

CATEGORICAL EXCLUSION LEVEL 1 FORM

GENERAL PROJECT INFORMATION

Road No./County:

SR 46 / Franklin and Ripley County

Designation Number(s):

2001905

**Project
Description/Termini:**

Roadway Right-Sizing, 0.29 Mile West of SR 229 to 1.07 Miles East of SR 129

☒**CE Level 1 documentation for
exempted projects****Additional Information
to CE Level 1****Approval:**_____
INDOT DE/ESD Signature and Date**Release for Public Involvement:**_____
INDOT DE/ESD Initials and Date**Certification of Public involvement:**_____
INDOT Consultant Services Signature and Date**INDOT DE/ESD Reviewer:**_____
Signature and Date**CE Preparer:**_____
Erin Carleton – INDOT Seymour District
Name and Organization

Indiana Department of Transportation

County Franklin / Ripley Route SR 46 Des. No. 2001905

GENERAL PROJECT INFORMATION, DESCRIPTION, AND DESIGN INFORMATION	
Purpose and Need:	<p>Need:</p> <p>Within the project limits, SR 46 is an undivided 4 lane roadway with a speed limit of 55 MPH at the east end of the project that changes to 40 MPH; and 45 MPH at the west end of the project, that changes to 40 MPH. According to traffic data, (https://purdueuniversity.maps.arcgis.com/apps/insant/sidebar/index.html?appid=3361048f015e4176b75522e8588e108b) motorists frequently speed in this section of SR 46. Between 2021 to 2023, between Bedel Blvd and Fisherman St, there were a total of 3 fatal or incapacitating injury crashes, 14 non-incapacitating/possible injury crashes, and 30 property damage only crashes.</p> <p>Purpose:</p> <p>The purpose of this project is to improve safety and reduce crash frequency on SR 46 between SR 129 and SR 229.</p>
Project Description (Preferred Alternative):	<p>INDOT and the Federal Highway Administration (FHWA) intend to proceed with a project to reduce the 4-lane roadway to a 2-lane roadway with a middle turn lane.</p> <p>Location:</p> <p>This project is located on SR 46 within Batesville, Township 10N, Range 12E, Sections 20, 28, and 29. The project extends 0.29 mile west of SR 229 (just west of Columbus Ave.) to 1.07 miles east of SR 129 (just east of Fisherman road). The project is within Franklin County and Ripley County. See Appendix B for graphics.</p> <p>Existing Conditions:</p> <p>This section of SR 46 is an undivided 4-lane principal arterial roadway. The lanes are 12 feet wide with a curb ramp shoulder. The surrounding area is commercial and suburban, and includes residential and business complexes, factories, and a community college campus. Active railroad tracks run parallel south along SR 46 near SR 129. This roadway is used as a detour for I-74 as needed. The posted speed limit varies from 55 MPH to 40 MPH. Signalized intersections in this section of SR 46 include SR 129, E Pearl St, Tekulve Rd, and SR 229. A sidewalk is present on the south side of SR 46 between E Pearl St to Winding Way (0.5 mile east of SR 129). A pedestrian crossing exists across SR 129. SR 229 is located approximately 800 feet east of the project limits of SR 46. At the intersection of SR 229 and SR 46, there is an adjacent golf course and restaurant, which has a cart path that goes under a small structure under SR 229.</p> <p>Preferred Alternative:</p> <p>The original scope of this project included a hot mixed asphalt (HMA) overlay with curb ramp repair from SR 229 to SR 1. A Limited Red Flag (LRFI) report, and Minor Projects PA (MPPA) determination were made based on the original scope.</p>

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	<p>The preferred alternative is to reduce the number of lanes, and convert the median to a two-way center turn lane; a roadway treatment generally known as 'right-sizing'. Deep patching will take place as needed, and then the pavement will be overlaid with hot mixed asphalt (HMA). By the use of pavement markings, the existing two lanes in each direction will be reduced to one, and a 16 foot-wide center two-way turn lane will be created. At the intersection of SR 229 and SR 46, SR 229 will receive a HMA overlay that will extend 615 feet south. All work is confined to the roadway, in existing INDOT Right-of-Way (ROW). See appendix B for project plans.</p> <p>Because of the lane reduction, traffic signals at various intersections will be modified. At SR 229 signal heads will be replaced and new backplates will be added to aid in visibility. A new "Right Turn Only" sign will be added facing eastbound SR 46 traffic. At the Tekulve Rd intersection, 2 new dedicated left turn lane 4-section signal heads will be added for east and westbound SR 46 traffic.</p> <p>This alternative is expected to reduce travel speed through this corridor, while providing a left turn area that will not impede through traffic. Right-sizing projects on US 40 in Greenfield, US 40 in Brazil, and SR 662 in Evansville have demonstrated crash reductions of 68%, 93%, and 100%, respectively.</p>		
Other Alternatives Considered:	<p>No Build: this alternative involves leaving the existing roadway as-is. Although this alternative would avoid impacting surrounding resources, it would not meet the purpose and need. The roadway would continue to deteriorate until a more extensive repair is required. Because this alternative does not address safety concerns for motorists, it was dismissed from further consideration.</p> <p>Roadway Resurface with Curb Repair: Resurface the roadway starting at SR 229 and end at SR 1 in Dearborn County. This plan also included modifying curb ramps at Winding Way, Batesville Blvd, and SR 129. The curb ramp construction was moved to another des. number (2201203). This alternative was rejected as it would not provide crash reductions.</p>		
Funding Source(s):	<input checked="" type="checkbox"/> Federal <input checked="" type="checkbox"/> State <input type="checkbox"/> Local <input type="checkbox"/> Other		
Project Sponsor:	INDOT		
Estimated Cost:	\$3,616,236.00	Project Length:	2.96 Miles
Public Involvement:		No:	Yes: X
<p>All work will be completed within the existing INDOT right of way; therefore, Notice of Entry letters were not required.</p> <p>Project Does Meet The project would require the project sponsor to offer the public an opportunity to submit comments and/or request a public hearing. Anticipating public interest, INDOT has opted to proceed with a public hearing, which will be held May 5, 2025 at 6pm, at Batesville High School. A legal notice will appear in a</p>			

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local publication contingent upon the release of this document for public involvement. The document will be revised after the public involvement requirements are fulfilled.

Right-of-Way:	No: X	Yes:
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ROW within this section of SR 46 extends 50 feet from the centerline, with the exception of the south side near SR 129 where it meets railroad ROW at 46 feet from the centerline.

No right-of-way (ROW) required

This project will occur within existing right-of-way (ROW). No permanent or temporary ROW will be required for this project.

If the scope of work or permanent or temporary right-of-way amounts change, the INDOT Environmental Services Division (ESD) and the INDOT District Environmental Section will be contacted immediately.

Maintenance of Traffic (MOT) During Construction:	No:	Yes: X
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The MOT for the project will require single lane closures with flagging on both SR 46 and SR 229. Flagging at intersections and drive approaches will be incorporated as needed. No detour will be necessary.

The closures/lane restrictions will pose a temporary inconvenience to traveling motorists (including school buses and emergency services); however, no significant delays are anticipated, and all inconveniences and delays will cease upon project completion.

Bridge(s) and/or Small Structure(s) (include structure number(s)):	No:	Yes: X
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One (1) bridge is located within the project area. Structure 229-69-09562 (NBI #029350) is located on SR 229, 0.12 mile south of SR 46. The bridge is included in the HMA overlay portion of the project. No work will be done to the bridge substructure. Nine (9) other small structures are located in the project area but will not be impacted.

IDENTIFICATION AND EVALUATION OF IMPACTS

Early Coordination:

Early coordination letters have not been sent for this project. This document was elevated to a CE-1 due to the need for a public hearing. Anticipated impacts of this project, however, are consistent with that of a PCE.

Streams, Rivers, and Other Jurisdictional Features Impacted:	No: X	Yes:
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Based on the desktop review, the aerial map of the project area, there are 6 streams, rivers, watercourses, or other jurisdictional features within the 0.5-mile search radius. There is one non-jurisdictional stream within the project area, which was confirmed by the site visit by INDOT Seymour Design Team. The segment of the stream in the project area flows under the bridge on SR 229. The stream has been widened and armored by the golf course, which has property on both sides of the

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roadway. The stream will not be impacted by this project. All work is confined to the roadway, and no work will take place to any culverts or drainage structures. Therefore, no impacts are expected.

Open Water Feature(s):

No: X

Yes:

Based on the desktop review, the aerial map of the project area, there are 18 open water features within the 0.5-mile search radius. There are no open water features within or adjacent to the project area, which was confirmed by the site visit by INDOT Seymour Design Team. All work is confined to the roadway, and no work will take place to any culverts or drainage structures. Therefore, no impacts are expected.

Wetlands:

No: X

Yes:

Based on the desktop review, the aerial map of the project area, there are 22 wetlands within the 0.5-mile search radius. There are no wetlands within or adjacent to the project area, which was confirmed by the site visit by INDOT Seymour Design Team. All work is confined to the roadway, and no work will take place to any culverts or drainage structures. Therefore, no impacts are expected.

