

FHWA-Indiana Environmental Document
CATEGORICAL EXCLUSION / ENVIRONMENTAL ASSESSMENT FORM
GENERAL PROJECT INFORMATION

Road No./County:

Park Road/Jennings

Designation Number(s):

2200148

Project

Description/Termini:

Bridge Rehabilitation on Park Road over the Muscatatuck River in the Crosley Fish and Wildlife Area, approximately 155 feet north and south of the center of the bridge

	Categorical Exclusion, Level 2 – Required Signatories: INDOT DE and/or INDOT ESD
	Categorical Exclusion, Level 3 – Required Signatories: INDOT ESD
X	Categorical Exclusion, Level 4 – Required Signatories: INDOT ESD and FHWA
	Environmental Assessment (EA) – Required Signatories: INDOT ESD and FHWA
	Additional Investigation (AI) – The proposed action included a design change from the original approved environmental document. Required Signatories must include the appropriate environmental approval authority

Approval

INDOT DE Signature and Date

INDOT ESD Signature and Date

FHWA Signature and Date

Release for Public Involvement

N/A

INDOT DE Initials and Date

ADWP

March 25, 2025

INDOT ESD Initials and Date

Certification of Public Involvement

INDOT Consultant Services Signature and Date

INDOT DE/ESD Reviewer Signature and Date:

Name and Organization of CE/EA Preparer:

Kristin Wing, Egis BLN USA, Inc. (Egis)

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Note: Refer to the most current INDOT CE Manual, guidance language, and other ESD resources for further guidance regarding any section of this form.

Part I – Public Involvement

Every Federal action requires some level of public involvement, providing for early and continuous opportunities throughout the project development process. **The level of public involvement should be commensurate with the proposed action.**

	Yes	No
Does the project have a historic bridge processed under the Historic Bridges PA*?	<input checked="" type="checkbox"/>	<input type="checkbox"/>
If No, then: Opportunity for a Public Hearing Required?	<input type="checkbox"/>	<input type="checkbox"/>

**A public hearing is required for all historic bridges processed under the Historic Bridges Programmatic Agreement between INDOT, FHWA, SHPO, and the ACHP.*

Discuss what public involvement activities (legal notices, letters to affected property owners and residents (i.e., notice of entry), meetings, special purpose meetings, newspaper articles, etc.) have occurred for this project.

Notice of entry letters were not mailed because all work is expected to be completed within the existing right-of-way (ROW).

Section 106

To meet the public involvement requirements of Section 106, a legal notice of FHWA's finding of "No Historic Properties Affected" was published in the *North Vernon Plain Dealer and Sun* on August 23, 2024, offering the public an opportunity to submit comments pursuant to 36 CFR 800.2(d), 800.3(e), and 800.6(a)(4). The public comment period closed 30 days later on September 20, 2024. The text of the public notice and the affidavit of publication appear in Appendix D, pages 78 to 79. No comments were received.

Historic Bridge

Pursuant to the Historic Bridge Programmatic Agreement (PA), a public hearing is required. A legal notice will appear in a local publication contingent upon the release of this document for public involvement. This document will be revised after the public involvement requirements are fulfilled.

Public Controversy on Environmental Grounds

Discuss public controversy concerning community and/or natural resource impacts, including what is being done during the project to minimize impacts.

No controversy

At this time, there is no substantial public controversy concerning impacts to the community or natural resources.

Part II - General Project Identification, Description, and Design Information

Sponsor of the Project: Indiana Department of Transportation (INDOT) INDOT District: Seymour

Local Name of the Facility: Park Road

Funding Source (mark all that apply): Federal ☒ State ☒ Local ☐ Other* ☐

*If other is selected, please identify the funding source: _____

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PURPOSE AND NEED:

The need should describe the specific transportation problem or deficiency that the project will address. The purpose should describe the goal or objective of the project. The solution to the traffic problem should NOT be discussed in this section.

Need: The need for this project is attributed to the distress and deterioration of the existing structure (P000-40-07088B, National Bridge Inventory [NBI] 60380) conveying Park Road over the Muscatatuck River that continues to worsen, compromising public safety and causing the bridge closure in 2019. The bridge is Non-Select with unique characteristics and historical significance. The Historic Bridge Alternative Analysis (HBAA) Report states that the deck and wearing surface are rated in critical condition (two out of nine) due to areas of severe deterioration in the timber bridge deck, with holes and rotten areas, as well as misaligned and warped planks, leading to a load rating of zero and the bridge closing in 2019 and has since been unusable. The superstructure is rated fair (five out of nine), with areas of deterioration and section loss limiting the functional life of the bridge. The substructure is rated satisfactory (six out of nine), with some abutments exhibiting cracking with leaching and minor spalling. The bridge is considered scour-critical due to the unknown foundations and the large scour depths calculated. The hydraulic scour analysis determined scour to be below all the substructure's foundations, causing the bridge to become scour critical. Please refer to the HBAA report excerpt in Appendix D, pages 47 to 56.

Condition ratings are based on a scale of zero to nine, with zero being failed and nine being excellent.

Purpose: The purpose of this project is to provide a sufficient Park Road crossing over the Muscatatuck River with a condition rating of six (satisfactory) or better, achieve a minimum 4-ton load rating, improve scour, and extend the structure's service life 25 years. Additionally, the project's purpose includes maintaining the historical integrity of the existing bridge, which, although designated as Non-Select, has unique characteristics and historical significance.

PROJECT DESCRIPTION (PREFERRED ALTERNATIVE):

County: JenningsMunicipality: N/ALimits of Proposed Work: Approximately 155 feet north and south of the bridge center – Park Road over the Muscatatuck RiverTotal Work Length: 0.042 Mile(s)Total Work Area: 0.30 Acre(s)Is an Interstate Access Document (IAD)¹ required?

If yes, when did the FHWA provide a Determination of Engineering and Operational Acceptability?

Yes ¹	No
<input type="checkbox"/>	<input checked="" type="checkbox"/>
Date: <input type="text"/>	

¹If an IAD is required; a copy of the approved CE/EA document must be submitted to the FHWA with a request for final approval of the IAD.

Describe location of project including township, range, city, county, roads, etc. Existing conditions should include current conditions, current deficiencies, roadway description, surrounding features, etc. Preferred alternative should include the scope of work, anticipated impacts, and how the project will meet the Purpose and Need. Logical termini and independent utility also need discussed.

The Indiana Department of Transportation (INDOT), in conjunction with the Indiana Department of Natural Resources (IDNR) and with funding from the Federal Highway Administration (FHWA), intends to proceed with a bridge rehabilitation on Park Road over the Muscatatuck River in Jennings County, Indiana.

Location:

The project is on Park Road over Muscatatuck River in the IDNR Crosley Fish and Wildlife Area. Specifically, the project is in Section 15, Township 6 North, Range 8 East in the IDNR Crosley Fish and Wildlife Area in Vernon Township, Jennings County, Indiana, as shown on the 7.5 Minute Vernon, Indiana United States Geological Survey (USGS) Quadrangle Map (Appendix B, page 2).

Existing Conditions:

The bridge (No. P000-40-07088B/NBI 60380) is approximately 212 feet long by 12.3 feet wide. It is a three-span truss bridge

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constructed in 1910 and rehabilitated in 1979, 2004, and 2015. According to the Indiana Historic Bridge Inventory, completed in 2010, the bridge was determined to be a Non-Select bridge eligible for listing in the NRHP under Criterion C. The main span is a Pratt-through truss, and the approach spans are Warren pony trusses. The bridge clear roadway width is 9 feet 4 inches, and the superstructure is composed of a timber plank deck supported by steel stringers and floor beams. In 2019, the bridge was closed due to the condition of the deck. According to the HBAA report, the deck is rated in critical condition (two out of nine), with deteriorated boards and planks, including a hole in the deck near the north end of the bridge. The wearing surface is rated in critical condition (two out of nine), with loose, misaligned, and warped planks. The superstructure is rated fair (five out of nine), with surface rust and moderate pitting near bearing locations, and minor section loss. The substructure is rated satisfactory (six out of nine), with cracking and minor spalling at the bridge corners. The bridge is considered scour-critical due to the unknown foundations and the large scour depths calculated.

Park Road is a one lane gravel road, 9 feet wide with no shoulders. It is functionally classified as recreational (primary access) and is not part of the National Highway System. Park Road is the primary access road through the Crosley Fish and Wildlife Area, and there is an existing detour in place, described below in the Maintenance of Traffic section.

The project is located within Crosley Fish and Wildlife Area, which is afforded protection under Section 4(f) of the U.S. Department of Transportation Act of 1966 as a publicly owned park. Surrounding land use is forested with no residential properties or businesses. A parking lot and boat ramp are adjacent to the subject bridge in the northeast corner of the project area.

Preferred Alternative:

The IDNR and INDOT, with funding from FHWA, intend to proceed with a bridge rehabilitation project on Park Road over the Muscatatuck River. A description of the preferred alternative, as noted in the HBAA (Appendix D, pages 47 to 56), can be found below.

Alternative B.1 Rehabilitation of the Existing Bridge for Continued Vehicular Use (Two-Way (One Lane) Option) Meeting Secretary of Interior's Standard (SOIS) For Rehabilitation

The preferred alternative consists of rehabilitating the existing Non-Select bridge for continued vehicular use, meeting the SOIS for Rehabilitation. This alternative addresses the deteriorated areas of the bridge and will prolong the life of the historic structure.

The rehabilitation efforts will include cleaning and painting the truss in a paint color that will match the existing color, replacing the bridge deck in all spans with a timber plank deck, repairing or replacing in kind some verticals, gusset plates, low chord, and secondary truss members. Deteriorated concrete abutments and pier caps will be removed and reconstructed, and the delaminated and spalled areas of the abutments and piers will be patched. Scour countermeasures will be placed at all substructure units. Any rivets that are removed to make repairs will be replaced with round-headed bolts wherever possible, and the bridge rail will be replaced with a steel handrail. All debris will be removed from the abutment and pier bridge seats. Roadway improvement is not part of the scope of this bridge rehabilitation. Approximately five trees will be removed to allow for construction access, and all other nearby trees will be trimmed to keep branches away from the truss and vehicles. The new bridge deck will match the existing out-to-out width, and the clear roadway width will be slightly larger due to the new steel handrail that will be installed on the bridge. The approach guardrail will not be modified leading up to the bridge, and no approach roadway work will be completed except as necessary to complete the scour countermeasures work. Scour countermeasures will include placing dumped partially grouted revetment riprap on the south spillslope, class 1 riprap around the north abutment and wings, a concrete underpin around pier 2, and dumped partially grouted class 2 riprap placed around three sides of pier 3 with regular buried class 2 riprap on the north side of pier 3.

Please refer to Appendix B, pages 9 to 16, for plan sheets illustrating the above scope of work.

This alternative will meet the project's purpose and need by correcting the deteriorated portions of the bridge and extending its service life. The rehabilitation activities will improve the overall condition rating to a six (satisfactory) or better and preserve the existing structure's historical integrity. The proposed improvements would protect the bridge from scour, remove it from scour critical status and allow for the bridge to be reopened for the public. The estimated construction cost of this project is currently \$950,000, as included in the HBAA (Appendix D, pages 55 to 56), and the estimated cost of construction in the Indiana State Transportation Improvement Program (STIP) for Fiscal Years (FY) 2024 to 2028 is \$750,000 (Appendix G, page 1). Modification M24-98, approved December 5, 2024, has added construction costs to FY 2026, and the updated construction costs in the STIP total \$942,000 (Appendix G, page 2). The previous FY 2022 to 2026 STIP included engineering costs under FY 2023 for \$187,500 (Appendix G, page 3). If necessary, the INDOT Project Manager will ensure the STIP is updated prior to the Ready-for-Contracts (RFC) date to reflect increasing costs.

The preferred maintenance of traffic (MOT) is a full closure that will utilize the existing detour since the bridge is already closed. Construction is anticipated to take approximately four months. Please refer to this document's *Maintenance of Traffic* section for additional details. No right-of-way (ROW) will be required for this project.

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The project termini were developed to minimize impacts to the extent possible while also addressing the project need. The project area extends approximately 155 feet north and south of the center of the bridge and encompasses the construction area for a total work length of 310 feet. This project has independent utility because it will improve the existing bridge function as an independent project that is not dependent on another in the area to meet the purpose and need of the project.

OTHER ALTERNATIVES CONSIDERED:

Provide a header for each alternative. Describe all discarded alternatives, including the No Build Alternative. Explain why each discarded alternative was not selected. Make sure to state how each alternative meets or does not meet the Purpose and Need and why.

No Build/Do Nothing

This alternative proposed the utilization of existing facilities with no expenditure of capital funds or improvement of the roadway. This alternative would not address the overall project purpose and need, which is to improve the overall condition rating of the bridge and reopen it to the public. If the No Build/Do Nothing alternative were selected, the condition of the bridge would continue to decline. Although this alternative is feasible, it is not prudent to allow the bridge to continue to deteriorate and remain impassable.

Bridge Replacement

This alternative was only used for cost comparison purposes (Appendix D, page 56, Table 2) and is considered a feasible but not prudent alternative. The replacement of the bridge is not prudent as it would not maintain the historical integrity of the bridge, requires more environmental impacts and tree clearing and is more expensive than the rehabilitation alternative.

Analysis of additional alternatives was not required since the feasible and prudent alternative was found with Alternative B.1.

The No Build Alternative is not feasible, prudent or practicable because (Mark all that apply)

It would not correct existing capacity deficiencies;

It would not correct existing safety hazards;

It would not correct the existing roadway geometric deficiencies;

It would not correct existing deteriorated conditions and maintenance problems; or

It would result in serious impacts to the motoring public and general welfare of the economy.

Other (Describe):

X
X

ROADWAY CHARACTER:

If the proposed action includes multiple roadways, complete and duplicate for each roadway.

Name of Roadway	<u>Park Road</u>			
Functional Classification:	<u>Recreational (Primary Access)</u>			
Current ADT:	<u>143</u>	<u>VPD (2025)</u>	Design Year ADT:	<u>143</u> <u>VPD (2025*)</u>
Design Hour Volume (DHV):	<u>5</u>	Truck Percentage (%)	<u>0</u>	
Designed Speed (mph):	<u>15</u>	Legal Speed (mph):	<u>15</u>	

*Please note: the design year is the current year per the INDOT Design Manual Fig.51-6B, which covers the "Geometric Design Criteria for Recreational Road."

	Existing		Proposed	
Number of Lanes:	1		1	
Type of Lanes:	Through		Through	
Pavement Width:	9	ft.	9	ft.
Shoulder Width:	0	ft.	0	ft.
Median Width:	N/A	ft.	N/A	ft.
Sidewalk Width:	N/A	ft.	N/A	ft.

Setting: ☐ Urban ☐ Suburban ☒ Rural

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Topography: ☐ Level ☒ Rolling ☐ Hilly

BRIDGES AND/OR SMALL STRUCTURE(S):

If the proposed action includes multiple structures, complete and duplicate for each bridge and/or small structure. Include both existing and proposed bridge(s) and/or small structure(s) in this section.

Structure/NBI Number(s): P000-40-07088B / 060380 Sufficiency Rating: 0.0/100 HBAA Report Appendix D, page 49
(Rating, Source of Information)

	Existing		Proposed	
Bridge/Structure Type:	Steel Through Truss		Steel Through Truss	
Number of Spans:	3		3	
Weight Restrictions:	0	ton	4	ton
Height Restrictions:	N/A	ft.	N/A	ft.
Curb to Curb Width:	9.3	ft.	9.6	ft.
Outside to Outside Width:	13.3	ft.	13.3	ft.
Shoulder Width:	0	ft.	0	ft.

Describe impacts and work involving bridge(s), culvert(s), pipe(s), and small structure(s). Provide details for small structure(s): structure number, type, size (length and dia.), location, and impacts to water. Use a table if the number of small structures becomes large. If the table exceeds a complete page, put it in the appendix and summarize the information below with a citation to the table.

The project involves Bridge No. P000-40-07088B (NBI #060380), a historic "Non-Select" bridge on Park Road over the Muscatatuck River in the Crosley Fish and Wildlife Area. The bridge is a three-span truss bridge constructed in 1910 and rehabilitated in 1979, 2004, and 2015. The main span is a Pratt-through truss, and the approach spans are Warren pony trusses. The bridge clear roadway width is 9 feet 4 inches, and the superstructure supports a timber plank deck and is composed of steel stringers and floor beams, and a steel truss. In 2019, the bridge was closed due to the condition of the deck.

The scope of work will include cleaning and painting the truss in a paint color that will match the existing color, replacing the bridge deck in all spans with a timber plank deck, repairing or replacing in kind some verticals, gusset plates, low chord, and secondary truss members. Deteriorated concrete abutments and pier caps will be removed and reconstructed, and the delaminated and spalled areas of the abutments and piers will be patched. Scour countermeasures will be placed at all substructure units. Any rivets that are removed to make repairs will be replaced with round-headed bolts wherever possible, and the bridge rail will be replaced with a steel handrail. All debris will be removed from the abutment and pier bridge seats. Approximately five trees will be removed to allow for construction access, and all other nearby trees will be trimmed to keep branches away from the truss and vehicles. The new bridge deck will match the existing out-to-out width, and the clear roadway width will be slightly larger due to the new steel handrail that will be installed on the bridge. The approach guardrail will not be modified leading up to the bridge, and no approach roadway work will be completed except as necessary to complete the scour countermeasures work. Scour countermeasures will include placing dumped partially grouted revetment riprap on the south spillside, class 1 riprap around the north abutment and wings, a concrete underpin around pier 2 and dumped partially grouted class 2 riprap placed around three sides of pier 3 with regular buried class 2 riprap on the north side of pier 3.

There are no other bridges or small structures in the project area.

MAINTENANCE OF TRAFFIC (MOT) DURING CONSTRUCTION:

Is a temporary bridge proposed?
Is a temporary roadway proposed?
Will the project involve the use of a detour or require a ramp closure? (describe below)
Provisions will be made for access by local traffic and so posted.
Provisions will be made for through-traffic dependent businesses.

Yes	No
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input checked="" type="checkbox"/>	<input type="checkbox"/>
<input checked="" type="checkbox"/>	<input type="checkbox"/>
<input checked="" type="checkbox"/>	<input type="checkbox"/>

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Provisions will be made to accommodate any local special events or festivals.
 Will the proposed MOT substantially change the environmental consequences of the action?
 Is there substantial controversy associated with the proposed method for MOT?
 Will the project require a sidewalk, curb ramp, and/or bicycle lane closure? (describe below)
 Provisions will be made for access by pedestrians and/or bicyclist and so posted (describe below).

Yes	No
<input checked="" type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>

Discuss closures, detours, and/or facilities (if any) that will be provided for maintenance of traffic. Any known impacts from these temporary measures should be quantified to the extent possible, particularly with respect to properties such as Section 4(f) resources and wetlands. Discuss any pedestrian/bicycle closures. Any local concerns about access and traffic flow should be detailed as well.

The bridge has been closed since 2019. Traveling motorists have been using an alternative route and there is an existing detour. The MOT will keep the bridge closed for the duration of the project and will utilize the existing detour. The existing detour route consists of SR 3, SR 7, Pike Street, Washington Street, County Road (CR) 25 West, CR 60 West, CR 115 South, and CR 75 West. Construction is anticipated to take approximately four months.

There are no facilities for pedestrians or bicyclists present. The closures/lane restrictions will pose a temporary inconvenience to traveling motorists (including school buses and emergency services); however, no significant delays are anticipated, and all inconveniences and delays will cease upon project completion.

ESTIMATED PROJECT COST AND SCHEDULE:

Engineering: \$ 187,500 (2023*) Right-of-Way: \$ N/A (20--) Construction: \$ 942,000 (2026)

*Engineering costs were included in the previous STIP under the year 2023. If necessary, the INDOT Project Manager will ensure the STIP will be updated prior to the RFC date to reflect increasing costs. This has been added as a firm commitment in the *Environmental Commitments* section of this document.

Anticipated Start Date of Construction: Spring 2026

RIGHT OF WAY (ROW):

Land Use Impacts	Amount (acres)	
	Permanent	Temporary
Residential	0.0	0.0
Commercial	0.0	0.0
Agricultural	0.0	0.0
Forest	0.0	0.0
Wetlands	0.0	0.0
Other:	0.0	0.0
Other:	0.0	0.0
TOTAL	0.0	0.0

Describe both Permanent and Temporary right-of-way and describe their current use. Typical and Maximum right-of-way widths (existing and proposed) should also be discussed. Any advance acquisition, reacquisition, or easements, either known or suspected, and their impacts on the environmental analysis should be discussed.

The existing ROW is owned by the Indiana Department of Natural Resources (IDNR) and is forested land. The project is entirely within the Crosley Fish and Wildlife Area, owned by IDNR, and IDNR is a co-sponsor of the project with INDOT.

No ROW required

This project will occur within existing ROW. No permanent or temporary ROW will be required for this project.

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If the scope of work or permanent or temporary right-of-way amounts change, the INDOT Environmental Services Division (ESD) and the INDOT District Environmental Section will be contacted immediately.

Part III – Identification and Evaluation of Impacts of the Proposed Action

SECTION A - EARLY COORDINATION:

List the date(s) coordination was sent and all resource agencies that were contacted as a part of the development of this Environmental Study. Also, include the date of their response or indicate that no response was received.

Early coordination letters were sent on September 25, 2023 (Appendix C, pages 1 to 4).

Agency	Date Sent	Date Response Received	Appendix C
Federal Highway Administration (FHWA)	September 25, 2023	No response Received	N/A
Indiana Geological and Water Survey (IGWS)	September 25, 2023	September 25, 2023	C-5
Indiana Department of Natural Resources Division of Fish and Wildlife (IDNR DFW)	September 25, 2023	October 25, 2023	C-7
Eighth Coast Guard District	September 25, 2023	No Response Received	N/A
US Department of Housing and Urban Development (US HUD)	September 25, 2023	No Response Received	N/A
National Park Service (NPS)	September 25, 2023	No Response Received	N/A
Jennings County Emergency Management Director	September 25, 2023	No Response Received	N/A
Southeastern Indiana Regional Planning Commission	September 25, 2023	No Response Received	N/A
Jennings County Commissioners	September 25, 2023	No Response Received	N/A
INDOT Environmental Section Manager, Seymour District	September 25, 2023	No Response Received	N/A
Crosley Fish and Wildlife Area	September 25, 2023	No Response Received	N/A
IDNR Division of Law Enforcement	September 25, 2023	No Response Received	N/A
Jennings County Highway Superintendent	September 25, 2023	No Response Received	N/A
Indiana Department of Environmental Management (IDEM) – Groundwater Section	September 25, 2023	No Response Received	N/A
US Army Corps of Engineers (USACE)	September 25, 2023	No Response Received	N/A
Jennings County Surveyor	September 25, 2023	No Response Received	N/A
INDOT Project Manager, Seymour District	September 25, 2023	No Response Received	N/A
US Fish and Wildlife Service (USFWS)	September 25, 2023	September 26, 2023	C-10
Jennings County Area Plan Commission	September 25, 2023	No Response Received	N/A

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INDOT Ecology, Waterway Permitting, and Stormwater Office (EWPSO)	September 25, 2023	September 26, 2023	C-28
INDOT Historic Bridge Specialist	September 25, 2023	No Response Received	N/A

All applicable recommendations are included in the *Environmental Commitments* section of this CE document.

SECTION B – ECOLOGICAL RESOURCES:

Streams, Rivers, Watercourses & Other Jurisdictional Features

Federal Wild and Scenic Rivers
State Natural, Scenic or Recreational Rivers
Nationwide Rivers Inventory (NRI) listed
Outstanding Rivers List for Indiana
Navigable Waterways

Presence

X
X
X

Impacts

Yes	No
X	
X	
X	

Total stream(s) in project area: 160 Linear feet Total impacted stream(s): 50 Linear feet

Stream Name	Classification	Total Size in Project Area (linear feet)	Impacted linear feet	Comments (i.e., location, flow direction, likely Water of the US, appendix reference)
Muscatatuck River	Perennial	160	50	Flows east to west under Bridge P000-40-07088B and is likely a Water of the U.S. (Appendix F, page 2)

Describe all streams, rivers, watercourses and other jurisdictional features adjacent or within the project area. Include whether or not impacts (both permanent and temporary) will occur to the features identified. Include if the streams or rivers are listed on any federal or state lists for Indiana. Include if features are likely subject to federal or state jurisdiction. Discuss measures to avoid, minimize, and mitigate if impacts will occur.

Presence, with impacts

Based on the desktop review, the aerial map of the project area, and the Red Flag Investigation (RFI) report (Appendix E, pages 1 to 8), there are 13 streams, rivers, watercourses, or other jurisdictional features within the 0.5-mile search radius. There is one stream, river, watercourse, or other jurisdictional feature within or adjacent to the project area, which was confirmed by the site visit on July 11, 2023, by Egis. There are no Federal, Wild and Scenic Rivers; State Natural, Scenic, and Recreational Rivers; or National Rivers Inventory waterways are present in the project area. The Muscatatuck River is listed as an Outstanding River and navigable waterway in Jennings County.

A *Waters of the U.S. Determination / Wetland Delineation Report* was approved by INDOT EWPSO on January 9, 2024. Please refer to Appendix F, pages 1 to 19, for the *Waters of the U.S. Determination / Wetland Delineation Report*. It was determined that one waterway, the Muscatatuck River, is present within the project area. The USACE makes all final determinations regarding jurisdiction.

The Muscatatuck River flows generally east to west and is identified as a perennial stream on the USGS topographic, National Wetland Inventory (NWI), and National Hydrography Dataset (NHD) maps. According to the USGS StreamStats website, the stream has an upstream drainage area of approximately 199.658 square miles (Appendix F, page 11). The riparian corridor within the project area consists of a wooded area with steep slopes on each side. The Muscatatuck River has an Ordinary High-Water Mark (OHWM) of 120 feet wide and 6 feet deep. Due to overhead cover, substrate of silt and rocks, presence of pools, and water clarity, the water quality was rated as average. Because the Muscatatuck River flows to the East Fork White River, which flows to the White River, which flows to the Wabash River, a Section 10 Traditionally Navigable Water, it would likely be considered a Water of the U.S. (WOTUS).

Permanent impacts to the Muscatatuck River as a result of the project is approximately 50 feet. There will be no temporary impacts. A

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Section 401/404 permit will likely be required. Mitigation will likely not be required. The IDNR Construction in a Floodway permit was approved by IDNR and is effective as of February 1, 2025, and expires January 15, 2023. According to the IDNR permit, no tree mitigation is required.

Early Coordination

IDNR DFW responded on October 25, 2023, with recommendations to avoid or minimize impacts on the Muscatatuck River (Appendix C, pages 7 to 9). IDNR DFW issued recommendations to minimize impacts on aquatic and terrestrial resources. IDNR DFW suggested the use of appropriately designed erosion control measures, minimizing construction related disturbance, and revegetating all bare and disturbed areas as soon as possible upon project completion. USFWS responded on September 26, 2023, noted that wetland and stream impacts may require permits from USACE, IDEM, and the IDNR (Appendix C, pages 10 to 11).

All applicable recommendations are included in the *Environmental Commitments* section of this CE document.

Open Water Feature(s)

Reservoirs
Lakes
Farm Ponds
Retention/Detention Basin
Storm Water Management Facilities
Other: _____

Presence

Impacts

Yes	No

Describe all open water feature(s) identified adjacent or within the project area. Include whether or not impacts (both permanent and temporary) will occur to the features identified. Include if features are likely subject to federal or state jurisdiction. Discuss measures to avoid, minimize, and mitigate if impacts will occur.

No presence, no impact

Based on the desktop review, the aerial map of the project area, and the RFI report (Appendix E, pages 1 to 8), there are two open water features within the 0.5-mile search radius. There are no open water features within or adjacent to the project area, which was confirmed by the site visit on July 11, 2023, by Egis. Therefore, no impacts are expected.

Wetlands

Presence

--

Impacts

Yes	No

Total wetland area: N/A Acre(s) Total wetland area impacted: N/A Acre(s)

(If a determination has not been made for non-isolated/isolated wetlands, fill in the total wetland area impacted above.)

Wetland No.	Classification	Total Size (Acres)	Impacted Acres	Comments (i.e., location, likely Water of the US, appendix reference)
N/A	N/A	N/A	N/A	N/A

Documentation

Wetlands (Mark all that apply)

Wetland Determination
Wetland Delineation
USACE Isolated Waters Determination

X

ESD Approval Dates

January 9, 2024

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County JenningsRoute Park RoadDes. No. 2200148**Improvements that will not result in any wetland impacts are not practicable because such avoidance would result in** (Mark all that apply and explain):

- Substantial adverse impacts to adjacent homes, business or other improved properties;
- Substantially increased project costs;
- Unique engineering, traffic, maintenance, or safety problems;
- Substantial adverse social, economic, or environmental impacts, or
- The project not meeting the identified needs.

Describe all wetlands identified adjacent or within the project area. Include whether or not impacts (both permanent and temporary) will occur to the features identified. Include if features are likely subject to federal or state jurisdiction. Discuss measures to avoid, minimize, and mitigate if impacts will occur.

Presence, no impact

Based on the desktop review, the aerial map of the project area, and the RFI report (Appendix E, pages 1 to 8), there are fourteen wetlands within the 0.5-mile search radius. There is one wetland within or adjacent to the project area. That number was updated to zero by the site visit on July 11, 2023, by Egis. Therefore, no impacts are expected.

A *Waters of the U.S. Determination / Wetland Delineation Report* was approved by INDOT EWPSO on January 9, 2024. Please refer to Appendix F, pages 1 to 19, for the *Waters of the U.S. Determination / Wetland Delineation Report*. It was determined that there are no wetlands within or adjacent to the project area. The USACE makes all final determinations regarding jurisdiction.

Terrestrial Habitat**Presence**

X

Impacts

Yes

NO

X

--

Total terrestrial habitat in project area: 0.20 Acre(s) Total tree clearing: 0.003 Acre(s)

Describe types of terrestrial habitat (i.e., forested, grassland, farmland, lawn, etc.) adjacent to or within the project area. Include whether or not impacts will occur to habitat identified. Include total terrestrial habitat impacted and total tree clearing that will occur. Discuss measures to avoid, minimize, and mitigate if impacts will occur.

Presence, with impacts

Based on a desktop review, a site visit on July 11, 2023, by Egis, and the aerial map of the project area (Appendix B, page 3), the project area is surrounded by forested areas. Dominant tree species include yellow poplar (*Liriodendron tulipifera*), sugar maple (*Acer saccharum*), American basswood (*Tilia americana*), and black walnut (*Juglans nigra*). Dominant vegetation includes small flowered leafcup (*Polymnia canadensis*), wild ginger (*Asarum canadense*), and purple Joe-Pye weed (*Eutrochium purpureum*). Five trees, or approximately 0.003 acre of trees, will be cleared. Trees that are not removed will be trimmed within 10 feet of the bridge. Seeding of disturbed areas will be completed according to the IDNR permit. Any disturbed areas will be re-seeded following completion of construction activities.

Early Coordination

IDNR DFW responded on October 25, 2023, with several recommendations to avoid or minimize impacts on terrestrial habitat. These recommendations included developing a mitigation plan for any unavoidable habitat impacts as well as recommendations for installing bank stabilization measures, revegetating all bare and disturbed areas, and installing appropriate erosion and sediment control measures. In their response letter, the IDNR DFW also indicated that if impacts to non-wetland riparian forest are less than one acre, the non-wetland forest removed in a rural setting should be replaced at a 1:1 ratio based on area (Appendix C, pages 7 to 9). Any disturbed areas will be restored following the completion of construction activities.

All applicable recommendations are included in the *Environmental Commitments* section of this CE document.

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Protected Species

Federally Listed Bats

Information for Planning and Consultation (IPaC) determination key completed
 Section 7 informal consultation completed (IPaC cannot be completed)
 Section 7 formal consultation Biological Assessment (BA) required

Yes

No

X

X
X

Determination Received for Listed Bats from USFWS:

NE

--

NLAA

X

LAA

--

Other Species not included in IPaC

Additional federal species found in project area (based on IPaC species list)
 State species (not bird) found in project area (based upon consultation with IDNR)

Yes

No

X
X

Migratory Birds

Known usage or presence of birds (i.e., nests)
 State bird species based upon coordination with IDNR

Yes

No

X

X

Discuss IDNR coordination and species identified. Describe USFWS Section 7 consultation and determination received for Indiana bat and northern long-eared bat impacts. Discuss if other federally listed species were identified. If so, include consultation that has occurred and the determination that was received. Discuss if migratory birds have been observed and any impacts.

Based on a desktop review and the RFI report (Appendix E, pages 1 to 8), completed by Egis on December 20, 2023, the IDNR Jennings County Endangered, Threatened and Rare (ETR) Species List has been checked. According to the IDNR-DFW early coordination response letter dated October 25, 2023 (Appendix C, pages 7 to 9), the Natural Heritage Program's Database has been checked, and two properties, three communities, and four species were documented within 0.5-mile of the project area. The Division of Nature Preserves does not anticipate any significant effects to the listed communities and flora, as follows: Bluegrass Mesic Upland Forest, Bluegrass Dry-mesic Upland Forest, and Limestone Cliff, and the Divided Toothwort (*Cardamine dissecta*, state endangered). The three fauna species within 0.5-mile of the project area are the state endangered Cerulean Warbler (*Setophaga cerulea*), species of special concern Little Spectaclecase (*Villosa lianosa*), and the state endangered A Millipede (*Pseudopolydesmus collinus*). IDNR noted that suitable habitat for the Cerulean Warbler exists within the project area and recommends removing trees outside of the spring migratory season and the breeding season (early April and late August) to minimize impacts. IDNR recommends not constructing causeways or crossing the stream using heavy equipment to avoid impacts to the Little Spectaclecase. No specific recommendations were provided for the A Millipede. An INDOT 0.5-mile bat review occurred on August 9, 2023, and indicated the presence of endangered bat species within 0.5 mile of the project area, and the project is within the Northern Long-Eared Bat hibernacula buffer. No critical habitats were identified.

Bats, Standard Coordination

Project information was submitted through the USFWS's Information for Planning and Consultation (IPaC) portal, and an official species list was generated (Appendix C, pages 12 to 23). The project is within range of the federally endangered Indiana bat (*Myotis sodalis*) and Northern Long-eared bat (NLEB) (*Myotis septentrionalis*). Other species were generated in the IPaC species list along with the Indiana bat and NLEB. Refer to the paragraph below.

Based on the presence of two protected bat species hibernacula points and one protected bat species capture within 0.5 mile of the project area and the fact that work will not be limited to one or more of the excepted activities (non-construction activities [assessments, surveys, etc.], maintenance of the surrounding landscape at existing facilities, or limited to activities that do not cause stressors to bat species) this project does not qualify for the *Rangewide Programmatic Informal Consultation for the Indiana bat and Northern Long-eared bat (NLEB)*. A bridge inspection occurred on June 26, 2024, and no evidence of bats or birds was observed (Appendix C, page 30). A standard coordination letter was prepared and submitted for INDOT review. INDOT reviewed the standard coordination letter and submitted it to USFWS for review on August 9, 2024. USFWS issued a concurrence letter on October 9, 2024, with the "Not Likely to Adversely Affect" finding (Appendix C, pages 24 to 27). USFWS also noted that tree clearing will occur between November 15 to March 30 to avoid the fall swarming period near the NLEB hibernacula and within the NLEB hibernacula buffer. Avoidance and Minimization Measures (AMMs) covering lighting and tree removal (General AMM1, Lighting AMM1, and Tree Removal AMMs 1 to 4) are included as firm commitments in the Environmental Commitments section of this document.

A bridge inspection occurred on June 26, 2024, and no evidence of bats or birds was observed at the bridge (Appendix C, page 30). USFWS Bridge/Structure Assessments are only valid for two years. If construction will begin after June 26, 2026, an inspection of the

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structure by a qualified individual must be performed. Inspection of the structure should check for presence of bats/bat indicators and/or presence of birds. The results of the inspection must indicate no signs of bats or birds. If signs of bats or birds are documented during this inspection, the INDOT District Environmental Manager must be contacted immediately. This firm commitment is included in the *Environmental Commitments* section of this document.

The official species list generated from IPaC indicated four other species present within the project area. The tricolored bat (*Perimyotis subflavus*, proposed endangered), whooping crane (*Grus americana*, experimental population, non-essential), salamander mussel (*Simpsonaias ambigua*, proposed endangered), and the Monarch butterfly (*Danaus plexippus*, candidate) are potentially within the project area. These species are proposed endangered, candidate or experimental populations and USFWS's response on October 9, 2024, noted that with the application of AMMs such as winter tree-clearing and minimizing lighting effects, the USFWS concurs that that project is not likely to adversely affect the protected bat species noted and these additional species (Appendix C, page 25). AMMs for protected bats will reduce concerns over impacts to the tricolored bat. The project qualifies for the most current INDOT/USFWS agreement. No impacts to the other species are anticipated.

Migratory Birds

Bridge no. P000-40-07088B (NBI #060380) over the Muscatatuck River and the project's surrounding habitat is conducive for use (i.e., nests) by a bird species protected under the Migratory Bird Treaty Act (MBTA). Prior to the start of nesting season (May 1), the structure must be inspected for birds or signs of birds. If birds or signs of birds are found during the inspection, avoidance and minimization measures must be implemented prior to the start of and during the nesting season. Nests without eggs or young should be removed prior to construction during the non-nesting season (September 8 – April 30) and during the nesting season if no eggs or young are present. Nests with eggs or young cannot be removed or disturbed during the nesting season (May 1 – September 7). Nests with eggs or young should be screened or buffered from active construction. Details of the required procedures are outlined in the "Potential Migratory Bird on Structure" Recurring Special Provision (RSP). This firm commitment is included in the *Environmental Commitments* of this document.

The IDNR-DFW responded on October 25, 2023, stating, "If any bird nests with eggs or young are found on the existing structures, do not work on the bridges from March 15 through September 7. If construction is planned during this time and active nests are present, prior approval from the USDA must be secured by contacting Wildlife Services State Director, USDA Wildlife Services, 901 W. State Street, W. Lafayette, IN 47907; (765) 494-6229; request Form 37 and any other required documentation and follow the USDA's instructions" (Appendix C, pages 8 to 9). This has been added as a firm commitment in the *Environmental Commitments* section of this CE document.

This precludes the need for further consultation on this project as required under Section 7 of the Endangered Species Act, as amended. If new information on endangered species at the site becomes available or if project plans are changed, USFWS will be contacted for consultation.

Geological and Mineral Resources

- Project located within the Indiana Karst Region
- Karst features identified within or adjacent to the project area
- Oil/gas or exploration/abandoned wells identified in the project area

Yes	No
<input checked="" type="checkbox"/>	<input type="checkbox"/>
<input checked="" type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>

Date Karst Evaluation reviewed by INDOT EWPO (if applicable): N/A

Discuss if project is located in the Indiana Karst Region and if any karst features have been identified in the project area (from RFI). Discuss response received from IGWS coordination. Discuss if any mines, oil/gas, or exploration/abandoned wells were identified and if impacts will occur. Include discussion of karst study/report was completed and results. (Karst investigation must comply with the current Protection of Karst Features during Planning and Construction guidance and coordinated and reviewed by INDOT EWPO)

Inside karst area: presence

Based on a desktop review and the Indiana Karst Region map, the project is located in the designated Indiana Karst Region as outlined in the most current *Protection of Karst Features during Project Development and Construction*. According to the topo map of the project area (Appendix B, page 2) and the RFI report (Appendix E, pages 1 to 8), there are karst features identified within or adjacent to the project area. The project area is within a cave entrance density polygon and is approximately 0.03 mile north of a sinkhole area, but no impacts to the karst feature is expected due to the work being completed in previously disturbed soils and within existing ROW. An additional karst evaluation was not required according to the coordination with INDOT EWPSO (Appendix C, pages 28 to 29). They

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indicated that the project should not extend to the mapped sinkhole area to the south, and the project area does not appear to contain surface-level karst features. In addition, INDOT EWPSO noted that the scope of work is limited to minimal excavation within close proximity to the roadway. The discovery of karst features unique special provision (USP) is included as a firm commitment in the *Environmental Commitments* section of this CE document. In the early coordination response from September 25, 2023, IGWS did indicate there is potential for karst features, a 1% annual chance flood hazard, and a high potential for bedrock resources. There are no sand and gravel or active or abandoned mineral resources extraction sites documented in the area (Appendix C, pages 5 to 6). Response from IGWS was communicated to the designer on September 25, 2023. No impacts are expected.

SECTION C – OTHER RESOURCES

Drinking Water Resources

Wellhead Protection Area(s)
Source Water Protection Area(s)
Water Well(s)
Urbanized Area Boundary
Public Water System(s)

Presence

Impacts

Yes	No

Is the project located in the St. Joseph Sole Source Aquifer (SSA):

If Yes, is the FHWA/EPA SSA MOU Applicable?

If Yes, is a Groundwater Assessment Required?

Yes	No
	X

Check the appropriate boxes and discuss each topic below. Provide details about impacts and summarize resource-specific coordination responses and any mitigation commitments. Reference responses in the Appendix.

Outside of Sole Source Aquifer (SSA)

The project is located in Jennings County, which is not located within the area of the St. Joseph Sole Source Aquifer, the only legally designated sole source aquifer in the state of Indiana. Therefore, the FHWA/EPA/INDOT Sole Source Aquifer Memorandum of Understanding (MOU) is not applicable to this project, a detailed groundwater assessment is not needed, and no impacts are expected.

Not located in a Wellhead Protection Area or Source Water Area

The IDEM Wellhead Proximity Determinator website (<http://www.in.gov/idem/cleanwater/pages/wellhead/>) was accessed on August 22, 2024, by Egis. This project is not located within a Wellhead Protection Area or Source Water Area. No impacts are expected.

No wells present, no impacts

The IDNR Water Well Record Database website (<https://www.in.gov/dnr/water/3595.htm>) was accessed on August 22, 2024, by Egis. No wells are located near this project. Therefore, no impacts are expected.

Not in an Urban Area Boundary Location

Based on a desktop review of IDEM's MS4 Boundaries Map for Indiana website (<https://www.in.gov/idem/cleanwater/ms4s-boundaries-map-for-indiana/>) by Egis on August 22, 2024, this project is not located in an Urban Area Boundary. No impacts are expected.

Not in a Public Water System Location

Based on a desktop review, a site visit on July 11, 2023, by Egis, and the aerial map of the project area (Appendix B, page 3), no public water systems were identified. Therefore, no impacts are expected.

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Floodplains

Project located within a regulated floodplain
 Longitudinal encroachment
 Transverse encroachment
 Homes located in floodplain within 1000' up/downstream from project

Presence

X
X

Impacts

Yes	No
X	
X	

If applicable, indicate the Floodplain Level?

Level 1 ☐

Level 2 ☐

Level 3 ☒

Level 4 ☐

Level 5 ☐

Use the IDNR Floodway Information Portal to help determine potential impacts. Include floodplain map in appendix. Discuss impacts according to the classification system. If encroachment on a flood plain will occur, coordinate with the Local Flood Plain Administrator during design to insure consistency with the local flood plain planning.

In floodplain

Based on a desktop review of the IDNR Indiana Floodway Information Portal website (<https://indnr.maps.arcgis.com/apps/webappviewer>) by Egis on December 8, 2023, and the RFI report, this project is located in a regulatory floodplain as determined from approved IDNR floodplain map (Appendix F, page 8). An early coordination letter was sent on September 25, 2023, to the local Floodplain Administrator. The floodplain administrator did not respond within the 30-day time frame.

This project qualifies as a Category 3 per the current INDOT CE Manual, which states the modifications to drainage structures included in this project will result in an insubstantial change in their capacity to carry flood water. This change could cause a minimal increase in flood heights and flood limits. These minimal increases will not result in any substantial adverse impacts on the natural and beneficial floodplain values; they will not result in a substantial change in flood risks or damage; and they do not have substantial potential for interruption or termination of emergency service or emergency routes; therefore, it has been determined that this encroachment is not substantial.

Farmland

Agricultural Lands
 Prime Farmland (per NRCS)

Presence

Impacts

Yes	No

Total Points (from Section VII of CPA-106/AD-1006*) _____

*If 160 or greater, see CE Manual for guidance.

Discuss existing farmland resources in the project area, impacts that will occur to farmland, and mitigation and minimization measures considered.

No presence, no impact

Based on a desktop review, a site visit on July 11, 2023, by Egis, the aerial map of the project area (Appendix B, page 3), there is no land that meets the definition of farmland under the Farmland Protection Policy Act (FPPA) within or adjacent to the project area. The requirements of the FPPA do not apply to this project; therefore, no impacts are expected.

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SECTION D – CULTURAL RESOURCES

Minor Projects PA	Category(ies) and Type(s)	INDOT Approval Date(s)	N/A
			X

Full 106 Effect Finding

No Historic Properties Affected ☒ No Adverse Effect ☐ Adverse Effect ☐

Eligible and/or Listed Resources Present

NRHP Building/Site/District(s) ☐ Archaeology ☐ NRHP Bridge(s) ☒

Documentation Prepared (mark all that apply)

APE, Eligibility and Effect Determination
800.11 Documentation
Historic Properties Report or Short Report
Archaeological Records Check and Assessment
Archaeological Phase Ia Survey Report
Archaeological Phase Ic Survey Report
Other: HBAA

☒
☒
☒
☒
☐
☐
☒

ESD Approval Date(s)

August 14, 2024
August 14, 2024
February 28, 2024
August 25, 2023

October 20, 2023

SHPO Approval Date(s)

September 13, 2024
September 13, 2024
April 3, 2024
September 25, 2023

April 3, 2024

Memorandum of Agreement (MOA)

☐

MOA Signature Dates (List all signatories)

If the project falls under the MPPA, describe the category(ies) that the project falls under and any approval dates. If the project requires full Section 106, use the headings provided. The completion of the Section 106 process requires that a Legal Notice be published in local newspapers. Please indicate the publication date, name of the paper(s) and the comment period deadline. Include any further Section 106 work which must be completed at a later date, such as mitigation from a MOA or avoidance commitments.

INDOT, acting on behalf of the FHWA, is required to comply with Section 106 of the National Historic Preservation Act of 1966 as amended (Section 106) and its implementing federal regulation, 36 CFR 800. Section 106 and 36 CFR 800 outline a process that requires INDOT to evaluate the effects of its undertakings on properties that are listed on or eligible for listing on the NRHP. The following information summarizes the steps taken to identify the cultural resources listed on or eligible for listing on the NRHP and the expected impacts the proposed project would have on those resources. Per the terms of the "Programmatic Agreement Regarding Management and Preservation of Indiana's Historic Bridges" (Historic Bridges PA), the FHWA-Indiana Division will satisfy its Section 106 responsibilities involving "Select" and "Non-Select" bridges through the Project Development Process (PDP) of the Historic Bridges PA (Stipulation III). Bridge No. P000-40-07088B (NBI No. 60380) has been classified as a "Non-Select" bridge by the INDOT-sponsored *Indiana Historic Bridge Inventory*. Thus, the procedures outlined in Stipulation III.B of the Historic Bridges PA will be followed to fulfill FHWA's Section 106 responsibilities for the bridge. Additionally, because the rehabilitation of the bridge is the preferred alternative, the standard treatment approach described in Attachment B of the Historic Bridges PA (Standard Treatment Approach for Historic Bridges) will be followed.

Area of Potential Effect (APE):

Per 36 CFR 800.16(d), the APE is defined as the "geographic area or areas within which an undertaking may directly or indirectly cause alterations in the character or use of historic properties if any such properties exist. The APE is influenced by the scale and nature of an undertaking and may be different for different kinds of effects caused by the undertaking," (Appendix D, page 33). The APE for this project was determined to include the project area and a 250-foot buffer zone based on topography and vegetation surrounding the project area. Visual impacts were the driving factor in the creation of the APE. The APE takes into account the potential direct and indirect effects of the proposed project within the immediate contextual setting, which is comprised of a gradually sloping, heavily forested area surrounding the bridge. A map illustrating the APE limits as described is provided in Appendix D, pages 34 to 35. No other resources aside from the subject structure were located within the APE.

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Coordination with Consulting Parties:

Section 106 of the National Historic Preservation Act (NHPA) of 1966, as amended, requires Federal Agencies or their representatives to take into account the effects of their undertakings on historic properties. In accordance with 36 CFR 800.2(c) and the INDOT Cultural Resources Manual, consulting parties were invited to participate in efforts to identify historic properties potentially affected by this undertaking, assess its effects, and seek ways to avoid, minimize, or mitigate any adverse effects on historic properties. On August 25, 2023, the following consulting parties were sent an early coordination letter with project information and invited to participate in the Section 106 evaluation process (Appendix D, pages 1 to 7). The State Historic Preservation Officer (SHPO) is an automatic consulting party due to their mandatory or designated roles as specified in 36 CFR § 800.2. Other parties that accepted consulting party status are shown in boldface type below.

Consulting Party	Response
Indiana SHPO	September 25, 2023
Indiana Department of Natural Resources (IDNR)	No response
Crosley Fish and Wildlife Area	No response
HistoricBridges.org	August 25, 2023
Historic Hoosier Bridges	August 28, 2023
Historic Spans Task Force	August 25, 2023
Indiana Landmarks Southern Regional Office	September 6, 2023
Jennings County Auditor	No response
Jennings County Commissioners	No response
Jennings County Highway Superintendent	No response
Jennings County Historical Society	No response
Southeastern Indiana Regional Planning Commission	No response
Delaware Tribe of Indians	No response
Eastern Shawnee Tribe of Oklahoma	October 12, 2023
Miami Tribe of Oklahoma	September 11, 2023
Osage Nation	No response
Peoria Tribe of Indians of Oklahoma	No response
Pokagon Band of Potawatomi Indians	No response
Shawnee Tribe	No response

In response to the early coordination letter on August 25, 2023, Historic Spans Task Force, Indiana Landmarks Southern Regional Office, Miami Tribe of Oklahoma, SHPO, and the Eastern Shawnee Tribe responded to the early coordination letter via email, accepting consulting party status (Appendix D, pages 8 to 22).

On August 25, 2023, a representative of the Indiana Historic SPANs Taskforce responded, accepting consulting party status and stating their support of the stated goal of a bridge rehabilitation (Appendix D, pages 8 to 10). On August 25, 2023, a representative of HistoricBridges.org responded to the early coordination letter via email and accepted consulting party status, and stated "I would like to express strong support for the preservation of this bridge. I am baffled that the bridge is considered "Non-Select" as this bridge is an unusually narrow highway bridge and that speaks to the bridge's original owner... which as I understand accommodated a retreat for famous businessman and inventor Powel Crosley Jr. who is noted in America's automotive history as an early promotor of compact cars. My understanding is that this bridge was specifically built to Crosley's specifications with a narrow deck as Crosley had no need of a wider bridge to accommodate his compact cars. It is therefore significant beyond its engineering design, but as a remarkable and unique surviving bridge that embodies this famous/noted man's interest in compact cars and is a tangible remnant of this retreat which is indeed noted as "Crosley Fish and Wildlife Area" and in my view is a historic centerpiece to the area. When I visited this bridge, I was not fully aware of this history. It would be nice to see interpretive signage added as part of this project describing Crosley and the bridge to make its significance clear to visitors." INDOT Cultural Resources Office (CRO) [former historic bridge specialist responded to the email on August 29, 2023, stating that they were added to the list of consulting parties for the project, and stated, "With regard to the bridge's Non-Select status, that was due to the bridge's low structural capacity and narrow width. The way the analysis fell out with those two items put the bridge on the Non-Select list. This does not mean that the bridge cannot be preserved. In fact, the plan is to preserve it. Given its location in the Crosley Fish & Wildlife Area, those limitations are not as big a deal as they would be on a more traveled public roadway. With regard to potential interpretive signage, there is nothing in the Historic Bridges Programmatic Agreement that requires it. That would be up to DNR if they would want to pursue interpretive signage," (Appendix D, pages 11 to 13).

