our agency appreciates this opportunity to be of service. Please contact the above staff member at (317) 232-4080 if we can be of further assistance.



Christie L. Stanifer
Environ. Coordinator
Division of Fish and Wildlife

Date: October 7, 2022



United States Department of the Interior

FISH AND WILDLIFE SERVICE
Indiana Ecological Services Field Office
620 South Walker Street
Bloomington, IN 47403-2121
Phone: (812) 334-4261 Fax: (812) 334-4273



In Reply Refer To:

January 12, 2024

Project Code: 2024-0004376

Project Name: Des. No. 1900009, US 20 over Kennedy Ave. Bridge Project, Lake County

Subject: List of threatened and endangered species that may occur in your proposed project location or may be affected by your proposed project

To Whom It May Concern:

The enclosed species list identifies threatened, endangered, proposed and candidate species, as well as proposed and final designated critical habitat, that may occur within the boundary of your proposed project and/or may be affected by your proposed project. The species list fulfills the requirements of the U.S. Fish and Wildlife Service (Service) under section 7(c) of the Endangered Species Act (Act) of 1973, as amended (16 U.S.C. 1531 *et seq.*).

New information based on updated surveys, changes in the abundance and distribution of species, changed habitat conditions, or other factors could change this list. Please feel free to contact us if you need more current information or assistance regarding the potential impacts to federally proposed, listed, and candidate species and federally designated and proposed critical habitat. Please note that under 50 CFR 402.12(e) of the regulations implementing section 7 of the Act, the accuracy of this species list should be verified after 90 days. This verification can be completed formally or informally as desired. The Service recommends that verification be completed by visiting the IPaC website at regular intervals during project planning and implementation for updates to species lists and information. An updated list may be requested through the IPaC system by completing the same process used to receive the enclosed list.

The purpose of the Act is to provide a means whereby threatened and endangered species and the ecosystems upon which they depend may be conserved. Under sections 7(a)(1) and 7(a)(2) of the Act and its implementing regulations (50 CFR 402 *et seq.*), Federal agencies are required to utilize their authorities to carry out programs for the conservation of threatened and endangered species and to determine whether projects may affect threatened and endangered species and/or designated critical habitat.

Please use the species list provided and visit the U.S. Fish and Wildlife Service's Region 3 Section 7 Technical Assistance website at - <http://www.fws.gov/midwest/endangered/section7/s7process/index.html>. This website contains step-by-step instructions which will help you

determine if your project will have an adverse effect on listed species and will help lead you through the Section 7 process. For all **wind energy projects and projects that include installing towers that use guy wires or are over 200 feet in height**, please contact this field office directly for assistance, even if no federally listed plants, animals or critical habitat are present within your proposed project or may be affected by your proposed project.

A Biological Assessment is required for construction projects (or other undertakings having similar physical impacts) that are major Federal actions significantly affecting the quality of the human environment as defined in the National Environmental Policy Act (42 U.S.C. 4332(2)(c)). For projects other than major construction activities, the Service suggests that a biological evaluation similar to a Biological Assessment be prepared to determine whether the project may affect listed or proposed species and/or designated or proposed critical habitat. Recommended contents of a Biological Assessment are described at 50 CFR 402.12.

If a Federal agency determines, based on the Biological Assessment or biological evaluation, that listed species and/or designated critical habitat may be affected by the proposed project, the agency is required to consult with the Service pursuant to 50 CFR 402. In addition, the Service recommends that candidate species, proposed species and proposed critical habitat be addressed within the consultation. More information on the regulations and procedures for section 7 consultation, including the role of permit or license applicants, can be found in the "Endangered Species Consultation Handbook" at:

<https://www.fws.gov/sites/default/files/documents/endangered-species-consultation-handbook.pdf>

Migratory Birds: In addition to responsibilities to protect threatened and endangered species under the Endangered Species Act (ESA), there are additional responsibilities under the Migratory Bird Treaty Act (MBTA) and the Bald and Golden Eagle Protection Act (BGEPA) to protect native birds from project-related impacts. Any activity, intentional or unintentional, resulting in take of migratory birds, including eagles, is prohibited unless otherwise permitted by the U.S. Fish and Wildlife Service (50 C.F.R. Sec. 10.12 and 16 U.S.C. Sec. 668(a)). For more information regarding these Acts, see <https://www.fws.gov/program/migratory-bird-permit/what-we-do>.

The MBTA has no provision for allowing take of migratory birds that may be unintentionally killed or injured by otherwise lawful activities. It is the responsibility of the project proponent to comply with these Acts by identifying potential impacts to migratory birds and eagles within applicable NEPA documents (when there is a federal nexus) or a Bird/Eagle Conservation Plan (when there is no federal nexus). Proponents should implement conservation measures to avoid or minimize the production of project-related stressors or minimize the exposure of birds and their resources to the project-related stressors. For more information on avian stressors and recommended conservation measures, see <https://www.fws.gov/library/collections/threats-birds>.

In addition to MBTA and BGEPA, Executive Order 13186: *Responsibilities of Federal Agencies to Protect Migratory Birds*, obligates all Federal agencies that engage in or authorize activities that might affect migratory birds, to minimize those effects and encourage conservation measures that will improve bird populations. Executive Order 13186 provides for the protection of both migratory birds and migratory bird habitat. For information regarding the implementation of

Executive Order 13186, please visit <https://www.fws.gov/partner/council-conservation-migratory-birds>.

We appreciate your concern for threatened and endangered species. The Service encourages Federal agencies to include conservation of threatened and endangered species into their project planning to further the purposes of the Act. **Please include the Consultation Code in the header of this letter with any request for consultation or correspondence about your project that you submit to our office.**

Attachment(s):

- Official Species List
- Bald & Golden Eagles
- Migratory Birds
- Wetlands

OFFICIAL SPECIES LIST

This list is provided pursuant to Section 7 of the Endangered Species Act, and fulfills the requirement for Federal agencies to "request of the Secretary of the Interior information whether any species which is listed or proposed to be listed may be present in the area of a proposed action".

This species list is provided by:

Indiana Ecological Services Field Office

620 South Walker Street
Bloomington, IN 47403-2121
(812) 334-4261

PROJECT SUMMARY

Project Code: 2024-0004376
Project Name: Des. No. 1900009, US 20 over Kennedy Ave. Bridge Project, Lake County
Project Type: Bridge - Replacement
Project Description: The Indiana Department of Transportation (INDOT) LaPorte District, with funding from the Federal Highway Administration (FHWA), intends to proceed with a bridge project along US 20 in Lake County, Indiana (Des. No. 1900009).

The proposed project is located along US 20, 0.99 mi. E of SR 152, along the southern limits of the city of East Chicago and the northern limits of the city of Hammond.

The existing bridge, Structure No. 020-45-02484 B/ NBI No. 005200, conveys US 20 over Kennedy Avenue and the Indiana Harbor Belt Railroad (IHB RR).

The preferred alternative involves replacement of the existing bridge with a two-span continuous composite steel plate girder structure with MSE wall abutments and a reinforced concrete pier. A curbed center median will be installed between the eastbound and westbound traffic lanes along US 20. A 10-foot-wide multi-use trail will be added to the north side of the new bridge and connect to the planned Marquette Greenway Trail. A raise in the profile grade will be necessary to provide adequate vertical clearance over the railroad and adequate stopping sight distance on US 20. Additionally, the US 20 roadway grade east of the bridge will require modification to achieve proper stopping sight distance at the intersection with the entry/exit ramps accessing Kennedy Avenue. A 12-inch metal culvert approximately 87 feet in length, located south of the southern access road, will be removed and replaced with either an open ditch or a new pipe. Streetlights along the north and south access roads are anticipated to be replaced. Additional permanent lighting is anticipated to be added to light the trail. Temporary lighting may be used if night work is required.

The maintenance of traffic (MOT) plan is expected to involve complete closure of US 20 with a detour. The proposed detour route utilizes US 20 (one mile west of the bridge) and US 12. Kennedy Avenue will remain open during construction but will be reduced to two lanes (one in each direction).

It is anticipated that less than 1.13 acres of additional permanent right-of-way (ROW) and 0.02 acre of temporary ROW will need to be acquired to

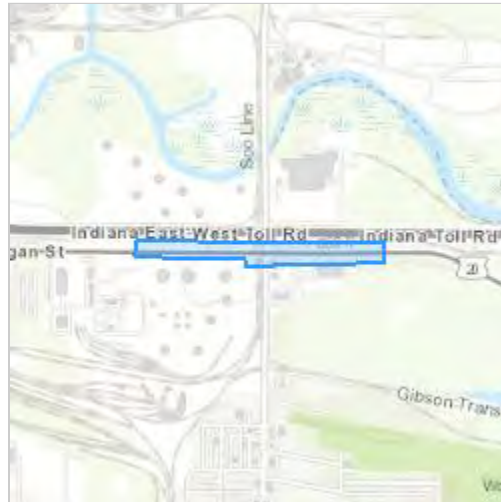
accommodate the proposed improvements.

Suitable summer bat habitat consisting of clusters of small to medium sized trees are present within and adjacent to the project area. The dominant tree species in the project area include Eastern cottonwood (*Populus deltoides*), Siberian elm (*Ulmus pumila*), and black walnut (*Juglans nigra*). Tree clearing totaling approximately 1.3 acres is anticipated. Tree clearing will be located adjacent to US 20 and access roads to Kennedy Ave. Tree clearing will occur within 100 feet of the roadway and will occur during the inactive season. Adjacent land use is primarily industrial, including nearby petroleum storage facilities and other industry, as well as wooded areas and dune and swale wetlands. Construction is anticipated to begin in spring of 2026.

A review of the USFWS database on October 27, 2021 by INDOT LaPorte District did not indicate the presence of endangered bat species in or within 0.5 mile of the project area. The subject bridge (Str. No. 020-45-02484 B) and additional culvert were inspected on October 19, 2021 by Lochmueller Group. No evidence of bat or bird usage was found.

Project Location:

The approximate location of the project can be viewed in Google Maps: <https://www.google.com/maps/@41.6101003,-87.46175586573968,14z>



Counties: Lake County, Indiana

ENDANGERED SPECIES ACT SPECIES

There is a total of 6 threatened, endangered, or candidate species on this species list.

Species on this list should be considered in an effects analysis for your project and could include species that exist in another geographic area. For example, certain fish may appear on the species list because a project could affect downstream species.

IPaC does not display listed species or critical habitats under the sole jurisdiction of NOAA Fisheries¹, as USFWS does not have the authority to speak on behalf of NOAA and the Department of Commerce.

See the "Critical habitats" section below for those critical habitats that lie wholly or partially within your project area under this office's jurisdiction. Please contact the designated FWS office if you have questions.

-
1. [NOAA Fisheries](#), also known as the National Marine Fisheries Service (NMFS), is an office of the National Oceanic and Atmospheric Administration within the Department of Commerce.

MAMMALS

NAME	STATUS
Indiana Bat <i>Myotis sodalis</i> There is final critical habitat for this species. Your location does not overlap the critical habitat. Species profile: https://ecos.fws.gov/ecp/species/5949	Endangered
Northern Long-eared Bat <i>Myotis septentrionalis</i> No critical habitat has been designated for this species. Species profile: https://ecos.fws.gov/ecp/species/9045	Endangered
Tricolored Bat <i>Perimyotis subflavus</i> No critical habitat has been designated for this species. Species profile: https://ecos.fws.gov/ecp/species/10515	Proposed Endangered

BIRDS

NAME	STATUS
Whooping Crane <i>Grus americana</i> Population: U.S.A. (AL, AR, CO, FL, GA, ID, IL, IN, IA, KY, LA, MI, MN, MS, MO, NC, NM, OH, SC, TN, UT, VA, WI, WV, western half of WY) No critical habitat has been designated for this species. Species profile: https://ecos.fws.gov/ecp/species/758	Experimental Population, Non- Essential

INSECTS

NAME	STATUS
Karner Blue Butterfly <i>Lycaeides melissa samuelis</i> There is proposed critical habitat for this species. Species profile: https://ecos.fws.gov/ecp/species/6656	Endangered
Monarch Butterfly <i>Danaus plexippus</i> No critical habitat has been designated for this species. Species profile: https://ecos.fws.gov/ecp/species/9743	Candidate

CRITICAL HABITATS

THERE ARE NO CRITICAL HABITATS WITHIN YOUR PROJECT AREA UNDER THIS OFFICE'S JURISDICTION.

YOU ARE STILL REQUIRED TO DETERMINE IF YOUR PROJECT(S) MAY HAVE EFFECTS ON ALL ABOVE LISTED SPECIES.

BALD & GOLDEN EAGLES

Bald and golden eagles are protected under the Bald and Golden Eagle Protection Act¹ and the Migratory Bird Treaty Act².

Any person or organization who plans or conducts activities that may result in impacts to bald or golden eagles, or their habitats³, should follow appropriate regulations and consider implementing appropriate conservation measures, as described in the links below. Specifically, please review the "[Supplemental Information on Migratory Birds and Eagles](#)".

1. The [Bald and Golden Eagle Protection Act](#) of 1940.
2. The [Migratory Birds Treaty Act](#) of 1918.
3. 50 C.F.R. Sec. 10.12 and 16 U.S.C. Sec. 668(a)

There are bald and/or golden eagles in your project area.

For guidance on when to schedule activities or implement avoidance and minimization measures to reduce impacts to migratory birds on your list, see the PROBABILITY OF PRESENCE SUMMARY below to see when these birds are most likely to be present and breeding in your project area.

NAME	BREEDING SEASON
Bald Eagle <i>Haliaeetus leucocephalus</i> This is not a Bird of Conservation Concern (BCC) in this area, but warrants attention because of the Eagle Act or for potential susceptibilities in offshore areas from certain types of development or activities. https://ecos.fws.gov/ecp/species/1626	Breeds Oct 15 to Aug 31

MIGRATORY BIRDS

Certain birds are protected under the Migratory Bird Treaty Act¹ and the Bald and Golden Eagle Protection Act².

Any person or organization who plans or conducts activities that may result in impacts to migratory birds, eagles, and their habitats³ should follow appropriate regulations and consider implementing appropriate conservation measures, as described in the links below. Specifically, please review the "[Supplemental Information on Migratory Birds and Eagles](#)".

1. The [Migratory Birds Treaty Act](#) of 1918.
2. The [Bald and Golden Eagle Protection Act](#) of 1940.
3. 50 C.F.R. Sec. 10.12 and 16 U.S.C. Sec. 668(a)

For guidance on when to schedule activities or implement avoidance and minimization measures to reduce impacts to migratory birds on your list, see the PROBABILITY OF PRESENCE SUMMARY below to see when these birds are most likely to be present and breeding in your project area.

NAME	BREEDING SEASON
American Golden-plover <i>Pluvialis dominica</i> This is a Bird of Conservation Concern (BCC) throughout its range in the continental USA and Alaska. https://ecos.fws.gov/ecp/species/10561	Breeds elsewhere
Bald Eagle <i>Haliaeetus leucocephalus</i> This is not a Bird of Conservation Concern (BCC) in this area, but warrants attention because of the Eagle Act or for potential susceptibilities in offshore areas from certain types of development or activities. https://ecos.fws.gov/ecp/species/1626	Breeds Oct 15 to Aug 31
Black-billed Cuckoo <i>Coccyzus erythrophthalmus</i> This is a Bird of Conservation Concern (BCC) throughout its range in the continental USA and Alaska. https://ecos.fws.gov/ecp/species/9399	Breeds May 15 to Oct 10
Bobolink <i>Dolichonyx oryzivorus</i> This is a Bird of Conservation Concern (BCC) throughout its range in the continental USA and Alaska. https://ecos.fws.gov/ecp/species/9454	Breeds May 20 to Jul 31
Chimney Swift <i>Chaetura pelagica</i> This is a Bird of Conservation Concern (BCC) throughout its range in the continental USA and Alaska. https://ecos.fws.gov/ecp/species/9406	Breeds Mar 15 to Aug 25

NAME	BREEDING SEASON
Eastern Whip-poor-will <i>Antrastomus vociferus</i> This is a Bird of Conservation Concern (BCC) throughout its range in the continental USA and Alaska. https://ecos.fws.gov/ecp/species/10678	Breeds May 1 to Aug 20
Henslow's Sparrow <i>Ammodramus henslowii</i> This is a Bird of Conservation Concern (BCC) throughout its range in the continental USA and Alaska. https://ecos.fws.gov/ecp/species/3941	Breeds May 1 to Aug 31
King Rail <i>Rallus elegans</i> This is a Bird of Conservation Concern (BCC) throughout its range in the continental USA and Alaska. https://ecos.fws.gov/ecp/species/8936	Breeds May 1 to Sep 5
Lesser Yellowlegs <i>Tringa flavipes</i> This is a Bird of Conservation Concern (BCC) throughout its range in the continental USA and Alaska. https://ecos.fws.gov/ecp/species/9679	Breeds elsewhere
Pectoral Sandpiper <i>Calidris melanotos</i> This is a Bird of Conservation Concern (BCC) throughout its range in the continental USA and Alaska. https://ecos.fws.gov/ecp/species/9561	Breeds elsewhere
Prothonotary Warbler <i>Protonotaria citrea</i> This is a Bird of Conservation Concern (BCC) throughout its range in the continental USA and Alaska. https://ecos.fws.gov/ecp/species/9439	Breeds Apr 1 to Jul 31
Red-headed Woodpecker <i>Melanerpes erythrocephalus</i> This is a Bird of Conservation Concern (BCC) throughout its range in the continental USA and Alaska. https://ecos.fws.gov/ecp/species/9398	Breeds May 10 to Sep 10
Rusty Blackbird <i>Euphagus carolinus</i> This is a Bird of Conservation Concern (BCC) only in particular Bird Conservation Regions (BCRs) in the continental USA https://ecos.fws.gov/ecp/species/9478	Breeds elsewhere
Short-billed Dowitcher <i>Limnodromus griseus</i> This is a Bird of Conservation Concern (BCC) throughout its range in the continental USA and Alaska. https://ecos.fws.gov/ecp/species/9480	Breeds elsewhere
Wood Thrush <i>Hylocichla mustelina</i> This is a Bird of Conservation Concern (BCC) throughout its range in the continental USA and Alaska. https://ecos.fws.gov/ecp/species/9431	Breeds May 10 to Aug 31

PROBABILITY OF PRESENCE SUMMARY

The graphs below provide our best understanding of when birds of concern are most likely to be present in your project area. This information can be used to tailor and schedule your project activities to avoid or minimize impacts to birds. Please make sure you read ["Supplemental Information on Migratory Birds and Eagles"](#), specifically the FAQ section titled "Proper Interpretation and Use of Your Migratory Bird Report" before using or attempting to interpret this report.

Probability of Presence (■)

Green bars; the bird's relative probability of presence in the 10km grid cell(s) your project overlaps during that week of the year.

Breeding Season (■)

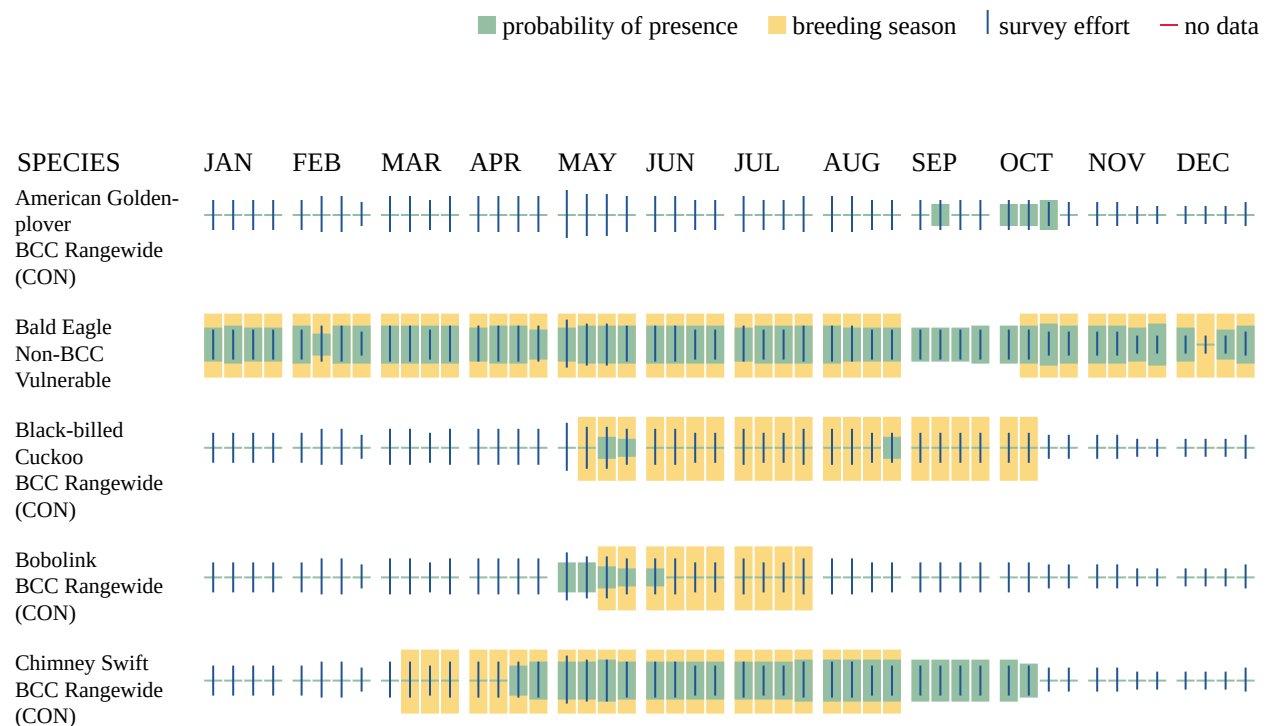
Yellow bars; liberal estimate of the timeframe inside which the bird breeds across its entire range.

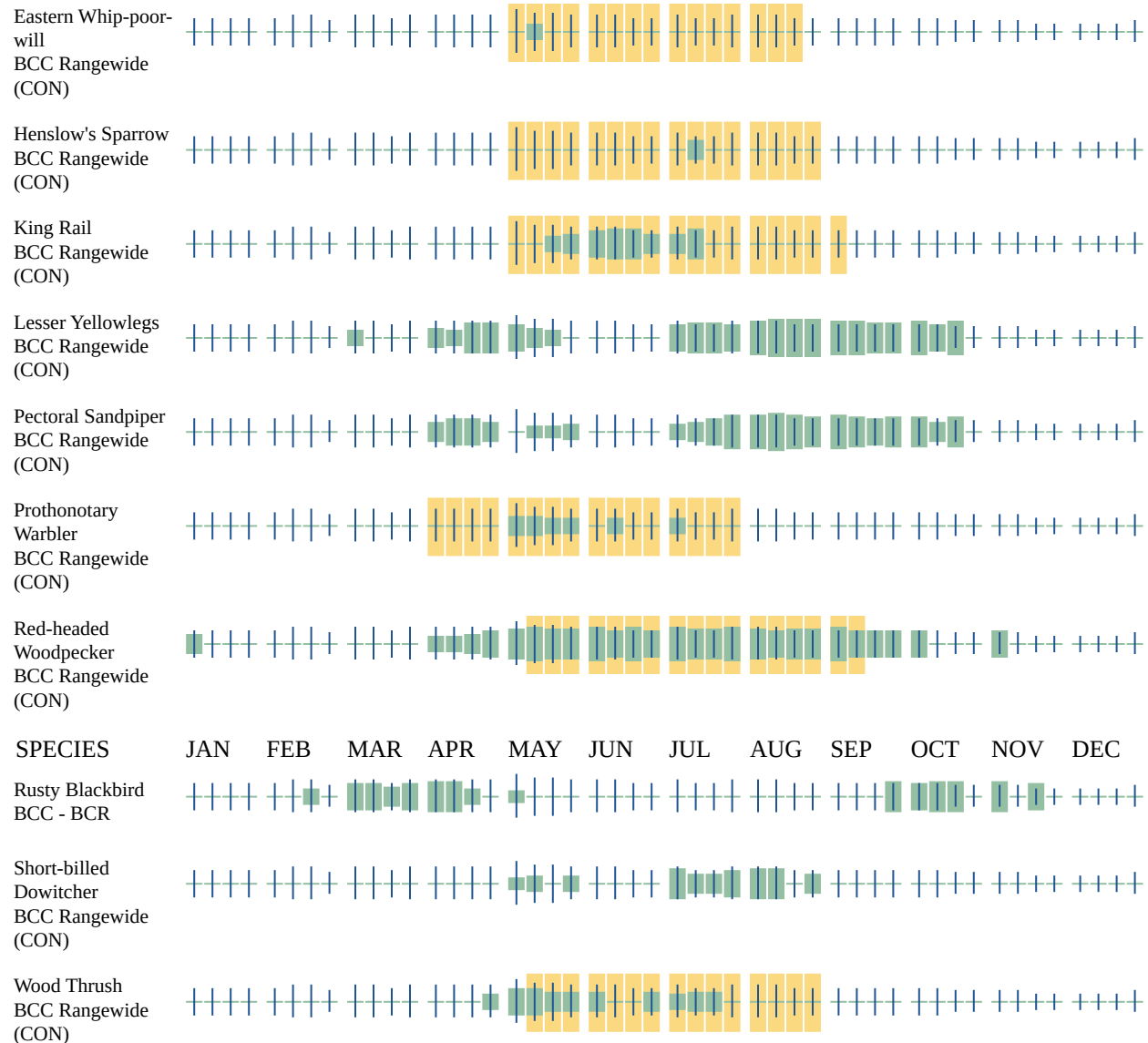
Survey Effort (|)

Vertical black lines; the number of surveys performed for that species in the 10km grid cell(s) your project area overlaps.

No Data (—)

A week is marked as having no data if there were no survey events for that week.





Additional information can be found using the following links:

- Eagle Management <https://www.fws.gov/program/eagle-management>
- Measures for avoiding and minimizing impacts to birds <https://www.fws.gov/library/collections/avoiding-and-minimizing-incidental-take-migratory-birds>
- Nationwide conservation measures for birds <https://www.fws.gov/sites/default/files/documents/nationwide-standard-conservation-measures.pdf>
- Supplemental Information for Migratory Birds and Eagles in IPaC <https://www.fws.gov/media/supplemental-information-migratory-birds-and-bald-and-golden-eagles-may-occur-project-action>

WETLANDS

Impacts to [NWI wetlands](#) and other aquatic habitats may be subject to regulation under Section 404 of the Clean Water Act, or other State/Federal statutes.

For more information please contact the Regulatory Program of the local [U.S. Army Corps of Engineers District](#).

Please note that the NWI data being shown may be out of date. We are currently working to update our NWI data set. We recommend you verify these results with a site visit to determine the actual extent of wetlands on site.

FRESHWATER FORESTED/SHRUB WETLAND

- PFO1/EM5C

IPAC USER CONTACT INFORMATION

Agency: Lochmueller Group, Inc.
Name: Carson Hoogewerf
Address: 112 W Jefferson Blvd
Address Line 2: #500
City: South Bend
State: IN
Zip: 46601
Email: choogewerf@lochgroup.com
Phone: 5743345491

LEAD AGENCY CONTACT INFORMATION

Lead Agency: Federal Highway Administration



United States Department of the Interior

FISH AND WILDLIFE SERVICE
Indiana Ecological Services Field Office
620 South Walker Street
Bloomington, IN 47403-2121
Phone: (812) 334-4261 Fax: (812) 334-4273



In Reply Refer To:

October 18, 2023

Project code: 2024-0004376

Project Name: Des. No. 1900009, US 20 over Kennedy Ave. Bridge Project, Lake County

Subject: Concurrence verification letter for the 'Des. No. 1900009, US 20 over Kennedy Ave. Bridge Project, Lake County' project under the amended February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion (dated March 23, 2023) for Transportation Projects within the Range of the Indiana Bat and Northern Long-eared Bat (NLEB).

To whom it may concern:

The U.S. Fish and Wildlife Service (Service) has received your request dated October 18, 2023 to verify that the **Des. No. 1900009, US 20 over Kennedy Ave. Bridge Project, Lake County** (Proposed Action) may rely on the concurrence provided in the amended February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion (dated March 23, 2023) for Transportation Projects within the Range of the Indiana Bat and Northern Long-eared Bat (PBO) to satisfy requirements under Section 7(a)(2) of the Endangered Species Act of 1973 (ESA) (87 Stat.884, as amended; 16 U.S.C. 1531 *et seq.*).

Based on the information you provided (Project Description shown below), you have determined that the Proposed Action is within the scope and adheres to the criteria of the PBO, including the adoption of applicable avoidance and minimization measures, and may affect, but is not likely to adversely affect (NLAA) the endangered Indiana bat (*Myotis sodalis*) and/or the endangered northern long-eared bat (*Myotis septentrionalis*). Consultation with the Service pursuant to section 7(a)(2) of ESA (87 Stat. 884, as amended; 16 U.S.C. 1531 *et seq.*) is required.

The Service has 14 calendar days to notify the lead Federal action agency or designated non-federal representative if we determine that the Proposed Action does not meet the criteria for a NLAA determination under the PBO. If we do not notify the lead Federal action agency or designated non-federal representative within that timeframe, you may proceed with the Proposed Action under the terms of the NLAA concurrence provided in the PBO. This verification period allows Service Field Offices to apply local knowledge to implementation of the PBO, as we may identify a small subset of actions having impacts that were unanticipated. In such instances,

Service Field Offices may request additional information that is necessary to verify inclusion of the proposed action under the PBO.

For Proposed Actions that include bridge/culvert or structure removal, replacement, and/or maintenance activities: If your initial bridge/culvert or structure assessment documented signs of bat use or occupancy, or an assessment failed to detect Indiana bats and/or NLEBs, yet are later detected prior to, or during construction, please submit the Post Assessment Discovery of Bats at Bridge/Culvert or Structure Form (User Guide Appendix E) to this Service Office within 2 working days of any potential take. In these instances, potential incidental take of Indiana bats and/or NLEBs is covered under the Incidental Take Statement in the 2018 FHWA, FRA, FTA PBO (provided that the take is reported to the Service).

If the Proposed Action is modified, or new information reveals that it may affect the Indiana bat and/or northern long-eared bat in a manner or to an extent not considered in the PBO, further review to conclude the requirements of ESA Section 7(a)(2) may be required.

For Proposed Actions that include bridge/culvert or structure removal, replacement, and/or maintenance activities:

If your initial bridge/culvert or structure assessments failed to detect Indiana bats and/or NLEB use or occupancy, yet bats are later detected prior to, or during construction, please submit the Post Assessment Discovery of Bats at Bridge/Culvert or Structure Form (User Guide Appendix E) to this Service Office within 2 working days of the incident. In these instances, potential incidental take of Indiana bats and/or NLEBs may be exempted provided that the take is reported to the Service.

If the Proposed Action may affect any other federally-listed or proposed species, and/or any designated critical habitat, additional consultation between the lead Federal action agency and this Service Office is required. If the proposed action has the potential to take bald or golden eagles, additional coordination with the Service under the Bald and Golden Eagle Protection Act may also be required. In either of these circumstances, please contact this Service Office.

The following species may occur in your project area and **are not** covered by this determination:

- Karner Blue Butterfly *Lycaeides melissa samuelis* Endangered
- Monarch Butterfly *Danaus plexippus* Candidate
- Tricolored Bat *Perimyotis subflavus* Proposed Endangered
- Whooping Crane *Grus americana* Experimental Population, Non-Essential

PROJECT DESCRIPTION

The following project name and description was collected in IPaC as part of the endangered species review process.

NAME

Des. No. 1900009, US 20 over Kennedy Ave. Bridge Project, Lake County

DESCRIPTION

The Indiana Department of Transportation (INDOT) LaPorte District, with funding from the Federal Highway Administration (FHWA), intends to proceed with a bridge project along US 20 in Lake County, Indiana (Des. No. 1900009).

The proposed project is located along US 20, 0.99 mi. E of SR 152, along the southern limits of the city of East Chicago and the northern limits of the city of Hammond.

The existing bridge, Structure No. 020-45-02484 B/ NBI No. 005200, conveys US 20 over Kennedy Avenue and the Indiana Harbor Belt Railroad (IHB RR).

The preferred alternative involves replacement of the existing bridge with a two-span continuous composite steel plate girder structure with MSE wall abutments and a reinforced concrete pier. A curbed center median will be installed between the eastbound and westbound traffic lanes along US 20. A 10-foot-wide multi-use trail will be added to the north side of the new bridge and connect to the planned Marquette Greenway Trail. A raise in the profile grade will be necessary to provide adequate vertical clearance over the railroad and adequate stopping sight distance on US 20. Additionally, the US 20 roadway grade east of the bridge will require modification to achieve proper stopping sight distance at the intersection with the entry/exit ramps accessing Kennedy Avenue. A 12-inch metal culvert approximately 87 feet in length, located south of the southern access road, will be removed and replaced with either an open ditch or a new pipe. Streetlights along the north and south access roads are anticipated to be replaced. Additional permanent lighting is anticipated to be added to light the trail. Temporary lighting may be used if night work is required.

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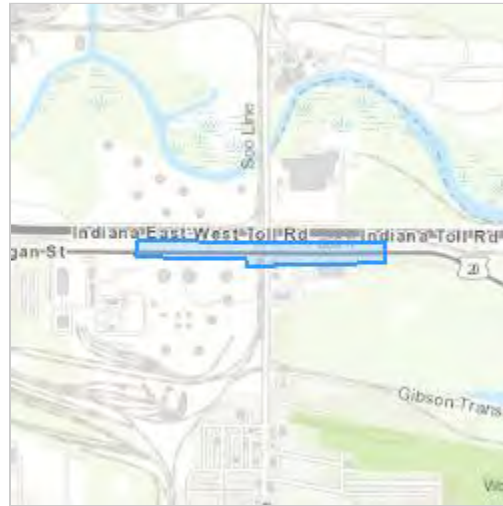
It is anticipated that less than 1.13 acres of additional permanent right-of-way (ROW) and 0.02 acre of temporary ROW will need to be acquired to accommodate the proposed improvements.

Suitable summer bat habitat consisting of clusters of small to medium sized trees are present within and adjacent to the project area. The dominant tree species in the project area include

Eastern cottonwood (*Populus deltoides*), Siberian elm (*Ulmus pumila*), and black walnut (*Juglans nigra*). Tree clearing totaling approximately 1.3 acres is anticipated. Tree clearing will be located adjacent to US 20 and access roads to Kennedy Ave. Tree clearing will occur within 100 feet of the roadway and will occur during the inactive season. Adjacent land use is primarily industrial, including nearby petroleum storage facilities and other industry, as well as wooded areas and dune and swale wetlands. Construction is anticipated to begin in spring of 2026.

A review of the USFWS database on October 27, 2021 by INDOT LaPorte District did not indicate the presence of endangered bat species in or within 0.5 mile of the project area. The subject bridge (Str. No. 020-45-02484 B) and additional culvert were inspected on October 19, 2021 by Lochmueller Group. No evidence of bat or bird usage was found.

The approximate location of the project can be viewed in Google Maps: <https://www.google.com/maps/@41.6101003,-87.46175586573968,14z>



DETERMINATION KEY RESULT

Based on your answers provided, this project(s) may affect, but is not likely to adversely affect the endangered Indiana bat and/or the endangered northern long-eared bat, therefore, consultation with the U.S. Fish and Wildlife Service pursuant to Section 7(a)(2) of the Endangered Species Act of 1973 (ESA) (87 Stat. 884, as amended 16 U.S.C. 1531 *et seq.*) is required. However, also based on your answers provided, this project may rely on the concurrence provided in the amended February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion (dated March 23, 2023) for Transportation Projects within the Range of the Indiana Bat and Northern Long-eared Bat.

QUALIFICATION INTERVIEW

1. Is the project within the range of the Indiana bat^[1]?

[1] See [Indiana bat species profile](#)

Automatically answered

Yes

2. Is the project within the range of the northern long-eared bat^[1]?

[1] See [northern long-eared bat species profile](#)

Automatically answered

Yes

3. Which Federal Agency is the lead for the action?

A) Federal Highway Administration (FHWA)

4. Are *all* project activities limited to non-construction^[1] activities only? (examples of non-construction activities include: bridge/abandoned structure assessments, surveys, planning and technical studies, property inspections, and property sales)

[1] Construction refers to activities involving ground disturbance, percussive noise, and/or lighting.

No

5. Does the project include *any* activities that are **greater than** 300 feet from existing road/rail surfaces^[1]?

[1] Road surface is defined as the actively used [e.g. motorized vehicles] driving surface and shoulders [may be pavement, gravel, etc.] and rail surface is defined as the edge of the actively used rail ballast.

No

6. Does the project include *any* activities **within** 0.5 miles of a known Indiana bat and/or NLEB hibernaculum^[1]?

[1] For the purpose of this consultation, a hibernaculum is a site, most often a cave or mine, where bats hibernate during the winter (see suitable habitat), but could also include bridges and structures if bats are found to be hibernating there during the winter.

No

7. Is the project located **within** a karst area?

No

8. Is there *any* suitable^[1] summer habitat for Indiana Bat or NLEB **within** the project action area^[2]? (includes any trees suitable for maternity, roosting, foraging, or travelling habitat)

[1] See the Service's [summer survey guidance](#) for our current definitions of suitable habitat.

[2] The action area is defined as all areas to be affected directly or indirectly by the Federal action and not merely the immediate area involved in the action (50 CFR Section 402.02). Further clarification is provided by the [User's Guide for the Range-wide Programmatic Consultation for Indiana Bat and Northern Long-eared Bat](#).

Yes

9. Will the project remove *any* suitable summer habitat^[1] and/or remove/trim any existing trees **within** suitable summer habitat?

[1] See the Service's [summer survey guidance](#) for our current definitions of suitable habitat.

Yes

10. Will the project clear more than 20 acres of suitable habitat per 5-mile section of road/rail?

No

11. Have presence/probable absence (P/A) summer surveys^{[1][2]} been conducted^{[3][4]} **within** the suitable habitat located within your project action area?

[1] See the Service's [summer survey guidance](#) for our current definitions of suitable habitat.

[2] Presence/probable absence summer surveys conducted within the fall swarming/spring emergence home range of a documented Indiana bat hibernaculum (contact local Service Field Office for appropriate distance from hibernacula) that result in a negative finding requires additional consultation with the local Service Field Office to determine if clearing of forested habitat is appropriate and/or if seasonal clearing restrictions are needed to avoid and minimize potential adverse effects on fall swarming and spring emerging Indiana bats.

[3] For projects within the range of either the Indiana bat or NLEB in which suitable habitat is present, and no bat surveys have been conducted, the transportation agency will assume presence of the appropriate species. This assumption of presence should be based upon the presence of suitable habitat and the capability of bats to occupy it because of their mobility.

[4] Negative presence/probable absence survey results obtained using the [summer survey guidance](#) are valid for a minimum of two years from the completion of the survey unless new information (e.g., other nearby surveys) suggest otherwise.

No

12. Does the project include activities **within documented Indiana bat habitat**^{[1][2]}?

[1] Documented roosting or foraging habitat – for the purposes of this consultation, we are considering documented habitat as that where Indiana bats and/or NLEB have actually been captured and tracked using (1) radio telemetry to roosts; (2) radio telemetry biangulation/triangulation to estimate foraging areas; or (3) foraging areas with repeated use documented using acoustics. Documented roosting habitat is also considered as suitable summer habitat within 0.25 miles of documented roosts.)

[2] For the purposes of this key, we are considering documented corridors as that where Indiana bats and/or NLEB have actually been captured and tracked to using (1) radio telemetry; or (2) treed corridors located directly between documented roosting and foraging habitat.

No

13. Will the removal or trimming of habitat or trees occur **within** suitable but **undocumented Indiana bat** roosting/foraging habitat or travel corridors?

Yes

14. What time of year will the removal or trimming of habitat or trees **within** suitable but **undocumented Indiana bat** roosting/foraging habitat or travel corridors occur^[1]?

[1] Coordinate with the local Service Field Office for appropriate dates.

B) During the inactive season

15. Does the project include activities **within documented NLEB habitat**^{[1][2]}?

[1] Documented roosting or foraging habitat – for the purposes of this consultation, we are considering documented habitat as that where Indiana bats and/or NLEB have actually been captured and tracked using (1) radio telemetry to roosts; (2) radio telemetry biangulation/triangulation to estimate foraging areas; or (3) foraging areas with repeated use documented using acoustics. Documented roosting habitat is also considered as suitable summer habitat within 0.25 miles of documented roosts.)

[2] For the purposes of this key, we are considering documented corridors as that where Indiana bats and/or NLEB have actually been captured and tracked to using (1) radio telemetry; or (2) treed corridors located directly between documented roosting and foraging habitat.

No

16. Will the removal or trimming of habitat or trees occur **within** suitable but **undocumented NLEB** roosting/foraging habitat or travel corridors?

Yes

17. What time of year will the removal or trimming of habitat or trees **within** suitable but **undocumented NLEB** roosting/foraging habitat or travel corridors occur?

B) During the inactive season

18. Will *any* tree trimming or removal occur **within** 100 feet of existing road/rail surfaces?

Yes

19. Will *any* tree trimming or removal occur **between** 100-300 feet of existing road/rail surfaces?

No

20. Are *all* trees that are being removed clearly demarcated?

Yes

21. Will the removal of habitat or the removal/trimming of trees include installing new or replacing existing **permanent** lighting?

Yes

22. Does the project include wetland or stream protection activities associated with compensatory wetland mitigation?

No

23. Does the project include slash pile burning?

No

24. Does the project include *any* bridge removal, replacement, and/or maintenance activities (e.g., any bridge repair, retrofit, maintenance, and/or rehabilitation work)?

Yes

25. Is there *any* suitable habitat^[1] for Indiana bat or NLEB **within** 1,000 feet of the bridge? (includes any trees suitable for maternity, roosting, foraging, or travelling habitat)

[1] See the Service's current [summer survey guidance](#) for our current definitions of suitable habitat.

Yes

26. Has a bridge assessment^[1] been conducted **within** the last 24 months^[2] to determine if the bridge is being used by bats?

[1] See [User Guide Appendix D](#) for bridge/structure assessment guidance

[2] Assessments must be completed no more than 2 years prior to conducting any work below the deck surface on all bridges that meet the physical characteristics described in the Programmatic Consultation, regardless of whether assessments have been conducted in the past. Due to the transitory nature of bat use, a negative result in one year does not guarantee that bats will not use that bridge/structure in subsequent years.

Yes

SUBMITTED DOCUMENTS

- *Bridge Culvert Bat Assessment Form_Bridge.pdf* <https://ipac.ecosphere.fws.gov/project/PKFYMIXN2FDNZEN6WB3ZWRBX3M/projectDocuments/133222749>
- *Bridge Culvert Bat Assessment Form_Pipe.pdf* <https://ipac.ecosphere.fws.gov/project/PKFYMIXN2FDNZEN6WB3ZWRBX3M/projectDocuments/133222750>

27. Did the bridge assessment detect *any* signs of Indiana bats and/or NLEBs roosting in/under the bridge (bats, guano, etc.)^[1]?

[1] If bridge assessment detects signs of *any* species of bats, coordination with the local FWS office is needed to identify potential threatened or endangered bat species. Additional studies may be undertaken to try to identify which bat species may be utilizing the bridge prior to allowing *any* work to proceed.

Note: There is a small chance bridge assessments for bat occupancy do not detect bats. Should a small number of bats be observed roosting on a bridge just prior to or during construction, such that take is likely to occur or does occur in the form of harassment, injury or death, the PBO requires the action agency to report the take. Report all unanticipated take within 2 working days of the incident to the USFWS. Construction activities may continue without delay provided the take is reported to the USFWS and is limited to 5 bats per project.

No

28. Will the bridge removal, replacement, and/or maintenance activities include installing new or replacing existing **permanent** lighting?

Yes

29. Does the project include the removal, replacement, and/or maintenance of *any* structure other than a bridge? (e.g., rest areas, offices, sheds, outbuildings, barns, parking garages, etc.)

No

30. Will the project involve the use of **temporary** lighting *during* the active season?

Yes

31. Is there *any* suitable habitat **within** 1,000 feet of the location(s) where **temporary** lighting will be used?

Yes

32. Will the project install *any* new or replace any existing **permanent** lighting in addition to the lighting already indicated for habitat removal (including the removal or trimming of trees) or bridge/structure removal, replacement or maintenance activities?

No

33. Does the project include percussives or other activities (**not including tree removal/trimming or bridge/structure work**) that will increase noise levels above existing traffic/background levels?

No

34. Are *all* project activities that are **not associated with** habitat removal, tree removal/trimming, bridge and/or structure activities, temporary or permanent lighting, or use of percussives, limited to actions that DO NOT cause any additional stressors to the bat species?

Examples: lining roadways, unlighted signage, rail road crossing signals, signal lighting, and minor road repair such as asphalt fill of potholes, etc.

Yes

35. Will the project raise the road profile **above the tree canopy**?

No

36. Are the project activities that are not associated with habitat removal, tree removal/trimming, bridge and/or structure activities, temporary or permanent lighting, or use of percussives consistent with a No Effect determination in this key?

Automatically answered

Yes, other project activities are limited to actions that DO NOT cause any additional stressors to the bat species as described in the BA/BO

37. Is the habitat removal portion of this project consistent with a Not Likely to Adversely Affect determination in this key?

Automatically answered

Yes, because the tree removal/trimming that occurs outside of the Indiana bat's active season occurs greater than 0.5 miles from the nearest hibernaculum, is less than 100 feet from the existing road/rail surface, includes clear demarcation of the trees that are to be removed, and does not alter documented roosts and/or surrounding summer habitat within 0.25 miles of a documented roost.

38. Is the habitat removal portion of this project consistent with a Not Likely to Adversely Affect determination in this key?

Automatically answered

Yes, because the tree removal/trimming that occurs outside of the NLEB's active season occurs greater than 0.5 miles from the nearest hibernaculum, is less than 100 feet from the existing road/rail surface, includes clear demarcation of the trees that are to be removed, and does not alter documented roosts and/or surrounding summer habitat within 0.25 miles of a documented roost.

39. Is the bridge removal, replacement, or maintenance activities portion of this project consistent with a No Effect determination in this key?

Automatically answered

Yes, because the bridge has been assessed using the criteria documented in the BA and no signs of bats were detected

40. **General AMM 1**

Will the project ensure *all* operators, employees, and contractors working in areas of known or presumed bat habitat are aware of *all* FHWA/FRA/FTA (Transportation Agencies) environmental commitments, including all applicable Avoidance and Minimization Measures?

Yes

41. **Tree Removal AMM 1**

Can *all* phases/aspects of the project (e.g., temporary work areas, alignments) be modified, to the extent practicable, to avoid tree removal^[1] in excess of what is required to implement the project safely?

Note: Tree Removal AMM 1 is a minimization measure, the full implementation of which may not always be practicable. Projects may still be NLAA as long as Tree Removal AMMs 2, 3, and 4 are implemented and LAA as long as Tree Removal AMMs 3, 5, 6, and 7 are implemented.

[1] The word “trees” as used in the AMMs refers to trees that are suitable habitat for each species within their range. See the USFWS’ current summer survey guidance for our latest definitions of suitable habitat.

Yes

42. **Tree Removal AMM 3**

Can tree removal be limited to that specified in project plans and ensure that contractors understand clearing limits and how they are marked in the field (e.g., install bright colored flagging/fencing prior to any tree clearing to ensure contractors stay within clearing limits)?

Yes

43. **Tree Removal AMM 4**

Can the project avoid cutting down/removal of *all* (1) **documented**^[1] Indiana bat or NLEB roosts^[2] (that are still suitable for roosting), (2) trees **within** 0.25 miles of roosts, and (3) documented foraging habitat any time of year?

[1] The word documented means habitat where bats have actually been captured and/or tracked.

[2] Documented roosting or foraging habitat – for the purposes of this consultation, we are considering documented habitat as that where Indiana bats and/or NLEB have actually been captured and tracked using (1) radio telemetry to roosts; (2) radio telemetry triangulation/triangulation to estimate foraging areas; or (3) foraging areas with repeated use documented using acoustics. Documented roosting habitat is also considered as suitable summer habitat within 0.25 miles of documented roosts.)

Yes

44. **Lighting AMM 2**

Does the lead agency use the BUG (Backlight, Uplight, and Glare) system developed by the Illuminating Engineering Society^[1] to rate the amount of light emitted in unwanted directions?

[1] Refer to [The BUG System—A New Way To Control Stray Light](#)

Yes

45. **Lighting AMM 2**

Will the **permanent** lighting used during removal of suitable habitat and/or the removal/trimming of trees within suitable habitat be designed to be as close to 0 for all three BUG ratings as possible, with a priority of "uplight" of 0 and "backlight" as low as practicable?

Yes

46. **Lighting AMM 1**

Will *all* **temporary** lighting be directed away from suitable habitat during the active season?

Yes

PROJECT QUESTIONNAIRE

1. Have you made a No Effect determination for *all* other species indicated on the FWS IPaC generated species list?

No

2. Have you made a May Affect determination for *any* other species on the FWS IPaC generated species list?

Yes

3. How many acres^[1] of trees are proposed for removal between 0-100 feet of the existing road/rail surface?

[1] If described as number of trees, multiply by 0.09 to convert to acreage and enter that number.

1.3

4. Please describe the proposed bridge work:

Bridge Replacement, addition of trail

5. Please state the timing of all proposed bridge work:

Start Spring of 2026, last 1.5 construction seasons

6. Please enter the date of the bridge assessment:

October 19, 2023

AVOIDANCE AND MINIMIZATION MEASURES (AMMS)

This determination key result includes the commitment to implement the following Avoidance and Minimization Measures (AMMs):

TREE REMOVAL AMM 2

Apply time of year restrictions for tree removal when bats are not likely to be present, or limit tree removal to 10 or fewer trees per project at any time of year within 100 feet of existing road/rail surface and **outside of documented** roosting/foraging habitat or travel corridors; visual emergence survey must be conducted with no bats observed.