Terrestrial Habitat:

No: X

Yes:

Based on a desktop review, a site visit by INDOT Seymour Design Team, the aerial map of the project area (Appendix B, page 1), there are rural habitats, surrounded by business, residential houses, factories, a college campus, a golf course and trees by the railroad ROW. All work is confined to the roadway, no replanting will be necessary. No tree trimming is anticipated. Therefore, no impacts are expected.

Protected Species:

No: X

Yes:

Based on a desktop review, the IDNR Franklin County and Ripley County Endangered, Threatened and Rare (ETR) Species List has been checked by INDOT Seymour District on February 27, 2025. The Natural Heritage Program's Database has been checked. No ETR species are located within 0.5 mile of the project. An INDOT 0.5-mile bat review occurred on October 19, 2023. No bats were found in the 0.5 mile search radius.

Indiana Bat and Northern Long-Eared Bat

Project information was submitted through the USFWS's Information for Planning and Consultation (IPaC) portal, and an official species list was generated (Appendix C, page 1). The project is within range of the federally endangered Indiana bat (*Myotis sodalis*) and the federally threatened northern long-eared bat (NLEB) (*Myotis septentrionalis*). No additional species were generated in the IPaC species list other than the Indiana bat and northern long-eared bat.

The project qualifies for the *Range-wide Programmatic Informal Consultation for the Indiana bat and northern long-eared bat (NLEB)*, dated May 2016 (revised February 2018), between FHWA, Federal Railroad Administration (FRA), Federal Transit Administration (FTA), and USFWS. An effect determination key was completed on March 10, 2025, and based on the responses provided, the project was found to "May Affect- Not Likely to Adversely Affect" the Indiana bat and/or the NLEB (Appendix C, page 14). INDOT reviewed and verified the effect finding on March 10, 2025, and requested USFWS's review of the finding. On March 24, 2025, the USFWS concurred with the effective finding (Appendix C, page 22). Avoidance and Minimization Measures (AMMs) included in this project area General AMM 1,

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Hibernacula AMM 1 and Lighting AMM 1 and 2. AMMs and/or commitments are included as firm commitments in the *Environmental Commitments* section of this document.

This precludes the need for further consultation on this project as required under Section 7 of the Endangered Species Act, as amended. If new information on endangered species at the site becomes available, or if project plans are changed, USFWS will be contacted for consultation.

Geological and Mineral Resources:

No: ☒ X

Yes: ☐

Inside karst area; no presence

Based on a desktop review and the Indiana Karst Region map, the project is located in the designated Indiana Karst Region as outlined in the most current *Protection of Karst Features during Project Development and Construction*. According to the topo map of the project area (Appendix B, page 1), and there are no karst features identified within or adjacent to the project area. In the coordination response (February 27, 2025), the Indiana Geological and Water Survey (IGWS) did not indicate that karst features exist in the project area (Appendix C, page 25) IGWS reported high liquefaction potential, and abandoned industrial mineral quarries. Response from IGWS has been communicated to the designer on February 27, 2025. No impacts are expected.

Drinking Water Resources:

No: ☒ X

Yes: ☐

Sole Source Aquifer

Outside of Sole Source Aquifer (SSA)

The project is located in Franklin and Ripley County, which is not located within the area of the St. Joseph Sole Source Aquifer, the only legally designated sole source aquifer in the state of Indiana. Therefore, the FHWA/EPA/INDOT Sole Source Aquifer Memorandum of Understanding (MOU) is not applicable to this project, a detailed groundwater assessment is not needed, and no impacts are expected.

Wellhead Protection Area and Source Water

Not located in a Wellhead Protection Area or Source Water Area

The Indiana Department of Environmental Management's Wellhead Proximity Determinator website (<http://www.in.gov/idem/cleanwater/pages/wellhead/>) was accessed on February 27, 2025 by INDOT Seymour District. This project is not located within a Wellhead Protection Area or Source Water Area. No impacts are expected.

Water Wells

Wells present, no impacts

The Indiana Department of Natural Resources Water Well Record Database website (<https://www.in.gov/dnr/water/3595.htm>) was accessed on February 27, 2025 by INDOT Seymour District. One well is located near the edge of INDOT ROW on private property. The features will not be affected because no work will take place off of the roadway, no excavation will occur, and no ROW is being purchased for this project. Therefore, no impacts are expected. Should it be determined during the right-of-way phase that these wells will be affected, a cost to cure will likely be included in the appraisal to restore the wells.

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Urban Area Boundary

In an Urban Area Boundary Location

Based on a desktop review of INDOT's Road Inventory FC Viewer <https://indot.maps.arcgis.com/apps/webappviewer/index.html?id=bfe9a3dede034fb588266593246342b8> by INDOT Seymour District on February 28, 2025; this project is located in an Urban Area Boundary (UAB). No impact is expected.

Public Water System

In a Public Water System Location

Based on a desktop review, a site visit by INDOT Seymour District, the aerial map of the project area (Appendix B, page 1), and <https://indwv.gecsws.com/> this project is located where there is a public water system. The public water system will not be affected because work is confined to the roadway and no excavation will occur. No impact is expected.

Floodplains:

No: ☒ X

Yes: ☐

Not in floodplain

The Indiana Department of Natural Resources Indiana Floodplain Information Portal website (<https://indnr.maps.arcgis.com/apps/webappviewer/index.html?id=05026dabc2e8461983e196d56a213c1e>) was accessed on February 28, 2025 by INDOT Seymour District. This project is not located in a regulatory floodplain as determined from approved IDNR floodplain maps (Appendix F, page 1). Therefore, it does not fall within the guidelines for the implementation of 23 CFR 650, 23 CFR 771, and 44 CFR. No impacts are expected.

Farmland:

No: ☒ X

Yes: ☐

Presence, no impact

Based on a desktop review, a site visit by INDOT Seymour District, the aerial map of the project area (Appendix B, page 1), there is farmland as defined by the Farmland Protection Policy Act adjacent to the project. The project will not convert any farmland due to the project being confined to the roadway. No alternatives other than those previously discussed in this document will be investigated without reevaluating impacts to prime farmland.

Cultural Resources:

No: ☒ X

Yes: ☐

An MPPA Project Submittal and Assessment Form was submitted January 9, 2023 under the original scope of the project which included curb ramp repair. On March 28, 2023 the INDOT Cultural Resource Office (CRO) determined that this project falls within the guidelines of Category B, Type 1 and 14 under the Minor Projects Programmatic Agreement, (Appendix D, page 1). Coordination occurred with INDOT CRO on March 4, 2025 and it was determined that under the new scope, this project is now under the MPPA Category A Type 4 and Type 5 (Appendix D, page 8). Category A Type 4 covers projects with roadway surface repair such as overlays, and Type 5 covers repair or replacement of lighting and signals. No further consultation is required. This completes the Section 106 process and the responsibilities of the FHWA under Section 106 have been fulfilled.

Section 4(f) and Section 6(f) Resources:

No: ☒ X

Yes: ☐

Section 4(f)

No presence, no impact

Section 4(f) of the U.S. Department of Transportation Act of 1966 prohibits the use of certain public and historic lands for federally funded transportation facilities unless there is no feasible and prudent

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alternative. The law applies to significant publicly owned parks, recreation areas, wildlife / waterfowl refuges, and NRHP eligible or listed historic properties regardless of ownership. Lands subject to this law are considered Section 4(f) resources.

Based on a desktop review, the aerial map of the project area (Appendix B, page 1), there are 6 potential 4(f) resources located within the 0.5-mile search radius. There are no Section 4(f) resources within or adjacent to the project area. Therefore, no use is expected.

Section 6(f)

No presence or presence, no impact

The U.S. Land and Water Conservation Fund Act of 1965 established the Land and Water Conservation Fund (LWCF), which was created to preserve, develop, and assure accessibility to outdoor recreation resources. Section 6(f) of this Act prohibits conversion of lands purchased with LWCF monies to a non-recreation use.

A review of 6(f) properties on the INDOT ESD website revealed a total of 19 properties in Franklin and Ripley County (Appendix H, page A). None of these properties are located within or adjacent to the project area. Therefore, there will be no impacts to 6(f) resources.

Air Quality:

No:

Yes:

Standalone Project or Lead DES number

This project is included in the Fiscal Year (FY) 2024-2028 Statewide Transportation Improvement Program (STIP) (Appendix G, page 1)

Attainment area

This project is located in Franklin and Ripley County, which is currently in attainment for all criteria pollutants according to (*cite source used*). Therefore, the conformity procedures of 40 CFR Part 93 do not apply.

MSAT

MSAT Level 1a Analysis

This project is of a type qualifying as a categorical exclusion (Group 1) under 23 CFR 771.117(c), or exempt under the Clean Air Act conformity rule under 40 CFR 93.126, and as such, a Mobile Source Air Toxics analysis is not required.