On August 28, 2023, a representative of Historic Hoosier Bridges responded to the early coordination letter via email and accepted consulting party status, and stated, "While vetting the Mead & Hunt determinations for Select/Non-Select bridges, Jim Cooper (Dr.

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James L. Cooper, the leading expert on historic bridges in Indiana) and I had a discussion about this one. While my initial reaction was to lobby for a reclassification, Jim was concerned with a possible backlash given the narrow width. We ultimately decided that it might be best to just let it lay low. I agree with the HistoricBridges.org representative wholeheartedly about the importance of this span and its preservation. Fortunately, that sentiment is shared by the park staff and preservationists in the area. There is nothing to suggest that anyone wants it gone...I would certainly help lobby for an historic marker once the bridge is rehabilitated." INDOT CRO responded to the Historic Hoosier Bridges email on August 29, 2023, stating that they "...copied the consultant as well as DNR staff, so they can be aware of the additional vote for an interpretive sign. We look forward to your continued participation as the project progresses," (Appendix D, pages 14 to 15).

In an email dated September 6, 2023, the Southern Regional Office of Indiana Landmarks responded, accepting consulting party status. Additionally, they noted the ZIP code and phone number of their office were listed incorrectly and provided the correct information (Appendix D, pages 16 to 17).

In a letter dated September 11, 2023, the Miami Tribe of Oklahoma responded, accepting consulting party status. Additionally, they "offer no objection to the above-referenced project at this time, as we are not currently aware of existing documentation directly linking a specific Miami cultural or historic site to the project site. However, given the Miami Tribe's deep and enduring relationship to its historic lands and cultural property within present-day Indiana, if any human remains or Native American cultural items falling under the Native American Graves Protection and Repatriation Act (NAGPRA) or archaeological evidence is discovered during any phase of this project, the Miami Tribe requests immediate consultation with the entity of jurisdiction for the location of discovery," (Appendix D, page 18).

On September 25, 2023, SHPO staff responded to the early coordination letter via a letter stating for Reynolds to include a list of accepted consulting parties in the next correspondence. They also agreed that no archaeological investigations need to occur for the proposed project (Appendix D, pages 19 to 21).

On October 12, 2023, the Eastern Shawnee Tribe of Oklahoma responded to the early coordination letter via a letter. They stated that they "...find our people occupied these areas historically and/or prehistorically. However, the project proposes NO Adverse Effect or endangerment to known sites of interest to the Eastern Shawnee Tribe. Please continue project as planned. However, should this project inadvertently discover an archeological site or object(s) we request that you immediately contact the Eastern Shawnee Tribe, as well as the appropriate state agencies (within 24 hours). We also ask that all ground disturbing activity stop until the Tribe and State agencies are consulted. Please note that any future changes to this project will require additional consultation," (Appendix D, page 22).

Consulting parties decided not to pursue classification as a Select bridge as long as the historic integrity of the bridge would be maintained.

Archaeology:

An archaeology records review of State Historic Architectural and Archaeological Research Database (SHAARD) records was completed by CRA. Ground disturbances associated with the project as planned appear to be on existing paved areas, previously disturbed soils adjacent to the roadway, or steeply sloping landforms on the side and underneath the bridge. A review of SHAARD records revealed that there are no previously recorded archaeological sites within or adjacent to the current project area.

SHPO responded on September 25, 2023, and stated that no archaeological investigations appear necessary provided all project activities remain within areas disturbed by previous construction (Appendix D, page 20).

Historic Properties:

A Historic Property Short Report (HPSR) was completed by Cultural Resource Analysts Inc. (CRA) (Reynolds, February 28, 2024). CRA recommended that Bridge P000-40-07088B, a "Non-Select" bridge, continues to be eligible for listing in the NRHP under Criterion C for its unique architectural and engineering design. No additional resources were identified as a result of survey efforts. The summary of the HPSR can be found in Appendix D, pages 28 to 46.

An HBAA was completed by Egis (formerly Beam, Longest and Neff, LLC) and approved by INDOT on October 20, 2023. Two alternatives for the proposed bridge project were discussed: Alternative A: The No Build/Do Nothing Alternative and Alternative B.1: Rehabilitation of the Existing Bridge for Continue Vehicular Use (Two-Way [One Lane] Option) Meeting Secretary of Interior's Standards (SOIS) for Rehabilitation. Alternative B.1 was recommended as the preliminary preferred alternative by Egis, which proved to be both feasible and prudent, enabling the bridge to be usable while maintaining its historical integrity, resulting in extending the life of the bridge by at least 25 years. A description of the rehabilitation work described in Section 1 of the Alternatives Analysis Report can be found in Appendix D, pages 47 to 56.

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On February 28, 2024, CRA distributed the HPSR and HBAA via email and the INDOT Section 106 Consultation and Outreach Portal Enterprise (IN SCOPE) to consulting parties for a 30-day review and comment period (Appendix D, pages 23 to 27).

On March 14, 2024, the Indiana Historic Spans Taskforce responded to the HPSR, archaeological assessment, and HBAA, stating that they were "...pleased to see the preliminary preferred alternative for this project is identified as: B1 Rehabilitation for Continued Vehicular Use Meeting the SOI Standards for rehabilitation and look forward to continued consultation during the project development," (Appendix D, pages 57 to 58).

The SHPO responded to the HPSR, archaeological assessment, and HBAA in a letter dated April 3, 2024. They agreed with the size of the APE, the archaeological assessment, and the recommendation that Bridge P000-40-07088B continues to be eligible for listing in the NRHP. They also agreed with the results of the HBAA, recognizing that Alternative B.1 is both feasible and prudent, and requested that the bridge be photographically documented (Appendix D, pages 59 to 61). The requested photo documentation was submitted to SHPO in conjunction with the 800.11(e) documentation, discussed below.

Documentation Findings:

On April 3, 2024, SHPO responded to the dual review request of the HPSR and HBAA and concurred with the findings (Appendix D, pages 59 to 61). On August 14, 2024, INDOT, on behalf of FHWA, signed the 800.11(e) finding of "No Historic Properties Affected," and it was distributed to consulting parties along with the 30% plans (Appendix D, pages 62 to 77). In a letter dated September 13, 2024, SHPO concurred with the "No Historic Properties Affected" determination (Appendix D, pages 80 to 82). They also stated they appreciated the 30% plans provided and had no comment on this set of plans and look forward to receiving the 60% and final rehabilitation plans. SHPO stated they had no comments on the draft photographic documentation of the subject bridge and found the documentation acceptable (Appendix D, page 81). They requested the name and location of the public or not-for-profit organization that is willing to accept a copy of this documentation. The Jennings County Public Library and Jennings Historical Society both agreed to host the photo package, and CRA notified SHPO in September 2024 (Appendix D, page 83).

Per the Historic Bridge PA, the 30%, 60%, and final plan sets are required to be submitted to SHPO for review. The 30% plans were sent to consulting parties and SHPO on August 14, 2024, with the 800.11(e) documentation. The SHPO responded to the 30% plans in a letter dated September 13, 2024, stating they did not have any comments on the 30% plans (Appendix D, pages 80 to 82). The 60% plan set was sent to consulting parties and SHPO for review on October 16, 2024 (Appendix D, pages 84 to 87). SHPO responded to the 60% plans in a letter dated November 15, 2024, stating they did not have any comments on the 60% plans (Appendix D, pages 88 to 90). The final plan set will be submitted to and approved by SHPO prior to the environmental consultation form (ECF) being submitted and approved. This has been added as a firm commitment in the *Environmental Commitments* section of this CE document.

With regard to state law, pursuant to Section 11.5(f) of the rule governing dual review, at the conclusion of the SHPO's review of the final plans, it is anticipated that the Division of Historic Preservation and Archaeology's (DHPA) Division Director would issue a letter of clearance exempting this project from obtaining a Certificate of Approval (COA) from the Indiana Historic Preservation Review Board (Review Board) under IC 14-21-1-18. Obtaining a letter of clearance before ECF approval is added as a firm commitment in the *Environmental Commitments* section of this CE document.

Public Involvement:

In accordance with 36 CFR 800.2(d), 800.3(e), and 800.6(a)(4), the views of the public were sought regarding the effect of the proposed project. An advertisement was placed in the *North Vernon Plain Dealer and Sun* on August 23, 2024, offering the public an opportunity to submit comments pursuant to 36 CFR 800.2(d), 800.3(e), and 800.6(a)(4). The public comment period closed 30 days later on September 20, 2024. The text of the public notice and the affidavit of publication appear in Appendix D, pages 78 to 79. No comments were received in response to the public notice.

Per Stipulation III of the Historic Bridges PA, INDOT will hold a public hearing for the project prior to the completion of NEPA studies. All consulting parties will be notified of the public hearing. A legal notice will appear in a local publication contingent upon the release of this document for public involvement. This document will be revised after the public involvement requirements are fulfilled. FHWA's final approval of the environmental document will affirm that all Historic Bridges PA requirements have been fully addressed and will serve to confirm that FHWA has concluded its responsibilities under Section 106.

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SECTION E – SECTION 4(f) RESOURCES/ SECTION 6(f) RESOURCES

	<u>Presence</u>	<u>Use</u>	
		Yes	No
Parks and Other Recreational Land			
Publicly owned park			
Publicly owned recreation area	X		X
Other (school, state/national forest, bikeway, etc.)			
Wildlife and Waterfowl Refuges			
National Wildlife Refuge			
National Natural Landmark			
State Wildlife Area	X		X
State Nature Preserve			
Historic Properties			
Site eligible and/or listed on the NRHP	X		X
<u>Evaluations</u>			
	<u>Prepared</u>		
Programmatic Section 4(f)			
"De minimis" Impact			
Individual Section 4(f)			
Any exception included in 23 CFR 774.13	X		

Discuss Programmatic Section 4(f) and "de minimis" Section 4(f) impacts in the discussion below. Individual Section 4(f) documentation must be included in the appendix and summarized below. Discuss proposed alternatives that satisfy the requirements of Section 4(f). FHWA has identified various exceptions to the requirement for Section 4(f) approval. Refer to 23 CFR § 774.13 - Exceptions.

Section 4(f) of the U.S. Department of Transportation Act of 1966 prohibits the use of certain public and historic lands for federally funded transportation facilities unless there is no feasible and prudent alternative. The law applies to significant publicly owned parks, recreation areas, wildlife / waterfowl refuges, and NRHP eligible or listed historic properties regardless of ownership. Lands subject to this law are considered Section 4(f) resources.

Based on a desktop review, the aerial map of the project area (Appendix B, page 3), and the RFI report (Appendix E, pages 1 to 8), there are two potential 4(f) resources located within the 0.5-mile search radius. According to additional research and the site visit on July 11, 2023, by Egis, there are two 4(f) resources within or adjacent to the project area. Bridge No. P000-40-07088 B and Crosley Fish and Wildlife Area are within the project area. The HPSR did not identify any other historic Section 4(f) resources, and SHPO concurred with the finding (Appendix D, page 59). A detailed discussion of these resources is provided below.

Bridge No. P000-40-07088 B

Bridge No. P000-40-07088 B (NBI #060380) on Park Road over the Muscatatuck River is listed in the *Indiana Historic Bridge Inventory*, completed in 2010, as eligible for listing in the NRHP. It was also determined to be a "Non-Select" bridge, which are those that may not be considered excellent examples of a given type of historic bridge or may not be as suitable candidates for preservation. Per FHWA's Section 4(f) guidance, a proposed action will "use" a bridge that is on or eligible for inclusion on the NRHP when the action will impair the historic integrity of the bridge either by rehabilitation or demolition. Rehabilitation that does not impair the historic integrity of the bridge as determined by procedures implementing the Nation Historic Preservation Act of 1966, as amended, is not subject to Section 4(f).

Because this bridge falls under the Historic Bridges Programmatic Agreement and the preferred alternative is to rehabilitate the existing bridge for continued vehicular use that meets the Secretary of the Interior's Standards for Rehabilitation (Appendix D, pages 53 to 55), the project will not impair the historical integrity of the bridge. As a result, the rehabilitation of the bridge is not considered a "use" and is not subject to Section 4(f). On February 28, 2024, CRA distributed the HPSP and Alternatives Analysis via email and the INDOT Section 106 Consultation and Outreach Portal Enterprise (IN SCOPE) to consulting parties for a 30-day review and comment period. In a letter dated April 3, 2024, SHPO acknowledged receipt of the HPSR, an archaeological assessment, and the HBAA and stated they agreed with the conclusions of the Alternatives Analysis that Alternative B.1: Rehabilitation for Continued Vehicular Use, meeting

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the Secretary of the Interior's Standards for Rehabilitation is the preferred alternative for this project compared to the No Build Alternative (Appendix D, pages 59 to 61).

The historical integrity of the bridge will be maintained through coordination and consultation with the SHPO during the design phase of the project with the required plan submittals pursuant to the Historic Bridge PA. Therefore, no Section 4(f) use of the bridge will occur. FHWA approval of this CE document confirms that there is no Section 4(f) use of the bridge.

Crosley Fish and Wildlife Area

The project area is within Crosley Fish and Wildlife Area, which is owned and operated by the IDNR Division of Fish and Wildlife, which falls within the definition outlined in 23 USC 138, 23 CFR Part 774 (23 CFR 774), and the July 20, 2012, FHWA Section 4(f) Policy Paper as "any publicly owned land from a public park, recreation area, or wildlife and waterfowl refuge of national, state, or local significance as so determined by such officials." There is a boat ramp and parking lot adjacent to the bridge in the northeast corner of the project area.

Based on its public ownership status as a wildlife area and recreation area, the Crosley Fish and Wildlife Area and its boat ramp and parking lot are afforded protection under Section 4(f). This project qualifies for the Section 4(f) exemption under 23 CFR 774.13(d) as a temporary occupancy. Pursuant to 23 CFR 774.13(d), temporary occupancy results when a Section 4(f) property, in whole or in part, is required for construction-related activities. A temporary occupancy is a type of exception to Section 4(f) approval. For Section 4(f) temporary occupancy to not constitute as a Section 4(f) use, the following conditions must be satisfied:

1. Duration must be temporary, i.e., less than the time needed for construction of the project, and there should be no change in ownership of the land.
 - The boat ramp closures will be intermittent and brief and will take place only during the installation of scour countermeasures at the piers. This work will be completed before the construction of the bridge is done. The project will not require permanent or temporary right-of-way from Section 4(f) resources.
2. Scope of the work must be minor, i.e., both the nature and the magnitude of the changes to the Section 4(f) property are minimal.
 - The scope of work for this project will be minor in nature. The majority of the parking lot will remain open during construction.
3. There are no anticipated permanent adverse physical impacts, nor will there be interference with the protected activities, features, or attributes of the property, on either a temporary or permanent basis.
 - Activities on the boat ramp and parking lot will be maintained throughout construction with signs and barricades, with any interruptions being brief in nature.
4. The land being used must be fully restored (i.e., the property must be returned to a condition at least as good as what existed prior to the start of the project).
 - Land adjacent to the bridge, boat ramp, and parking lot will be fully restored upon project completion.
5. There must be documented agreement of the official(s) with jurisdiction over the Section 4(f) resource regarding the above conditions.
 - The OWJ has agreed with the temporary occupancy and has signed the OWJ letter that will serve as concurrence that the conditions have been met (Appendix H, pages 2 to 4).

The project will involve construction access and staging on the boat ramp and parking lot, which are part of the Section 4(f) property's recreational resource near the bridge. No temporary or permanent right-of-way will be needed. Minor impacts to the property will be necessary to complete the project and will consist of removing 5 trees for construction access. During the construction of the scour countermeasures at the piers, the boat ramp may require a maximum of 5 intermittent temporary closures of no longer than 8 hours for each closure. Barricades and signs will be used to temporarily block access and alert users that the boat ramp is under a temporary closure. Signs will be attached to the bridge to alert recreational users of the boat ramp closure. A portion of the parking lot will be used to allow the Contractor easy access to the bridge and provide an area to securely store materials and equipment during construction. The Contractor may not use more than a third of the parking lot area for storage, and the remaining area will remain open to the public. The Contractor will restore the existing boat ramp, timber posts, parking lot, and surrounding area to the original condition. The Contractor will replace the timber posts near the bridge when work is completed. An IDNR construction in a floodway permit is approved and no tree mitigation was deemed necessary by IDNR.

There will be no change in ownership of the land. This work will not cause temporary or permanent adverse changes to the activities,

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features, or attributes of the property, and disturbed areas will be graded to match existing contours and reseeded with a floodplain seed mix.

The project will not use these resources by taking permanent right-of-way and will not indirectly use the resource in such a way that the protected activities, features, or attributes that qualify a resource for protection under Section 4(f) are substantially impaired. Construction is anticipated to begin in Spring 2026. The existing property to be affected is primarily gravel, concrete, and a parking lot. The project will not adversely impact the recreational amenities, will not disrupt Section 4(f) attributes, and will not lead to a change in ownership or use of the property.

It has been determined that the intermittent temporary closures of the boat ramp for construction access and the temporary staging in a portion of the parking lot meet the criteria listed above and classify as temporary occupancy. The OWJ of the Section 4(f) property, Crosley Fish and Wildlife Area, acknowledged and agreed that this project meets the conditions of 23 CFR 774.13(d) in that its intent is to enhance the park by providing a structurally sound bridge to convey motorists and pedestrians over the Muscatatuck River, and that the structure is important to the activities, features, and attributes that qualify Crosley Fish and Wildlife as a Section 4(f) property (Appendix H, pages 2 to 4).

Section 6(f) Involvement

Section 6(f) Property

Presence

Use

Yes

No

☐☐☐

Discuss Section 6(f) resources present or not present. Discuss if any conversion would occur as a result of this project. If conversion will occur, discuss the conversion approval.

No presence, no impact

The U.S. Land and Water Conservation Fund Act of 1965 established the Land and Water Conservation Fund (LWCF), which was created to preserve, develop, and assure accessibility to outdoor recreation resources. Section 6(f) of this Act prohibits conversion of lands purchased with LWCF monies to a non-recreation use.

A review of 6(f) properties on the INDOT ESD website revealed a total of one property in Jennings County (Appendix H, page 1). This property is not located within or adjacent to the project area. Therefore, there will be no impacts to 6(f) resources.

SECTION F – Air Quality

STIP/TIP and Conformity Status of the Project

Is the project in the most current STIP/TIP?

Is the project located in an MPO Area?

Is the project in an air quality non-attainment or maintenance area?

If Yes, then:

Is the project in the most current MPO TIP?

Is the project exempt from conformity?

If No, then:

Is the project in the Transportation Plan (TP)?

Is a hot spot analysis required (CO/PM)?

Yes

No

X

X
X

Location in STIP:

FY 2024-2028, pages 211 and 528

Name of MPO (if applicable):

N/A

Location in TIP (if applicable):

N/A

Level of MSAT Analysis required?

Level 1a

☒

Level 1b

☐

Level 2

☐

Level 3

☐

Level 4

☐

Level 5

☐

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Describe if the project is listed in the STIP and if it is in a TIP. Describe the attainment status of the county(ies) where the project is located. Indicate whether the project is exempt from a conformity determination. If the project is not exempt, include information about the TP and TIP. Describe if a hot spot analysis is required and the MSAT Level.

Standalone Project

This project is included in the Fiscal Year (FY) 2024 to 2028 Statewide Transportation Improvement Program (STIP) (Appendix G, pages 1 to 2).

Attainment Area

This project is located in Jennings County, which is currently in attainment for all criteria pollutants according to the Environmental Protection Agency's (EPA) Green Book site (<https://www.epa.gov/green-book>). Therefore, the conformity procedures of 40 CFR Part 93 do not apply.

MSAT Level 1a Analysis

This project is of a type qualifying as a categorical exclusion (Group 1) under 23 CFR 771.117(c) or exempt under the Clean Air Act conformity rule under 40 CFR 93.126, and as such, a Mobile Source Air Toxics analysis is not required.

If necessary, the INDOT Project Manager will ensure the STIP will be updated prior to the Ready-for-Contract (RFC) date to reflect increasing costs. This has been added as a firm commitment in the *Environmental Commitments* section of this document.

SECTION G - NOISE

Noise

Yes

No

Is a noise analysis required in accordance with FHWA regulations and INDOT's traffic noise policy?

☐☒

Date Noise Analysis was approved/technically sufficient by INDOT ESD: _____

Describe if the project is a Type I or Type III project. If it is a Type I project, describe the studies completed to date and if noise impacts were identified. If noise impacts were identified, describe if abatement is feasible and reasonable and include a statement of likelihood.

Type III Project

This project is a Type III project. In accordance with 23 CFR 772 and the current Indiana Department of Transportation Traffic Noise Analysis Procedure, this action does not require a formal noise analysis.

SECTION H – COMMUNITY IMPACTS

Regional, Community & Neighborhood Factors

Will the proposed action comply with the local/regional development patterns for the area?

☒

No

Will the proposed action result in substantial impacts to community cohesion?

☐☒

Will the proposed action result in substantial impacts to local tax base or property values?

☐☒

Will construction activities impact community events (festivals, fairs, etc.)?

☐☒

Does the community have an approved transition plan?

☒

If No, are steps being made to advance the community's transition plan?

☐

Does the project comply with the transition plan? (explain in the discussion below)

☒☐

Discuss how the project complies with the area's local/regional development patterns; whether the project will impact community cohesion; and impact community events. Discuss how the project conforms with the ADA Transition Plan.

The project will benefit the community by rehabilitating the unusable bridge on Park Road over the Muscatatuck River that has been

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closed since 2019 and reopening safe access across the river in the Crosley Fish and Wildlife Area. The project is not anticipated to impact the tax base for the area or result in a division of the community. While there may be temporary inconveniences associated with construction, such as possible construction noise and fugitive dust, there are no long-term, foreseeable economic impacts from the project. A search of local festivals, fairs, and events that this project could impact was conducted on August 5, 2024, by Egis. According to a review of the Visit Indiana website (<https://www.visitindiana.com/events/>) and the Jennings County public events calendar (<https://joinusinjennings.com/events/>), there are no scheduled festivals or other public events that will be impacted as a result of this project.

As required by the Americans with Disabilities Act (ADA), Jennings County, Indiana, has developed an ADA Transition Plan dated 2017 (<https://img1.wsimg.com/blobby/go/d7d56efd-dfd0-445b-a318-d671f73354dd/downloads/2017ADATransitionPlan.pdf>). There are no pedestrian facilities within or adjacent to the project area and none are proposed.

Indirect impacts are effects caused by the action and are later in time or farther removed in distance but are still reasonably foreseeable. Indirect effects may include growth-inducing effects and other effects related to induced changes in the pattern of land use, population density, or growth rate. Cumulative impacts affect the environment, which result from the incremental impact of the action when added to other past, present, and reasonably foreseeable future actions, regardless of what agency or person undertakes such actions. This project will not contribute to or stimulate an increase in commercial or residential development in the project area or cause additional impacts in the foreseeable future. This project is not anticipated to result in substantial impacts to community cohesion because it will not change access to properties within the area or divide existing communities. The project is not expected to impact the surrounding community or cause economic impacts on the surrounding area. No indirect or cumulative impacts are expected as a result of the project.

Public Facilities and Services

Discuss what public facilities and services are present in the project area and the impacts (such as MOT) that will occur to them. Include how the impacts have been minimized and what coordination has occurred. Some examples of public facilities and services include health facilities, educational facilities, public and private utilities, emergency services, religious institutions, airports, transportation, or public pedestrian and bicycle facilities.

Presence, with impacts

Based on a desktop review, the aerial map of the project area (Appendix B, page 3), and the RFI report (Appendix E, pages 1 to 8), there is one managed land located within 0.5 mile of the project area. The project area is within one managed land, Crosley Fish and Wildlife Area. That number was confirmed by the site visit on July 11, 2023, by Egis.

The project involves the rehabilitation of Bridge No. P000-40-07088B, which conveys Park Road over the Muscatatuck River. Due to the nature of the work, the bridge will remain closed during construction, which is anticipated to last four months.

It is the responsibility of the project sponsor to notify school corporations and emergency services at least two weeks prior to any construction that would block or limit access.

Relocation of People, Businesses or Farms

Will the proposed action result in the relocation of people, businesses or farms?

Is a BIS or CSRS required?

Yes No

	X
	X

Number of relocations: Residences: 0 Businesses: 0 Farms: 0 Other: 0

Discuss any relocations that will occur due to the project. If a BIS or CSRS is required, discuss the results in the discussion below.

No Relocations

No relocations of people, businesses, or farms will take place as a result of this project.

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SECTION I – HAZARDOUS MATERIALS & REGULATED SUBSTANCES

Hazardous Materials & Regulated Substances (Mark all that apply)

Red Flag Investigation (RFI)
Phase I Environmental Site Assessment (Phase I ESA)
Phase II Environmental Site Assessment (Phase II ESA)
Design/Specifications for Remediation required?

Documentation

X

Date RFI concurrence by INDOT SAM (if applicable): December 20, 2023

Include a summary of the potential hazardous material concerns found during review. Discuss in depth sites found within, directly adjacent to, or ones that could impact the project area. Refer to current INDOT SAM guidance. If additional documentation (special provisions, pay quantities, etc.) will be needed, include in discussion. Include applicable commitments.

No presence

Based on a review of Geographic Information Systems (GIS) and available public records, the RFI was completed on December 14, 2023, by Egis, and INDOT Site Assessment and Management (SAM) provided their concurrence on December 20, 2023 (Appendix E, pages 1 to 8). No sites with hazardous material concerns (hazmat sites) or sites involved with regulated substances were identified in or within 0.5 mile of the project area. Further investigation for hazardous material concerns or regulated substances is not required at this time.

Part IV – Permits and Commitments

PERMITS CHECKLIST

Permits (mark all that apply)

Likely Required

Army Corps of Engineers (404/Section10 Permit)

Nationwide Permit (NWP)
Regional General Permit (RGP)
Individual Permit (IP)
Other

X

IN Department of Environmental Management (401/Rule 5)

Nationwide Permit (NWP)
Regional General Permit (RGP)
Individual Permit (IP)
Isolated Wetlands
Construction Stormwater General Permit (CSGP)
Other

X

IN Department of Natural Resources

Construction in a Floodway
Navigable Waterway Permit
Other

X

Mitigation Required

US Coast Guard Section 9 Bridge Permit

Others (Please discuss in the discussion below)

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List the permits likely required for the project and summarize why the permits are needed, including permits designated as "Other."

A Section 401 Water Quality Certification and a Section 404 Permit for Discharge of Dredge or Fill Material below the Ordinary High-Water Mark are required for projects with any impacts to waterways. An IDNR Construction in a Floodway Permit is required for construction and was submitted to IDNR on July 16, 2024. The IDNR permit was approved and is effective on 2/1/2025 and no tree mitigation is required.

Coordination with local agencies and offices indicated that no local permits are required. No other permits are anticipated to be required for this project.

Applicable recommendations provided by resource agencies are included in the Environmental Commitments section of this document. If permits are found to be necessary, the conditions of the permit will be requirements of the project and will supersede these recommendations.

It is the responsibility of the project sponsor to identify and obtain all required permits.

ENVIRONMENTAL COMMITMENTS

List all commitments and include the name of agency/organization requesting/requiring the commitment(s). Listed commitments should be numbered.

Firm:

- 1) If the scope of work or permanent or temporary right-of-way amounts change, the INDOT Environmental Services Division (ESD) and the INDOT District Environmental Section will be contacted immediately. (INDOT ESD and INDOT Seymour District)
- 2) It is the responsibility of the project sponsor to notify school corporations and emergency services at least two weeks prior to any construction that would block or limit access. (INDOT ESD)
- 3) Hibernacula AMM 1: For projects located within karst areas, on-site personnel will use best management practices, secondary containment measures, or other standard spill prevention and countermeasures to avoid impacts to possible hibernacula. Where practicable, a 300-foot buffer will be employed to separate fueling areas and other major containment risk activities from caves, sinkholes, losing streams, and springs in karst topography. (USFWS)
- 4) General AMM 1: Ensure all operators, employees, and contractors working in areas of known or presumed bat habitat are aware of all FHWA/FRA/FTA (Transportation Agencies) environmental commitments, including all applicable AMMs. (USFWS)
- 5) Lighting AMM 1: Direct Temporary lighting away from suitable habitat during the active season. (USFWS)
- 6) Tree Removal AMM 1: Modify all phases/aspects of the project (e.g., temporary work areas, alignments) to avoid tree removal. (USFWS)
- 7) Tree Removal AMM 2: Apply time of year restrictions for tree removal when bats are not likely to be present (November 15 to March 31 in fall swarming areas), or limit tree removal to 10 or fewer trees per project at any time of year within 100 feet of existing road/rail surface and outside of documented roosting/foraging habitat or travel corridors; visual emergence survey must be conducted with no bats observed. Do not cut any trees suitable for Indiana bat or Northern Long-eared bat roosting (3 inches or greater diameter-at-breast height, living or dead, with loose hanging bark, or with cracks, crevices, or cavities) from April 1 through September 30. (USFWS, IDNR DFW)
- 8) Tree Removal AMM 3: Ensure tree removal is limited to that specified in project plans and ensure that contractors understand clearing limits and how they are marked in the field (e.g., install bright colored flagging/fencing prior to any tree clearing to ensure contractors stay within clearing limits). (USFWS)

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- 9) Tree Removal AMM 4: Do not remove documented Indiana bat or NLEB roosts that are still suitable for roosting, or trees within 0.25 mile of roosts, or documented foraging habitat any time of year. (USFWS)
- 10) USFWS Bridge/Structure Assessments are only valid for two years. If construction will begin after June 26, 2026, an inspection of the structure by a qualified individual must be performed. Inspection of the structure should check for presence of bats/bat indicators and/or presence of birds. The results of the inspection must indicate no signs of bats or birds. If signs of bats or birds are documented during this inspection, the INDOT District Environmental Manager must be contacted immediately. (INDOT ESD)
- 11) Bridge no. P000-40-07088B over the Muscatatuck River and the project's surrounding habitat is conducive for use (i.e. nests) by a bird species protected under the Migratory Bird Treaty Act (MBTA). Prior to the start of nesting season (May 1), the structure must be inspected for birds or signs of birds. If birds or signs of birds are found during the inspection avoidance and minimization measures must be implemented prior to the start of and during the nesting season. Nests without eggs or young should be removed prior to construction during the non-nesting season (September 8 – April 30) and during the nesting season if no eggs or young are present. Nests with eggs or young cannot be removed or disturbed during the nesting season (May 1 – September 7). Nests with eggs or young should be screened or buffered from active construction. Details of the required procedures are outlined in the "Potential Migratory Bird on Structure" Recurring Special Provision (RSP). (INDOT EWPSO)
- 12) The project will follow the INDOT "Protection of Karst Features During Project Development and Construction" guidance. If unknown karst features are discovered during construction, all work within 100 feet of the feature shall stop and the Engineer shall be notified immediately. Karst features include, but are not limited to voids, caves, sinking streams, and sinkholes. INDOT will provide the treatment measures to be incorporated for the feature. The karst feature shall be protected from sedimentation runoff. Work shall not resume in the area until directed by the Engineer. (INDOT EWPSO)
- 13) The 60% and final plan sets need to be approved by SHPO prior to ECF approval. (SHPO)
- 14) In order to comply with IC 14-21-1-18, a COA must be obtained from the Review Board or a director's letter of clearance must be obtained from the Indiana DHPA exempting the project from obtaining a COA. This must be completed before ECF approval. (SHPO)
- 15) If necessary, the INDOT Project Manager will ensure the STIP will be updated prior to the Ready-for-Contract (RFC) date to reflect increasing costs. (INDOT ESD)
- 16) A portion of the parking lot may be used to allow the Contractor easy access to the bridge and provide an area to store materials and equipment during construction, but it will remain open to the public. (INDOT ESD)
- 17) For the Crosley Fish and Wildlife Area, the scope of the work must be minor, i.e., both the nature and the magnitude of the changes to the Section 4(f) property are minimal; The scope of work for this project will be minor in nature. The boat ramp may require intermittent temporary closures, and the parking lot will remain open during construction. (INDOT ESD)
- 18) For the Crosley Fish and Wildlife Area, there are no anticipated permanent adverse physical impacts, nor will there be interference with the protected activities, features, or attributes of the property, on either a temporary or permanent basis; Activities on the boat ramp and parking lot will be maintained throughout construction with signs, barricades, and tape. (INDOT ESD)
- 19) If any bird nests with eggs or young are found on the existing structures, do not work on the bridges from April 1 through September 7. If construction is planned during this time and active nests are present, prior approval from the USDA must be secured by contacting Wildlife Services State Director, USDA Wildlife Services, 901 W. State Street, W. Lafayette, IN 47907; (765) 494-6229; request Form 37 and any other required documentation and follow the USDA's instructions. (IDNR DFW)
- 20) Post DO NOT DISTURB signs at the construction zone boundaries and do not clear trees or understory vegetation outside the boundaries. (USFWS)
- 21) Suitable habitat for Cerulean Warblers exists within the project area. To minimize impacts to this species, remove trees outside of the spring migratory season and the breeding season (early April and late August). (IDNR DFW)

Indiana Department of Transportation

County Jennings

Route Park Road

Des. No. 2200148

- 22) To avoid impacts to the Little Spectaclecase, do not construct any causeways or cross the stream using heavy equipment. (IDNR DFW)
- 23) Implement pollution prevention and control measures during construction to reduce the potential for hazardous chemicals, construction material, and debris to enter the river. Any material that inadvertently enters the river should be removed as soon as possible by lifting it out as opposed to dragging it over or through the streambed. (USFWS)
- 24) Place refueling staging areas, fuel storage, and hazardous materials away from the river. If hydro-demolition is required, some sort of tarp or collection system should be in place to prevent debris from falling into the river. (USFWS)
- 25) For the Crosley Fish and Wildlife Area, duration must be temporary, i.e., less than the time needed for construction of the project, and there should be no change in ownership of the land. (INDOT ESD)
- 26) For the Crosley Fish and Wildlife Area, the land being used must be fully restored (i.e., the property must be returned to a condition at least as good as what existed prior to the start of the project). (INDOT ESD)
- 27) The boat ramp may require a maximum of 5 intermittent temporary closures of no longer than 8 hours for each closure. (INDOT ESD)
- 28) Barricades or tape will be used to temporarily block access to the boat ramp. Signs will be attached to the bridge to alert recreational users of the boat ramp closure. (INDOT ESD)
- 29) A portion of the parking lot may be used to allow the Contractor easy access to the bridge and provide an area to securely store materials and equipment during construction. The Contractor may not use more than a third of the parking lot area for storage, and the remaining area will remain open to the public. (INDOT ESD)

For Further Consideration:

- 30) Impacts to non-wetland forest of one (1) acre or more should be mitigated at a minimum 2:1 ratio. If less than one acre of non-wetland forest is removed in a rural setting, replacement should be at a 1:1 ratio based on area. Impacts to non-wetland forest under one (1) acre in an urban setting should be mitigated by planting five trees, at least 2 inches in diameter-at-breast height (dbh), for each tree which is removed that is 10 inches dbh or greater (5:1 mitigation based on the number of large trees). (IDNR DFW)
- 31) Evaluate wildlife crossings under bridges/culverts projects in appropriate situations. Suitable crossings include flat areas below bridge abutments with suitable ground cover, high water shelves in culverts, amphibian tunnels and diversion fencing. (USFWS)
- 32) Do not excavate in the low flow area except for the placement of piers, foundations, and riprap, or removal of the old structure. (IDNR DFW)
- 33) Do not construct any temporary runarounds, access bridges, causeways, cofferdams, diversions, or pumparounds. (IDNR DFW)
- 34) Use a minimum average 6-inch graded riprap stone extended below the normal water level to provide habitat for aquatic organisms in the voids. (IDNR DFW)
- 35) Implement pollution prevention and control measures during construction to reduce the potential for hazardous chemicals, construction material, and debris to enter the river. Any material that inadvertently enters the river should be removed as soon as possible by lifting it out as opposed to dragging it over or through the streambed. (USFWS)
- 36) Restrict below low-water work to placement of piers, pilings and/or footings, shaping of the spill slopes around the bridge abutments, and placement of riprap. (USFWS)
- 37) Avoid all work within the inundated part of the stream channel (in perennial streams and larger intermittent streams) during the fish spawning season (April 1 through June 30), except for work within sealed structures such as caissons or cofferdams that were installed prior to the spawning season. No equipment should be operated below Ordinary High-Water Mark during this time unless the machinery is within the caissons or on the cofferdams. (USFWS)

This is page 28 of 29 Project name: Park Road over Muscatatuck River Bridge Date: March 21, 2025
Rehabilitation, P000-40-07088B

Indiana Department of Transportation

County Jennings

Route Park Road

Des. No. 2200148

38) Re-vegetate all disturbed soil areas immediately upon project completion, using native trees and shrubs in the riparian zone wherever possible. (USWFS)

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Appendix A:

INDOT Supporting Documentation

Categorical Exclusion Level Thresholds

	PCE	Level 1	Level 2	Level 3	Level 4 ¹
Section 106	Falls within guidelines of Minor Projects PA	“No Historic Properties Affected”	“No Adverse Effect”	-	“Adverse Effect” Or Historic Bridge involvement ²
Stream Impacts³	No construction in waterways or water bodies	< 300 linear feet of stream impacts	≥ 300 linear feet of stream impacts	-	USACE Individual 404 Permit ⁴
Wetland Impacts³	No adverse impacts to wetlands	< 0.1 acre	-	< 1.0 acre	≥ 1.0 acre
Right-of-way⁵	Property acquisition for preservation only or none	< 0.5 acre	≥ 0.5 acre	-	-
Relocations⁶	None	-	-	< 5	≥ 5
Threatened/Endangered Species (Species Specific Programmatic for Indiana bat & northern long eared bat)*	“No Effect”, “Not likely to Adversely Affect” (With select AMMs ⁷)	“Not likely to Adversely Affect” (With any AMMs or commitments)	-	“Likely to Adversely Affect”	Project does not fall under Species Specific Programmatic ⁸
Threatened/Endangered Species (Any other species)*	Falls within guidelines of USFWS 2013 Interim Policy or “No Effect”	“Not likely to Adversely Affect”	-	-	“Likely to Adversely Affect”
		-	-	-	
Sole Source Aquifer	No Detailed Groundwater Assessment	-	-	-	Detailed Groundwater Assessment
Floodplain	No Substantial Impacts	-	-	-	Substantial Impacts
Section 4(f) Impacts	None	-	-	-	Any ¹⁰
Section 6(f) Impacts	None	-	-	-	Any
Permanent Traffic Alteration	None	-	-	-	Any
Noise Analysis Required	No	-	-	-	Yes
Air Quality Analysis Required	No	-	-	-	Yes ¹¹
Approval Level					
<ul style="list-style-type: none"> • District Env. (DE) • Env. Serv. Div. (ESD) • FHWA 	Concurrence by DE or ESD	DE or ESD	DE or ESD	DE and/or ESD	DE and/or ESD; and FHWA

¹ Coordinate with INDOT Environmental Services Division. INDOT will then coordinate with the appropriate FHWA Environmental Specialist.

² Any involvement with a bridge processed under the Historic Bridge Programmatic Agreement.

³ Total permanent impacts to streams (linear feet) and wetlands (acres).

⁴ US Army Corps of Engineers Individual 404 Permit

⁵ Total permanent and temporary right-of-way. This does not include reacquisition of existing apparent right-of-way.

⁶ If [REDACTED] greater than 5 relocations, a conversation with FHWA, through INDOT ESD, is needed to confirm NEPA classification and outreach plan for the project.

⁷ Avoidance and Mitigation Measures (AMMs) determined by the IPAC determination key to be required that are not tree AMMs, bridge AMMs, or structure AMMs.

⁸ Projects that do not fall under a Species Specific Programmatic and results in a “Likely to Adversely Affect”. Other findings can be processed as a lower-level CE.

¹⁰ Section 4(f) use resulting in an Individual, Programmatic, or *de minimis* evaluation. The only exception is a *de minimis* evaluation for historic properties (Effective January 2, 2020). If a historic property *de minimis* and no other use, mark the *None* column.

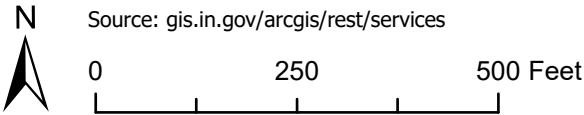
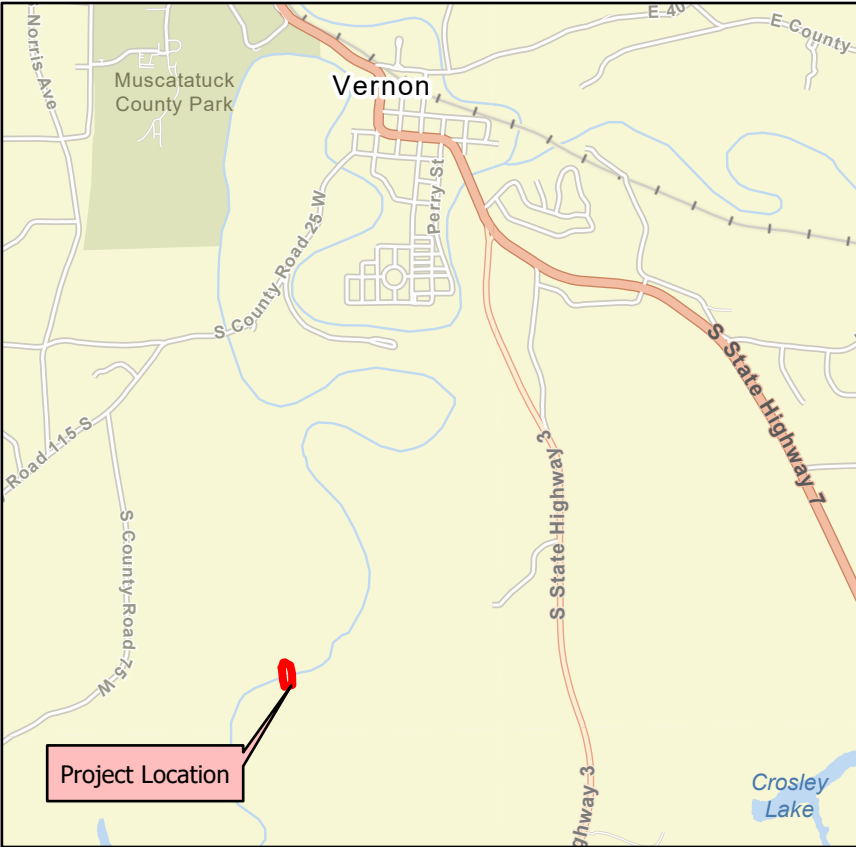
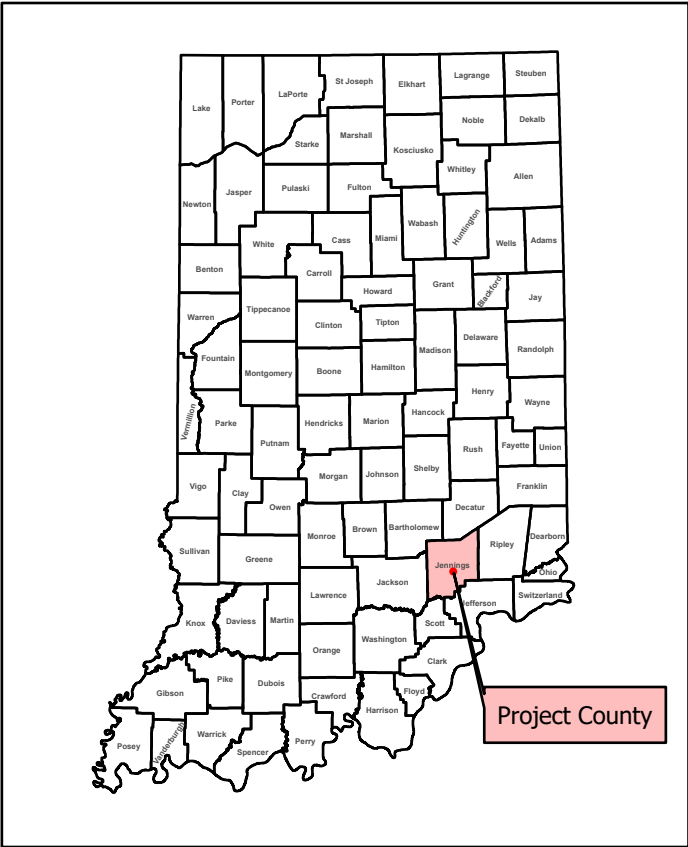
¹¹ Hot Spot Analysis and/or MSAT Quantitative Emission Analysis.

* Includes the threatened/endangered species critical habitat

Note: Substantial public or agency controversy may require a higher-level NEPA document.

Appendix B:

Graphics

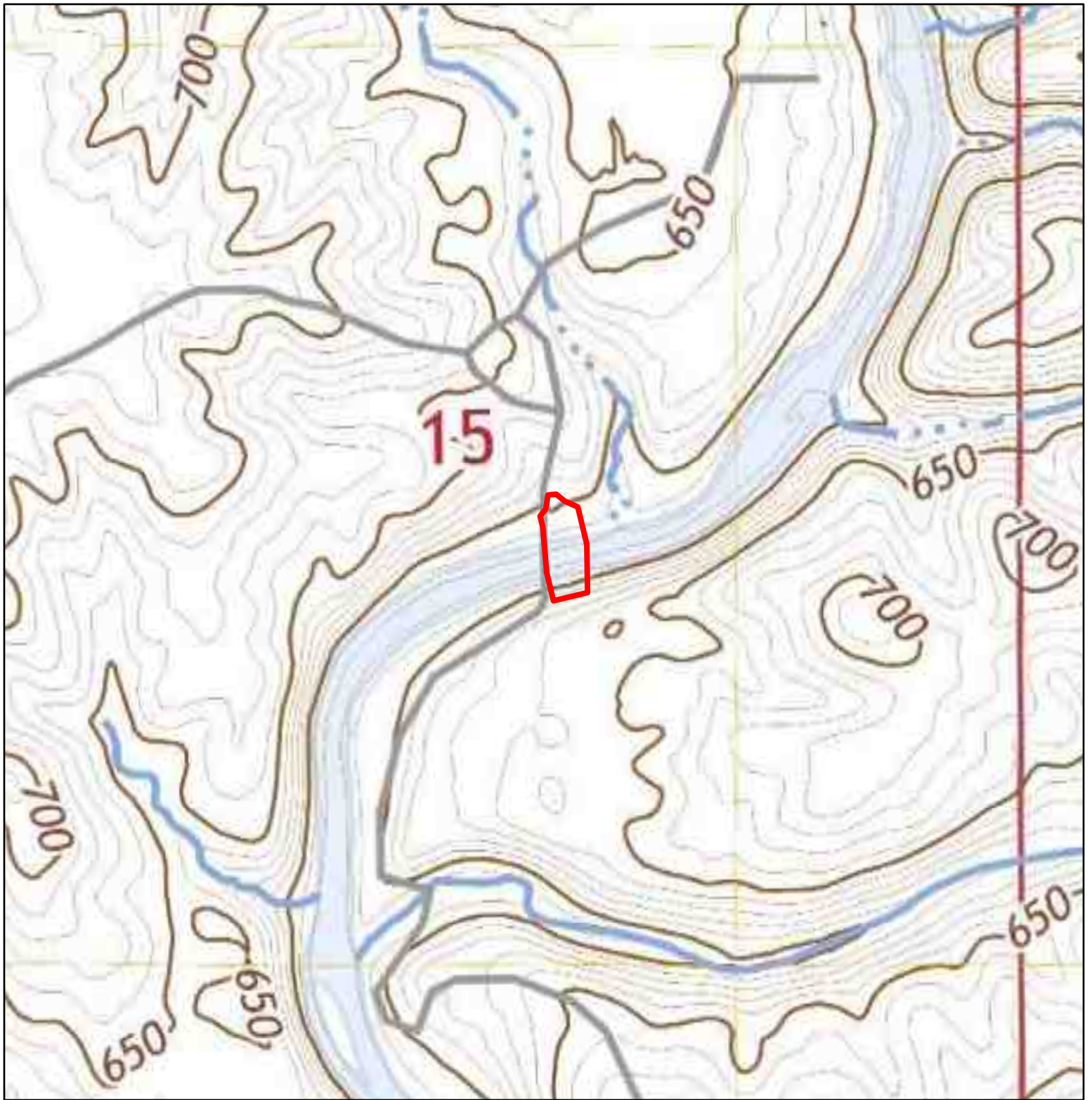


Legend

Project Area

County Selection

Figure 1: Project Location Map
 Bridge Rehabilitation
 Park Road over Muscatatuck River
 Jennings County, Indiana
 Des 2200148
 Author: Kristin Wing



Source: gis.in.gov/arcgis/rest/services

0 500 1,000 Feet



Legend

 Project Area

Figure 2: USGS Topo Map - Vernon Quad

Bridge Rehabilitation
Park Road over Muscatatuck River
Jennings County, Indiana
Des 2200148
Author: Kristin Wing



Source: gis.in.gov/arcgis/rest/services, Google.com

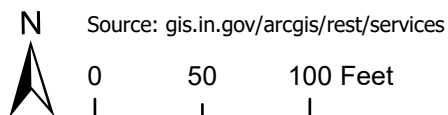
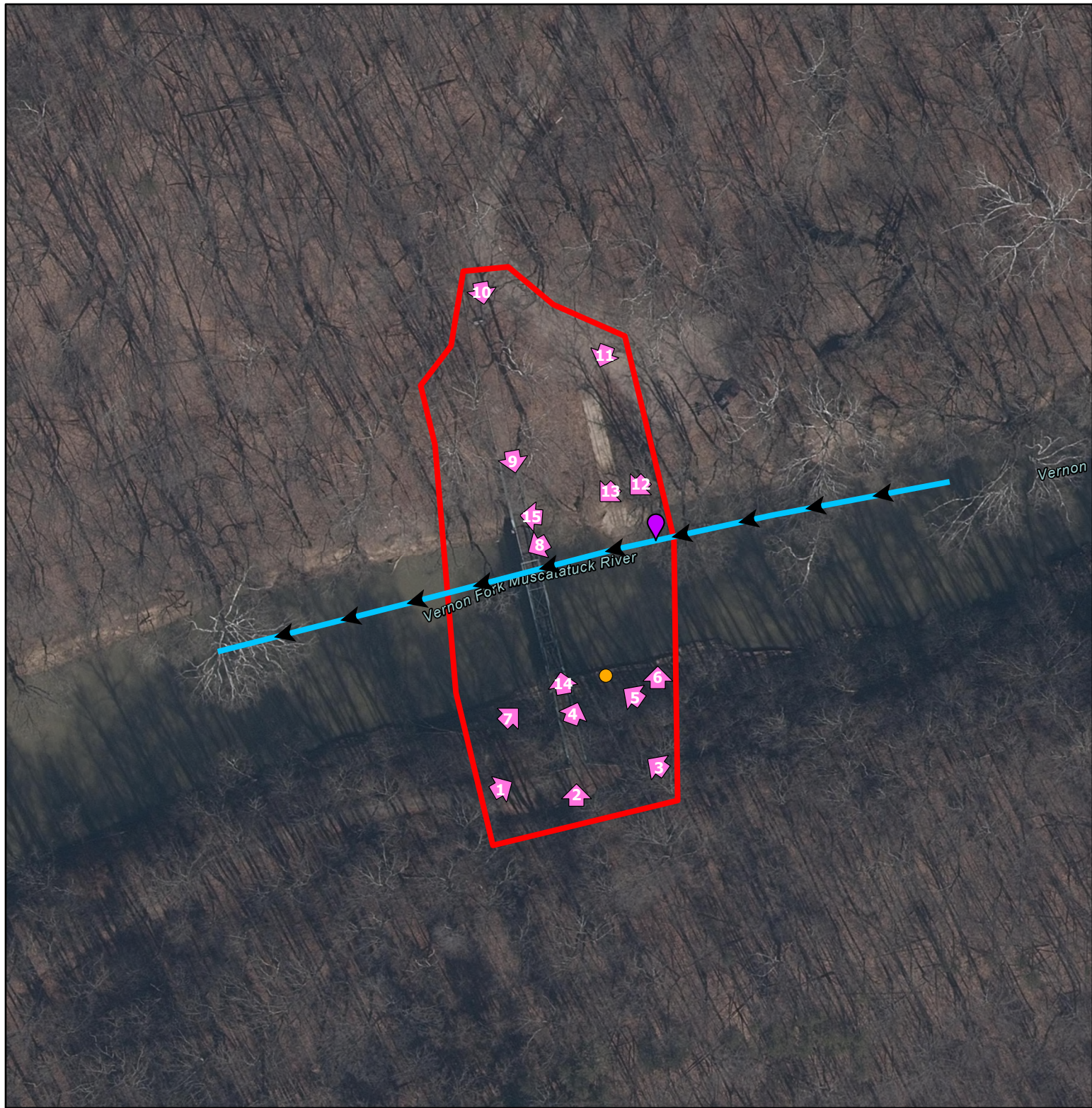
0 100 200 Feet



Legend

 Project Area

Figure 3: Aerial Map
Bridge Rehabilitation
Park Road over Muscatatuck River
Jennings County, Indiana
Des 2200148
Author: Kristin Wing



Legend

- Project Area
- Upland Point
- Muscatatuck River
- OHWM
- Photos

Figure 4: Photo Location Map
 Bridge Rehabilitation
 Park Road over Muscatatuck River
 Jennings County, Indiana
 Des 2200148
 Author: Kristin Wing



Photo 1: Facing northeast from the southwest quadrant of the investigated area, looking at the trail leading to the bridge and the steep slopes down to the Muscatatuck River.