LIGHTING AMM 2

When installing new or replacing existing permanent lights, use downward-facing, full cut-off lens lights (with same intensity or less for replacement lighting); or for those transportation agencies using the BUG system developed by the Illuminating Engineering Society, be as close to 0 for all three ratings with a priority of "uplight" of 0 and "backlight" as low as practicable.

TREE REMOVAL AMM 3

Ensure tree removal is limited to that specified in project plans and ensure that contractors understand clearing limits and how they are marked in the field (e.g., install bright colored flagging/fencing prior to any tree clearing to ensure contractors stay within clearing limits).

TREE REMOVAL AMM 4

Do not remove **documented** Indiana bat or NLEB roosts that are still suitable for roosting, or trees within 0.25 miles of roosts, or **documented** foraging habitat any time of year.

GENERAL AMM 1

Ensure all operators, employees, and contractors working in areas of known or presumed bat habitat are aware of all FHWA/FRA/FTA (Transportation Agencies) environmental commitments, including all applicable AMMs.

TREE REMOVAL AMM 1

Modify all phases/aspects of the project (e.g., temporary work areas, alignments) to avoid tree removal.

LIGHTING AMM 1

Direct temporary lighting away from suitable habitat during the active season.

DETERMINATION KEY DESCRIPTION: FHWA, FRA, FTA PROGRAMMATIC CONSULTATION FOR TRANSPORTATION PROJECTS AFFECTING NLEB OR INDIANA BAT

This key was last updated in IPaC on July 27, 2023. Keys are subject to periodic revision.

This decision key is intended for projects/activities funded or authorized by the Federal Highway Administration (FHWA), Federal Railroad Administration (FRA), and/or Federal Transit Administration (FTA), which may require consultation with the U.S. Fish and Wildlife Service (Service) under Section 7 of the Endangered Species Act (ESA) for the endangered **Indiana bat** (*Myotis sodalis*) and the endangered **northern long-eared bat** (NLEB) (*Myotis septentrionalis*).

This decision key should only be used to verify project applicability with the Service's [amended February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion \(dated March 23, 2023\) for Transportation Projects](#). The programmatic biological opinion covers limited transportation activities that may affect either bat species, and addresses situations that are both likely and not likely to adversely affect either bat species. This decision key will assist in identifying the effect of a specific project/activity and applicability of the programmatic consultation. The programmatic biological opinion is not intended to cover all types of transportation actions. Activities outside the scope of the programmatic biological opinion, or that may affect ESA-listed species other than the Indiana bat or NLEB, or any designated critical habitat, may require additional ESA Section 7 consultation.

IPAC USER CONTACT INFORMATION

Agency: Indiana Department of Transportation

Name: Stewart Michels

Address: 315 East Boyd Blvd.

City: LaPorte

State: IN

Zip: 46350










Email: smichels@indot.in.gov

Phone: 2193257560








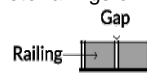

LEAD AGENCY CONTACT INFORMATION

Lead Agency: Federal Highway Administration

Bridge/Structure Bat Assessment Form

Date & Time of Assessment 10/26/2023 11:00 AM		DOT Project Des. No. 1900009		Route/Facility US 20 over Kennedy Ave		County Lake	
Federal Structure ID Str. No. 020-45-02484 C		Structure Coordinates 41.6100894 (latitude and longitude) -87.4616040		Structure Height (approximate) 50 ft		Structure Length 193 feet	
Structure Type (check one)				Structure Material (check all that apply)			
Bridge Construction Style				Deck Material		Beam Material	
<input type="radio"/> Cast-in-place  <input type="radio"/> Pre-stressed Girder 				<input type="checkbox"/> Metal <input checked="" type="checkbox"/> Concrete <input type="checkbox"/> Timber <input type="checkbox"/> Open grid <input type="checkbox"/> Other:		<input checked="" type="checkbox"/> None <input checked="" type="checkbox"/> Concrete <input type="checkbox"/> Steel <input type="checkbox"/> Timber <input type="checkbox"/> Other:	
<input type="radio"/> Flat Slab/Box  <input type="radio"/> Steel I-beam 						<input type="checkbox"/> Concrete <input type="checkbox"/> Timber <input type="checkbox"/> Stone/Masonry <input type="checkbox"/> Other:	
<input type="radio"/> Truss  <input type="radio"/> Covered 						Creosote Evidence <input type="radio"/> Yes <input checked="" type="radio"/> No	
<input type="radio"/> Parallel Box Beam  <input type="radio"/> Other:				Culvert Material <input type="checkbox"/> Metal <input type="checkbox"/> Concrete <input type="checkbox"/> Plastic <input type="checkbox"/> Stone/Masonry <input type="checkbox"/> Other:		<input type="radio"/> Unknown Notes:	
Culvert Type		Other Structure					
<input type="radio"/> Box <input type="radio"/> Pipe/Round <input type="radio"/> Other:		<input type="radio"/> <input type="radio"/> <input type="radio"/> Other:					
Crossings Traversed (check all that apply)				Surrounding Habitat (check all that apply)			
<input type="checkbox"/> Bare ground <input type="checkbox"/> Rip-rap <input type="checkbox"/> Flowing water <input type="checkbox"/> Standing water <input type="checkbox"/> Seasonal water				<input type="checkbox"/> Open vegetation <input type="checkbox"/> Closed vegetation <input checked="" type="checkbox"/> Railroad <input checked="" type="checkbox"/> Road/trail - Type: Minor Arterial <input type="checkbox"/> Other:			
				<input type="checkbox"/> Agricultural <input checked="" type="checkbox"/> Commercial <input type="checkbox"/> Residential-urban <input type="checkbox"/> Residential-rural <input type="checkbox"/> Woodland/forested			
				<input type="checkbox"/> Grassland <input type="checkbox"/> Ranching <input checked="" type="checkbox"/> Riparian/wetland <input type="checkbox"/> Mixed use <input type="checkbox"/> Other:			
Areas Assessed (check all that apply)							
Check all areas that apply. If an area is not present in the structure, check the "not present" box.							
Document all bat indicators observed during the assessment. Include the species present, if known, and provide photo documentation as indicated.							
Area (check if assessed)		Assessment Notes		Evidence of Bats (include photos if present)			
<input type="checkbox"/> All crevices and cracks: Bridges/culverts: rough surfaces or imperfections in concrete Other structures: soffits, rafters, attic areas		<input type="checkbox"/> Not present No bat indicators detected		<input type="checkbox"/> Visual - live # dead # <input type="checkbox"/> Guano <input type="checkbox"/> Staining <input type="checkbox"/> Audible <input type="checkbox"/> Odor <input type="checkbox"/> Photos <input type="checkbox"/> Species			
<input checked="" type="checkbox"/> Concrete surfaces (open roosting on concrete)		<input type="checkbox"/> Not present No bat indicators detected		<input type="checkbox"/> Visual - live # dead # <input type="checkbox"/> Guano <input type="checkbox"/> Staining <input type="checkbox"/> Audible <input type="checkbox"/> Odor <input type="checkbox"/> Photos <input type="checkbox"/> Species			
<input checked="" type="checkbox"/> Spaces between concrete end walls and the bridge deck		<input type="checkbox"/> Not present No bat indicators detected		<input type="checkbox"/> Visual - live # dead # <input type="checkbox"/> Guano <input type="checkbox"/> Staining <input type="checkbox"/> Audible <input type="checkbox"/> Odor <input type="checkbox"/> Photos <input type="checkbox"/> Species			
<input checked="" type="checkbox"/> Crack between concrete railings on top of the bridge deck <div style="text-align: center;">  </div>		<input type="checkbox"/> Not present No bat indicators detected		<input type="checkbox"/> Visual - live # dead # <input type="checkbox"/> Guano <input type="checkbox"/> Staining <input type="checkbox"/> Audible <input type="checkbox"/> Odor <input type="checkbox"/> Photos <input type="checkbox"/> Species			
<input checked="" type="checkbox"/> Vertical surfaces on concrete I-beams		<input type="checkbox"/> Not present No bat indicators detected		<input type="checkbox"/> Visual - live # dead # <input type="checkbox"/> Guano <input type="checkbox"/> Staining <input type="checkbox"/> Audible <input type="checkbox"/> Odor <input type="checkbox"/> Photos <input type="checkbox"/> Species			
<input checked="" type="checkbox"/> Spaces between walls, ceiling joists		<input type="checkbox"/> Not present No bat indicators detected		<input type="checkbox"/> Visual - live # dead # <input type="checkbox"/> Guano <input type="checkbox"/> Staining <input type="checkbox"/> Audible <input type="checkbox"/> Odor <input type="checkbox"/> Photos <input type="checkbox"/> Species			
<input checked="" type="checkbox"/> Weep holes, scupper drains, and inlets/pipes		<input type="checkbox"/> Not present No bat indicators detected		<input type="checkbox"/> Visual - live # dead # <input type="checkbox"/> Guano <input type="checkbox"/> Staining <input type="checkbox"/> Audible <input type="checkbox"/> Odor <input type="checkbox"/> Photos <input type="checkbox"/> Species			
<input checked="" type="checkbox"/> All guiderails		<input type="checkbox"/> Not present No bat indicators detected		<input type="checkbox"/> Visual - live # dead # <input type="checkbox"/> Guano <input type="checkbox"/> Staining <input type="checkbox"/> Audible <input type="checkbox"/> Odor <input type="checkbox"/> Photos <input type="checkbox"/> Species			
<input checked="" type="checkbox"/> All expansion joints		<input type="checkbox"/> Not present No bat indicators detected		<input type="checkbox"/> Visual - live # dead # <input type="checkbox"/> Guano <input type="checkbox"/> Staining <input type="checkbox"/> Audible <input type="checkbox"/> Odor <input type="checkbox"/> Photos <input type="checkbox"/> Species			
Name: C.J. Cunningham				Signature: 			

Bridge/Structure Bat Assessment Form

Date & Time of Assessment 10/26/2023 10:45 AM		DOT Project Des. No. 1900009		Route/Facility Carried South of South Kennedy ramp		County Lake	
Federal Structure ID N/A		Structure Coordinates 41.609654, -87.459969 (latitude and longitude)		Structure Height (approximate) 12 in		Structure Length 87 feet	
Structure Type (check one)				Structure Material (check all that apply)			
Bridge Construction Style				Deck Material		Beam Material	
<input type="radio"/> Cast-in-place 		<input type="radio"/> Pre-stressed Girder 		<input type="checkbox"/> Metal		<input type="checkbox"/> None	
<input type="radio"/> Flat Slab/Box 		<input type="radio"/> Steel I-beam 		<input type="checkbox"/> Concrete		<input type="checkbox"/> Concrete	
<input type="radio"/> Truss 		<input type="radio"/> Covered 		<input type="checkbox"/> Timber		<input type="checkbox"/> Steel	
<input type="radio"/> Parallel Box Beam 		<input type="radio"/> Other:		<input type="checkbox"/> Open grid		<input type="checkbox"/> Timber	
				<input type="checkbox"/> Other:		<input type="checkbox"/> Other:	
Culvert Type				Culvert Material		Creosote Evidence	
<input type="radio"/> Box		<input type="radio"/> Other Structure		<input checked="" type="checkbox"/> Metal		<input type="radio"/> Yes <input checked="" type="radio"/> No	
<input checked="" type="radio"/> Pipe/Round				<input type="checkbox"/> Concrete		<input type="radio"/> Unknown	
<input type="radio"/> Other:				<input type="checkbox"/> Plastic		Notes:	
				<input type="checkbox"/> Stone/Masonry			
				<input type="checkbox"/> Other:			
Crossings Traversed (check all that apply)				Surrounding Habitat (check all that apply)			
<input type="checkbox"/> Bare ground		<input type="checkbox"/> Open vegetation		<input type="checkbox"/> Agricultural		<input type="checkbox"/> Grassland	
<input type="checkbox"/> Rip-rap		<input checked="" type="checkbox"/> Closed vegetation		<input checked="" type="checkbox"/> Commercial		<input type="checkbox"/> Ranching	
<input type="checkbox"/> Flowing water		<input type="checkbox"/> Railroad		<input type="checkbox"/> Residential-urban		<input checked="" type="checkbox"/> Riparian/wetland	
<input checked="" type="checkbox"/> Standing water		<input type="checkbox"/> Road/trail - Type:		<input type="checkbox"/> Residential-rural		<input checked="" type="checkbox"/> Mixed use	
<input type="checkbox"/> Seasonal water		<input type="checkbox"/> Other:		<input type="checkbox"/> Woodland/forested		<input type="checkbox"/> Other:	
Areas Assessed (check all that apply)							
Check all areas that apply. If an area is not present in the structure, check the "not present" box.							
Document all bat indicators observed during the assessment. Include the species present, if known, and provide photo documentation as indicated.							
Area (check if assessed)		Assessment Notes		Evidence of Bats (include photos if present)			
<input type="checkbox"/> All crevices and cracks: Bridges/culverts: rough surfaces or imperfections in concrete Other structures: soffits, rafters, attic areas		<input type="checkbox"/> Not present		<input type="checkbox"/> Visual - live # dead #			
<input checked="" type="checkbox"/>		No bat indicators detected		<input type="checkbox"/> Audible <input type="checkbox"/> Species			
				<input type="checkbox"/> Odor			
				<input type="checkbox"/> Guano			
				<input type="checkbox"/> Photos			
				<input type="checkbox"/> Staining			
<input type="checkbox"/> Concrete surfaces (open roosting on concrete)		<input checked="" type="checkbox"/> Not present		<input type="checkbox"/> Visual - live # dead #			
				<input type="checkbox"/> Audible <input type="checkbox"/> Species			
				<input type="checkbox"/> Odor			
				<input type="checkbox"/> Guano			
				<input type="checkbox"/> Photos			
				<input type="checkbox"/> Staining			
<input type="checkbox"/> Spaces between concrete end walls and the bridge deck		<input checked="" type="checkbox"/> Not present		<input type="checkbox"/> Visual - live # dead #			
				<input type="checkbox"/> Audible <input type="checkbox"/> Species			
				<input type="checkbox"/> Odor			
				<input type="checkbox"/> Guano			
				<input type="checkbox"/> Photos			
				<input type="checkbox"/> Staining			
<input type="checkbox"/> Crack between concrete railings on top of the bridge deck 		<input checked="" type="checkbox"/> Not present		<input type="checkbox"/> Visual - live # dead #			
				<input type="checkbox"/> Audible <input type="checkbox"/> Species			
				<input type="checkbox"/> Odor			
				<input type="checkbox"/> Guano			
				<input type="checkbox"/> Photos			
				<input type="checkbox"/> Staining			
<input type="checkbox"/> Vertical surfaces on concrete I-beams		<input checked="" type="checkbox"/> Not present		<input type="checkbox"/> Visual - live # dead #			
				<input type="checkbox"/> Audible <input type="checkbox"/> Species			
				<input type="checkbox"/> Odor			
				<input type="checkbox"/> Guano			
				<input type="checkbox"/> Photos			
				<input type="checkbox"/> Staining			
<input checked="" type="checkbox"/> Spaces between walls, ceiling joists		<input checked="" type="checkbox"/> Not present		<input type="checkbox"/> Visual - live # dead #			
				<input type="checkbox"/> Audible <input type="checkbox"/> Species			
				<input type="checkbox"/> Odor			
				<input type="checkbox"/> Guano			
				<input type="checkbox"/> Photos			
				<input type="checkbox"/> Staining			
<input checked="" type="checkbox"/> Weep holes, scupper drains, and inlets/pipes		<input type="checkbox"/> Not present		<input type="checkbox"/> Visual - live # dead #			
		No bat indicators detected		<input type="checkbox"/> Audible <input type="checkbox"/> Species			
				<input type="checkbox"/> Odor			
				<input type="checkbox"/> Guano			
				<input type="checkbox"/> Photos			
				<input type="checkbox"/> Staining			
<input type="checkbox"/> All guiderails		<input checked="" type="checkbox"/> Not present		<input type="checkbox"/> Visual - live # dead #			
				<input type="checkbox"/> Audible <input type="checkbox"/> Species			
				<input type="checkbox"/> Odor			
				<input type="checkbox"/> Guano			
				<input type="checkbox"/> Photos			
				<input type="checkbox"/> Staining			
<input type="checkbox"/> All expansion joints		<input checked="" type="checkbox"/> Not present		<input type="checkbox"/> Visual - live # dead #			
				<input type="checkbox"/> Audible <input type="checkbox"/> Species			
				<input type="checkbox"/> Odor			
				<input type="checkbox"/> Guano			
				<input type="checkbox"/> Photos			
				<input type="checkbox"/> Staining			
Name: C.J. Cunningham				Signature: 			

Categorical Exclusion

Appendix D

**Section 106 of the National Historic
Preservation Act (NHPA)**

US 20 Bridge Improvement Project Finding/800.11 Document

*US 20 over Kennedy Avenue & Indiana
Harbor Belt Railroad, 0.99 mile east of
SR 152*

City of Hammond, Lake County, IN

Des. No. 1900009

DHPA No. 28563



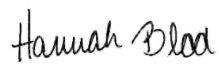
Lochmueller Group, Inc.

112 W. Jefferson Blvd., Suite 500

South Bend, Indiana 46601

Phone: 574.334.5460

Prepared for:
Indiana Department of Transportation
Federal Highway Administration

A handwritten signature in black ink that reads "Hannah Blad". The script is cursive and fluid.

Hannah Blad

September 25, 2023

**FEDERAL HIGHWAY ADMINISTRATION'S
SECTION 4(F) COMPLIANCE REQUIREMENTS (FOR HISTORIC PROPERTIES) AND
SECTION 106 FINDINGS AND DETERMINATIONS
AREA OF POTENTIAL EFFECTS
ELIGIBILITY DETERMINATIONS
EFFECT FINDING
US 20 BRIDGE IMPROVEMENT PROJECT
US 20 OVER KENNEDY AVENUE & INDIANA HARBOR BELT RAILROAD, 0.99 MILE EAST OF SR 152
CITY OF HAMMOND, LAKE COUNTY, INDIANA
DES. NO.: 1900009**

**AREA OF POTENTIAL EFFECTS
(Pursuant to 36 CFR Section 800.4(a)(1))**

The Area of Potential Effects (APE) for this project encompasses all resources immediately adjacent to the project area and those that may not be immediately adjacent but that have a proximate viewshed of the project area. The project area encompasses the area required to support the purpose and need of the project. Along US 20/Michigan Street the APE extends approximately 1,000 feet east and approximately 1,300 feet west of the project area. Along Kennedy Avenue the APE extends approximately 1,200 feet north and approximately 930 feet south of the project area. In other areas, the Indiana Toll Road (I-90) limits viewshed and therefore the APE is located on the outside edge of the Indiana Toll Road north of the project area. Additionally, the southern edge of the APE outside of Kennedy Avenue is limited by petroleum storage tanks and vegetation. The archaeological APE for the undertaking is represented by the 10.3 ha (25.5 acres) survey area encompassing portions of US 20, I-94, and Kennedy Avenue examined for the presence of archaeological resources. Please see the APE map in Appendix A, page 3.

**ELIGIBILITY DETERMINATIONS
(Pursuant to 36 CFR Section 800.4(c)(2))**

There are no properties currently listed in the National Register of Historic Places (NRHP) within the APE.

There is one property recommended eligible for listing in the NRHP within the APE:

Pumping Station (Lochmueller #2). The Pumping Station at 5721 Kennedy Avenue is a c. 1940 Art Moderne style building. Generally, the building is rectangular with central projections on the façade (west) and east elevation forming a stunted cruciform shape. Features on the building include six glass block windows, multi-light metal windows with a central hopper, smooth limestone square panels around the front entrance, and three horizontal bands of projecting limestone on the front projection. The Pumping Station is recommended eligible for listing in the NRHP under Criterion A for its association with Community Planning and Development and the New Deal as well as under Criterion C for its architectural significance.

EFFECT FINDING

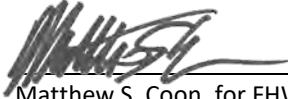
Pumping Station (Lochmueller #2) – No Adverse Effect

INDOT, acting on FHWA's behalf, has determined a "No Adverse Effect" finding is appropriate for this undertaking.

INDOT respectfully requests the Indiana State Historic Preservation Officer provide written concurrence with the Section 106 determination of effect.

SECTION 4(F) COMPLIANCE REQUIREMENTS (for historic properties)

Pumping Station (Lochmueller #2) - This undertaking will convert property from the Pumping Station (Lochmueller #2), a Section 4(f) historic property, to a transportation use; INDOT, acting on FHWA's behalf has determined the appropriate Section 106 finding is "No Adverse Effect"; therefore FHWA hereby intends to issue a "de minimis" finding for the Pumping Station (Lochmueller #2), pursuant to SAFETEA-LU, thereby satisfying FHWA's responsibilities under Section 4(f) for this historic property.



Matthew S. Coon, for FHWA
Manager
INDOT Cultural Resources

November 13, 2023

Approval Date

**FEDERAL HIGHWAY ADMINISTRATION
DOCUMENTATION OF SECTION 106 FINDING OF
NO ADVERSE EFFECT
SUBMITTED TO THE STATE HISTORIC PRESERVATION OFFICER
PURSUANT TO 36 CFR 800.5(c)
US 20 BRIDGE IMPROVEMENT PROJECT
US 20 OVER KENNEDY AVENUE & INDIANA HARBOR BELT RAILROAD, 0.99 MILE EAST OF SR 152
CITY OF HAMMOND, LAKE COUNTY, INDIANA
DES. NO.: 1900009**

1. DESCRIPTION OF THE UNDERTAKING

The Indiana Department of Transportation (INDOT) and the FHWA propose to proceed with a bridge improvement project. The proposed project is located within the City of Hammond in Lake County. The FHWA is providing funding and is the lead federal agency for this Section 106 undertaking. The project proposes to replace Structure No. 020-45-02484 B (NBI No. 005200) at its current location with a new structure. It is within North Township, Highland USGS Topographic Quadrangle, in Section 4, Township 36 North, Range 9 West and in Section 33, Township 37 North, Range 9 West. Adjacent land use consists of commercial and industrial properties, as well as recreational facilities. Please see maps and photographs of the project area in Appendices A and B.

The need for the project is evidenced by the deteriorated condition of the existing structure, Bridge No. 020-45-02484 B. The most recent INDOT Bridge Inspection Report from January 2021, noted that the abutment wall and northeast wing wall are exhibiting cracking. The superstructure was noted to have minor chipping on the beam edges in spans. The superstructure was given a condition rating of 6, which is considered to be in “satisfactory” condition and indicates minor deterioration. The substructure was given a condition rating of 5 which is considered “fair” condition and indicates minor section loss. Condition ratings range from 0, which indicates a failing structure, to 9, which indicates a new structure with no deficiencies. The purpose of the project is to improve the condition of the structure to a rating of 8 or greater, which represents a structure in “very good” condition.

The existing structure (Structure No. 020-45-02484 B/NBI No. 005200) is a three-span, continuous prestressed concrete beam bridge carrying US 20 over Kennedy Avenue and the Indiana Harbor Belt Railroad (IHBR). The out-to-out length is approximately 196 feet, and the existing clear roadway width is 52 feet. The preferred alternative involves replacement of the existing bridge with a two-span structure with mechanically stabilized earth (MSE) wall abutments, a reinforced concrete pier, a length of approximately 222 feet, a clear roadway width of 53 feet 4 inches, and an out-to-out width of approximately 70 feet 8 inches. A 4-foot-wide curbed center median will be installed between the eastbound and westbound traffic lanes along US 20. A raise in the profile grade will be necessary to provide adequate vertical clearance over the railroad and Kennedy Ave. The access roads intersection with US 20 will be shifted east to accommodate the increased profile grade and configured for right-in/right-out only movements. Additionally, the US 20 roadway grade east of the bridge will require modification to achieve proper stopping sight distance at the intersection with the access roads to Kennedy Avenue. A 10-foot-wide multi-use trail will be added to the north side of the new bridge and connect to the planned Marquette Greenway Trail. Approximately 1.13 acres of new permanent ROW and 0.020 acre of temporary ROW will be required. No relocations are anticipated.

Section 106 of the National Historic Preservation Act requires federal agencies to take into account the effects of their undertakings on above-ground and below-ground properties. The federal involvement is funding from the FHWA. The Section 106 process involves efforts to identify historic properties potentially affected by the undertaking, assess its effects and seek ways to avoid, minimize or mitigate any adverse effects on historic properties.

The APE for this project encompasses all resources immediately adjacent to the project area and those that may not be immediately adjacent but that have a proximate viewshed of the project area. The project area encompasses the area required to support the purpose and need of the project. Along US 20/Michigan Street the APE extends approximately 1,000 feet east and approximately 1,300 feet west of the project area. Along Kennedy Avenue the APE extends approximately 1,200 feet north and approximately 930 feet south of the project area. In other areas, the Indiana Toll Road (I-90) limits viewshed and therefore the APE is located on the outside edge of the Indiana Toll Road north of the project area. Additionally, the southern edge of the APE, outside of Kennedy Avenue, is limited by petroleum storage tanks and vegetation. Please see Appendix A, page 3 for a map of the aboveground APE.

2. EFFORTS TO IDENTIFY HISTORIC PROPERTIES

The NRHP, Indiana Register of Historic Sites and Structures (State Register), the Indiana Historic Sites and Structures Inventory (IHSSI), the State Historic Architectural and Archaeological Research Database (SHAARD), and the Indiana Historic Buildings, Bridges, and Cemeteries Map (IHBBM) were consulted. No resources currently listed in the NRHP are located within the APE.

The Indiana Historic Bridge Inventory Volume 2: Listing of Historic and Non-Historic Bridges (February 2009) by Mead & Hunt was reviewed. No bridges eligible for listing in the NRHP are located within the project area.

Hannah Blad, a Lochmueller Group historian who meets the Secretary of the Interior's Professional Qualification Standards, performed a site inspection of the project area on December 15, 2021, and documented resources that will be at least 50 years of age at the time of the project letting within the APE. The APE was investigated for the existence of any historic properties, structures, objects, or districts listed in or eligible for listing in the NRHP. The historian walked the APE, taking photographs of all resources meriting a Contributing or higher rating. Non-Contributing resources or those that did not meet the age requirements were noted but not documented other than in general view photographs. No previously surveyed resources were documented and four (4) newly identified aboveground resources with a recommended rating of Contributing or higher were recorded within the APE. No previously surveyed resources were found to be demolished during the field visit. As a result of this field survey, one property is recommended eligible for the NRHP within the APE: Pumping Station (Lochmueller #2, 5721 Kennedy Avenue). See Appendix E, pages 1-3 for a summary of the HPR.

A Phase Ia archaeological reconnaissance survey was conducted by Cultural Resource Analysts, Inc. (CRA) between November 30 and December 2, 2021. The field reconnaissance yielded two newly documented archaeological sites, 12La736 and 12La737. Both sites are historic artifact scatter dating to the twentieth century. The portions of the sites within the survey area are not recommended eligible for listing in the NRHP. No further archaeological work was recommended. See Appendix E, pages 4-5 for a summary of the Phase Ia archaeological reconnaissance survey.

Early coordination was initiated on December 10, 2021, with an email to consulting parties. The email asked consulting parties to review the early coordination letter attached to the email and via IN SCOPE, which is INDOT's Section 106 document website <http://erms.indot.in.gov/Section106Documents/>. A hard copy of these materials was mailed to the State Historic Preservation Officer (SHPO), Lake County Commissioners, and the Lake County Historical Society and Museum that same day. Those who were invited to become consulting parties at that time are shown below, with those accepting consulting party status shown in bold below and in Appendix C, page 1.

- **SHPO (automatic consulting party)**
- Lake County Commissioners
- **Lake County Historian**
- Hammond Historical Society
- Lake County Historic Society and Museum
- Hammond Historic Preservation Commission
- Indiana Landmarks, Northwest Field Office
- Lake County Highway Superintendent

- Northwestern Indiana Regional Planning Commission
- **Eastern Shawnee Tribe of Oklahoma**
- Forest County Potawatomi Community
- **Miami Tribe of Oklahoma**
- **Peoria Tribe of Indians of Oklahoma**
- **Pokagon Band of Potawatomi Indians**
- Shawnee Tribe

In an email dated December 14, 2021, the Lake County Historian responded to the early coordination email stating, “I have reviewed the submitted the documents and feel confident that the necessary fieldwork will identify any historical and archaeological resources that may be adversely affected by this project.” See Appendix D, page 8-9 for a copy of the communication.

In a letter dated December 14, 2021, the Peoria Tribe of Indians of Oklahoma responded to the early coordination letter stating that, “[t]he Peoria Tribe of Indians of Oklahoma is unaware of a direct link to the newly proposed project location ... The Peoria Tribe has no objection at this time to the proposed project.” See Appendix D, page 10 for a copy of the communication.

In a letter dated December 16, 2021, the SHPO staff responded to the early coordination letter stating they did not know of any other parties that should be invited to participate in the Section 106 consultation process. In that same letter, the SHPO staff asked that property owners be invited as soon as possible if right-of-way is planned to be taken from adjacent historic properties. See Appendix D, pages 11-12 for a copy of the communication.

In a letter dated December 22, 2021, the Pokagon Band of Potawatomi Indians responded to the early coordination letter stating that, “... all the proposed and alternative project site locations will have No Adverse Effect on any historic, religious, or culturally significant resources to the Pokagon Band of Potawatomi Indians.” See Appendix D, page 13 for a copy of the communication.

In a letter dated January 3, 2022, the Miami Tribe of Oklahoma responded to the early coordination letter stating that, “[t]he Miami Tribe offers no objection to the above-referenced project at this time, as we are not currently aware of existing documentation directly linking a specific Miami cultural or historic site to the project site.” See Appendix D, page 14 for a copy of the communication.

In a letter dated January 5, 2022, the Eastern Shawnee Tribe of Oklahoma responded to the early coordination letter stating that, “... the project proposes NO Adverse Effect or endangerment to known sites of interest to the Eastern Shawnee Tribe.” See Appendix D, page 15 for a copy of the communication.

No additional comments were received after the distribution of the early coordination materials.

A Historic Property Report (HPR), based on the results of the December 15, 2021, aboveground field survey, was completed (Blad, February 28, 2022) which recommended the Pumping Station (Lochmueller #2) as eligible for listing in the NRHP. Please see Appendix E, page 1-3, for a summary of the HPR.

The HPR and Phase Ia Archaeology Report were uploaded to IN SCOPE, and an email was sent to consulting parties notifying them of the availability of the reports online on February 28, 2022. Hard copies of these materials were also mailed to the SHPO on that same day.

In an email dated February 28, 2022, the Lake County Historian responded to the HPR stating, “I have reviewed the documentation sent and feel confident that the Section 106 process has been properly fulfilled and that this project can continue.” See Appendix D, page 21 for a copy of the correspondence.

In a letter dated March 1, 2022, the Peoria Tribe of Indians of Oklahoma responded to the HPR and Phase Ia Archaeology Report stating that, “[t]he Peoria Tribe has no objection at this time to the proposed project.” See Appendix D, page 22 for a copy of the correspondence.

In a letter dated March 7, 2022, the Eastern Shawnee Tribe of Oklahoma responded to the HPR and Phase Ia Archaeology Report stating that, “... the project proposes NO Adverse Effect or endangerment to known sites of interest to the Eastern Shawnee Tribe.” See Appendix D, page 23 for a copy of the correspondence.

In a letter dated March 30, 2022, the SHPO responded to the HPR concurring with most of the conclusions in the report including the eligibility of the Pumping Station (Lochmueller #2) under Criterion C. The SHPO subsequently recommended that the Pumping Station is eligible for the NRHP under Criterion A for its association with Community Planning and Development and the New Deal. In addition, in the same letter the SHPO responded to the Phase Ia Archaeology Report agreeing with the conclusions therein. In addition, the SHPO noted that though the portions of the sites 12LA0736 and 12LA0737 within the survey area lacked sufficient information to determine if the sites were eligible, the portions of the sites outside the survey area need to be avoided by construction activities and those locations clearly marked on plans. If the sites cannot be avoided, the SHPO asked that additional archaeological investigations be completed. Please see Appendix D, pages 24-25 for a copy of this communication.

No additional comments were received from consulting parties regarding the identification of historic properties.

3. DESCRIBE AFFECTED HISTORIC PROPERTIES

Pumping Station (Lochmueller #2). The Pumping Station at 5721 Kennedy Avenue is a c. 1940 Art Moderne style building. Generally, the building is rectangular with central projections on the façade (west) and east elevation forming a stunted cruciform shape. Features on the building include six glass block windows, multi-light metal windows with a central hopper, smooth limestone square panels around the front entrance, and three horizontal bands of projecting limestone on the front projection. The Pumping Station is eligible for listing in the NRHP under Criterion A for its association with Community Planning and Development and the New Deal as well as under C for its architectural significance.

4. DESCRIBE THE UNDERTAKING’S EFFECT ON HISTORIC PROPERTIES

Pumping Station (Lochmueller #2) – No Adverse Effect

The proposed undertaking will encroach upon the recommended NRHP boundary for the Pumping Station. Adjacent to the north side of the property, the ramp leading to US 20 will be reconstructed, and as a result grading will take within the northern section of the recommended NRHP boundary. This work will take place between the existing road and fence surrounding the property. Temporary ROW will be required for this grading. The fence will remain in place during the undertaking. At the northwest corner of the recommended NRHP boundary, below-ground storm sewer work will require both temporary and permanent ROW from the historic property. In total, approximately 0.011 acre of permanent ROW and 0.02 acre of temporary ROW will be acquired from the historic property.

The project will have “No Adverse Effect” to this resource because the proposed changes will not alter the Pumping Station in a manner that would diminish its historic integrity or its eligibility for listing in the NRHP. Please see Appendix A, page 4 for a map of the historic property, Appendix B, page 3-6 for photographs of this resource and Appendix F, page 20 for plan sheets showing the proposed work around the property.

5. EXPLAIN APPLICATION OF CRITERIA OF ADVERSE EFFECT – INCLUDE CONDITIONS OR FUTURE ACTIONS TO AVOID, MINIMIZE OR MITIGATE ADVERSE EFFECTS

According to 36 CFR 800.5(a)(1) “An adverse effect is found when an undertaking may alter, directly or indirectly, any of the characteristics of a historic property that qualify the property for inclusion in the National Register in a

manner that would diminish the integrity of the property's location, design, setting, materials, workmanship, feeling or association." The following discusses potential effects to the Pumping Station (Lochmueller #2). Please see maps and photographs of these resources in Appendices A and B.

Pumping Station (Lochmueller #2) – According to 36 CFR 800.5(a)(1) the criteria of adverse effect do not apply. The undertaking will alter the existing setting within the property beyond its present condition, but it will not alter it in a way that will affect its historic integrity.

Per 800.5(a)(2)(i), the undertaking will not result in the "Physical destruction of or damage to all or part of the property." Though grading and below-ground storm sewer work will take place on the north side of the historic property, this work will not result in the damage or physical destruction to contributing elements to the historic property.

Per 36 CFR 800.5(a)(2)(ii), the undertaking will not cause "Alteration of a property, including restoration, rehabilitation, repair, maintenance, stabilization, hazardous material remediation and provision of handicapped access, that is not consistent with the Secretary's Standards for the Treatment of Historic Properties and/or other applicable guidelines." Though grading and below-ground storm sewer work will take place on the north side of the historic property, this work will not result in the damage or physical destruction to contributing elements to the historic property. A Phase II Environmental Site Assessment (ESA) will occur on the Buckeye Pipeline property west of Kennedy Avenue, not on the property associated with the Pumping Station.

Per 36 CFR 800.5(a)(2)(iii), the undertaking will not result in the "Removal of the property from its historic location." The project will not remove the historic property from its current/historic location.

Per 36 CFR 800.5(a)(2)(iv), the undertaking will not result in a "Change of the character of the property's use or of physical features within the property setting that contribute to its historic significance." Though grading and below-ground storm sewer work will take place on the north side of the historic property, this work will not result in the damage or physical destruction to contributing elements to the historic property.

Per 36 CFR 800.5(a)(2)(v), the undertaking will not cause the "Introduction of visual, atmospheric or audible elements that diminish the integrity of the property's significant historic features." Generally, what is visible from the historic property will remain the same following the completion of the undertaking. The proposed work will not increase audible sound levels or atmospheric elements to the historic property.

Per 36 CFR 800.5(a)(2)(vi), the undertaking will not cause the "Neglect of a property which causes its deterioration..."

Per 36 CFR 800.5(a)(2)(vii), the undertaking will not cause the "Transfer, lease, or sale of property out of Federal ownership or control..." Ownership of the historic resource will not change as a result of this project.

6. SUMMARY OF CONSULTING PARTIES AND PUBLIC VIEWS

The following is a summary of the comments of the consulting parties following the distribution of the early coordination materials and HPR/Phase Ia (December 2021 through March 2022). These comments have been previously presented in detail above in "Section 2. Efforts to Identify Historic Properties" and the correspondence may be viewed in Appendix D, pages 1-25:

- December 14, 2021: An email from the Lake County Historian responded to the early coordination letter accepting consulting party status.
- December 14, 2021: A letter from the Peoria Tribe of Indians of Oklahoma responded to the early coordination letter accepting consulting party status.

- December 16, 2021: A letter from the SHPO stated that they were unaware of any additional consulting parties that should be invited to participate in the Section 106 process but that if ROW is to be taken from the historic properties their owners should be invited as soon as possible.
- December 22, 2021: A letter from the Pokagon Band of Potawatomi Indians responded to the early coordination letter accepting consulting party status.
- January 5, 2022: A letter from the Eastern Shawnee Tribe of Oklahoma responded to the early coordination letter accepting consulting party status.
- February 28, 2022: The HPR and Phase Ia Archaeology Report were distributed to consulting parties.
- February 28, 2022: An email from the Lake County Historian concurs with the findings therein.
- March 1, 2022: A letter from the Peoria Tribe of Indians of Oklahoma noting that the project will not affect any known sites important to the tribe.
- March 7, 2022: A letter from the Eastern Shawnee Tribe of Oklahoma noting that the project will have a No Adverse Effect.
- March 30, 2022: A letter from SHPO concurred with the conclusions from the HPR including the APE and eligibility of the Pumping Station (Lochmueller #2) under Criterion C and stating its eligibility under Criterion A as well. In the same letter, the SHPO staff concurred with the findings within the Phase Ia Archaeology Report.

On August 22, 2023, a preliminary effects report recommending a finding of “No Adverse Effect” to the North Pumping Station and for the overall project finding was uploaded to IN SCOPE and an email was sent to consulting parties notifying them of the report. A hard copy of the report was mailed to SHPO on that same day. Please see Appendix E, pages 6-8 for a summary of the report.

In an email dated August 22, 2023, the Lake County Historian responded to the effects report stating, “I have reviewed the effects report and understand the proposed work within this project poses no direct or indirect threats to any historical or archaeological resources.” Please see Appendix D, page 32 for a copy of the correspondence.

On September 18, 2023, the SHPO responded to the preliminary effects report stating, “[w]e agree with the conclusions of the effects report that the proposed work and ROW acquisition will not diminish the characteristics of the Kennedy Avenue Pumping Station which make it significant.” In the same letter in regard to the archaeological resources, the SHPO reiterated that portions of sites 12LA0736 and 12LA0737 outside the survey area need to be avoided and marked clearly on plans. If the sites outside the survey area cannot be avoided, then SHPO recommended that subsurface archaeological investigations must occur. See Appendix D, pages 33-34 for a copy of the correspondence.

The portions of sites 12LA0736 and 12LA0737 outside the survey area were unable to be marked on the plan sheets in the attached Appendix F because the project area does not extend past the portions of the sites previously surveyed by CRA. The archaeology survey area encompassed a large area wherein potential disturbance or ROW acquisition would occur for this undertaking, when in fact the actual project area where construction activities will take place and ROW acquisition will occur is much smaller than the survey area. The proposed undertaking will not encroach upon archaeology sites 12LA0736 and 12LA0737 as mapped in the above-referenced archaeological reconnaissance report. The portions of the sites outside the survey area are too far away from the construction area to show up on the plan sheets for the project.

No other consulting party comments were received. No consulting parties expressed an interest in participating in a consulting party meeting.

A public notice will be published in *The Times of Northwest Indiana* newspaper seeking the views of the public regarding the effects of the proposed project on the historic elements within the APE. Comments from the public will be accepted for 30 days following the publication of the notice. If any substantive comments are received during this period, this document will be revised to include them.

APPENDICES

A – Maps

B – General Photographs

C – Consulting Parties List

D – Consulting Parties Correspondence

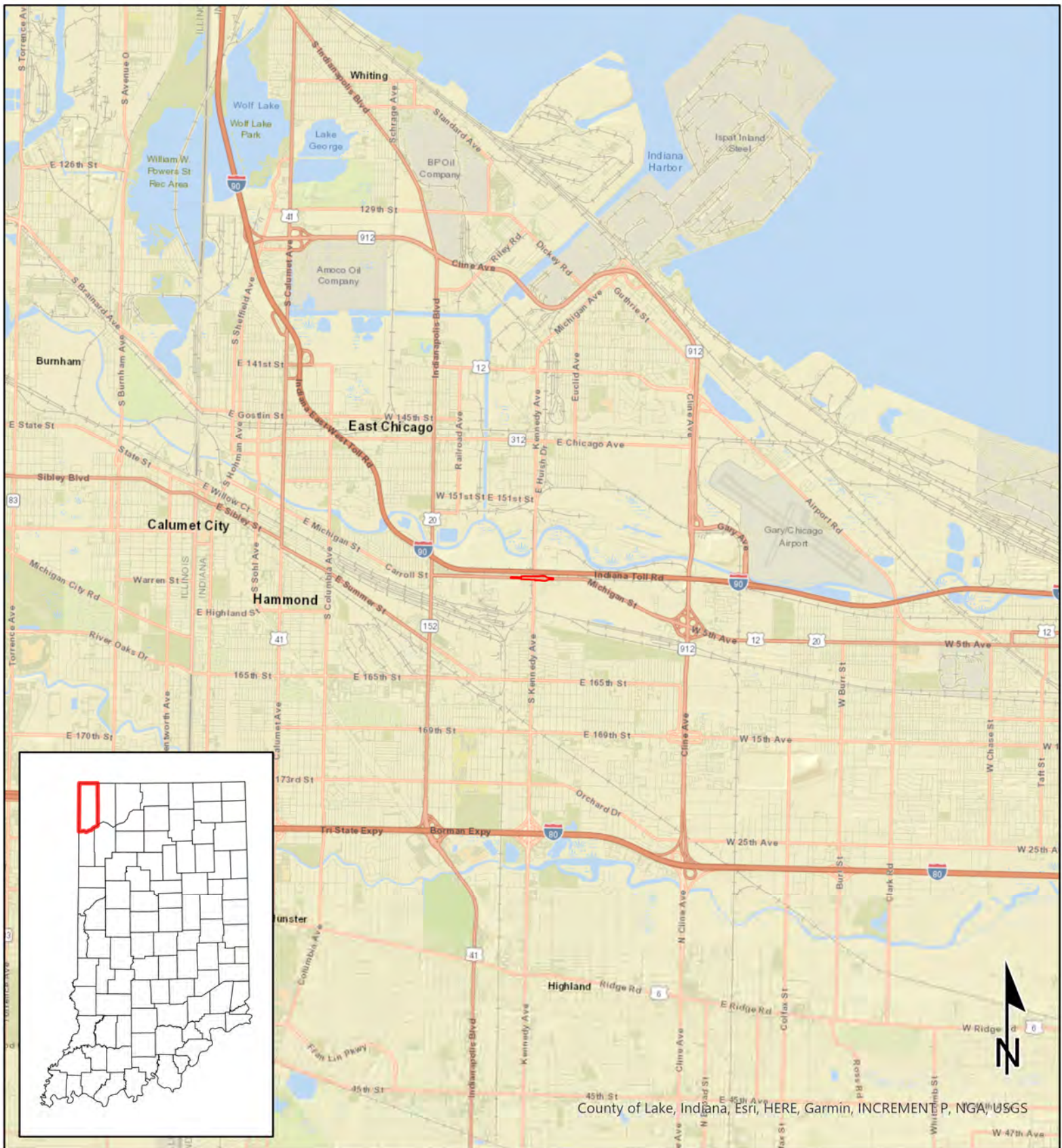
E – Historic Property Report Summary, Archaeology Report Summary, Effects Report Summary

F – Preliminary Field Check Plans

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Appendix A

Maps




Sources:
Non Orthophotography
Data - Obtained from the State of Indiana Geographical Information Office Library
Orthophotography - Obtained from Indiana Map Framework Data (www.indianamap.org)
Map Projection: UTM Zone 16 N **Map Datum:** NAD83

This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.

