

**FHWA-Indiana Environmental Document**  
**CATEGORICAL EXCLUSION / ENVIRONMENTAL ASSESSMENT FORM**  
**GENERAL PROJECT INFORMATION**

<b>Road No./County:</b>	State Road 46/Dearborn County
<b>Designation Number(s):</b>	1383721
<b>Project Description/Termini:</b>	Bridge Rehabilitation, 0.44 mile west of United States (US) 52 over Whitewater River, Approximately 815 Feet East and 835 Feet West of the Center of the Bridge

	<b>Categorical Exclusion, Level 2</b> – Required Signatories: INDOT DE and/or INDOT ESD
	<b>Categorical Exclusion, Level 3</b> – Required Signatories: INDOT ESD
<b>X</b>	<b>Categorical Exclusion, Level 4</b> – Required Signatories: INDOT ESD and FHWA
	<b>Environmental Assessment (EA)</b> – Required Signatories: INDOT ESD and FHWA
	<b>Additional Investigation (AI)</b> – The proposed action included a design change from the original approved environmental document. Required Signatories must include the appropriate environmental approval authority

**Approval**

INDOT DE Signature and Date

INDOT ESD Signature and Date

FHWA Signature and Date

**Release for Public Involvement**

N/A

INDOT DE Initials and Date

*ADWP*

March 15, 2023

INDOT ESD Initials and Date

**Certification of Public Involvement**

INDOT Consultant Services Signature and Date

INDOT DE/ESD Reviewer Signature and Date:

\_\_\_\_\_

Name and Organization of CE/EA Preparer:

Kristin Wing - Beam, Longest and Neff

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### Part I – Public Involvement

Every Federal action requires some level of public involvement, providing for early and continuous opportunities throughout the project development process. **The level of public involvement should be commensurate with the proposed action.**

	Yes	No
Does the project have a historic bridge processed under the Historic Bridges PA*?	<input checked="" type="checkbox"/> X	<input type="checkbox"/>
If No, then:		
Opportunity for a Public Hearing Required?	<input type="checkbox"/>	<input type="checkbox"/>

*\*A public hearing is required for all historic bridges processed under the Historic Bridges Programmatic Agreement between INDOT, FHWA, SHPO, and the ACHP.*

*Discuss what public involvement activities (legal notices, letters to affected property owners and residents (i.e., notice of entry), meetings, special purpose meetings, newspaper articles, etc.) have occurred for this project.*

Notice of Entry letters were mailed to potentially affected property owners near the project area on September 19, 2019, notifying them about the project and that individuals responsible for land surveying and field activities may be seen in the area. A sample copy of the Notice of Entry letter is included in Appendix G, page 1.

#### Section 106

To meet the public involvement requirements of Section 106, a legal notice of FHWA's finding of "No Adverse Effect" was published in the Dearborn County Register on March 8, 2022, offering the public an opportunity to submit comment pursuant to 36 CFR 800.2(d), 800.3(e), and 800.6(a)(4). The public comment period closed 30 days later, on April 8, 2022. The text of the public notice and the affidavit of publication appear in Appendix D, pages 51-53. No comments were received.

#### Project Does Meet

The project will meet the minimum requirements described in the current *Indiana Department of Transportation (INDOT) Public Involvement Manual*, which requires the project sponsor to offer the public an opportunity to submit comments and/or request a public hearing. Therefore, a legal notice will appear in a local publication contingent upon the release of this document for public involvement. This document will be revised after the public involvement requirements are fulfilled.

#### Historic Bridge

Pursuant to the Historic Bridge Programmatic Agreement (PA), a public hearing is required. A legal notice will appear in a local publication contingent upon the release of this document for public involvement. This document will be revised after the public involvement requirements are fulfilled.

### **Public Controversy on Environmental Grounds**

*Discuss public controversy concerning community and/or natural resource impacts, including what is being done during the project to minimize impacts.*

#### No controversy

At this time, there is no substantial public controversy concerning impacts to the community or to natural resources.

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## Part II - General Project Identification, Description, and Design Information

Sponsor of the Project: Indiana Department of Transportation, Seymour District INDOT District: Seymour

Local Name of the Facility: State Road 46

Funding Source (*mark all that apply*):      Federal       State       Local       Other\*

\*If other is selected, please identify the funding source: \_\_\_\_\_

### **PURPOSE AND NEED:**

*The need should describe the specific transportation problem or deficiency that the project will address. The purpose should describe the goal or objective of the project. The solution to the traffic problem should NOT be discussed in this section.*

#### **Need:**

The project need is evidenced by the heavy deterioration and distress exhibited by the bridge's truss members, stringers, floor beams, and deck. The deterioration noted has resulted in a deck condition rating of fair (5) and a superstructure condition rating of poor (4), both ratings are out of 9 (excellent) according to the September 8, 2021, INDOT Bridge Inspection Report (Appendix C, page 38).

#### **Purpose:**

The purpose is to correct the deficiencies and improve the overall condition of the bridge's deck and superstructure to satisfactory (6) or better and improve the structural capacity of the truss members and floor system so that all operating (legal) loads have rating factors greater than 1.0. A load rating factor of 1.0 or greater signifies that the bridge is structurally adequate for the vehicle analyzed, and no load posting is required.

### **PROJECT DESCRIPTION (PREFERRED ALTERNATIVE):**

County: Dearborn      Municipality: \_\_\_\_\_

Limits of Proposed Work: Approximately 815 feet east and 835 feet west of the center of the bridge – SR 46 over Whitewater River

Total Work Length: 0.180 Mile(s)      Total Work Area: 2.4 Acre(s)

<b>Yes<sup>1</sup></b>	<b>No</b>
<input type="checkbox"/>	<input checked="" type="checkbox"/>
Date: _____	

Is an Interstate Access Document (IAD)<sup>1</sup> required?

If yes, when did the FHWA provide a Determination of Engineering and Operational Acceptability?

*<sup>1</sup>If an IAD is required, a copy of the approved CE/EA document must be submitted to the FHWA with a request for final approval of the IAD.*

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*Describe location of project, including township, range, city, county, roads, etc. Existing conditions should include current conditions, current deficiencies, roadway description, surrounding features, etc. Preferred alternative should include the scope of work, anticipated impacts, and how the project will meet the Purpose and Need. Logical termini and independent utility also need discussed.*

The Indiana Department of Transportation (INDOT) and the Federal Highway Administration (FHWA) intend to proceed with a bridge rehabilitation on State Road (SR) 46 over Whitewater River.

**Location:**

The project is on SR 46, approximately 0.44 mile west of US 52 over Whitewater River. Specifically, the project is in the southwest quarter of Sections 9 and 10, Township 7 North, Range 1 West in Logan and Harrison Townships, Dearborn County, Indiana, as shown on the 7.5 Minute West Harrison, Indiana United States Geological Survey (USGS) Quadrangle Map (Appendix B, page 2).

**Existing Conditions:**

Bridge No. 046-15-01987 A (NBI #017540) is a 5-span Parker Through steel truss structure constructed in 1937. It is 886 feet long and has five equal 175-foot-long spans. The bridge was rehabilitated in 1985 and painted in 1996 and perpetuates drainage under SR 46 for Whitewater River. The clear roadway width on the bridge measures 24 feet with two 10-foot-wide lanes (one in each direction) with 2-foot-wide shoulders (1 foot paved, 1 foot unpaved). The underside of the bridge has transverse cracking with efflorescence, delaminations, and concrete spalling with exposed steel reinforcement. All joints are leaking, and many of the deck drain extensions are damaged or missing. There is cracking throughout the wearing surface, as well as spalling at numerous spans. The truss members have numerous holes, moderate to heavy pitting, expansion rust and section loss throughout (Appendix C, page 38). The deterioration noted has resulted in a deck condition rating of fair (5) and a superstructure condition rating of poor (4), both ratings are out of 9 (excellent) according to the September 8, 2021, INDOT Bridge Inspection Report (Appendix C, page 38).

The existing SR 46 roadway approach consists of two 12-foot-wide travel lanes with 2-foot-wide shoulders (1 foot paved, 1 foot unpaved). The existing roadway facility is classified as a *State Collector* within a Rural area and is not on the US National Highway System or National Truck Network. The posted speed limit at the project location is 55 miles per hour (MPH).

**Preferred Alternative:**

Alternative B1: Rehabilitation of the Existing Bridge for Continued Vehicle Use (Two-Way Option) Meeting Secretary of Interior's Standards for Rehabilitation

The preferred alternative includes the rehabilitation of the existing bridge and the east and west approach roadway to current INDOT and FHWA criteria for load capacity and condition. The concrete deck, deck drains and metal forms in all spans will be replaced, and the new deck will incorporate a PF-1 bridge railing. Existing steel bridge posts and portions of the existing steel handrailing that have holes will be replaced in-kind. The existing concrete bridge railing transitions will be replaced, and the bridge joints will be replaced with pre-compressed foam joints or an INDOT-approved joint. New approach slabs will be placed, with milling and surfacing taking place. Full-depth pavement will be placed at the ends of the approach slabs to tie the resurfacing work to the concrete approach slabs.

Deteriorated truss members (low chords, diagonals, verticals, and end posts) with holes 2 inches in diameter and larger will be repaired or replaced to restore lost section and improve condition. Holes smaller than 2 inches may either be ignored or have a bolt placed through them. Truss members will be repaired by placing a plate over the deteriorated area, or the members will be replaced in-kind, and connection plates and lattice bars with heavy section loss and holes will be replaced in-kind. Lower lateral bracing and stay plates with heavy section loss will be repaired or replaced in-kind, and the existing gusset plates that have areas of heavy deterioration will be replaced in-kind. Stringers in poor condition or that have heavy section loss will be replaced in-kind or repaired, and floor beams with heavy section loss will be repaired. Rivets that are removed to make necessary repairs will be replaced with round-headed bolts. Expansion bearings that have moved or are over-rotated will be reset to the proper orientation, and deteriorated anchor bolts or nuts, and portions of the bearing assemblies will be replaced in-kind. All areas of the truss will be cleaned and painted and will match the existing color, and the concrete in the low chord of Span B will be removed.

Deteriorated concrete caps of Pier 5 and the East Abutment will be removed and reconstructed, and any delaminated and spalled areas of abutments and piers will be patched. The scour countermeasures, if required, shall be placed at the appropriate substructure units, and any debris that is lodged against the substructure units will be removed. No tree clearing is required for this project, but tree branches, brush, and other vegetation will be trimmed on both sides of the road to keep branches away from the truss and vehicles.

The new bridge deck out-to-out width will match the existing deck width; however, because PF-1 bridge rail will be used, the clear roadway will be reduced by 10 1/2 inches. The travel lanes and shoulder widths will be transitioned from the bridge (10 feet-0 inches lanes and 1 foot-6 3/4 inches shoulders) to the roadway approaches (12 foot-0 inch lanes and 2 foot-0 inch shoulders) at each bridge end. The rehabilitation will require approximately 250 feet of approach work east and west of the bridge. There will be 3 level

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one design exceptions required for this alternative (lane width, shoulder width, and bridge clear roadway width).

Improvements will result in increased deck and superstructure condition ratings (6 out of 9 or higher; satisfactory or higher) so that the maximum inspection frequency may be increased from 12 months to 24 months. In addition, this alternative will eliminate the level one criteria deficiency of substandard bridge railing and travel lane cross slope; however, the lane, shoulder, and bridge clear roadway widths will all remain substandard. These widths cannot be satisfied unless the bridge is widened. In order to widen the bridge, most truss members and the floor beams would be replaced. The widening option is not economical to construct and would cause the bridge to lose most of its historic integrity. Refer to Appendix I for more detail on the preferred alternative and its components.

The existing Duke Energy facilities that are attached to the north truss of the existing bridge will be relocated north of the bridge within the existing right of way. The relocated electric lines will be bored underground, starting approximately 835 feet west of the bridge centerline to about 815 feet east of the bridge centerline. The relocation will require clearing and grubbing brush and trimming some trees in the boring locations.

The existing buried Sycamore Gas line will be relocated further to the south within the existing right of way. The relocated gas line will be bored underground, starting approximately 415 feet west of the bridge centerline to about 415 feet east of the bridge centerline. The relocation will require clearing and grubbing brush and trimming some trees in the boring locations.

The project will not require acquisition of permanent or temporary right-of-way (ROW). Maintenance of traffic (MOT) will consist of a full closure with a 1.5-mile-long detour route utilizing US 52, I-74, and SR 1. The bridge will be closed for approximately eight months. No trees will be cleared. Construction will begin in August of 2024. Refer to the ROW and MOT sections below for further detail.

### **Logical Termini/Independent Utility:**

The project termini were developed to minimize impacts to the extent possible while also addressing the project need. The project area is approximately 815 feet east and 835 feet west of the center of the bridge and encompasses the construction area for a total work length of approximately 1,330 feet. This project has independent utility because it will improve existing bridge function as an independent project that is not dependent on another in the area.

### **OTHER ALTERNATIVES CONSIDERED:**

*Provide a header for each alternative. Describe all discarded alternatives, including the No Build Alternative. Explain why each discarded alternative was not selected. Make sure to state how each alternative meets or does not meet the Purpose and Need and why.*

#### **No Build/Do Nothing**

This alternative proposed utilization of existing facilities with no expenditure of capital funds or improvement of the roadway. The No-Build/Do-Nothing Alternative would not address the overall project purpose and need, which is to improve the overall condition rating of the bridge's superstructure to satisfactory (6 out of 9) or better and improve the structural capacity of the truss members and floor system so that all operating (legal) loads have rating factors greater than 1.0. If the No-Build/Do-Nothing Alternative were selected, the condition of the bridge would continue to decline, and the structural capacity would decrease and limit operating loads. Although this alternative is feasible, it is not prudent to allow the bridge to deteriorate until it is impassable, causing a significant community disruption and does not meet the purpose and need.

#### **Bridge Replacement**

This alternative was only used for cost comparison purposes and it was not considered as a feasible and prudent alternative.

Analysis of additional alternatives was not required since the feasible and prudent alternative was found with Alternative B1.

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**The No Build Alternative is not feasible, prudent or practicable because (Mark all that apply)**

It would not correct existing capacity deficiencies;  
 It would not correct existing safety hazards;  
 It would not correct the existing roadway geometric deficiencies;  
 It would not correct existing deteriorated conditions and maintenance problems; or  
 It would result in serious impacts to the motoring public and general welfare of the economy.  
 Other (Describe):

X

## ROADWAY CHARACTER:

*If the proposed action includes multiple roadways, complete and duplicate for each roadway.*

Name of Roadway	<u>SR 46</u>			
Functional Classification:	<u>State Collector</u>			
Current ADT:	<u>3,705</u>	VPD (2023)	Design Year ADT:	<u>4,169</u> VPD (2043)
Design Hour Volume (DHV):	<u>12%</u>	Truck Percentage (%)	<u>5%</u>	
Designed Speed (mph):	<u>55</u>	Legal Speed (mph):	<u>55</u>	

Existing	Proposed	
Number of Lanes:	2	2
Type of Lanes:	Travel	
Pavement Width:	26	26
Shoulder Width:	2	2
Median Width:	N/A	N/A
Sidewalk Width:	N/A	N/A
Setting:	<input type="checkbox"/>	Urban
Topography:	<input checked="" type="checkbox"/>	Level
	<input type="checkbox"/>	Suburban
	<input type="checkbox"/>	Rolling
	<input checked="" type="checkbox"/>	Rural
	<input type="checkbox"/>	Hilly

## BRIDGES AND/OR SMALL STRUCTURE(S):

*If the proposed action includes multiple structures, complete and duplicate for each bridge and/or small structure. Include both existing and proposed bridge(s) and/or small structure(s) in this section.*

Structure/NBI Number(s):	<u>Bridge No. 046-15-01987A / National Bridge Inventory (NBI) #017540</u>	Sufficiency Rating:	<u>35.4, INDOT Bridge Inspection Report 9/8/2021 (Appendix C, page 40)</u>
(Rating, Source of Information)			

Existing	Proposed	
Bridge/Structure Type:	Steel Truss	
Number of Spans:	5	5
Weight Restrictions:	N/A	ton
Height Restrictions:	N/A	ft.
Curb to Curb Width:	24	ft.
Outside to Outside Width:	25	ft.
Shoulder Width:	2	ft.
	N/A	ton
	N/A	ft.
	23.125	ft.
	25	ft.
	1.56	ft.

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Describe impacts and work involving bridge(s), culvert(s), pipe(s), and small structure(s). Provide details for small structure(s): structure number, type, size (length and dia.), location and impacts to water. Use a table if the number of small structures becomes large. If the table exceeds a complete page, put it in the appendix and summarize the information below with a citation to the table.

The project involves Bridge No. 046-15-01987 A (NBI #017540), a historic "Non-Select" bridge on State Road (SR) 46 over Whitewater River, approximately 0.44 mile west of US 52 near West Harrison, Indiana. The bridge is a 5-span, Parker through steel truss structure constructed in 1937, rehabilitated in 1985 and painted in 1996. The existing bridge has five equal 175-foot-long spans and perpetuates drainage under SR 46 for Whitewater River. The clear roadway width on the bridge measures 24 feet (two 10-foot-wide lanes with 2-foot-wide shoulders). The existing roadway approach consists of two 12-foot-wide travel lanes with 2-foot-wide shoulders. The existing roadway facility is classified as a State Collector that is in a Rural area and is not on the US National Highway System or National Truck Network. The posted speed limit at the project location is 55 miles per hour (MPH). This bridge will be rehabilitated according to the preferred alternative described above, following the Secretary of the Interior's (SOI) Standards for Rehabilitation. There are no other bridges or small structures located in the project area.

### MAINTENANCE OF TRAFFIC (MOT) DURING CONSTRUCTION:

Is a temporary bridge proposed?  Yes  No

Is a temporary roadway proposed?  Yes  No

Will the project involve the use of a detour or require a ramp closure? (describe below)  Yes  No

    Provisions will be made for access by local traffic and so posted.  Yes  No

    Provisions will be made for through-traffic dependent businesses.  Yes  No

    Provisions will be made to accommodate any local special events or festivals.  Yes  No

Will the proposed MOT substantially change the environmental consequences of the action?  Yes  No

Is there substantial controversy associated with the proposed method for MOT?  Yes  No

Discuss closures and/or facilities (if any) that will be provided for maintenance of traffic. Any known impacts from these temporary measures should be quantified to the extent possible, particularly with respect to properties such as Section 4(f) resources and wetlands. Any local concerns about access and traffic flow should be detailed as well.

The MOT for the project will require a full closure for approximately eight months with a 1.5-mile-long detour route utilizing US 52, I-74, and SR 1. Please see Appendix B, page 11, for MOT details.

The closures/lane restrictions will pose a temporary inconvenience to traveling motorists (including school buses and emergency services); however, no significant delays are anticipated, and all inconveniences and delays will cease upon project completion.

### ESTIMATED PROJECT COST AND SCHEDULE:

Engineering: \$ NA Right-of-Way: \$ 20,000 (2021) Construction: \$ 4,580,293 (2024)

Anticipated Start Date of Construction: Summer 2024

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### RIGHT OF WAY:

Land Use Impacts	Amount (acres)	
	Permanent	Temporary
Residential	0	0
Commercial	0	0
Agricultural	0	0
Forest	0	0
Wetlands	0	0
Other:	0	0
Other:	0	0
TOTAL	0	0

Describe both Permanent and Temporary right-of-way and describe their current use. Typical and Maximum right-of-way widths (existing and proposed) should also be discussed. Any advance acquisition, reacquisition or easements, either known or suspected, and their impacts on the environmental analysis should be discussed.

The existing right-of-way (ROW) includes 100 feet on either side of SR 46. Current land use in existing ROW consists of floodplain with some tree cover.

#### No right-of-way (ROW) required

This project will occur within existing ROW. No permanent or temporary ROW will be required.

If the scope of work or permanent or temporary right-of-way amounts change, the INDOT Environmental Services Division (ESD) and the INDOT District Environmental Section will be contacted immediately.

## Part III – Identification and Evaluation of Impacts of the Proposed Action

### SECTION A - EARLY COORDINATION:

Early coordination letters were sent on July 26, 2021 (Appendix C, page 1.)

Agency	Date Sent	Date Response Received	Appendix
Federal Highway Administration	July 26, 2021	No response received	NA
National Park Service - Regional Environmental Coordinator	July 26, 2021	No response received	NA
US Department of Housing and Urban Development	July 26, 2021	No response received	NA
US Army Corps of Engineers	July 26, 2021	No response received	NA
US Fish and Wildlife Service	July 26, 2021	No response received	Appendix C, page 12
Eighth Coast Guard District	July 26, 2021	No response received	NA

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Natural Resources Conservation Service	July 26, 2021	No response received	NA
Indiana Geological and Water Survey	July 20, 2022	July 20, 2022	Appendix C, page 9
INDOT Environmental Policy Office	July 26, 2021	No response received	NA
INDOT Project Manager, Seymour District	July 26, 2021	No response received	NA
INDOT Environmental Section Manager, Seymour District	July 26, 2021	No response received	NA
Indiana Department of Natural Resources	July 26, 2021	August 24, 2021	Appendix C, page 5
INDOT Site Assessment and Management	July 26, 2021	No response received	NA
St. Leon Wastewater Treatment Plant	July 26, 2021	No response received	NA
Ohio-Kentucky-Indiana Regional Council of Governments	July 26, 2021	No response received	NA
Dearborn County Commissioners	July 26, 2021	No response received	NA
Dearborn County Historical Society	July 26, 2021	July 27, 2021	Appendix C, page 42
Dearborn County Highway Superintendent	July 26, 2021	No response received	NA
Dearborn County Emergency Management Director	July 26, 2021	No response received	NA
Dearborn County Surveyor	July 26, 2021	No response received	NA
Dearborn Floodway/Zoning Administrator	July 26, 2021	Jul 26, 2021	Appendix C, page 43

All applicable recommendations are included in the Environmental Commitments section of this CE document.

*List the date(s) coordination was sent and all resource agencies that were contacted as a part of the development of this Environmental Study. Also, include the date of their response or indicate that no response was received.*

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**SECTION B – ECOLOGICAL RESOURCES:**

**Streams, Rivers, Watercourses & Other Jurisdictional Features**

Federal Wild and Scenic Rivers  
State Natural, Scenic or Recreational Rivers  
Nationwide Rivers Inventory (NRI) listed  
Outstanding Rivers List for Indiana  
Navigable Waterways

<u>Presence</u>	<u>Impacts</u>	
	<u>Yes</u>	<u>No</u>
X		
X		
X	X	

Total stream(s) in project area: 400 Linear feet      Total impacted stream(s): 150 Linear feet

Stream Name	Classification	Total Size in Project Area (linear feet)	Impacted linear feet	Comments (i.e., location, flow direction, likely Water of the US, appendix reference)
Whitewater River	Perennial	200	110	Likely Water of the US, average quality
UNT to Whitewater River	Intermittent	200	40	Likely Water of the US, poor quality

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Describe all streams, rivers, watercourses and other jurisdictional features adjacent or within the project area. Include whether or not impacts (both permanent and temporary) will occur to the features identified. Include if the streams or rivers are listed on any federal or state lists for Indiana. Include if features are subject to federal or state jurisdiction. Discuss measures to avoid, minimize, and mitigate if impacts will occur.

### **Presence, with impacts**

Based on the desktop review, the aerial map of the project area, and the RFI report (Appendix E, page 1), there are twelve (12) streams, rivers, watercourses, or other jurisdictional features within the 0.5-mile search radius. There are four (4) streams, rivers, watercourses, or other jurisdictional features within or adjacent to the project area. That number was updated to two (2) by the site visit on September 2, 2021. There are no Federal, Wild and Scenic, State Natural, or Scenic and Recreational rivers present within the project area. The Whitewater River is listed as an Outstanding River and navigable waterway in Dearborn County.

### **Waters Report**

A *Waters of the US Determination / Wetland Delineation Report* was approved by INDOT Ecology and Waterway Permitting Office on December 30, 2021. Please refer to Appendix F, page 1, for the *Waters of the US Determination / Wetland Delineation Report*. It was determined that two (2) waterways, Whitewater River and unnamed tributary (UNT) to Whitewater River, are present within the project area. The USACE makes all final determinations regarding jurisdiction.

The USGS topographic map identifies Whitewater River as a solid, perennial blue-line stream. Whitewater River flows under the SR 46 bridge from the north to the south. The Ordinary High-Water Mark (OHWM) is 200 feet wide and 9 feet deep. Whitewater River is listed as impaired for PCBs in fish tissue. Exposure to PCBs in fish tissue is considered low, assuming workers are not eating biota surrounding or associated with the water body. Workers will be informed. Whitewater River has previously been listed as impaired for E. coli, nutrients, impaired biotic communities, dissolved oxygen and siltation. Although these impairments are not currently listed for the river, precautions should be taken.

The USGS topographic map identifies UNT to Whitewater River as a dashed, intermittent blue-line stream. UNT to Whitewater River flows under the SR 46 bridge and generally flows from northeast to the south. The OHWM is 11 feet wide and 1.5 feet deep. There will be approximately 0 linear feet of permanent impacts and approximately 40 linear feet of temporary impacts for installation of a temporary construction access road.

There will be approximately 110 linear feet of permanent impacts and approximately 0 linear feet of temporary impacts for placement of riprap scour countermeasures around piers 2 and 3. Plans call for 0.86 acre of early successional habitat disturbance below the base flood elevation (BFE) that will be reseeded and placement of scour protection within the channel below the OHWM at piers 2 and 3 but there will be no excavation associated with scour protection. A Section 401/404 permit will likely be needed based on the anticipated impacts to these water features.

### **Early Coordination**

The Indiana Department of Natural Resources (IDNR) Division of Fish and Wildlife (DFW) responded on August 24, 2021, with recommendations to avoid or minimize impacts to fish, wildlife, and botanical resources to the greatest extent possible and compensate for impacts (Appendix C, page 5). IDNR DFW recommendations included consideration of wildlife passage, bank stabilization, and riparian habitat. All applicable recommendations are included in the Environmental Commitments section of this CE document.

#### **Open Water Feature(s)**

Reservoirs  
Lakes  
Farm Ponds  
Retention/Detention Basin  
Storm Water Management Facilities  
Other: \_\_\_\_\_

#### **Presence**


#### **Impacts**

<b>Yes</b>	<b>No</b>

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Describe all open water feature(s) identified adjacent or within the project area. Include whether or not impacts (both permanent and temporary) will occur to the features identified. Include if features are subject to federal or state jurisdiction. Discuss measures to avoid, minimize, and mitigate if impacts will occur.

### **No presence, no impact**

Based on the desktop review, the aerial map of the project area, and the RFI report (Appendix E, page 1), there are no open water features within the 0.5-mile search radius. There are no open water features within or adjacent to the project area, which was confirmed by the site visit on September 2, 2021, by BLN. Therefore, no impacts are expected.

### **Waters Report**

A *Waters of the US Determination / Wetland Delineation Report* was approved by INDOT Ecology and Waterway Permitting Office on December 30, 2021. Please refer to Appendix F, page 1, for the *Waters of the US Determination / Wetland Delineation Report*. It was determined that no open water features are present within or adjacent to the project area. The USACE makes all final determinations regarding jurisdiction.

### **Wetlands**

	<u>Presence</u>	<u>Impacts</u>	
	<u>Yes</u>	<u>No</u>	

Total wetland area: N/A Acre(s) Total wetland area impacted: N/A Acre(s)

(If a determination has not been made for non-isolated/isolated wetlands, fill in the total wetland area impacted above.)

Wetland No.	Classification	Total Size (Acres)	Impacted Acres	Comments (i.e. location, likely Water of the US, appendix reference)

### **Wetlands (Mark all that apply)**

- Wetland Determination
- Wetland Delineation
- USACE Isolated Waters Determination

### Documentation

<input checked="" type="checkbox"/>

### ESD Approval Dates

<u>12/30/2021</u>

### **Improvements that will not result in any wetland impacts are not practicable because such avoidance would result in (Mark all that apply and explain):**

- Substantial adverse impacts to adjacent homes, business or other improved properties;
- Substantially increased project costs;
- Unique engineering, traffic, maintenance, or safety problems;
- Substantial adverse social, economic, or environmental impacts, or
- The project not meeting the identified needs.


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Describe all wetlands identified adjacent or within the project area. Include whether or not impacts (both permanent and temporary) will occur to the features identified. Include if features are subject to federal or state jurisdiction. Discuss measures to avoid, minimize, and mitigate if impacts will occur.

### **Presence, no impact**

Based on the desktop review, the aerial map of the project area, and the RFI report (Appendix E, page 1), there are twenty-seven (27) wetlands located within the 0.5-mile search radius. There are four (4) wetlands present adjacent to the project area. The wetlands were not located by the site visit on September 2, 2021, by BLN.

### **Waters Report**

A *Waters of the US Determination / Wetland Delineation Report* was approved by INDOT Ecology and Waterway Permitting Office on December 30, 2021. Please refer to Appendix F, page 1, for the *Waters of the US Determination / Wetland Delineation Report*. It was determined that no wetlands are present within or adjacent to the project area. The USACE makes all final determinations regarding jurisdiction.

### **Terrestrial Habitat**

	<u>Presence</u>	<u>Impacts</u>	
	Yes	No	
	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Total terrestrial habitat in project area: 1.65 Acre(s) Total tree clearing: N/A Acre(s)

Describe types of terrestrial habitat (i.e., forested, grassland, farmland, lawn, etc.) adjacent or within the project area. Include whether or not impacts will occur to habitat identified. Include total terrestrial habitat impacted and total tree clearing that will occur. Discuss measures to avoid, minimize, and mitigate if impacts will occur.

### **Presence, with impacts**

Based on a desktop review, a site visit on September 9, 2019, by BLN, and the aerial map of the project area (Appendix B, page 3), the project area is surrounded by maintained and mowed grass areas as well as forested areas. Dominant vegetation includes American elm (*Ulmus americana*), American sycamore (*Platanus occidentalis*), and Canada goldenrod (*Solidago canadensis*). No trees will be cleared, but approximately 1.003 acres of brush and vegetation clearing will occur. Mitigation or additional plantings beyond seeding and stabilizing disturbed areas are not anticipated. Any disturbed areas will be re-seeded following completion of construction activities.

### **Early Coordination**

Indiana Department of Natural Resources (IDNR) Division of Fish and Wildlife (DFW) responded on August 24, 2021, with recommendations to avoid and minimize impacts to fish, wildlife, and botanical resources to the greatest extent possible and to compensate for impacts (Appendix C, page 5). If less than one (1) acre of non-wetland forest is removed in a rural setting, replacement should be at a 1:1 ratio based on the area.

All applicable recommendations are included in the Environmental Commitments section of this CE document.

### **Protected Species**

#### **Federally Listed Bats**

Information for Planning and Consultation (IPaC) determination key completed  
Section 7 informal consultation completed (IPaC cannot be completed)  
Section 7 formal consultation Biological Assessment (BA) required

<b>Yes</b>	<b>No</b>
<input checked="" type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>

Determination Received for Listed Bats from USFWS:

NE  NLAA  LAA

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### **Other Species not included in IPaC**

Additional federal species found in project area (based on IPaC species list)  
State species (not bird) found in project area (based upon consultation with IDNR)

Yes  
 X

No  
 X

### **Migratory Birds**

Known usage or presence of birds (i.e., nests)  
State bird species based upon coordination with IDNR

Yes  
 X

No  
 X

*Discuss IDNR coordination and species identified. Describe USFWS Section 7 consultation and determination received for Indiana bat and northern long-eared bat impacts. Discuss if other federally listed species were identified. If so, include consultation that has occurred and the determination that was received. Discuss if migratory birds have been observed and any impacts.*

Based on a desktop review and the RFI report (Appendix E, page 1), completed by BLN on July 19, 2021, the IDNR Dearborn County Endangered, Threatened, and Rare (ETR) Species List has been checked. According to the IDNR-DFW early coordination response letter dated August 24, 2021 (Appendix C, page 5), the Natural Heritage Program's Database has been checked, and three (3) species were documented within 0.5-mile of the project area. The cobblestone tiger beetle (*Cicindela marginipennis*) was documented within 0.5-mile; the Division of Nature Preserves does not anticipate impacts to this species. The variegated darter (*Etheostoma variatum*) was documented within 0.5-mile, and IDNR stated that if a causeway is required for this project, it must not be placed across any rocky riffles with fast-moving water, which is prime habitat for this species. Because the utility line will be bored under the waterway, no impact to the Variegated Darter should occur. The bald eagle (*Haliaeetus leucocephalus*) was documented within 0.5-mile of the project area, and an active nest was located less than 660 feet from the bridge.

Coordination between INDOT EWPO and IDNR DFW occurred due to the relocation of the utility line on the north bridge truss by boring the line around and to the north of the bridge. IDNR DFW offered comments in accordance with the Flood Control Act (IC 14-28-1) and Administrative Rules 312 IAC 10-5-0.3. Approval from IDNR DFW is required prior to construction as the project is located along a stream listed as an Outstanding River. In a March 31, 2023 letter to INDOT EWPO, IDNR DFW indicated that an active bald eagle nest has been documented within 660 feet of the project area (Appendix I, page 2). This nest was active in 2022 with chicks and in 2023, an adult has been seen sitting in the nest incubating eggs or chicks. Given its success in the past, it is likely that the nest will continue to be active in the coming years. The eagle nest is located approximately 400 feet northwest from the center portion of the bridge over the Whitewater River. The bald eagle nest can be seen from the project site. There is clear visibility to the bridge from the nest, and it is inside the 660-foot buffer. Since the activity at the bridge is closer than 660 feet, a bald eagle permit is required, and this requires all clearing, construction, and related activities to occur outside the nesting season (i.e., outside the nesting season is from August through mid-January) (Appendix I, page 4). This permit requirement has been added as a firm commitment in the Environmental Commitments section of this document. IDNR DFW provided approval for the project in the March 31, 2023 letter so long as the bald eagle requirements and four additional conditions intended to minimize impacts to fish, wildlife and botanical resources are met. These four additional requirements include revegetating all bare and disturbed areas, containing disturbance within the project limits, using appropriately designed measures for controlling erosion, and if erosion control blankets are used they need to be heavy-duty, biodegradable and net free. These four requirements have been added as firm commitments in the Environmental Commitments section of this document.

### **Bats, Programmatic Informal Consultation (i.e., IPaC) – Not Likely to Adversely Affect**

An INDOT 0.5-mile bat check review occurred on July 27, 2022, and it did not indicate the presence of endangered bat species. No critical habitats were identified. Project information was submitted through the USFWS's Information for Planning and Consultation (IPaC) portal, and an official species list was generated (Appendix C, page 12). The project is within range of the federally endangered Indiana bat (*Myotis sodalis*) and the federally threatened northern long-eared bat (NLEB) (*Myotis septentrionalis*). One additional species was generated in the IPaC species list along with the Indiana bat and northern long-eared bat. Refer to the paragraph below.

The project qualifies for the Rangewide Programmatic Informal Consultation for the Indiana bat and northern long-eared bat (NLEB), dated May 2016 (revised February 2018), between FHWA, Federal Railroad Administration (FRA), Federal Transit Administration (FTA), and USFWS. An INDOT bridge inspection occurred on September 8, 2021, and no evidence of bats or birds was found (Appendix C, page 39). BLN completed a bridge inspection on September 2, 2021, and December 2, 2022, and no evidence of bats or birds was found (Appendix C, pages 41 and 44). An effect determination key was completed on July 27, 2022, and based on the responses provided, the project was found to "May Affect – Not Likely to Adversely Affect" the Indiana bat and/or the NLEB (Appendix C, page 27). INDOT reviewed and verified the effect finding on August 5, 2022, and requested USFWS's review of the finding. No response was received from USFWS within the 14-day review period; therefore, it was concluded they concur with the finding. Avoidance and Minimization Measures (AMMs) (General AMM 1, Lighting AMM 1) are included as firm commitments in the Environmental Commitments section of this document.

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The official species list generated from IPaC indicated one (1) other species present within the project area. The Monarch Butterfly (*Danaus plexippus*) was generated and is listed as a Candidate species. The project is not anticipated to impact the Monarch, and there is currently no guidance on the Monarch.

## **Migratory Birds**

Structure no. 046-15-01987A (NBI #017540) and the project's surrounding habitat is conducive for use (i.e., nests) by a bird species protected under the Migratory Bird Treaty Act (MBTA). Prior to the start of nesting season (May 1), the structure must be inspected for birds or signs of birds. If birds or signs of birds are found during the inspection, avoidance and minimization measures must be implemented prior to the start of and during the nesting season. Nests without eggs or young should be removed prior to construction during the non-nesting season (September 8 – April 30) and during the nesting season if no eggs or young are present. Nests with eggs or young cannot be removed or disturbed during the nesting season (May 1 – September 7). Nests with eggs or young should be screened or buffered from active construction. Details of the required procedures are outlined in the "107-C-273 Migratory Bird Protection" Recurring Special Provision (RSP). This firm commitment is included in the *Environmental Commitments* of this document.

This precludes the need for further consultation on this project as required under Section 7 of the Endangered Species Act, as amended. If new information on endangered species at the site becomes available or if project plans are changed, USFWS will be contacted for consultation.

## **Geological and Mineral Resources**

Project located within the Potential Karst Features Area of Indiana

Karst features identified within or adjacent to the project area

Oil/gas or exploration/abandoned wells identified in the project area

**Yes**

X

**No**

X
X

Date Karst Study/Report reviewed by INDOT EWPO (if applicable): \_\_\_\_\_

*Discuss if project is located in Potential Karst Features Area of Indiana and if any karst features have been identified in the project area (from RFI). Discuss response received from IGWS coordination. Discuss if any mines, oil/gas, or exploration/abandoned wells were identified and if impacts will occur. Describe if any impacts will occur to any karst features. Include discussion of karst study/report was completed and results. (Karst investigation must comply with the current Karst MOU and coordinated and reviewed by INDOT EWPO)*

## **Inside karst area; no presence**

Based on a desktop review and the Indiana Karst Region Map, the project is located in the designated Indiana Karst Region as outlined in the most current Protection of Karst Features during Project Development and Construction. According to the topo map of the project area (Appendix B, page 2) and the RFI report (Appendix E, page 1), there are no karst features identified within or adjacent to the project area. In the early coordination response on July 20, 2022, the Indiana Geological and Water Survey (IGWS) did not indicate that karst features exist in the project area (Appendix C, page 9). IGWS indicated there is a high liquefaction potential, potential slope instability, and that the project is in a floodway. There is a low potential for both bedrock resources and sand and gravel resources, and there are also petroleum exploration wells near the project area. Response from IGWS has been communicated to the designer on July 20, 2022. No impacts are expected.

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## SECTION C – OTHER RESOURCES

### Drinking Water Resources

Wellhead Protection Area(s)  
Source Water Protection Area(s)  
Water Well(s)  
Urbanized Area Boundary  
Public Water System(s)

<u>Presence</u>	<u>Impacts</u>	
<u>Yes</u>	<u>No</u>	
X		

Is the project located in the St. Joseph Sole Source Aquifer (SSA):

If Yes, is the FHWA/EPA SSA MOU Applicable?  
If Yes, is a Groundwater Assessment Required?

<u>Yes</u>	<u>No</u>

*Check the appropriate boxes and discuss each topic below. Provide details about impacts and summarize resource-specific coordination responses and any mitigation commitments. Reference responses in the Appendix.*

### Outside of Sole Source Aquifer (SSA)

The project is located in Dearborn County, which is not located within the area of the St. Joseph Sole Source Aquifer, the only legally designated sole source aquifer in the state of Indiana. Therefore, the FHWA/EPA Sole Source Aquifer Memorandum of Understanding (MOU) is not applicable to this project, a detailed groundwater assessment is not needed, and no impacts are expected.

### Not located in a Wellhead Protection Area or Source Water Area

The Indiana Department of Environmental Management's Wellhead Proximity Determinator website (<http://www.in.gov/idem/cleanwater/pages/wellhead/>) was accessed on July 27, 2021, by BLN. This project is not located within a Wellhead Protection Area or Source Water Area. No impacts are expected.

### No wells present, no impacts

The Indiana Department of Natural Resources Water Well Record Database website (<https://www.in.gov/dnr/water/3595.htm>) was accessed on September 20, 2021, by BLN. No wells are located near this project. Therefore, no impacts are expected.

### Not in an Urban Area Boundary Location

Based on a desktop review of the Urban Area Boundary GIS layer from INDOT by BLN on July 20, 2022, this project is not located in an Urban Area Boundary. No impacts are expected.

### In a Public Water System Location

Based on a desktop review, a site visit on September 9, 2021, by BLN and the aerial map of the project area (Appendix B, page 3), this project is located where there is a public water system. The public water system, Tri-Township Water Corporation, will not be affected because during utility coordination, it was determined that the 12-inch ductile iron water main runs parallel to the west construction entrance approximately 55 feet north from SR 46 pavement and is not in conflict with the project.

### **Floodplains**

Project located within a regulated floodplain  
Longitudinal encroachment  
Transverse encroachment  
Homes located in floodplain within 1000' up/downstream from project

<u>Presence</u>	<u>Impacts</u>	
<u>Yes</u>	<u>No</u>	
X		

If applicable, indicate the Floodplain Level?

Level 1

Level 2

Level 3

Level 4

Level 5

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Use the IDNR Floodway Information Portal to help determine potential impacts. Include floodplain map in appendix. Discuss impacts according to the classification system. If encroachment on a flood plain will occur, coordinate with the Local Flood Plain Administrator during design to ensure consistency with the local flood plain planning.

## In floodplain

Based on a desktop review of The Indiana Department of Natural Resources Indiana Floodway Information Portal website (<http://dnrmaps.dnr.in.gov/appspage/fdms/>) by BLN on July 21, 2022, and the RFI report, this project is located in a regulatory floodplain as determined from approved IDNR floodplain maps (Appendix F, page 19). An early coordination letter was sent on July 26, 2021, to the local Floodplain Administrator. The Dearborn County Zoning Administrator responded on July 26, 2021, stating that the project is located in the floodway and floodplain and will require an approved permit through IDNR for the construction in the floodway and a permit through the Dearborn County Planning and Zoning Department (Appendix C, page 42). This project qualifies as a Category 3 per the current INDOT CE Manual, which states, "The modifications to drainage structures included in this project will result in an insubstantial change in their capacity to carry flood water. This change could cause a minimal increase in flood heights and flood limits. These minimal increases will not result in any substantial adverse impacts on the natural and beneficial floodplain values; they will not result in substantial change in flood risks or damage; and they do not have substantial potential for interruption or termination of emergency service or emergency routes; therefore, it has been determined that this encroachment is not substantial."

## Farmland

Agricultural Lands

Prime Farmland (per NRCS)

### Presence


### Impacts

Yes

No

Total Points (from Section VII of CPA-106/AD-1006\*)

\_\_\_\_\_

\*If 160 or greater, see CE Manual for guidance.

Discuss existing farmland resources in the project area, impacts that will occur to farmland, and mitigation and minimization measures considered.

## No presence, no impact

Based on a desktop review, a site visit on September 9, 2021, by BLN, the aerial map of the project area (Appendix B, page 3), there is no land that meets the definition of farmland under the Farmland Protection Policy Act (FPPA) within or adjacent to the project area. The requirements of the FPPA do not apply to this project; therefore, no impacts are expected. An early coordination letter was sent on July 26, 2021, to Natural Resources Conservation Service (NRCS), and no response was received.

## **SECTION D – CULTURAL RESOURCES**

<b>Minor Projects PA</b>	<b>Category(ies) and Type(s)</b>	<b>INDOT Approval Date(s)</b>	<b>N/A</b>
	<input type="text"/>	<input type="text"/>	<input checked="" type="checkbox"/>

## **Full 106 Effect Finding**

No Historic Properties Affected

No Adverse Effect

Adverse Effect

## **Eligible and/or Listed Resources Present**

NRHP Building/Site/District(s)

Archaeology

NRHP Bridge(s)

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**Documentation Prepared** (mark all that apply)

- APE, Eligibility and Effect Determination
- 800.11 Documentation
- Historic Properties Report or Short Report
- Archaeological Records Check and Assessment
- Archaeological Phase Ia Survey Report
- Archaeological Phase Ic Survey Report
- Other:

X
X
X
X

**ESD Approval Date(s)**

February 16, 2022
February 17, 2022
October 17, 2021
October 17, 2021

**SHPO Approval Date(s)**

March 2, 2022
March 2, 2022
November 18, 2021
November 18, 2021

Memorandum of Agreement (MOA)

--

**MOA Signature Dates** (List all signatories)

--

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If the project falls under the MPPA, describe the category(ies) that the project falls under and any approval dates. If the project requires full Section 106, use the headings provided. The completion of the Section 106 process requires that a Legal Notice be published in local newspapers. Please indicate the publication date, name of the paper(s) and the comment period deadline. Include any further Section 106 work which must be completed at a later date, such as mitigation from an MOA or avoidance commitments.

INDOT, acting on behalf of the FHWA, is required to comply with Section 106 of the National Historic Preservation Act of 1966, as amended (Section 106), and its implementing federal regulation, 36 CFR § 800. Section 106 and 36 CFR § 800 outline a process that requires INDOT to evaluate the effects of its undertakings on properties that are listed or eligible for the National Register of Historic Places (NRHP). The following information summarizes the steps INDOT took to identify the cultural resources listed or eligible for the NRHP and the expected impacts the project would have. Per the terms of the "Programmatic Agreement Regarding Management and Preservation of Indiana's Historic Bridges" (Historic Bridges PA), the FHWA-Indiana Division will satisfy its Section 106 responsibilities involving "Select" and "Non-Select" bridges through the Project Development Process (PDP) of the Historic Bridges PA (Stipulation III). Because State Bridge # 046-15-01987A (NBI 17540) is a "Non-Select" bridge, the procedures outlined in Stipulation IIIB. of the Historic Bridges PA will be followed to fulfill FHWA's Section 106 responsibilities for the project.

### **Area of Potential Effect (APE):**

The area of potential effect (APE) includes all properties adjacent to and within a proximate viewshed of the project. The dimensions of the APE were defined by lines of trees along either side of SR 46, open agricultural fields on the west end of the project limits, and elevation changes on the east end of the project limits. The archaeological APE consists of all proposed new, temporary, or existing right-of-way. A map illustrating the APE limits as described is provided in Appendix D, page 16.

### **Coordination with Consulting Parties:**

Section 106 of the National Historic Preservation Act (NHPA) of 1966, as amended, requires Federal Agencies, or their representatives, to take into account the effects of their undertakings on historic properties. In accordance with 36 CFR 800.2(c) and the INDOT Cultural Resources Manual, consulting parties were invited to participate in efforts to identify historic properties potentially affected by this undertaking, assess its effects, and seek ways to avoid, minimize or mitigate any adverse effects on historic properties. The early coordination letter (ECL) was sent to consulting parties on July 23, 2020 (Appendix D, page 1). The State Historic Preservation Officer (SHPO) is an automatic consulting party due to their mandatory or designated roles as specified in 36 CFR § 800.2. Other parties that accepted consulting party status are shown in boldface type below. Early coordination letters were sent to the following parties:

- Indiana Landmarks - Southeast Field Office
- Indiana Historic Spans Task Force
- Historic Bridge Foundation
- Historic Hoosier Bridges
- **Historicbridges.org**
- Dearborn County Historical Society
- Aurora Historic Preservation Commission
- Dearborn County Trust for Historic Preservation
- Ohio-Kentucky-Indiana Regional Council of Governments
- Dearborn County Commissioners
- Dearborn County Highway Supervisor
- Dearborn County Historian
- Dr. James Cooper
- **Eastern Shawnee Tribe of Oklahoma**
- **Miami Tribe of Oklahoma**
- **Peoria Tribe of Indians of Oklahoma**
- Pokagon Band of Potawatomi Indians
- United Keetoowah Band of Cherokee Indians
- Delaware Tribe of Indians

Historicbridges.org, Eastern Shawnee Tribe of Oklahoma, Miami Tribe of Oklahoma, Peoria Tribe of Indians of Oklahoma and SHPO indicated that they would like to be consulting parties (Appendix D, pages 7-10). Historicbridges.org responded on August 11, 2020, and did not have any comments at the time. Eastern Shawnee Tribe of Oklahoma responded on December 8, 2021, and noted that while their people historically occupied the area, the project proposes no adverse effect or endangerment to known sites of interest. If an archaeological site or object is inadvertently discovered, the Eastern Shawnee request immediate notification and consultation. The Miami Tribe of Oklahoma responded on August 18, 2020, and noted that at the time, they are not aware of any existing

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documentation directly linking a specific Miami cultural or historic site to the project area. If at any time items are discovered which fall under the protection of Native American Graves Protection and Reparation Act (NAGPRA), they request immediate notification and consultation. The Peoria Tribe of Indians of Oklahoma responded on November 2, 2021, and noted that the project location does not endanger any known cultural or religious sites of interest. If at any time items are discovered which fall under the protection of NAGPRA, they request immediate notification and consultation. No other parties responded.

### **Archaeology:**

With regard to archaeology, SJCA Inc. reviewed the proposed project area and ascertained that the proposed rehabilitation of the State Road 46 Bridge over the Whitewater River (Bridge No. 046-15-01987A [NBI No. 017540]) in Harrison Township, Dearborn County, Indiana will not likely affect archaeological resources based on the project setting. All proposed work will occur in the existing right-of-way. The proposed work will not likely affect archaeological resources because the existing bridge right-of-way is disturbed, with the disturbance consisting of roadside ditches, manmade/ landscaped slopes, and utility easements. The proposed access road is disturbed, for it will be utilizing an existing gravel farm road (Appendix D, page 46).

A review of the State Historic Architectural and Archaeological Research Database (SHAARD), which occurred on July 21, 2021, indicated that seven professional investigations have either occurred in or adjacent to the project area. Five sites have been inventoried near the project area; none of these sites will be directly impacted by the proposed project. Based on the project setting, which is disturbed, there are no archaeological concerns, and no further work is recommended. However, state law (Indiana Code 14-21-1-27 and -29) requires that if any prehistoric or historic archaeological artifacts or human remains are uncovered during construction, demolition, or earth moving activities, the discovery must be reported to the Department of Natural Resources within two (2) business days (Appendix D, page 46).

No known archaeological resources eligible or listed for inclusion in the National Register of Historic Places (NHRP) have been recorded within the project area. No archaeological survey was required because previous surveys have been conducted in the area and the portions present in the project area were determined to be ineligible. In a letter dated November 18, 2021, as noted in the 800.11 documentation, the SHPO staff stated the APE "proposed in the Historic Properties Report (HPR) appears to be of adequate size to encompass the geographic area in which direct and indirect effects of a project of this nature could occur" (Appendix D, page 49). With regard to the archaeological resources, SHPO concurred with the results and recommendations of the Archaeological Assessment and agreed that an archaeological reconnaissance of the project area was not necessary.

### **Historic Properties:**

On January 21, 2021, a Historic Bridge Alternative Analysis Report (HBAA) prepared by BLN was distributed via email and IN SCOPE to consulting parties for a 30-day review and comment period. The HBAA's recommended preferred alternative is to rehabilitate the existing bridge for continued vehicular use (two-way option) that meets the Secretary of the Interior's (SOI) Standards for Rehabilitation. The rehabilitation efforts include replacing the concrete deck, deck drains and metal forms in all spans. Bridge rail transitions and bridge joints will be replaced (Appendix D, pages 65-66). Deteriorated truss members with holes 2 inches in diameter and larger will be repaired or replaced to restore lost section and improve condition. Holes smaller than 2 inches may either be left in place or have a bolt placed through the hole. Truss members will be repaired by placing a plate over the deteriorated area or be replaced in-kind. Connection plates and lattice bars with heavy section loss and holes will be replaced in-kind. In a letter dated February 15, 2021 (Appendix D, pages 68-69), SHPO acknowledged receipt of the HBAA and stated they agree with the HBAA's recommendation for rehabilitation of the existing bridge for continued vehicular use (Alternative B-1) as the most feasible and prudent alternative.

To further assist the FHWA in carrying out its responsibilities pursuant to 36 CFR § 800.4(b), Secretary of the Interior (SOI) Qualified Historians or Architectural Historians employed by SJCA completed an HPR. The National Register of Historic Places (NRHP) and the Indiana Historic Buildings, Bridges, and Cemeteries Map (IHBBCM), showing results of the IHSSI, were consulted.

A Historic Property Report (HPR) was prepared by SJCA, Inc. on October 20, 2021. The HPR identified one property within the APE recommended eligible for inclusion in the NRHP: SR 46 Bridge (Indiana Historic Sites and Structures Inventory (IHSSI) No. 029-263-00017, rated "contributing"). In a letter dated November 18, 2021, SHPO staff agreed with the bridge being eligible for listing under Criterion C as the longest example of a standard Parker design from the 1930s by I.E. Smith Construction. The NRHP boundary is the footprint of the bridge. SHPO also recommended that the Notable-rated farm within the APE (IHSSI No. 029-103-05008) is eligible for the National Register under Criterion A for agriculture and Criterion C for architecture (Appendix D, page 38).

### **Documentation Findings:**

Because the rehabilitation of the bridge is the preferred alternative, the standard treatment approach described in Attachment B of the Historic Bridges PA (Standard Treatment Approach for Historic Bridges) will be followed. Therefore, the finding for this project only applies to the other resource located within the APE (notable-rated farm) and not the bridge.

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On February 16, 2022, INDOT CRO, on behalf of the FHWA, issued a "No Adverse Effect" determination for the project (Appendix D, pages 41-43). In a letter dated March 2, 2022, the SHPO concurred with the "No Adverse Effect" determination (Appendix D, page 54). On March 1, 2022, both the Peoria Tribe of Indians of Oklahoma and the Eastern Shawnee Tribe of Oklahoma responded to the determination and did not have any objections (Appendix D, pages 57-58). No comments were received in response to the public notice.

Three plan reviews are required, and the 30% and 60% plan reviews were completed, and SHPO concurred on November 18, 2021, and March 2, 2022, respectively. A review of the final plans by SHPO is still required and is included as a firm commitment in this document. SHPO requested photo documentation, which was completed on April 18, 2022.

Per state laws dealing with historic resources, it is required that either a letter of clearance or a certification of approval be obtained under IC 14-21-1-18. A letter of clearance is anticipated to be obtained since INDOT is requesting that this project be subjected to "dual review." This is included as a firm commitment in this document.

**Public Involvement:**

In accordance with 36 CFR 800.2(d), 800.3(e), and 800.6(a)(4), the views of the public were sought regarding the effect of the proposed project. An advertisement was placed in the Dearborn County Register on March 8, 2022, offering the public an opportunity to submit comments pursuant to 36 CFR 800.2(d), 800.3(e), and 800.6(a)(4). The public comment period closed 30 days later, on April 8, 2022. The text of the public notice and the affidavit of publication appear in Appendix D, pages 51-53. No comments were received in response to the public notice.

Per the Historic Bridges Programmatic Agreement, a public hearing for this project must be offered. Once this document has been released for public involvement, a legal notice advertising the public hearing will be placed in the *Bright Beacon* newspaper two times and at least fifteen days prior to the hearing. After the hearing, this document will be updated to reflect any public comments received as a result of the public involvement efforts. All originally invited consulting parties will be notified of the public hearing. The Section 106 process will be complete after the public hearing is held and the public involvement section of this document is updated.

## **SECTION E – SECTION 4(f) RESOURCES/ SECTION 6(f) RESOURCES**

**Parks and Other Recreational Land**

- Publicly owned park
- Publicly owned recreation area
- Other (school, state/national forest, bikeway, etc.)

<u>Presence</u>	<u>Use</u>	
	<u>Yes</u>	<u>No</u>
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

**Wildlife and Waterfowl Refuges**

- National Wildlife Refuge
- National Natural Landmark
- State Wildlife Area
- State Nature Preserve

<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

**Historic Properties**

- Site eligible and/or listed on the NRHP

<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
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**Evaluations**

**Prepared**

- Programmatic Section 4(f)
- "De minimis" Impact
- Individual Section 4(f)
- Any exception included in 23 CFR 774.13

<input type="checkbox"/>
<input type="checkbox"/>
<input type="checkbox"/>
<input type="checkbox"/>

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Discuss Programmatic Section 4(f) and "de minimis" Section 4(f) impacts in the discussion below. Individual Section 4(f) documentation must be included in the appendix and summarized below. Discuss proposed alternatives that satisfy the requirements of Section 4(f). FHWA has identified various exceptions to the requirement for Section 4(f) approval. Refer to 23 CFR § 774.13 - Exceptions.

### **Presence, no impact, no use**

Section 4(f) of the US Department of Transportation Act of 1966 prohibits the use of certain public and historic lands for federally funded transportation facilities unless there is no feasible and prudent alternative. The law applies to significant publicly owned parks, recreation areas, wildlife / waterfowl refuges, and NRHP-eligible or listed historic properties regardless of ownership. Lands subject to this law are considered Section 4(f) resources.

Based on a desktop review, the aerial map of the project area (Appendix B, page 3), and the RFI report (Appendix E, page 1), there is one potential 4(f) resource located within the 0.5-mile search radius. According to additional research and by the site visit on September 9, 2021, by BLN, that number was updated to two potential Section 4(f) resources, the SR 46 bridge and a farm rated notable, located in the northwest quadrant of the project area, approximately 0.05 mile from the bridge.

For the purpose of this programmatic Section 4(f) evaluation, a proposed action will "use" a bridge that is on or eligible for inclusion on the National Register of Historic Places when the action will impair the historic integrity of the bridge either by rehabilitation or demolition. Rehabilitation that does not impair the historic integrity of the bridge, such as the preferred alternative of this project, as determined by procedures implementing the National Historic Preservation Act of 1966, as amended (FHWA), is not subject to Section 4(f).

The project will occur within existing ROW and will not use the farm rated notable by taking permanent right of way and will not indirectly use the resource in such a way that the protected activities, features, or attributes that qualify a resource for protection under Section 4(f) are substantially impaired. Additionally, the main cluster of buildings associated with the notable farm is approximately 0.05 mile north of the project area boundary. Therefore, no 4(f) use is expected.

### **Section 6(f) Involvement**

### **Section 6(f) Property**

### **Presence**

**Yes**  **No**

Discuss Section 6(f) resources present or not present. Discuss if any conversion would occur as a result of this project. If conversion will occur, discuss the conversion approval.

### **No presence, no impact**

The US Land and Water Conservation Fund Act of 1965 established the Land and Water Conservation Fund (LWCF), which was created to preserve, develop, and assure accessibility to outdoor recreation resources. Section 6(f) of this Act prohibits conversion of lands purchased with LWCF monies to a non-recreation use.

A review of 6(f) properties on the INDOT ESD website revealed a total of three properties in Dearborn County (Appendix I, page 1). None of these properties are located within or adjacent to the project area. Therefore, there will be no impacts to 6(f) resources.

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### SECTION F – Air Quality

#### STIP/TIP and Conformity Status of the Project

Is the project in the most current STIP/TIP?

Yes	No
X	
X	

Is the project located in an MPO Area?

Is the project in an air quality non-attainment or maintenance area?

If Yes, then:

Is the project in the most current MPO TIP?


Is the project exempt from conformity?

If No, then:

Is the project in the Transportation Plan (TP)?

X	
	X

Is a hot spot analysis required (CO/PM)?

Location in STIP:

Fiscal Year (FY) 2022-2026

Name of MPO (if applicable):

Ohio-Kentucky – Indiana Regional Council  
of Governments

Location in TIP (if applicable):

FY 2021-2024 TIP

Level of MSAT Analysis required?

Level 1a  Level 1b  Level 2  Level 3  Level 4  Level 5

*Describe if the project is listed in the STIP and if it is in a TIP. Describe the attainment status of the county(ies) where the project is located. Indicate whether the project is exempt from a conformity determination. If the project is not exempt, include information about the TP and TIP. Describe if a hot spot analysis is required and the MSAT Level.*

#### Project Bundled in Contract

This project is included in the Fiscal Year (FY) 2021-2024 Ohio-Kentucky-Indiana Regional Council of Governments (OKI) Transportation Improvement Program (TIP), which has been directly incorporated into the FY 2022-2026 Statewide Transportation Improvement Program (STIP) (Appendix H, pages 1-5). This project has been bundled with Des No 1900094, which is the lead Des No, under contract number B-42401, as shown in the November 15, 2021, FY 2020-2024 STIP Amendment A20-54 (Appendix H, page 6).

#### Attainment area

This project is located in Logan and Harrison Townships in Dearborn County, which is currently in attainment for all criteria pollutants according to the EPA Green Book. Therefore, the conformity procedures of 40 CFR Part 93 do not apply.

#### MSAT Level 1a Analysis

This project is of a type qualifying as a categorical exclusion (Group 1) under 23 CFR 771.117(c) or exempt under the Clean Air Act conformity rule under 40 CFR 93.126, and as such, a Mobile Source Air Toxics analysis is not required.

### SECTION G – NOISE

#### Noise

Yes      No

Is a noise analysis required in accordance with FHWA regulations and INDOT's traffic noise policy?

Date Noise Analysis was approved/technically sufficient by INDOT ESD: \_\_\_\_\_

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Describe if the project is a Type I or Type III project. If it is a Type I project, describe the studies completed to date and if noise impacts were identified. If noise impacts were identified, describe if abatement is feasible and reasonable and include a statement of likelihood.

## Type III Project

This project is a Type III project. In accordance with 23 CFR 772 and the current *Indiana Department of Transportation Traffic Noise Analysis Procedure*, this action does not require a formal noise analysis.

## SECTION H – COMMUNITY IMPACTS

### Regional, Community & Neighborhood Factors

Will the proposed action comply with the local/regional development patterns for the area?

Yes	No
X	
X	
X	
X	
X	
X	

Will the proposed action result in substantial impacts to community cohesion?

Will the proposed action result in substantial impacts to local tax base or property values?

Will construction activities impact community events (festivals, fairs, etc.)?

Does the community have an approved transition plan?

If No, are steps being made to advance the community's transition plan?

Does the project comply with the transition plan? (explain in the discussion below)

Discuss how the project complies with the area's local/regional development patterns; whether the project will impact community cohesion; and impact community events. Discuss how the project conforms with the ADA Transition Plan.

This project will not impact community cohesion or community events. Dearborn County currently has an adopted ADA transition plan, and this project will comply with the plan as there are no pedestrian facilities within the project limits, and none are proposed.

### Public Facilities and Services

Discuss what public facilities and services are present in the project area and impacts (such as MOT) that will occur to them. Include how the impacts have been minimized and what coordination has occurred. Some examples of public facilities and services include health facilities, educational facilities, public and private utilities, emergency services, religious institutions, airports, transportation or public pedestrian and bicycle facilities.

### No presence, no impact

Based on a desktop review, the aerial map of the project area (Appendix B, page 3), and the RFI report (Appendix E, page 1), there are no public facilities within the 0.5-mile search radius. There are no public-use airports within 3.8 miles of the project area; therefore, INDOT Aviation was not contacted. There are no public facilities within or adjacent to the project area, which was confirmed by the site visit on September 9, 2021, by BLN. Therefore, no impacts are expected. Access to all properties will be maintained during construction.

It is the responsibility of the project sponsor to notify school corporations and emergency services at least two weeks prior to any construction that would block or limit access.

### Environmental Justice (EJ) (Presidential EO 12898)

During the development of the project were EJ issues identified?

Yes	No
	X
	X

Does the project require an EJ analysis?

If YES, then:

Are any EJ populations located within the project area?


Will the project result in adversely high and disproportionate impacts to EJ populations?

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Indicate if EJ issues were identified during project development. If an EJ analysis was not required, discuss why. If an EJ analysis was required, describe how the EJ population was identified. Include if the project has a disproportionately high and adverse effect on EJ populations and explain your reasoning. If yes, describe actions to avoid, minimize and mitigate these effects.

## **No EJ analysis required**

Under FHWA Order 6640.23A, FHWA and the project sponsor, as a recipient of funding from FHWA, are responsible to ensure that their programs, policies, and activities do not have a disproportionately high and adverse effect on minority or low-income populations. This project will have no relocations and will require less than 0.5 acre of additional permanent right-of-way; therefore, an EJ analysis is not required per the current INDOT Categorical Exclusion Manual.

## **Relocation of People, Businesses or Farms**

Will the proposed action result in the relocation of people, businesses or farms?

Is a BIS or CSRS required?

**Yes** **No**

<input type="checkbox"/>	<input checked="" type="checkbox"/>
--------------------------	-------------------------------------

Number of relocations: Residences: 0 Businesses: 0 Farms: 0 Other: 0

Discuss any relocations that will occur due to the project. If a BIS or CSRS is required, discuss the results in the discussion below.

## **No Relocations**

No relocations of people, businesses, or farms will take place as a result of this project.

## **SECTION I – HAZARDOUS MATERIALS & REGULATED SUBSTANCES**

### **Documentation**

#### **Hazardous Materials & Regulated Substances (Mark all that apply)**

Red Flag Investigation (RFI)

Phase I Environmental Site Assessment (Phase I ESA)

Phase II Environmental Site Assessment (Phase II ESA)

Design/Specifications for Remediation required?

<input checked="" type="checkbox"/>
<input type="checkbox"/>
<input type="checkbox"/>
<input type="checkbox"/>

Date RFI concurrence by INDOT SAM (if applicable): February 4, 2022

Include a summary of the potential hazardous material concerns found during review. Discuss in depth sites found within, directly adjacent to, or ones that could impact the project area. Refer to current INDOT SAM guidance. If additional documentation (special provisions, pay quantities, etc.) will be needed, include in discussion. Include applicable commitments.

## **Presence, no impact**

Based on a review of GIS and available public records, the RFI was completed on July 19, 2021, and INDOT SAM provided their concurrence on February 4, 2022 (Appendix E, page 1). One NPDES facility and two NPDES pipe locations, both managed by the St. Leon Wastewater Treatment Plant, are located within 0.5 mile of the project area. An early coordination letter was sent on July 22, 2021, to the St. Leon Wastewater Treatment Plant and no response was received. None of the hazmat sites identified will impact the project. Further investigation for hazardous material concerns is not required at this time.

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## Part IV – Permits and Commitments

### **PERMITS CHECKLIST**

**Permits** (mark all that apply)

**Likely Required**

**Army Corps of Engineers (404/Section10 Permit)**

- Nationwide Permit (NWP)
- Regional General Permit (RGP)
- Individual Permit (IP)
- Other

X

**IN Department of Environmental Management (401/Rule 5)**

- Nationwide Permit (NWP)
- Regional General Permit (RGP)
- Individual Permit (IP)
- Isolated Wetlands
- Rule 5
- Other

X
X

**IN Department of Natural Resources**

- Construction in a Floodway
- Navigable Waterway Permit
- Other

X

**Mitigation Required**

**US Coast Guard Section 9 Bridge Permit**  
**Others (Please discuss in the discussion below)**

X
---

*List the permits likely required for the project and summarize why the permits are needed, including permits designated as "Other."*

A Section 401 Water Quality Certification and a Section 404 Permit for Discharge of Dredge or Fill Material below the Ordinary High-Water Level are required for projects with any impacts to waterways. An IDNR Construction in a Floodway Permit and a permit through the Dearborn County Planning and Zoning Department are required for construction. A Construction Stormwater General permit, formerly Rule 5, is required for disturbance of more than one acre of land.

The bald eagle (*Haliaeetus leucocephalus*) was documented within 0.5-mile of the project area, and an active nest was located less than 660 feet from the bridge. Since the activity of the bridge is closer than 660 feet, a bald eagle permit is required.

Applicable recommendations provided by resource agencies are included in the Environmental Commitments section of this document. If permits are found to be necessary, the conditions of the permit will be requirements of the project and will supersede these recommendations.

It is the responsibility of the project sponsor to identify and obtain all required permits.