Community Impacts:

No: X

Yes:

Environmental Justice (EJ)

Due to the issuance of recent federal Executive Orders (EO) from January 2025, including EO 14154, EO 14148, and EO 14173, EO 12898 has been rescinded and this section is no longer applicable.

Public Facilities and Services (e.g. schools, emergency services):

No: X

Yes:

No presence, no impact

Based on a desktop review, the aerial map of the project area (Appendix B, page 1), there are 6 public facilities within the 0.5-mile search radius. There are no public facilities within or adjacent to the project area, which was confirmed by the site visit by INDOT Seymour District. Therefore, no impacts are expected. Access to all properties will be maintained during construction.

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It is the responsibility of the project sponsor to notify school corporations and emergency services at least two weeks prior to any construction that would block or limit access.

Hazardous Materials and Regulated Substances:

No: X

Yes:

Limited RFI Completed

The level of this Categorical Exclusion (CE) document was elevated due to the need for a public hearing. A LRFI was prepared based on the original scope of the project, and approved by INDOT SAM on January 18, 2023 (Appendix F, Page 1). Only the hazardous material 0.5-mile radius search was reviewed for this LRFI. Four underground storage tank sites, 2 leaking underground storage sites, 3 NPDES facilities, and 3 NPDES pipe locations are located within 0.5 mile of the project area. No recommendations were generated from this LRFI. Coordination with INDOT SAM on March 4, 2025, and it was agreed that no RFI addendum was needed due to the lack of excavation in the current scope.

Permits:

No: X

Yes:

No permits will be needed for this project.

Applicable recommendations provided by resource agencies are included in the Environmental Commitments section of this document. If permits are found to be necessary, the conditions of the permit will be requirements of the project and will supersede these recommendations.

It is the responsibility of the project sponsor to identify and obtain all required permits.

ENVIRONMENTAL COMMITMENTS:

Firm:

1) If the scope of work or permanent or temporary right-of-way amounts change, the INDOT Environmental Services Division (ESD) and the INDOT District Environmental Section will be contacted immediately. (INDOT ESD and INDOT District)

2) It is the responsibility of the project sponsor to notify school corporations and emergency services at least two weeks prior to any construction that would block or limit access. (INDOT ESD)

3) Any work in a wetland area within right-of-way or in borrow/waste areas is prohibited unless specifically allowed in the U.S. Army Corps of Engineers permit. (INDOT ESD)

4) General AMM 1: Ensure all operators, employees, and contractors working in areas of known or presumed bat habitat are aware of all FHWA/FRA/FTA (Transportation Agencies) environmental commitments, including all applicable AMMs. (USFWS)

5) Lighting AMM 1: Direct temporary lighting away from suitable habitat during the active season. (USFWS)

6) Lighting AMM 2: When installing new or replacing existing permanent lights, use downward-facing, full cut-off lens lights (with same intensity or less for replacement lighting); or for those transportation agencies using the BUG system developed by the Illuminating Engineering Society, be as close to 0 for all three ratings with a priority of "uplight" of 0 and "backlight" as low as practicable. (USFWS)

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7) Hibernacula AMM 1: For projects located within karst areas, on-site personnel will use best management practices, secondary containment measures, or other standard spill prevention and countermeasures to avoid impacts to possible hibernacula. Where practicable, a 300 foot buffer will be employed to separate fueling areas and other major containment risk activities from caves, sinkholes, losing streams, and springs in karst topography. (USFWS)

Appendices

A. INDOT Supporting Documentation

Threshold Chart.....A-1

B. Graphics

Project Location Map.....B-1

Topographical Map.....B-2

Project Area Photos.....B-3 to B-7

Stage 2 Plans.....B-8 to B-12

C. Coordination

USFWS Species List and Appendix B Project Submittal Form.....C-1 to C-21

Coordination Response with USFWS.....C-22 to C-23

Coordination Response from IGWS.....C-24 to C-26

D. Cultural Resources

MPPA.....D-1 to D-8

E. Red Flag and Hazardous Materials

Limited Red Flag Investigation.....E-1 to E-9

F. Water Resources

DNR Floodplain Maps..... F-1 to F-2

G. Air Quality

STIP.....G-1

H. Additional Studies

LWCF Resources.....H-1

Appendix A

INDOT Supporting Documentation

Categorical Exclusion Level Thresholds

	PCE	Level 1	Level 2	Level 3	Level 4 ¹
Section 106	Falls within guidelines of Minor Projects PA	"No Historic Properties Affected"	"No Adverse Effect"	-	"Adverse Effect" Or Historic Bridge involvement ²
Stream Impacts³	No construction in waterways or water bodies	< 300 linear feet of stream impacts	≥ 300 linear feet of stream impacts	-	USACE Individual 404 Permit ⁴
Wetland Impacts³	No adverse impacts to wetlands	< 0.1 acre	-	< 1.0 acre	≥ 1.0 acre
Right-of-way⁵	Property acquisition for preservation only or none	< 0.5 acre	≥ 0.5 acre	-	-
Relocations⁶	None	-	-	< 5	≥ 5
Threatened/Endangered Species (Species Specific Programmatic for Indiana bat & northern long eared bat)*	"No Effect", "Not likely to Adversely Affect" (With select AMMs ⁷)	"Not likely to Adversely Affect" (With any AMMs or commitments)	-	"Likely to Adversely Affect"	Project does not fall under Species Specific Programmatic ⁸
Threatened/Endangered Species (Any other species)*	Falls within guidelines of USFWS 2013 Interim Policy or "No Effect"	"Not likely to Adversely Affect"	-	-	"Likely to Adversely Affect"
Environmental Justice	No disproportionately high and adverse impacts	-	-	-	Potential ⁹
Sole Source Aquifer	No Detailed Groundwater Assessment	-	-	-	Detailed Groundwater Assessment
Floodplain	No Substantial Impacts	-	-	-	Substantial Impacts
Section 4(f) Impacts	None	-	-	-	Any ¹⁰
Section 6(f) Impacts	None	-	-	-	Any
Permanent Traffic Alteration	None	-	-	-	Any
Noise Analysis Required	No	-	-	-	Yes
Air Quality Analysis Required	No	-	-	-	Yes ¹¹
Approval Level <ul style="list-style-type: none"> District Env. (DE) Env. Serv. Div. (ESD) FHWA 	Concurrence by DE or ESD	DE or ESD	DE or ESD	DE and/or ESD	DE and/or ESD; and FHWA

¹ Coordinate with INDOT Environmental Services Division. INDOT will then coordinate with the appropriate FHWA Environmental Specialist.

² Any involvement with a bridge processed under the Historic Bridge Programmatic Agreement.

³ Total permanent impacts to streams (linear feet) and wetlands (acres).

⁴ US Army Corps of Engineers Individual 404 Permit

⁵ Total permanent and temporary right-of-way. This does not include reacquisition of existing apparent right-of-way.

⁶ If any relocations are within an area with a known or suspected Environmental Justice (EJ) or disadvantaged population, or has greater than 5 relocations, a conversation with FHWA, through INDOT ESD, is needed to confirm NEPA classification and outreach plan for the project.

⁷ Avoidance and Mitigation Measures (AMMs) determined by the IPAC determination key to be required that are not tree AMMs, bridge AMMs, or structure AMMs.

⁸ Projects that do not fall under a Species Specific Programmatic and results in a "Likely to Adversely Affect". Other findings can be processed as a lower-level CE.

⁹ Potential for causing a disproportionately high and adverse impact.

¹⁰ Section 4(f) use resulting in an Individual, Programmatic, or *de minimis* evaluation. The only exception is a *de minimis* evaluation for historic properties (Effective January 2, 2020). If a historic property *de minimis* and no other use, mark the *None* column.

¹¹ Hot Spot Analysis and/or MSAT Quantitative Emission Analysis.