Des. No. 1900009
 US 20 Bridge Improvement Project
 US 20 over Kennedy Avenue & IHBR, 0.99 miles east of SR 152
 City of Hammond, Lake County, Indiana

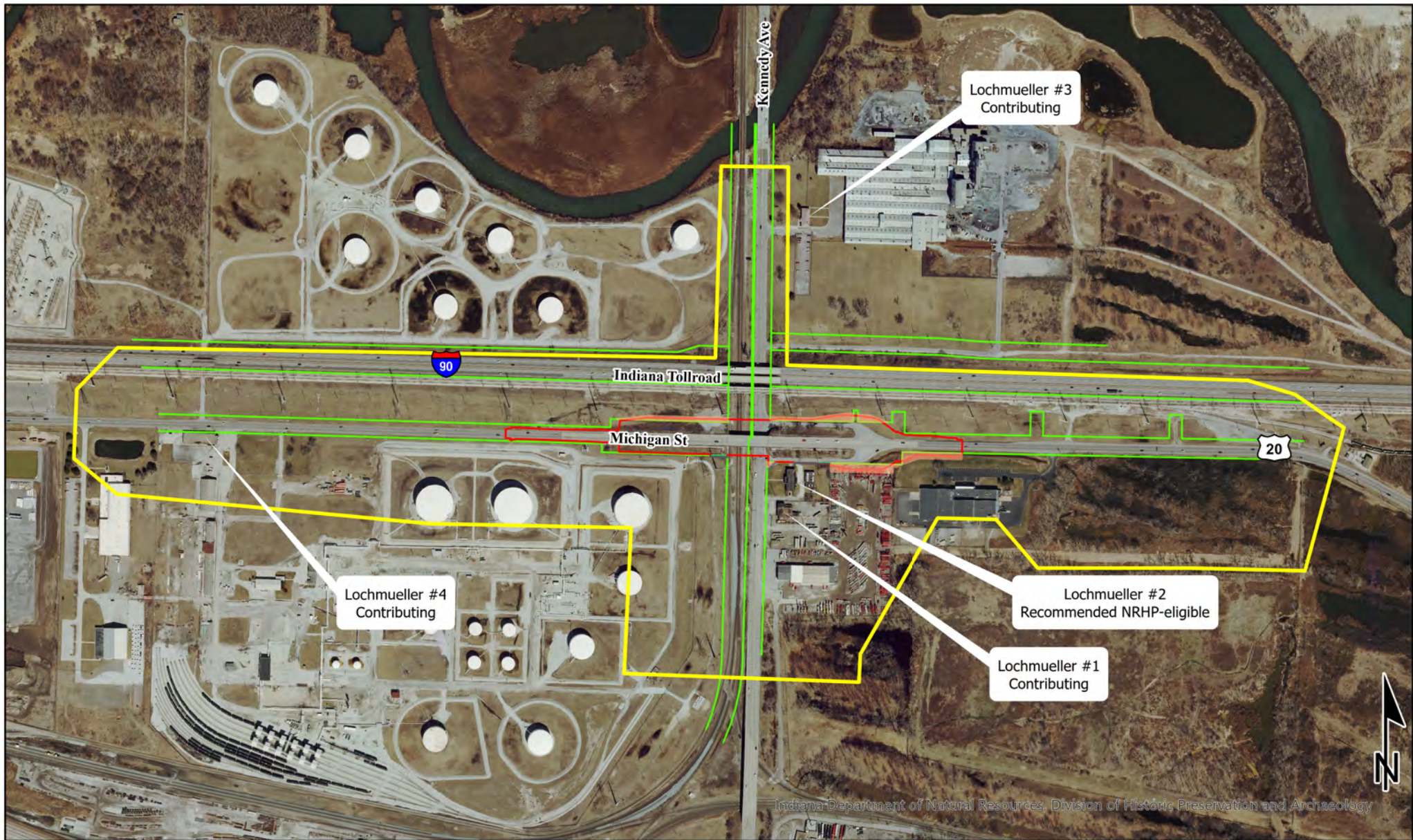
General Location Map

— Construction Limits

Sources: 0.25 0.13 0 0.25
Non Orthophotography  Miles
Data - Obtained from the State of Indiana Geographical Information Office Library
Orthophotography - Obtained from Indiana Map Framework Data (www.indianamap.org)
Map Projection: UTM Zone 16 N **Map Datum:** NAD83
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City of Hammond, Lake County, Indiana

HIGHLAND QUADRANGLE
INDIANA
7.5 MINUTE SERIES (TOPOGRAPHIC)
— Construction Limits



Sources: 400 200 0 400 Feet
Non Orthophotography
Data - Obtained from the State of Indiana Geographical Information Office Library
Orthophotography - Obtained from Indiana Map Framework Data (www.indianamap.org)
Map Projection: UTM Zone 16 N **Map Datum:** NAD83
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Des. No. 1900009
 US 20 Bridge Improvement Project
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 City of Hammond, Lake County, Indiana

Area of Potential Effects Map

	County Survey Sites	Historic Bridges
 APE	RATING	RATING
 Construction Limits	● Outstanding	■ Outstanding
 Permanent ROW	● Notable	■ Notable
 Temporary ROW	● Contributing	■ Contributing
 Existing ROW	● Non-Contributing	■ Non-Contributing
★ National Register Sites	● Demolished	■ Demolished
 Historic Districts	● Unknown	■ Unknown
▲ Cemeteries		



Indiana Department of Natural Resources, Division of Historic Preservation and Archaeology

Sources:
Non Orthophotography - Obtained from the State of Indiana Geographical Information Office Library
Orthophotography - Obtained from Indiana Map Framework Data (www.indianamap.org)
Map Projection: UTM Zone 16 N **Map Datum:** NAD83

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40 20 0 40
 Feet

Des. No. 1900009
 US 20 Bridge Improvement Project
 US 20 over Kennedy Avenue & IHBR, 0.99 miles east of SR 152
 City of Hammond, Lake County, Indiana

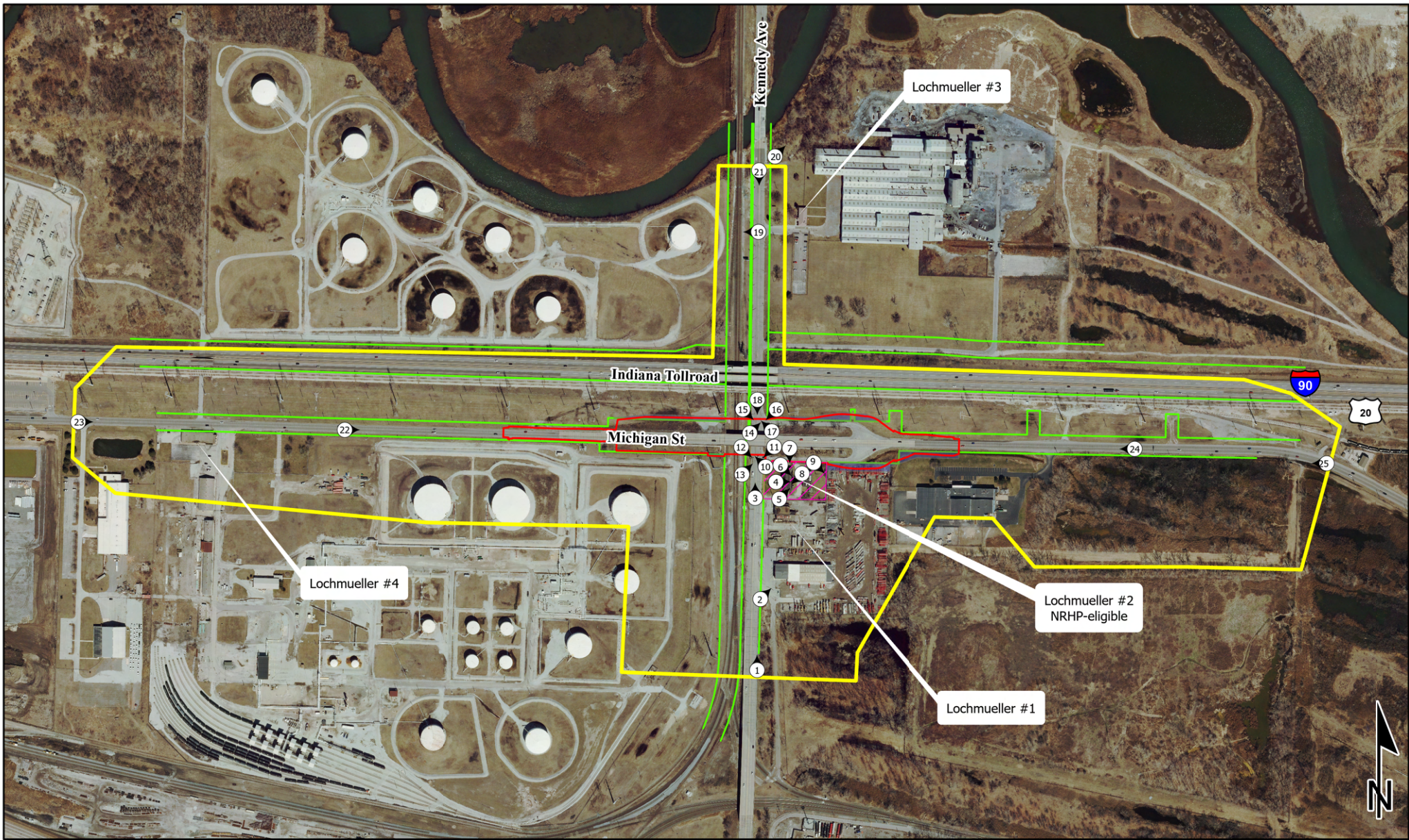
Pumping Station (Lochmueller #2) NRHP Boundary Map

Recommended NRHP Boundary	County Survey Sites	Historic Bridges
APE	RATING	RATING
Construction Limits	Outstanding	Outstanding
Permanent ROW	Notable	Notable
Temporary ROW	Contributing	Contributing
Existing ROW	Non-Contributing	Non-Contributing
National Register Sites	Demolished	Demolished
Historic Districts	Unknown	Unknown
Cemeteries		

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Appendix B

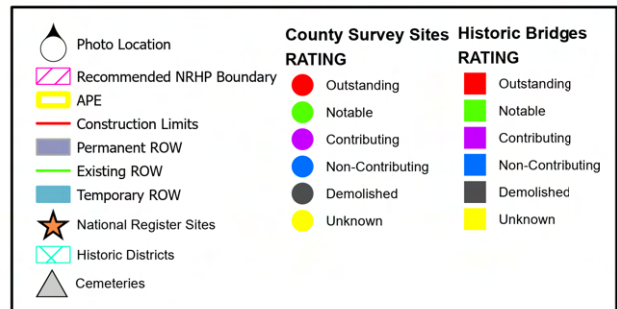
General Photographs



Sources: 400 200 0 400 Feet
Non Orthophotography
Data - Obtained from the State of Indiana Geographical Information Office Library
Orthophotography - Obtained from Indiana Map Framework Data (www.indianamap.org)
Map Projection: UTM Zone 16 N **Map Datum:** NAD83
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Des. No. 1900009
 US 20 Bridge Improvement Project
 US 20 over Kennedy Avenue & IHBR, 0.99 miles east of SR 152
 City of Hammond, Lake County, Indiana

Photo Location Map





1. Looking N from end of APE on Kennedy Avenue



2. Looking NE at Non-Contributing commercial building



3. Looking N from S end of project area on Kennedy Avenue



4. Looking E at Lochmueller #2 (Pumping Station, Notable, 5721 Kennedy Avenue)



5. Looking NE at Lochmueller #2 (Pumping Station, Notable, 5721 Kennedy Avenue)



6. Looking SE at Lochmueller #2 (Pumping Station, Notable, 5721 Kennedy Avenue)



7. Looking S at Lochmueller #2 (Pumping Station, Notable, 5721 Kennedy Avenue)



8. Looking SW at Lochmueller #2 (Pumping Station, Notable, 5721 Kennedy Avenue)



9. Looking SE at auxiliary building behind Lochmueller #2 (Pumping Station, Notable, 5721 Kennedy Avenue)



10. Looking NW at the intersection of Kennedy Avenue and the south US 20 entrance/exit ramp



11. Looking SW at the intersection of Kennedy Avenue and the south US 20 entrance/exit ramp



12. Looking SE at the intersection of Kennedy Avenue and the south US 20 entrance/exit ramp



13. Looking NE at the intersection of Kennedy Avenue and the south US 20 entrance/exit ramp



14. Looking NE at the intersection of Kennedy Avenue and the north US 20 entrance/exit ramp



15. Looking SE at the intersection of Kennedy Avenue and the north US 20 entrance/exit ramp



16. Looking SW at the intersection of Kennedy Avenue and the north US 20 entrance/exit ramp



17. Looking NW at the intersection of Kennedy Avenue and the north US 20 entrance/exit ramp



18. Looking S from N end of project area on Kennedy Avenue



19. Looking W at oil farm of Buckeye Terminal, Non-Contributing resources



20. Looking NE at communication shelter, Non-Contributing resource



21. Looking S on Kennedy Avenue from N end of APE



22. Looking E on US 20/Michigan Street from W end of project area



23. Looking E on US 20/Michigan Street from W end of APE



24. Looking W on US 20/Michigan Street from E end of project area



25. Looking W on US 20/Michigan Street from E end of APE

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Appendix C

Consulting Parties List

Consulting Party List

Des. No. 1900009

US 20 Bridge Improvement Project

US 20 over Kennedy Avenue, 0.99 mile east of SR 152

City of Hammond, Lake County, Indiana

Automatic Section 106 Consulting Party:

Indiana Department of Natural Resources

Division of Historic Preservation & Archaeology

Chad Slider, Assistant Director for Environmental Review

402 W. Washington St., Room W274

Indianapolis, Indiana 46204

cslider@dnr.in.gov

Invited Consulting Parties:

Lake County Commissioners

Building 'A', 3rd Floor

2293 N. Main Street

Crown Point, IN 46307

Mail hardcopy

Northwestern Indiana Regional Planning Commission

6100 Southport Road

Portage, IN 46368

nirpc@nirpc.org

Lake County Historian

Scott Bocock

13206 Parrish Ave.

Cedar Lake, IN 46303

scott.bocock@yahoo.com

Eastern Shawnee Tribe of Oklahoma

Forest County Potawatomi Community

Miami Tribe of Oklahoma

Peoria Tribe of Indians of Oklahoma

Pokagon Band of Potawatomi Indians

Shawnee Tribe

Hammond Historical Society

564 State St.

Hammond, IN 46320-1532

hammondhistory@hotmail.com

Lake County Historical Society and Museum

Courthouse Square, Suite 205

Crown Point, IN 46307

Mail hardcopy

Hammond Historic Preservation Commission

Brian Poland, Director of City Planning

City of Hammond Department of Planning

5925 Calumet Avenue Room G17

Hammond, IN 46320

polandb@gohammond.com

Indiana Landmarks – Northwest Field Office

Brad Miller, Director

541 South Lake Street

Gary, IN 46403

bmiller@indianalandmarks.org

Lake County Highway Superintendent

Jan S. Smoljan

1100 East Monitor Street

Crown Point, IN 46307

smoljjs@lakecountyin.org

***Participating Consulting Parties in BOLD**

Section 106 800.11(e)

Appendix D

Consulting Parties Correspondence

Hannah Blad

From: Hannah Blad
Sent: Friday, December 10, 2021 8:46 AM
To: Slider, Chad (DNR); scott.bocock@yahoo.com; hammondhistory@hotmail.com; Brian Poland; Brad Miller; smoljjs@lakecountyin.org; nirpc@nirpc.org
Cc: Miller, Shaun (INDOT); sbranigin; Michels, Stewart; Borden, Andrew A; Gary Quigg; C.J. Cunningham; Michael Vereb; Jason Hickle
Subject: FHWA Project: Des. No. 1900009; ECL, US 20 Bridge Improvement Project, Lake County, Indiana
Attachments: US20BridgeImprovementProject_Des1900009_EarlyCoordinationLetter_2021-12-10.pdf

Des. No.: 1900009

Project Description: Bridge Improvement

Location: US 20 over Kennedy Avenue, City of Hammond, Lake County, Indiana

The Indiana Department of Transportation, with funding from the Federal Highway Administration, proposes to proceed with a bridge improvement project (Des. No. 1900009).

Section 106 of the National Historic Preservation Act requires federal agencies to take into account the effects of their undertakings on historic properties. The following agencies/individuals are being invited to become consulting parties:

- State Historic Preservation Officer
- Lake County Commissioners
- Lake County Historian
- Hammond Historical Society
- Lake County Historic Society and Museum
- Hammond Historic Preservation Commission
- Indiana Landmarks, Northwest Field Office
- Lake County Highway Superintendent
- Northwestern Indiana Regional Planning Commission
- Eastern Shawnee Tribe of Oklahoma
- Forest County Potawatomi Community
- Miami Tribe of Oklahoma
- Peoria Tribe of Indians of Oklahoma
- Pokagon Band of Potawatomi Indians
- Shawnee Tribe

This letter is part of the early coordination phase of the environmental review process requesting comments associated with this project. We are requesting comments from your area of expertise regarding any possible environmental effects associated with this project. Please use the above Des. Number and project description in your reply and your comments will be incorporated into the formal environmental study.

Please review the attached letter, which is also located in IN SCOPE at <http://erms.indot.in.gov/Section106Documents/> (the Des. No. is the most efficient search term, once in IN SCOPE), and respond with your comments on any historic resource impacts incurred as a result of this project so that an environmental report can be completed. We also welcome your related opinions and other input to be considered in the preparation of the environmental document. If a hard copy of the materials is needed, please respond to this email with your request as soon as you can.

Consulting parties have thirty (30) calendar days from receipt of this information to review and provide comments. If we do not receive a response from an invited consulting party within the time allotted, the project will proceed consistent with the proposed design. Tribal consulting parties may enter the process at any time and are encouraged to respond to this notification with any comments or concerns at their earliest convenience.

Tribal contacts may contact Shaun Miller at smiller@indot.in.gov or 317-416-0876 or Kari Carmany-George at FHWA at K.CarmanyGeorge@dot.gov or 317-226-5629.

Thank you in advance for your input,

Hannah Blad

Hist/Sec 106 Specialist II

Lochmueller Group



Direct: 574.334.5487

Mobile: 574.248.2121

Hannah Blad

From: Miller, Shaun (INDOT) <smiller@indot.IN.gov>
Sent: Friday, December 10, 2021 9:36 AM
To: thpo@estoo.net; Michael LaRonge; Diane Hunter; kstand@peoriatribe.com; Matthew.Bussler@pokagonband-nsn.gov; tonya@shawnee-tribe.com
Cc: Hannah Blad; Ross, Anthony; Carmany-George, Karstin (FHWA)
Subject: FHWA Project: Des. No. 1900009; ECL, US 20 Bridge Improvement Project, Lake County, Indiana
Attachments: US20BridgeImprovementProject_Des1900009_EarlyCoordinationLetter_2021-12-10.pdf

Des. No.: 1900009

Project Description: Bridge Improvement

Location: US 20 over Kennedy Avenue, City of Hammond, Lake County, Indiana

The Indiana Department of Transportation, with funding from the Federal Highway Administration, proposes to proceed with a bridge improvement project (Des. No. 1900009).

Section 106 of the National Historic Preservation Act requires federal agencies to take into account the effects of their undertakings on historic properties. The following agencies/individuals are being invited to become consulting parties:

- State Historic Preservation Officer
- Lake County Commissioners
- Lake County Historian
- Hammond Historical Society
- Lake County Historic Society and Museum
- Hammond Historic Preservation Commission
- Indiana Landmarks, Northwest Field Office
- Lake County Highway Superintendent
- Northwestern Indiana Regional Planning Commission
- Eastern Shawnee Tribe of Oklahoma
- Forest County Potawatomi Community
- Miami Tribe of Oklahoma
- Peoria Tribe of Indians of Oklahoma
- Pokagon Band of Potawatomi Indians
- Shawnee Tribe

This letter is part of the early coordination phase of the environmental review process requesting comments associated with this project. We are requesting comments from your area of expertise regarding any possible environmental effects associated with this project. Please use the above Des. Number and project description in your reply and your comments will be incorporated into the formal environmental study.

Please review the attached letter, which is also located in IN SCOPE at <http://erms.indot.in.gov/Section106Documents/> (the Des. No. is the most efficient search term, once in IN SCOPE), and respond with your comments on any historic resource impacts incurred as a result of this project so that an environmental report can be completed. We also welcome your related opinions and other input to be considered in the preparation of the environmental document. If a hard copy of the materials is needed, please respond to this email with your request as soon as you can.

Consulting parties have thirty (30) calendar days from receipt of this information to review and provide comments. If we do not receive a response from an invited consulting party within the time allotted, the project will proceed consistent

with the proposed design. Tribal consulting parties may enter the process at any time and are encouraged to respond to this notification with any comments or concerns at their earliest convenience.

Tribal contacts may contact Shaun Miller at smiller@indot.in.gov or 317-416-0876 or Kari Carmany-George at FHWA at K.CarmanyGeorge@dot.gov or 317-226-5629.

Thank you in advance for your input,

Shaun Miller
INDOT, Cultural Resources Office
Archaeology Team Lead
(317) 416-0876



INDIANA DEPARTMENT OF TRANSPORTATION

100 North Senate Avenue
Room N758-ES
Indianapolis, Indiana 46204

PHONE: (317) 296-0799

Eric Holcomb, Governor
Joe McGuinness, Commissioner

December 10, 2021

This letter was sent to the listed parties.

RE: Des. No. 1900009
US 20 Bridge Improvement Project
US 20 over Kennedy Avenue, 0.99 mile east of SR 152
City of Hammond, Lake County

Dear Consulting Party (see attached list),

The Indiana Department of Transportation (INDOT), with funding from the Federal Highway Administration, proposes to proceed with a bridge improvement project (Des. No. 1900009). Lochmueller Group is under contract with INDOT to advance the environmental documentation for the referenced project.

This letter is part of the early coordination phase of the environmental review process requesting comments associated with this project. We are requesting comments from your area of expertise regarding any possible environmental effects associated with this project. Please use the above Des. Number and project description in your reply and your comments will be incorporated into the formal environmental study.

The proposed undertaking is on US 20 over Kennedy Avenue in Lake County, Indiana. It is within North Township, Highland USGS Topographic Quadrangle, in Section 4, Township 36 North, Range 9 West and in Section 33, Township 37 North, Range 9 West.

The need for the project is evidenced by the deteriorated condition of the existing structure, Bridge No. 020-45-02484 B. The most recent INDOT Bridge Inspection Report from January 2021, noted that the abutment wall and northeast wing wall are exhibiting cracking. The superstructure was noted to have minor chipping on the beam edges in spans. The superstructure was given a condition rating of 6, which is considered to be in "satisfactory" condition and indicates minor deterioration. The substructure was given a condition rating of 5 which is considered "fair" condition and indicates minor section loss. Condition ratings range from 0, which indicates a failing structure, to 9, which indicates a new structure with no deficiencies. The purpose of the project is to improve the condition of the structure to a rating of 8 or greater, which represents a structure in "very good" condition.

The existing structure is a three-span, continuous prestressed concrete beam bridge carrying US 20 over Kennedy Avenue and IHB Railroad. The out-to-out length is approximately 196 feet, and the existing clear roadway width is 52 feet. The preferred alternative involves replacement of the existing bridge with a two-span structure with MSE wall abutments, a reinforced concrete pier, an out-to-out length of approximately 260 feet and a clear roadway width of 40 feet. A 4-foot-wide curbed center median will be installed between the eastbound and westbound traffic lanes along US 20. A multi-use path separated from vehicular traffic with a

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pedestrian traffic barrier will be added to the north side of the structure. It is anticipated that a raise in the profile grade will be necessary to provide adequate vertical clearance over the railroad and Kennedy Ave. Additionally, the US 20 roadway grade east of the bridge will require modification to achieve proper stopping sight distance at the intersection with the entry/exit ramps accessing Kennedy Avenue.

It is anticipated that additional permanent right-of-way (ROW) may need to be acquired to accommodate the proposed improvements. Most of the new ROW, if necessary, would be acquired east of Kennedy Avenue and would accommodate any necessary realignments or grade adjustments for the US 20 entry/exit ramps. New permanent ROW amounts are not likely to exceed 0.5 acre. No relocations are anticipated.

Section 106 of the National Historic Preservation Act requires federal agencies to take into account the effects of their undertakings on historic and archaeological properties. In accordance with 36 CFR 800.2 (c), you are hereby requested to be a consulting party to participate in the Section 106 process. Entities that have been invited to participate in the Section 106 consultation process for this project are identified in the attached list. Per 36 CFR 800.3(f), we hereby request that the Indiana State Historic Preservation Officer (SHPO) notify this office if the SHPO staff is aware of any other parties that may be entitled to be consulting parties or should be contacted as potential consulting parties for the project.

The Section 106 process involves efforts to identify historic properties potentially affected by the undertaking, assess its effects and seek ways to avoid, minimize or mitigate any adverse effects on historic properties. For more information regarding the protection of historic resources, please see the Advisory Council on Historic Preservation's guide: *Protecting Historic Properties: A Citizen's Guide to Section 106 Review* available online at <https://www.achp.gov/sites/default/files/documents/2017-01/CitizenGuide.pdf>.

The Area of Potential Effects (APE) is the area in which the proposed project may cause alterations in the character or use of historic resources. At this time, no cultural resource investigations have occurred; however, the results of cultural resource identification and evaluation efforts, both above-ground and archaeological, will be forthcoming. Consulting parties will receive notification when these reports are completed.

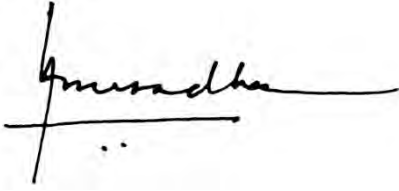
Please review the information and comment within thirty (30) calendar days of receipt. If you indicate that you do not desire to be a consulting party, or if you do not respond, you will not be included on the list of consulting parties for this project. If we do not receive your response in the time allotted, the project will proceed consistent with the proposed design and you will not receive further information about the project unless the design changes. Tribal consulting parties may enter the process at any time and are encouraged to respond to this notification with any comments or concerns at their earliest convenience.

For questions concerning specific project details, you may contact Hannah Blad of Lochmueller Group at 574.334.5487 or hblad@lochgroup.com. All future responses regarding the proposed project should be forwarded to Lochmueller Group at the following address:

Hannah Blad
Section 106 Specialist/Historian
Lochmueller Group
112 W. Jefferson Blvd., Suite 500
South Bend, IN 46601
hblad@lochgroup.com

Tribal contacts may contact Shaun Miller at smiller@indot.in.gov or 317-416-0876 or Kari Carmany-George at FHWA at K.CarmanyGeorge@dot.gov or 317-226-5629.

Sincerely,



Anuradha V. Kumar, Manager
Cultural Resources Office
Environmental Services

Enclosures:

- Topographic Map

Enclosures removed to
prevent duplication

Distribution List:

- State Historic Preservation Officer
- Lake County Commissioners
- Lake County Historian
- Hammond Historical Society
- Lake County Historic Society and Museum
- Hammond Historic Preservation Commission
- Indiana Landmarks, Northwest Field Office
- Lake County Highway Superintendent
- Northwestern Indiana Regional Planning Commission
- Eastern Shawnee Tribe of Oklahoma
- Forest County Potawatomi Community
- Miami Tribe of Oklahoma
- Peoria Tribe of Indians of Oklahoma
- Pokagon Band of Potawatomi Indians
- Shawnee Tribe

Hannah Blad

From: Scott Bocock <scott.bocock@yahoo.com>
Sent: Tuesday, December 14, 2021 3:24 PM
To: Hannah Blad
Subject: RE: FHWA Project: Des. No. 1900009; ECL, US 20 Bridge Improvement Project, Lake County, Indiana
Attachments: image765843.png; image469851.png; image430240.png; image141646.png; image832556.png; image959237.png; image441524.png; image752376.png; image430189.png

Hi Hannah,

I would like to participate in the Section 106 process as a consulting party.
Thank you.

Take care,

Scott

Sent from [Mail](#) for Windows

From: [Hannah Blad](#)
Sent: Tuesday, December 14, 2021 9:58 AM
To: [Scott Bocock](#)
Subject: RE: FHWA Project: Des. No. 1900009; ECL, US 20 Bridge Improvement Project, Lake County, Indiana

Scott,

Thank you for your reply to the early coordination letter. Would you like participate in the Section 106 process as a consulting party and continue receiving project documents/information or would you rather not become a consulting party for this project?

Best,



Hannah Blad

Hist/Sec 106 Specialist II



Lochmueller Group

112 W Jefferson Blvd, Suite 500, South Bend, IN 46601



Email: HBlad@lochgroup.com



Direct: 574.334.5487

Mobile: 574.248.2121

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From: Scott Bocock <scott.bocock@yahoo.com>
Sent: Friday, December 10, 2021 3:27 PM

To: Hannah Blad <HBlad@lochgroup.com>

Subject: RE: FHWA Project: Des. No. 1900009; ECL, US 20 Bridge Improvement Project, Lake County, Indiana

Hi Ms. Blad,

I have reviewed the submitted documents and feel confident that the necessary fieldwork will identify any historical and archaeological resources that may be adversely affected by this project. Thank you.

Sincerely,
Scott Bocoock,
Lake County Historian

Sent from [Mail](#) for Windows

From: [Hannah Blad](#)

Sent: Friday, December 10, 2021 7:46 AM

To: [Slider, Chad \(DNR\)](#); scott.bocoock@yahoo.com; hammondhistory@hotmail.com; [Brian Poland](#); [Brad Miller](#); smolijis@lakecountyin.org; nirpc@nirpc.org

Cc: [Miller, Shaun \(INDOT\)](#); [sbranigin](#); [Michels, Stewart](#); [Borden, Andrew A](#); [Gary Quigg](#); [C.J. Cunningham](#); [Michael Vereb](#); [Jason Hickie](#)

Subject: FHWA Project: Des. No. 1900009; ECL, US 20 Bridge Improvement Project, Lake County, Indiana

Des. No.: 1900009

Project Description: Bridge Improvement

Location: US 20 over Kennedy Avenue, City of Hammond, Lake County, Indiana

The Indiana Department of Transportation, with funding from the Federal Highway Administration, proposes to proceed with a bridge improvement project (Des. No. 1900009).

Section 106 of the National Historic Preservation Act requires federal agencies to take into account the effects of their undertakings on historic properties. The following agencies/individuals are being invited to become consulting parties:

- State Historic Preservation Officer
- Lake County Commissioners
- Lake County Historian
- Hammond Historical Society
- Lake County Historic Society and Museum
- Hammond Historic Preservation Commission
- Indiana Landmarks, Northwest Field Office
- Lake County Highway Superintendent
- Northwestern Indiana Regional Planning Commission
- Eastern Shawnee Tribe of Oklahoma
- Forest County Potawatomi Community
- Miami Tribe of Oklahoma
- Peoria Tribe of Indians of Oklahoma
- Pokagon Band of Potawatomi Indians
- Shawnee Tribe



PEORIA TRIBE OF INDIANS OF OKLAHOMA

118 S. Eight Tribes Trail (918) 540-2535 FAX (918) 540-2538

P.O. Box 1527

MIAMI, OKLAHOMA 74355

CHIEF
Craig Harper

SECOND CHIEF
Rosanna Dobbs

December 14, 2021

Shaun Miller
Tribal Contact
INDOT
100 N Senate Ave., Rm N642
Indianapolis, IN 46204

Re: Des.#1900009, Bridge improvements US 20 over Kennedy Ave. City of Hammond

Thank you for providing notice of the referenced project. The Peoria Tribe of Indians of Oklahoma is unaware of a direct link to the newly proposed project location.

The Peoria Tribe of Indians of Oklahoma is also unaware of items covered under Native American Graves Protection and Repatriation Act (NAGPRA) to be associated with the proposed project site, including funerary or sacred objects; objects of cultural patrimony; or ancestral human remains.

The Peoria Tribe has no objection at this time to the proposed project. If, however, at any time items are discovered which fall under the protection of NAGPRA, the Peoria Tribe requests immediate notification and consultation. In addition, state, local and tribal authorities should be advised as to the findings and construction halted until consultation with all concerned parties has occurred.

Please feel free to contact me directly at the number above if additional consultation is necessary. Thank you again for your consideration with this matter.

Sincerely,

Charla K. EchoHawk
Director of Cultural Preservation

TREASURER
Hank Downum

SECRETARY
Tonya Mathews

FIRST COUNCILMAN
Carolyn Ritchey

SECOND COUNCILMAN
Kara North

THIRD COUNCILMAN
Isabella Burrell



Division of Historic Preservation & Archaeology · 402 W. Washington Street, W274 · Indianapolis, IN 46204-2739
Phone 317-232-1646 · Fax 317-232-0693 · dhpa@dnr.IN.gov



December 16, 2021

Hannah Blad
Section 106 Specialist/Historian
Lochmueller Group
112 W. Jefferson Blvd., Suite 500
South Bend, IN 46601

Federal Agency: Indiana Department of Transportation (“INDOT”),
on behalf of Federal Highway Administration, Indiana Division (“FHWA”)

Re: Early coordination letter for the US 20 Bridge Improvement Project (Des. No. 1900009; DHPA
No. 28563)

Dear Ms. Blad:

Pursuant to Section 106 of the National Historic Preservation Act of 1966, as amended (54 U.S.C. § 306108), 36 C.F.R. Part 800, and the “Programmatic Agreement (PA) Among the Federal Highway Administration, the Indiana Department of Transportation, the Advisory Council on Historic Preservation and the Indiana State Historic Preservation Officer Regarding the Implementation of the Federal Aid Highway Program In the State of Indiana,” the staff of the Indiana State Historic Preservation Officer (“Indiana SHPO”) has reviewed your December 10, 2021, review request submittal form which enclosed INDOT’s early coordination letter, received by our office the same day, for this project in Hammond, Lake County, Indiana.


We are not aware of any parties who should be invited to participate in the Section 106 consultation on this federal undertaking, beyond those whom INDOT already has invited. However, if right-of-way is likely to be taken from a potentially historic property, it might be advisable to invite the owner of that property as soon as possible. In your next regular correspondence on this project, please advise us as to which of the invited consulting parties has accepted the invitation.

We look forward to reviewing the proposed area of potential effects and the reports on investigations of above-ground cultural resources and archaeological resources that the early coordination letter indicated will be forthcoming.

The Indiana SHPO staff’s archaeological reviewer for this project is Wade T. Tharp, and the structures reviewer is Caitlin Lehman. However, if you have a question about the Section 106 process, please contact initially the INDOT Cultural Resources staff members who are assigned to this project.

In all future correspondence about the US 20 Bridge Improvement project in Lake County (Des. No. 1900009), please refer to DHPA No. 28563.

Very truly yours,

A handwritten signature in black ink, appearing to read "Beth K. McCord". The signature is fluid and cursive, with the first letters of the first and last names being capitalized and prominent.

Beth K. McCord
Deputy State Historic Preservation Officer

BKM:CML:cml

emc: Anuradha Kumar, INDOT
Shaun Miller, INDOT
Susan Branigin, INDOT
Mary Kennedy, INDOT
Hannah Blad, Lochmueller Group
Chad Costa, Lochmueller Group
Gary Quigg, Lochmueller Group
Wade T. Tharp, DNR-DHPA
Caitlin Lehman, DNR-DHPA



Pokégnek Bodéwadmik

POKAGON BAND OF POTAWATOMI
HISTORY & CULTURE CENTER

12/22/2021

Shaun Miller
INDOT, Cultural Resources Office
Archaeology Team Lead
(317) 416-0876

FHWA Project - Des. No. 1900009

Dear Responsible Party:

Migwêthh for contacting me regarding this project. As THPO, I am responsible for handling Section 106 Consultations on behalf of the tribe. I am writing to inform you that I have reviewed the details for the project referenced above. The proposed work is occurring within a mile of known archaeological sites, historic sites or features that are considered sensitive or recorded in the Pokagon Band Historic Inventory Database. I have made the determination that all the proposed and alternative project site locations will have **No Adverse Effect** on any historic, religious, or culturally significant resources to the Pokagon Band of Potawatomi Indians.

If any cultural or archaeological resources are uncovered during construction, please stop work, and contact me immediately. Should you have any other questions, please don't hesitate to contact me at your earliest convenience.

Sincerely,

Matthew J.N. Bussler
Tribal Historic Preservation Officer
Pokagon Band of Potawatomi Indians
Office: (269) 462-4316
Cell: (269) 519-0838
Matthew.Bussler@Pokagonband-nsn.gov



Miami Tribe of Oklahoma

3410 P St. NW, Miami, OK 74354 • P.O. Box 1326, Miami, OK 74355
Ph: (918) 541-1300 • Fax: (918) 542-7260
www.miamination.com



Via email: smiller@indot.in.gov

January 3, 2022

Shaun Miller, Archaeological Team Lead
Cultural Resources Office
Indiana DOT
575 North Pennsylvania Street
Indianapolis, IN 46204

Re: Des. No. 1900009, US 20 Bridge Improvements, Lake County, Indiana – Comments of the Miami Tribe of Oklahoma

Dear Mr. Miller:

Aya, kikwehsitoole – I show you respect. The Miami Tribe of Oklahoma, a federally recognized Indian tribe with a Constitution ratified in 1939 under the Oklahoma Indian Welfare Act of 1936, respectfully submits the following comments regarding Des. No. 1900009 in Lake County, Indiana.

The Miami Tribe offers no objection to the above-referenced project at this time, as we are not currently aware of existing documentation directly linking a specific Miami cultural or historic site to the project site. However, given the Miami Tribe's deep and enduring relationship to its historic lands and cultural property within present-day Indiana, if any human remains or Native American cultural items falling under the Native American Graves Protection and Repatriation Act (NAGPRA) or archaeological evidence is discovered during any phase of this project, the Miami Tribe requests immediate consultation with the entity of jurisdiction for the location of discovery. In such a case, please contact me at 918-541-8966 or by email at dhunter@miamination.com to initiate consultation.

The Miami Tribe accepts the invitation to serve as a consulting party to the proposed project. In my capacity as Tribal Historic Preservation Officer I am the point of contact for consultation.

Respectfully,

Diane Hunter
Tribal Historic Preservation Officer



EASTERN SHAWNEE CULTURAL PRESERVATION DEPARTMENT

70500 East 128 Road, Wyandotte, OK 74370

January 5, 2022

INDOT - Indiana Department of Transportation

100 N. Senate Ave. IGCN642

Indianapolis, IN 46201

RE: Des No. 1900009 Hammond, Lake County, Indiana

Dear Mr. Miller,

The Eastern Shawnee Tribe has received your letter regarding the above referenced project(s) within Lake County, Indiana. The Eastern Shawnee Tribe is committed to protecting sites important to Tribal Heritage, Culture and Religion. Furthermore, the Tribe is particularly concerned with historical sites that may contain but not limited to the burial(s) of human remains and associated funerary objects.

As described in your correspondence, and upon research of our database(s) and files, we find our people occupied these areas historically and/or prehistorically. However, the project proposes **NO Adverse Effect** or endangerment to known sites of interest to the Eastern Shawnee Tribe. Please continue project as planned. However, should this project inadvertently discover an archeological site or object(s) we request that you immediately contact the Eastern Shawnee Tribe, as well as the appropriate state agencies (within 24 hours). We also ask that all ground disturbing activity stop until the Tribe and State agencies are consulted. Please note that any future changes to this project will require additional consultation.

In accordance with the NHPA of 1966 (16 U.S.C. § 470-470w-6), federally funded, licensed, or permitted undertakings that are subject to the Section 106 review process must determine effects to significant historic properties. As clarified in Section 101(d)(6)(A-B), historic properties may have religious and/or cultural significance to Indian Tribes. Section 106 of NHPA requires Federal agencies to consider the effects of their actions on all significant historic properties (36 CFR Part 800) as does the National Environmental Policy Act of 1969 (43 U.S.C. § 4321-4347 and 40 CFR § 1501.7(a)). This letter evidences NHPA and NEPA historic properties compliance pertaining to consultation with this Tribe regarding the referenced proposed projects.

Thank you, for contacting the Eastern Shawnee Tribe, we appreciate your cooperation. Should you have any further questions or comments please contact our Office.

Sincerely,

Paul Barton, Tribal Historic Preservation Officer (THPO)

Eastern Shawnee Tribe of Oklahoma

(918) 666-5151 Ext:1833

Hannah Blad

From: Hannah Blad
Sent: Monday, February 28, 2022 12:46 PM
To: WTharp1 (WTharp1@dnr.IN.gov); Lehman, Caitlin M; Scott Bocock
Cc: sbranigin; Coon, Matthew; Gary Quigg; C.J. Cunningham; Borden, Andrew A; Lisa Kelley; amartin; Chad Costa
Subject: FHWA Project: Des. No. 1900009; HPR & Archaeology Report, US 20 Bridge Improvement Project, Lake County, Indiana

Des. No.: 1900009

Project Description: Bridge Improvement

Location: US 20 over Kennedy Avenue and Indiana Harbor Belt Railroad, City of Hammond, Lake County, Indiana

The Indiana Department of Transportation, with funding from the Federal Highway Administration, proposes to proceed with a bridge improvement project (Des. No. 1900009). The Section 106 Early Coordination Letter for this project was originally distributed on December 10, 2021.

As part of Section 106 of the National Historic Preservation Act, a Historic Property Report and Archaeology Report (Tribes only) have been prepared and are ready for review and comment by consulting parties.

Please review this documentation located in IN SCOPE at <http://erms.indot.in.gov/Section106Documents/> (the Des. No. is the most efficient search term, once in IN SCOPE), and respond with any comments that you may have. If a hard copy of the materials is needed, please respond to this email with your request as soon as you can.

Consulting parties have thirty (30) calendar days from receipt of this information to review and provide comment. Tribal consulting parties may enter the process at any time and are encouraged to respond to this notification with any comments or concerns at their earliest convenience.

Tribal contacts may contact Patty Jo Korzeniewski at pkorzeniewski@indot.in.gov or 317-416-4377 or Kari Carmany-George at FHWA at K.CarmanyGeorge@dot.gov or 317-226-5629.

Thank you in advance for your input,



Hannah Blad

Hist/Sec 106 Specialist II



Lochmueller Group

112 W Jefferson Blvd, Suite 500, South Bend, IN 46601



Email: HBlad@lochgroup.com



Direct: 574.334.5487

Mobile: 574.248.2121

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Hannah Blad

From: Branigin, Susan <SBranigin@indot.IN.gov>
Sent: Monday, February 28, 2022 3:51 PM
To: thpo@estoo.net; cechohawk@peoriatribe.com; Matthew.Bussler@pokagonband-nsn.gov
Cc: Carmany-George, Karstin (FHWA); Korzeniewski, Patricia J; Coon, Matthew; Hannah Blad; Gary Quigg
Subject: FHWA Project: Des. No. 1900009; HPR & Archaeology Report, US 20 Bridge Improvement Project, Lake County, Indiana
Attachments: US20BridgeImprovementProject_Des1900009_ReportsDistributionLetter_2022-02-28.pdf

Des. No.: 1900009

Project Description: Bridge Improvement

Location: US 20 over Kennedy Avenue and Indiana Harbor Belt Railroad, City of Hammond, Lake County, Indiana

Dear Consulting Parties:

The Indiana Department of Transportation, with funding from the Federal Highway Administration, proposes to proceed with a bridge improvement project (Des. No. 1900009). The Section 106 Early Coordination Letter for this project was originally distributed on December 10, 2021.

As part of Section 106 of the National Historic Preservation Act, a Historic Property Report and Archaeology Report (Tribes only) have been prepared and are ready for review and comment by consulting parties.

Please review this documentation located in IN SCOPE at <http://erms.indot.in.gov/Section106Documents/> (the Des. No. is the most efficient search term, once in IN SCOPE), and respond with any comments that you may have. If a hard copy of the materials is needed, please respond to this email with your request as soon as you can.

Consulting parties have thirty (30) calendar days from receipt of this information to review and provide comment. Tribal consulting parties may enter the process at any time and are encouraged to respond to this notification with any comments or concerns at their earliest convenience.

Tribal contacts may contact Patty Jo Korzeniewski at pkorzeniewski@indot.in.gov or 317-416-4377 or Kari Carmany-George at FHWA at K.CarmanyGeorge@dot.gov or 317-226-5629.

Thank you in advance for your input,

Susan R. Branigin

History Team Lead

Cultural Resources Office

Environmental Services

100 N. Senate Ave., Rm. N758-ES

Indianapolis IN 46204

Office: 317.417.1622

Email: sbranigin@indot.in.gov

Work Hours: M-F 7:30 a.m.-3:30 p.m.





INDIANA DEPARTMENT OF TRANSPORTATION

100 North Senate Avenue
Room N758-ES
Indianapolis, Indiana 46204

PHONE: (317) 296-0799

Eric Holcomb, Governor
Michael Smith, Commissioner

February 28, 2022

This letter was sent to the listed parties.

RE: Des. No. 1900009/DHPA No. 28563
US 20 Bridge Improvement Project
US 20 over Kennedy Avenue & Indiana Harbor Belt Railroad, 0.99 mile east of SR 152
City of Hammond, Lake County

Dear Consulting Party,

The Indiana Department of Transportation (INDOT), with funding from the Federal Highway Administration, proposes to proceed with a bridge improvement project (Des. No. 1900009).

This letter is part of the Section 106 review process for this project. Section 106 of the National Historic Preservation Act requires federal agencies to take into account the effects of their undertakings on historic and archaeological properties. We are requesting comments from you regarding the possible effects of this project. Please use the above Des. Number and project description in your reply and your comments will be incorporated into the formal environmental study.

A Section 106 early coordination letter was distributed on December 10, 2021.

The proposed undertaking is on US 20 over Kennedy Avenue and Indiana Harbor Belt Railroad (IHBR) in Lake County, Indiana. It is within North Township, Highland USGS Topographic Quadrangle, in Section 4, Township 36 North, Range 9 West and in Section 33, Township 37 North, Range 9 West.

The need for the project is evidenced by the deteriorated condition of the existing structure, Bridge No. 020-45-02484 B/NBI No. 005200. The most recent INDOT Bridge Inspection Report from January 2021, noted that the abutment wall and northeast wing wall are exhibiting cracking. The superstructure was noted to have minor chipping on the beam edges in spans. The superstructure was given a condition rating of 6, which is considered to be in "satisfactory" condition and indicates minor deterioration. The substructure was given a condition rating of 5 which is considered "fair" condition and indicates minor section loss. Condition ratings range from 0, which indicates a failing structure, to 9, which indicates a new structure with no deficiencies. The purpose of the project is to improve the condition of the structure to a rating of 8 or greater, which represents a structure in "very good" condition.

The existing structure is a three-span, continuous prestressed concrete beam bridge carrying US 20 over Kennedy Avenue and IHBR. The out-to-out length is approximately 196 feet, and the existing clear roadway width is 52 feet. The preferred alternative involves replacement of the existing bridge with a two-span structure

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with mechanically stabilized earth (MSE) wall abutments, a reinforced concrete pier, an out-to-out length of approximately 260 feet and a clear roadway width of 40 feet. A 4-foot-wide curbed center median will be installed between the eastbound and westbound traffic lanes along US 20. A multi-use path separated from vehicular traffic with a pedestrian traffic barrier will be added to the north side of the structure. It is anticipated that a raise in the profile grade will be necessary to provide adequate vertical clearance over the railroad and Kennedy Ave. Additionally, the US 20 roadway grade east of the bridge will require modification to achieve proper stopping sight distance at the intersection with the entry/exit ramps accessing Kennedy Avenue.

It is anticipated that additional permanent right-of-way (ROW) may need to be acquired to accommodate the proposed improvements. Most of the new ROW, if necessary, would be acquired east of Kennedy Avenue and would accommodate any necessary realignments or grade adjustments for the US 20 entry/exit ramps. New permanent ROW amounts are not likely to exceed 0.5 acre. No relocations are anticipated.

Lochmueller Group is under contract with INDOT to advance the environmental documentation for the referenced project. Cultural Resource Analysts Inc. (CRA) has been subcontracted to complete the archaeological Section 106 documentation for the project.

In accordance with 36 CFR 800.2 (c), you were invited to become a consulting party as part of the Section 106 process, or you are hereby invited to become a consulting party as part of the Section 106 process. Entities that have previously accepted consulting party status--as well as additional entities that are currently being invited to become consulting parties--are identified in the attached list.

The Section 106 process involves efforts to identify historic properties potentially affected by the undertaking, to assess the undertaking's effects and to seek ways to avoid, minimize, or mitigate any adverse effects on historic properties. For more information regarding the protection of historic resources, please see the Advisory Council on Historic Preservation's guide: *Protecting Historic Properties: A Citizen's Guide to Section 106 Review* available online at <https://www.achp.gov/sites/default/files/documents/2017-01/CitizenGuide.pdf>.

The Area of Potential Effects (APE) is the area in which the proposed project may cause alterations in the character or use of historic resources. The APE contains no resources listed in the National Register of Historic Places (NRHP).

A historian who meets the Secretary of the Interior's Professional Qualification Standards identified and evaluated above-ground resources within the APE for potential eligibility for the NRHP. As a result of the historic property identification and evaluation efforts, Pumping Station (Lochmueller #2, 5721 Kennedy Avenue, Hammond, IN) is recommended as eligible for listing in the NRHP.

With regard to archaeological resources, an archaeologist who meets the Secretary of the Interior's Professional Qualification Standards identified two sites within the project area. As a result of these efforts, sites 12La736 and 12La737 were recommended not eligible for listing in the NRHP and no further work is recommended.

Since the distribution of the early coordination materials the State Historic Preservation Officer, Eastern Shawnee Tribe of Oklahoma, the Peoria Tribe of Indians of Oklahoma, the Pokagon Band of Potawatomi Indians, and the Lake County Historian all accepted consulting party status.

The Historic Property Report and Archaeology Report (Tribes only) are available for review in IN SCOPE at <http://erms.indot.in.gov/Section106Documents/> (the Des. No. is the most efficient search term, once in IN SCOPE). You are invited to review these documents and to respond with comments on any historic resource impacts incurred as a result of this project so that an environmental report can be completed. We also welcome

your related opinions and other input to be considered in the preparation of the environmental document. If you prefer a hard-copy of this material, please respond to this email with your request as soon as you can.

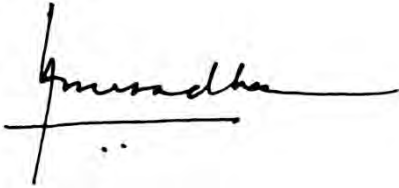
Please review the information and comment within thirty (30) calendar days of receipt. If you indicate that you do not desire to be a consulting party or if you have not previously accepted consulting party status and you do not respond to this letter, you will not be included on the list of consulting parties for this project and will not receive further information about the project unless the design changes. Tribal consulting parties may enter the process at any time and are encouraged to respond to this notification with any comments or concerns at their earliest convenience.

For questions concerning specific project details, you may contact Hannah Blad of Lochmueller Group at 574.334.5487 or hblad@lochgroup.com. All future responses regarding the proposed project should be forwarded to Lochmueller Group at the following address:

Hannah Blad
Section 106 Specialist/Historian
Lochmueller Group
112 W. Jefferson Blvd., Suite 500
South Bend, IN 46601
hblad@lochgroup.com

Tribal contacts may contact Patty Jo Korzeniewski at pkorzeniewski@indot.in.gov or 317-416-4377 or Kari Carmany-George at FHWA at K.CarmanyGeorge@dot.gov or 317-226-5629.

Sincerely,



Anuradha V. Kumar, Manager
Cultural Resources Office
Environmental Services

Enclosures:

- Area of Potential Effects Map

Enclosures removed to
prevent duplication

Distribution List:

- State Historic Preservation Officer
- Eastern Shawnee Tribe of Oklahoma
- Peoria Tribe of Indians of Oklahoma
- Pokagon Band of Potawatomi Indians
- Lake County Historian

Hannah Blad

From: Scott Bocock <scott.bocock@yahoo.com>
Sent: Monday, February 28, 2022 3:41 PM
To: Hannah Blad
Subject: RE: FHWA Project: Des. No. 1900009; HPR & Archaeology Report, US 20 Bridge Improvement Project, Lake County, Indiana
Attachments: image818809.png; image287010.png; image132458.png; image046804.png; image704962.png; image024946.png; image846632.png; image750477.png; image553450.png

Hi Ms. Blad,

I have reviewed the documentation sent and feel confident that the Section 106 process has been properly fulfilled and that this project can continue. Thank you.

Scott Bocock,
Lake County Historian

Sent from [Mail](#) for Windows

From: [Hannah Blad](#)
Sent: Monday, February 28, 2022 11:46 AM
To: [WTharp1 \(WTharp1@dnr.IN.gov\)](#); [Lehman, Caitlin M](#); [Scott Bocock](#)
Cc: [sbranigin](#); [Coon, Matthew](#); [Gary Quigg](#); [C.J. Cunningham](#); [Borden, Andrew A](#); [Lisa Kelley](#); [amartin](#); [Chad Costa](#)
Subject: FHWA Project: Des. No. 1900009; HPR & Archaeology Report, US 20 Bridge Improvement Project, Lake County, Indiana

Des. No.: 1900009

Project Description: Bridge Improvement

Location: US 20 over Kennedy Avenue and Indiana Harbor Belt Railroad, City of Hammond, Lake County, Indiana

The Indiana Department of Transportation, with funding from the Federal Highway Administration, proposes to proceed with a bridge improvement project (Des. No. 1900009). The Section 106 Early Coordination Letter for this project was originally distributed on December 10, 2021.

As part of Section 106 of the National Historic Preservation Act, a Historic Property Report and Archaeology Report (Tribes only) have been prepared and are ready for review and comment by consulting parties.

Please review this documentation located in IN SCOPE at <http://erms.indot.in.gov/Section106Documents/> (the Des. No. is the most efficient search term, once in IN SCOPE), and respond with any comments that you may have. If a hard copy of the materials is needed, please respond to this email with your request as soon as you can.

Consulting parties have thirty (30) calendar days from receipt of this information to review and provide comment. Tribal consulting parties may enter the process at any time and are encouraged to respond to this notification with any comments or concerns at their earliest convenience.