Photo 2: Facing north looking at the closed bridge over the Muscatatuck River. The slopes on each side are steep down to the river.



Photo 3: Facing northwest from the southeast quadrant of the investigated area, looking at the bridge and the steep slopes down to the Muscatatuck River.

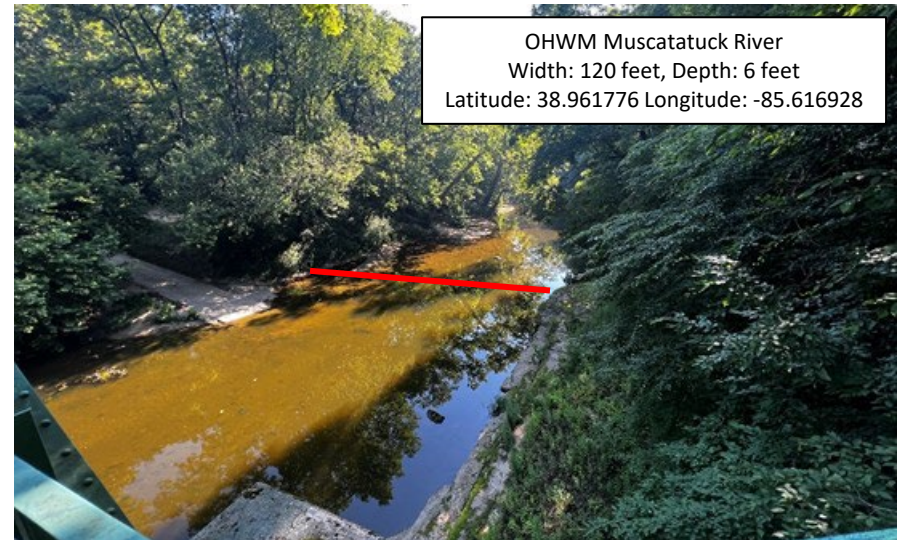


Photo 4: Facing northeast from the bridge deck looking at the Muscatatuck River and the boat ramp in the northeast quadrant. The Ordinary High-Water Mark (OHWM) was taken upstream of the boat ramp.



Photo 5: Facing northwest from the southeast bank of the Muscatatuck River, looking at the bridge and the rock shelves along the southern bank. A sample point was taken on this bank, and it was determined to be an upland point. A boat ramp can be seen on the far right of the picture.



Photo 6: Facing northeast looking at the bridge and the boat ramp on the northeast bank of the Muscatatuck River.



Photo 7: Facing northeast looking at the Muscatatuck River, the bridge and the boat ramp.



Photo 8: Facing southwest from the bridge deck looking downstream at the Muscatatuck River.



Photo 9: Facing south from the roadway looking toward the bridge.



Photo 10: Facing south from the northern terminus of the investigated area, looking at the road leading to the bridge, the parking lot, boat ramp, and sloping forested area on the west side of the road.



Photo 11: Facing southwest looking at the boat ramp east of the bridge.



Photo 12: Facing west looking downstream toward the bridge from the boat ramp on the northeast bank.



Photo 13: Facing south looking at animal prints under the bridge on the northeast bank.



Photo 14: Facing north from center of channel looking at steep slopes on the northern bank. Photo taken 3/2/23.



Photo 15: Facing west from the bridge deck looking at the northwest quadrant of the investigated area.

UTILITIES

No Utilities Found within Project Limits



**Know what's below.
Call before you dig.**

INDIANA UNDERGROUND
1-800-382-5544 OR CALL 811
24 HOURS A DAY 7 DAYS A WEEK

INDEX

[illegible]

REVISIONS

SHEET NO.	DATE	REVISED

RECOMMENDED FOR APPROVAL _____

 DESIGN ENGINEER DATE

DESIGNED: <u>AE</u>	DRAWN: <u>LLG</u>
CHECKED: <u>AVW</u>	CHECKED: <u>AVW</u>

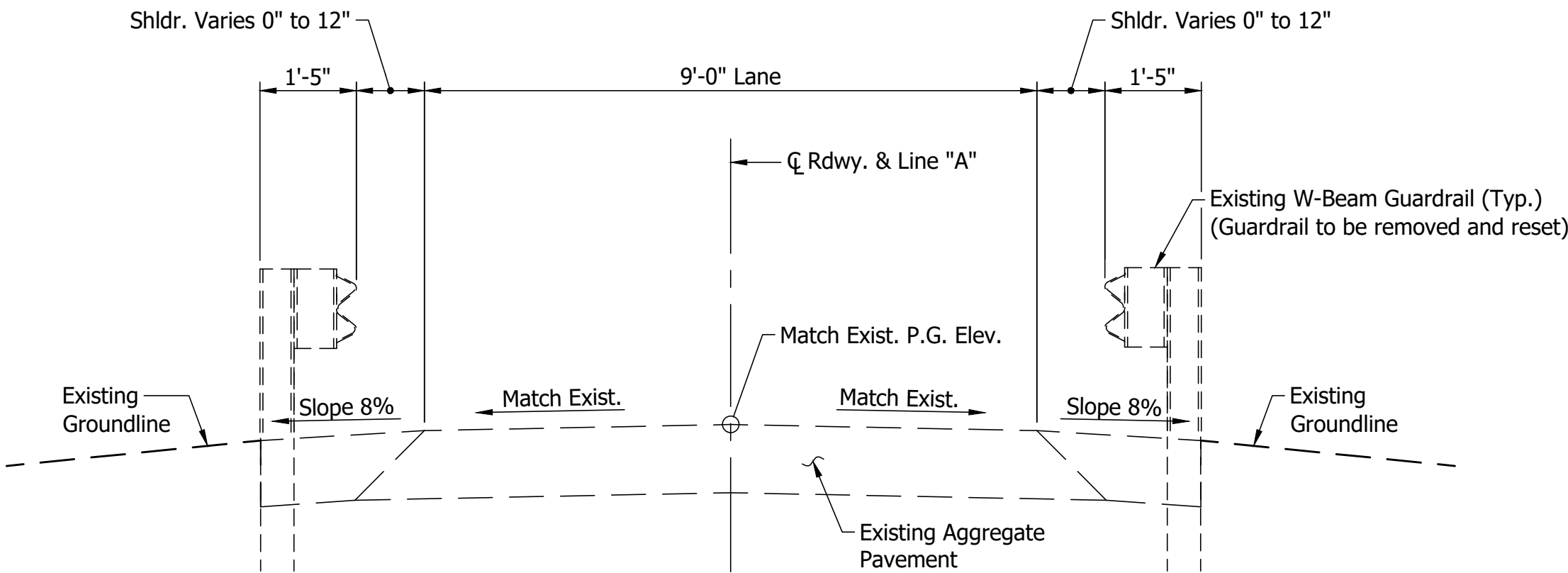
INDIANA
DEPARTMENT OF TRANSPORTATION

INDEX SHEET

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VERTICAL SCALE	DESIGNATION		
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DRAWING NO.	SHEETS		
	2	of	10
CONTRACT	PROJECT		
B-44218	2200148		

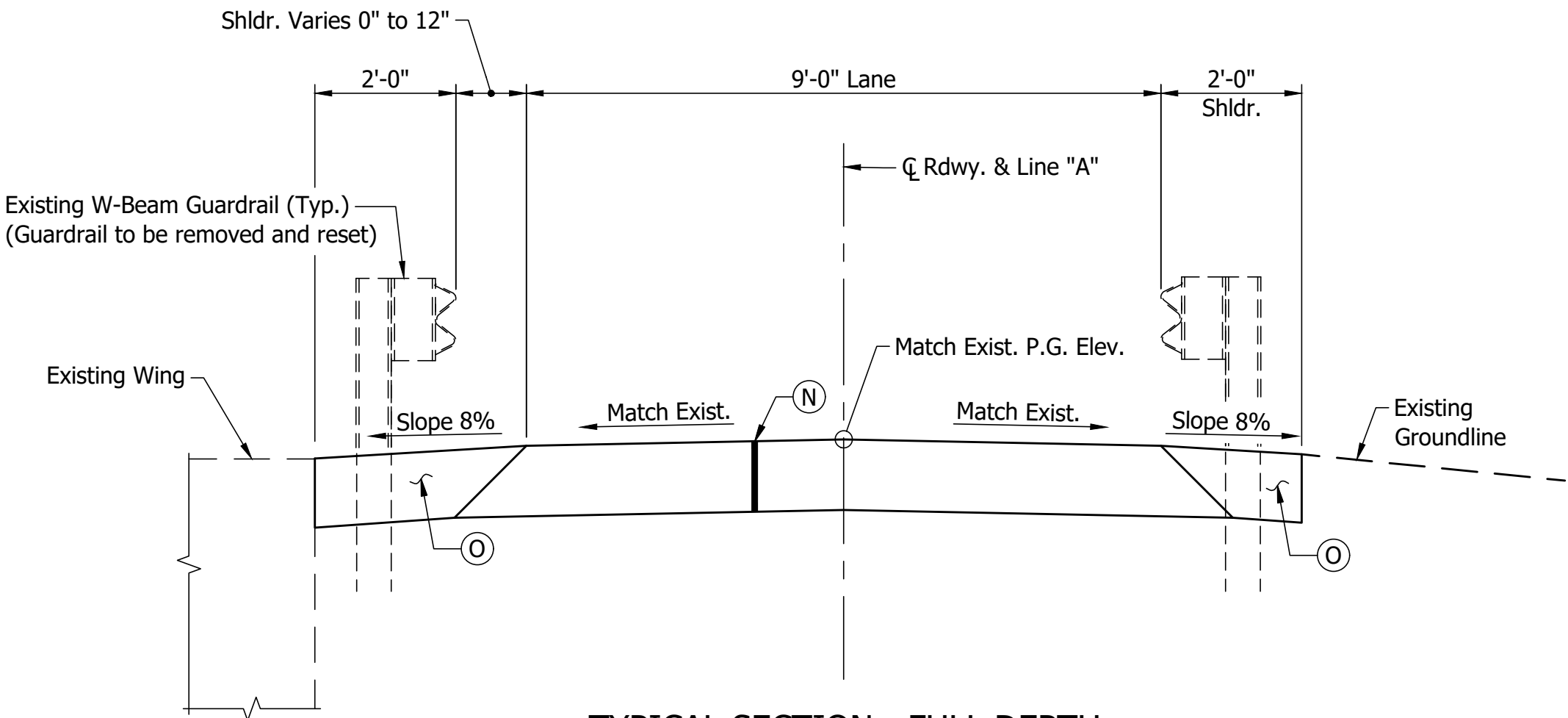
LEGEND

- (N) 12" Compacted Aggregate, No.73
- (O) Compacted Aggregate, No.53
- (26) 24" Dumped Class 1 Riprap



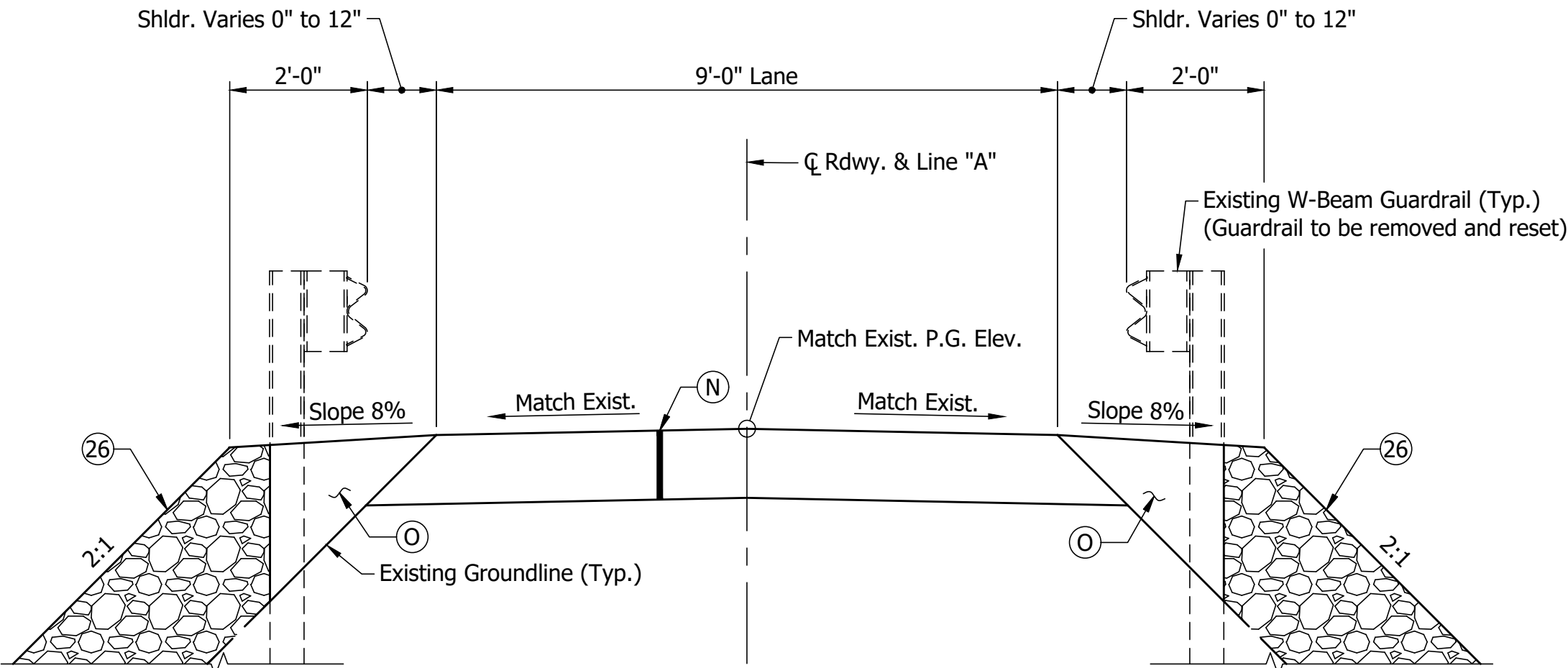
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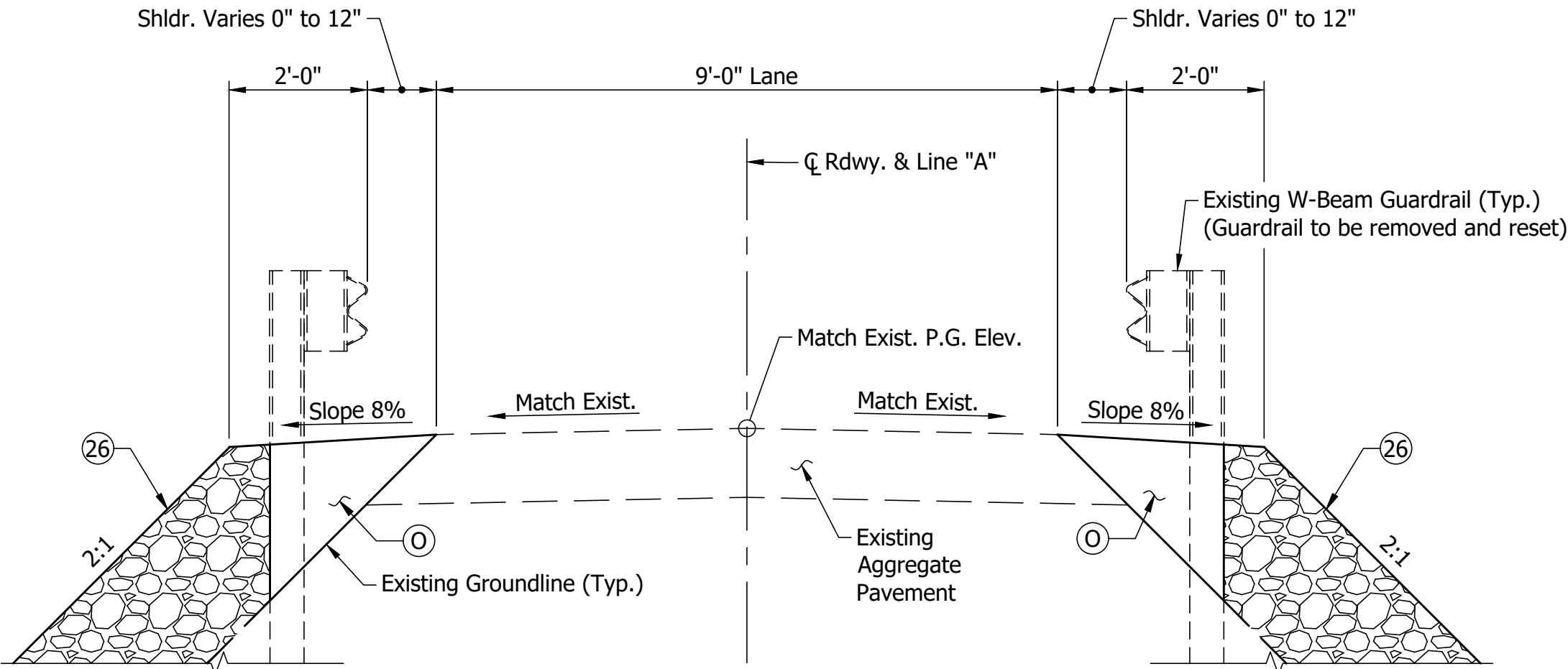
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TYPICAL SECTION - FULL DEPTH

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Scale: 1/2" = 1'-0"



TYPICAL SECTION - INCIDENTAL CONSTRUCTION

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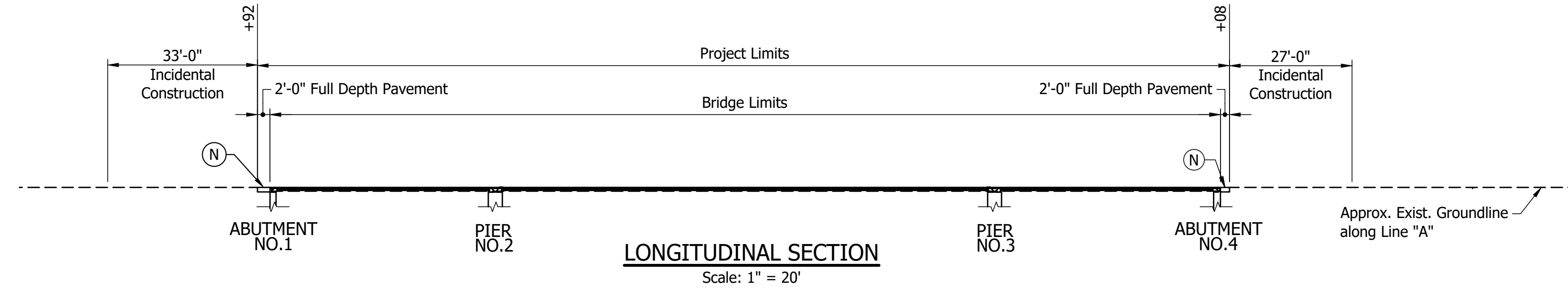
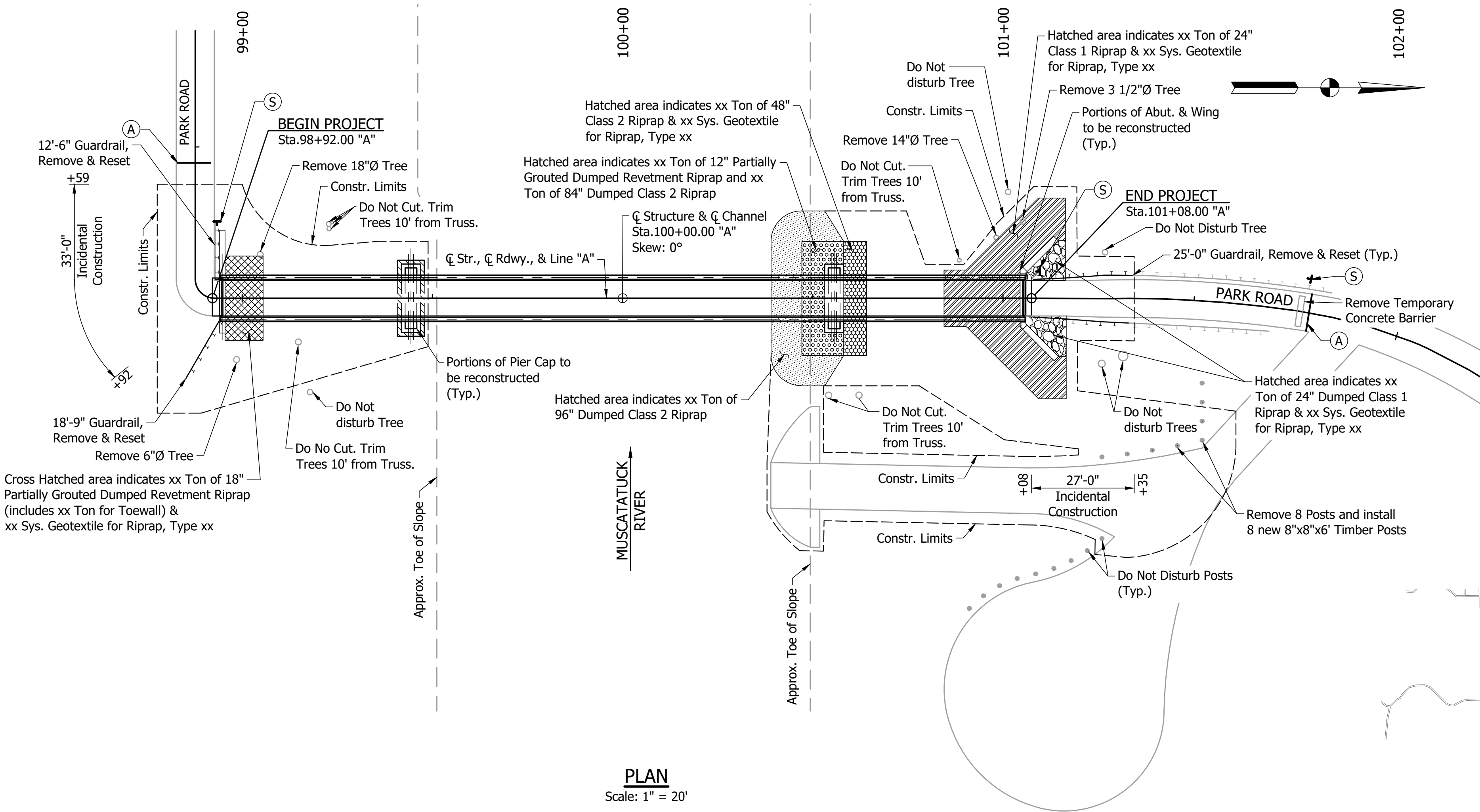
RECOMMENDED FOR APPROVAL _____
DESIGN ENGINEER DATE

DESIGNED: AE DRAWN: LLG
CHECKED: AVW CHECKED: AVW

INDIANA
DEPARTMENT OF TRANSPORTATION

TYPICAL CROSS SECTIONS

HORIZONTAL SCALE	BRIDGE FILE		
1/2" = 1'-0"	P000-40-07088 C		
VERTICAL SCALE	DESIGNATION		
1/2" = 1'-0"	2200148		
DRAWING NO.	SHEETS		
	3	of	10
CONTRACT	PROJECT		
B-44218	2200148		



- LEGEND**
- (N) 12" Compacted Aggregate, No. 73
 - (S) R12-7 (Emergency Vehicle Weight Limit Single Axle 8 Ton Tandem 10 Ton Gross 13 Ton) R12-1 (Weight Limit 4 Ton)
 - 18" Partially Grouted Dumped Revetment Riprap
 - 24" Class 1 Riprap
 - 24" Dumped Class 1 Riprap
 - 48" Class 2 Riprap
 - 12" Partially Grouted Dumped Revetment Riprap on 84" Dumped Class 2 Riprap
 - 96" Dumped Class 2 Riprap
 - Concrete Underpin

EXISTING STRUCTURE

Existing Structure is a 3 span steel truss bridge (49'-0", 110'-0", 49'-0") with a 9'-4" Clear Roadway. (To be Rehabilitated)

EARTHWORK SUMMARY*

Common Excavation	xxx Cys
Usable Common Excavation	xxx Cys
Fill + 20%	xxx Cys
Waterway Excavation	xx Cys
Usable Waterway Excavation (50%)	xx Cys
Borrow	xx Cys

* Quantities shown are to be used as final pay items.

HYDRAULIC DATA

Drainage Area	199.66 Sq Mi
Design Discharge, Q100	52,902 cfs
High Water Elevation, Q100	El. 614.52

Existing Bridge	
Skew	0°
Flowline Elevation	El. 583.30
Contraction Scour, Q100	16.29 ft
Total Scour, Q100	36.06 ft
Low Scour Elevation, Q100	El. 541.68
Max. Velocity, Q100 (Q Channel)	12.99 ft/sec
Avg. Velocity, Q100	9.85 ft/sec
Low Structure Elevation (Approx.)	El. 612.74

LEGEND

- (A) Barricade Type III-A & Road Closure Sign Assembly
- (B) Barricade Type III-B
- (C) Barricade Type III-B & Road Closure Sign Assembly
- Project Location

CONSTRUCTION SIGNS TYPE "A"

- (1) R11-2 Road Closed

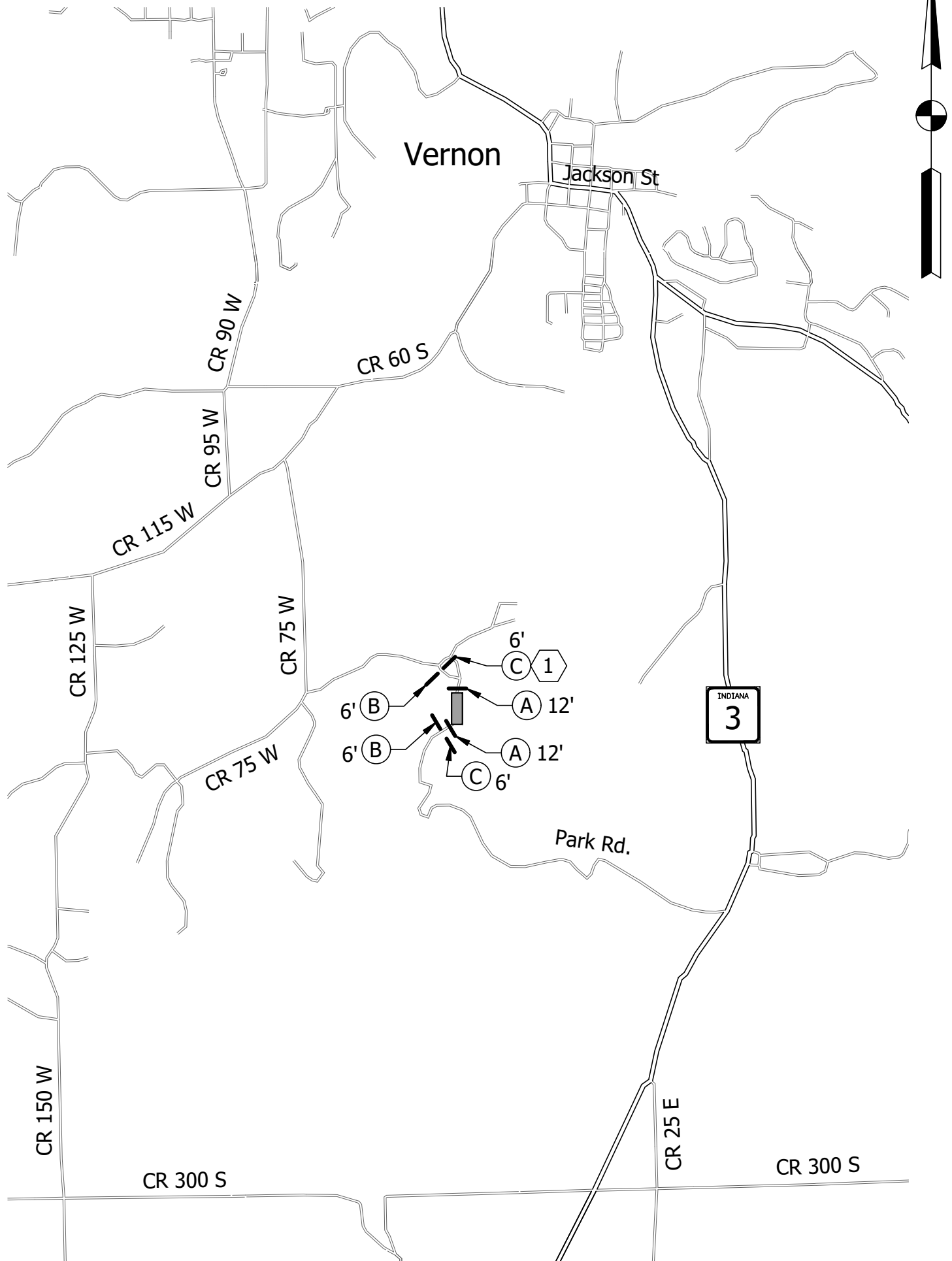
TRAFFIC MAINTENANCE SUMMARY TABLE

ITEM DESCRIPTION	PAY QUANTITY
Barricade, Type III-A	24 Lft.
Barricade, Type III-B	24 Lft.
Construction Sign, A	1 Ea.
Road Closure Sign Assembly	4 Ea.
Maintaining Traffic	1 LSum

Notes:

Trees within or near construction limits shall not be cut or trimmed unless noted. Do Not Disturb Trees outside of Construction Limits.

Entire Project within IDNR Right-of-Way.



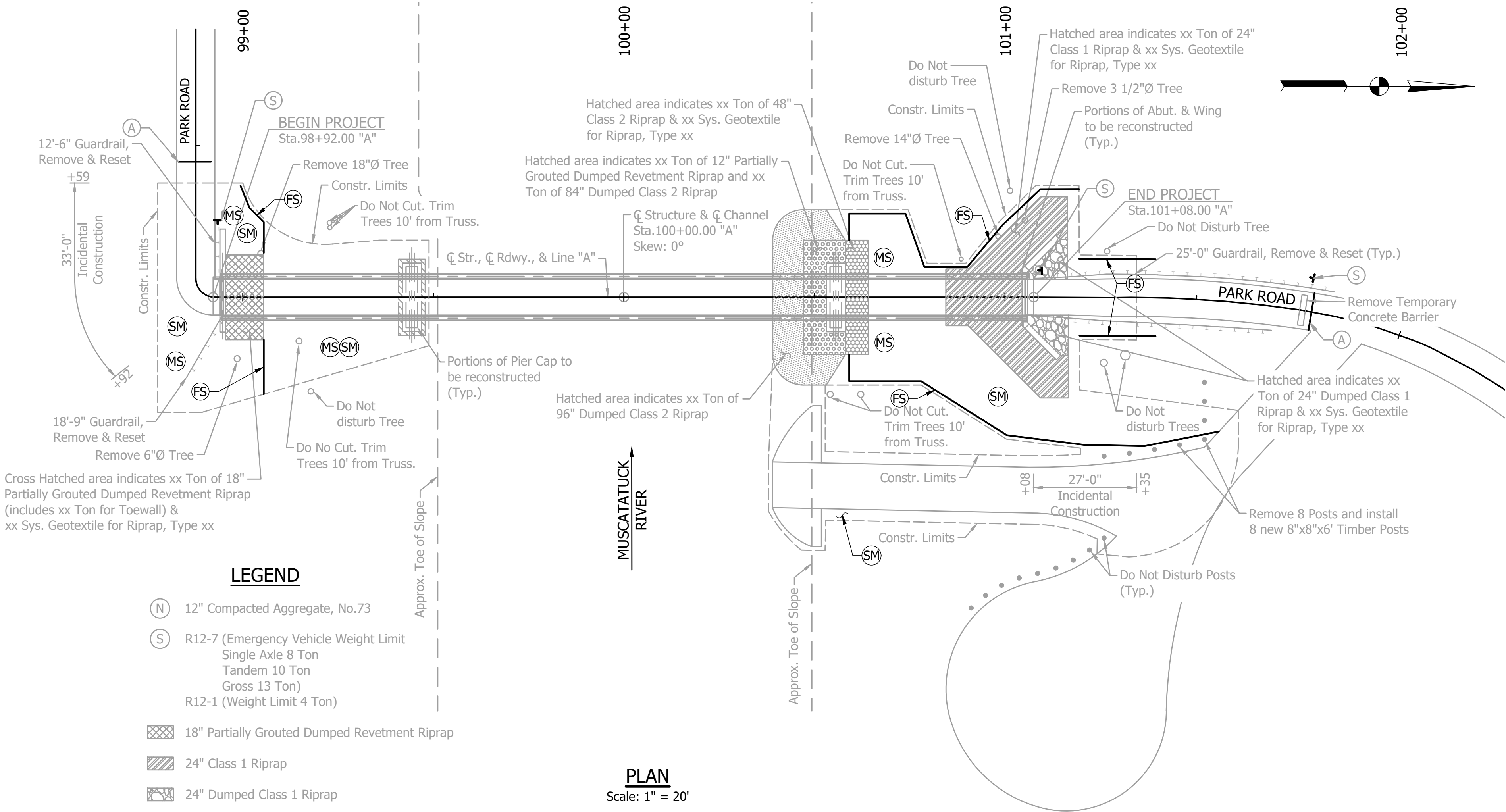
RECOMMENDED FOR APPROVAL _____ DESIGN ENGINEER _____ DATE _____

DESIGNED: AE _____ DRAWN: LLG _____
CHECKED: AVW _____ CHECKED: AVW _____

INDIANA
DEPARTMENT OF TRANSPORTATION

CONSTRUCTION LAYOUT DETAILS

HORIZONTAL SCALE	BRIDGE FILE
AS NOTED	P000-40-07088 C
VERTICAL SCALE	DESIGNATION
AS NOTED	2200148
DRAWING NO.	SHEETS
	4 of 10
CONTRACT	PROJECT
B-44218	2200148



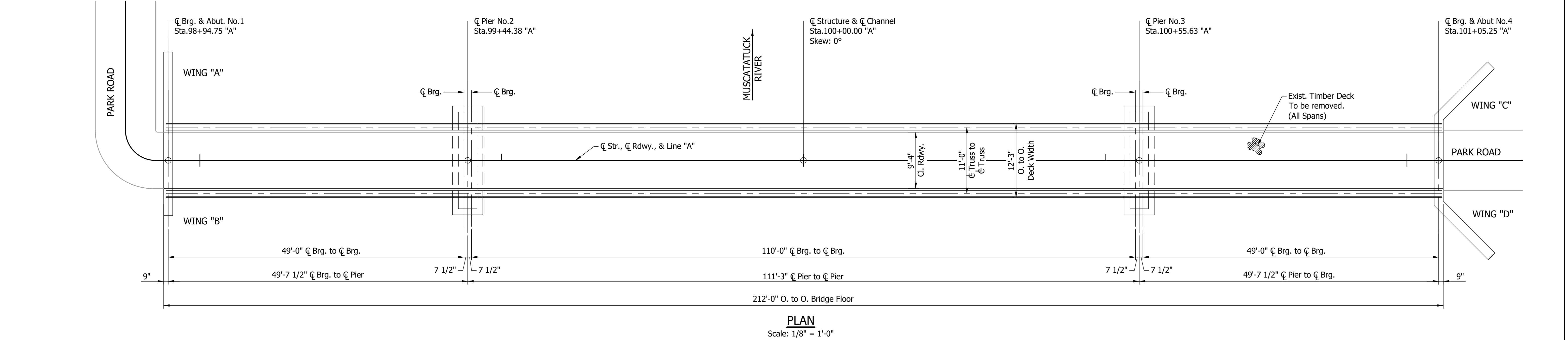
- LEGEND**
- (N) 12" Compacted Aggregate, No.73
 - (S) R12-7 (Emergency Vehicle Weight Limit
Single Axle 8 Ton
Tandem 10 Ton
Gross 13 Ton)
R12-1 (Weight Limit 4 Ton)
 - [Cross-hatched] 18" Partially Grouted Dumped Revetment Riprap
 - [Diagonal lines] 24" Class 1 Riprap
 - [Stippled] 24" Dumped Class 1 Riprap
 - [Grid pattern] 48" Class 2 Riprap
 - [Dotted] 12" Partially Grouted Dumped Revetment Riprap on
84" Dumped Class 2 Riprap
 - [Horizontal lines] 96" Dumped Class 2 Riprap
 - [Solid grey] Concrete Underpin

PLAN
Scale: 1" = 20'

- LEGEND**
- (MS) Manufactured Surface Protection Product
 - (SM) Seed Mixture, Floodplain
 - (FS) Filter Sock

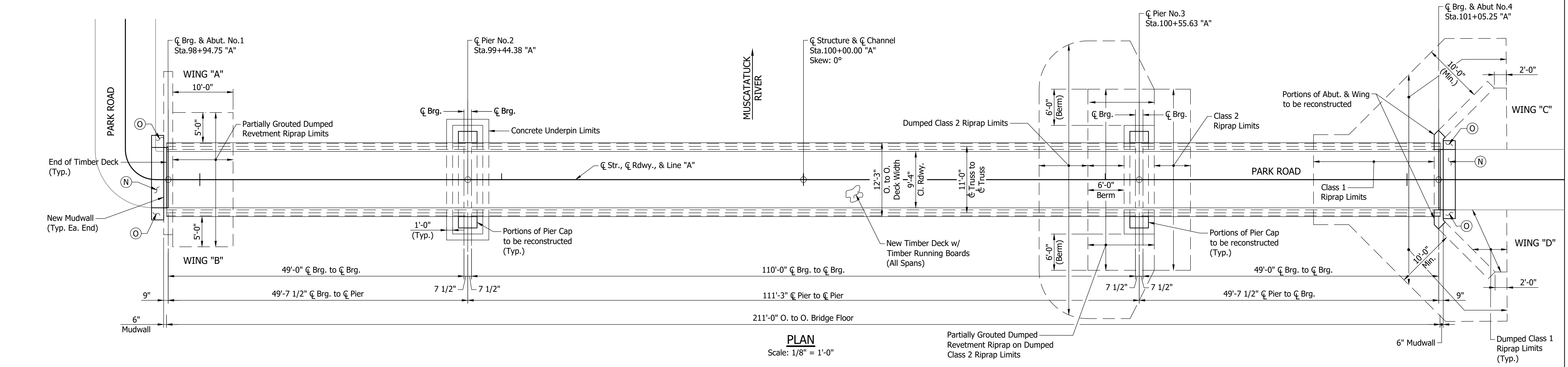
Notes:
Trees within or near construction limits shall not be cut or trimmed unless noted. Do Not Disturb Trees outside of Construction Limits.
Entire Project within IDNR Right-of-Way.

		RECOMMENDED FOR APPROVAL _____		INDIANA DEPARTMENT OF TRANSPORTATION	BRIDGE FILE	
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		DATE _____			VERTICAL SCALE	DESIGNATION
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		DESIGNED: AE _____	DRAWN: LLG _____	EROSION CONTROL PLAN - LINE "A"	DRAWING NO.	
					SHEETS	
					5	of 10
		CHECKED: AVW _____	CHECKED: AVW _____		CONTRACT	PROJECT
				B-44218 2200148		



STEEL THROUGH & PONY TRUSS BRIDGE
3 SPANS: 49'-0", 110'-0", 49'-0"
9'-8" CLEAR ROADWAY SKEW: 0°
PARK ROAD OVER MUSCATATUCK RIVER
JENNINGS COUNTY

<div>RECOMMENDED FOR APPROVAL</div> <div>DESIGN ENGINEER</div> <div>DATE</div>		<div>INDIANA DEPARTMENT OF TRANSPORTATION</div>		HORIZONTAL SCALE		BRIDGE FILE			
				1/8" = 1'-0"		P000-40-07088 C			
				VERTICAL SCALE		DESIGNATION			
				1/8" = 1'-0"		2200148			
DESIGNED: <u>AE</u>		DRAWN: <u>LLG</u>		<div>GENERAL PLAN EXISTING</div>		DRAWING NO.		SHEETS	
		S1 of S3				6 of 10			
CHECKED: <u>AVW</u>		CHECKED: <u>AVW</u>				PROJECT			
						2200148			



STEEL THROUGH & PONY TRUSS BRIDGE
3 SPANS: 49'-0", 110'-0", 49'-0"
9'-8" CLEAR ROADWAY SKEW: 0°
PARK ROAD OVER MUSCATATUCK RIVER
JENNINGS COUNTY

RECOMMENDED FOR APPROVAL			INDIANA DEPARTMENT OF TRANSPORTATION		HORIZONTAL SCALE		BRIDGE FILE		
					1/8" = 1'-0"		P000-40-07088 C		
					VERTICAL SCALE		DESIGNATION		
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DESIGNED: AE		DRAWN: LLG		GENERAL PLAN PROPOSED		DRAWING NO.		SHEETS	
						S2 OF S3		7 of 10	
CHECKED: AWW		CHECKED: AWW				CONTRACT		PROJECT	
						B-44218		2200148	

Reinforcing Steel covering shall be 2", unless noted.

Reinforcing Steel in mudwalls, piers, and abutments shall be epoxy coated.

Portions of the present structure shall be Removed.

Missing or deteriorated bolts and rivets shall be replaced as directed by the Engineer

All bolts and rivets that are Removed or open holes shall be replaced or filled with A325 round headed bolts of the applicable size. At no time shall standard bolts be utilized without the written consent of the Engineer.

The Contractor shall be responsible for and provide adequate Jacking, Shoring, and temporary support prior to all structural repairs. The Contractor shall submit to the Project Engineer/Supervisor (PE/S), 14 days prior to indicated work, a plan detailing the proposed method for jacking, shoring, and temporary support of the existing structure during the construction of bridge members. Each drawing must include Contract Number, Contractors Name, and shall be designed and sealed by a Professional Engineer Licensed in the State of Indiana. See Special Provisions for additional details.

All exposed faces of reconstructed abutment and pier caps and exposed faces of mudwalls to be sealed in accordance with 702.21. of the Specifications.

(Estimated Quantity = xxx Sft.)

Where new work is to be fitted to the old work, the Contractor shall check and verify all dimensions, elevations, and conditions in the field and report any errors or discrepancies to the Engineer and assume responsibility for their correctness and the fit of the new construction to the existing structure.

No Original Plans exist for this structure. The original design loading is unknown. Plans for 1979, 2004, and 2015 rehabilitations are on file in the Research and Documents Section at the Indiana Department of Transportation, as Bridge File No.P000-40-07088 and are available upon request.

All Cleaning and Coating shall be in accordance with the current Standard Specifications and Special Provision 619-B-321. The dried coating film shall match color number 14260 Green, of Federal Standard 595. The bridge was last painted in 2004 and the presence of lead is unknown.

Concrete in mudwalls and substructure to be Class "A".

LIVE LOAD

LIVE LOAD

Based on recent Load Rating, the existing bridge has a H-20 Design Loading of 5 Tons.

Bridge to be posted for 4 tons at the request of the Indiana Department of Natural Resources (IDNR) and INDOT.

DESIGN STRENGTHS

To be in accordance with 2002 AASHTO Standard Specifications for Highway Bridges and all Interims.

CONCRETE:

Class "A": $f'_c = 3,500$ psi

REINFORCING STEEL:

Grade 60: $f_y=60,000$ psi

STRUCTURAL STEEL:

ASTM A709 Grade 50: $f_y=50,000$ psi

SEISMIC DATA

AASHTO Guide Design Specifications for LRFD Seismic Bridge Design

Seismic Zone Category A

$$S1 = XX$$

Site Class xx

$$F_v = xx$$


(All Spans)

Scale: 1/2" = 1'-0"



(All Spans)

Scale: $1/2'' = 1'-0''$

1. Remove existing timber deck, portions of handrails, and portion of approaches.
2. Replace or repair truss gusset plates, bearings, verticals, and low chords.
3. Install Scour Countermeasures.
4. Construct timber deck, timber running boards, portions of handrails and portions of mudwalls.
5. Clean and coat truss, floor beams, stringers and bridge railing.
6. Reconstruct or patch portions of abutments, piers, and wings.
7. Complete all other work as shown in the detail plans.

The sequence of the above notes does not necessarily indicate sequence of construction operations.

Structure to be closed to traffic during all phases of work. See Maintenance of Traffic Details.

Notes:

Hatched area indicates portions to be removed.

For Existing Elevation and Plan, see Dwg.S1.

For Proposed Elevation and Plan, see Dwg.S2.

lgrooms | p:\200057-park rd over muscatatuck river\02bridge\04plans\200057 -sht. general plan.dwg | typical sections | 2/26/2025 2:55:02 PM |

Appendix C:

Early Coordination



INDIANA DEPARTMENT OF TRANSPORTATION

100 North Senate Avenue
Room N758-ES
Indianapolis, Indiana 46204

PHONE: (855) 463-6848
(855) INDOT4U

Eric Holcomb, Governor
Michael Smith, Commissioner

September 25, 2023

Sample Early Coordination Letter

Early Coordination Agency

Re: Early Coordination Letter, Des. No. 2200148, Bridge Rehabilitation on Park Road over the Muscatatuck River in the Crosley Fish and Wildlife Area, in Jennings County, Indiana.

Dear Early Coordination Agency:

The Indiana Department of Transportation (INDOT), with federal funding, intends to proceed with a project involving the referenced bridge (No. P000-40-07088 B, NBI: 060380) in Jennings County, Indiana. This letter is part of the early coordination phase of the environmental review process. We are requesting comments from your area of expertise regarding any possible environmental effects associated with this project. **Please use the referenced designation numbers and description in your reply.** We will incorporate your comments into a study of the project's environmental impacts.

This project is located on Park Road over the Muscatatuck River in the Indiana Department of Natural Resources' (IDNR) Crosley Fish and Wildlife Area, created in 1931 in Jennings County, Indiana. The proposed work will occur on the Park Road bridge (No. P000-40-07088/NBI 60380) over the Muscatatuck River. The existing bridge is approximately 212 feet long by 12.3 feet wide. It is a three-span truss bridge constructed in 1910 and rehabilitated in 1979, 2004, and 2015. The main span is a Pratt-through truss, and the approach spans are Warren pony trusses. The bridge clear roadway width is 9 feet 4 inches, and the superstructure is composed of a timber plank deck supported by steel stringers and floor beams. In 2019, the bridge was closed due to the condition of the deck. According to the March 9, 2023, INDOT bridge inspection, the deck is rated in critical condition (2 out of 9), the superstructure is rated fair (5 out of 9), and the substructure is rated satisfactory (6 out of 9). The bridge is considered scour-critical due to the unknown foundations and the large scour depths calculated. According to the Indiana Historic Bridge Inventory, completed in 2010, the bridge was determined eligible for listing in the National Register of Historic Places (NRHP) under Criterion C. The existing roadway is functionally classified as recreational (primary access) and is not part of the National Highway System. The project is located within a forested area with no residential properties or businesses.

The purpose of this project is to correct the deficiencies in the bridge deck, superstructure, and substructure to reopen the bridge to vehicular traffic, and protect the bridge while extending its service life. The final objective of the project is to protect the bridge from scour and remove it from scour critical status. The draft need of this project is to address distress and deterioration of the existing bridge deck that continues to worsen, compromising public safety and causing the bridge closure in 2019. The deck has areas of severe deterioration with holes and rotten areas, while the superstructure has areas of deterioration and section loss, limiting the functional life of the bridge. The draft need is a result of the hydraulic scour analysis that determined scour to be below all the substructure's foundations, causing the bridge to become scour critical.

Per the terms of the "Programmatic Agreement Regarding Management and Preservation of Indiana's Historic Bridges" (Historic Bridge PA), the Federal Highway Administration-Indiana Division (FHWA) will satisfy its Section 106 responsibilities involving "Select" and "Non-Select" bridges through the Project Development Process (PDP) of the Historic Bridges PA (Stipulation III). Because Bridge No. P000-40-07088 (NBI 60380) is a "Non-Select" bridge, the procedures outlined in Stipulation III B of the Historic Bridges PA will be followed to fulfill FHWA's Section 106 responsibilities.

The Historic Bridge PA Development Process and Section 4(f) of the Department of Transportation Act of 1966 require the development of a Section 4(f) Alternatives Analysis in an effort to minimize harm and reduce impacts to historic structures. Per the guidelines of the Historic Bridges Programmatic Agreement, the applicable alternates will be evaluated to determine the most prudent and feasible option to meet the desired transportation need.

The preferred alternative proposes rehabilitating the existing bridge by cleaning and painting the truss, replacing the bridge deck, repairing or replacing verticals, gusset plates, and low chord truss members or connections, replacing some truss secondary members, removing and reconstructing concrete abutments and pier caps, patching abutments and piers, placing scour countermeasures at all substructure units, and replacing the bridge rail with a steel handrail. Riprap will be installed as scour protection.

The maintenance of traffic (MOT) will keep the bridge closed, which is anticipated to be less than 120 days. No right-of-way (ROW) acquisition is anticipated. Approximately 0.003 acre of tree clearing will take place. Anticipated letting date for this project is January 14, 2026, with construction expected to begin in 2026. Land use in the vicinity is primarily densely wooded slopes, and a boat launch is in the northeast quadrant. The project is located within Crosley Fish and Wildlife Area, which is not a Section 6(f) property.

BLN will prepare a Waters of the US Report (WOUSR) that will be reviewed by the INDOT Ecology and Waterway Permitting Office. The project is anticipated to qualify for the Rangewide Programmatic Agreement for the Indiana Bat and the Northern Long-eared Bat by completing the Information for Planning and Consultation (IPaC). The proposed project area is within the Indiana karst region, and it is approximately 0.03 mile north of a potential karst feature (Figure 6).

In addition, BLN will have Qualified Professionals (QPs) investigate the project area for archaeological and historic resources for compliance with Section 106 of the National Historic Preservation Act (NHPA). The results of this investigation will be forwarded to the Indiana State Historic Preservation Officer (IN SHPO) for review and concurrence.

Please provide your response within thirty (30) calendar days from the date of this letter. However, should you find that an extension to the response time is necessary, a reasonable amount may be granted upon request. If you have any questions or if we can be of any further assistance, please contact either Matt Walker, INDOT Project Manager, at matwalker@indot.in.gov or telephone 812-528-1868 or Kristin Wing at kwing@b-l-n.com or telephone 317-806-4342. Thank you for your cooperation.