* Includes the threatened/endangered species critical habitat

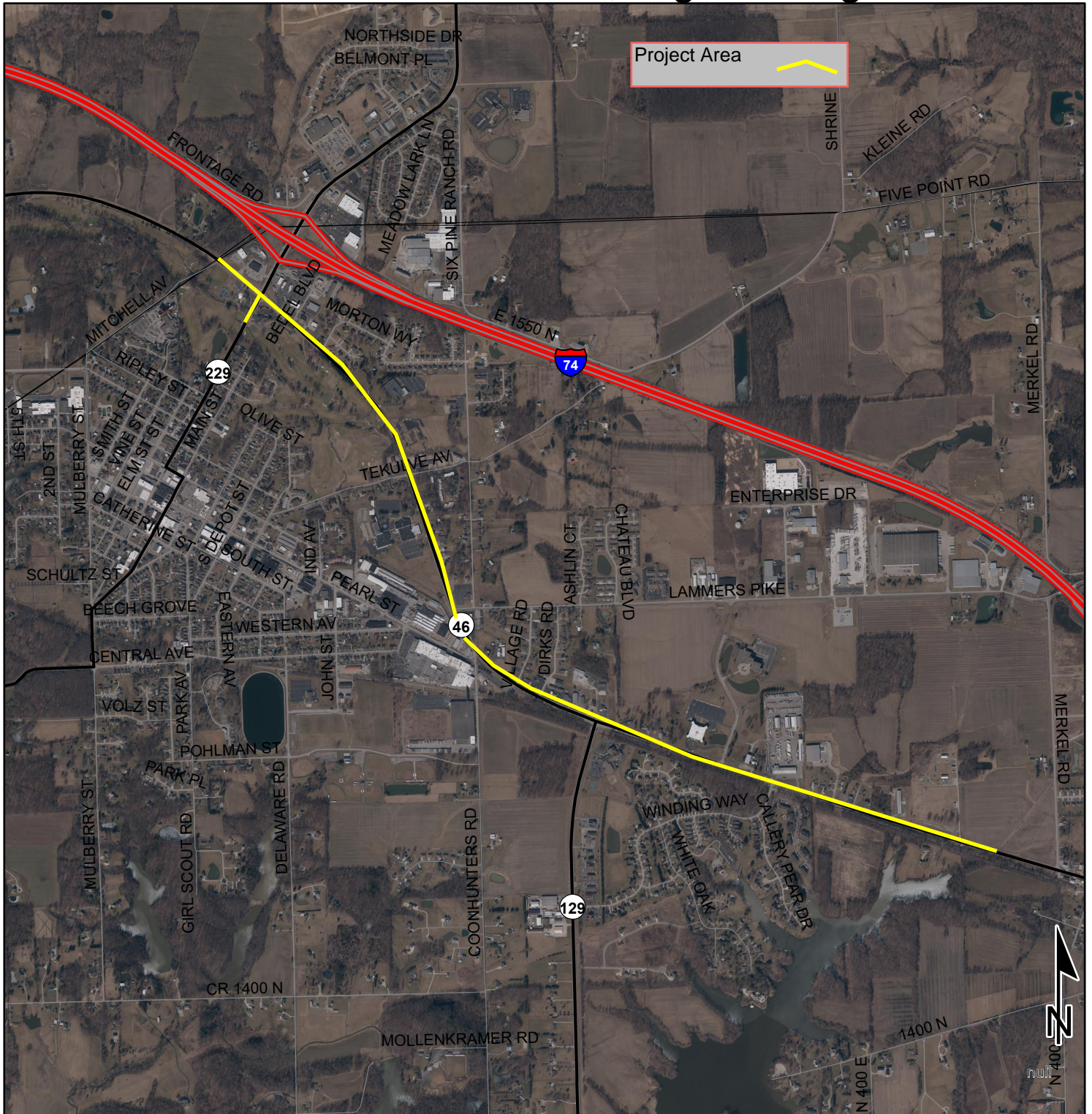
Note: Substantial public or agency controversy may require a higher-level NEPA document.

Note: The document was elevated to Level 1 due to the need for public involvement.

Appendix B

Graphics

0.29 Mile W of SR 229 to 1.07 Miles E of SR 129 SR 46, Ripley and Franklin County, Indiana Des No. 2001905, Right-Sizing



This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.

Sources: Non Orthophotography

Data - Obtained from the State of Indiana Geographical Information Office Library

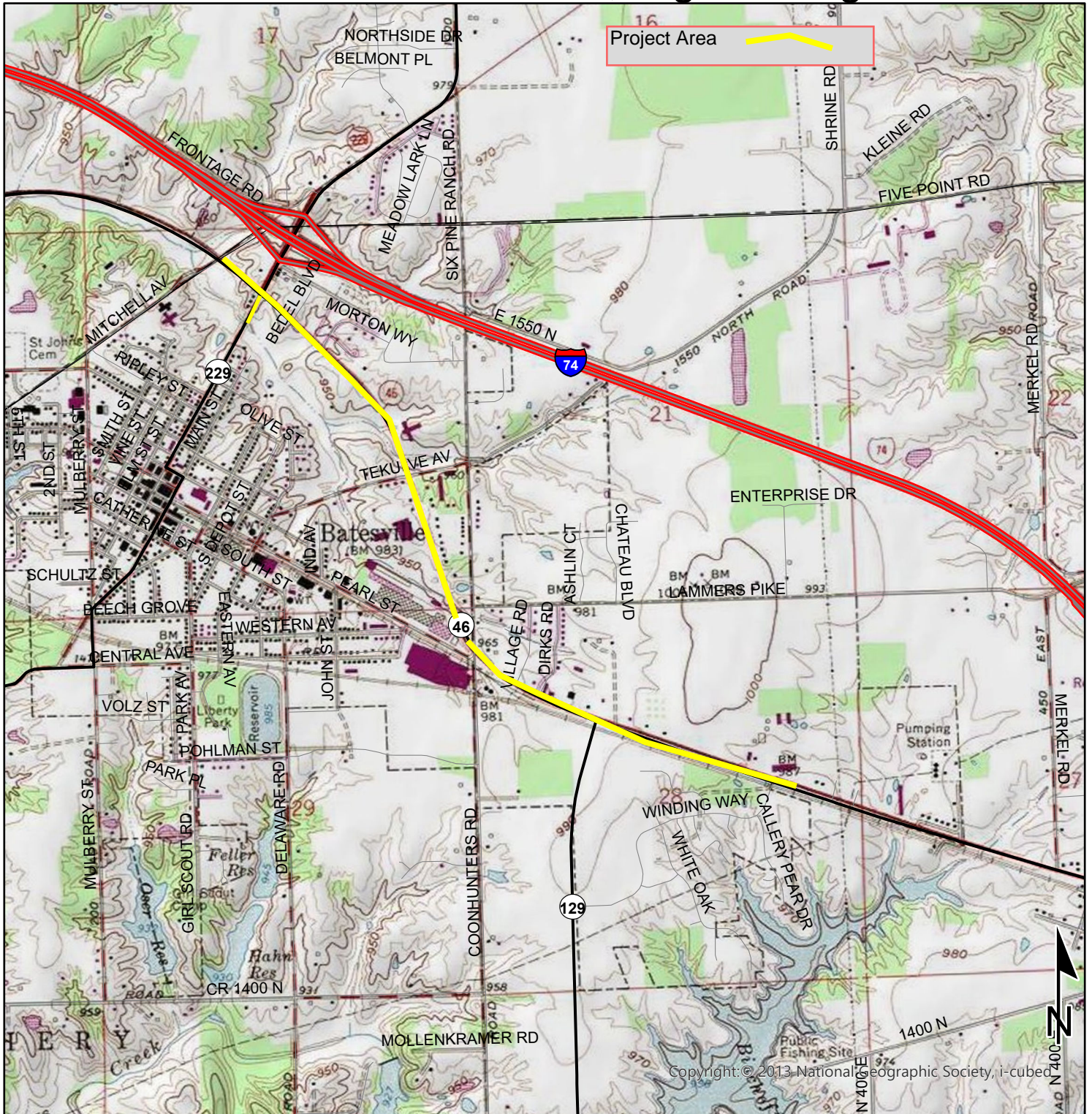
Orthophotography - Obtained from Indiana Map Framework Data (www.indianamap.org)

Map Projection: UTM Zone 16 N **Map Datum:** NAD83

Scale 1:23,294 0.3 0.15 0 0.3 Miles

- | | | | |
|--|------------------------|--|-------------|
| | Section Town and Range | | Interstate |
| | County Boundary | | State Route |
| | Local Road | | US Route |

0.29 Mile W of SR 229 to 1.07 Miles E of SR 129 SR 46, Ripley and Franklin County, Indiana Des No. 2001905, Right-Sizing



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Map Projection: UTM Zone 16 N **Map Datum:** NAD83

Scale 1:23,294 0.3 0.15 0 0.3 Miles

- | | | | |
|--|------------------------|--|-------------|
| | Section Town and Range | | Interstate |
| | County Boundary | | State Route |
| | Local Road | | US Route |