Tribal contacts may contact Patty Jo Korzeniewski at pkorzeniewski@indot.in.gov or 317-416-4377 or Kari Carmany-George at FHWA at K.CarmanyGeorge@dot.gov or 317-226-5629.



PEORIA TRIBE OF INDIANS OF OKLAHOMA

118 S. Eight Tribes Trail (918) 540-2535 FAX (918) 540-2538

P.O. Box 1527

MIAMI, OKLAHOMA 74355

CHIEF
Craig Harper

SECOND CHIEF
Rosanna Dobbs

March 1, 2022

Patricia Korzeniewski
Archaeologist/Tribal Contact
INDOT
100 N Senate Ave., Rm 758-ES
Indianapolis, IN 46204

Re: Des. #1900009; DHPA#28563; US 20 Bridge improvements over Kennedy Ave

Thank you for providing notice of the referenced project. The Peoria Tribe of Indians of Oklahoma is unaware of a direct link to the newly proposed project location.

The Peoria Tribe of Indians of Oklahoma is also unaware of items covered under Native American Graves Protection and Repatriation Act (NAGPRA) to be associated with the proposed project site, including funerary or sacred objects; objects of cultural patrimony; or ancestral human remains.

The Peoria Tribe has no objection at this time to the proposed project. If, however, at any time items are discovered which fall under the protection of NAGPRA, the Peoria Tribe requests immediate notification and consultation. In addition, state, local and tribal authorities should be advised as to the findings and construction halted until consultation with all concerned parties has occurred.

Please feel free to contact me directly at the number above if additional consultation is necessary. Thank you again for your consideration with this matter.

Sincerely,

Charla K. EchoHawk
Director of Cultural Preservation

TREASURER
Hank Downum

SECRETARY
Tonya Mathews

FIRST COUNCILMAN
Carolyn Ritchey

SECOND COUNCILMAN
Kara North

THIRD COUNCILMAN
Isabella Burrell



EASTERN SHAWNEE
CULTURAL PRESERVATION DEPARTMENT
70500 East 128 Road, Wyandotte, OK 74370

March 7, 2022

INDOT - Indiana Department of Transportation
100 N. Senate Ave. IGCN642
Indianapolis, IN 46201

RE: Des No. 1900009, Hammond, Lake County, Indiana

Dear Ms. Korzeniewski,

The Eastern Shawnee Tribe has received your letter regarding the above referenced project(s) within Lake County, Indiana. The Eastern Shawnee Tribe is committed to protecting sites important to Tribal Heritage, Culture and Religion. Furthermore, the Tribe is particularly concerned with historical sites that may contain but not limited to the burial(s) of human remains and associated funerary objects.

As described in your correspondence, and upon research of our database(s) and files, we find our people occupied these areas historically and/or prehistorically. However, the project proposes **NO Adverse Effect** or endangerment to known sites of interest to the Eastern Shawnee Tribe. Please continue project as planned. However, should this project inadvertently discover an archeological site or object(s) we request that you immediately contact the Eastern Shawnee Tribe, as well as the appropriate state agencies (within 24 hours). We also ask that all ground disturbing activity stop until the Tribe and State agencies are consulted. Please note that any future changes to this project will require additional consultation.

In accordance with the NHPA of 1966 (16 U.S.C. § 470-470w-6), federally funded, licensed, or permitted undertakings that are subject to the Section 106 review process must determine effects to significant historic properties. As clarified in Section 101(d)(6)(A-B), historic properties may have religious and/or cultural significance to Indian Tribes. Section 106 of NHPA requires Federal agencies to consider the effects of their actions on all significant historic properties (36 CFR Part 800) as does the National Environmental Policy Act of 1969 (43 U.S.C. § 4321-4347 and 40 CFR § 1501.7(a)). This letter evidences NHPA and NEPA historic properties compliance pertaining to consultation with this Tribe regarding the referenced proposed projects.

Thank you, for contacting the Eastern Shawnee Tribe, we appreciate your cooperation. Should you have any further questions or comments please contact our Office.

Sincerely,

Paul Barton, Tribal Historic Preservation Officer (THPO)
Eastern Shawnee Tribe of Oklahoma
(918) 666-5151 Ext:1833

Division of Historic Preservation & Archaeology · 402 W. Washington Street, W274 · Indianapolis, IN 46204-2739
Phone 317-232-1646 · Fax 317-232-0693 · dhpa@dnr.IN.gov ·



March 30, 2022

Hannah Blad
Section 106 Specialist/Historian
Lochmueller Group
112 W. Jefferson Blvd., Suite 500
South Bend, Indiana 46601

Federal Agency: Indiana Department of Transportation (“INDOT”),
on behalf of Federal Highway Administration, Indiana Division (“FHWA”)

Re: Historic property report (Blad, 2/28/2022), and Phase Ia archaeological investigations report (Harth, 02/22/2022), for the US 20 Bridge Improvement Project (Des. No. 1900009; DHPA No. 28563)

Dear Ms. Blad:

Pursuant to Section 106 of the National Historic Preservation Act of 1966, as amended (54 U.S.C. § 306108), 36 C.F.R. Part 800, and the “Programmatic Agreement (PA) Among the Federal Highway Administration, the Indiana Department of Transportation, the Advisory Council on Historic Preservation and the Indiana State Historic Preservation Officer Regarding the Implementation of the Federal Aid Highway Program In the State of Indiana,” the staff of the Indiana State Historic Preservation Officer (“Indiana SHPO”) has reviewed your February 28, 2022, review request submittal form, which enclosed the historic property report (“HPR”, Blad, 2/28/2022) and the Phase Ia archaeological investigations report (Harth, 02/22/2022), and which was received by our office February 28, 2022, for this project in the City of Hammond, Lake County, Indiana.

The area of potential effects (“APE”) proposed in the HPR appears to be of adequate size to encompass the geographic area in which direct and indirect effects of a project of this nature could occur.

For the purposes of the Section 106 review of this federal undertaking, we agree with the conclusions of the HPR that the Kennedy Avenue Pumping Station at 5721 Kennedy Avenue is eligible for inclusion in the National Register of Historic Places (“NRHP”) under Criterion C. Having been constructed in response to the We also believe that this building would be eligible under Criterion A for its association with Community Planning and Development and the New Deal. Furthermore, we agree that there are no other historic properties listed in or eligible for inclusion in the NRHP located within the project’s APE.

Additionally, in regard to archaeological resources, based upon the submitted information and the documentation available to the staff of the Indiana SHPO, there is insufficient information regarding archaeological sites 12-La-0736 and 12-La-0737 (both of which were identified during these investigations) to determine whether they are eligible for inclusion in the NRHP. However, we concur with the opinion of the archaeologist, as expressed in the Phase Ia archaeological investigations report (Harth, 02/22/2022), that the portions of these sites that lie within the proposed project area do not appear to contain significant archaeological deposits, and that no further archaeological investigations appear necessary at the proposed project area. The portions of these sites that lie outside the proposed project area must either be avoided or subjected to further archaeological investigations. Additionally, those areas of these sites should be clearly marked so that they are avoided by all ground-disturbing project activities. If avoidance is not feasible, then a plan for subsurface archaeological investigations must be submitted to the Division of Historic Preservation and Archaeology (“DHPA”) for review and comment. Any further archaeological investigations must be done in accordance with the “Secretary of the Interior’s Standards and guidelines for Archeology and Historic Preservation” (48 F.R. 44716).

We note that the archaeological site survey record forms for archaeological sites 12-La-0736 and 12-La-0737 have not yet been submitted to the Indiana DNR-DHPA SHAARD system database. Once they have been submitted, it will be reviewed.

If any prehistoric or historic archaeological artifacts or human remains are uncovered during construction, demolition, or earthmoving activities, state law (Indiana Code 14-21-1-27 and Indiana Code 14-21-1-29) requires that the discovery be reported to Indiana Department of Natural Resource, Division of Historic Preservation and Archaeology within two (2) business days. In that event, please call (317) 232-1646. Be advised that adherence to Indiana Code 14-21-1-27 and Indiana Code 14-21-1-29 does not obviate the need to adhere to applicable federal statutes and regulations, including but not limited to 36 C.F.R. Part 800.

The Indiana SHPO staff's archaeological reviewer for this project is Wade T. Tharp, and the structures reviewer is Caitlin Lehman. However, if you have a question about the Section 106 process, please contact initially the INDOT Cultural Resources staff members who are assigned to this project.

In all future correspondence about the US 20 Bridge Improvement project in Lake County (Des. No. 1900009), please refer to DHPA No. 28563.

Very truly yours,



Beth K. McCord
Deputy State Historic Preservation Officer

BKM:CML:WTT:wt

emc: Kari Carmany-George, FHWA
Anuradha Kumar, INDOT
Matt Coon, INDOT
Susan Branigin, INDOT
Mary Kennedy, INDOT
Hannah Blad, Lochmueller Group
Chad Costa, Lochmueller Group
Gary Quigg, Lochmueller Group
Eastern Shawnee Tribe of Oklahoma
Peoria Tribe of Indians of Oklahoma
Pokagon Band of Potawatomi Indians
Scott Bocock, Lake County Historian
Caitlin Lehman, Indiana DNR-DHPA
Wade T. Tharp, Indiana DNR-DHPA

Hannah Blad

From: Hannah Blad
Sent: Tuesday, August 22, 2023 9:21 AM
To: scott.bocock@yahoo.com; WTharp1 (WTharp1@dnr.IN.gov); Lehman, Caitlin M
Cc: Branigin, Susan; Coon, Matthew; Chad Costa; Gary Quigg; C.J. Cunningham; Borden, Andrew A; Michael Vereb; Jason Hickie; Barbie Swihart Stutzman
Subject: FHWA Project: Des. No. 1900009; Effects Report, US 20 Bridge Improvement Project, Lake County, Indiana

Des. No.: 1900009

Project Description: Bridge Improvement

Location: US 20 over Kennedy Avenue and Indiana Harbor Belt Railroad, City of Hammond, Lake County, Indiana

The Indiana Department of Transportation, with funding from the Federal Highway Administration (FHWA), proposes to proceed with a bridge improvement project (Des. No. 1900009). The Section 106 Early Coordination Letter for this project was originally distributed on December 10, 2021.

As part of Section 106 of the National Historic Preservation Act, an Effects Report has been prepared and is ready for review and comment by consulting parties.

Please review this documentation located in IN SCOPE at <https://erms12c.indot.in.gov/Section106Documents> (the Des. No. is the most efficient search term, once in IN SCOPE), and respond with any comments that you may have. If a hard copy of the materials is needed, please respond to this email with your request within seven (7) days.

Consulting parties have thirty (30) calendar days from receipt of this information to review and provide comment. Therefore, if we do not receive a response within thirty (30) days, your agency or organization will not receive any further information on the project unless the scope of work changes.

Tribal Contacts please respond to INDOT's Acting Tribal Liaison, Matt Coon at mcoon@indot.in.gov (317-697-9752) with any responses pertaining to this project including to provide INDOT/Indiana FHWA additional information about Tribal resources/concerns and questions/comments regarding cultural resources. The FHWA point of contact is Kari Carmany-George at K.CarmanyGeorge@dot.gov (317-226-5629).

Thank you in advance for your input,



 **Web:** <http://lochgroup.com>
  

Hannah Blad

Hist/Sec 106 Specialist II



Lochmueller Group

112 W Jefferson Blvd, Suite 500, South Bend, IN 46601



Email: HBlad@lochgroup.com



Direct: 574.334.5487

Mobile: 574.248.2121

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Hannah Blad

From: Branigin, Susan <SBranigin@indot.IN.gov>
Sent: Tuesday, August 22, 2023 9:48 AM
To: thpo@estoo.net; bfletcher@peoriatribe.com; Matthew.Bussler@pokagonband-nsn.gov
Cc: Carmany-George, Karstin (FHWA); Coon, Matthew; Branigin, Susan; Gary Quigg; Hannah Blad
Subject: FHWA Project: Des. No. 1900009; Effects Report, US 20 Bridge Improvement Project, Lake County, Indiana
Attachments: US20BridgeImprovementProject_Des1900009_RDL_2023-08-22.pdf

EXTERNAL

Des. No.: 1900009

Project Description: Bridge Improvement

Location: US 20 over Kennedy Avenue and Indiana Harbor Belt Railroad, City of Hammond, Lake County, Indiana

Dear Consulting Parties:

The Indiana Department of Transportation, with funding from the Federal Highway Administration (FHWA), proposes to proceed with a bridge improvement project (Des. No. 1900009). The Section 106 Early Coordination Letter for this project was originally distributed on December 10, 2021.

As part of Section 106 of the National Historic Preservation Act, an Effects Report has been prepared and is ready for review and comment by consulting parties.

Please review this documentation located in IN SCOPE at <https://erms12c.indot.in.gov/Section106Documents> (the Des. No. is the most efficient search term, once in IN SCOPE), and respond with any comments that you may have. If a hard copy of the materials is needed, please respond to this email with your request within seven (7) days.

Consulting parties have thirty (30) calendar days from receipt of this information to review and provide comment. Therefore, if we do not receive a response within thirty (30) days, your agency or organization will not receive any further information on the project unless the scope of work changes.

Tribal Contacts please respond to INDOT's Acting Tribal Liaison, Matt Coon at mcoon@indot.in.gov (317-697-9752) with any responses pertaining to this project including to provide INDOT/Indiana FHWA additional information about Tribal resources/concerns and questions/comments regarding cultural resources. The FHWA point of contact is Kari Carmany-George at K.CarmanyGeorge@dot.gov (317-226-5629).

Thank you in advance for your input,

Susan R. Branigin, MS

History Unit Team Lead/Supervisor

Cultural Resources Office (CRO)

Indiana Department of Transportation

100 North Senate Ave., N758 —Environmental Services

Indianapolis, IN 46204

Office: 317.417.1622



INDIANA DEPARTMENT OF TRANSPORTATION

100 North Senate Avenue
Room N758-ES
Indianapolis, Indiana 46204

PHONE: (855) 463-6848

Eric Holcomb, Governor
Michael Smith, Commissioner

August 22, 2023

This letter was sent to the listed parties.

RE: Des. No. 1900009/DHPA No. 28563
US 20 Bridge Improvement Project
US 20 over Kennedy Avenue & Indiana Harbor Belt Railroad, 0.99 mile east of SR 152
City of Hammond, Lake County

Dear Consulting Party,

The Indiana Department of Transportation (INDOT), with funding from the Federal Highway Administration (FHWA), proposes to proceed with a bridge improvement project (Des. No. 1900009).

This letter is part of the Section 106 review process for this project. Section 106 of the National Historic Preservation Act requires federal agencies to take into account the effects of their undertakings on historic properties. We are requesting comments from you regarding the possible effects of this project. Please use the above Des. Number and project description in your reply and your comments will be incorporated into the formal environmental study.

A Section 106 early coordination letter was distributed on December 10, 2021. In addition, a letter distributed on February 28, 2022, notified consulting parties that a historic property report and archaeology report were available for review and comment.

The proposed undertaking is on US 20 over Kennedy Avenue and Indiana Harbor Belt Railroad (IHBR) in Lake County, Indiana. It is within North Township, Highland USGS Topographic Quadrangle, in Section 4, Township 36 North, Range 9 West and in Section 33, Township 37 North, Range 9 West. The project area can be viewed online at <https://arcg.is/jqueP> (the Des. No. is the most efficient search term once in the CRO - Public Web Map App).

The need for the project is evidenced by the deteriorated condition of the existing structure, Bridge No. 020-45-02484 B/NBI No. 005200. The most recent INDOT Bridge Inspection Report from January 2021, noted that the abutment wall and northeast wing wall are exhibiting cracking. The superstructure was noted to have minor chipping on the beam edges in spans. The superstructure was given a condition rating of 6, which is considered to be in "satisfactory" condition and indicates minor deterioration. The substructure was given a condition rating of 5 which is considered "fair" condition and indicates minor section loss. Condition ratings range from 0, which indicates a failing structure, to 9, which indicates a new structure with no deficiencies. The purpose of

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the project is to improve the condition of the structure to a rating of 8 or greater, which represents a structure in “very good” condition.

The existing structure (Structure No. 020-45-02484 B/NBI No. 005200) is a three-span, continuous prestressed concrete beam bridge carrying US 20 over Kennedy Avenue and the Indiana Harbor Belt Railroad (IHBR). The out-to-out length is approximately 196 feet, and the existing clear roadway width is 52 feet. As identified in the February 28, 2022 distribution letter to the Historic Property Report, the preferred alternative involves replacement of the existing bridge with a two-span structure with mechanically stabilized earth (MSE) wall abutments, a reinforced concrete pier, a length of approximately 222 feet, a clear roadway width of 48 feet, and an out-to-out width of approximately 55 feet. A 4-foot-wide curbed center median will be installed between the eastbound and westbound traffic lanes along US 20. A raise in the profile grade will be necessary to provide adequate vertical clearance over the railroad and Kennedy Ave. Additionally, the US 20 roadway grade east of the bridge will require modification to achieve proper stopping sight distance at the intersection with the access roads to Kennedy Avenue.

Since the last correspondence on February 28, 2022, the project scope has been revised to include a 10-foot-wide multi-use trail. The trail will be added to the north side of the new bridge and connect to the planned Marquette Greenway Trail.

In addition to the trail, since the last correspondence right-of-way (ROW) amounts have been determined for this project. Approximately 1.03 acres of new permanent ROW and 0.036 acre of temporary ROW will be required. Previously, additional ROW was anticipated not to exceed 0.5 acre. No relocations are anticipated.

In regard to the Pumping Station, approximately 0.011 acre of permanent ROW and 0.02 acre of temporary ROW will be acquired from the north side of the historic property. Adjacent to the north side of the property the ramp leading from eastbound US 20 to Kennedy Avenue will be reconstructed, and as a result, grading will take place within the northern section of the recommended NRHP boundary. This work will occur between the existing road and fence surrounding the property. The temporary ROW required for this grading work will be from maintained lawn and existing sidewalk. The fence will remain in place during the undertaking. At the northwest corner of the recommended NRHP boundary, below-ground storm sewer work will require both temporary and permanent ROW from the historic property. The permanent ROW required from the historic property will be from existing sidewalk and a portion of roadway that extends into the ramp.

Lochmueller Group is under contract with INDOT to advance the environmental documentation for the referenced project. Cultural Resource Analysts Inc. (CRA) has been subcontracted to complete the archaeological Section 106 documentation for the project.

In accordance with 36 CFR 800.2 (c), you were invited to become a consulting party as part of the Section 106 process, or you are hereby invited to become a consulting party as part of the Section 106 process. Entities that have previously accepted consulting party status--as well as additional entities that are currently being invited to become consulting parties--are identified in the attached list.

The Section 106 process involves efforts to identify historic properties potentially affected by the undertaking, to assess the undertaking’s effects and to seek ways to avoid, minimize, or mitigate any adverse effects on historic properties. For more information regarding the protection of historic resources, please see the Advisory Council on Historic Preservation’s guide: *Protecting Historic Properties: A Citizen’s Guide to Section 106 Review* available online at <https://www.achp.gov/sites/default/files/documents/2017-01/CitizenGuide.pdf>.

The Area of Potential Effects (APE) is the area in which the proposed project may cause alterations in the character or use of historic resources. The APE contains no resources listed in the National Register of Historic Places (NRHP).

A historian who meets the Secretary of the Interior's Professional Qualification Standards identified and evaluated above-ground resources within the APE for potential eligibility for the NRHP. As a result of the historic property identification and evaluation efforts, Pumping Station (Lochmueller #2, 5721 Kennedy Avenue, Hammond, IN) is recommended as eligible for listing in the NRHP.

With regard to archaeological resources, an archaeologist who meets the Secretary of the Interior's Professional Qualification Standards identified two sites within the project area. As a result of these efforts, sites 12La736 and 12La737 were recommended not eligible for listing in the NRHP and no further work is recommended.

The Effects Report is available for review in IN SCOPE at <https://erms12c.indot.in.gov/Section106Documents> (the Des. No. is the most efficient search term, once in IN SCOPE). You are invited to review these documents and to respond with comments on any historic resource impacts incurred as a result of this project so that an environmental report can be completed. We also welcome your related opinions and other input to be considered in the preparation of the environmental document. If a hard copy of the materials is needed, please respond to this email with your request within seven (7) days.

Please review the information and comment within thirty (30) calendar days of receipt. If you indicate that you do not desire to be a consulting party or if you have not previously accepted consulting party status and you do not respond to this letter, you will not be included on the list of consulting parties for this project and will not receive further information about the project unless the design changes.

For questions concerning specific project details, you may contact Hannah Blad of Lochmueller Group at 574.334.5487 or hblad@lochgroup.com. All future responses regarding the proposed project should be forwarded to Lochmueller Group at the following address:

Hannah Blad
Section 106 Specialist/Historian
Lochmueller Group
112 W. Jefferson Blvd., Suite 500
South Bend, IN 46601
hblad@lochgroup.com

Tribal Contacts please respond to INDOT's Acting Tribal Liaison, Matt Coon at mcoon@indot.in.gov (317-697-9752) with any responses pertaining to this project including to provide INDOT/Indiana FHWA additional information about Tribal resources/concerns and questions/comments regarding cultural resources. The FHWA point of contact is Kari Carmany-George at K.CarmanyGeorge@dot.gov (317-226-5629).

Sincerely,



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Matthew S. Coon, Manager
Cultural Resources Office
Environmental Services

Enclosures:

- APE Map

Enclosures removed to
prevent duplication

Distribution List:

- State Historic Preservation Officer
- Eastern Shawnee Tribe of Oklahoma
- Peoria Tribe of Indians of Oklahoma
- Pokagon Band of Potawatomi Indians
- Lake County Historian

Hannah Blad

From: Scott Bocock <scott.bocock@yahoo.com>
Sent: Tuesday, August 22, 2023 4:00 PM
To: Hannah Blad
Subject: RE: FHWA Project: Des. No. 1900009; Effects Report, US 20 Bridge Improvement Project, Lake County, Indiana
Attachments: image757239.png; image416375.png; image349410.png; image795149.png; image790222.png; image253931.png; image330496.png; image427676.png

EXTERNAL

Hi Ms. Blad,

I have reviewed the effects report and understand that the proposed work with this project poses no direct or indirect threats to any historical or archaeological resources. Based upon this, I feel comfortable with the progression of this project to the next phase. Thank you!

Scott Bocock,
Lake County Historian

Sent from [Mail](#) for Windows

From: [Hannah Blad](#)
Sent: Tuesday, August 22, 2023 8:21 AM
To: scott.bocock@yahoo.com; [WTharp1 \(WTharp1@dnr.IN.gov\)](mailto:WTharp1@dnr.IN.gov); [Lehman, Caitlin M](#)
Cc: [Branigin, Susan](#); [Coon, Matthew](#); [Chad Costa](#); [Gary Quigg](#); [C.J. Cunningham](#); [Borden, Andrew A](#); [Michael Vereb](#); [Jason Hickie](#); [Barbie Swihart Stutzman](#)
Subject: FHWA Project: Des. No. 1900009; Effects Report, US 20 Bridge Improvement Project, Lake County, Indiana

Des. No.: 1900009

Project Description: Bridge Improvement

Location: US 20 over Kennedy Avenue and Indiana Harbor Belt Railroad, City of Hammond, Lake County, Indiana

The Indiana Department of Transportation, with funding from the Federal Highway Administration (FHWA), proposes to proceed with a bridge improvement project (Des. No. 1900009). The Section 106 Early Coordination Letter for this project was originally distributed on December 10, 2021.

As part of Section 106 of the National Historic Preservation Act, an Effects Report has been prepared and is ready for review and comment by consulting parties.

Please review this documentation located in IN SCOPE at <https://erms12c.indot.in.gov/Section106Documents> (the Des. No. is the most efficient search term, once in IN SCOPE), and respond with any comments that you may have. If a hard copy of the materials is needed, please respond to this email with your request within seven (7) days.

Consulting parties have thirty (30) calendar days from receipt of this information to review and provide comment. Therefore, if we do not receive a response within thirty (30) days, your agency or organization will not receive any further information on the project unless the scope of work changes.

Division of Historic Preservation & Archaeology · 402 W. Washington Street, W274 · Indianapolis, IN 46204-2739
Phone 317-232-1646 · Fax 317-232-0693 · dhpa@dnr.IN.gov



September 18, 2023

Hannah Blad
Section 106 Specialist/Historian
Lochmueller Group
112 West Jefferson Boulevard, Suite 500
South Bend, Indiana 46601

Federal Agency: Indiana Department of Transportation (“INDOT”),
on behalf of Federal Highway Administration, Indiana Division (“FHWA”)

Re: Effects report (Blad, 8/22/2023) for the US 20 Bridge Improvement Project (Des. No. 1900009;
DHPA No. 28563)

Dear Ms. Blad:

Pursuant to Section 106 of the National Historic Preservation Act of 1966, as amended (54 U.S.C. § 306108), 36 C.F.R. Part 800, and the “Programmatic Agreement (PA) Among the Federal Highway Administration, the Indiana Department of Transportation, the Advisory Council on Historic Preservation and the Indiana State Historic Preservation Officer Regarding the Implementation of the Federal Aid Highway Program In the State of Indiana,” the staff of the Indiana State Historic Preservation Officer (“Indiana SHPO”) has reviewed your August 22, 2023, submission, received by our office August 22, 2023, for this project in the City of Hammond, Lake County, Indiana.

As previously stated, we agree with the conclusions of the HPR that the Kennedy Avenue Pumping Station at 5721 Kennedy Avenue is eligible for inclusion in the National Register of Historic Places (“NRHP”) and that this is the only historic property located in this project’s area of potential effects (“APE”).

We understand that .011 acre of permanent right-of-way and .02 acre of temporary right-of-way will be taken along the north and northwest sides of the Kennedy Avenue Pumping Station property to accommodate grading and below-ground storm sewer work within the defined historic property boundary. When comparing aerial imagery and Google Streetview imagery, the brick sidewalk on the northwest corner of the property appears to have been installed between 2005 and 2009 and does not contribute to the significance of the historic property. Due to the industrial nature of the surrounding area, it also does not appear that adjacent bridge, which will have a modified grade profile, will introduce new elements that are out of character with the setting of the property. Therefore, we agree with the conclusions of the effects report that this proposed work and ROW acquisition will not diminish the characteristics of the Kennedy Avenue Pumping Station which make it significant.

As previously indicated, in regard to archaeological resources, based upon the submitted information and the documentation available to the staff of the Indiana SHPO, there is insufficient information regarding archaeological sites 12-La-0736 and 12-La-0737 (both of which were identified during these investigations) to determine whether they are eligible for inclusion in the NRHP. However, we concur with the opinion of the archaeologist, as expressed in the Phase Ia archaeological investigations report (Harth, 02/22/2022), that the portions of these sites that lie within the proposed project area do not appear to contain significant archaeological deposits, and that no further archaeological investigations appear necessary at the proposed project area. The portions of these sites that lie outside the proposed project area must either be avoided or subjected to further archaeological investigations. Additionally, those areas of these sites should be clearly marked so that they are avoided by all ground-disturbing project activities. If avoidance is not feasible, then a plan for subsurface archaeological investigations must be submitted to the Division of Historic Preservation and Archaeology (“DHPA”) for review and comment. Any further

archaeological investigations must be done in accordance with the "Secretary of the Interior's Standards and guidelines for Archeology and Historic Preservation" (48 F.R. 44716).

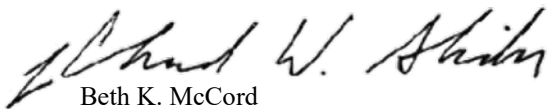
If any prehistoric or historic archaeological artifacts or human remains are uncovered during construction, demolition, or earthmoving activities, state law (Indiana Code 14-21-1-27 and Indiana Code 14-21-1-29) requires that the discovery be reported to Indiana Department of Natural Resource, Division of Historic Preservation and Archaeology, within two (2) business days. In that event, please call (317) 232-1646. Be advised that adherence to Indiana Code 14-21-1-27 and Indiana Code 14-21-1-29 does not obviate the need to adhere to applicable federal statutes and regulations, including but not limited to 36 C.F.R. Part 800.

Unless another consulting party expresses a different opinion about this project's effects, it might now be appropriate to ask INDOT for a finding.

The Indiana SHPO staff's archaeological reviewer for this project is Wade T. Tharp, and the structures reviewer is Caitlin Lehman. However, if you have a question about the Section 106 process, please contact initially the INDOT Cultural Resources staff members who are assigned to this project.

In all future correspondence about the US 20 Bridge improvement project in Lake County (Des. No. 1900009), please refer to DHPA No. 28563.

Very truly yours,



Beth K. McCord
Deputy State Historic Preservation Officer

BKM:CML:WTT:wt

emc: Kari Carmany-George, Federal Highway Administration
Matt Coon, Indiana Department of Transportation
Susan Branigin, Indiana Department of Transportation
Mary Kennedy, Indiana Department of Transportation
Hannah Blad, Lochmueller Group
Chad Costa, Lochmueller Group
Gary Quigg, Lochmueller Group
Scott Bocock, Lake County Historian
Caitlin Lehman, Indiana DNR-DHPA
\
Indiana Wade T. Tharp, DNR-DHPA

Section 106 800.11(e)

Appendix E

Historic Property Report Summary

Archaeology Report Summary

Effects Report Summary

US 20 BRIDGE IMPROVEMENT PROJECT

Historic Property Report

*US 20 over Kennedy Avenue &
Indiana Harbor Belt Railroad,
0.99 mile east of SR 152*

City of Hammond, Lake County, Indiana

Des. No. 1900009
DHPA No. 28563



Lochmueller Group, Inc.

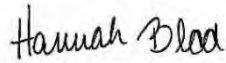
112 W. Jefferson Blvd., Suite 500

South Bend, Indiana 46601

Phone: 574.334.5460

Prepared For:
Indiana Department of Transportation
Federal Highway Administration

Prepared By:

A handwritten signature in black ink that reads "Hannah Blad". The signature is written in a cursive, flowing style.

Hannah Blad

February 28, 2022

US 20 Bridge Improvement Project
US 20 over Kennedy Avenue & Indiana Harbor Belt Railroad, 0.99 mile east of SR 152
City of Hammond, Lake County, Indiana
Des. No. 1900009
Historic Property Report

I. MANAGEMENT SUMMARY

This report documents the identification and evaluation efforts for properties included in the Area of Potential Effects (APE) for the US 20 Bridge Improvement Project on US 20 over Kennedy Avenue and the Indiana Harbor Belt Railroad (IHBR) in the City of Hammond, Lake County, Indiana. Above-ground resources located within the project APE were identified and evaluated in accordance with Section 106, National Historic Preservation Act (NHPA) of 1966, as amended, and the regulations implementing Section 106 (36 CFR Part 800).

As a result of the NHPA, as amended, and CFR Part 800, federal agencies are required to take into account the impact of federal undertakings upon historic properties in the area of the undertaking. Historic properties include buildings, structures, sites, objects, and/or districts that are eligible for or listed in the National Register of Historic Places (NRHP). As this project is receiving funding from the Federal Highway Administration (FHWA), it is subject to a Section 106 review.

The APE contains no properties listed in the NRHP.

The APE contains one property that is recommended eligible for listing in the NRHP:

- Pumping Station; 5721 Kennedy Avenue, Hammond, IN

**A PHASE IA ARCHAEOLOGICAL RECONNAISSANCE
FOR A BRIDGE REPLACEMENT PROJECT ON US 20 AT
KENNEDY AVENUE, IHB RR, 0.99 MI E OF SR 152 IN
LAKE COUNTY, INDIANA (INDOT DES. NO. 1900009)**

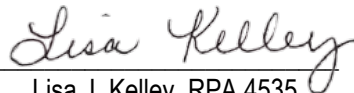
by
Aaron L. Harth

Prepared for

C.J. Cunningham
Lochmueller Group
112 W Jefferson Blvd, Suite 500
South Bend, Indiana 46601
(574) 334-5464
CCunningham@lochgroup.com

Prepared by

Cultural Resource Analysts, Inc.
201 NW 4th Street, Suite 204
Evansville, Indiana 47708
Phone: (812) 253-3009
Fax: (812) 253-3010
Email: amartin@crai-ky.com
CRA Project No.: I210470



Lisa J. Kelley, RPA 4535
Principal Investigator

February 22, 2022

Lead Agency: Indiana Department of Transportation Des. No.: 1900009
Applied Archaeology Laboratories at Ball State University Accession #: 21.21

ABSTRACT

Between November 30 and December 2, 2021, Cultural Resource Analysts, Inc., personnel conducted a phase Ia archaeological reconnaissance for a bridge replacement project on US 20 in Lake County, Indiana (Indiana Department of Transportation Designation Number 1900009). The survey was conducted at the request of Lochmueller Group with the purpose of identifying archaeological resources that could be affected by the project and assessing their eligibility for the National Register of Historic Places. The survey area for the proposed project has a total length of approximately 1.1 km (0.7 mi), and encompasses approximately 10.3 ha (25.5 acres) of new, temporary, and existing right-of-way. The survey area was investigated using methods consisting of systematic screened shovel testing and visual inspection of obviously disturbed areas.

Prior to conducting this survey, an archaeological records review was completed using the Indiana Division of Historic Preservation and Archaeology's State Historic Architectural and Archaeological Research Database. The review indicated that there are no previously recorded sites mapped within the survey area.

As a result of the current survey, two new archaeological sites (12La736 and 12La737) were recorded. The newly documented sites are historic artifact scatters that date to the twentieth century. Overall, the portions of these sites that were investigated within the survey area lack the potential to provide important information regarding the history of this region and are recommended not eligible for inclusion in the National Register of Historic Places. Thus, no further work is recommended for the sites, and archaeological clearance is recommended for the proposed project.

US 20 Bridge Improvement Project Effects Report

*US 20 over Kennedy Avenue & Indiana
Harbor Belt Railroad, 0.99 mile east of
SR 152*

City of Hammond, Lake County, IN

Des. No. 1900009

DHPA No. 28563



Lochmueller Group, Inc.

112 W. Jefferson Blvd., Suite 500

South Bend, Indiana 46601

Phone: 574.334.5460

Prepared For:
Indiana Department of Transportation
Federal Highway Administration

Prepared By:

A handwritten signature in black ink that reads "Hannah Blad". The signature is written in a cursive, flowing style.

Hannah Blad

August 22, 2023

Per 800.5(a)(2)(i), the undertaking will not result in the “Physical destruction of or damage to all or part of the property.” Though grading and below-ground storm sewer work will take place on the north side of the historic property, this work will not result in the damage or physical destruction to contributing elements to the historic property.

Per 36 CFR 800.5(a)(2)(ii), the undertaking will not cause “Alteration of a property, including restoration, rehabilitation, repair, maintenance, stabilization, hazardous material remediation and provision of handicapped access, that is not consistent with the Secretary’s Standards for the Treatment of Historic Properties and/or other applicable guidelines.” Though grading and below-ground storm sewer work will take place on the north side of the historic property, this work will not result in the damage or physical destruction to contributing elements to the historic property. A Phase II Environmental Site Assessment (ESA) will occur on the Buckeye Pipeline property west of Kennedy Avenue, not on the property associated with the Pumping Station.

Per 36 CFR 800.5(a)(2)(iii), the undertaking will not result in the “Removal of the property from its historic location.” The project will not remove the historic property from its current/historic location.

Per 36 CFR 800.5(a)(2)(iv), the undertaking will not result in a “Change of the character of the property’s use or of physical features within the property setting that contribute to its historic significance.” Though grading and below-ground storm sewer work will take place on the north side of the historic property, this work will not result in the damage or physical destruction to contributing elements to the historic property.

Per 36 CFR 800.5(a)(2)(v), the undertaking will not cause the “Introduction of visual, atmospheric or audible elements that diminish the integrity of the property’s significant historic features.” Generally, what is visible from the historic property will remain the same following the completion of the undertaking. The proposed work will not increase audible sound levels or atmospheric elements to the historic property.

Per 36 CFR 800.5(a)(2)(vi), the undertaking will not cause the “Neglect of a property which causes its deterioration...”

Per 36 CFR 800.5(a)(2)(vii), the undertaking will not cause the “Transfer, lease, or sale of property out of Federal ownership or control...” Ownership of the historic resource will not change as a result of this project.

V. Conclusion

It is the opinion of the Qualified Professionals that the preliminary effect finding for the individual NRHP-eligible property is as follows:

- Pumping Station (Lochmueller #2) – **No Adverse Effect**

It is also the opinion of the Qualified Professionals that the preliminary effect finding for the overall undertaking is **No Adverse Effect**.

Division of Historic Preservation & Archaeology · 402 W. Washington Street, W274 · Indianapolis, IN 46204-2739
Phone 317-232-1646 · Fax 317-232-0693 · dhpa@dnr.IN.gov ·



December 12, 2023

Hannah Blad
Historian/Section 106 Specialist
Lochmueller Group
112 W. Jefferson Blvd., Suite 500
South Bend, Indiana 46601

Federal Agency: Indiana Department of Transportation ("INDOT"),
on behalf of Federal Highway Administration, Indiana Division ("FHWA")

Re: Indiana Department of Transportation's finding of "no adverse effect," on behalf of the Federal
Highway Administration, for the US 20 Bridge Improvement Project (Des. No. 1900009; DHPA No.
28563)

Dear Ms. Blad:

Pursuant to Section 106 of the National Historic Preservation Act of 1966, as amended (54 U.S.C. § 306108), 36 C.F.R. Part 800, and the "Programmatic Agreement (PA) Among the Federal Highway Administration, the Indiana Department of Transportation, the Advisory Council on Historic Preservation and the Indiana State Historic Preservation Officer Regarding the Implementation of the Federal Aid Highway Program In the State of Indiana," the staff of the Indiana State Historic Preservation Officer ("Indiana SHPO") has reviewed your November 13, 2023, submission, which enclosed INDOT's finding and supporting documentation, received by our office November 13, 2023, for this project in the City of Hammond, Lake County, Indiana.

As previously stated, we agree with the conclusions of the HPR that the Kennedy Avenue Pumping Station at 5721 Kennedy Avenue is eligible for inclusion in the National Register of Historic Places ("NRHP") and that this is the only historic property located in this project's area of potential effects ("APE"). Additionally, we agree that this historic resource will not be adversely affected by this federal undertaking.

In regard to archaeological resources, the present submission states:

"With regard to archaeological resources, an archaeologist who meets the Secretary of the Interior's Professional Qualification Standards identified two sites within the project area. As a result of these efforts, sites 12La736 and 12La737 were recommended not eligible for listing in the NRHP and no further work is recommended."

"In multiple previous correspondence, the State Historic Preservation Officer (SHPO) has asked that the portions of sites 12LA0736 and 12LA0737 outside the survey area be clearly marked on the plans as an area of no disturbance. The portions of sites 12LA0736 and 12LA0737 outside the survey area were unable to be marked on the plan sheets in the attached Appendix F of the Finding/800.11 Documentation because the project area does not extend past the portions of the sites previously surveyed by CRA. The archaeology survey area encompassed a large area wherein potential disturbance or ROW acquisition would occur for this undertaking, when in fact the actual project area where construction activities will take place and ROW acquisition will occur is much smaller than the survey area. The proposed undertaking will not even encroach upon archaeology sites 12LA0736 and 12LA0737 as mapped in the above-referenced archaeological reconnaissance report. The portions of the sites outside the survey area are too far away from the construction area to show up on the plan sheets for the project."

However, we note that the Phase Ia archaeological investigations report (Harth, 02/22/2022), includes the following language:

"[...] However, the artifact scatter does potentially extend south of the survey boundary and the NRHP status of the entire site could not be fully assessed in this direction. Nevertheless, the portion of Site 12La736 that was investigated during the current survey demonstrated a lack of significant information and is not recommended to be eligible for the NRHP. Therefore, no further work is recommended for the portion of this site within the survey area" (This is from p. 30.)

"[...] Overall, the site deposits have little research potential beyond the data which has already been obtained. However, the artifact scatter does potentially extend west of the survey boundary and the NRHP status of the entire site could not be fully assessed in this direction. Nevertheless, the portion of Site 12La737 that was investigated during the current

survey demonstrated a lack of significant information and is not recommended to be eligible for the NRHP. Therefore, no further work is recommended for the portion of this site within the survey area.” (This is from p. 36.)

As previously indicated, in regard to archaeological resources, based upon the submitted information and the documentation available to the staff of the Indiana SHPO, there is insufficient information regarding archaeological sites 12-La-0736 and 12-La-0737 (both of which were identified during these investigations) to determine whether they are eligible for inclusion in the NRHP. However, we concur with the opinion of the archaeologist, as expressed in the Phase Ia archaeological investigations report (Harth, 02/22/2022), that the portions of these sites that lie within the proposed project area do not appear to contain significant archaeological deposits, and that no further archaeological investigations appear necessary at the proposed project area. It is our understanding, from the present submission, that the proposed undertaking will not encroach upon these two sites as mapped in the archaeology report; and that, because the sites lie outside the survey area, and they are too far away from the construction area to show up on the plan sheets for the project, in this particular case marking the portions of these two sites that lie outside the proposed project is unfeasible. If the proposed project area in these areas were to be altered to potentially include additional portions of either of these two sites, then additional archaeological investigations may be necessary. As a reminder, plans relating to subsurface archaeological investigations must be submitted to the Division of Historic Preservation and Archaeology (“DHPA”) for review and comment. Any further archaeological investigations must be done in accordance with the “Secretary of the Interior’s Standards and guidelines for Archeology and Historic Preservation” (48 F.R. 44716).

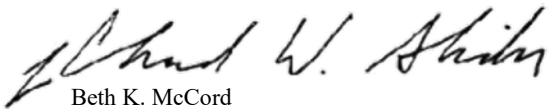
Accordingly, we concur with INDOT’s November 13, 2023, Section 106 finding of “No Adverse Effect” on behalf of FHWA for this federal undertaking.

If any prehistoric or historic archaeological artifacts or human remains are uncovered during construction, demolition, or earthmoving activities, state law (Indiana Code 14-21-1-27 and Indiana Code 14-21-1-29) requires that the discovery be reported to Indiana Department of Natural Resource, Division of Historic Preservation and Archaeology, within two (2) business days. In that event, please call (317) 232-1646. Be advised that adherence to Indiana Code 14-21-1-27 and Indiana Code 14-21-1-29 does not obviate the need to adhere to applicable federal statutes and regulations, including but not limited to 36 C.F.R. Part 800.

The Indiana SHPO staff’s archaeological reviewer for this project is Wade T. Tharp, and the structures reviewer is Caitlin Lehman. However, if you have a question about the Section 106 process, please contact initially the INDOT Cultural Resources staff members who are assigned to this project.

In all future correspondence about the US 20 Bridge Improvement project in Lake County (Des. No. 1900009), please refer to DHPA No. 28563.

Very truly yours,



Beth K. McCord
Deputy State Historic Preservation Officer

BKM:CML:WTT:wt

emc: Kari Carmany-George, Federal Highway Administration
Matt Coon, Indiana Department of Transportation
Susan Branigin, Indiana Department of Transportation
Mary Kennedy, Indiana Department of Transportation
Hannah Blad, Lochmueller Group
Chad Costa, Lochmueller Group
Gary Quigg, Lochmueller Group
Scott Bocock, Lake County Historian
Caitlin Lehman, Indiana DNR-DHPA
Wade T. Tharp, Indiana DNR-DHPA

Public Notice
Des. No. 1900009

The Indiana Department of Transportation (INDOT) is planning to undertake a bridge improvement project, funded in part by the Federal Highway Administration (FHWA). The project is located on US 20 within the City of Hammond, Lake County, Indiana.

Under the preferred alternative, the proposed project would involve the replacement of the existing bridge with a two-span structure with mechanically stabilized earth (MSE) wall abutments, a reinforced concrete pier, a length of approximately 222 feet, a clear roadway width of 53 feet 4 inches, and an out-to-out width of approximately 70 feet 8 inches. A 4-foot-wide curbed center median will be installed between the eastbound and westbound traffic lanes along US 20. A raise in the profile grade will be necessary to provide adequate vertical clearance over the railroad and Kennedy Ave. The access roads intersection with US 20 will be shifted east to accommodate the increased profile grade and configured for right-in/right-out only movements. Additionally, the US 20 roadway grade east of the bridge will require modification to achieve proper stopping sight distance at the intersection with the access roads to Kennedy Avenue. A 10-foot-wide multi-use trail will be added to the north side of the new bridge and connect to the planned Marquette Greenway Trail. Approximately 1.13 acres of new permanent ROW and 0.020 acre of temporary ROW will be required. No relocations are anticipated.

Properties listed in or eligible for the National Register of Historic Places (NRHP) located within the Area of Potential Effects (APE) include the Pumping Station at 5721 Kennedy Avenue. The proposed action impacts properties listed in or eligible for the NRHP. The Indiana Department of Transportation (INDOT), on behalf of the FHWA, has issued a “No Adverse Effect” finding for the project because the project will not diminish the integrity of the characteristics that qualify the historic properties within the APE for inclusion in the NRHP. In accordance with the National Historic Preservation Act, the views of the public are being sought regarding the effect of the proposed project on the historic elements as per 36 CFR 800.2(d), 800.3(e) and 800.6(a)(4). Pursuant to 36 CFR 800.4(d)(2), the documentation specified in 36 CFR 800.11(e) is available for inspection in Lochmueller Group offices. Additionally, this documentation can be viewed electronically by accessing INDOT’s Section 106 document posting website IN SCOPE at <https://erms12c.indot.in.gov/Section106Documents>. This documentation serves as the basis for the “No Adverse Effect” finding. The views of the public on this effect finding are being sought. Please reply with any comments to Hannah Blad, 112 W. Jefferson Blvd, Suite 500, South Bend, IN, 46601, 574.334.5487, hblad@lochgroup.com no later than December 20, 2023.

In accordance with the “Americans with Disabilities Act”, if you have a disability for which INDOT needs to provide accessibility to the document(s) such as interpreters or readers, please contact Andrew Borden, 219.851.4481, aborden1@indot.in.gov.

Northwest Indiana Times
601 W. 45th Ave.
(219) 933-3333

I, Stefan Edward Pla, of lawful age, being duly sworn upon oath depose and say that I am an agent of Column Software, PBC, duly appointed and authorized agent of the Publisher of Northwest Indiana Times, a publication that is a "legal newspaper" as that phrase is defined for the city of Munster, for the County of Lake, in the state of Indiana, that this affidavit is Page 1 of 2 with the full text of the sworn-to notice set forth on the pages that follow, and that the attachment hereto contains the correct copy of what was published in said legal newspaper in consecutive issues on the following dates:

PUBLICATION DATES:
Nov. 20, 2023

Notice ID: L9CccfQ7GNpt1bj3qkr
Publisher ID: 64530
Notice Name: Des. No. 1900009

PUBLICATION FEE: \$48.39

Stefan Pla

Agent

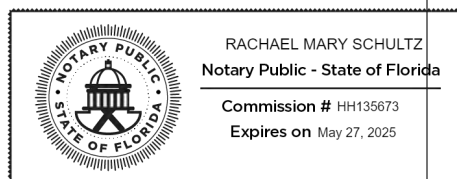
VERIFICATION

State of Florida
County of Charlotte

Signed or attested before me on this: 11/20/2023

Rachael Mary Schultz

Notary Public
Notarized online using audio-video communication



PUBLIC NOTICE
Des. No. 1900009

The Indiana Department of Transportation (INDOT) is planning to undertake a bridge improvement project, funded in part by the Federal Highway Administration (FHWA). The project is located on US 20 within the City of Hammond, Lake County, Indiana.

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Properties listed in or eligible for the National Register of Historic Places (NRHP) located within the Area of Potential Effects (APE) include the Pumping Station at 5721 Kennedy Avenue. The proposed action impacts properties listed in or eligible for the NRHP. The Indiana Department of Transportation (INDOT), on behalf of the FHWA, has issued a "No Adverse Effect" finding for the project because the project will not diminish the integrity of the characteristics that qualify the historic properties within the APE for inclusion in the NRHP. In accordance with the National Historic Preservation Act, the views of the public are being sought regarding the effect of the proposed project on the historic elements as per 36 CFR 800.2(d), 800.3(e) and 800.6(a)(4). Pursuant to 36 CFR 800.4(d)(2), the documentation specified in 36 CFR 800.11(e) is available for inspection in Lochmuller Group offices. Additionally, this documentation can be viewed electronically by accessing INDOT's Section 106 document posting website IN SCOPE at <https://enr12c.indot.in.gov/>. Section 106 Documents. This documentation serves as the basis for the "No Adverse Effect" finding. The views of the public on this effect finding are being sought. Please reply with any comments to Hannah Blad, 112 W. Jefferson Blvd, Suite 500, South Bend, IN, 46601, 574.334.5487, hblad@lochgroup.com no later than December 20, 2023.

In accordance with the "Americans with Disabilities Act", if you have a disability for which INDOT needs to provide accessibility to the document(s) such as interpreters or

Carson Hoogewerf

From: Branigin, Susan <SBranigin@indot.IN.gov>
Sent: Monday, January 29, 2024 8:22 AM
To: Hannah Blad
Cc: Coon, Matthew; Branigin, Susan; Passmore, Andrew D; Michels, Stewart; Borden, Andrew A; Gary Francis Quigg; C.J. Cunningham; Carson Hoogewerf
Subject: RE: US 20 Bridge Improvement Project, Des. No. 1900009, Project Changes

EXTERNAL

Hi Hannah,

Matt & I discussed the project changes described in your email. We agree that a Note to File would be appropriate to document these changes; this email may be used for that purpose in the environmental document.