Sincerely,



Kristin Wing
Senior Environmental Analyst
Beam, Longest and Neff

Attachments: **Some attachments have been removed to limit duplication.**

Mailing List

Maps (Location, Topographic, Aerial, NWI/NHD, Floodplain and Karst)

Ground-Level Photographs

EARLY COORDINATION MAILING LIST

<p>Federal Highway Administration Federal Office Building, Room 254 575 North Pennsylvania Street Indianapolis, IN 46204 Electronic Coordination – erica.tait@dot.gov</p>	<p>David Dye Environmental Section Manager Seymour District Indiana Department of Transportation Electronic Coordination – ddye@indot.in.gov</p>
<p>Indiana Geological and Water Survey 611 North Walnut Grove Bloomington, IN 47405 Electronic Coordination – igws.indiana.edu/eAssessment/</p>	<p>Crosley Fish and Wildlife 2010 S. State Highway 3 North Vernon, IN 47265 Electronic Coordination – CrosleyFWA@dnr.IN.gov</p>
<p>Environmental Coordinator Indiana Department of Natural Resources Division of Fish and Wildlife 402 West Washington Street, Room W273 Indianapolis, IN 46204 Electronic Coordination – environmentalreview@dnr.in.gov</p>	<p>IDNR Division of Law Enforcement District 9 1387 East US 50 Versailles, IN 47042 Electronic Coordination – icodist9@dnr.in.gov</p>
<p>Commander, Eighth Coast Guard District Attn: Bridge Branch 1222 Spruce Street, Rm 2. 102D ST. Louis, MO 63103-2832 Electronic Coordination – eric.washburn@uscf.mil</p>	<p>Jennings County Highway Superintendent Jim Reeves P.O. Box 198 North Vernon, Indiana 47265 Electronic Coordination – highway@jenningscounty-in.gov</p>
<p>Field Environmental Officer Chicago Regional Officer U.S. Department of Housing and Urban Development Metcalf Federal Building 77 W. Jackson Blvd, Room 2401 Chicago, IL 60604 Electronic Coordination – erik.r.sandsted@hud.gov</p>	<p>Chief, Groundwater Section Indiana Department of Environmental Management 100 N. Senate Ave. Indianapolis, IN 46204 Electronic Coordination – IDEM's Wellhead Proximity Determinator www.in.gov/idem/cleanwater/pages/wellhead/</p>
<p>Regional Environmental Coordinator Midwest Regional Office National Park Service 601 Riverfront Drive Omaha, NE 68102 Electronic Coordination – mwro_compliance@nps.gov</p>	<p>Ms. Deborah Snyder U.S. Army Corps of Engineers Louisville District, Indianapolis Regulatory Office Indianapolis, IN 46216 Electronic Coordination – regulatoryapplicationsLRL@usace.army.mil</p>
<p>Jennings County Emergency Management Director Jerry Shepherd Electronic Coordination - jshepherd@jenningscounty-in.gov</p>	<p>Jennings County Surveyor Chad Ebinger Electronic Coordination – cebinger@fpbhone.com</p>
<p>Southeastern Indiana Regional Planning Commission Susan Craig- Executive Director 405 West US 50 Versailles, IN 47042 Electronic Coordination- susan.craig@sirpc.org</p>	<p>Matt Walker Project Manager, INDOT Seymour District 185 Agrico Lane Seymour, Indiana 47274 Electronic Coordination - matwalker@indot.in.gov</p>
<p>Jennings County Commissioners PO Box 383 Vernon, Indiana 47282 Electronic Coordination – mattsporleder@jcsc.org commissionerwillhite@aol.com ssboswell@frontier.com</p>	<p>Field Supervisor US Fish and Wildlife Service Bloomington Indiana Field Office 620 South Walker Street Bloomington, Indiana 47403-2121 Electronic Coordination – robin_mckilliams@fws.gov</p>

<p>Marie Shepherd, Executive Director, Area Plan Commission, Jennings County Electronic Coordination – mshepherd@jenningscounty-in.gov</p>	<p>Mary Kennedy, Historic Bridge Specialist INDOT Environmental Services – Cultural Resources 100 N. Senate Ave, Room N758-ES Indianapolis, IN 46204 Electronic Coordination – mkennedy@indot.IN.gov</p>
<p>Justus McGill INDOT Ecology and Waterway Permitting Office 100 N. Senate Ave. IGCN 758-ES Indianapolis, IN 46204 Electronic Coordination – jmcgill@indot.in.gov</p>	

Organization and Project Information

Project ID:
Des. ID: 2200148
Project Title: Bridge Project on Park Road over Muscatatuck River
Name of Organization: Beam, Longest and Neff
Requested by: Kristin Wing

Environmental Assessment Report

1. Geological Hazards:
 - Potential Karst
 - 1% Annual Chance Flood Hazard
2. Mineral Resources:
 - Bedrock Resource: High Potential
 - Sand and Gravel Resource: None documented in the area
3. Active or abandoned mineral resources extraction sites:
 - None documented in the area

*All map layers from Indiana Map (maps.indiana.edu)

DISCLAIMER:

This document was compiled by Indiana University, Indiana Geological Survey, using data believed to be accurate; however, a degree of error is inherent in all data. This product is distributed "AS-IS" without warranties of any kind, either expressed or implied, including but not limited to warranties of suitability to a particular purpose or use. No attempt has been made in either the design or production of these data and document to define the limits or jurisdiction of any federal, state, or local government. The data used to assemble this document are intended for use only at the published scale of the source data or smaller (see the metadata links below) and are for reference purposes only. They are not to be construed as a legal document or survey instrument. A detailed on-the-ground survey and historical analysis of a single site may differ from these data and this document.

This information was furnished by Indiana Geological Survey

Address: 1001 E. 10th St., Bloomington, IN 47405

Email: IGSEnvir@indiana.edu

Phone: 812 855-7428

Date: September 25, 2023

THIS IS NOT A PERMIT

**State of Indiana
DEPARTMENT OF NATURAL RESOURCES
Division of Fish and Wildlife
Early Coordination/Environmental Assessment**

DNR#: ER-25971

Request Received: September 25, 2023

Requestor:

Kristin Wing
Beam, Longest and Neff
8320 Craig Street
Indianapolis, IN 46250

Project:

Park Road bridge (#P000-40-07088 B / NBI 060380) rehabilitation, including replacing the bridge deck and riprap installation, over Muscatatuck River in the Crosley Fish and Wildlife Area; Des #2200148

County/Site Info: Jennings County

The Indiana Department of Natural Resources has reviewed the above referenced project per your request. Our agency offers the following comments for your information and in accordance with the National Environmental Policy Act of 1969.

If our agency has regulatory jurisdiction over the project, the recommendations contained in this letter may become requirements of any permit issued. If we do not have permitting authority, all recommendations are voluntary.

Regulatory Assessment:

This proposal will require the formal approval of our agency for construction in a floodway, pursuant to the Flood Control Act (IC 14-28-1), unless it qualifies under the INDOT and IDNR Memorandum of Understanding for Maintenance Activity Exemption, dated March 1997. Please include a copy of this letter with the permit application, if required.

Natural Heritage Database:

The Natural Heritage Program's data have been checked. The Division of Nature Preserves does not anticipate any significant effects to the below-listed flora and communities. The following have been documented within .5 mile of the project area:

Properties

Crosley Fish and Wildlife Area
Toothwort Woods Nature Preserve

Fauna

Cerulean Warbler (*Setophaga cerulea*), State endangered
Little Spectaclecase (*Villosa lianosa*), Species of special concern
A Millipede (*Pseudopolydesmus collinus*), State endangered

Communities

Bluegrass Mesic Upland Forest
Bluegrass Dry-mesic Upland Forest
Limestone Cliff

Flora

Divided Toothwort (*Cardamine dissecta*), State endangered

Fish and Wildlife Comments:

Avoid and minimize impacts to fish, wildlife, and botanical resources to the greatest extent possible, and compensate for impacts. The following are recommendations that address potential impacts identified in the proposed project area:

A) Heritage Species

Suitable habitat for Cerulean Warblers exists within the project area. To minimize impacts to this species, remove trees outside of the spring migratory season and the breeding season (early April and late August).

To avoid impacts to the Little Spectaclecase, do not construct any causeways or cross the stream using heavy equipment.

B) Riparian Habitat

We recommend a mitigation plan be developed (and submitted with the permit application, if required) for any unavoidable habitat impacts that will occur. The DNR's Habitat Mitigation Guidelines (and plant lists) can be found online at: <https://www.in.gov/nrc/files/IB-17.pdf>.

Impacts to non-wetland forest of one (1) acre or more in a rural or urban area should be mitigated at a minimum 2:1 ratio based on area of impact. Impacts to non-wetland forest under one (1) acre but at least 0.10 acre in a rural or urban area should be mitigated at a minimum 1:1 ratio based on area of impact. Impacts under 0.10 acre in a rural area typically do not require mitigation or additional plantings beyond seeding and stabilizing disturbed areas, though there are exceptions for high quality habitat sites. Impacts under 0.10 acre in an urban area should be mitigated by replacing trees that are 10" diameter-at-breast height (dbh) or greater by planting five trees, 1" to 2" in dbh, for each tree which is removed that is 10" dbh or greater. Seeding and stabilizing disturbed areas is required regardless of the impact amount and location. If floodway impacts to forested wetland and non-wetland habitat areas combine to be 0.10 acres or more, mitigation should be done and coordinated with the biologist, as needed.

The mitigation site should be located in the floodway, downstream of the one (1) square mile drainage area of that stream (or another stream within the 8-digit HUC, preferably as close to the impact site as possible) and adjacent to existing forested riparian habitat.

C) Wildlife Passage

In many cases, riprap placed for scour protection can have negative impacts on wildlife passage along the banks of a river or smaller waterbody. Wildlife passage in transportation infrastructure projects and wildlife passage generally are priority issues for the Division of Fish and Wildlife. The following resources provide information on designing or maintaining wildlife passage under structures:

There are several techniques and materials for incorporating wildlife passage into the design of a crossing structure. Coordination with a Regional Environmental Biologist to address wildlife passage issues before submitting a permit application (if required) is encouraged to avoid delays in the permitting process. The following links are good resources to consider in the design of stream crossing structures to maintain fish and wildlife passage:

<https://www.fs.usda.gov/ccrc/tool/fishxing-fish-passage-learning-systems>

<https://www.fs.usda.gov/wildlifecrossings/library/index.php>

https://www.fhwa.dot.gov/clas/ctip/wildlife_crossing_structures/

<https://www.fhwa.dot.gov/engineering/hydraulics/pubs/11008/hif11008.pdf>

D) Nesting Birds

Monitor the bridges for bird nesting activity prior to construction. If any bird nests with eggs or young are found on the existing structures, do not work on the bridges from April 1 through September 7. If construction is planned during this time and active nests are present, prior approval from the USDA must be secured by contacting:

Wildlife Services State Director, USDA Wildlife Services, 901 W. State Street, W. Lafayette, IN 47907; (765) 494-6229; request Form 37 and any other required documentation and follow the USDA's instructions. If active nests are not found, project activities may move forward. It is recommended that bird deterrents are installed in the meantime to prevent birds from nesting underneath the structure during the remainder of the project.

The additional measures listed below should be implemented to avoid, minimize, or compensate for impacts to fish, wildlife, and botanical resources:

1. Revegetate all bare and disturbed areas that are not currently mowed and maintained with a mixture of grasses, sedges, and wildflowers native to Southern Indiana and specifically for stream bank/floodway stabilization purposes as soon as possible upon completion; turf-type grasses (including low-endophyte, friendly endophyte, and endophyte free tall fescue but excluding all other varieties of tall fescue) may be used in currently mowed areas only. A native herbaceous seed mixture must include at least 5 species of grasses and sedges and 5 species of wildflowers.
2. Minimize and contain within the project limits in-channel disturbance and the clearing of trees and brush.
3. Do not work in the waterway from April 1 through June 30 without the prior written approval of the Division of Fish and Wildlife.
4. Do not cut any trees suitable for Indiana Bat or Northern Long-eared Bat roosting (3 inches or greater diameter-at-breast height, living or dead, with loose hanging bark, or with cracks, crevices, or cavities) from April 1 through September 30.
5. Do not excavate in the low flow area except for the placement of piers, foundations, and riprap, or removal of the old structure.
6. Do not construct any temporary runarounds, access bridges, causeways, cofferdams, diversions, or pumparounds.
7. Use minimum average 6-inch graded riprap stone extended below the normal water level to provide habitat for aquatic organisms in the voids.
8. Do not deposit or allow construction/demolition materials or debris to fall or otherwise enter the waterway. Any incidental fallen material or debris in the waterway must be removed within 24 hours using best management practices, particularly lifting material out of the waterway and not dragging it across the streambed whenever possible.
9. Appropriately designed measures for controlling erosion and sediment must be implemented to prevent sediment from entering the waterbody or leaving the construction site; maintain these measures until construction is complete and all disturbed areas are stabilized.
10. Seed and protect all disturbed streambanks and slopes not protected by other methods that are 3:1 or steeper with erosion control blankets that are heavy-duty, biodegradable, and net free or that use loose-woven / Leno-woven netting to minimize the entrapment and snaring of small-bodied wildlife such as snakes and turtles (follow manufacturer's recommendations for selection and installation); seed and apply mulch on all other disturbed areas.

Contact Staff:

Our agency appreciates this opportunity to be of service. Please contact me at RVanVoorhis@dnr.IN.gov or (317) 232-8163 if we can be of further assistance.

Rachel Van Voorhis
Rachel Van Voorhis
Environmental Coordinator
Division of Fish and Wildlife

Date: October 25, 2023

From: [McWilliams, Robin](#)
To: [Kristin Wing](#); [Dye, David](#); CrosleyFWA@dnr.IN.gov
Cc: [Brian Shaw](#); [Walker, Matthew C](#)
Subject: Re: [EXTERNAL] Des No 2200148 Bridge Project, Jennings County, IN - Early Coordination Letter
Date: Tuesday, September 26, 2023 11:37:37 AM

This Message Is From an External Sender

This message came from outside your organization.

[Report Suspicious](#)

Dear Kristin,

These comments have been prepared under the authority of the Fish and Wildlife Coordination Act (16 U.S.C. 661 et. seq.) and are consistent with the intent of the National Environmental Policy Act of 1969, the Endangered Species Act of 1973, and the U. S. Fish and Wildlife Service's Mitigation Policy.

The project is within the range of the Indiana bat (*Myotis sodalis*) and northern long-eared bat (*Myotis septentrionalis*; NLEB) and should follow the Indiana bat/NLEB Federal Highway Administration, Federal Rail Administration, and Federal Transit Administration's programmatic consultation process, if applicable (*i.e.* a federal transportation nexus is established). The Service has 14 days after a “not likely to adversely affect” determination letter is generated to review the project and provide additional comments or request additional information (there is no review for projects with a "no effect" determination); if you do not receive a response from us within 14 days, we have no additional comments.

There are records of NLEB hibernacula within 0.15 miles of the site and capture records of the Indiana bat within 0.25 miles; therefore the project is within documented NLEB and Indiana bat habitat and the Information for Planning and Consultation (IPAC) key for federal transportation projects should be answered accordingly.

Other Species of Concern

Tricolored Bat

On September 14, 2022, the Service published a proposal in the Federal Register to list the tricolored bat (*Perimyotis subflavus*; TCB) as endangered under the Endangered Species Act (ESA). The Service has up to 12 months from the date the proposal was published to make a final determination, either to list the tricolored bat under the Act or to withdraw the proposal. The Service determined the bat faces extinction primarily due to the range-wide impacts of WNS. Because TCB populations have been greatly reduced due to WNS, surviving bat populations are now more vulnerable to other stressors such as human disturbance and habitat loss. Species proposed for listing are not afforded protection under the Act; however, as soon as a listing becomes effective (typically 30 days after publication of the final rule in the Federal Register), the prohibitions against jeopardizing its continued existence and “take” will apply. Therefore, if this project or other future or existing projects have the potential to adversely affect the TCB after the potential new listing goes into effect, we recommend that

the effects of the project on TCBs and their habitat be analyzed to determine whether authorization under ESA section 7 or 10 is necessary. Projects or programs with an existing section 7 biological opinion may require reinitiation of consultation, and projects with an existing section 10 incidental take permit may require an amendment to provide uninterrupted authorization for covered activities. Contact your local U.S. Fish and Wildlife Service Ecological Services Office for assistance.

The TCB is a small insectivorous bat that typically overwinters in caves, abandoned mines and tunnels, and road-associated culverts (southern portion of the range) and spends the rest of the year in forested habitats, typically roosting among live and dead leaf clusters in tree branches. For more information on TCB and the proposed rule, please see: <https://www.fws.gov/species/tricolored-bat-perimyotis-subflavus> and for more information on WNS, please see: <https://www.whitenosesyndrome.org/>

Wetland and stream impacts may require permits from the U.S. Army Corps of Engineers, the Indiana Department of Environmental Management's Water Quality Certification program, and the Indiana Department of Natural Resources. Wetland impacts should be avoided, and any unavoidable impacts should be compensated for in accordance with agency mitigation guidelines.

Section 4(f) of the Transportation Act of 1966 requires that land from a publicly owned park, recreation area or wildlife or waterfowl refuge or any significant public or private historical site shall not be used by the Federal Highway Administration for highway right-of-way unless a determination is made that there is no feasible and prudent alternative to the use of land from such property. Further, the proposed action must include all possible planning to minimize harm to the property which results from such use. A Section 4(f) determination concerning project impacts may be necessary as part of the environmental review process if federal funds are utilized.

Based on a review of the information you provided, the U.S. Fish and Wildlife Service has no other comments on the projects as currently proposed. However, should new information arise pertaining to project plans or a revised species list be published, it will be necessary for the Federal agency to reinitiate consultation. Standard recommendations are provided below.

We appreciate the opportunity to comment at this early stage of project planning. If you have any questions about our recommendations, please contact me at robin_mcwilliams@fws.gov or you may call 812-334-4261 x. 207.

Sincerely,
Robin McWilliams Munson

Robin McWilliams Munson
Fish and Wildlife Biologist
U.S. Fish and Wildlife Service
620 South Walker Street
Bloomington, IN 47403

***NEW* 812-902-1752**



United States Department of the Interior

FISH AND WILDLIFE SERVICE
Indiana Ecological Services Field Office
620 South Walker Street
Bloomington, IN 47403-2121
Phone: (812) 334-4261 Fax: (812) 334-4273



In Reply Refer To:

09/09/2024 16:51:32 UTC

Project Code: 2024-0122694

Project Name: Des 2200148 Bridge Rehabilitation on Park Road over Muscatatuck River

Subject: List of threatened and endangered species that may occur in your proposed project location or may be affected by your proposed project

To Whom It May Concern:

The enclosed species list identifies threatened, endangered, proposed and candidate species, as well as proposed and final designated critical habitat, that may occur within the boundary of your proposed project and/or may be affected by your proposed project. The species list fulfills the requirements of the U.S. Fish and Wildlife Service (Service) under section 7(c) of the Endangered Species Act (Act) of 1973, as amended (16 U.S.C. 1531 *et seq.*).

New information based on updated surveys, changes in the abundance and distribution of species, changed habitat conditions, or other factors could change this list. Please feel free to contact us if you need more current information or assistance regarding the potential impacts to federally proposed, listed, and candidate species and federally designated and proposed critical habitat. Please note that under 50 CFR 402.12(e) of the regulations implementing section 7 of the Act, the accuracy of this species list should be verified after 90 days. This verification can be completed formally or informally as desired. The Service recommends that verification be completed by visiting the IPaC website at regular intervals during project planning and implementation for updates to species lists and information. An updated list may be requested through the IPaC system by completing the same process used to receive the enclosed list.

The purpose of the Act is to provide a means whereby threatened and endangered species and the ecosystems upon which they depend may be conserved. Under sections 7(a)(1) and 7(a)(2) of the Act and its implementing regulations (50 CFR 402 *et seq.*), Federal agencies are required to utilize their authorities to carry out programs for the conservation of threatened and endangered species and to determine whether projects may affect threatened and endangered species and/or designated critical habitat.

Please use the species list provided and visit the U.S. Fish and Wildlife Service's Region 3 Section 7 Technical Assistance website at - <http://www.fws.gov/midwest/endangered/section7/s7process/index.html>. This website contains step-by-step instructions which will help you

determine if your project will have an adverse effect on listed species and will help lead you through the Section 7 process. For all **wind energy projects and projects that include installing towers that use guy wires or are over 200 feet in height**, please contact this field office directly for assistance, even if no federally listed plants, animals or critical habitat are present within your proposed project or may be affected by your proposed project.

A Biological Assessment is required for construction projects (or other undertakings having similar physical impacts) that are major Federal actions significantly affecting the quality of the human environment as defined in the National Environmental Policy Act (42 U.S.C. 4332(2)(c)). For projects other than major construction activities, the Service suggests that a biological evaluation similar to a Biological Assessment be prepared to determine whether the project may affect listed or proposed species and/or designated or proposed critical habitat. Recommended contents of a Biological Assessment are described at 50 CFR 402.12.

If a Federal agency determines, based on the Biological Assessment or biological evaluation, that listed species and/or designated critical habitat may be affected by the proposed project, the agency is required to consult with the Service pursuant to 50 CFR 402. In addition, the Service recommends that candidate species, proposed species and proposed critical habitat be addressed within the consultation. More information on the regulations and procedures for section 7 consultation, including the role of permit or license applicants, can be found in the "Endangered Species Consultation Handbook" at:

<https://www.fws.gov/sites/default/files/documents/endangered-species-consultation-handbook.pdf>

Migratory Birds: In addition to responsibilities to protect threatened and endangered species under the Endangered Species Act (ESA), there are additional responsibilities under the Migratory Bird Treaty Act (MBTA) and the Bald and Golden Eagle Protection Act (BGEPA) to protect native birds from project-related impacts. Any activity, intentional or unintentional, resulting in take of migratory birds, including eagles, is prohibited unless otherwise permitted by the U.S. Fish and Wildlife Service (50 C.F.R. Sec. 10.12 and 16 U.S.C. Sec. 668(a)). For more information regarding these Acts, see <https://www.fws.gov/program/migratory-bird-permit/what-we-do>.

The MBTA has no provision for allowing take of migratory birds that may be unintentionally killed or injured by otherwise lawful activities. It is the responsibility of the project proponent to comply with these Acts by identifying potential impacts to migratory birds and eagles within applicable NEPA documents (when there is a federal nexus) or a Bird/Eagle Conservation Plan (when there is no federal nexus). Proponents should implement conservation measures to avoid or minimize the production of project-related stressors or minimize the exposure of birds and their resources to the project-related stressors. For more information on avian stressors and recommended conservation measures, see <https://www.fws.gov/library/collections/threats-birds>.

In addition to MBTA and BGEPA, Executive Order 13186: *Responsibilities of Federal Agencies to Protect Migratory Birds*, obligates all Federal agencies that engage in or authorize activities that might affect migratory birds, to minimize those effects and encourage conservation measures that will improve bird populations. Executive Order 13186 provides for the protection of both migratory birds and migratory bird habitat. For information regarding the implementation of

Executive Order 13186, please visit <https://www.fws.gov/partner/council-conservation-migratory-birds>.

We appreciate your concern for threatened and endangered species. The Service encourages Federal agencies to include conservation of threatened and endangered species into their project planning to further the purposes of the Act. **Please include the Consultation Code in the header of this letter with any request for consultation or correspondence about your project that you submit to our office.**

Attachment(s):

- Official Species List
- Bald & Golden Eagles
- Migratory Birds
- Wetlands

OFFICIAL SPECIES LIST

This list is provided pursuant to Section 7 of the Endangered Species Act, and fulfills the requirement for Federal agencies to "request of the Secretary of the Interior information whether any species which is listed or proposed to be listed may be present in the area of a proposed action".

This species list is provided by:

Indiana Ecological Services Field Office

620 South Walker Street
Bloomington, IN 47403-2121
(812) 334-4261

PROJECT SUMMARY

Project Code: 2024-0122694
Project Name: Des 2200148 Bridge Rehabilitation on Park Road over Muscatatuck River
Project Type: Bridge - Maintenance
Project Description: The proposed project consists of a bridge rehabilitation on Park Road over Muscatatuck River in the Crosley Fish and Wildlife Area in Jennings County, Indiana. The anticipated project limits extend approximately 155 feet north and south of the bridge. The preferred alternative will consist of rehabilitating the existing bridge by cleaning and painting the truss, replacing the bridge deck, repairing or replacing verticals, gusset plates, and low chord truss members, replacing some truss secondary members, removing and reconstructing portions of the concrete abutments and pier caps, patching abutments and piers, placing scour countermeasures at all substructure units, and replacing the bridge rail with a steel handrail. Suitable summer habitat is present within the project area, and some may be removed as part of this project. Approximately 0.003 acre of tree clearing will take place. Tree removal dates for projects located within the hibernacula buffer or critical habitat are from November 15 to March 31 (instead of the standard October 1 to March 31) to allow for the conclusion of fall swarming around the hibernacula. No permanent or temporary right-of-way will be required.

The review of the USFWS database on August 9, 2023, indicated the presence of endangered bat species in or within 0.5 mile of the project area; and the project is also located within Northern Long-Eared Bat hibernacula buffer. Two northern long eared bat hibernacula are located 0.11 northwest and 0.10 mile north of the project area. Also, one Indiana bat capture is 0.23 mile south of the project area. A bridge inspection was completed by Egis on July 11, 2024, and no evidence of bats or birds was present. Temporary lighting may be used for this project. The project's letting date is January 14, 2026, and construction is anticipated to begin Fall 2026.

Project Location:

The approximate location of the project can be viewed in Google Maps: <https://www.google.com/maps/@38.96180255,-85.61708239558115,14z>



Counties: Jennings County, Indiana

ENDANGERED SPECIES ACT SPECIES

There is a total of 6 threatened, endangered, or candidate species on this species list.

Species on this list should be considered in an effects analysis for your project and could include species that exist in another geographic area. For example, certain fish may appear on the species list because a project could affect downstream species.

IPaC does not display listed species or critical habitats under the sole jurisdiction of NOAA Fisheries¹, as USFWS does not have the authority to speak on behalf of NOAA and the Department of Commerce.

See the "Critical habitats" section below for those critical habitats that lie wholly or partially within your project area under this office's jurisdiction. Please contact the designated FWS office if you have questions.

-
1. [NOAA Fisheries](#), also known as the National Marine Fisheries Service (NMFS), is an office of the National Oceanic and Atmospheric Administration within the Department of Commerce.

MAMMALS

NAME	STATUS
Indiana Bat <i>Myotis sodalis</i> There is final critical habitat for this species. Your location does not overlap the critical habitat. Species profile: https://ecos.fws.gov/ecp/species/5949	Endangered
Northern Long-eared Bat <i>Myotis septentrionalis</i> No critical habitat has been designated for this species. Species profile: https://ecos.fws.gov/ecp/species/9045	Endangered
Tricolored Bat <i>Perimyotis subflavus</i> No critical habitat has been designated for this species. Species profile: https://ecos.fws.gov/ecp/species/10515	Proposed Endangered

BIRDS

NAME	STATUS
Whooping Crane <i>Grus americana</i> Population: U.S.A. (AL, AR, CO, FL, GA, ID, IL, IN, IA, KY, LA, MI, MN, MS, MO, NC, NM, OH, SC, TN, UT, VA, WI, WV, western half of WY) No critical habitat has been designated for this species. Species profile: https://ecos.fws.gov/ecp/species/758	Experimental Population, Non- Essential

CLAMS

NAME	STATUS
Salamander Mussel <i>Simpsonaias ambigua</i> There is proposed critical habitat for this species. Your location does not overlap the critical habitat. Species profile: https://ecos.fws.gov/ecp/species/6208	Proposed Endangered

INSECTS

NAME	STATUS
Monarch Butterfly <i>Danaus plexippus</i> No critical habitat has been designated for this species. Species profile: https://ecos.fws.gov/ecp/species/9743	Candidate

CRITICAL HABITATS

THERE ARE NO CRITICAL HABITATS WITHIN YOUR PROJECT AREA UNDER THIS OFFICE'S JURISDICTION.

YOU ARE STILL REQUIRED TO DETERMINE IF YOUR PROJECT(S) MAY HAVE EFFECTS ON ALL ABOVE LISTED SPECIES.

BALD & GOLDEN EAGLES

Bald and golden eagles are protected under the Bald and Golden Eagle Protection Act¹ and the Migratory Bird Treaty Act².

Any person or organization who plans or conducts activities that may result in impacts to bald or golden eagles, or their habitats³, should follow appropriate regulations and consider implementing appropriate conservation measures, as described in the links below. Specifically, please review the ["Supplemental Information on Migratory Birds and Eagles"](#).

-
1. The [Bald and Golden Eagle Protection Act](#) of 1940.
 2. The [Migratory Birds Treaty Act](#) of 1918.
 3. 50 C.F.R. Sec. 10.12 and 16 U.S.C. Sec. 668(a)

There are likely bald eagles present in your project area. For additional information on bald eagles, refer to [Bald Eagle Nesting and Sensitivity to Human Activity](#)

For guidance on when to schedule activities or implement avoidance and minimization measures to reduce impacts to migratory birds on your list, see the PROBABILITY OF PRESENCE SUMMARY below to see when these birds are most likely to be present and breeding in your project area.

NAME	BREEDING SEASON
Bald Eagle <i>Haliaeetus leucocephalus</i> This is not a Bird of Conservation Concern (BCC) in this area, but warrants attention because of the Eagle Act or for potential susceptibilities in offshore areas from certain types of development or activities. https://ecos.fws.gov/ecp/species/1626	Breeds Sep 1 to Jul 31

PROBABILITY OF PRESENCE SUMMARY

The graphs below provide our best understanding of when birds of concern are most likely to be present in your project area. This information can be used to tailor and schedule your project activities to avoid or minimize impacts to birds. Please make sure you read ["Supplemental Information on Migratory Birds and Eagles"](#), specifically the FAQ section titled "Proper Interpretation and Use of Your Migratory Bird Report" before using or attempting to interpret this report.

Probability of Presence (■)

Green bars; the bird's relative probability of presence in the 10km grid cell(s) your project overlaps during that week of the year.

Breeding Season (■)

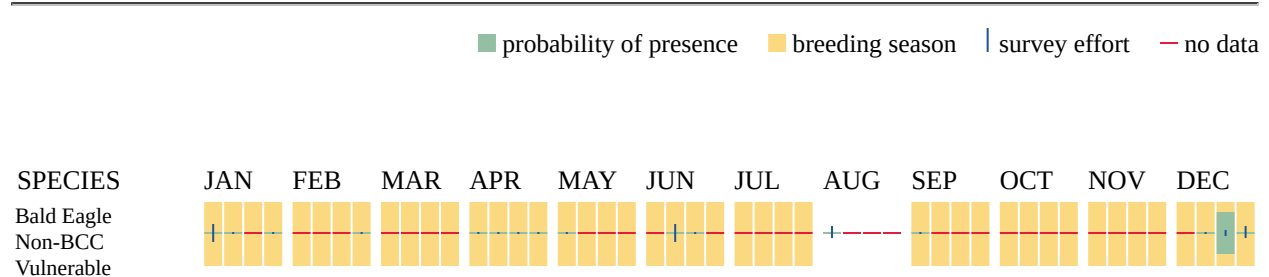
Yellow bars; liberal estimate of the timeframe inside which the bird breeds across its entire range.

Survey Effort (|)

Vertical black lines; the number of surveys performed for that species in the 10km grid cell(s) your project area overlaps.

No Data (—)

A week is marked as having no data if there were no survey events for that week.



Additional information can be found using the following links:

- Eagle Management <https://www.fws.gov/program/eagle-management>
- Measures for avoiding and minimizing impacts to birds <https://www.fws.gov/library/collections/avoiding-and-minimizing-incident-take-migratory-birds>
- Nationwide conservation measures for birds <https://www.fws.gov/sites/default/files/documents/nationwide-standard-conservation-measures.pdf>
- Supplemental Information for Migratory Birds and Eagles in IPaC <https://www.fws.gov/media/supplemental-information-migratory-birds-and-bald-and-golden-eagles-may-occur-project-action>

MIGRATORY BIRDS

Certain birds are protected under the Migratory Bird Treaty Act¹ and the Bald and Golden Eagle Protection Act².

Any person or organization who plans or conducts activities that may result in impacts to migratory birds, eagles, and their habitats³ should follow appropriate regulations and consider implementing appropriate conservation measures, as described in the links below. Specifically, please review the "[Supplemental Information on Migratory Birds and Eagles](#)".

-
1. The [Migratory Birds Treaty Act](#) of 1918.
 2. The [Bald and Golden Eagle Protection Act](#) of 1940.
 3. 50 C.F.R. Sec. 10.12 and 16 U.S.C. Sec. 668(a)

For guidance on when to schedule activities or implement avoidance and minimization measures to reduce impacts to migratory birds on your list, see the PROBABILITY OF PRESENCE SUMMARY below to see when these birds are most likely to be present and breeding in your project area.

NAME	BREEDING SEASON
Bald Eagle <i>Haliaeetus leucocephalus</i> This is not a Bird of Conservation Concern (BCC) in this area, but warrants attention because of the Eagle Act or for potential susceptibilities in offshore areas from certain types of development or activities. https://ecos.fws.gov/ecp/species/1626	Breeds Sep 1 to Jul 31
Chimney Swift <i>Chaetura pelagica</i> This is a Bird of Conservation Concern (BCC) throughout its range in the continental USA and Alaska. https://ecos.fws.gov/ecp/species/9406	Breeds Mar 15 to Aug 25
Field Sparrow <i>Spizella pusilla</i> This is a Bird of Conservation Concern (BCC) only in particular Bird Conservation Regions (BCRs) in the continental USA https://ecos.fws.gov/ecp/species/9446	Breeds Mar 1 to Aug 15
Grasshopper Sparrow <i>Ammodramus savannarum perpallidus</i> This is a Bird of Conservation Concern (BCC) only in particular Bird Conservation Regions (BCRs) in the continental USA https://ecos.fws.gov/ecp/species/8329	Breeds Jun 1 to Aug 20
Kentucky Warbler <i>Geothlypis formosa</i> This is a Bird of Conservation Concern (BCC) throughout its range in the continental USA and Alaska. https://ecos.fws.gov/ecp/species/9443	Breeds Apr 20 to Aug 20
Red-headed Woodpecker <i>Melanerpes erythrocephalus</i> This is a Bird of Conservation Concern (BCC) throughout its range in the continental USA and Alaska. https://ecos.fws.gov/ecp/species/9398	Breeds May 10 to Sep 10
Wood Thrush <i>Hylocichla mustelina</i> This is a Bird of Conservation Concern (BCC) throughout its range in the continental USA and Alaska. https://ecos.fws.gov/ecp/species/9431	Breeds May 10 to Aug 31

PROBABILITY OF PRESENCE SUMMARY

The graphs below provide our best understanding of when birds of concern are most likely to be present in your project area. This information can be used to tailor and schedule your project activities to avoid or minimize impacts to birds. Please make sure you read "[Supplemental Information on Migratory Birds and Eagles](#)", specifically the FAQ section titled "Proper Interpretation and Use of Your Migratory Bird Report" before using or attempting to interpret this report.

Probability of Presence (■)

Green bars; the bird's relative probability of presence in the 10km grid cell(s) your project overlaps during that week of the year.

Breeding Season (■)

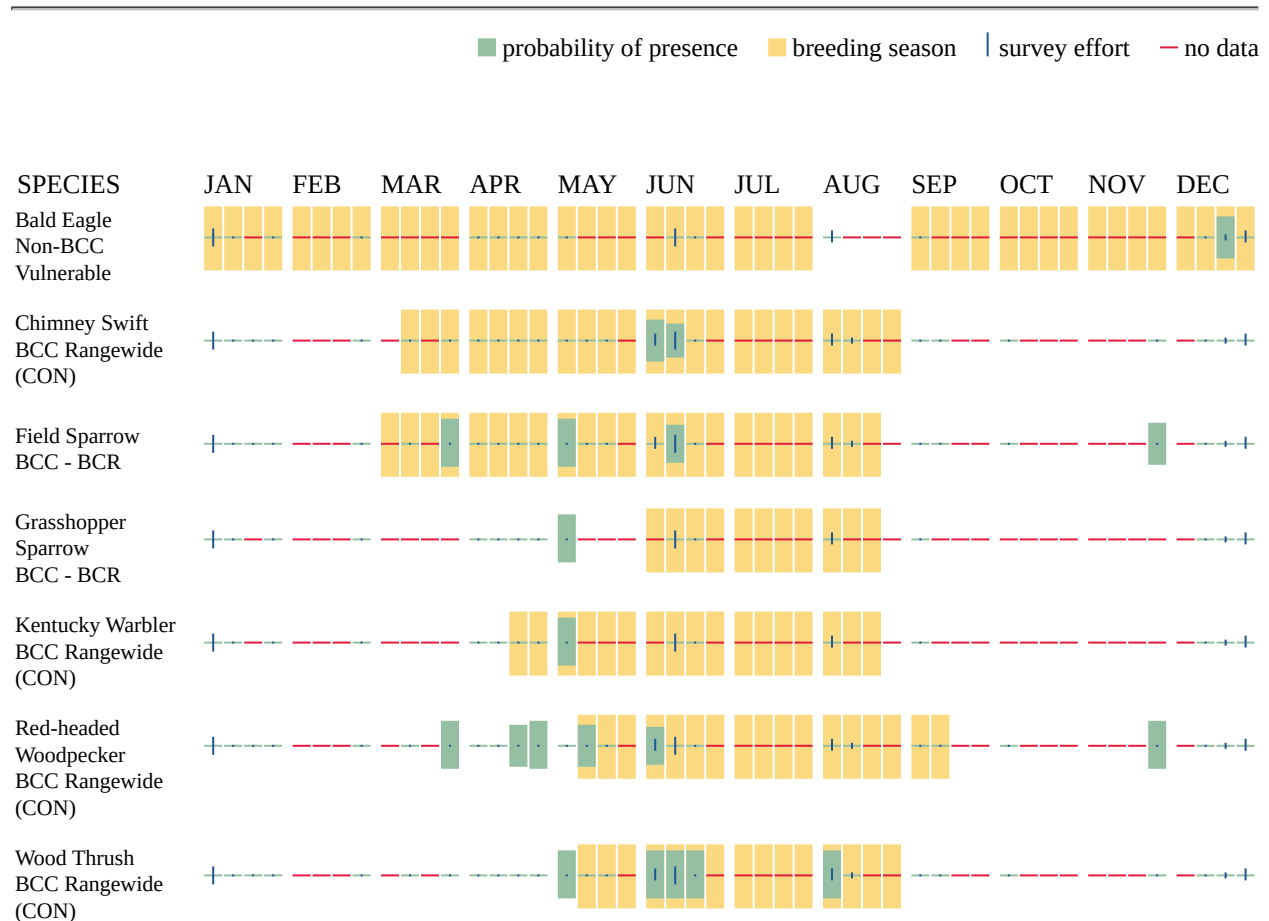
Yellow bars; liberal estimate of the timeframe inside which the bird breeds across its entire range.

Survey Effort (|)

Vertical black lines; the number of surveys performed for that species in the 10km grid cell(s) your project area overlaps.

No Data (—)

A week is marked as having no data if there were no survey events for that week.



Additional information can be found using the following links:

- Eagle Management <https://www.fws.gov/program/eagle-management>
- Measures for avoiding and minimizing impacts to birds <https://www.fws.gov/library/collections/avoiding-and-minimizing-incidental-take-migratory-birds>

- Nationwide conservation measures for birds <https://www.fws.gov/sites/default/files/documents/nationwide-standard-conservation-measures.pdf>
- Supplemental Information for Migratory Birds and Eagles in IPaC <https://www.fws.gov/media/supplemental-information-migratory-birds-and-bald-and-golden-eagles-may-occur-project-action>

WETLANDS

Impacts to [NWI wetlands](#) and other aquatic habitats may be subject to regulation under Section 404 of the Clean Water Act, or other State/Federal statutes.

For more information please contact the Regulatory Program of the local [U.S. Army Corps of Engineers District](#).

Please note that the NWI data being shown may be out of date. We are currently working to update our NWI data set. We recommend you verify these results with a site visit to determine the actual extent of wetlands on site.

RIVERINE

- R2UBH

USFWS Concurrence Letter Response 10.9.24

From: [Schwering, Taylor](#)
To: [WING Kristin](#)
Cc: [Carleton, Erin](#); [Dye, David](#)
Subject: FW: [EXTERNAL] FW: Des 2200148 Park Road over Muscatatuck River Section 7 Letter Review
Date: Wednesday, October 9, 2024 12:39:14 PM
Attachments: [image002.png](#)

Hi Kristin,

Please see below on USFWS response to your letter.
Let us know if you have any questions.

Thanks!

Taylor Schwering

Environmental Manager

185 Agrico Lane

Seymour, IN 47274

Office: (812) 524-3794

Email: tschwering@indot.in.gov

[Find us on social media!](#)



From: McWilliams, Robin <robin_mcwilliams@fws.gov>
Sent: Wednesday, October 9, 2024 12:15 PM
To: Schwering, Taylor <TSchwering@indot.IN.gov>
Cc: Carleton, Erin <ECarleton@indot.IN.gov>; Dye, David <DDYE@indot.IN.gov>
Subject: Re: [EXTERNAL] FW: Des 2200148 Park Road over Muscatatuck River Section 7 Letter Review

EXTERNAL EMAIL: This email was sent from outside your organization. Exercise caution when clicking links, opening attachments or taking further action, before validating its authenticity.

Dear Taylor,

This email responds to your request for the U.S. Fish and Wildlife Service's (Service) concurrence on a "not likely to adversely affect" determination for the Indiana bat (*Myotis sodalis*), northern long-eared bat (*Myotis septentrionalis*), tricolored bat (*Perimyotis subflavus*), whooping crane (*Grus americana*), salamander mussel (*Simpsonia ambigua*), and the monarch butterfly (*Danaus plexippus*) made by the Federal Highway Administration and Indiana Department of Transportation for the Park

Road over Muscatatuck River Project near Vernon, Indiana. Due to bat and hibernacula records in the project vicinity, this project does not qualify for the FHWA, Federal Rail Administration (FRA), and Federal Transit Administration (FTA) Range-wide Indiana bat and Northern long-eared bat Section 7 Programmatic Consultation process.

These comments have been prepared under the authority of the Fish and Wildlife Coordination Act (16 U.S.C. 661 et. seq.) and are consistent with the intent of the National Environmental Policy Act of 1969, the Endangered Species Act of 1973, and the U. S. Fish and Wildlife Service's Mitigation Policy.

The project is on Park Road over the Muscatatuck River in the Crosley Fish and Wildlife Area in Jennings County, Indiana. The existing bridge is approximately 212 feet long by 12.3 feet wide. It is a three-span truss bridge constructed in 1910 and rehabilitated in 1979, 2004, and 2015. The main span is a Pratt-through truss, and the approach spans are Warren pony trusses. The superstructure is composed of a timber plank deck supported by steel stringers and floor beams. In 2019, the bridge was closed due to the condition of the deck. The bridge is considered scour-critical due to the unknown foundations and the large scour depths calculated.

The project proposes rehabilitating the existing bridge by cleaning and painting the truss, replacing the bridge deck, repairing or replacing verticals, gusset plates, and low chord truss members or connections, replacing some truss secondary members, removing and reconstructing concrete abutments and pier caps, patching abutments and piers, placing scour countermeasures at all substructure units, and replacing the bridge rail with a steel handrail. Five trees will be removed, and trees will be trimmed 10 feet from the bridge. This work will occur during the bat inactive season between November 15 and March 30. Personnel inspected the bridge for the presence and signs of bats on June 26, 2024; no bats or signs of bats were identified. No permanent lighting will be installed as part of this project; however, temporary lighting during construction may be necessary. Temporary lighting will be directed away from suitable habitat and removed upon project completion. Riprap or concrete will be installed as scour protection around one pier. No causeways or cofferdams will be used. No temporary or permanent right-of-way is anticipated. The project's scheduled letting date is November 13, 2025, and construction is anticipated to begin in Spring 2026.

Based on the information provided in your August 9th letter, including the application of avoidance and minimization measures such as winter tree-clearing and minimizing lighting effects, the Service concurs that this project is not likely to adversely affect the Indiana bat, northern long-eared bat, tricolored bat, monarch butterfly, whooping crane,

and salamander mussel.

This precludes the need for further consultation on this project as required under section 7 of the ESA. If, however, new information on endangered species or the extent of impacts at the site becomes available, or if project plans are changed significantly, please contact our office for further consultation. We appreciate the opportunity to review and comment at this early stage of project planning. Additional recommendations are provided below. If you have any questions or concerns, please feel free to contact me.

Sincerely,
Robin

RECOMMENDATIONS

Based on a review of the information you provided, we recommend the following mitigation measures be included in the final project plans to minimize adverse impacts to fish and wildlife resources. Bolded recommendations are more specific to this project than our standard ones:

- 1. Implement pollution prevention and control measures during construction to reduce the potential for hazardous chemicals, construction material, and debris to enter the river. Any material that inadvertently enters the river should be removed as soon as possible by lifting it out as opposed to dragging it over or through the streambed.**
- 2. Place refueling staging areas, fuel storage, and hazardous materials away from the river. If hydro-demolition is required, some sort of tarp or collection system should be in place to prevent debris from falling into the river.**
3. Restrict below low-water work to placement of piers, pilings and/or footings, shaping of the spill slopes around the bridge abutments, and placement of riprap.
4. Restrict channel work and vegetation clearing to the minimum necessary and avoid, if possible, any heavy equipment in the stream.
5. Implement temporary erosion and siltation control devices such as placement of riprap check dams in drainage ways and ditches, installation of silt fences, covering exposed areas with erosion control materials, and grading slopes to retain runoff in basins.
6. Avoid all work within the inundated part of the stream channel (in perennial streams and larger intermittent streams) during the fish spawning season (April 1 through June 30), except for work within sealed structures such as caissons or cofferdams that were installed prior to the spawning season. No equipment should be operated below

Ordinary High-Water Mark during this time unless the machinery is within the caissons or on the cofferdams.

7. Re-vegetate all disturbed soil areas immediately upon project completion, using native trees and shrubs in the riparian zone wherever feasible.

8. Post DO NOT DISTURB signs at the construction zone boundaries and do not clear trees or understory vegetation outside the boundaries.

Robin McWilliams Munson
Fish and Wildlife Biologist/Transportation Liaison
U.S. Fish and Wildlife Service
Indiana Ecological Services Field Office
620 South Walker Street
Bloomington, IN 47403
Robin_McWilliams@fws.gov

***NEW* 812-902-1752**

Mon-Tues 8:30-4:30p

Wed-Thurs 8:30-4:30p Telework

From: [Burskey, Jacob L](#)
To: [Kristin Wing](#)
Cc: [Brian Shaw](#); [Mcgill, Justus](#); [Curry, Jennifer](#); [Rehder, Crystal](#)
Subject: RE: Des No 2200148 Bridge Project, Jennings County, IN - Early Coordination Letter & Karst Question
Date: Tuesday, September 26, 2023 8:25:04 AM
Attachments: [image001.png](#)
[USP DISCOVERY OF KARST FEATURES.pdf](#)

This Message Is From an External Sender

This message came from outside your organization.

[Report Suspicious](#)

Kristin,

At this time, EWPO does not recommend that a karst investigation be completed for this project. Based off desktop review, the project should not extend to the mapped sinkhole area south of the bridge and it does not appear that the project area contains surface level karst features. Additionally, the scope of work is limited to minimal excavation within close proximity to the roadway. EWPO does advise that the attached karst USP be included into the commitments and noted in the NEPA document.

Thanks

Jacob Burskey
Seymour District Specialist, Ecology and Waterway Permitting Office
100 N Senate Ave, N758 – Environmental Services
Indianapolis, IN 46204-2216
Phone: 317-646-2266
Email: jburskey@indot.in.gov



From: Rehder, Crystal <CRehder@indot.IN.gov>
Sent: Monday, September 25, 2023 12:05 PM
To: 'Kristin Wing' <kwing@b-l-n.com>
Cc: Brian Shaw <bshaw@b-l-n.com>; McGill, Justus <JMcgill@indot.IN.gov>; Burskey, Jacob L <JBurskey@indot.IN.gov>; Curry, Jennifer <JCurry1@indot.IN.gov>
Subject: RE: Des No 2200148 Bridge Project, Jennings County, IN - Early Coordination Letter & Karst Question

Kristin – This is in Seymour District so Jake will be able to make this determination for you.

Crystal Rehder

(317) 499-3274

From: Kristin Wing <kwing@b-l-n.com>
Sent: Monday, September 25, 2023 11:37 AM
To: McGill, Justus <JMcgill@indot.IN.gov>
Cc: Rehder, Crystal <CREhder@indot.IN.gov>; Brian Shaw <bshaw@b-l-n.com>
Subject: Des No 2200148 Bridge Project, Jennings County, IN - Early Coordination Letter & Karst Question

**** This is an EXTERNAL email. Exercise caution. DO NOT open attachments or click links from unknown senders or unexpected email. ****

Good Morning Justus,

Please find the attached Early Coordination Letter for the bridge project on Park Road over the Muscatatuck River (des 2200148) in Jennings County, Indiana. Our project is just north of a karst feature and we wanted to know if a karst study would be needed.