IN 46 Roadway Improvement Project - Batesville, IN

Columbus/Mitchell Avenue - SR 229 - Bedel Boulevard



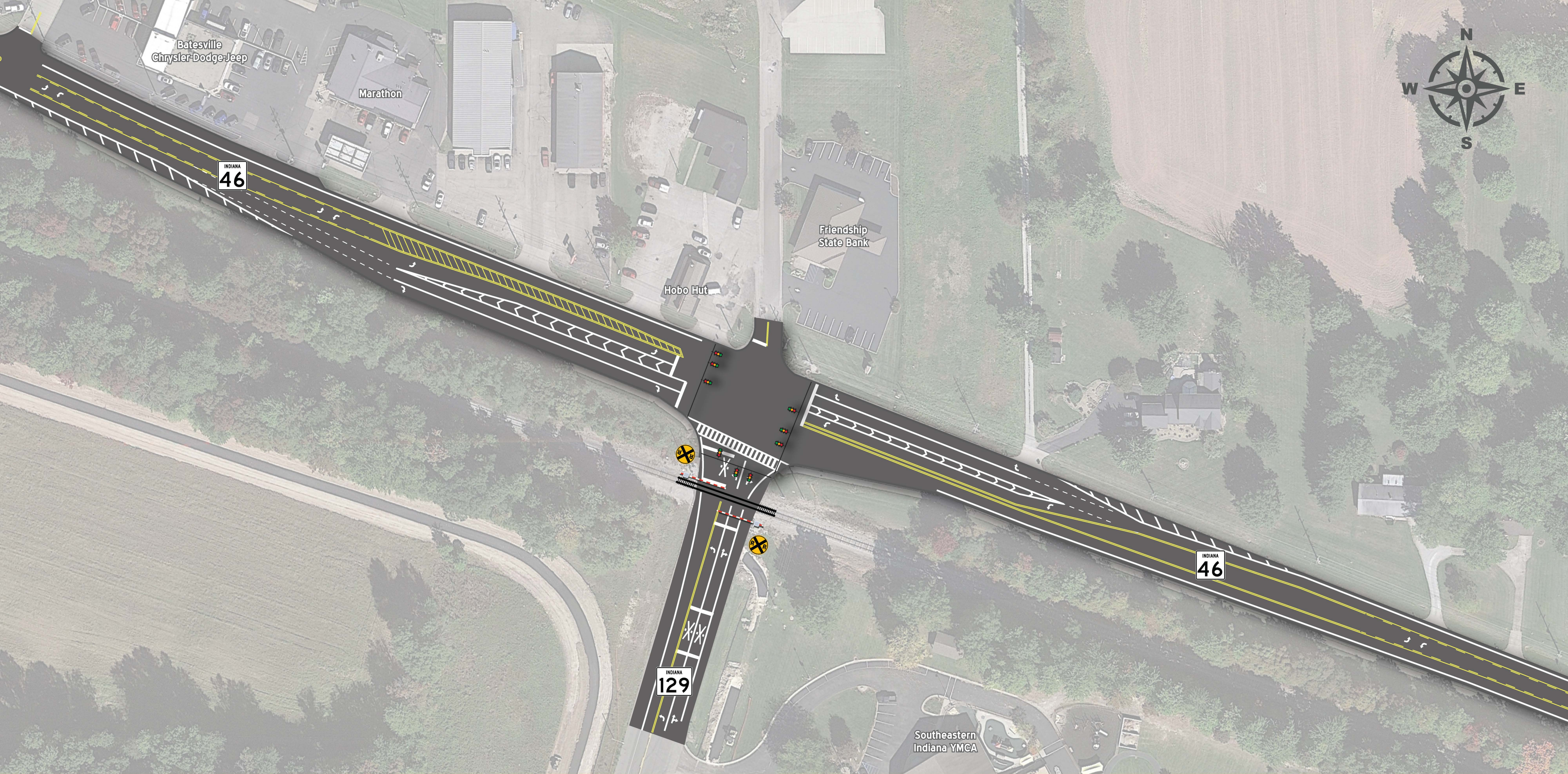
IN 46 Roadway Improvement Project - Batesville, IN

Telukve Road



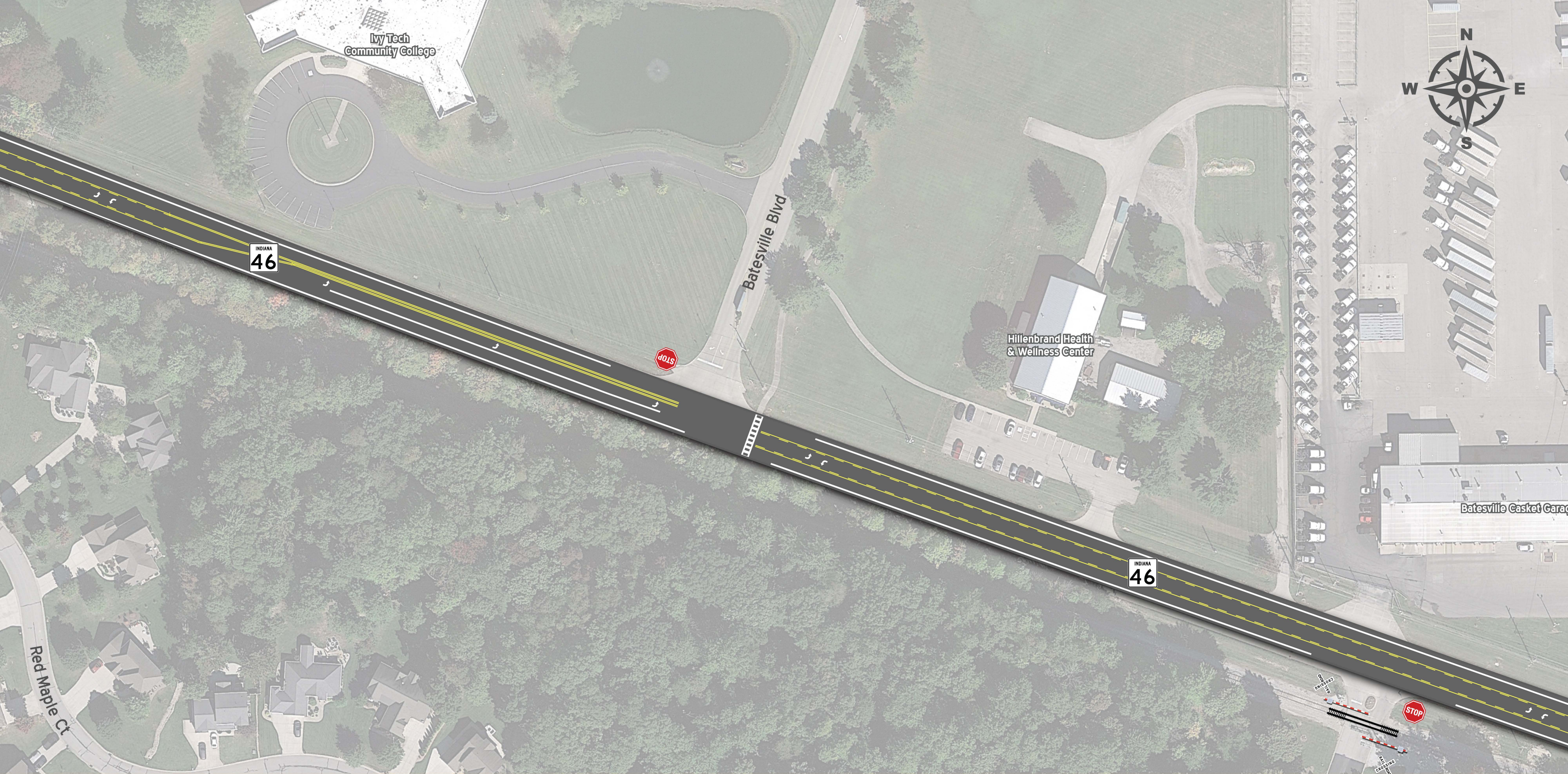
IN 46 Roadway Improvement Project - Batesville, IN