Best regards,

Susan R. Branigin, MS

History Unit Team Lead/Supervisor

Cultural Resources Office (CRO)

Indiana Department of Transportation

100 North Senate Ave., N758 —Environmental Services

Indianapolis, IN 46204

Cell: 317.417.1622

Email: sbranigin@indot.in.gov

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****For the latest updates from INDOT's Cultural Resources Office, subscribe to the Environmental Services listserv: <https://www.in.gov/indot/3217.htm>**

****Link to the CRO-Public Web Map App can be found [here](#)**

From: Hannah Blad <HBlad@lochgroup.com>
Sent: Wednesday, January 24, 2024 3:50 PM
To: Branigin, Susan <SBranigin@indot.IN.gov>
Cc: Coon, Matthew <mcoon@indot.IN.gov>; Gary Francis Quigg <GQuigg@lochgroup.com>; C.J. Cunningham <CCunningham@lochgroup.com>; Carson Hoogewerf <CHoogewerf@lochgroup.com>
Subject: US 20 Bridge Improvement Project, Des. No. 1900009, Project Changes

****** This is an EXTERNAL email. Exercise caution. DO NOT open attachments or click links from unknown senders or unexpected email. ******

Hi Susan,


Upon receiving updated plans for Des. No. 1900009, some design details have changed slightly from what was reported in the finding document.

- Construction limits have been slightly extended along US 20 at the far east end of the project area and a corner was extended in an area to the northwest of the bridge
- Temporary ROW has increased from 0.036 acre to 0.06 acre, due to an added strip located northwest of the bridge necessitated by the above change to construction limits

None of these changes will affect the NRHP-eligible property and the APE does not need to be revised. The archaeology survey area covers all these changes as well. Do you concur that a Note to File will be appropriate to document these changes to the project?

Thanks,



 **Web:** <http://lochgroup.com>



Hannah Blad

Hist/Sec 106 Specialist II



Lochmueller Group

112 W Jefferson Blvd, Suite 500, South Bend, IN 46601



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Categorical Exclusion

Appendix E

**Red Flag Investigation
& Hazardous Materials**



INDIANA DEPARTMENT OF TRANSPORTATION

100 North Senate Avenue
Room N758-ES
Indianapolis, Indiana 46204

PHONE: (855) 463-6848
(855) INDOT4U

Eric Holcomb, Governor

**Michael Smith,
Commissioner**

Date: June 7, 2022

To: Site Assessment & Management (SAM)
Environmental Policy Office - Environmental Services Division (ESD)
Indiana Department of Transportation (INDOT)
100 N Senate Avenue, Room N758-ES
Indianapolis, IN 46204

From: Carson Hoogewerf
Lochmueller Group
112 W. Jefferson Blvd #500
South Bend, IN 46601
CHoogewerf@lochgroup.com

Re: RED FLAG INVESTIGATION
DES 1900009, State Project
Bridge Project
US 20 over Kennedy Ave. and Indiana Harbor Belt (IHB) Railroad, 0.99 Mile East of State Road (SR) 152
Lake County, Indiana

PROJECT DESCRIPTION

Brief Description of Project:

The Federal Highway Administration (FHWA) and the Indiana Department of Transportation (INDOT), LaPorte District propose a bridge project located along US 20 approximately 0.99 mile east of SR 152. The existing structure (Bridge # 020-45-02484 B) is a three-span, prestressed concrete beam bridge spanning a dry crossing over Kennedy Avenue and Indiana Harbor Belt (IHB) Railroad. The out-to-out length is approximately 196 feet and the existing clear roadway width 52 feet. The preferred alternative involves replacement of the existing bridge with a two-span structure with mechanically stabilized earth (MSE) wall abutments, a new reinforced concrete pier, an out-to-out length of approximately 260 feet, and a clear roadway width of 40 feet. A 4-foot-wide curbed center median will be installed between the eastbound and westbound traffic lanes along US 20. A multi-use path separated from vehicular traffic with a pedestrian traffic barrier will be added to the north side of the structure. It is anticipated that a raise in the profile grade of the bridge and roadway will be necessary. It is anticipated that additional permanent right-of-way (ROW) may need to be acquired. New permanent ROW amounts are not likely to exceed 0.5 acre. Most of the new ROW, if necessary, would be acquired east of Kennedy Avenue and would accommodate any necessary realignments or grade adjustments for the US 20 entry/exit ramps. No relocations are anticipated.

Bridge Work Included in Project: Yes ☒ No ☐ Structure #(s) 020-45-02484 B

If this is a bridge project, is the bridge Historical? Yes ☐ No ☒ , Select ☐ Non-Select ☐

(Note: If the project involves a historical bridge, please include the bridge information in the Recommendations Section of the report).

Culvert Work Included in Project: Yes ☐ No ☒ Structure #(s) _____

Proposed right of way: Temporary ☐ # Acres _____ Permanent ☒ # Acres <0.5, Not Applicable ☐

Type and proposed depth of excavation: Maximum depth of excavation for the project will be approximately 15 feet below ground surface (bgs) with bridge piles driven 75 feet or greater. Excavation along US 20 on approach to the bridge will occur at approximately 4 feet below the original (pre-roadway embankment) grade. Excavation at the on/off ramps will occur at approximately 2 feet below the original grade for ramp pavement construction. Excavation up to 10 feet bgs will occur at the end bents, located east of Kennedy Avenue and west of IHB Railroad. Excavation up to 15 feet bgs will occur at the pier pile caps west of Kennedy Avenue and east of IHB railroad.

Maintenance of traffic (MOT): The MOT will likely include a full closure with detour route. The specific route has not yet been developed.

Work in waterway: Yes ☐ No ☒ Below ordinary high water mark: Yes ☐ No ☐

State Project: ☒ LPA: ☐

Any other factors influencing recommendations: N/A

INFRASTRUCTURE TABLE AND SUMMARY

Infrastructure Indicate the number of items of concern found within the 0.5 mile search radius. If there are no items, please indicate N/A:			
Religious Facilities	N/A	Recreational Facilities	2
Airports ¹	1	Pipelines	20
Cemeteries	N/A	Railroads	20
Hospitals	N/A	Trails	5
Schools	N/A	Managed Lands	6

¹In order to complete the required airport review, a review of public-use airports within 3.8 miles (20,000 feet) is required.

Explanation:

Airports: Although not located within the 0.5 mile search radius, one (1) public-use airport, Gary Municipal Airport, is located within 3.8 miles (20,000 feet) of the project area. The public use airport is located approximately 1.6 miles east of the project area; therefore, early coordination with INDOT Aviation will occur.

Recreational Facilities: Two (2) recreational facilities are located within the 0.5 mile search radius. The nearest facility, Seidner Dune and Swale Nature Preserve, is located 0.05 mile northeast of the project area. No impact is expected.

Pipelines: Twenty (20) pipeline segments are located within the 0.5 mile search radius. Eight (8) pipeline segments cross or are adjacent to the project area, including two pipelines from Buckeye Pipeline Co. (refined products and crude oil), two pipelines from Wolverine Pipe Line Co. (refined products), and one pipeline each from Northern Indiana Public Service Co. (NIPSCO) (natural gas), Phillips Pipeline Co. (refined products), Amoco Pipeline Co. (crude oil), and Explorer Pipeline Co. (refined products). Coordination with INDOT Utilities and Railroads will occur.

Railroads: Twenty (20) railroad segments are located within the 0.5 mile search radius. Two (2) railroad segments, CSX Railroad and Norfolk Southern Railroad, cross the project area. Standard coordination will occur with INDOT Utilities and Railroads by the Project Management Team or their consultant no later than the Ready for Contracts (RFC) date.

Trails: Five (5) trail segments are located within the 0.5 mile search radius. Two (2) planned trail segments are located in project area. Coordination with East Chicago Parks and Recreation Department and Hammond Parks and Recreation Department will occur.

Managed Lands: Six (6) Managed Lands are located within the 0.5 mile search radius. The nearest Managed Land, Seidner Dune and Swale Nature Preserve, is located 0.05 mile northeast of the project area. No impact is expected.

WATER RESOURCES TABLE AND SUMMARY

Water Resources Indicate the number of items of concern found within the 0.5 mile search radius. If there are no items, please indicate N/A:			
NWI - Points	N/A	Canal Routes - Historic	N/A
Karst Springs	N/A	NWI - Wetlands	33
Canal Structures – Historic	N/A	Lakes	9
NPS NRI Listed	N/A	Floodplain - DFIRM	10
NWI-Lines	6	Cave Entrance Density	N/A
IDEM 303d Listed Streams and Lakes (Impaired)	N/A	Sinkhole Areas	N/A
Rivers and Streams	4	Sinking-Stream Basins	N/A

If unmapped water features are identified that might impact the project area, direct coordination with INDOT ESD Ecology and Waterway Permitting will occur.

Explanation:

NWI-Lines: Six (6) NWI-lines are located within the 0.5 mile search radius. The nearest NWI-line is located 0.35 mile north of the project area. No impact is expected.

Rivers and Streams: Four (4) rivers and streams are located within the 0.5 mile search radius. The nearest river, Grand Calumet River, is located 0.17 mile north of the project area. No impact is expected.

NWI – Wetlands: Thirty-three (33) wetlands are located within the 0.5 mile search radius. One (1) wetland is located within the project area. A Waters of the US Report is recommended based on mapped features, and coordination with INDOT ESD Ecology and Waterway Permitting will occur.

Lakes: Nine (9) lakes are located within the 0.5 mile search radius. The nearest lake is located 0.19 mile north of the project area. No impact is expected.

Floodplain – DFIRM: Ten (10) floodplain polygons are located within the 0.5 mile search radius. The nearest floodplain polygon is located 0.14 mile north of the project area. No impact is expected.

MINING AND MINERAL EXPLORATION TABLE AND SUMMARY

Mining/Mineral Exploration Indicate the number of items of concern found within the 0.5 mile search radius. If there are no items, please indicate N/A:			
Petroleum Wells	1	Mineral Resources	N/A
Mines – Surface	N/A	Mines – Underground	N/A

Explanation:

Petroleum Wells: One (1) petroleum well is located within the 0.5 mile search radius. The nearest well is located 0.45 mile northeast of the project area. No impact is expected.

HAZARDOUS MATERIAL CONCERNS TABLE AND SUMMARY

Hazardous Material Concerns Indicate the number of items of concern found within the 0.5 mile search radius. If there are no items, please indicate N/A:			
Superfund	1*	Manufactured Gas Plant Sites	N/A
RCRA Generator/ TSD	5	Open Dump Waste Sites	N/A
RCRA Corrective Action Sites	N/A	Restricted Waste Sites	N/A
State Cleanup Sites	4	Waste Transfer Stations	N/A
Septage Waste Sites	N/A	Tire Waste Sites	N/A
Underground Storage Tank (UST) Sites	1	Confined Feeding Operations (CFO)	N/A
Voluntary Remediation Program	2	Brownfields	2
Construction Demolition Waste	N/A	Institutional Controls	10
Solid Waste Landfill	1	NPDES Facilities	10
Infectious/Medical Waste Sites	N/A	NPDES Pipe Locations	17
Leaking Underground Storage (LUST) Sites	5	Notice of Contamination Sites	N/A

Unless otherwise noted, site specific details presented in this section were obtained from documents reviewed on the Indiana Department of Environmental Management (IDEM) Virtual File Cabinet (VFC).

Explanation:

Superfund:* One (1) Superfund site, USS Lead Refinery Inc, located at 5300 Kennedy Ave, AID 20151, although not mapped, is located 0.19 mile north of the project area, on the north bank of Grand Calumet River. This superfund site is divided into a northern residential section (OU1) and a southern section including the refinery facility and 79-acre grounds (OU2). Only the southern site is within the 0.5 mile search radius. According to an IDEM Office Memorandum dated February 25, 2020, the site media of concern include surface soil, subsurface soil, sediments, and groundwater around the USS Lead Facility. Heavy metal concentrations (arsenic and lead) are elevated in the soil and groundwater beneath the site as a result of deposition from air and various waste piles. According to The First Five-Year Review Report

for OU1, dated August 24, 2021, the southern site in question is still undergoing remedial investigation, and no Record of Decision has been signed. Groundwater generally flows southwest toward the Grand Calumet River. Due to the history of heavy metal deposition, groundwater contamination, and airborne contaminant deposition nearby the project area, a Phase II Environmental Site Assessment is recommended to occur before RFC. Prior to any investigation activities, an SOW plan will be prepared and submitted to INDOT SAM for review and approval.

RCRA Generator/TSD: Two (2) RCRA Generator/TSD sites are located within or adjacent to the project area:

- Dana Transport, located at 5723 Kennedy Ave, AID 17690, is located 0.01 mile south of the project area. According to the IDEM hazardous waste handler identification form, dated October 3, 2003, the site was a large quantity generator (LQG) of petroleum products. The facility ceased operations in April of 2003. Contaminants of Concern (COCs) exist in the soil and groundwater at the site including benzene, toluene, ethylbenzene, and xylenes. Although not mapped, the site is also a LUST site with Institutional Control. See *Leaking Underground Storage Tank (LUST)* and *Institutional Control* sections below.
- Buckeye Terminals LLC Hammond, located at 2400 Michigan St., AID 11991, is located within the project area. The site is an active petroleum storage terminal currently operated by Buckeye. According to the latest hazardous waste handler identification Form dated February 21, 2012, this site was a LQG of hazardous waste including lead until 2011, then switched to a Conditionally Exempt Small Quantity Generator in 2012. The site is also in the voluntary remediation program. See *Voluntary Remediation Program* section below.

State Cleanup: One (1) State Cleanup site, Buckeye Terminals LLC Hammond, located at 2400 Michigan Street, AID 11991, is located within the project area. According to email correspondence dated January 29, 2003, authority for cleanup of the site was transferred from State Cleanup section to the Voluntary Remediation Program. See the *Voluntary Remediation Program* Section below.

Underground Storage Tank (UST) Site: One (1) UST site, Buckeye Terminals LLC Hammond, located at 2400 Michigan Street, AID 11991, is located within the project area. According to the UST Removal Notification Form dated August 23, 1989, one 1,000 gallon used oil UST was removed in October of 1988. No releases were indicated. See the *Voluntary Remediation Program* Section below.

Voluntary Remediation Program: One (1) Voluntary Remediation Program, Buckeye Terminals LLC Hammond, located at 2400 Michigan Street, AID 11991, is located within the project area. The facility is an active petroleum storage terminal and contains above ground storage tanks (ASTs), truck loading racks, and rail car loading racks. Multiple releases of petroleum products have occurred at the site with resulting remediation actions. According to the IDEM Covenant Not to Sue letter dated June 30, 2020, the requirements for the VRP have been fulfilled and a Certificate of Completion was issued February 20, 2020; however, contamination remains on the property and has migrated off-site.

- If excavation occurs in this area, it is possible that petroleum contamination will be encountered. Proper handling, removal, and disposal of soil and/or groundwater may be necessary. Refer to Appendix G of the SAM Manual for the recommended procedure to manage and report contamination.
- If groundwater monitoring wells are encountered in the project area, they should be maintained in place. If they cannot be maintained, then the contractor must contact the INDOT Project Manager who will notify the INDOT Permits Group. The INDOT Permits Group will notify the permit holder that the well must be removed prior to construction. The permit holder is responsible for coordination with IDEM and the INDOT Permits Group for replacement or relocation of the well. If a property owner cannot be found in connection with the monitoring well, then well abandonment will be included in the project contract. All well abandonment activities must be completed by an Indiana Licensed Well Driller in accordance with IAC 312-13-10. Regardless of whether the well is abandoned by the contractor or the property owner, a record of well abandonment, including the well driller's license number, must be provided to the INDOT Project Manager once the well has been abandoned.

Solid Waste Landfill: One (1) Solid Waste Landfill, East Chicago Dump, located approximately 500 feet north of the Indiana East-West Tollway between Indianapolis Boulevard and Wicker Avenue, AID 20153, is located 0.34 mile northwest of the project area. No impact is expected.

Leaking Underground Storage Tank (LUST) Sites: One (1) LUST site is located adjacent to the project area:

- USS Lead Refinery Inc, located at 5300 Kennedy Ave, AID 20151, is located 0.19 mile north of the project area and is the nearest mapped LUST. According to the No Further Action Approval letter dated September 11, 2020, tanks were removed 27 years prior, petroleum contamination is below residential screening levels, and no further response actions were required. No impact is expected.
- Dana Transport, located at 5723 Kennedy Ave, AID 17690, is located 0.01 mile south of the project area. This LUST is not mapped on the GIS database. See *Institutional Control* section below.

Brownfields: One (1) Brownfield site, Trident Machine Technologies Incorporated, located at 2300 & 2320 Michigan Street, AID 25669, is located 0.17 mile southwest of the project area. According to the most recent IDEM site status letter dated December 22, 2021, soil sampling showed mercury, lead, and chromium contamination and groundwater samples showed benzene contamination. Because contamination exists at the site, if excavation occurs in this area, proper handling, removal, and disposal of soil and/or groundwater may be necessary. Refer to Appendix G of the SAM Manual for the recommended procedure to manage and report contamination.

Institutional Controls: Two (2) Institutional Controls are located within or adjacent to the project area:

- Buckeye Terminals LLC Hammond, located at 2400 Michigan Street, AID 11991, is located within the project area. The property is located both north of the Indiana Toll Road and south of US 20 within the project area. Two institutional control polygons representing this site extend into the project area. An Environmental Restrictive Covenant (ERC) was instituted for this site on December 5, 2019. The ERC dictates the following: the site shall not be used for residential purposes, extraction of groundwater, or agriculture; soil disturbed as a result of excavation and construction activities shall be restored; written evidence including sampling data showing the excavated and restored area affected by the excavation shall be provided; disposal of soils must be done in accordance with all applicable federal and state laws; the Gundle Barrier (located approximately 0.33 mile northwest of the project area) shall not be removed; finally IDEM shall be notified in writing at least 15 days in advance of conducting any construction or excavation work that may impact an engineered control. Volatile organic compounds (VOCs) exist in the soil at this site. If excavation occurs in this area, it is possible that petroleum contamination will be encountered. Proper handling, removal, and disposal of soil and/or groundwater may be necessary. Coordination will be conducted with the IDEM Institutional Controls section (institutionalcontrols@idem.IN.gov) before RFC. Refer to Appendix G of the SAM Manual for the recommended procedure to manage and report contamination.
- Dana Transport, located at 5723 Kennedy Ave, AID 17690, is located 0.01 mile south of the project area. COCs exist in the soil and groundwater at the site including benzene, toluene, ethylbenzene, and xylenes. The ERC forbids the site from being used as residential property, extraction of groundwater, and agriculture. It forbids use of the property for occupancy of a dwelling or workspace unless vapor mitigation is performed. Finally, it dictates that soil disturbed as a result of excavation and construction activities shall be restored in a manner that the remaining contaminant concentrations do not present a threat to human health or the environment, and that excavation and disposal of contaminated soils must be managed in accordance with all applicable federal and state laws. The ERC only applies to the parcel immediately adjacent to Kennedy Ave. (Parcel 201-003); the ERC does not apply to the storage yard to the east that extends into the project area. If excavation occurs in this area, it is possible that petroleum contamination will be encountered. Proper handling, removal, and disposal of soil and/or groundwater may be necessary. Coordination will be conducted with the IDEM Institutional Controls section (institutionalcontrols@idem.IN.gov) before RFC. Refer to Appendix G of the SAM Manual for the recommended procedure to manage and report contamination.

NPDES Facility: Five (5) NPDES Facilities are located within or adjacent to the project area:

- RESCO Products Incorporated, located at 5501 N Kennedy, AID 14646, permit number INRM01871, is located 0.01 mile north of the project area. No information is present in the IDEM VFC or the IDEM Stormwater nSITE website. According to the GIS database, the permit was issued on April 15, 2015 and was set to expire April 12, 2020. Because the permit appears to be expired, no impact is expected.
- East Branch Grand Calumet Remediation Dredge Water Treatment System, located at north of Kennedy Avenue and Michigan Street, AID 105918, is located 0.06 mile north of the project area. According to the IDEM letter dated October 28, 2014, the water treatment system was taken out of service on October 16, 2014, and thus the NPDES Permit was terminated. No impact is expected
- Shell Oil, East Chicago Plant, 2400 Michigan street, AID 25669, permit number ING340006, is located within the project area. The site is a petroleum products terminal. According to the IDEM letter dated December 15, 1998, the NPDES permit was issued January 10, 1995 and was voided after Shell Oil combined with Texaco, Inc. No impact is expected.
- Shell Oil Products East Chicago Terminal ING340042 – According to the letter dated May 15, 2008, the property was sold to Buckeye Partners effective October 1, 2004, who had their own NPDES permit, thus an NPDES permit was no longer needed for this site. No impact is expected.
- Buckeye Terminals Hammond Terminal ING340066, Located at 2400 Michigan St, AID 11991. The facility is located within the project area, and the three outfall pipes associated with the site are located 0.15 mile north of the project area. The master NPDES general permit was renewed on November 1, 2020, effective May 1, 2021, and does not specify an end date. Coordination with the facility and/or permit owner should occur.

NPDES Pipe Locations: One (1) NPDES Pipe Location, Buckeye Terminals Hammond Terminal #003, located at 2400 Michigan St, AID 11991, is located 0.15 mile north of the project area. According to the most recent monitoring report for the month of September 2021, the site is currently undergoing monthly monitoring stormwater discharge and tank bottom/hydrostatic testing and was in compliance. No impact is expected.

ECOLOGICAL INFORMATION SUMMARY

The Lake County listing of the Indiana Natural Heritage Data Center information on endangered, threatened, or rare (ETR) species and high quality natural communities is provided at (https://www.in.gov/dnr/nature-preserves/files/np_lake.pdf). A preliminary review of the Indiana Natural Heritage Database by INDOT ESD did indicate the presence of ETR species within the 0.5 mile search radius. Coordination with USFWS and IDNR will occur.

A review of the USFWS database did not indicate the presence of endangered bat species in or within 0.5 mile of the project area. The project area is located in an urban area. The January 13, 2021 inspection report for Bridge #020-45-02484 B states that no evidence of bats was seen or heard under the bridge. The range-wide programmatic consultation for the Indiana Bat and Northern Long-eared Bat will be completed according to the most recent “Using the USFWS’s IPaC System for Listed Bat Consultation for INDOT Projects”.

RECOMMENDATIONS SECTION

Include recommendations from each section. If there are no recommendations, please indicate N/A:

INFRASTRUCTURE:

Airports: Although not located within the 0.5 mile search radius, one (1) public-use airport, Gary Municipal Airport, is located within 3.8 miles (20,000 feet) of the project area. The public use airport is located approximately 1.6 miles east of the project area; therefore, early coordination with INDOT Aviation will occur.

Pipelines: Eight (8) pipeline segments cross or are adjacent to the project area, including two pipelines from Buckeye Pipe Line Co. (refined products and crude oil), two pipelines from Wolverine Pipe Line Co. (refined products), and one pipeline each from Northern Indiana Public Service Co. (NIPSCO) (natural gas), Phillips Pipe Line Co. (refined products), Amoco Pipeline Co. (crude oil), and Explorer Pipeline Co. (refined products). Coordination with INDOT Utilities and Railroads should occur.

Railroads: Two (2) railroad segments cross the project area. Standard coordination will occur with INDOT Utilities and Railroads by the Project Management Team or their consultant no later than the Ready for Contracts (RFC) date.

Trails: Two (2) planned trail segments cross the project area. Coordination with East Chicago Parks and Recreation Department and Hammond Parks and Recreation Department will occur.

WATER RESOURCES:

NWI – Wetlands: One (1) wetland is located within the project area. A Waters of the US Report will be prepared, and coordination with INDOT ESD Ecology and Waterway Permitting will occur.

MINING/MINERAL EXPLORATION: N/A

HAZARDOUS MATERIAL CONCERNS:

Superfund: One (1) Superfund site, USS Lead Refinery Inc, located at 5300 Kennedy Ave, AID 20151, although not mapped, is located 0.19 mile north of the project area, on the north bank of Grand Calumet River. Heavy metal concentrations are elevated in the soil and groundwater beneath the site as a result of deposition from air and various waste piles. Due to the history of heavy metal deposition and groundwater contamination nearby the project area, a Phase II Environmental Site Assessment is recommended to occur before RFC. Prior to any investigation activities, an SOW plan will be prepared and submitted to INDOT SAM for review and approval.

Voluntary Remediation Program: One (1) Voluntary Remediation Program, Buckeye Terminals LLC Hammond, located at 2400 Michigan Street, AID 11991, is located within the project area. The facility is an active petroleum storage terminal and contains ASTs, truck loading racks, and rail car loading racks. Multiple releases of petroleum products have occurred at the site with resulting remediation actions. According to the IDEM Covenant Not to Sue letter dated June 30, 2020, the requirements for the VRP have been fulfilled and a Certificate of Completion was issued February 20, 2020; however, contamination remains on the property and has migrated off-site.

- If excavation occurs in this area, it is possible that petroleum contamination will be encountered. Proper handling, removal, and disposal of soil and/or groundwater may be necessary. Refer to Appendix G of the SAM Manual for the recommended procedure to manage and report contamination.

- If groundwater monitoring wells are encountered in the project area, they should be maintained in place. If they cannot be maintained, then the contractor must contact the INDOT Project Manager who will notify the INDOT Permits Group. The INDOT Permits Group will notify the permit holder that the well must be removed prior to construction. The permit holder is responsible for coordination with IDEM and the INDOT Permits Group for replacement or relocation of the well. If a property owner cannot be found in connection with the monitoring well, then well abandonment will be included in the project contract. All well abandonment activities must be completed by an Indiana Licensed Well Driller in accordance with IAC 312-13-10. Regardless of whether the well is abandoned by the contractor or the property owner, a record of well abandonment, including the well driller's license number, must be provided to the INDOT Project Manager once the well has been abandoned.

Brownfield: One (1) Brownfield, Trident Machine Technologies Incorporated, located at 2300 & 2320 Michigan Street, AID 25669, is located 0.17 mile southwest of the project area. According to the most recent IDEM site status letter dated December 22, 2021, soil sampling showed mercury, lead, and chromium contamination and groundwater samples showed benzene contamination. Because contamination exists at the site, if excavation occurs in this area, proper handling, removal, and disposal of soil and/or groundwater may be necessary. Refer to Appendix G of the SAM Manual for the recommended procedure to manage and report contamination.

Institutional Controls: Two (2) Institutional Control sites are located within or adjacent to the project area.

- Buckeye Terminals LLC Hammond, located at 2400 Michigan Street, AID 11991, is located within the project area. The property is located both north of the Indiana Toll Road and south of US 20 within the project area. Two institutional control polygons representing this site extend into the project area. An ERC was instituted for this site on December 5, 2019. The ERC dictates the following: the site shall not be used for residential purposes, extraction of groundwater, or agriculture; soil disturbed as a result of excavation and construction activities shall be restored; written evidence including sampling data showing the excavated and restored area affected by the excavation shall be provided; disposal of soils must be done in accordance with all applicable federal and state laws; the Gundle Barrier (located approximately 0.33 mile northwest of the project area) shall not be removed; finally IDEM shall be notified in writing at least 15 days in advance of conducting any construction or excavation work that may impact an engineered control. Volatile organic compounds (VOCs) exist in the soil at this site. If excavation occurs in this area, it is possible that petroleum contamination will be encountered. Proper handling, removal, and disposal of soil and/or groundwater may be necessary. Coordination will be conducted with the IDEM Institutional Controls section (institutionalcontrols@idem.IN.gov) before RFC. Refer to Appendix G of the SAM Manual for the recommended procedure to manage and report contamination.
- Dana Transport, located at 5723 Kennedy Ave, AID 17690, is located 0.01 mile south of the project area. COCs exist in the soil and groundwater at the site including benzene, toluene, ethylbenzene, and xylenes. The ERC forbids the site from being used as residential property, extraction of groundwater, and agriculture. It forbids use of the property for occupancy of a dwelling or workspace unless vapor mitigation is performed. Finally, it dictates that soil disturbed as a result of excavation and construction activities shall be restored in a manner that the remaining contaminant concentrations do not present a threat to human health or the environment, and that excavation and disposal of contaminated soils must be managed in accordance with all applicable federal and state laws. The ERC only applies the parcel immediately adjacent to Kennedy Ave. (Parcel 201-003) and does not apply to the scrap/storage yard to the east that extends into the project area. If excavation occurs in this area, it is possible that petroleum contamination will be encountered. Proper handling, removal, and disposal of soil and/or groundwater may be necessary. Coordination will be conducted with the IDEM Institutional Controls section (institutionalcontrols@idem.IN.gov) before RFC. Refer to Appendix G of the SAM Manual for the recommended procedure to manage and report contamination.

NPDES Facility: One (1) NPDES Facility, Buckeye Terminals Hammond Terminal ING340066, located at 2400 Michigan St, AID 11991, is located within the project area, and the three (3) outfall pipes associated with the site are located 0.15 mile north of the project area. The master NPDES general permit was renewed on November 1, 2020, effective May 1, 2021, and does not specify an end date. Coordination with the facility and/or permit owner should occur.

ECOLOGICAL INFORMATION: Coordination with USFWS and IDNR will occur. The range-wide programmatic consultation for the Indiana Bat and Northern Long-eared Bat will be completed according to the most recent "Using the USFWS's IPaC System for Listed Bat Consultation for INDOT Projects".

INDOT ESD concurrence: **Chad Pitcher,** Digitally signed by Chad Pitcher, CHMM
Date: 2022.06.07 09:54:56 -04'00'
CHMM _____ (Signature)

Prepared by:



Carson J. Hoogewerf
Environmental Specialist I
Lochmueller Group, Inc.

Graphics:

A map for each report section with a 0.5 mile search radius buffer around all project area(s) showing all items identified as possible items of concern is attached. If there is not a section map included, please change the YES to N/A:

SITE LOCATION: YES

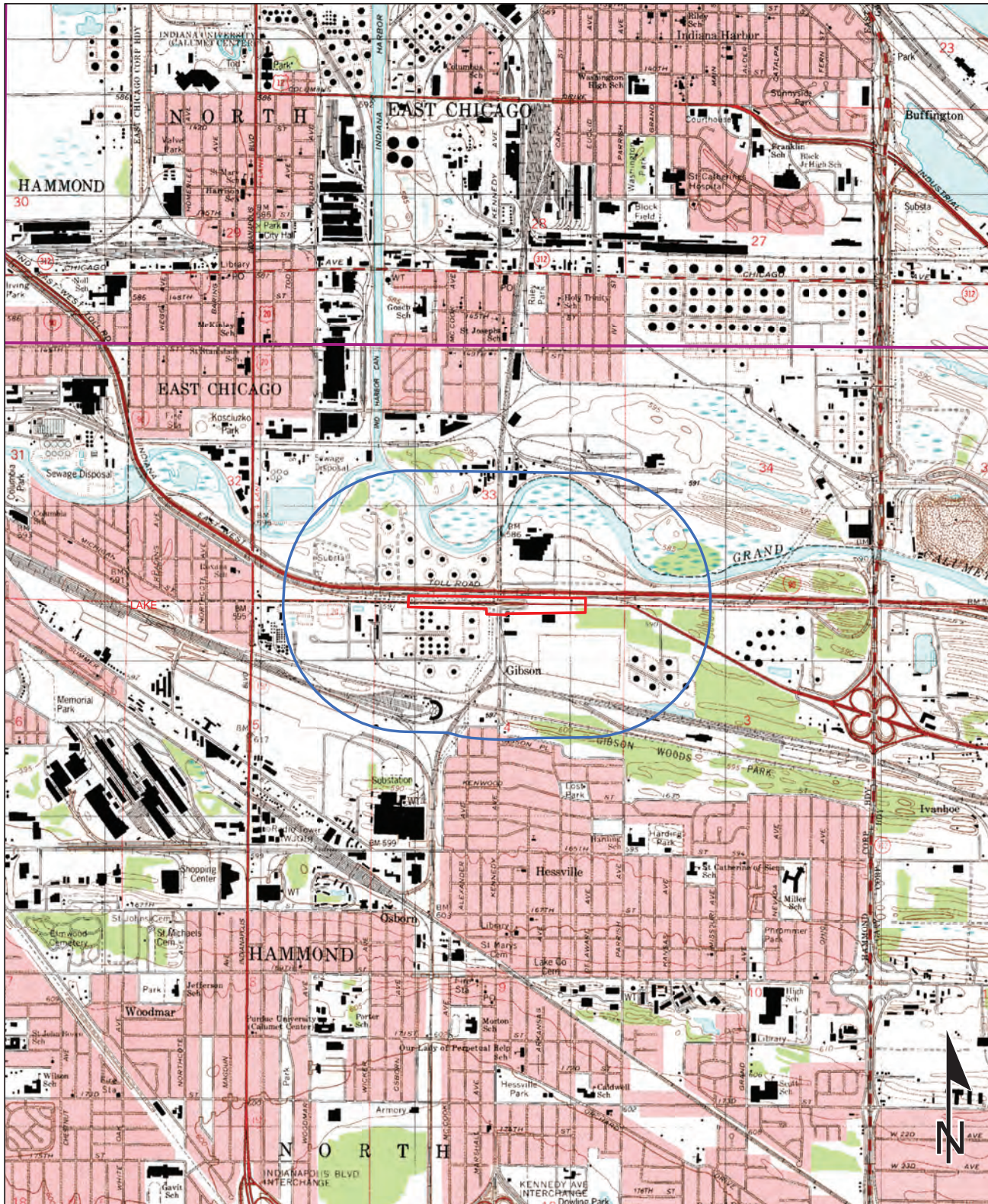
INFRASTRUCTURE: YES

WATER RESOURCES: YES

MINING/MINERAL EXPLORATION: YES

HAZARDOUS MATERIAL CONCERNS: YES

Red Flag Investigation - Site Location
US 20 over Kennedy Ave, 0.99 Mile E of SR 152
Des. No. 1900009, Bridge Project
Lake County, Indiana



Sources: 0.45 0.23 0 0.45 Miles
Non Orthophotography
Data - Obtained from the State of Indiana Geographical Information Office Library
Orthophotography - Obtained from Indiana Map Framework Data (www.indianamap.org)
Map Projection: UTM Zone 16 N **Map Datum:** NAD83
 This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.

HIGHLAND QUADRANGLE
INDIANA
7.5 MINUTE SERIES
(TOPOGRAPHIC)

Red Flag Investigation - Infrastructure

US 20 over Kennedy Ave, 0.99 Mile E of SR 152

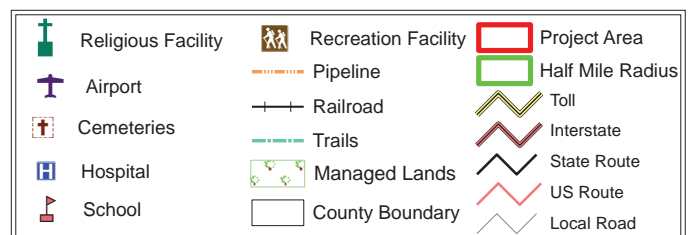
Des. No. 1900009, Bridge Project

Lake County, Indiana

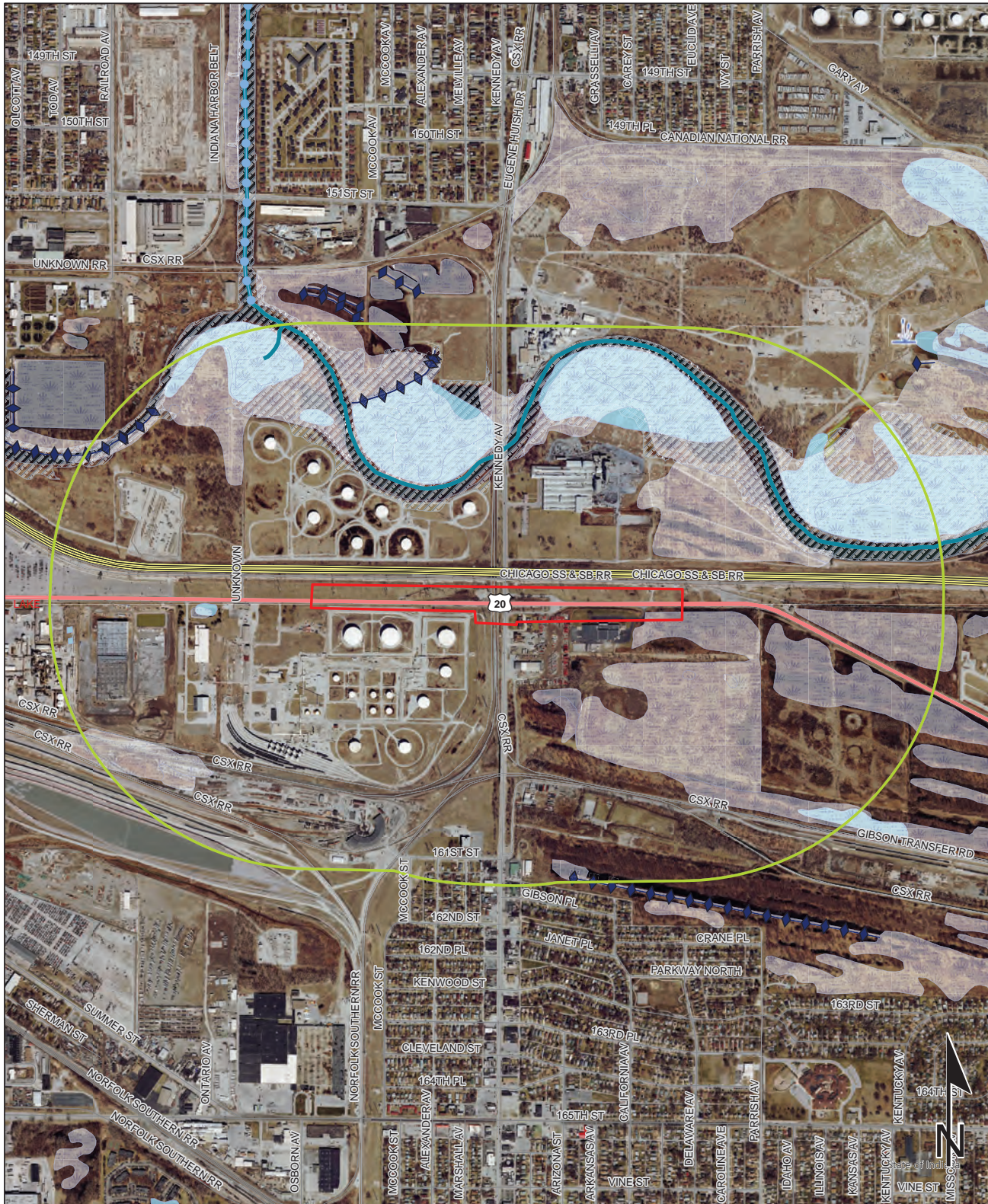


Sources:
Non Orthophotography
 Data - Obtained from the State of Indiana Geographical Information Office Library
Orthophotography - Obtained from Indiana Map Framework Data (www.indianamap.org)
Map Projection: UTM Zone 16 N **Map Datum:** NAD83

This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.

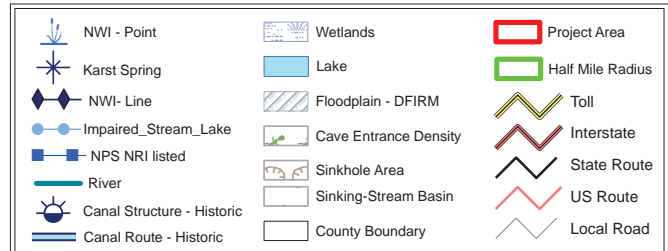


Red Flag Investigation - Water Resources US 20 over Kennedy Ave, 0.99 Mile E of SR 152 Des. No. 1900009, Bridge Project Lake County, Indiana



Sources:
Non Orthophotography
 Data - Obtained from the State of Indiana Geographical Information Office Library
Orthophotography - Obtained from Indiana Map Framework Data (www.indianamap.org)
Map Projection: UTM Zone 16 N **Map Datum:** NAD83

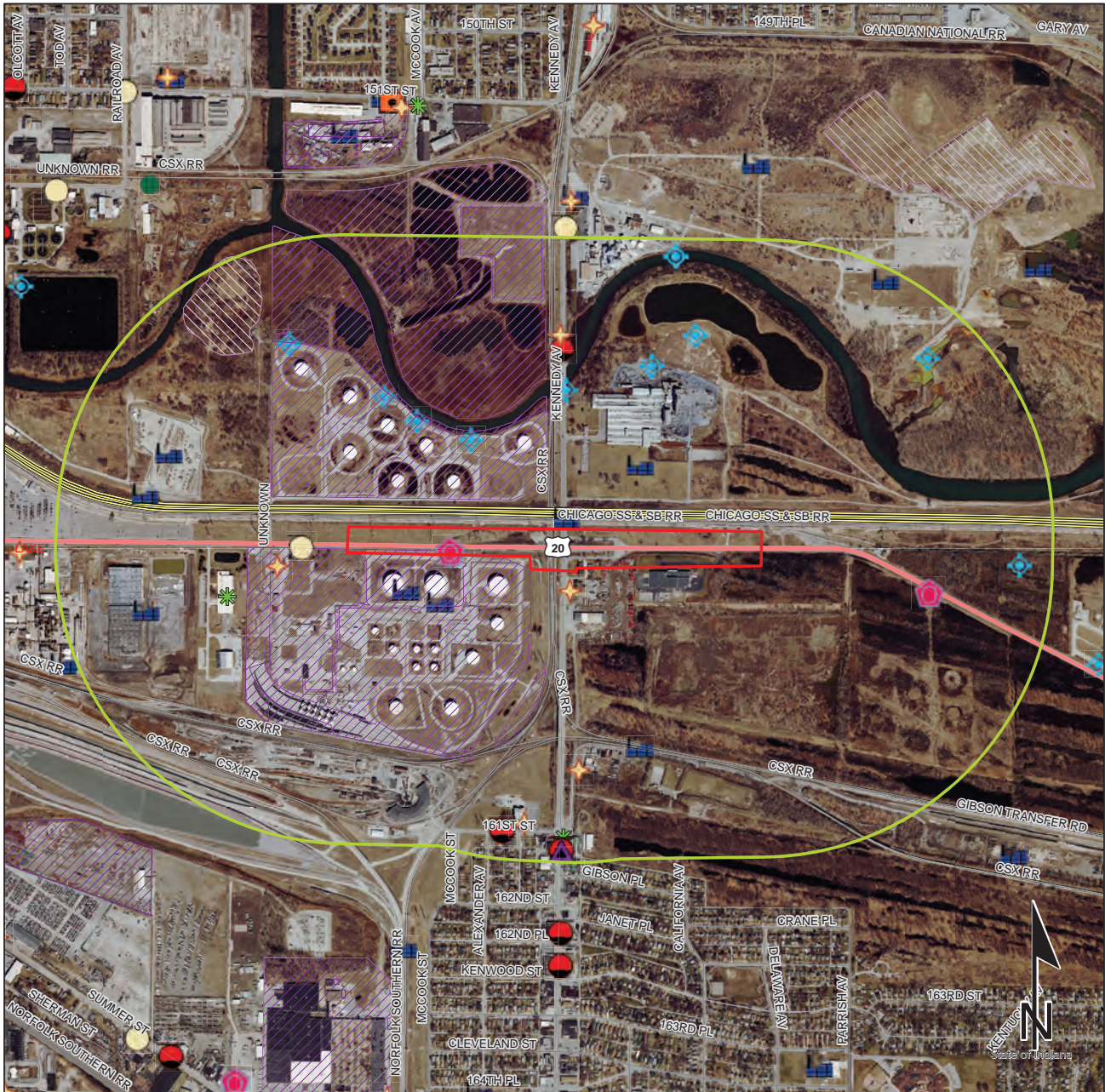
This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.

































Lake County, Indiana

 US Route 101

Lake County, Indiana



- | | | | | | |
|---|----------------------------------|---|-------------------------------|---|------------------------|
|  | Brownfield |  | RCRA Generator/TSD |  | Institutional Controls |
|  | RCRA Corrective Action Sites |  | Restricted Waste Site |  | County Boundary |
|  | Confined Feeding Operation |  | Septage Waste Site |  | Project Area |
|  | Notice Of Contamination |  | Solid Waste Landfill |  | Half Mile Radius |
|  | Construction/Demolition Site |  | State Cleanup Site |  | Toll |
|  | Infectious/Medical Waste Site |  | Superfund |  | Interstate |
|  | Leaking Underground Storage Tank |  | Tire Waste Site |  | State Route |
|  | Manufactured Gas Plant |  | Underground Storage Tank |  | US Route |
|  | NPDES Facilities |  | Voluntary Remediation Program |  | Local Road |
|  | NPDES Pipe Locations |  | Waste Transfer Station | | |
|  | Open Dump Waste Site | | | | |

0.2 0.1 0 0.2 Miles

This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.

Des. No. 1900009

Appendix E: Red Flag Investigation & Hazardous Materials

Sources:
Non Orthophotography
Data - Obtained from the State of Indiana Geographical Information Office Library
Photography - Obtained from Indiana Map Framework Data (www.indianamap.org)
Map Projection: UTM Zone 16 N **Map Datum:** NAD83

Categorical Exclusion

Appendix F

Water Resources



Approved 8/12/2022

US 20 over Kennedy Ave. and IHB RR
Bridge Replacement Project (Des. No. 1900009)
Lake County, Indiana
Waters of the U.S. Determination

Waters of the U.S. Determination
US 20 over Kennedy Avenue and IHB Railroad, Bridge Project
Lake County, Indiana
Des. No. 1900009

Date of Field Reconnaissance:

October 19, 2021

Report Completion Date:

August 10, 2022

Location

The project is located along US 20, 0.99 mile east of SR 152 (Pages A1 to A3).

- Section 4, Township 36 North, Range 9 West; Section 33, Township 37 North, Range 9 West
- Highland United States Geological Survey (USGS) 1:24,000 Quadrangle
- North Township, Lake County, Indiana
- Latitude: 41.6100894, Longitude: -87.4616040

Project Description

The proposed project involves replacement of the existing bridge, Bridge No. 020-45-02484 B, carrying US 20, also known as Michigan Street, over Kennedy Avenue and Indiana Harbor Belt (IHB) Railroad (RR). The existing bridge is a 3-span concrete beam structure with length, clear roadway width, and out-to-out width of 193 feet, 52 feet, and 55.3 feet, respectively. The replacement bridge will be a two-span structure with mechanically stabilized earth (MSE) wall abutments, a new reinforced concrete pier, an out-to-out length of approximately 260 feet, and a clear roadway width of 40 feet. A 4-foot-wide curbed center median will be installed between the eastbound and westbound traffic lanes along US 20. A multi-use path separated from vehicular traffic with a pedestrian traffic barrier will be added to the north side of the structure. It is anticipated that a raise in the profile grade of the bridge and roadway will be necessary. It is anticipated that realignments and grade adjustments for the US 20 exit ramps will be necessary.

Six wetlands, Wetlands A, B, C, D, E, and F, were identified within the investigation area. Surrounding landscape consists of mowed and maintained roadside vegetation, wooded areas, wetlands, businesses, and IHB RR. The investigation area is not located within a mapped floodplain.

Soils

According to the 2020 USDA Soil Survey Geographic (SSURGO) Database for Lake County, the investigation area contains soil areas considered to be partially hydric (Page A4).

Soil Summary Table

Soil Name	Map Abbreviation	Hydric Range
Oakville-Adrian complex, 0 to 6 percent slopes	OkB	Partially Hydric (33-65%)
Urban Land	Ur	Nonhydric (0%)



National Wetland Inventory Information

One National Wetland Inventory (NWI) wetland was identified within the investigation area (Page A5). The U.S. Fish and Wildlife Indiana wetlands geodatabase (IN_geodatabase_wetlands.gdb) identified one palustrine, forested, broadleaf deciduous/emergent, persistent, semipermanently flooded (PFO1/EM1F) wetland associated with Wetland A. Wetland types are based on *Classification of Wetlands and Deepwater Habitats of the United States* (Cowardin et al. 1979).

12-digit HUC and Flood Zones

The US 20 over Kennedy and IHB RR bridge investigation area is within the 071200030406 (Headwaters Grand Calumet River) 12-digit HUC watershed (Page A2). The USGS StreamStats website (<https://streamstats.usgs.gov/ss/>) generated two unnamed watersheds within the investigation area. No blue-line features were present on the StreamStats interface within the investigation area. Unnamed Watershed A had a drainage area of 0.002 square miles, Unnamed Watershed B had a drainage area of 0.014 square miles (Page A6). The Indiana Floodplain Information Portal (<https://indnr.maps.arcgis.com/apps/webappviewer/index.html?id=05026dabc2e8461983e196d56a213c1e>) Best Available Flood Zones data indicated that the investigation area is not within a mapped floodway (Page A7).

Attached Documents

- | | |
|---|---|
| • Location Map | |
| • USGS Quad Map (1:24,000) | |
| • USGS Quad Map (1:12,000) | Removed to avoid duplication. See Appendix B. |
| • USDA SSURGO Soils Map | |
| • USFWS NWI Map | |
| • StreamStats Watershed Map | |
| • Best Available Flood Hazard Map | |
| • Water Resources Map | |
| • Photo Location Map and Project Photos | Removed to avoid duplication. See Appendix B. |
| • Wetland Determination Data Sheets | Removed for brevity. |
| • Preliminary Jurisdiction Determination Form | |

Field Reconnaissance

The Waters of the U.S. (WOTUS) investigation area limits were established based on the scope of work expected for the US 20 over Kennedy and IHB bridge project. Six wetlands, Wetlands A, B, C, D, E, and F, were identified during the field reconnaissance. Weather during the investigation was sunny, with last recorded precipitation in the area occurring three days prior at 0.15 inches, according to NOAA Online Weather Data (<https://www.weather.gov/wrh/Climate?wfo=lot>).