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## ENVIRONMENTAL COMMITMENTS

*List all commitments and include the name of agency/organization requesting/requiring the commitment(s). Listed commitments should be numbered.*

### **FIRM**

- 1) If the scope of work or permanent or temporary right-of-way amounts change, the INDOT Environmental Services Division (ESD) and the INDOT District Environmental Section will be contacted immediately (INDOT ESD and INDOT District).
- 2) It is the responsibility of the project sponsor to notify school corporations and emergency services at least two weeks prior to any construction that would block or limit access (INDOT ESD).
- 3) This project is located in the floodway and floodplain. The project will require an approved permit through IDNR for the construction in the floodway and a permit through the Dearborn County Planning and Zoning Department, as some of the work will be in the floodplain (Dearborn County Plan Commission).
- 4) General AMM 1: Ensure all operators, employees, and contractors working in areas of known or presumed bat habitat are aware of all FHWA/FRA/FTA (Transportation Agencies) environmental commitments, including all applicable AMMs (USFWS).
- 5) Lighting AMM 1: Direct Temporary lighting away from suitable habitat during the active season (USFWS).
- 6) Whitewater River is listed as impaired for PCBs in fish tissue. Exposure to PCBs in fish tissue is considered low, assuming workers are not eating biota surrounding or associated with the water body. Whitewater River has previously been listed for E. Coli, nutrients, impaired biotic communities, dissolved oxygen, and siltation. Best Management Practices (BMPs) will be used to avoid further degradation to the stream, and Workers who are working in or near water with E. coli should take care to wear appropriate PPE, observe proper hygiene procedures, including regular hand washing, and limit personal exposure. Workers will be informed (INDOT SAM).
- 7) Structure number #046-15-01987 A (NBI #017540) and the project's surrounding habitat is conducive for use (i.e., nests) by a bird species protected under the Migratory Bird Treaty Act (MBTA). Prior to the start of nesting season (May 1), the structure must be inspected for birds or signs of birds. If birds or signs of birds are found during the inspection avoidance and minimization measures must be implemented prior to the start of and during the nesting season. Nests without eggs or young should be removed prior to construction during the non-nesting season (September 8 – April 30) and during the nesting season if no eggs or young are present. Nests with eggs or young cannot be removed or disturbed during the nesting season (May 1 – September 7). Nests with eggs or young should be screened or buffered from active construction. Details of the required procedures are outlined in the "107-C-273 Migratory Bird Protection" Recurring Special Provision (RSP) (INDOT ESD).
- 8) A final plan review per HBPA with concurrence from SHPO is required prior to ECF approval (IDNR DHPA).
- 9) Per state laws dealing with historic resources, it is required that either a letter of clearance or a certification of approval be obtained under IC 14-21-1-18. A letter of clearance is anticipated to be obtained since INDOT is requesting that this project be subjected to "dual review," and this will occur prior to ECF approval (IDNR DHPA).
- 10) If a causeway is required for this project, it must not be placed across any rocky riffles with fast moving water and must comply with eagle requirements (IDNR DFW).
- 11) A bald eagle nest is located within 660 feet of the bridge. The bald eagle is protected under the Bald and Golden Eagle Protection Act. The BGEPAct prohibits anyone, without a federal permit, from taking or disturbing bald eagles, which includes their parts, nests, or eggs. No construction activity, to include utility relocation, shall take place during the bald eagle nesting season. The bald eagle nesting season is from January 15 to July 31. The Bald Eagle Protection USP shall be included in the contract documents (INDOT EWPO).
- 12) Revegetate all bare and disturbed areas with a mixture of grasses (excluding all varieties of tall fescue) and legumes as soon as possible upon completion; turf-type grasses (including low-endophyte, friendly endophyte, and endophyte free tall

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fescue but excluding all other varieties of tall fescue) may be used in regularly mowed areas only (IDNR DFW).

- 13) Do not excavate in the waterway, do not remove bank vegetation, and contain disturbance to within the project limits (IDNR DFW).
- 14) Appropriately designed measures for controlling erosion and sediment must be implemented to prevent sediment from entering the stream or leaving the construction site; maintain these measures until construction is complete and all disturbed areas are stabilized (IDNR DFW).
- 15) If erosion control blankets are used, they shall be heavy-duty, biodegradable, and net free or use loose-woven/Leno-woven netting to minimize the entrapment and snaring of small-bodied wildlife such as snakes and turtles (follow manufacturer's recommendations for selection and installation); seed and apply mulch on all other disturbed areas (IDNR DFW).
- 16) The TIP/STIP will be updated prior to RFC (INDOT ESD).

### **FOR FURTHER CONSIDERATION**

- 17) Do not excavate in the low flow area except for the placement of piers, foundations, and riprap, or removal of the old structure (IDNR DFW).
- 18) Do not construct any temporary runarounds, access bridges, causeways, cofferdams, diversions, or pumparounds (IDNR DFW).
- 19) Use minimum average 6 inch graded riprap stone extended below the normal water level to provide habitat for aquatic organisms in the voids (IDNR DFW).
- 20) Impacts to non-wetland forest of one (1) acre or more should be mitigated at a minimum 2:1 ratio. If less than one acre of non-wetland forest is removed in a rural setting, replacement should be at a 1:1 ratio based on area. Impacts to nonwetland forest under one (1) acre in an urban setting should be mitigated by planting five trees, at least 2 inches in diameter-at-breast height (dbh), for each tree which is removed that is 10 inches dbh or greater (5:1 mitigation based on the number of large trees) (IDNR DFW).
- 21) Do not cut any trees suitable for Indiana bat or Northern Long-eared bat roosting (greater than 5 inches dbh, living or dead, with loose hanging bark, or with cracks, crevices, or cavities) from April 1 through September 30 (IDNR DFW).

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# **Appendix A:**

# **INDOT Supporting Documentation**

## Categorical Exclusion Level Thresholds

	PCE	Level 1	Level 2	Level 3	Level 4 <sup>1</sup>
<b>Section 106</b>	Falls within guidelines of Minor Projects PA	“No Historic Properties Affected”	“No Adverse Effect”	-	“Adverse Effect” Or Historic Bridge involvement <sup>2</sup>
<b>Stream Impacts<sup>3</sup></b>	No construction in waterways or water bodies	< 300 linear feet of stream impacts	≥ 300 linear feet of stream impacts	-	USACE Individual 404 Permit <sup>4</sup>
<b>Wetland Impacts<sup>3</sup></b>	No adverse impacts to wetlands	< 0.1 acre	-	< 1.0 acre	≥ 1.0 acre
<b>Right-of-way<sup>5</sup></b>	Property acquisition for preservation only or none	< 0.5 acre	≥ 0.5 acre	-	-
<b>Relocations<sup>6</sup></b>	None	-	-	< 5	≥ 5
<b>Threatened/Endangered Species (Species Specific Programmatic for Indiana bat &amp; northern long eared bat)*</b>	“No Effect”, “Not likely to Adversely Affect” (With select AMMs <sup>7</sup> )	“Not likely to Adversely Affect” (With any AMMs or commitments)	-	“Likely to Adversely Affect”	Project does not fall under Species Specific Programmatic <sup>8</sup>
<b>Threatened/Endangered Species (Any other species)*</b>	Falls within guidelines of USFWS 2013 Interim Policy or “No Effect”	“Not likely to Adversely Affect”	-	-	“Likely to Adversely Affect”
<b>Environmental Justice</b>	No disproportionately high and adverse impacts	-	-	-	Potential <sup>9</sup>
<b>Sole Source Aquifer</b>	No Detailed Groundwater Assessment	-	-	-	Detailed Groundwater Assessment
<b>Floodplain</b>	No Substantial Impacts	-	-	-	Substantial Impacts
<b>Section 4(f) Impacts</b>	None	-	-	-	Any <sup>10</sup>
<b>Section 6(f) Impacts</b>	None	-	-	-	Any
<b>Permanent Traffic Alteration</b>	None	-	-	-	Any
<b>Noise Analysis Required</b>	No	-	-	-	Yes
<b>Air Quality Analysis Required</b>	No	-	-	-	Yes <sup>11</sup>
<b>Approval Level</b>					
• <b>District Env. (DE)</b>	Concurrence by DE or ESD	DE or ESD	DE or ESD	DE and/or ESD	DE and/or ESD; and FHWA
• <b>Env. Serv. Div. (ESD)</b>					
• <b>FHWA</b>					

<sup>1</sup> Coordinate with INDOT Environmental Services Division. INDOT will then coordinate with the appropriate FHWA Environmental Specialist.

<sup>2</sup> Any involvement with a bridge processed under the Historic Bridge Programmatic Agreement.

<sup>3</sup> Total permanent impacts to streams (linear feet) and wetlands (acres).

<sup>4</sup> US Army Corps of Engineers Individual 404 Permit

<sup>5</sup> Total permanent and temporary right-of-way. This does not include reacquisition of existing apparent right-of-way.

<sup>6</sup> If any relocations are within an area with a known or suspected Environmental Justice (EJ) or disadvantaged population, or has greater than 5 relocations, a conversation with FHWA, through INDOT ESD, is needed to confirm NEPA classification and outreach plan for the project.

<sup>7</sup> Avoidance and Mitigation Measures (AMMs) determined by the IPAC determination key to be required that are not tree AMMs, bridge AMMs, or structure AMMs.

<sup>8</sup> Projects that do not fall under a Species Specific Programmatic and results in a “Likely to Adversely Affect”. Other findings can be processed as a lower-level CE.

<sup>9</sup> Potential for causing a disproportionately high and adverse impact.

<sup>10</sup> Section 4(f) use resulting in an Individual, Programmatic, or *de minimis* evaluation. The only exception is a *de minimis* evaluation for historic properties (Effective January 2, 2020). If a historic property *de minimis* and no other use, mark the *None* column.

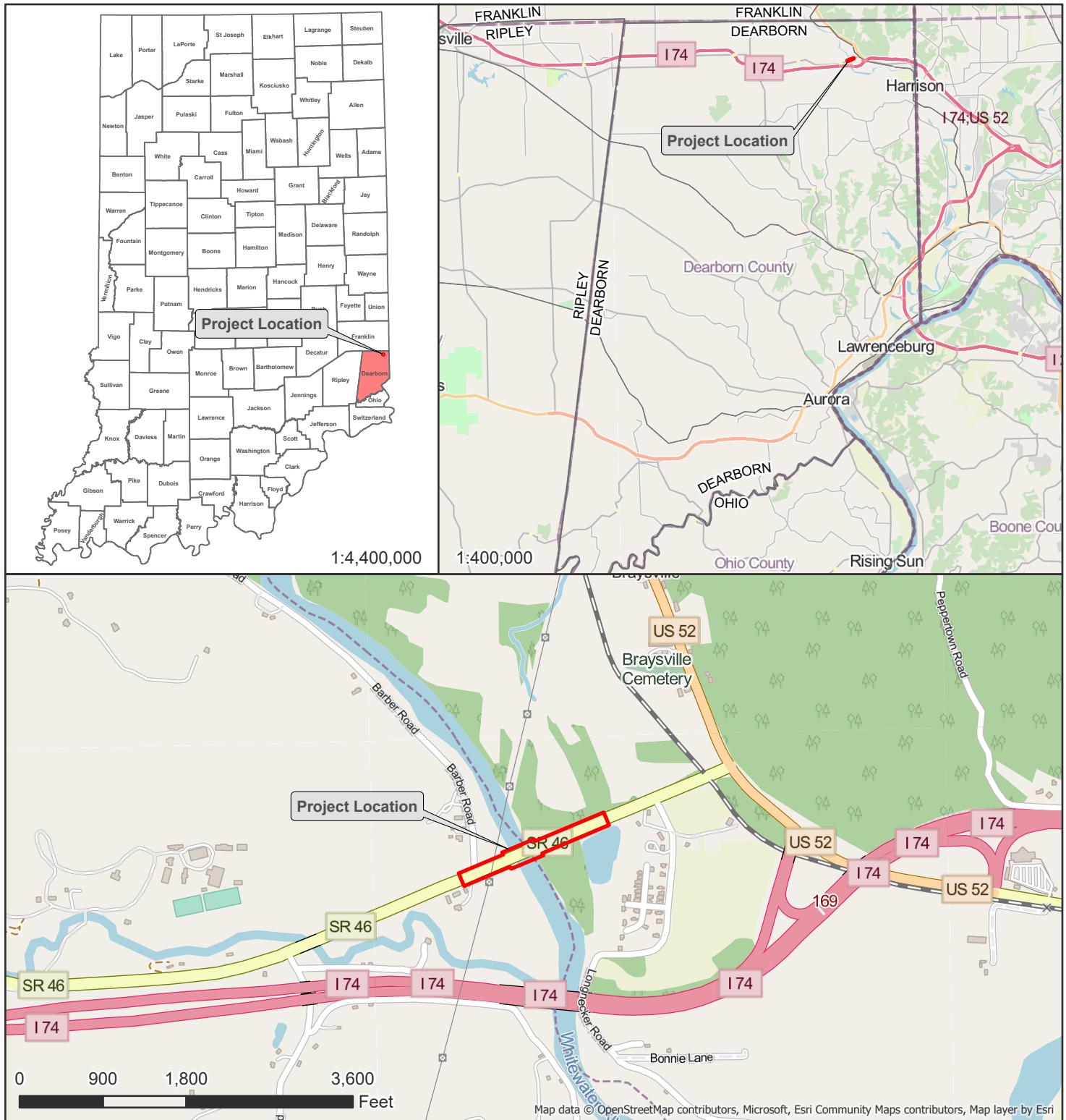
<sup>11</sup> Hot Spot Analysis and/or MSAT Quantitative Emission Analysis.

\* Includes the threatened/endangered species critical habitat

Note: Substantial public or agency controversy may require a higher-level NEPA document.

# **Appendix B:**

# **Graphics**



Note: Project Area illustrates project survey boundaries and not construction limits. No improvements will be made to the 136th Street and Prairie Baptist Road intersection.

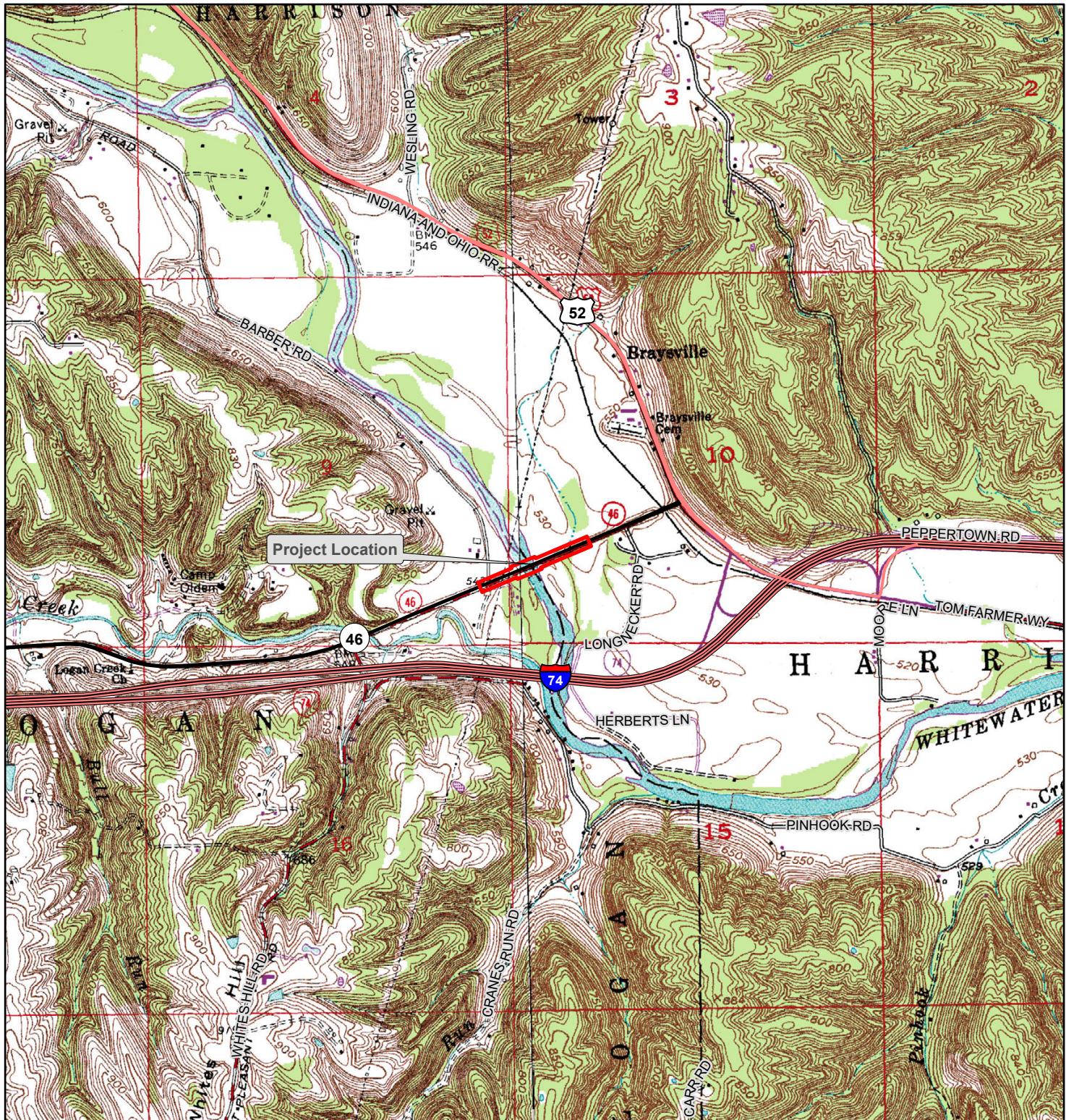


## LEGEND

- County Boundary
- Project Area
- Project County

## PROJECT LOCATION MAP

PROJECT LOCATION MAP  
Bridge Project  
SR 46 over Whitewater River  
Dearborn County, Indiana  
Des. No. 1383721



Source: U.S. Geological Survey (USGS); IndianaMap



1:24,000

Note: Project Area illustrates project survey boundaries and not construction limits.

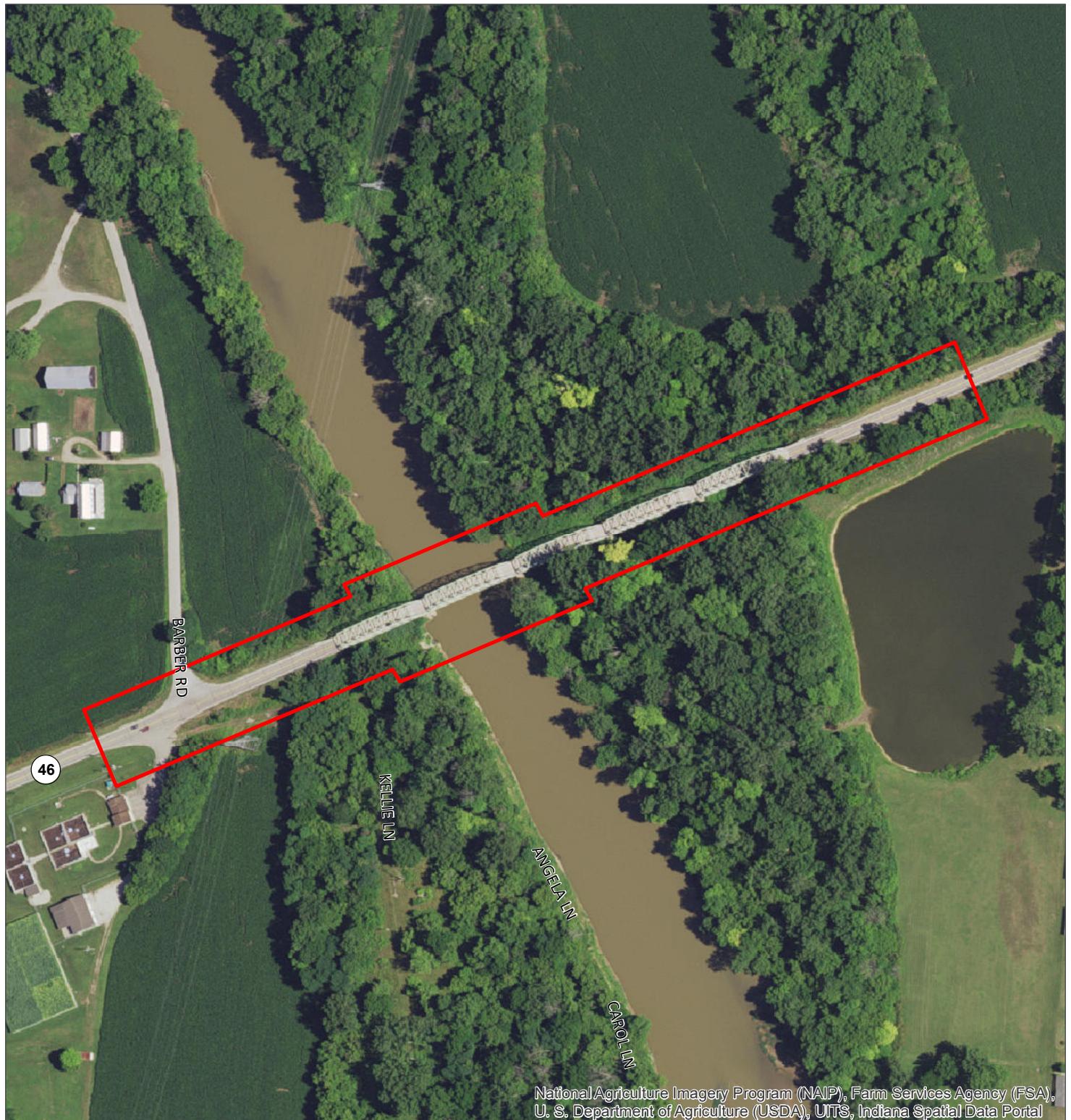
**BLN**  
BEAM·LONGEST·NEFF

#### LEGEND

Project Area

7/20/2021

**USGS TOPOGRAPHIC MAP**  
Bridge Project  
SR 46 over Whiteewater River  
Dearborn County, Indiana  
Des. No. 1383721



National Agriculture Imagery Program (NAIP), Farm Services Agency (FSA),  
U. S. Department of Agriculture (USDA), UITS, Indiana Spatial Data Portal



Source: Indiana MAP

1:3,000

1 in = 250 ft

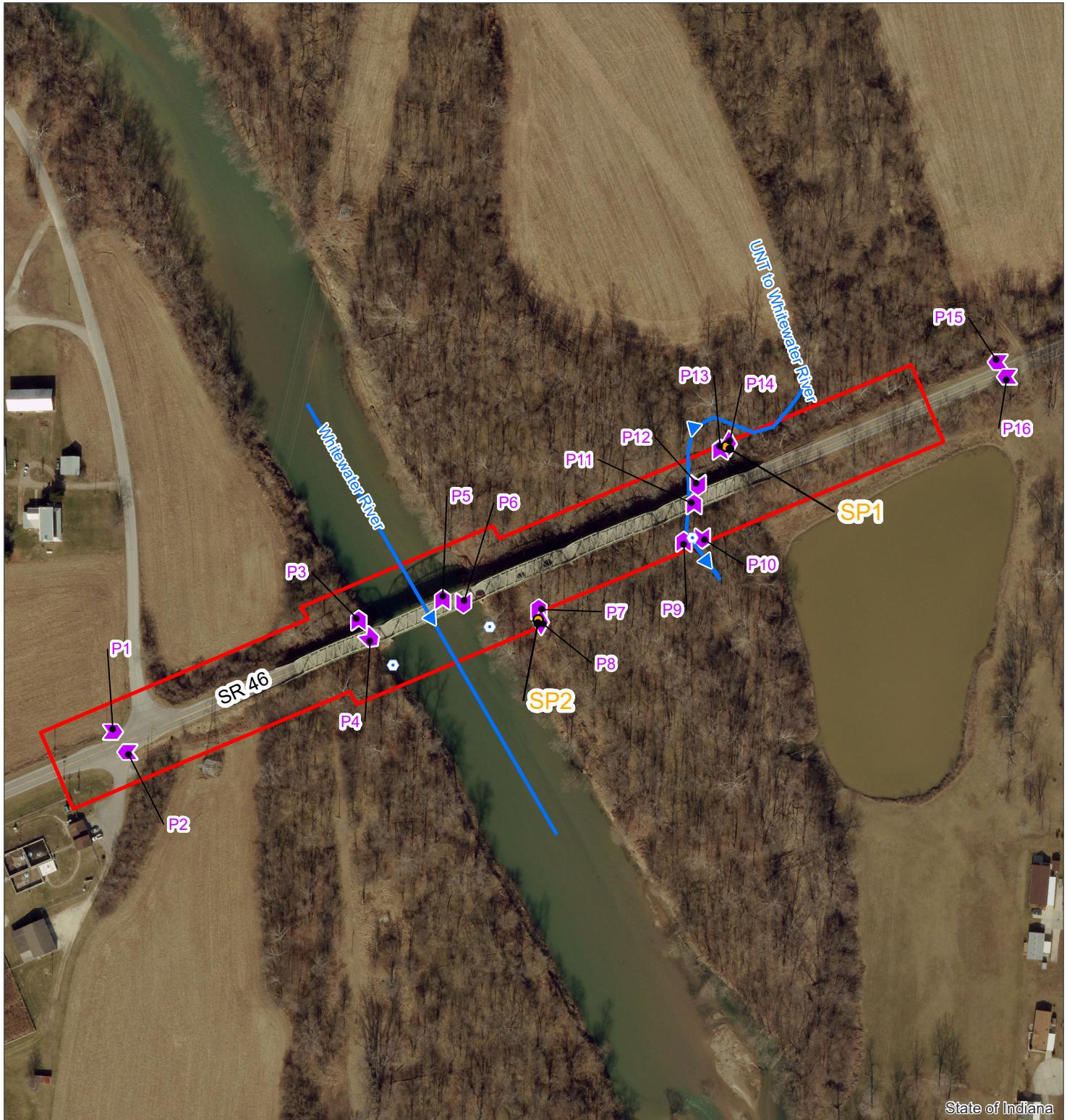
**BLN**  
BEAM LONGEST NEFF

**Legend**

Investigated Area

**Aerial Photograph**  
Bridge Project  
SR 46 over Whitewater River  
Dearborn County, Indiana  
Des. No. 1383721

October 25, 2021



State of Indiana



Source: Indiana Map

1:3,000

1 in = 250 ft

**BLN**  
BEAM LONGEST NEFF

Legend

- Investigated Area
- OHWM Location
- Sampling Points
- Photo Location
- Streams

**Water Resources and Photo Location Map**

Bridge Project  
SR 46 over Whitewater River  
Dearborn County, Indiana  
Des. No. 1383721

December 22, 2021



**Photo 1: Looking east on SR 46 toward Bridge No. 046-15-01987A from western end of investigated area at the intersection with Barber Street.**



**Photo 2: Looking west on SR 46 toward Bridge No. 046-15-01987A from eastern end of investigated area.**



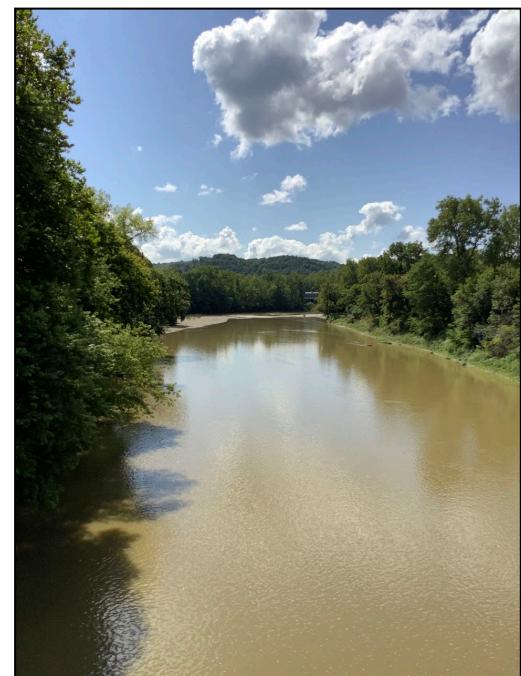
**Photo 3: Looking north along the Whitewater River from the northwest quadrant of Bridge No. 046-15-01987A.**



**Photo 4: Looking southeast along the Whitewater River from the southwest quadrant of Bridge No. 046-15-01987A.**



**Photo 5: Looking north from Bridge No. 046-15-01987A toward the Whitewater River.**



**Photo 6. Looking south from Bridge No. 046-15-01987A toward the Whitewater River.**



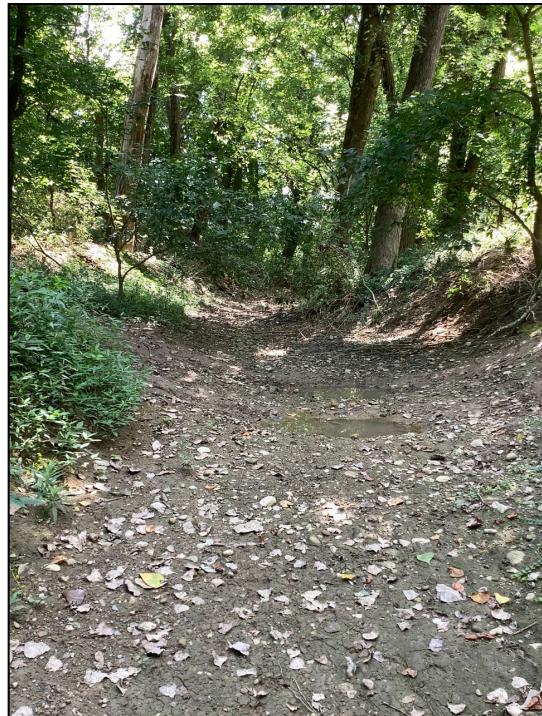
**Photo 7: Looking north at SP 2.**



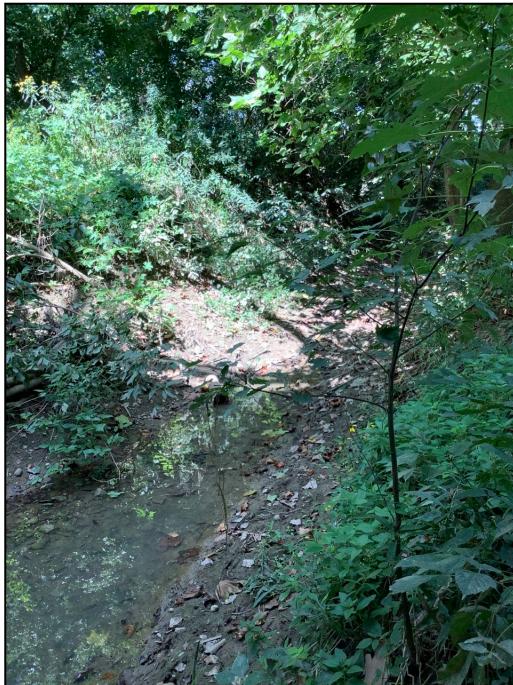
**Photo 8. Looking at soil profile for SP 2.**



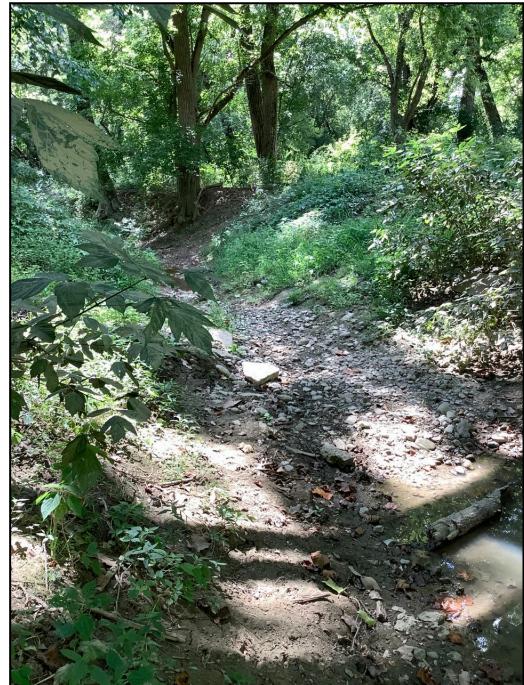
**Photo 9: Looking north along UNT to Whitewater River.**



**Photo 10. Looking south along UNT to Whitewater River.**



**Photo 11: Looking north along UNT to Whitewater River.**



**Photo 12. Looking south along UNT to Whitewater River.**



**Photo 13: Looking north at SP 1.**



**Photo 14. Looking at soil profile for SP 1.**



**Photo 15: Looking west on SR 46 toward Bridge No. 046-15-01987A from eastern end of investigated area.**



**Photo 16: Looking west on SR 46 toward Bridge No. 046-15-01987A from eastern end of investigated area.**

PROJECT	DESIGNATION
1900094	1383721
CONTRACT	BRIDGE FILE
B-42401	046-15-01987 B

STRUCTURE INFORMATION				
STRUCTURE	TYPE	SPAN AND SKEW	OVER	STATION
046-15-01987 B	REHABILITATION TO STEEL THROUGH TRUSS BRIDGE	5 SPANS: 175'-0" SKEW: 0	WHITEWATER RIVER	STA.1082+85.00 "NN"

KIN PROJECT INFORMATION	
DESIGNATION	PROJECT DESCRIPTION
1383721	Bridge Deck Replacement for Structure 046-15-01987 B
1900094	Bridge Deck Replacement for Structure 046-15-03054 B (Lead)

# INDIANA DEPARTMENT OF TRANSPORTATION



TRAFFIC DATA	
A.A.D.T.	(2023)
A.A.D.T.	(2043)
D.H.V.	(2043)
DIRECTIONAL DISTRIBUTION	
TRUCKS	10 % A.A.D.T.
	5 % D.H.V.
DESIGN DATA	
DESIGN SPEED	55 M.P.H.
PROJECT DESIGN CRITERIA	3R (NON-FREEWAY)
FUNCTIONAL CLASSIFICATION	STATE COLLECTOR
RURAL/URBAN	RURAL
TERRAIN	LEVEL
ACCESS CONTROL	NONE

## BRIDGE REHABILITATION PLANS

FOR SPANS OVER 20 FEET

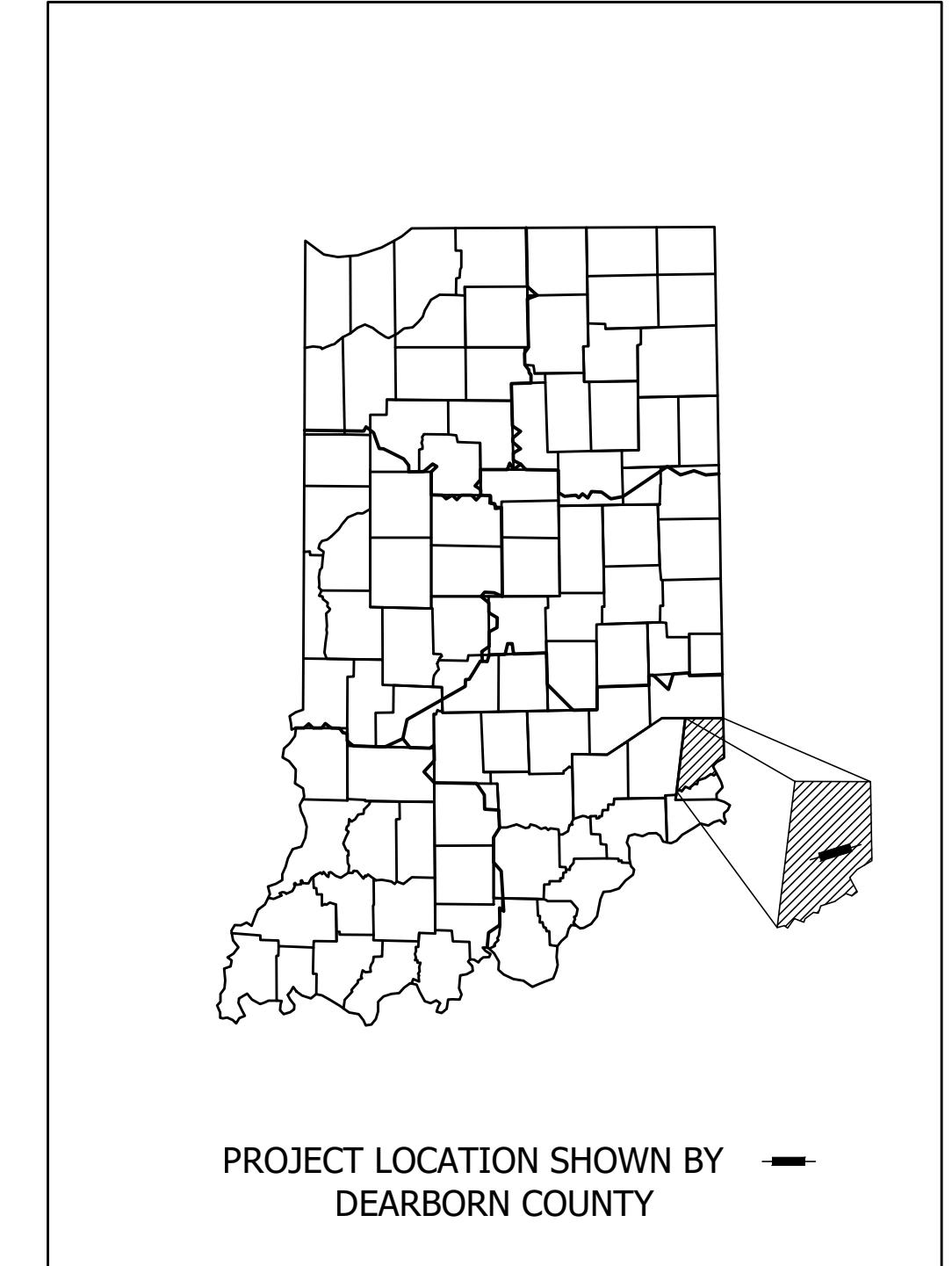
ROUTE: SR 46 AT: RP 153+43

PROJECT NO. 1383721 P.E.  
R/W  
1383721 CONST.

NO ADDITIONAL RIGHT-OF-WAY  
REQUIRED FOR THIS PROJECT

Bridge Deck Replacement on SR 46 over Whitewater River  
Located 0.44 Miles West of US 52 in

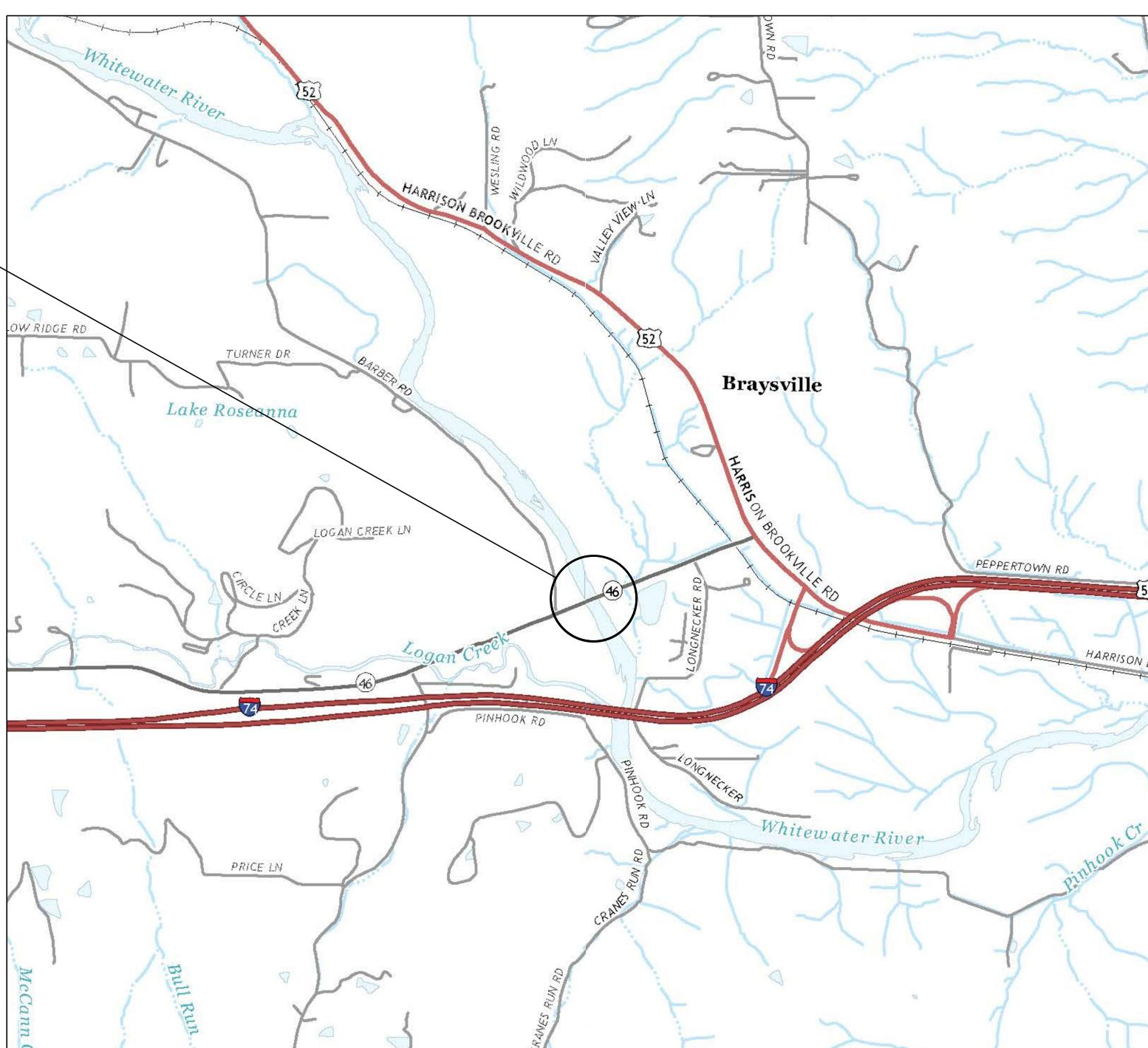
Sections 9 & 10, T-7-N, R-1-W, Logan and Harrison Townships, Dearborn County, Indiana



LATITUDE: 39°16'48.14"N LONGITUDE: 84°52'26.5"W

BRIDGE LENGTH: 0.168 MI.  
ROADWAY LENGTH: 0.007 MI.  
TOTAL LENGTH: 0.180 MI.  
MAX. GRADE: -0.97 %

HUC 12: 050800030806  
HUC 14: 05080003080090



LOCATION MAP  
SCALE: 1:24000

INDIANA DEPARTMENT OF TRANSPORTATION  
STANDARD SPECIFICATIONS DATED 2022  
TO BE USED WITH THESE PLANS.

PLANS PREPARED BY:

**BLN:**  
BEAM LONGEST NEFF  
8320 CRAIG STREET | INDIANAPOLIS, IN 46250  
317.849.5832 | f: 317.841.4280 | WWW.BLN.COM

PLANS  
PREPARED BY: BEAM, LONGEST & NEFF, LLC

(317)849-5832  
PHONE NUMBER

CERTIFIED BY: \_\_\_\_\_

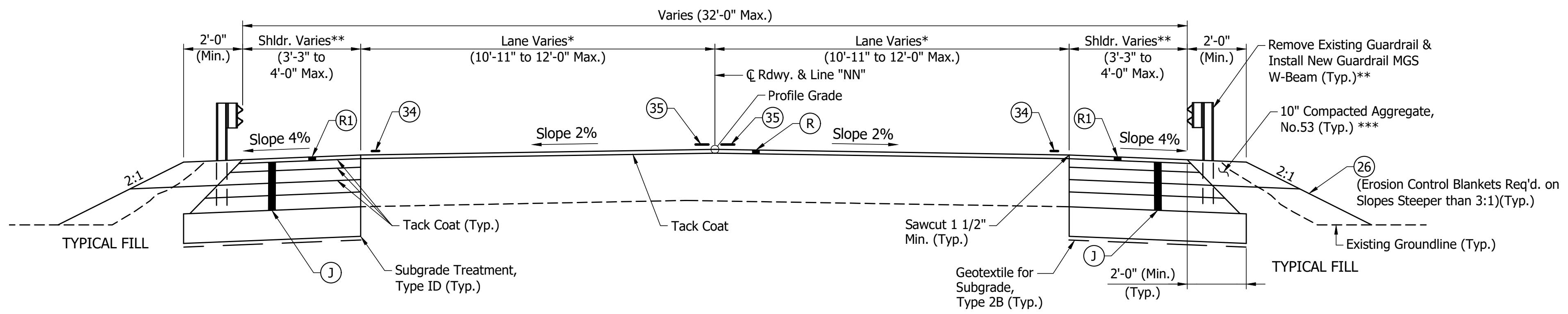
DATE

APPROVED  
FOR LETTING: \_\_\_\_\_

INDIANA DEPARTMENT OF TRANSPORTATION

DATE

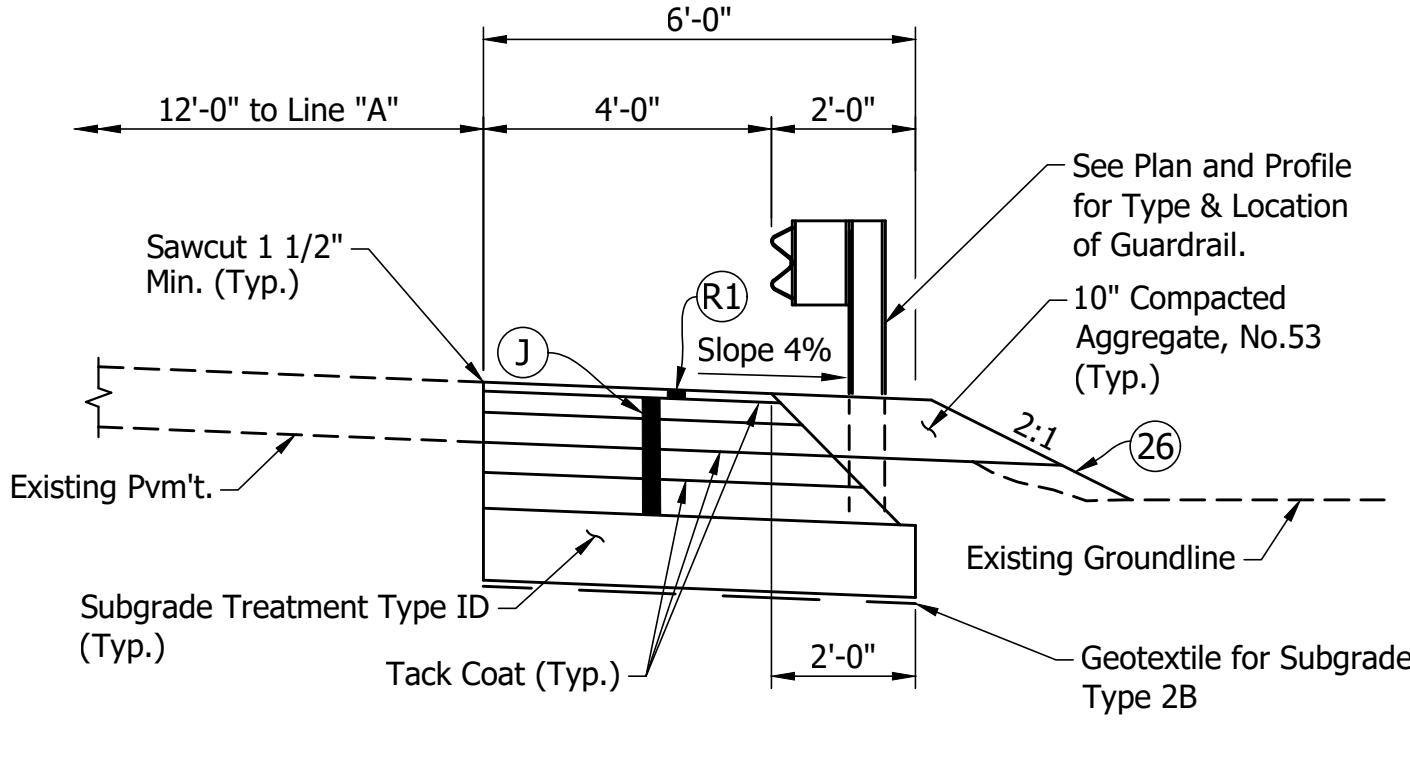
BRIDGE FILE
046-15-01987 B
DESIGNATION
1383721
DRAWING NO.
1 of 36
CONTRACT
PROJECT
1900094



### TYPICAL SECTION THROUGH RESURFACE

STA.1077+18.50 "NN" to STA.1077+78.50 "NN"  
 STA.1087+91.50 "NN" to STA.1088+51.50 "NN"

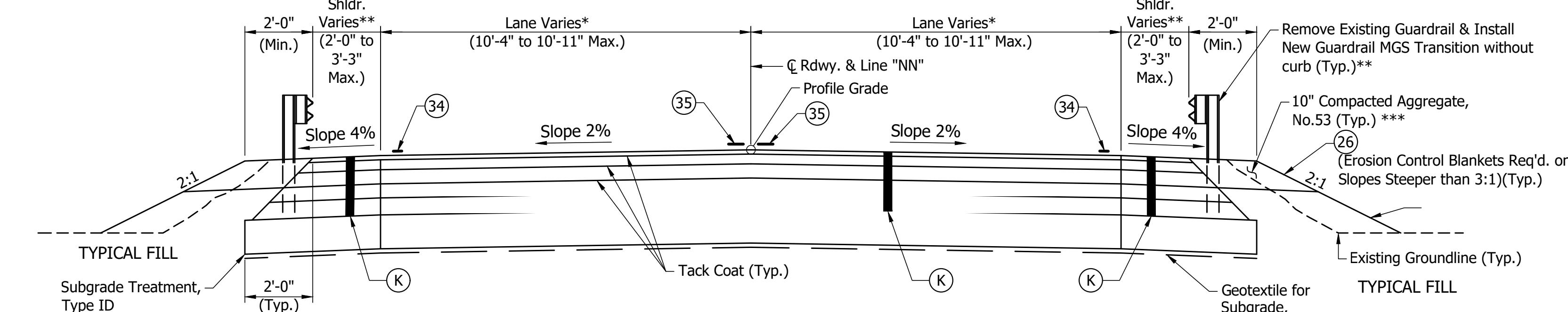
Scale: 3/8" = 1'-0"



#### SECTION A-A

STA.1077+06.50 "NN" to STA.1077+18.50 "NN" LT.  
 STA.1076+62.75 "NN" to STA.1077+18.50 "NN" RT.  
 STA.1088+51.50 "NN" to STA.1089+07.25 "NN" LT.  
 STA.1088+51.50 "NN" to STA.1088+63.50 "NN" RT.

Scale: 3/8" = 1'-0"



### TYPICAL SECTION THROUGH FULL DEPTH PAVEMENT

STA.1077+78.50 "NN" to STA.1078+08.50 "NN"  
 STA.1087+61.50 "NN" to STA.1087+91.50 "NN"

Scale: 3/8" = 1'-0"

#### LEGEND

(26) Mulched Seeding, R:  
 Apply to Areas above El. 534.0  
 Seed Mixture Floodplain:  
 Apply to Areas below El. 534.0

(34) Line, Paint, Solid, White, 4 in.  
 (35) Line, Paint, Solid, Yellow, 4 in.

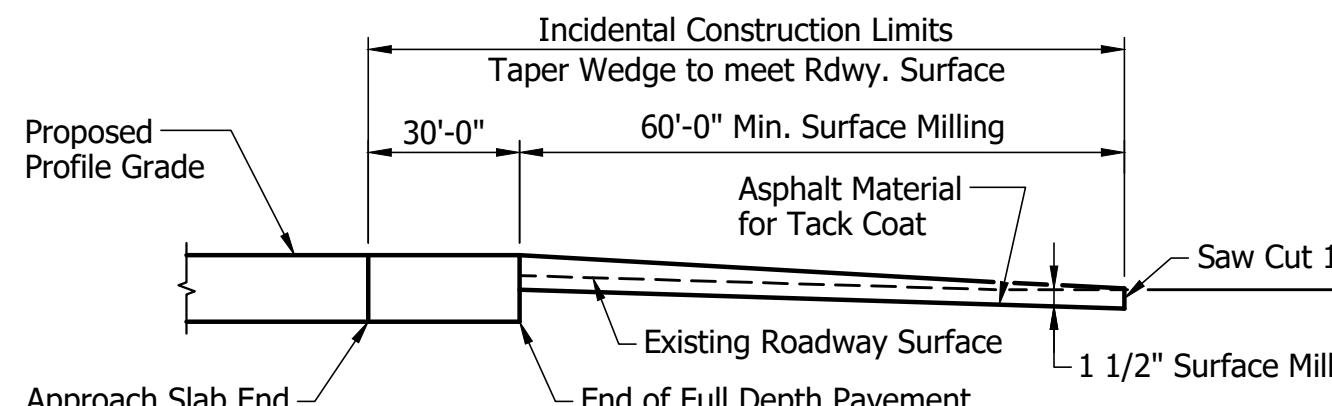
(J) Widening with HMA, Type B  
 (385 lb/syd, HMA Intermediate, Type B on  
 550 lb/syd, HMA Base, Type B on  
 550 lb/syd, HMA Base, Type B) on  
 6" Compacted Aggregate, No. 53  
 (Tack Coat between HMA Layers)

(K) 165 lb/syd, QC/QA-HMA, 3, 64, Surface, 9.5 mm on  
 385 lb/syd, QC/QA-HMA, 3, 64, Intermediate, 19.0 mm on  
 550 lb/syd, QC/QA-HMA, 3, 64, Base, 25.0 mm on  
 550 lb/syd, QC/QA-HMA, 3, 64, Base, 25.0 mm on  
 6" Compacted Aggregate, No. 53  
 (Tack Coat between HMA Layers)

(R) 165 lb/syd, QC/QA-HMA, 3, 64, Surface, 9.5 mm on  
 Milling, Asphalt, 1.5 in.  
 After Milling, seal visible cracks 0.25 in. wide or wider, before  
 applying tack coat, with PG 64-22 only. No Emulsion shall be  
 used. The sealed cracks shall not be overbonded.

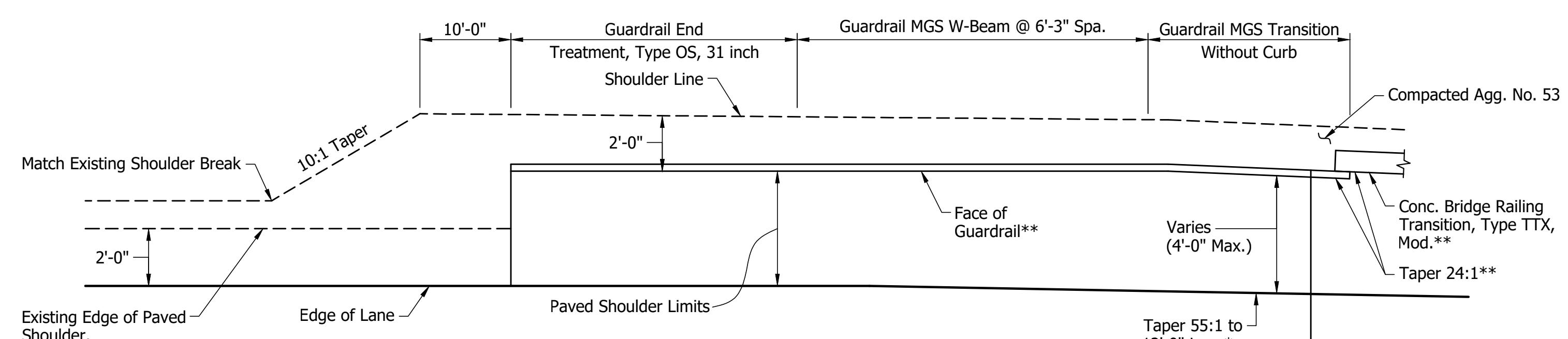
(R1) 165 lb/syd, QC/QA-HMA, 3, 64, Surface, 9.5 mm on  
 Widening with HMA, Type B

Note to Reviewer:  
 Pavement Design is Preliminary



### MATCHING EXISTING PAVEMENT - LINE "NN"

(REQ'D. @ END OF PROJECT)  
 (NO SCALE)



### SHOULDER AND GUARDRAIL DETAILS

(Typ. all 4 Corners)  
 (NO SCALE)

Notes:

\* Travel Lanes shall Taper from 10'-0" on Bridge to 12'-0" on Approaches utilizing a 55:1 Taper.

\*\* New Guardrail and Shoulder at all Quadrants shall Taper 24:1 until Front Face of Guardrail is 16'-0" from Line 'NN'.

\*\*\* Place Compacted Aggregate only within limits of New Guardrail and areas to tie into existing shoulders at end of New Guardrail.

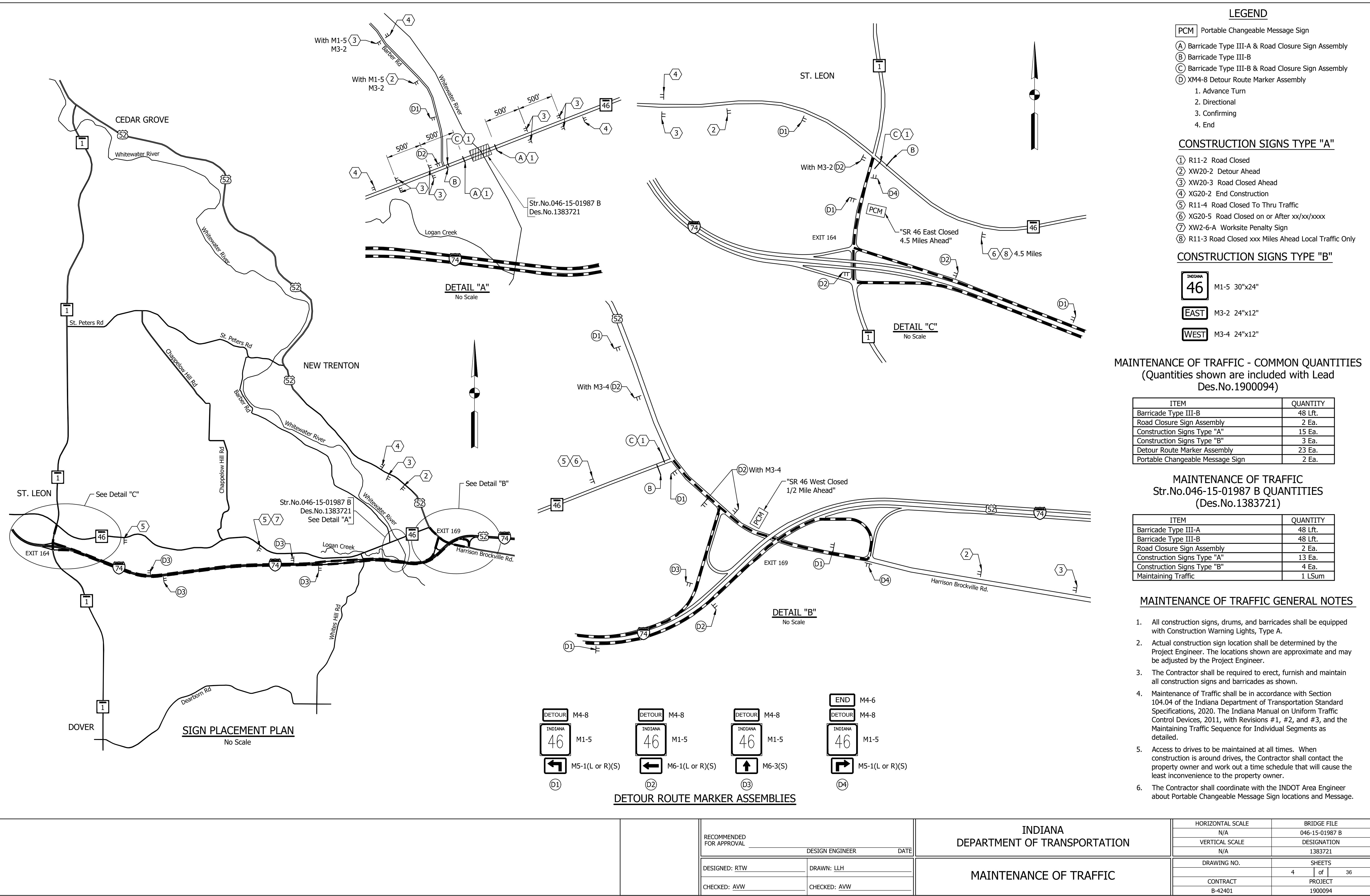
For Location of Section A-A, see Sh. XX.

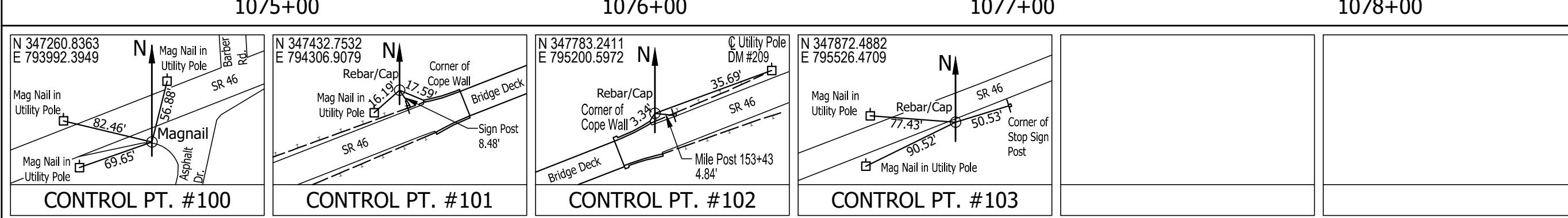
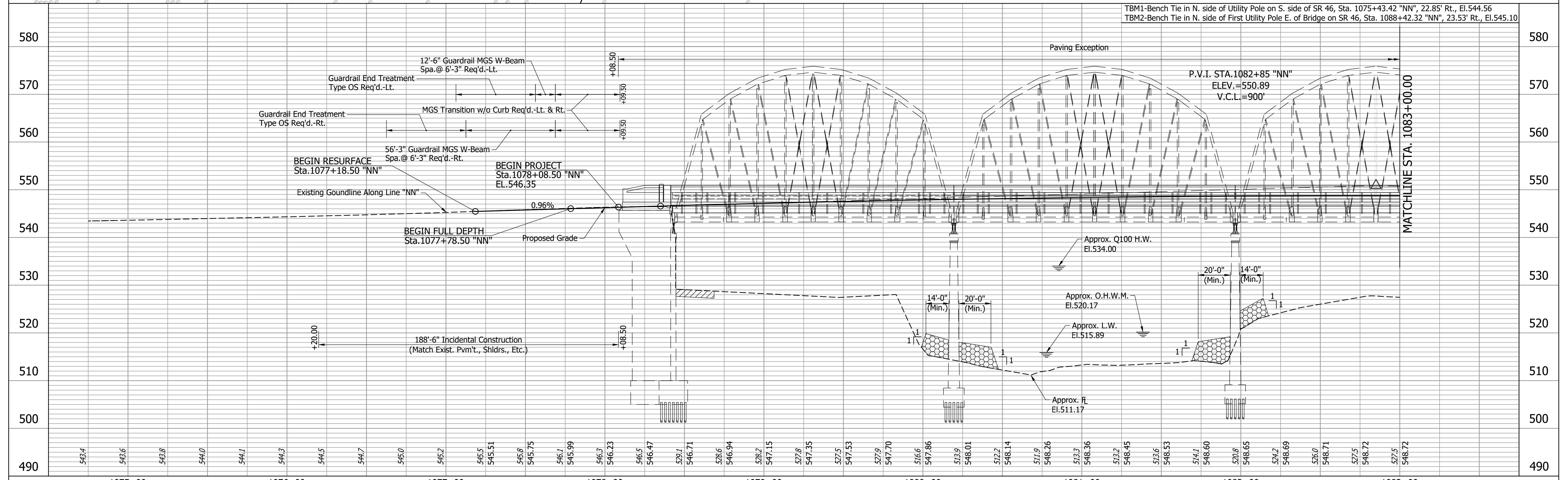
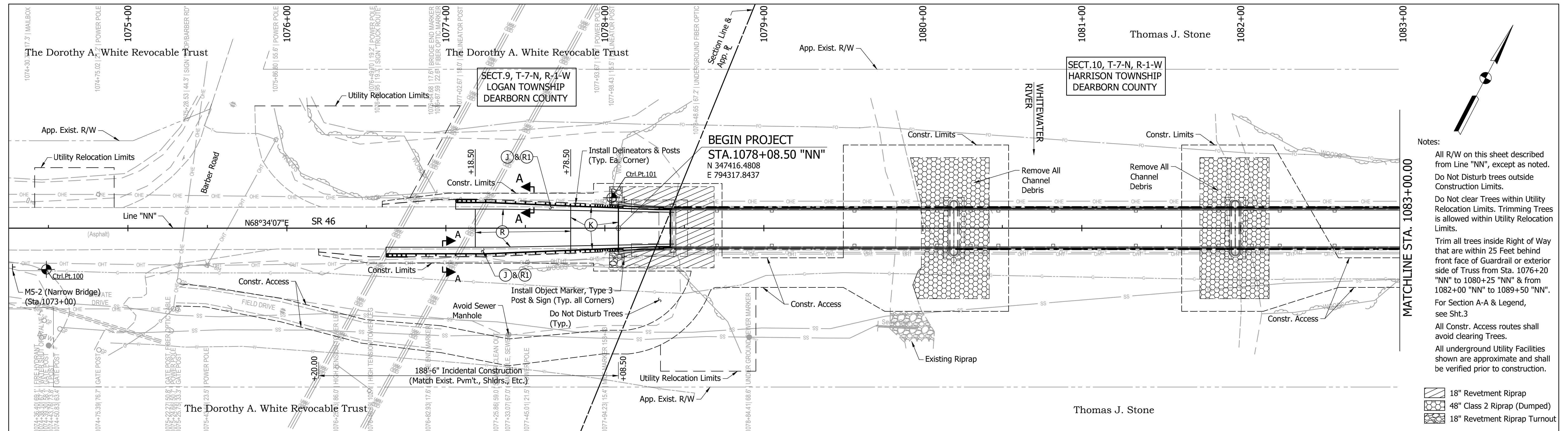
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RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: RTW	DRAWN: LLH	
CHECKED: AVW	CHECKED: AVW	

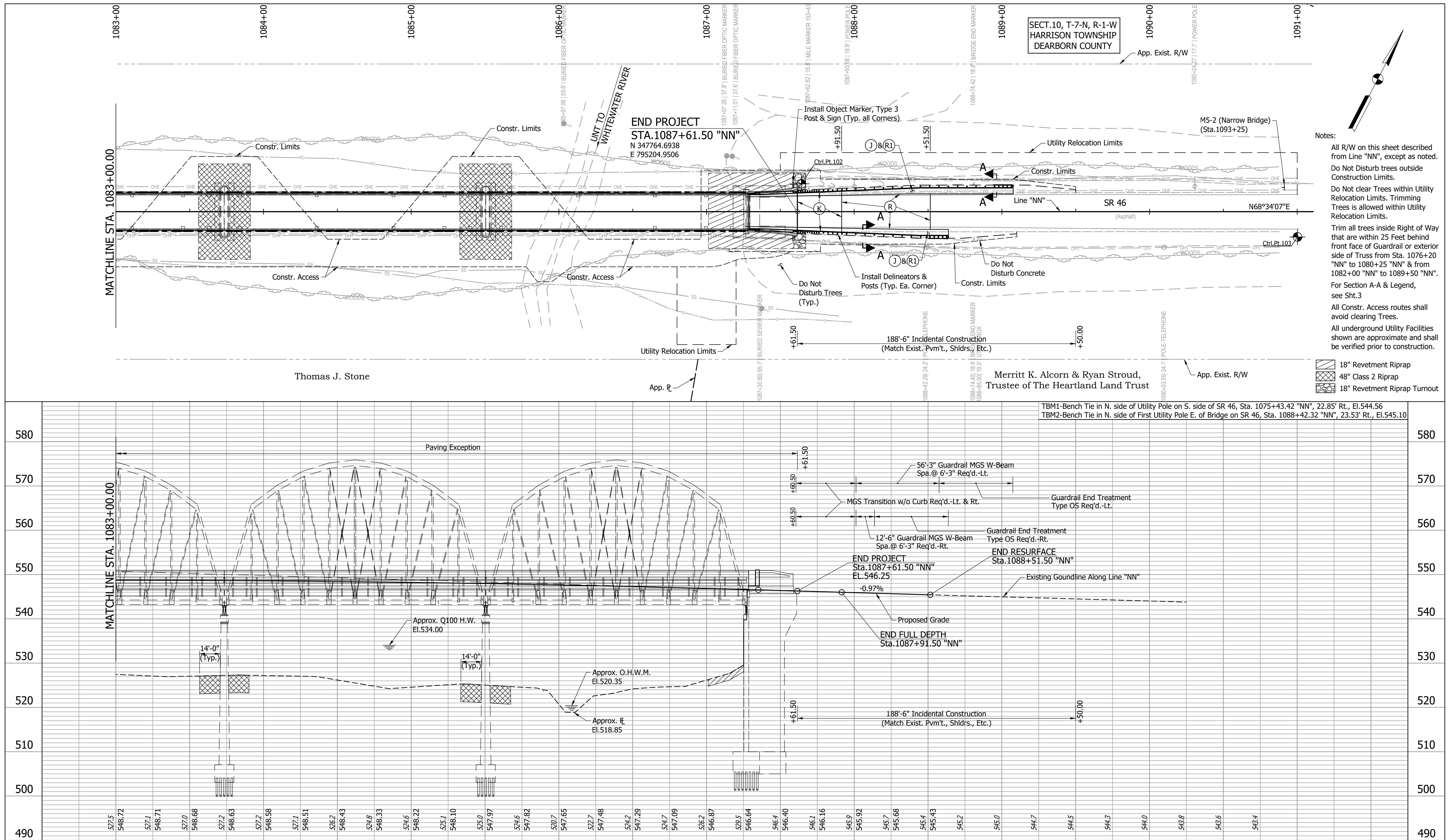
INDIANA DEPARTMENT OF TRANSPORTATION	
TYPICAL SECTIONS	

HORIZONTAL SCALE	BRIDGE FILE
AS NOTED	046-15-01987 B
VERTICAL SCALE	DESIGNATION
AS NOTED	1383721
DRAWING NO.	Sheets
	3 of 36
CONTRACT	PROJECT
B-42401	1900094





RECOMMENDED FOR APPROVAL			INDIANA DEPARTMENT OF TRANSPORTATION	
DESIGNED: RTW	DRAWN: MEN	DATE	PLAN & PROFILE LINE "NN"	
CONTROL PT. #100	CONTROL PT. #101	CONTROL PT. #102	CONTROL PT. #103	CONTROL PT. #100
RECOMMENDED FOR APPROVAL	DESIGNED: RTW	DRAWN: MEN	CONTROL PT. #100	CONTROL PT. #100
RECOMMENDED FOR APPROVAL	CHECKED: AVW	CHECKED: AVW	RECOMMENDED FOR APPROVAL	RECOMMENDED FOR APPROVAL



**PT. #100 (Left):**  
 N 347260.8363  
 E 793992.3949  
 N  
 Mag Nail in Utility Pole  
 Barer Rd.  
 SR 46  
 Mag Nail in Utility Pole  
 82.46'  
 56.88'  
 Magnail  
 Mag Nail in Utility Pole  
 69.65'  
 Asphalt Dr.