Lammers Pike & Pearl Street



IN 46 Roadway Improvement Project - Batesville, IN

SR 129

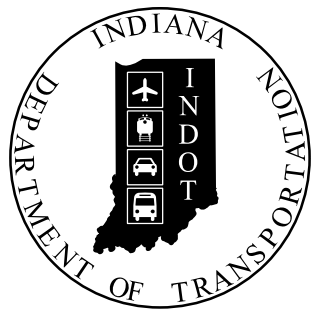


IN 46 Roadway Improvement Project - Batesville, IN

Batesville Boulevard

PROJECT	DESIGNATION
2001905	2001905
CONTRACT	
R-43370	

INDIANA DEPARTMENT
OF TRANSPORTATION



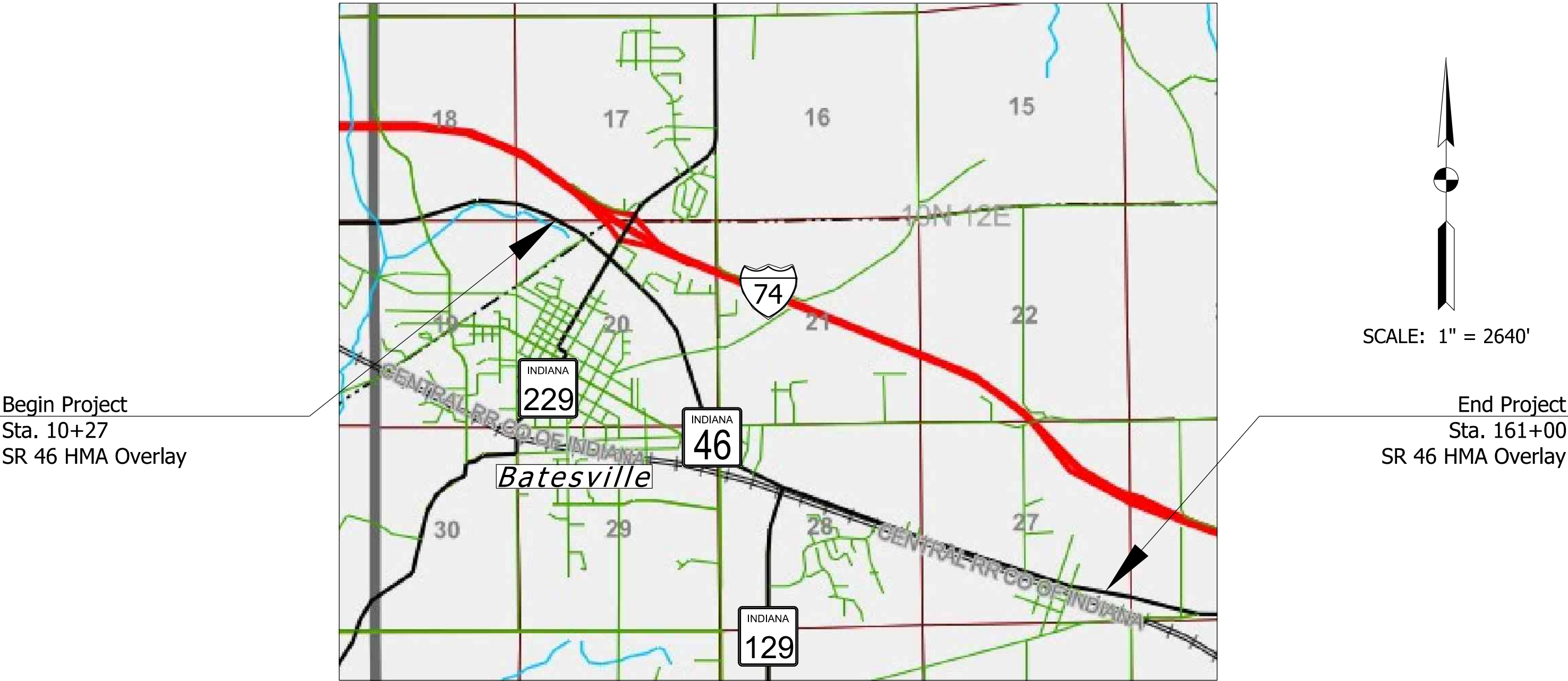
ROAD PLANS

ROUTE: SR 46 FROM: RP 133+43 TO: RP 136+22

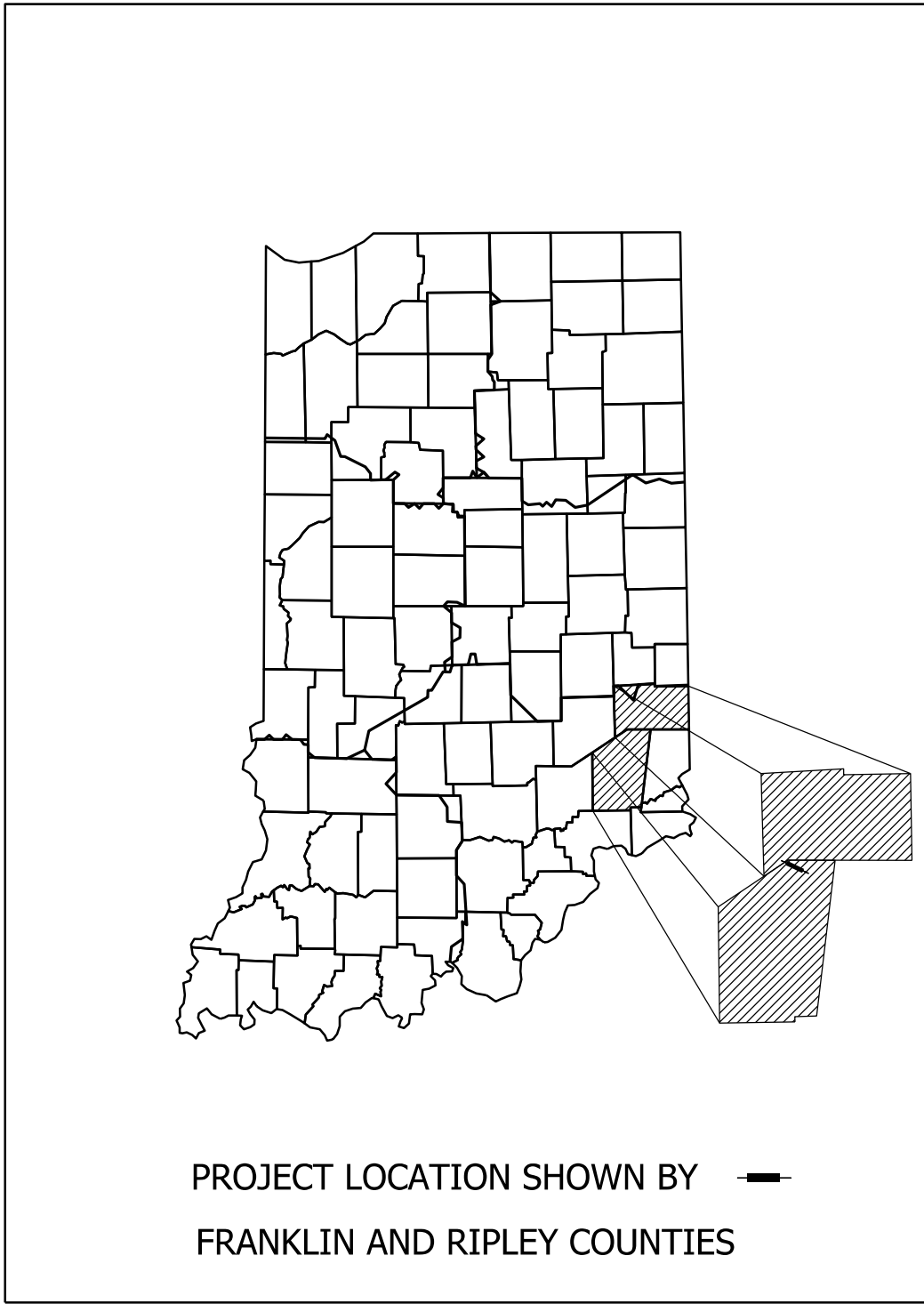
PROJECT NO. 2001905 P.E. N/A R/W 2001905 CONST.

NO ADDITIONAL RIGHT OF WAY
REQUIRED FOR THIS PROJECT

HMA Overlay, Preventive Maintenance on SR 46
From 0.29 Miles West of SR 229 to 1.07 Miles East of SR 129
Section 20 & 27-29, T-10-N, R-12-E, Adams, Laughery & Ray Townships, Franklin & Ripley Counties, Indiana



TRAFFIC DATA		
A.A.D.T.	(2026)	14723 V.P.D.
A.A.D.T.	(2036)	15561 V.P.D.
D.H.V	(2036)	9.18 V.P.H.
DIRECTIONAL DISTRIBUTION		48.93 %
TRUCKS		5.96 % A.A.D.T. 5.30 % D.H.V.
DESIGN DATA		
DESIGN SPEED		40 M.P.H.
PROJECT DESIGN CRITERIA		PARTIAL 3R (NON-FREEWAY)
FUNCTIONAL CLASSIFICATION		PRINCIPAL ARTERIAL
RURAL/URBAN		RURAL
TERRAIN		LEVEL

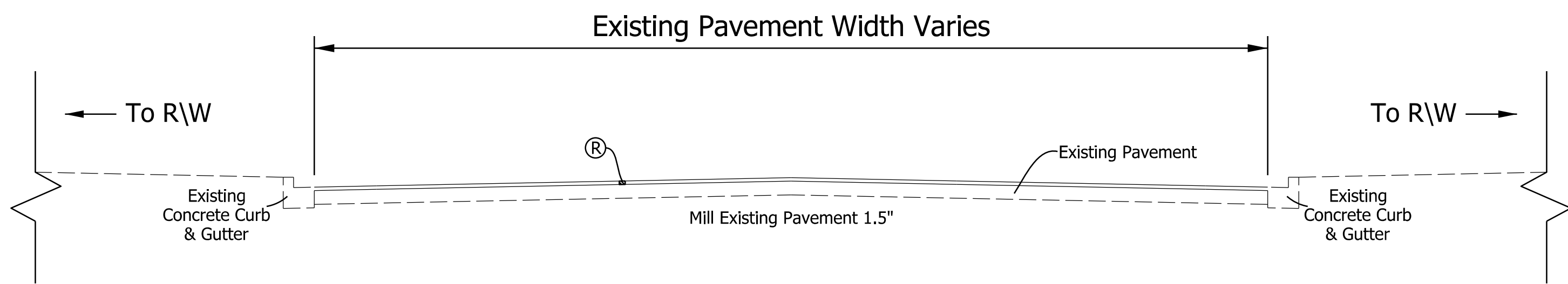


LATITUDE: N 39°17'27" LONGITUDE: W 85°12'19"