Wetland determinations were conducted in accordance with the *Corps of Engineers Wetland Delineation Manual* (U.S. Army Corps of Engineers 1987) and the *Regional Supplement to the Corps of Engineers Wetland Delineation Manual: Northcentral and Northeast Region Version 2.0* (U.S. Army Corps of Engineers 2010). Wetland data sheets from the U.S. Army Corps of Engineers website (https://www.usace.army.mil/Missions/Civil-Works/Regulatory-Program-and-Permits/reg_supp/) were used to make wetland determinations.



Water resource boundaries were mapped using a Trimble R1 receiver (sub-meter accuracy) and ArcCollector as the GIS data collection platform. For those features that displayed bed and bank, the ordinary high water mark (OHWM) width and depth was measured at the maximum dimension observed beyond the influence of bridge and culvert structures. OHWM measurements were also documented for any stream features observed in the field that were not included as blue-line or National Hydrography Dataset (NHD) features.

Stream Feature(s)

According to the USGS Highland 1:24,000 topographic quadrangle, no blue-line stream features are present within the investigation area (Pages A2 and A3). The NHD GIS dataset contained three classified local NHD Flowlines. One classified flowline was associated with Wetlands A, B, and C; this flowline progresses from Wetland A west underneath a private driveway via three parallel unnamed 36-inch by 24-inch corrugated metal pipe (CMP) culverts, travels through Wetland B, enters another unnamed 24-inch CMP culvert into Wetland C, and finally leads into an unnamed underground culvert west of Kennedy Avenue, meeting up with the second NHD line flowing north toward Grand Calumet River. The final classified flowline flows north and joins the first flowline within Wetland B (Page A8). The NHD GIS dataset also contained two unclassified local NHD flowlines. One unclassified flowline was associated with Wetland D and the other was associated with Wetland F, eventually leading north toward Grand Calumet River, however neither flowline exhibited identifiable features in the field (Page A8).

Stream features were not identified within the investigation area.

Wetlands

The October 19, 2021 field investigation identified six wetland features, Wetlands A, B, C, D, E, and F, within the investigation area (Pages A28 to A63).

Wetland A

Wetland A is a 0.99-acre emergent wetland situated along the south side of US 20 and east of Kennedy Avenue. Wetland A is connected to Wetland B via three 36-inch by 24-inch CMP culverts running underneath a private driveway. Wetland A is part of a large *Phragmites australis* and *Salix nigra* dominated wetland/pond complex present within and extending beyond the southeast investigation area limits. As defined by Cowardin et al. (1979), Wetland A would be classified as palustrine, forested, broad-leaf deciduous (PFO1)/ emergent, persistent, semi-permanently flooded (EM1F). Wetland A is connected to underground and surface drainage systems flowing north to Grand Calumet River. Grand Calumet River is a traditional navigable waterway (TNW) within Lake County from the Illinois border to Marquette Park in Gary. As a result, Wetland A possesses a direct hydrologic connection to a TNW and thus would fall under Clean Water Act jurisdiction. Based on a qualitative assessment of Wetland A, this wetland is of low quality due to its large size and part of a greater wetland/pond complex but prevalence of invasive species.

Data Point 1

This data point represents wetland conditions within Wetland A, an area south of US 20 and east of Kennedy Avenue. There are no sapling/shrub or woody vine strata identified within the plot area. The dominant and only species within the tree stratum was black willow (*Salix nigra*, OBL). The dominant and only species within the herb stratum consisted of common reed (*Phragmites australis*, FACW). The plant community passes the rapid test for hydrophytic vegetation; therefore, hydrophytic vegetation is present.

Primary indicators of hydrology included surface water (A1) at a depth of 6 inches, high water table (A2) at a depth of 9 inches, saturation (A3) at the surface, and oxidized rhizospheres on living roots (C3). Secondary indicators of hydrology included crayfish burrows (C8) and FAC-Neutral test (D5). Therefore, wetland hydrology is present. The 2020 Indiana SSURGO database for Lake County indicates that this data point is located in Oakville-Adrian complex, 0 to 6 percent slopes. The Oakville-Adrian series is considered to be partially hydric soil. The soil profile was excavated from a pit to a depth of 21 inches. The soil profile consisted of three layers. Layer one was a 10YR 2/1 (100%) sandy soil to a depth of 2 inches. Layer two was a 10YR 4/1 (80%) sandy soil with 10YR 5/8 (20%) redox concentrations within the matrix and pore linings to a depth of 9 inches. Layer three was a 10YR 4/1 (100%) sandy layer to a depth of 21 inches. The soil examined at this location meets the Sandy Redox (S5) hydric soil indicator; therefore, hydric soil is present. This datapoint meets the requirements for hydrophytic vegetation, hydrology, and hydric soils; therefore, this data point is within a wetland (Pages A28 to A31).

Data Point 2

This data point represents non-wetland conditions for Wetland A, an area south of US 20 and east of Kennedy Avenue. There are no tree, shrub/sapling, or woody vine strata identified within the plot area. The dominant species within the herb stratum was red fescue (*Festuca rubra*, FACU) and common plantain (*Plantago major*, FACU). Hydrophytic vegetation was not present since 50% of the dominant species are FAC or drier. No indicators of hydrology were present; therefore, wetland hydrology is not present. The 2020 Indiana SSURGO database for Lake County indicates that this data point is located in Oakville-Adrian complex, 0 to 6 percent slopes. The Oakville-Adrian series is considered to be partially hydric soil. The soil profile was excavated from a pit to a depth of 19 inches and consisted of two layers. Layer one consisted of a 10YR 2/2 (95%) sandy soil with 10YR 3/6 (5%) redox concentrations in the matrix to a depth of 13 inches. Layer two consisted of a 10YR 5/4 (60%) and 10YR 5/6 (40%) sandy soil to a depth of 19 inches. The soil profile meets the sandy redox (S5) hydric soil indicator; therefore, hydric soil is present. Only one of the three required wetland criteria were present; therefore, this data point is not within a wetland (Pages A32 to A35).

Wetland B

Wetland B is an 0.40-acre emergent wetland located along the south side of US 20 within an excavated concave drainage area. Wetland B serves as a roadside drainage area and is partially mowed. Wetland B is connected to Wetland A to the east via three CMP culverts, and is connected to Wetland C to the west via a single 24-inch CMP culvert. As defined by Cowardin et al. (1979), Wetland B would be classified as palustrine emergent, persistent, seasonally flooded (EM1C). Wetland B is connected to underground and surface drainage systems flowing north to Grand Calumet River. Grand Calumet River is a traditional navigable waterway (TNW) within Lake County from the Illinois border to Marquette Park in Gary. As a result, Wetland B possesses a direct hydrologic connection to a TNW and thus would fall under Clean Water Act jurisdiction. Based on a qualitative assessment of Wetland B, this wetland is of low quality due to its prevalence of invasive species.

Data Points were not taken within Wetland B. Based on field observations, dominant species within Wetland B are herbaceous and include common reed (*Phragmites australis*) and narrow-leaf cat-tail (*Typha angustifolia*). Soils are disturbed and fall within two soil units, including the Oakville-Adrian complex, 0 to 6 percent slopes (OkB), a partially hydric soil, and Urban Land (Ur), which is nonhydric. Hydrology included surface water at approximately 1 inch in depth. The boundary of Wetland B is formed

both by the elevation change from the concave drainage depression and a change in vegetation from hydrophytes to upland grasses. Despite lack of a data point, field observations including prevalence of wetland vegetation, hydrology, and soils resembling adjacent confirmed wetlands, indicate that this area should be considered a wetland.

Wetland C

Wetland C is an 0.01-acre emergent wetland located south of the southern US 20 on/off ramp. Wetland C is a sparsely vegetated concave area subject to ponding with a hydrophyte-dominated fringe and shaded by trees located outside of the wetland. Wetland C is located between a culvert outlet leading from Wetland B to the east and a CMP culvert inlet to the west which empties outside the investigation area, presumably following the classified NHD flowline north to Grand Calumet River. Grand Calumet River is a traditional navigable waterway (TNW) within Lake County from the Illinois border to Marquette Park in Gary. As a result, Wetland C possesses a direct hydrologic connection to a TNW and thus would fall under Clean Water Act jurisdiction. Based on a qualitative assessment of Wetland C, this wetland is of low quality due its prevalence of invasive species.

Data Point 3

This data point represents wetland conditions for Wetland C, an area south of US 20, east of Kennedy Avenue, and south of the southern US 20 on-ramp. There are no tree, shrub/sapling, or woody vine strata identified within the plot area. The dominant species within the herb stratum was jewelweed (*Impatiens capensis*, FACW) and Pennsylvania smartweed (*Persicaria pensylvanica*, FACW). The plant community passes the rapid test for hydrophytic vegetation; therefore, hydrophytic vegetation is present. Primary indicators of hydrology included Saturation (A3) at a depth of 9 inches and Sparsely Vegetated Concave Surface (B8). Secondary indicators of hydrology included Drainage Patterns (B10) and FAC-Neutral Test (D5). Therefore, wetland hydrology is present. The 2020 Indiana SSURGO database for Lake County indicates that this data point is located in an Urban Land soil, which is considered nonhydric. The soil profile was excavated from a pit to a depth of 20 inches and consisted of three layers. Layer one was a 10YR 2/1 (100%) sandy soil to a depth of 6 inches. Layer two was a 10YR 3/1 (60%) and 10YR 2/2 (30%) sandy soil with 10YR 4/6 (10%) redox concentrations within the matrix to a depth of 10 inches. Layer three was a 10YR 2.5/1 (100%) sandy soil to a depth of 20 inches. The soil examined at this location meets the Dark Surface (S7) and Thin Dark Surface (S9) hydric soil indicators; therefore, hydric soil is present. This datapoint meets the requirements for hydrophytic vegetation, hydrology, and hydric soils; therefore, this data point is within a wetland (Pages A36 to A39).

An upland data point was not taken for Wetland C.

Wetland D

Wetland D is a 0.28-acre emergent wetland situated on the north side of US 20, east of Kennedy Avenue, and west of a field entrance drive. As defined by Cowardin et al. (1979), this wetland would be classified as palustrine emergent, persistent, seasonally flooded/saturated (PEM1E). No features draining Wetland D were field identified. However, the NHD dataset shows an unclassified drainage flowline originates at Wetland D, converges with other NHD lines, and outlets to Grand Calumet River. Wetland D occurs approximately 1,640 feet southwest of the Grand Calumet River, a TNW, and is separated from Grand Calumet River via roads, embankments, and other manmade structures. Despite presence of barriers, due to an ephemeral connection and proximity to Grand Calumet River, Wetland D would contribute to the

significant nexus of a TNW. Thus, Wetland D would fall under Clean Water Act jurisdiction. Based on a qualitative assessment of Wetland D, this wetland is of low quality due to disturbance via mowing and relatively low floristic quality.

Data Point 4

This data point represents wetland conditions within Wetland D, an area north of US 20, east of Kennedy Avenue, and west of the field entrance drive. There are no tree, sapling/shrub, or woody vine strata identified within the plot area. The dominant species within the herb stratum consisted of shining flat sedge (*Cyperus bipartitus*, FACW). The plant community passes the rapid test for hydrophytic vegetation; therefore, hydrophytic vegetation is present. One primary indicator of hydrology, Oxidized Rhizospheres on Living Roots (C3), was present. Secondary indicators of hydrology included Saturation Visible on Aerial Imagery (C9), Geomorphic Position (D2), and FAC-Neutral Test (D5). Therefore, wetland hydrology is present. The 2020 Indiana SSURGO database for Lake County indicates that this data point is located in Oakville-Adrian complex, 0 to 6 percent slopes. The Oakville-Adrian series is considered to be partially hydric soil. The soil profile was excavated from a pit to a depth of 21 inches and consisted of four layers. Layer one was a 10YR 2/2 (100%) loamy/clayey layer to a depth of 3 inches. Layer two was a 2.5Y 3/1 (97%) sandy soil with 10YR 3/6 (3%) redox concentrations to a depth of 11 inches within the matrix and pore linings. Layer three was a 10YR 5/2 (98%) sandy soil with 10YR 4/6 (2%) redox concentrations within the matrix to a depth of 14 inches. Layer four was a 10YR 3/1 (70%) and 10YR 2/1 (30%) sandy soil to a depth of 21 inches. The soil examined at this location meets the Sandy Redox (S5) hydric soil indicator; therefore, hydric soil is present. This datapoint meets the requirements for hydrophytic vegetation, hydrology, and hydric soils; therefore, this data point is within a wetland (Pages A40 to A43).

Data Point 5

This data point represents non-wetland conditions for Wetland D, an area north of US 20, east of Kennedy Avenue, and west of the field entrance drive. There are no tree, shrub/sapling, or woody vine strata identified within the plot area. The dominant species within the herb stratum was red fescue (*Festuca rubra*, FACU). Hydrophytic vegetation is not present since 50% of the dominant species are FAC or drier. No indicators of hydrology were present; therefore, wetland hydrology is not present. The 2020 Indiana SSURGO database for Lake County indicates that this data point is located in Oakville-Adrian complex, 0 to 6 percent slopes. The Oakville-Adrian series is considered to be partially hydric soil. The soil profile was excavated from a pit to a depth of 18 inches and consisted of four layers. Layer one consisted of a 10YR 2/2 (98%) sandy soil with 10YR 3/6 (2%) redox concentrations in the matrix to a depth of 5 inches. Layer two consisted of a 10YR 4/4 (95%) sandy soil with 10YR 3/6 (5%) redox concentrations within the matrix to a depth of 9 inches. Layer three consisted of a 10YR 4/3 (65%) and 10YR 2/1 (30%) soil with 10YR 3/6 (5%) redox concentrations within the matrix to a depth of 13 inches. Layer four consisted of a 10YR 4/6 (100%) sandy soil to a depth of 18 inches. The soil profile meets the sandy redox (S5) hydric soil indicator; therefore, hydric soil is present. Only one of the three required wetland criteria were present; therefore, this data point is not within a wetland (Pages A44 to A47).

Wetland E

Wetland E is a 0.15-acre emergent wetland situated on the north side of US 20, east of Kennedy Avenue, and east of the field entrance drive, and continues east outside the investigation area. As defined by Cowardin et al. (1979), this wetland would be classified as palustrine emergent, persistent, seasonally flooded/saturated (PEM1E). No features directly draining Wetland E were field identified within the

investigation area. However, Wetland E drains to Grand Calumet River outside the investigation area. Wetland E occurs approximately 1,270 feet southwest of the Grand Calumet River, a TNW, and is separated from Grand Calumet River via roads, embankments, and other manmade structures. Despite presence of barriers, due to an ephemeral connection and proximity to Grand Calumet River, Wetland E would contribute to the significant nexus of a TNW. Thus, Wetland E would fall under Clean Water Act jurisdiction. Based on a qualitative assessment of Wetland E, this wetland is of low quality due to severe disturbance via mowing and relatively low floristic quality.

Data Point 6

This data point represents wetland conditions within Wetland E, an area north of US 20, east of Kennedy Avenue, and east of the field entrance drive. There are no tree, sapling/shrub, or woody vine strata identified within the plot area. The dominant species within the herb stratum consisted of path rush (*Juncus tenuis*, FAC), red fescue (*Festuca rubra*, FACU), and shining flat sedge (*Cyperus biparitus*, FACW). The plant community passes the dominance test for hydrophytic vegetation; therefore, hydrophytic vegetation is present. One primary indicator of hydrology, Oxidized Rhizospheres on Living Roots (C3), was present. Secondary indicators of hydrology included Saturation Visible on Aerial Imagery (C9) and Geomorphic Position (D2). Therefore, wetland hydrology is present. The 2020 Indiana SSURGO database for Lake County indicates that this data point is located in Oakville-Adrian complex, 0 to 6 percent slopes. The Oakville-Adrian series is considered to be partially hydric soil. The soil profile was excavated from a pit to a depth of 11 inches. A restrictive layer was present at a depth of 11 inches consisting of large (first sized and greater) chunks of industrial slag. The soil profile consisted of one layer. Layer one was a 10YR 3/1 (50%) and 10YR 2/2 (45%) sandy layer with 10YR 3/6 (5%) redox concentrations within the matrix and pore linings to a depth of 11 inches. The soil examined at this location meets the Sandy Redox (S5) hydric soil indicator; therefore, hydric soil is present. This datapoint meets the requirements for hydrophytic vegetation, hydrology, and hydric soils; therefore, this data point is within a wetland (Pages A48 to A51).

Data Point 7

This data point represents non-wetland conditions for Wetland E, an area north of US 20, east of Kennedy Avenue, and east of the field entrance drive. There are no tree, shrub/sapling, or woody vine strata identified within the plot area. The dominant species within the herb stratum was red fescue (*Festuca rubra*, FACU). Hydrophytic vegetation is not present since 50% of the dominant species are FAC or drier. No indicators of hydrology were present; therefore, wetland hydrology is not present. The 2020 Indiana SSURGO database for Lake County indicates that this data point is located in Oakville-Adrian complex, 0 to 6 percent slopes. The Oakville-Adrian series is considered to be partially hydric soil. The soil profile was excavated from a pit to a depth of 18 inches and consisted of four layers. Layer one consisted of a 10YR 2/2 (100%) sandy soil to a depth of 4 inches. Layer two consisted of a 2.5Y 3/2 (95%) sandy soil with 10YR 4/6 (5%) redox concentrations within the matrix to a depth of 10 inches. Layer three consisted of a 10YR 6/2 (50%) and 10YR 3/1 (40%) soil with 10YR 4/6 (10%) redox concentrations within the matrix to a depth of 12 inches. Layer four consisted of a 10YR 3/2 (95%) sandy soil with 10YR 3/6 (5%) redox concentrations within the matrix to a depth of 18 inches. The soil profile meets the sandy redox (S5) hydric soil indicator; therefore, hydric soil is present. Only one of the three required wetland criteria were present; therefore, this data point is not within a wetland (Pages A52 to A55).

Wetland F

Wetland F is a 0.27-acre emergent wetland situated on the north side of US 20, west of Kennedy Avenue. As defined by Cowardin et al. (1979), this wetland would be classified as palustrine emergent, persistent, seasonally flooded/saturated (PEM1E). No features directly draining Wetland F were field identified within the investigation area. However, the NHD dataset shows an unclassified drainage flowline originates at Wetland F, converges with other NHD lines, and outlets to Grand Calumet River. Wetland F occurs approximately 950 feet south of the Grand Calumet River, a TNW, and is separated from Grand Calumet River via roads, embankments, and other manmade structures. Despite presence of barriers and lack of a relatively permanent surface connection to Grand Calumet River, due to an ephemeral connection and proximity to Grand Calumet River, Wetland F would contribute to the significant nexus of a TNW. Thus, Wetland F would fall under Clean Water Act jurisdiction. Based on a qualitative assessment of Wetland F, this wetland is of low quality due to severe disturbance via mowing and relatively low floristic quality.

Data Point 8

This data point represents wetland conditions within Wetland F, an area north of US 20 and west of Kennedy Avenue. There are no tree, sapling/shrub or woody vine strata identified within the plot area. The dominant species within the herb stratum consisted of red fescue (*Festuca rubra*, FACU) and shining flat sedge (*Cyperus biparitus*, FACW). The plant community does not pass the dominance test for hydrophytic vegetation but contains a Prevalence Index below 3.0 (2.71); therefore, hydrophytic vegetation is present. Primary indicators of hydrology included Saturation (A3) at a depth of 9 inches and Oxidized Rhizospheres on Living Roots (C3). Secondary indicators of hydrology included Geomorphic Position (D2) and Saturation Visible on Aerial Imagery (C9). Therefore, wetland hydrology is present. The 2020 Indiana SSURGO database for Lake County indicates that this data point is located in Urban Land soils that are considered nonhydric. The soil profile was excavated from a pit to a depth of 19 inches and consisted of three layers. Layer one was a 10YR 4/2 (95%) sandy soil with 10YR 3/6 (5%) concentrations within the matrix and pore linings to a depth of 5 inches. Layer two consisted of a 10YR 4/1 (60%) and 10YR 2/1 (38%) sandy soil with 10YR 3/6 (2%) redox concentration within the matrix and pore linings to a depth of 14 inches. Layer three consisted of a 10YR 3/1 (98%) and 10YR 2/1 (2%) sandy layer to a depth of 19 inches. The soil examined at this location meets the Sandy Redox (S5) hydric soil indicator; therefore, hydric soil is present. This datapoint meets the requirements for hydrophytic vegetation, hydrology, and hydric soils; therefore, this data point is within a wetland (Pages A56 to A59).

Data Point 9

This data point represents non-wetland conditions for Wetland F, an area north of US 20 and west of Kennedy Avenue. There are no tree, sapling/shrub, or woody vine strata identified within the plot area. The dominant species within the herb stratum was red fescue (*Festuca rubra*, FACU). Hydrophytic vegetation is not present since 50% of the dominant species are FAC or drier. No indicators of hydrology were present; therefore, wetland hydrology is not present. The 2020 Indiana SSURGO database for Lake County indicates that this data point is located in Urban Land soils that are considered nonhydric. The soil profile was excavated from a pit to a depth of 18 inches and consisted of two layers. Layer one consisted of a 10YR 4/3 (97%) and 10YR 3/4 (3%) sandy soil to a depth of 5 inches. Layer two consisted of a 10YR 5/3 (50%), 10YR 5/1 (30%), and 10YR 3/6 (20%) sandy soil to a depth of 18 inches. The soil profile does not meet any hydric soil indicator; therefore, hydric soil is not present. None of the three required wetland criteria were present; therefore, this data point is not within a wetland (Pages A60 to A63).

Data Point Summary Table

Data Point	Vegetation	Soil	Hydrology	Wetland
DP 1	Yes	Yes	Yes	Yes
DP 2	No	Yes	No	No
DP 3	Yes	Yes	Yes	Yes
DP 4	Yes	Yes	Yes	Yes
DP 5	No	Yes	No	No
DP 6	Yes	Yes	Yes	Yes
DP 7	No	Yes	No	No
DP 8	Yes	Yes	Yes	Yes
DP 9	No	No	No	No

Wetland Summary Table

Wetland Name	Photos	Lat/Long	Type	Total Area (acres)	Quality	Likely Waters of U.S?
Wetland A	46-48, 51-57, 59	41.6098274 -87.4558046	PFO1/ EM1F	0.99	Low	Yes
Wetland B	25, 27-35	41.6097802 -87.4591270	PEM1E	0.40	Low	Yes
Wetland C	19-24	41.6096522 -87.4603194	PEM1E	0.01	Low	Yes
Wetland D	36-41, 44	41.6102608 -87.4569263	PEM1E	0.28	Low	Yes
Wetland E	58, 62-66	41.6100638 -87.4550342	PEM1E	0.15	Low	Yes
Wetland F	6-7, 10-13	41.6102759 -87.4630722	PEM1E	0.27	Low	Yes

Roadside Ditch Features

No roadside ditch features were identified within the investigation area.

Open Water Features

No open water features were identified within the investigation area.

Conclusion

The October 19, 2021 field review for the US 20 over Kennedy Avenue and IHB RR bridge project identified six wetlands, Wetlands A, B, C, D, E, and F. Wetlands A, B, and C would be subject to USACE jurisdiction under Section 404 of the Clean Water Act due to their direct hydrologic connectivity to Grand Calumet River, a TNW. Wetlands D, E, and F would be subject to USACE jurisdiction under Section 404 of the Clean Water Act due to their relatively nonpermanent hydrologic connectivity to Grand Calumet River, a TNW, and their situation adjacent to a TNW.

Every effort should be taken to avoid and minimize impacts to stream and wetland features. If impacts are necessary, then mitigation may be required. The INDOT Environmental Services Division should be

contacted immediately if impacts will occur. The final determination of jurisdictional waters is ultimately made by the U.S. Army Corps of Engineers. This report is our best judgment based on the guidelines set forth by the Corps.

All drainage structures within the investigation area were examined on October 19, 2021 for the presence of bats and migratory birds and were found to show no direct or indirect signs of occupation.

Acknowledgement

This waters determination has been prepared based on the best available information, interpreted in the light of the investigator's training, experience, and professional judgement in conformance with the 1987 Corps of Engineers Wetlands Delineation Manual, the appropriate regional supplement, the USACE Jurisdictional Determination Form Instructional Guidebook, and other appropriate agency guidelines.

Carson J. Hoogewerf

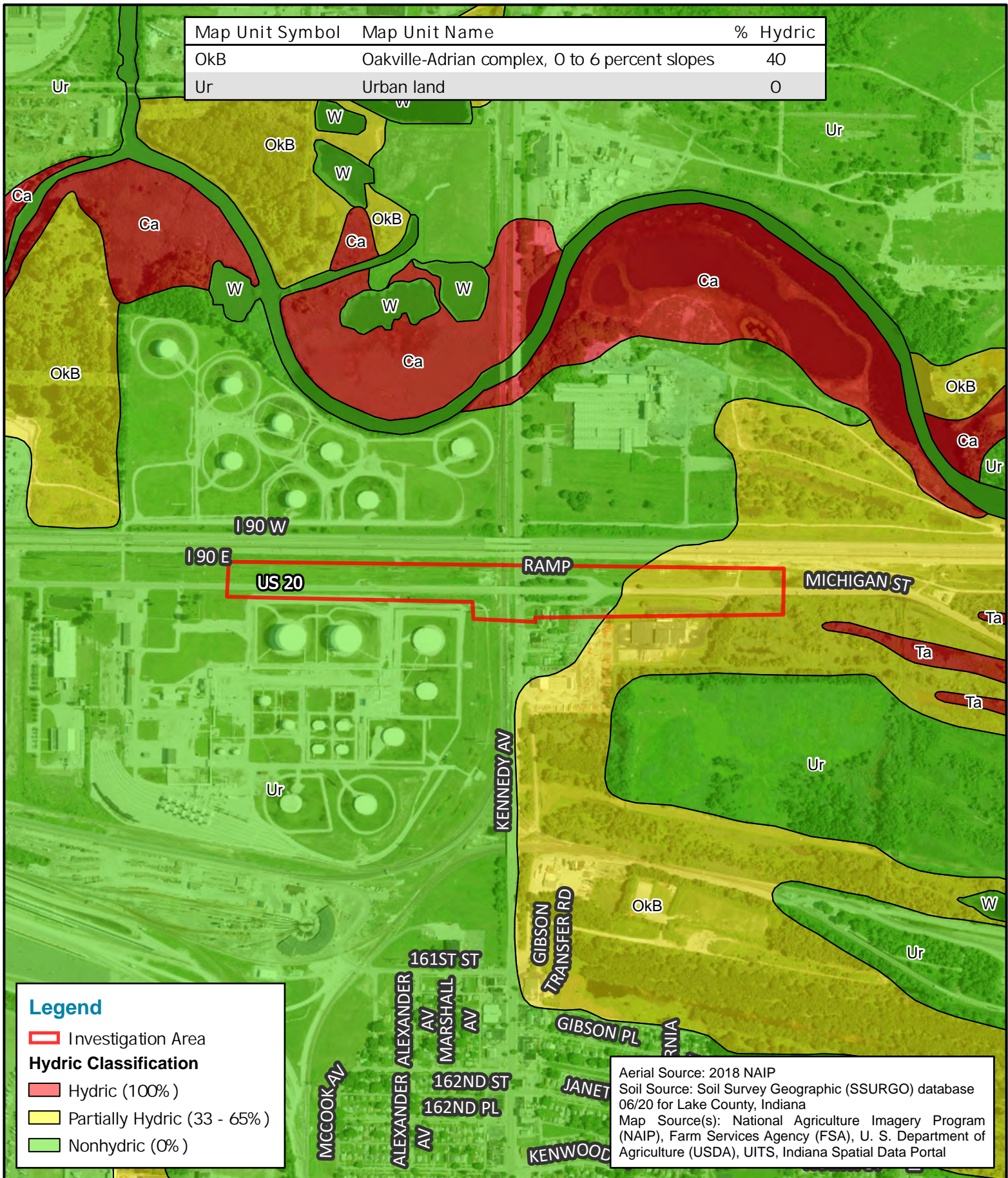


Environmental Specialist I
Lochmueller Group, Inc.

Preparers

Lochmueller Group, Inc. Staff	Position	Contributing Effort
Carson Hoogewerf	Environmental Specialist	Field Data Collection, Report Preparation
C.J. Cunningham	Environmental PM	Field Data Collection

Map Unit Symbol	Map Unit Name	% Hydric
OkB	Oakville-Adrian complex, 0 to 6 percent slopes	40
Ur	Urban land	0



Legend

 Investigation Area

Hydric Classification

- Hydric (100%)
- Partially Hydric (33 - 65%)
- Nonhydric (0%)

Aerial Source: 2018 NAIP
 Soil Source: Soil Survey Geographic (SSURGO) database 06/20 for Lake County, Indiana
 Map Source(s): National Agriculture Imagery Program (NAIP), Farm Services Agency (FSA), U. S. Department of Agriculture (USDA), UITS, Indiana Spatial Data Portal



112 West Jefferson Blvd, Suite 500
 South Bend, IN 46601
 Phone: (574) 334-5460

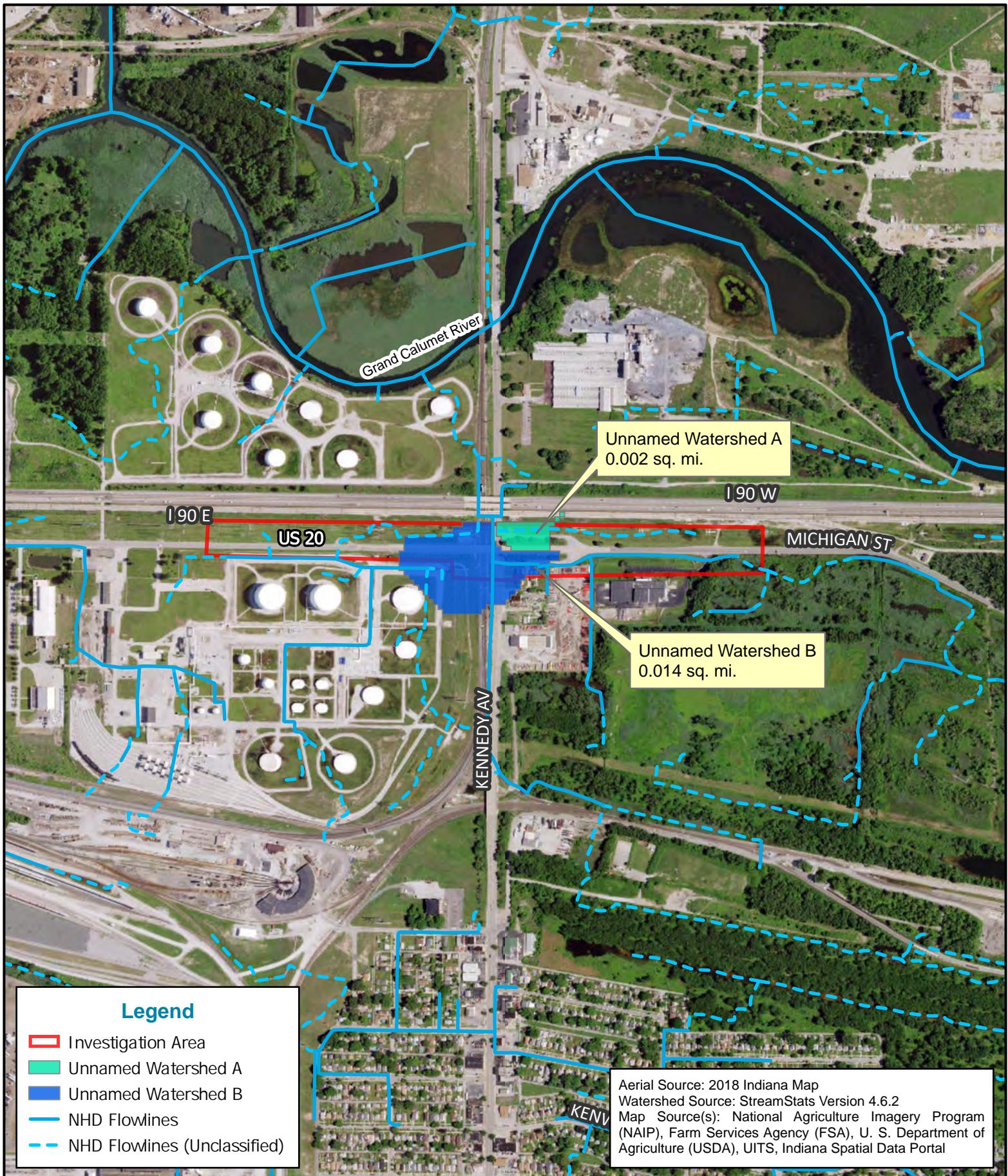
USDA SSURGO Soils Map

Des. No. 1900009
 Waters of the U.S. Report

0 500 1,000
 Feet

County: Lake
 Township: North
 State: Indiana

US 20 over Kennedy and IHB RR
 Bridge Replacement Project
 Created: 7/20/2022, CHoogewerf



112 West Jefferson Blvd, Suite 500
 South Bend, IN 46601
 Phone: (574) 334-5460

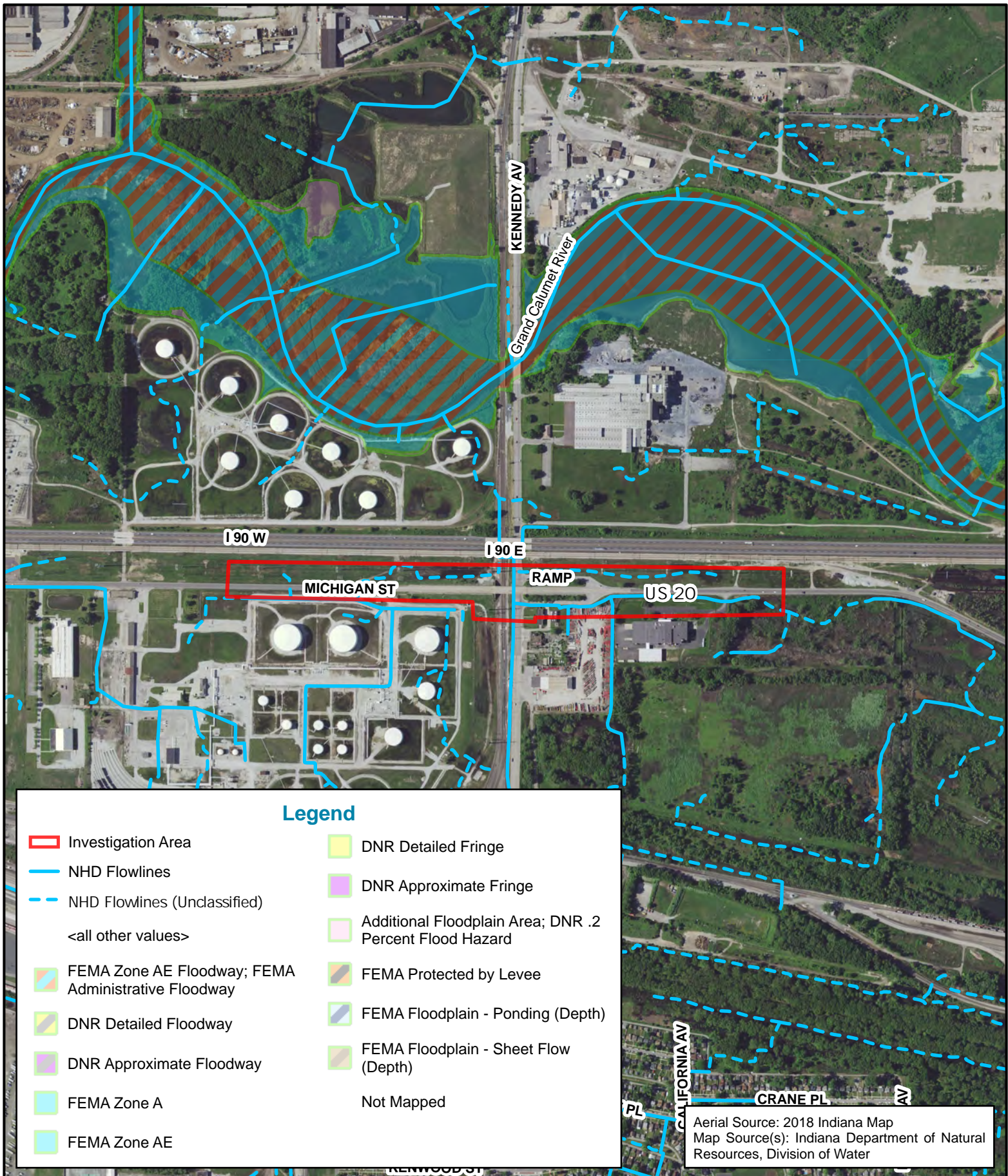
StreamStats Watershed Map

Des. No. 1900009
 Waters of the U.S. Report

0 500 1,000
 Feet

County: Lake
 Township: North
 State: Indiana

US 20 over Kennedy and IHB RR
 Bridge Replacement Project
 Created: 7/19/2022, CHoogewerf



112 West Jefferson Blvd, Suite 500
South Bend, IN 46601
Phone: (574) 334-5460

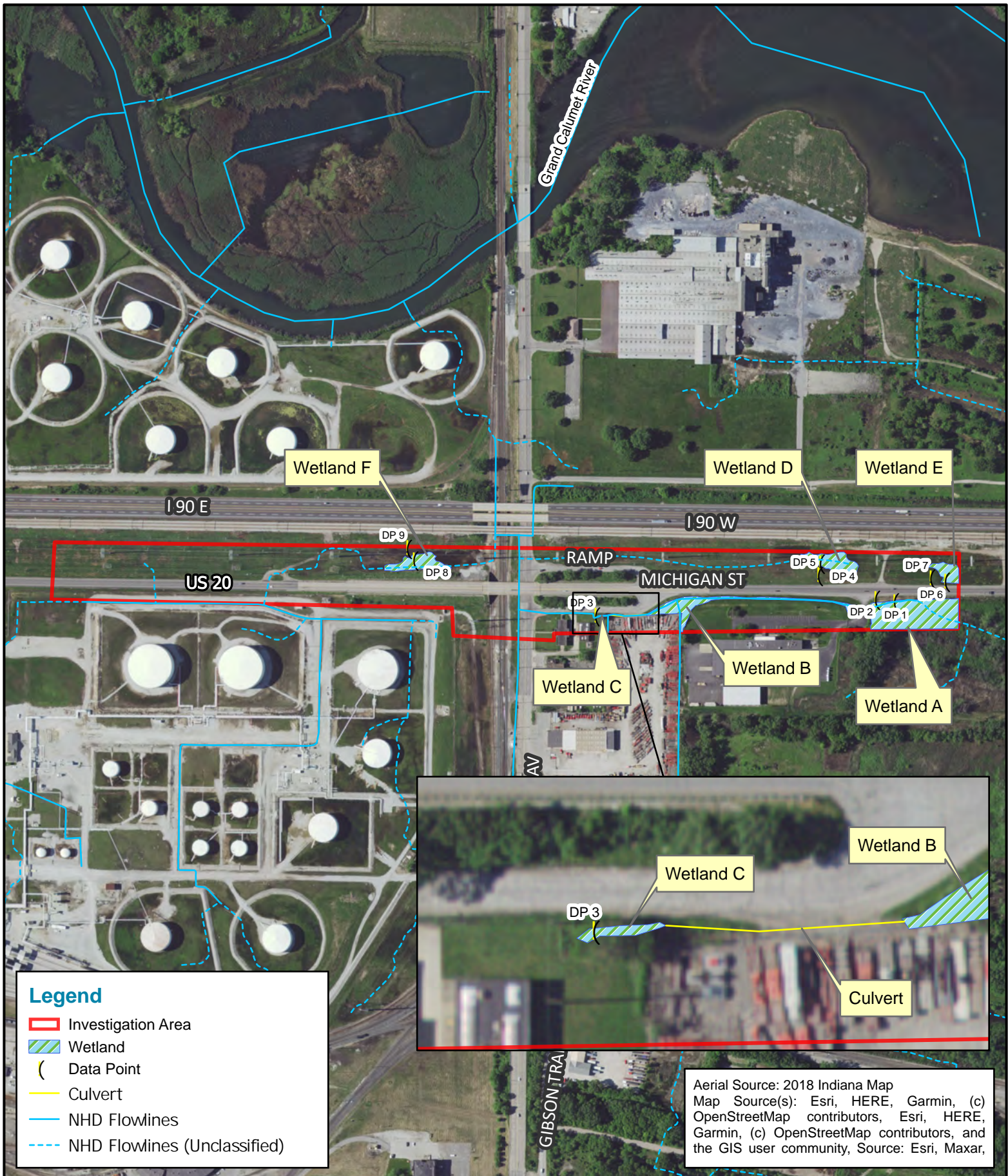
Best Available Flood Hazard Map

Des. No. 1900009
Waters of the U.S. Report

0 500 1,000
Feet

County: Lake
Township: North
State: Indiana

US 20 over Kennedy and IHB RR
Bridge Replacement Project
Created: 7/20/2022, CHoogewerf



Appendix 2 - PRELIMINARY JURISDICTIONAL DETERMINATION (PJD) FORM

BACKGROUND INFORMATION

A. REPORT COMPLETION DATE FOR PJD: August 10, 2022

B. NAME AND ADDRESS OF PERSON REQUESTING PJD: Carson Hoogewerf, 112 W. Jefferson Blvd, Suite 500, South Bend, In 46601

C. DISTRICT OFFICE, FILE NAME, AND NUMBER:

D. PROJECT LOCATION(S) AND BACKGROUND INFORMATION:

The proposed project (Des. No. 1900009) involves a replacement of the existing bridge carrying US 20 over Kennedy Avenue and Indiana Harbor Belt (IHB) Railroad. The replacement bridge will be a two-span structure with earthen wall abutments and a new reinforced concrete pier. A 4-foot-wide curbed center median will be installed between the eastbound and westbound US 20. A multi-use path with a pedestrian traffic barrier will be added to the north side of the structure. A raise in the profile grade of the bridge and roadway are likely. Realignment and grade adjustments for the US 20 exits ramps are likely.

Six wetlands, Wetlands A, B, C, D, E, and F, were identified within the investigation area.

(USE THE TABLE BELOW TO DOCUMENT MULTIPLE AQUATIC RESOURCES AND/OR AQUATIC RESOURCES AT DIFFERENT SITES)

State: Indiana County/parish/borough: Lake County City: Hammond

Center coordinates of site (lat/long in degree decimal format):

Lat.: 41.6100894 Long.: -87.4616040

Universal Transverse Mercator: 4606589 North, 461539 East, Zone 16N

Name of nearest waterbody: Grand Calumet River

E. REVIEW PERFORMED FOR SITE EVALUATION (CHECK ALL THAT APPLY):

☐ Office (Desk) Determination. Date:

☐ Field Determination. Date(s):

TABLE OF AQUATIC RESOURCES IN REVIEW AREA WHICH “MAY BE” SUBJECT TO REGULATORY JURISDICTION.

Site number	Latitude (decimal degrees)	Longitude (decimal degrees)	Estimated amount of aquatic resource in review area (acreage and linear feet, if applicable)	Type of aquatic resource (i.e., wetland vs. non-wetland waters)	Geographic authority to which the aquatic resource “may be” subject (i.e., Section 404 or Section 10/404)
Wetland A	41.6098274	-87.4558046	0.99 acre	wetland	Section 404
Wetland B	41.6097802	-87.4591270	0.40 acre	wetland	Section 404
Wetland C	41.6096522	-87.4603194	0.01 acre	wetland	Section 404
Wetland D	41.6102608	-87.4569263	0.28 acre	wetland	Section 404
Wetland E	41.6100638	-87.4550342	0.15 acre	wetland	Section 404
Wetland F	41.6102759	-87.4630722	0.27 acre	wetland	Section 404

- 1) The Corps of Engineers believes that there may be jurisdictional aquatic resources in the review area, and the requestor of this PJD is hereby advised of his or her option to request and obtain an approved JD (AJD) for that review area based on an informed decision after having discussed the various types of JDs and their characteristics and circumstances when they may be appropriate.
- 2) In any circumstance where a permit applicant obtains an individual permit, or a Nationwide General Permit (NWP) or other general permit verification requiring "pre-construction notification" (PCN), or requests verification for a non-reporting NWP or other general permit, and the permit applicant has not requested an AJD for the activity, the permit applicant is hereby made aware that: (1) the permit applicant has elected to seek a permit authorization based on a PJD, which does not make an official determination of jurisdictional aquatic resources; (2) the applicant has the option to request an AJD before accepting the terms and conditions of the permit authorization, and that basing a permit authorization on an AJD could possibly result in less compensatory mitigation being required or different special conditions; (3) the applicant has the right to request an individual permit rather than accepting the terms and conditions of the NWP or other general permit authorization; (4) the applicant can accept a permit authorization and thereby agree to comply with all the terms and conditions of that permit, including whatever mitigation requirements the Corps has determined to be necessary; (5) undertaking any activity in reliance upon the subject permit authorization without requesting an AJD constitutes the applicant's acceptance of the use of the PJD; (6) accepting a permit authorization (e.g., signing a proffered individual permit) or undertaking any activity in reliance on any form of Corps permit authorization based on a PJD constitutes agreement that all aquatic resources in the review area affected in any way by that activity will be treated as jurisdictional, and waives any challenge to such jurisdiction in any administrative or judicial compliance or enforcement action, or in any administrative appeal or in any Federal court; and (7) whether the applicant elects to use either an AJD or a PJD, the JD will be processed as soon as practicable. Further, an AJD, a proffered individual permit (and all terms and conditions contained therein), or individual permit denial can be administratively appealed pursuant to 33 C.F.R. Part 331. If, during an administrative appeal, it becomes appropriate to make an official determination whether geographic jurisdiction exists over aquatic resources in the review area, or to provide an official delineation of jurisdictional aquatic resources in the review area, the Corps will provide an AJD to accomplish that result, as soon as is practicable. This PJD finds that there "*may be*" waters of the U.S. and/or that there "*may be*" navigable waters of the U.S. on the subject review area, and identifies all aquatic features in the review area that could be affected by the proposed activity, based on the following information:

SUPPORTING DATA. Data reviewed for PJD (check all that apply)

Checked items should be included in subject file. Appropriately reference sources below where indicated for all checked items:

- ☒ Maps, plans, plots or plat submitted by or on behalf of the PJD requestor:
Map: Location, Topo, Soils, NWI, Floodplain, StreamStats, Water Resources.
- ☒ Data sheets prepared/submitted by or on behalf of the PJD requestor.
☐ Office concurs with data sheets/delineation report.
☐ Office does not concur with data sheets/delineation report. Rationale: _____.
- ☐ Data sheets prepared by the Corps: _____.
- ☐ Corps navigable waters' study: _____.
- ☒ U.S. Geological Survey Hydrologic Atlas: Hydrography_LocalRes_Flowline_Classified_NHD_IN; Water_Bodies_Flowlines_Unclassified_LocalRes.
☒ USGS NHD data.
☒ USGS 8 and 12 digit HUC maps.
- ☒ U.S. Geological Survey map(s). Cite scale & quad name: 1:24,000 Highland Quadrangle.
- ☒ Natural Resources Conservation Service Soil Survey. Citation: 2020 Indiana SSURGO Lake County.
- ☒ National wetlands inventory map(s). Cite name: USFWS Indiana Wetlands geodatabase (IN_geodatabase_wetlands.gdb).
- ☐ State/local wetland inventory map(s): _____.
- ☒ FEMA/FIRM maps: Indiana Floodplain Information Portal (<https://indnr.maps.arcgis.com/apps/webappviewer/index.html?id=05026dabc2e8461983e196d56a213c1e>) Best Available Flood Zones.
- ☒ 100-year Floodplain Elevation is: 585.0 Feet. (National Geodetic Vertical Datum of 1929)
- ☒ Photographs: ☒ Aerial (Name & Date): 2018 Indiana Map.
or ☒ Other (Name & Date): Field photos taken October 19, 2021.
- ☐ Previous determination(s). File no. and date of response letter: _____.
- ☐ Other information (please specify): _____.