Thank you,

KRISTIN WING









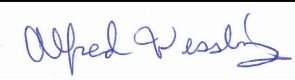
Senior Analyst
Environmental Services
o: [317-806-4342](tel:317-806-4342)
B-L-N.COM

BEAM, LONGEST and NEFF

Egis GROUP

A Tradition of Excellence Since 1945

Bridge/Structure Bat Assessment Form

Date & Time of Assessment 6/26/2024; 5 pm		DOT Project Number 2200148		Route/Facility Carried Park Road		County Jennings	
Federal Structure ID P000-40-07088 B		Structure Coordinates 38.96177, -85.61714 (latitude and longitude)		Structure Height (approximate) 34 feet		Structure Length 212'-0"	
Structure Type (check one)				Structure Material (check all that apply)			
Bridge Construction Style				Deck Material		Beam Material	
<input type="radio"/> Cast-in-place 		<input type="radio"/> Pre-stressed Girder 		<input type="checkbox"/> Metal		<input type="checkbox"/> None	
<input type="radio"/> Flat Slab/Box 		<input type="radio"/> Steel I-beam 		<input type="checkbox"/> Concrete		<input type="checkbox"/> Concrete	
<input checked="" type="radio"/> Truss 		<input type="radio"/> Covered 		<input checked="" type="checkbox"/> Timber		<input checked="" type="checkbox"/> Steel	
<input type="radio"/> Parallel Box Beam 		<input type="radio"/> Other:		<input type="checkbox"/> Open grid		<input type="checkbox"/> Timber	
				<input type="checkbox"/> Other:		<input type="checkbox"/> Other:	
Culvert Type				Culvert Material		Creosote Evidence	
<input type="radio"/> Box		<input type="radio"/> Other Structure		<input type="checkbox"/> Metal		<input type="radio"/> Yes <input checked="" type="radio"/> No	
<input type="radio"/> Pipe/Round				<input type="checkbox"/> Concrete		<input type="radio"/> Unknown	
<input type="radio"/> Other:				<input type="checkbox"/> Plastic		Notes:	
				<input type="checkbox"/> Stone/Masonry			
				<input type="checkbox"/> Other:			
Crossings Traversed (check all that apply)				Surrounding Habitat (check all that apply)			
<input type="checkbox"/> Bare ground		<input checked="" type="checkbox"/> Open vegetation		<input type="checkbox"/> Agricultural		<input type="checkbox"/> Grassland	
<input type="checkbox"/> Rip-rap		<input type="checkbox"/> Closed vegetation		<input type="checkbox"/> Commercial		<input type="checkbox"/> Ranching	
<input checked="" type="checkbox"/> Flowing water		<input type="checkbox"/> Railroad		<input type="checkbox"/> Residential-urban		<input type="checkbox"/> Riparian/wetland	
<input type="checkbox"/> Standing water		<input type="checkbox"/> Road/trail - Type:		<input type="checkbox"/> Residential-rural		<input type="checkbox"/> Mixed use	
<input type="checkbox"/> Seasonal water		<input type="checkbox"/> Other:		<input checked="" type="checkbox"/> Woodland/forested		<input type="checkbox"/> Other:	
Areas Assessed (check all that apply)							
Check all areas that apply. If an area is not present in the structure, check the "not present" box.							
Document all bat indicators observed during the assessment. Include the species present, if known, and provide photo documentation as indicated.							
Area (check if assessed)		Assessment Notes		Evidence of Bats (include photos if present)			
<input type="checkbox"/> All crevices and cracks: Bridges/culverts: rough surfaces or imperfections in concrete Other structures: soffits, rafters, attic areas		<input type="checkbox"/> Not present		<input type="checkbox"/> Visual - live # 0 dead # 0		<input type="checkbox"/> Audible <input type="checkbox"/> Species	
<input checked="" type="checkbox"/> Concrete surfaces (open roosting on concrete)				<input type="checkbox"/> Guano		<input type="checkbox"/> Odor	
				<input type="checkbox"/> Staining		<input type="checkbox"/> Photos	
<input checked="" type="checkbox"/> Spaces between concrete end walls and the bridge deck		<input type="checkbox"/> Not present		<input type="checkbox"/> Visual - live # 0 dead # 0		<input type="checkbox"/> Audible <input type="checkbox"/> Species	
				<input type="checkbox"/> Guano		<input type="checkbox"/> Odor	
				<input type="checkbox"/> Staining		<input type="checkbox"/> Photos	
<input type="checkbox"/> Crack between concrete railings on top of the bridge deck 		<input checked="" type="checkbox"/> Not present		<input type="checkbox"/> Visual - live # dead #		<input type="checkbox"/> Audible <input type="checkbox"/> Species	
				<input type="checkbox"/> Guano		<input type="checkbox"/> Odor	
				<input type="checkbox"/> Staining		<input type="checkbox"/> Photos	
<input type="checkbox"/> Vertical surfaces on concrete I-beams		<input checked="" type="checkbox"/> Not present		<input type="checkbox"/> Visual - live # dead #		<input type="checkbox"/> Audible <input type="checkbox"/> Species	
				<input type="checkbox"/> Guano		<input type="checkbox"/> Odor	
				<input type="checkbox"/> Staining		<input type="checkbox"/> Photos	
<input checked="" type="checkbox"/> Weep holes, scupper drains, and inlets/pipes		<input type="checkbox"/> Not present		<input type="checkbox"/> Visual - live # 0 dead # 0		<input type="checkbox"/> Audible <input type="checkbox"/> Species	
				<input type="checkbox"/> Guano		<input type="checkbox"/> Odor	
				<input type="checkbox"/> Staining		<input type="checkbox"/> Photos	
<input type="checkbox"/> All guiderails		<input checked="" type="checkbox"/> Not present		<input type="checkbox"/> Visual - live # dead #		<input type="checkbox"/> Audible <input type="checkbox"/> Species	
				<input type="checkbox"/> Guano		<input type="checkbox"/> Odor	
				<input type="checkbox"/> Staining		<input type="checkbox"/> Photos	
<input type="checkbox"/> All expansion joints		<input checked="" type="checkbox"/> Not present		<input type="checkbox"/> Visual - live # dead #		<input type="checkbox"/> Audible <input type="checkbox"/> Species	
				<input type="checkbox"/> Guano		<input type="checkbox"/> Odor	
				<input type="checkbox"/> Staining		<input type="checkbox"/> Photos	
Name: Alfred Wessling				Signature: 			

Appendix D:

Section 106 of the NHPA



INDIANA DEPARTMENT OF TRANSPORTATION

100 North Senate Avenue
Room N758-ES
Indianapolis, Indiana 46204

PHONE: (855) 463-6848

Eric Holcomb, Governor
Michael Smith, Commissioner

August 25, 2023

This letter was sent to the listed parties.

RE: Dual Review FHWA Project: Des. No. 2200148; Park Road Bridge Project, Jennings County, Indiana

Dear Consulting Party (see attached list),

The Indiana Department of Natural Resources (IDNR) and the Indiana Department of Transportation (INDOT), with funding from the Federal Highway Administration (FHWA), propose to proceed with the Park Road bridge project (Des. No. 2200148). Cultural Resource Analysts, Inc. (CRA) is under contract with INDOT to advance the Section 106 documentation while Beam, Longest, & Neff (BLN), LLC will be completing the environmental documentation for the referenced project.

This letter is part of the Section 106 review process for this project. Section 106 of the National Historic Preservation Act (NRHP) requires federal agencies to take into account the effects of their undertakings on historic properties. We are requesting comments from you regarding the possible effects of this project. Please use the above Des. Number and project description in your reply and your comments will be incorporated into the formal environmental study.

The proposed bridge undertaking is located within the Crosley Fish and Wildlife Area, established in 1931, in Jennings County, Indiana. More specifically the project area is within Vernon Township as shown on the Vernon, Indiana USGS Topographic Quadrangle, in Section 15, Township 6 N, Range 8 E. The project area along Park Road encompasses approximately 1.0 acre within the Crosley Fish and Wildlife Area. The project area can be viewed online at <https://arcg.is/jqueP> (the Des. No. is the most efficient search term once in the CRO - Public Web Map App).

The project need is based on the current condition of the structure elements and notable deficiencies in the wearing surface, deck, superstructure, and substructure, and the scour critical status of the bridge. The bridge's deck deterioration caused it to be closed to traffic in 2019. The purpose of this project is to correct the deficiencies in the bridge wearing surface, deck, superstructure, and substructure (scour protection) to reopen the bridge to vehicular traffic, and protect the bridge and extend its service life.

The proposed work will occur on the Park Road bridge (P000-40-07088; National Bridge Inventory [NBI] Number [No.] 60380) over Muscatatuck River. The existing bridge is an approximately 212 feet by 12.3 feet, three-span, truss bridge constructed in 1910. The main span of the bridge is a Pratt-through truss and the approach spans are both Warren pony trusses. The bridge was rehabilitated in 2004 when the deck was replaced with new wood planks and the truss was cleaned and painted (Des. No. 0300801). The gusset plates at Abutment Number (No.) 1 were replaced (Des. No. 1400643) in 2015 along with a portion of the south span deck. According to the *Indiana Historic Bridge Inventory*, completed in 2010, the bridge was determined eligible for listing in the National Register of Historic Places (NRHP) under Criterion C. It was also determined to be a "Non-Select" bridge, which are those that may not be considered excellent examples of a given type of historic bridge or may not be as suitable candidates for preservation.

All proposed work is assumed to be located within the roadway or adjacent to the roadway and the bridge. The proposed scope of work is to rehabilitate the bridge. A more detailed scope of work will be provided in the Historic Bridge Alternatives Analysis (HBAA) document.

No right-of-way (ROW) acquisition is occurring as the project takes place within an IDNR property.

The road will remain closed during construction, which is anticipated to be less than 120 days. As the bridge is currently closed and the roadway runs through the Crosley Fish and Wildlife Area, no detour signs will be placed along the roadway.

Section 106 of the National Historic Preservation Act requires federal agencies to take into account the effects of their undertakings on historic properties. In accordance with 36 CFR 800.2 (c), you are hereby requested to be a consulting party to participate in the Section 106 process. Entities that have been invited to participate in the Section 106 consultation process for this project are identified in the attached list. Per 36 CFR 800.3(f), we hereby request that the Indiana State Historic Preservation Officer (SHPO) notify this office if SHPO staff is aware of any other parties that may be entitled to be consulting parties or should be contacted as potential consulting parties for the project.

The Section 106 process involves efforts to identify historic properties potentially affected by the undertaking, assess its effects and seek ways to avoid, minimize or mitigate any adverse effects on historic properties. For more information regarding the protection of historic resources, please see the Advisory Council on Historic Preservation's guide: *Protecting Historic Properties: A Citizen's Guide to Section 106 Review* available online at <https://www.achp.gov/sites/default/files/documents/2017-01/CitizenGuide.pdf>.

Per the terms of the "Programmatic Agreement Regarding Management and Preservation of Indiana's Historic Bridges" (Historic Bridges PA), the FHWA-Indiana Division will satisfy its Section 106 responsibilities involving "Select" and "Non-Select" bridges through the Project Development Process (PDP) of the Historic Bridges PA (Stipulation III). Because the bridge (P000-40-07088; NBI No. 60380) is a "Non-Select" bridge, the procedures outlined in Stipulation III.B. of the Historic Bridges PA will be followed to fulfill FHWA's Section 106 responsibilities for the project. (A copy of the Historic Bridges PA can be downloaded here: <http://www.in.gov/indot/2530.htm>).

Please note that per the permanent rule issued by the IDNR effective August 14, 2013 (312 IAC 20-4-11.5), INDOT is requesting that this project be subjected to "dual review"; that is, reviewed by the Division of Historic Preservation and Archaeology simultaneously under 54 U.S.C. 306108 (Section 106) and IC 14-21-1-18 (Indiana Preservation and Archaeology Law dealing with alterations of historic sites and structures requiring a Certificate of Approval). Pursuant to Section 11.5(f) of this rule, at the conclusion of the review process we anticipate the Division Director would issue a letter of clearance exempting this project from obtaining a Certificate of Approval under IC 14-21-1-18. Enclosed with this letter is a detailed list of the consulting parties, including contact information including email addresses, for processing the dual review submission.

The Area of Potential Effects (APE) is the area in which the proposed project may cause alterations in the character or use of historic resources. A historian who meets the Secretary of the Interior's Professional Qualification Standards is conducting a survey of above-ground resources within the APE for potential eligibility for the NRHP. A report of that investigation is forthcoming and will be distributed to the appropriate consulting parties for review at a later date.

With regards to archaeological resources, Andrew Martin, an archaeologist who meets the Secretary of the Interior's Professional Qualification Standards, conducted an archaeological records review of Indiana State Historic Architectural and Archaeological Research Database (SHAARD) records. It is understood that ground disturbances associated with the project as planned appear to be on existing paved areas, previously disturbed soils adjacent to the roadway, or steeply sloping landforms on the side and underneath the bridge. A review of SHAARD records revealed that there are no previously recorded archaeological sites within or adjacent to the current project area.

Given the project scope and setting, there is a low potential for the proposed project to affect archaeological sites eligible for the NRHP. However, state law (Indiana Code 14-21-1-27 and -29) requires that if any prehistoric or historic archaeological artifacts or human remains are uncovered during construction, demolition, or earth moving activities, that the discovery must be reported to the IDNR within two (2) business days.

Please review the information and comment within thirty (30) calendar days of receipt. If you indicate that you do not desire to be a consulting party, or if you do not respond, you will not be included on the list of consulting parties for this

project. If we do not receive your response in the time allotted, the project will proceed consistent with the proposed design and you will not receive further information about the project unless the design changes.

For questions concerning specific project details, you may contact Alyssa Reynolds of Cultural Resource Analysts, Inc. (CRA) at (812) 253-3009 or adreynolds@crai-ky.com. All future responses regarding the proposed project should be forwarded to CRA at the following address:

Alyssa Reynolds
Architectural Historian
Cultural Resource Analysts, Inc. (CRA)
201 NW 4th Street, Suite 204
Evansville, Indiana 47708
adreynolds@crai-ky.com

Tribal Contacts please respond to INDOT's Acting Tribal Liaison, Matt Coon mcoon@indot.in.gov; (317-697-9752) with any responses pertaining to this project including to provide INDOT/Indiana FHWA additional information about Tribal resources/concerns and questions/comments regarding cultural resources. The FHWA point of contact is Kari Carmany-George at K.CarmanyGeorge@dot.gov (317-226-5629).

Sincerely,



Matthew S. Coon, Manager
Cultural Resources Office
Environmental Services

Enclosures: Consulting Party Distribution List, USGS Topographic map, Aerial View Map.

Consulting Party Early Coordination

Automatic Section 106 Consulting Parties:

**Indiana Department of Natural Resources,
Division of Historic Preservation & Archaeology,
Indiana State Historic Preservation Office
(SHPO)**

402 West Washington Street, Room W274
Indianapolis, Indiana 46204

Invited Consulting Parties:

**Indiana Department of Natural Resources
(IDNR)**

Jomary Baller, Project Manager
{jballer@dnr.IN.gov}

Ben Clark, Chief of Cultural Resources of State
Parks
{BClark@dnr.IN.gov}

Lucas Green, South Region Manager
{lgreen@dnr.in.gov}

David Nance, Professional Geologist
317.234.591
{dnance@dnr.in.gov}

Department of Natural Resources
402 West Washington Street
Indianapolis, Indiana 46204
317.234.8731

Crosley Fish and Wildlife Area
Chad Springer Assistant Property Manager
{cspringer@dnr.in.gov}

2010 IN-3
North Vernon, Indiana 47265
812.346.5596

HistoricBridges.org
Nathan Holth
{nathan@historicbridges.org}

Historic Hoosier Bridges
Tony Dillon
{spansaver@hotmail.com}

Historic Spans Task Force

Pal Brandenburg, Chair
{paul@prbrandy.com}

Indiana Landmarks Southern Regional Office

Greg Sekula, Director
911 State Street
New Albany, Indiana 46202
317.639.4534
{gsekula@indianalandmarks.org}
{lrenwick@indianalandmarks.org}

Jennings County Auditor

Sarah Abel
P.O. Box 383
Vernon, Indiana 47282
812.352.3016
{sabel@jenningscounty-in.gov}

Jennings County Commissioners

Shane Boswell, Matt Sporleder (President), and
Robert Willhite (Vice-President)
P.O. Box 383
Vernon, Indiana 47282
812.873.6120
812.592.5408
812.873.6818
{ssboswell@frontier.com}
{mattsporleder@jcs.org}
{commissionerwhillhite@aol.com}

Jennings County Highway Superintendent

Jim Reeves
P.O. Box 198
North Vernon, Indiana 47265
812.346.2967
{highway@jenningscounty-in.gov}

Jennings County Historical Society

Chris Asher, President
134 East Brown Street
Vernon, Indiana 47265
812.346.8989
{jenningshistoricalsociety@gmail.com}

**Southeastern Indiana Regional Planning
Commission**

Susan Craig, Executive Director
405 West US Highway 50
Versailles, Indiana 47042
812.689.5505
susan.craig@sirpc.org

Delaware Tribe of Indians

Eastern Shawnee Tribe of Oklahoma

Miami Tribe of Oklahoma

Osage Nation

Peoria Tribe of Indians of Oklahoma

Pokagon Band of Potawatomi Indians

Shawnee Tribe

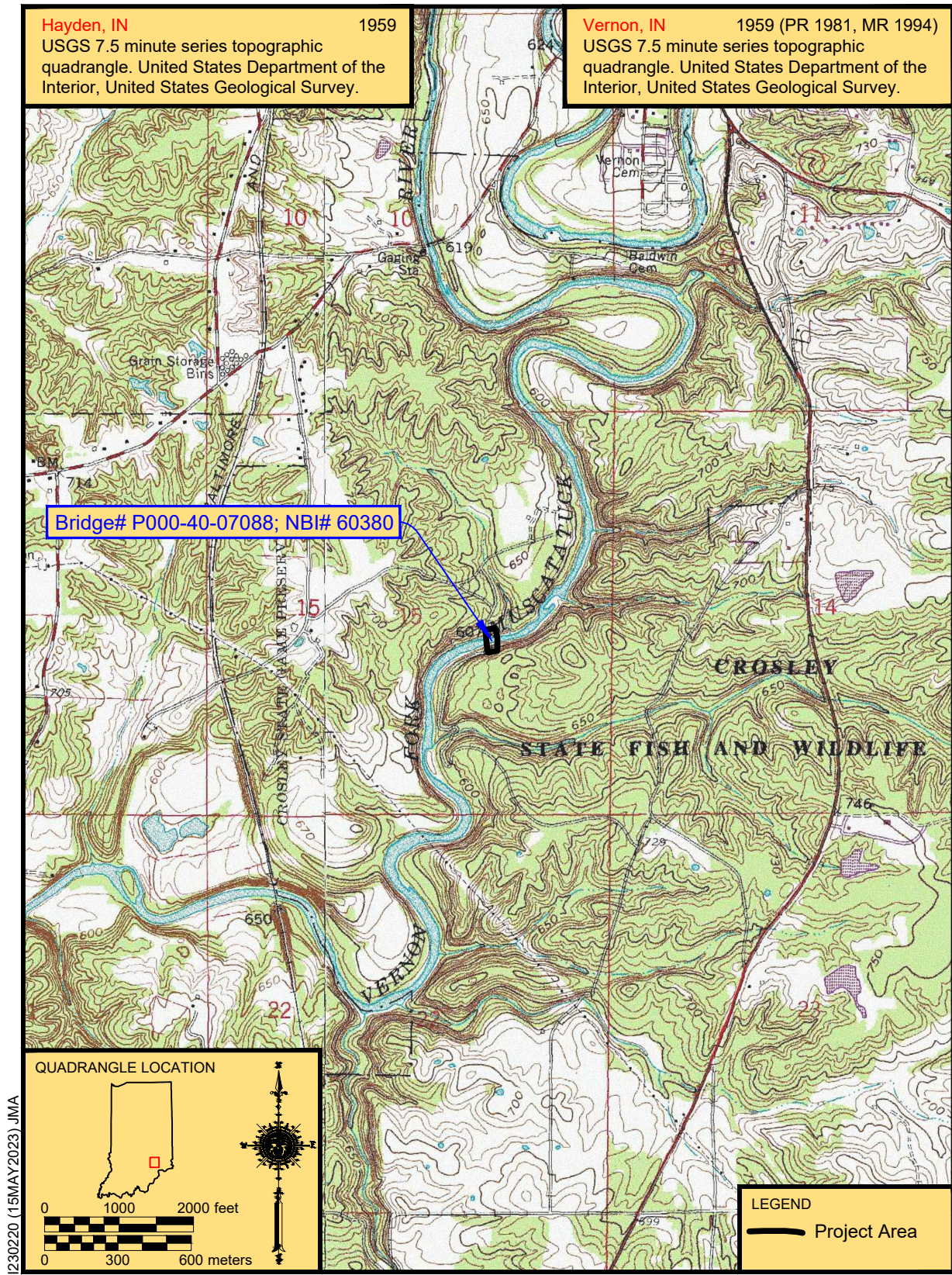


Figure 1. USGS topographic map showing the location of the project area and the structure.

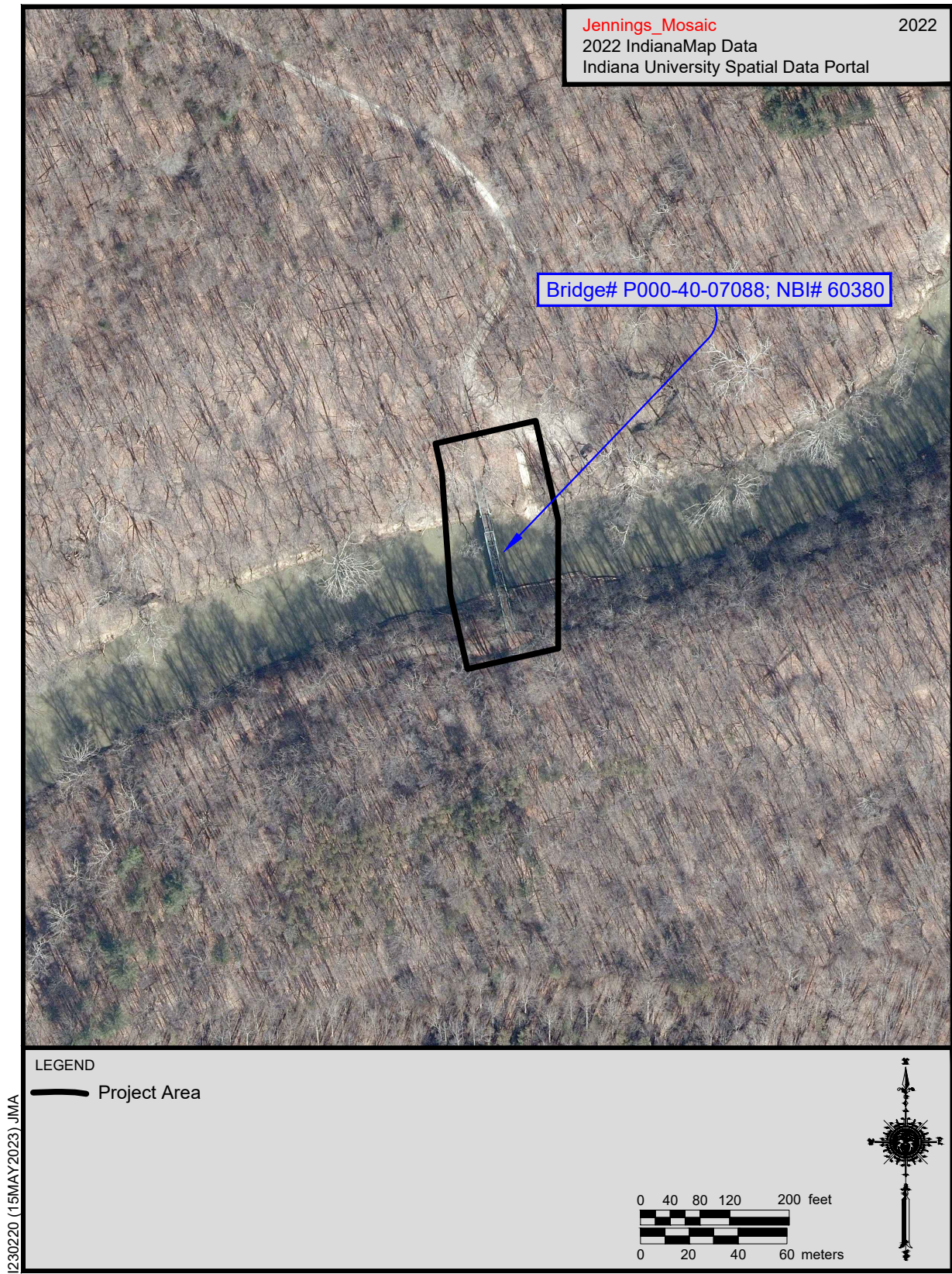


Figure 2. Aerial map showing the location of the project area and the structure.

ECL Response from Indiana Historic SPANs Taskforce

From: [Alyssa Reynolds](#)
To: ["Paul Brandenburg"](#)
Cc: ["Mary Kennedy"](#)
Subject: RE: FHWA Project: Des. No. 2200148; Park Road Bridge Project, ECL & Archaeological Assessment, Jennings County, Indiana
Date: Monday, August 28, 2023 9:03:37 AM
Attachments: [image001.jpg](#)

Paul,

Good morning. Your response has been forwarded to INDOT CRO. Thank you for agreeing to be a consulting party for this project. We look forward to your continued participation as the project continues to progress.

Thank you,

Alyssa Reynolds
Architectural Historian
adreynolds@crai-ky.com

Indiana Office
201 NW 4th Street, Suite 204
Evansville, Indiana 47708
812.253.3009 office
812.253.3010 fax
812.549.4503 cell
<http://www.crai-ky.com>

Logo



Celebrating 40 Years in Business!

From: Paul Brandenburg <paul@prbrandy.com>
Sent: Friday, August 25, 2023 3:04 PM
To: Alyssa Reynolds <adreynolds@crai-ky.com>
Cc: Mary Kennedy <mkennedy@indot.state.in.us>
Subject: RE: FHWA Project: Des. No. 2200148; Park Road Bridge Project, ECL & Archaeological Assessment, Jennings County, Indiana

Alyssa – Indiana Historic SPANs Taskforce supports the stated goal of: “The proposed scope of work is to rehabilitate the bridge” as identified in the Early Coordination Letter and wish to remain a consulting party to review the detailed rehabilitation plans.

Cheers,

Paul

From: Alyssa Reynolds <adreynolds@crai-ky.com>
Sent: Friday, August 25, 2023 9:13 AM
To: jballer@dnr.IN.gov; BClark@dnr.IN.gov; lgreen@dnr.in.gov; dnance@dnr.in.gov; cspringer@dnr.in.gov; nathan@historicbridges.org; spansaver@hotmail.com; Paul Brandenburg <paul@prbrandy.com>; gsekula@indianalandmarks.org; LRenwick@indianalandmarks.org; sabel@jenningscounty-in.gov; ssboswell@frontier.com; mattsporleder@jcs.org; commissionerwhillhite@aol.com; highway@jenningscounty-in.gov; jenningshistoricalsociety@gmail.com; susan.craig@sirpc.org
Cc: 'Kennedy, Mary' <MKENNEDY@indot.IN.gov>; 'Branigin, Susan' <SBranigin@indot.IN.gov>; 'Coon, Matthew' <mcoon@indot.IN.gov>; Andrew Martin <amartin@crai-ky.com>; 'Elizabeth Gallow' <eagallow@crai-ky.com>; 'Al Wessling' <awessling@b-l-n.com>; 'Brian Shaw' <bshaw@b-l-n.com>; 'Celine Finney' <cfinney@b-l-n.com>; 'Walker, Matthew C' <MatWalker@indot.IN.gov>; 'Dye, David' <DDYE@indot.IN.gov>
Subject: FHWA Project: Des. No. 2200148; Park Road Bridge Project, ECL & Archaeological Assessment, Jennings County, Indiana

All,

Des. No. 2200148

Project Description: Bridge Project, Bridge No. P000-40-07088 (NBI No. 60380)

Location: Park Road over Muscatatuck River, Crosley Fish and Wildlife Area, Jennings County, Indiana

The Indiana Department of Natural Resources (IDNR) and the Indiana Department of Transportation, with funding from the Federal Highway Administration, propose to proceed with the Park Road bridge project (Des. No. 2200148) in the Crosley Fish and Wildlife Area, Jennings County, Indiana.

Section 106 of the National Historic Preservation Act requires federal agencies to take into account the effects of their undertakings on historic properties. The following agencies/individuals are being invited to become consulting parties:

Indiana State Historic Preservation Officer (SHPO)
Crosley Fish and Wildlife Area
HistoricBridges.org
Historic Hoosier Bridges
Indiana Department of Natural Resources (IDNR)
Indiana Landmarks-Southern Regional Office
Jennings County Auditor
Jennings County Commissioners
Jennings County Highway Superintendent
Jennings County Historical Society

Southern Indiana Regional Planning Commission
Delaware Tribe of Indians
Eastern Shawnee Tribe of Oklahoma
Miami Tribe of Oklahoma
Osage Nation
Peoria Tribe of Indians of Oklahoma
Pokagon Band of Potawatomi Indians
Shawnee Tribe

This letter is part of the early coordination phase of the environmental review process requesting comments associated with this project. We are requesting comments from your area of expertise regarding any possible environmental effects associated with this project. Please use the above Des. No. and project description in your reply and your comments will be incorporated into the formal environmental study.

Please review the attached letter, which contains an archaeological assessment and is also in IN SCOPE at <https://erms12c.indot.in.gov/Section106Documents/> (the Des. No. is the most efficient search term, once in IN SCOPE), and respond with your comments on any historic resource impacts incurred as a result of this project so that an environmental report can be completed. We also welcome your related opinions and other input to be considered in the preparation of the environmental document. If a hard copy of the materials is needed, please respond to this email with your request within seven (7) days.

Consulting parties have thirty (30) calendar days from receipt of this information to review and provide comment. If we do not receive a response from an invited consulting party within the time allotted, the project will proceed consistent with the proposed design. Therefore, if we do not receive a response within thirty (30) days, your agency or organization will not receive any further information on the project unless the scope of work changes.

Tribal Contacts please respond to INDOT's Acting Tribal Liaison, Matt Coon at mcoon@indot.in.gov (317-697-9752) with any responses pertaining to this project including providing INDOT/Indiana FHWA additional information about Tribal resources/concerns and questions/comments regarding cultural resources. The FHWA point of contact is Kari Carmany-George at K.CarmanyGeorge@dot.gov (317-226-5629).

Thank you in advance for your input,

Alyssa Reynolds
Architectural Historian
adreynolds@crai-ky.com

Indiana Office
201 NW 4th Street, Suite 204
Evansville, Indiana 47708
812.253.3009 office
812.253.3010 fax

From: [Sarah Abel](#)
To: [Kennedy, Mary](#); [Nathan Holth](#)
Cc: [Branigin, Susan](#); [Coon, Matthew](#); [Andrew Martin](#); ["Elizabeth Gallow"](#); [Alfred Wessling](#); [Brian Shaw](#); [Celine Finney](#); [Walker, Matthew C](#); [Dye, David](#); [Alyssa Reynolds](#); [Baller, Jomary](#); [Clark, Benjamin J.](#); [Green, Lucas](#); [Nance, David](#); [Springer, Chad A](#); [spansaver@hotmail.com](#); [paul@prbrandy.com](#); [gsekula@indianalandmarks.org](#); [LRenwick@indianalandmarks.org](#); [Shane Boswell](#); [mattsporleder@jcsc.org](#); [commissionerwhillhite@aol.com](#); [Jennings County Highway Department](#); [jenningshistoricalsociety@gmail.com](#); [susan.craig@sirpc.org](#)
Subject: RE: FHWA Project: Des. No. 2200148; Park Road Bridge Project, ECL & Archaeological Assessment, Jennings County, Indiana
Date: Tuesday, August 29, 2023 9:48:18 AM
Attachments: [image014.jpg](#)
[image016.jpg](#)
[image001.jpg](#)
[image002.jpg](#)
[image003.jpg](#)
[image004.png](#)
[image005.jpg](#)
[image006.png](#)

Mary,
Thank you for the clarification.

From: Kennedy, Mary <MKENNEDY@indot.IN.gov>
Sent: Monday, August 28, 2023 1:21 PM
To: Nathan Holth <nathan@historicbridges.org>
Cc: Branigin, Susan <SBranigin@indot.IN.gov>; Coon, Matthew <mcoon@indot.IN.gov>; Andrew Martin <amartin@crai-ky.com>; 'Elizabeth Gallow' <eagallow@crai-ky.com>; Al Wessling <awessling@b-l-n.com>; 'Brian Shaw' <bshaw@b-l-n.com>; 'Celine Finney' <cfinney@b-l-n.com>; Walker, Matthew C <MatWalker@indot.IN.gov>; Dye, David <DDYE@indot.IN.gov>; Alyssa Reynolds <adreynolds@crai-ky.com>; Baller, Jomary <jballer@dnr.IN.gov>; Clark, Benjamin J. <BClark@dnr.IN.gov>; Green, Lucas <LGreen@dnr.IN.gov>; Nance, David <dnance@dnr.IN.gov>; Springer, Chad A <CSpringer@dnr.IN.gov>; spansaver@hotmail.com; paul@prbrandy.com; gsekula@indianalandmarks.org; LRenwick@indianalandmarks.org; Sarah Abel <SAbel@jenningscounty-in.gov>; Shane Boswell <ssboswell@frontier.com>; mattsporleder@jcsc.org; commissionerwhillhite@aol.com; Jennings County Highway Department <Highway@jenningscounty-in.gov>; jenningshistoricalsociety@gmail.com; susan.craig@sirpc.org
Subject: RE: FHWA Project: Des. No. 2200148; Park Road Bridge Project, ECL & Archaeological Assessment, Jennings County, Indiana

Hi Nathan,

Thank you for the information. We will add you to the list of consulting parties for this project. With regard to the bridge's Non-Select status, that was due to the bridge's low structural capacity and narrow width. The way the analysis fell out with those two items put the bridge on the Non-Select list. This does not mean that the bridge cannot be preserved. In fact, the plan is to preserve it. Given its location in the fish & wildlife area, those limitations are not as big a deal as they would be on a more traveled public roadway. With regard to potential interpretive signage, there is nothing in the Historic Bridges Programmatic Agreement that requires it. That would be up to DNR if they would want to pursue interpretive signage.

We look forward to your continued participation as the project progresses.

Regards,

Mary E. Kennedy

Historic Bridge Specialist

Indiana Department of Transportation

100 North Senate Ave., N758 — Environmental Services

Indianapolis, IN 46204

Office/Cell: 317-694-3607

Email: mkennedy@indot.in.gov



From: Nathan Holth <nathan@historicbridges.org>

Sent: Friday, August 25, 2023 3:20 PM

To: Alyssa Reynolds <adreyolds@crai-ky.com>; Baller, Jomary <jballer@dnr.IN.gov>; Clark, Benjamin J. <BClark@dnr.IN.gov>; Green, Lucas <LGreen@dnr.IN.gov>; Nance, David <dnance@dnr.IN.gov>; Springer, Chad A <CSpringer@dnr.IN.gov>; spansaver@hotmail.com; paul@prbrandy.com; gsekula@indianalandmarks.org; LRenwick@indianalandmarks.org; sabel@jenningscounty-in.gov; ssboswell@frontier.com; mattsporleder@jcs.org; commissionerwhillhite@aol.com; highway@jenningscounty-in.gov; jenningshistoricalsociety@gmail.com; susan.craig@sirpc.org

Cc: Kennedy, Mary <MKENNEDY@indot.IN.gov>; Branigin, Susan <SBranigin@indot.IN.gov>; Coon, Matthew <mcoon@indot.IN.gov>; Andrew Martin <amartin@crai-ky.com>; 'Elizabeth Gallow' <eagallow@crai-ky.com>; Al Wessling <awessling@b-l-n.com>; 'Brian Shaw' <bshaw@b-l-n.com>; 'Celine Finney' <cfinney@b-l-n.com>; Walker, Matthew C <MatWalker@indot.IN.gov>; Dye, David <DDYE@indot.IN.gov>

Subject: Re: FHWA Project: Des. No. 2200148; Park Road Bridge Project, ECL & Archaeological Assessment, Jennings County, Indiana

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I would like to express strong support for the preservation of this bridge. I am baffled that the bridge is considered "Non-Select" as this bridge is an unusually narrow highway bridge and that speaks to the bridge's original owner... which as I understand accommodated a retreat for famous businessman and inventor Powel Crosley Jr. who is noted in America's automotive history as an early promotor of compact cars. My understanding is that this bridge was specifically built to Crosely's specifications with a narrow deck as Crosley had no need of a wider bridge to accomodate his compact cars. It is therefore significant beyond its engineering design, but as a remarkable and unique surviving bridge that embodies this famous/noted man's interest in compact cars and is a tangible remnant of this retreat which is indeed noted as "Crosley Fish and Wildlife Area" and in my view is a

historic centerpiece to the area.

When I visited this bridge, I was not fully aware of this history. It would be nice to see interpretive signage added as part of this project describing Crosley and the bridge to make its significance clear to visitors.

Thanks,
-Nathan Holth

=====

Nathan Holth

Author/ Photographer/Webmaster

-----HistoricBridges.org-----

"Promoting the Preservation Of Our Transportation Heritage"

nathan@historicbridges.org

www.historicbridges.org

=====

Disclaimer: HistoricBridges.org is a volunteer group of private citizens. HistoricBridges.org is NOT a government agency, does not represent or work with any governmental agencies, nor is it in any way associated with any government agency or any non-profit organization. While we strive for accuracy in our factual content, HistoricBridges.org offers no guarantee of accuracy. Opinions and commentary are the opinions of the respective HistoricBridges.org member who made them and do not necessarily represent the views of anyone else. HistoricBridges.org does not bear any responsibility for any consequences resulting from the use of this communication or any other HistoricBridges.org information. Owners and users of bridges have the responsibility of correctly following all applicable laws, rules, and regulations, regardless of any HistoricBridges.org communications or information.

ECL Response from Historic Hoosier Bridges

From: [Kennedy, Mary](#)
To: [Tony Dillon](#)
Cc: [Nathan Holth](#); [Alyssa Reynolds](#); [Springer, Chad A](#); [Clark, Benjamin J.](#); [Baller, Jomary](#); [Nance, David](#); [Walker, Matthew C](#)
Subject: RE: FHWA Project: Des. No. 2200148; Park Road Bridge Project, ECL & Archaeological Assessment, Jennings County, Indiana
Date: Tuesday, August 29, 2023 8:56:39 AM
Attachments: [image002.jpg](#)
[image003.jpg](#)
[image004.jpg](#)
[image005.png](#)
[image006.jpg](#)
[image007.jpg](#)
[image008.png](#)
[image009.jpg](#)
[image010.jpg](#)

Hi Tony,

Thank you for agreeing to be a consulting party for this project. I've copied the consultant as well as DNR staff, so they can be aware of the additional vote for an interpretive sign. We look forward to your continued participation as the project progresses.

Regards,

Mary E. Kennedy

Historic Bridge Specialist

Indiana Department of Transportation
100 North Senate Ave., N758 — Environmental Services
Indianapolis, IN 46204

Office/Cell: 317-694-3607

Email: mkennedy@indot.in.gov



From: Tony Dillon <spansaver@hotmail.com>
Sent: Monday, August 28, 2023 11:14 PM
To: Kennedy, Mary <MKENNEDY@indot.IN.gov>
Cc: Nathan Holth <nathan@historicbridges.org>
Subject: Re: FHWA Project: Des. No. 2200148; Park Road Bridge Project, ECL & Archaeological Assessment, Jennings County, Indiana

**** This is an EXTERNAL email. Exercise caution. DO NOT open attachments or click links from unknown senders or unexpected email. ****

Mary,

Count me in on any consulting that may arise concerning the Crosley Bridge.

While vetting the Mead & Hunt determinations for Select/Non-Select bridges, Jim Cooper and I had a discussion about this one. While my initial reaction was to lobby for a reclassification, Jim was concerned with a possible backlash given the narrow width. We ultimately decided that it might be best to just let it lay low. I agree with Nathan wholeheartedly about the importance of this span and its preservation. Fortunately, that sentiment is shared by the park staff and preservationists in the area. There is nothing to suggest that anyone wants it gone... Although I'm guessing that it's been cursed at on many occasions, given all the pieces of side-view mirrors I've seen during my visits.

I would certainly help lobby for an historic marker once the bridge is rehabilitated.

Tony

Tony Dillon
Historic Hoosier Bridges
208 North 17th Street
New Castle, IN 47362
(765)624-6558
spansaver@hotmail.com

ECL Response from Indiana Landmarks Southern Regional Office

From: [Alyssa Reynolds](#)
To: ["Laura Renwick"](#)
Cc: ["Kennedy, Mary"](#); [Alfred Wessling](#); [Kristin Wing](#); [Brian Shaw](#); [Celine Finney](#)
Subject: RE: FHWA Project: Des. No. 2200148; Park Road Bridge Project, ECL & Archaeological Assessment, Jennings County, Indiana
Date: Monday, September 11, 2023 9:23:38 AM
Attachments: [image001.jpg](#)

CAUTION: This email originated from outside your organization. Exercise caution when opening attachments or clicking links, especially from unknown senders.

Laura,

Good morning. Thank you for your interest in being a consulting party for this project. Your response has been forwarded to INDOT CRO. Thank you for noting that your address and phone were also listed incorrectly. In future correspondence, this will be changed accordingly.

Thank you,

Alyssa Reynolds
Architectural Historian
adreynolds@crai-ky.com

Indiana Office
201 NW 4th Street, Suite 204
Evansville, Indiana 47708
812.253.3009 office
812.253.3010 fax
812.549.4503 cell
<http://www.crai-ky.com>



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From: Laura Renwick <LRenwick@indianalandmarks.org>
Sent: Wednesday, September 6, 2023 12:21 PM
To: Alyssa Reynolds <adreynolds@crai-ky.com>
Subject: RE: FHWA Project: Des. No. 2200148; Park Road Bridge Project, ECL & Archaeological Assessment, Jennings County, Indiana

Thank you for your email and the preliminary info on this project. The Southern Regional Office of

Indiana Landmarks would like to be included as a consulting party as planning proceeds. Please note that the ZIP code and phone number for our office that is included in the list of invited parties is incorrect, and should be:

911 State Street
New Albany, IN 47150
Phone 812/284-4534

Thank you,
Laura Renwick

.....
Laura Renwick
Community Preservation Specialist

.....
Indiana Landmarks
Southern Regional Office
911 State Street
New Albany, IN 47150
Ph. 812-284-4534, 800-450-4534
www.indianalandmarks.org

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Miami Tribe of Oklahoma

3410 P St. NW, Miami, OK 74354 • P.O. Box 1326, Miami, OK 74355
Ph: (918) 541-1300 • Fax: (918) 542-7260
www.miamination.com



Via email: mcoon@indot.in.gov

September 11, 2023

Matt Coon, Tribal Liaison
INDOT, Cultural Resources Office
100 North Senate Avenue, N758-ES
Indianapolis, Indiana 46204

Re: Des. No. 2200148, Bridge Project, Bridge No. P000-40-07088 (NBI No. 60380), Jennings County, Indiana – Comments of the Miami Tribe of Oklahoma

Dear Mr. Coon:

Aya, kweehsitoolaani– I show you respect. The Miami Tribe of Oklahoma, a federally recognized Indian tribe with a Constitution ratified in 1939 under the Oklahoma Indian Welfare Act of 1936, respectfully submits the following comments regarding Des. No. 2200148, Bridge Project, Bridge No. P000-40-07088 (NBI No. 60380) in Jennings County, Indiana.

The Miami Tribe offers no objection to the above-referenced project at this time, as we are not currently aware of existing documentation directly linking a specific Miami cultural or historic site to the project site. However, given the Miami Tribe's deep and enduring relationship to its historic lands and cultural property within present-day Indiana, if any human remains or Native American cultural items falling under the Native American Graves Protection and Repatriation Act (NAGPRA) or archaeological evidence is discovered during any phase of this project, the Miami Tribe requests immediate consultation with the entity of jurisdiction for the location of discovery. In such a case, please contact me at 918-541-7885 or by email at THPO@miamination.com to initiate consultation.

The Miami Tribe accepts the invitation to serve as a consulting party to the proposed project. In my capacity as Tribal Historic Preservation Officer I am the point of contact for consultation.

Respectfully,

Logan York

Logan York
Tribal Historic Preservation Officer
Miami Tribe of Oklahoma

Division of Historic Preservation & Archaeology · 402 W. Washington Street, W274 · Indianapolis, IN 46204-2739
Phone 317-232-1646 · Fax 317-232-0693 · dhpa@dnr.IN.gov ·



September 25, 2023

Alyssa Reynolds
Architectural Historian
Cultural Resource Analysts, Inc. (CRAI)
201 NW 4th Street, Suite 204
Evansville, Indiana 47708

State Agency: Indiana Department of Transportation (“INDOT”),
Federal Agency: Federal Highway Administration, Indiana Division (“FHWA”)

Re: DUAL REVIEW: Early coordination letter, and archaeological assessment, for the Park Road bridge (Jennings County Bridge No. P000-40-07088; National Bridge Inventory “NBI” No. 60380) over Muscatatuck River Project (Des. No. 2200148; DHPA No. 31257)

Dear Ms. Reynolds:

The Indiana Department of Natural Resources, Division of Historic Preservation and Archaeology (“DNR-DHPA”), which also serves as the staff of the Indiana State Historic Preservation Officer (“Indiana SHPO”), is in receipt of INDOT’s early coordination letter, dated August 25, 2023, transmitting your proposal for a dual review, pursuant to 312 Indiana Administrative Code (“IAC”) 20-4-11.5, of the aforementioned project in Vernon Township, Jennings County, Indiana. We received this submission August 25, 2023.

The Indiana SHPO/DNR-DHPA will review the information submitted under Section 106 of the National Historic Preservation Act of 1966, as amended (54 U.S.C. § 306108), implementing regulations at 36 C.F.R. Part 800, as well as Indiana Code 14-21-1-18 and 312 IAC 20-4. By copy of this letter, DNR-DHPA is providing notification of the commencement of the dual review to interested persons and members of the Indiana Historic Preservation Review Board (“Review Board”). Notice of the commencement will also be posted on the division’s website (<https://www.in.gov/dnr/historic-preservation/help-for-professionals/check-project-status/>).

For the purposes of Indiana Code 14-21-1-18 and 312 IAC 20-4, we have added the members of the Review Board and additional, potentially interested parties to the list of parties we intend to copy with our comment letters. Anyone receiving an e-mailed copy of this letter who does not wish to receive future copies of our correspondence about this project is asked to reply by e-mail to tgiffin@dnr.in.gov and advise us that he or she does not wish to receive any further copies of our e-mails on this project.

In your next regular submission, please include which consulting parties agree to participate in the consultation of this dual review.

We see in INDOT’s August 25, 2023, letter that FHWA will satisfy its Section 106 responsibilities involving “Select” and “Non-Select” bridges through the Project Development Process of the “Programmatic Agreement Regarding Management and Preservation of Indiana’s Historic Bridges.” We note within the *Indiana Historic Bridge Inventory*, the Park Road bridge (Jennings County Bridge No. P000-40-07088; National Bridge Inventory “NBI” No. 60380) over Muscatatuck River is a three-

span, truss bridge constructed in 1910 and is listed as eligible for listing in the National Register of Historic Places and classified as a "Non-Select" bridge.

As INDOT's August 25, 2023, letter indicates, additional information regarding above-ground historic resources will be forthcoming. Once the indicated information is received, the Indiana SHPO will resume identification and evaluation procedures for this project. Please keep in mind that additional information may be requested in the future.

Additionally, in regard to archaeological resources, based upon the documentation available to the staff of the Indiana SHPO, no currently known archaeological resources eligible for inclusion in the National Register of Historic Places ("NRHP") have been recorded within the proposed project area. No archaeological investigations appear necessary provided that all project activities remain within areas disturbed by previous construction. Please be advised that archaeological resources may exist underneath modern development.

If any prehistoric or historic archaeological artifacts or human remains are uncovered during construction, demolition, or earthmoving activities, state law (Indiana Code 14-21-1-27 and Indiana Code 14-21-1-29) requires that the discovery must be reported to the State of Indiana Department of Natural Resources, Division of Historic Preservation and Archaeology within two (2) business days. In that event, please call (317) 232-1646. Be advised that adherence to Indiana Code 14-21-1-27 and Indiana Code 14-21-1-29 does not obviate the need to adhere to applicable federal statutes and regulations, including but not limited to 36 C.F.R. 800.

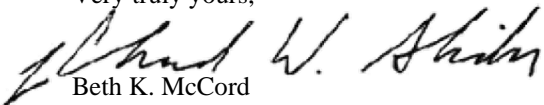
If ground disturbing activities will be within 100 feet of any cemetery, please be aware of the cemetery development plan requirements in Indiana Code 14-21-1-26.5 (<http://iga.in.gov/legislative/laws/2016/ic/titles/014/articles/021/chapters/001/#section-26.5>). All cemeteries must be avoided by all project activities, and provisions of relevant state statutes regarding cemeteries (including IC 14-21-1 and IC 23-14) must be adhered to. Please also be aware of Indiana Code 23-14-44-1 and Indiana Code 23-14-44-2, regarding restrictions on roads and utility construction in cemeteries.

For the benefit of those recipients of a copy of this letter who are not Section 106 consulting parties, please be aware that a copy of INDOT's August 25, 2023, letter can be found online at <https://erms12c.indot.in.gov/Section106Documents/>. From there, search by this project's designation number: 2200148.

If you have questions regarding our dual review of the aforementioned project, please contact DNR-DHPA. Questions about archaeological issues should be directed to contact at Wade T. Tharp at (317) 232-1650 or wtharp1@dnr.in.gov. Questions about historic buildings or structures pertaining to this review should be directed to Toni Lynn Giffin at (317) 233-2803 or tgiffin@dnr.IN.gov.

In all future correspondence regarding the dual review of this Park Road bridge over Muscatatuck River project in Vernon Township, Jennings County, Indiana (Des. No. 2200148), please refer to DHPA No. 31257.

Very truly yours,



Beth K. McCord
Deputy State Historic Preservation Officer
Director, Division of Historic Preservation and Archaeology

BKM:TLG:WTT:wt

EMC: Patrick Carpenter, Federal Highway Administration
Matt Coon, Ph.D., Indiana Department of Transportation
Susan Branigin, Indiana Department of Transportation
Alyssa Reynolds, Cultural Resource Analysts, Inc.
Toni Lynn Giffin, Indiana DNR-DHPA
Wade T. Tharp, Indiana DNR-DHPA

EMC to Indiana Historic Preservation Review Board Members:
J. Scott Keller, Indiana Historic Preservation Review Board
Daniel Kloc, AIA, Indiana Historic Preservation Review Board
Chandler Lighty, Indiana Historic Preservation Review Board
Anne Shaw, Indiana Historic Preservation Review Board
April Sievert, Ph.D., Indiana Historic Preservation Review Board

Jason Larrison, AIA, Indiana Historic Preservation Review Board
Ryan Mueller, Deputy Director, Indiana Department of Natural Resources
Beth McCord, Division of Historic Preservation and Archaeology

emc: potentially interested parties

Jomary Baller, Project Manager, Indiana Department of Natural Resources
Ben Clark, Chief of Cultural Resources of State Parks, Indiana Department of Natural Resources
Lucas Green, South Region Manager, Indiana Department of Natural Resources
David Nance, Professional Geologist, Indiana Department of Natural Resources
Chad Springer Assistant Property Manager, Indiana Department of Natural Resources, Crosley Fish and Wildlife Area
Nathan Holth, HistoricBridges.org
Tony Dillon, Historic Hoosier Bridges
Pal Brandenburg, Chair, Historic Spans Task Force
Greg Sekula, Director, Indiana Landmarks Southern Regional Office
Sarah Abel, Jennings County Auditor
Jennings County Commissioners
Jim Reeves, Jennings County Highway Superintendent
Chris Asher, President Jennings County Historical Society
Susan Craig, Executive Director - Southeastern Indiana Regional Planning Commission



EASTERN SHAWNEE
CULTURAL PRESERVATION DEPARTMENT
70500 East 128 Road, Wyandotte, OK 74370

October 12, 2023

INDOT - Indiana Department of Transportation
100 N. Senate Ave. IGCN642
Indianapolis, IN 46201

RE: Des No. 2200148, Jennings County, Indiana

Dear Mr. Coon,

The Eastern Shawnee Tribe has received your letter regarding the above referenced project(s) within Jennings County, Indiana. The Eastern Shawnee Tribe is committed to protecting sites important to Tribal Heritage, Culture and Religion. Furthermore, the Tribe is particularly concerned with historical sites that may contain but not limited to the burial(s) of human remains and associated funerary objects.

As described in your correspondence, and upon research of our database(s) and files, we find our people occupied these areas historically and/or prehistorically. However, the project proposes **NO Adverse Effect** or endangerment to known sites of interest to the Eastern Shawnee Tribe. Please continue project as planned. However, should this project inadvertently discover an archeological site or object(s) we request that you immediately contact the Eastern Shawnee Tribe, as well as the appropriate state agencies (within 24 hours). We also ask that all ground disturbing activity stop until the Tribe and State agencies are consulted. Please note that any future changes to this project will require additional consultation.

In accordance with the NHPA of 1966 (16 U.S.C. § 470-470w-6), federally funded, licensed, or permitted undertakings that are subject to the Section 106 review process must determine effects to significant historic properties. As clarified in Section 101(d)(6)(A-B), historic properties may have religious and/or cultural significance to Indian Tribes. Section 106 of NHPA requires Federal agencies to consider the effects of their actions on all significant historic properties (36 CFR Part 800) as does the National Environmental Policy Act of 1969 (43 U.S.C. § 4321-4347 and 40 CFR § 1501.7(a)). This letter evidences NHPA and NEPA historic properties compliance pertaining to consultation with this Tribe regarding the referenced proposed projects.

Thank you, for contacting the Eastern Shawnee Tribe, we appreciate your cooperation. Should you have any further questions or comments please contact our Office.

Sincerely,

Paul Barton, Tribal Historic Preservation Officer (THPO)
Eastern Shawnee Tribe of Oklahoma
(918) 666-5151 Ext:1833
THPO@estoo.net



INDIANA DEPARTMENT OF TRANSPORTATION

100 North Senate Avenue
Room N758-ES
Indianapolis, Indiana 46204

PHONE: (855) 463-6848

Eric Holcomb, Governor
Michael Smith, Commissioner

February 28, 2024

This letter was sent to the listed parties.