**PT. #101 (Center):**  
 N 347432.7532  
 E 794306.9079  
 N  
 Rebar/Cap  
 Corner of Cope Wall  
 Mag Nail in Utility Pole  
 16.19'  
 17.59'  
 SR 46  
 Bridge Deck  
 Sign Post 8.48'  
 Asphalt Dr.

**PT. #102 (Right):**  
 N 347783.2411  
 E 795200.5972  
 Rebar/  
 Corner of Cope Wall  
 Bridge Deck

A surveyor's sketch showing a utility pole marked with a square symbol. The pole is on a hillside with a grade of 35.69'. A north arrow (N) is shown. A 'Cap' is indicated on the pole. A 'Mag Nail in Utility Pole' is marked with a square symbol. The sketch also shows 'SR 46' and 'Mile Post 153+43 4.84'.

1000100


1007-100	RECOMMEND FOR APPROVAL
	DESIGNED: <u>R</u>
	CHECKED: A

ED /AL	DESIGN END
RTW	DRAWN: M
VW	CHECKED:

ENGINEER	DATE	
EN		
AVW		

IND  
DEPARTMENT OF  
PLAN &  
INFO

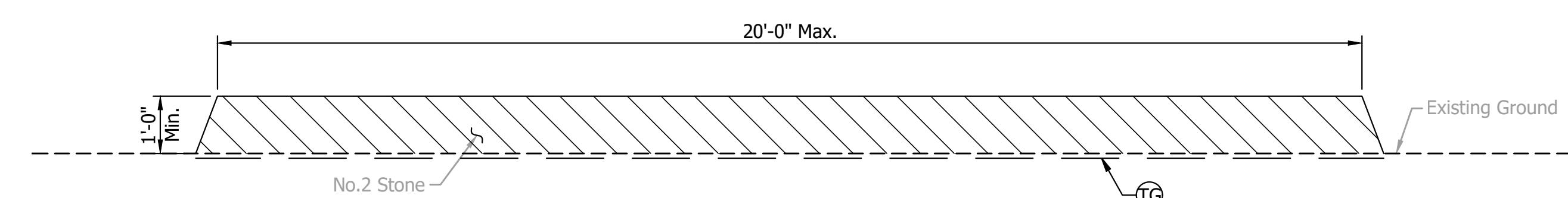
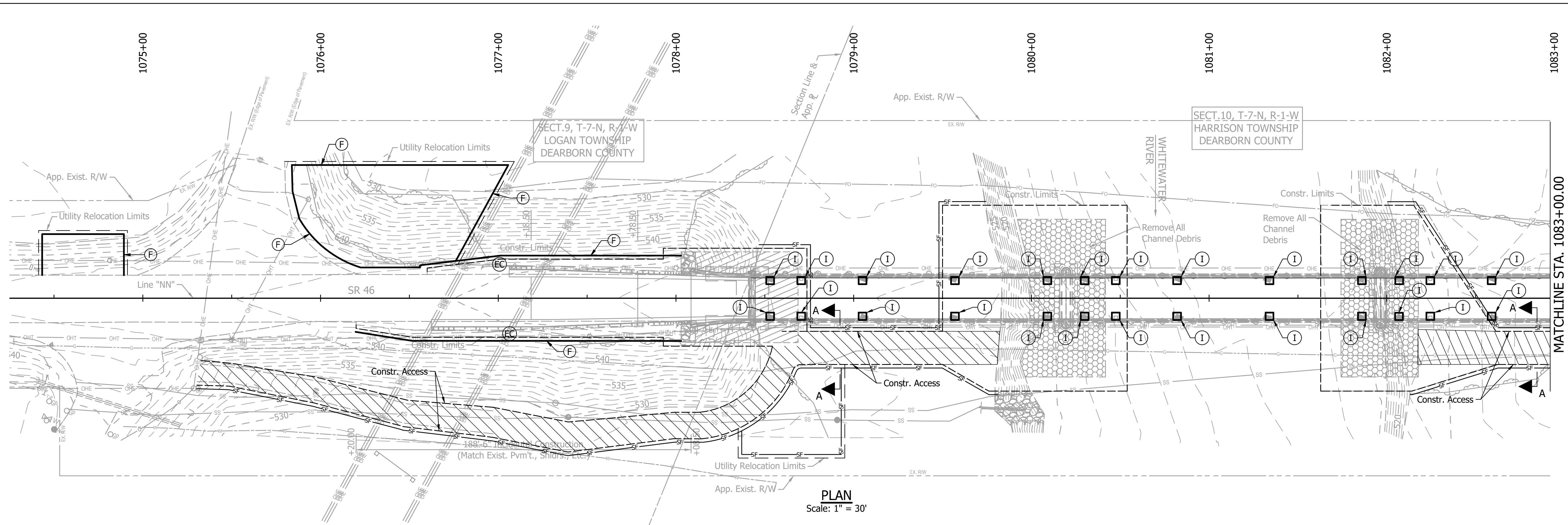
1050100

# IANA TRANSPORTATION PROFILE "NN"

INDIANA  
DEPARTMENT OF TRANSPORTATION

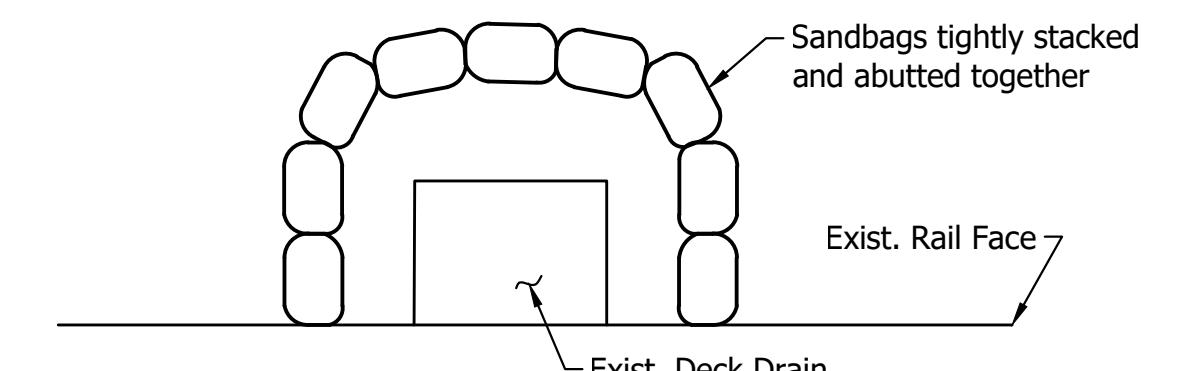
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HORIZONTAL SCALE		BRIDGE FILE		
1"=30'		046-15-01987 B		
VERTICAL SCALE		DESIGNATION		
1"=10'		1383721		
DRAWING NO.		SHEETS		
		6	of	49
CONTRACT		PROJECT		
B-42401		1900094		



## CONSTRUCTION ACCESS - SECTION A-A

Scale: 1/2" =



## TYPICAL TEMPORARY INLET PROTECTION DETAIL

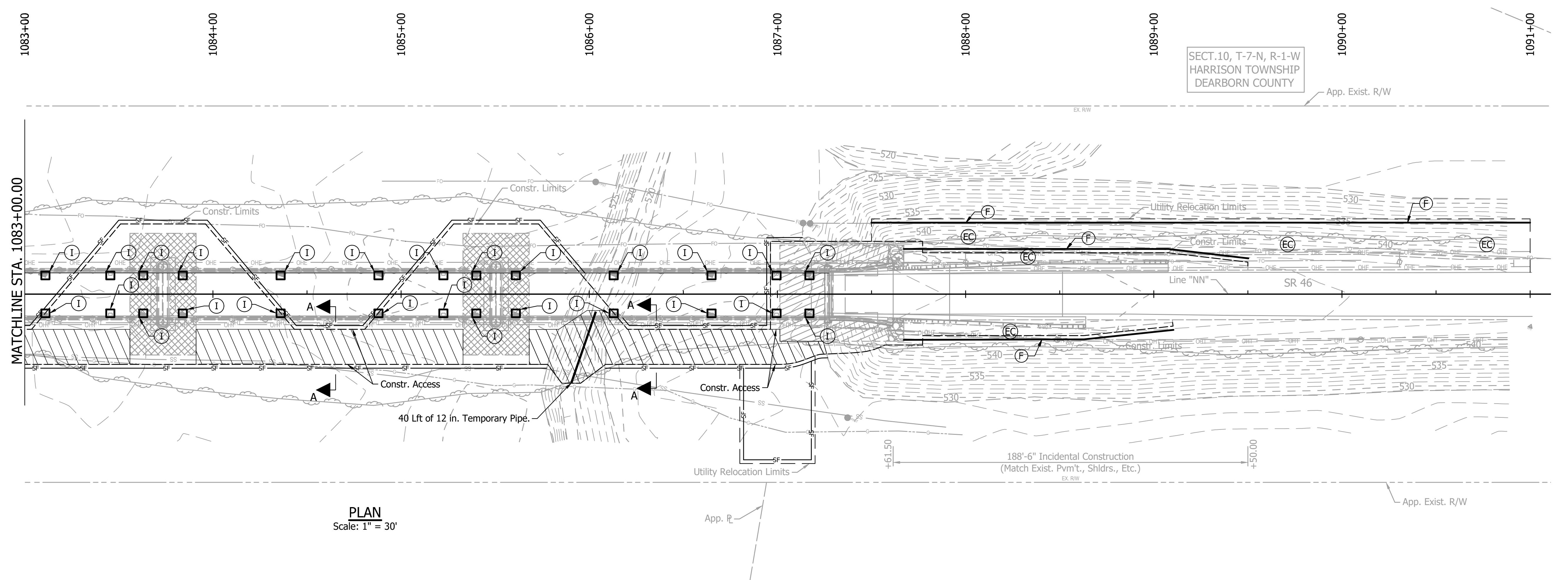
## No Scale

## LEGEND

- (F) Filter Sock
- SF — Temporary Silt Fence
- (I) Temporary Inlet Protection
- (EC) Manufactured Surface Protection Product, Type 1
- (TG) Temporary Geotextile
-  No. 2 Stone

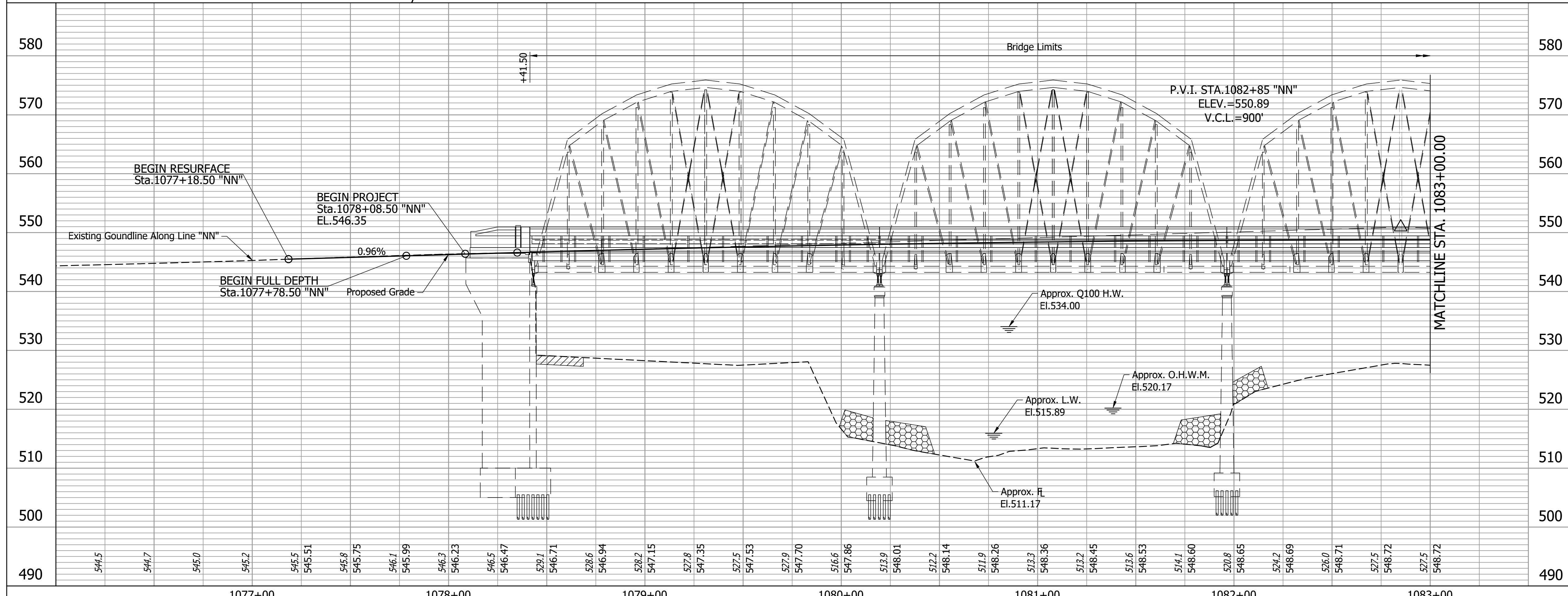
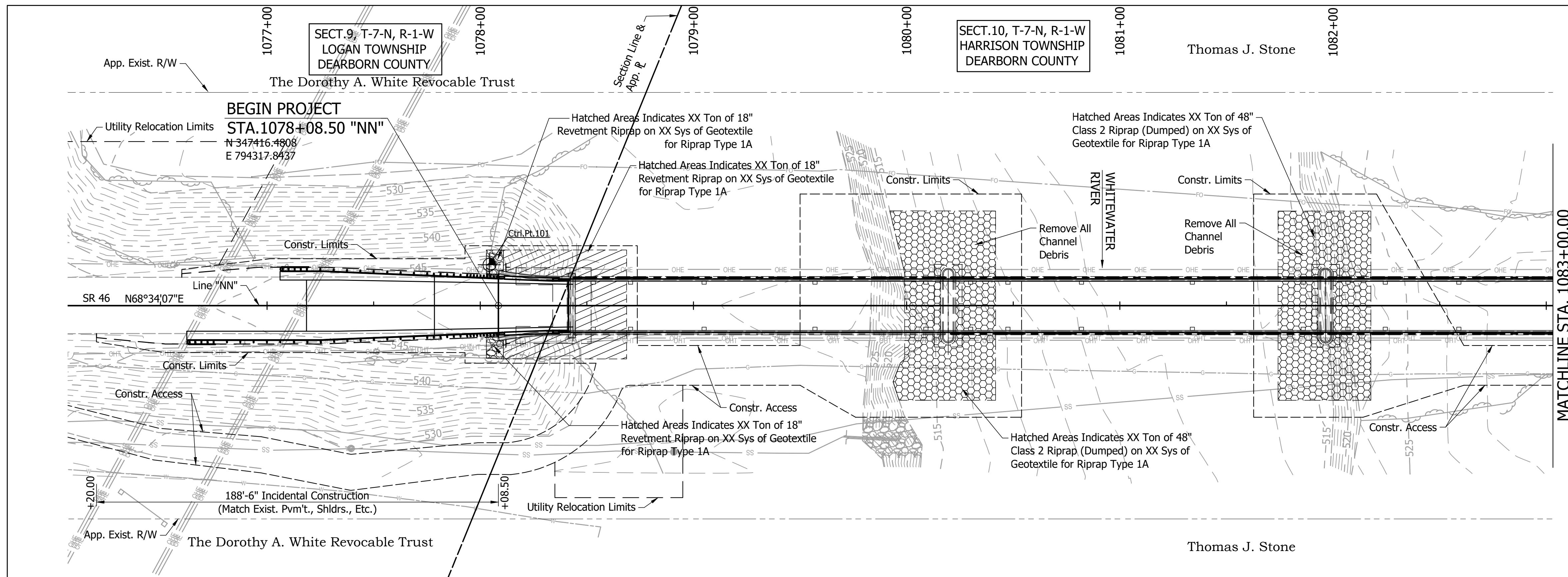
**Note:**  
Install Temporary Inlet Protection at each Existing Deck Drain.

<b>INDIANA</b> <b>DEPARTMENT OF TRANSPORTATION</b>		<b>EROSION CONTROL</b> <b>LINE "NN"</b>	
<b>RECOMMENDED FOR APPROVAL</b>		<b>DESIGN ENGINEER</b> _____ <b>DATE</b> _____	
<b>RECOMMENDED FOR APPROVAL</b>		<b>HORIZONTAL SCALE</b> _____ <b>AS NOTED</b> _____ <b>VERTICAL SCALE</b> _____ <b>AS NOTED</b> _____	
<b>RECOMMENDED FOR APPROVAL</b>		<b>BRIDGE FILE</b> _____ <b>461-0987 B</b> <b>DESIGNATION</b> _____ <b>1832</b>	
<b>DESIGNED: RTW</b> _____		<b>DRAWN: LHL</b> _____ <b>7</b> of <b>49</b>	
<b>CHECKED: AVW</b> _____ <b>B-241</b>		<b>CHECKED: AVW</b> _____ <b>10004</b>	



Note: For Legend, Typical Temporary Inlet Protection Detail, and Construction Acess Section A-A, see Sht. 7. Install Temporary Inlet Protection at each Existing Deck Drain.

<p style="text-align: center;"><b>INDIANA</b> <b>DEPARTMENT OF TRANSPORTATION</b></p>		<p style="text-align: center;">HORIZONTAL SCALE      BRIDGEFILE 1' = 3'      4615-0937B</p>	
<p style="text-align: center;">DESIGN ENGINEER      DATE</p>		<p style="text-align: center;">DRAWING NO.      SHEETS DESIGNED: RTW      DRAWN: LH 8      of      49</p>	
<p style="text-align: center;">CHECKED: AVW      CONTRACT B-241</p>		<p style="text-align: center;">PROJECT CHECKED: AW 9004</p>	
<p><b>EROSION CONTROL</b> <b>LINE "NN"</b></p>			



EXISTING STRUCTURE	
Existing Structure is a 5 Span Steel Through Bridge (5@ 175'-0") with a 24'-0" Clear Roadway. (To Be Rehabilitated)	

#### EARTHWORK SUMMARY

Common Excavation	xxx Cys
Usable Common Excavation (xx%)	xxx Cys
Fill + 20%	xxx Cys
Waterway Excavation	xx Cys
Usable Waterway Excavation (50%)	xx Cys
Borrow/Waste	xx Cys

The estimated quantities for Benching are xxx Cys for Cut and xxx Cys for Fill and are not included in the Earthwork Summary.

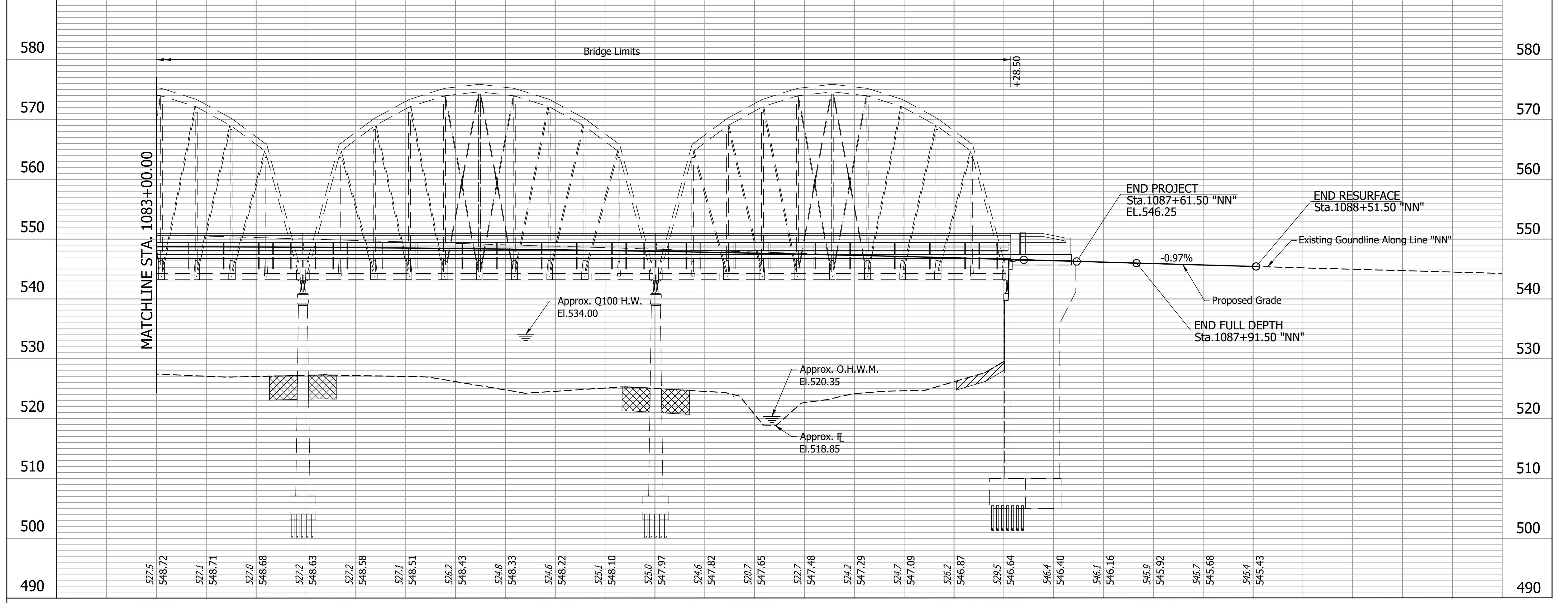
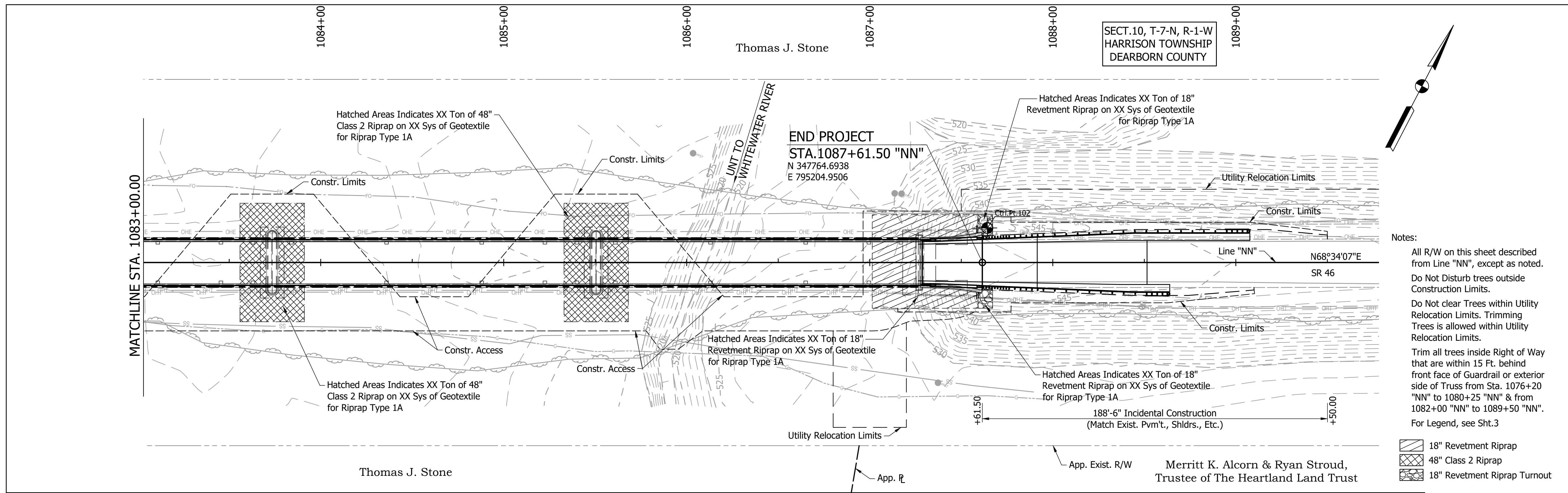
#### HYDRAULIC DATA

Drainage Area	1331 Sq Mi
Design Discharge, Q100	62,000 cfs
Design Discharge, Q500	N/A cfs
High Water Elevation, Q100	El.534.00
Existing Bridge	
Skew	0°
Flowline Elevation (@ Upstream Coping)	El.511.17
Contraction Scour, Q100	0.40 ft
Pier Scour, Q100 (Pier 2 Controls)	14.96 ft
Total Scour, Q100 (Pier 2 Controls)	15.36 ft
Low Scour Elevation, Q100 (Pier 2 Controls)	El.495.81
Max. Velocity, Q100 (C Channel)	10.32 ft/sec
Avg. Velocity, Q100	6.38 ft/sec

#### STEEL THROUGH TRUSS BRIDGE

5 SPANS @ 175'-0"  
23'-1 1/2" CLEAR ROADWAY SKEW: 0°  
SR 46 OVER WHITEWATER RIVER  
DEARBORN COUNTY

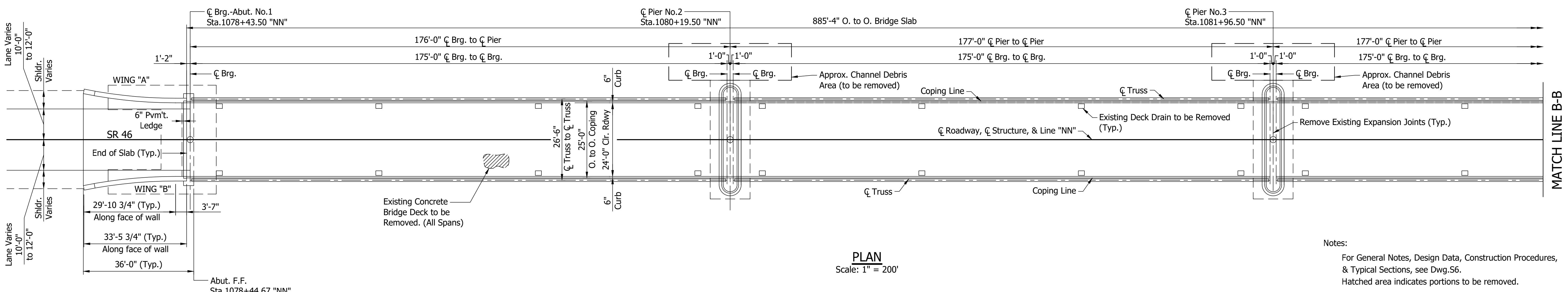
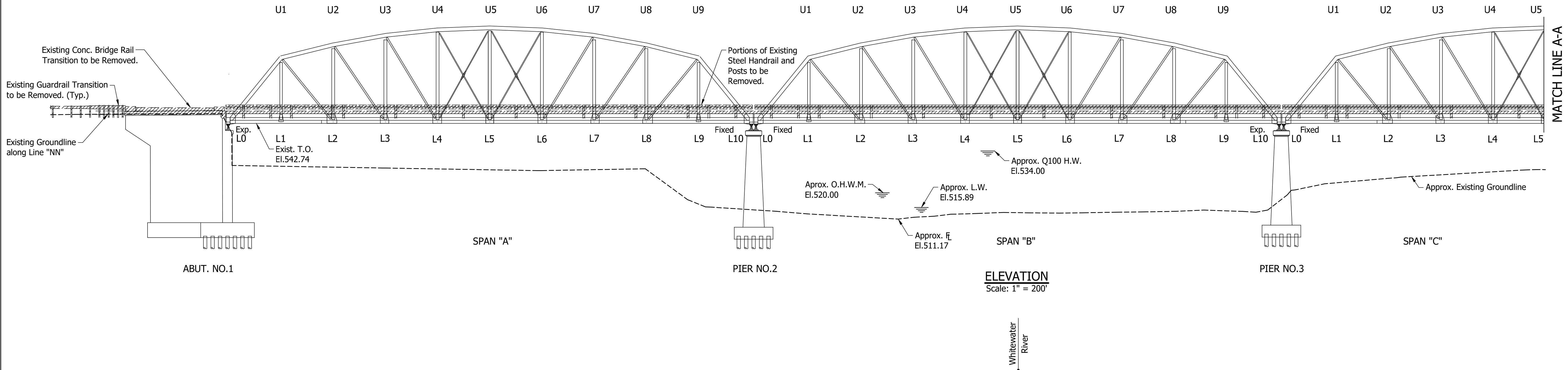
RECOMMENDED FOR APPROVAL		INDIANA DEPARTMENT OF TRANSPORTATION	
DESIGNED: RTW	DRAWN: MEN	LAYOUT LINE "NN"	
RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE	INDIANA DEPARTMENT OF TRANSPORTATION
RECOMMENDED FOR APPROVAL	DESIGNED: RTW	DRAWN: MEN	LAYOUT LINE "NN"
RECOMMENDED FOR APPROVAL	CHECKED: AVW	CHECKED: AVW	LAYOUT LINE "NN"
RECOMMENDED FOR APPROVAL	CHECKED: AVW	CHECKED: AVW	LAYOUT LINE "NN"



STEEL THROUGH TRUSS BRIDGE  
5 SPANS @ 175'-0"  
1 1/2" CLEAR ROADWAY      SKEW: 0°  
SR 46 OVER WHITEWATER RIVER  
DEARBORN COUNTY

1083+00	1084+00	1085+00	1086+00	1087+00	1088+00
<b>RECOMMENDED FOR APPROVAL</b>			<b>INDIANA DEPARTMENT OF TRANSPORTATION</b>		
<b>RECOMMENDED FOR APPROVAL</b>			<b>HORIZONTAL SCALE</b> $1' = 30'$ <b>VERTICAL SCALE</b> $1' = 10'$		
<b>RECOMMENDED FOR APPROVAL</b>			<b>BRIDGEFILE</b> 461-0937 B <b>DESIGNATION</b> 38371		
<b>RECOMMENDED FOR APPROVAL</b>			<b>DESIGN ENGINEER</b> _____ <b>DATE</b> _____		
<b>DESIGNED: RTW</b> S20 of S20			<b>DRAWN: MHN</b> 10 of 49		
<b>CHECKED: AVW</b> B-2401			<b>SHETS</b> <b>PROJECT</b> 10004		
<b>LINE 'NN'</b>			<b>LINE 'NN'</b>		

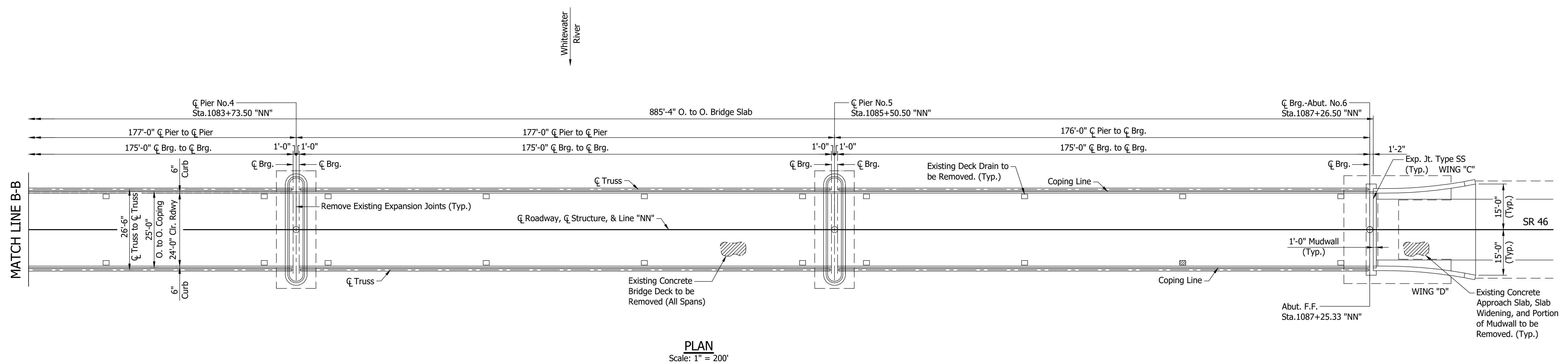
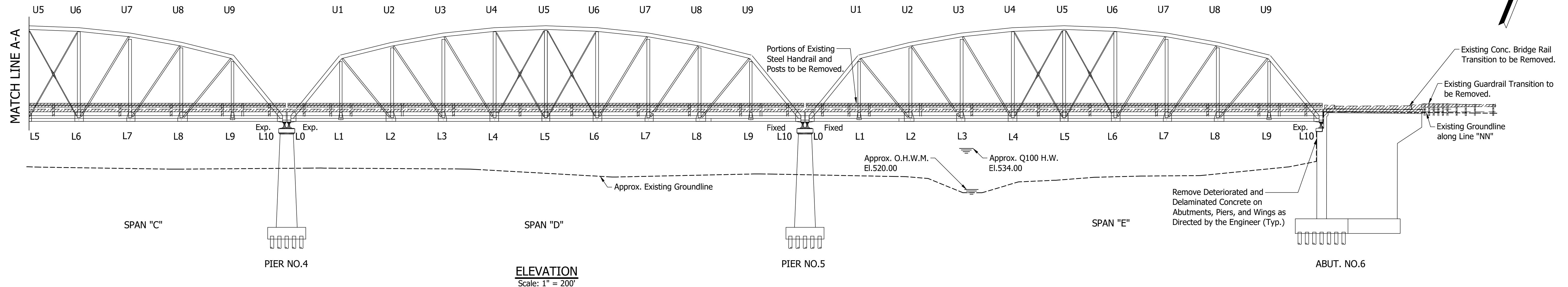
## EXISTING STRUCTURE BUILT TO 900' VERTICAL CURVE



STEEL THROUGH TRUSS BRIDGE  
5 SPANS @ 175'-0"  
24'-0" CLEAR ROADWAY SKEW: 0°  
SR 46 OVER WHITEWATER RIVER  
DEARBORN COUNTY

RECOMMENDED FOR APPROVAL	INDIANA DEPARTMENT OF TRANSPORTATION		HORIZONTAL SCALE 1" = 200'	BRIDGE FILE 046-15-01987 B
	DESIGN ENGINEER	DATE		
	DESIGNED: RTW	DRAWN: LLH		
GENERAL PLAN EXISTING	CHECKED: AVW	CHECKED: AVW	VERTICAL SCALE 1" = 200'	DESIGNATION 1383721
	CONTRACT	PROJECT		
	B-42401	1900094		

# STRUCTURE BUILT TO 900' VERTICAL CURVE

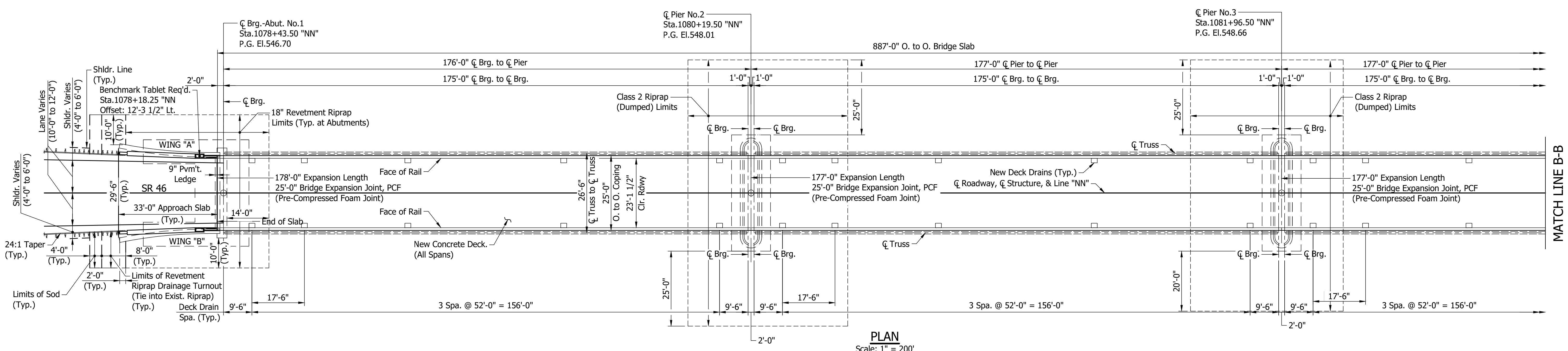
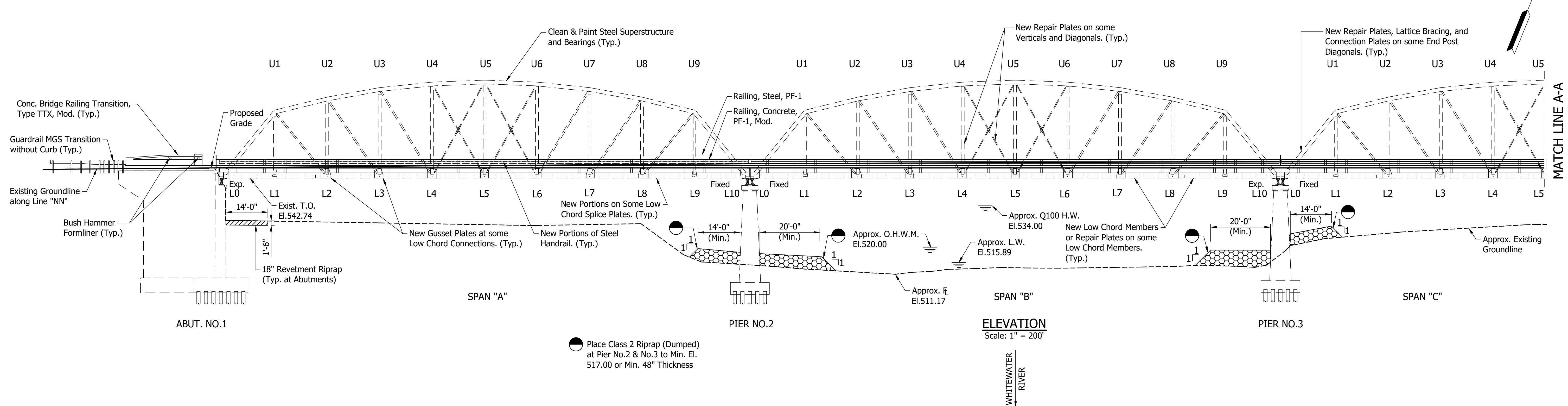


Notes:  
For General Notes, Design Data, Construction Procedures, & Typical Sections, see Dwg.S6.  
Hatched area indicates portions to be removed.

STEEL THROUGH TRUSS BRIDGE  
5 SPANS @ 175'-0"  
24'-0" CLEAR ROADWAY SKEW: 0°  
SR 46 OVER WHITEWATER RIVER  
DEARBORN COUNTY

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER		DATE	INDIANA DEPARTMENT OF TRANSPORTATION	HORIZONTAL SCALE		BRIDGE FILE	
	DESIGNED: RTW	DRAWN: LLH			AS NOTED		046-15-01987 B	
	CHECKED: AVW	CHECKED: AVW			VERTICAL SCALE		DESIGNATION	
				GENERAL PLAN EXISTING	DRAWING NO.	SHEETS		
					S4 of S7	12 of 36	CONTRACT	PROJECT
					B-42401			190094

## PROPOSED STRUCTURE BUILT TO 900' VERTICAL CURVE



## STEEL THROUGH TRUSS BRIDGE

SPANS @ 175'-0"

23'-1 1/2" CLEAR ROADWAY      SKEW: 0°

# OVER WHITEWATER RIV

# EARBORN COUNTY

## Notes

For General Notes, Design Data, Construction Procedures, & Typical Sections, see Dwg.S7.

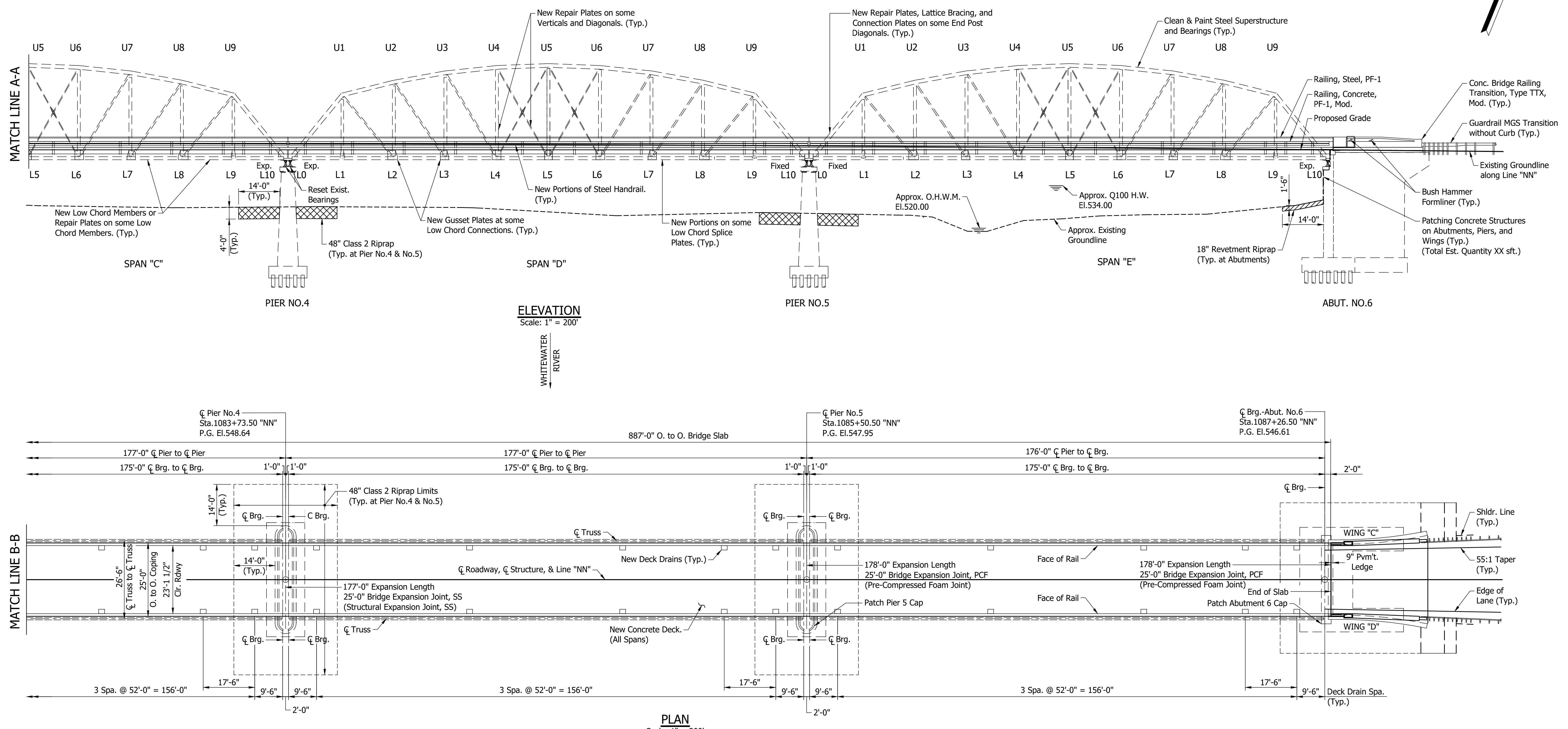
INDIANA  
DEPARTMENT OF TRANSPORTATION

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GENERAL PLAN

<p style="text-align: center;"><b>INDIANA</b> <b>DEPARTMENT OF TRANSPORTATION</b></p>		<p style="text-align: center;">GENERAL PLAN PROPOSED</p>	
		<p style="text-align: center;">HORIZONTAL SCALE      BRIDGE FILE</p> <p style="text-align: center;">1" = 200'      04-15-1987 B</p>	
<p style="text-align: center;">RECOMMENDED FOR APPROVAL</p>		<p style="text-align: center;">VERTICAL SCALE      DESIGNATION</p> <p style="text-align: center;">1" = 200'      1-3-821</p>	
<p style="text-align: center;">DESIGN ENGINEER      DATE</p>		<p style="text-align: center;">DRAWING NO.      SHEETS</p>	
<p style="text-align: center;">DESIGNED: RTW</p>		<p style="text-align: center;">DRAWN: LH</p>	
<p style="text-align: center;">S55fS7</p>		<p style="text-align: center;">13    of    35</p>	
<p style="text-align: center;">CONTRACT</p>		<p style="text-align: center;">PROJECT</p>	
<p style="text-align: center;">CHECKED: AVW</p>		<p style="text-align: center;">CHECKED: AVW</p>	
<p style="text-align: center;">B-4201</p>		<p style="text-align: center;">190094</p>	

# PROPOSED STRUCTURE BUILT TO 900' VERTICAL CURVE



STEEL THROUGH TRUSS BRIDGE  
5 SPANS @ 175'-0"  
23'-1 1/2" CLEAR ROADWAY SKEW: 0°  
SR 46 OVER WHITEWATER RIVER  
DEARBORN COUNTY

	RECOMMENDED FOR APPROVAL		INDIANA DEPARTMENT OF TRANSPORTATION	
	DESIGNED: RTW	DRAWN: LLH	GENERAL PLAN PROPOSED	
	CHECKED: AVW	CHECKED: AVW		
HORIZONTAL SCALE 1" = 200'		BRIDGE FILE 046-15-01987 B		
VERTICAL SCALE 1" = 200'		DESIGNATION 1383721		
DRAWING NO. S6 of S7		SHEETS 14 of 36		
CONTRACT B-42401		PROJECT 1900094		



# **Appendix C:**

# **Early Coordination**



# INDIANA DEPARTMENT OF TRANSPORTATION

100 North Senate Avenue  
Room N758-ES  
Indianapolis, Indiana 46204

Eric Holcomb, Governor  
Joe McGuinness, Commissioner

July 26, 2021

## SAMPLE EARLY COORDINATION LETTER

### Early Coordination Agency

Re: Early Coordination Letter, Des. No. 1383721, Bridge Project, in Dearborn County, Indiana, SR 46, 0.44 mile west of US 52 over Whitewater River.

Dear Early Coordination Agency:

The Indiana Department of Transportation (INDOT), with funding from the Federal Highway Administration (FHWA), intends to proceed with a project involving the aforementioned bridge (Bridge No. 046-15-01987A) in Dearborn County, Indiana. Beam, Longest and Neff, LLC (BLN) is under contract to advance the environmental documentation for the referenced project. This letter is part of the early coordination phase of the environmental review process. We are requesting comments from your area of expertise regarding any possible environmental effects associated with this project. **Please use the above designation numbers and description in your reply.** We will incorporate your comments into a study of the project's environmental impacts.

The proposed project involves Bridge No. 046-15-01987A, a historic "Non-Select" bridge on State Road (SR) 46 over Whitewater River, located approximately 0.44 mile west of US 52 near West Harrison, Indiana. Bridge No. 046-15-01987A is a five-span steel truss structure constructed in 1937. The existing structure has five equal 175-foot-long spans. The bridge was rehabilitated in 1985 and painted in 1996. The bridge perpetuates drainage under SR 46 for Whitewater River. The clear roadway width on the bridge measures 24 feet (two, 10-foot-wide lanes with 2-foot-wide shoulders). The existing roadway approach consists of two, 12-foot-wide travel lanes with 2-foot-wide shoulders. The existing roadway facility is classified as a *Rural Major Collector* and is not on the US National Highway System or National Truck Network. The posted speed limit at the project location is 55 miles per hour.

Per the terms of the "Programmatic Agreement Regarding Management and Preservation of Indiana's Historic Bridges" (Historic Bridge PA), the Federal Highway Administration-Indiana Division (FHWA) will satisfy its Section 106 responsibilities involving "Select" and "Non-Select" bridges through the Project Development Process (PDP) of the Historic Bridges PA (Stipulation III). Because Bridge No. 046-15-01987A is a "Non-Select" bridge, the procedures outlined in Stipulation III.B. of the Historic Bridges PA will be followed to fulfill FHWA's Section 106 responsibilities for the bridge.

The Historic Bridge PA PDP and Section 4(f) of the Department of Transportation Act of 1966 require the development of a Section 4(f) Alternatives Analysis in an effort to minimize harm and reduce impacts to historic structures. Per the guidelines of the Historic Bridges Programmatic Agreement, the applicable alternates will be evaluated to determine the most prudent and feasible option to meet the desired transportation need.

The preliminary preferred alternative for this project includes the rehabilitation of the existing bridge and the east and west approach roadway to current INDOT and FHWA criteria for load capacity and condition. The concrete deck, deck drains, and metal forms in all spans will be replaced, and new PF-1 bridge rail will be incorporated. Because PF-1 bridge rail will be used, the clear roadway will be reduced by 10 ½ inches. The travel lanes and shoulder widths will be transitioned from the bridge (10-foot-wide lanes and 1-foot, 6 ¾-inch wide shoulders) to the roadway approaches (12-foot-wide lanes and 2-foot-wide shoulders). The bridge joints will be replaced, and other deteriorated elements will be repaired or replaced in kind. All areas of the truss will be cleaned and painted. Deteriorated concrete caps of pier 5 and the east abutment will be removed and reconstructed. Delaminated and spalled areas of abutments and piers will be patched. Scour countermeasures,

if required, and debris removal will also be implemented. New approach slabs will be placed, and the rehabilitation will require approximately 250 feet of approach work east and west of the bridge. The existing approach guardrail will be replaced with MASH guardrail and end treatments.

The project will not require the acquisition of permanent or temporary right-of-way. Maintenance of traffic will consist of a full closure with a 1.5-mile long detour route utilizing US 52, I-74, and SR 1. The bridge will be closed for 6 months. Trees may need to be cleared to accommodate the project. Construction is anticipated to start in the spring of 2024.

Land use in the vicinity of the project is primarily agricultural and wooded floodplain with a few rural residential properties. Whitewater River and an unnamed tributary to Whitewater River are within the project area, and the project is also within an Indiana Department of Natural Resources (IDNR) floodplain. Waters and wetlands determinations will be performed by BLN to identify water resources that may be present. This project is anticipated to qualify for the application of the USFWS Range-Wide Programmatic informal consultation for the Indiana bat and northern long-eared bat, and project information will be submitted through USFWS's Information for Planning and Consultation (IPaC). Qualified Professionals will evaluate the project area for archaeological and historic resources for Section 106 compliance. The results of this investigation will be forwarded to the INDOT Cultural Resources Office (CRO) and the State Historic Preservation Officer (SHPO) for review and concurrence.

Please provide your response **within thirty (30) calendar days** from the date of this letter. However, should you find that an extension to the response time is necessary, a reasonable amount may be granted upon request. If you have any questions regarding this matter, please feel free to contact this office by email at [klewis@b-l-n.com](mailto:klewis@b-l-n.com) or by telephone at (317) 806-3056 or the INDOT Project Manager, Greg Prince, by email at [gprince@indot.in.gov](mailto:gprince@indot.in.gov) or by telephone at (812) 524-3783. Thank you in advance for your input.

Sincerely,



Kirsten Lewis  
Senior Environmental Analyst  
Beam, Longest and Neff, LLC  
8320 Craig Street  
Indianapolis, Indiana 46250

Early Coordination Mailing List

Maps (Location, Topographic, Initial Red Flag Summary)

Ground-Level Photograph

**Maps and photos are available in Appendix B**



# INDIANA DEPARTMENT OF TRANSPORTATION

100 North Senate Avenue  
Room N758-ES  
Indianapolis, Indiana 46204

PHONE: (317) 515-7908

**Eric Holcomb, Governor**  
**Joe McGuinness, Commissioner**

## EARLY COORDINATION MAILING LIST

Federal Highway Administration  
Federal Office Building, Room 254  
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erica.tait@dot.gov

David Dye  
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Seymour District  
Indiana Department of Transportation  
ddye@indot.in.gov

Indiana Geological and Water Survey  
611 North Walnut Grove  
Bloomington, IN 47405  
(Electronic Coordination)

Field Supervisor  
U.S. Fish and Wildlife Service  
Bloomington Indiana Field Office  
620 South Walker Street  
Bloomington, IN 47403  
robin\_mcwilliams@fws.gov

Environmental Coordinator  
Indiana Department of Natural Resources  
Division of Fish and Wildlife  
402 West Washington Street, Room W273  
Indianapolis, IN 46204  
Electronic Coordination –  
environmentalreview@dnr.in.gov

State Conservationist  
Natural Resources Conservation Service  
6013 Lakeside Blvd.  
Indianapolis, IN 46278  
rick.neilson@in.usda.gov

Indiana Department of Environmental Management  
(Electronic Coordination)

Attn: Bridge Branch  
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St. Louis, Missouri 63103-2832  
eric.washburn@uscg.mil

Regional Environmental Coordinator  
Midwest Regional Office  
National Park Service  
601 Riverfront Drive  
Omaha, NE 68102  
mwro\_compliance@nps.gov

Chief, Groundwater Section  
Indiana Department of Environmental Management  
100 N. Senate Ave.  
Indianapolis, IN 46204  
(Electronic Coordination)

Field Environmental Officer  
Chicago Regional Officer  
U.S. Department of Housing and Urban Development  
Metcalf Federal Building  
77 W. Jackson Blvd, Room 2401  
Chicago, IL 60604  
melanie.h.castillo@hud.gov

Ms. Deborah Snyder  
U.S. Army Corps of Engineers  
Louisville District, Indianapolis Regulatory Office  
Indianapolis, IN 46216  
regulatoryapplicationsLRL@usace.army.mil

Dearborn County Highway Superintendent  
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Project Manager, Seymour District  
Indiana Department of Transportation  
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West Harrison, IN 47060  
[scanter@enveng.com](mailto:scanter@enveng.com); [stleon@enveng.com](mailto:stleon@enveng.com)

Site Assessment & Management  
Environmental Policy Office - Environmental Services  
Indiana Department of Transportation  
100 N Senate Avenue, Room N758-ES  
Indianapolis, IN 46204  
[esd.sam@indot.in.gov](mailto:esd.sam@indot.in.gov)

Dearborn County Historical Society  
508 West High Street  
Lawrenceburg, IN 47025  
[dearborncohis@gmail.com](mailto:dearborncohis@gmail.com)

**THIS IS NOT A PERMIT**

**State of Indiana  
DEPARTMENT OF NATURAL RESOURCES  
Division of Fish and Wildlife  
Early Coordination/Environmental Assessment**

**DNR #:** ER-23938

**Request Received:** July 26, 2021

**Requestor:** Beam, Longest and Neff, LLC  
Kirsten Lewis  
8320 Craig Street  
Indianapolis, IN 46250

**Project:** SR 46 bridge (#046-15-01987A) rehabilitation over Whitewater River, about 0.44 mile west of US 52; Des #1383721

**County/Site info:** Dearborn

The Indiana Department of Natural Resources has reviewed the above referenced project per your request. Our agency offers the following comments for your information and in accordance with the National Environmental Policy Act of 1969.

If our agency has regulatory jurisdiction over the project, the recommendations contained in this letter may become requirements of any permit issued. If we do not have permitting authority, all recommendations are voluntary.

**Regulatory Assessment:** This proposal will require the formal approval of our agency for construction in a floodway, pursuant to the Flood Control Act (IC 14-28-1), unless it qualifies under the INDOT and IDNR Memorandum of Understanding for Maintenance Activity Exemption, dated March 1997. Please include a copy of this letter with the permit application, if required.

**Natural Heritage Database:** The Natural Heritage Program's data have been checked. The species below have been documented within 1/2 mile of the project area. The Division of Nature Preserves does not anticipate any impacts to the insect species as a result of this project.  
A) INSECT: Cobblestone Tiger Beetle (*Cicindela marginipennis*); state endangered  
B) ANIMALS:  
1. Bald Eagle (*Haliaeetus leucocephalus*); federally protected  
2. Variegate Darter (*Etheostoma variatum*); state endangered

**Fish & Wildlife Comments:** Avoid and minimize impacts to fish, wildlife, and botanical resources to the greatest extent possible, and compensate for impacts. The following are recommendations that address potential impacts identified in the proposed project area:

1) Animals:

a. BALD EAGLE:

The Bald Eagle is no longer a state species of special concern. However, this species is still federally protected (see <https://www.fws.gov/midwest/eagle/history/protections.html>). The recommended buffer between any disturbance and an active eagle nest is 660 feet. To minimize impacts to Bald Eagles, follow the National Bald Eagle Management Guidelines found at <http://www.fws.gov/midwest/eagle/pdf/NationalBaldEagleManagementGuidelines.pdf>. Please contact the US Fish and Wildlife Service if further consultation is needed regarding Bald Eagles.

b. VARIEGATE DARTER:

In addition to the Variegate Darter, the Northern Madtom (*Noturus stigmosus*), a state species of special concern, has also recently been documented near this site. If a causeway is required for this project, it must not be placed across any rocky riffles with fast moving water, which is prime habitat for these fish species.

State of Indiana  
DEPARTMENT OF NATURAL RESOURCES  
Division of Fish and Wildlife  
Early Coordination/Environmental Assessment

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2) Wildlife Passage:

Maintaining or improving fish and wildlife passage at existing or proposed crossing locations is a priority for the Division of Fish & Wildlife (DFW) to reduce wildlife mortality along roadways. The DFW has outlined different requirements for different types of crossing structure impacts. For brand new crossings in areas that currently do not have a crossing, the new structure must accommodate white-tailed deer passage where appropriate. Minimum structure dimensions for white-tailed deer passage are 20 feet of width clearance (overall size of the structure span) and 8 feet of height clearance measured from the OHWM to the low chord elevation and where deer passage is provided. For crossing replacements, the new structure must include wildlife passage appropriate for the type of replacement structure being proposed. If the replacement structure is sized to accommodate white-tailed deer passage then it should be included in the design of the new structure. If white-tailed deer passage is not possible with the existing structure, deer passage still needs to be considered in the design and at minimum the bank lines must be restored within structures to allow for smaller wildlife passage above the ordinary high water mark. All wildlife passage designs must include a smooth level pathway a minimum of 1-2 feet in width composed of natural substrate (soil, sand, gravel, etc.) or compacted aggregate fill over riprap (#2, #53, #73, etc.) tied into existing elevations both upstream and downstream. The stream crossing repairs or modifications, and any bank stabilization under or around the structure, must not create conditions that are less favorable for wildlife passage when compared to existing conditions. Upgrading wildlife passage for rehabilitated/modified structures is encouraged whenever possible to improve wildlife/vehicle safety.

There are a number of techniques and materials for incorporating wildlife passage into the design of a crossing structure. Coordination with a Regional Environmental Biologist to address wildlife passage issues before submitting a permit application (if required) is encouraged to avoid delays in the permitting process. The following links are good resources to consider in the design of stream crossing structures to maintain fish and wildlife passage: <http://www.fs.fed.us/wildlifecrossings/library/>, [https://roadecology.ucdavis.edu/files/content/projects/DOT-FHWA\\_Wildlife\\_Crossing\\_Structures\\_Handbook.pdf](https://roadecology.ucdavis.edu/files/content/projects/DOT-FHWA_Wildlife_Crossing_Structures_Handbook.pdf), [https://www.fs.fed.us/biology/nsaec/fishxing/aop\\_pdfs.html](https://www.fs.fed.us/biology/nsaec/fishxing/aop_pdfs.html), <https://www.fhwa.dot.gov/engineering/hydraulics/pubs/11008/hif11008.pdf>.

3) Bank Stabilization:

Establishing vegetation along the banks is critical for stabilization and erosion control. In addition to vegetation, some other form of bank stabilization may be needed. While hard armoring alone (e.g. riprap or glacial stone) may be needed in certain instances, soft armoring and bioengineering techniques should be considered first. In many instances, one or more methods are necessary to increase the likelihood of vegetation establishment. Combining vegetation with most bank stabilization methods can provide additional bank protection and help reduce impacts upon fish and wildlife. Information about bioengineering techniques can be found at <http://www.in.gov/legislative/iac/20120404-IR-312120154NRA.xml.pdf>. Also, the following is a USDA/NRCS document that outlines many different bioengineering techniques for streambank stabilization: <http://directives.sc.egov.usda.gov/17553.wba>.

Riprap must not be placed in the active thalweg channel or placed in the streambed in a manner that precludes fish or aquatic organism passage (riprap must not be placed above the existing streambed elevation). Riprap may be used only at the toe of the sideslopes up to the ordinary high water mark (OHWM). The banks above the OHWM must be restored, stabilized, and revegetated using geotextiles and a mixture of grasses, sedges, wildflowers, shrubs, and trees native to southeastern Indiana and specifically for stream bank/floodway stabilization purposes as soon as possible upon completion.

State of Indiana  
DEPARTMENT OF NATURAL RESOURCES  
Division of Fish and Wildlife  
Early Coordination/Environmental Assessment

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4) Riparian Habitat:

We recommend a mitigation plan be developed (and submitted with the permit application, if required) for any unavoidable habitat impacts that will occur. The DNR's Habitat Mitigation Guidelines (and plant lists) can be found online at: <http://iac.iga.in.gov/iac/20200527-IR-312200284NRA.xml.pdf>.

Impacts to non-wetland forest of one (1) acre or more should be mitigated at a minimum 2:1 ratio. If less than one acre of non-wetland forest is removed in a rural setting, replacement should be at a 1:1 ratio based on area. Impacts to non-wetland forest under one (1) acre in an urban setting should be mitigated by planting five trees, at least 2 inches in diameter-at-breast height (dbh), for each tree which is removed that is 10" dbh or greater (5:1 mitigation based on the number of large trees) or by using the 1:1 replacement ratio based on area depending on the type of habitat impacted (individual canopy tree removal in an urban streetscape or park-like environment versus removal of habitat supporting a tree canopy, woody understory, and herbaceous layer). Impacts under 0.10 acre in an urban area may still involve the replacement of large diameter trees but typically do not require any additional mitigation or additional plantings beyond seeding and stabilizing disturbed areas. There are exceptions for high quality habitat sites however.

5) Stream/Wetland Habitat:

For any stream and/or wetland impacts, you may need to contact the Indiana Department of Environmental Management (IDEM) 401 program and the US Army Corps of Engineers (USACE) 404 program.

The additional measures listed below should be implemented to avoid, minimize, or compensate for impacts to fish, wildlife, and botanical resources:

1. Revegetate all bare and disturbed areas that will not be mowed and maintained with a mixture of grasses, sedges, and wildflowers native to Eastern Indiana and specifically for stream bank/floodway stabilization purposes as soon as possible upon completion; turf-type grasses (including low-endophyte, friendly endophyte, and endophyte free tall fescue but excluding all other varieties of tall fescue) may be used in regularly mowed areas only.
2. Minimize and contain within the project limits inchannel disturbance and the clearing of trees and brush.
3. Do not work in the waterway from April 1 through June 30 without the prior written approval of the Division of Fish and Wildlife.
4. Do not cut any trees suitable for Indiana bat or Northern Long-eared bat roosting (greater than 5 inches dbh, living or dead, with loose hanging bark, or with cracks, crevices, or cavities) from April 1 through September 30.
5. Do not excavate in the low flow area except for the placement of piers, foundations, and riprap, or removal of the old structure.
6. Do not construct any temporary runarounds, access bridges, causeways, cofferdams, diversions, or pumparounds.
7. Use minimum average 6 inch graded riprap stone extended below the normal water level to provide habitat for aquatic organisms in the voids.
8. Do not use broken concrete as riprap.
9. Underlay the riprap with a bedding layer of well graded aggregate or a geotextile to prevent piping of soil underneath the riprap.
10. Minimize the movement of resuspended bottom sediment from the immediate project area.
11. Appropriately designed measures for controlling erosion and sediment must be implemented to prevent sediment from entering the stream or leaving the construction site; maintain these measures until construction is complete and all disturbed areas are stabilized.

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Division of Fish and Wildlife  
Early Coordination/Environmental Assessment

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12. Seed and protect all disturbed streambanks and slopes not protected by other methods that are 3:1 or steeper with erosion control blankets that are heavy-duty, biodegradable, and net free or that use loose-woven / Leno-woven netting to minimize the entrapment and snaring of small-bodied wildlife such as snakes and turtles (follow manufacturer's recommendations for selection and installation); seed and apply mulch on all other disturbed areas.
13. Do not excavate or place fill in any riparian wetland.

**Contact Staff:**

Christie L. Stanifer, Environ. Coordinator, Fish & Wildlife  
Our agency appreciates this opportunity to be of service. Please contact the above staff member at (317) 232-4080 if we can be of further assistance.