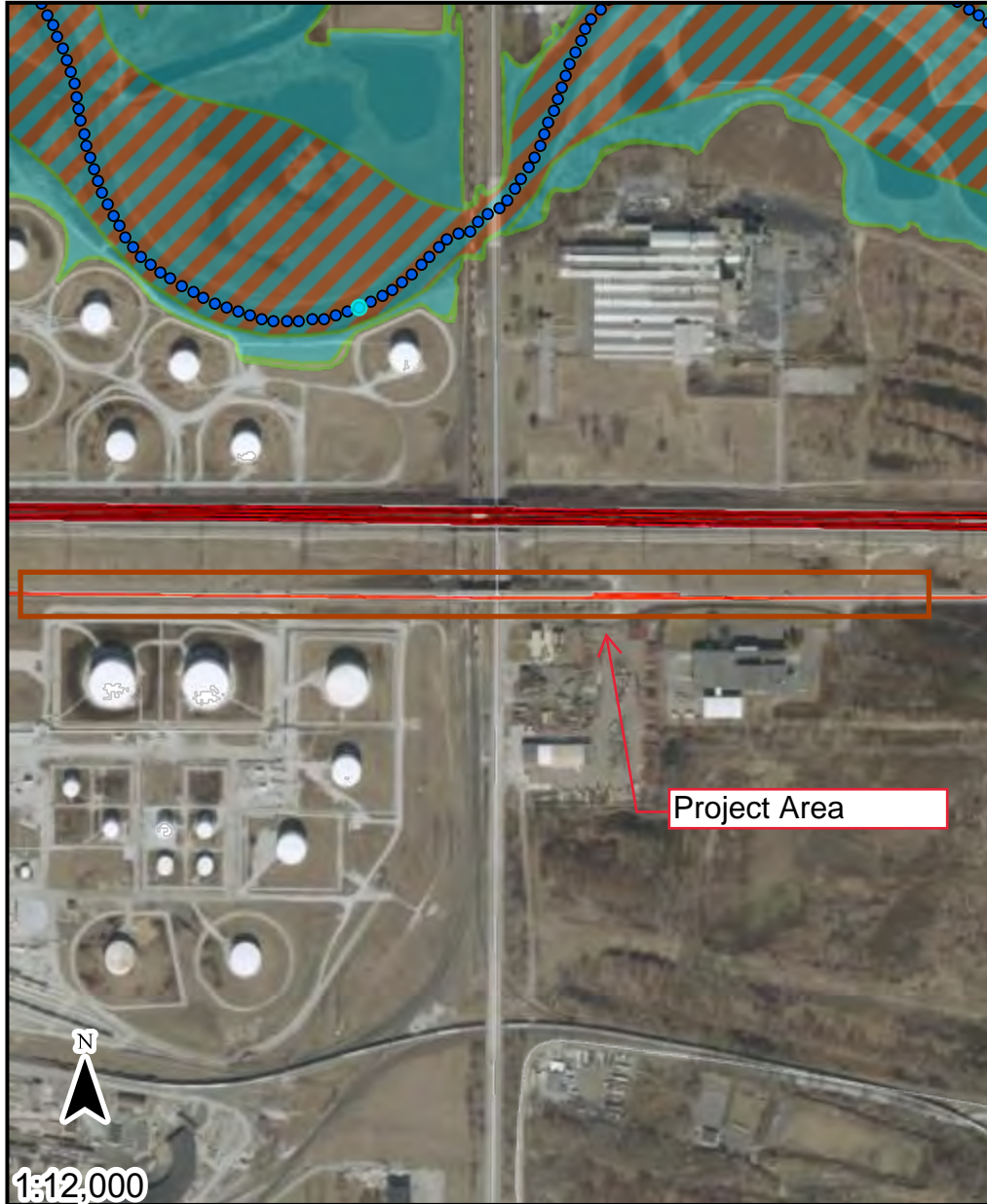
IMPORTANT NOTE: The information recorded on this form has not necessarily been verified by the Corps and should not be relied upon for later jurisdictional determinations.

Signature and date of
Regulatory staff member
completing PJD

Carson J Hoogewerf Digitally signed by Carson J Hoogewerf
Date: 2022.08.10 08:42:59 -04'00'

Signature and date of
person requesting PJD
(REQUIRED, unless obtaining
the signature is impracticable)¹

¹ Districts may establish timeframes for requestor to return signed PJD forms. If the requestor does not respond within the established time frame, the district may presume concurrence and no additional follow up is necessary prior to finalizing an action.



● Point of Interest

● Base Flood Elevation Point

VERSION

● 1.0

FLD_ZONE, SOURCE_DNR,
ZONE_SUBTY

■ FEMA Zone AE Floodway; FEMA
Administrative Floodway

■ FEMA Zone AE

■ Additional Floodplain Area; DNR
.2 Percent Flood Hazard

Not Mapped

Long: -87.46138213633392

Lat: 41.609879465174565

The information provided below is based on the point of interest shown in the map above.

County: **Lake**

Approximate Ground Elevation: **589.2 feet (NAVD88)**

Stream Name:

Base Flood Elevation: **585.0 Feet (NAVD88)**

Grand Calumet River

Drainage Area: **Not Available**

Best Available Flood Hazard Zone: **Not Mapped**

National Flood Hazard Zone: **Not Mapped**

Is a Flood Control Act permit from the DNR needed for this location? **See following pages**

Is a local floodplain permit needed for this location? **Contact your local Floodplain Administrator-**

Floodplain Administrator: **James Portalatin, Manager**

Community Jurisdiction: **City Of East Chicago, City proper**

Phone: **(219) 391-8294**

Email: **jportalatin@eastchicago.com**

US Army Corps of Engineers District: **Chicago**

Date Generated: 1/8/2024

Categorical Exclusion

Appendix G

Public Involvement

NOTICE OF SURVEY



June November 4, 2021

Sample Letter

RE: Notification of field surveys for a road improvement in your area.

Dear Property Owner:

Our firm has been retained by Lochmueller Group, Inc. on behalf of the Indiana Department of Transportation to prepare a survey for road improvement in your area. The project involves improvement along Carrol Street, Michigan Street, and Kennedy Avenue near your property.

Records indicate that you either own or occupy property near this proposed project. We are planning to gather topographic information of the area. To do this we must enter onto your property to map the location of features (i.e., sidewalks, trees, buildings, fences, utilities, and driveways) and obtain ground elevations. The proposed survey will include locating sanitary, storm and water structures that may be located on your property. The survey is needed to depict existing conditions for the proper planning and design of the improvement project. The survey work may also include identification and mapping of wetlands. Geotechnical and/or environmental investigation may also occur.

The topographic data will be collected by land surveyors using surveying equipment and will be employees of Lochmueller Group, Inc. and Northpointe Engineering and Surveying, Inc. Please be aware that at this stage we generally do not know what impact, if any, this project may eventually have on your property. We will be holding public information meetings soon to share plans for the project.

The survey work may include the identification and mapping of wetlands and historic resources, archaeological investigations (which may involve the survey, testing, or excavation of identified archaeological sites) and various other environmental studies. The information we obtain from these studies is necessary for the proper planning and design of the transportation project.

Entry on your property is allowed by law pursuant to Indiana Code IC-25—21.5-9-7 and IC 8-23-7-26. Our employees will identify themselves, if you are available, before coming onto your property to perform their work. If you have sold this property, or it is occupied by someone else, kindly provide me the name and address of the new owner or current occupant so that we may contact them about the survey.

Please know that it is our sincere desire to cause you as little inconvenience as possible. If any problems do occur because of our survey work, please contact our field crew on site, or the project manager Michael Vereb, (574) 334-5483, mvereb@lochgroup.com.

Sincerely,
NORTHPOINTE ENGINEERING & SURVEYING, INC.

Martin K. Spees, PE
Vice President

6125 South East Street, Suite B, Indianapolis, Indiana 46227
Office - 317.884.3020 / Fax - 317.721.0027 / www.npesindy.com
Engineering Surveying Consulting Inspection

Categorical Exclusion

Appendix H

Air Quality

NIRPC 2022-2026 TIP

TIP ID	Work Type	Project Title	Lead Agency	FED Fund Type	FED	STATE	LOC	PE	RW	CN	CE	Funding in Prior Years	2022	2023	2024	2025	2026	Total Project Costs	Air Quality Exempt
2001572	Bridge Rehabilitation or Repair	Bridge rehabilitation; Lake County	INDOT	NHPP Interstate	\$270,000	\$30,000	\$0	\$50,000	\$0	\$250,000	\$0	\$0	\$300,000	\$0	\$0	\$0	\$0	\$300,000	Yes
1703000	Bridge Removal	Pedestrian bridge removal; Lake County	INDOT	NHPP Non Interstate	\$443,137	\$110,784	\$0	\$93,200	\$40,000	\$420,721	\$0	\$43,200	\$90,000	\$420,721	\$0	\$0	\$0	\$553,921	Yes
1900012	Bridge Replacement	District Bridge Project; Lake County	INDOT	State Funds	\$0	\$4,287,589	\$0	\$795,000	\$0	\$3,492,589	\$0	\$485,000	\$0	\$15,000	\$3,787,589	\$0	\$0	\$4,287,589	Yes
1900009	Bridge Replacement	District Bridge Project; Lake County	INDOT	State Funds	\$0	\$4,589,364	\$0	\$735,000	\$0	\$3,854,364	\$0	\$420,000	\$0	\$0	\$25,000	\$4,144,364	\$0	\$4,589,364	Yes
1701446	Bridge Replacement	Bridge Replacement; Porter County	INDOT	State Funds	\$0	\$2,520,313	\$0	\$0	\$0	\$2,520,313	\$0	\$0	\$2,520,313	\$0	\$0	\$0	\$0	\$2,520,313	Yes
2001042	Bridge Rehabilitation or Repair	District Bridge Project (Rehabilitation); Lake County	INDOT	State Funds	\$0	\$1,219,000	\$0	\$169,000	\$0	\$940,000	\$110,000	\$169,000	\$1,050,000	\$0	\$0	\$0	\$0	\$1,219,000	Yes
2000138	Bridge Rehabilitation or Repair	District Bridge Project (Replacement); Porter County	INDOT	State Funds	\$0	\$168,195	\$0	\$19,800	\$0	\$123,395	\$25,000	\$19,800	\$5,000	\$143,395	\$0	\$0	\$0	\$168,195	Yes
2000051	Bridge Rehabilitation or Repair	District Bridge Project (Rehabilitation); LaPorte County	INDOT	State Funds	\$0	\$547,165	\$0	\$210,000	\$0	\$337,165	\$0	\$150,000	\$25,000	\$372,165	\$0	\$0	\$0	\$547,165	Yes
2000058	Bridge Rehabilitation or Repair	District Bridge Project (Rehabilitation); LaPorte County	INDOT	State Funds	\$0	\$179,931	\$0	\$20,000	\$0	\$149,931	\$10,000	\$20,000	\$10,000	\$149,931	\$0	\$0	\$0	\$179,931	Yes
2000045	Bridge Rehabilitation or Repair	District Bridge Project (Rehabilitation); Lake County	INDOT	State Funds	\$0	\$2,065,457	\$0	\$320,000	\$0	\$1,745,457	\$0	\$180,000	\$120,000	\$1,765,457	\$0	\$0	\$0	\$2,065,457	Yes
2000040	Bridge Rehabilitation or Repair	District Bridge Project (Rehabilitation); Lake County	INDOT	State Funds	\$0	\$221,034	\$0	\$28,800	\$0	\$182,234	\$10,000	\$28,800	\$10,000	\$182,234	\$0	\$0	\$0	\$221,034	Yes
2000042	Bridge Rehabilitation or Repair	District Bridge Project (Rehabilitation); Lake County	INDOT	State Funds	\$0	\$197,000	\$0	\$27,000	\$0	\$160,000	\$10,000	\$27,000	\$10,000	\$160,000	\$0	\$0	\$0	\$197,000	Yes
2000402	Bridge Rehabilitation or Repair	District Bridge Project (Rehabilitation); LaPorte County	INDOT	NHPP Non Interstate	\$386,327	\$195,000	\$0	\$120,000	\$0	\$411,327	\$50,000	\$120,000	\$70,000	\$5,000	\$386,327	\$0	\$0	\$581,327	Yes
2000449	Bridge Rehabilitation or Repair	District Bridge Project (Rehabilitation); LaPorte County	INDOT	NHPP Non Interstate	\$235,483	\$46,000	\$0	\$36,000	\$0	\$230,483	\$15,000	\$36,000	\$0	\$10,000	\$235,483	\$0	\$0	\$281,483	Yes



INDIANA DEPARTMENT OF TRANSPORTATION

100 North Senate Avenue
Room N758-Executive Office
Indianapolis, Indiana 46204

PHONE: (855) 463-6848

Eric Holcomb, Governor
Michael Smith, Commissioner

August 28, 2023

Mr. Jermaine R. Hannon, Division Administrator
FHWA Indiana Division
575 North Pennsylvania St., Room 254
Indianapolis, IN 46204

Ms. Kelley Brookins, Regional Administrator
FTA Region 5
200 West Adams St.
Suite 320
Chicago, IL 60606-5253

Dear Mr. Hannon /Ms. Brookins:

The Indiana Department of Transportation is pleased to submit its FY 2024-2028 Statewide Transportation Improvement Program (STIP) for review and approval by your offices.

Included in the final submitted document is a listing of the state's expansion/preservation and local small urban and rural and rural transit projects. The following Metropolitan Planning Organization TIPs will be included in the FY 2024-2028 STIP by reference.

Area Plan Commission of Tippecanoe County (APCTC)	FY 2024-2028
<ul style="list-style-type: none">https://www.tippecanoe.in.gov/DocumentCenter/View/40728/FY-2024-2028-TIP-including-0-amendments	
Bloomington-Monroe County Metropolitan Planning Organization (BMCMPPO)	FY 2024-2028
<ul style="list-style-type: none">https://bloomington.in.gov/sites/default/files/2023-08/BMCMPPO%20FY%202024%20-%202028%20TIP%20-%2006-30-23%20-%20ADOPTED%20FINAL.pdf	
Columbus Area Metropolitan Planning Organization (CAMPO)	FY 2024-2028
<ul style="list-style-type: none">https://www.columbus.in.gov/planning/tip/	
Delaware-Muncie Metropolitan Plan Commission (DMMPC)	FY 2022-2025
<ul style="list-style-type: none"><i>Including Amendments/modifications through 2/14/23</i>https://www.co.delaware.in.us/egov/documents/1692987897_47263.pdf	
Evansville Metropolitan Planning Organization (EMPO)	FY 2024-2028
<ul style="list-style-type: none">http://www.evansvillempo.com/Docs/TIP/TIP_2024-2028/TIP_2024-2028.pdf	
Kokomo-Howard County Governmental Coordinating Council (KHCGCC)	FY 2022-2026
<ul style="list-style-type: none"><i>Including Amendments/modification through 7/28/23</i>https://www.kokomompo.com/project/tip-2020-2024/	

www.in.gov/dot/

An Equal Opportunity Employer



Kentuckiana Regional Planning and Development Agency (KIPDA)	FY 2023-2026
<ul style="list-style-type: none"> • https://www.kipda.org/wp-content/uploads/2023/05/FY2023-TIP-FINAL-5-25.pdf 	
Indianapolis Metropolitan Planning Organization (IMPO)	FY 2024-2027
<ul style="list-style-type: none"> • https://www.indympo.org/whats-underway/irtip 	
Michiana Area Council of Governments (MACOG)	FY 2024-2028
<ul style="list-style-type: none"> • http://www.macog.com/docs/transportation/tip/approved/fy2028tip_projects.pdf 	
Madison County Council of Governments (MCCOG)	FY 2022-2026
<ul style="list-style-type: none"> • <i>Including Amendments/modifications through 7/28/23</i> • https://irp.cdn-website.com/65a760a0/files/uploaded/TIP%202022-2026%20-%20updated%205-1-23.pdf 	
Northeastern Indiana Regional Coordinating Council (NIRCC)	FY 2024-2028
<ul style="list-style-type: none"> • https://www.nircc.com/uploads/1/2/9/8/129837621/final_2024-2028_tip_5-25-23.pdf 	
Northwestern Indiana Regional Planning Commission (NIRPC)	FY 2022-2026
<ul style="list-style-type: none"> • <i>Including Amendments/modifications through 7/25/23</i> • https://nirpc.org/2040-plan/mobility/transportation-improvement-program/ 	
Ohio-Kentucky-Indiana Regional Council of Governments (OKI)	FY 2024-2027
<ul style="list-style-type: none"> • https://www.oki.org/transportation-planning/transportation-improvement-program-tip/ 	
Terre Haute Area Metropolitan Planning Organization (THAMPO)	FY 2024-2028
<ul style="list-style-type: none"> • https://www.terrehautempo.com/images/THAMPO_2024_2028_AdoptionTIP.pdf 	

In addition, INDOT has expanded our public involvement process by taking advantage of virtual meeting techniques and allowing accessibility to online documents, materials, virtual meeting registration, recorded virtual meetings, and comment forms. INDOT also leveraged our planning partner contacts (MPOs, RPOs, LTAP), social media, and notifications sent to local libraries, housing authorities, senior aging centers, and local newspapers across the state.

We greatly appreciate FHWA/FTA support in the development of the STIP 2024-2028 and look forward to working together to achieve our mutual goals. Should you have any questions pertaining to this amendment, please contact April Leckie, STIP Administration at 317-232-5466 or at aleckie@indot.in.gov.

Sincerely,



Michael Smith, Commissioner
Indiana Department of Transportation

cc: (w/enclosure): Angelica Salgado, FTA
Cecilia Crenshaw, FTA
Erica Tait, FHWA
Lyndsay Quist, INDOT
Kristin Brier, INDOT
Kathy Eaton-McKalip, INDOT
Louis Feagans, INDOT

April Leckie, INDOT
Roy Nunnally, INDOT
Larry Buckel, INDOT
Jay Mitchell, INDOT
Jason Casteel, INDOT
Michael McNeil, INDOT

**Federal Transit
Administration**
Region V
200 West Adams St., Suite 320
Chicago, IL 60606-5253



**U.S. Department
of Transportation**

Federal Highway Administration
Indiana Division
575 N. Pennsylvania St., Rm 254
Indianapolis, IN 46204-1576

September 1, 2023

Mr. Michael Smith
Commissioner
Indiana Department of Transportation
100 N Senate Ave. N955
Indianapolis, IN 46204

SUBJECT: Indiana FY2024-2028 STIP Approval and Associated Federal Planning Finding

Dear Mr. Smith:

The Federal Highway Administration (FHWA) and the Federal Transit Administration (FTA) have completed our review of the FY2024-2028 Indiana Statewide Transportation Improvement Program (INSTIP), which was submitted by the Indiana Department of Transportation (INDOT) request letter dated August 23, 2023.

Based on our review of the information provided, certifications of the Statewide and Metropolitan transportation planning processes for and within the state of Indiana, and our participation in those transportation planning processes (including planning certification reviews conducted in Transportation Management Areas), FHWA and FTA are jointly approving the FY2024-2028 STIP, including the Metropolitan Planning Organization (MPO) Transportation Improvement Programs (TIPs) incorporated into the STIP by reference, subject to the corrective actions identified in the attached Federal Planning Finding (FPF) report. FHWA and FTA consider the projects in the 5th year for informational purposes only, and our approval does not exceed four years per 23 CFR 450.220(c).

FHWA and FTA are required under 23 CFR 450.220(b) to document and issue an FPF in conjunction with the approval of the FY2024-2028 STIP. At a minimum, the FPF verifies that the development of the STIP is consistent with the provisions of both the Statewide and Metropolitan transportation planning requirements. FHWA and FTA find that the Indiana FY2024-2028 STIP substantially meets the transportation planning requirements and are approving the STIP subject to the corrective actions outlined in the FPF. This approval is effective September 1, 2023 and is given with the understanding that an eligibility determination of individual projects for funding must be met, and INDOT must ensure the satisfaction of all administrative and statutory requirements, as well as address the corrective actions outlined in the attached report.

If you have questions or need additional information concerning our approval and the FPF, please contact Ms. Erica Tait of the FHWA Indiana Division at (317) 226-7481, or by email at erica.tait@dot.gov, or Mr. Tony Greep of the FTA Region 5 Office at (312) 353-1646, or by email at anthony.greep@dot.gov.

Sincerely,

**KELLEY
BROOKINS** Digitally signed by
KELLEY BROOKINS
Date: 2023.08.31
17:33:15 -05'00'

Kelley Brookins
Regional Administrator
FTA Region V

Sincerely,

**JERMAINE
R HANNON** Digitally signed by
JERMAINE R HANNON
Date: 2023.09.01
11:46:31 -04'00'

Jermaine R. Hannon
Division Administrator
FHWA Indiana Division

Categorical Exclusion

Appendix I

Other

Land and Water Conservation Fund (LWCF) County Property List for Indiana (Last Updated March 2022)

ProjectNumber	SubProjectCode	County	Property
1800005	1800005	Lake	Dowling Park
1800011	1800011	Lake	Tolleston Park
1800012	1800012	Lake	Washington Park
1800040	1800040	Lake	Homestead Park
1800055	1800055	Lake	Sheppard Memorial Park
1800059	1800059	Lake	Cheever Park
1800062	1800062	Lake	Leroy Township Park
1800063	1800063	Lake	Markley Memorial ParkEllendale Park
1800071	1800071	Lake	Cheever Park
1800087	1800087	Lake	Sheppard Memorial Park
1800102	1800102	Lake	Grand Boulevard Lake Recreation Area
1800108	1800108	Lake	Riverview Park
1800137	1800137	Lake	Northgate Park
1800150	1800150	Lake	Meadows Park
1800168	1800168	Lake	Sunnyside Park
1800170	1800170	Lake	Howe Park
1800189	1800189	Lake	Dowling Park
1800193	1800193	Lake	Harrison Park
1800194	1800194	Lake	Martin Luther King Jr. Park (Formerly Maywood Park
1800199	1800199	Lake	Ridgeway Park
1800202	1800202	Lake	Hatcher Park
1800206	1800206	Lake	Meadows Park
1800226	1800226	Lake	Hoosier Prairie Nature Preserve
1800227	1800227	Lake	Liberty Park
1800231	1800231	Lake	Pheasant Hills Community Park & Cherry Hill Tot-Lot
1800237	1800237	Lake	Wolf Lake Park (N & S)
1800239	1800239	Lake	Bluebird Park
1800253	1800253	Lake	Centennial Park
1800272	1800272	Lake	Wolf Lake Park (N & S)
1800273	1800273	Lake	Grand Kankakee Marsh County Park
1800302	1800302	Lake	Munster Community Park
1800329	1800329	Lake	Jackson Park
1800369	1800369H	Lake	Harrison Park
1800369	1800369D	Lake	Lemon Lake County Park
1800377	1800377	Lake	Main Square Park
1800386	1800386	Lake	Gibson Woods Nature Preserve & Tolleston Ridges Nature Preserve
1800405	1800405G	Lake	Clark and Pine Dune Swale Nature Preserve
1800414	1800414	Lake	Wolf Lake Park (N & S)
1800417	1800417	Lake	Centennial (Dan Rabin) Plaza & Trail
1800424	1800424	Lake	Lake Etta County Park
1800455	1800455	Lake	Deep River - Woods Mill County Park
1800464	1800464	Lake	Festival Park & Lakefront Park
1800473	1800473	Lake	Oak Ridge Prairie Co. Park
1800488	1800488	Lake	Marquette Park
1800489	1800489	Lake	Festival Park & Lakefront Park
1800522	1800522	Lake	Pavese Park
1800523	1800523	Lake	Lakewood Park
1800523.5	1800523.5	Lake	River Drive Park
1800528	1800528	Lake	Lowell Sports Park
1800533	1800533	Lake	Hobart City Ball Park
1800555	1800555	Lake	Scherwood Golf Course
1800580	1800580	Lake	Oak Ridge Park
1800586	1800586	Lake	Teibel Nature Park
1800586.1	1800586.1	Lake	Teibel Nature Park
1800590	1800590	Lake	Deep River County Park
1800622	1800622	Lake	Fireman's Park
1800636	1800636	Lake	Parrish Avenue Park

*Park names may have changed. If acquisition of publically owned land or impacts to publically owned land is anticipated, coordination with IDNR, Division of Outdoor Recreation, should occur.

Excerpt From:

Bridge Inspection Report

020-45-02484 C
US 20
over
KENNEDY AVE, IHB RR



Inspection Date: 07/11/2022

Inspected By: Linda Holzinger

Inspection Type(s): Routine

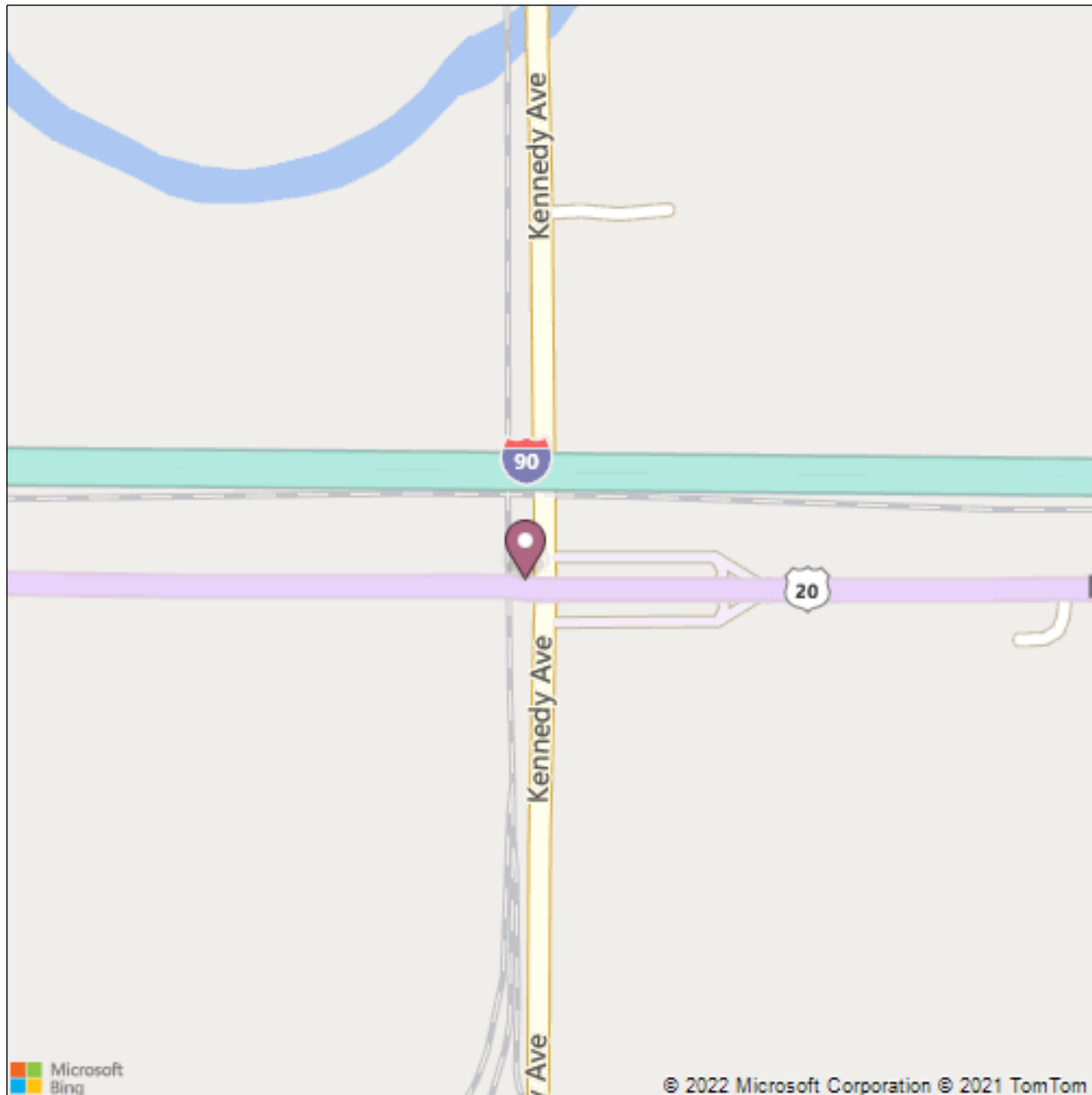
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Inspector: Linda Holzinger
Inspection Date: 07/11/2022

Asset Name: 020-45-02484 C
Facility Carried: US 20

Bridge Inspection Report

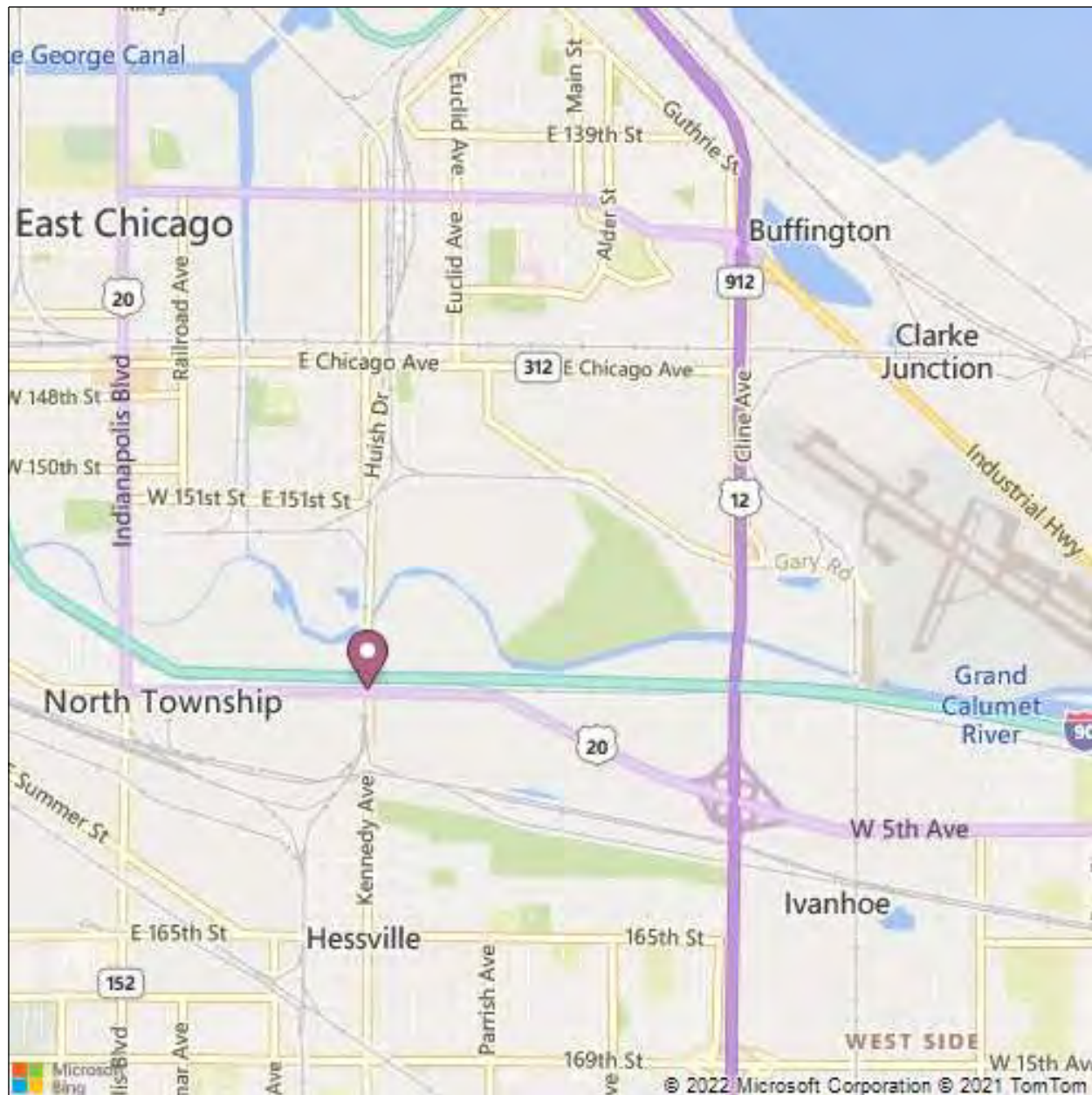


Latitude: 41.61002
Longitude: -87.46164

Inspector: Linda Holzinger
Inspection Date: 07/11/2022

Asset Name: 020-45-02484 C
Facility Carried: US 20

Bridge Inspection Report



Latitude: 41.61002
Longitude: -87.46164

Inspector: Linda Holzinger
Inspection Date: 07/11/2022

Asset Name: 020-45-02484 C
Facility Carried: US 20

Bridge Inspection Report

1934 New Bridge; No Plans on file

1979- "A" rehab, Replace Superstructure, (Contract No.B-11348)

1999- "B" rehab, Bridge Deck Overlay, (Des No. 9407170, Contract No.23755)

2020- "C" rehab, Repair or Replace Joints, (Des No.1701054, Contract No. B-37737)

2024-Programmed for Bridge Replacement, (Des No. 1900009, Contract No. B-42440)

Overall Condition; Bridge is in Satisfactory Condition

Wearing Surface; Fair condition, Hairline to Narrow transverse and longitudinal cracking across deck.

Deck; Satisfactory condition, Minor hairline cracking in all spans

Superstructure; Satisfactory condition,

Substructure- Satisfactory Condition, Abutments and Pier walls have vertical cracking.

Inspector: Linda Holzinger
Inspection Date: 07/11/2022

Asset Name: 020-45-02484 C
Facility Carried: US 20

Bridge Inspection Report

IDENTIFICATION

(1) STATE CODE:	185 - Indiana	(12) BASE HIGHWAY NETWORK:	0
(8) STRUCTURE:	005200	(13A) INVENTORY ROUTE:	
(5 A-B-C-D-E) INV. ROUTE:	1 - 2 - 1 - 00020 - 0	(13B) SUBROUTE NUMBER:	
(2) HIGHWAY AGENCY DISTRICT:	04 - La Porte	(16) LATITUDE:	41.61002
(3) COUNTY CODE:	045 - LAKE	(17) LONGITUDE:	-87.46164
(4) PLACE CODE:	31000 - HAMMOND	(98) BORDER	
(6) FEATURES INTERSECTED:	KENNEDY AVE, IHB RR	A) STATE NAME:	
(7) FACILITY CARRIED:	US 20	B) PERCENT	%
(9) LOCATION:	00.99 E SR 152	(99) BORDER BRIDGE STRUCT. NO:	
(11) MILEPOINT:	0008.180		

STRUCTURE TYPE AND MATERIAL

(43) STRUCTURE TYPE, MAIN:		(45) NUMBER OF SPANS IN MAIN	003
A) KIND OF MATERIAL/DESIGN:	6 - Prestressed concrete continuous	UNIT:	
B) TYPE OF DESIGN/CONSTR:	02 - Stringer/Multi-beam or Girder	(46) NUMBER OF APPROACH SPANS:	0000
(44) STRUCTURE TYPE, APPROACH SPANS:		(107) DECK STRUCTURE TYPE:	1 - Concrete Cast-in-Place
A) KIND OF MATERIAL/DESIGN:	0 - Other	(108) WEARING SURFACE/PROT SYS:	
B) TYPE OF DESIGN/CONSTR:	00 - Other	A) WEARING SURFACE:	3 - Latex Concrete or similar additive
		B) DECK MEMBRANE:	0 - None
		C) DECK PROTECTION:	1 - Epoxy Coated Reinforcing

AGE OF SERVICE

(27) YEAR BUILT:	1934	(28) LANES:	
(106) YEAR RECONSTRUCTED:	1999	A) ON BRIDGE:	02
(42) TYPE OF SERVICE:		B) UNDER BRIDGE:	04
A) ON BRIDGE:	1 - Highway	(29) AVERAGE DAILY TRAFFIC:	014062
B) UNDER BRIDGE:	4 - Highway - railroad	(30) YEAR OF AVERAGE DAILY TRAFFIC:	2021
		(109) AVERAGE DAILY TRUCK TRAFFIC:	14 %
		(19) BYPASS DETOUR LENGTH:	001 MI

Inspector: Linda Holzinger
Inspection Date: 07/11/2022

Asset Name: 020-45-02484 C
Facility Carried: US 20

Bridge Inspection Report

GEOMETRIC DATA

(48) LENGTH OF MAX SPAN:	0082.7	FT	(35) STRUCTURE FLARED:	0 - No flare
(49) STRUCTURE LENGTH:	00193.0	FT	(10) INV RTE, MIN VERT CLEARANCE:	99.99 FT
(50) CURB/SIDEWALK WIDTHS:			(47) TOT HORIZ CLEARANCE:	052.0 FT
A) LEFT	00.0	FT	(53) VERT CLEAR OVER BR RDWY:	99.99 FT
B) RIGHT:	00.0	FT	(54) MIN VERTICAL UNDERCLEARANCE:	
(51) BRDG RDWY WIDTH CURB-TO-CURB:	052.0	FT	A) REFERENCE FEATURE:	R
(52) DECK WIDTH, OUT-TO-OUT:	055.3	FT	B) MIN VERT UNDERCLEAR:	22.26 FT
(32) APPROACH ROADWAY	044.0	FT	(55) LATERAL UNDERCLEARANCE RIGHT:	
(33) BRIDGE MEDIAN:	0 - No median		A) REFERENCE FEATURE:	H
(34) SKEW:	00	DEG	B) MIN LATERAL UNDERCLEAR:	000.0 FT
			(56) MIN LATERAL UNDERCLEAR ON LEFT:	00.0 FT

INSPECTIONS

(90) INSPECTION DATE:	07/11/2022	(91) DESIGNATED INSPECTION FREQUENCY:	24 MONTHS
(92) CRITICAL FEATURE INSPECTION:		(93) CRITICAL FEATURE INSPECTION DATE:	
A) FRACTURE CRITICAL REQUIRED/FREQUENCY:	N	A) FRACTURE CRITICAL DATE:	
B) UNDERWATER INSPECTION REQUIRED/FREQUENCY:	N	B) UNDERWATER INSP DATE:	
C) OTHER SPECIAL INSPECTION REQUIRED/FREQUENCY:	N	C) OTHER SPECIAL INSP DATE:	

CONDITION

(58) DECK:	6 - Satisfactory Condition (minor deterioration)	(60) SUBSTRUCTURE:	5 - Fair Condition (minor section loss)
(58.01) WEARING SURFACE:	5 - Fair Condition	(61) CHANNEL/CHANNEL PROTECTION:	N - Not Applicable
(59) SUPERSTRUCTURE:	6 - Satisfactory Condition (minor deterioration)	(62) CULVERTS:	N - Not Applicable

CONDITION COMMENTS

(58) DECK: 6 - Satisfactory Condition (minor deterioration)

Comments:

Minor hairline cracking in all spans.
Spans B and C, over RR tracks have black soot from trains.

(58.01) WEARING SURFACE: 5 - Fair Condition

Comments:

Wearing surface has minor abrasion across entire surface. 3' spalling at center line (missing RPs).
Widely spaced hairline to narrow transverse cracking.
Large unsound HMA patch in East Bound Lane.

Inspector: Linda Holzinger
Inspection Date: 07/11/2022

Asset Name: 020-45-02484 C
Facility Carried: US 20

Bridge Inspection Report

(59) SUPERSTRUCTURE: 6 - Satisfactory Condition (minor deterioration)

Comments:

Beams and Bearings appear to be in Satisfactory condition. Rating not changed due to previous report of beam end chipping, I was not able to see beam ends.

(60) SUBSTRUCTURE: 5 - Fair Condition (minor section loss)

Comments:

Northeast wingwall has a wide vertical crack at center of wall. All other wingwalls have minor hairline vertical cracking. East Abutment Cap under Beam 2, bearing, (from the South) has a large 2' area of spalling with rust staining. Both Abutment Caps have closely spaced vertical cracking with rust staining. Bents 2&3 have minor hairline cracking. Bent 3 has a broken metal drain on the West end.

(61) CHANNEL/CHANNEL PROTECTION N - Not Applicable

Comments:

(62) CULVERTS: N - Not Applicable

Comments:

LOAD RATING AND POSTING

(31) DESIGN LOAD:	4 - H 20	(66) INVENTORY RATING:	0.971
(70) BRIDGE POSTING	5 - Equal to or above legal loads	(65) INVENTORY RATING METHOD:	8 - Load and Resistance Factor Rating (LRFR) rating report by rating factor (RF) method using HL-93 loadings.
(41) STRUCTURE OPEN/POSTED/CLOSED:	A - Open	(66B) INVENTORY RATING (H):	
(64) OPERATING RATING:	1.258	(66C) TONS POSTED :	
(63) OPERATING RATING METHOD:	8 - Load and Resistance Factor Rating (LRFR) rating report by rating factor (RF) method using HL-93 loadings.	(66D) DATE POSTED/CLOSED:	

APPRAISAL

SUFFICIENCY RATING:	83.1	(36) TRAFFIC SAFETY FEATURE:	
STATUS:	2	36A) BRIDGE RAILINGS:	1
(67) STRUCTURAL EVALUATION:	5	36B) TRANSITIONS:	1
(68) DECK GEOMETRY:	9	36C) APPROACH GUARDRAIL:	1
(69) UNDERCLEARANCES, VERTICAL & HORIZONTAL:	3	36D) APPROACH GUARDRAIL ENDS:	1

(71) WATERWAY ADEQUACY: N - Not Applicable

Comments:

(72) APPROACH ROADWAY ALIGNMENT: 8 - Equal to present desirable criteria

Comments:

No substantial reduction in speed is necessary for traffic to safely cross the bridge.

(113) SCOUR CRITICAL BRIDGES: N - Not over waterway

Comments:

Inspector: Linda Holzinger
Inspection Date: 07/11/2022

Asset Name: 020-45-02484 C
Facility Carried: US 20

Bridge Inspection Report

CLASSIFICATION

(20) TOLL:	3 - On Free Road	(21) MAINT. RESPONSIBILITY:	01 - State Highway Agency
(22) OWNER:	01 - State Highway Agency	(26) FUNCTIONAL CLASS OF INVENTORY RTE:	14 - Urban - Other Principal Arterial
(37) HISTORICAL SIGNIFICANCE:	5 - Not eligible	(100) STRAHNET HIGHWAY:	Not a STRAHNET route
(101) PARALLEL STRUCTURE:	N - No parallel structure	(102) DIRECTION OF TRAFFIC:	2-way traffic
(103) TEMPORARY STRUCTURE:		(104) HIGHWAY SYSTEM OF INVENTORY ROUTE:	0 - Structure/Route is NOT on NHS
(105) FEDERAL LANDS HIGHWAYS:	0-Not Applicable	(110) DESIGNATED NATIONAL NETWORK:	Inventory route on National Truck Network
(112) NBIS BRIDGE LENGTH:	Yes		

NAVIGATION DATA

(38) NAVIGATION CONTROL:	N - Not applicable, no waterway	(39) NAVIGATION VERTICAL CLEAR:	000.0 FT
(111) PIER OR ABUTMENT PROTECTION:		(116) MINIMUM NAVIGATION VERT. CLEARANCE, VERT. LIFT BRIDGE:	FT
		(40) NAV HORIZONTAL CLEARANCE:	0000.0 FT

PROPOSED IMPROVEMENTS

(75A) TYPE OF WORK:	(95) ROADWAY IMPROVEMENT COST: \$	000000
(75B) WORK DONE BY:	(96) TOTAL PROJECT COST:	\$ 000000
(76) LENGTH OF IMPROVEMENT:	(97) YR OF IMPROVEMENT COST EST:	
(94) BRIDGE IMPROVEMENT COST:	(114) FUTURE AVG DAILY TRAFFIC:	028062
	(115) YR OF FUTURE ADT:	2032

Inspector: Holzinger,Linda
Inspection Date: 07/11/2022

Structure Number: 005200
Facility Carried: US 20

Bridge Inspection Report

Miscellaneous Asset Data
Asset Management

005200

Load Rating 2:

Has the dead load or the structural condition of the primary load carrying members changed since the last inspection? No

Extended Frequency:

Submittal Date:

Inspector:

INDOT Reviewer:

This bridge has been accepted into the Extended Frequency Program.

Approval Date:

Joints: * Indicate location, type, and rating of lowest rated joint.

Transverse
North/East

O

7 - Good Condition

Comments:

Minor chipping and spalling at joint edges. filled with debris

Terminal Joints: *Rating of lowest rated terminal joint.

7

Comments:

Good Condition, new in 2020

Concrete Slopewall: *Rating of lowest rated slopewall.

N

Comments:

Bearings: * Indicate type, and rating of lowest rated bearing.

2 - Elastmeric

Comments:

Cannot be seen from ground inspection, no rating assigned.

Inspector: Holzinger,Linda
Inspection Date: 07/11/2022

Structure Number: 005200
Facility Carried: US 20

Bridge Inspection Report

Approach Slabs: * Indicate if present & condition rating.

1 - Approach Slabs

5 - Fair condition, no settlement, moderate cracking and
spalls, crack spacing > .5'

Comments:

Both Approaches have longitudinal and transverse cracking.

Paint: * Indicate if paint present , year painted & condition rating.

N - No Paint

N

Comments:

Endangered Species: * If yes, add one photo to the dropdown field

Bats: seen or heard under structure? *

N - No evidence of bats

Birds/swallows/nests seen? Empty nests present? *

N - No Birds and/or Nests Visi

BRIDGE Culvert Geometry:

Barrel Length:

Height:

Width:

LOAD RATING - BRADIN

Load Rating Date: 24-APR-07

National Bridge Inventory (NBI):

(65) INVENTORY RATING METHOD:	8	(31) DESIGN LOAD:	4
(66) INVENTORY RATING:	0.971	(70) BRIDGE POSTING:	5
(63) OPERATING RATING METHOD:	8	(41) STRUCTURE OPEN/POSTED/CLOSED:	A
(64) OPERATING RATING:	1.258	(66C) TONS POSTED:	
		(66D) DATE POSTED/CLOSED:	

Posting Configurations:

Emergency Vehicles:

EV2: LEGAL RF:	2.195
EV3: LEGAL RF:	1.503

5-Axles:

AASHTO TYPE 3S2: LEGAL RF:	2.325
SU5: LEGAL RF:	1.791
TOLL ROAD LOADING NO. 1: ROUTINE PERMIT RF:	1.881

2-Axles:

H20-44: LEGAL RF:	2.101
ALTERNATE MILITARY: LEGAL RF:	1.693

6+-Axles:

AASHTO TYPE 3-3: LEGAL RF:	2.89
LANE TYPE: LEGAL RF:	4.756
SU6: LEGAL RF:	1.67
SPECIAL TOLL ROAD TRUCK: ROUTINE PERMIT RF:	1.852
SU7: LEGAL RF:	1.608

3-Axles:

HS20: LEGAL RF:	1.651
AASHTO TYPE 3: LEGAL RF:	2.152

4-Axles:

SU4: LEGAL RF:	1.891	MICHIGAN TRAIN TRUCK NO. 5: ROUTINE PERMIT RF:	1.836
TOLL ROAD LOADING NO. 2: ROUTINE PERMIT RF:	2.113	MICHIGAN TRAIN TRUCK NO. 8: ROUTINE PERMIT RF:	1.756

Other Configurations:

H20-44: DESIGN RF:	1.561	SUPERLOAD-11 AXLES: SPECIAL PERMIT RF:	1.987
NRL: LEGAL RF:	1.556	SUPERLOAD-13 AXLES: SPECIAL PERMIT RF:	2.368
		SUPERLOAD-14 AXLES: SPECIAL PERMIT RF:	1.719
		SUPERLOAD-19 AXLES (152.5T): SPECIAL PERMIT RF:	2.08
		SUPERLOAD-19 AXLES (240.045T): SPECIAL PERMIT RF:	1.613

Inspector: Linda Holzinger
Inspection Date: 07/11/2022

Asset Name: 020-45-02484 C
Facility Carried: US 20

Bridge Inspection Report

Date Reported: 07/12/2022
Priority: Yellow - 2
Work Code: Guardrail / Barrier Wall Repair

Deficiency Description:
3-4' deep wash out at end of SE corner Barrier Wall around guardrail, post. Fill as necessary

Work Description:

Date Repairs Completed:

Maintenance Comments:

Stage: Open



PHOTO 1 Description SE corner wash out

Stage: Open



PHOTO 2 Description SE corner

ADDENDUM No. 1 TO ABBREVIATED ENGINEER'S REPORT

Project Number: 1900009
 Route / Feature US 20 over Kennedy Avenue & Indiana Harbor
 Crossed: Belt Railroad
 Project Location: 0.99 miles East of SR 152
 Date: 11/20/2023

ADDENDUM JUSTIFICATION:

The purpose of this addendum is to describe and justify the scope changes from the signed Abbreviated Engineer's Report (AER) dated September 17, 2019.

- At the Scope of Services meeting on February 20, 2020, reconstructing the vertical profile grade to address the inadequacies and reconstructing the ramps to be right-in/right-out ramps was discussed and implemented in design.
- After the preliminary design began it was determined that the project should be more appropriately designed according to 3R Design Criteria versus 4R Design Criteria. The US 20 roadway design will still meet 4R Design Criteria with the exception of maximum vertical grade; however, 3R Design is more appropriate for the scope because it is a spot improvement, and the overall corridor is not planned to be changed in the future.
- At the preliminary field check meeting, INDOT agreed to include the Marquette Greenway Trail through the project limits with funding provided by the Northwestern Indiana Regional Planning Commission.

REVISION TO ORIGINAL SCOPE DOCUMENT:

The Abbreviated Engineer's Report is being revised as follows:
(Signed AER document text struck-through, addendum values bolded).

Table 1 – Roadway Information			
Geometric Criteria			
<i>Design Speed</i>	<i>45 mph</i>	<i>Functional Class</i>	<i>Principal Arterial-Other</i>
<i>Design Criteria</i>	4R (Non-Freeway) 3R (Non-Freeway)	<i>Rural/Urban</i>	<i>Urban</i>
<i>Terrain</i>	<i>Level</i>	<i>Access Control</i>	<i>None</i>
Approach Cross Section			
<i>IDM Figure Reference</i>	53-7 55-3F	<i>Travel Lane Width</i>	<i>12 ft (Existing)</i> <i>Maintain existing (Proposed)</i>
<i>Travel Lanes</i>	<i>Two</i>	<i>Curb Offset</i>	<i>2 ft</i>
<i>Shoulder Width (Usable)</i>	<i>14 ft (Existing)</i> <i>Maintain existing (proposed)</i> <i>12 ft 8 in (Proposed, greater than desired criteria, allows for center curb)</i>	<i>Shoulder Width (Paved)</i>	<i>14 ft (Existing)</i> <i>Maintain existing (proposed)</i> <i>12 ft 8 in (Proposed, greater than desired criteria, allows for center curb)</i>

<i>Mainline Pavement</i>	<i>Concrete (Existing) Maintain Existing (Proposed)</i>	<i>Shoulder Pavement</i>	<i>HMA (Existing) Maintain existing (proposed) Concrete in Full Depth Replacement Sections (Proposed)</i>
Alignment			
<i>Horizontal</i>	<i>Tangent (existing) Maintain existing (proposed)</i>	<i>Vertical</i>	<i>Crest and sag curves (existing) Maintain existing (proposed) Redesign curves to meet sight distance criteria</i>

Table 1A – Trail Information			
Geometric Criteria			
<i>Design Speed</i>	<i>18 mph (85th percentile speed)</i>	<i>Classification</i>	<i>Shared Use Path</i>
<i>Design Criteria</i>	<i>AASHTO Guide for the Development of Bicycle Facilities</i>		
Approach Cross Section			
<i>Trail Width</i>	<i>Min. 10 ft typical Min. 8 ft where pedestrian use is expected to be low</i>	<i>Shoulder</i>	<i>Des. 3-5 ft, slope 6:1 Min. 2 ft, slope 6:1 May be 1 ft for smooth features such as railing</i>
<i>Barrier Warrant</i>	<i>Recovery Area < 5 ft and - Slopes $\geq 3:1$, 6 ft drop or parallel water or obstacle - Slopes $\geq 2:1$, 4 ft drop - Slopes $\geq 1:1$, 1 ft drop</i>	<i>Minimum Radii (Horizontal Curve)</i>	<i>60 ft (based on design speed)</i>
<i>Cross Slope</i>	<i>Des. 1% Max. 2%</i>	<i>Grade</i>	<i>Max. 5%, unless adjacent to roadway with > 5% then may exceed 5%</i>

Drainage

*Proposed drainage consists of sheet flow from the road to drainage inlets along to bridge rail.
Proposed drainage consists of sheet flow to inlets along curb. The necessity of deck drains on the bridge will be checked with final vertical alignment and bridge geometry.*

Attachments:

A1 – 14 CFR 77.9

A2 – Scope of Service Meeting Agenda

A3 – Correspondence with Project Manager, Andrew Borden

A4 – Preliminary Field Check Meeting Minutes

Does the revision change the project's Purpose & Need statement?