GROSS LENGTH:	2.85	MI.
NET LENGTH:	2.85	MI.
MAX. GRADE:	NA	%

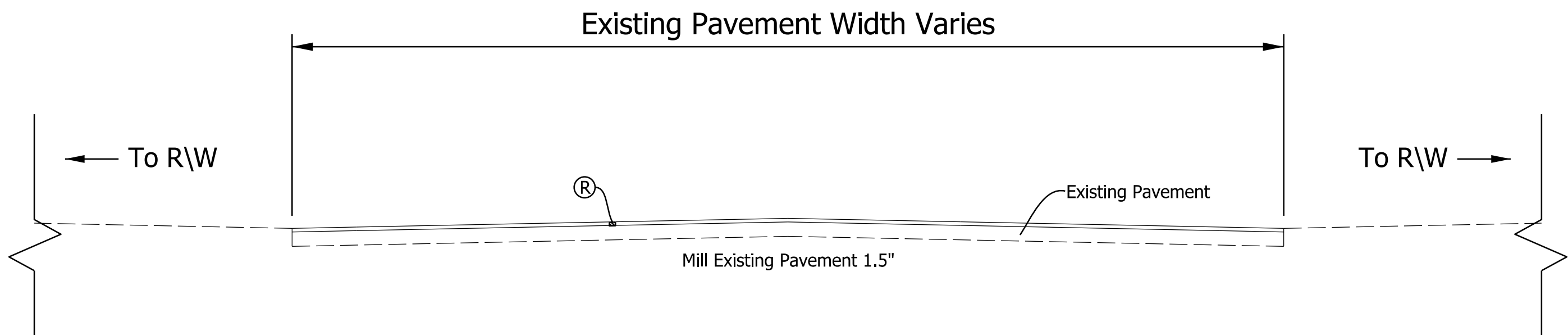
INDIANA DEPARTMENT OF TRANSPORTATION
STANDARD SPECIFICATIONS DATED 2024
TO BE USED WITH THESE PLANS

PLANS PREPARED BY: Andrea R. Mikrut, PE		812-524-3936 PHONE NUMBER	
CERTIFIED BY:		DATE	
RECOMMENDED FOR LETTING:		DATE	
INDIANA DEPARTMENT OF TRANSPORTATION			
		DESIGNATION 2001905	
		SHEETS 1 of 7	
CONTRACT R-43370		PROJECT 2001905	



Ⓡ 165 lbs/SYS QC/QA HMA, 3, 58H, Surface, 9.5 mm

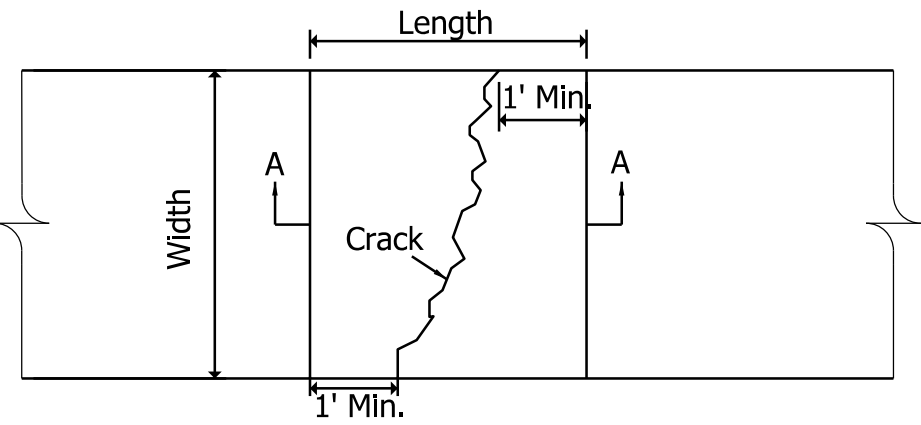
DETAIL SHEETS - TYPICAL SECTION WITH CURB SECTION
Sta. 11+89 to Sta. 157+63



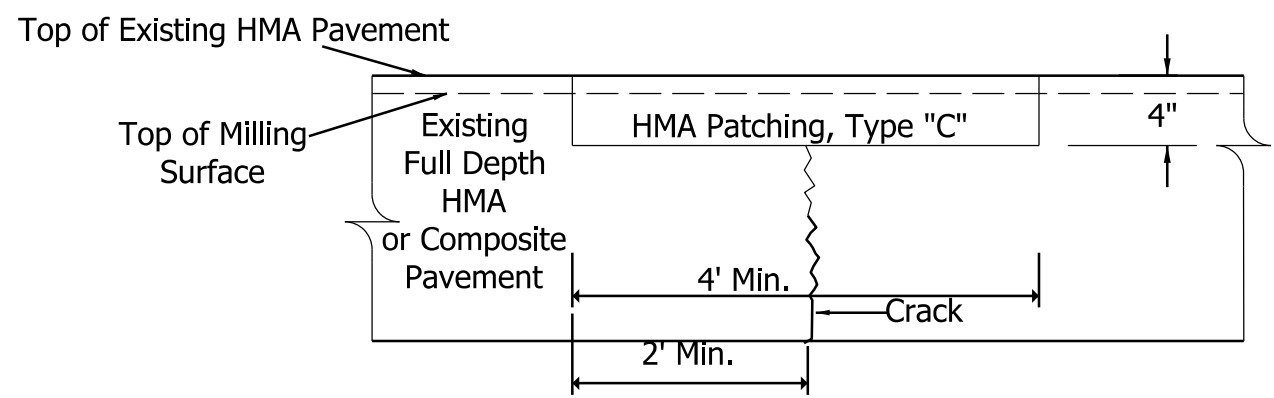
DETAIL SHEETS - TYPICAL SECTION WITH SHOULDER
Sta. 10+27 to Sta. 11+89,
Sta. 157+63 to Sta. 161+00

See Detail Sheets for Widths and Pavement Marking Locations

PLAN VIEW (PARTIAL OR FULL)

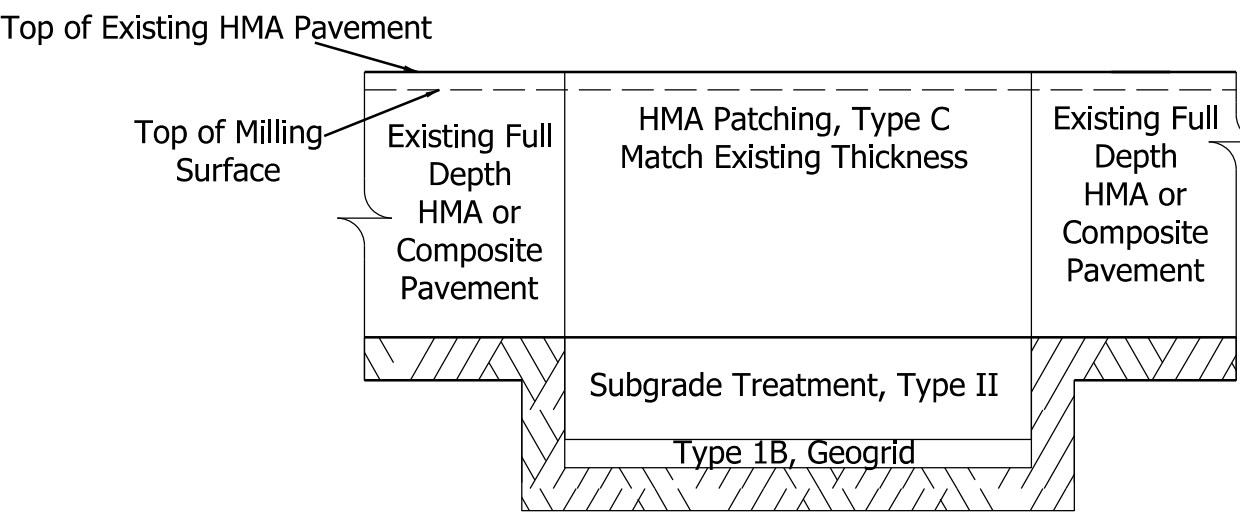


HMA PARTIAL DEPTH PATCHING DETAIL - SECTION A-A



- NOTES FOR PARTIAL DEPTH:
- Asphalt milling to be performed after patching is completed.
 - Patching locations from details are shown for information only. actual locations, dimensions, and type of patching will be determined by Project Engineer.
 - An undistributed quantity of HMA Patching, Type C = 100 tons is provided and is to be placed at the discretion of the Project Engineer.
 - HMA Patching Partial Depth, Type "C" consisting of:
4" Depth Milling, then
4" Depth HMA Type "C" Intermediate (440 lbs/syd.) on existing pavement
 - Coring Information can be found in the Geotechnical Report.