RE: Dual Review FHWA Project: Des. No. 2200148; DHPA No. 31257; Park Road Bridge Project, Jennings County, Indiana

Dear Consulting Party (see attached list),

The Indiana Department of Natural Resources (IDNR) and the Indiana Department of Transportation (INDOT), with funding from the Federal Highway Administration (FHWA), propose to proceed with the Park Road bridge project (Des. No. 2200148).

This letter is part of the Section 106 review process for this project. Section 106 of the National Historic Preservation Act (NRHP) requires federal agencies to take into account the effects of their undertakings on historic properties. We are requesting comments from you regarding the possible effects of this project. Please use the above Des. Number and project description in your reply and your comments will be incorporated into the formal environmental study.

A Section 106 early coordination letter (ECL) was distributed on August 25, 2023.

The proposed bridge undertaking is located within the Crosley Fish and Wildlife Area, established in 1931, in Jennings County, Indiana. More specifically the project area is within Vernon Township as shown on the Vernon, Indiana USGS Topographic Quadrangle, in Section 15, Township 6 N, Range 8 E. The project area along Park Road encompasses approximately 1.0 acre within the Crosley Fish and Wildlife Area. The project area can be viewed online at <https://arcg.is/jqueP> (the Des. No. is the most efficient search term once in the CRO - Public Web Map App).

The proposed work will occur on the Park Road bridge (P000-40-07088 B; National Bridge Inventory [NBI] Number [No.] 060380) over Muscatatuck River. The existing bridge is an approximately 212 feet by 12.3 feet, three-span, truss bridge constructed in 1910. The main span of the bridge is a steel Pratt-through truss and the approach spans are both steel Warren pony trusses. The bridge was rehabilitated in 2004 when the deck was replaced with new wood planks and the truss was cleaned and painted (Des. No. 0300801). The gusset plates at Abutment Number (No.) 1 were replaced (Des. No. 1400643) in 2015 along with a portion of the south span deck. The bridge was closed in 2019 due to holes and deterioration in the timber deck. According to the *Indiana Historic Bridge Inventory*, completed in 2010, the bridge was determined eligible for listing in the National Register of Historic Places (NRHP) under Criterion C. It was also determined to be a "Non-Select" bridge, which are those that may not be considered excellent examples of a given type of historic bridge or may not be as suitable candidates for preservation.

All proposed work is assumed to be located within the roadway or adjacent to the roadway and the bridge. The proposed scope of work is to rehabilitate the bridge.

The proposed work will include:

- Cleaning and painting the truss. The paint color will match the existing color and the bridge rail will be painted to match the truss.
- Replacing the bridge deck in all spans with a timber plank deck that has timber running boards.

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- Repairing or replacing in kind some verticals, gusset plates, and low chord truss members or connections.
- Replacing some miscellaneous truss secondary members in kind such as bottom lateral bracing and lattice.
- Deteriorated concrete abutment and pier caps will be removed and reconstructed.
- Delaminated and spalled areas of abutments and piers will be patched.
- Placing scour countermeasures at all substructure units.
- Rivets removed to make necessary repairs will be replaced with round headed bolts wherever possible. The rounded head will be installed on the face of the member that is most visible from the roadway or exterior face of member.
- Replacing the bridge rail with a steel hand rail.
- Some trees may need to be removed to install the scour countermeasures. All other nearby trees will be trimmed to keep branches away from truss and vehicles.
- Clean the abutment and pier bridge seats of all debris.

The clear roadway width will be slightly larger due to the installation of the new steel handrail on the bridge. The approach guardrail will not be modified. No approach roadway work will be completed as part of this project, except that portions of the south approach guardrail will be removed and reset if necessary to complete the scour countermeasure work. The scour countermeasure will consist of:

- Placing either dumped, partially grouted revetment riprap or an articulated concrete mattress on the south spillslope;
- Placing Class 1 riprap at a 20 feet width around the north abutment and wings, a concrete underpin will be placed around Pier 2 (south pier); and
- Placing dumped partially grouted Class 2 riprap around three (3) sides (channelside and each pier nose) of Pier 3 with regular buried Class 2 riprap on the north side of Pier 3.

No right-of-way (ROW) acquisition is occurring as the project takes place within an IDNR property.

The road will remain closed during construction, which is anticipated to be less than 120 days. As the bridge is currently closed and the roadway runs through the Crosley Fish and Wildlife Area, no detour signs will be placed along the roadway.

The project need is based on the current condition of the structure elements and notable deficiencies in the wearing surface, deck, superstructure, and substructure, and the scour critical status of the bridge. The bridge's deck deterioration caused it to be closed to traffic in 2019. The purpose of this project is to correct the deficiencies in the bridge wearing surface, deck, superstructure, and substructure (scour protection) to reopen the bridge to vehicular traffic, and protect the bridge and extend its service life.

Cultural Resource Analysts, Inc. (CRA) is under contract with INDOT to advance the Section 106 documentation for the referenced project. Beam, Longest & Neff (BLN) Corporation will be completing the environmental documentation for the referenced project.

In accordance with 36 CFR 800.2 (c), you were invited to become a consulting party as part of the Section 106 process, or you are hereby invited to become a consulting party as part of the Section 106 process. Entities that have previously accepted consulting party status – as well as additional entities that are currently being invited to become consulting parties – are identified in the attached list.

Section 106 of the National Historic Preservation Act requires federal agencies to take into account the effects of their undertakings on historic and archaeological properties. In accordance with 36 CFR 800.2 (c), you are hereby requested to be a consulting party to participate in the Section 106 process. Entities that have been invited to participate in the Section 106 consultation process for this project are identified in the attached list. Per 36 CFR 800.3(f), we hereby request that the Indiana State Historic Preservation Officer (SHPO) notify this office if the SHPO staff is aware of any other parties that may be entitled to be consulting parties or should be contacted as potential consulting parties for the project.

The Section 106 process involves efforts to identify historic properties potentially affected by the undertaking, assess its effects and seek ways to avoid, minimize or mitigate any adverse effects on historic properties. For more information regarding the protection of historic resources, please see the Advisory Council on Historic Preservation's guide: *Protecting Historic Properties: A Citizen's Guide to Section 106 Review* available online at <https://www.achp.gov/sites/default/files/documents/2017-01/CitizenGuide.pdf>.

Per the terms of the "Programmatic Agreement Regarding Management and Preservation of Indiana's Historic Bridges" (Historic Bridges PA), the FHWA-Indiana Division will satisfy its Section 106 responsibilities involving "Select" and "Non-Select" bridges through the Project Development Process (PDP) of the Historic Bridges PA (Stipulation III). Because the bridge (P000-40-07088; NBI No. 60380) is a "Non-Select" bridge, the procedures outlined in Stipulation III.B. of the Historic Bridges PA will be followed to fulfill FHWA's Section 106 responsibilities for the project. (A copy of the Historic Bridges PA can be downloaded here: <http://www.in.gov/indot/2530.htm>).

Please note that per the permanent rule issued by the Indiana Department of Natural Resources effective August 14, 2013 (312 IAC 20-4-11.5), INDOT is requesting that this project be subjected to "dual review"; that is, reviewed by the Division of Historic Preservation and Archaeology simultaneously under 54 U.S.C. 306108 (Section 106) and IC 14-21-1-18 (Indiana Preservation and Archaeology Law dealing with alterations of historic sites and structures requiring a Certificate of Approval). Pursuant to Section 11.5(f) of this rule, at the conclusion of the review process we anticipate that the Division Director would issue a letter of clearance exempting this project from obtaining a Certificate of Approval under IC 14-21-1-18.

The Area of Potential Effects (APE) is the area in which the proposed project may cause alterations in the character or use of historic resources. A historian who meets the Secretary of the Interior's Professional Qualification Standards conducted an above-ground survey of resources within the APE for potential eligibility for the National Register of Historic Places (NRHP). The APE contains no resources listed in the National Register of Historic Places (NRHP). One resource was identified, the previously mentioned "Non-Select" and NRHP-eligible bridge (P000-40-07088 B; NBI No. 60380) on Park Road over Muscatatuck River. CRA recommends that the bridge continue to be eligible for listing in the NRHP under Criterion C for its unique architectural and engineering design.

An archaeologist who meets the Secretary of the Interior's Professional Qualification Standards reviewed the proposed project area and prepared an archaeological assessment recommending the project be allowed to proceed without additional archaeological study.

On August 25, 2023, Nathan Holt of HistoricBridge.org, responded to the ECL via an email. He stated, "I would like to express strong support for the preservation of this bridge. I am baffled that the bridge is considered "Non-Select" as this bridge is an unusually narrow highway bridge and that speaks to the bridge's original owner... which as I understand accommodated [accommodated] a retreat for famous businessman and inventor Powel Crosley Jr. who is noted in America's automotive history as an early promotor of compact cars. My understanding is that this bridge was specifically built to Crosely's [Crosley's] specifications with a narrow deck as Crosley had no need of a wider bridge to accomodate [accommodate] his compact cars. It is therefore significant beyond its engineering design, but as a remarkable and unique surviving bridge that embodies this famous/noted man's interest in compact cars and is a tangible remnant of this retreat which is indeed noted as "Crosley Fish and Wildlife Area" and in my view is a historic centerpiece to the area. When I visited this bridge, I was not fully aware of this history. It would be nice to see interpretive signage added as part of this project describing Crosley and the bridge to make its significance clear to visitors."

Mary Kennedy (INDOT Cultural Resource Office [CRO] staff) responded to Holt's email on August 29, 2023, stating that he was added to the list of consulting parties for the project. Additionally, she responded by stating, "With regard to the bridge's Non-Select status, that was due to the bridge's low structural capacity and narrow width. The way the analysis fell out with those two items put the bridge on the Non-Select list. This does not mean that the bridge cannot be preserved. In fact, the plan is to preserve it. Given its location in the fish & wildlife area, those limitations are not as big a deal as they would be on a more traveled public roadway. With regard to potential interpretive signage, there is nothing in the Historic Bridges Programmatic Agreement that requires it. That would be up to DNR if they would want to pursue interpretive signage. "

On August 25, 2023, Paul Brandenburg of the Indiana Historic Spans Taskforce, responded to the ECL, stating that the, “Indiana Historic SPANs Taskforce supports the stated goal of: ‘The proposed scope of work is to rehabilitate the bridge’ as identified in the Early Coordination Letter and wish to remain a consulting party to review the detailed rehabilitation plans.”

Alyssa Reynolds of CRA responded to Brandenburg on August 28, 2023, stating that his response had been forwarded to INDOT CRO staff.

Tony Dillon of Historic Hoosier Bridges responded to the ECL in an email on August 28, 2023. He wished to be a consulting party for the proposed project. Additionally, he also mentioned that, “While vetting the Mead & Hunt determinations for Select/Non-Select bridges, Jim Cooper and I had a discussion about this one. While my initial reaction was to lobby for a reclassification, Jim was concerned with a possible backlash given the narrow width. We ultimately decided that it might be best to just let it lay low. I agree with Nathan wholeheartedly about the importance of this span and its preservation. Fortunately, that sentiment is shared by the park staff and preservationists in the area. There is nothing to suggest that anyone wants it gone...I would certainly help lobby for an historic marker once the bridge is rehabilitated.”

Mary Kennedy responded to Dillon’s email on August 29, 2023, stating that she, “...copied the consultant as well as DNR staff, so they can be aware of the additional vote for an interpretive sign. We look forward to your continued participation as the project progresses.”

On September 6, 2023, Laura Renwick of Indiana Landmarks Southern Regional Office responded to the ECL via an email stating that they would like to be a consulting party. She also noted that part of their address was incorrectly listed in the ECL. Reynolds responded on September 11, 2023, letting her know that her response had been forwarded to INDOT CRO.

On September 11, 2023, the Miami Tribe of Oklahoma responded to the ECL via a letter. They stated that, “The Miami Tribe offers no objection to the above-referenced project at this time, as we are not currently aware of existing documentation directly linking a specific Miami cultural or historic site to the project site. However, given the Miami Tribe’s deep and enduring relationship to its historic lands and cultural property within present-day Indiana, if any human remains or Native American cultural items falling under the Native American Graves Protection and Repatriation Act (NAGPRA) or archaeological evidence is discovered during any phase of this project, the Miami Tribe requests immediate consultation with the entity of jurisdiction for the location of discovery.”

On September 25, 2023, SHPO staff responded to the ECL via a letter stating for Reynolds to include a list of accepted consulting parties in the next correspondence. They also agreed that no archaeological investigations need to occur for the proposed project.

On October 12, 2023, the Eastern Shawnee Tribe responded to the ECL via a letter. They stated that they “...find our people occupied these areas historically and/or prehistorically. However, the project proposes **NO Adverse Effect** or endangerment to known sites of interest to the Eastern Shawnee Tribe. Please continue project as planned. However, should this project inadvertently discover an archeological site or object(s) we request that you immediately contact the Eastern Shawnee Tribe, as well as the appropriate state agencies (within 24 hours). We also ask that all ground disturbing activity stop until the Tribe and State agencies are consulted. Please note that any future changes to this project will require additional consultation.”

No further responses were received regarding the ECL or archaeological assessment.

The Historic Property Short Report (HPSR) and Historic Bridge Alternatives Analysis (HBAA) are available for review in IN SCOPE at <http://erms12c.indot.in.gov/Section106Documents/> (the Des. No. is the most efficient search term, once in IN SCOPE). You are invited to review this document and to respond with comments on any historic resource impacts incurred as a result of this project so that an environmental report can be completed. We also welcome your related opinions and other input to be considered in the preparation of the environmental document. If you prefer a hard-copy of this material, please respond to this email within seven (7) days.

Please review the information and comment within thirty (30) calendar days of receipt. If you indicate that you do not desire to be a consulting party or if you have not previously accepted consulting party status and you do not respond to this letter, you will not be included on the list of consulting parties for this project and will not receive further information about the project unless the design changes.

For questions concerning specific project details, you may contact Alyssa Reynolds of Cultural Resource Analysts, Inc. (CRA) at (812) 253-3009 or adreynolds@crai-ky.com. All future responses regarding the proposed project should be forwarded to CRA at the following address:

Alyssa Reynolds
Architectural Historian
Cultural Resource Analysts, Inc. (CRA)
201 NW 4th Street, Suite 204
Evansville, Indiana 47708
adreynolds@crai-ky.com

Tribal Contacts please respond to INDOT's Acting Tribal Liaison, Matt Coon mcoon@indot.in.gov; (317-697-9752) with any responses pertaining to this project including to provide INDOT/Indiana FHWA additional information about Tribal resources/concerns and questions/comments regarding cultural resources. The FHWA point of contact is Kari Carmany-George at K.CarmanyGeorge@dot.gov (317-226-5629).

Sincerely,



Matthew S. Coon, Manager
Cultural Resources Office
Environmental Services

~~Enclosures: Accepted Consulting Parties List, HPSR, & HBAA~~

HISTORIC PROPERTY SHORT REPORT FOR THE PROPOSED PARK ROAD BRIDGE PROJECT IN VERNON TOWNSHIP, JENNINGS COUNTY, INDIANA (INDOT DES. NO. 2200148; DHPA NO. 30377)

Excerpt



by
Alyssa Reynolds, MS

Prepared for



Prepared by



Kentucky | West Virginia | Wyoming
Indiana | Louisiana | Tennessee | Virginia

**HISTORIC PROPERTY SHORT REPORT FOR THE
PROPOSED PARK ROAD BRIDGE PROJECT IN VERNON
TOWNSHIP, JENNINGS COUNTY, INDIANA
(INDOT DES. NO. 2200148; DHPA NO. 30377)**

by

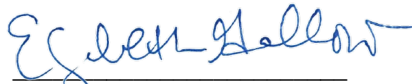
Alyssa Reynolds, MS

Prepared for

Brian Shaw
Beam, Longest & Neff, LLC
Phone: (317) 849-5832
Email: bshaw@b-l-n.com

Prepared by

Cultural Resource Analysts, Inc.
201 NW 4th Street, Suite 204
Evansville, Indiana 47708
Email: amartin@crai-ky.com
Phone: (812) 253-3009
Fax: (812) 253-3010
CRA Project No.: I230220



Elizabeth Gallow, MHP
Director, Architectural & Cultural
History

February 28, 2024

INDOT Des. No.: 2200148
DHPA No.: 30377

ABSTRACT

This report documents the identification and evaluation efforts for properties included in the Area of Potential Effects (APE) for the proposed Park Road Bridge Project in the Crosley Fish and Wildlife Area, Jennings County, Indiana (INDOT Des. No. 2200148). One aboveground resource located within the project APE was identified and evaluated in accordance with Section 106 of the National Historic Preservation Act (NHPA) of 1966, as amended, and the regulations implementing Section 106 (36 CFR Part 800).

As a result of the NHPA, as amended, and CFR Part 800, federal agencies are required to take into account the impact of federal undertakings upon historic properties in the area of the undertaking. Historic properties include buildings, structures, sites, objects, and/or districts that are eligible for or listed in the National Register of Historic Places (NRHP). As this project is receiving funding from the Federal Highway Administration (FHWA), it is subject to a Section 106 review.

Cultural Resource Analysts, Inc. (CRA), reviewed the Indiana Historic Sites and Structure Inventory (IHSSI) and found that there are no previously recorded resources within the APE. However, after reviewing the Indiana Historic Bridge Inventory (IHBI), completed in 2010, the subject bridge was identified as being previously determined eligible for the NRHP. The truss bridge (P000-40-07088 B; National Bridge Inventory [NBI] Number [No.] 060380) was also determined to be a “Non-Select” bridge, which are those that may not be considered excellent examples of a given type of historic bridge or may not be suitable candidates for preservation.

Following the records review, CRA conducted a field survey of all aboveground resources 50 years of age or older by the time of project letting within the APE. No additional resources were identified within the APE. The bridge is located within the Crosley Fish and Wildlife Area, but no other resources associated with the wildlife area are located within the APE. The portion of the wildlife area outside of the APE was not surveyed as this survey work would be outside of the recommended scope. Therefore, the APE only contains the truss bridge, which CRA recommends as continuing to be eligible for listing in the NRHP.

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I. INTRODUCTION AND PROJECT DESCRIPTION

In August 2023, Cultural Resource Analysts, Inc. (CRA), completed a historic property survey for the proposed Park Road bridge project (Des. No. 2200148) in Jennings County, Indiana (Figures 1 and 2). The survey was performed at the request of Beam, Longest & Neff, LLC (BLN), on behalf of the Indiana Department of Natural Resources (IDNR) and the Indiana Department of Transportation (INDOT). The Federal Highway Administration (FHWA) is the lead federal agency for the project.

The project area is located in the Crosley Fish and Wildlife Area along Park Road in Vernon Township. The need for the project is based on the current condition of the structure elements and notable deficiencies in the wearing surface, deck, superstructure, and substructure, and the scour critical status of the bridge. The purpose of the project is to correct the deficiencies in the bridge wearing surface, deck, superstructure, and substructure (scour protection) to reopen the bridge to vehicular traffic, and to protect the bridge and extend its service life (Figure 3).

All proposed work is assumed to be located within the roadway or adjacent to the roadway and focuses on the rehabilitation of the bridge.

The survey was conducted to comply with federal regulations concerning the impact of federal actions on sites and structures listed in, or eligible for nomination to, the National Register of Historic Places (NRHP). These regulations include Section 106 of the National Historic Preservation Act of 1966 and the regulations published in the Code of Federal Regulations at 36 CFR Part 800. As such, CRA conducted this historic property survey to:

- 1) Identify and document all resources (aboveground resources 50 years of age or older) by the time of project letting (2025) located within the Area of Potential Effects (APE).
- 2) Evaluate their eligibility for listing in the NRHP and recommend boundaries, if eligible.

Guidelines provided in the following documents were adhered to during the investigation: *Archaeology and Historic Preservation: Secretary of the Interior's Standards and Guidelines* (National Park Service 1983); *Guidelines for Local Surveys: A Basis for Preservation Planning: National Register Bulletin No. 24* (National Park Service 1985); and the Indiana Cultural Resources Manual (INDOT Cultural Resources Office n.d.).

According to 36 CFR Section 800.16(d), the APE is the geographic area or areas within which an undertaking may directly or indirectly cause alterations in the character or use of historic properties, if any such properties exist. The APE is influenced by the scale and nature of an undertaking and may be different for different kinds of effects caused by the undertaking. Given the nature of the proposed project, the APE was determined to include the proposed project area and a 250 ft buffer zone based on topography and vegetation surrounding the proposed work. Visual impacts were the driving factor in the creation of the APE. The APE takes into account the potential direct and indirect effects of the proposed project within the immediate contextual setting, which is comprised of a gradually sloping, heavily forested area surrounding the bridge. (Figures 4 and 5).

The following report is a summary of the survey findings. Fieldwork was completed on August 28, 2023, by Alyssa Reynolds, who meets the Secretary of the Interior's Professional Qualification Standards. Reynolds also authored this report.

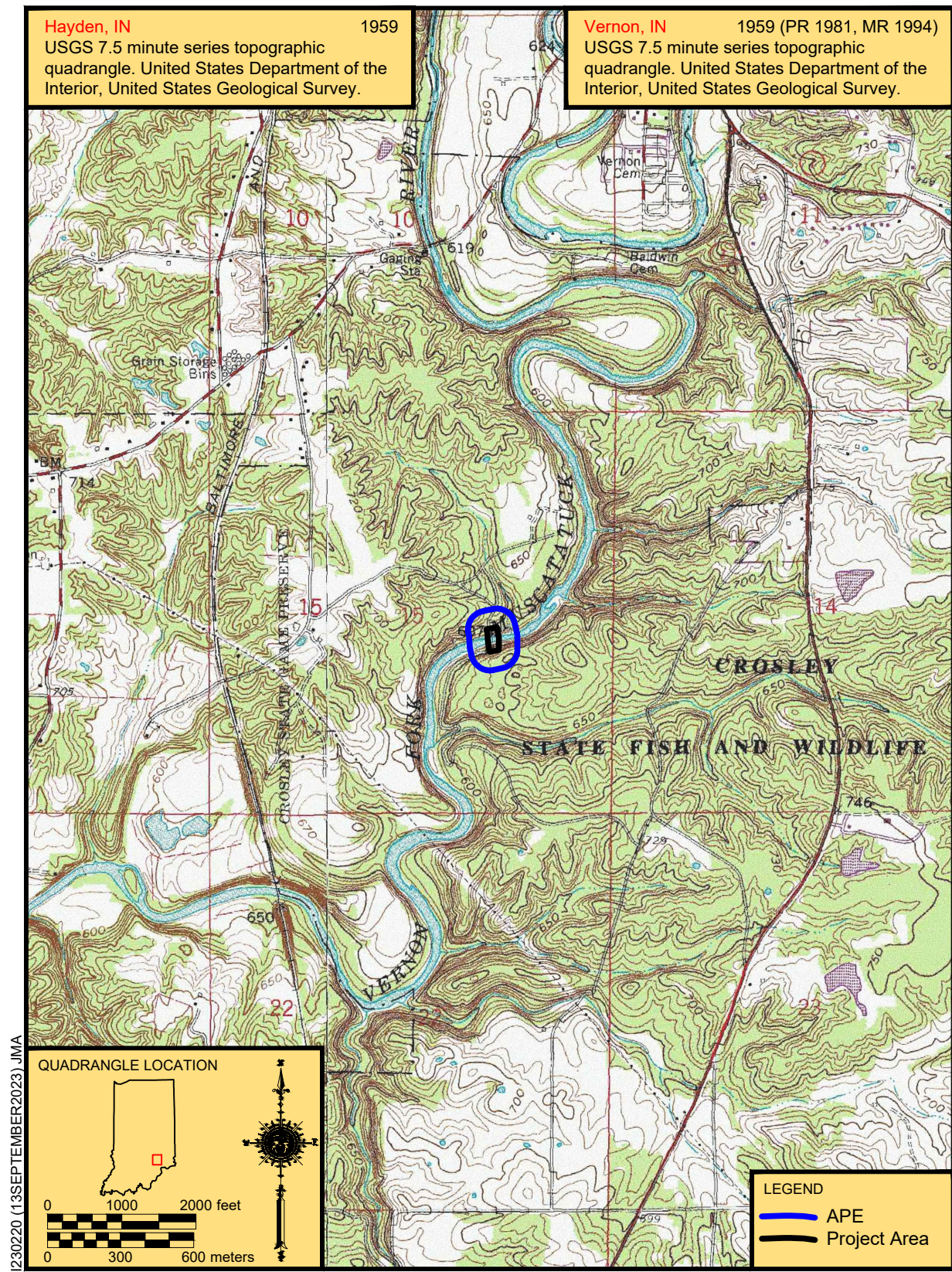


Figure 1. Topographic quadrangle showing the location of the proposed project and the APE.



Figure 2. Aerial map showing the location of the proposed project, the APE, and the location of the structure.



Figure 3. Overview of project area along Park Road, looking northeast.



Figure 4. Overview of the APE along Park Road, looking southwest.



Figure 5. Overview of project area along Park Road, looking northeast.

II. LITERATURE REVIEW

Before entering the field, available surveys, reports, studies, maps, and other data pertinent to the APE were identified and reviewed to develop an understanding of the project area and to identify previously documented resources (should there be any) located in the APE. This research included a review of the NRHP database, Indiana Historic Buildings, Bridges, and Cemeteries (IHBBC) Map, and the Indiana Register of Historic Sites and Structures (State Register), and the Indiana Historic Sites and Structures Inventory (IHSSI).

This task began with a review of the Jennings County Interim Report published in 1989 (Historic Landmarks Foundation of Indiana [HLFI]) and the State Historic Architecture and Archaeology Database (SHAARD) (Indiana Department of Natural Resources [IDNR] Division of Historic Preservation and Archaeology [DHPA] 2023). Structures surveyed for inclusion in the county interim reports and

SHAARD database are rated as “outstanding,” “notable,” “contributing,” or “non-contributing.”

Properties rated “outstanding” are those that convey enough local, state, or national historic or architectural significance that they are already listed in, or should be considered eligible for listing in, the NRHP. Properties rated “notable” are those that are above average in their importance; further research is needed to determine their eligibility for listing in the NRHP. Those properties rated as “contributing” meet the basic criterion of being 50 years or older and are important to an area’s historic fabric. They are not individually eligible for listing in the NRHP, but could be considered eligible as part of a historic district, should one exist. Those properties rated as “non-contributing” are not included in the survey unless they are located within a historic district. These structures are usually less than 50 years of age or their integrity has been compromised so that they have lost their historic character. No inventoried resources were identified during the review of the NRHP, State Register, IHSSI, and SHAARD data. The entirety of the Crosley Fish and Wildlife Area has not

been previously surveyed. INDOT determined that survey and evaluation of the entire fish and wildlife area was beyond the scope of work required for this report given that the current project's scope of work is limited in its impacts to the surrounding area.

CRA also reviewed the Indiana Historic Bridge Inventory (IHBI), completed by Mead and Hunt Architecture in 2010 (Mead and Hunt Architecture 2010), as part of the records review. The bridge in question (P000-40-07088 B; NBI No. 060380) was identified as previously determined eligible for listing in the NRHP. It was also determined to be a "Non-Select" bridge, which are those that may not be considered excellent examples of a given type of historic bridge or may not be suitable candidates for preservation.

As part of the archival research, online resources such as a county history were consulted and used to identify potential historic resources within the APE and to develop an understanding of the historical development of the APE and resources within it. These resources are listed in the bibliography. Archival research also included a review of the following available maps to help identify potential historic resources in the APE. A sampling of these resources is located in Appendix A.

1859 Map of Jennings County, Indiana (Conner)

1875 Jennings & Ripley County Maps Showing Residences in Each Township (Borden)

1884 An Atlas of Jennings County, Indiana (D.J. Lake & Company)

1925-1940 Plat Book of Jennings County, Indiana (W. W. Hixson & Company)

1936 Map of Jennings County, Indiana (Indiana Highway Survey Commission [IHSC])

1959 (Photorevised 1960) Vernon, Indiana, 7.5-minute topographic quadrangle (United States Geological Survey [USGS])

1959 (Photorevised 1981) Vernon, Indiana, 7.5-minute topographic quadrangle (USGS)

A review of the various map data from the late nineteenth century through the late twentieth century indicates that the area in and around the APE was originally small, wooded parcels. By

1931, the smaller parcels had been consolidated into one large parcel (4,048 acres) by Powell Crosley, Jr., as his private fishing and hunting grounds. By 1958, Crosley sold the land to the IDNR, forming the Crosley Fish and Wildlife Area, which comprises 4,228 acres at present (2024) (Crosley Fish & Wildlife Area 2010).

III. SURVEY METHODOLOGY

Following the literature review, CRA conducted a field survey of the APE, during which all properties that will be 50 years of age or older by the time of the project letting (2025) were surveyed. Field documentation included architectural analysis, digital photography, and mapping of all such aboveground resources in the APE. During the field survey, only one resource was identified as eligible for listing in the NRHP—the bridge (P000-40-07088 B; NBI No. 060380).

The results of the survey are documented in this historic property short report (HPSR). This HPSR follows recent changes by the Indiana DHPA to the methodology of the IHSSI program. Specifically, the IHSSI will no longer survey properties that are rated "contributing" and located outside of historic districts. The following resources will continue to be surveyed for the IHSSI: all properties that are rated "notable" or "outstanding," properties that are rated "contributing" and located within historic districts, all bridges, and all cemeteries.

Notwithstanding DHPA's amendment of IHSSI methodology, INDOT still requires all "contributing" properties within a proposed project's APE to be surveyed and documented by a qualified professional historian. However, in recognition of the change to IHSSI methodology, "contributing" properties that are located outside of a historic district do not receive an individual NRHP-eligibility evaluation within the text of the HPSR. As before, the IHSSI will serve as an aid in rating properties, but the historian continues to be responsible for confirming or adjusting this rating—using the IHSSI criteria—based on their own fieldwork and research. Likewise, the

historian continues to be responsible for identifying previously unsurveyed individual resources and historic districts. With the exception of resources already listed in the NRHP, all historic districts and all properties that the historian has rated “notable” or “outstanding”—whether previously surveyed or not—receives an NRHP-eligibility evaluation within the text of the HPSR. As always, the historian who prepares the HPSR must consider the potential NRHP eligibility of every aboveground resource within the APE.

Historic maps depicting the area associated with the property are located in Appendix A. Additional photographs are depicted in Appendix B along with a photograph map. In addition to documentation and evaluation of individual architectural resources, CRA also evaluated the area for potential historic districts and cultural landscapes with concentrations or contiguous areas of related properties with appropriate integrity. However, based on guidance from INDOT, the entirety of the Crosley Fish and Wildlife Area was not surveyed as a potential historic district as this falls outside of the project scope. No resources, except for the bridge in question, that could contribute to a potential historic district, are located in the APE for the project. As such, no potential historic districts were identified as part of the survey.

In general, in order for a property to be considered “notable” or “outstanding” and potentially eligible for listing in the NRHP, it must be at least 50 years old and possess both historic significance and integrity. Significance may be found in the following aspects of American history recognized by the NRHP Criteria:

- A. Association with historic events or activities
- B. Association with important persons
- C. Distinctive design or physical characteristics

A fourth criterion, Criterion D, or the potential to yield important information in prehistory or history, is typically not used for aboveground resources. A property must meet at least one of the criteria for listing. Integrity must also be evident through historic qualities,

including location, design, setting, materials, workmanship, feeling, and association.

IV. EVALUATION

The bridge is located within Crosley Fish and Wildlife Area, a heavily wooded area. The bridge is surrounded by a forest of coniferous and deciduous trees. The bridge is located on Park Road that crosses over Muscatatuck River. The gravel road is along a steep incline with a ravine on both sides.

Only the bridge (P000-40-07088 B; NBI No. 060380) was identified within the APE. The existing bridge is an approximately 212 ft by 12.3 ft, three-span, truss bridge with concrete abutments constructed in 1910 (Figures 6–9) (Mead and Hunt Architecture 2010). The main span of the bridge is a steel Pratt-through truss and the approach spans are both steel Warren pony trusses (Figures 10 and 11). The bridge features a replacement metal bridge rail (see Figure 11). The bridge was rehabilitated in 2004 when the deck was replaced with new wood planks and the truss was cleaned and painted. The gusset plates at Abutment Number (No.) 1 were replaced in 2015 along with a portion of the south span deck. The bridge was closed in 2019 due to holes and deterioration in the timber deck (Waggoner 2021).

Observed on several of the vertical steel members of the bridge was the embossing of “JONES & LAUGHLIN.” Research did not reveal any original plans for the bridge. Therefore, the builder of the bridge remains unknown. However, Jones and Laughlin Steel Corporation manufactured the steel components of the bridge (Figure 12). While the corporation did design bridges, research did not reveal if they designed this bridge. The Jones and Laughlin Steel Cooperation was owned by the Jones and Laughlin families who established the American Iron Works in Pittsburgh, Pennsylvania in 1853. The families formed Laughlin and Company in 1859 to manufacture blast furnaces. As the enterprise continued to grow, they expanded to become Jones & Laughlin Steel Company in the 1860s, quickly becoming the largest independent steelmaker in Pittsburgh and a producer of steel

only second in the United States to US Steel, also located in Pittsburgh. After several merges, the company continues to function as a part of Republic Steel (Davis 2015).

Powel Crosley, Jr., was the previous proprietor of the land now known as the Crosley Fish and Wildlife Area, as previously mentioned. Crosley was a famous Ohio businessman and inventor during the early twentieth century. Crosley, Jr., got his start in the automobile industry in 1916 after cofounding the American Automobile Accessory Company, where he invented a new tire liner (Banks 2007).

In 1921, Crosley, Jr., expanded his business into the radio industry building affordable radios for the everyday consumer at his company, the Crosley Radio Corporation. The Crosley Radio Corporation was a manufacturing facility that

created radio parts for the rapidly growing radio industry. By 1925, it was the largest radio manufacturer in the world. While Crosley, Jr., was creating these radios, he simultaneously established the Crosley Broadcasting Corporation, his own broadcasting company (Horstman 1999).

During the 1930s, Crosley, Jr., designed an affordable, compact automobile at his company, Crosley Motors, Inc. The compact car, debuting in 1939, was manufactured at his car-manufacturing plants in Camp Washington, Ohio, Richmond, Indiana, and Marion, Indiana. The compact cars were marketed as small, lightweight, affordable vehicles. Crosley Motors, Inc. closed in 1952, at which point the company had manufactured approximately 84,000 vehicles (Banks 2007).



Figure 6. East elevation of the bridge (P000-40-07088 B; NBI No. 060380), looking southwest.

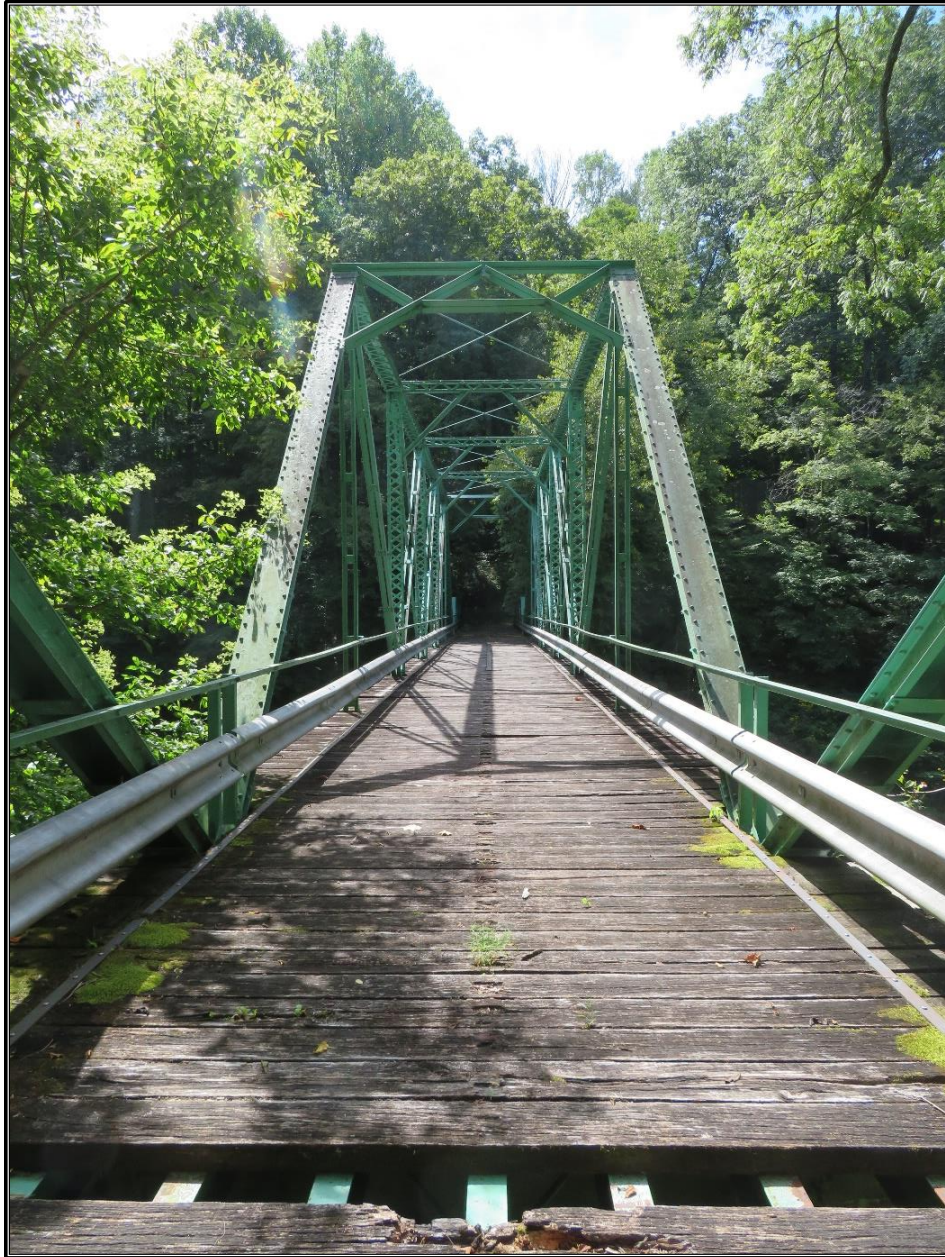


Figure 7. Deck of the bridge (P000-40-07088 B; NBI No. 060380), looking south.



Figure 8. Substructure of the bridge (P000-40-07088 B; NBI No. 060380) and north pier, looking south.



Figure 9. North concrete abutment of the bridge (P000-40-07088 B; NBI No. 060380), looking northeast.



Figure 10. Detail of the main span of the bridge (P000-40-07088 B; NBI No. 060380) showing the Parker truss, looking northwest.



Figure 11. Detail of the approach span of the bridge (P000-40-07088 B; NBI No. 060380) showing the Warren truss, looking southwest.



Figure 12. Detail of a vertical, steel member of the bridge (P000-40-07088 B; NBI No. 060380) featuring the embossing of "JONES & LAUGHLIN," looking south.

According to Nathan Holt of HistoricBridges.org, via email correspondence, Crosley, Jr., ordered the construction of the bridge in question for his retreat area to the specifications of the compact cars he designed (Nathan Holt, email to Alyssa Reynolds, August 25, 2023). However, the construction date identified for this bridge in the IHBI is 1910, prior to when Crosley, Jr., had acquired the parcel and his construction of compact automobiles (Mead and Hunt Architecture 2010). Further email correspondence between Holt and Tony Dillon of Historic Hoosier Bridges, revealed that no records confirming Crosley's construction of the bridge had been identified (Tony Dillon, email to Holt, November 14, 2023). CRA also contacted the Jennings County Historical Society, but no documents regarding Crosley's construction of the bridge have been located as of January 2024. Thus, while there is a possibility of the bridge being constructed by Crosley, Jr., at a later date, CRA does not have firm evidence to refute the 1910 date listed in the IHBI (Mead and Hunt Architecture 2010).

According to the IHBI, the bridge is significant as a unique example of a narrow-width, three-span, Pratt-through and Warren pony truss bridge. While a portion of the replacement wood planks are missing and there is replacement metal bridge rail, the original metal trusses are minimally rusted and retain their material integrity. The bridge retains its ability to convey its architectural significance. Therefore, CRA recommends that the bridge continue to be eligible for listing in the NRHP under Criterion C for its unique architectural and engineering design. The bridge's period of significance dates to its construction—1910. Research did not reveal any known associations warranting individual eligibility under Criterion A. Full evaluation of the Crosley Fish and Wildlife Area as a potential historic district would be necessary to determine if the bridge possesses significance under Criterion A, possibly as a contributing resource within the larger fish and wildlife property; however, such evaluation was beyond the scope of the current project.

While Crosley, Jr., was a pioneer in the compact car automobile industry in Indiana and Ohio, and it is reported, but unconfirmed, that the

bridge in question was built according to his specifications, his significance as an inventor and businessman is better represented at these businesses, such as the Crosley Building (Cincinnati, Ohio) or his residences, such as Pinecroft (Cincinnati, Ohio) and Seagate (Sarasota Bay, Florida), all listed in the NRHP. These places are more emblematic of his success in the automobile industry and radio broadcasting industry and as an individual. As such, the bridge is not individually eligible for listing in the NRHP under Criterion B.

The recommended NRHP boundary for this bridge encompasses the bridge footprint, including the bridge approaches (Figure 13).

V. CONCLUSIONS

The APE contains only one resource, a bridge (P000-40-07088 B; NBI No. 060380), 50 years or older, identified as being eligible for listing in the NRHP under Criterion C for its unique architectural and engineering design as a narrow-width, three-span, Pratt-through and Warren pony truss bridge. The bridge was identified in the IHBI as being previously determined eligible for the NRHP and was designated as a "Non-Select" bridge. As a result, CRA recommends that the bridge continue to be eligible for listing in the NRHP.

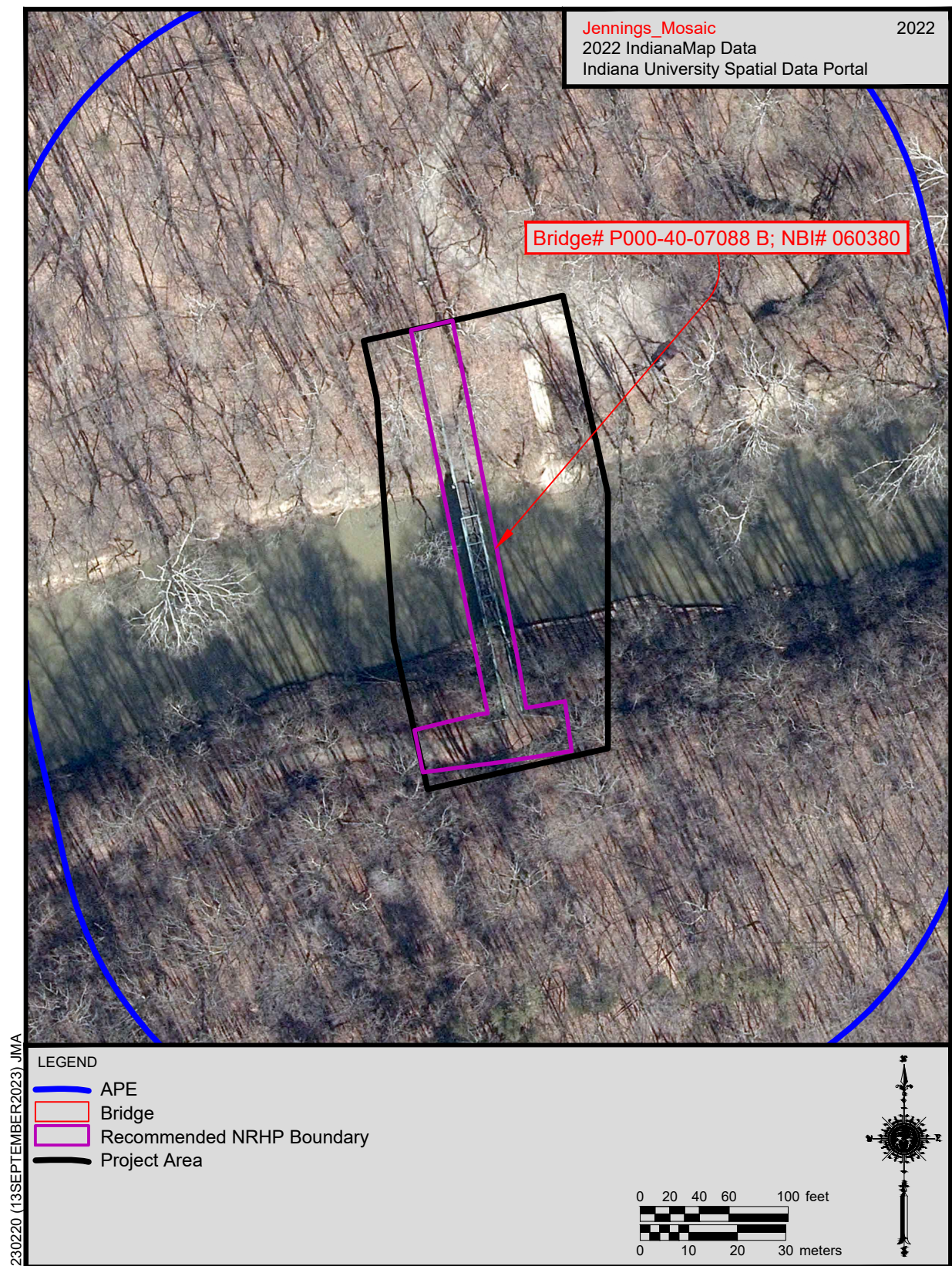


Figure 13. Recommended NRHP boundary for the bridge (P000-40-07088 B; NBI No. 60380).

HISTORIC BRIDGE ALTERNATIVE ANALYSIS REPORT

PARK ROAD OVER MUSCATATUCK RIVER

Excerpt

EXISTING BRIDGE FILE #: P000-40-07088 B

NBI NO.: 060380

DESIGNATION #: 2200148



PREPARED FOR:

PREPARED BY: BEAM, LONGEST & NEFF

INDIANA DEPARTMENT OF TRANSPORTATION

Date: October 2023



This bridge was evaluated by personnel from the Indiana Department of Transportation (INDOT) Bridge Design Unit, the District Office and the designer. The attached DRAFT Historic Bridge Alternatives Analysis has been reviewed by the INDOT Bridge Design Unit and Cultural Resources Office for thoroughness of the rehabilitation option and compliance with INDOT design policies. Concurrence by INDOT with the proposed Scope of Work does not constitute Final Approval of the Historic Bridge Alternative Analysis. This draft HBAA may now be distributed to the historic consulting parties for review.

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HISTORIC BRIDGE ALTERNATIVE ANALYSIS REPORT
EXISTING BRIDGE NO. P000-40-07088 B (NATIONAL BRIDGE INVENTORY NO. 060380)
PARK ROAD OVER MUSCATATUCK RIVER
JENNINGS COUNTY, INDIANA
DES. NO. 2200148

I. EXISTING STRUCTURE DATA

A. Identification/History

Existing Bridge No.: P000-40-07088 B

Project Location: Crosley Fish and Wildlife Area

Latitude: N 38° 57' 42.37" (38.96177)

Longitude: W 85° 37' 01.7" (-85.61714)

Des. No.: 2200148

Year Built: 1910

Year Rehabilitated: 1979 - new 3 in. x 8 in. timber plank deck, some steel stringer replaced and added, truss and steel superstructure cleaned and painted, replaced the bridge rail and approach guardrail with w-beam rail.

Year Rehabilitated: 2004 – New 3 in. x 8 in. timber plank deck, truss and steel superstructure cleaned and painted.

Year Repaired: 2015 – Span 1 low chord gusset plates replaced at south abutment, and portions of south span timber deck was replaced.

Year: 2019 – Bridge closed due to deterioration of the deck.

Most Recent Field Inspection Date: 12/20/2022 (Other inspection by BLN), 03/15/2022 (Routine inspection by INDOT), and 03/25/2020 (Routine and Fracture Critical (FC) inspection by INDOT)

Average Daily Traffic (ADT)/Year of ADT: 143 vpd /2019

Percentage of Commercial Vehicles: 0 %

Low Volume Road: Yes; Less than 400 vpd

Functional Classification: Recreational (Primary Access)

Detour Length: Approximately 3 miles

Load Rating (H-20 Inventory Rating): 5 Ton for floor beams, 7 Ton for stringers, and 10 Ton for truss (Appendix J)

Sufficiency Rating: 0.0

Seismic Zone: Preliminary investigation, Seismic Design Category A

National Register of Historic Places (NRHP) Status: Eligible

Historic Bridge Prioritization Status: Non-Select

Historic Character-Defining Features:

Pratt through truss with Warren pony truss.

B. Structure/Dimensions

MAIN SPAN

Surface Type: 3 in. x 8 in. Timber Plank Deck

Out to Out of Copings: 12 ft. 3 in.

Out to Out of Bridge Floor: 212 ft. 0 in.

Clear Roadway Width: 9 ft. 4 in.

Number of Lanes on Structure: 1

Skew: 0 degrees

Type of Superstructure: Steel Pratt Through Truss

Main Span: 110 ft. 0 in. (Span B)

Type of Substructure/Foundation: Concrete Wall Piers assumed on Concrete Spread Footings on Rock.

APPROACH SPANS

Surface Type: 3 in. x 8 in. Timber Plank Deck.

Out to Out of Copings: 12 ft. 3 in.

Clear Roadway Width: 9 ft. 4 in.

Number of Lanes on Structure: 1

Skew: 0 degrees

Type of Superstructure: Steel Warren Pony Truss

Approach Spans: 49 ft. 0 in., South and North Approach spans (Span A and C)

Type of Substructure/Foundation: Concrete Abutments on assumed Concrete Spread Footings with Concrete Wall Piers on assumed Concrete Spread Footings on Rock.

C. Appurtenances

Bridge Railing: 2 in. x 2 in. x 3/8 in. Steel Angle with a Steel W-Beam Rail

Curbs: N/A

Sidewalks: N/A

Utilities: N/A

Railroad: N/A

D. Approaches

Roadway Width: 9 ft. 0 in. lane with no shoulders

Surface Type: Gravel

Guardrail: W-Beam

Guardrail Transition: None

II. EXISTING CONDITIONS

Photos detailing the existing conditions are included in Appendix C. Original plans are not available for this structure; however, the 1979 and 2004 rehabilitation plans, and 2015 repair plans are available and provided in Appendix H.

A. Bridge Deck

General: The deck currently has an overall condition rating of critical (2 out of 9).

Surface Condition: The wearing surface currently has an overall condition rating of critical (2 out of 9).

Repair/Maintenance Work: The bridge deck was fully replaced in 1979 and 2004, and the south span partially replaced in 2015.

Deficiencies: The timber planks have major deterioration. There are many holes, rotten boards, and missing boards in the deck. There are some loose planks and splits in the planks.

Underside Condition: The underside of the deck planks have some staining and splits.

Drainage: N/A

Bridge Railing: The bridge railing currently has an overall condition rating of satisfactory (6 out of 9). The existing bridge railing and approach guardrail were replaced as part of the 1979 rehabilitation.

Sidewalks: N/A

Median: N/A

B. Superstructure

MAIN & APPROACH SPANS

General: The steel truss has an overall condition rating of fair (5 out of 9).

Repair/Maintenance Work: Some steel stringers replaced and added during 1979 rehab. Truss and steel superstructure cleaned and painted in 1979 and 2004.