*Christie L. Stanifer*

**Date:** August 24, 2021

Christie L. Stanifer  
Environ. Coordinator  
Division of Fish and Wildlife

## Organization and Project Information

**Project ID:**

**Des. ID:** 1383721

**Project Title:** Bridge Rehabilitation on SR 46 over Whitewater River

**Name of Organization:** Beam, Longest and Neff

**Requested by:** Kristin Wing

## Environmental Assessment Report

### 1. Geological Hazards:

- High liquefaction potential
- Floodway
- Potential Slope Instability

### 2. Mineral Resources:

- Bedrock Resource: Low Potential
- Sand and Gravel Resource: Low Potential

### 3. Active or abandoned mineral resources extraction sites:

- Petroleum Exploration Wells

\*All map layers from Indiana Map ([maps.indiana.edu](http://maps.indiana.edu))

### **DISCLAIMER:**

This document was compiled by Indiana University, Indiana Geological Survey, using data believed to be accurate; however, a degree of error is inherent in all data. This product is distributed "AS-IS" without warranties of any kind, either expressed or implied, including but not limited to warranties of suitability to a particular purpose or use. No attempt has been made in either the design or production of these data and document to define the limits or jurisdiction of any federal, state, or local government. The data used to assemble this document are intended for use only at the published scale of the source data or smaller (see the metadata links below) and are for reference purposes only. They are not to be construed as a legal document or survey instrument. A detailed on-the-ground survey and historical analysis of a single site may differ from these data and this document.

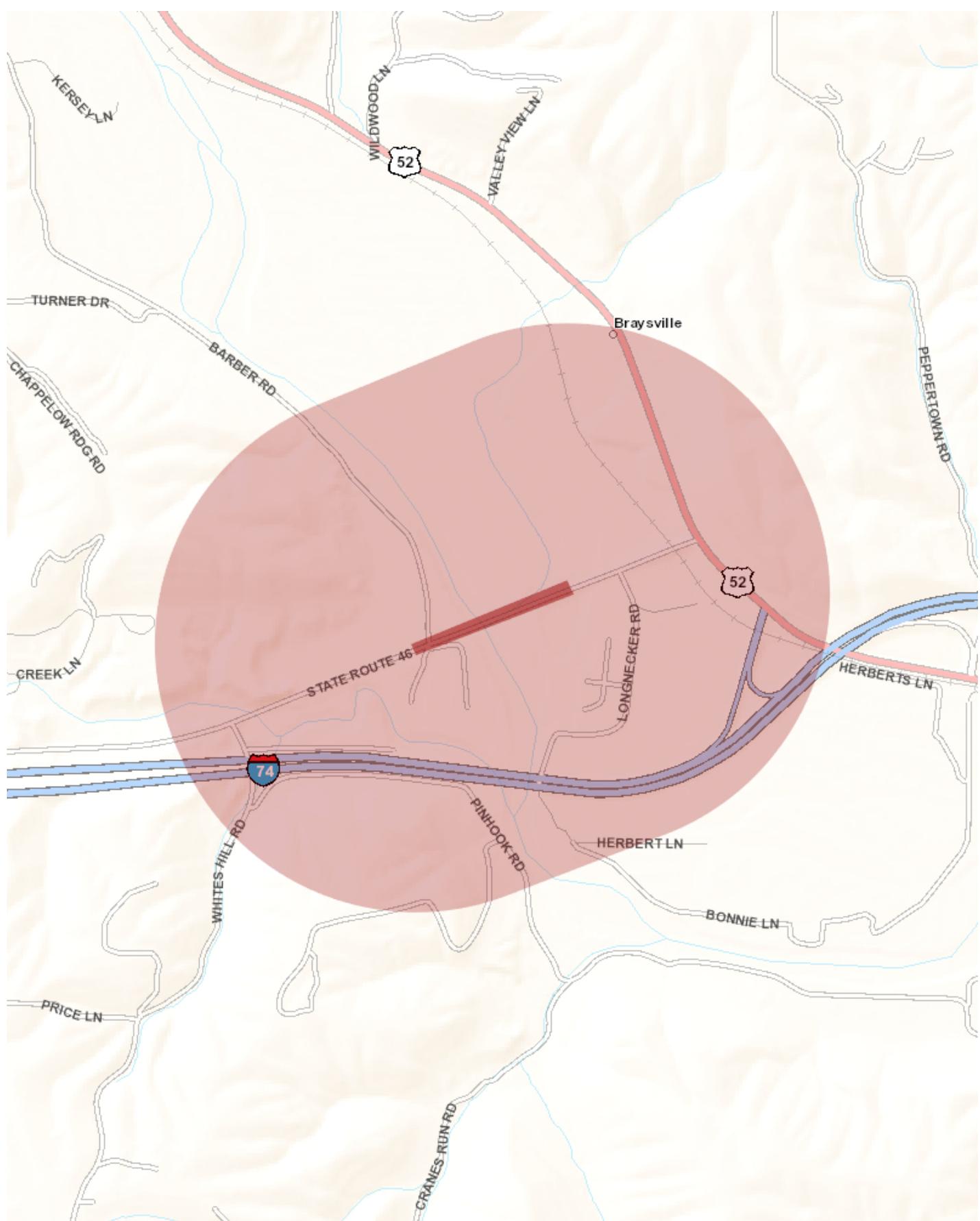
This information was furnished by Indiana Geological Survey

Address: 1001 E. 10th St., Bloomington, IN 47405

Email: [IGSEnvir@indiana.edu](mailto:IGSEnvir@indiana.edu)

Phone: 812 855-7428

Date: July 20, 2022



# Metadata:

- [https://maps.indiana.edu/metadata/Geology/Petroleum\\_Wells.html](https://maps.indiana.edu/metadata/Geology/Petroleum_Wells.html)
- [https://maps.indiana.edu/metadata/Geology/Seismic\\_Earthquake\\_Liquefaction\\_Potential.html](https://maps.indiana.edu/metadata/Geology/Seismic_Earthquake_Liquefaction_Potential.html)
- [https://maps.indiana.edu/metadata/Geology/Industrial\\_Minerals\\_Sand\\_Gravel\\_Resources.html](https://maps.indiana.edu/metadata/Geology/Industrial_Minerals_Sand_Gravel_Resources.html)
- [https://maps.indiana.edu/metadata/Hydrology/Floodplains\\_FIRM.html](https://maps.indiana.edu/metadata/Hydrology/Floodplains_FIRM.html)
- [https://maps.indiana.edu/metadata/Geology/Bedrock\\_Geology.html](https://maps.indiana.edu/metadata/Geology/Bedrock_Geology.html)



# United States Department of the Interior

FISH AND WILDLIFE SERVICE  
Indiana Ecological Services Field Office  
620 South Walker Street  
Bloomington, IN 47403-2121  
Phone: (812) 334-4261 Fax: (812) 334-4273



In Reply Refer To:

June 30, 2022

Project Code: 2022-0059549

Project Name: SR 46 over Whitewater River Bridge Deck Overlay DES 1383721

Subject: List of threatened and endangered species that may occur in your proposed project location or may be affected by your proposed project

To Whom It May Concern:

The enclosed species list identifies threatened, endangered, proposed and candidate species, as well as proposed and final designated critical habitat, that may occur within the boundary of your proposed project and/or may be affected by your proposed project. The species list fulfills the requirements of the U.S. Fish and Wildlife Service (Service) under section 7(c) of the Endangered Species Act (Act) of 1973, as amended (16 U.S.C. 1531 *et seq.*).

New information based on updated surveys, changes in the abundance and distribution of species, changed habitat conditions, or other factors could change this list. Please feel free to contact us if you need more current information or assistance regarding the potential impacts to federally proposed, listed, and candidate species and federally designated and proposed critical habitat. Please note that under 50 CFR 402.12(e) of the regulations implementing section 7 of the Act, the accuracy of this species list should be verified after 90 days. This verification can be completed formally or informally as desired. The Service recommends that verification be completed by visiting the ECOS-IPaC website at regular intervals during project planning and implementation for updates to species lists and information. An updated list may be requested through the ECOS-IPaC system by completing the same process used to receive the enclosed list.

The purpose of the Act is to provide a means whereby threatened and endangered species and the ecosystems upon which they depend may be conserved. Under sections 7(a)(1) and 7(a)(2) of the Act and its implementing regulations (50 CFR 402 *et seq.*), Federal agencies are required to utilize their authorities to carry out programs for the conservation of threatened and endangered species and to determine whether projects may affect threatened and endangered species and/or designated critical habitat.

Please use the species list provided and visit the U.S. Fish and Wildlife Service's Region 3 Section 7 Technical Assistance website at - <http://www.fws.gov/midwest/endangered/section7/s7process/index.html>. This website contains step-by-step instructions which will help you

determine if your project will have an adverse effect on listed species and will help lead you through the Section 7 process. For all **wind energy projects** and **projects that include installing towers that use guy wires or are over 200 feet in height**, please contact this field office directly for assistance, even if no federally listed plants, animals or critical habitat are present within your proposed project or may be affected by your proposed project.

A Biological Assessment is required for construction projects (or other undertakings having similar physical impacts) that are major Federal actions significantly affecting the quality of the human environment as defined in the National Environmental Policy Act (42 U.S.C. 4332(2)(c)). For projects other than major construction activities, the Service suggests that a biological evaluation similar to a Biological Assessment be prepared to determine whether the project may affect listed or proposed species and/or designated or proposed critical habitat. Recommended contents of a Biological Assessment are described at 50 CFR 402.12.

If a Federal agency determines, based on the Biological Assessment or biological evaluation, that listed species and/or designated critical habitat may be affected by the proposed project, the agency is required to consult with the Service pursuant to 50 CFR 402. In addition, the Service recommends that candidate species, proposed species and proposed critical habitat be addressed within the consultation. More information on the regulations and procedures for section 7 consultation, including the role of permit or license applicants, can be found in the "Endangered Species Consultation Handbook" at:

<http://www.fws.gov/endangered/esa-library/pdf/TOC-GLOS.PDF>

**Migratory Birds:** In addition to responsibilities to protect threatened and endangered species under the Endangered Species Act (ESA), there are additional responsibilities under the Migratory Bird Treaty Act (MBTA) and the Bald and Golden Eagle Protection Act (BGEPA) to protect native birds from project-related impacts. Any activity, intentional or unintentional, resulting in take of migratory birds, including eagles, is prohibited unless otherwise permitted by the U.S. Fish and Wildlife Service (50 C.F.R. Sec. 10.12 and 16 U.S.C. Sec. 668(a)). For more information regarding these Acts see <https://www.fws.gov/birds/policies-and-regulations.php>.

The MBTA has no provision for allowing take of migratory birds that may be unintentionally killed or injured by otherwise lawful activities. It is the responsibility of the project proponent to comply with these Acts by identifying potential impacts to migratory birds and eagles within applicable NEPA documents (when there is a federal nexus) or a Bird/Eagle Conservation Plan (when there is no federal nexus). Proponents should implement conservation measures to avoid or minimize the production of project-related stressors or minimize the exposure of birds and their resources to the project-related stressors. For more information on avian stressors and recommended conservation measures see <https://www.fws.gov/birds/bird-enthusiasts/threats-to-birds.php>.

In addition to MBTA and BGEPA, Executive Order 13186: *Responsibilities of Federal Agencies to Protect Migratory Birds*, obligates all Federal agencies that engage in or authorize activities that might affect migratory birds, to minimize those effects and encourage conservation measures that will improve bird populations. Executive Order 13186 provides for the protection of both migratory birds and migratory bird habitat. For information regarding the implementation of

Executive Order 13186, please visit <https://www.fws.gov/birds/policies-and-regulations/executive-orders/e0-13186.php>.

We appreciate your concern for threatened and endangered species. The Service encourages Federal agencies to include conservation of threatened and endangered species into their project planning to further the purposes of the Act. **Please include the Consultation Code in the header of this letter with any request for consultation or correspondence about your project that you submit to our office.**

Attachment(s):

- Official Species List
- Migratory Birds
- Wetlands

## **Official Species List**

This list is provided pursuant to Section 7 of the Endangered Species Act, and fulfills the requirement for Federal agencies to "request of the Secretary of the Interior information whether any species which is listed or proposed to be listed may be present in the area of a proposed action".

This species list is provided by:

**Indiana Ecological Services Field Office**

620 South Walker Street  
Bloomington, IN 47403-2121  
(812) 334-4261

## Project Summary

Project Code: 2022-0059549  
Event Code: None  
Project Name: SR 46 over Whitewater River Bridge Deck Overlay DES 1383721  
Project Type: Bridge - Maintenance  
Project Description: The preliminary preferred alternative for this project includes the rehabilitation of the existing bridge (Structure # 046-15-01987A) and the east and west approach roadway to current INDOT and FHWA criteria for load capacity and condition. The concrete deck, deck drains, and metal forms in all spans will be replaced, and new PF-1 bridge rail will be incorporated. Because PF-1 bridge rail will be used, the clear roadway will be reduced by 10 ½ inches. The travel lanes and shoulder widths will be transitioned from the bridge (10-foot-wide lanes and 1-foot, 6 ¾-inch wide shoulders) to the roadway approaches (12-foot-wide lanes and 2-foot-wide shoulders). The bridge joints will be replaced, and other deteriorated elements will be repaired or replaced in kind. All areas of the truss will be cleaned and painted. Deteriorated concrete caps of pier 5 and the east abutment will be removed and reconstructed. Delaminated and spalled areas of abutments and piers will be patched. Scour counter-measures, if required, and debris removal will also be implemented. New approach slabs will be placed, and the rehabilitation will require approximately 250 feet of approach work east and west of the bridge. The existing approach guardrail will be replaced with MASH guardrail and end treatments.

The project will not require the acquisition of permanent or temporary right-of-way.

\_\_\_\_\_ acres of trees may need to be cleared (within 100 feet of the roadway) to accommodate the project. Dominant tree species in the project area include: American sycamore (*Platanus occidentalis*), and American elm (*Ulmus americana*).

Bat Check Email date and results:

INDOT BIAS date:

A consultant bat check was completed on September 9, 2021. No bats or evidence of bats were observed within the project area.

Temporary or permanent lighting:

Mitigation:

Construction is anticipated to start in the spring of 2024.

Land use in the vicinity of the project is primarily agricultural and wooded floodplain with a few rural residential properties.

**Project Location:**

Approximate location of the project can be viewed in Google Maps: <https://www.google.com/maps/@39.27997000000006,-84.87431380246792,14z>



Counties: Dearborn County, Indiana

## Endangered Species Act Species

There is a total of 3 threatened, endangered, or candidate species on this species list.

Species on this list should be considered in an effects analysis for your project and could include species that exist in another geographic area. For example, certain fish may appear on the species list because a project could affect downstream species. Note that 1 of these species should be considered only under certain conditions.

IPaC does not display listed species or critical habitats under the sole jurisdiction of NOAA Fisheries<sup>1</sup>, as USFWS does not have the authority to speak on behalf of NOAA and the Department of Commerce.

See the "Critical habitats" section below for those critical habitats that lie wholly or partially within your project area under this office's jurisdiction. Please contact the designated FWS office if you have questions.

1. [NOAA Fisheries](#), also known as the National Marine Fisheries Service (NMFS), is an office of the National Oceanic and Atmospheric Administration within the Department of Commerce.

### Mammals

NAME	STATUS
Indiana Bat <i>Myotis sodalis</i>	Endangered
There is <b>final</b> critical habitat for this species. The location of the critical habitat is not available. Species profile: <a href="https://ecos.fws.gov/ecp/species/5949">https://ecos.fws.gov/ecp/species/5949</a>	
Northern Long-eared Bat <i>Myotis septentrionalis</i>	Threatened
No critical habitat has been designated for this species. This species only needs to be considered under the following conditions: <ul style="list-style-type: none"> <li>Incidental take of the NLEB is not prohibited here. Federal agencies may consult using the 4(d) rule streamlined process. Transportation projects may consult using the programmatic process. See <a href="http://www.fws.gov/midwest/endangered/mammals/nleb/index.html">www.fws.gov/midwest/endangered/mammals/nleb/index.html</a></li> </ul> Species profile: <a href="https://ecos.fws.gov/ecp/species/9045">https://ecos.fws.gov/ecp/species/9045</a>	

### Insects

NAME	STATUS
Monarch Butterfly <i>Danaus plexippus</i>	Candidate

No critical habitat has been designated for this species.  
Species profile: <https://ecos.fws.gov/ecp/species/9743>

### Critical habitats

THERE ARE NO CRITICAL HABITATS WITHIN YOUR PROJECT AREA UNDER THIS OFFICE'S JURISDICTION.

# Migratory Birds

Certain birds are protected under the Migratory Bird Treaty Act<sup>1</sup> and the Bald and Golden Eagle Protection Act<sup>2</sup>.

Any person or organization who plans or conducts activities that may result in impacts to migratory birds, eagles, and their habitats should follow appropriate regulations and consider implementing appropriate conservation measures, as described [below](#).

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1. The [Migratory Birds Treaty Act](#) of 1918.
2. The [Bald and Golden Eagle Protection Act](#) of 1940.
3. 50 C.F.R. Sec. 10.12 and 16 U.S.C. Sec. 668(a)

**The birds listed below are birds of particular concern either because they occur on the [USFWS Birds of Conservation Concern](#) (BCC) list or warrant special attention in your project location.** To learn more about the levels of concern for birds on your list and how this list is generated, see the [FAQ below](#). This is not a list of every bird you may find in this location, nor a guarantee that every bird on this list will be found in your project area. To see exact locations of where birders and the general public have sighted birds in and around your project area, visit the [E-bird data mapping tool](#) (Tip: enter your location, desired date range and a species on your list). For projects that occur off the Atlantic Coast, additional maps and models detailing the relative occurrence and abundance of bird species on your list are available. Links to additional information about Atlantic Coast birds, and other important information about your migratory bird list, including how to properly interpret and use your migratory bird report, can be found [below](#).

For guidance on when to schedule activities or implement avoidance and minimization measures to reduce impacts to migratory birds on your list, click on the PROBABILITY OF PRESENCE SUMMARY at the top of your list to see when these birds are most likely to be present and breeding in your project area.

NAME	BREEDING SEASON
Bald Eagle <i>Haliaeetus leucocephalus</i> This is not a Bird of Conservation Concern (BCC) in this area, but warrants attention because of the Eagle Act or for potential susceptibilities in offshore areas from certain types of development or activities. <a href="https://ecos.fws.gov/ecp/species/1626">https://ecos.fws.gov/ecp/species/1626</a>	Breeds Sep 1 to Jul 31
Henslow's Sparrow <i>Ammodramus henslowii</i> This is a Bird of Conservation Concern (BCC) throughout its range in the continental USA and Alaska. <a href="https://ecos.fws.gov/ecp/species/3941">https://ecos.fws.gov/ecp/species/3941</a>	Breeds May 1 to Aug 31

NAME	BREEDING SEASON
Kentucky Warbler <i>Oporornis formosus</i> This is a Bird of Conservation Concern (BCC) throughout its range in the continental USA and Alaska.	Breeds Apr 20 to Aug 20
Prairie Warbler <i>Dendroica discolor</i> This is a Bird of Conservation Concern (BCC) throughout its range in the continental USA and Alaska.	Breeds May 1 to Jul 31
Red-headed Woodpecker <i>Melanerpes erythrocephalus</i> This is a Bird of Conservation Concern (BCC) throughout its range in the continental USA and Alaska.	Breeds May 10 to Sep 10
Rusty Blackbird <i>Euphagus carolinus</i> This is a Bird of Conservation Concern (BCC) only in particular Bird Conservation Regions (BCRs) in the continental USA	Breeds elsewhere
Wood Thrush <i>Hylocichla mustelina</i> This is a Bird of Conservation Concern (BCC) throughout its range in the continental USA and Alaska.	Breeds May 10 to Aug 31

## Probability Of Presence Summary

The graphs below provide our best understanding of when birds of concern are most likely to be present in your project area. This information can be used to tailor and schedule your project activities to avoid or minimize impacts to birds. Please make sure you read and understand the FAQ "Proper Interpretation and Use of Your Migratory Bird Report" before using or attempting to interpret this report.

### Probability of Presence (■)

Each green bar represents the bird's relative probability of presence in the 10km grid cell(s) your project overlaps during a particular week of the year. (A year is represented as 12 4-week months.) A taller bar indicates a higher probability of species presence. The survey effort (see below) can be used to establish a level of confidence in the presence score. One can have higher confidence in the presence score if the corresponding survey effort is also high.

How is the probability of presence score calculated? The calculation is done in three steps:

1. The probability of presence for each week is calculated as the number of survey events in the week where the species was detected divided by the total number of survey events for that week. For example, if in week 12 there were 20 survey events and the Spotted Towhee was found in 5 of them, the probability of presence of the Spotted Towhee in week 12 is 0.25.
2. To properly present the pattern of presence across the year, the relative probability of presence is calculated. This is the probability of presence divided by the maximum probability of presence across all weeks. For example, imagine the probability of presence in week 20 for the Spotted Towhee is 0.05, and that the probability of presence at week 12

(0.25) is the maximum of any week of the year. The relative probability of presence on week 12 is  $0.25/0.25 = 1$ ; at week 20 it is  $0.05/0.25 = 0.2$ .

3. The relative probability of presence calculated in the previous step undergoes a statistical conversion so that all possible values fall between 0 and 10, inclusive. This is the probability of presence score.

### Breeding Season (■)

Yellow bars denote a very liberal estimate of the time-frame inside which the bird breeds across its entire range. If there are no yellow bars shown for a bird, it does not breed in your project area.

### Survey Effort (|)

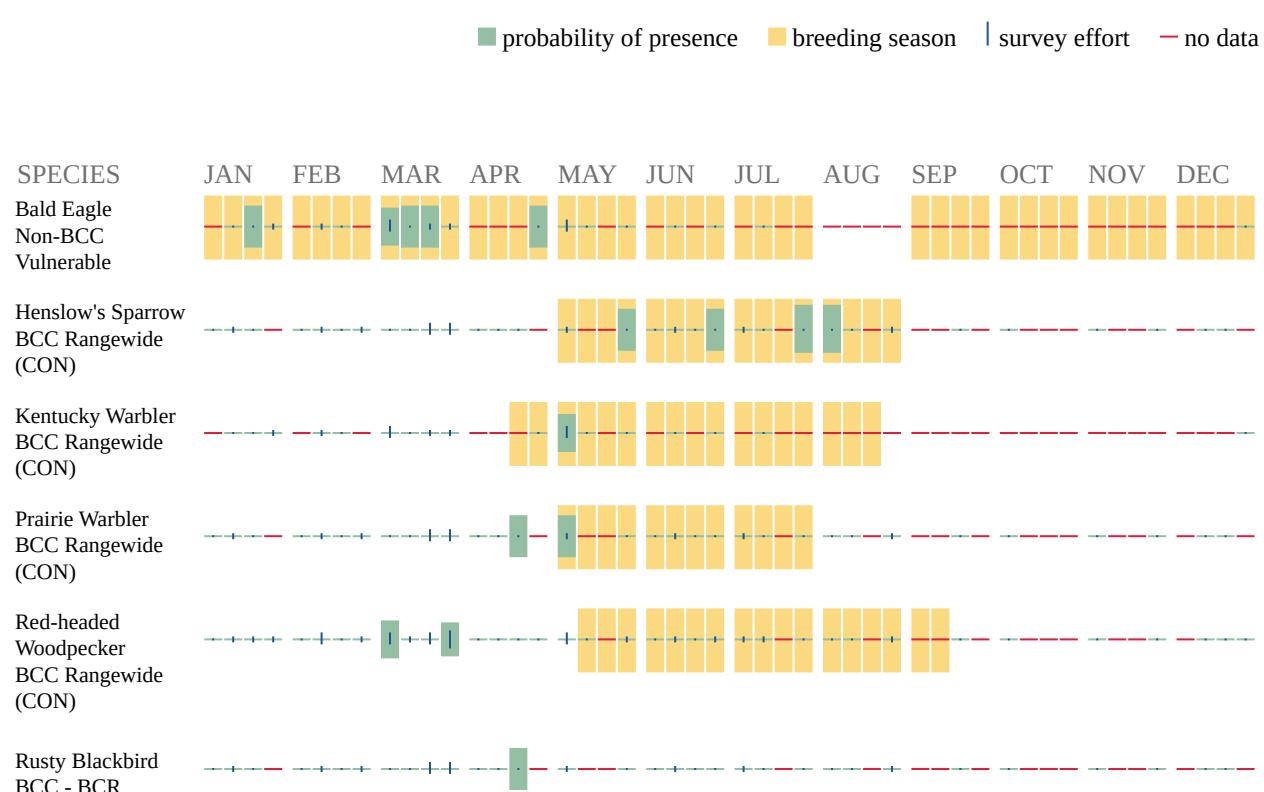
Vertical black lines superimposed on probability of presence bars indicate the number of surveys performed for that species in the 10km grid cell(s) your project area overlaps. The number of surveys is expressed as a range, for example, 33 to 64 surveys.

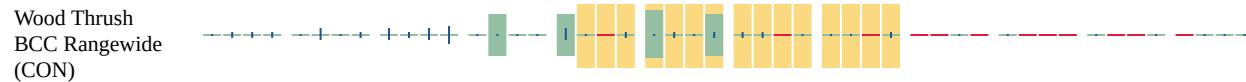
### No Data (-)

A week is marked as having no data if there were no survey events for that week.

### Survey Timeframe

Surveys from only the last 10 years are used in order to ensure delivery of currently relevant information. The exception to this is areas off the Atlantic coast, where bird returns are based on all years of available data, since data in these areas is currently much more sparse.





Additional information can be found using the following links:

- Birds of Conservation Concern <https://www.fws.gov/program/migratory-birds/species>
- Measures for avoiding and minimizing impacts to birds <https://www.fws.gov/library/collections/avoiding-and-minimizing-incidental-take-migratory-birds>
- Nationwide conservation measures for birds <https://www.fws.gov/sites/default/files/documents/nationwide-standard-conservation-measures.pdf>

## Migratory Birds FAQ

**Tell me more about conservation measures I can implement to avoid or minimize impacts to migratory birds.**

[Nationwide Conservation Measures](#) describes measures that can help avoid and minimize impacts to all birds at any location year round. Implementation of these measures is particularly important when birds are most likely to occur in the project area. When birds may be breeding in the area, identifying the locations of any active nests and avoiding their destruction is a very helpful impact minimization measure. To see when birds are most likely to occur and be breeding in your project area, view the Probability of Presence Summary. [Additional measures](#) or [permits](#) may be advisable depending on the type of activity you are conducting and the type of infrastructure or bird species present on your project site.

### What does IPaC use to generate the migratory birds potentially occurring in my specified location?

The Migratory Bird Resource List is comprised of USFWS [Birds of Conservation Concern \(BCC\)](#) and other species that may warrant special attention in your project location.

The migratory bird list generated for your project is derived from data provided by the [Avian Knowledge Network \(AKN\)](#). The AKN data is based on a growing collection of [survey, banding, and citizen science datasets](#) and is queried and filtered to return a list of those birds reported as occurring in the 10km grid cell(s) which your project intersects, and that have been identified as warranting special attention because they are a BCC species in that area, an eagle ([Eagle Act](#) requirements may apply), or a species that has a particular vulnerability to offshore activities or development.

Again, the Migratory Bird Resource list includes only a subset of birds that may occur in your project area. It is not representative of all birds that may occur in your project area. To get a list of all birds potentially present in your project area, please visit the [AKN Phenology Tool](#).

### What does IPaC use to generate the probability of presence graphs for the migratory birds potentially occurring in my specified location?

The probability of presence graphs associated with your migratory bird list are based on data provided by the [Avian Knowledge Network \(AKN\)](#). This data is derived from a growing collection of [survey, banding, and citizen science datasets](#).

Probability of presence data is continuously being updated as new and better information becomes available. To learn more about how the probability of presence graphs are produced and how to interpret them, go the Probability of Presence Summary and then click on the "Tell me about these graphs" link.

### **How do I know if a bird is breeding, wintering, migrating or present year-round in my project area?**

To see what part of a particular bird's range your project area falls within (i.e. breeding, wintering, migrating or year-round), you may refer to the following resources: [The Cornell Lab of Ornithology All About Birds Bird Guide](#), or (if you are unsuccessful in locating the bird of interest there), the [Cornell Lab of Ornithology Neotropical Birds guide](#). If a bird on your migratory bird species list has a breeding season associated with it, if that bird does occur in your project area, there may be nests present at some point within the timeframe specified. If "Breeds elsewhere" is indicated, then the bird likely does not breed in your project area.

### **What are the levels of concern for migratory birds?**

Migratory birds delivered through IPaC fall into the following distinct categories of concern:

1. "BCC Rangewide" birds are [Birds of Conservation Concern](#) (BCC) that are of concern throughout their range anywhere within the USA (including Hawaii, the Pacific Islands, Puerto Rico, and the Virgin Islands);
2. "BCC - BCR" birds are BCCs that are of concern only in particular Bird Conservation Regions (BCRs) in the continental USA; and
3. "Non-BCC - Vulnerable" birds are not BCC species in your project area, but appear on your list either because of the [Eagle Act](#) requirements (for eagles) or (for non-eagles) potential susceptibilities in offshore areas from certain types of development or activities (e.g. offshore energy development or longline fishing).

Although it is important to try to avoid and minimize impacts to all birds, efforts should be made, in particular, to avoid and minimize impacts to the birds on this list, especially eagles and BCC species of rangewide concern. For more information on conservation measures you can implement to help avoid and minimize migratory bird impacts and requirements for eagles, please see the FAQs for these topics.

### **Details about birds that are potentially affected by offshore projects**

For additional details about the relative occurrence and abundance of both individual bird species and groups of bird species within your project area off the Atlantic Coast, please visit the [Northeast Ocean Data Portal](#). The Portal also offers data and information about other taxa besides birds that may be helpful to you in your project review. Alternately, you may download the bird model results files underlying the portal maps through the [NOAA NCCOS Integrative Statistical Modeling and Predictive Mapping of Marine Bird Distributions and Abundance on the Atlantic Outer Continental Shelf](#) project webpage.

Bird tracking data can also provide additional details about occurrence and habitat use throughout the year, including migration. Models relying on survey data may not include this information. For additional information on marine bird tracking data, see the [Diving Bird Study](#) and the [nanotag studies](#) or contact [Caleb Spiegel](#) or [Pam Loring](#).

**What if I have eagles on my list?**

If your project has the potential to disturb or kill eagles, you may need to [obtain a permit](#) to avoid violating the Eagle Act should such impacts occur.

**Proper Interpretation and Use of Your Migratory Bird Report**

The migratory bird list generated is not a list of all birds in your project area, only a subset of birds of priority concern. To learn more about how your list is generated, and see options for identifying what other birds may be in your project area, please see the FAQ "What does IPaC use to generate the migratory birds potentially occurring in my specified location". Please be aware this report provides the "probability of presence" of birds within the 10 km grid cell(s) that overlap your project; not your exact project footprint. On the graphs provided, please also look carefully at the survey effort (indicated by the black vertical bar) and for the existence of the "no data" indicator (a red horizontal bar). A high survey effort is the key component. If the survey effort is high, then the probability of presence score can be viewed as more dependable. In contrast, a low survey effort bar or no data bar means a lack of data and, therefore, a lack of certainty about presence of the species. This list is not perfect; it is simply a starting point for identifying what birds of concern have the potential to be in your project area, when they might be there, and if they might be breeding (which means nests might be present). The list helps you know what to look for to confirm presence, and helps guide you in knowing when to implement conservation measures to avoid or minimize potential impacts from your project activities, should presence be confirmed. To learn more about conservation measures, visit the FAQ "Tell me about conservation measures I can implement to avoid or minimize impacts to migratory birds" at the bottom of your migratory bird trust resources page.

## Wetlands

Impacts to [NWI wetlands](#) and other aquatic habitats may be subject to regulation under Section 404 of the Clean Water Act, or other State/Federal statutes.

For more information please contact the Regulatory Program of the local [U.S. Army Corps of Engineers District](#).

Please note that the NWI data being shown may be out of date. We are currently working to update our NWI data set. We recommend you verify these results with a site visit to determine the actual extent of wetlands on site.

### FRESHWATER FORESTED/SHRUB WETLAND

- [PFO1A](#)

### RIVERINE

- [R2UBH](#)
- [R5UBH](#)
- [R4SBC](#)

## IPaC User Contact Information

Agency: Beam, Longest & Neff LLC

Name: Eleanor Prescott

Address: 8320 Craig Street

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State: IN

Zip: 46250

Email: eprescott@b-l-n.com

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# United States Department of the Interior



FISH AND WILDLIFE SERVICE  
Indiana Ecological Services Field Office  
620 South Walker Street  
Bloomington, IN 47403-2121  
Phone: (812) 334-4261 Fax: (812) 334-4273

In Reply Refer To:

August 05, 2022

Project code: 2022-0059549

Project Name: SR 46 over Whitewater River Bridge Deck Overlay DES 1383721

Subject: Concurrence verification letter for the 'SR 46 over Whitewater River Bridge Deck Overlay DES 1383721' project under the revised February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion for Transportation Projects within the Range of the Indiana Bat and Northern Long-eared Bat.

To whom it may concern:

The U.S. Fish and Wildlife Service (Service) has received your request dated August 05, 2022 to verify that the **SR 46 over Whitewater River Bridge Deck Overlay DES 1383721** (Proposed Action) may rely on the concurrence provided in the February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion for Transportation Projects within the Range of the Indiana Bat and Northern Long-eared Bat (PBO) to satisfy requirements under Section 7(a)(2) of the Endangered Species Act of 1973 (ESA) (87 Stat. 884, as amended; 16 U.S.C 1531 *et seq.*).

Based on the information you provided (Project Description shown below), you have determined that the Proposed Action is within the scope and adheres to the criteria of the PBO, including the adoption of applicable avoidance and minimization measures, and may affect, but is not likely to adversely affect (NLAA) the endangered Indiana bat (*Myotis sodalis*) and/or the threatened Northern long-eared bat (*Myotis septentrionalis*). Consultation with the Service pursuant to Section 7(a)(2) of the Endangered Species Act of 1973 (ESA) (87 Stat. 884, as amended; 16 U.S.C. 1531 *et seq.*) is required.

The Service has 14 calendar days to notify the lead Federal action agency or designated non-federal representative if we determine that the Proposed Action does not meet the criteria for a NLAA determination under the PBO. If we do not notify the lead Federal action agency or designated non-federal representative within that timeframe, you may proceed with the Proposed Action under the terms of the NLAA concurrence provided in the PBO. This verification period allows Service Field Offices to apply local knowledge to implementation of the PBO, as we may identify a small subset of actions having impacts that were unanticipated. In such instances, Service Field Offices may request additional information that is necessary to verify inclusion of the proposed action under the PBO.

**For Proposed Actions that include bridge/culvert or structure removal, replacement, and/or maintenance activities:** If your initial bridge/culvert or structure assessments failed to detect Indiana bats, but you later detect bats prior to, or during construction, please submit the Post Assessment Discovery of Bats at Bridge/Culvert or Structure Form (User Guide Appendix E) to this Service Office. In these instances, potential incidental take of Indiana bats may be exempted provided that the take is reported to the Service.

If the Proposed Action is modified, or new information reveals that it may affect the Indiana bat and/or Northern long-eared bat in a manner or to an extent not considered in the PBO, further review to conclude the requirements of ESA Section 7(a)(2) may be required. If the Proposed Action may affect any other federally-listed or proposed species, and/or any designated critical habitat, additional consultation between the lead Federal action agency and this Service Office is required. If the proposed action has the potential to take bald or golden eagles, additional coordination with the Service under the Bald and Golden Eagle Protection Act may also be required. In either of these circumstances, please contact this Service Office.

The following species may occur in your project area and **are not** covered by this determination:

- Monarch Butterfly *Danaus plexippus* Candidate

## Project Description

The following project name and description was collected in IPaC as part of the endangered species review process.

### **Name**

SR 46 over Whitewater River Bridge Deck Overlay DES 1383721

### **Description**

The preliminary preferred alternative for this project includes the rehabilitation of the existing bridge (Structure # 046-15-01987A) and the east and west approach roadway to current INDOT and FHWA criteria for load capacity and condition. The concrete deck, deck drains, and metal forms in all spans will be replaced, and new PF-1 bridge rail will be incorporated. Because PF-1 bridge rail will be used, the clear roadway will be reduced by 10 ½ inches. The travel lanes and shoulder widths will be transitioned from the bridge (10-foot-wide lanes and 1-foot, 6 ¾-inch wide shoulders) to the roadway approaches (12-foot-wide lanes and 2-foot-wide shoulders). The bridge joints will be replaced, and other deteriorated elements will be repaired or replaced in kind. All areas of the truss will be cleaned and painted. Deteriorated concrete caps of pier 5 and the east abutment will be removed and reconstructed. Delaminated and spalled areas of abutments and piers will be patched. Scour counter-measures, if required, and debris removal will also be implemented. New approach slabs will be placed, and the rehabilitation will require approximately 250 feet of approach work east and west of the bridge. The existing approach guardrail will be replaced with MASH guardrail and end treatments.

The project will not require the acquisition of permanent or temporary right-of-way. Suitable summer habitat is located around the project area, but no suitable summer habitat will be removed as a result of this project.

The review of the USFWS database on July 27, 2022 indicated no presence of endangered bat species. BLN completed a structure assessment on September 9, 2021 and no evidence of bats was found. Temporary lighting may be used for this project. The project scheduled letting date is 9/13/2023 with construction anticipated to begin in spring of 2024.

# Determination Key Result

Based on your answers provided, this project(s) may affect, but is not likely to adversely affect the endangered Indiana bat and/or the threatened Northern long-eared bat, therefore, consultation with the U.S. Fish and Wildlife Service pursuant to Section 7(a)(2) of the Endangered Species Act of 1973 (ESA) (87 Stat. 884, as amended 16 U.S.C. 1531 *et seq.*) is required. However, also based on your answers provided, this project may rely on the concurrence provided in the revised February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion for Transportation Projects within the Range of the Indiana Bat and Northern Long-eared Bat.

## Qualification Interview

1. Is the project within the range of the Indiana bat<sup>[1]</sup>?

[1] See [Indiana bat species profile](#)

**Automatically answered**

**Yes**

2. Is the project within the range of the Northern long-eared bat<sup>[1]</sup>?

[1] See [Northern long-eared bat species profile](#)

**Automatically answered**

**Yes**

3. Which Federal Agency is the lead for the action?

**A) Federal Highway Administration (FHWA)**

4. Are *all* project activities limited to non-construction<sup>[1]</sup> activities only? (examples of non-construction activities include: bridge/abandoned structure assessments, surveys, planning and technical studies, property inspections, and property sales)

[1] Construction refers to activities involving ground disturbance, percussive noise, and/or lighting.

**No**

5. Does the project include *any* activities that are **greater than** 300 feet from existing road/rail surfaces<sup>[1]</sup>?

[1] Road surface is defined as the actively used [e.g. motorized vehicles] driving surface and shoulders [may be pavement, gravel, etc.] and rail surface is defined as the edge of the actively used rail ballast.

**No**

6. Does the project include *any* activities **within** 0.5 miles of a known Indiana bat and/or NLEB hibernaculum<sup>[1]</sup>?

[1] For the purpose of this consultation, a hibernaculum is a site, most often a cave or mine, where bats hibernate during the winter (see suitable habitat), but could also include bridges and structures if bats are found to be hibernating there during the winter.

**No**

7. Is the project located **within** a karst area?

**No**

8. Is there *any* suitable<sup>[1]</sup> summer habitat for Indiana Bat or NLEB **within** the project action area<sup>[2]</sup>? (includes any trees suitable for maternity, roosting, foraging, or travelling habitat)

[1] See the Service's [summer survey guidance](#) for our current definitions of suitable habitat.

[2] The action area is defined as all areas to be affected directly or indirectly by the Federal action and not merely the immediate area involved in the action (50 CFR Section 402.02). Further clarification is provided by the [User's Guide for the Range-wide Programmatic Consultation for Indiana Bat and Northern Long-eared Bat](#).

*Yes*

9. Will the project remove *any* suitable summer habitat<sup>[1]</sup> and/or remove/trim any existing trees **within** suitable summer habitat?

[1] See the Service's [summer survey guidance](#) for our current definitions of suitable habitat.

*No*

10. Have presence/probable absence (P/A) summer surveys<sup>[1][2]</sup> been conducted<sup>[3][4]</sup> **within** the suitable habitat located within your project action area?

[1] See the Service's [summer survey guidance](#) for our current definitions of suitable habitat.

[2] Presence/probable absence summer surveys conducted within the fall swarming/spring emergence home range of a documented Indiana bat hibernaculum (contact local Service Field Office for appropriate distance from hibernacula) that result in a negative finding requires additional consultation with the local Service Field Office to determine if clearing of forested habitat is appropriate and/or if seasonal clearing restrictions are needed to avoid and minimize potential adverse effects on fall swarming and spring emerging Indiana bats.

[3] For projects within the range of either the Indiana bat or NLEB in which suitable habitat is present, and no bat surveys have been conducted, the transportation agency will assume presence of the appropriate species. This assumption of presence should be based upon the presence of suitable habitat and the capability of bats to occupy it because of their mobility.

[4] Negative presence/probable absence survey results obtained using the [summer survey guidance](#) are valid for a minimum of two years from the completion of the survey unless new information (e.g., other nearby surveys) suggest otherwise.

*No*

11. Does the project include activities **within documented Indiana bat habitat**<sup>[1][2]</sup>?

[1] Documented roosting or foraging habitat – for the purposes of this consultation, we are considering documented habitat as that where Indiana bats and/or NLEB have actually been captured and tracked using (1) radio telemetry to roosts; (2) radio telemetry triangulation to estimate foraging areas; or (3) foraging areas with repeated use documented using acoustics. Documented roosting habitat is also considered as suitable summer habitat within 0.25 miles of documented roosts.)

[2] For the purposes of this key, we are considering documented corridors as that where Indiana bats and/or NLEB have actually been captured and tracked to using (1) radio telemetry; or (2) treed corridors located directly between documented roosting and foraging habitat.

*No*

12. Does the project include activities **within documented NLEB habitat**<sup>[1][2]</sup>?

[1] Documented roosting or foraging habitat – for the purposes of this consultation, we are considering documented habitat as that where Indiana bats and/or NLEB have actually been captured and tracked using (1) radio telemetry to roosts; (2) radio telemetry triangulation to estimate foraging areas; or (3) foraging areas with repeated use documented using acoustics. Documented roosting habitat is also considered as suitable summer habitat within 0.25 miles of documented roosts.)

[2] For the purposes of this key, we are considering documented corridors as that where Indiana bats and/or NLEB have actually been captured and tracked to using (1) radio telemetry; or (2) treed corridors located directly between documented roosting and foraging habitat.

*No*

13. Does the project include wetland or stream protection activities associated with compensatory wetland mitigation?

*No*

14. Does the project include slash pile burning?

*No*

15. Does the project include *any* bridge removal, replacement, and/or maintenance activities (e.g., any bridge repair, retrofit, maintenance, and/or rehabilitation work)?

*Yes*

16. Is there *any* suitable habitat<sup>[1]</sup> for Indiana bat or NLEB **within** 1,000 feet of the bridge? (includes any trees suitable for maternity, roosting, foraging, or travelling habitat)

[1] See the Service's current [summer survey guidance](#) for our current definitions of suitable habitat.

*Yes*

17. Has a bridge assessment<sup>[1]</sup> been conducted **within** the last 24 months<sup>[2]</sup> to determine if the bridge is being used by bats?

[1] See [User Guide Appendix D](#) for bridge/structure assessment guidance

[2] Assessments must be completed no more than 2 years prior to conducting any work below the deck surface on all bridges that meet the physical characteristics described in the Programmatic Consultation, regardless of whether assessments have been conducted in the past. Due to the transitory nature of bat use, a negative result in one year does not guarantee that bats will not use that bridge/structure in subsequent years.

*Yes*

**SUBMITTED DOCUMENTS**

- *Bat Inspection 9-2-21.pdf* <https://ipac.ecosphere.fws.gov/project/35SFXB4SGJE7LBADOX536VZ7HI/projectDocuments/115418299>

18. Did the bridge assessment detect *any* signs of Indiana bats and/or NLEBs roosting in/under the bridge (bats, guano, etc.)<sup>[1]</sup>?

[1] If bridge assessment detects signs of *any* species of bats, coordination with the local FWS office is needed to identify potential threatened or endangered bat species. Additional studies may be undertaken to try to identify which bat species may be utilizing the bridge prior to allowing *any* work to proceed.

Note: There is a small chance bridge assessments for bat occupancy do not detect bats. Should a small number of bats be observed roosting on a bridge just prior to or during construction, such that take is likely to occur or does occur in the form of harassment, injury or death, the PBO requires the action agency to report the take. Report all unanticipated take within 2 working days of the incident to the USFWS. Construction activities may continue without delay provided the take is reported to the USFWS and is limited to 5 bats per project.

No

19. Will the bridge removal, replacement, and/or maintenance activities include installing new or replacing existing **permanent** lighting?

No

20. Does the project include the removal, replacement, and/or maintenance of *any* structure other than a bridge? (e.g., rest areas, offices, sheds, outbuildings, barns, parking garages, etc.)

No

21. Will the project involve the use of **temporary** lighting *during* the active season?

Yes

22. Is there *any* suitable habitat **within** 1,000 feet of the location(s) where **temporary** lighting will be used?

Yes

23. Will the project install new or replace existing **permanent** lighting?

No

24. Does the project include percussives or other activities (**not including tree removal/trimming or bridge/structure work**) that will increase noise levels above existing traffic/background levels?

No

25. Are *all* project activities that are **not associated with** habitat removal, tree removal/trimming, bridge and/or structure activities, temporary or permanent lighting, or use of percussives, limited to actions that DO NOT cause any additional stressors to the bat species?

Examples: lining roadways, unlighted signage, rail road crossing signals, signal lighting, and minor road repair such as asphalt fill of potholes, etc.

Yes

26. Will the project raise the road profile **above the tree canopy**?

No

27. Are the project activities that are not associated with habitat removal, tree removal/trimming, bridge and/or structure activities, temporary or permanent lighting, or use of percussives consistent with a No Effect determination in this key?

**Automatically answered**

*Yes, other project activities are limited to actions that DO NOT cause any additional stressors to the bat species as described in the BA/BO*

28. Is the bridge removal, replacement, or maintenance activities portion of this project consistent with a No Effect determination in this key?

**Automatically answered**

*Yes, because the bridge has been assessed using the criteria documented in the BA and no signs of bats were detected*

29. **General AMM 1**

Will the project ensure *all* operators, employees, and contractors working in areas of known or presumed bat habitat are aware of *all* FHWA/FRA/FTA (Transportation Agencies) environmental commitments, including all applicable Avoidance and Minimization Measures?

*Yes*

30. **Lighting AMM 1**

Will *all temporary* lighting be directed away from suitable habitat during the active season?

*Yes*

## Project Questionnaire

1. Have you made a No Effect determination for *all* other species indicated on the FWS IPaC generated species list?

*N/A*

2. Have you made a May Affect determination for *any* other species on the FWS IPaC generated species list?

*N/A*

3. Please describe the proposed bridge work:

*The concrete deck, deck drains, and metal forms in all spans will be replaced, and new PF-1 bridge rail will be incorporated. Because PF-1 bridge rail will be used, the clear roadway will be reduced by 10.5 inches. The travel lanes and shoulder widths will be transitioned from the bridge (10-foot-wide lanes and 1-foot, 6.75-inch wide shoulders) to the roadway approaches (12-foot-wide lanes and 2-foot-wide shoulders). The bridge joints will be replaced, and other deteriorated elements will be repaired or replaced in-kind. All areas of the truss will be cleaned and painted. Deteriorated concrete caps of pier 5 and the east abutment will be removed and reconstructed. Delaminated and spalled areas of abutments and piers will be patched. Scour countermeasures, if required, and debris removal will also be implemented. New approach slabs will be placed, and the rehabilitation will require approximately 250 feet of approach work east and west of the*

*bridge. The existing approach guardrail will be replaced with MASH guardrail and end treatments.*

4. Please state the timing of all proposed bridge work:

*Summer 2023*

5. Please enter the date of the bridge assessment:

*9/2/21*

## **Avoidance And Minimization Measures (AMMs)**

This determination key result includes the commitment to implement the following Avoidance and Minimization Measures (AMMs):

### **LIGHTING AMM 1**

Direct temporary lighting away from suitable habitat during the active season.

### **GENERAL AMM 1**

Ensure all operators, employees, and contractors working in areas of known or presumed bat habitat are aware of all FHWA/FRA/FTA (Transportation Agencies) environmental commitments, including all applicable AMMs.

## **Determination Key Description: FHWA, FRA, FTA Programmatic Consultation For Transportation Projects Affecting NLEB Or Indiana Bat**

This key was last updated in IPaC on April 28, 2022. Keys are subject to periodic revision.

This decision key is intended for projects/activities funded or authorized by the Federal Highway Administration (FHWA), Federal Railroad Administration (FRA), and/or Federal Transit Administration (FTA), which may require consultation with the U.S. Fish and Wildlife Service (Service) under Section 7 of the Endangered Species Act (ESA) for the endangered **Indiana bat** (*Myotis sodalis*) and the threatened **Northern long-eared bat** (NLEB) (*Myotis septentrionalis*).

This decision key should only be used to verify project applicability with the Service's [February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion for Transportation Projects](#). The programmatic biological opinion covers limited transportation activities that may affect either bat species, and addresses situations that are both likely and not likely to adversely affect either bat species. This decision key will assist in identifying the effect of a specific project/activity and applicability of the programmatic consultation. The programmatic biological opinion is not intended to cover all types of transportation actions. Activities outside the scope of the programmatic biological opinion, or that may affect ESA-listed species other than the Indiana bat or NLEB, or any designated critical habitat, may require additional ESA Section 7 consultation.

## IPaC User Contact Information

Agency: Indiana Department of Transportation  
Name: Taylor Schwering  
Address: 185 Agrico Lane  
City: Seymour  
State: IN  
Zip: 47201  
Email: [tschwering@indot.in.gov](mailto:tschwering@indot.in.gov)  
Phone: 8127160748

# Bridge Inspection Report

046-15-01987 A

SR 46

over

WHITEWATER RIVER



Inspection Date: 09/08/2021

Inspected By: Stephen F. Hurst

Inspection Type(s): Routine  
Special

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**Paint:** \* *Indicate if paint present, year painted & condition rating.*

1 - Steel Beams	5 - Fair Condition – areas of light rust and minor peeling	1996
-----------------	--	------

Comments:

Some rusting and peeling of the paint. Many areas of rust and scaling at the lower chord joints, stringers and floor beams.

Paint Color: Green

**Endangered Species:** \* *If yes, add one photo to the dropdown field*

Bats: seen or heard under structure? \* N - No evidence of bats

Birds/swallows/nests seen? Empty nests present? \* N - No Birds and/or Nests Visi

---

#### **BRIDGE Culvert Geometry:**

Barrel Length:

Height:

Width:

Bridge Inspection Report

## APPRAISAL

SUFFICIENCY RATING:	<b>35.4</b>	(36) TRAFFIC SAFETY FEATURE:
STATUS:	<b>1</b>	36A) BRIDGE RAILINGS: <b>0</b>
(67) STRUCTURAL EVALUATION:	<b>4</b>	36B) TRANSITIONS: <b>0</b>
(68) DECK GEOMETRY:	<b>2</b>	36C) APPROACH GUARDRAIL: <b>1</b>
(69) UNDERCLEARANCES, VERTICAL & HORIZONTAL:	<b>N</b>	36D) APPROACH GUARDRAIL ENDS: <b>1</b>
(71) WATERWAY ADEQUACY:		<b>6 - Occasional Overtopping of Approaches - Insignificant Delays</b>
Comments:		
(72) APPROACH ROADWAY ALIGNMENT:		<b>6 - Equal to present minimum criteria</b>
Comments:		
(113) SCOUR CRITICAL BRIDGES:		<b>3 - Foundations unstable for scour conditions</b>
Comments:		

7/24/2018 U-W Insp.- Minor embankment erosion along both shorelines.

This is based on all of the foundations being set on piles.

There is no Riprap on the banks of the River, under the bridge.

The 1952 Flow Line elv. = 677.16'  
The 2003 Flow Line elv. = 509.40' UW Report  
The 2008 Flow Line elv. = 510.20' UW Report  
The Q-100 Flow elv. =  
The Q-100 Scour Depth

Changed to 3 due to scour letter dated 4/30/2020.

## CLASSIFICATION

(20) TOLL:	<b>3 - On Free Road</b>	(21) MAINT. RESPONSIBILITY: <b>01 - State Highway Agency</b>
(22) OWNER:	<b>01 - State Highway Agency</b>	(26) FUNCTIONAL CLASS OF INVENTORY RTE: <b>07 - Rural - Major Collector</b>
(37) HISTORICAL SIGNIFICANCE:	<b>2 - Eligible for National Register</b>	(100) STRAHNET HIGHWAY: <b>Not a STRAHNET route</b>
(101) PARALLEL STRUCTURE:	<b>N - No parallel structure</b>	(102) DIRECTION OF TRAFFIC: <b>2-way traffic</b>
(103) TEMPORARY STRUCTURE:		(104) HIGHWAY SYSTEM OF INVENTORY ROUTE: <b>0 - Structure/Route is NOT on NHS</b>
(105) FEDERAL LANDS HIGHWAYS:	<b>0-Not Applicable</b>	(110) DESIGNATED NATIONAL NETWORK: <b>Inventory route not on network</b>
(112) NBIS BRIDGE LENGTH:	<b>Yes</b>	

INDOT Bridge/Small Structure Bat Inspection Data Sheet (Rev 4/29/2016)

General Information		
Date of Inspection: 9/2/21	Initial Inspection <input checked="" type="checkbox"/>	Temp: 80
Time of Inspection: 12:30	Follow-up Inspection <input type="checkbox"/>	Wind: 0
County: Dearborn	Construction <input type="checkbox"/>	Precip: 0
Inspected by: Aimee Cooper, Kayla Swoveland		
GPS Northing: Easting: UTM Zone: 16	Contract Number: B- 42401	Sunrise: 7:13am Sunset: 8:13pm Anticipated Start Date for Construction:

Bridge or Culvert		Bridge or Culvert	
Stream or Road Crossed: Whitewater River	Bridge/Culvert number: 046-15-01987B	Station: 1082 + 85.00" MN"	
Type of Structure:		Number of Spans: 5	
<input type="checkbox"/> Concrete box beam <input type="checkbox"/> Concrete I-beam <input type="checkbox"/> Concrete bulb tee beam <input type="checkbox"/> Concrete arch <input type="checkbox"/> Concrete girder <input type="checkbox"/> Concrete slab <input type="checkbox"/> Multi-plate arch <input checked="" type="checkbox"/> Other (list): Steel through truss	<input type="checkbox"/> Steel beam <input type="checkbox"/> Steel girder <input type="checkbox"/> Steel pony truss <input type="checkbox"/> Welded steel thru girder <input type="checkbox"/> Concrete box culvert <input type="checkbox"/> Concrete pipe <input type="checkbox"/> Corrugated steel pipe	Material: <input type="checkbox"/> Concrete <input checked="" type="checkbox"/> Steel <input type="checkbox"/> Other (describe):	
Searched entire structure? If not, why not? Yes		Shape: <input type="checkbox"/> Box Culvert <input type="checkbox"/> Arch <input checked="" type="checkbox"/> Other (describe) truss	<input type="checkbox"/> Pipe <input type="checkbox"/> Slab
Bats Present? <input type="checkbox"/> Seen? <input type="checkbox"/> Heard? No		Location of bats or signs of use (w/drawing and photos): N/A	
In Clusters? Number of clusters: N/A			
Number of bats in largest cluster: N/A			
Approximate total number of bats found: N/A			
Signs of previous bat use? <input type="checkbox"/> Guano <input type="checkbox"/> Staining	No		

If Bats Present	
Date and Time Project Supervisor was notified:	
Name of Project Supervisor notified:	

**From:** [Dearborn Co. Historical Society](#)  
**To:** [Kirsten Lewis](#)  
**Subject:** Re: Early Coordination, Des. No. 1383721, Bridge Project, SR 46 over Whitewater River, Dearborn County, IN  
**Date:** Tuesday, July 27, 2021 7:46:12 AM

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**EXTERNAL EMAIL**

Received, thank you.

On Mon, Jul 26, 2021 at 12:16 PM Kirsten Lewis <[klewis@b-l-n.com](mailto:klewis@b-l-n.com)> wrote:

Good afternoon,

Please find the Early Coordination Letter for Des. No. 1383721, Bridge Project, SR 46 over Whitewater River, Dearborn County, Indiana appended to this email. Thank you!

Sincerely,

**Kirsten Lewis**

**Senior Environmental Analyst**

office: 317.849.5832

direct: 317.806.3056

[B-L-N.com](http://B-L-N.com)

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**From:** [Nicole Daily](#)  
**To:** [Kirsten Lewis](#)  
**Subject:** Early Coordination, Des. No. 1383721, Bridge Project, SR 46 over Whitewater River, Dearborn County, IN---  
**Comments**  
**Date:** Monday, July 26, 2021 3:12:53 PM

---

**EXTERNAL EMAIL**

Kristen:

I have reviewed the early coordination submittal of the proposed bridge work along SR 46. My only comments are this project is located in the floodway and floodplain. You will need an approved permit through IDNR for the construction in the floodway and a permit through the Dearborn County Planning and Zoning Department as some of the work will be in the floodplain. I have no other environmental concerns except for the floodway and floodplain areas.

If you have any questions please let me know.

Thanks,

*Nicole Daily*

Zoning Administrator  
[ndaily@dearborncounty.in.gov](mailto:ndaily@dearborncounty.in.gov)  
T: 812-537-8821  
F: 812-532-2029  
Dearborn County Government Center  
Dearborn County Plan Commission  
165 Mary Street  
Lawrenceburg, IN 47025

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**From:** Kirsten Lewis <klewis@b-l-n.com>  
**Sent:** Monday, July 26, 2021 12:14 PM  
**To:** Nicole Daily <ndaily@dearborncounty.in.gov>  
**Subject:** Early Coordination, Des. No. 1383721, Bridge Project, SR 46 over Whitewater River, Dearborn County, IN

Good afternoon,

Please find the Early Coordination Letter for Des. No. 1383721, Bridge Project, SR 46 over Whitewater River, Dearborn County, Indiana appended to this email. Thank you!

Sincerely,

**Kirsten Lewis**

# Bridge/Structure Bat Assessment Form

Date & Time of Assessment	DOT Project Number	Route/Facility SR 46/ Whitewater River Carried	County Dearborn	
12/02/2022	1383721			
Federal Structure ID	046-15-01987 A	Structure Coordinates (latitude and longitude)	Structure Height (approximate) 15feet	
			Structure Length 883 feet	
<b>Structure Type</b> (check one)		<b>Structure Material</b> (check all that apply)		
<i>Bridge Construction Style</i>		<i>Deck Material</i>	<i>Beam Material</i>	
<input type="radio"/> Cast-in-place 	<input type="radio"/> Pre-stressed Girder 	Metal <input type="checkbox"/>	None <input type="checkbox"/>	Concrete <input checked="" type="checkbox"/>
<input type="radio"/> Flat Slab/Box 	<input type="radio"/> Steel I-beam 	Concrete <input checked="" type="checkbox"/>	Concrete <input type="checkbox"/>	Timber <input type="checkbox"/>
<input checked="" type="radio"/> Truss 	<input type="radio"/> Covered 	Timber <input type="checkbox"/>	Steel <input checked="" type="checkbox"/>	Stone/Masonry <input type="checkbox"/>
<input type="radio"/> Parallel Box Beam 	<input type="radio"/> Other: _____	Open grid <input type="checkbox"/>	Timber <input type="checkbox"/>	Other: _____ <input type="checkbox"/>
				<i>Creosote Evidence</i> <input type="checkbox"/>
<i>Culvert Type</i>		<i>Culvert Material</i>		
<input type="radio"/> Box	<input type="radio"/> Other Structure	Metal <input type="checkbox"/>	Concrete <input type="checkbox"/>	<i>Notes:</i> _____
<input type="radio"/> Pipe/Round		Plastic <input type="checkbox"/>		
<input type="radio"/> Other: _____		Stone/Masonry <input type="checkbox"/>		
		Other: _____ <input type="checkbox"/>		
<b>Crossings Traversed</b> (check all that apply)		<b>Surrounding Habitat</b> (check all that apply)		
<input checked="" type="checkbox"/> Bare ground	Open vegetation <input type="checkbox"/>	<input checked="" type="checkbox"/> Agricultural <input type="checkbox"/>	Grassland <input type="checkbox"/>	
<input type="checkbox"/> Rip-rap	Closed vegetation <input type="checkbox"/>	Commercial <input type="checkbox"/>	Ranching <input type="checkbox"/>	
<input checked="" type="checkbox"/> Flowing water	Railroad <input type="checkbox"/>	Residential-urban <input type="checkbox"/>	Riparian/wetland <input type="checkbox"/>	
<input type="checkbox"/> Standing water	Road/trail - Type: _____	Residential-rural <input type="checkbox"/>	Mixed use <input type="checkbox"/>	
<input type="checkbox"/> Seasonal water	Other: _____	<input checked="" type="checkbox"/> Woodland/forested <input type="checkbox"/>	Other: _____ <input type="checkbox"/>	
<b>Areas Assessed</b> (check all that apply)				
Check all areas that apply. If an area is not present in the structure, check the "not present" box.				
Document all bat indicators observed during the assessment. Include the species present, if known, and provide photo documentation as indicated.				
<b>Area</b> (check if assessed)	<b>Assessment Notes</b>	<b>Evidence of Bats</b> (include photos if present)		
<input type="checkbox"/> All crevices and cracks:	<input checked="" type="checkbox"/> Not present	<input type="checkbox"/> Visual - live #	<input type="checkbox"/> dead #	<input type="checkbox"/> Audible
<input checked="" type="checkbox"/> Bridges/culverts: rough surfaces or imperfections in concrete		<input type="checkbox"/> Guano	<input type="checkbox"/> Odor	<b>Species</b>
<input checked="" type="checkbox"/> Other structures: soffits, rafters, attic areas		<input type="checkbox"/> Staining	<input type="checkbox"/> Photos	
<input checked="" type="checkbox"/> Concrete surfaces (open roosting on concrete)	<input checked="" type="checkbox"/> Not present	<input type="checkbox"/> Visual - live #	<input type="checkbox"/> dead #	
<input checked="" type="checkbox"/> Spaces between concrete end walls and the bridge deck		<input type="checkbox"/> Guano	<input type="checkbox"/> Odor	
<input type="checkbox"/> Crack between concrete railings on top of the bridge deck	<input type="checkbox"/> Not present	<input type="checkbox"/> Staining	<input type="checkbox"/> Photos	
		<input type="checkbox"/> Visual - live #	<input type="checkbox"/> dead #	
<input type="checkbox"/> Vertical surfaces on concrete I-beams	<input type="checkbox"/> Not present	<input type="checkbox"/> Guano	<input type="checkbox"/> Odor	
		<input type="checkbox"/> Staining	<input type="checkbox"/> Photos	
<input type="checkbox"/> Spaces between walls, ceiling joists	<input type="checkbox"/> Not present	<input type="checkbox"/> Visual - live #	<input type="checkbox"/> dead #	
		<input type="checkbox"/> Guano	<input type="checkbox"/> Odor	
		<input type="checkbox"/> Staining	<input type="checkbox"/> Photos	
<input checked="" type="checkbox"/> Weep holes, scupper drains, and inlets/pipes	<input checked="" type="checkbox"/> Not present	<input type="checkbox"/> Visual - live #	<input type="checkbox"/> dead #	
		<input type="checkbox"/> Guano	<input type="checkbox"/> Odor	
		<input type="checkbox"/> Staining	<input type="checkbox"/> Photos	
<input checked="" type="checkbox"/> All guiderails	<input checked="" type="checkbox"/> Not present	<input type="checkbox"/> Visual - live #	<input type="checkbox"/> dead #	
		<input type="checkbox"/> Guano	<input type="checkbox"/> Odor	
		<input type="checkbox"/> Staining	<input type="checkbox"/> Photos	
<input checked="" type="checkbox"/> All expansion joints	<input checked="" type="checkbox"/> Not present	<input type="checkbox"/> Visual - live #	<input type="checkbox"/> dead #	
		<input type="checkbox"/> Guano	<input type="checkbox"/> Odor	
		<input type="checkbox"/> Staining	<input type="checkbox"/> Photos	
Name: Alfred V. Wessling		Signature: 		

# **Appendix D:**

# **Section 106 of the NHPA**



# INDIANA DEPARTMENT OF TRANSPORTATION

100 North Senate Avenue  
Room N642  
Indianapolis, Indiana 46204

PHONE: (317) 234-5168

**Eric Holcomb, Governor**  
**Joe McGuinness, Commissioner**

July 23, 2020

This letter was sent to the listed parties.

RE: Dual Review Project; SR 46 over the Whitewater River Bridge No. 046-15-01987A (NBI No. 17540) Project, Des No. 1383721, Dearborn County, Indiana

Dear Consulting Party (see attached list),

The Indiana Department of Transportation (INDOT), with funding from the Federal Highway Administration (FHWA), proposes to proceed with the State Road (SR) 46 over the Whitewater River, Bridge No. 046-15-01987A (NBI No. 17540) Project, Des No. 1383721. Beam, Longest and Neff, LLC is under contract with INDOT to advance the environmental documentation for the referenced project.

This letter is part of the early coordination phase of the environmental review process requesting comments associated with this project. We are requesting comments from your area of expertise regarding any possible environmental effects associated with this project. Please use the above Des. Number and project description in your reply and your comments will be incorporated into the formal environmental study.

The proposed undertaking is on SR 46 over the Whitewater River, Bridge No. 046-15-01987A (NBI No. 17540), 0.44 mile west of US 52 in Dearborn County, Indiana. It is within Logan and Harrison Townships, Harrison USGS Topographic Quadrangle; in Section 10, Township 7 North, Range 1 West.

Bridge No. 046-15-01987 A is a five span steel truss structure constructed in 1937. The existing structure has five equal 175'-0" spans. The existing clear roadway over the bridge is 24'-0" and consists of two 10'-0" lanes with 2'-0" shoulders. The bridge was rehabilitated in 1985 and painted in 1996.

The need of this project is to address current structural deficiencies, including deck and superstructure condition. There is heavy deterioration and distress exhibited by some of the truss verticals, diagonals, low chords, stringers, floor beams, and deck. The bridge was load rated with the noted existing deterioration. The analysis found that the verticals and stringers controlled the load rating with design and legal load rating factors less than 1.0. The purpose of this project is to improve the overall condition of the bridge's superstructure and the structural capacity. Right-of-way acquisition is anticipated; the amounts of right-of-way acquisition (permanent and/or temporary) are unknown at this time.

Section 106 of the National Historic Preservation Act requires federal agencies to take into account the effects of their undertakings on historic and archaeological properties. In accordance with 36 CFR 800.2 (c), you are hereby requested to be a consulting party to participate in the Section 106 process. Entities that have been invited to participate in the Section 106 consultation process for this project are identified in the attached list. Per 36 CFR 800.3(f), we hereby request that the Indiana State Historic Preservation Officer (SHPO) notify this office if the SHPO staff is aware of any other parties that may be entitled to be consulting parties or should be contacted as potential consulting parties for the project.

[www.in.gov/dot/](http://www.in.gov/dot/)  
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The Section 106 process involves efforts to identify historic properties potentially affected by the undertaking, assess its effects and seek ways to avoid, minimize or mitigate any adverse effects on historic properties. For more information regarding the protection of historic resources, please see the Advisory Council on Historic Preservation's guide: *Protecting Historic Properties: A Citizen's Guide to Section 106 Review* available online at <https://www.achp.gov/sites/default/files/documents/2017-01/CitizenGuide.pdf>.