☐ Yes

☒ No

Does the revision change the project's recommended treatment?

☒ Yes

☐ No

Alternative B – Bridge Replacement

Details of Preferred Alternative

*The preferred bridge project will consist of a full replacement. The designer shall develop a Structure Size and Type Analysis to define the proposed structure. The designer shall consider a two-span bridge with integral end bents and MSE walls. The existing approach roadway presently does not meet Level One Design Criteria for both the crest curve over the bridge as well as the sag curve immediately to the east. ~~Stopping Sight Distances should also be checked for the project location. Level One Design exceptions are required for this project.~~ **The vertical alignment will be designed to meet design criteria, provide sufficient stopping sight distance, and tie-in appropriately to the approach roads/ramps.***

*Depending on utility coordination, alternative substructure types may be required due to the proximity of overhead facilities. This may impact the span lengths of the bridge and thus potentially increase the superstructure depth. As the low chord must match or exceed the existing conditions (presently matching the minimum vertical clearance over a railroad of 23'-0"), the roadway profile grade may require an increase in elevation. The vertical curve and vertical stopping sight distances will require review to meet design criteria ~~or a Level I Design Exception will be required. Additional roadway improvement should be minimized when possible.~~ **The approach roads/ramps will be re-designed to be right-in/right-out approaches and to meet the revised vertical alignment of US 20.***

*As part of this preferred alternative, terminal joints will be per RSP 503-R-692d. ~~Bridge deck drains will be required for the preferred alternative.~~ **The necessity of deck drains on the bridge will be checked with final vertical alignment and bridge geometry.***

The Northwest Indiana Regional Planning Commission (NIRPC) is constructing the Marquette Greenway Trail to the west of the project with Des. No. 1901900, from the US 20 bridge replacement project limits to US 20 north/Indianapolis Blvd, and to the east of the project with Des. No. 1601146, from Kennedy Avenue to US 12. NIRPC has reached an agreement to pay for the cost of a shared-use path, within the project limits, on the north side of US 20. The multi-use path will be 10'-0" wide on the bridge and protected by traffic barrier.

This structure replacement is proposed to take place in construction year ~~2024.~~ **2026.**

Does the revision change the project's cost estimate?

☒ Yes

☐ No

Cost Estimate

The construction cost of the preferred alternative is expected to be ~~\$3,527,000~~. **\$16,240,000**. This includes removal of the existing structure, fabrication and installation of the new structure, and associated roadway items with a 15% contingency.

Estimated Total Project Costs	Revised Amount	Original Amount
Right of Way Purchase	Temporary R/W: 0.065 acres Permanent R/W: 1.13 acres Cost/Acre Temp. R/W: \$78,408 Cost/Acre Perm. R/W: \$261,360 Total Cost: \$300,500	\$0

Estimated Total Project Costs	Revised Amount	Original Amount
Right of Way Services	\$35,000	N/A
Preliminary Engineering	\$2,436,000 (15% of CN)	\$527,000 (15% of CN)
Railroad PE	\$300,000	\$300,000
Utilities PE	\$30,000	\$30,000
Construction Total:		
Construction (Primary asset group)	\$16,240,000	\$3,527,000
ADA	N/A	N/A
Sidewalks	N/A	N/A
Small Culvert / Drainage	N/A	N/A
Other Considerations		
TOTAL:	\$19,041,000	\$4,384,00

Does the revision change the project's environmental impacts?

☒ Yes

☐ No

Environmental Considerations

~~The project location is proximity to a navigation facility and may impact the assurance of a navigation signal reception. The Federal Aviation Administration, in accordance with 77.9, requests that the project is filed. The project does not meet the requirements of 14 CFR 77.9 for filing notice with the FAA. The project will remain below high voltage overhead distribution electric lines and should not impact the functions Gary/Chicago International Airport.~~

The increase in project scope causes additional impacts to three wetlands and requires permanent and temporary right-of-way to be purchased from the property at the corner of the south access road and Kennedy Avenue that is eligible for the National Register of Historic Places (however, the project is anticipated to have "No Adverse Effect" upon this property).

The project will also disturb a larger area of ground and require additional tree clearing.

Does the revision require additional Right-of-Way?

☒ Yes

☐ No

Right of Way Impacts

*The existing apparent right-of-way west of the bridge is 80 ft left and 100 right while east of the bridge is 100.5 ft left and 99.5 ft right of the centerline according to existing plans. The project lies within the right-of-way limits of the IHB Railroad. A preliminary records search was not completed for this project. ~~This project is not expected to require the purchase of temporary right-of-way or permanent right-of-way.~~ **Reconstruction of the vertical alignment requires temporary right-of-way for grading in the southwest quadrant of the bridge and a combination of temporary and permanent right-of-way for grading in the northwest quadrant. Redesign of the approach roads/ramps requires permanent right-of-way in the northeast and southeast quadrants of the bridge and additional temporary right-of-way for grading in the southeast quadrant.***

Does the revision change the project's schedule (design or construction)?

☒ Yes

☐ No

	<i>Revised Schedule</i>	<i>Original Schedule</i>
Stage 2 Submittal	11/20/2023	11/4/2022
Stage 3 Submittal	9/27/2024	1/1/2024
Final Tracings Submittal	1/27/2025	3/1/2024
Ready for Contracts	2/26/2025	5/12/2024
Letting Date	5/7/2025	7/10/2024
Construction	Summer 2026	Spring 2025

Does the revision require additional coordination with utility companies?

☒ Yes

☐ No

The increase in the project scope requires additional engagement with all utility owners. The timing of the scope change requires the conflict analysis be requested again, and a second preliminary field check meeting for utilities held along with individual meetings with utility owners. The conflict analysis responses will be reviewed with the updated design to minimize conflicts and relocations, where possible.

ADDENDUM CONCURRENCE

This document was prepared by:

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Reviewed by:

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Scoping Manager, LaPorte District

Approved by:

Steve Benczik, P.E.
Systems Asset Manager, LaPorte District

Excerpt From:

ABBREVIATED ENGINEER'S REPORT

PROJECT NUMBER: 1900009

EXISTING BRIDGE FILE NUMBER: 020-45-02484 B

NBI NUMBER: 005200

BRIDGE PROJECT

ROUTE IDENTIFICATION AND FEATURE CROSSED:

**US 20 over Kennedy Ave, IHB RR, 0.99 mile east of SR 152
Lake County, Indiana**

September 17, 2019

Purpose of Report >>

The purpose of this report is to document the engineering assessment phase of project development, including all coordination that has been completed in preparation for this bridge project. This document outlines the proposal and is intended to serve as a guide for subsequent survey, design, environmental, right of way and other project activities leading to construction. The preferred alternative identified in this document is considered predecisional, pending the outcome of environmental studies.

Project Location >>

This project is located on US 20, 0.99 mile east of SR 152 at reference post 8+0.179 in Lake County. The GPS Coordinates are 41°36'36" North and 87°27'42" West. The project is in the Indiana Department of Transportation's LaPorte District, Gary Sub-District. This location is in a Metropolitan Planning Organization, Northwestern Indiana Regional Planning Commission.

The project location map and project site photos are included in Appendix A.

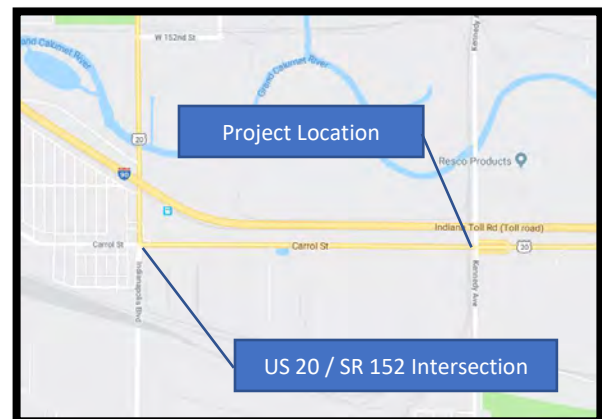


Figure 1 – Project Location Map

Project Purpose and Need >>

The primary purpose of this project is to achieve an overall condition rating of "7" or greater on all bridge elements. The need for this project is due to the existing structure showing signs of minor section loss in the deck, superstructure, and substructure. There is cracking and spalling in the underside of the deck, chipping on the beams, and cracking / leaking abutments.

Existing Facility >>

The existing facility is classified as an Urban Principal Arterial - Other and is not part of the US National Highway System (NHS). The roadway is on the National Truck Network (NTN). The posted speed limit at the project location is 45 mph.

The bridge was originally constructed in 1934. The bridge underwent significant rehabilitation in 1978 under contract B-11328. During the 1978 Rehabilitation Project all bridge components were replaced except for the bottom portion of the 1934 abutments. The bridge then underwent a deck rehabilitation and overlay with contract B-23755 in 1999. The bridge has received preventative maintenance on the joints with contract B-37737 in 2019.

The current bridge's vertical clearance over the railroad is currently the minimum allowable at 23 ft.

Roadway

The existing clear roadway width on the bridge is 52 ft through the project limits with existing concrete bridge railing and approach rail / guardrail. The existing roadway consists of two 12 ft travel lanes and 14 ft paved shoulders.

Table 1 - Roadway Information			
Geometric Criteria			
Design Speed	45 mph	Functional Class	Principal Arterial - Other
Design Criteria	4R (Non-Freeway)	Rural/Urban	Urban
Terrain	Level	Access Control	None
Approach Cross Section			
IDM Figure Reference	IDM 53-7		
Travel Lane Count	Two	Travel Lane Width	12 ft (existing) Maintain existing (proposed)
Shoulder Width (Usable)	14 ft (existing) Maintain existing (proposed)	Shoulder Width (Paved)	14 ft (existing) Maintain existing (proposed)
Mainline Pavement	Concrete (existing) Maintain existing (proposed)	Shoulder Pavement	HMA (existing) Maintain existing (proposed)
Alignment			
Horizontal	Tangent (existing) Maintain existing (proposed)	Vertical	Crest Curve (existing) Maintain existing (proposed)

Road History

Table 2 - Pavement History Within Project Limits		
Year	Width	Type of Work
1978	52'	Bridge Replacement Project – Existing plans show HMA on concrete
Unknown	52'	Full depth, concrete Mainline Replacement with HMA shoulders
1998	52'	Partial depth shoulders installed adjacent to existing full depth, concrete mainline

Structure 020-45-02484 B

The existing bridge data is as follows:

Structure Number:	020-45-02484 B	
NBI Number:	005200	
Feature Intersected:	Kennedy Ave, IHB RR	
Superstructure Type:	Continuous Prestressed Reinforced Concrete I-Beam	
Substructure Type:	Reinforced Concrete on Footings	
Span Length:	32'-3", 82'-9", 77'-0"	
Structure Length:	193'-2"	
Deck Width:	55'-4"	
Inventory Rating:	HS20-44	48 Tons
Operating Rating:	HS20-44	82 Tons
Design Load:	HS20-44	
Deck Geometry:	665' Vertical Crest Curve	
Bridge Railing:	Concrete Bridge Railing, Type FC	
Skew Angle:	0°	
Joint Type:	Type IA – Replaced With Pre-Compressed Foam Joint in 2019	

Structure Inspection Observations

TABLE 3 – Existing Bridge Data		
Bridge Element	Rating	Description
Deck	6 - Satisfactory	Some minor transverse cracking on the underside spans.
Wearing Surface	5 - Fair	Cracking throughout surface, some map cracking areas with spalling beginning.
Superstructure	6 - Satisfactory	Minor chipping on the beam edges in spans.
Substructure	5 - Fair	Abutment wall and northeast wing wall cracking with leaking and rust staining.
Channel / Channel Protection	N/A	N/A
Culverts	N/A	N/A
Joints	3 - Poor	Joint leaking is heavy on abutments, heavy efflorescence and water staining. Joints were previously sealed but has failed. Joints were replaced in 2019 under contract B-37737 after the 2019 Bridge Inspection.
Bridge Approach Slabs	5 - Fair	No settlement, moderate cracking and spalls, crack spacing > 0.5'.
Bridge Railings	N/A	Condition State 1 per 2017 NBIS Report.
Bearings	0 – Unable to Inspect	Bearings cannot be seen from ground inspection.

Drainage

Existing drainage through the project is primarily through sheet flow away from the road into deck drains. From available imagery, the shoulders of the deck along the bridge rail have sediment built up and water staining throughout. Proposed drainage consists of sheet flow from the road to drainage inlets along to bridge rail.

Traffic Data >>

Traffic data was obtained from the Indiana Department of Transportation TCDS. A summary of traffic data follows:

Table 4 - Traffic Data Summary Table			
YEAR	AADT	DHV	COMMERCIAL VEHICLES
2018	14,944 vpd	9%	15% AADT

Traffic forecast is to be requested during the design phase of this project. The directional distribution is 51% positive direction.

Alternatives and Recommendations >>

Alternative A – Do Nothing

This alternative allows the existing roadway and structure to remain in place with no improvements. This alternative will result in continued deterioration of the bridge, specifically the bridge substructure. This alternative does not meet the need nor achieves the purpose of the project and will not be considered further.

Alternative B – Bridge Replacement

This alternative meets the need and purpose of the project and is the preferred alternative.

Details of Preferred Alternative

The preferred bridge project will consist of a full bridge replacement. The designer shall develop a Structure Size and Type Analysis to define the proposed structure. The designer shall consider a two-span bridge with integral end bents and MSE walls. The existing approach roadway presently does not meet Level One Design Criteria for both the crest curve over the bridge as well as the sag curve immediately to the east. Stopping Sight Distances should also be checked for the project location. Level One Design exceptions are required for this project.

Depending on utility coordination, alternative substructure types may be required due to proximity of overhead facilities. This may impact the span lengths of the bridge and thus potentially increase the superstructure depth. As the low chord elevation must match or exceed the existing conditions (presently matching the minimum vertical clearance over a railroad of 23'-0"), the roadway profile grade may require an increase in elevation. The vertical curve and vertical stopping sight distances will require review to meet design criteria or a Level 1 Design Exception will be required. Additional roadway improvement should be minimized when possible.

As part of this preferred alternative, terminal joints will be per RSP 503-R-692d. Bridge deck drains will be required for the preferred alternative.

Pavement design and geotechnical investigation will be needed for this project. New reinforced concrete bridge approaches as well as MASH rated bridge railing and guardrail will be included in the project.

This structure replacement is proposed to take place in construction year 2024.

Maintenance of Traffic During Construction >>

TABLE 5 – Maintenance of Traffic Concept	
Is this a mobility significant project / require a TMP?	Yes
Can the road be closed to traffic (detour)?	Yes
Does the project require an IHCP Exception?	No

Phased construction would include the following: one 10' lane for each direction of travel, 1' shoulders, and less than 1' between the construction sawcut and the back of the anchored Temporary Traffic Barriers (TTB). Phased construction would required the addition of threaded tie-bars and temporary shoring for retained fill behind the existing abutments.

Complete closure of US 20 with a detour is recommended for this project due to limited roadway width and potential constructability concerns for phased construction. The proposed detour route utilizes US 20 (one mile west of the bridge) and US 12. Access to local businesses and residents shall be maintained at all times. The official detour route length will be approximately 6.8 miles, but only requires an additional 4.0 miles of travel. A local detour should be coordinated with the City of East Chicago and the City of Hammond.

Kennedy Avenue will be detoured utilizing the US 20 ramps immediately east of the bridge. The District or the District's Consultant is instructed to revisit and refine this strategy to ensure that the impact to the surrounding emergency services and schools are minimized.

Cost Estimate >>

The construction cost of the preferred alternative is expected to be \$3,527,000. This includes removal of the existing structure, fabrication and installation of the new structure, and associated roadway items. It is noted that the quantities used for the construction estimate are preliminary; detailed plans and an approved pavement design are not available for the approach roadway reconstruction. A summary table of the project costs is provided below.

TABLE 6 – Project Costs		
Phase	Amount	Comments
Right of Way Purchase (RW1):		N/A
Right of Way Services (RW2):		N/A
Preliminary Engineering 1 (PE1):	\$527,000	15% of CN
Railroad PE (RR1):	\$300,000	
Utilities PE (UT1):	\$30,000	
Utilities CN (UT2):	\$15,000	
Construction Total:	\$3,527,000	
Construction (CN):	\$3,512,000	
Railroad Construction:		Included in CN
Maintenance of Traffic:		Included in CN
Environmental Mitigation:	\$15,000	
ADA:		N/A
Construction Engineering (CE):	\$527,000	15% of CN
Other Considerations:		N/A
TOTAL:	\$4,926,000	

Environmental Considerations >>

TABLE 7 – Environmental Impacts

	<i>Description</i>	<i>Notes</i>
<input checked="" type="checkbox"/>	Tree Clearing	Restriction prohibiting tree clearing from April 1 st to September 30 th in consideration of the Indiana Bat.
<input type="checkbox"/>	Fish	N/A
<input checked="" type="checkbox"/>	Bats	Per the USFWS IPaC site, the Indiana Bat and the Northern Long-eared Bat could be potentially affected in the project area. Site investigation will be required as nothing was noted in the 2018 NBIS.
<input checked="" type="checkbox"/>	Historical	Potential historic properties listed on or eligible for the National Register of Historic Places and the Indiana Register of Historic Sites and Structures are known to be located within one mile of the project limits. This known property, Northern States Life Insurance Company (5935 Hohman Ave, Hammond IN) is located 0.98 mile from the project limits and is not likely to be impacted. Based on the Indiana Historic Bridge Inventory Report, 020-45-02484 B is not listed as historic. Cultural resources will be further investigated during preparation of the Environmental Document. Section 106 coordination with the Indiana Department of Natural Resources Division of Historic Preservation and Archeology shall be included in the Environmental Document.
CE-1	CE Type	The District or the District's consultant will prepare an environmental document in accordance with the National Environmental Policy Act, National Historic Preservation Act, and other relevant laws.

TABLE 8 – Permits Required

	<i>Description</i>	<i>Notes</i>
<input type="checkbox"/>	USACE 404	N/A
<input type="checkbox"/>	IDEM 401	N/A
<input type="checkbox"/>	IDNR CIF	N/A
<input type="checkbox"/>	IDNR Navigable Waterway	N/A
<input type="checkbox"/>	IDEM Rule 5	Best management practices for erosion and sediment control will be incorporated into the plans and implemented during construction. A Rule 5 permit from the Indiana Department of Environmental Management (IDEM) will not be required for this project, unless the project results in land disturbances greater than one acre pursuant to 327 IAC 15-5. Best management practices regarding storm water quality management will be implemented regardless of the project requirement of Rule 5 permit.
1	Storm Water Quality Manager Level	Very tight right of way, nearby sensitive resources, and major environmental impacts are not currently present. This will be further defined during design

No publicly owned parks, recreational areas or historic sites considered as Section 4(f) properties were identified within the project limits. A Section 4(f) analysis is likely to not be necessary as part of the environmental documentation prepared for the project.

The project location is in proximity to a navigation facility and may impact the assurance of a navigation signal reception. The Federal Aviation Administration, in accordance with 77.9, requests that the project is filed.

Survey Requirements >>

Full survey is anticipated for the limits of the project. Survey limits will extend approximately 1800 ft along US 20 centered at the structure location. The survey will include a width up to existing right-of-way. Coordination with the railroad will be required for survey within the railroad right-of-way.

Right of Way Impacts >>

The existing apparent right-of-way west of the bridge is 80 ft left and 100 ft right while east of the bridge is 100.5 ft left and 99.5 ft right of the centerline according to existing plans. The project lies within the right-of-way limits of the IHB Railroad. A preliminary records search was not completed for this project. This project is not expected to require the purchase of temporary right-of-way or permanent right-of-way.

Railroad Impacts >>

There is one railroad crossing with three tracks within the project limits. The IHB Railroad crosses US 20 at 41°36'36" North, 87°27'42" West, and is crossing #326965M. This crossing is under US 20 and will require significant coordination with IHB Railroad. The Utility and Railroad Division may negotiate a construction agreement with the railroad's engineering department. Railroad coordination will follow the INDOT policy. The design of temporary shoring will be required for new pier construction adjacent to the railroad tracks.

Utility Impacts >>

Based on the existing plans, various utilities appear to exist within the project limits. A list of potential existing utilities believed to be located within the proposed project limits are provided in Appendix B. Wolverine Pipeline, Buckeye (Pipeline), BP Pipeline, West Shore Pipeline, and Marathon Pipeline were all included as potential utilities located in the area. Any pipeline crossings will require additional coordination due to possible restrictions associated with working above pipelines. There are visible overhead lines crossing the bridge in multiple location and also running parallel to the bridge. Existing plans show telephone conduit affixed to the bridge. Additional utilities will be identified during the utility coordination for the project that will be performed by the District or the District's consultant.

Related Projects >>

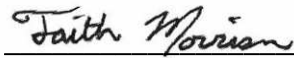
There are no known related projects at this time. Coordination with the District and local public agencies will be required as this project progresses.

Coordination, Meetings >>

A meeting with District staff was held on February 19, 2019 to discuss this project and other projects within this On-Call Engineering Assessment Services Contract.

Concurrence >>

This document was prepared by:

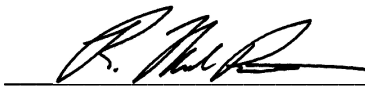
DATE: 17 September 2019

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Paul South, PE
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Recommend: APPROVAL / ~~DISAPPROVAL~~

Approved by:

Steve J
Benczik

Digitally signed by Steve J
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Date: 2020.01.15
15:15:21 -06'00'

DATE: _____

Steve Benczik, PE
System Asset Manager, LaPorte District

EJ Analysis

Under FHWA Order 6640.23A, FHWA and the project sponsor, as a recipient of funding from FHWA, are responsible to ensure that their programs, policies, and activities do not have a disproportionately high and adverse effect on minority or low-income populations. Per the current INDOT *Categorical Exclusion Manual*, an EJ analysis is required for any project requiring two or more relocations or more than 0.5 acre of new permanent right-of-way (ROW). Because the project is expected to require more than 0.5 acre of new permanent ROW (approximately 1.12 acre), an EJ analysis was conducted. Relocations are not anticipated to occur for this project.

Potential EJ impacts are detected by locating minority populations and low-income populations in and near the project area, calculating their percentage in the area relative to a reference population to determine if, in fact, populations of EJ concern do exist, and determining whether there will be disproportionate adverse impacts to them. The reference population may be a county, city, or town and is called the community of comparison (COC). For this project the COC is North Township in Lake County, Indiana. North Township was chosen as the COC because the project area is divided by municipal limits separating two municipalities - the City of East Chicago and the City of Hammond. The community that overlaps the project limits is called the affected community (AC). For this project there are three ACs. AC1 is Census Tract 209, AC2 is Census Tract 210, and AC3 is Census Tract 306.

An AC has a population of concern for EJ if the population is more than 50% low-income or minority or if the low-income population or minority population is greater than 125% of the same populations in the COC. Data from the American Community Survey five-year estimates (2017-2021) was obtained from the U.S. Census Bureau's webpage (<https://data.census.gov/cedsci/>) on November 20, 2023, by Lochmueller Group. The data collected for minority and low-income populations within the ACs is summarized in the below table.

Table 1. Low-Income and Minority 5-Year Census Data. 2021.

	COC	AC1	AC2	AC3
	North Township, Lake County, IN	Census Tract 209, Lake County, IN	Census Tract 210, Lake County, IN	Census Tract 306, Lake County, IN
LOW-INCOME POPULATION				
Total Population for Whom Poverty Status is Determined	154,348	3,545	6,481	4,177
Total Population Below Poverty Level	28,068	833	1,262	925
Percent Low-Income	18.2%	23.5%	19.5%	22.1%
125 Percent of COC	22.7%			
AC Percent Low-Income Greater Than 125 Percent of COC?		YES	NO	NO
AC Percent Low-Income Greater Than 50 Percent?		NO	NO	NO
Population of EJ Concern?		YES	NO	NO
MINORITY POPULATION				
Total Population	155,976	4,140	6,481	4,231
Minority Population	90,360	2,479	3,368	3,592
Percent Minority	57.9%	59.9%	52.0%	84.9%
125 Percent of COC	72.4%			
AC Percent Minority Greater Than 125 Percent of COC?		NO	NO	YES
AC Percent Minority Greater Than 50 Percent?		YES	YES	YES

AC1, Census Tract 209, has a percent low-income of 23.5%, which is below 50% and is above the 125% COC threshold of 22.7%. Therefore, AC1 contains a low-income population of EJ concern. AC2, Census Tract 210, has a percent low-income of 19.5%, which is below 50% and is below the 125% COC threshold of 22.7%. Therefore, AC2 does not contain low-income populations of EJ concern. AC3, Census Tract 306, has a percent low-income of 22.1%, which is below 50% and is below the 125% COC threshold of 22.7%. Therefore, AC3 does not contain low-income populations of EJ concern.

AC1, Census Tract 209, has a percent minority of 59.9%, which is above 50% and is below the 125% COC threshold of 72.4%. Therefore, AC1 contains a minority population of EJ concern. AC2, Census Tract 210, has a percent minority of 52.0%, which is above 50% and is below the 125% COC threshold of 72.4%. Therefore, AC2 contains a minority population of EJ concern. AC3, Census Tract 306, has a percent minority of 84.9%, which is above 50% and is above the 125% COC threshold of 72.4%. Therefore, AC3 contains a minority population of EJ concern.

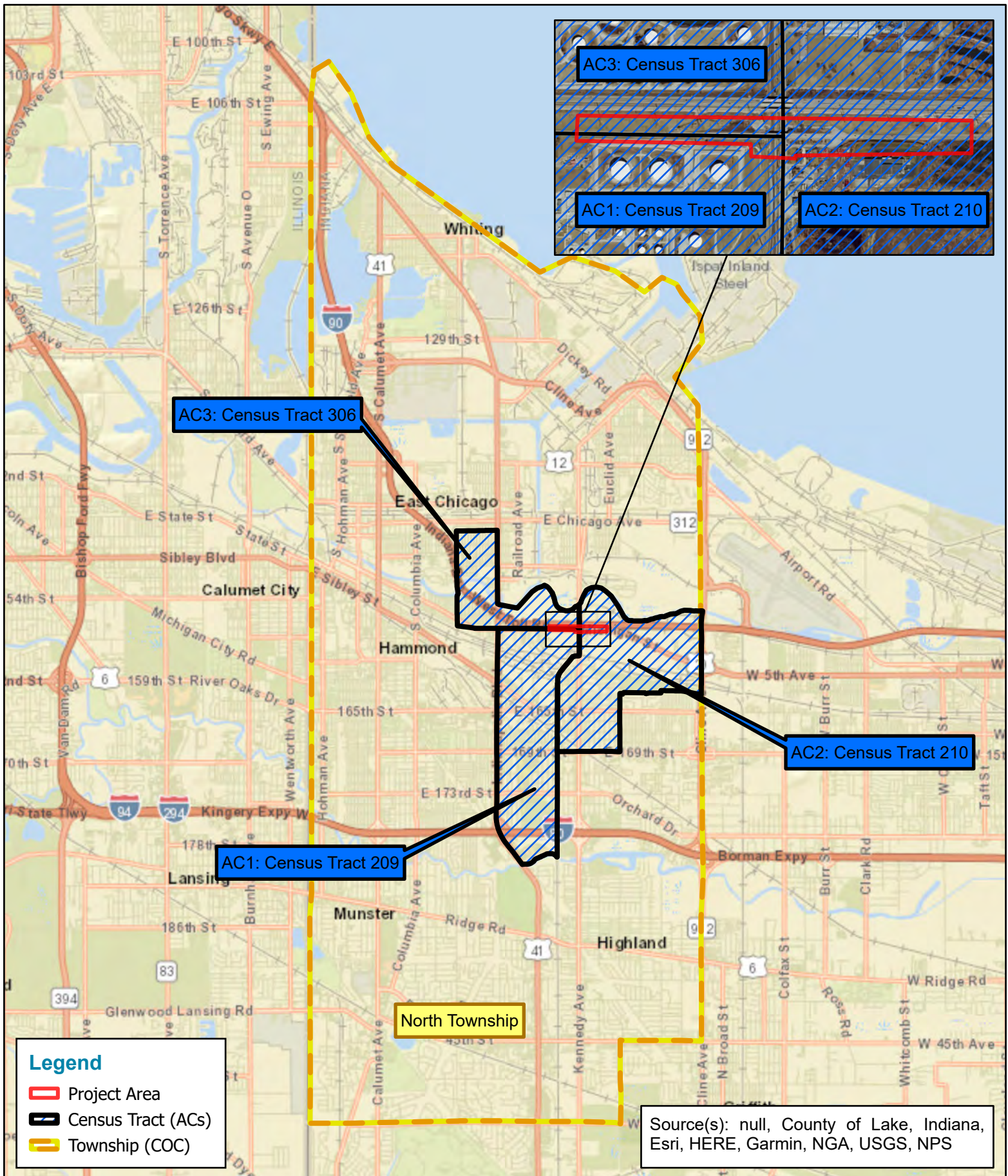
The proposed project is expected to require the acquisition of approximately 1.12 acres of permanent ROW. This ROW will be acquired from the immediate property frontage nearest US 20 and the access roads east of Kennedy Avenue. This ROW will be acquired from within ACs containing populations of EJ concern. Permanent ROW will be acquired from an industrial parcel,

a Hammond municipally-owned parcel used for public utility distribution, and parcels used for private utility distribution. Permanent ROW will not be acquired from residential parcels. Moreover, the small amounts of ROW being acquired from these parcels' roadway frontages will not affect their use or function. Land use within the proposed permanent ROW consists of maintained open fields, wetlands, an industrial equipment storage area, and a lawn area surrounding a municipal building.

The negative impacts to property owners within the project area will be minimal and consist primarily of short-term construction impacts and the loss of portions of ROW along US 20. No relocations are anticipated. The ROW to be acquired will not substantially diminish the remaining properties' existing use or function.

The MOT plan, which is still being finalized, will involve a closure and detour for US 20, lane closures for Kennedy Ave., and potential intermittent closures of Kennedy Ave. The detour route for US 20 will utilize US 20/Indianapolis Blvd. (one mile west of the bridge), Columbus Dr./US 12, and US 12. The detour route is expected to result in 5.4 additional travel miles and 14 additional minutes of travel time. Lane closures on Kennedy Ave. will take place in two phases. Phase I will involve the closure of the southbound lanes and shift southbound traffic to the left (west) northbound lane. Phase II will involve closure of the northbound lanes and shift northbound traffic to the right (east) southbound lane. One lane of traffic in both directions will be maintained. If required, the detour route for intermittent closure of Kennedy Ave. will utilize E 165th St., US 20/ Indianapolis Blvd., and W Chicago Ave./SR 312. The detour route is expected to result in 1.7 additional travel miles and 5 additional minutes of travel time. The sidewalk located along the east side of Kennedy Ave. will be closed for the duration of the MOT. A pedestrian MOT for the sidewalk closure east of Kennedy Ave. has not been developed at this time, however the need for a pedestrian MOT is still currently being investigated. Should one be required, a pedestrian detour would be in place for the duration of the MOT. The MOT is expected to begin in April of 2026 and be in place for 18 months. Access will be maintained for property owners during construction of the project.

The project will benefit any EJ community in this area due to the improved condition of the bridge and the added non-vehicular connectivity associated with the new trail. It is expected that the project will not have a disproportionately high and adverse environmental impact to low-income or minority populations of EJ concern when compared to non-EJ populations.



112 West Jefferson Blvd., Suite 500
South Bend, IN 46601
Phone: (574) 334-5460

Environmental Justice Map

Des. No. 1900009

0 1.25 2.5
Miles



County: Lake
Township: North
State: Indiana

US 20 over Kennedy Ave.
Bridge Project, 0.99 Mi. E. of SR 152
Created: 11/22/2023, CHOogewerf

HISPANIC OR LATINO ORIGIN BY RACE		
Note: The table shown may have been modified by user selections. Some information may be missing.		
DATA NOTES		
TABLE ID:	B03002	
SURVEY/PROGRAM:	American Community Survey	
VINTAGE:	2021	
DATASET:	ACSDT5Y2021	
PRODUCT:	ACS 5-Year Estimates Detailed Tables	
UNIVERSE:	Total population	
MLA:	U.S. Census Bureau. "HISPANIC OR LATINO ORIGIN BY RACE." American Community Survey, ACS 5-Year Estimates Detailed Tables, Table B03002, 2021, https://data.census.gov/table/ACSDT5Y2021.B03002?q=B03002&g=060XX00US1808954414_1400000US18089020900,18089021000,18089030600	
FTP URL:	None	
API URL:	https://api.census.gov/data/2021/acs/acs5	
USER SELECTIONS		
TABLES	B03002	
GEOS	North township, Lake County, Indiana; Census Tract 209; Lake County; Indiana; Census Tract 210; Lake County; Indiana; Census Tract 306; Lake County; Indiana	
EXCLUDED COLUMNS	North township, Lake County, Indiana!!Margin of Error Census Tract 209, Lake County, Indiana!!Margin of Error Census Tract 210, Lake County, Indiana!!Margin of Error Census Tract 306, Lake County, Indiana!!Margin of Error	
APPLIED FILTERS	None	
APPLIED SORTS	None	
PIVOT & GROUPING		
PIVOT COLUMNS	None	
PIVOT MODE	Off	
ROW GROUPS	None	
VALUE COLUMNS	None	
WEB ADDRESS	https://data.census.gov/table/ACSDT5Y2021.B03002?q=B03002&g=060XX00US1808954414_1400000US18089020900,18089021000,18089030600	
TABLE NOTES	<p>Although the American Community Survey (ACS) produces population, demographic and housing unit estimates, it is the Census Bureau's Population Estimates Program that produces and disseminates the official estimates of the population for the nation, states, counties, cities, and towns and estimates of housing units for states and counties.</p> <p>Supporting documentation on code lists, subject definitions, data accuracy, and statistical testing can be found on the American Community Survey website in the Technical Documentation section.</p> <p>Sample size and data quality measures (including coverage rates, allocation rates, and response rates) can be found on the American Community Survey website in the Methodology section.</p> <p>Source: U.S. Census Bureau, 2017-2021 American Community Survey 5-Year Estimates</p>	

	Data are based on a sample and are subject to sampling variability. The degree of uncertainty for an estimate arising from sampling variability is represented through the use of a margin of error. The value shown here is the 90 percent margin of error. The margin of error can be interpreted roughly as providing a 90 percent probability that the interval defined by the estimate minus the margin of error and the estimate plus the margin of error (the lower and upper confidence bounds) contains the true value. In addition to sampling variability, the ACS estimates are subject to nonsampling error (for a discussion of nonsampling variability, see ACS Technical Documentation). The effect of nonsampling error is not represented in these tables.
	The Hispanic origin and race codes were updated in 2020. For more information on the Hispanic origin and race code changes, please visit the American Community Survey Technical Documentation website.
	The 2017-2021 American Community Survey (ACS) data generally reflect the March 2020 Office of Management and Budget (OMB) delineations of metropolitan and micropolitan statistical areas. In certain instances, the names, codes, and boundaries of the principal cities shown in ACS tables may differ from the OMB delineation lists due to differences in the effective dates of the geographic entities.
	Estimates of urban and rural populations, housing units, and characteristics reflect boundaries of urban areas defined based on Census 2010 data. As a result, data for urban and rural areas from the ACS do not necessarily reflect the results of ongoing urbanization.
	Explanation of Symbols:- The estimate could not be computed because there were an insufficient number of sample observations. For a ratio of medians estimate, one or both of the median estimates falls in the lowest interval or highest interval of an open-ended distribution. For a 5-year median estimate, the margin of error associated with a median was larger than the median itself.N The estimate or margin of error cannot be displayed because there were an insufficient number of sample cases in the selected geographic area. (X) The estimate or margin of error is not applicable or not available.median- The median falls in the lowest interval of an open-ended distribution (for example "2,500-")median+ The median falls in the highest interval of an open-ended distribution (for example "250,000+").** The margin of error could not be computed because there were an insufficient number of sample observations.*** The margin of error could not be computed because the median falls in the lowest interval or highest interval of an open-ended distribution.***** A margin of error is not appropriate because the corresponding estimate is controlled to an independent population or housing estimate. Effectively, the corresponding estimate has no sampling error and the margin of error may be treated as zero.
COLUMN NOTES	None

Table: ACSDT5Y2021.B03002

	North township, Lake County, Indiana	Census Tract 209, Lake County, Indiana	Census Tract 210, Lake County, Indiana	Census Tract 306, Lake County, Indiana
Label	Estimate	Estimate	Estimate	Estimate
Total:	155,976	4,140	6,481	4,231
Not Hispanic or Latino:	100,083	3,449	3,433	1,383
White alone	65,616	1,661	3,113	639
Black or African American alone	29,184	1,524	320	744
American Indian and Alaska Native alone	408	22	0	0
Asian alone	2,724	153	0	0
Native Hawaiian and Other Pacific Islander alone	20	0	0	0
Some other race alone	122	0	0	0
Two or more races:	2,009	89	0	0
Two races including Some other race	176	6	0	0
Two races excluding Some other race, and three or more races	1,833	83	0	0
Hispanic or Latino:	55,893	691	3,048	2,848
White alone	15,784	243	630	1,028
Black or African American alone	718	7	0	25
American Indian and Alaska Native alone	402	0	49	10
Asian alone	20	8	0	0
Native Hawaiian and Other Pacific Islander alone	0	0	0	0
Some other race alone	30,571	218	1,969	1,283
Two or more races:	8,398	215	400	502
Two races including Some other race	7,262	141	302	407
Two races excluding Some other race, and three or more races	1,136	74	98	95

POVERTY STATUS IN THE PAST 12 MONTHS BY SEX BY AGE		United States [®] Census Bureau
Note: The table shown may have been modified by user selections. Some information may be missing.		
DATA NOTES		
TABLE ID:	B17001	
SURVEY/PROGRAM:	American Community Survey	
VINTAGE:	2021	
DATASET:	ACSDT5Y2021	
PRODUCT:	ACS 5-Year Estimates Detailed Tables	
UNIVERSE:	Population for whom poverty status is determined	
MLA:	U.S. Census Bureau. "POVERTY STATUS IN THE PAST 12 MONTHS BY SEX BY AGE." American Community Survey, ACS 5-Year Estimates Detailed Tables, Table B17001, 2021, https://data.census.gov/table/ACSDT5Y2021.B17001?q=B17001: Poverty Status in the Past 12 Months by Sex by Age&g=060XX00US1808954414_1400000US18089020900,18089021000,18089030600 . Accessed on November 20, 2023.	
FTP URL:	None	
API URL:	https://api.census.gov/data/2021/acs/acs5	
USER SELECTIONS		
TABLES	B17001	
GEOS	Census Tract 210; Lake County; Indiana; Census Tract 209; Lake County; Indiana; Census Tract 306; Lake County; Indiana; North township, Lake County, Indiana	
EXCLUDED COLUMNS	North township, Lake County, Indiana!!Margin of Error Census Tract 209, Lake County, Indiana!!Margin of Error Census Tract 210, Lake County, Indiana!!Margin of Error Census Tract 306, Lake County, Indiana!!Margin of Error	
APPLIED FILTERS	None	
APPLIED SORTS	None	
PIVOT & GROUPING		
PIVOT COLUMNS	None	
PIVOT MODE	Off	
ROW GROUPS	None	
VALUE COLUMNS	None	
WEB ADDRESS	https://data.census.gov/table/ACSDT5Y2021.B17001?q=B17001:%20Poverty%20Status%20in%20the%20Past%2012%20Months%20by%20Sex%20by%20Age&g=060XX00US1808954414_1400000US18089020900,18089021000,18089030600	
TABLE NOTES	<p>Although the American Community Survey (ACS) produces population, demographic and housing unit estimates, it is the Census Bureau's Population Estimates Program that produces and disseminates the official estimates of the population for the nation, states, counties, cities, and towns and estimates of housing units for states and counties.</p> <p>Supporting documentation on code lists, subject definitions, data accuracy, and statistical testing can be found on the American Community Survey website in the Technical Documentation section.</p> <p>Sample size and data quality measures (including coverage rates, allocation rates, and response rates) can be found on the American Community Survey website in the Methodology section.</p> <p>Source: U.S. Census Bureau, 2017-2021 American Community Survey 5-Year Estimates</p>	

	Data are based on a sample and are subject to sampling variability. The degree of uncertainty for an estimate arising from sampling variability is represented through the use of a margin of error. The value shown here is the 90 percent margin of error. The margin of error can be interpreted roughly as providing a 90 percent probability that the interval defined by the estimate minus the margin of error and the estimate plus the margin of error (the lower and upper confidence bounds) contains the true value. In addition to sampling variability, the ACS estimates are subject to nonsampling error (for a discussion of nonsampling variability, see ACS Technical Documentation). The effect of nonsampling error is not represented in these tables.
	The 2017-2021 American Community Survey (ACS) data generally reflect the March 2020 Office of Management and Budget (OMB) delineations of metropolitan and micropolitan statistical areas. In certain instances, the names, codes, and boundaries of the principal cities shown in ACS tables may differ from the OMB delineation lists due to differences in the effective dates of the geographic entities.
	Estimates of urban and rural populations, housing units, and characteristics reflect boundaries of urban areas defined based on Census 2010 data. As a result, data for urban and rural areas from the ACS do not necessarily reflect the results of ongoing urbanization.
	Explanation of Symbols:- The estimate could not be computed because there were an insufficient number of sample observations. For a ratio of medians estimate, one or both of the median estimates falls in the lowest interval or highest interval of an open-ended distribution. For a 5-year median estimate, the margin of error associated with a median was larger than the median itself.N The estimate or margin of error cannot be displayed because there were an insufficient number of sample cases in the selected geographic area. (X) The estimate or margin of error is not applicable or not available.median- The median falls in the lowest interval of an open-ended distribution (for example "2,500-")median+ The median falls in the highest interval of an open-ended distribution (for example "250,000+").** The margin of error could not be computed because there were an insufficient number of sample observations.*** The margin of error could not be computed because the median falls in the lowest interval or highest interval of an open-ended distribution.***** A margin of error is not appropriate because the corresponding estimate is controlled to an independent population or housing estimate. Effectively, the corresponding estimate has no sampling error and the margin of error may be treated as zero.
COLUMN NOTES	None

	North township, Lake County, Indiana	Census Tract 209, Lake County, Indiana	Census Tract 210, Lake County, Indiana	Census Tract 306, Lake County, Indiana
Label	Estimate	Estimate	Estimate	Estimate
Total:	154,348	3,545	6,481	4,177
Income in the past 12 months below poverty level:	28,068	833	1,262	925
Male:	12,120	391	518	346
Under 5 years	1,496	163	69	44
5 years	356	0	0	5
6 to 11 years	1,722	31	70	142
12 to 14 years	898	14	21	44
15 years	324	0	0	0
16 and 17 years	440	0	39	28
18 to 24 years	1,161	57	40	44
25 to 34 years	1,178	26	58	0
35 to 44 years	1,458	52	57	0
45 to 54 years	984	8	89	10
55 to 64 years	1,098	10	19	0
65 to 74 years	733	30	56	14
75 years and over	272	0	0	15
Female:	15,948	442	744	579
Under 5 years	1,405	0	91	10
5 years	323	11	28	0
6 to 11 years	1,584	61	10	67
12 to 14 years	919	22	134	42
15 years	354	5	43	11
16 and 17 years	506	0	0	71
18 to 24 years	1,729	94	21	18
25 to 34 years	2,313	12	83	85
35 to 44 years	2,587	181	158	110
45 to 54 years	1,206	30	103	116
55 to 64 years	1,665	14	21	30
65 to 74 years	658	12	18	9
75 years and over	699	0	34	10
Income in the past 12 months at or above poverty level:	126,280	2,712	5,219	3,252
Male:	63,460	1,384	2,431	1,739
Under 5 years	3,364	47	127	98
5 years	604	0	20	52
6 to 11 years	4,088	54	151	91
12 to 14 years	2,604	41	193	75
15 years	883	15	10	36
16 and 17 years	1,647	37	77	56
18 to 24 years	5,882	161	253	163
25 to 34 years	8,876	199	371	368
35 to 44 years	8,738	200	305	234
45 to 54 years	8,689	223	136	206
55 to 64 years	9,319	189	555	201
65 to 74 years	5,283	116	193	109
75 years and over	3,483	102	40	50
Female:	62,820	1,328	2,788	1,513
Under 5 years	2,792	23	185	74
5 years	436	14	25	0
6 to 11 years	4,372	113	228	79
12 to 14 years	2,480	48	139	55
15 years	871	13	23	0
16 and 17 years	1,864	14	197	86
18 to 24 years	5,296	123	176	167
25 to 34 years	7,301	195	386	172
35 to 44 years	8,315	132	365	201
45 to 54 years	8,644	193	292	291
55 to 64 years	9,492	220	494	120
65 to 74 years	6,040	115	212	218
75 years and over	4,917	125	66	50

Calculation Table

	<u>COC</u>	<u>AC1</u>	<u>AC2</u>	<u>AC3</u>
	North Township, Lake County, IN	Census Tract 209, Lake County, IN	Census Tract 210, Lake County, IN	Census Tract 306, Lake County, IN
LOW INCOME				
Total Population for Whom Poverty Status is Determined	154,348	3,545	6,481	4,177
Income in the past 12 months below poverty level:	28,068	833	1,262	925
Percent Low-Income	18.2%	23.5%	19.5%	22.1%
125% Reference Increment (Applied to COC Only and Compared Against the AC)	22.7%			
AC Percent Low-Income > 125% of COC?		YES	NO	NO
AC Percent Low-Income > 50%?		NO	NO	NO
Elevated Low-Income Population Present?		YES	NO	NO
MINORITY				
Total Population:	155,976	4,140	6,481	4,231
Not Hispanic or Latino:	100,083	3,449	3,433	1,383
White alone	65,616	1,661	3,113	639
Black or African American alone	29,184	1,524	320	744
American Indian and Alaska Native alone	408	22	0	0
Asian alone	2,724	153	0	0
Native Hawaiian and Other Pacific Islander alone	20	0	0	0
Some other race alone	122	0	0	0
Two or more races:	2,009	89	0	0
Two races including Some other race	176	6	0	0
Two races excluding Some other race, and three or more races	1,833	83	0	0
Hispanic or Latino:	55,893	691	3,048	2,848
White alone	15,784	243	630	1,028
Black or African American alone	718	7	0	25
American Indian and Alaska Native alone	402	0	49	10
Asian alone	20	8	0	0
Native Hawaiian and Other Pacific Islander alone	0	0	0	0
Some other race alone	30,571	218	1,969	1,283
Two or more races:	8,398	215	400	502
Two races including Some other race	7,262	141	302	407
Two races excluding Some other race, and three or more races	1,136	74	98	95
Number Non-White / Minority (Total Pop - Not Hisp or Lat: White Alone)	90,360	2,479	3,368	3,592
Percent Non-White / Minority	57.9%	59.9%	52.0%	84.9%
125% Reference Increment (Applied to COC Only and Compared Against the AC)	72.4%			
AC Percent Minority > 125% of COC?		NO	NO	YES
AC Percent Minority > 50%?		YES	YES	YES
Elevated Minority Population Present?		YES	YES	YES

Carson Hoogewerf

From: Fair, Terri <TFair@indot.IN.gov>
Sent: Wednesday, February 21, 2024 11:00 AM
To: Carson Hoogewerf
Cc: Michels, Stewart
Subject: FW: US 20 over Kennedy Ave. Bridge Project - Des. No. 1900009 - Environmental Justice Analysis
Attachments: DesNo1900009_US 20 Bridge_DRAFT EJ Analysis_20240220.pdf

EXTERNAL

INDOT-Environmental Services Division (ESD) has reviewed the project information along with the Environmental Justice (EJ) Analysis for the above referenced project. With the information provided, the project may require right-of-way, requires no relocations, and would not disrupt community cohesion or create a physical barrier. With the information provided, INDOT-ESD would not consider the impacts associated with this project as causing a disproportionately high and adverse effect on minority and/or low-income populations of EJ concern relative to non-EJ populations in accordance with the provisions of Executive Order 12898 and FHWA Order 6640.23a. No further EJ Analysis is required.