Deficiencies: The stringers have areas of surface rust and moderate pitting near bearing locations due to dirt accumulation and areas of minor section loss on the top flange. The floor beams have areas of surface rust, minor pitting, minor expansion rust at some stringer connections, and moderate pitting on bottom of web along the entire beam length. The truss members and connections overall have areas of surface rust and minor pitting. In spans A & C, the vertical members have moderate to heavy expansion between the angles at low chord gusset plates (See

photos 11, 12, 14, 17, 20, 45, 47, 48, and 50). In Span B at the floor beam connection at L1 of the west truss and at L1 of the east truss, the floor beam web has a 1 inch horizontal tear with out of plane distortion of the web below the exterior gusset plate connection (see photos 25-30, 35-37). In addition, span B floor beam connection at L3 of the east truss has minor out of plane distortion of the web below the exterior gusset plate connection. The exterior low chord angle at connection L3 on both the east and west trusses has moderate expansion rust, moderate pitting, and section loss at the gusset plate and low chord angle at the west truss has minor out of plane distortion about 1 foot away from L3 (see photos 31, 32, 41, and 42). Span B east truss exterior angle of the low chord between L1L2 and L2L3 has locations of out of plane distortion near L2 and L3 (see photos 75-77). Please see Appendix C for photos showing the location and severity of the deterioration.

Fracture Critical Members: This structure is fracture critical and has details that have lower fatigue resistance that should be highlighted during inspections. A fracture critical structure has steel members that are in tension and whose failure would probably cause a portion of or the entire bridge to collapse. Fatigue is the initiation and/or propagation of a crack by the repeated variation of normal stress in a tension member. This structure has fatigue details that are category D due to the connections being riveted. Category D is one of eight categories that signifies fatigue resistance. Category D are details that are a transition between details influenced by discontinuities (A, B, B', C, and C') and those whose performance is dominated by geometry and stress concentrations (D, E, and E'). Some members were noted having tack welds and previous repairs were completed with welds. Fatigue is not anticipated to be a problem with this bridge since there is no truck traffic.

Bearings/Pedestals: The trusses are supported by steel bearings supported by concrete piers or abutments. Minimal deterioration was noted in the bearings. Moderate debris accumulation was noted at the truss bearings.

Damage: N/A

Load Rating: A Load Factor Rating (LFR) analysis was completed (see Appendix F correspondence with Jennifer Hart) for the existing structure and the summary may be found in Appendix J. The controlling bridge elements are the steel floor beams that have a 4 ton H-20 inventory load capacity.

C. Substructure and Foundations

General: The substructure has an overall condition rating of satisfactory (6 out of 9). Abutments are assumed to have concrete spread footings on soil and the piers are assumed to have concrete foundations on rock. The south pier (Pier 2) is on rock but does not appear to be keyed into rock. The north pier (Pier 3) was probed, and rock was found at the same depth as the channel; therefore, the pier is assumed to be founded on rock like Pier 2.

Repair/Maintenance Work: The abutment cap corners were reconstructed during the 2015 repairs.

Deficiencies: Abutment 4 (North abutment) has cracking with leaching and minor spalling at the bridge corners and on various locations on its wings. Heavy debris is on the abutment bridge seats that encase the stringer ends. Pier cap ends are cracked with leaching and have minor spalls. Please see Appendix C for photos showing the location and severity of the deterioration.

Drainage: N/A

Scour: The channel bottom is rock. The current scour critical bridge rating is a 3-scour critical. INDOT has recently completed a scour analysis and classified this bridge as scour critical due to the substructure foundations being unknown and the hydraulic scour analysis calculated large scour depths (see Appendix E for scour memo). There is no scour noted, and no maintenance issues were reported by Indiana Department of Natural Resources (IDNR) property manager. The bridge has had some flooding with the northeast parking lot having had about 6 feet of water; however, no known flood water has ever overtopped the bridge deck.

D. Approaches

General: The road is straight from the north and over the bridge; however, at the south bridge end, the road turns 90° to the west. The bridge is approximately level and is above both approach roadways. The south approach roadway has a slight grade down away from the bridge. The north approach has a steeper grade down and is lower than the south approach roadway. The posted speed limit is 15 mph on this roadway.

Approach Pavement: The approach roadway is gravel and is in satisfactory condition (6 out of 9).

Guardrail: There is w-beam guardrail at all four corners of the bridge that has substandard height and post spacing. The approach guardrail was installed as part of the 1979 rehabilitation.

Drives and Public Roads: There is a driveway to a large parking lot and boat ramp approximately 100 feet northeast of the bridge.

Miscellaneous: No utilities near the bridge.

E. Slopewalls (Channel)

General: The channel has an overall condition rating of fair (5 out of 9). The channel bottom is rock and rock outcroppings are present on the south channel bank and around the south pier.

Deficiencies: The slopes leading down to the river at Abutment 1 and near Pier 3 are steep. There is minor sediment accumulation on the north side of Pier 3.

III. PURPOSE AND NEED

A. Background

This bridge carries Park Road over the Muscatatuck River within the Crosley Fish and Wildlife Area. The bridge is located in Section 15, Township 6 North and Range 8 East on the 7.5 minute Vernon, Indiana USGS quadrangle map. The bridge is located in Jennings County, Indiana. Muscatatuck River flows from northeast to southwest under the bridge and the bridge has approximate 199.66 square mile drainage area. Muscatatuck River is considered a navigable waterway in this location (see Appendix A Maps) and it is listed on the IDNR Listing of Outstanding Rivers and Streams.

The bridge consists of 3 spans that include one 49 ft approach span A, one 110 ft main span B, and one 49 ft approach span C with a total out-to-out bridge floor length of 212 ft. The main Span B is a steel through truss, and the approach spans A and C are steel pony trusses. The bridge clear roadway width is 9 ft.-4 in. The superstructure is composed of a timber plank deck that is supported by steel stringers and floor beams. The bridge was built in 1910 and was rehabilitated in 1979 and 2004 and repaired in 2015. In 2019, the bridge was closed due to the condition of the deck. The latest bridge inspection report (3/25/2022, see appendix G) assigned a bridge sufficiency rating of 0.0 out of a possible 100. The deck is currently assessed as being in critical condition, the superstructure is currently rated fair, and the substructure is currently rated in satisfactory condition. The bridge is considered scour critical due to the unknown foundations and the large scour depths calculated. The bridge was posted 4 tons in 1984 by the park supervisor to protect the bridge. The existing bridge inspection report states there are 143 vehicles per day, and this was confirmed to be the design year Annual Average Daily Traffic, AADT, during the initial field check discussion, see Appendix F for the meeting minutes.

According to the Indiana Historic Bridge Inventory, Bridge No. P000-40-07088 B is listed as a “Non-Select” bridge eligible for inclusion in the National Register of Historic Places (NRHP). “Non-Select” bridges identify those historic bridges that are not considered excellent examples of a given type of historic bridge or are not suitable candidates for preservation.

B. Need

The Indiana Department of Transportation (INDOT) and IDNR have identified three needs that will be addressed by this project: the structural deficiencies, the deck and superstructure conditions, and the scour critical status of the bridge.

The first need of this project is to address the distress and deterioration of the existing bridge’s deck that continues to worsen and has compromised the safety of the public causing the bridge to be closed in 2019. Currently, the wearing surface and deck are rated in critical condition (2 out of 9), due to areas of severe deterioration in the timber bridge deck. The superstructure is rated in fair condition (5 out of 9) and has areas of deterioration and section loss that limit the functional life of the bridge. The final bridge’s need is a result of the hydraulic scour analysis that determined scour to be below all the substructure’s foundations causing the bridge to become scour critical.

C. Purpose

The purpose of this project is to open the bridge to vehicular traffic, improve the deteriorated portions and the bridge condition to satisfactory (6 out of 9) or better, provide adequate safety to the traveling public, maintain a minimum 4 Ton H-20 inventory load rating, and preserve the historic aspects of the bridge. The final objective of the project is to protect the bridge from scour and remove it from scour critical status.

IV. ALTERNATIVES

The alternatives described in this document are based on the guidance for a writing historic bridge Section 4(f) alternatives analysis produced by INDOT, Cultural Resource Office and finalized on December 14, 2012. Per the guidance, alternatives A through F must be analyzed in consecutive order until a feasible and prudent alternative has been determined. Once a feasible and prudent alternative has been determined, the remaining alternatives do not need to be discussed. A feasible alternative is one that is possible to engineer, design, and build. A prudent alternative is one that does not present significantly unique or unusual factors (e.g. cost; social, economic, or environmental impacts; community disruption).

A. The No-Build/Do-Nothing Alternative

The No-Build/Do-Nothing Alternative is not a prudent solution for the proposed project. The No-Build/Do-Nothing Alternative would not address the overall purpose of the project. If the No-Build/Do-Nothing Alternative were selected, the bridge will remain closed, deterioration of the deck and superstructure elements would continue causing lower condition ratings, and even lower weight restrictions would eventually be placed on the bridge. Closing the bridge for a prolonged period of time would have a negative impact on the Crosley Fish and Wildlife Area and surrounding community. Although this alternative is feasible, it is not prudent to keep the bridge closed and continue to cause a significant disruption to the Crosley Fish and Wildlife Area and surrounding community.

B1. Rehabilitation of the Existing Bridge for Continued Vehicular Use (Two-Way (One Lane) Option) Meeting Secretary of Interior's Standards (SOIS) for Rehabilitation

The Secretary of the Interior (SOI) defines rehabilitation as "the act or process of making possible a compatible use for a property through repair, alterations, and additions while preserving those portions or features which convey its historical, cultural, or architectural values" (<https://www.nps.gov/tps/standards/four-treatments/treatment-rehabilitation.htm>). The Standards for rehabilitation follow the Department of Interior regulations 36 CFR 67, which defines rehabilitation as "the process of returning a building (structure) to a state of utility, through repair or alteration, which makes possible an efficient use while preserving those portions and features of the structure and its site and environment which are significant to its historic, architectural, and cultural values as determined by the Secretary". The 10 standards outlined shall be followed for each rehabilitation project.

Two-Way (One Lane) Option Bridge Rehabilitation with No Roadway Reconstruction

This alternative includes the rehabilitation of the existing bridge to address the deteriorated areas of the bridge but not the approach roadways. The structural deterioration addressed under this alternative would prolong the life of the historic structure. The improvements will result in an increased deck, wearing surface, and superstructure condition ratings (satisfactory or higher) so that the maximum inspection frequency may be increased from 12 to 24 months. In addition, this alternative will provide adequate scour countermeasures at all substructure units; therefore, the bridge will not be considered scour critical.

The scope of work for the rehabilitation will include:

- Cleaning and painting the truss. The paint color will match the existing color and the bridge rail will be painted to match the truss.
- Replacing the bridge deck in all spans with a timber plank deck that has timber running boards.
- Repairing or replacing in kind some verticals, gusset plates, and low chord truss members or connections (see Figures 1, 2, and 3).
- Replacing some miscellaneous truss secondary members in kind such as bottom lateral bracing and lattice.

- Deteriorated concrete abutment and pier caps will be removed and reconstructed.
- Delaminated and spalled areas of abutments and piers will be patched.
- Placing scour countermeasures at all substructure units.
- Rivets removed to make necessary repairs will be replaced with round headed bolts wherever possible. The rounded head will be installed on the face of the member that is most visible from the roadway or exterior face of member.
- Replacing the bridge rail with a steel hand rail (see Figure 4 for an example).
- Some trees may need to be removed to install the scour countermeasures. All other nearby trees will be trimmed to keep branches away from truss and vehicles.
- Clean the abutment and pier bridge seats of all debris.

The new bridge deck will match the existing out to out width. The original bridge rail type and configuration is unknown since we do not have the original plans. The bridge rail is expected to be replaced with a miscellaneous steel hand rail that is common for similar truss bridges. The new steel hand rail will be like the example shown in Figure 4. The clear roadway width will be slightly larger due to installing the new steel hand rail on the bridge. The approach guardrail will not be modified leading up to the bridge. No approach roadway work will be completed as part of this project, except portions of the south approach guardrail will be removed and reset if necessary to complete the scour countermeasure work. The scour countermeasure will consist of placing either dumped partially grouted revetment riprap or an articulated concrete mattress on the south spillslope, placing class 1 riprap at a 20 feet width around the north abutment and wings, a concrete underpin will be placed around pier 2 (south pier), and dumped partially grouted class 2 riprap will be placed around 3 sides (channel side and each pier nose) of pier 3 with regular buried class 2 riprap on the north side of pier 3. These proposed scour countermeasures were discussed and approved by INDOT hydraulics and the email correspondence is included in Appendix E.

The proposed project will require multiple design exceptions for this rehabilitation alternative (travel lane width, usable and paved shoulder width, bridge clear roadway width, structural capacity, travel lane cross slope, bridge rail, and vertical clearance). These are expected to be approved since widening the truss is exceedingly difficult, expensive, and would cause modifications to a historic bridge. The historical bridge inspection report stated that the bridge has been posted 4 tons since 1984 due to the park supervisor recommending this posting to protect the bridge and Central Office Bridge Inspection has always concurred with this posting (see end of Appendix G for portions of the Historical Bridge Inspection Report). There is no existing bridge load rating on file; however, a preliminary load rating of the floor system determined that the stringers rate at 8 tons and the floor beams rate at 4 tons. A complete existing load rating analysis of the truss, stringers, floor beams, and gusset plates using LFR analysis will be done prior to final approval of this HBAA Report (see Appendix F email correspondence). This bridge and roadway through the Crosley Fish and Wildlife Area is the primary road through the park and the roadway has very sharp turns and curves with a low water (ford) crossing south of the bridge. IDNR wants to deter larger vehicles from using this bridge and road; therefore, IDNR preferred the bridge remain at a 4 ton load posting, and INDOT agreed. Any truss member or portion of the floor system that does not satisfy the 4 ton load limit will be replaced in kind or repaired. The level one controlling criterion affected by this alternative are summarized in Table 1. The project is not considered mobility significant per IDM Chapter 81-1.02. A transportation management plan (TMP) is not anticipated for this project.

This structure crosses the Muscatatuck River in a rural area that has an upstream drainage area of approximately 199.66 square miles. The ground is primarily tree covered along the banks and floodplain. Tree trimming will be required to keep branches off the truss and away from vehicles and some tree clearing is anticipated to install the scour countermeasures. A 401/404 permit will be required for work below the OHWM Elevation. An IDNR construction in a floodway permit will be required due to the large upstream drainage area and modeling is anticipated due to the proposed scour countermeasures that will be placed. There may be wetlands near the project. An IDEM Rule 5 permit is not anticipated because the land disturbance is expected to be less than 1 acre. A level 4 CE is required due to the anticipated structure rehabilitation impacts and historical significance of the bridge. No bat or bird habitat was found within the structure.

This alternative will be constructed under full closure with no detour since the bridge is already closed. It is anticipated the duration of construction will be less than 120 days. This project is within IDNR's right of way limits since it is within the Crosley Fish and Wildlife Area; therefore, no right of way will be required to complete this project. In addition, no utilities are within the project site; therefore, no utilities impacts are anticipated. Table 1 highlights the design criteria for this alternative.

The estimated cost of this rehabilitation alternative is approximately \$950,000 which is 65% of the estimated total bridge replacement cost of \$1,475,000. The breakdown of the cost for the rehabilitation and replacement alternatives are attached in Appendix D. This rehabilitation alternative will increase the remaining life of the bridge by mitigating the existing deterioration and preventing scour damage. It is anticipated that the next rehabilitation would be required in approximately 25 years, which would consist of a deck replacement. Since this rehabilitation alternative meets the purpose and need at a reasonable cost, it is both a feasible and prudent alternative.

Table 1: B1. ALTERNATIVE DESIGN CRITERIA SUMMARY					
Design Element	Indiana Design Manual section	Minimum Design Criteria	Existing Condition	Proposed Condition	Design Exception Required**
Design Speed	IDM 412-5.03	15 mph	15 mph	15 mph	No
Travel Lane Width	Fig. 51-6B	11 ft.	9.33 ft.	9.66 ft.	Yes
Usable Shoulder Width	Fig. 51-6B	2 ft.	0 ft.	0 ft.	Yes
Paved Shoulder Width	Fig. 51-6B	2 ft.	0 ft.	0 ft.	Yes
Bridge Clear Roadway Width	Fig. 412-2B	16 ft.	9.33 ft.	9.66 ft.	Yes
Structural Capacity	Fig. 412-2A	HS-15 27 Ton	5 Ton* H-20 Inv.	5 Ton* H-20 Inv.	Yes
Horizontal Curve, Minimum Radius	N/A	N/A	N/A	N/A	N/A
Superelevation Transition Lengths	N/A	N/A	N/A	N/A	N/A
Stopping Sight Distance Horizontal Curves	N/A	N/A	N/A	N/A	N/A
Stopping Sight Distance Vertical Curves	N/A	N/A	N/A	N/A	N/A
Maximum Grade	Fig. 51-6B	7 %	Approx. 0 %	Approx. 0 %	No
Travel Lane Cross Slope	Fig. 51-6B	2 %	Approx. 0 %	Approx. 0 %	Yes
Superelevation Rate	N/A	N/A	N/A	N/A	N/A
Vertical Clearance	Fig. 51-6B	14.5 ft.	13.33 ft.	13.33 ft.	Yes
Bridge Railing Test Level	IDM 412-5.03	In Kind Replacement	Slight Damage	In Kind Replacement	Yes

*See Appendix J for existing load rating summary.

**Process for Design Exceptions shall be discussed with INDOT Design prior to Design Exception submittals.

V. MINIMIZATION AND MITIGATION

A. Minimization

Only the deteriorated and substandard portions of the bridge will be repaired or replaced. The members and connections that are replaced will be replaced in kind. All in kind replacements refer to replacing a bridge element with the same material and same size or dimensions. The entire bridge will be rehabilitated to help preserve the bridge. Minimal tree removal will be conducted to install adequate scour countermeasures at the bridge.

Per Attachment B (Standard Treatment Approach for Historic Bridges) of the Historic Bridges PA, IDNR/INDOT will provide rehabilitation plans to the Indiana SHPO when the design is approximately 30 % complete, 60 % complete, and when final design plans are complete. All available plans are provided in Appendix H. Current design plans are not yet available.

B. Bridge Marketing

The existing bridge will not be put into the bridge marketing program because IDNR will continue to maintain jurisdiction over the bridge.

C. Mitigation

Per Attachment B (Standard Treatment Approach for Historic Bridges) of the Historic Bridges PA, IDNR/INDOT will consult with the Indiana SHPO to determine if any photo documentation will be necessary because of the preferred alternative.

VI. PRELIMINARY PREFERRED ALTERNATIVE

The preliminary preferred alternative for this project is B1 Rehabilitation for Continued Vehicular Use Meeting the SOI Standards for rehabilitation. The estimated rehabilitation and replacement costs are provided in Appendix D. The rehabilitation cost is approximately 65% of the replacement cost and the rehabilitation of the bridge would extend the life of the bridge to approximately 25 years. According to the 2012 version of the INDOT Design Manual, figure 72-2C "Evaluating Alternative Improvement Strategies", the rehabilitation to replacement cost percentage is below the curve and threshold of 76% assuming a 25 year extended life, making the bridge rehabilitation a more cost effective option than replacement (Appendix D). In addition, the HBPA indicates that the goal of a bridge rehabilitation is to extend the life 25 years and we would be matching this with our bridge rehabilitation (25 years). The rehabilitation alternate is both feasible and prudent and is the preferred alternative. See Table 2 for the alternative cost summary table for the cost comparison of each alternative.

Table 2: ALTERNATIVE COST AND COMPAISON SUMMARY						
Alternative	Meets Project Purpose and Need?	Construction Cost	Right of Way Amount & Cost	Total Cost	Other Factors	Feasible & Prudent
A. No Build	No	\$0	No R/W \$0	\$0	N/A	No
B.1 Rehabilitation for Continued Vehicular Use	Yes	\$950,000	No R/W \$0	\$950,000	N/A	Yes
*F. Replacement	Yes	\$1,475,000	No R/W \$0	\$1,475,000	N/A	No

*Note: Alternative F (Replacement alternative) was only included for cost comparison purposes and shall not be considered as a feasible and prudent alternative.

Only the narrative of the Alternatives Analysis Report has been included; attachments, graphics, and additional pages have been removed. The full report can be made available upon request.

HPSR Response from Indiana Historic SPANs Taskforce

From: [Alyssa Reynolds](#)
To: ["Paul Brandenburg"](#)
Cc: ["Brinker, Haley"](#); ["Branigin, Susan"](#); ["Coon, Matthew"](#); ["Elizabeth Gallow"](#); [Alfred Wessling](#); [Brian Shaw](#); [Céline Finney](#); [Kristin Wing](#)
Subject: RE: FHWA Project: Des. No. 2200148; Park Road Bridge Project, HPSR & HBAA, Jennings County, Indiana.
Date: Thursday, March 14, 2024 2:36:42 PM
Attachments: [image001.jpg](#)

This Message Is From an External Sender

This message came from outside your organization.

[Report Suspicious](#)

Paul,

Thank you for your response to the HPSR. Your response has been forwarded to INDOT CRO. As the project continues to develop, we continue to look forward to your input.

Thank you,

Alyssa Reynolds

Sent from my iPhone

From: Paul Brandenburg <paul@prbrandy.com>
Sent: Thursday, March 14, 2024 1:17 PM
To: Alyssa Reynolds <adreynolds@crai-ky.com>
Subject: RE: FHWA Project: Des. No. 2200148; Park Road Bridge Project, HPSR & HBAA, Jennings County, Indiana.

Alyssa – Indiana Historic Spans Taskforce is pleased to see the preliminary preferred alternative for this project is identified as: B1 Rehabilitation for Continued Vehicular Use Meeting the SOI Standards for rehabilitation and look forward to continued consultation during the project development.

Cheers,

Paul Brandenburg

From: Alyssa Reynolds <adreynolds@crai-ky.com>
Sent: Monday, March 4, 2024 2:10 PM
To: 'Laura Renwick' <LRenwick@indianalandmarks.org>; 'Nathan Holth' <nathan@historicbridges.org>; spansaver@hotmail.com; Paul Brandenburg <paul@prbrandy.com>

Cc: 'Brinker, Haley' <HBrinker@indot.IN.gov>; 'Branigin, Susan' <SBranigin@indot.IN.gov>; 'Coon, Matthew' <mcoon@indot.IN.gov>; 'Elizabeth Gallow' <eagallow@crai-ky.com>; 'Alfred Wessling' <awessling@b-l-n.com>; 'Brian Shaw' <bshaw@b-l-n.com>; 'Celine Finney' <cfinney@b-l-n.com>; 'Walker, Matthew C' <MatWalker@indot.IN.gov>; 'Dye, David' <DDYE@indot.IN.gov>; 'Kristin Wing' <kwing@b-l-n.com>

Subject: FHWA Project: Des. No. 2200148; Park Road Bridge Project, HPSR & HBAA, Jennings County, Indiana.

Hello,

Des. No. 2200148

Project Description: Bridge Project, Bridge No. P000-40-07088 (NBI No. 060380)

Location: Park Road over Muscatatuck River, Crosley Fish and Wildlife Area, Jennings County, Indiana

The Indiana Department of Natural Resources (IDNR) and the Indiana Department of Transportation (INDOT), with funding from the Federal Highway Administration (FHWA), propose to proceed with the Park Road bridge project (Des. No. 2200148) in the Crosley Fish and Wildlife Area, Jennings County, Indiana.

As part of Section 106 of the National Historic Preservation Act, a Historic Property Short Report (HPSR) and Historic Bridge Alternatives Analysis (HBAA) have been prepared and are ready for review and comment by consulting parties.

Please review the above mentioned documents, which are located in IN SCOPE at <https://erms12c.indot.in.gov/Section106Documents/>

(the Des. No. is the most efficient search term, once in IN SCOPE), and respond with your comments on any historic resource impacts incurred as a result of this project so that an environmental report can be completed. We also welcome your related opinions and other input to be considered in the preparation of the environmental document. If a hard copy of the materials is needed, please respond to this email with your request within seven (7) days.

Consulting parties have thirty (30) calendar days from receipt of this information to review and provide comment.

Tribal Contacts please respond to INDOT's Acting Tribal Liaison, Matt Coon at mcoon@indot.in.gov (317-697-9752) with any responses pertaining to this project including to provide INDOT/Indiana FHWA additional information about Tribal resources/concerns and questions/comments regarding cultural resources. The FHWA point of contact is Kari Carmany-George at K.CarmanyGeorge@dot.gov (317-226-5629).

Thank you in advance for your input,

Alyssa Reynolds
Architectural Historian

Division of Historic Preservation & Archaeology · 402 W. Washington Street, W274 · Indianapolis, IN 46204-2739
Phone 317-232-1646 · Fax 317-232-0693 · dhpa@dnr.IN.gov ·



April 3, 2024

Alyssa Reynolds
Architectural Historian
Cultural Resource Analysts, Inc. ("CRA, Inc.")
201 NW 4th Street, Suite 204
Evansville, Indiana 47708

State Agency: Indiana Department of Transportation ("INDOT"),
Federal Agency: Federal Highway Administration, Indiana Division ("FHWA")

Re: DUAL REVIEW: Historic property short report (Reynolds, 2/28/2024), and historic bridge alternative analysis report, for the Park Road Bridge (Jennings County Bridge No. P000-40-07088; National Bridge Inventory "NBI" No. 60380) over Muscatatuck River Project (Des. No. 2200148; DHPA No. 31257)

Dear Ms. Reynolds:

Pursuant to Section 106 of the National Historic Preservation Act of 1966, as amended (54 U.S.C. § 306108); implementing regulations at 36 C.F.R. Part 800; the "Programmatic Agreement Among the Federal Highway Administration, the Indiana Department of Transportation, the Indiana Historic Preservation Officer, and the Advisory Council on Historic Preservation Regarding Management and Preservation of Indiana's Historic Bridges" ("Indiana Historic Bridges PA"); and the "Programmatic Agreement (PA) Among the Federal Highway Administration, the Indiana Department of Transportation, the Advisory Council on Historic Preservation and the Indiana State Historic Preservation Officer Regarding that Implementation of the Federal Aid Highway Program In the State of Indiana" ("Indiana Minor Projects PA"); and also pursuant to Indiana Code 14-21-1-18 and 312 Indiana Administrative Code ("IAC") 20-4, the staff of the Indiana State Historic Preservation Officer ("Indiana SHPO") has reviewed your March 4, 2024, submission which included the historic property short report ("HPSR", Reynolds, 2/28/2024) and the historic bridge alternative analysis ("HBAA") report, received by our office March 4, 2024, for the Park Road Bridge over Muscatatuck River Project.

The area of potential effects ("APE") proposed in the HPSR appears to be of adequate size to encompass the geographic area in which direct and indirect effects of a project of this nature could occur.

We note that the HPSR states that the Indiana Historic Sites and Structure Inventory (IHSSI) was reviewed and found that there are no previously recorded resources within the APE. However, based on the information available to our staff, we note that subject bridge, truss bridge (P000-40-07088 B; National Bridge Inventory [NBI] Number [No.] 060380), was identified in the 1989 Jennings County Survey as IHSSI # 079-646-40018 Crosley Bridge and is rated "Outstanding" given its significance in the areas of Engineering and Transportation. We believe this IHSSI identification is part of the reasoning for this bridge being included in the Indiana Historic Bridge Inventory (IHBI, 2010), as the IHBI states "This bridge has been previously listed or determined eligible for listing in the National Register or is a contributing resource within a historic district listed in the National Register."

Subsequently, we agree with the HPSR that the APE contains only one resource, Crosley Bridge (IHSSI # 079-646-40018; P000-40-07088 B; NBI No. 060380), identified as being eligible for listing in the NRHP under Criterion C for its unique architectural and engineering design, in addition to transportation, as a narrow-width, three-span, Pratt-through and Warren

pony truss bridge. Additionally, we agree the bridge was identified in the IHBI as being previously determined eligible for the NRHP and was designated as a "Non-Select" bridge.

In regard to the HBAA provided, our staff has reviewed the documentation and the two proposed alternatives. We appreciate the Historic Bridge Alternatives Analysis ("HBAA") which provides consulting parties with a more thorough description and analysis of the current condition of the bridge and the proposed work for this federal undertaking.

We understand and agree with the conclusions of the Historic Bridge Alternatives Analysis (HBAA) that the preferred alternative for this project is Alternative B1 Rehabilitation of the Existing Bridge for Continued Vehicular Use (Two-Way [One Lane] Option) Meeting Secretary of Interior's Standards (SOIS) for Rehabilitation, which meets the purpose and need of the proposed project. It is our understanding that this alternative would involve rehabilitation of the existing structure. It is our understanding that the scope of work for the rehabilitation will include:

- Cleaning and painting the truss. The paint color will match the existing color and the bridge rail will be painted to match the truss.
- Replacing the bridge deck in all spans with a timber plank deck that has timber running boards.
- Repairing or replacing in kind some verticals, gusset plates, and low chord truss members or connections.
- Replacing some miscellaneous truss secondary members in kind such as bottom lateral bracing and lattice.
- Deteriorated concrete abutments and pier caps will be removed and reconstructed.
- Delaminated and spalled areas of abutments and piers will be patched.
- Placing scour countermeasures at all substructure units.
- Rivets removed to make necessary repairs will be replaced with round headed bolts wherever possible. The rounded head will be installed on the face of the member that is most visible from the roadway or exterior face of member.
- Replacing the bridge rail with a steel handrail.

Specifically, in relationship to the replacement of the bridge rail with a steel handrail, we appreciate the example provided. We request that as rehabilitation and design plans are developed further, that the final design of the steel handrail be submitted to our office for review.

Additionally, it is clear why Alternative A—No Build/Do Nothing—is not a preferred alternative, and the SHPO acknowledges the reasons given as to why this alternative is not considered prudent.

As referenced in Section V.C of the HBAA, we request that this bridge be photographically documented. We request color, digital images that provide overviews of the resource, along with detailed shots of character-defining features. In addition to the photographs, please provide a photo log that corresponds to the photographs, a photo key, and an overview thumbnail sheet. Please submit a draft copy of this documentation on CD, flash drive, or any other previously approved storage device for our review and approval. Upon approval, this documentation should be provided to a public or not-for-profit organization that is willing to accept a copy of this documentation and make it available to the public. Please inform us which local or not-for-profit organization is willing to accept this documentation.

Additionally, as previously indicated, in regard to archaeological resources, based upon the documentation available to the staff of the Indiana SHPO, no currently known archaeological resources eligible for inclusion in the National Register of Historic Places ("NRHP") have been recorded within the proposed project area. No archaeological investigations appear necessary provided that all project activities remain within areas disturbed by previous construction. Please be advised that archaeological resources may exist underneath modern development.

If any prehistoric or historic archaeological artifacts or human remains are uncovered during construction, demolition, or earthmoving activities, state law (Indiana Code 14-21-1-27 and Indiana Code 14-21-1-29) requires that the discovery must be reported to the State of Indiana Department of Natural Resources, Division of Historic Preservation and Archaeology within two (2) business days. In that event, please call (317) 232-1646. Be advised that adherence to Indiana Code 14-21-1-27 and Indiana Code 14-21-1-29 does not obviate the need to adhere to applicable federal statutes and regulations, including but not limited to 36 C.F.R. 800.

If ground disturbing activities will be within 100 feet of any cemetery, please be aware of the cemetery development plan requirements in Indiana Code 14-21-1-26.5 (<http://iga.in.gov/legislative/laws/2016/ic/titles/014/articles/021/chapters/001/#section-26.5>). All cemeteries must be avoided by all project activities, and provisions of relevant state statutes regarding cemeteries (including IC 14-21-1 and IC 23-14) must

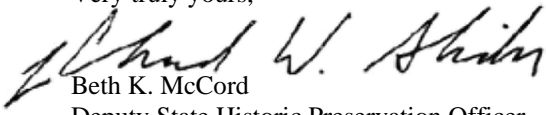
be adhered to. Please also be aware of Indiana Code 23-14-44-1 and Indiana Code 23-14-44-2, regarding restrictions on roads and utility construction in cemeteries.

For the benefit of those recipients of a copy of this letter who are not Section 106 consulting parties, please be aware that a copy of INDOT's August 25, 2023, letter can be found online at <https://erms12c.indot.in.gov/Section106Documents/>. From there, search by this project's designation number: 2200148.

If you have questions regarding our dual review of the aforementioned project, please contact DNR-DHPA. Questions about archaeological issues should be directed to contact at Wade T. Tharp at (317) 232-1650 or wtharp1@dnr.in.gov. Questions about historic buildings or structures pertaining to this review should be directed to Toni Lynn Giffin at (317) 233-2803 or tgiffin@dnr.IN.gov.

In all future correspondence regarding the dual review of this Park Road Bridge over Muscatatuck River Project, in Vernon Township, Jennings County, Indiana (Des. No. 2200148), please refer to DHPA No. 31257.

Very truly yours,



Beth K. McCord
Deputy State Historic Preservation Officer
Director, Division of Historic Preservation and Archaeology

BKM:TLG:WTT:wt

EMC: Patrick Carpenter, Federal Highway Administration
Matt Coon, Ph.D., Indiana Department of Transportation
Susan Branigin, Indiana Department of Transportation
Alyssa Reynolds, Cultural Resource Analysts, Inc.
Toni Lynn Giffin, Indiana DNR-DHPA
Wade T. Tharp, Indiana DNR-DHPA

EMC to Indiana Historic Preservation Review Board Members:
J. Scott Keller, Indiana Historic Preservation Review Board
Daniel Kloc, AIA, Indiana Historic Preservation Review Board
Chandler Lighty, Indiana Historic Preservation Review Board
Anne Shaw, Indiana Historic Preservation Review Board
April Sievert, Ph.D., Indiana Historic Preservation Review Board
Jason Larrison, AIA, Indiana Historic Preservation Review Board
Ryan Mueller, Deputy Director, Indiana Department of Natural Resources
Beth McCord, Division of Historic Preservation and Archaeology

EMC to potentially interested persons:
Jomary Baller, Project Manager, Indiana Department of Natural Resources
Ben Clark, Chief of Cultural Resources of State Parks, Indiana Department of Natural Resources
Lucas Green, South Region Manager, Indiana Department of Natural Resources
David Nance, Professional Geologist, Indiana Department of Natural Resources
Chad Springer Assistant Property Manager, Indiana Department of Natural Resources, Crosley Fish and Wildlife Area
Nathan Holth, HistoricBridges.org
Tony Dillon, Historic Hoosier Bridges
Pal Brandenburg, Chair, Historic Spans Task Force
Greg Sekula, Director, Indiana Landmarks Southern Regional Office
Sarah Abel, Jennings County Auditor
Jennings County Commissioners
Jim Reeves, Jennings County Highway Superintendent
Chris Asher, President Jennings County Historical Society
Susan Craig, Executive Director - Southeastern Indiana Regional Planning Commission



INDIANA DEPARTMENT OF TRANSPORTATION

100 North Senate Avenue
Room N758-ES
Indianapolis, Indiana 46204

PHONE: (855) 463-6848

Eric Holcomb, Governor
Michael Smith, Commissioner

August 14, 2024

This letter was sent to the listed parties.

RE: Dual Review FHWA Project: Des. No. 2200148; DHPA No. 31257; Park Road Bridge Project,
Jennings County, Indiana

Dear Consulting Party (see attached list),

The Indiana Department of Natural Resources (IDNR) and the Indiana Department of Transportation (INDOT), with funding from the Federal Highway Administration (FHWA), propose to proceed with the Park Road bridge project (Des. No. 2200148).

This letter is part of the Section 106 review process for this project. Section 106 of the National Historic Preservation Act (NRHP) requires federal agencies to take into account the effects of their undertakings on historic properties. We are requesting comments from you regarding the possible effects of this project. Please use the above Des. Number and project description in your reply and your comments will be incorporated into the formal environmental study.

A Section 106 early coordination letter (ECL) was distributed on August 25, 2023. In addition, a letter distributed on March 4, 2024 notified consulting parties that a historic property short report (HPSR), archaeological assessment, and Historic Bridge Alternatives Analysis (HBAA) was available for review and comment.

The proposed bridge undertaking is located within the Crosley Fish and Wildlife Area, established in 1931, in Jennings County, Indiana. More specifically the project area is within Vernon Township as shown on the Vernon, Indiana USGS Topographic Quadrangle, in Section 15, Township 6 N, Range 8 E. The project area along Park Road encompasses approximately 1.0 acre within the Crosley Fish and Wildlife Area. The project area can be viewed online at <https://arcg.is/jqueP> (the Des. No. is the most efficient search term once in the CRO - Public Web Map App).

The proposed work will occur on the Park Road bridge (P000-40-07088 B; National Bridge Inventory [NBI] Number [No.] 060380) over Muscatatuck River. The existing bridge is an approximately 212 feet by 12.3 feet, three-span, truss bridge constructed in 1910. The main span of the bridge is a steel Pratt-through truss and the approach spans are both steel Warren pony trusses. A bridge rehabilitation that consisted of a new deck with some new stringers, as well as cleaning and painting of the truss, was completed in 1979. The bridge was rehabilitated in 2004 when the deck was replaced with new wood planks and the truss was cleaned and painted (Des. No. 0300801). The gusset plates at abutment number (no.) 1 were replaced (Des. No. 1400643) in 2015 along with a portion of the south span deck. The bridge was closed in 2019 due to holes and deterioration in the timber deck. According to the *Indiana Historic Bridge Inventory*, completed in 2010, the bridge was determined eligible for listing in the National Register of Historic Places (NRHP) under Criterion C. It was also determined

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to be a “Non-Select” bridge, which are those that may not be considered excellent examples of a given type of historic bridge or may not be as suitable candidates for preservation.

All proposed work is assumed to be located within the roadway or adjacent to the roadway and the bridge. The proposed scope of work is to rehabilitate the bridge.

The proposed work will include:

- Cleaning and painting the truss. The paint color will match the existing color and the bridge rail will be painted to match the truss.
- Replacing the bridge deck in all spans with a timber plank deck that has timber running boards.
- Repairing or replacing in kind some verticals, gusset plates, and low chord truss members or connections.
- Replacing some miscellaneous truss secondary members in kind such as bottom lateral bracing and lattice.
- Removing and reconstructing deteriorated concrete abutment and pier caps.
- Patching delaminated and spalled areas of abutments and piers.
- Placing scour countermeasures at all substructure units.
- Rivets removed to make necessary repairs will be replaced with round headed bolts wherever possible. The rounded head will be installed on the face of the member that is most visible from the roadway or exterior face of member.
- Replacing the bridge rail with a steel hand rail.
- Some trees may need to be removed to install the scour countermeasures. All other nearby trees will be trimmed to keep branches away from truss and vehicles.
- Clean the abutment and pier bridge seats of all debris.

The clear roadway width will be slightly larger due to the installation of the new steel handrail on the bridge. The approach guardrail will not be modified. No approach roadway work will be completed as part of this project, except that portions of the south approach guardrail will be removed and reset if necessary to complete the scour countermeasure work. The scour countermeasure will consist of:

- Placing either dumped, partially grouted revetment riprap or an articulated concrete mattress on the south spillslope;
- Placing Class 1 riprap at a 20 feet width around the north abutment and wings;
- A concrete underpin will be placed around Pier 2 (south pier);
- Placing dumped partially grouted Class 2 riprap around three (3) sides (channel side and each pier nose) of Pier 3 with regular buried Class 2 riprap on the north side of Pier 3.

No right-of-way (ROW) acquisition is occurring as the project takes place within an IDNR property.

The road will remain closed during construction, which is anticipated to be less than 120 days. As the bridge is currently closed and the roadway runs through the Crosley Fish and Wildlife Area, no detour signs will be placed along the roadway.

The project need is based on the current condition of the structure elements and notable deficiencies in the wearing surface, deck, superstructure, and substructure, and the scour critical status of the bridge. The bridge's deck deterioration caused it to be closed to traffic in 2019. The purpose of this project is to correct the deficiencies in the bridge wearing surface, deck, superstructure, and substructure (scour protection) to reopen the bridge to vehicular traffic, and protect the bridge and extend its service life.

Cultural Resource Analysts, Inc. (CRA) is under contract with INDOT to advance the Section 106 documentation for the referenced project. Egis Beam, Longest, & Neff (BLN) USA, Inc. (Egis) will be completing the environmental documentation for the referenced project.

In accordance with 36 CFR 800.2 (c), you were invited to become a consulting party as part of the Section 106 process, or you are hereby invited to become a consulting party as part of the Section 106 process. Entities that have previously accepted consulting party status – as well as additional entities that are currently being invited to become consulting parties – are identified in the attached list.

The Section 106 process involves efforts to identify historic properties potentially affected by the undertaking, assess its effects and seek ways to avoid, minimize or mitigate any adverse effects on historic properties. For more information regarding the protection of historic resources, please see the Advisory Council on Historic Preservation’s guide: *Protecting Historic Properties: A Citizen’s Guide to Section 106 Review* available online at <https://www.achp.gov/sites/default/files/documents/2017-01/CitizenGuide.pdf>.

Per the terms of the “Programmatic Agreement Regarding Management and Preservation of Indiana’s Historic Bridges” (Historic Bridges PA), the FHWA-Indiana Division will satisfy its Section 106 responsibilities involving “Select” and “Non-Select” bridges through the Project Development Process (PDP) of the Historic Bridges PA (Stipulation III). Because the bridge (P000-40-07088; NBI No. 60380) is a “Non-Select” bridge, the procedures outlined in Stipulation III.B. of the Historic Bridges PA will be followed to fulfill FHWA’s Section 106 responsibilities for the project. (A copy of the Historic Bridges PA can be downloaded here: <http://www.in.gov/indot/2530.htm>).

Please note that per the permanent rule issued by the Indiana Department of Natural Resources effective August 14, 2013 (312 IAC 20-4-11.5), INDOT is requesting that this project be subjected to “dual review”; that is, reviewed by the Division of Historic Preservation and Archaeology simultaneously under 54 U.S.C. 306108 (Section 106) and IC 14-21-1-18 (Indiana Preservation and Archaeology Law dealing with alterations of historic sites and structures requiring a Certificate of Approval). Pursuant to Section 11.5(f) of this rule, at the conclusion of the review process we anticipate that the Division Director would issue a letter of clearance exempting this project from obtaining a Certificate of Approval under IC 14-21-1-18.

The Area of Potential Effects (APE) is the area in which the proposed project may cause alterations in the character or use of historic resources. The APE contains no resources listed in the National Register of Historic Places (NRHP).

A historian who meets the Secretary of the Interior’s Professional Qualification Standards conducted an above-ground survey of resources within the APE for potential eligibility for the National Register of Historic Places (NRHP). As a result of the historic property identification and evaluation efforts, Bridge No. P00-40-07088 B (NBI No. 60380) is recommended eligible for listing in the NRHP under Criterion C for its unique architectural and engineering design.

An archaeologist who meets the Secretary of the Interior’s Professional Qualification Standards reviewed the proposed project area and prepared an archaeological assessment recommending the project be allowed to proceed without additional archaeological study.

On March 14, 2024, Paul Brandenburg of the Indiana Historic Spans Taskforce, responded to the HPSR, archaeological assessment, and HBAA, stating that he was, “...pleased to see the preliminary preferred alternative for this project is identified as: B1 Rehabilitation for Continued Vehicular Use Meeting the SOI Standards for rehabilitation and look forward to continued consultation during the project development.”

The SHPO responded to the HPSR, archaeological assessment, and HBAA in a letter dated April 3, 2024. They agreed with the size of the APE for the HPSR. They also noted that the subject bridge (P000-40-07088 B; NBI No. 060380), "...was identified in the 1989 Jennings County Survey as IHSSI # 079-646-40018 Crosley Bridge and is rated "Outstanding" given its significance in the areas of Engineering and Transportation. We believe this IHSSI identification is part of the reasoning for this bridge being included in the Indiana Historic Bridge Inventory (IHBI, 2010), as the IHBI states "This bridge has been previously listed or determined eligible for listing in the National Register or is a contributing resource within a historic district listed in the National Register."

"Subsequently, we agree with the HPSR that the APE contains only one resource, Crosley Bridge (IHSSI # 079-646-40018; P000-40-07088 B; NBI No. 060380), identified as being eligible for listing in the NRHP under Criterion C for its unique architectural and engineering design, in addition to transportation, as a narrow-width, three-span, Pratt-through and Warren pony truss bridge. Additionally, we agree the bridge was identified in the IHBI as being previously determined eligible for the NRHP and was designated as a "Non-Select" bridge."

They also agreed with the recommendation of Alternative B1 Rehabilitation of the Existing Bridge for Continued Vehicular Use (Two-way [One-Lane] Option) as being the preferred alternative for the project.

The SHPO also stated that, "As referenced in Section V.C of the HBAA, we request that this bridge be photographically documented."

In regard to archaeological resources, the SHPO stated, "Additionally, as previously indicated, in regard to archaeological resources, based upon the documentation available to the staff of the Indiana SHPO, no currently known archaeological resources eligible for inclusion in the National Register of Historic Places ("NRHP") have been recorded within the proposed project area. No archaeological investigations appear necessary provided that all project activities remain within areas disturbed by previous construction. Please be advised that archaeological resources may exist underneath modern development"

The photo documentation of the bridge requested by SHPO has been submitted to them in conjunction with this finding.

No further responses were received regarding the HPSR, archaeological assessment, or HBAA.

The finding of "No Historic Properties Affected" and associated 800.11(d) documentation, as well as the 30% Plans (located in Appendix F of the 800.11(d)), are available for review in IN SCOPE at <http://erms12c.indot.in.gov/Section106Documents/> (the Des. No. is the most efficient search term, once in IN SCOPE). You are invited to review these documents and to respond with comments on any historic resource impacts incurred as a result of this project so that an environmental report can be completed. We also welcome your related opinions and other input to be considered in the preparation of the environmental document. If a hard copy of the materials is needed, please respond to this email with your request within seven (7) days.

Please review the information and comment within thirty (30) calendar days of receipt. If you indicate that you do not desire to be a consulting party or if you have not previously accepted consulting party status and you do not respond to this letter, you will not be included on the list of consulting parties for this project and will not receive further information about the project unless the design changes.

For questions concerning specific project details, you may contact Alyssa Reynolds of Cultural Resource Analysts, Inc. (CRA) at (812) 253-3009 or adreynolds@crai-ky.com. All future responses regarding the proposed project should be forwarded to CRA at the following address:

Alyssa Reynolds
Architectural Historian
Cultural Resource Analysts, Inc. (CRA)
201 NW 4th Street, Suite 204
Evansville, Indiana 47708
adreynolds@crai-ky.com

Tribal Contacts please respond to INDOT's Acting Tribal Liaison, Matt Coon mcoon@indot.in.gov; (317-697-9752) with any responses pertaining to this project including to provide INDOT/Indiana FHWA additional information about Tribal resources/concerns and questions/comments regarding cultural resources. The FHWA point of contact is Kari Carmany-George at K.CarmanyGeorge@dot.gov (317-226-5629).

Sincerely,



Matthew S. Coon, Manager
Cultural Resources Office
Environmental Services

Enclosures: Consulting Party Acceptance List and Finding of "No Historic Properties Affected" (with ~~30% Plans [Appendix F]~~)

The plan set has been removed to avoid duplication and can be found in Appendix B.

Accepted Consulting Parties:

**Indiana Department of Natural Resources,
Division of Historic Preservation & Archaeology,
Indiana State Historic Preservation Office
(SHPO)**

402 West Washington Street, Room W274
Indianapolis, Indiana 46204

Indiana Landmarks Southern Regional Office

Laura Renwick

lrenwick@indianalandmarks.org

HistoricBridges.org

Nathan Holth

{nathan@historicbridges.org}

Historic Hoosier Bridges

Tony Dillon

{spansaver@hotmail.com}

Historic Spans Task Force

Paul Brandenburg, Chair

{paul@prbrandy.com}

Eastern Shawnee Tribe of Oklahoma

Miami Tribe of Oklahoma

Invited Tribal Parties:

Osage Nation

Peoria Tribe of Indians of Oklahoma

Pokagon Band of Potawatomi Indians

Shawnee Tribe

**SECTION 4(F) COMPLIANCE REQUIREMENTS (for historic properties) AND
SECTION 106 FINDINGS AND DETERMINATIONS
AREA OF POTENTIAL EFFECT
ELIGIBILITY DETERMINATIONS
EFFECT FINDING**

**PARK ROAD BRIDGE PROJECT
JENNINGS COUNTY, INDIANA
DES. NO. 2200148
DHPA NO. 31257**

**AREA OF POTENTIAL EFFECTS
(Pursuant to 36 CFR Section 800.4(a)(1))**

Given the nature of the proposed project, the area of potential effects (APE) was determined to include the proposed project area and a 250-foot buffer zone based on topography and vegetation surrounding the proposed bridge project location. Visual impacts were the driving factor in the creation of the APE. The APE takes into account the potential direct and indirect effects of the proposed project within the immediate contextual setting, which is comprised of a gradually sloping, heavily forested area surrounding the bridge. The APE for archaeology is the project footprint. The project location and the APE can be found in Appendix A (maps).

**ELIGIBILITY DETERMINATIONS
(Pursuant to 36 CFR 800.4(c)(2))**

Bridge (P000-40-07088 B; National Bridge Inventory [NBI] Number [No.] 060380) – The three-span truss bridge carries Park Road over Muscatatuck River and was constructed in 1910. The main span of the bridge is a steel Pratt-through truss and the approach spans are both steel Warren pony trusses. The bridge was determined eligible for listing in the National Register of Historic Places (NRHP) under Criterion C for its unique architectural and engineering design. It was also determined to be a “Non-Select” bridge, which are those that may not be considered excellent examples of a given type of historic bridge or may not be as suitable candidates for preservation.

EFFECT FINDING

Per the terms of the “Programmatic Agreement Regarding Management and Preservation of Indiana’s Historic Bridges” (Historic Bridges PA), the Federal Highway Administration—Indiana Division (FHWA) will satisfy its Section 106 responsibilities involving “Select” and “Non-Select” bridges through the Project Development Process (PDP) of the Historic Bridges PA (Stipulation III). Bridge P000-40-07088 B has been classified as a “Non-Select” bridge by the INDOT Historic Bridge Inventory and, thus, the procedures outlined in Stipulation III.B of the Historic Bridges PA will be followed to fulfill FHWA’s Section 106 responsibilities for the bridge.

Therefore, the finding for this project only applies to other resources located within the APE and not Bridge P000-40-07088 B. This document will satisfy the Section 106 responsibilities for other resources located in the APE. Regarding other resources located in the project area, the Indiana Department of Transportation (INDOT), acting on FHWA's behalf has determined a "No Historic Properties Affected" finding is appropriate for this undertaking.

INDOT, acting on behalf of the FHWA, has determined a "No Historic Properties Affected" finding is appropriate for this undertaking.

INDOT respectfully requests the Indiana State Historic Preservation Officer provide a written concurrence with the Section 106 determination of "No Historic Properties Affected."

SECTION 4(F) COMPLIANCE REQUIREMENTS (for historic properties)

Bridge (P000-40-07088 B; NBI No. 060380) –This resource is used for transportation purposes and no Section 4(f) conversion will take place with this project; therefore, no Section 4(f) evaluation must be completed for Bridge (P000-40-07088 B; NBI No. 060380).



Matthew S. Coon, Manager
Cultural Resource Offices
Environmental Services

August 14, 2024

Approved Date

**FEDERAL HIGHWAY ADMINISTRATION
DOCUMENTATION OF SECTION 106 FINDING OF
NO HISTORIC PROPERTIES AFFECTED
SUBMITTED TO THE STATE HISTORIC PRESERVATION OFFICER
PURSUANT TO 36 CFR Section 800.4(d)(1)**

**PARK ROAD BRIDGE PROJECT
JENNINGS COUNTY, INDIANA
DES. NO. 2200148
DHPA NO. 31257**

1. DESCRIPTION OF THE UNDERTAKING

The Indiana Department of Natural Resources (IDNR) and the Indiana Department of Transportation (INDOT), with funding from the Federal Highway Administration (FHWA), propose to proceed with the Park Road bridge project (Des. No. 2200148). The proposed project is located within the Crosley Fish and Wildlife Area in Jennings County, Indiana. It is within Vernon Township as shown on the Vernon, Indiana USGS Topographic Quadrangle, in Section 15, Township 6 N, Range 8 E. The project area along Park Road encompasses approximately 1.0 acre within the Crosley Fish and Wildlife Area (see Appendix A [maps]). The 212-foot by 12.3-foot, three-span truss bridge (P000-40-07088 B; National Bridge Inventory [NBI] Number [No.] 060380) carries Park Road over Muscatatuck River.