Per the terms of the “Programmatic Agreement Regarding Management and Preservation of Indiana’s Historic Bridges” (Historic Bridges PA), the FHWA-Indiana Division will satisfy its Section 106 responsibilities involving “Select” and “Non-Select” bridges through the Project Development Process (PDP) of the Historic Bridges PA (Stipulation III). Because State Bridge # 046-15-01987A (NBI 17540) is a “Non-Select” bridge, the procedures outlined in Stipulation III.B. of the Historic Bridges PA will be followed to fulfill FHWA’s Section 106 responsibilities for the project. (A copy of the Historic Bridges PA can be downloaded here: <http://www.in.gov/indot/2530.htm>).

Please note that per the permanent rule issued by the Indiana Department of Natural Resources effective August 14, 2013 (312 IAC 20-4-11.5), INDOT is requesting that this project be subjected to “dual review”; that is, reviewed by the Division of Historic Preservation and Archaeology simultaneously under 54 U.S.C. 306108 (Section 106) and IC 14-21-1-18 (Indiana Preservation and Archaeology Law dealing with alterations of historic sites and structures requiring a Certificate of Approval). Pursuant to Section 11.5(f) of this rule, at the conclusion of the review process we anticipate that the Division Director would issue a letter of clearance exempting this project from obtaining a Certificate of Approval under IC 14-21-1-18. Enclosed with this letter is a detailed list of the consulting parties, including contact information including email addresses, for processing the dual review submission.

The Area of Potential Effects (APE) is the area in which the proposed project may cause alterations in the character or use of historic resources. At this time, no cultural resource investigations have occurred; however, the results of cultural resource identification and evaluation efforts, both above-ground and archaeological, will be forthcoming. Consulting parties will receive notification when these reports are completed.

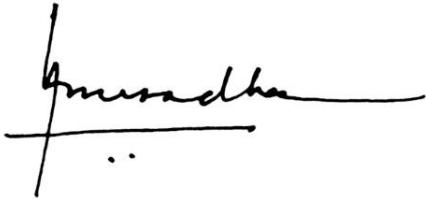
Please review the information and comment within thirty (30) calendar days of receipt. If you indicate that you do not desire to be a consulting party, or if you do not respond, you will not be included on the list of consulting parties for this project. If we do not receive your response in the time allotted, the project will proceed consistent with the proposed design and you will not receive further information about the project unless the design changes.

For questions concerning specific project details, you may contact Karen Wood of Green 3, LLC at (317) 634-4110 or [karen@green3studio.com](mailto:karen@green3studio.com). All future responses regarding the proposed project should be forwarded to Green 3, LLC at the following address:

Karen Wood  
Environmental and Cultural Resources Manager  
Green 3, LLC  
1104 Prospect Street  
Indianapolis, IN, 46203  
[karen@green3studio.com](mailto:karen@green3studio.com)

Tribal contacts may contact Shaun Miller at [smiller@indot.in.gov](mailto:smiller@indot.in.gov) or 317-233-6795 or Michelle Allen at FHWA at [michelle.allen@dot.gov](mailto:michelle.allen@dot.gov) or 317-226-7344.

Sincerely,



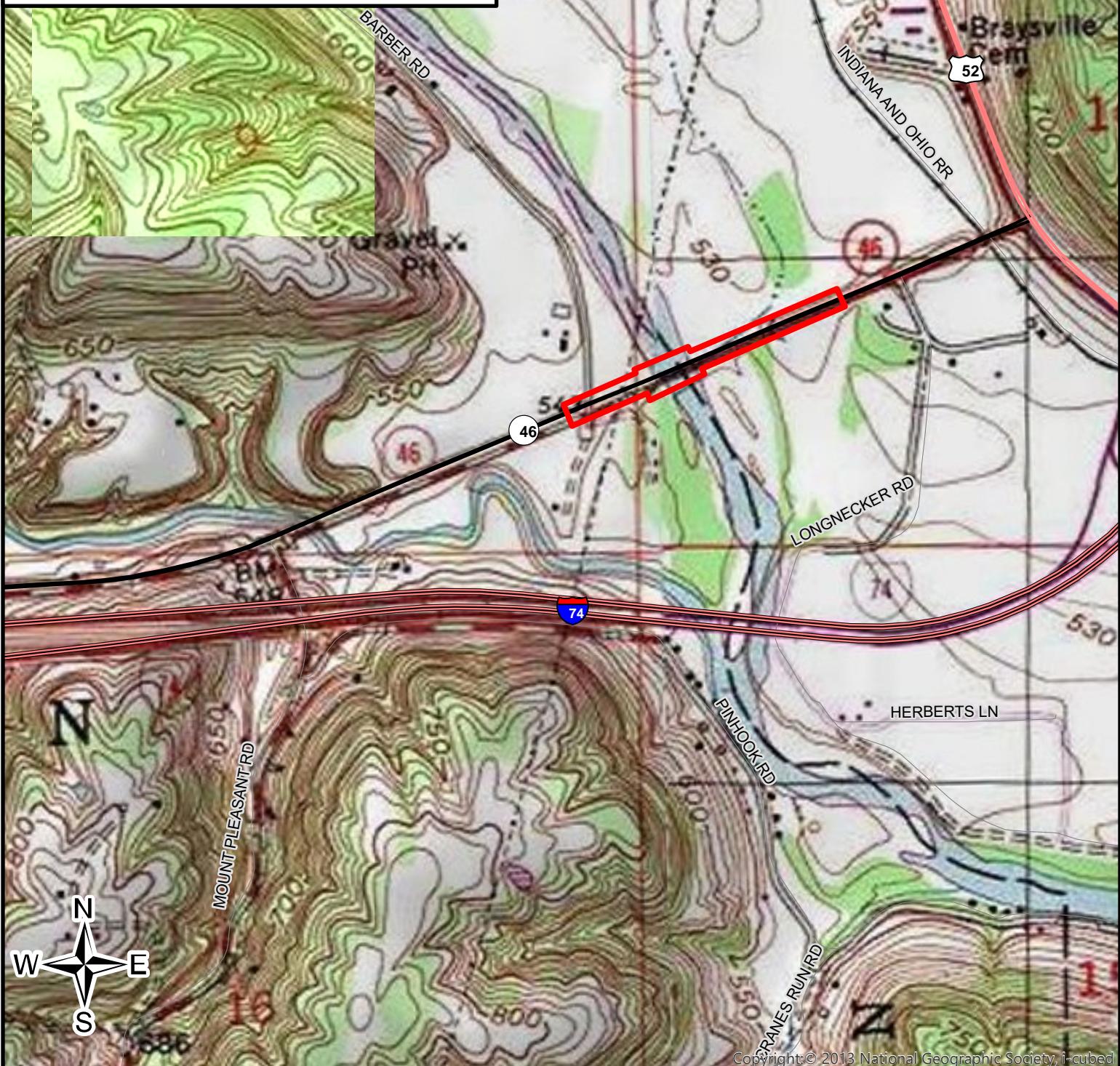
Anuradha V. Kumar, Manager  
Cultural Resources Office  
Environmental Services

Enclosures:  
Topographic Map

Distribution List:

Indiana State Historic Preservation Officer, [cslider@dnr.in.gov](mailto:cslider@dnr.in.gov), [bmccord@dnr.in.gov](mailto:bmccord@dnr.in.gov),  
Indiana Landmarks – Southeast Field Office, [jholbrook@indianalandmarks.org](mailto:jholbrook@indianalandmarks.org),  
Indiana Historic Spans Task Force, [indianabridges@sbcglobal.net](mailto:indianabridges@sbcglobal.net),  
Historic Bridge Foundation, [kitty@historicbridgefoundation.com](mailto:kitty@historicbridgefoundation.com),  
Historic Hoosier Bridges, [spansaver@hotmail.com](mailto:spansaver@hotmail.com),  
Historicbridges.org, [nathan@historicbridges.org](mailto:nathan@historicbridges.org),  
Dearborn County Historical Society, [deahistory@embargmail.com](mailto:deahistory@embargmail.com), [cmchenry@seidata.com](mailto:cmchenry@seidata.com),  
Aurora Historic Preservation Commission, [jborgman@aurora.in.us](mailto:jborgman@aurora.in.us),  
Dearborn County Trust for Historic Preservation, 29 East High Street, Lawrenceburg, IN 47025  
Ohio-Kentucky-Indiana Regional Council of Governments, [mpolicinski@oki.org](mailto:mpolicinski@oki.org),  
Dearborn County Commissioners, [alittle@dearborncounty.in.gov](mailto:alittle@dearborncounty.in.gov), [klynch@dearborncounty.in.gov](mailto:klynch@dearborncounty.in.gov),  
[smchenry@dearborncounty.in.gov](mailto:smchenry@dearborncounty.in.gov),  
Dearborn County Highway Supervisor, [tgreive@dearborncounty.in.gov](mailto:tgreive@dearborncounty.in.gov),  
Dearborn County Historian, [jbaer1@embargmail.com](mailto:jbaer1@embargmail.com),  
Dr. James Cooper, [jcooper@ccrtc.com](mailto:jcooper@ccrtc.com),  
Eastern Shawnee Tribe of Oklahoma  
Miami Tribe of Oklahoma  
Peoria Tribe of Indians of Oklahoma  
Pokagon Band of Potawatomi Indians  
United Keetoowah Band of Cherokee Indians  
Delaware Tribe of Indians

Topographic Map  
Bridge Project  
SR 46 over Whitewater River  
Des. No. 1383721  
Dearborn County, Indiana  
Source: Cedar Grove IN/ Harrison OH  
Quadrangles



Copyright: © 2013 National Geographic Society, i-cubed

0      0.1      0.2  
Miles

## Project Area

green • 3

Three colored circles are arranged horizontally. The first circle on the left is dark green. The second circle in the middle is medium green. The third circle on the right is light green.

1:10,500

2/25/2020



Division of Historic Preservation & Archaeology • 402 W. Washington Street, W274 • Indianapolis, IN 46204-2739  
Phone 317-232-1646 • Fax 317-232-0693 • [dhpa@dnri.IN.gov](mailto:dhpa@dnri.IN.gov) • [www.IN.gov/dnr/historic](http://www.IN.gov/dnr/historic)



August 10, 2020

Karen Wood  
Green 3, LLC  
1104 Prospect Street  
Indianapolis, Indiana 46203

Federal Agency: Indiana Department of Transportation (“INDOT”),  
on behalf of Federal Highway Administration, Indiana Division (“FHWA”)

Re: DUAL REVIEW: Early coordination letter and proposal for dual review for the SR 46 over the Whitewater River Bridge (Bridge No. 046-15-01987A; NBI No. 17540) project in Harrison and Logan townships, Dearborn County (Des. No. 1383721; DHPA No. 26124)

Dear Ms. Wood:

The Indiana Department of Natural Resources, Division of Historic Preservation and Archaeology (“INDNR-DHPA”), which also serves as the staff of the Indiana State Historic Preservation Officer (“Indiana SHPO”), is in receipt of INDOT’s early coordination letter, dated July 23, 2020, transmitting your proposal for a dual review, pursuant to 312 Indiana Administrative Code (“IAC”) 20-4-11.5, of the aforementioned project in Harrison and Logan townships, Dearborn County. We received this submission on July 23, 2020.

The Indiana SHPO/INDNR-DHPA will review the information submitted under Section 106 of the National Historic Preservation Act of 1966, as amended (54 U.S.C. § 306108), implementing regulations at 36 C.F.R. Part 800, as well as Indiana Code 14-21-1-18 and 312 IAC 20-4. By copy of this letter, INDNR-DHPA is providing notification of the commencement of the dual review to interested persons and members of the Indiana Historic Preservation Review Board (“Review Board”). Notice of the commencement will also be posted on the division’s website ([www.in.gov/dnr/historic/7440.htm](http://www.in.gov/dnr/historic/7440.htm)).

For the purposes of Indiana Code 14-21-1-18 and 312 IAC 20-4, we have added the members of the Review Board and additional, potentially interested parties to the list of parties we intend to copy with our comment letters. Anyone receiving an e-mailed copy of this letter who *does not wish to receive future copies of our correspondence about this project* is asked to reply by e-mail to [dkauermann@dnri.in.gov](mailto:dkauermann@dnri.in.gov) or to (317) 232-0582 or by letter to the address in our letterhead and advise us that he or she does not wish to receive any further copies of our e-mails on this project.

In your next regular submission, please include which consulting parties agree to participate in the consultation of this dual review.

We see in INDOT’s July 23 letter that FHWA will satisfy its Section 106 responsibilities involving “Select” and “Non-Select” bridges through the Project Development Process of the “Programmatic Agreement Regarding Management and Preservation of Indiana’s Historic Bridges.” We note within the *Indiana Historic Bridge Inventory*, the five-span steel truss bridge constructed c. 1937 that carries SR 46 over the Whitewater River (Bridge No. 046-15-01987A; NBI 17540) is listed as eligible for listing in the National Register of Historic Places under Criterion C, and classified as a “Non-Select” bridge.

As INDOT’s July 23 letter indicates, additional information regarding aboveground historic resources and archaeological resources are forthcoming. Once the indicated information is received, the Indiana SHPO will resume identification and evaluation procedures for this project. Please keep in mind that additional information may be requested in the future.

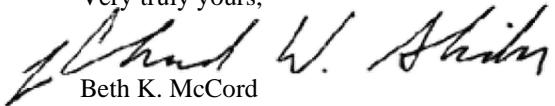
For the benefit of those recipients of a copy of this letter who are not Section 106 consulting parties, please be aware that a copy of INDOT’s July 23 letter can be found online at <http://erms.indot.in.gov/Section106Documents/>. From there, search by this project’s designation number: 1383721.

Karen Wood  
August 10, 2020  
Page 2

If you have questions regarding our dual review of the aforementioned project, please contact INDNR-DHPA. Questions about archaeological issues should be directed to contact Rachel Sharkey at (317) 234-5254 or [rsharkey@dnr.in.gov](mailto:rsharkey@dnr.in.gov). Questions about historic buildings or structures pertaining to this review should be directed to Danielle Kauffmann at (317) 232-0582 or [dkauffmann@dnr.IN.gov](mailto:dkauffmann@dnr.IN.gov).

In all future correspondence regarding the dual review of the SR 46 over the Whitewater River Bridge project in Harrison and Logan townships, Dearborn County (Des. No. 1383721), please refer to DHPA No. 26124.

Very truly yours,



Beth K. McCord  
Deputy State Historic Preservation Officer  
Director, Division of Historic Preservation and Archaeology

BKM:DMK:dmk

EMC to federal and state agency or consultant staff members:

Erica Tait, FHWA  
Anuradha Kumar, INDOT  
Mary Kennedy, INDOT  
Shaun Miller, INDOT  
Susan Branigin, INDOT  
Karen Wood, Green 3, LLC  
Danielle Kauffmann, INDNR-DHPA  
Rachel Sharkey, INDNR-DHPA

EMC to Indiana Historic Preservation Review Board Members:

J. Scott Keller, Review Board  
Anne Shaw Kingery, Review Board  
Daniel Kloc, AIA, Review Board  
Jason Larrison, AIA, Review Board  
Chandler Lighty, Review Board  
Beth K. McCord, INDNR-DHPA, Review Board  
Joshua Palmer, AIA, Review Board  
April Sievert, Ph.D., Review Board  
Christopher Smith, Deputy Director, INDNR, and Chairman, Review Board

EMC to potentially interested persons:

Eastern Shawnee Tribe of Oklahoma  
Miami Tribe of Oklahoma  
Peoria Tribe of Indians of Oklahoma  
Pokagon Band of Potawatomi Indians  
United Keetoowah Band of Cherokee Indians  
Delaware Tribe of Indians

Jim Thatcher, Dearborn County Commissioner, District 1  
Art Little, Dearborn County Commissioner, District 2  
Rick Probst, Dearborn County Commissioner, District 3  
Tim Grieve, Dearborn County Highway Superintendent  
Mark Policinski, Ohio-Kentucky-Indiana Regional Council of Governments

Jarrad Holbrook, Indiana Landmarks Southeast Field Office  
Dearborn County Historian  
Dearborn County Historical Society  
Dr. James L. Cooper, DePauw University Professor Emeritus of History  
Paul Brandenburg, Indiana Historic Spans Task Force  
Tony Dillon, Historic Hoosier Bridges  
Nathan Holth, [HistoricBridges.org](http://HistoricBridges.org)  
Kitty Henderson, Historic Bridge Foundation

CC to potentially interested persons:

Aurora Historic Preservation Commission  
Dearborn County Trust for Historic Preservation

## Karen Wood

---

**From:** Nathan Holth <nathan@historicbridges.org>  
**Sent:** Tuesday, August 11, 2020 4:25 AM  
**To:** Heather Dewey; Slider, Chad (DNR); McCord, Beth K  
**Cc:** sbranigin@indot.in.gov; Karen Wood; gprince@indot.in.gov; Kennedy, Mary  
**Subject:** Re: FHWA Project: Des. No.1383721, SR 46 over the Whitewater River Bridge No. 046-15-01987A (NBI No. 17540) in Dearborn County, Indiana

**Follow Up Flag:** Follow up  
**Flag Status:** Flagged

Please include me as a consulting party on this project.

Thanks,  
Nathan Holth

---

Nathan Holth  
Author/ Photographer/Webmaster  
----HistoricBridges.org----  
"Promoting the Preservation Of Our Transportation Heritage"

---

nathan@historicbridges.org  
www.historicbridges.org

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---

On 7/24/2020 1:28:23 AM, Heather Dewey <heather@green3studio.com> wrote:

The Indiana Department of Transportation, with funding from the Federal Highway Administration, proposes to proceed with SR 46 over the Whitewater River Bridge No. 046-15-01987A (NBI No. 17540), Des. No. 1383721.



# Miami Tribe of Oklahoma

3410 P St. NW, Miami, OK 74354 • P.O. Box 1326, Miami, OK 74355  
Ph: (918) 541-1300 • Fax: (918) 542-7260  
[www.miamination.com](http://www.miamination.com)



Via email: [smiller@indot.in.gov](mailto:smiller@indot.in.gov)

August 18, 2020

Shaun Miller  
Archaeological Team Lead, Cultural Resources Office  
Indiana DOT  
575 North Pennsylvania Street  
Indianapolis, IN 46204

Re: Des. No. 1383721, SR 46 over the Whitewater River, Dearborn County, Indiana –  
Comments of the Miami Tribe of Oklahoma

Dear Mr. Miller:

Aya, kikwehsitoole – I show you respect. The Miami Tribe of Oklahoma, a federally recognized Indian tribe with a Constitution ratified in 1939 under the Oklahoma Indian Welfare Act of 1936, respectfully submits the following comments regarding Des. No. 1383721.

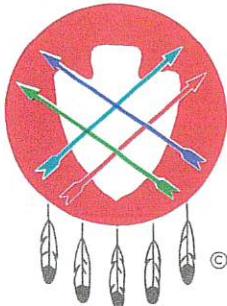
The Miami Tribe offers no objection to the above-referenced project at this time, as we are not currently aware of existing documentation directly linking a specific Miami cultural or historic site to the project site. However, given the Miami Tribe's deep and enduring relationship to its historic lands and cultural property within present-day Indiana, if any human remains or Native American cultural items falling under the Native American Graves Protection and Repatriation Act (NAGPRA) or archaeological evidence is discovered during any phase of this project, the Miami Tribe requests immediate consultation with the entity of jurisdiction for the location of discovery. In such a case, please contact me at 918-541-8966 or by email at [dhunter@miamination.com](mailto:dhunter@miamination.com) to initiate consultation.

The Miami Tribe accepts the invitation to serve as a consulting party to the proposed project. In my capacity as Tribal Historic Preservation Officer I am the point of contact for consultation.

Respectfully,

*Diane Hunter*

Diane Hunter  
Tribal Historic Preservation Officer



## PEORIA TRIBE OF INDIANS OF OKLAHOMA

118 S. Eight Tribes Trail (918) 540-2535 FAX (918) 540-2538

P.O. Box 1527

MIAMI, OKLAHOMA 74355

CHIEF  
Craig Harper

SECOND CHIEF  
vacant

November 2, 2021

Shaun Miller  
Tribal Contact  
INDOT  
100 N Senate Ave., Rm N642  
Indianapolis, IN 46204

Re: Des. No.#1383721 SR 46 over the Whitewater River Bridge #046-15-01987A  
(NBI#17540) Dearborn County, IN.

Thank you for providing notice of the referenced project. The Peoria Tribe of Indians of Oklahoma is unaware of a direct link to the newly proposed project location.

The Peoria Tribe of Indians of Oklahoma is unaware of items covered under Native American Graves Protection and Repatriation Act (NAGPRA) to be associated with the proposed project site, including funerary or sacred objects; objects of cultural patrimony; or ancestral human remains.

The Peoria Tribe has no objection at this time to the proposed project. If, however, at any time items are discovered which fall under the protection of NAGPRA, the Peoria Tribe requests immediate notification and consultation. In addition, state, local and tribal authorities should be advised as to the findings and construction halted until consultation with all concerned parties has occurred.

Please feel free to contact me directly at the number above if additional consultation is necessary. Thank you again for your consideration with this matter.

Sincerely,

Charla K. EchoHawk  
Director of Cultural Preservation



**EASTERN SHAWNEE  
CULTURAL PRESERVATION DEPARTMENT**  
70500 East 128 Road, Wyandotte, OK 74370

December 8, 2021

INDOT - Indiana Department of Transportation  
100 N. Senate Ave. IGCN642  
Indianapolis, IN 46201

**RE: Des No 1383721, Dearborn County, Indiana**

Dear Mrs. Kennedy,

The Eastern Shawnee Tribe has received your letter regarding the above referenced project(s) within Dearborn County, Indiana. The Eastern Shawnee Tribe is committed to protecting sites important to Tribal Heritage, Culture and Religion. Furthermore, the Tribe is particularly concerned with historical sites that may contain but not limited to the burial(s) of human remains and associated funerary objects.

As described in your correspondence, and upon research of our database(s) and files, we find our people occupied these areas historically and/or prehistorically. However, the project proposes **NO Adverse Effect** or endangerment to known sites of interest to the Eastern Shawnee Tribe. Please continue project as planned. However, should this project inadvertently discover an archeological site or object(s) we request that you immediately contact the Eastern Shawnee Tribe, as well as the appropriate state agencies (within 24 hours). We also ask that all ground disturbing activity stop until the Tribe and State agencies are consulted. Please note that any future changes to this project will require additional consultation.

In accordance with the NHPA of 1966 (16 U.S.C. § 470-470w-6), federally funded, licensed, or permitted undertakings that are subject to the Section 106 review process must determine effects to significant historic properties. As clarified in Section 101(d)(6)(A-B), historic properties may have religious and/or cultural significance to Indian Tribes. Section 106 of NHPA requires Federal agencies to consider the effects of their actions on all significant historic properties (36 CFR Part 800) as does the National Environmental Policy Act of 1969 (43 U.S.C. § 4321-4347 and 40 CFR § 1501.7(a)). This letter evidences NHPA and NEPA historic properties compliance pertaining to consultation with this Tribe regarding the referenced proposed projects.

Thank you, for contacting the Eastern Shawnee Tribe, we appreciate your cooperation. Should you have any further questions or comments please contact our Office.

Sincerely,

Paul Barton, Tribal Historic Preservation Officer (THPO)  
Eastern Shawnee Tribe of Oklahoma  
(918) 666-5151 Ext:1833

**SR 46 over Whitewater River (Historic Bridge Project)**  
**HISTORIC PROPERTY REPORT**

**Dearborn County, Indiana**  
**Des. No. 1383721**

October 2021



Prepared for:  
Beam, Longest and Neff, LLC  
8320 Craig St.  
Indianapolis, IN 46250

By:



Karen Wood  
Project Manager  
SJCA, Inc.  
9102 N. Meridian St., Suite 200  
Indianapolis, IN 46260

p.317.566.0629

f. 317.566.0633

kwood@sjcainc.com



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## **Management Summary**

This report documents the identification and evaluation efforts for properties included in the Area of Potential Effects (APE) for the SR 46 over Whitewater River Historic Bridge Project in Dearborn County, Indiana (Des. No. 1383721). Above-ground resources located within the project APE were identified and evaluated in accordance with Section 106, National Historic Preservation Act (NHPA) of 1966, as amended, and the regulations implementing Section 106 (36 CFR Part 800).

As a result of the NHPA, as amended, and CFR Part 800, federal agencies are required to take into account the impact of federal undertakings upon historic properties in the area of the undertaking. Historic properties include buildings, structures, sites, objects, and/or districts that are eligible for or listed in the National Register of Historic Places (NRHP). As this project is receiving funding from the Federal Highway Administration (FHWA), it is subject to a Section 106 review.

The APE contains no properties that are listed in the NRHP. The APE contains one (1) property that is recommended eligible for listing in the NRHP: State Road 46 Bridge (IHSSI No. 029-263-00017, rated “contributing”).

## Introduction/Project Description

The Indiana Department of Transportation (INDOT), with funding from the Federal Highway Administration (FHWA), proposes to proceed with the SR 46 over Whitewater River Historic Bridge Project, Des. No. 1383721. The proposed undertaking is at SR 46 over Whitewater River in Dearborn County, Indiana. It is within both Logan and Harrison township, Harrison USGS Topographic Quadrangle, in Section 10, Township 7 North, Range 1 West (see Fig. 1 below).

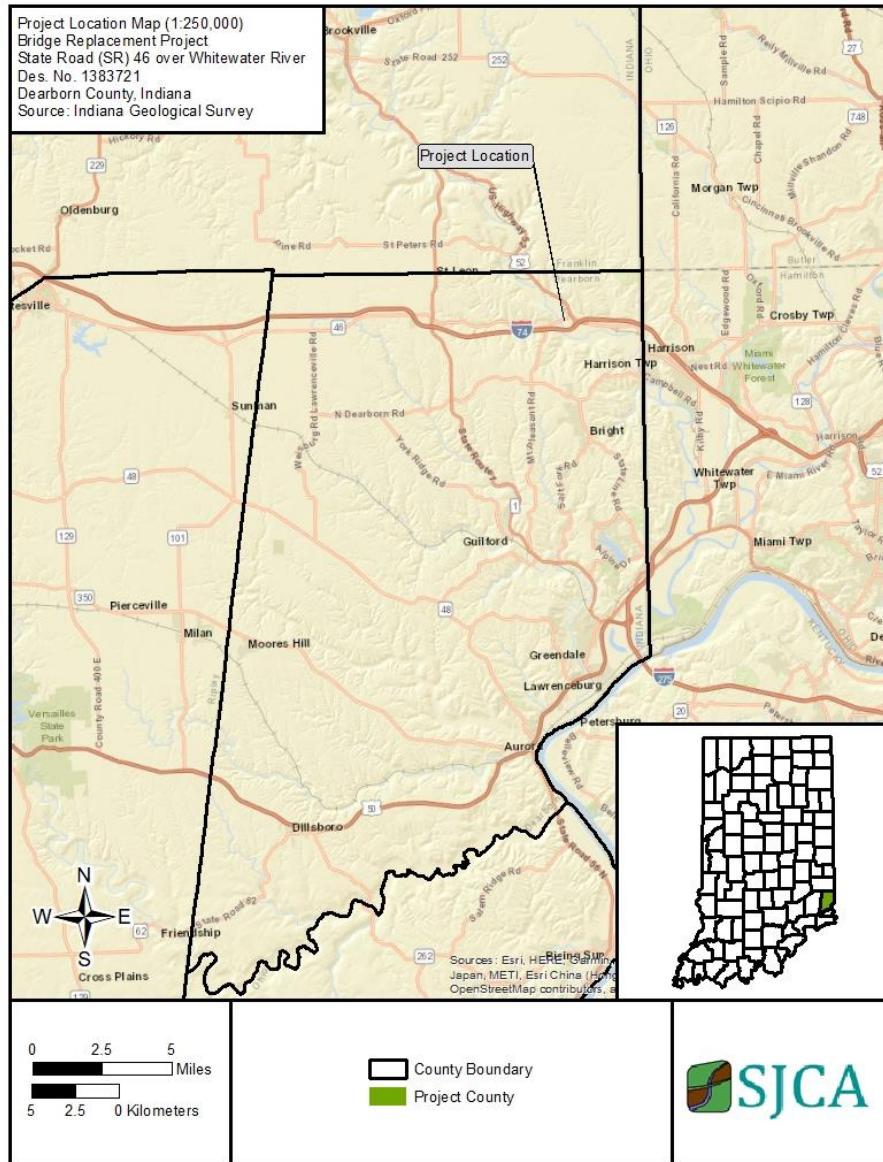


Figure 1: Project Location map

Karen Wood, a qualified professional (QP) historian who meets the Secretary of the Interior Standards, conducted the site investigation, evaluated properties in the APE, and prepared this Historic Property Report.

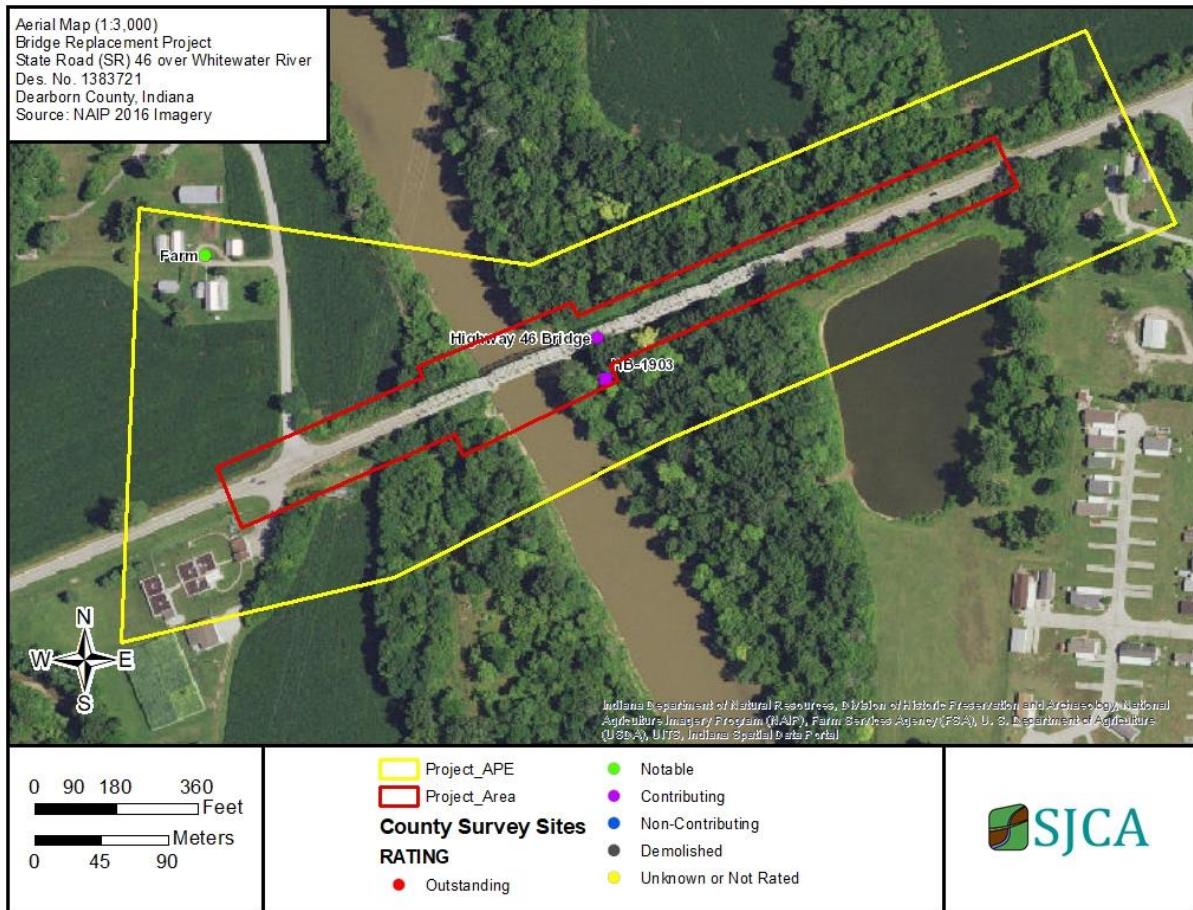


Figure 2: Aerial Map showing Area of Potential Effects

The proposed project involves the rehabilitation of the existing bridge and the east and west approach roadway. In the chosen alternative, Bridge No. 046-15-01987A (NBI No. 17540) will undergo minimization and mitigation wherein only the severely deteriorated and substandard portions of the bridge will be replaced in kind to preserve integrity and improve longevity.

It is anticipated that no permanent or temporary right-of-way (ROW) will be required.

The APE, as defined in 36 CFR 800.16(d), is “the geographic area or areas within which an undertaking may directly or indirectly cause alterations in the character or use of historic properties, if any such properties exist.” The APE of the project includes all properties adjacent to the project and those with a proximate viewshed of the project. Land use within the APE consists primarily of residential and agricultural property. The dimensions of the APE were defined by lines of trees along either side of SR 46, open agricultural fields on the west end of the project limits, and elevation changes on the east end of the project limits. From the center of the project, the APE extends east 0.22 mile, south 0.05 mile, west 0.25 mile, and to the north 0.09 mile (see Fig. 2 above).

## **Literature Review/Previous Investigations**

Research has indicated that no previous Section 106 review investigations have occurred within the project APE.

The NRHP, Indiana Register of Historic Sites and Structures, the State Historic Architectural and Archaeological Database (SHAARD), the *Indiana Historic Bridges, Buildings, and Cemetery Map* (IHBBCM), showing the results of the Indiana Historic Sites and Structures Inventory (IHSSI), and the *Dearborn County Interim Report* (1983) were consulted. The APE contains no properties listed in the NRHP. Two (2) above-ground structures had been previously surveyed: Farm (IHSSI No. 029-103-05008, rated “notable”) and State Road 46 Bridge (HB-1903, IHSSI No. 029-263-00017, rated “contributing”).

There are no resources documented in the Historic American Buildings Survey (HABS), the Historic American Engineering Record (HAER), and Historic American Landscapes Survey (HALS) within the vicinity of the project. The INDOT-sponsored *Historic Bridge Inventory (HBI)* (February 2009) by M & H Architecture, Inc. was also reviewed. One (1) historic bridge was listed within the APE: Bridge No. 046-15-01987A (“contributing”), NBI No. 17540, classified as “Non-Select.” It is recommended eligible under Criterion C because it represents a significant phase of work of a master and “because it represents a variation, evolution, or transition that is conveyed through important features or innovations related to bridge construction, design, or engineering.”<sup>1</sup> It is listed on pg. 38 of the *Historic Bridge Inventory*.

A full list of resources used in evaluating the properties in the APE is provided in the “Bibliography” section.

---

<sup>1</sup> M&H Architecture, Inc. *Indiana Historic Bridge Inventory Volume 2: Listing of Historic and Non-Historic Bridges* (February 2009): p. 38.

## Historic Context

The above-ground resource in the APE with a rating of “notable” or higher was constructed in 1838 in Harrison Township, Dearborn County, Indiana. This resource is the Farm (IHSSI No. 029-103-05008). This resource falls within the themes of architecture and agriculture.

The Farm (IHSSI No. 029-103-05008) is three (3) miles outside of the town of West Harrison, 500 ft. north of SR 46 and on the western side of Barber Rd. West Harrison was and continues to be the twin city of nearby Harrison, Ohio, and the two towns rapidly grew into one another and relied on one another for commercial and community development. They were and are divided by State Street, which marks the state borders of Indiana and Ohio. West Harrison was incorporated in 1886.<sup>2</sup>

### Agriculture

The economic development of West Harrison was slow and relied on agriculture. When the first buildings were constructed in 1800, most of the population was young and male. These citizens were typically farmers and frontiersmen. According to multiple historical accounts, West Harrison and the surrounding region in eastern Indiana experienced a population boom following the signing of the 1814 Treaty of Greenville.<sup>3</sup> The 1814 Treaty of Greenville was labeled a “treaty of peace and friendship” between the United States of America and a Native American delegation comprised of members of the Wyandots, Delaware, Shawanoese, Senacas, and Miamis.<sup>4</sup>

The primary industries for early European American settlers were farming and logging. The first buildings constructed in the region reflect these industries. In 1808, William Purcell and Thomas Breckinridge built the first sawmill in the area.<sup>5</sup> In 1810, it was joined by Bond’s Mill, a water-powered structure for grinding wheat. In 1824, a grist mill was built by Purcell and Breckinridge near their still-running sawmill.<sup>6</sup> Once these industries were established, more settlers arrived to work in the region as farmers.

Before farming could begin, the settlers needed to clear the wooded territory of Dearborn County. Sawmills played an important role in preparing the land for agriculture and formed an essential part of rural Indiana commerce in the 19th century. A sawmill was the first commercial structure built in West Harrison, and it was utilized by farmers to prepare the lumber needed for their homes. A sawmill was a water-powered structure used to turn felled trees into accessible lumber that was then used to make farmhouses, barns, and other necessary structures.<sup>7</sup> The lumber not used in West Harrison was sold to nearby Harrison, Ohio or transported on the Whitewater River to other nearby settlements in Indiana.<sup>8</sup>

The most common agricultural products produced by West Harrison farms in the 19<sup>th</sup> century were corn, tomatoes, watermelon, pork, dairy, and soybeans. Farmers also grew smaller crops of

---

<sup>2</sup> Archibald Shaw, *History of Dearborn County, Indiana: Her People, Industries and Institutions*, (Unigraphic, 1915), p. 177.

<sup>3</sup> Ibid.

<sup>4</sup> 1814 Treaty of Greenville, 7 Stat. 118, 1814, <https://govtrackus.s3.amazonaws.com/legislink/pdf/stat/7/STATUTE-7-Pg118.pdf>.

<sup>5</sup> Shaw, *History of Dearborn County Indiana*, p. 178.

<sup>6</sup> *Dearborn County Interim Report*, (Indianapolis: Indiana, 1983), p. 4.

<sup>7</sup> F.E. Weakley, *History of Dearborn and Ohio Counties, Indiana*, 1885, p. 112.

<sup>8</sup> Ibid.

potatoes, strawberries, blueberries, and carrots.<sup>9</sup> Grains, like corn, were sent to the local gristmill along the Whitewater River to be ground down and prepared for transportation to other parts of Indiana.<sup>10</sup>

The modern layout of the Farm (IHSSI No. 029-103-05008) has changed little from when it was constructed in 1838. It still possesses the original farmhouse as well as three of the barns and outbuildings which were built between 1838 and 1877.

Dearborn County and the town of West Harrison were primarily settled by English and German immigrants between the 1820s and 1870s.<sup>11</sup> The majority of the community was rural until the Whitewater Canal was built between 1836 to 1843. Once the canal reached West Harrison, river and canal trade dominated Dearborn county's commerce. Canal trade was the primary source of commerce until the Ohio and Mississippi Railroad was built in the 1850s, and trade by the railroad became dominant. Some key examples of the urban commerce of West Harrison include the Commercial Building (IHSSI No. 029-263-01024) and the Industrial Mill Building (IHSSI No. 029-263-01018). Although the canal and railroad helped boost commerce in West Harrison, agriculture was still essential because over 90% of the products sold were those produced by farming and logging.<sup>12</sup>

### **Architecture**

The farmhouse on the Farm (IHSSI No. 029-103-05008, rated "notable") was constructed in 1838 in the Federal style. According to SHAARD, the architect/builder was a man with the last name Hickson.<sup>13</sup> The property has remained a combined residence/commercial farm to the present day. There are four outbuildings, three of which are over 50 years of age. These include 2 small wood-framed sheds along the side and behind the house; 1 lean-to barn; and 1 large, 2-story, wood-framed barn. Only one of the small wood-framed sheds is new. The other three structures have been dated to the mid-19<sup>th</sup> century.

Federal style architecture was a form of classicizing architecture used in the United States c. 1780 to 1830.<sup>14</sup> Some of its key features include streamlined shapes, columns, symmetrical windows, and plain surfaces with attenuated details like friezes.<sup>15</sup> According to the SHPO's style guide "Because the style was popular so early in Indiana's history (1810-1840), most examples are located in southern Indiana, especially near navigable waterways and early roads."<sup>16</sup> In Dearborn County, over 250 Federal-style houses have been surveyed in SHAARD. The highest concentration of Federal-style houses is in the City of Lawrenceburg. Lawrenceburg was one of the first towns settled in the relatively untamed Indiana Territory and its proximity to the Ohio River made it ripe for commercial and industrial development. Structures built from 1806 to

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<sup>9</sup> Shaw, *History of Dearborn County, Indiana: Her People, Industries and Institutions*, p. 178.

<sup>10</sup> Ibid.

<sup>11</sup> Shaw, *History of Dearborn County, Indiana: Her People, Industries and Institutions*, p. 179.

<sup>12</sup> Shaw, *History of Dearborn County, Indiana*, p. 177.

<sup>13</sup> "SHAARD Database." State Historic Architectural and Archaeological Research Database and Structures Map. Indiana Department of Natural Resources and Division of Historic Preservation and Archaeology, 2019. <https://www.in.gov/dnr/historic/4505.htm>.

<sup>14</sup> Virginia Savage McAlester, *A Field Guide to American Houses: The Definitive Guide to Identifying and Understanding America's Domestic Architecture*, (New York: Alfred A. Knopf, 2014), p. 222.

<sup>15</sup> Ibid.

<sup>16</sup> Historic Preservation & Archaeology, "Federal Style," Division of Historic Preservation & Archaeology, January 29, 2021, <https://www.in.gov/dnr/historic-preservation/learn-about-topics/buildings-and-bridges/architectural-styles/federal-style/>.

1835 in Lawrenceburg were built primarily in the Federal style.<sup>17</sup> More locally, in relation to this project, Harrison and Logan townships have a total of approximately twenty-five (25) Federal style houses documented in SHAARD.

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<sup>17</sup> *Dearborn County: Interim Report* (Indianapolis, IN: Historic Landmarks Foundation of Indiana, 1983), 37.

## Methodology

Following a literature review of the project area, Karen Wood, a QP historian, conducted a site visit on July 13, 2020. Wood documented above-ground resources within the APE that are at least 50 years of age or that will be at least 50 years of age at the time of the project letting, expected 2022.

The APE was investigated for the existence of any properties, structures, objects, or districts listed in or eligible for listing in the NRHP. The historian walked and drove the project alignment and the APE. She took general photographs of the project area. There are five (5) above-ground resources which can be categorized as residential, commercial, or agricultural. Of these resources, three (3) resources will be at least fifty (50) years old at the time of project letting (anticipated summer 2022). Of these three (3) resources, one (1) has received a “non-contributing” rating due to a loss of integrity due to alterations with modern materials that include vinyl siding, replacement windows, a replacement front door, and a lean-to addition to the rear of the house. The remaining two (2) have received a “contributing” or higher rating per the IHSSI criteria and are discussed more in the next section: Farm (IHSSI No. 029-103-05008, rated “notable”) and the State Road 46 Bridge (IHSSI No. 029-263-00017, rated “contributing”).

Of the remaining two (2) resources, both are of modern construction. They include one (1) residential property and one (1) commercial property. These “modern” resources appear in streetscape photographs (see Appendix B for Photographs).

It should be noted that the Division of Historic Preservation and Archaeology (DHPA) has changed the methodology of the IHSSI program. Specifically, the IHSSI will no longer survey properties that are rated “contributing” and located outside of historic districts. The following resources will continue to be surveyed for the IHSSI: all properties that are rated “notable” or “outstanding,” properties that are rated “contributing” and located within historic districts, all bridges, and all cemeteries.

Notwithstanding DHPA’s amendment of IHSSI methodology, INDOT still requires all “contributing” properties within a proposed project’s APE to be surveyed and documented by a qualified professional historian. However, in recognition of the change to IHSSI methodology, “contributing” properties that are located outside of a historic district did not receive an individual NRHP-eligibility evaluation within the text of this HPR. Instead, if such properties are present within the APE they were documented in a table in the appendix, which includes photographs. As before, the IHSSI served as an aid in rating properties, but the historian was responsible for confirming or adjusting this rating—using the IHSSI criteria—based on their own field work and research. Likewise, the historian was responsible for identifying previously un-surveyed individual resources and historic districts.

With the exception of resources already listed in the NRHP (either individually and/or as part of a historic district), the text of the HPR includes NRHP-eligibility evaluations of all potential historic districts and all properties that the historian rated “notable” or “outstanding,” whether previously surveyed or not. The historian who prepared the HPR considered the potential NRHP eligibility of every above-ground resource within the APE.

## **NRHP Eligibility and Evaluations**

Above-ground resources within the APE were identified and evaluated for listing in the NRHP. Eligible above-ground properties may be “districts, sites, buildings, structures, and objects that possess integrity of location, design, setting, materials, workmanship, feeling, and association and:

- A. that are associated with the events that have made a significant contribution to the broad patterns of our history; or
- B. that are associated with the lives of persons significant in our past; or
- C. that embody the distinctive characteristics of a type, period, or method of construction, or that represent the work of a master, or that possess high artistic values, or that represent a significant and distinguishable entity whose components may lack individual distinction; or
- D. that have yielded, or may be likely to yield, information important in prehistory or history.”<sup>18</sup>

The historian took into account seven criteria considerations related to types of resources that ordinarily are not eligible for the NRHP: “cemeteries, birthplaces, or graves of historical figures, properties owned by religious institutions or used for religious purposes, structures that have been moved from their original locations, reconstructed historic buildings, properties primarily commemorative in nature, and properties that have achieved significance within the past 50 years.” Although these types of properties are ordinarily exempt from listing, they were documented if they occurred within the APE and if they met the minimum age requirement.

There are no resources listed in the NRHP within the APE of the project.

A total of two (2) above-ground resources were identified within the APE meriting a rating of “contributing” or higher: Farm (IHSSI No. 029-103-05008, rated “notable”) and State Road 46 Bridge No. 046-15-01987A, (IHSSI No. 029-263-00017 “contributing”), NBI No. 17540. No newly identified resources were identified during the site visit that have been given a rating of “contributing.”

Additionally, the project area was investigated for potential historic districts. After a review of historic aerials and topographic maps, no potential historic districts were identified.

Described below are the two (2) resources that are evaluated for NRHP eligibility.

### **Resources Listed in/Previously Determined Eligible for the NRHP**

**State Road 46 Bridge No. 046-15-01987A, IHSSI No. 029-263-00017, NBI No. 17540**  
**Construction Date:** 1937-1938

**Address:** State Road 46 over Whitewater River, West Harrison, Dearborn County, Indiana  
**Rating:** “Contributing”

State Road 46 Bridge No. 046-15-01987A is a five span Parker through Truss with each span measuring 175 feet long. According to the HBI, Bridge No. 046-15-01987A (NBI No. 17540) was determined eligible for the NRHP under Criterion C because it represents a significant phase of

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<sup>18</sup> National Park Service, “How to Complete the National Registration Form,” *National Register Bulletin*, 16a, 1997.

work of I.E. Smith Construction, a master builder in Indiana, and it demonstrates innovations related to bridge construction because it is the longest example of the ISHC's standard design for 175' Parkers in the mid-1930s.<sup>19</sup> One of the main innovations utilized in the bridge design is the use of multiple thru-truss spans, which allowed significant distances to be achieved. The thru-truss spans limited the amount of substructure construction required. Because the bridge still exhibits the qualities which resulted in it being recommended eligible in the HBI, it retains eligibility for the NRHP.



Figure 3: Facing Northeast towards State Road 46 Bridge No. 046-15-01987A (NBI No. 17540)

**Verbal Boundary:** The proposed boundary encompasses the area immediately surrounding State Road 46 Bridge No. 046-15-01987A (NBI No. 17540), which comprises the legal parcel boundary. The proposed boundary is approximately 0.88 acre in area. See Appendix A for a graphic depicting the proposed boundary, labeled “Historic Boundary Map.”

### **Resources Recommended Not Eligible for the NRHP**

**Farm, IHSSI No. 029-103-05008**

**Construction Date:** 1838

**Address:** RR3 Box 319, West Harrison, Dearborn County, Indiana

**Rating:** “Notable”

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<sup>19</sup> M&H Architecture, Inc. *Indiana Historic Bridge Inventory Volume 2: Listing of Historic and Non-Historic Bridges* (February 2009): p. 38.



Figure 4: Front east-facing façade of Farm, IHSSI No. 029-103-05008, photo taken 13 July 2020



Figure 5: Front, east-facing façade of Farm, IHSSI No. 029-103-05008, photo taken 1982



Figure 6: Outbuildings to the North of Farm, IHSSI No. 029-103-05008, photo taken 13 July 2020



Figure 7: Outbuildings to the North of Farm, IHSSI No. 029-103-05008, photo taken 13 July 2020



Figure 8: Hoosier Homestead Farm Sign for Farm, IHSSI No. 029-103-05008, photo taken 13 July 2020

### Description:

The Farm (IHSSI No. 029-103-05008) has a two-story farmhouse built in the Federal style in 1838. The farmhouse features white-painted brick masonry walls with a plain, unadorned exterior surface and battered columns along the first floor that support an overhanging porch with a hipped roof. According to SHAARD, the columns and front porch were added in 1910 and demonstrate elements of the Craftsman style.<sup>20</sup> The front door is modern but remains in the original location. There is a row of five bays on each floor with the front door in the center bay on the first floor. The other bays hold windows. The windows along both floors possess modern shutters. The roof is side gabled with new metal paneling.

The Farm (IHSSI No. 029-103-05008) sits on a 60.2-acre plot of land that is intersected by Barber Rd., SR 46, and the Whitewater River (see A-3). In addition to the farmhouse, on the property are four outbuildings. Of these buildings, one is new, and the three others were built between 1838 and 1877. These include 2 small wood-framed sheds along the side and behind the house; 1 lean-to barn; and 1 large, 2-story, wood-framed barn. These three outbuildings are unchanged except for the addition of metal roofs.

Near the front of the gravel driveway is a Hoosier Homestead Farm Sign awarded by the Indiana State Department of Agriculture. The sign indicates the property has been owned by the same family for 150 years and remains active.

<sup>20</sup>IHSSI, “Farm, No. 029-103-05008.” <https://secure.in.gov/apps/dnr/shaard/print.html?printType=countySurvey> “SHAARD Database.” State Historic Architectural and Archaeological Research Database and Structures Map. Indiana Department of Natural Resources and Division of Historic Preservation and Archaeology, 2019. <https://www.in.gov/dnr/historic/4505.htm>.

The farmhouse features several Federal style characteristics. Its main design is that of a rectangular box, two stories high, and two rooms deep. It also has plain, unadorned exterior surfaces and symmetrical, ungrouped windows. A renovation in the Craftsman style can be seen in the inclusion of a front porch along the first floor with battered columns.

When the property was first surveyed in 1982, there had already been a major alteration performed in 1910 with the inclusion of the Craftsman-styled battered columns on the farmhouse.<sup>21</sup> Between 1982 and 2021, it appears the Farm has undergone several structural alterations, including new metal paneling on the roof, a new front door, the replacement of the second floor 8/1 windows with 6/1 windows, and the addition of shutters to the first and second floor windows. These alterations have compromised the architectural integrity of the farmhouse and its outbuildings by replacing key elements like the second-floor windows and the unadorned façade of a Federal style home.

These changes, in addition to the inclusion of a Craftsman porch in 1910, have compromised the overall historical and structural integrity of the structure. Because it possesses elements of two different architectural styles, it is no longer notable in either one. Additionally, key architectural elements have been replaced with modern variants, resulting in the loss of historical integrity.

#### **Statement of Significance:**

The Farm (IHSSI No. 029-103-05008) is associated with agriculture. The structure was built in 1838 when the town of West Harrison was first developed, and the surrounding rural territory was settled by farmers and lumber workers. Although West Harrison experienced a surge in urban commerce through the completion of the canal in 1843 and the Ohio and Mississippi Railroad in the 1850s, agriculture remained the most important cornerstone of the town's economy.<sup>22</sup> Crops and products produced by rural farms like the Farm (IHSSI No. 029-103-05008) were the most traded goods along the canal and the railroad.<sup>23</sup>

In terms of agricultural importance, the Farm (IHSSI No. 029-103-05008) continues to operate on 31 acres but is not a significant example of the agricultural development of the township of West Harrison and the surrounding area.<sup>24</sup> It is also not a significant example of a 19th century farm in Dearborn County and does not offer a significant cultural contribution to the surrounding area. This is because there are numerous other farms in operation in the township of West Harrison and Dearborn County. Although the Farm (IHSSI No. 029-103-05008) continues to operate in an agricultural capacity, it is one of many in Dearborn County.<sup>25</sup>

#### **Evaluation and Eligibility Recommendation:**

The Farm (IHSSI No. 029-103-05008) is associated with agriculture in Indiana, in which the overall historic significance and integrity of the Farm (IHSSI No. 029-103-05008) does not rise to the degree necessary to make a significant contribution to agriculture. The National Park

<sup>21</sup> IHSSI, "Farm, No. 029-103-05008." <https://secure.in.gov/apps/dnr/shaard/print.html?printType=countySurvey> "SHAARD Database." State Historic Architectural and Archaeological Research Database and Structures Map. Indiana Department of Natural Resources and Division of Historic Preservation and Archaeology, 2019. <https://www.in.gov/dnr/historic/4505.htm>.

<sup>22</sup> Weakley, *History of Dearborn and Ohio Counties, Indiana*, p. 112.

<sup>23</sup> Ibid.

<sup>24</sup> AcreValue, "Harrison County Parcel Map," 2020, <https://www.acrevalue.com/plat-map/IN/Harrison/?lat=39.280362&lng=-84.876709&zoom=17#!>, accessed July 8, 2021.

<sup>25</sup> Indiana Department of Agriculture. "Hoosier Homestead Farm Recipients." [https://www.in.gov/isda/files/1976-2014\\_Hoosier\\_Homestead\\_List\\_pdf.pdf](https://www.in.gov/isda/files/1976-2014_Hoosier_Homestead_List_pdf.pdf).

Services' National Register Bulletin, No. 15, "How to Apply the National Register Criteria for Evaluation" states there are seven qualities that define integrity – location, design, setting, materials, workmanship, feeling, and association. Based upon these qualities of integrity, the Farm (IHSSI No. 029-103-05008) has lost a moderate amount of integrity due to alterations to the original structure. The property further retains only a moderate amount of integrity due to location, design, setting, workmanship, feeling, and association. The resource is one of a number of different farms continuing to operate in the area: however, no evidence was found to indicate it contributed significantly to the agriculture of Dearborn County or the state of Indiana during the period of significance. Therefore, this resource is not recommended under Criterion A.

Research conducted for this report did not find an association with a significant individual; thus, this resource is not recommended eligible under Criterion B.

The Farm (IHSSI No. 029-103-05008) contains a house that is an example of a two-story Federal farmhouse popular in the Midwest in the 19<sup>th</sup> century as well as several original outbuildings from 1838 to 1877. However, the buildings have undergone minor to moderate alterations which affect the design, workmanship, and integrity of the buildings, especially the farmhouse. Modifications include the addition of modern windows and window shutters, a new front door on the farmhouse, a Craftsman-styled front porch on the farmhouse, and metal paneled roofs on all the buildings. Therefore, the house is not a noteworthy example of the Federal style and it is not recommended eligible under Criterion C.

This HPR does not evaluate for Criterion D.

## **Conclusions**

The APE contains no properties in the NRHP. As a result of identification and evaluation efforts for this project, the State Road 46 Bridge No. 046-15-01987A (IHSSI No. 029-263-00017, NBI No. 17540) is recommended as eligible for listing in the NRHP. No other properties are recommended eligible for listing in the NRHP.



# INDIANA DEPARTMENT OF TRANSPORTATION

100 North Senate Avenue  
Room N758-ES  
Indianapolis, Indiana 46204

PHONE: (317) 296-0799

**Eric Holcomb, Governor**  
**Joe McGuinness, Commissioner**

September 20, 2021

This letter was sent to the listed parties.

RE: Dual Review Project; SR 46 over the Whitewater River Bridge No. 046-15-01987A (NBI No. 17540) Project, Dearborn County, Indiana, Des No. 1383721, DHPA No. 26124

Dear Consulting Party,

The Indiana Department of Transportation (INDOT), with funding from the Federal Highway Administration (FHWA), proposes to proceed with the State Road (SR) 46 over Whitewater River Bridge No. 046-15-01987A (NBI No. 17540) Project, Des. No. 1383721.

This letter is part of the Section 106 review process for this project. Section 106 of the National Historic Preservation Act requires federal agencies to take into account the effects of their undertakings on historic and archaeological properties. We are requesting comments from you regarding the possible effects of this project. Please use the above Des. Number and project description in your reply and your comments will be incorporated into the formal environmental study.

A Section 106 early coordination letter was distributed on July 23, 2020. A Historic Bridge Alternatives Analysis (HBAA) was distributed to consulting parties on January 21, 2021.

The proposed undertaking is on SR 46 over the Whitewater River, Bridge No. 046-15-01987A (NBI No. 17540), 0.44 mile west of US 52 in Dearborn County, Indiana. It is within Logan and Harrison Townships, Harrison USGS Topographic Quadrangle; in Section 10, Township 7 North, Range 1 West.

Bridge No. 046-15-01987 A is a five span steel truss structure constructed in 1937. The existing structure has five equal 175'-0" spans. The existing clear roadway over the bridge is 24'-0" and consists of two 10'-0" lanes with 2'-0" shoulders. The bridge was rehabilitated in 1985 and painted in 1996.

The need of this project is to address current structural deficiencies, including deck and superstructure condition. There is heavy deterioration and distress exhibited by some of the truss verticals, diagonals, low chords, stringers, floor beams, and deck. The bridge was load rated with the noted existing deterioration. The analysis found that the verticals and stringers controlled the load rating with design and legal load rating factors less than 1.0. The purpose of this project is to improve the overall condition of the bridge's superstructure and the structural capacity. More details about the purpose and need of this project are contained in the HBAA.

The proposed project involves the rehabilitation of the existing bridge and the east and west approach roadway. In the proposed Alternative B1: Rehabilitation of the Existing Bridge for Continued Vehicle Use (Two-Way Option) Meeting Secretary of Interior's Standards for Rehabilitation, Bridge No. 046-15-01987A (NBI No. 17540) will undergo minimization and mitigation wherein only the severely deteriorated and substandard portions of the bridge will be replaced in kind to preserve integrity and improve longevity. Enclosed are the 30% plans along with a more in-depth written description of the proposed project.

The project is currently scheduled for letting in 2022. It is anticipated that no permanent or temporary right-of-way acquisition will be required.

Beam, Longest and Neff, LLC is under contract with INDOT to advance the environmental documentation for the referenced project. SJCA Inc. has been subcontracted to complete the Section 106 documentation for the project.

In accordance with 36 CFR 800.2 (c), you were invited to become a consulting party as part of the Section 106 process, or you are hereby invited to become a consulting party as part of the Section 106 process. Entities that have previously accepted consulting party status--as well as additional entities that are currently being invited to become consulting parties--are identified in the attached list.

The Section 106 process involves efforts to identify historic properties potentially affected by the undertaking, to assess the undertaking's effects and to seek ways to avoid, minimize, or mitigate any adverse effects on historic properties. For more information regarding the protection of historic resources, please see the Advisory Council on Historic Preservation's guide: *Protecting Historic Properties: A Citizen's Guide to Section 106 Review* available online at <https://www.achp.gov/sites/default/files/documents/2017-01/CitizenGuide.pdf>.

Per the terms of the "Programmatic Agreement Regarding Management and Preservation of Indiana's Historic Bridges" (Historic Bridges PA), the FHWA-Indiana Division will satisfy its Section 106 responsibilities involving "Select" and "Non-Select" bridges through the Project Development Process (PDP) of the Historic Bridges PA (Stipulation III). Because State Bridge # 046-15-01987A (NBI 17540) is a "Non-Select" bridge, the procedures outlined in Stipulation III.B. of the Historic Bridges PA will be followed to fulfill FHWA's Section 106 responsibilities for the project. (A copy of the Historic Bridges PA can be downloaded here: <http://www.in.gov/indot/2530.htm>).

Please note that, per the permanent rule issued by the Indiana Department of Natural Resources effective August 14, 2013 (312 IAC 20-4-11.5), INDOT is requesting that this project be subjected to "dual review"; that is, reviewed by the Division of Historic Preservation and Archaeology simultaneously under 54 U.S.C. 306108 (Section 106) and IC 14-21-1-18 (Indiana Preservation and Archaeology Law dealing with alterations of historic sites and structures requiring a Certificate of Approval). Pursuant to Section 11.5(f) of this rule, at the conclusion of the review process we anticipate that the Division Director would issue a letter of clearance exempting this project from obtaining a Certificate of Approval under IC 14-21-1-18. Enclosed with this letter is a detailed list of the consulting parties with contact information, including email addresses, for processing the dual review submission.

The Area of Potential Effects (APE) is the area in which the proposed project may cause alterations in the character or use of historic resources. The APE contains no resources listed in the National Register of Historic Places (NRHP).

A historian who meets the Secretary of the Interior's Professional Qualification Standards identified and evaluated above-ground resources within the APE for potential eligibility for the NRHP. As a result of the

historic property identification and evaluation efforts, one resource is recommended as eligible for listing in the NRHP: State Road 46 Bridge No. 046-15-01987A, IHSSI No. 029-263-00017, NBI No. 17540.

Concerning archaeological resources, Christopher Jackson, M.S., RPA, an INDOT Qualified Professional archaeologist, reviewed the proposed project area and ascertained that the proposed rehabilitation of the State Road 46 Bridge over the Whitewater River (Bridge No. 046-15-01987A [NBI No. 17540]) {Des 1383721} in Harrison Township, Dearborn County, Indiana will not likely affect archaeological resources based on the project setting.

The bridge will undergo minimization and mitigation wherein only the severely deteriorated and substandard portions of the bridge will be replaced in kind to preserve its integrity and improve longevity. All proposed work will occur in the existing right-of-way.

The proposed work will not likely affect archaeological resources because the existing bridge right-of-way is disturbed, with the disturbance consisting of roadside ditches, man-made/landscaped slopes, and utility easements. The proposed access road is disturbed for it will be utilizing an existing gravel farm road.

A review of SHAARD, which occurred on July 21, 2021, indicated that seven professional investigations have either occurred in or adjacent to the project area. The following is a brief description of these studies:

In 1980, the Indiana Highway Survey Commission undertook a records review and recommendation for Project RS-5615, 46-15-2106, which was the proposed replacement of the SR 46 bridge over the Penn Central Railroad; the project is situated just east of the current project area. It was noted that site 12-D-003 could be in the proposed project area and that site 12-D-126 was nearby. It was recommended that a Phase Ia archaeological survey should be undertaken (Tomak 1980).

The reconnaissance survey was conducted in 1980 by E.J. Fabyan for the Division of Roadside Development of the Indiana Highway Survey Commission. The field investigation noted that most of the proposed area had been disturbed from the construction of the SR 46 right-of-way and the Penn Central railroad track. The study was able to relocate sites 12-D-003 and 12-D-126 and determined that both sites were located outside the proposed project area; thus, neither site would be impacted by the proposed project (Fabyan 1980).

In 1994, Archaeological Resources Management Service (ARMS) conducted a Phase Ia archaeological study for a proposed sewer plant; the investigation was at the request of the Southeastern Indiana Regional Planning Commission. The survey, which was south of SR 46 and west of the river, traversed the western terminus of the current investigation's project area. The ARMS study examined three acres and inventoried one previously unrecorded site (12-D-377 [Kramer Site]). The site was a lithic scatter, and no diagnostic artifacts were collected; consequently, the cultural/temporal association of the site could not be determined. It was recommended that the site was not eligible for placement on the National Register of Historic Places (NRHP). It was also determined from shovel probes, as well as two subsurface tests in Franklin County and one test in Wayne County on the Whitewater River floodplain that the floodplain is characterized by high energy alluvial deposition, which means that buried archaeological deposits is minimal. Therefore, it was also recommended that a Phase Ic investigation was not warranted (Parish 1994).

A Phase Ia archaeological survey of 1,000 acres in Dearborn County was conducted by ARMS via a Survey and Planning Grant from the Division of Historic Preservation and Archaeology. The purpose of the study, which occurred in 1994, was to not only increase the number of sites documented in the county (at the time of the investigation less than 400 sites had been recorded in the county), but to also develop a better understanding of the settlement of the upland areas. It was noted that a great majority of previous studies had been centered

around the stream valleys and very little information was known for the upland regions of the county. A total of 975 acres were examined and inventoried 125 previously undocumented sites, as well as re-examined 5 previously recorded sites. Site 12-D-377 was re-investigated and it was ascertained that the site extended north of SR 46 and that it entailed a lithic scatter and historic scatter. No diagnostic artifacts were collected during the re-examination; thus, the cultural/temporal association of the site could not be ascertained. It was recommended that although the portion of the site south of the highway was not eligible for placement in the NRHP, additional study needed to be conducted on the portion of the site north of SR 46, which would include a Phase Ic investigation (Parish and McCord 1995).

Algonquin Archaeological Consultants (AAC), in 1997, undertook a Phase Ia archaeological investigation for a proposed 6-inch sewer line that was situated south of SR 46 and the river. At the behest of Chaffee and Associates, the study traversed the eastern section of the current investigation's study area. The AAC survey examined 1.9 acres and re-examined one previously documented site (12-D-126). The site, which is outside of the current investigation's project area, was a lithic scatter and historic scatter. No prehistoric diagnostic artifacts were collected; thus, the cultural/temporal affiliation of the site could not be determined. Historic artifacts indicate a 19th to 20th-century utilization of the site. Although the surface scatter suggests that the site was not eligible for placement in the NRHP, due to alluvial deposition, there was the possibility for buried archaeological deposits at the site location. Consequently, archaeological monitoring was recommended during the excavation of the trench (Walley 1997).

At the behest of Robert E. Curry and Associates, ARMS conducted a Phase Ia archaeological study for the proposed Tri-Township waterline project. The survey, which occurred in 1999, traversed the central section of the current investigation's project area; specifically near the crossing of the Whitewater River. The ARMS survey examined approximately 0.7 acres and documented no sites. No evidence of buried archaeological deposits was encountered either in the excavation of augers or an inspection of the soil banks. Thus, it was recommended that a Phase Ic investigation was not warranted (Waters 1999).

In 2019, Cultural Resource Analysts undertook a Phase Ia archaeological survey for the proposed SR 46 bridge rehabilitation project; the study, which was east of the eastern terminus of the current investigation, was at the request of Beam, Longest & Neff. The survey examined 3.1 acres and recorded one previously undocumented site (12-D-717). The site, which was an isolated find and, was recommended not to be eligible for listing in the NRHP. Three bucket augers were excavated in the Logan Creek floodplain with no buried archaeological deposits encountered. Based on the results of the bucket augers and the small amount of floodplain that was to be acquired, it was recommended that a Phase Ic investigation was not warranted (Dickerson 2020).

According to SHAARD and SHAARD GIS, which was examined on July 21, 2021, one archaeological site (Kramer Site) has been recorded at the western terminus of the project area. The Kramer Site, which was originally documented in 1994 and re-investigated later that year, was a lithic scatter and historic scatter situated on the floodplain (Parrish 1994; Parrish and McCord 1995). No prehistoric diagnostic artifacts have been collected from the site; thus, the cultural/temporal association could not be ascertained. Historic artifacts indicate that the site was utilized in the 19th and/or 20th centuries. The site measured 290 m by 150 m. It was recommended that the portion of the site south of SR 46 was not eligible for placement in the NRHP, while additional study was recommended for the portion north of the highway. The proposed access road is situated south of the highway thereby traversing that section of the site that has been recommended at not being eligible for listing in the NRHP.

Five sites (12-D-003, 12-D-126, 12-D-4724, 12-D-425, and 12-D-717) have been inventoried near the project area; none of these sites will be directly impacted by the proposed project.