The need for this project is based on the current condition of the structure elements and notable deficiencies in the wearing surface, deck, superstructure, and substructure, and the scour critical status of the bridge. The bridge's deck deterioration caused it to be closed to traffic in 2019. The purpose of this project is to correct the deficiencies in the bridge wearing surface, deck, superstructure, and substructure (scour protection) to reopen the bridge to vehicular traffic, protect the bridge, and extend its service life.

The proposed work is summarized below, with a more detailed description in the Historic Bridge Alternatives Analysis (HBAA) document, which can be found in IN SCOPE: <https://erms12c.indot.in.gov/Section106Documents/>. All proposed work is assumed to be located within the roadway or adjacent to the roadway and the bridge. The proposed scope of work is to rehabilitate the bridge.

The proposed work will include:

- Cleaning and painting the truss. The paint color will match the existing color and the bridge rail will be painted to match the truss.
- Replacing the bridge deck in all spans with a timber plank deck that has timber running boards.
- Repairing or replacing in kind some verticals, gusset plates, and low chord truss members or connections.
- Replacing some miscellaneous truss secondary members in kind such as bottom lateral bracing and lattice.
- Removing and reconstructing deteriorated concrete abutment and pier caps.

- Patching delaminated and spalled areas of abutments and piers.
- Placing scour countermeasures at all substructure units.
- Rivets removed to make necessary repairs will be replaced with round headed bolts wherever possible. The rounded head will be installed on the face of the member that is most visible from the roadway or exterior face of member.
- Replacing the bridge rail with a steel hand rail.
- Some trees may need to be removed to install the scour countermeasures. All other nearby trees will be trimmed to keep branches away from truss and vehicles.
- Clean the abutment and pier bridge seats of all debris.

The clear roadway width will be slightly larger due to the installation of the new steel handrail on the bridge. The approach guardrail will not be modified. No approach roadway work will be completed as part of this project, except that portions of the south approach guardrail will be removed and reset if necessary to complete the scour countermeasure work. The scour countermeasure will consist of:

- Placing either dumped, partially grouted revetment riprap or an articulated concrete mattress on the south spillslope;
- Placing Class 1 riprap at a 20 feet width around the north abutment and wings;
- A concrete underpin will be placed around Pier 2 (south pier);
- Placing dumped partially grouted Class 2 riprap around three (3) sides (channel side and each pier nose) of Pier 3 with regular buried Class 2 riprap on the north side of Pier 3.

No right-of-way (ROW) acquisition is occurring as the project takes place within an IDNR property.

Given the nature of the proposed project, the APE was determined to include the proposed project area and a 250-foot buffer zone based on topography and vegetation surrounding the proposed work. Visual impacts were the driving factor in the creation of the APE. The APE takes into account the potential direct and indirect effects of the proposed project within the immediate contextual setting, which is comprised of a gradually sloping, heavily forested area surrounding the bridge. The APE for archaeology is the project footprint. The project location and the APE can be found in Appendix A (maps).

The road and bridge will remain closed during construction, which is anticipated to be less than 120 days. As the bridge is currently closed and the roadway runs through the Crosley Fish and Wildlife Area, no detour signs will be placed along the roadway.

2. EFFORTS TO IDENTIFY HISTORIC PROPERTIES.

The National Register of Historic Places (NRHP), Indiana Register of Historic Sites and Structures (State Register), the State Historic Architectural and Archaeological Research Database (SHAARD), the 1989 *Jennings County Interim Report*, and Indiana Historic Building, Bridges, and Cemeteries (IHBBC) Map were consulted. CRA also reviewed the *Indiana Historic Bridge Inventory* (IHBI) (Mead and Hunt 2010), as part of the records review. Bridge P000-40-07088 B was identified as previously determined eligible for listing in the NRHP. It was also

determined to be a “Non-Select” bridge, which are those that may not be considered excellent examples of a given type of historic bridge or may not be as suitable candidates for preservation.

As a result of the historic property identification and evaluation efforts, Bridge P000-40-07088 B was identified and evaluated. CRA recommends the bridge as continuing to be eligible for listing in the NRHP.

With regards to archaeological resources, Andrew Martin, a qualified professional archaeologist who meets the Secretary of the Interior’s Professional Qualification Standards, conducted an archaeological records review of Indiana SHAARD records. It is understood that ground disturbances associated with the project as planned appear to be on existing paved areas, previously disturbed soils adjacent to the roadway, or steeply sloping landforms on the side and underneath the bridge. A review of SHAARD records revealed that there are no previously recorded archaeological sites within or adjacent to the current project area.

Given the project scope and setting, there is a low potential for the proposed project to affect archaeological sites eligible for the NRHP. However, state law (Indiana Code 14-21-1-27 and -29) requires that if any prehistoric or historic archaeological artifacts or human remains are uncovered during construction, demolition, or earth moving activities, that the discovery must be reported to the Indiana Department of Natural Resources IDNR within two (2) business days.

A Section 106 early coordination letter (ECL) was distributed on August 25, 2023. A list of the invited consulting parties is as follows and can be found in Appendix C (consulting party list):

- Indiana State Historic Preservation Officer (SHPO)
- Crosley Fish and Wildlife Area
- HistoricBridges.org
- Historic Hoosier Bridges
- Historic Spans Task Force
- Indiana Department of Natural Resources (IDNR)
- Indiana Landmarks-Southern Regional Office
- Jennings County Auditor
- Jennings County Commissioners
- Jennings County Highway Superintendent
- Jennings County Historical Society
- Southern Indiana Regional Planning Commission
- Delaware Tribe of Indians
- Eastern Shawnee Tribe of Oklahoma
- Miami Tribe of Oklahoma
- Osage Nation
- Peoria Tribe of Indians of Oklahoma
- Pokagon Band of Potawatomi Indians
- Shawnee Tribe

On August 25, 2023, Nathan Holth of HistoricBridges.org, responded to the ECL via email. He stated, “I would like to express strong support for the preservation of this bridge. I am baffled that the bridge is considered "Non-Select" as this bridge is an unusually narrow highway bridge and that speaks to the bridge's original owner... which as I understand accommodated [accommodated] a retreat for famous businessman and inventor Powel Crosley Jr. who is noted in America's automotive history as an early promotor of compact cars. My understanding is that this bridge was specifically built to Crosely's [Crosley's] specifications with a narrow deck as Crosley had no need of a wider bridge to accomodate [accommodate] his compact cars. It is therefore significant beyond its engineering design, but as a remarkable and unique surviving bridge that embodies this famous/noted man's interest in compact cars and is a tangible remnant of this retreat which is indeed noted as "Crosley Fish and Wildlife Area" and in my view is a historic centerpiece to the area. When I visited this bridge, I was not fully aware of this history. It would be nice to see interpretive signage added as part of this project describing Crosley and the bridge to make its significance clear to visitors.”

Mary Kennedy (INDOT Cultural Resource Office [CRO] staff) responded to Holt's email on August 29, 2023, stating that he was added to the list of consulting parties for the project. Additionally, she responded by stating, “With regard to the bridge's Non-Select status, that was due to the bridge's low structural capacity and narrow width. The way the analysis fell out with those two items put the bridge on the Non-Select list. This does not mean that the bridge cannot be preserved. In fact, the plan is to preserve it. Given its location in the fish & wildlife area, those limitations are not as big a deal as they would be on a more traveled public roadway. With regard to potential interpretive signage, there is nothing in the Historic Bridges Programmatic Agreement that requires it. That would be up to DNR if they would want to pursue interpretive signage.”

On August 28, 2023, Paul Brandenburg of the Indiana Historic Spans Taskforce, responded to the ECL, stating that the “Indiana Historic SPANs [Spans] Taskforce [Task Force] supports the stated goal of: ‘The proposed scope of work is to rehabilitate the bridge’ as identified in the Early Coordination Letter and wish to remain a consulting party to review the detailed rehabilitation plans.”

Alyssa Reynolds of CRA responded to Brandenburg on August 28, 2023, stating that his response had been forwarded to INDOT CRO staff (Appendix D [correspondence]).

Tony Dillon of Historic Hoosier Bridges responded to the ECL in an email on August 28, 2023. He wished to be a consulting party for the proposed project. Additionally, he also mentioned that “While vetting the Mead & Hunt determinations for Select/Non-Select bridges, Jim Cooper and I had a discussion about this one. While my initial reaction was to lobby for a reclassification, Jim was concerned with a possible backlash given the narrow width. We ultimately decided that it might be best to just let it lay low. I agree with Nathan wholeheartedly about the importance of this span and its preservation. Fortunately, that sentiment is shared by the park staff and preservationists in the area. There is nothing to suggest that anyone wants it gone...I would certainly help lobby for an historic marker once the bridge is rehabilitated.”

Mary Kennedy responded to Dillon's email on August 29, 2023, stating that she, "...copied the consultant as well as DNR staff, so they can be aware of the additional vote for an interpretive sign. We look forward to your continued participation as the project progresses" (Appendix D [correspondence]).

On September 6, 2023, Laura Renwick of Indiana Landmarks Southern Regional Office responded to the ECL via an email stating that they would like to be a consulting party. She also noted that part of their address was incorrectly listed in the ECL. Reynolds responded on September 11, 2023, letting her know that her response had been forwarded to INDOT CRO (Appendix D [correspondence]).

On September 11, 2023, the Miami Tribe of Oklahoma responded to the ECL via a letter. They stated that, "The Miami Tribe offers no objection to the above-referenced project at this time, as we are not currently aware of existing documentation directly linking a specific Miami cultural or historic site to the project site. However, given the Miami Tribe's deep and enduring relationship to its historic lands and cultural property within present-day Indiana, if any human remains or Native American cultural items falling under the Native American Graves Protection and Repatriation Act (NAGPRA) or archaeological evidence is discovered during any phase of this project, the Miami Tribe requests immediate consultation with the entity of jurisdiction for the location of discovery" (Appendix D [correspondence]).

On September 25, 2023, SHPO staff responded to the ECL via a letter stating for Reynolds to include a list of accepted consulting parties in the next correspondence. They also agreed that no archaeological investigations need to occur for the proposed project (Appendix D [correspondence]).

On October 12, 2023, the Eastern Shawnee Tribe responded to the ECL via a letter. They stated that they "...find our people occupied these areas historically and/or prehistorically. However, the project proposes **NO Adverse Effect** or endangerment to known sites of interest to the Eastern Shawnee Tribe. Please continue project as planned. However, should this project inadvertently discover an archeological site or object(s) we request that you immediately contact the Eastern Shawnee Tribe, as well as the appropriate state agencies (within 24 hours). We also ask that all ground disturbing activity stop until the Tribe and State agencies are consulted. Please note that any future changes to this project will require additional consultation" (Appendix D [correspondence]).

No further responses were received regarding the ECL or archaeological assessment.

In August 2023, Cultural Resource Analysts (CRA) conducted a site visit of the APE and documented all above-ground resources that will be 50 years of age or older at the time of project letting (2025). The APE was investigated for the existence of any buildings, structures, objects, or districts listed in or eligible for listing in the NRHP.

As previously mentioned, an archaeological records review of SHAARD records was completed in August 2023. It is understood that ground disturbances associated with the project as planned appear to be on existing paved areas, previously disturbed soils adjacent to the roadway, or

steeply sloping landforms on the side and underneath the bridge. A review of SHAARD records revealed that there are no previously recorded archaeological sites within or adjacent to the current project area.

A Historic Property Short Report (HPSR) was completed (Reynolds, February 28, 2024). CRA recommended that Bridge P000-40-07088 B, a “Non-Select” bridge, continues to be eligible for listing in the NRHP under Criterion C for its unique architectural and engineering design as a narrow-width, three-span, Pratt-through and Warren pony truss bridge. No additional resources were identified as a result of survey efforts. The summary of the HPSR is found in Appendix E.

An HBAA (*Alternatives Analysis Report: Park Road Over Muscatatuck River*) was completed by Egis Beam, Longest, & Neff (BLN) USA, Inc. (Egis) and approved for distribution by the INDOT on March 4, 2024. Two alternatives for the proposed bridge project were discussed, Alternative A: The No-Build/Do-Nothing Alternative and Alternative B.1: Rehabilitation of the Existing Bridge for Continued Vehicular Use (Two-Way [One-Lane] Option) Meeting Secretary of Interior’s Standards (SOIS) for Rehabilitation. Alternative B.1 was recommended as the preliminary preferred alternative by Egis, which proved to be both feasible and prudent, resulting in extending the life of the bridge. The rehabilitation work described in Section 1 of this finding document is discussed in more detail in the HBAA (see Appendix E [report abstracts]).

The HPSR, report distribution letter (RDL), archaeological assessment, and HBAA were sent to consulting parties and SHPO on March 4, 2024. No tribal email was sent.

Paul Brandenburg responded to the HPSR, archaeological assessment, and HBAA in an email dated March 14, 2024. He was pleased that the chosen alternative for the bridge project was to rehabilitate the subject bridge (Appendix D [correspondence]).

The SHPO responded to the HPSR, archaeological assessment, and HBAA in a letter dated April 3, 2024. They agreed with the size of the APE for the HPSR. They also noted that the subject bridge (P000-40-07088 B; NBI No. 060380), “...was identified in the 1989 Jennings County Survey as IHSSI # 079-646-40018 Crosley Bridge and is rated “Outstanding” given its significance in the areas of Engineering and Transportation. We believe this IHSSI identification is part of the reasoning for this bridge being included in the Indiana Historic Bridge Inventory (IHBI, 2010), as the IHBI states “This bridge has been previously listed or determined eligible for listing in the National Register or is a contributing resource within a historic district listed in the National Register.”

“Subsequently, we agree with the HPSR that the APE contains only one resource, Crosley Bridge (IHSSI # 079-646-40018; P000-40-07088 B; NBI No. 060380), identified as being eligible for listing in the NRHP under Criterion C for its unique architectural and engineering design, in addition to transportation, as a narrow-width, three-span, Pratt-through and Warren pony truss bridge. Additionally, we agree the bridge was identified in the IHBI as being previously determined eligible for the NRHP and was designated as a “Non-Select” bridge.”

They also agreed with the recommendation of Alternative B1 Rehabilitation of the Existing Bridge for Continued Vehicular Use (Two-way [One-Lane] Option) as being the preferred alternative for the project.

The SHPO also stated that, “As referenced in Section V.C of the HBAA, we request that this bridge be photographically documented.”

In regard to archaeological resources, the SHPO stated, “Additionally, as previously indicated, in regard to archaeological resources, based upon the documentation available to the staff of the Indiana SHPO, no currently known archaeological resources eligible for inclusion in the National Register of Historic Places (“NRHP”) have been recorded within the proposed project area. No archaeological investigations appear necessary provided that all project activities remain within areas disturbed by previous construction. Please be advised that archaeological resources may exist underneath modern development” (Appendix D [correspondence]).

No further responses were received regarding the HPSR, archaeological assessment, or the HBAA.

In conjunction with the submission of the finding, the 30% plans are located in Appendix F [plans] for the SHPO’s review.

Per the "Standard Treatment Approach for Historic Bridges" (Standard Treatment Approach) from the Historic Bridges PA, the bridge owner will provide rehabilitation plans to the Indiana SHPO when the design is approximately 30% complete, 60% complete, and when final design plans are complete. The purpose of these reviews is to ensure compliance with the Secretary of Interior’s Standards for Rehabilitation, and to incorporate context sensitive design features, where practicable.

Additionally, per the Standard Treatment Approach, before construction commences, the bridge owner will complete any photo documentation of the bridge as requested by the SHPO. Any photo documentation requirements will be added as a firm commitment in the environmental document if not yet implemented.

A public notice of the “No Historic Properties Affected” finding will be published in *The Madison Courier* seeking the views of the public regarding the effects of the proposed project on the historic property within the APE. Comments from the public will be accepted for 30 days following the publication of the notice. If any substantive comments are received during this period, this document will be revised to include them.

Per Stipulation III of the Historic Bridges PA, the project sponsor will hold a public hearing for the project prior to completion of National Environmental Policy Act (NEPA) studies. All originally invited consulting parties will be notified of the public hearing.

It should be noted that per the permanent rule issued by the IDNR effective August 14, 2013 (312 IAC 20-4-11.5), INDOT is requesting that this project be subjected to “dual review”; that is, reviewed by the Division of Historic Preservation and Archaeology simultaneously under 54

U.S.C. 306108 (Section 106) and IC 14-21-1-18 (Indiana Preservation and Archaeology Law dealing with alterations of historic sites and structures requiring a Certificate of Approval [COA]). Pursuant to Section 11.5(f) of this rule, at the conclusion of the review process INDOT anticipates that the Division Director would issue a letter of clearance exempting this project from obtaining a COA under IC 14-21-1-18. It is anticipated that the letter of clearance will be issued after review of the final plans. Obtaining a letter of clearance before environmental consultation form (ECF) approval will be added as a firm commitment in the environmental document.

3. BASIS FOR FINDING

No historic properties, besides Bridge P000-40-07088 B, are present within the APE. The project proposes to rehabilitate this “Non-Select” bridge, which CRA continues to recommend as eligible for listing in the NRHP under Criterion C. Per the terms of the Historic Bridges PA, the FHWA will satisfy its Section 106 responsibilities involving “Select” and “Non-Select” bridges through the Project Development Process (PDP) of the Historic Bridges PA (Stipulation III) and, therefore, the finding for this project only applies to other resources located within the APE and not the bridge. Therefore, CRA recommends that a finding of “No Historic Properties Affected” is appropriate for this project.

~~Appendix A: Maps~~

~~Appendix B: Photos~~

~~Appendix C: Consulting Party List~~

~~Appendix D: Correspondence~~

~~Appendix E: Report Abstracts~~

~~Appendix F: Plans~~

Appendices have been removed to limit duplication and the size of the CE document. The full report can be made available upon request.

Alyssa Reynolds
Cultural Resource Analysts, Inc
201 NW 4th St., Suite 204
Evansville, IN 47708

The Plain Dealer & Sun
FIN 61-0301090
PO Box 1200
Paducah, KY 42002

PUBLISHER'S CLAIM

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Display Master (Must not exceed two actual lines, neither of which shall
total more than four solid lines of the type in which the body of the
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COMPUTATION OF CHARGES

110.49 at	0.4877	
cents per line		\$ 53.89
Additional charges for notices containing rule or tabular work (50 per cent of above amount)		\$ 26.94
Charge for extra proofs of publication (\$1.00 for each proof in excess of two)		\$
TOTAL AMOUNT OF CLAIM		\$ 80.83

DATA FOR COMPUTING COST

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Pursuant to the provisions and penalties of IC 5-11-10-1, I hereby certify that the foregoing account is
just and correct, that the amount claimed is legally due, after allowing all just credits, and that no part of the same
has been paid.

I also certify that the printed matter attached hereto is a true copy, of the same column width and type size,
which was duly published in said paper 1 times. The dates of publication being as follows:

August 23, 2024 ad 71030266

Additionally, the statement checked below is true and correct:

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☒ Newspaper has a Web site and this public notice was posted on the same day as it was published in
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Title: Accounting Clerk

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Alyssa Reynolds
Cultural Resource Analysts, Inc
201 NW 4th St., Suite 204
Evansville, IN 47708
(812)253-3009


Class Display Ad #71030266 Summary:

Slug Line: Public Notice Des. No. 2200148
Size: 1.00 x 13.10

Publication Cost	\$80.83	Payment Date	8/14/24
Adjustments	\$0.00	Amount	\$-80.83
Net Cost	\$80.83	Description	Prepayment from AMPWeb
Prepaid Amount	\$-80.83	Payment Type	cc Credit Card
Amount Due	\$0.00		

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**Public Notice
Des. No. 2200148**

The Indiana Department of Natural Resources (IDNR) and the Indiana Department of Transportation (INDOT) are planning to undertake a bridge project, funded in part by the Federal Highway Administration (FHWA). The project is located along Park Road n within the Crosley Fish and Wildlife Area and encompasses approximately 1 acre in Vernon Township, Jennings County, Indiana.

Under the preferred alternative, the proposed project would involve rehabilitation of the existing bridge over Muscatatuck River (Bridge P000-40-07088 B). The proposed rehabilitation includes replacement of the existing timber plank deck with a new timber plank deck. The existing bridge railing will be replaced along with deteriorated concrete abutments and piers. No right-of-way (ROW) acquisition is occurring as the project takes place within an IDNR property. Properties listed in or eligible for the National Register of Historic Places located within the Area of Potential Effects (APE) include Bridge P000-40-07088 B, National Bridge Inventory (NBI) Number (No.) 060380.

This bridge has been classified as a "Non-Select" bridge by the INDOT Historic Bridge Inventory and, thus, the procedures outlined in Stipulation III. B of the Historic Bridges PA will be followed to fulfill FHWA's Section 106 responsibilities for the bridge. Therefore, the finding for this project only applies to other resources located within the APE and not Bridge P000-40-07088 B. The INDOT, on behalf of the FHWA, has issued a "No Historic Properties Affected" finding for the project since the NRHP listed/eligible properties within the APE will not be impacted by the undertaking. In accordance with the National Historic Preservation Act, the views of the public are being sought regarding the effect of the proposed project on the historic elements per 36 CFR 800.2(d), 800.3(e), and 800.6(a)(4). Pursuant to 36 CFR 800.4(d)(1), the documentation specified in 36 CFR 800. 11(d) is available for inspection in the Evansville, IN office of Cultural Resource Analysts, Inc. Additionally, this documentation can be viewed electronically by accessing INDOT's Section 106 document posting website IN SCOPE at <https://erms12c.indot.in.gov/Section106Documents/>. This documentation serves as the basis for the "No Historic Properties Affected" finding. The views of the public on this effect finding are being sought. Please reply with any comments to Alyssa Reynolds at 812.253.3009 or adreynolds@crai-ky.com at the offices of Cultural Resource Analysts, Inc., at 201 NW 4th St, Suite 204, Evansville, Indiana 47708 no later than September 20, 2024. In accordance with the "Americans with Disabilities Act," if you have a disability for which INDOT needs to provide accessibility to the document(s) such as interpreters or readers, please contact Matt Walker, 812-528-1868 or MatWalker@indot.IN.gov.

hspaxlp

Division of Historic Preservation & Archaeology · 402 W. Washington Street, W274 · Indianapolis, IN 46204-2739
Phone 317-232-1646 · Fax 317-232-0693 · dhpa@dnr.IN.gov

September 13, 2024

Alyssa Reynolds
Architectural Historian
Cultural Resource Analysts, Inc. ("CRAI")
201 NW 4th Street, Suite 204
Evansville, Indiana 47708

State Agency: Indiana Department of Transportation ("INDOT"),
Federal Agency: Federal Highway Administration, Indiana Division ("FHWA")

Re: Dual Review: Indiana Department of Transportation's finding of "no historic properties affected." on behalf of the Federal Highway Administration, 30% plans, and photographic documentation for the Park Road Bridge (Jennings County Bridge No. P000-40-07088; National Bridge Inventory "NBI" No. 60380) over Muscatatuck River Project (Des. No. 2200148; DHPA No. 31257)

Dear Ms. Reynolds:

Pursuant to Section 106 of the National Historic Preservation Act of 1966, as amended (54 U.S.C. § 306108); implementing regulations at 36 C.F.R. Part 800; the "Programmatic Agreement Among the Federal Highway Administration, the Indiana Department of Transportation, the Indiana Historic Preservation Officer, and the Advisory Council on Historic Preservation Regarding Management and Preservation of Indiana's Historic Bridges" ("Indiana Historic Bridges PA"); and the "Programmatic Agreement (PA) Among the Federal Highway Administration, the Indiana Department of Transportation, the Advisory Council on Historic Preservation and the Indiana State Historic Preservation Officer Regarding that Implementation of the Federal Aid Highway Program In the State of Indiana" ("Indiana Minor Projects PA"); and also pursuant to Indiana Code 14-21-1-18 and 312 Indiana Administrative Code ("IAC") 20-4, the staff of the Indiana State Historic Preservation Officer ("Indiana SHPO") has reviewed your August 14, 2024, submission which enclosed INDOT's finding, supporting documentation, 30% plans, and photographic documentation for the aforementioned project, which is proposed for areas located within the Crosley Fish and Wildlife Area, Vernon Township, Jennings County, Indiana.

For the benefit of the Indiana Historic Preservation Review Board ("Review Board") members and other recipients of this letter who are not Section 106 consulting parties, please be aware that documents submitted for review of this project can be found online at IN SCOPE at <https://erms12c.indot.in.gov/Section106Documents/>. From there, search by this project's designation number: 2200148.

As previously indicated, the area of potential effects ("APE") proposed in the historic properties short report ("HPSR") appears to be of adequate size to encompass the geographic area in which direct and indirect effects of a project of this nature could occur. Additionally, we agreed with the HPSR that the APE contains only one resource identified as being eligible for listing in the National Register of Historic Places ("NRHP"), the Crosley Bridge (Jennings County Bridge No. P000-40-07088; National Bridge Inventory "NBI" No. 60380; Indiana Historic Sites and Structure Inventory (IHSSI) # 079-646-40018.

Additionally, as previously indicated, in regard to archaeological resources, based upon the documentation available to the staff of the Indiana SHPO, no currently known archaeological resources eligible for inclusion in the National Register of Historic Places ("NRHP") have been recorded within the proposed project area. No archaeological investigations appear necessary

provided that all project activities remain within areas disturbed by previous construction. Please be advised that archaeological resources may exist underneath modern development.

If any prehistoric or historic archaeological artifacts or human remains are uncovered during construction, demolition, or earthmoving activities, state law (Indiana Code 14-21-1-27 and Indiana Code 14-21-1-29) requires that the discovery must be reported to the State of Indiana Department of Natural Resources, Division of Historic Preservation and Archaeology within two (2) business days. In that event, please call (317) 232-1646. Be advised that adherence to Indiana Code 14-21-1-27 and Indiana Code 14-21-1-29 does not obviate the need to adhere to applicable federal statutes and regulations, including but not limited to 36 C.F.R. 800.

Additionally, as previously indicated, if project-related ground-disturbing activities will be within 100 feet of any cemetery, please be aware of the cemetery development plan requirements in Indiana Code 14-21-1-26.5 (<http://iga.in.gov/legislative/laws/2016/ic/titles/014/articles/021/chapters/001/#section-26.5>). All cemeteries must be avoided by all project activities, and provisions of relevant state statutes regarding cemeteries (including IC 14-21-1 and IC 23-14) must be adhered to. Please also be aware of Indiana Code 23-14-44-1 and Indiana Code 23-14-44-2, regarding restrictions on roads and utility construction in cemeteries.

Accordingly, we concur with INDOT's August 14, 2024, Section 106 finding of "No Historic Properties Affected," on behalf of FHWA, for this federal undertaking.

Thank you for providing the 30% bridge plans for our review, at this time we have no comment. We look forward to receiving the 60%, and 90% final bridge plans for this rehabilitation, after which we will decide whether it is appropriate to issue a Director's Letter of Clearance for this project, indicating compliance with Indiana Code 14-21-1-18.

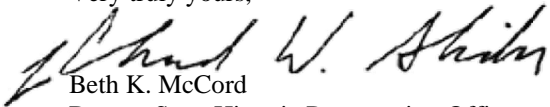
Additionally, thank you for providing the draft photographic documentation of the Crosley Bridge (Jennings County Bridge No. P000-40-07088; National Bridge Inventory "NBI" No. 60380; Indiana Historic Sites and Structure Inventory (IHSSI) # 079-646-40018). We have no comments and find this documentation to be acceptable. We will add this information to SHAARD and submit a copy to the Indiana State Archives. Please advise us the name and location of the public or not-for-profit organization that is willing to accept a copy of this documentation.

For the benefit of those recipients of a copy of this letter who are not Section 106 consulting parties, please be aware that a copy of INDOT's August 25, 2023, letter can be found online at <https://erms12c.indot.in.gov/Section106Documents/>. From there, search by this project's designation number: 2200148.

If you have questions regarding our dual review of the aforementioned project, please contact DNR-DHPA. Questions about archaeological issues should be directed to contact at Wade T. Tharp at (317) 232-1650 or wtharp1@dnr.in.gov. Questions about historic buildings or structures pertaining to this review should be directed to Toni Lynn Giffin at (317) 233-2803 or tgiffin@dnr.IN.gov.

In all future correspondence regarding the dual review of this Park Road Bridge over Muscatatuck River Project, in Vernon Township, Jennings County, Indiana (Des. No. 2200148), please refer to DHPA No. 31257.

Very truly yours,



Beth K. McCord
Deputy State Historic Preservation Officer
Director, Division of Historic Preservation and Archaeology

BKM:TLG:WTT:wt

EMC: Patrick Carpenter, Federal Highway Administration
Matt Coon, Ph.D., Indiana Department of Transportation
Susan Branigin, Indiana Department of Transportation
Haley Brinker, Indiana Department of Transportation
Alyssa Reynolds, Cultural Resource Analysts, Inc.
Toni Lynn Giffin, Indiana DNR-DHPA
Wade T. Tharp, Indiana DNR-DHPA

EMC to Indiana Historic Preservation Review Board Members:
J. Scott Keller, Indiana Historic Preservation Review Board

Daniel Kloc, AIA, Indiana Historic Preservation Review Board
Chandler Lighty, Indiana Historic Preservation Review Board
Anne Shaw, Indiana Historic Preservation Review Board
April Sievert, Ph.D., Indiana Historic Preservation Review Board
Jason Larrison, AIA, Indiana Historic Preservation Review Board
Ryan Mueller, Deputy Director, Indiana Department of Natural Resources
Beth McCord, Division of Historic Preservation and Archaeology

EMC to potentially interested persons:

Jomary Baller, Project Manager, Indiana Department of Natural Resources
Ben Clark, Chief of Cultural Resources of State Parks, Indiana Department of Natural Resources
Lucas Green, South Region Manager, Indiana Department of Natural Resources
David Nance, Professional Geologist, Indiana Department of Natural Resources
Chad Springer Assistant Property Manager, Indiana Department of Natural Resources, Crosley Fish and Wildlife Area
Nathan Holth, HistoricBridges.org
Tony Dillon, Historic Hoosier Bridges
Pal Brandenburg, Chair, Historic Spans Task Force
Greg Sekula, Director, Indiana Landmarks Southern Regional Office
Sarah Abel, Jennings County Auditor
Jennings County Commissioners
Jim Reeves, Jennings County Highway Superintendent
Chris Asher, President Jennings County Historical Society
Susan Craig, Executive Director - Southeastern Indiana Regional Planning Commission

WING Kristin

From: Alyssa Reynolds <adreynolds@crai-ky.com>

Sent: Friday, September 20, 2024 2:54 PM

To: 'Giffin, Toni L' <TGiffin@dnr.IN.gov>

Cc: 'Elizabeth Gallow' <eagallow@crai-ky.com>; 'Alfred Wessling' <awessling@b-l-n.com>; 'Kristin Wing' <kwing@b-l-n.com>; 'Brinker, Haley' <HBrinker@indot.IN.gov>; 'Coon, Matthew' <mcoon@indot.IN.gov>; 'Branigin, Susan' <SBranigin@indot.IN.gov>; 'Walton, David' <DWalton@indot.IN.gov>; 'Tharp, Wade' <WTharp1@dnr.IN.gov>; 'Alexander, Kelyn' <KAlexander3@indot.IN.gov>

Subject: Photo Documentation Package Acceptance INDOT Des. No. 2200148; DHPA No. 31257

Toni,

Good afternoon. I wanted to inform you that the Jennings County Historical Society and the Jennings County Public Library were both willing to accept the photo documentation package for INDOT Des. No. 2200148; DHPA No. 31257.

Thank you,

Alyssa Reynolds

Architectural Historian

adreynolds@crai-ky.com

Indiana Office

201 NW 4th Street, Suite 204

Evansville, Indiana 47708

812.253.3009 office

812.253.3010 fax

812.549.4503 cell

<http://www.crai-ky.com>





INDIANA DEPARTMENT OF TRANSPORTATION

100 North Senate Avenue
Room N758-ES
Indianapolis, Indiana 46204

PHONE: (855) 463-6848

Eric Holcomb, Governor
Michael Smith, Commissioner

October 16, 2024

This letter was sent to the listed parties.

RE: Dual Review FHWA Project: Des. No. 2200148; DHPA No. 31257; Park Road Bridge Project,
Jennings County, Indiana

Dear Consulting Party (see attached list),

The Indiana Department of Natural Resources (IDNR) and the Indiana Department of Transportation (INDOT), with funding from the Federal Highway Administration (FHWA), propose to proceed with the Park Road Bridge Project (Des. No. 2200148).

This letter is part of the Section 106 review process for this project. Section 106 of the National Historic Preservation Act (NRHP) requires federal agencies to take into account the effects of their undertakings on historic properties. We are requesting comments from you regarding the possible effects of this project. Please use the above Des. Number and project description in your reply and your comments will be incorporated into the formal environmental study.

A Section 106 early coordination letter (ECL) was distributed on August 25, 2023. In addition, a letter distributed on March 4, 2024, notified consulting parties that a historic property short report (HPSR), archaeological assessment, and Historic Bridge Alternatives Analysis (HBAA) were available for review and comment. A finding of "No Historic Properties Affected" was signed on August 14, 2024, and included the 30% plans. The finding and plans were available for review and comment the same day.

The proposed undertaking is located within the Crosley Fish and Wildlife Area, established in 1931, in Jennings County, Indiana. It is within Vernon Township as shown on the Vernon, Indiana, USGS Topographic Quadrangle, in Section 15, Township 6 N, Range 8 E. The project area along Park Road encompasses approximately 1.0 acre within the Crosley Fish and Wildlife Area. The project area can be viewed online at <https://arcg.is/jqueP> (the Des. No. is the most efficient search term once in the CRO - Public Web Map App).

The proposed work will occur on the Park Road bridge (P000-40-07088 B; National Bridge Inventory [NBI] Number [No.] 060380) over Muscatatuck River. The existing bridge is an approximately 212 feet by 12.3 feet, three-span, truss bridge constructed in 1910. The main span of the bridge is a steel Pratt-through truss and the approach spans are both steel Warren pony trusses. A bridge rehabilitation that consisted of a new deck with some new stringers, as well as cleaning and painting of the truss, was completed in 1979. The bridge was rehabilitated in 2004 when the deck was replaced with new wood planks and the truss was cleaned and painted (Des. No. 0300801). The gusset plates at abutment number (no.) 1 were replaced (Des. No. 1400643) in 2015 along with a portion of the south span deck. The bridge was closed in 2019 due to holes and deterioration in the timber deck. According to the *Indiana Historic Bridge Inventory*, completed in 2010, the bridge was determined eligible for listing in the National Register of Historic Places (NRHP) under Criterion C. It was also determined

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to be a “Non-Select” bridge, which are those that may not be considered excellent examples of a given type of historic bridge or may not be as suitable candidates for preservation.

All proposed work is assumed to be located within the roadway or adjacent to the roadway and the bridge. The proposed scope of work is to rehabilitate the bridge.

The proposed work will include:

- Cleaning and painting the truss. The paint color will match the existing color, and the bridge rail will be painted to match the truss.
- Replacing the bridge deck in all spans with a timber plank deck that has timber running boards.
- Repairing or replacing in kind some verticals, gusset plates, and low chord truss members or connections.
- Replacing some miscellaneous truss secondary members in kind such as bottom lateral bracing and lattice.
- Removing and reconstructing deteriorated concrete abutment and pier caps.
- Patching delaminated and spalled areas of abutments and piers.
- Placing scour countermeasures at all substructure units.
- Rivets removed to make necessary repairs will be replaced with round headed bolts wherever possible. The round headed bolts will be installed on the face of the member that is most visible from the roadway or exterior face of member.
- Replacing the bridge rail with a steel handrail.
- Some trees may need to be removed to install the scour countermeasures. All other nearby trees will be trimmed to keep branches away from truss and vehicles.
- Cleaning the abutment and pier bridge seats of all debris.

The clear roadway width will be slightly larger due to the installation of the new steel handrail on the bridge. The approach guardrail will not be modified. No approach roadway work will be completed as part of this project, except that portions of the south approach guardrail will be removed and reset if necessary to complete the scour countermeasure work. The scour countermeasure will consist of:

- Placing either dumped, partially grouted revetment riprap or an articulated concrete mattress on the south spillslope;
- Placing Class 1 riprap at a 20 feet width around the north abutment and wings;
- A concrete underpin will be placed around Pier 2 (south pier);
- Placing dumped partially grouted Class 2 riprap around three (3) sides (channel side and each pier nose) of Pier 3 with regular buried Class 2 riprap on the north side of Pier 3.

No right-of-way (ROW) acquisition is occurring as the project takes place within an IDNR-owned property.

The road will remain closed during construction, which is anticipated to be less than 120 days. As the bridge is currently closed and the roadway runs through the Crosley Fish and Wildlife Area, no detour signs will be placed along the roadway.

The project need is based on the current condition of the structure elements and notable deficiencies in the wearing surface, deck, superstructure, and substructure, and the scour critical status of the bridge. The bridge's deck deterioration caused it to be closed to traffic in 2019. The purpose of this project is to correct the deficiencies in the bridge wearing surface, deck, superstructure, and substructure (scour protection) to reopen the bridge to vehicular traffic, and protect the bridge and extend its service life.

Cultural Resource Analysts, Inc. (CRA) is under contract with INDOT to advance the Section 106 documentation for the referenced project. Egis Beam, Longest, & Neff (BLN) USA, Inc. (Egis) will be completing the environmental documentation for the referenced project.

Section 106 of the National Historic Preservation Act requires federal agencies to take into account the effects of their undertakings on historic and archaeological properties. In accordance with 36 CFR 800.2 (c), you were invited to become a consulting party as part of the Section 106 process. Entities that have accepted consulting party status are identified in the attached list.

The Section 106 process involves efforts to identify historic properties potentially affected by the undertaking, assess its effects and seek ways to avoid, minimize or mitigate any adverse effects on historic properties. For more information regarding the protection of historic resources, please see the Advisory Council on Historic Preservation's guide: *Protecting Historic Properties: A Citizen's Guide to Section 106 Review* available online at <https://www.achp.gov/sites/default/files/documents/2017-01/CitizenGuide.pdf>.

Per the terms of the "Programmatic Agreement Regarding Management and Preservation of Indiana's Historic Bridges" (Historic Bridges PA), the FHWA-Indiana Division will satisfy its Section 106 responsibilities involving "Select" and "Non-Select" bridges through the Project Development Process (PDP) of the Historic Bridges PA (Stipulation III). Because the bridge (P000-40-07088; NBI No. 60380) is a "Non-Select" bridge, the procedures outlined in Stipulation III.B. of the Historic Bridges PA will be followed to fulfill FHWA's Section 106 responsibilities for the project. (A copy of the Historic Bridges PA can be downloaded here: <http://www.in.gov/indot/2530.htm>).

Please note that per the permanent rule issued by the Indiana Department of Natural Resources effective August 14, 2013 (312 IAC 20-4-11.5), INDOT is requesting that this project be subjected to "dual review"; that is, reviewed by the Division of Historic Preservation and Archaeology simultaneously under 54 U.S.C. 306108 (Section 106) and IC 14-21-1-18 (Indiana Preservation and Archaeology Law dealing with alterations of historic sites and structures requiring a Certificate of Approval). Pursuant to Section 11.5(f) of this rule, at the conclusion of the review process we anticipate that the Division Director would issue a letter of clearance exempting this project from obtaining a Certificate of Approval under IC 14-21-1-18.

The Area of Potential Effects (APE) is the area in which the proposed project may cause alterations in the character or use of historic resources. The APE contains no resources listed in the National Register of Historic Places (NRHP).

A historian who meets the Secretary of the Interior's Professional Qualification Standards conducted an above-ground survey of resources within the APE for potential eligibility for the National Register of Historic Places (NRHP). As a result of the historic property identification and evaluation efforts, Bridge No. P00-40-07088 B (NBI No. 60380) is recommended eligible for listing in the NRHP under Criterion C for its unique architectural and engineering design.

An archaeologist who meets the Secretary of the Interior's Professional Qualification Standards reviewed the proposed project area and prepared an archaeological assessment recommending the project be allowed to proceed without additional archaeological study.

On September 13, 2024, State Historic Preservation Office (SHPO) staff responded to the finding of "No Historic Properties Affected" and 30% plans via a letter. They concurred with the finding and had no comment on the 30% plans. They asked to review the 60% plans and 90% plans when available to "...decide whether it is appropriate to issue a Director's Letter of Clearance for this project, indicating compliance with Indiana Code

14-21-1-18.” Additionally, SHPO staff had no comment on the photographic documentation that was submitted. They asked for the name and location of the public or not-for-profit organization that will be accepting a copy of the documentation when the information is known.

Per Attachment B of the Historic Bridges PA, the 60% plans are available for review in IN SCOPE at <http://erms12c.indot.in.gov/Section106Documents/> (the Des. No. is the most efficient search term, once in IN SCOPE). You are invited to review these documents and to respond with comments on any historic resource impacts incurred as a result of this project so that an environmental report can be completed. We also welcome your related opinions and other input to be considered in the preparation of the environmental document. If a hard copy of the materials is needed, please respond to this email with your request within seven (7) days.

Please review the information and comment within thirty (30) calendar days of receipt. If you indicate that you do not desire to be a consulting party or if you have not previously accepted consulting party status and you do not respond to this letter, you will not be included on the list of consulting parties for this project and will not receive further information about the project unless the design changes.

For questions concerning specific project details, you may contact Alyssa Reynolds of Cultural Resource Analysts, Inc. (CRA) at (812) 253-3009 or adreynolds@crai-ky.com. All future responses regarding the proposed project should be forwarded to CRA at the following address:

Alyssa Reynolds
Architectural Historian
Cultural Resource Analysts, Inc. (CRA)
201 NW 4th Street, Suite 204
Evansville, Indiana 47708
adreynolds@crai-ky.com

Tribal Contacts please respond to INDOT's Acting Tribal Liaison, Matt Coon mcoon@indot.in.gov; (317-697-9752) with any responses pertaining to this project including to provide INDOT/Indiana FHWA additional information about Tribal resources/concerns and questions/comments regarding cultural resources. The FHWA point of contact is Kari Carmany-George at K.CarmanyGeorge@dot.gov (317-226-5629).

Sincerely,



Matthew S. Coon, Manager
Cultural Resources Office
Environmental Services

Enclosures: Consulting Party Acceptance List and 60% Plans

Attachments have been removed to limit duplication and the size of the CE document.

2200148 Division of Historic Preservation & Archaeology · 402 W. Washington Street, W274 · Indianapolis, IN 46204-2739
Phone 317-232-1646 · Fax 317-232-0693 · dhpa@dnr.IN.gov ·

November 15, 2024

Alyssa Reynolds
Architectural Historian
Cultural Resource Analysts, Inc. ("CRAI")
201 NW 4th Street, Suite 204
Evansville, Indiana 47708

State Agency: Indiana Department of Transportation ("INDOT"),
Federal Agency: Federal Highway Administration, Indiana Division ("FHWA")

Re: Dual Review: 60% plans for the Park Road Bridge (Jennings County Bridge No. P000-40-07088; National Bridge Inventory "NBI" No. 60380) over Muscatatuck River Project (Des. No. 2200148; DHPA No. 31257)

Dear Ms. Reynolds:

Pursuant to Section 106 of the National Historic Preservation Act of 1966, as amended (54 U.S.C. § 306108); implementing regulations at 36 C.F.R. Part 800; the "Programmatic Agreement Among the Federal Highway Administration, the Indiana Department of Transportation, the Indiana Historic Preservation Officer, and the Advisory Council on Historic Preservation Regarding Management and Preservation of Indiana's Historic Bridges" ("Indiana Historic Bridges PA"); and the "Programmatic Agreement (PA) Among the Federal Highway Administration, the Indiana Department of Transportation, the Advisory Council on Historic Preservation and the Indiana State Historic Preservation Officer Regarding that Implementation of the Federal Aid Highway Program In the State of Indiana" ("Indiana Minor Projects PA"); and also pursuant to Indiana Code 14-21-1-18 and 312 Indiana Administrative Code ("IAC") 20-4, the staff of the Indiana State Historic Preservation Officer ("Indiana SHPO") has reviewed your October 16, 2024, submission which enclosed 60% plans for the aforementioned project, which is proposed for areas located within the Crosley Fish and Wildlife Area, Vernon Township, Jennings County, Indiana.

For the benefit of the Indiana Historic Preservation Review Board ("Review Board") members and other recipients of this letter who are not Section 106 consulting parties, please be aware that documents submitted for review of this project can be found online at IN SCOPE at <https://erms12c.indot.in.gov/Section106Documents/>. From there, search by this project's designation number: 2200148.

As previously indicated, the area of potential effects ("APE") proposed in the historic properties short report ("HPSR") appears to be of adequate size to encompass the geographic area in which direct and indirect effects of a project of this nature could occur. Additionally, we agreed with the HPSR that the APE contains only one resource identified as being eligible for listing in the National Register of Historic Places ("NRHP"), the Crosley Bridge (Jennings County Bridge No. P000-40-07088; National Bridge Inventory "NBI" No. 60380; Indiana Historic Sites and Structure Inventory (IHSSI) # 079-646-40018.

Additionally, as previously indicated, in regard to archaeological resources, based upon the documentation available to the staff of the Indiana SHPO, no currently known archaeological resources eligible for inclusion in the National Register of Historic Places ("NRHP") have been recorded within the proposed project area. No archaeological investigations appear necessary provided that all project activities remain within areas disturbed by previous construction. Please be advised that archaeological resources may exist underneath modern development.

If any prehistoric or historic archaeological artifacts or human remains are uncovered during construction, demolition, or earthmoving activities, state law (Indiana Code 14-21-1-27 and Indiana Code 14-21-1-29) requires that the discovery must be reported to the State of Indiana Department of Natural Resources, Division of Historic Preservation and Archaeology within two (2) business days. In that event, please call (317) 232-1646. Be advised that adherence to Indiana Code 14-21-1-27 and Indiana Code 14-21-1-29 does not obviate the need to adhere to applicable federal statutes and regulations, including but not limited to 36 C.F.R. 800.

Additionally, as previously indicated, if project-related ground-disturbing activities will be within 100 feet of any cemetery, please be aware of the cemetery development plan requirements in Indiana Code 14-21-1-26.5 (<http://iga.in.gov/legislative/laws/2016/ic/titles/014/articles/021/chapters/001/#section-26.5>). All cemeteries must be avoided by all project activities, and provisions of relevant state statutes regarding cemeteries (including IC 14-21-1 and IC 23-14) must be adhered to. Please also be aware of Indiana Code 23-14-44-1 and Indiana Code 23-14-44-2, regarding restrictions on roads and utility construction in cemeteries.

Thank you for providing the 60% bridge plans for our review. As indicated, it is our understanding that the proposed work will include:

- Cleaning and painting the truss. The paint color will match the existing color, and the bridge rail will be painted to match the truss.
- Replacing the bridge deck in all spans with a timber plank deck that has timber running boards.
- Repairing or replacing in kind some verticals, gusset plates, and low chord truss members or connections.
- Replacing some miscellaneous truss secondary members in kind such as bottom lateral bracing and lattice.
- Removing and reconstructing deteriorated concrete abutment and pier caps.
- Patching delaminated and spalled areas of abutments and piers.
- Placing scour countermeasures at all substructure units.
- Rivets removed to make necessary repairs will be replaced with round headed bolts wherever possible. The round headed bolts will be installed on the face of the member that is most visible from the roadway or exterior face of member.
- Replacing the bridge rail with a steel handrail.
- Some trees may need to be removed to install the scour countermeasures. All other nearby trees will be trimmed to keep branches away from truss and vehicles.
- Cleaning the abutment and pier bridge seats of all debris.

The clear roadway width will be slightly larger due to the installation of the new steel handrail on the bridge. The approach guardrail will not be modified. No approach roadway work will be completed as part of this project, except that portions of the south approach guardrail will be removed and reset if necessary to complete the scour countermeasure work. The scour countermeasure will consist of:

- Placing either dumped, partially grouted revetment riprap or an articulated concrete mattress on the south spillside;
- Placing Class 1 riprap at a 20 feet width around the north abutment and wings;
- A concrete underpin will be placed around Pier 2 (south pier);
- Placing dumped partially grouted Class 2 riprap around three (3) sides (channel side and each pier nose) of Pier 3 with regular buried Class 2 riprap on the north side of Pier 3.

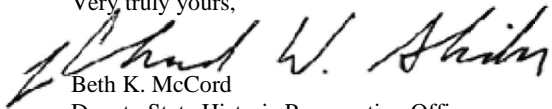
We appreciated the detailing of the work being performed, at this time we have no comment. We look forward to receiving the final bridge plans for this rehabilitation, after which we will decide whether it is appropriate to issue a Director's Letter of Clearance for this project, indicating compliance with Indiana Code 14-21-1-18.

For the benefit of those recipients of a copy of this letter who are not Section 106 consulting parties, please be aware that a copy of INDOT's October 16, 2024, letter can be found online at <https://erms12c.indot.in.gov/Section106Documents/>. From there, search by this project's designation number: 2200148.

If you have questions regarding our dual review of the aforementioned project, please contact DNR-DHPA. Questions about archaeological issues should be directed to contact at Wade T. Tharp at (317) 232-1650 or wtharp1@dnr.in.gov. Questions about historic buildings or structures pertaining to this review should be directed to Toni Lynn Giffin at (317) 233-2803 or tgiffin@dnr.IN.gov.

In all future correspondence regarding the dual review of this Park Road Bridge over Muscatatuck River Project, in Vernon Township, Jennings County, Indiana (Des. No. 2200148), please refer to DHPA No. 31257.

Very truly yours,



Beth K. McCord
Deputy State Historic Preservation Officer
Director, Division of Historic Preservation and Archaeology

BKM:TLG:WTT:wt

EMC: Patrick Carpenter, Federal Highway Administration
Matt Coon, Ph.D., Indiana Department of Transportation
Susan Branigin, Indiana Department of Transportation
Haley Brinker, Indiana Department of Transportation
Alyssa Reynolds, Cultural Resource Analysts, Inc.
Toni Lynn Giffin, Indiana DNR-DHPA
Wade T. Tharp, Indiana DNR-DHPA

EMC to Indiana Historic Preservation Review Board Members:
J. Scott Keller, Indiana Historic Preservation Review Board
Daniel Kloc, AIA, Indiana Historic Preservation Review Board
Chandler Lighty, Indiana Historic Preservation Review Board
Anne Shaw, Indiana Historic Preservation Review Board
April Sievert, Ph.D., Indiana Historic Preservation Review Board
Jason Larrison, AIA, Indiana Historic Preservation Review Board
Ryan Mueller, Deputy Director, Indiana Department of Natural Resources
Beth McCord, Division of Historic Preservation and Archaeology

EMC to potentially interested persons:

Jomary Baller, Project Manager, Indiana Department of Natural Resources
Ben Clark, Chief of Cultural Resources of State Parks, Indiana Department of Natural Resources
Lucas Green, South Region Manager, Indiana Department of Natural Resources
David Nance, Professional Geologist, Indiana Department of Natural Resources
Chad Springer Assistant Property Manager, Indiana Department of Natural Resources, Crosley Fish and Wildlife Area
Nathan Holth, HistoricBridges.org
Tony Dillon, Historic Hoosier Bridges
Pal Brandenburg, Chair, Historic Spans Task Force
Greg Sekula, Director, Indiana Landmarks Southern Regional Office
Sarah Abel, Jennings County Auditor
Jennings County Commissioners
Jim Reeves, Jennings County Highway Superintendent
Chris Asher, President Jennings County Historical Society
Susan Craig, Executive Director - Southeastern Indiana Regional Planning Commission