Based on the project setting, which is disturbed, there are no archaeological concerns and no further work is recommended. However, state law (Indiana Code 14-21-1-27 and -29) requires that if any prehistoric or historic archaeological artifacts or human remains are uncovered during construction, demolition, or earth moving activities, the discovery must be reported to the Department of Natural Resources within two (2) business days.

In a letter dated February 15, 2021, Indiana State Historic Preservation Officer (SHPO) staff stated that they "agree with the conclusions of the HBAA that Alternative B1: Rehabilitation of the Existing Bridge for Continued Vehicle Use (Two-Way Option)" is a feasible and prudent alternative; therefore, the preferred alternative for this project. Also, SHPO staff requested that this bridge be documented according to the "Indiana DNR – Division of Historic Preservation and Archaeology Minimum Architectural Documentation Standards" as noted in the Mitigation section of the HBAA.

The Historic Property Report and 30% Plans are available for review in IN SCOPE at <http://erms.indot.in.gov/Section106Documents/> (the Des. No. is the most efficient search term, once in IN SCOPE). You are invited to review these documents and to respond with comments on any historic resource impacts incurred as a result of this project so that an environmental report can be completed. We also welcome your related opinions and other input to be considered in the preparation of the environmental document. If you prefer a hard-copy of this material, please respond to this email with your request as soon as you can.

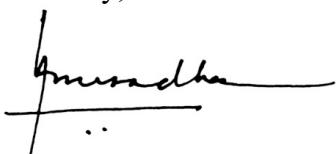
Please review the information and comment within thirty (30) calendar days of receipt. If you indicate that you do not desire to be a consulting party or if you have not previously accepted consulting party status and you do not respond to this letter, you will not be included on the list of consulting parties for this project and will not receive further information about the project unless the design changes. Tribal consulting parties may enter the process at any time and are encouraged to respond to this notification with any comments or concerns at their earliest convenience.

For questions concerning specific project details, you may contact Karen Wood of SJCA Inc. at (317) 566-0629 or [kwood@sjcainc.com](mailto:kwood@sjcainc.com). All future responses regarding the proposed project should be forwarded to SJCA Inc. at the following address:

Karen Wood  
Cultural Resources and Environmental Manager  
SJCA Inc.  
9102 N. Meridian St., Suite 200  
Indianapolis, IN 46260  
[kwood@sjcainc.com](mailto:kwood@sjcainc.com)

Tribal contacts may contact Shaun Miller at [smiller@indot.in.gov](mailto:smiller@indot.in.gov) or 317-416-0876 or Kari Carmany-George at FHWA at [K.CarmanyGeorge@dot.gov](mailto:K.CarmanyGeorge@dot.gov) or 317-226-5629.

Sincerely,



Anuradha V. Kumar, Manager  
Cultural Resources Office  
Environmental Services

Enclosures:

Historic Property Report (available via IN SCOPE)

30% Plans (available via IN SCOPE)

2021-2-15 Letter from SHPO to Karen Wood, SJCA Inc.

30% Plans Written Description

Distribution List:

Indiana State Historic Preservation Officer, dkauffmann@dnr.in.gov, rsharkey@dnr.in.gov

Historicbridges.org, nathan@historicbridges.org

Eastern Shawnee Tribe of Oklahoma

Miami Tribe of Oklahoma

Peoria Tribe of Indians of Oklahoma

Pokagon Band of Potowatomi Indians

United Keetoowah Band of Cherokee Indians

Delaware Tribe of Indians

References Cited:

Dickerson, John P.

2020 A Phase Ia Archaeological Reconnaissance Survey for the Proposed SR 46 Bridge (Structure No. 46-15-3054A) Rehabilitation Project in Dearborn County, Indiana (INDOT Des No. 1900094). Cultural Resource Analysts. Submitted to Beam, Longest & Neff, Indianapolis. Copies on file at Division of Historic Preservation and Archaeology, Indianapolis.

Fabyan, E.J.

1980 Archaeological Reconnaissance of Indiana State Highway Commission Project: RS-5615( ), Structure: 46-15-2106, S.R. 46, Dearborn County, Indiana. E.J. Fabyan. Submitted to Division of Roadside Development, Indiana Highway Survey Commission, Indianapolis. Copies on file at Division of Historic Preservation and Archaeology, Indianapolis.

Parish, Cindy K.

1994 Archaeological Field Reconnaissance: St. Leon Sewer Project, Dearborn County, Indiana. Archaeological Resources Management Service. Submitted to Southeastern Indiana Regional Planning Commission. Copies on file at Division of Historic Preservation and Archaeology, Indianapolis.

Parish, Cindy K., and Beth K. McCord

1995 An Archaeological Survey of Dearborn County, Indiana. Archaeological Resources Management Service. Submitted to and copies on file at Division of Historic Preservation and Archaeology, Indianapolis.

Tomak, Curtis H.

1980 An Archaeological Records Check and Recommendations for State Highway Project RS-5615( ), 46-15-2106, Dearborn County, Indiana. Indiana Highway Survey Commission. Submitted to Indiana Highway Survey Commission, Indianapolis. Copies on file at Division of Historic Preservation and Archaeology, Indianapolis.

Walley, Scott A.

1997 Phase Ia Survey of Proposed Sewer Corridor, Harrison Township, Dearborn County, Indiana. Algonquin Archaeological Consultants. Submitted to Chaffee and Associates, Batesville, Indiana. Copies on file at Division of Historic Preservation and Archaeology, Indianapolis.

Waters, Nicki A.

1999 Archaeological Field Reconnaissance: Tri-Township Waterline Project, Dearborn County, Indiana. Archaeological Resources Management Service. Submitted to Robert E. Curry and Associates, Danville, Indiana. Copies on file at Division of Historic Preservation and Archaeology, Indianapolis.

## Written Description of 30% Plans

The concrete deck, deck drains, and metal forms in all spans shall be replaced. The new deck will provide a standard 2% cross slope and metal forms may be used on the underside to form and place the deck. The new deck will incorporate a PF-1 bridge railing in place of the concrete curbs in front of the existing steel bridge posts and handrail. Some of the existing steel bridge posts and portions of the existing steel handrailing that have holes due to deterioration will be replaced in kind. The existing concrete bridge railing transitions will be replaced with new concrete railing transitions that meet the current crash test level requirements and shall be placed on top of the new concrete bridge approach slabs. The new concrete railing transitions will be like the existing and include a bush hammered concrete finish and will taper to the widened approach guardrail. The existing approach guardrail will be replaced with MASH guardrail and end treatments.

The bridge joints will be replaced with pre-compressed foam joints or an INDOT approved joint. At the double expansion joint, replace bridge joint with a modular expansion joint or an INDOT approved joint. New approach slabs will be placed. The approach work will include milling and resurfacing the existing bituminous approaches to tie the new construction in with the existing roadway. Full depth pavement will be placed at the ends of the approach slabs to tie the resurfacing work to the concrete approach slabs. Deteriorated truss members (low chords, diagonals, verticals, and end posts) that have holes 2 inch in diameter and larger to restore lost section and improve condition shall be repaired or replaced. Holes smaller than 2 inches may either be ignored or have a bolt placed through the hole. Truss members will be repaired by placing a plate over the deteriorated area or the members will be replaced in kind. Replace in kind existing gusset plates that have areas of heavy deterioration. Connection plates and lattice bars with heavy section loss and holes will be replaced in kind. Lower lateral bracing and stay plates with heavy section loss will be repaired or replaced in kind. Steel stringers in poor condition or that have heavy section loss will be replaced in kind or repaired. Floor beams with heavy section loss will be repaired and at this time no floor beams are expected to be replaced. Repairing stringers and floor beams will be done by placing a plate over the deteriorated area.

Rivets removed to make necessary repairs will be replaced with round headed bolts. The rounded head will be installed on the face of the member that is most visible from the roadway or exterior face of member. The expansion bearings that have moved or are over-rotated will be reset to the proper orientation. Deteriorated anchor bolts or nuts and portions of the bearing assemblies will be replaced in kind. All areas of the truss will be cleaned and painted. The paint color will match the existing color. The concrete in the low chord of Span B will be removed. The deteriorated concrete caps of Pier 5 and the East Abutment will be removed and reconstructed. Delaminated and spalled areas of abutments and piers will be patched. The riprap scour countermeasures shall be placed around all substructure units. Debris that is lodged against the substructure units will be removed. Tree branches will be trimmed on both sides of the road to keep branches away from the truss and roadway.

The work described above matches the preferred alternative for this project as outlined in the approved Historic Bridge Alternative Analysis Report.



Division of Historic Preservation & Archaeology · 402 W. Washington Street, W274 · Indianapolis, IN 46204-2739  
Phone 317-232-1646 · Fax 317-232-0693 · dhp@dnr.IN.gov ·



November 18, 2021

Karen Wood  
Cultural Resources & Environmental Manager  
SJCA, Inc.  
9102 N. Meridian Street, Suite 200  
Indianapolis, Indiana 46260

State Agency: Indiana Department of Transportation (“INDOT”),  
Federal Agency: Federal Highway Administration, Indiana Division (“FHWA”)

Re: DUAL REVIEW: Historic property report Wood, 10/2021) and 30% bridge rehabilitation plans for the SR 46 over the Whitewater River Bridge (Bridge No. 046-15-01987A) project (Des. No. 1383721; DHPA No. 26124)

Dear Ms. Wood:

Pursuant to Section 106 of the National Historic Preservation Act of 1966, as amended (54 U.S.C. § 306108); implementing regulations at 36 C.F.R. Part 800; the “Programmatic Agreement Among the Federal Highway Administration, the Indiana Department of Transportation, the Indiana Historic Preservation Officer, and the Advisory Council on Historic Preservation Regarding Management and Preservation of Indiana’s Historic Bridges” (“Indiana Historic Bridges PA”); and the “Programmatic Agreement (PA) Among the Federal Highway Administration, the Indiana Department of Transportation, the Advisory Council on Historic Preservation and the Indiana State Historic Preservation Officer Regarding that Implementation of the Federal Aid Highway Program In the State of Indiana” (“Indiana Minor Projects PA”); and also pursuant to Indiana Code 14-21-1-18 and 312 Indiana Administrative Code (“IAC”) 20-4, the staff of the Indiana State Historic Preservation Officer (“Indiana SHPO”) has reviewed your October 27, 2021, submission which included the historic property report (“HPR”; Wood, 10/2021) and 30% bridge rehabilitation plans received by our office the same day for this project.

The area of potential effects (“APE”) proposed in the HPR appears to be of adequate size to encompass the geographic area in which direct and indirect effects of a project of this nature could occur.

We respectfully disagree with the conclusions presented in the HPR. While we agree that the subject bridge carrying SR 46 over the Whitewater River (Bridge No. 046-15-01987A) was previously determined eligible for inclusion in the National Register of Historic Places (“NRHP”), we disagree with the evaluation of the Notable-rated farm (Indiana Historic Sites and Structures Inventory #029-103-05008) located within the project’s APE.

The farm and its setting are highly representative of early agriculture and its evolution in Dearborn County. The bottomland setting of the farm appears to be largely intact, but further research is needed to reveal whether the whole 60-acre site has significantly changed in configuration, use, and land use patterns. The setting is evident in satellite and street view images, while the photographs provided in the report were tightly framed to showcase the buildings alone.

This five-bay brick I-house is also an intact example of a significant vernacular type. The builder arranged for larger rooms by placing the standard window openings further apart than usual, creating an atypical, elongated variation of plan from the more compact I-house form found on most examples. Whether this was done for family arrangements, space for hired hands, or simply out of builder’s preference, it is not clear based on the information provided. The Craftsman-era porch, c. 1910 has been part of the house’s fabric for over a century; indeed, the house has had the porch for a longer period than not.

Wood  
November 18, 2021  
Page 2

Additionally, the farm's impressive array of outbuildings provides a significant glimpse into agricultural practice locally. It appears that the large English barn could have housed dairy cattle, there is what appears to be a granary, a machine shed, or workshop, and two sheds of unknown use, one of which appears to have been a side-aisle corn crib (furthest SW on the property). Use of agricultural censuses from 1840 – 1880 would reveal much about the possible uses of these buildings. All the buildings appear to date within the historic period (c. 1840s – c. 1970). Our office has nominated numerous farms with similar configurations (and with more recent Craftsman porches as this one has). Overall, we believe that this farm is eligible for inclusion in the National Register under Criterion A and C.

Furthermore, thank you for providing 30% bridge rehabilitation plans. We are satisfied with both the written description and the plan sheets and have no comments at this time. As a reminder, we also request, pursuant to the Indiana Historic Bridges PA, that this bridge be photographically documented prior to commencement of relocation. We request color, digital images that provide overall views of the bridge, along with detailed shots of any character-defining features. Please provide a photo log that corresponds to the photographs, a mapped photo key, and an overview thumbnail sheet for our review and comment.

In terms of archaeology, no currently known archaeological resources eligible for inclusion in the National Register of Historic Places have been recorded within the proposed project area. We concur with the recommendation that no archaeological reconnaissance is necessary for the currently proposed project. We note that the western extension of the project area overlaps with the boundary of archaeological site 12-D-0377. This site was investigated in 1994 and 1995 (Parish 1994, Parish and McCord 1995) and those portions within the current project boundary were determined to be ineligible for inclusion in the National Register of Historic Places. Parish and McCord (1995:B-22, 23) recommended that further work may be needed for portions of the site north of SR 46. As currently presented, it does not appear that the current project plans will impact this portion of the site.

While we appreciate the thorough investigation into the archaeological records available through SHAARD by Christopher Jackson that was provided in the cover letter for this submission, we feel that such detailed information may be better suited as an archaeological records check and thus submitted as an Archaeological Short Report. In the future, the inclusion of the archaeological assessment of a project area in the cover letter is more appropriate in cases where the patterns of disturbance are more pronounced and there is not a need for such detailed justification for no archaeological reconnaissance. We also note that the 2019 archaeological reconnaissance for Des. No. 1900094 by CRA, Inc. is described in the letter as being "east of the eastern terminus of the current investigation." According to the records on file at the DHPA, this survey occurred to the west of the current investigation.

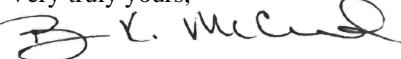
If any prehistoric or historic archaeological artifacts or human remains are uncovered during construction, demolition, or earthmoving activities, state law (Indiana Code 14-21-1-27 and -29) requires that the discovery be reported to the Indiana SHPO within two (2) business days. In that event, please call (317) 232-1646. Be advised that adherence to Indiana Code 14-21-1-27 and -29 does not obviate the need to adhere to applicable federal statutes and regulations, including but not limited to 36 C.F.R. Part 800.

For the benefit of those recipients of a copy of this letter who are not Section 106 consulting parties, please be aware that a copy of this submission can be found online at <http://erms.indot.in.gov/Section106Documents/>. From there, search by this project's designation number: 1383721.

If you have questions regarding our dual review of the aforementioned project, please contact DNR-DHPA. Questions about archaeological issues should be directed to contact Rachel Sharkey at (317) 234-5254 or rsharkey@dnr.in.gov. Questions about historic buildings or structures pertaining to this review should be directed to Danielle Kauffmann at (317) 232-0582 or dkauffmann@dnr.IN.gov. If you have any questions regarding the National Register eligibility of cultural resources within the APE, please contact Paul Diebold or Holly Tate.

In all future correspondence regarding the dual review of this bridge rehabilitation project on SR 46 over the Whitewater River in Dearborn County (Des. No. 1383721), please refer to DHPA No. 26124.

Very truly yours,



Beth K. McCord  
Deputy State Historic Preservation Officer  
Director, Division of Historic Preservation and Archaeology

BKM:DMK:RAS:ras

EMC to federal and state agency or consultant staff members:

Erica Tait, FHWA  
Anuradha Kumar, INDOT

Wood  
November 18, 2021  
Page 3

Mary Kennedy, INDOT  
Shaun Miller, INDOT  
Susan Branigin, INDOT  
Karen Wood, SJCA, Inc.  
Danielle Kauffmann, DNR-DHPA  
Rachel Sharkey, DNR-DHPA  
Paul Diebold, DNR-DHPA  
Holly Tate, DNR-DHPA

EMC to Indiana Historic Preservation Review Board Members:

J. Scott Keller, Review Board  
Daniel Kloc, AIA, Review Board  
Jason Larrison, AIA, Review Board  
Chandler Lighty, Review Board  
Beth K. McCord, DNR-DHPA, Review Board  
Ryan Mueller, Deputy Director, DNR, and Chairman, Review Board  
Anne Shaw, Review Board  
April Sievert, Ph.D., Review Board

EMC to potentially interested persons:

Eastern Shawnee Tribe of Oklahoma  
Miami Tribe of Oklahoma  
Peoria Tribe of Indians of Oklahoma  
Pokagon Band of Potawatomi Indians  
United Keetoowah Band of Cherokee Indians  
Delaware Tribe of Indians

Dearborn County Commissioners  
Dearborn County Highway Superintendent  
Ohio-Kentucky-Indiana Regional Council of Governments  
Indiana Landmarks Southeast Field Office  
Dearborn County Historian  
Dearborn County Historical Society  
Paul Brandenburg, Indiana Historic Spans Task Force  
Tony Dillon, Historic Hoosier Bridges  
Kitty Henderson, Historic Bridge Foundation  
Nathan Holth, historicbridges.org

CC to potentially interested persons:

Aurora Historic Preservation Commission  
Dearborn County Trust for Historic Preservation

## **SR 46 over Whitewater River (Historic Bridge Project)**

### **800.11 (e) Documentation and Effects Finding**

**Logan and Harrison Townships, Dearborn County, Indiana  
Des. No. 1383721; DHPA No. 26124**

February 2022



Prepared for:  
Beam, Longest and Neff, LLC  
8320 Craig St.  
Indianapolis, IN 46250

By:

Karen Wood  
Environmental Services Program Manager / Qualified Professional  
SJCA Inc.  
9102 N. Meridian St., Suite 200  
Indianapolis, IN 46260

p.317.566.0629

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kwood@sjcainc.com



**FEDERAL HIGHWAY ADMINISTRATION'S  
SECTION 4(F) COMPLIANCE REQUIREMENTS (for historic properties) AND  
SECTION 106 FINDINGS AND DETERMINATIONS  
AREA OF POTENTIAL EFFECT  
ELIGIBILITY DETERMINATIONS  
EFFECT FINDING**

**State Road (SR) 46 over the Whitewater River, Bridge No. 046-15-01987A (NBI No. 017540)  
DES. NO.: 1383721; DHPA No. 26124**

**AREA OF POTENTIAL EFFECTS  
(Pursuant to 36 CFR Section 800.4(a)(1))**

The area of potential effects (APE) of the project includes all properties adjacent to the project and with a proximate viewshed of the project. The dimensions of the APE were defined by lines of trees along either side of SR 46, open agricultural fields on the west end of the project limits, and elevation changes on the east end of the project limits. The archaeological APE consists of all proposed new, temporary, or existing right-of-way as well as any additional areas investigated beyond it. See Appendix A for a map of the APE.

**ELIGIBILITY DETERMINATIONS  
(Pursuant to 36 CFR 800.4(c)(2))**

There are two resources eligible for listing in the National Register of Historic Places (NRHP):

**Bridge No. 046-15-01987A (NBI No. 017540; IHSSI No. 029-263-00017)** – is a five-span Parker through Truss bridge with each span measuring 175 feet long. The steel truss bridge was built by I. E. Smith Construction in 1937. At 876.33 ft. long, the bridge is the longest example of the Indiana State Highway Commission's (ISHC) standard design for Parker through Truss bridges from the mid-1930s. The bridge is eligible under Criterion C because it represents a variation that is conveyed through important innovations related to bridge design, and the bridge represents a significant phase of the work of a master. Further, it is distinguishable when compared with similar structures and retains the historic integrity necessary to convey its engineering significance. The bridge is classified as "Non-Select."

**Farm (IHSSI No. 029-103-05008)** – is a Federal-style five-bay brick I-house of a significant vernacular type built in 1838. It sits on 60.2 acres of land at the intersection of Barber Road and SR 46 within a rural setting and possesses elongated rooms with wider window bays than the standard I-house and has a Craftsman-era porch constructed in 1910. Four outbuildings are located on the farmstead, built within the period of significance (c. 1840s – 1970). It is recommended eligible for the NRHP under Criterion A because of the property's local significance in early agriculture and evolution in Dearborn County and under Criterion C for architecture.

No other properties within the APE are listed in or recommended eligible for listing in the NRHP.

**EFFECT FINDING**  
**(Pursuant to 36 CFR 800.4(d)(1))**

**Farm (IHSSI No. 029-103-05008)** – The undertaking will have “No Adverse Effect” on the Farm (IHSSI No. 029-103-05008).

**Bridge No. 046-15-01987A (NBI No. 017540; IHSSI No. 029-263-00017):** Per the terms of the “Programmatic Agreement Regarding Management and Preservation of Indiana’s Historic Bridges” (Historic Bridges PA), the Federal Highway Administration—Indiana Division (FHWA) will satisfy its Section 106 responsibilities involving “Select” and “Non-Select” bridges through the Project Development Process (PDP) of the Historic Bridges PA (Stipulation III). Bridge No. 046-15-01987A (NBI No. 017540) has been classified as a Non-Select Bridge by the INDOT Historic Bridge Inventory and, thus, the procedures outlined in Stipulation III.B of the Historic Bridges PA will be followed to fulfill FHWA’s Section 106 responsibilities for the bridge.

Therefore, the finding for this project only applies to other resources located within the APE and not Bridge No. 046-15-01987A (NBI No. 017540). This document will satisfy the Section 106 responsibilities for other resources located in the APE. Regarding other resources located in the project area, INDOT, acting on FHWA’s behalf, has determined a "No Adverse Effect" finding is appropriate for this undertaking.

INDOT respectfully requests the Indiana State Historic Preservation Officer provide written concurrence with the Section 106 determination of effect.

**SECTION 4(F) COMPLIANCE REQUIREMENTS (for historic properties)**

**Farm (IHSSI No. 029-103-05008)** – This undertaking will not convert property from Farm (IHSSI No. 029-103-05008), a Section 4(f) historic property, to a transportation use; INDOT, acting on FHWA’s behalf, has determined the appropriate Section 106 finding is "No Adverse Effect"; therefore, no Section 4(f) evaluation is required for Farm (IHSSI No. 029-103-05008).

**Bridge No. 046-15-01987A (NBI No. 017540)** – This resource is used for transportation purposes and no Section 4(f) conversion will take place with this project; therefore, no Section 4(f) evaluation must be completed for Bridge No. 046-15-01987A).

*Anuradha V. Kumar*

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Anuradha V. Kumar, for FHWA  
Manager  
INDOT Cultural Resources

02/16/2022

---

Approved Date

**FEDERAL HIGHWAY ADMINISTRATION  
DOCUMENTATION OF SECTION 106 FINDING OF  
NO ADVERSE EFFECT  
SUBMITTED TO THE STATE HISTORIC PRESERVATION OFFICER  
PURSUANT TO 36 CFR Section 800.5(c)**

**State Road (SR) 46 over the Whitewater River Bridge No. 046-15-01987A (NBI No. 017540)  
DES. NO.: 1383721; DHPA No. 26124**

## **1. DESCRIPTION OF THE UNDERTAKING**

The Indiana Department of Transportation (INDOT), with funding from the Federal Highway Administration (FHWA), plans to proceed with the SR 46 over the Whitewater River Bridge No. 046-15-01987A (NBI No. 017540) Project, Des. No. 1383721. The project is located on State Road (SR) 46 over the Whitewater River, located 0.44 mile west of US 52 in Dearborn County, Indiana. Specifically, the project is within Logan and Harrison Townships, Harrison USGS Topographic Quadrangle; in Sections 9 and 10, Township 7 North, Range 1 West.

The need of this project is to address current structural deficiencies, including deck and superstructure condition. There is heavy deterioration and distress exhibited by some of the truss verticals, diagonals, low chords, stringers, floor beams, and deck. The bridge was load rated with the noted existing deterioration. The analysis found that the verticals and stringers controlled the load rating with design and legal load rating factors less than 1.0. The purpose of this project is to improve the overall condition of the bridge's superstructure and the structural capacity. More details about the purpose and need of this project are contained in the Historic Bridge Alternatives Analysis (HBA).

Bridge No. 046-15-01987A is a five-span steel truss structure constructed in 1937. The existing structure has five equal 175'-0" spans. The existing clear roadway over the bridge is 24'-0" and consists of two 10'-0" lanes with 2'-0" shoulders. The bridge was rehabilitated in 1985 and painted in 1996. It was determined eligible for listing in the National Register of Historic Places (NRHP) and classified as "Non-Select" in the INDOT-sponsored *Historic Bridge Inventory* (HBI).

The proposed work to the bridge is as follows: The concrete deck, deck drains, and metal forms in all spans shall be replaced. The existing steel bridge posts, concrete bridge railing transitions, and approach guardrail will be replaced in kind where possible or updated to meet current crash safety standards. Deteriorated truss members, connection plates, lattice bars, lower lateral bracing, steel stringers, and floor beams will be repaired or replaced in kind where necessary to maintain the architecture and structure of the bridge. Rivets which need to be moved will be replaced with round-headed bolts and the expansion bearings will be returned to their original positions. Deteriorated anchor bolts, nuts, and bearings will be replaced in kind. All parts of the truss will be cleaned and repainted in the original color. Deteriorated portions of the concrete caps will be removed and replaced in kind, spalled areas of the abutments and piers will be patched, and riprap scour countermeasures will be placed around the substructure units.

The area of potential effects (APE), as defined in 36 CFR 800.16(d), is "the geographic area or areas within which an undertaking may directly or indirectly cause alterations in the character or use of historic properties, if any such properties exist." The APE of the project includes all properties within or adjacent to the project and those with a proximate viewshed of the project. The APE consists primarily of residential and agricultural property. The dimensions of the APE were defined by lines of trees along either side of SR 46, open agricultural fields on the west end of the project limits, and elevation changes on the east end of

the project limits. From the center of the project, the APE extends east 0.22 mile, south 0.05 mile, west 0.25 mile, and north 0.09 mile. The archaeological APE consists of all proposed new, temporary, or existing right-of-way as well as any additional areas investigated beyond it. See map of APE in Appendix A.

## **2. EFFORTS TO IDENTIFY HISTORIC PROPERTIES**

The NRHP, Indiana Register of Historic Sites and Structures (State Register), the *Dearborn County Interim Report* (1983), the State Historic Architectural and Archaeological Research Databases (SHAARD) and the Indiana Historic Buildings, Bridges and Cemeteries Map (IHBBC) were consulted. The APE contains no resources listed in the NRHP. The INDOT-sponsored *Indiana Historic Bridge Inventory* (February 2009) by M&H Architecture, Inc. was also reviewed. Bridge No. 046-15-01987A (NBI No. 017540) is listed as “eligible” and classified as “Non-Select.” Research has indicated that no previous Section 106 review investigations have occurred within the project APE. There are no Historic American Buildings Survey (HABS), Historic American Engineering Record (HAER), or Historic American Landscapes Survey (HALS) resources identified within the vicinity of the project.

Karen Wood, a qualified professional historian who meets the Secretary of the Interior Standards, conducted a site visit of the project area on July 13, 2020. The historian walked and drove the project area and the APE documenting above-ground resources. She investigated the APE for the existence of any historic properties, structures, objects, or districts listed in or eligible for listing in the NRHP. All resources that will be at least 50 years of age at the anticipated time of the project letting were surveyed and photographic documentation of “Contributing” resources and representative “Non-Contributing” resources was prepared. Aside from Bridge No. 046-15-01987A (NBI No. 017540), no resources in the APE were found to be listed in or eligible for the NRHP.

The following parties/agencies were invited to become consulting parties (CPs) to this project and were sent early coordination information dated July 23, 2020. The State Historic Preservation Officer (SHPO) is an automatic consulting party; that office and others that accepted consulting party status are shown in boldface type below. All consulting party correspondence is located in Appendix C.

### **Indiana State Historic Preservation Officer**

Indiana Landmarks - Southeast Field Office

Indiana Historic Spans Task Force

Historic Bridge Foundation

Historic Hoosier Bridges

### **Historicbridges.org**

Dearborn County Historical Society

Aurora Historic Preservation Commission

Dearborn County Trust for Historic Preservation

Ohio-Kentucky-Indiana Regional Council of Governments

Dearborn County Commissioners

Dearborn County Highway Supervisor

Dearborn County Historian

Dr. James Cooper

### **Eastern Shawnee Tribe of Oklahoma**

### **Miami Tribe of Oklahoma**

### **Peoria Tribe of Indians of Oklahoma**

Pokagon Band of Potawatomi Indians

United Keetoowah Band of Cherokee Indians

Delaware Tribe of Indians

In a letter dated August 10, 2020, SHPO staff stated that “For the purposes of Indiana Code 14-21-1-18 and 312 IAC 20-4, we have added the members of the Review Board and additional, potentially interested parties to the list of parties we intend to copy with our comment letters.”

In an email dated August 11, 2020, Nathan Holth of Historicbridges.org accepted consulting party status.

In a letter dated August 18, 2020, the Miami Tribe of Oklahoma accepted consulting party status.

A Historic Properties Report (HPR) (Wood, October 2021) was completed for this project. The report recommended Bridge No. 046-15-01987A (NBI No. 017540) to remain eligible for listing in the NRHP. No other resources were recommended eligible. The HPR and the 30% plans for the rehabilitation of the bridge were sent to consulting parties on October 27, 2021. The summary of the HPR is found in Appendix D.

With regard to archaeology, Christopher Jackson, M.S., RPA, reviewed the proposed project area and ascertained that the proposed rehabilitation of the State Road 46 Bridge over the Whitewater River (Bridge No. 046-15-01987A [NBI No. 017540]) in Harrison Township, Dearborn County, Indiana will not likely affect archaeological resources based on the project setting. All proposed work will occur in the existing right-of-way. The proposed work will not likely affect archaeological resources because the existing bridge right-of-way is disturbed, with the disturbance consisting of roadside ditches, man-made/landscaped slopes, and utility easements. The proposed access road is disturbed, for it will be utilizing an existing gravel farm road.

A review of SHAARD, which occurred on July 21, 2021, indicated that seven professional investigations have either occurred in or adjacent to the project area. Five sites (12-D-003, 12-D-126, 12-D-4724, 12-D-425, and 12-D-717) have been inventoried near the project area; none of these sites will be directly impacted by the proposed project. Based on the project setting, which is disturbed, there are no archaeological concerns, and no further work is recommended. However, state law (Indiana Code 14-21-1-27 and -29) requires that if any prehistoric or historic archaeological artifacts or human remains are uncovered during construction, demolition, or earth moving activities, the discovery must be reported to the Department of Natural Resources within two (2) business days.

It should be noted that the original archaeological assessment indicated the archaeological reconnaissance conducted by CRA, Inc. in 2019 occurred “east of the eastern terminus of the current investigation.” A review of the records on file at the DHPA indicated the assessment actually occurred west of the current investigation.

In a letter dated November 2, 2021, The Peoria Tribe of Indians of Oklahoma accepted consulting party status.

In a letter dated November 18, 2021, the SHPO staff stated the APE “proposed in the HPR appears to be of adequate size to encompass the geographic area in which direct and indirect effects of a project of this nature could occur.” They further stated that “We respectfully disagree with the conclusions presented in the HPR. While we agree that the subject bridge carrying SR 46 over the Whitewater River (Bridge No. 046-15-01987A) was previously determined eligible for inclusion in the National Register of Historic Places (“NRHP”), we disagree with the evaluation of the Notable-rated farm (Indiana Historic Sites and Structures Inventory #029-103-05008) located within the project’s APE.” SHPO staff stated IHSSI #029-103-05008 is an “intact example of a significant vernacular type,” in this case a modified five-bay brick I-house with a c.1910 Craftsman porch and outbuildings from c.1840s – c.1970s. They recommend it eligible for the National Register under Criterion A for agriculture and Criterion C for architecture. With

regard to the archaeological resources, SHPO concurred with the results and recommendations of the Archaeological Assessment and agreed that an archaeological reconnaissance of the project area was not necessary.

In a letter dated December 8, 2021, the Eastern Shawnee Tribe accepted consulting party status and stated, “we find our people occupied these areas historically and/or prehistorically. However, the project proposes NO Adverse Effect or endangerment to known sites of interest.”

None of the other consulting parties provided any additional comments regarding the early coordination letter or related to the identification of historic properties within the APE. Please see Appendix C for Consulting Party Correspondence.

### **3. DESCRIBE AFFECTED HISTORIC PROPERTIES**

**Farm (IHSSI No. 029-103-05008)** is recommended eligible for listing in the NRHP under Criterion A because of the property’s local significance in early agriculture and evolution in Dearborn County and under Criterion C for architecture. The resource is a Federal-style five-bay brick I-house of a significant vernacular type built in 1838. It sits on 60.2 acres of land at the intersection of Barber Road and SR 46 within a rural setting and possesses elongated rooms with wider window bays than the standard I-house. It also has a Craftsman-era porch constructed in 1910. Four outbuildings are located on the farmstead, built within the period of significance (c. 1840s – 1970).

**Bridge No. 046-15-01987A (NBI No. 017540; IHSSI No. 029-263-00017)** – is a five-span Parker through Truss bridge with each span measuring 175 feet long. The steel truss bridge was built by I. E. Smith Construction in 1937. At 876.33 ft. long, the bridge is the longest example of the Indiana State Highway Commission’s (ISHC) standard design for Parker through Truss bridges from the mid-1930s. The bridge is eligible under Criterion C because it represents a variation that is conveyed through important innovations related to bridge design, and the bridge represents a significant phase of the work of a master. Further, it is distinguishable when compared with similar structures and retains the historic integrity necessary to convey its engineering significance. The bridge is classified as “Non-Select.”

### **4. DESCRIBE THE UNDERTAKING'S EFFECTS ON HISTORIC PROPERTIES**

**Farm (IHSSI No. 029-103-05008), RR3 Box 319** – The project will not acquire right-of-the-way from the property. The project area is 0.05 mile (264 ft.) from the resource, and the resource has a limited viewshed of the project area because it is obscured by the tree line. Within the viewshed of this property, the bridge’s physical appearance will change only slightly as a result of the rehabilitation work that will occur. The work to the bridge should not be highly noticeable from the Farm and should have little impact on the farm’s visual setting.

**Bridge No. 046-15-01987A (NBI No. 017540; IHSSI No. 029-263-00017)** – The bridge will be rehabilitated and Stipulation III.B of the Historic Bridges Programmatic Agreement will be followed to fulfill FHWA’s Section 106 responsibilities for the bridge.

### **5. EXPLAIN APPLICATION OF CRITERIA OF ADVERSE EFFECT -- INCLUDE CONDITIONS OR FUTURE ACTIONS TO AVOID, MINIMIZE OR MITIGATE ADVERSE EFFECTS**

According to 36 CFR 800.5(a)(1), “An adverse effect is found when an undertaking may alter, directly or indirectly, any of the characteristics of a historic property that qualify the property for inclusion in the

National Register in a manner that would diminish the integrity of the property's location, design, setting, materials, workmanship, feeling or association.”

**Farm (IHSSI No. 029-103-05008)** – The project will have “No Adverse Effect” on the resource.

Per 800.5(a)(2)(i), “Physical destruction of or damage to all or part of the property” will not occur.

Per 36 CFR 800.5(a)2(ii), “Alteration of a property, including restoration, rehabilitation, repair, maintenance, stabilization, hazardous material remediation and provision of handicapped access, that is not consistent with the Secretary’s Standards for the Treatment of Historic Properties and/or other applicable guidelines” will not occur.

Per 36 CFR 800.5(a)2(iii), “Removal of the property from its historic location” will not occur.

Per 36 CFR 800.5(a)2(iv), “Change of the character of the property’s use or of physical features within the property’s setting that contribute to its historic significance” will not occur. The bridge will remain in place and will undergo rehabilitation. The proposed rehabilitation work will not detract from the character of the Farm (IHSSI No. 029-103-05008), which is 0.05 mile (264 ft.) from the resource with a limited viewshed of the bridge.

Per 36 CFR 800.5(a)2(v), the “Introduction of visual, atmospheric or audible elements that diminish the integrity of the property’s significant historic features” will not occur. The bridge will remain in place with parts replaced in kind and as much of the original structure preserved as possible. There will not be the introduction of any elements that will diminish the integrity of the property’s significant historic features.

Per 36 CFR 800.5(a)2(vi), “Neglect of a property which causes its deterioration...” will not occur as a result of this project. The undertaking will not cause neglect of the historic property.

Per 36 CFR 800.5(a)2(vii), the “Transfer, lease, or sale of property out of Federal ownership or control...” will not occur. Ownership of the historic property will not change as a result of this project.

**Bridge No. 046-15-01987A (NBI No. 017540; IHSSI No. 029-263-00017)** – The procedures outlined in Stipulation III.B of the Historic Bridge Programmatic Agreement will be followed to fulfill FHWA’s Section 106 responsibilities for the bridge and any effects to the bridge are resolved through the Historic Bridges PA PDP. Therefore, the finding for this project only applies to other resources located within the APE and not Bridge No. 046-15-01987A.

In terms of minimization and mitigation, only the deteriorated and substandard portions of the bridge will be replaced. The members and connections that are replaced will be replaced in kind except for the rivets, which will be removed and replaced with round-headed bolts. An in-kind replacement refers to replacing a bridge element with the same material and same size or dimensions. The entire bridge will be rehabilitated to ensure its long-term preservation.

Per Attachment B (Standard Treatment Approach for Historic Bridges) of the Historic Bridges PA, INDOT will provide rehabilitation plans to the Indiana SHPO when the design is approximately 30% complete, 60% complete, and when final design plans are complete. The 30% plan submittal has already occurred, and the 60% plans are provided in Appendix F.

The only potential mitigation is photo documentation, per Attachment B of the Historic Bridges PA. The IN-SHPO requested that photographs be taken by a qualified professional historian,

architectural historian, or architect in a letter dated February 15, 2021. A qualified professional will complete the photo documentation before construction activities begin.

## **6. SUMMARY OF CONSULTING PARTIES AND PUBLIC VIEWS**

On July 23, 2020, an early coordination letter under Des. No. 1383721 was distributed to consulting parties.

On August 10, 2020, SHPO staff stated that “For the purposes of Indiana Code 14-21-1-18 and 312 IAC 20-4, we have added the members of the Review Board and additional, potentially interested parties to the list of parties we intend to copy with our comment letters.”

On August 11, 2020, Nathan Holth of Historicbridges.org accepted consulting party status.

On August 18, 2020, the Miami Tribe of Oklahoma accepted consulting party status.\

Per Stipulation III.A.3, a Purpose and Need statement and HBAA were prepared and distributed to consulting parties on January 21, 2021. Per the terms of the “Programmatic Agreement Regarding Management and Preservation of Indiana’s Historic Bridges” (Historic Bridges PA), the FHWA-Indiana Division will satisfy its Section 106 responsibilities involving “Select” and “Non-Select” bridges through the Project Development Process (PDP) of the Historic Bridges PA (Stipulation III). Because Bridge No. 046-15-01987A (NBI 017540) is a “Non-Select” bridge, the procedures outlined in Stipulation III.B. of the Historic Bridges PA will be followed to fulfill FHWA’s Section 106 responsibilities for the project. (A copy of the Historic Bridges PA can be downloaded here: <http://www.in.gov/indot/2530.html>). The summary of the HBAA is located in Appendix E.

In a letter dated February 15, 2021, the SHPO staff stated: “We agree with the conclusions of the HBAA that Alternative B1: Rehabilitation of the Existing Bridge for Continued Vehicle Use (Two-Way Option) Meeting Secretary of Interior’s Standards for Rehabilitation is both a feasible and prudent alternative and is, therefore, the preferred alternative for this project.” Also, SHPO staff requested that this bridge be documented according to the “Indiana DNR – Division of Historic Preservation and Archaeology Minimum Architectural Documentation Standards,” as noted in the Mitigation section of the HBAA. Photographic documentation will be completed as requested by SHPO.

On October 27, 2021, the HPR was distributed to invited consulting parties. With regards to archaeology, Christopher Jackson, M.S., RPA, reviewed the proposed project area and ascertained that the proposed rehabilitation of the State Road 46 Bridge over the Whitewater River (Bridge No. 046-15-01987A [NBI No. 17540]) in Harrison Township, Dearborn County, Indiana will not likely affect archaeological resources based on the project setting. The summary of the HPR is found in Appendix D.

On November 2, 2021, the Peoria Tribe of Indians of Oklahoma accepted consulting party status.

On November 18, 2021, the Indiana State Historic Preservation Officer (SHPO) staff stated the APE “proposed in the HPR appears to be of adequate size to encompass the geographic area in which direct and indirect effects of a project of this nature could occur.” They further stated that “We respectfully disagree with the conclusions presented in the HPR. While we agree that the subject bridge carrying SR 46 over the Whitewater River (Bridge No. 046-15-01987A) was previously determined eligible for inclusion in the National Register of Historic Places (“NRHP”), we disagree with the evaluation of the Notable-rated farm (Indiana Historic Sites and Structures Inventory #029-103-05008) located within the project’s APE.” SHPO again requested photo documentation of the bridge. SHPO staff stated they were

“satisfied with both the written description and the plan sheets” of the 30% plans and had no more comments.

With regard to archaeological resources, SHPO concurred with the results and recommendations of the Archaeological Assessment and agreed that an archaeological reconnaissance of the project area was not necessary.

On December 3, 2021, the Peoria Tribe of Indians of Oklahoma responded to the SHPO letter dated November 18, 2021, stating that they have “no objection at this time to the proposed project.”

In a letter dated December 8, 2021, the Eastern Shawnee Tribe accepted consulting party status and stated, “we find our people occupied these areas historically and/or prehistorically. However, the project proposes NO Adverse Effect or endangerment to known sites of interest.”

The procedures outlined in the Historic Bridge PA will be followed to fulfill FHWA’s Section 106 responsibilities for the bridge and any effects to the bridge. Therefore, the finding for this project only applies to other resources located within the APE. Regarding other resources located in the project area, INDOT, acting on FHWA’s behalf, has determined a “No Adverse Effect” finding is appropriate for this undertaking.

The finding will be advertised as a legal notice in a local paper, the *Harrison Review* (West Harrison, IN), and the public will be given a 30-day period in which to comment on the finding of effects. This documentation will be revised to reflect any substantive comments received. Per Stipulation III.B.5 of the Historic Bridge PA, INDOT will hold a public hearing for the project prior to completion of the National Environmental Policy Act (NEPA) studies. All originally invited consulting parties will be notified of the public hearing.

No other consulting party comments were received. Copies of all consulting party correspondence can be found in Appendix C.

## **APPENDIX Appendices not included to reduce redundancy**

- A – Maps
- B – Photographs
- C – Consulting Party Correspondence
- D – Historic Property Report Summary
- E – HBAA Summary
- F- 60% Plans

ATTACH COPY OF ADVERTISEMENT HERE

*SJCA*  
 (Governmental Unit)  
Public Notice - Bridge Project  
 Dearborn County, Indiana

To: Journal Press  
 126 West High St., Lawrenceburg, IN 47025  
 Fed I.D. #35-1889520  
 Acct. #15002

## PUBLISHER'S CLAIM

## LINE COUNT

Display Master (Must not exceed two actual lines, neither of which shall total more than four solid lines of the type in which the body of the advertisement is set) - number of equivalent lines

Head - number of lines

Body - number of lines

Tail - number of lines

Total number of lines in notice

77

77

## COMPUTATION OF CHARGES

77 lines, ..... columns wide equals ..... equivalent lines at .7568  
 cents per line

Additional charges for notices containing rule or tabular work (50 per cent of above amount)

Charge for extra proofs of publication (\$1.00 for each proof in excess of two)

**TOTAL AMOUNT OF CLAIM**

58.27

58.27

## DATA FOR COMPUTING COST

Width of single column in picas.....15.5.....

Size of type...7.5...point

Number of insertions.....1.....

Pursuant to the provisions and penalties of IC 5-11-10-1, I hereby certify that the foregoing account is just and correct, that the amount claimed is legally due, after allowing all just credits, and that no part of the same has been paid.

I also certify that the printed matter attached hereto is a true copy, of the same column width and type size, which was duly published in said paper .....1..... times. The dates of publication being as follows:

3-8-22

Additionally, the statement checked below is true and correct:

..... Newspaper does not have a Web site.

...X..Newspaper has a Web site and this public notice was posted on the same day as it was published in the newspaper.

..... Newspaper has a Web site, but due to technical problem or error, public notice was posted on .....

..... Newspaper has a Web site but refuses to post the public notice.

Date ..... 3-8-22.....

*Chip Munich*  
*Chip Munich*  
 Title: Public Notices Administrator

## 15.5 PICA COLUMN

TYPE SIZE

75

See table of legal rates in the [supplementary section](#) of [Government Buildings](#).

Claim No. _____	Warrant No. _____
IN FAVOR OF _____	
S _____	
ON ACCOUNT OF APPROPRIATION FOR _____	
Appropriation No. _____	
ALLOWED _____	
IN THE SUM OF \$ _____	

I have examined the within claim and hereby certify as follows:

### That it Is in Proper Form

That it is duly authenticated as required by law.

That it is based upon statutory authority.

That it is apparently correct

I certify that the within claim is true and correct; that the services herein itemized and for which charge is made were ordered by me and were necessary to the public business.

By law and were necessary to the public business  
City Marshal  
Public Notices Admin.

3-8-22

#### NUMBER OF INSERTIONS

1

0.7568

1,1317

1

15106

1 9985





March 2, 2022

Karen Wood  
Environmental Services Program Manager  
SJCA, Inc.  
9102 N. Meridian Street, Suite 200  
Indianapolis, Indiana 46260

State Agency: Indiana Department of Transportation (“INDOT”),  
Federal Agency: Federal Highway Administration, Indiana Division (“FHWA”)

Re: DUAL REVIEW: Indiana Department of Transportation’s finding of “No Adverse Effect” on behalf of the Federal Highway Administration and 60% design plans for the SR 46 over the Whitewater River Bridge (Bridge No. 046-15-01987A) project (Des. No. 1383721; DHPA No. 26124)

Dear Ms. Wood:

Pursuant to Section 106 of the National Historic Preservation Act of 1966, as amended (54 U.S.C. § 306108); implementing regulations at 36 C.F.R. Part 800; the “Programmatic Agreement Among the Federal Highway Administration, the Indiana Department of Transportation, the Indiana Historic Preservation Officer, and the Advisory Council on Historic Preservation Regarding Management and Preservation of Indiana’s Historic Bridges” (“Indiana Historic Bridges PA”); and the “Programmatic Agreement (PA) Among the Federal Highway Administration, the Indiana Department of Transportation, the Advisory Council on Historic Preservation and the Indiana State Historic Preservation Officer Regarding that Implementation of the Federal Aid Highway Program In the State of Indiana” (“Indiana Minor Projects PA”); and also pursuant to Indiana Code 14-21-1-18 and 312 Indiana Administrative Code (“IAC”) 20-4, the staff of the Indiana State Historic Preservation Officer (“Indiana SHPO”) has reviewed your February 18, 2022, submission which enclosed INDOT’s finding and supporting documentation for the aforementioned project in Logan and Harrison townships, Dearborn County, Indiana.

For the benefit of the Indiana Historic Preservation Review Board (“Review Board”) members and other recipients of this letter who are not Section 106 consulting parties, please be aware that documents submitted for review of this project can be found online at IN SCOPE (<http://www.ermis.indot.in.gov/Section106Documents/>). From there, search by this project’s designation number: 1383721.

As previously indicated, the only historic properties eligible for inclusion in the National Register of Historic Places (“NRHP”) located within the project’s area of potential effects are the subject bridge carrying SR 46 over the Whitewater River (Bridge No. 046-15-01987A) and the Notable-rated farm (Indiana Historic Sites & Structures Inventory #029-103-05008). Bridge No. 046-15-01987A is a five-span Parker through Truss constructed c. 1937 and categorized as a “Non-Select” Bridge in the *Indiana Historic Bridges Inventory*. Accordingly, the procedures outlined in Stipulation III.B of the Historic Bridges PA will be followed to fulfill FHWA’s Section 106 responsibilities for this project.

Also as previously indicated, in terms of archaeology, no currently known archaeological resources eligible for inclusion in the NRHP have been recorded within the proposed project area. We concur with the recommendation that no archaeological reconnaissance is necessary for the currently proposed project. We note that the western extension of the project area overlaps with the boundary of archaeological site 12-D-0377. This site was investigated in 1994 and 1995 (Parish 1994, Parish and McCord 1995) and those portions within the current project boundary were determined to be ineligible for inclusion in the NRHP. Parish and McCord (1995:B-22, 23) recommended that further work may be needed for portions of the site north of SR 46. As currently presented, it does not appear that the current project plans will impact this portion of the site.

If any prehistoric or historic archaeological artifacts or human remains are uncovered during construction, demolition, or earthmoving activities, state law (Indiana Code 14-21-1-27 and Indiana Code 14-21-1-29) requires that the discovery be reported to the Indiana Department of Natural Resources, Division of Historic Preservation and Archaeology within two (2) business days. In that event, please call (317) 232-1646. Be advised that adherence to Indiana Code 14-21-1-27 and Indiana Code 14-21-1-29 does not obviate the need to adhere to applicable federal statutes and regulations, including but not limited to 36 C.F.R. Part 800.

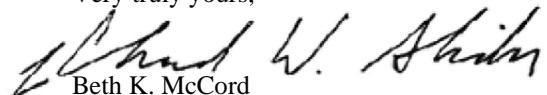
Because the Indiana Historic Bridges PA takes into account the effects of projects on all historic, Select and Non-Select bridges in Indiana, a Section 106 finding in a bridge project applies only to historic properties within the APE, *other than the bridge*. **Accordingly, we concur with INDOT's February 16, 2022, Section 106 finding of "No Adverse Effect" on behalf of FHWA, for this federal undertaking.**

Thank you for providing the 60% design plans. At this time, we have no comments or questions regarding this set of plans for the proposed bridge rehabilitation. We look forward to receiving the 90% final bridge plans for this rehabilitation and photographic documentation, after which we will decide whether it is appropriate to issue a Director's Letter of Clearance for this project, indicating compliance with Indiana Code 14-21-1-18.

If you have questions regarding our dual review of the aforementioned project, please contact DNR-DHPA. Questions about archaeological issues should be directed to contact Rachel Sharkey at (317) 234-5254 or rsharkey@dnr.in.gov. Questions about historic buildings or structures pertaining to this review should be directed to Danielle Kauffmann at (317) 232-0582 or dkauffmann@dnr.IN.gov.

In all future correspondence regarding the dual review of this bridge rehabilitation project on SR 46 over the Whitewater River in Dearborn County (Des. No. 1383721), please refer to DHPA No. 26124.

Very truly yours,

  
Beth K. McCord  
Deputy State Historic Preservation Officer  
Director, Division of Historic Preservation and Archaeology

BKM:DMK:dmk

EMC to federal and state agency or consultant staff members:

Erica Tait, FHWA  
Anuradha Kumar, INDOT  
Mary Kennedy, INDOT  
Matt Coon, INDOT  
Susan Branigin, INDOT  
Karen Wood, SJCA, Inc.  
Danielle Kauffmann, DNR-DHPA  
Rachel Sharkey, DNR-DHPA

EMC to Indiana Historic Preservation Review Board Members:

J. Scott Keller, Review Board  
Daniel Kloc, AIA, Review Board  
Jason Larrison, AIA, Review Board  
Chandler Lighty, Review Board  
Beth K. McCord, DNR-DHPA, Review Board  
Ryan Mueller, Deputy Director, DNR, and Chairman, Review Board  
Anne Shaw, Review Board  
April Sievert, Ph.D., Review Board

EMC to potentially interested persons:

Eastern Shawnee Tribe of Oklahoma  
Miami Tribe of Oklahoma  
Peoria Tribe of Indians of Oklahoma  
Pokagon Band of Potawatomi Indians  
United Keetoowah Band of Cherokee Indians  
Delaware Tribe of Indians

Dearborn County Commissioners  
Dearborn County Highway Superintendent

Karen Wood  
March 2, 2022  
Page 3

Ohio-Kentucky-Indiana Regional Council of Governments  
Indiana Landmarks Southeast Field Office  
Dearborn County Historian  
Dearborn County Historical Society  
Paul Brandenburg, Indiana Historic Spans Task Force  
Tony Dillon, Historic Hoosier Bridges  
Kitty Henderson, Historic Bridge Foundation  
Nathan Holth, historicbridges.org

CC to potentially interested persons:  
Aurora Historic Preservation Commission  
Dearborn County Trust for Historic Preservation



**EASTERN SHAWNEE  
CULTURAL PRESERVATION DEPARTMENT**  
70500 East 128 Road, Wyandotte, OK 74370

March 1, 2022

INDOT - Indiana Department of Transportation  
100 N. Senate Ave. IGCN642  
Indianapolis, IN 46201

**RE: Des. No. 1383721, Dearbon County, Indiana**

Dear Ms. Korzeniewski,

The Eastern Shawnee Tribe has received your letter regarding the above referenced project(s) within Dearbon County, Indiana. The Eastern Shawnee Tribe is committed to protecting sites important to Tribal Heritage, Culture and Religion. Furthermore, the Tribe is particularly concerned with historical sites that may contain but not limited to the burial(s) of human remains and associated funerary objects.

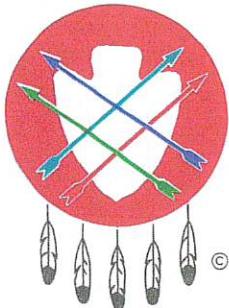
As described in your correspondence, and upon research of our database(s) and files, we find our people occupied these areas historically and/or prehistorically. However, the project proposes **NO Adverse Effect** or endangerment to known sites of interest to the Eastern Shawnee Tribe. Please continue project as planned. However, should this project inadvertently discover an archeological site or object(s) we request that you immediately contact the Eastern Shawnee Tribe, as well as the appropriate state agencies (within 24 hours). We also ask that all ground disturbing activity stop until the Tribe and State agencies are consulted. Please note that any future changes to this project will require additional consultation.

In accordance with the NHPA of 1966 (16 U.S.C. § 470-470w-6), federally funded, licensed, or permitted undertakings that are subject to the Section 106 review process must determine effects to significant historic properties. As clarified in Section 101(d)(6)(A-B), historic properties may have religious and/or cultural significance to Indian Tribes. Section 106 of NHPA requires Federal agencies to consider the effects of their actions on all significant historic properties (36 CFR Part 800) as does the National Environmental Policy Act of 1969 (43 U.S.C. § 4321-4347 and 40 CFR § 1501.7(a)). This letter evidences NHPA and NEPA historic properties compliance pertaining to consultation with this Tribe regarding the referenced proposed projects.

Thank you, for contacting the Eastern Shawnee Tribe, we appreciate your cooperation. Should you have any further questions or comments please contact our Office.

Sincerely,

Paul Barton, Tribal Historic Preservation Officer (THPO)  
Eastern Shawnee Tribe of Oklahoma  
(918) 666-5151 Ext:1833



## PEORIA TRIBE OF INDIANS OF OKLAHOMA

118 S. Eight Tribes Trail (918) 540-2535 FAX (918) 540-2538

P.O. Box 1527

MIAMI, OKLAHOMA 74355

CHIEF  
Craig Harper

SECOND CHIEF  
Rosanna Dobbs

March 1, 2022

Patricia Korzeniewski, Archaeologist  
Tribal Contact  
INDOT  
100 N Senate Ave., Rm 758-ES  
Indianapolis, IN 46204

Re: Des. #1383721; SR 46; over Whitewater River, Bridge#046-15-01987A

Thank you for providing notice of the referenced project. The Peoria Tribe of Indians of Oklahoma is unaware of a direct link to the newly proposed project location.

The Peoria Tribe of Indians of Oklahoma is also unaware of items covered under Native American Graves Protection and Repatriation Act (NAGPRA) to be associated with the proposed project site, including funerary or sacred objects; objects of cultural patrimony; or ancestral human remains.

The Peoria Tribe has no objection at this time to the proposed project. If, however, at any time items are discovered which fall under the protection of NAGPRA, the Peoria Tribe requests immediate notification and consultation. In addition, state, local and tribal authorities should be advised as to the findings and construction halted until consultation with all concerned parties has occurred.

Please feel free to contact me directly at the number above if additional consultation is necessary. Thank you again for your consideration with this matter.

Sincerely,

Charla K. EchoHawk  
Director of Cultural Preservation



# HISTORIC BRIDGE ALTERNATIVE ANALYSIS REPORT

## SR 46 OVER WHITEWATER RIVER

### PROJECT NO. 1383721

**Location: 0.44 Miles West of US 52 (R.P. 153+43), Dearborn County, Indiana**  
**EXISTING BRIDGE FILE #: 046-15-01987 A; NBI #: 017540 DESIGNATION #: 1383721**



**PREPARED FOR:**

**INDIANA DEPARTMENT OF TRANSPORTATION**

**PREPARED BY: BEAM, LONGEST & NEFF**

**DATE: JUNE 2020**



This bridge was evaluated by personnel from the Indiana Department of Transportation (INDOT) Bridge Design Unit, the District Office, and the designer. The attached Draft Historic Bridge Alternative Analysis has been reviewed by the INDOT Bridge Design Unit and Cultural Resources Office for thoroughness of the rehabilitation option and compliance with INDOT design policies. Concurrence by INDOT with the proposed Scope of Work does not constitute Final Approval of the Historic Bridge Alternatives Analysis (HBAA). This draft HBAA may now be distributed to the historic consulting parties for review.

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**HISTORIC BRIDGE ALTERNATIVE ANALYSIS REPORT**  
**EXISTING BRIDGE NO. 046-15-01987 A (NBI No. 017540)**  
**SR 46 OVER WHITEWATER RIVER**  
**LOGAN AND HARRISON TOWNSHIPS, DEARBORN COUNTY, INDIANA**  
**DES No. 1383721**

**I. EXISTING STRUCTURE DATA**

**A. Identification/History**

**Existing INDOT Bridge No.:** 046-15-01987 A

**Project Location:** 0.44 miles West of US 52 in Sections 9 and 10, T-7-N, R-1-W

**Latitude:** 39°16'48.14" N

**Longitude:** 84°52'26.5" W

**Des. No.:** 1383721

**Year Built:** 1937

**Year Rehabilitated/Repaired:** 1985; 1996

**Most Recent Field Inspection Date:** 07/31/2019 (BLN), 09/23/2019 (INDOT)

**Average Daily Traffic (ADT)/Year of ADT:** 3,705 VPD / 2023 & 4,169 VPD / 2043 (Appendix H)

**Percentage of Commercial Vehicles:** 10% (Appendix H)

**Low Volume Road:** No

**Functional Classification:** State Collector

**Rural/Urban:** Rural

**National Highway System:** No

**National Truck Network:** No

**Detour Length:** 1.5 Miles (US 52 to I-74 to SR 1)

**Load Rating:** 14 Ton and 26 Ton H and HS Inventory, respectively (see current SI&A Report)

**Sufficiency Rating:** 38.7 out of 100

**National Register of Historic Places (NRHP) Status:** Eligible

**Historic Bridge Prioritization Status:** Non-Select

**Historic Character-Defining Features:** This bridge is historically significant as a variation of the much used third-generation Indiana State Highway Commission (ISHC) standard plan and it represents an unusually long example of its type that was built by an important Indiana builder (I.E. Smith). The trusses retain their original members and bush hammered concrete approach rails. The bridge retains historic integrity necessary to convey its engineering significance in Indiana under Criterion C (Parker steel through truss bridge).

**B. Structure/Dimensions**

**MAIN SPANS**

**Surface Type:** 1.75" modified portland cement concrete overlay on a reinforced concrete deck.

**Out to Out of Copings:** 25'-0"

**Out to Out of Bridge Floor:** 886'-4"

**Clear Roadway Width:** 24'-0"

**Number of Lanes on Structure:** Two 10'-0" wide lanes with 2'-0" shoulders

**Skew:** 00 degrees

**Type of Superstructure:** Parker Steel Through Truss Bridge

**Spans:** 5 spans @ 175'-0"

**Type of Substructure/Foundation:** Concrete abutments and concrete wall piers on timber piling

**Seismic Design Category:** Preliminary investigation, Seismic Design Category A

**APPROACH SPANS**

**Surface Type:** N/A

**Out to Out of Copings:** N/A

**Out to Out of Bridge Floor:** N/A

**Clear Roadway Width:** N/A

**Number of Lanes on Structure:** N/A

**Skew:** N/A

**Type of Superstructure:** N/A

**Spans:** N/A

**Type of Substructure/Foundation:** N/A

**Seismic Zone (only if Zone 2):** N/A

### C. Appurtenances

**Bridge Railing:** 3'-2" miscellaneous steel bridge rail

**Curbs:** 6" by 6" concrete curb

**Sidewalks:** N/A

**Utilities:** There are electric and telephone lines attached to outside of north and south trusses at approximately 15' above roadway.

**Railroad:** N/A

### D. Approaches

#### ROADWAY

**Roadway Width:** Two, 12'-0" wide lanes with 1'-0" paved shoulders and 1'-0" earth shoulders (2'-0" total usable shoulder).

**Surface Type:** Bituminous

**Guardrail:** W-Beam (The guardrail tapers from the bridge to approximately 4'-0")

**Guardrail Transition:** Miscellaneous concrete barrier rail transition with bush hammered finish.

**Guardrail End Treatment:** Type 1

## II. EXISTING CONDITIONS

Photos detailing the existing conditions are included in Appendix B

### A. Bridge Deck

**General:** The overall condition rating of the deck is fair (5 out of 9).

**Repair/Maintenance Work:** In 1985 the deck rehabilitation involved repairing a portion of the concrete curbs in Spans A, B, and C and replacing the deck in Spans D and E. The underside of the deck in Spans D and E have metal deck forms that were placed in the 1985 rehabilitation. A concrete overlay was also placed over all spans. In addition, new joints and new concrete approach slabs were constructed.

**Surface Condition:** The condition of the deck surface is unknown due to the presence of the existing overlay. The north concrete curb in Span B has heavy deterioration and spalling with exposed steel reinforcement (see photo 213). In 1985, a 1.75" thick overlay was then placed over the entire length and width of the five spans.

**Overlay:** The overlay is in fair condition with cracking throughout, delaminations, and some concrete spalls. There are two large patches in the westbound lane in Span C (see photos 214 and 215).

**Underside Condition:** There is transverse cracking with efflorescence, delaminations, and concrete spalling with exposed steel reinforcement on the underside of the deck in Spans A, B, and C (see photos 205 to 210). There are a couple areas of the metal forms that have rust stains starting to develop.

**Joints:** The joints are rated in fair condition. There is leaking at all joints (see photo 212)

**Drainage:** Many of the deck drain extensions are damaged or missing (see photo 204). There is typically more deck and superstructure deterioration at each of the drains.

**Bridge Railing:** The original steel bridge rail is connected directly to the truss verticals and bridge rail posts and consists of steel channels and angles riveted together to form the railing (see photo 213). Many bridge rail posts and much of the steel railing have holes due to corrosion.

### B. Superstructure

#### MAIN SPANS

**General:** The overall condition rating of the superstructure is poor (4 out of 9).

**Repair/Maintenance Work:** The bridge was rehabilitated in 1985 and painted in 1996. The area between the truss vertical channels and lower chord gusset plates is not easily accessed and was not painted. The exterior stringers in Span E in the west bay were repaired with a welded plate at an unknown date.

**Deficiencies:** The structure's primary deterioration consists of section loss on the truss members, stringers, and floor beams due to steel corrosion. A few exterior stringers in the floor system have heavy section loss that has caused large holes at the connections to the floor beams (see photos 2 through 27 and 137 through 142). A few floor beams have minor to moderate expansion rust and section loss at each end, particularly near the truss bearings. The section loss in the truss members is primarily located along the low chord and lower portions of the verticals and diagonals. Corrosion has advanced such that holes have formed through some of the truss verticals (see photos 29 through 89), areas of heavy section loss on diagonals at the lower gusset plate are present (see photos 105, 108, 111, 146, 158, and 179), and portions of the lattice bars in the end posts have corroded away (see photos 96, 201, and 202). Moderate section loss has occurred between the steel angles that form the low chord of the trusses (see photos 90 to 93). Deterioration of the gusset plates, stay plates, lower lateral bracing, and lower lateral connection plates is also present in each span (see photos 99 to 104 and 143 to 145).

**Fracture Critical Members:** This structure is fracture critical and has details that have lower fatigue resistance that should be highlighted during inspections. A fracture critical structure has steel members that are in tension and whose failure would probably cause a portion of or the entire bridge to collapse. Fatigue is the initiation and/or propagation of a crack by the repeated variation of normal stress in a tension member. This structure has many fatigue details that are typically within category D due to the connections being riveted. Category D is one of eight categories that signifies fatigue resistance. Category D are details that are a transition between details influenced by discontinuities (A, B, B', C, and C') and those whose performance is dominated by geometry and stress concentrations (D, E, and E'). Some members were noted having tack welds and previous repairs were completed with welds.

**Bearings/Pedestals:** Each span is supported by steel bearings supported by concrete piers or abutments. One end of each span is fixed (does not move) and the other end has expansion bearings that are designed to accommodate expansion by rotating. Portions of the bearings have expansion rust and many of the anchor nuts and angles have moderate to heavy section loss. The expansion bearings for Spans C and D at Pier 4 have moved from their original position and are over rotated (see photos 117 to 120).

**Damage:** All the sway bracing that connect the north and south trusses above traffic was struck by a vehicle on an unknown date (see photo 192 to 194).

## APPROACH SPANS – N/A

### C. Substructure and Foundations

**General:** The existing substructure was rated in satisfactory condition (6 out of 9). The existing substructure consists of concrete abutments and wall piers on timber piles.

**Repair/Maintenance Work:** In 1985 the top 1'-9" of the mudwalls were reconstructed.

**Deficiencies:** Concrete spalling at the south truss bearings on the East Abutment and Pier No. 5 caps that extend from the south edge of the cap up to the base plate of the bearing (see photos 217 to 219). In addition, some areas of the abutments and piers have minor spalling.

**Drainage:** N/A

**Scour:** According to the current bridge inspection report (see Appendix E), this bridge is considered as Low Risk for Vulnerability for Scour and was recorded as stable for scour conditions. A scour analysis will be completed by Beam, Longest, and Neff, LLC to determine the necessity of scour countermeasures.

### D. Approaches

**General:** The road is straight and there is a vertical crest curve across the bridge.

**Approach Pavement:** There are concrete approach slabs at each bridge end with a bituminous overlay. The existing bituminous approach pavement appears to be in good condition.

**Guardrail:** A guardrail transition type TGB and w-beam guardrail are at all four corners of the bridge.

**Drives and Public Roads:** A paved drive for the St. Leon Wastewater Facility is located near the southwest quadrant of the bridge. The intersection of SR 46 and Barber Road is located near the northwest quadrant of the bridge.

**Traffic-control Devices:** The speed limit is posted 55 mph over the bridge.

**Miscellaneous:** Electrical power lines (Duke Energy) and telephone cable (Cincinnati Bell) are attached to each truss of the bridge. The electrical power lines are attached to the north truss, and the telephone cable is attached to the south truss. In addition, high voltage transmission lines run near the west end of the bridge.

### E. Slopewalls

**General:** The channel is rated in good condition (7 out of 9). There is no slopewall or channel protection at the abutments and piers. The banks are steep and the channel flows around Pier 2.

**Deficiencies:** A large tree is lodged against Pier 2 and a second tree is located adjacent to Pier 3 (see photo 220).

### III. PURPOSE AND NEED

#### A. Background

This bridge carries SR 46 over Whitewater River. The project is located approximately 0.44 miles West of US 52 in Dearborn County. Please see Appendix A for location maps. Specifically, the project is located in the southwest quarter of Section 10, Township 7 North, and Range 1 West as shown on the 7.5 minute West Harrison, Indiana USGS quadrangle map. The Whitewater River flows north to south and a segment of this stream is listed on the Indiana Department of Natural Resources (IDNR) Listing of Outstanding Rivers and Streams. The segment starts at Cambridge City and ends at the Indiana/Ohio line.

Bridge No. 046-15-01987 A is a five span steel truss structure constructed in 1937. The existing structure has five equal 175'-0" spans. The existing clear roadway over the bridge is 24'-0" and consists of two 10'-0" lanes with 2'-0" shoulders. The bridge was rehabilitated in 1985 and painted in 1996. The latest bridge inspection report (9/23/2019, see Appendix E) assigned a bridge sufficiency rating of 38.7 out of a possible 100. The deck is currently assessed as being in fair condition. The superstructure is currently rated as poor condition and the substructure is currently rated in satisfactory condition. Due to the poor condition rating of the superstructure, the bridge is required to be inspected on an annual basis. Please see Appendix B for photographs of the existing road and bridge. In addition scoping field check meeting minutes and correspondence is provided in Appendix G.

According to the Indiana Historic Bridge Inventory, Bridge No. 046-15-01987 A is listed as a "Non-Select" bridge, eligible for inclusion in the National Register of Historic Places (NRHP) under Criterion C. This bridge is eligible under Criterion C because it represents a variation, evolution, or transition that is conveyed through important features or innovations related to bridge construction, design, or engineering, and it retains historic integrity necessary to convey its engineering significance.

#### B. Need

The Indiana Department of Transportation (INDOT) has identified two needs that will be addressed by this project: the structural deficiencies and the deck and superstructure conditions.

The bridge's needs are a result of the heavy deterioration and distress exhibited by some of the truss verticals, diagonals, low chords, stringers, floor beams, and deck. The bridge was load rated with the noted existing deterioration. The analysis found that the verticals and stringers controlled the load rating with design and legal load rating factors less than 1.0. These areas of the superstructure will need to be addressed to improve their load capacity and enhance their condition. The deterioration noted has resulted in a deck condition rating of fair (5) and a superstructure condition rating of poor (4).

#### C. Purpose

The purpose of this project is to improve the overall condition of the bridge's superstructure to satisfactory (6) or better and improve the structural capacity of the truss members and floor system such that all operating (legal) loads have rating factors greater than 1.0.

#### D. Other Goals/Objective

In accordance with the Programmatic Agreement among the Federal Highway Administration, INDOT, the Indiana Historic Preservation Officer, and the Advisory Council on Historic Preservation regarding the Management and Preservation of Historic Bridges (HBPA) signed August 11, 2006, INDOT is planning for work on this bridge to follow the Secretary of the Interior's Standards for Rehabilitation, as specified in the HBPA's "Standard Treatment Approach for Historic Bridges."

The current bridge and roadway have 5 level one design criteria deficiencies: lane width, usable and paved shoulder width, bridge clear roadway width, through travel lane cross slope, and bridge railing test level. The first three level one criteria deficiencies are all caused by the existing bridge width being too narrow. The minimum lane, usable shoulder, and clear roadway width are 12'-0", 6'-0", and 28'-0", respectively. The travel lane cross slope is approximately 1.5% and is less than the required 2%. The current bridge railing safety performance criteria requires a TL-3 crash tested rail whereas, the existing bridge rail provides an unknown test level rating. Please see Appendix C for the level one controlling criteria checklist and Table 1 for a summary of level one criteria. The level one design

criteria for travel lane cross slope and current bridge railing safety performance will be satisfied; however, a level one design exception for lane, shoulder, and bridge clear roadway widths shall be submitted.

A bridge rehabilitation project requires that a scour analysis be performed for stream crossings. A scour analysis model was created to determine if the abutments and piers are scour critical; however, the results are pending a review by INDOT bridge design. If the substructure units are deemed scour critical, scour countermeasures shall be installed around the existing substructure units to alleviate any scour concerns.

#### **IV. ALTERNATIVES**

The alternatives described in this document are based on the guidance for writing historic bridge Section 4(f) alternatives analysis produced by INDOT, Cultural Resource Office and finalized on December 14, 2012. Per the guidance, alternatives A through F must be analyzed in consecutive order until a feasible and prudent alternative has been determined. Once a feasible and prudent alternative has been determined, the remaining alternatives do not need to be discussed. A feasible alternative is one that is possible to engineer, design, and build. A prudent alternative is one that does not present significantly unique or unusual factors (e.g. cost; social, economic, or environmental impacts; community disruption).

##### **A. The No Build/Do Nothing Alternative**

The No-Build/Do-Nothing Alternative was considered as a possible solution for the proposed project. This alternative proposed utilization of the existing facilities with no expenditure of capital funds or improvement of the roadway. The No-Build/Do-Nothing Alternative would not address the overall purpose and need of the project, which is to improve the physical condition of the bridge and prevent further deterioration. If the No-Build/Do-Nothing Alternative would be selected, deterioration of the bridge would continue. If the deterioration is not abated, weight restrictions will eventually be implemented until it becomes impassable to all vehicles. Although this alternative is feasible, it is not prudent to allow the bridge to deteriorate until it is impassable causing a significant community disruption.

##### **B1. Rehabilitation of the Existing Bridge for Continued Vehicular Use (Two-Way Option) Meeting Secretary of Interior's Standards for Rehabilitation**

The Secretary of the Interior (SOI) defines rehabilitation as “the act or process of making possible a compatible use for a property through repair, alterations, and additions while preserving those portions or features which convey its historical, cultural, or architectural values” ([http://www.cr.nps.gov/local-law/arch\\_stnds\\_10.htm](http://www.cr.nps.gov/local-law/arch_stnds_10.htm)). The Standards for Rehabilitation follow the Department of Interior regulations 36 CFR 67, which defines rehabilitation as “the process of returning a building (structure) to a state of utility, through repair or alteration, which makes possible an efficient use while preserving those portions and features of the structure and its site and environment which are significant to its historic, architectural, and cultural values as determined by the Secretary”. The 10 standards outlined shall be followed for each rehabilitation project.

###### **Two-Way Option (Bridge Rehabilitation Including Roadway Reconstruction)**

This alternative includes the rehabilitation of the existing bridge and the east and west approach roadway to current INDOT and FHWA criteria for load capacity and condition. The structural deficiencies addressed under this alternative would increase the rating factors of the truss members and floor system to 1.0 or above for all design and legal loads. The improvements will result in increased deck and superstructure condition ratings (satisfactory or higher) so that the maximum inspection frequency may be increased from 12 months to 24 months. In addition, this alternative will eliminate the level one criteria deficiency of substandard bridge railing and travel lane cross slope; however, the lane, shoulder, and bridge clear roadway widths will all remain substandard. These widths can not be satisfied unless the bridge is widened. In order to widen the bridge, most truss members and the floor beams would be replaced. The widening option is not economical to construct and would cause the bridge to lose most of its historic integrity. Please see Table 1 for a summary of the level one criteria for this alternative.

The rehabilitation items have been summarized below. Please see Figures 1 through 6 for additional information on the proposed rehabilitation work.:

1. The concrete deck, deck drains, and metal forms in all spans shall be replaced. The new deck will provide a standard 2% cross slope and metal forms may be used on the underside to form to place the deck. The new deck will incorporate a PF-1 bridge railing in place of the concrete curbs in front of the existing steel bridge posts and handrail. The PF-1 bridge railing will meet TL-3 requirements and will improve the safety on the

bridge while providing a taller vertical barrier to help shield the truss from salt and water spray. The proposed bridge railing may be seen in the proposed typical section shown in Appendix I.

2. Replace in kind some of the existing steel bridge posts and portions of the existing steel handrailing that have holes due to deterioration.
3. The existing concrete bridge railing transitions will be replaced with new concrete railing transitions that meet the current crash test level requirements and shall be placed on top of the new concrete bridge approach slabs. The new concrete railing transitions will be similar to the existing and include a bush hammered concrete finish (Figure 6) and taper to the widened approach guardrail. The existing approach guardrail will be replaced with MASH guardrail and end treatments.
4. Replace bridge joints with pre-compressed foam joints or an INDOT approved joint. At the double expansion joint, replace bridge joint with a modular expansion joint or an INDOT approved joint.
5. New approach slabs will be placed. The approach work will include milling and resurfacing the existing bituminous approaches to tie the new construction in with the existing roadway. Full depth pavement will be placed at the ends of the approach slabs to tie the resurfacing work to the concrete approach slabs.
6. Repair or replace deteriorated truss members (low chords, diagonals, verticals, and end posts) that have holes 2 inch in diameter and larger to restore lost section and improve condition. Holes smaller than 2 inches may either be ignored or have a bolt placed through the hole. Truss members will be repaired by placing a plate over the deteriorated area or the members will be replaced in kind. Replace in kind existing gusset plates that have areas of heavy deterioration. Connection plates and lattice bars with heavy section loss and holes will be replaced in kind. Lower lateral bracing and stay plates with heavy section loss will be repaired or replaced in kind.
7. Stringers in poor condition or that have heavy section loss will be replaced in kind or repaired. Floor beams with heavy section loss will be repaired and at this time no floor beams are expected to be replaced. Repairing stringers and floor beams will be done by placing a plate over the deteriorated area.
8. Rivets removed to make necessary repairs will be replaced with round headed bolts. The rounded head will be installed on the face of the member that is most visible from the roadway or exterior face of member.
9. Expansion bearings that have moved or are over-rotated will be reset to the proper orientation. Deteriorated anchor bolts or nuts and portions of the bearing assemblies will be replaced in kind.
10. All areas of the truss will be cleaned and painted. The paint color will match the existing color.
11. Concrete in the low chord of Span B will be removed.
12. Deteriorated concrete caps of Pier 5 and the East Abutment will be removed and reconstructed. Delaminated and spalled areas of abutments and piers will be patched.
13. The scour countermeasures, if required, shall be placed at the appropriate substructure units. Debris that is lodged against the substructure units will be removed.
14. Tree branches will be trimmed on both sides of the road to keep branches away from the truss and vehicles.

The new bridge deck out to out width will match the existing deck width; however, because PF-1 bridge rail will be used, the clear roadway will be reduced by 10 1/2 inches. The travel lanes and shoulder widths will be transitioned from the bridge (10'-0" lanes and 1'-6 3/4" shoulders) to the roadway approaches (12'-0" lanes and 2'-0" shoulders) at each bridge end. The rehabilitation will require approximately 250 feet of approach work east and west of the bridge. There will be 3 level one design exceptions required for this alternative (lane width, shoulder width, and bridge clear roadway width). These are expected to be approved since widening the truss is very difficult and expensive and there was only 1 record of an accident on the bridge. The level one controlling criterion affected by this alternative are summarized in Table 1.

The bridge will be closed approximately 6 months during construction for this alternative. The detour will route traffic on either US 52 to I-74 (1.5 mile detour) or US 52 to SR 1 (9 mile detour). The US 52 to I-74 route is preferred; however, the official detour route shall be submitted to INDOT Traffic for final approval. The project is not considered a mobility significant project per IDM Chapter 81-1.02. A transportation management plan (TMP) is not anticipated for this project. Coordination will be required with all affected utilities in the area. The power lines and telephone lines attached to the truss will need to be relocated off the bridge; however, further discussions and coordination is required. The remainder of the utilities are to be outside the project construction limits.

The existing right of way is located at the edges of pavement because the right of way was not recorded in a timely fashion. This alternative will require reacquisition of the original right of way (100 feet on either side of SR 46) in order to complete this project.

This structure crosses Whitewater River in a rural area and has an upstream drainage area of approximately 1,330.74 square miles. The ground is mainly tree covered along the banks with some areas of farm fields in the floodplain. Tree trimming will be required to keep branches off the trusses and allow sunlight to reach the low chord members. A

401/404 permit will be required if scour countermeasures or channel clearing for debris are required. An Indiana Department of Natural Resources (IDNR) construction in a floodway permit will be required because the drainage area is greater than 50 square miles. There are possible wetlands near the project. An IDEM Rule 5 permit is not anticipated because the land disturbance is expected to be less than 1 acre. A level 4 CE is required due to the anticipated structure rehabilitation impacts and historical significance of the bridge. No bat or bird habitat was found within the superstructure.

This structure is listed on the INDOT historic bridge inventory list as a “Non-Select” bridge. Per the terms of the “Programmatic Agreement Regarding Management and Preservation of Indiana’s Historic Bridges” (Historic Bridge PA), the Federal Highway Administration–Indiana Division (FHWA) will satisfy its Section 106 responsibilities involving “Select” and “Non-Select” bridges through the Project Development Process (PDP) of the Historic Bridges PA (Stipulation III).

<b>Table 1: B1. ALTERNATIVE LEVEL ONE CRITERIA CHECKLIST SUMMARY</b>				
<b>Design Element</b>	<b>Minimum Design Criteria</b>	<b>Existing Condition</b>	<b>Proposed Condition</b>	<b>Design Exception Required</b>
Design Speed	55 mph	55 mph	55 mph	No
Lane Width	12 ft.	10 ft.	10 ft.	Yes
Usable Shoulder Width	6 ft.	2 ft.	1'-6 3/4"	Yes
Paved Shoulder Width	2 ft.	2 ft.	1'-6 3/4"	Yes
Bridge Clear Roadway Width	28'-0"	24'-0"	23'-1 1/2"	Yes
Structural Capacity	HS-15	H-20*	HS-15	No
Horizontal Curve, Minimum Radius	N/A	N/A	N/A	No
Superelevation Transition Lengths	N/A	N/A	N/A	No
Stopping Sight Distance Horizontal Curves	N/A	N/A	N/A	No
Stopping Sight Distance Vertical Curves	495 ft.	> 495 ft.	> 495 ft.	No
Maximum Grade	7.5%	1.0%	1.0%	No
Travel Lane Cross Slope	2.0%	1.5%	2.0%	No
Superelevation Rate	N/A	N/A	N/A	No
Vertical Clearance	N/A	N/A	N/A	No
Bridge Railing Test Level	TL-3	Unknown	TL-3	No

\*See existing plans

**\*Only the narrative of the Alternative Analysis Report has been included; attachments, graphics and additional pages have been removed. The full Report can be made available upon request.**

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February 15, 2021

Karen Wood  
Environmental & Cultural Resources Manager  
SJCA Inc.  
1104 Prospect Street  
Indianapolis, Indiana 46203

State Agency: Indiana Department of Transportation (“INDOT”),  
Federal Agency: Federal Highway Administration, Indiana Division (“FHWA”)

Re: DUAL REVIEW: Historic bridge alternatives analysis (Beam, Longest & Neff, 6/2020) for the SR 46 over the Whitewater River Bridge (Bridge No. 06-15-01987A; NBI No. 17540) project in Harrison and Logan townships, Dearborn County, Indiana (Des. No. 1383721; DHPA No. 26124

Dear Ms. Wood:

Pursuant to Section 106 of the National Historic Preservation Act of 1966, as amended (54 U.S.C. § 306108); implementing regulations at 36 C.F.R. Part 800; the “Programmatic Agreement Among the Federal Highway Administration, the Indiana Department of Transportation, the Indiana Historic Preservation Officer, and the Advisory Council on Historic Preservation Regarding Management and Preservation of Indiana’s Historic Bridges” (“Indiana Historic Bridges PA”); and the “Programmatic Agreement (PA) Among the Federal Highway Administration, the Indiana Department of Transportation, the Advisory Council on Historic Preservation and the Indiana State Historic Preservation Officer Regarding that Implementation of the Federal Aid Highway Program In the State of Indiana”; and also pursuant to Indiana Code 14-21-1-18 and 312 Indiana Administrative Code (“IAC”) 20-4, the staff of the Indiana State Historic Preservation Officer (“Indiana SHPO”) has reviewed your January 22, 2021 submission, which enclosed the aforementioned historic bridge alternatives analysis (“HBAA”), received by our office the same day.

We agree with the conclusions of the HBAA that Alternative B1: Rehabilitation of the Existing Bridge for Continued Vehicle Use (Two-Way Option) Meeting Secretary of Interior’s Standards for Rehabilitation is both a feasible and prudent alternative and is therefore, the preferred alternative for this project. We appreciate the project team’s willingness to rehabilitate this “Non-Select” bridge as metal truss bridges are becoming increasingly rare within the state.

As noted in the Mitigation section of the HBAA, we do request this bridge to be documented according to the “Indiana DNR – Division of Historic Preservation and Archaeology Minimum Architectural Documentation Standards.” We have enclosed a copy of those state standards for your reference. Specifically, we request digital, color photographs, a photo log that corresponds to the photographs, a photo key, and an overview thumbnail sheet. Any additional drawings or historic bridge plans, such as those in Appendix F of the HBAA will be accepted. Please ensure that the photographs provide overviews of the resource from all directions, as well as any character-defining features. Please submit a copy of this draft documentation on CD, flash drive, or previously approved storage device for our review and approval. Upon approval, this documentation should be provided to a public or not-for-profit organization that is willing to accept a copy of this documentation and make it available to the public. Please inform us which local or not-for-profit organization is willing to accept this documentation.

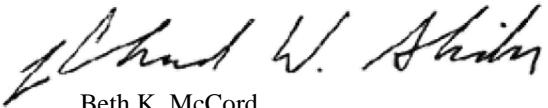
As INDOT’s January 21, 2021 letter indicates, additional information regarding above-ground and archaeological resources are forthcoming. Once the indicated information is received, the Indiana SHPO will resume identification and evaluation procedures for this project. Please keep in mind that additional information may be requested in the future.

If you have questions regarding our dual review of the aforementioned project, please contact DHPA. Questions about archaeological issues should be directed to Rachel Sharkey at (317) 234-5254 or rsharkey@dnr.IN.gov. Questions about historic buildings or structures pertaining to this review should be directed to Danielle Kauffmann at (317) 232-0582 or dkauffmann@dnr.IN.gov.

For the benefit of those recipients of a copy of this letter who are not Section 106 consulting parties, please be aware that the documents discussed here can be found online in INSCOPE at <http://erms.indot.in.gov/Section106Documents/>. From there, search by this project's designation number: 1383721. Anyone receiving an e-mailed copy of this letter who does *not* wish to receive future copies of our correspondence about this bridge project is asked to reply to dkauffmann@dnr.IN.gov and so advise us.

In all future correspondence regarding the dual review of this bridge project on SR 46 over the Whitewater River in Dearborn County (Des. No. 1383721), please continue to refer to DHPA No. 26124.

Very truly yours,



Beth K. McCord  
Deputy State Historic Preservation Officer

BKM:DMK:dmk

Enclosure: Minimum Architectural Documentation Standards

EMC to federal and state agency or consultant staff members:

Erica Tait, FHWA  
Anuradha Kumar, INDOT  
Mary Kennedy, INDOT  
Shaun Miller, INDOT  
Susan Branigin, INDOT  
Karen Wood, SJCA, Inc.  
Danielle Kauffmann, DNR-DHPA  
Rachel Sharkey, DNR-DHPA

EMC to Indiana Historic Preservation Review Board Members:

J. Scott Keller, Review Board  
Anne Shaw Kingery, Review Board  
Daniel Kloc, Review Board  
Jason Larrison, Review Board  
Chandler Lighty, Review Board  
Beth K. McCord, DNR-DHPA, Review Board  
April Sievert, Review Board  
Christopher Smith, Deputy Director DNR, Chairman Review Board

EMC to Potentially Interested Persons:

Eastern Shawnee Tribe of Oklahoma  
Miami Tribe of Oklahoma  
Peoria Tribe of Indians of Oklahoma  
Pokagon Band of Potawatomi Indians  
United Keetoowah Band of Cherokee Indians  
Delaware Tribe of Indians

Jim Thatcher, Dearborn County Commissioner, District 1  
Art Little, Dearborn County Commissioner, District 2  
Rick Probst, Dearborn County Commissioner, District 3  
Tim Grieve, Dearborn County Highway Superintendent  
Mark Policinski, Ohio-Kentucky-Indiana Regional Council of Governments  
Jarrad Holbrook, Indiana Landmarks Southeast Field Office  
Dearborn County Historian  
Dearborn County Historical Society  
Dr. James L. Cooper, DePauw University Professor Emeritus of History  
Paul Brandenburg, Indiana Historic Spans Task Force  
Tony Dillon, Historic Hoosier Bridges  
Nathan Holth, Historicbridges.org  
Kitty Henderson, Historic Bridge Foundation

CC to Potentially Interested Persons:

Aurora Historic Preservation Commission  
Dearborn County Trust for Historic Preservation

# **Appendix E:**

# **Red Flag and Hazardous**

# **Materials**



# INDIANA DEPARTMENT OF TRANSPORTATION

100 North Senate Avenue  
Room N758-ES  
Indianapolis, Indiana 46204

PHONE: (855) 463-6848  
(855) INDOT4U

**Eric Holcomb, Governor**  
**Joe McGuinness, Commissioner**

Date: February 4, 2022

To: Site Assessment & Management (SAM)  
Environmental Policy Office - Environmental Services Division (ESD)  
Indiana Department of Transportation (INDOT)  
100 N Senate Avenue, Room N758-ES  
Indianapolis, IN 46204

From: Kirsten Lewis  
Beam, Longest and Neff, LLC  
8320 Craig St  
Indianapolis, Indiana  
klewis@b-l-n.com

Re: RED FLAG INVESTIGATION  
DES #1383721, State Project  
Bridge Project  
SR 46 over Whitewater River, 0.44 Mile West of US 52  
Dearborn County, Indiana

## PROJECT DESCRIPTION

**Brief Description of Project:** The preliminary preferred alternative for this project includes the rehabilitation of the existing bridge, a five-span steel truss structure constructed in 1937, that carries SR 46 over the Whitewater River, 0.44 mile west of US 52, and the east and west approach roadway to current INDOT and FHWA criteria for load capacity and condition. The concrete deck, deck drains, and metal forms in all spans will be replaced, and new PF-1 bridge rail will be incorporated. Because PF-1 bridge rail will be used, the clear roadway will be reduced by 10 ½ inches. The travel lanes and shoulder widths will be transitioned from the bridge (10-foot-wide lanes and 1-foot, 6 ¾-inch wide shoulders) to the roadway approaches (12-foot-wide lanes and 2-foot-wide shoulders). The bridge joints will be replaced, and other deteriorated elements will be repaired or replaced in kind. All areas of the truss will be cleaned and painted. Deteriorated concrete caps of Pier 5 and the East Abutment will be removed and reconstructed. Delaminated and spalled areas of abutments and piers will be patched. Scour countermeasures, if required, and debris removal will also be implemented. New approach slabs will be placed, and the rehabilitation will require approximately 250 feet of approach work east and west of the bridge. The existing approach guardrail will be replaced with MASH guardrail and end treatments.

Bridge and/or Culvert Project: Yes  No  Structure # 046-15-01987A

If this is a bridge project, is the bridge Historical? Yes  No  , Select  Non-Select

(Note: If the project involves a historical bridge, please include the bridge information in the Recommendations Section of the report).

Proposed right of way: Temporary  # Acres \_\_\_\_\_ Permanent  # Acres \_\_\_\_\_, Not Applicable

Type and proposed depth of excavation: Excavation will occur to remove the existing road approach and install the new road approach. The depth of excavation will be approximately 1.5 feet deep.

Maintenance of traffic: Maintenance of traffic will consist of a full closure with a 1.5-mile-long detour route utilizing US 52, I-74, and SR 1. The bridge will be closed for 6 months.

Work in waterway: Yes  No  Below ordinary high water mark: Yes  No

State Project:  LPA:

Any other factors influencing recommendations: N/A

#### INFRASTRUCTURE TABLE AND SUMMARY

<b>Infrastructure</b>			
Indicate the number of items of concern found within the 0.5 mile search radius. If there are no items, please indicate N/A:			
Religious Facilities	<b>N/A</b>	Recreational Facilities	<b>N/A</b>
Airports <sup>1</sup>	<b>N/A</b>	Pipelines	<b>N/A</b>
Cemeteries	<b>3</b>	Railroads	<b>1</b>
Hospitals	<b>N/A</b>	Trails	<b>N/A</b>
Schools	<b>N/A</b>	Managed Lands	<b>N/A</b>

<sup>1</sup>In order to complete the required airport review, a review of public-use airports within 3.8 miles (20,000 feet) is required.

Explanation:

Cemeteries: Three (3) cemetery icons representing two (2) cemeteries are located within the 0.5-mile search radius. The nearest cemetery icon is mapped incorrectly, and the nearest cemetery, Braysville Cemetery, is located 0.31 mile northeast of the project area. No impact is expected.

Railroads: One (1) active railroad is located within the 0.5 mile search radius. The railroad (Indiana & Ohio Railway) is 0.24 mile east of the project area. No impact is expected.

#### WATER RESOURCES TABLE AND SUMMARY

<b>Water Resources</b>			
Indicate the number of items of concern found within the 0.5 mile search radius. If there are no items, please indicate N/A:			
NWI - Points	<b>1</b>	Canal Routes - Historic	<b>1</b>
Karst Springs	<b>N/A</b>	NWI - Wetlands	<b>27</b>
Canal Structures – Historic	<b>1</b>	Lakes	<b>N/A</b>
NPS NRI Listed	<b>N/A</b>	Floodplain - DFIRM	<b>11</b>
NWI-Lines	<b>5</b>	Cave Entrance Density	<b>N/A</b>
IDEM 303d Listed Streams and Lakes (Impaired)	<b>8</b>	Sinkhole Areas	<b>N/A</b>
Rivers and Streams	<b>12</b>	Sinking-Stream Basins	<b>N/A</b>

Explanation:

NWI – Points: One (1) NWI – point is located within the 0.5 mile search radius. The point is 0.14 mile southeast of the project area. No impact is expected.

Canal Routes – Historic: One (1) historic canal route is located within the 0.5 mile search radius. The canal route (Whitewater Canal) is 0.22 mile northeast of the project area. No impact is expected.

NWI – Wetlands: Twenty-seven (27) wetlands are located within the 0.5 mile search radius. Four (4) wetlands are located adjacent to the project area. A Waters of the US Report will be prepared and coordination with INDOT ESD Ecology and Waterway permitting will occur.

Canal Structures - Historic: One (1) historic canal structure is located within the 0.5 mile search radius. The canal structure (Marshall's Lock #8) is 0.40 mile east of the project area. No impact is expected.

Floodplain – DFIRM: Eleven (11) floodplain polygons are located within the 0.5 mile search radius. The project area is located within four (4) of the floodplain polygons. Coordination with INDOT ESD Ecology and Waterway Permitting will occur.

NWI – Lines: Five (5) NWI – lines are located within the 0.5 mile search radius. The closest NWI – line is located 0.17 mile north of the project area. No impact is expected.

Rivers and Streams: Twelve (12) rivers and stream segments are located within the 0.5 mile search radius. Four (4) river and stream segments are located within the project area. A Waters of the US Report will be prepared and coordination with INDOT ESD Ecology and Waterway Permitting will occur.

IDEML 303d Listed Streams and Lakes (Impaired): Eight (8) 303d Listed Streams are located within the 0.5 mile search radius. Two (2) segments associated with the Whitewater River cross the project area. Whitewater River is listed as impaired for PCBs in fish tissue. Exposure to PCBs in fish tissue is considered low, assuming workers are not eating biota surrounding or associated with the water body. Workers will be informed. If there will be sediment and/or soils disturbed by construction, additional investigation may be necessary. Coordination with INDOT ESD Site Assessment & Management (SAM) will occur.

#### MINING AND MINERAL EXPLORATION TABLE AND SUMMARY

##### **Mining/Mineral Exploration**

Indicate the number of items of concern found within the 0.5 mile search radius. If there are no items, please indicate N/A:

Petroleum Wells	<b>N/A</b>	Mineral Resources	<b>N/A</b>
Mines – Surface	<b>N/A</b>	Mines – Underground	<b>N/A</b>

Explanation: No mining/mineral exploration sites are within the 0.5 mile search radius.

#### HAZARDOUS MATERIAL CONCERNS TABLE AND SUMMARY

##### **Hazardous Material Concerns**

Indicate the number of items of concern found within the 0.5 mile search radius. If there are no items, please indicate N/A:

Superfund	<b>N/A</b>	Manufactured Gas Plant Sites	<b>N/A</b>
RCRA Generator/ TSD	<b>N/A</b>	Open Dump Waste Sites	<b>N/A</b>
RCRA Corrective Action Sites	<b>N/A</b>	Restricted Waste Sites	<b>N/A</b>
State Cleanup Sites	<b>N/A</b>	Waste Transfer Stations	<b>N/A</b>
Septage Waste Sites	<b>N/A</b>	Tire Waste Sites	<b>N/A</b>
Underground Storage Tank (UST) Sites	<b>N/A</b>	Confined Feeding Operations (CFO)	<b>N/A</b>
Voluntary Remediation Program	<b>N/A</b>	Brownfields	<b>N/A</b>

Construction Demolition Waste	<b>N/A</b>	Institutional Controls	<b>N/A</b>
Solid Waste Landfill	<b>N/A</b>	NPDES Facilities	<b>1</b>
Infectious/Medical Waste Sites	<b>N/A</b>	NPDES Pipe Locations	<b>2</b>
Leaking Underground Storage (LUST) Sites	<b>N/A</b>	Notice of Contamination Sites	<b>N/A</b>

Unless otherwise noted, site specific details presented in this section were obtained from documents reviewed on the Indiana Department of Environmental Management (IDEM) Virtual File Cabinet (VFC).

**Explanation:**

NPDES Facilities: One (1) NPDES facility is located within the 0.5 mile search radius. The facility, St. Leon Wastewater Treatment Plant (WWTP - NPDES Permit No. IN0058408), is located adjacent to the western terminus of the project area at 3059 SR 46, West Harrison, IN. Coordination with the WWTP facility will occur.

NPDES Pipe Locations: Two (2) NPDES pipe locations are located within the 0.5 mile search radius and are associated with the St. Leon WWTP permit (NPDES Permit No. IN0058408). Coordination with the WWTP facility will occur.

**ECOLOGICAL INFORMATION SUMMARY**

The Dearborn County listing of the Indiana Natural Heritage Data Center information on endangered, threatened, or rare (ETR) species and high quality natural communities is provided at [https://www.in.gov/dnr/nature-preserves/files/np\\_dearborn.pdf](https://www.in.gov/dnr/nature-preserves/files/np_dearborn.pdf). A preliminary review of the Indiana Natural Heritage Database by INDOT ESD did indicate the presence of ETR species within the 0.5 mile search radius. Coordination with USFWS and IDNR will occur.

A review of the USFWS database did not indicate the presence of endangered bat species in or within 0.5 mile of the project area. The project area is located in a rural area surrounded by farm fields. The September 1, 2020 inspection report for Bridge No. 046-15-01987A states that no evidence of bats was seen or heard under the bridge. The range-wide programmatic consultation for the Indiana Bat and Northern Long-eared Bat will be completed according to the most recent "Using the USFWS's IPaC System for Listed Bat Consultation for INDOT Projects".

**RECOMMENDATIONS SECTION**

Include recommendations from each section. If there are no recommendations, please indicate N/A:

The proposed project involves one historic bridge, Bridge No. 046-15-01987A (Non-Select). Coordination with INDOT-Cultural Resources and the Indiana State Historic Preservation Office (SHPO) will occur.

INFRASTRUCTURE: N/A

WATER RESOURCES: The presence of the following water resources will require the preparation of a Waters of the US Report and coordination with INDOT ESD Ecology and Waterway Permitting:

- Four (4) wetlands are located adjacent to the project area.
- The project area is located within a floodplain (coordination only).
- Four (4) stream segments flow through the project area.

Whitewater River is listed as impaired for PCBs in fish tissue. Exposure to PCBs in fish tissue is considered low, assuming workers are not eating biota surrounding or associated with the water body. Workers will be informed. If there will be sediment and/or soils disturbed by construction, additional investigation may be necessary. Coordination with INDOT ESD SAM will occur.

MINING/MINERAL EXPLORATION: N/A

HAZARDOUS MATERIAL CONCERNS: One (1) NPDES facility, the St. Leon WWTP, is located adjacent to the western terminus of the project area and two (2) associated NPDES pipes are located within the project area. Coordination with St. Leon WWTP facility will occur.

ECOLOGICAL INFORMATION: Coordination with USFWS and IDNR will occur. The range-wide programmatic consultation for the Indiana Bat and Northern Long-eared Bat will be completed according to the most recent "Using the USFWS's IPaC System for Listed Bat Consultation for INDOT Projects".

INDOT ESD concurrence: Nicole Fohey-Breting (Signature)

Nicole Fohey-Breting  
Digitally signed by  
Nicole Fohey-Breting  
Date: 2022.02.04  
14:55:48 -05'00'

Prepared by:

Kirsten Lewis

Senior Environmental Analyst  
Beam, Longest and Neff, LLC

#### Graphics:

A map for each report section with a 0.5 mile search radius buffer around all project area(s) showing all items identified as possible items of concern is attached. If there is not a section map included, please change the YES to N/A:

SITE LOCATION: YES

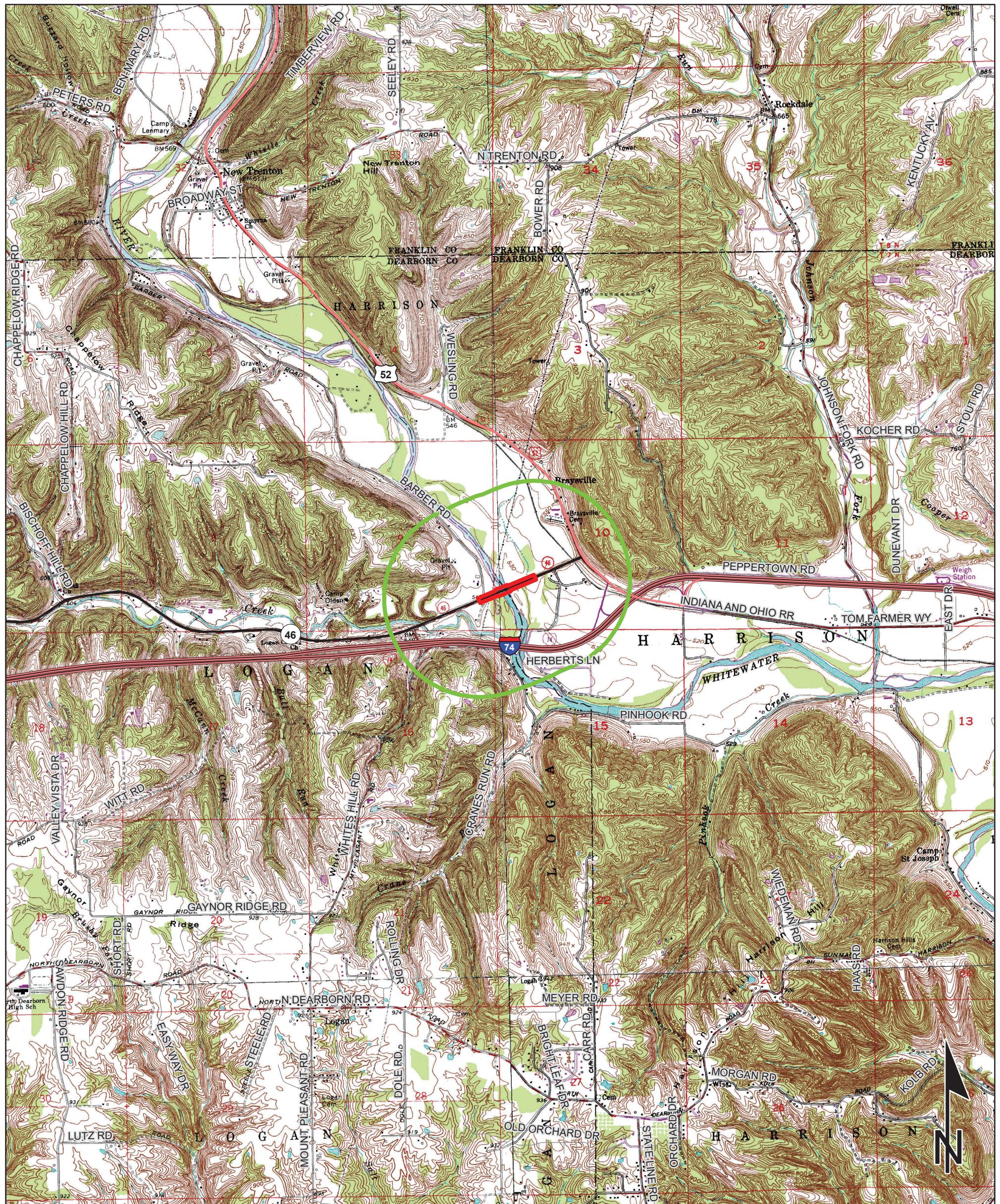
INFRASTRUCTURE: YES

WATER RESOURCES: YES

MINING/MINERAL EXPLORATION: N/A

HAZARDOUS MATERIAL CONCERNS: YES

**Red Flag Investigation - Site Location**  
**SR 46, 0.44 Mile West of US 52**  
**Des. No. 1383721, Bridge Rehabilitation**  
**Dearborn County, Indiana**



Sources: 0.65 0.325 0 0.65 Miles  
**Non Orthophotography**

Data - Obtained from the State of Indiana Geographical Information Office Library

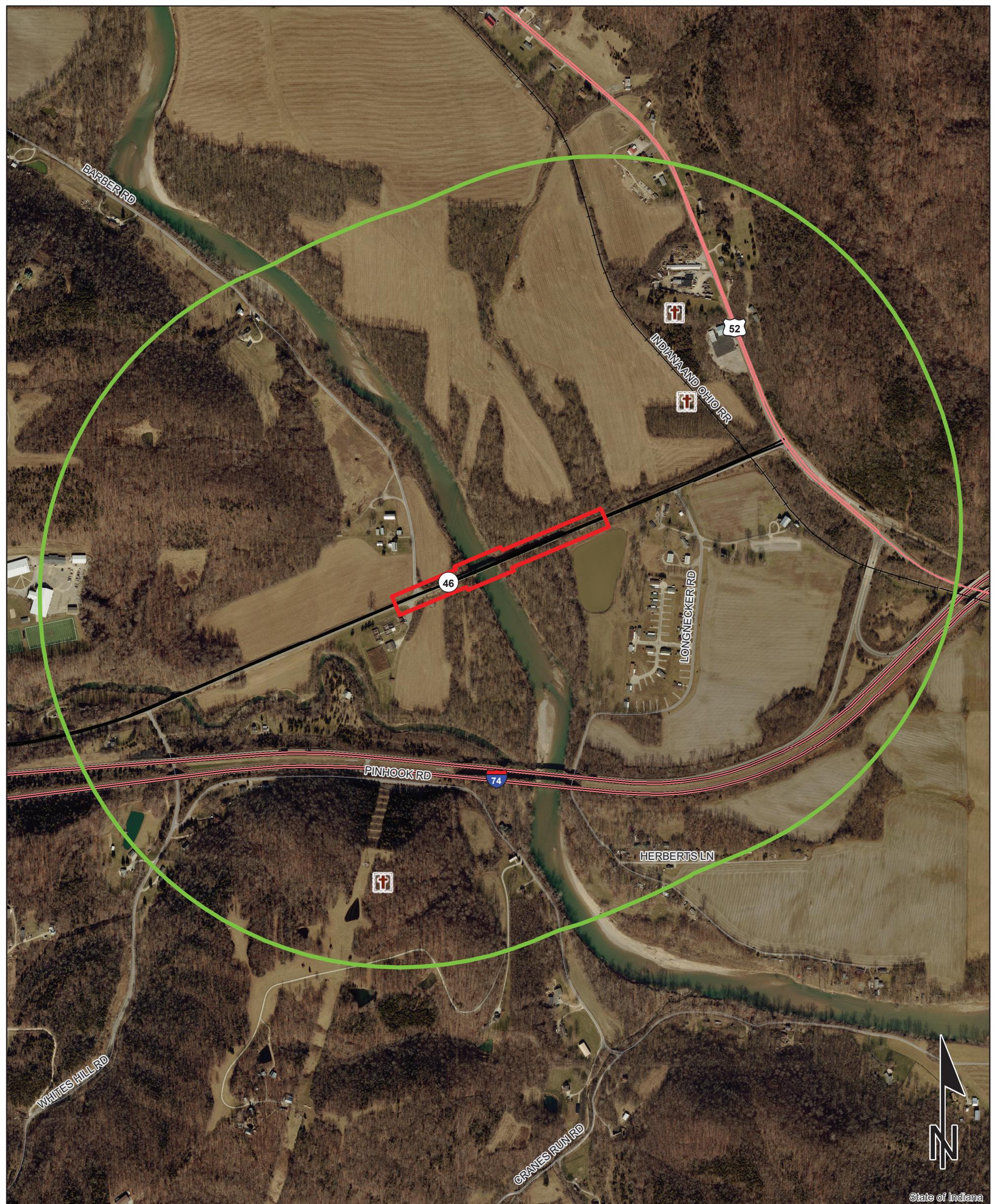
Orthophotography - Obtained from Indiana Map Framework Data ([www.indianamap.org](http://www.indianamap.org))

Map Projection: UTM Zone 16 N Map Datum: NAD83

This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.

**HARRISON & CEDAR GROVE  
QUADRANGLES INDIANA  
7.5 MINUTE SERIES  
(TOPOGRAPHIC)**

**Red Flag Investigation - Infrastructure**  
**SR 46, 0.44 Mile West of US 52**  
**Des. No. 1383721, Bridge Rehabilitation**  
**Dearborn County, Indiana**



Sources: 0.15 0.075 0 0.15 Miles  
**Non Orthophotography**

**Data** - Obtained from the State of Indiana Geographical Information Office Library

**Orthophotography** - Obtained from Indiana Map Framework Data ([www.indianamap.org](http://www.indianamap.org))

**Map Projection:** UTM Zone 16 N **Map Datum:** NAD83

This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.

	Religious Facility		Recreation Facility		Project Area
	Airport		Pipeline		Half Mile Radius
	Cemeteries		Railroad		Toll
	Hospital		Trails		Interstate
	School		Managed Lands		State Route
			County Boundary		US Route
					Local Road

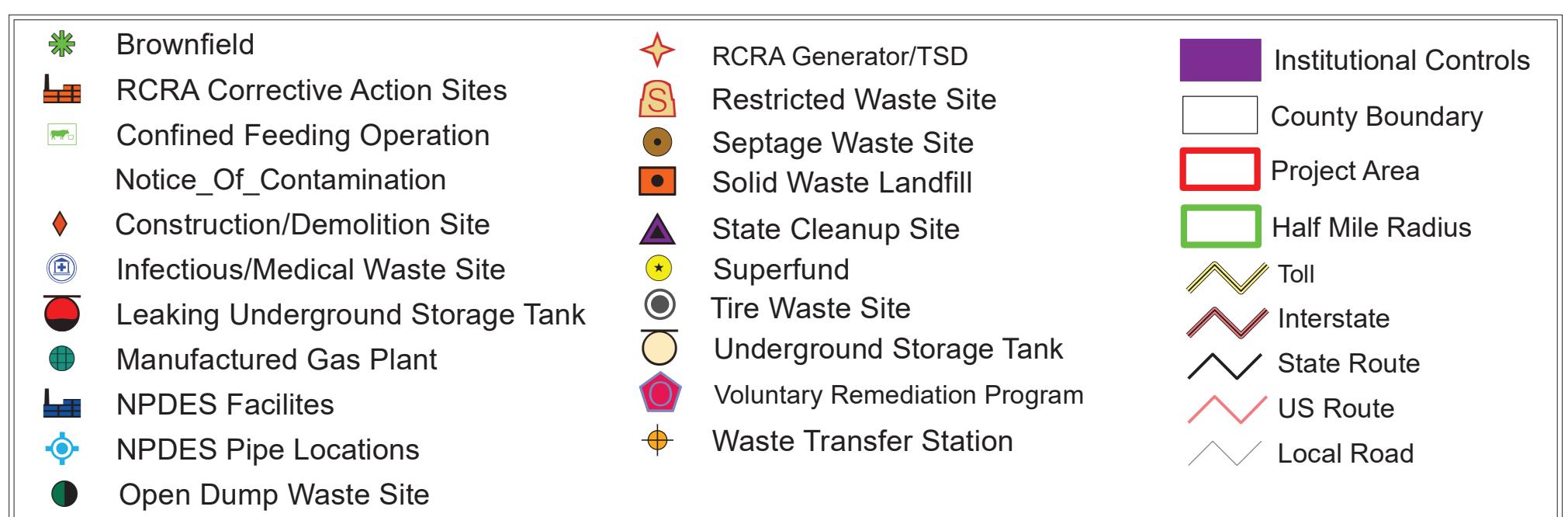
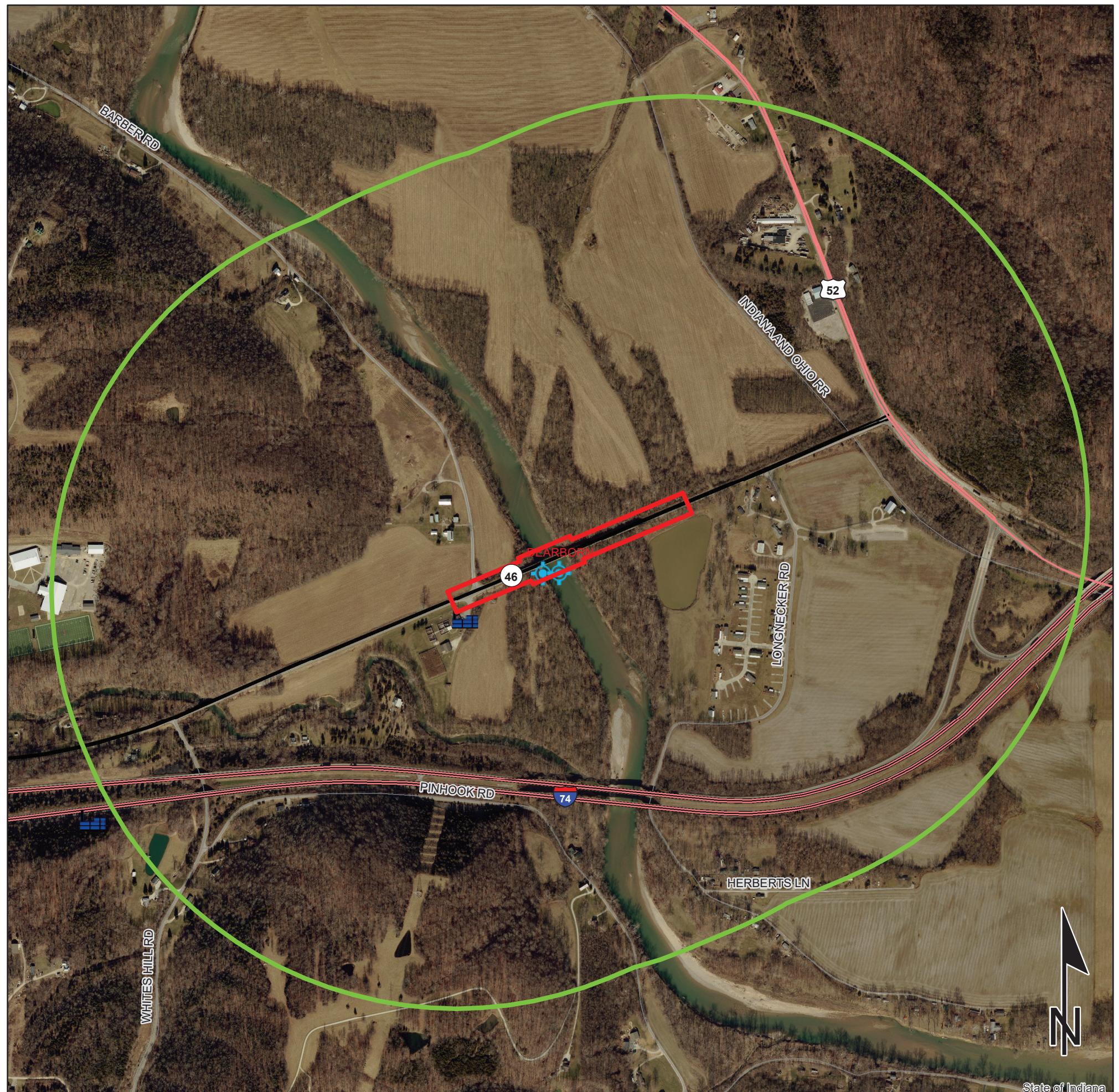


# Red Flag Investigation - Hazardous Material Concerns

## SR 46, 0.44 Mile West of US 52

### Des. No. 1383721, Bridge Rehabilitation

### Dearborn County, Indiana



0.15 0.075 0 0.15  
Miles

This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.

Sources:

Non Orthophraphy

Data - Obtained from the State of Indiana Geographical Information Office Library

Orthophraphy - Obtained from Indiana Map Framework Data

([www.indianamap.org](http://www.indianamap.org))

Map Projection: UTM Zone 16 N Map Datum: NAD83

E-9

# **Appendix F:**

# **Water Resources**

Waters Report  
SR 46 over Whitewater River  
Dearborn County, Indiana  
Bridge Project  
Des. Nos. 1383721

*Jacob Burskey*  
Approved 12/30/2021

Prepared by: Hillary Shaffer  
[hshaffer@b-l-n.com](mailto:hshaffer@b-l-n.com); 317-849-5832  
Beam, Longest and Neff, LLC (BLN)  
Completed Date: December 16, 2021

## PROJECT INFORMATION

### Date of Field Reconnaissance

September 2, 2021

### Location

Sections 9&10, Township 7 North, Range 1 West  
Cedar Grove and Harrison Quadrangles, Indiana Quadrangle Map  
Latitude: 39.280039 North, Longitude: 84.874028 West

## PROJECT DESCRIPTION

The proposed project (Des No. 1383721) consists of a bridge rehabilitation for Bridge No. 046-15-01987A/NBI No. 017540, a historic non-select bridge on SR 46 constructed ca. 1937 over the Whitewater River in Dearborn County, Indiana (Appendix A-1), located 0.44 mile west of US 52. The existing bridge has five equal 175-foot-long spans. The roadway consists of two, 10-foot-wide lanes with 2-foot-wide shoulders on the north and south side of the bridge. The existing roadway approach consists of two, 12-foot-wide travel lanes, one in each direction, with 2-foot-wide shoulders. Project area posted speed limit is 55 miles per hour. Topographic and aerial maps showing the project location can be found in Appendix A. The investigated area was selected because it encompasses the anticipated work area. No additional permanent or temporary right-of-way will be required for this project. Maintenance of traffic will consist of a full closure with a 1.5-mile-long detour route utilizing US 52, I-74 and SR 1 anticipated to last ten months. General topography is primarily flat with a steep decline leading from the roadway down to the surrounding areas on the west end of the bridge, north and south of SR 46. The east end of the investigated area is relatively flat with a small decline from the roadway down to the surrounding area north and south of SR 46. According to the United States Geological Survey (USGS) Cedar Grove and Harrison Indiana 7.5-minute topographic quadrangle maps, the roadway elevation is approximately 543 feet above Mean Sea Level (MSL). The elevation of Whitewater River within the project area is approximately 530 feet above MSL (Appendix A-3).

## DESKTOP DATA REVIEW

### **National Wetland Inventory (NWI) Information**

Four NWI-mapped wetlands are mapped within the investigated area. The wetlands are classified as PFO1A (palustrine forested board-leaved deciduous temporarily flooded) which has two segments in the investigated area, R4SBC (riverine intermittent streambed seasonally flooded) and R2UBH (riverine lower perennial unconsolidated bottom permanently flooded) which is the NWI-mapped linear water feature for Whitewater River. One NWI-mapped wetland is mapped slightly within and adjacent to the investigated area to the southeast and is classified as PUBFx (palustrine unconsolidated bottom semi permanently flooded excavated).

### **National Hydrography Dataset (NHD)**

Appendix A-7 depicts data from the National Hydrography Dataset (NHD), which represents the water drainage network surface water component on the USGS topographic maps. Three NHD flowlines are depicted flowing southeast within the project area. One NHD flowline represents Whitewater River, an artificial path; one NHD flowline represents UNT Whitewater River, an intermittent stream; one NHD flowline was not field verified during the September 2, 2021, field reconnaissance, as there was no stream present at the mapped location.

### **Soils**

According to the Soil Survey Geographic (SSURGO) Database for Dearborn County, the project area does contain soil types on the National List of Hydric Soils (Appendix A-7). Table 1 lists the soil types present within the project area.

**Table 1: Soil Series in the Project Area**

<b>Soil</b>	<b>Map Abbreviation</b>	<b>Hydric Range</b>
Chagrin silt loam, frequently flooded	Ch	0% (Not Hydric)
Stonelick sandy loam, 0 to 2 percent slopes, frequently flooded	St	1-32% (Hydric)

**Floodplain**

A Flood Insurance Rate Map (FIRM) obtained from the Federal Emergency Management Agency (FEMA) shows part of the project area and its surroundings are within a mapped floodway or flood hazard zone (Appendix A-8). The project area is in a 1% annual chance flood hazard floodplain.

**Hydrological Unit Code (HUC)**

The project is in the Blackburn Creek-Whitewater River and Johnson Fork – Whitewater River Watersheds, which are identified by the 12-digit HUCs 050800030806 and 050800030810, respectively (Appendix A-9).

**Karst**

The project is not located in a region of the state with the potential for karst topography to be present.

**FIELD RECONNAISSANCE**

A field visit to inspect the investigated area for aquatic resources was conducted by BLN staff on September 2, 2021. Existing land use in the project area is forested and grassland, with some agricultural land northeast of the project area. The investigated area extended approximately 1,700 feet along SR 46 and approximately 210 feet along Whitewater River to encompass the maximum estimated area of disturbance based on the design for the project. The field investigation identified two waterways, Whitewater River and UNT to Whitewater River, in the survey area (Appendix A-11). No unusual circumstances were present during field reconnaissance.

**Streams**

The investigated area was inspected for the presence of streams; any streams found were mapped, measured, and informally evaluated using the techniques described in *Methods for Assessing Habitat in Flowing Waters: Using the Qualitative Habitat Evaluation Index* (Ohio EPA, 2006). Recommendations on the jurisdictional status of water resources within the project area were made based on guidance from the *US Army Corps of Engineers Jurisdictional Determination Form Instructional Guidebook* (2007).

**Whitewater River**

The USGS topographic map identifies Whitewater River as a solid, perennial blue-line stream. Whitewater River flows under the SR 46 bridge from the northwest to the southeast. According to the USGS *Stream Stats* application, Whitewater River exhibits an upstream drainage area of 1,330.74 square miles. It has an Ordinary High-Water Mark (OHWM) of 200 feet wide and 9 feet deep, and the OHWM was measured approximately 40 feet downstream of the bridge (Appendix A-10). Approximately 210 feet of Whitewater River is present within the investigated area. The quality of Whitewater River was rated average; no riffles or pools were identified within the investigated area, and the substrate was sandy. There was tree cover and overhanging vegetation on the east and west banks, woody debris present, and the river was fast flowing. The Cowardin classification for Whitewater River is riverine, upper perennial, unconsolidated bottom (R3UB). Because the Whitewater River flows to the Great Miami River, which flows to the Ohio River, a Section 10 Traditional Navigable Water, it would likely be considered a Water of the U.S.

**UNT to Whitewater River**

The USGS topographic map identifies UNT to Whitewater River as a dashed (intermittent) blue-line stream. UNT to Whitewater River flows under the SR 46 bridge and generally flows from northeast to the south. According to the USGS *Stream Stats* application, UNT to Whitewater River exhibits an upstream drainage area of 0.50 square mile. UNT to Whitewater River has an OHWM of 11 feet wide and 1.5 feet deep, and the OHWM was measured approximately 50 feet downstream of the bridge (Appendix A-10). Approximately 340 feet of UNT to Whitewater River is present within the investigated area. The quality of UNT Whitewater River was rated poor; no riffles or pools were identified within the investigated area, and the substrate consisted of gravel, cobble, silt, and detritus. There was tree cover present on both banks of UNT to Whitewater River, and the water was slow flowing. In some portions of UNT to Whitewater River, no flow was present. The Cowardin classification for UNT to Whitewater River is riverine, intermittent, unconsolidated bottom (R4UB). Because the UNT to the Whitewater River flows to the Whitewater River which flows to the Great Miami River, which flows to the Ohio River, a Section 10 Traditional Navigable Water, it would likely be considered a Water of the U.S.

**Table 2: Stream Summary**

Water Feature Name	Photos	Lat/Long	OHWM Width (ft)	OHWM Depth (ft)	USGS Blue-line? Type?	Quality	Substrate	Riffles/ Pools?	Likely Water of US?
Whitewater River	3-6	39.27989°N -84.87452°W	200	9	Yes, perennial	Average	Sand, cobble	R: Yes P: Yes	Yes
UNT to Whitewater River	9-12	39.280178°N -84.872881°W	11	1.5	Yes, intermittent	Poor	Silt, cobble, gravel, detritus	R: No P: N	Yes

### Wetlands

The *US Army Corps of Engineers (USACE) 1987 Wetland Delineation Manual* and the *2012 Regional Supplement to the Corps of Engineers Wetland Delineation Manual: Midwest Region (Version 2.0)* were used to determine whether wetlands were present within the investigated area. Wetland/non-wetland determinations were made using guidance and techniques provided by the *Delineation Manual* and *Regional Supplement*, including vegetation (*National Wetland Plant List*, Lichvar et al, 2016), hydrology, and soil characteristics. Sample points were taken in areas where potential wetland hydrology and/or hydrophytic vegetation were observed. The soils observed at each sample point location were compared to the hydric soil indicators presented in the *Field Indicators of Hydric Soils in the United States*.

A total of two sample points were collected; one was collected on the north side of SR 46, and one was collected on the southside of SR 46 near the Whitewater River. An aerial photograph that illustrated the sample point locations and identified water resources is provided as Appendix A-10. Attached to this report are ground-level photographs of the investigated area and sample points (Appendix B-1 to B-11), a photo orientation map (Appendix A-11), and wetland determination data forms for the sample points.

**Table 3: Wetland Data Point Summary**

Data Point	Photos	Lat/Long	Vegetation	Soils	Hydrology	Wetland
SP1	13, 14	39.2086367 N, 84.8726295 W	Yes	No	Yes	No
SP2	7, 8	39.2797993 N, 84.8738863 W	Yes	No	Yes	No

#### Sample Point 1 (SP1)

SP1 was taken approximately 60 feet north of SR 46 in the eastern portion of the investigated area. Dominant vegetation at SP1 included American elm (*Ulmus americana*), FACW, American sycamore (*Platanus occidentalis*), FACW, Canadian honewort (*Cryptotaenia canadensis*), FAC, and downy yellow violet (*Viola pubescens*), FACU. This observation meets the vegetation criteria for a positive wetland determination, through the dominance test and the prevalence test. The soils in the test pit consisted of a clay loam with a matrix color of 10YR 3/2 (100%) from 0 to 20 inches. This observation does not meet the hydric soil criteria for a positive wetland determination. Two primary indicators (Drift Deposits (B3) and Water-Stained Leaves (B9)) and two secondary indicators (Surface Soil Cracks (B6) and FAC-Neutral Test (D3)) of wetland hydrology were observed. This observation meets the hydrology criterion for a positive wetland determination. SP1 had sufficient hydrology and hydrophytic vegetation but lacked hydric soil indicators and therefore was designated as an upland point.

#### Sample Point 2 (SP2)

SP2 was taken approximately 90 feet south of the bridge to investigate an area within a mapped NWI wetland. Dominant vegetation at SP2 included American elm (*Ulmus americana*), FACW, Canada goldenrod (*Solidago canadensis*), FACU, and spotted lady's thumb (*Persicaria maculosa*), FACW. This observation meets the vegetation criteria for a positive wetland determination with the dominance test and prevalence test. The soils in the test pit consisted of a sand with a matrix color of 10YR 5/4 (100%) from 0 to 20 inches. This observation does not meet the hydric soil criteria for a positive wetland determination. Two primary indicators (Drift Deposits (B3)) and Water-Stained Leaves (B9) and one secondary indicator (FAC-Neutral Test (D3)) of wetland hydrology were observed. This observation meets the hydrology criterion for a positive wetland determination. SP2 had sufficient hydrology and hydric vegetation indicators but lacked hydric soil indicators and therefore was designated as an upland point. SP2 was taken in the area corresponding with the mapped PFO1A NWI wetland within the southeast portion of the investigated; however, since SP2 did not contain hydric soils, this area was not delineated as a wetland.

### Other Features

The investigated area was also surveyed for other aquatic features, including roadside ditches and open water bodies. Any features identified were evaluated for potential jurisdictional status.

#### Roadside Ditches

No roadside ditches were observed within the investigated area.

#### Open Water

No lakes, ponds, or other bodies of open water were observed in the investigated area.

#### **Conclusions**

BLN conducted a field investigation for wetlands and Waters of the U.S. at the investigated area on July 22 and 28, 2021. The investigation identified two waterways (Whitewater River and UNT to Whitewater River) within the investigated area. Whitewater River and UNT to Whitewater River are likely Waters of the U.S. Every effort should be taken to avoid and minimize impacts to the waterways. If impacts are necessary, mitigation may be required. The INDOT Environmental Services Division should be contacted immediately if impacts will occur. The final determination of jurisdictional waters is ultimately made by USACE. This report is our best judgment based on the guidelines set forth by USACE.

#### **Acknowledgement**

This waters determination has been prepared based on the best available information, interpreted in the light of the investigator's training, experience, and professional judgement, in conformance with the 1987 *Corps of Engineers Wetlands Delineation Manual*, the appropriate regional supplement, the USACE *Jurisdictional Determination Form Instructional Guidebook*, and other appropriate agency guidelines.



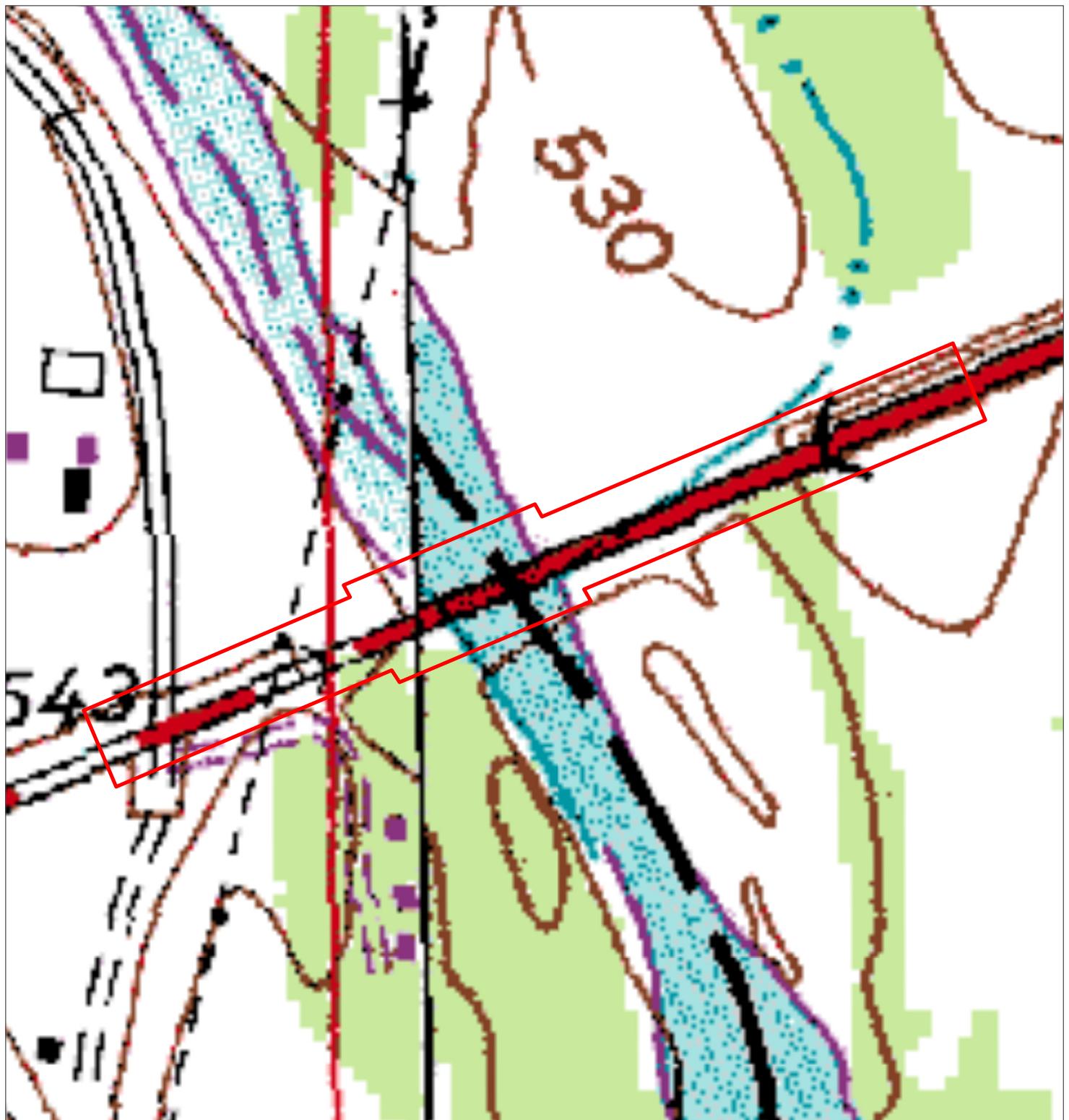
12/16/2021

Hillary Shaffer  
Sr. Environmental Analyst  
Beam, Longest and Neff, LLC

#### **Attached Documents**

- Graphics (Appendix A 1-10, B-1)
- Ground Level Photographs
- Wetland Data Sheets

**Select maps and the ground level photos have been removed to reduce duplication. Please refer to Appendix B for maps and photos.**



Source: U.S. Geological Survey  
1:3,000  
1 in = 250 ft

**BLN**  
BEAM·LONGEST·NEFF

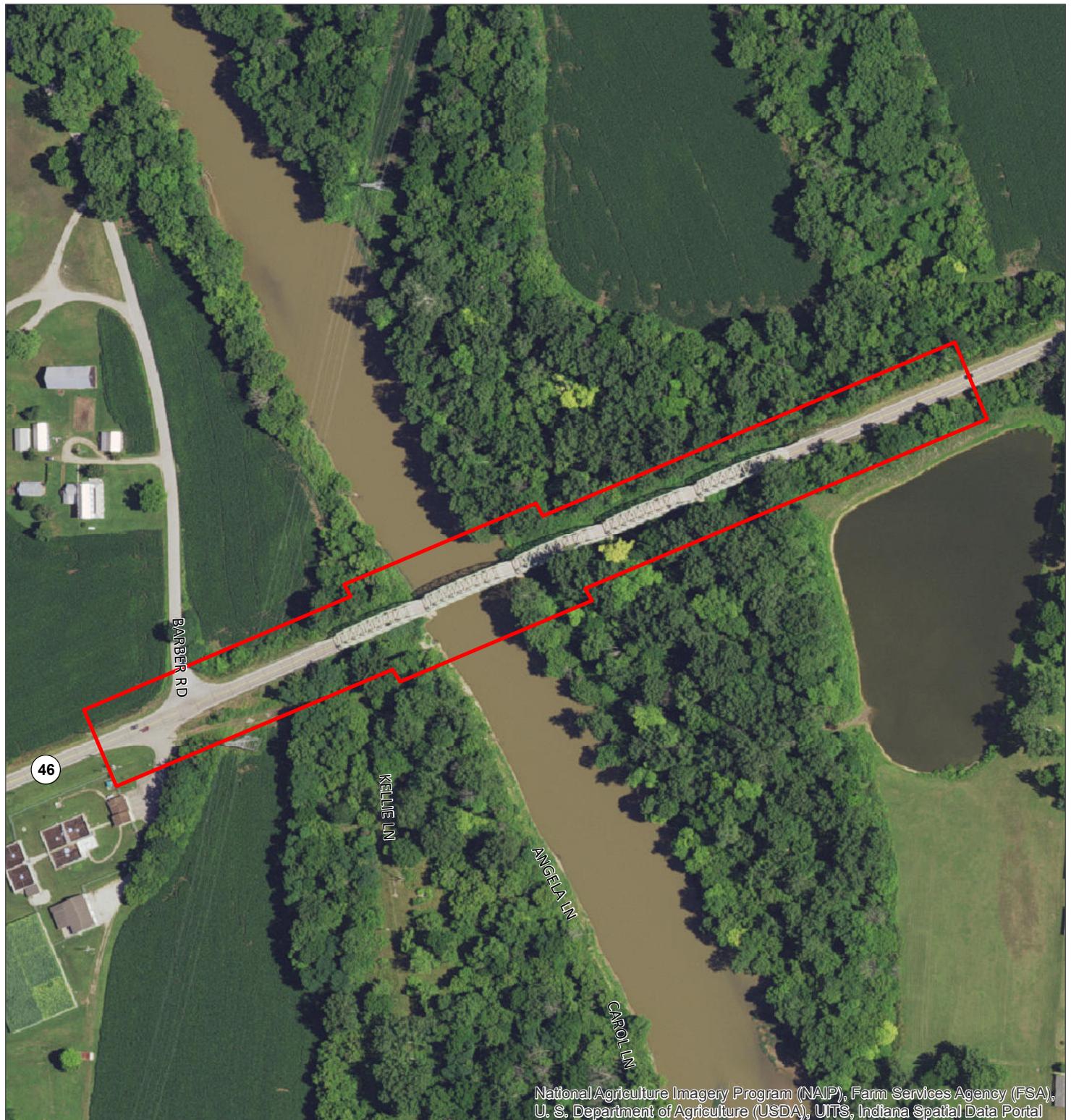
**USGS Topographic Map**  
Cedar Grove and Harrison Quadrangles - 7.5 Minute Series  
Author: Hillary Shaffer  
Bridge Project  
SR 46 over Whitewater River  
Dearborn County, Indiana  
Des. No. 1383721

A-3

**Legend**

Investigated Area

October 25, 2021



Source: Indiana MAP

1:3,000

1 in = 250 ft

**BLN**  
BEAM LONGEST NEFF

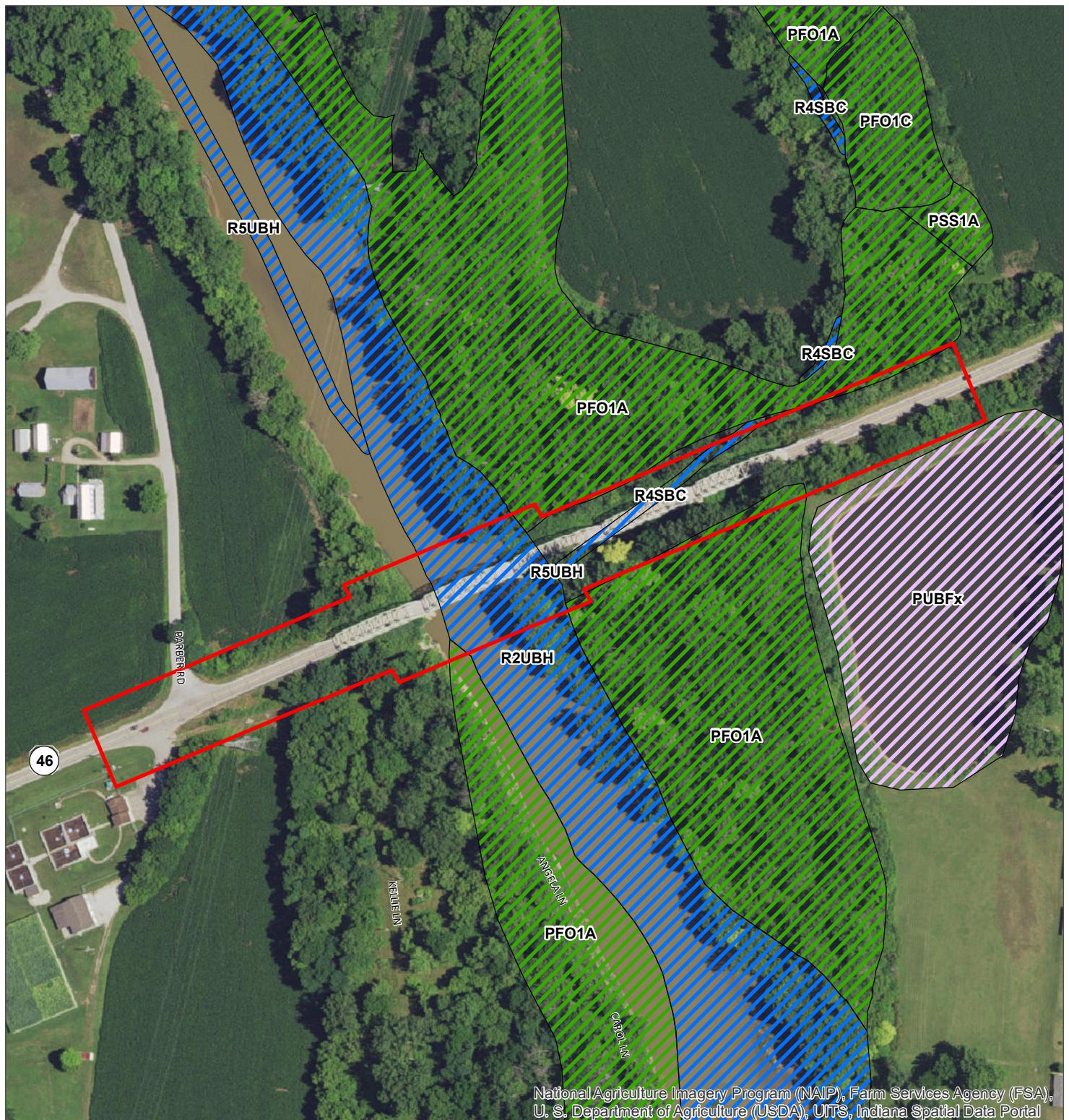
Legend

Investigated Area

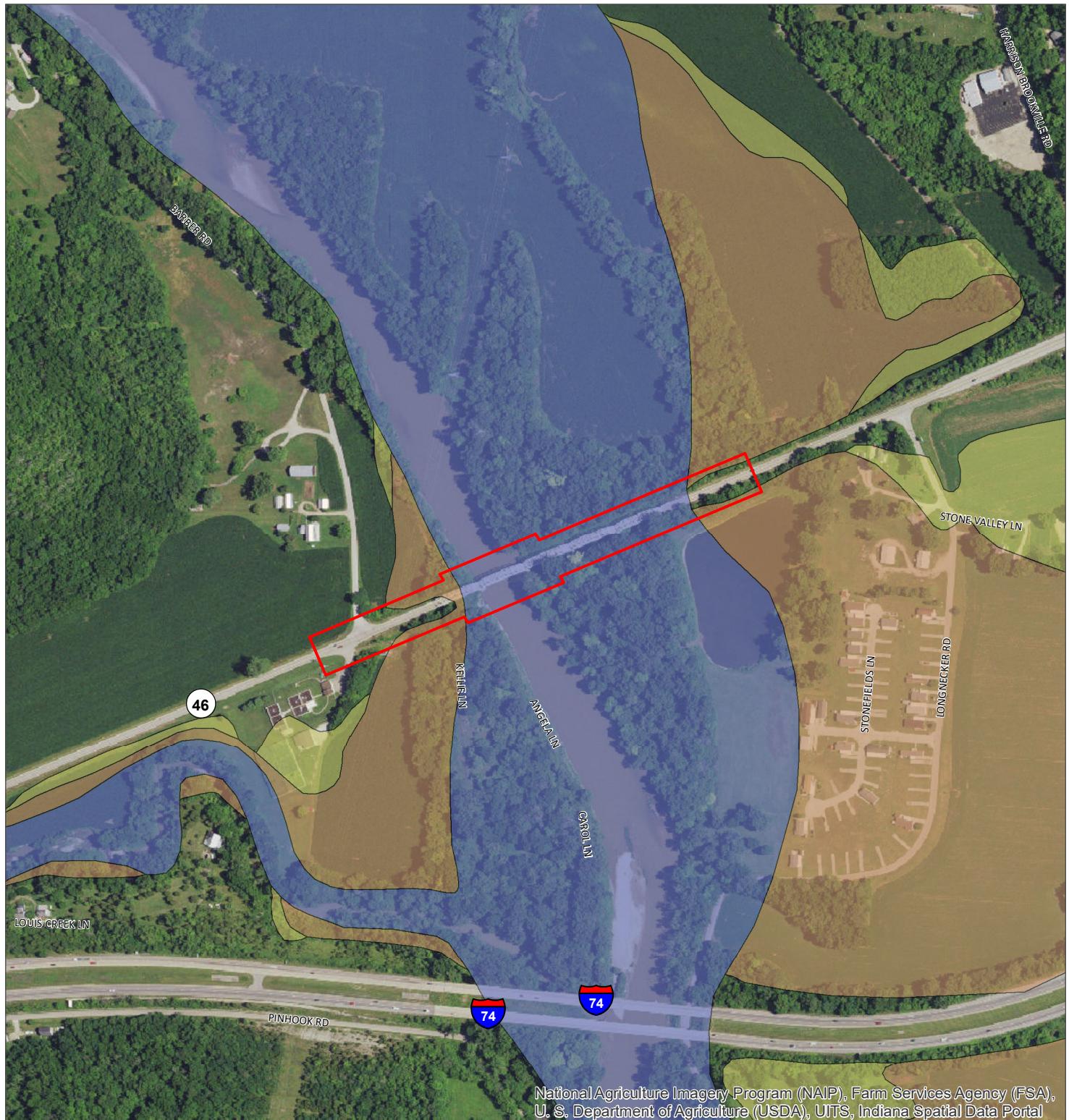
**Aerial Photograph**  
Author: Hillary Shaffer  
Bridge Project  
SR 46 over Whitewater River  
Dearborn County, Indiana  
Des. No. 1383721

A-4

October 25, 2021



**National Wetlands Inventory Map**  
Author: Hillary Shaffer  
Bridge Project  
SR 46 over Whitewater River  
Dearborn County, Indiana  
Des. No. 1383721



Source: Federal Emergency Management Agency Flood Insurance Rate Map  
 1:6,000  
 1 in = 500 ft

**BLN**  
 BEAM LONGEST NEFF

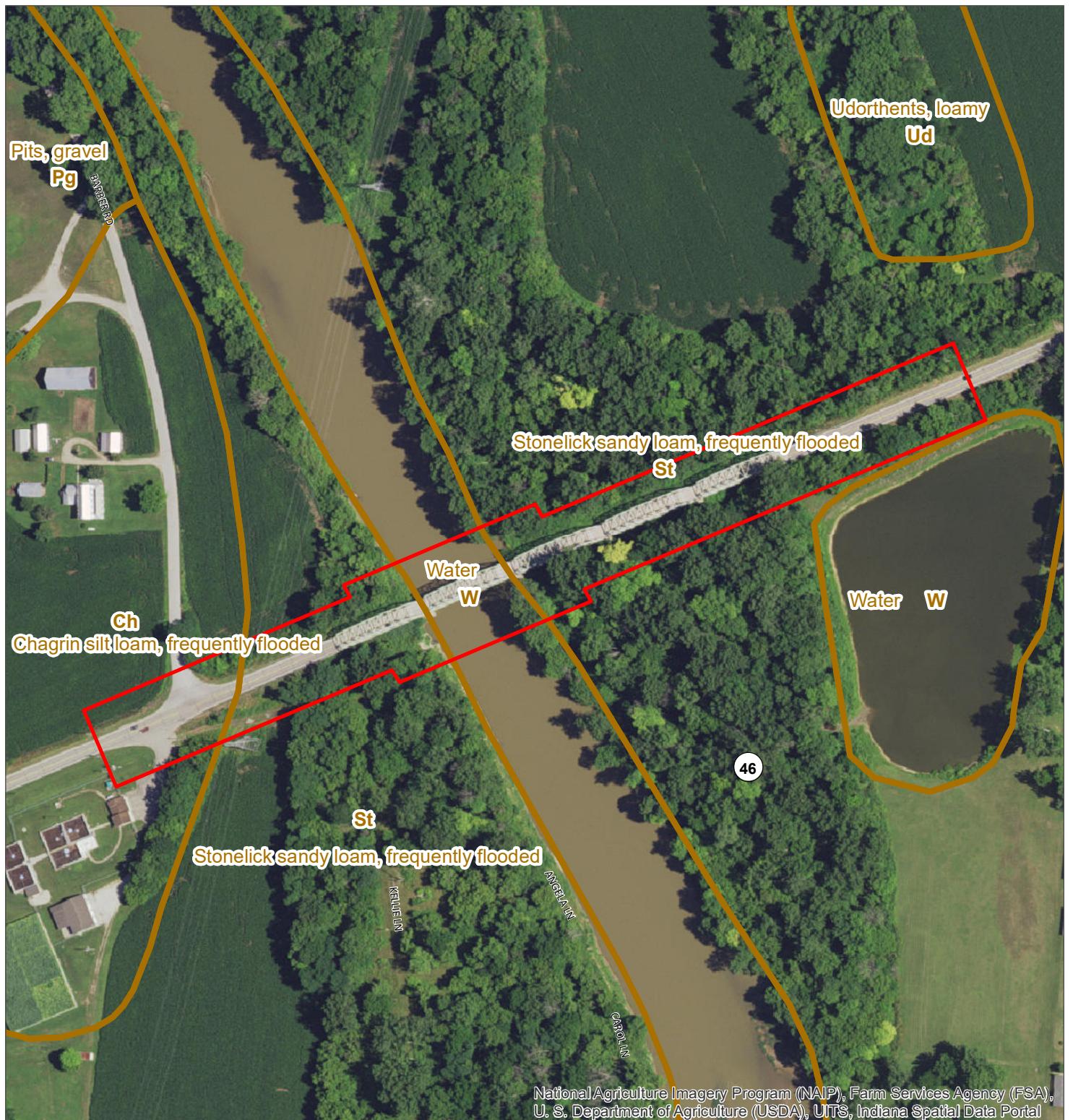
Legend

- Floodway
- 1 % Annual Chance Flood Hazard
- 0.2 % Annual Chance Flood Hazard

October 22, 2021

**Floodplain Map**

Author: Hillary Shaffer  
 Bridge Project  
 SR 46 over Whitewater River  
 Dearborn County, Indiana  
 Des. No. 1383721



Source: Natural Resources Conservation Service, U.S. Department of Agriculture

1:3,000

1 in = 250 ft

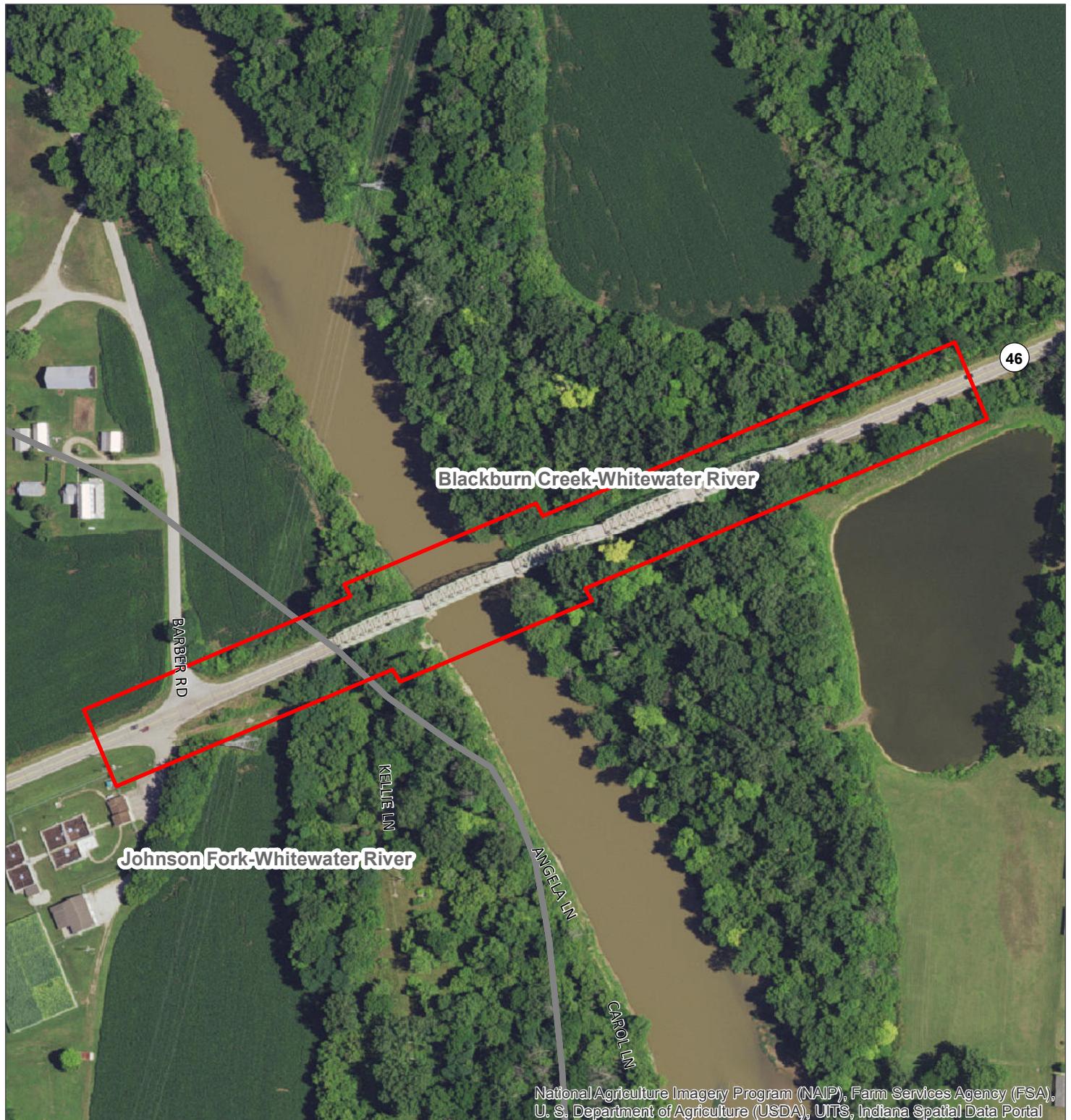
**BLN**  
BEAM LONGEST NEFF

Legend

- Investigated Area
- Soils

**Soil Survey Map**

Author: Hillary Shaffer  
Bridge Project  
SR 46 over Whitewater River  
Dearborn County, Indiana  
Des. No. 1383721  
A-8



Source: Indiana Department of Environmental Management  
1:3,000  
1 in = 250 ft

**BLN**  
BEAM LONGEST NEFF

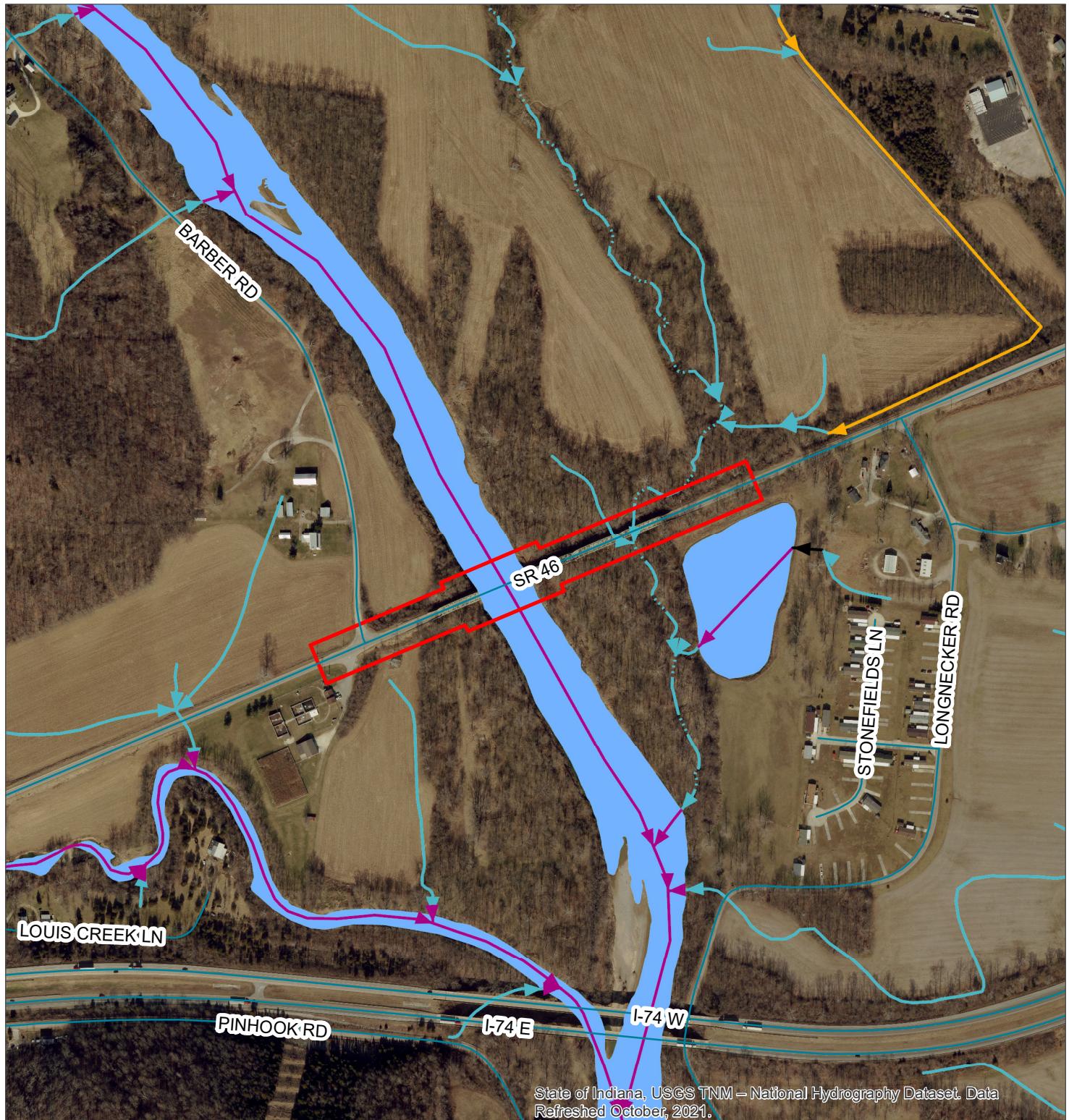
Legend

- Investigated Area
- Watershed - HUC 12

**Watershed Map**  
Author: Hillary Shaffer  
Bridge Project  
SR 46 over Whitewater River  
Dearborn County, Indiana  
Des. No. 1383721

A-9

October 25, 2021



N  
Source: Indiana Map  
E  
1:6,000  
W  
S  
1 in = 500 ft

Legend	
	Investigated Area
<b>Flowline - Small Scale</b>	
	Perennial
	Intermittent
	Artificial Path
	Canal Ditch
	Coastline
	Pipeline
	Connector

October 22, 2021

**BLN**  
BEAM LONGEST NEFF

**National Hydrography Dataset Map**  
Author: Hillary Shaffer  
Bridge Project  
SR 46 over Whitewater River  
Dearborn County, Indiana  
Des. No. 1383721  
A-10

## WETLAND DETERMINATION DATA FORM – Midwest Region

Project/Site: SR 46 over Whitewater River City/County: Dearborn County Sampling Date: 2021-09-02  
Applicant/Owner: INDOT Seymour District State: IN Sampling Point: SP 1  
Investigator(s): Aimee Cooper, Kayla Swoveland Section, Township, Range: \_\_\_\_\_  
Landform (hillslope, terrace, etc.): Floodplain Local relief (concave, convex, none): Concave  
Slope (%): 0-5 Lat: 39.2806367 Long: -84.8726295 Datum: WGS 84  
Soil Map Unit Name: St, Stonelick sandy loam NWI classification: PFO1A

Are climatic / hydrologic conditions on the site typical for this time of year? Yes  No  (If no, explain in Remarks.)

Are Vegetation \_\_\_\_\_, Soil \_\_\_\_\_, or Hydrology \_\_\_\_\_ significantly disturbed? Are "Normal Circumstances" present? Yes  No

Are Vegetation \_\_\_\_\_, Soil \_\_\_\_\_, or Hydrology \_\_\_\_\_ naturally problematic? (If needed, explain any answers in Remarks.)

**SUMMARY OF FINDINGS – Attach site map showing sampling point locations, transects, important features, etc.**

Hydrophytic Vegetation Present?	Yes <input checked="" type="checkbox"/>	No <input type="checkbox"/>	Is the Sampled Area within a Wetland?	Yes <input type="checkbox"/> No <input checked="" type="checkbox"/>
Hydric Soil Present?	Yes <input type="checkbox"/>	No <input checked="" type="checkbox"/>		
Wetland Hydrology Present?	Yes <input checked="" type="checkbox"/>	No <input type="checkbox"/>		
Remarks:				

**VEGETATION** – Use scientific names of plants.

Tree Stratum (Plot size: <u>30 ft r</u> )		Absolute % Cover	Dominant Species?	Indicator Status	Dominance Test worksheet:	
1. <u>Ulmus americana</u>		50	✓	FACW	Number of Dominant Species That Are OBL, FACW, or FAC: <u>3</u> (A)	
2. <u>Platanus occidentalis</u>		40	✓	FACW	Total Number of Dominant Species Across All Strata: <u>4</u> (B)	
3. <u>Robinia pseudoacacia</u>		5		FACU	Percent of Dominant Species That Are OBL, FACW, or FAC: <u>75</u> (A/B)	
4. _____					Prevalence Index worksheet:	
5. _____					Total % Cover of:	
Sapling/Shrub Stratum (Plot size: <u>15 ft r</u> )		95%	= Total Cover			Multiply by:
1. _____					OBL species <u>20</u>	$\times 1 =$ <u>20</u>
2. _____					FACW species <u>90</u>	$\times 2 =$ <u>180</u>
3. _____					FAC species <u>60</u>	$\times 3 =$ <u>180</u>
4. _____					FACU species <u>55</u>	$\times 4 =$ <u>220</u>
5. _____					UPL species <u>0</u>	$\times 5 =$ <u>0</u>
					Column Totals: <u>225</u>	(A) <u>600</u> (B)
Herb Stratum (Plot size: <u>5 ft r</u> )					Prevalence Index = B/A = <u>2.7</u>	
1. <u>Cryptotaenia canadensis</u>		60	✓	FAC	Hydrophytic Vegetation Indicators:	
2. <u>Viola pubescens</u>		40	✓	FACU	1 - Rapid Test for Hydrophytic Vegetation	
3. <u>Boehmeria cylindrica</u>		20		OBL	<input checked="" type="checkbox"/> 2 - Dominance Test is >50%	
4. <u>Solidago canadensis</u>		10		FACU	<input type="checkbox"/> 3 - Prevalence Index is $\leq 3.0^1$	
5. _____					<input type="checkbox"/> 4 - Morphological Adaptations <sup>1</sup> (Provide supporting data in Remarks or on a separate sheet)	
6. _____					<input type="checkbox"/> Problematic Hydrophytic Vegetation <sup>1</sup> (Explain)	
7. _____						
8. _____						
9. _____						
10. _____						
Woody Vine Stratum (Plot size: <u>30 ft r</u> )		130%	= Total Cover			
1. _____						
2. _____						
Remarks: (Include photo numbers here or on a separate sheet.)						Hydrophytic Vegetation Present? Yes <input checked="" type="checkbox"/> No <input type="checkbox"/>



## WETLAND DETERMINATION DATA FORM – Midwest Region

Project/Site: SR 46 over Whitewater River City/County: Dearborn County Sampling Date: 2021-09-02  
 Applicant/Owner: INDOT Seymour District State: IN Sampling Point: SP2  
 Investigator(s): Aimee Cooper, Kayla Swoveland Section, Township, Range: \_\_\_\_\_  
 Landform (hillslope, terrace, etc.): Floodplain Local relief (concave, convex, none): Concave  
 Slope (%): 0-5 Lat: 39.2797993 Long: -84.8738863 Datum: WGS 84  
 Soil Map Unit Name: St. Stonelick sandy loam NWI classification: PFO1A

Are climatic / hydrologic conditions on the site typical for this time of year? Yes        No ✓ (If no, explain in Remarks.)

Are Vegetation       , Soil       , or Hydrology        significantly disturbed? Are "Normal Circumstances" present? Yes        No ✓

Are Vegetation       , Soil       , or Hydrology        naturally problematic? (If needed, explain any answers in Remarks.)

### **SUMMARY OF FINDINGS – Attach site map showing sampling point locations, transects, important features, etc.**

Hydrophytic Vegetation Present?	Yes <u>✓</u> No <u>      </u>	Is the Sampled Area within a Wetland?
Hydric Soil Present?	Yes <u>      </u> No <u>✓</u>	Yes <u>      </u> No <u>✓</u>
Wetland Hydrology Present?	Yes <u>✓</u> No <u>      </u>	
Remarks:		

### **VEGETATION – Use scientific names of plants.**

Tree Stratum (Plot size: <u>30 ft r</u> )	Absolute % Cover	Dominant Species?	Indicator Status	Dominance Test worksheet:
1. <u>Ulmus americana</u>	<u>60</u>	<u>✓</u>	<u>FACW</u>	Number of Dominant Species That Are OBL, FACW, or FAC: <u>2</u> (A)
2. <u>Acer saccharinum</u>	<u>10</u>		<u>FACW</u>	Total Number of Dominant Species Across All Strata: <u>3</u> (B)
3. <u>Platanus occidentalis</u>	<u>10</u>		<u>FACW</u>	Percent of Dominant Species That Are OBL, FACW, or FAC: <u>67</u> (A/B)
4. _____	_____	_____	_____	Prevalence Index worksheet:
5. _____	_____	_____	_____	Total % Cover of: _____ Multiply by: _____
80% = Total Cover				OBBL species <u>0</u> x 1 = <u>0</u>
= Total Cover				FACW species <u>130</u> x 2 = <u>260</u>
= Total Cover				FAC species <u>0</u> x 3 = <u>0</u>
= Total Cover				FACU species <u>80</u> x 4 = <u>320</u>
= Total Cover				UPL species <u>0</u> x 5 = <u>0</u>
= Total Cover				Column Totals: <u>210</u> (A) <u>580</u> (B)
				Prevalence Index = B/A = <u>2.8</u>
				Hydrophytic Vegetation Indicators:
				— 1 - Rapid Test for Hydrophytic Vegetation
				✓ 2 - Dominance Test is >50%
				— 3 - Prevalence Index is ≤3.0 <sup>1</sup>
				— 4 - Morphological Adaptations <sup>1</sup> (Provide supporting data in Remarks or on a separate sheet)
				— Problematic Hydrophytic Vegetation <sup>1</sup> (Explain)
				<sup>1</sup> Indicators of hydric soil and wetland hydrology must be present, unless disturbed or problematic.
				Hydrophytic Vegetation Present? Yes <u>✓</u> No <u>      </u>
Remarks: (Include photo numbers here or on a separate sheet.)				



## **PRELIMINARY JURISDICTIONAL DETERMINATION (PJD) FORM**

### **BACKGROUND INFORMATION**

#### **A. REPORT COMPLETION DATE FOR PJD:**

October 21, 2021

#### **B. NAME AND ADDRESS OF PERSON REQUESTING PJD:**

Hillary Shaffer  
Beam, Longest and Neff, LLC  
8320 Craig Street  
Indianapolis, Indiana 46250  
317-849-5832

#### **C. DISTRICT OFFICE, FILE NAME, AND NUMBER:**

#### **D. PROJECT LOCATION(S) AND BACKGROUND INFORMATION:**

Des No. 1383721 consists of a bridge rehabilitation for Bridge No. 046-15-01987A, a historic bridge on SR 46 over the Whitewater River in Dearborn County, Indiana, located 0.44 mile west of US 52. The NBI No. is 017540, and the bridge is eligible for inclusion in the National Register of Historic Places (NRHP) but is "Non-Select". Specifically, the project is located in Sections 9&10, Township 7 North, Range 1 West in Dearborn County, Indiana on the 7.5 minute Cedar Grove and Harrison Indiana USGS quadrangle map. No right-of-way is anticipated for this project. The maintenance of traffic will consist of a full closure with a 1.5-mile long detour route utilizing US 52, I-74 and SR 1 that is anticipated to last six months.

#### **(USE THE TABLE BELOW TO DOCUMENT MULTIPLE AQUATIC RESOURCES AND/OR AQUATIC RESOURCES AT DIFFERENT SITES)**

State: Indiana County: Dearborn City: Seymour  
Center coordinates of site (lat/long in degree decimal format):  
Lat. 36.280039 N Long. 84.874028 W  
Universal Transverse Mercator: 16N  
Name of nearest waterbody: Whitewater River

#### **E. REVIEW PERFORMED FOR SITE EVALUATION (CHECK ALL THAT APPLY):**

- Office (Desk) Determination. Date: October 21, 2021
- Field Determination. Date(s): September 2, 2021

**TABLE OF AQUATIC RESOURCES IN REVIEW AREA WHICH “MAY BE” SUBJECT TO REGULATORY JURISDICTION**

Site Number	Latitude (decimal degrees)	Longitude (decimal degrees)	Estimated amount of aquatic resource in review area (acreage and linear feet, if applicable)	Type of aquatic resource (i.e., wetland vs. non-wetland waters)	Geographic authority to which the aquatic resource “may be” subject (i.e., Section 404 or Section 10/404)
Whitewater River	39.27989 N	84.87452 W	210 linear feet	non-wetland waters	CWA Section 404
UNT to White-water River	39.280178 N	84.872881W	340 linear feet	non-wetland waters	CWA Section 404

1. The Corps of Engineers believes that there may be jurisdictional aquatic resources in the review area, and the requestor of this PJD is hereby advised of his or her option to request and obtain an approved JD (AJD) for that review area based on an informed decision after having discussed the various types of JDs and their characteristics and circumstances when they may be appropriate.
2. In any circumstance where a permit applicant obtains an individual permit, or a Nationwide General Permit (NWP) or other general permit verification requiring “pre-construction notification” (PCN), or requests verification for a non-reporting NWP or other general permit, and the permit applicant has not requested an AJD for the activity, the permit applicant is hereby made aware that: (1) the permit applicant has elected to seek a permit authorization based on a PJD, which does not make an official determination of jurisdictional aquatic resources; (2) the applicant has the option to request an AJD before accepting the terms and conditions of the permit authorization, and that basing a permit authorization on an AJD could possibly result in less compensatory mitigation being required or different special conditions; (3) the applicant has the right to request an individual permit rather than accepting the terms and conditions of the NWP or other general permit authorization; (4) the applicant can accept a permit authorization and thereby agree to comply with all the terms and conditions of that permit, including whatever mitigation requirements the Corps has determined to be necessary; (5) undertaking any activity in reliance upon the subject permit authorization without requesting an AJD constitutes the applicant’s acceptance of the use of the PJD; (6) accepting a permit authorization (e.g., signing a proffered individual permit) or undertaking any activity in reliance on any form of Corps permit authorization based on a PJD constitutes agreement that all aquatic resources in the review area affected in any way by that activity will be treated as jurisdictional, and waives any challenge to such jurisdiction in any administrative or judicial compliance or enforcement action, or in any administrative appeal or in any Federal court; and (7) whether the applicant elects to use either an AJD or a PJD, the JD will be processed as soon as practicable. Further, an AJD, a proffered individual permit (and all terms and conditions contained therein), or individual permit denial can be administratively appealed pursuant to 33 C.F.R. Part 331. If, during an administrative appeal, it becomes appropriate to make an official determination whether geographic jurisdiction exists over aquatic resources in the review area, or to provide an official delineation of jurisdictional aquatic resources in the review area, the Corps will provide an AJD to accomplish that result, as soon as is practicable. This PJD finds that there “*may be*” waters of the U.S. and/or that there “*may be*” navigable waters of the U.S. on the subject review area, and identifies all aquatic features in the review area that could be affected by the proposed activity, based on the following information:

**SUPPORTING DATA. Data reviewed for PJD (check all that apply)**

Checked items should be included in case file. Appropriately reference sources below where indicated for all checked items:

- Maps, plans, plots or plat submitted by or on behalf of the PJD requestor. Figures 1-9.
- Data sheets prepared/submitted by or on behalf of the PJD requestor.
  - Office concurs with data sheets/delineation report.
  - Office does not concur with data sheets/delineation report. Rationale:
- Data sheets prepared by the Corps.
- Corps navigable waters' study.
- U.S. Geological Survey Hydrologic Atlas.
  - USGS NHD data.
  - USGS 8 and 12 digit HUC maps.
- U.S. Geological Survey map(s). Cite scale & quad name: USGS 7.5' Cedar Grove and Harrison Quadrangles
- Natural Resources Conservation Service Soil Survey. Citation: SSURGO.
- National wetlands inventory map(s). Cite name: NWI, USFWS.
- State/Local wetland inventory map(s).
- FEMA/FIRM maps.
- 100-year Floodplain Elevation is: N/A (project is not in a floodplain).
- Photographs:  Aerial (Name & Date): State of Indiana Best Available Orthophotography various years 2016-2019 or  Other (Name & Date): Ground-Level Photos, September 2, 2021
- Previous determination(s). File no. and date of response letter:
- Other information (please specify):

**IMPORTANT NOTE: The information recorded on this form has not necessarily been verified by the Corps and should not be relied upon for later jurisdictional determinations.**

Hillary E Shaffer

Digitally signed by Hillary E Shaffer  
DN: C=US, E=hillaryshaffer@b-in.com, CN=Hillary E Shaffer  
Reason: I am the author of this document  
Date: 2021.10.21 17:51:40-04'00

October 21, 2021

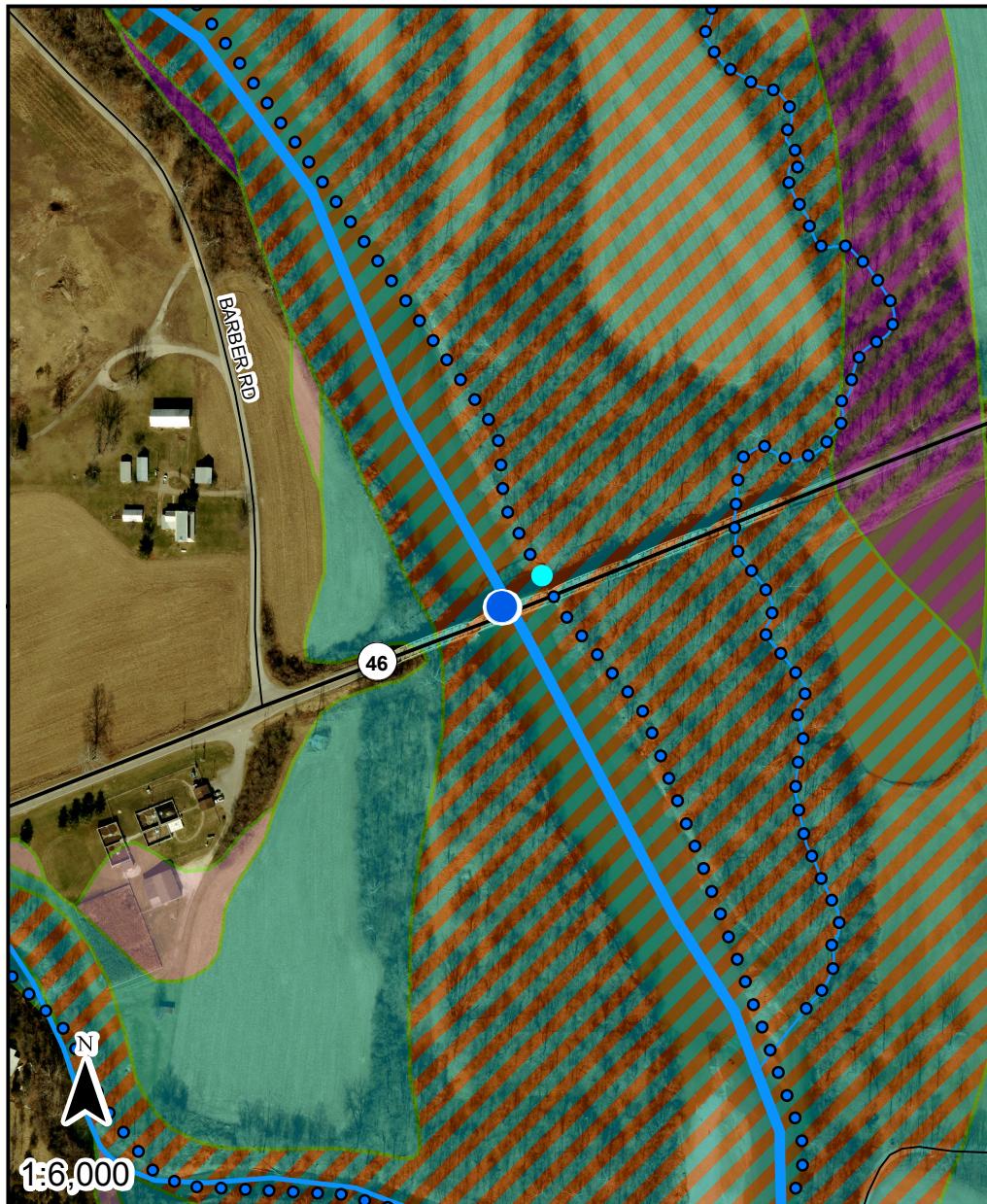
---

Signature and date of Regulatory staff member  
completing PJD

---

Signature and date of person requesting PJD  
(REQUIRED, unless obtaining the signature is  
impracticable)<sup>1</sup>

<sup>1</sup> Districts may establish timeframes for requestor to return signed PJD forms. If the requestor does not respond within the established time frame, the district may presume concurrence and no additional follow up is necessary prior to finalizing an action.



Point of Interest Coordinates  
(WGS84)

Long: -84.8746688103

Lat: 39.2798884612

***The information provided below is based on the point of interest shown in the map above.***

County: Dearborn

Approximate Ground Elevation: 517.0 feet (NAVD88)

Stream Name:

Whitewater River

Base Flood Elevation: 534.0 feet (NAVD88)

Drainage Area: Not available

Best Available Flood Hazard Zone: **FEMA Zone AE Floodway**

National Flood Hazard Zone: **FEMA Zone AE Floodway**

Is a Flood Control Act permit from the DNR needed for this location? **yes**

Is a local floodplain permit needed for this location? **yes-**

Floodplain Administrator: **Nicole Daily, Zoning Administrator**

Community Jurisdiction: **Dearborn County, County proper**

Phone: **(812) 537-8821**

Email: **ndaily@dearborncounty.in.gov**

US Army Corps of Engineers District: **Louisville**

Date Generated: 7/21/2022

# **Appendix G:**

# **Public Involvement**

**NOTICE OF SURVEY**  
September 19, 2019**Property Owner Address**

Re: SR 46 Bridge Rehab  
West Harrison, Indiana  
Des. No. 1383721

Dear Property Owner:

Our information indicates that you own or occupy property near this proposed bridge rehab project. Our employees will be doing a survey of the project area in the near future. It may be necessary for them to come onto your property to complete this work. This is permitted by law per Indiana Code IC 8-23-7-26. They will show you their identification, if you are available, before coming onto your property. If you have sold this property, or it is occupied by someone else, please let us know the name and address of the new owner or current occupant so we can contact them about the survey.

At this stage, we generally do not know what effect, if any, our project may eventually have on your property. If it is determined that your property is involved, you will be contacted with additional information.

The survey work will include mapping the location of features such as streets, utilities, buildings, trees, fences, drives, creeks, ditches and property corners. This work is needed for the proper planning and design of this bridge rehab project. Please be assured of our sincere desire to cause you as little inconvenience as possible during this survey. If any problems do occur, please speak to our field crew or contact me at the telephone number or address shown above.

Sincerely,

BEAM, LONGEST AND NEFF, L.L.C.



Edward J. Sweetland, PS  
Survey Department Manager  
xc: File 190029

# **Appendix H:**

# **Air Quality**



# INDIANA DEPARTMENT OF TRANSPORTATION

100 North Senate Avenue  
Room N758-Executive Office  
Indianapolis, Indiana 46204

PHONE: (855) 463-6848

**Eric Holcomb, Governor**  
**Michael Smith, Commissioner**

April 26, 2022

Mr. Jermaine R. Hannon, Division Administrator  
FHWA Indiana Division  
575 North Pennsylvania St., Room 254  
Indianapolis, IN 46204

Ms. Kelley Brookins, Regional Administrator  
FTA Region 5  
200 West Adams St.  
Suite 320  
Chicago, IL 60606-5253

Dear Mr. Hannon /Ms. Brookins:

The Indiana Department of Transportation is pleased to submit its Draft FY 2022-2026 Statewide Transportation Improvement Program (STIP) for review and comment by your offices.

Included in the final submitted document is a listing of the state's expansion/preservation and local small urban and rural and rural transit projects. The following Metropolitan Planning Organization TIP's will be included in the FY 2022-2026 STIP by reference, pending FHWA approval in May 2022.

Area Plan Commission of Tippecanoe County (APCTC)	FY 2022-2026
• <i>Version 3/10/2022</i>	
Bloomington-Monroe County Metropolitan Planning Organization (BMCMPO)	FY 2022-2026
• <i>Version 3/11/2022</i>	
Columbus Area Metropolitan Planning Organization (CAMPO)	FY 2022-2026
• <i>Version 3/22/2021</i>	
Delaware-Muncie Metropolitan Plan Commission (DMMPC)	FY 2022-2025
• <i>Version 12/15/2021</i>	
Evansville Metropolitan Planning Organization (EMPO)	FY 2022-2026
• <i>Version 3/10/2022</i>	
Kokomo-Howard County Governmental Coordinating Council (KHGCC)	FY 2022-2026
• <i>Version 3/10/2022</i>	
Kentuckiana Regional Planning and Development Agency (KIPDA)	FY 2020-2025
• <i>Version 3/29/2022</i>	
Indianapolis Metropolitan Planning Organization (IMPO)	FY 2022-2025
• <i>Version 8/18/2021</i>	
Michiana Area Council of Governments (MACOG)	FY 2022-2026
• <i>Version 3/09/2022</i>	

Madison County Council of Governments (MCCOG)	FY 2022-2026
• <i>Version 7/13/2021</i>	
Northeastern Indiana Regional Coordinating Council (NIRCC)	FY 2022-2026
• <i>Version 3/28/2022</i>	
Northwestern Indiana Regional Planning Commission (NIRPC)	FY 2022-2026
• <i>Version 3/17/2022</i>	
Ohio-Kentucky-Indiana Regional Council of Governments (OKI)	FY 2020-2023
• <i>Version 03/10/2022</i>	
Terre Haute Area Metropolitan Planning Organization (THAMPO)	FY 2020-2024
• <i>Version 08/26/2021</i>	

In addition, INDOT has expanded our public involvement process by taking advantage of virtual meeting techniques and allowing accessibility to online documents, materials, virtual meeting registration, recorded virtual meetings, and comment forms. INDOT also leveraged our planning partner contacts (MPOs, RPOs, LTAP), social media, and notifications sent to local libraries, housing authorities, senior aging centers, and local newspapers across the state.

We greatly appreciate FHWA/FTA support in the development of the STIP 2022-2026 and look forward to working together to achieve our mutual goals. Should you have any questions pertaining to this amendment, please contact Michael McNeil, STIP Specialist at 317-232-0223 or at [mmcneil@indot.in.gov](mailto:mmcneil@indot.in.gov).

Sincerely,



Michael Smith, Commissioner  
Indiana Department of Transportation

cc: (w/enclosure): FTA

Michelle Allen, FHWA  
Jeffrey Brooks, INDOT  
Kristin Brier, INDOT  
Kathy Eaton-McKalip, INDOT  
Louis Feagans, INDOT  
Roy Nunnally, INDOT  
Larry Buckel, INDOT  
Jay Mitchell, INDOT  
Jason Casteel, INDOT  
Michael McNeil, INDOT



**Federal Transit Administration**  
Region V  
200 West Adams St., Suite 320  
Chicago, IL 60606-5253

**U.S. Department  
of Transportation**

**Federal Highway Administration**  
Indiana Division  
575 N. Pennsylvania St., Rm 254  
Indianapolis, IN 46204-1576

June 17, 2022

Mr. Michael Smith  
Commissioner  
Indiana Department of Transportation  
100 N Senate Ave. N955  
Indianapolis, IN 46204

**SUBJECT:** Indiana FY2022-2026 STIP Approval and Associated Federal Planning Finding

Dear Mr. Smith:

The Federal Highway Administration (FHWA) and the Federal Transit Administration (FTA) have completed our review of the FY2022-2026 Indiana Statewide Transportation Improvement Program (INSTIP), which was submitted by the INDOT request letter dated April 27, 2022.

Based on our review of the information provided, certifications of the Statewide and Metropolitan transportation planning processes for and within the state of Indiana, and our participation in those transportation planning processes (including planning certification reviews conducted in Transportation Management Areas), FHWA and FTA are jointly approving the FY2022-2026 STIP, including the Metropolitan Planning Organization (MPO) Transportation Improvement Programs (TIPs) directly incorporated into the STIP, subject to the corrective actions identified in the attached Federal Planning Finding (FPF) report. FHWA and FTA consider the projects in the 5<sup>th</sup> year for informational purposes only, and our approval does not exceed four years per 23 CFR 450.220(c).

FHWA and FTA are required under 23 CFR 450.220(b) to document and issue an FPF in conjunction with the approval of the FY2022-2026 STIP. At a minimum, the FPF verifies that the development of the STIP is consistent with the provisions of both the Statewide and Metropolitan transportation planning requirements. FHWA and FTA find that the Indiana FY2022-2026 STIP substantially meets the transportation planning requirements and are approving the STIP subject to the corrective actions outlined in the FPF. This approval is effective June 17, 2022, and is given with the understanding that an eligibility determination of individual projects for funding must be met, and INDOT must ensure the satisfaction of all administrative and statutory requirements, as well as address the corrective actions outlined in the attached report. FHWA and FTA will continue to partner with INDOT to ensure the previously developed action plan (attached) is implemented to address the corrective actions. If progress is not made in addressing the corrective actions, future amendments to the FY2022-2026 STIP, or adoption of the FY2024-2028 STIP, may not be approved by USDOT.

If you have questions or need additional information concerning our approval and the FPF, please contact Ms. Michelle Allen of the FHWA Indiana Division at (317) 226-7344, or by email at [michelle.allen@dot.gov](mailto:michelle.allen@dot.gov), or Mr. Jason Ciavarella of the FTA Region 5 Office at (312) 353-1653, or by email at [jason.ciavarella@dot.gov](mailto:jason.ciavarella@dot.gov).

Sincerely,

**KELLEY**  
**BROOKINS**  
Digitally signed by  
KELLEY BROOKINS  
Date: 2022.06.13  
10:08:34 -05'00'

Kelley Brookins  
Regional Administrator  
FTA Region V

Sincerely,

**JERMAINE**  
**R HANNON**  
Digitally signed by  
JERMAINE R  
HANNON  
Date: 2022.06.13  
15:57:46 -04'00'

Jermaine R. Hannon  
Division Administrator  
FHWA Indiana Division

cc: (transmitted by e-mail)  
Louis Feagans, INDOT  
Roy Nunnally, INDOT  
Karen Hicks, INDOT

Attachments have been removed for the purposes of this NEPA document.

**Dearborn County****Location****Description****AQ conformity****Sponsor****Award/  
Let Date**

TIP ID	Facility	BMP	EMP	Fund Type	Phase	Pre 21	FY 21	FY 22	FY 23	FY 24	FUTR
1383721	SR 46	0.00	0.00	0.44 mile west of US 52 over Whitewater River		Bridge Deck Replacement			Exempt	INDOT	4Q23
				STP	PE	144,000	0	0	0	0	0
				State	PE	36,000	0	0	0	0	0
				STBG	RW	0	16,000	0	0	0	0
				State	RW	0	4,000	0	0	0	0
				STBG	CO	0	0	0	3,664,234	0	0
				State	CO	0	0	0	916,059	0	0
				Uncommitted	CO	0	0	0	0	0	1,641,319
				<b>Total :</b>							
				<b>\$6,421,612</b>							
1383726	SR 62			3.7 miles west of SR 262 over Hayes Branch		Bridge replacement			Exempt	INDOT	FY23
				STBG	CO	0	0	0	1,475,383	0	0
				State	CO	0	0	0	368,846	0	0
				<b>Total :</b>							
				<b>\$1,844,229</b>							
1600539	SR 46			3.2 miles east of SR 1		Slide correction			Exempt	INDOT	FY23
				STBG	RW	0	53,600	0	0	0	0
				State	RW	0	13,400	0	0	0	0
				STBG	CO	0	0	0	1,784,214	0	0
				State	CO	0	0	0	446,054	0	0
				<b>Total :</b>							
				<b>\$2,297,268</b>							
1700136	SR 1			Intersection of SR 1 and Oberting Road		New Signal Installation			Exempt	INDOT	FY22
				STP	PE	80,000	0	0	0	0	0
				State	PE	20,000	0	0	0	0	0
				STBG	RW	0	16,000	0	0	0	0
				State	RW	0	4,000	0	0	0	0
				STBG	CO	0	0	0	144,221	0	0
				State	CO	0	0	0	36,055	0	0
				<b>Total :</b>							
				<b>\$300,276</b>							
1800225	US 50	0.00	0.00	At the intersection of Front Street in Dillsboro		Intersection improvement with added turn lanes			Exempt	INDOT	4Q23
				NHPP	PE	100,000	0	0	0	0	0
				State	PE	25,000	0	0	0	0	0
				HSIP	CO	0	0	0	921,620	0	0
				State	CO	0	0	0	230,405	0	0
				<b>Total :</b>							
				<b>\$1,277,025</b>							
1801685	SR 1	0.00	0.00	3.6 miles south of I-74		Slide correction			Exempt	INDOT	4Q23
				STP	PE	80,000	0	0	0	0	0
				State	PE	20,000	0	0	0	0	0
				STBG	CO	0	0	0	228,114	0	0
				State	CO	0	0	0	57,028	0	0
				<b>Total :</b>							
				<b>\$385,142</b>							
1900348	SR 1	0.00	0.00	4.5 miles north of the junction with US 50		Slide Correction			Exempt	INDOT	FY24
				STBG	RW	0	0	40,000	0	0	0
				State	RW	0	0	10,000	0	0	0
				STBG	CO	0	0	0	832,000	0	0
				State	CO	0	0	0	208,000	0	0
				<b>Total :</b>							
				<b>\$1,090,000</b>							

TIP ID	MTP ID	Facility	Location		Description		AQ conformity	Sponsor	Award/ Let Date		
			Fund Type	Phase	FY 21	FY 22					
1900094	O/M	SR 46	0.95 miles W of US52 @ Logan Creek		Bridge deck replacement		Exempt	INDOT	1Q24		
Grouped Project: Update PE and CO amounts			STBG	RW	0	17,600	0	0	0		
			State	RW	0	4,400	0	0	0		
			STBG	PE	0	0	0	48,000	0		
			State	PE	0	0	0	12,000	0		
			STBG	CO	0	0	0	1,102,114	0		
			State	CO	0	0	0	275,528	0		
			Carroll Cropper Bridge over the Ohio River		Bridge Inspection FY23		Exempt	INDOT	FY23		
Add to Grouped Project			NHPP	PE	0	0	90,000	0	0		
			State	PE	0	0	10,000	0	0		
1900662	O/M	I-74	EB over White's Hill Road		Bridge Painting		Exempt	INDOT	FY22		
Add to Grouped Project			NHPP	CO	0	3,250,308	0	0	0		
			State	CO	0	361,145	0	0	0		
1901790	O/M	US 50	US 50 and Front St, US 50 and Arch St in Lawrenceburg		Traffic signal modernization		Exempt	INDOT	1Q24		
Update funding amounts			HSIP	RW	0	68,000	0	0	0		
			State	RW	0	17,000	0	0	0		
			HSIP	CO	0	0	0	385,600	0		
			State	CO	0	0	0	96,400	0		
1901969	O/M	US 50	Various		Traffic Signal Visibility Improvements		Exempt	INDOT	1Q22		
Grouped Project: Update funding amounts			HSIP	CO	0	560,000	0	0	0		
			State	CO	0	140,000	0	0	0		
1902019	O/M	US 50	Various locations along US 50 in Dearborn		Guard rail attenuators, new or modernize		Exempt	INDOT	3Q23		
Grouped Project: Update funding amounts			HSIP	CO	0	0	1,616,000	0	0		
			State	CO	0	0	404,000	0	0		
2000313	O/M	I-74	WB Bridge over Johnson Fork/Johnson Fork Road, 1.23 mile E. of US 52		Bridge Thin Deck Overlay		Exempt	INDOT	2Q24		
Grouped Project: Update funding amounts and year			NHPP	PE	72,000	0	0	0	0		
			State	PE	8,000	0	0	0	0		
			NHPP	CO	0	0	0	754,304	0		
			State	CO	0	0	0	83,812	0		
2001806	O/M	SR 56	From 2 miles S of US 50 to US 50 and one location on SR 101 from US 50 to SR 48		Small Structures and Drains Construction		Exempt	INDOT	FY25		
Grouped Project: Add RW phase			STBG	PE	0	280,000	0	0	0		
			State	PE	0	70,000	0	0	0		
			STBG	RW	0	0	0	28,000	0		
			State	RW	0	0	0	7,000	0		
			STBG	CO	0	0	0	0	676,594		
			State	CO	0	0	0	0	169,148		
2002035	O/M	SR 46	Over Logan Creek, 0.94 mile E of SR 1		Arch Reconstruction or Repair		Exempt	INDOT	FY25		
Grouped Project: Add RW phase			STBG	PE	0	160,000	0	0	0		
			State	PE	0	40,000	0	0	0		
			STBG	RW	0	0	0	8,000	0		
			State	RW	0	0	0	2,000	0		
			STBG	CO	0	0	0	0	201,090		
			State	CO	0	0	0	0	50,272		

**From:** [Prince, Greg](#)  
**To:** [Alfred Wessling](#)  
**Cc:** [Michael L. McCool Jr.](#); [Metcalf, Karlei A](#)  
**Subject:** RE: Des 1383721 Contract No. B-42401 - SR 46 over Whitewater River Monthly Status Report  
**Date:** Monday, July 26, 2021 1:03:22 PM  
**Attachments:** [image002.png](#)  
[image003.png](#)  
[image004.png](#)  
[image006.png](#)  
[image007.png](#)

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**EXTERNAL EMAIL**

AI,  
1900094 & 1383721 are now bundled together with 1900094 being the lead, Contract B-42401,  
Letting on 9/13/2023. If you have any questions or concerns let me know.

Thanks,

**Greg Prince**

**Project Manager**

Capital Program Management  
Indiana Department of Transportation  
185 Agrico Lane  
Seymour, IN 47274  
**Office:** (812) 524-3783  
**Cell:** (812) 528-6549

**Email:** [gprince@indot.in.gov](mailto:gprince@indot.in.gov)



---

**From:** Al Wessling <[awessling@b-l-n.com](mailto:awessling@b-l-n.com)>  
**Sent:** Friday, July 23, 2021 7:58 AM  
**To:** Prince, Greg <[gprince@indot.IN.gov](mailto:gprince@indot.IN.gov)>  
**Cc:** Mike McCool <[mmccool@b-l-n.com](mailto:mmccool@b-l-n.com)>  
**Subject:** Des 1383721 Contract No. B-42401 - SR 46 over Whitewater River Monthly Status Report  
**\*\*\*\* This is an EXTERNAL email. Exercise caution. DO NOT open attachments or click links from unknown senders or unexpected email. \*\*\*\***

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Greg,

Here is the current monthly report for the subject project. Has the change in management been approved for this project so that this project is officially being kinned with Logan Creek Des.

1900094?

If you need anything further please let us know.

Thanks

AI

**ALFRED V. WESSLING, P.E.**

**Senior Bridge Engineer**

o: 317.849.5832

o: 317.806.3002 (direct)

# **Appendix I:**

## **Additional Studies**

**Land and Water Conservation Fund (LWCF) County Property List for Indiana (Last Updated March 2022)**

ProjectNumber	SubProjectCode	County	Property
1800296	1800296	Dearborn	Aurora City Park & Pool
1800304	1800304A	Dearborn	Lubbe Woods
1800516	1800516	Dearborn	Bright Park II

\*Park names may have changed. If acquisition of publically owned land or impacts to publically owned land is anticipated, coordination with IDNR, Division of Outdoor Recreation, should occur.

March 31, 2023

Environmental Unit  
Division of Fish and Wildlife  
402 W. Washington Street, Rm W273  
Indianapolis, IN 46204  
Phone (317) 232-4080  
[www.in.gov/dnr/fishwild/](http://www.in.gov/dnr/fishwild/)

Jacob Burskey  
Indiana Department of Transportation  
100 North Senate Avenue, Room N758-ES  
Indianapolis, IN 46204

**Re: ER-25430 Relocation of the utility line on the north bridge truss of SR 46 over Whitewater River (Outstanding River) by boring the line around and to the north of the bridge; Dearborn County  
\*\*General License/Exemption Request\*\***

Dear Mr. Burskey:

The Indiana Department of Natural Resources has reviewed the above referenced project per your request, which was received at the Division of Fish and Wildlife on February 3, 2023. Our agency offers the following comments for your information and in accordance with the Flood Control Act (IC 14-28-1) and Administrative Rules 312 IAC 10-5-0.3. Because the proposed project is located along a stream listed as an Outstanding River, approval from the Division of Fish and Wildlife is required prior to construction.

The Natural Heritage Program's data have been checked. The species below have been documented within 1/2 mile of the project area. The Division of Nature Preserves does not anticipate any impacts to the insect species as a result of this project.

- A. INSECT: Cobblestone Tiger Beetle (*Cicindela marginipennis*), State endangered
- B. FISH: Variegate Darter (*Etheostoma variatum*), State endangered
- C. BIRD: Bald Eagle (*Haliaeetus leucocephalus*)

Because the utility line will be bored under the waterway, no impact to the Variegate Darter should occur.

The Bald Eagle is federally protected (<https://fws.gov/law/bald-and-golden-eagle-protection-act>). The documented Bald Eagle nest is within 660 feet of the proposed project area. This nest was active in 2022 with chicks and in 2023, an adult has been seen sitting in the nest incubating eggs or chicks. Given its success in the past, the nest will likely be active in the coming years. Since this project consists of high disturbance activities, please coordinate with Deanne Endrizzi ([deanne\\_endrizzi@fws.gov](mailto:deanne_endrizzi@fws.gov)) of the U.S. Fish and Wildlife Service (USFWS) to determine whether a Bald Eagle disturbance permit is needed. The National Bald Eagle Management Guidelines should be followed ([https://www.fws.gov/sites/default/files/documents/national-bald-eagle-management-guidelines\\_0.pdf](https://www.fws.gov/sites/default/files/documents/national-bald-eagle-management-guidelines_0.pdf)) or guidance from the USFWS should be attained.

The following conditions must be implemented to minimize impacts to fish, wildlife, and botanical resources:

1. Revegetate all bare and disturbed areas with a mixture of grasses (excluding all varieties of tall fescue) and legumes as soon as possible upon completion; turf-type grasses (including low-endophyte, friendly endophyte, and endophyte free tall fescue but excluding all other varieties of tall fescue) may be used in regularly mowed areas only.

2. Do not excavate in the waterway, do not remove bank vegetation, and contain disturbance to within the project limits.
3. Appropriately designed measures for controlling erosion and sediment must be implemented to prevent sediment from entering the stream or leaving the construction site; maintain these measures until construction is complete and all disturbed areas are stabilized.
4. If erosion control blankets are used, they shall be heavy-duty, biodegradable, and net free or use loose-woven / Leno-woven netting to minimize the entrapment and snaring of small-bodied wildlife such as snakes and turtles (follow manufacturer's recommendations for selection and installation); seed and apply mulch on all other disturbed areas.

Approval is hereby granted for this project; however, the above requirements and the general license criteria (copy enclosed) must be met for this approval to be valid. Failure to do so would be a violation of 312 IAC 10-5-0.3. You are also responsible to make sure any other necessary permits or approvals are obtained, if required.

Our agency appreciates this opportunity to be of service. Please do not hesitate to contact me at (317) 233-4666 or [mbuffington@dnr.in.gov](mailto:mbuffington@dnr.in.gov) if we can be of further assistance.

Sincerely,

*J. Matthew Buffington*

J. Matthew Buffington  
Environmental Supervisor  
Division of Fish and Wildlife

Enclosure

**Date:** April 5, 2023

**Project:** SR 46 Des 1383721 Bridge Deck Replacement

**Subject:** Bald Eagle Permit: Incidental Take Analysis

### **Project Description**

The existing bridge will be rehabilitated to include replacement of the concrete deck and joints, and installation of new bridge rail. Travel lanes and shoulder widths will be transitioned from the bridge to the roadway approaches. Deteriorated elements will be repaired or replaced in-kind. All areas of the truss will be cleaned and painted green to match the existing color. Deteriorated concrete caps of Pier 5 and the east abutment will be removed and reconstructed, and all delaminated and spalled areas of abutments and piers will be patched. The riprap scour countermeasures will be placed around all substructure units, and debris removal will also be implemented. New approach slabs will be placed, and the rehabilitation will require approximately 200 feet of approach work east and west of the bridge ends. The project will require relocation of utilities from the structure and bored under the stream.

### **Incidental Take Analysis**

**Step 1. Activity Category** – Construction of roads

**Step 2. Is the nest visible from the project or activity area? Yes**

**Nest Visibility** - Nests may be shielded by rolling topography, trees, or other screening factors. Visibility is a factor because, in general, eagles are more prone to disturbance when an activity occurs in full view.

The eagle nest is located approximately 400' northwest from the center of portion of the bridge over the Whitewater River. The bald eagle nest (active or inactive) can be seen from the project site. There is clear visibility to the bridge from the nest and it is inside of the 660 foot buffer.

**Step 3. To avoid the Incidental take of bald eagles or their young we recommend that you do the following.**

Since the activity at the bridge is closer than 660 feet, all clearing, construction, and related activities must occur **outside the nesting season** (i.e., outside the nesting season is from August through mid-January).

### **Step 4. Documentation**

The following firm commitment will be added to the NEPA document -

A bald eagle nest is located within 660 feet of the bridge. The bald eagle is protected under the Bald and Golden Eagle Protection Act. The BGEPA prohibits anyone, without a federal permit, from taking or disturbing bald eagles, which includes their parts, nests, or eggs. No construction activity, to include utility relocation, shall take place during the bald eagle nesting season. The bald eagle nesting season is

from January 15 to July 31. The Bald Eagle Protection USP shall be included in the contract documents.

The following USP will be added to the contract document -

#### BALD EAGLE PROTECTION

##### **Description**

This work shall consist of protecting bald eagles as required under the Bald and Golden Eagle Protection Act, BGEPA, in accordance with 105.03.

##### **Construction Requirements**

The bald eagle (*Haliaeetus leucocephalus*), a species protected under the BGEPA, has been documented within 1/2 mile of the project area. The BGEPA prohibits anyone, without a federal permit, from taking or disturbing bald eagles, which includes their parts, nests, or eggs. No construction activity, to include utility relocation, shall take place during the bald eagle nesting season. The bald eagle nesting season is from January 15 to July 31.

Therefore, incidental take of bald eagles is unlikely to occur.

Signature: *Sandra Bowman*

Date: 4.5.2023