HMA FULL DEPTH PATCHING DETAIL - SECTION A-A



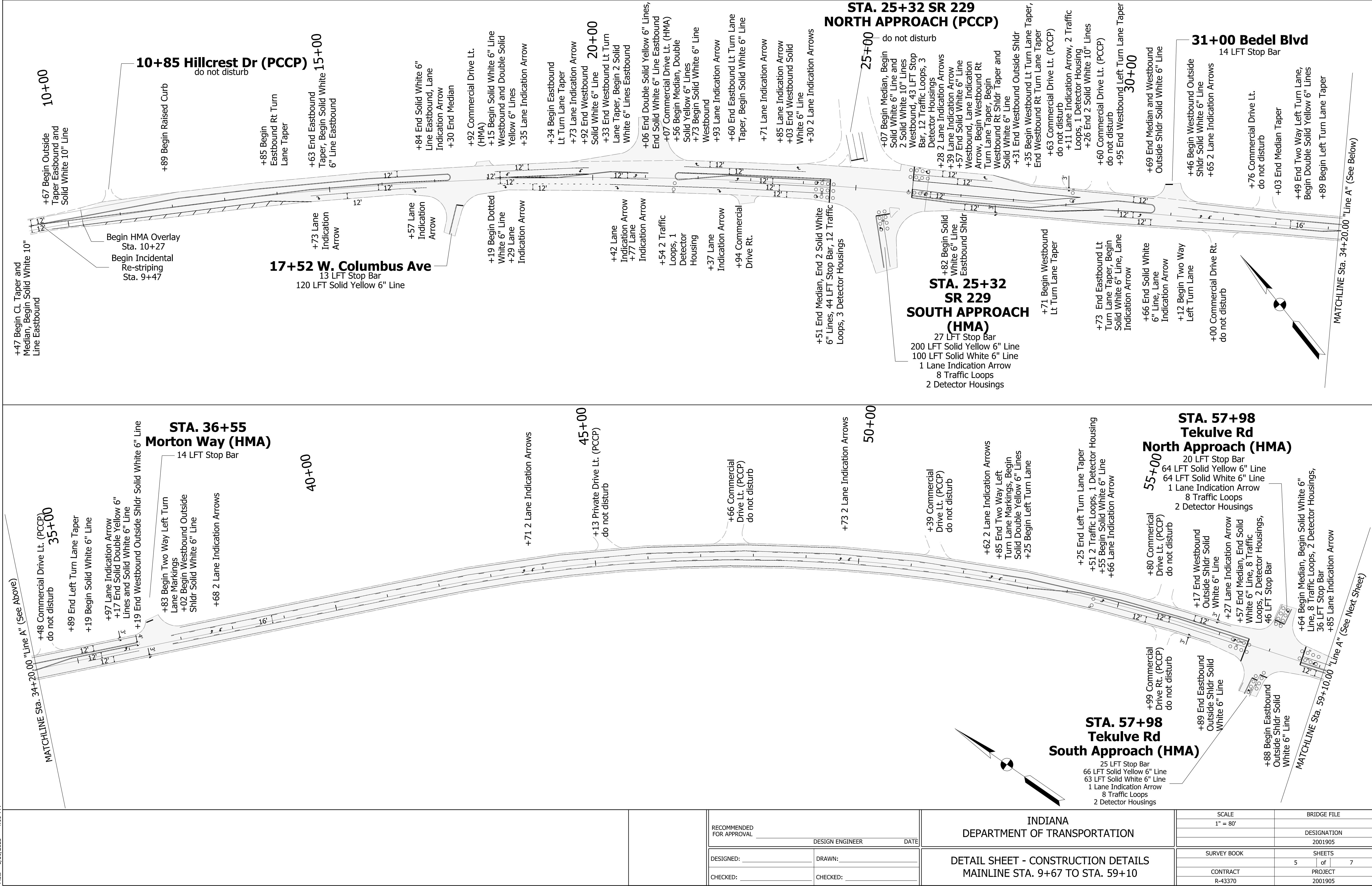
- NOTES FOR FULL DEPTH:
- Asphalt milling to be performed after patching is completed.
 - Patching locations from details are shown for information only. actual locations, dimensions, and type of patching will be determined by Project Engineer.
 - Remove the existing asphalt layers and sawcut through the entire thickness.
 - HMA Patching, Type C, consisting of:
440 lbs/sys. HMA Intermediate, Type C, on
660 lbs/sys. HMA Base, Type C min, or thickness required to match adjacent HMA pavement, on Subgrade Treatment, Type II, overlaying a Type IB geogrid.

Plot: 1/30/2025 4:10 PM

			RECOMMENDED FOR APPROVAL _____		INDIANA DEPARTMENT OF TRANSPORTATION	SCALE		BRIDGE FILE		
			DESIGN ENGINEER _____			DATE _____		DESIGNATION		
								2001905		
			DESIGNED: _____			DRAWN: _____		SHEETS		
								3 of 7		
			CHECKED: _____			CHECKED: _____		PROJECT		
				R-43370		2001905				
					TYPICAL SECTIONS					

Plot: 1/30/2025 4:10 PM

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Model: 06-Detail 01



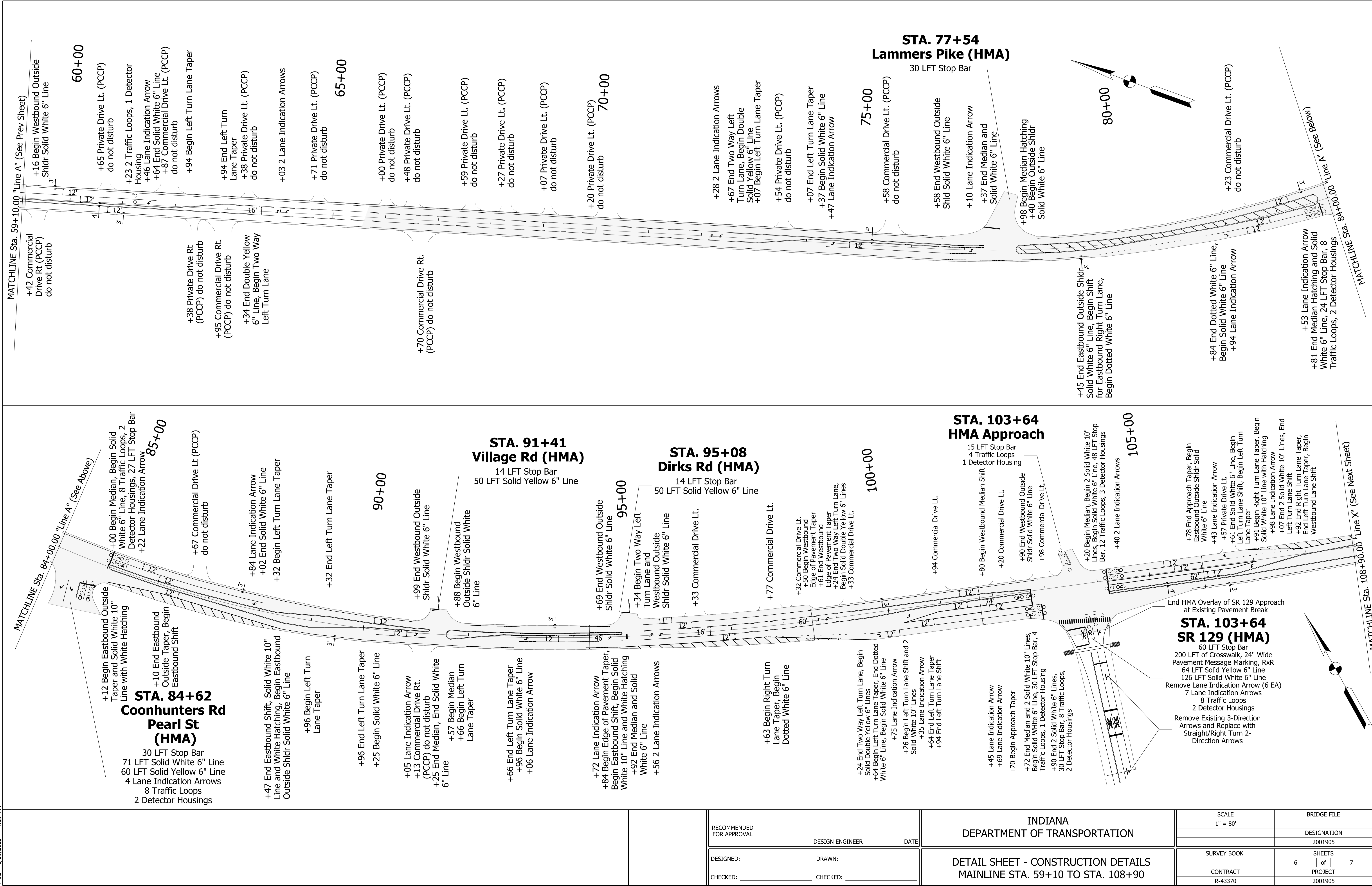
RECOMMENDED FOR APPROVAL _____	
DESIGNED: _____	DRAWN: _____
CHECKED: _____	CHECKED: _____

INDIANA DEPARTMENT OF TRANSPORTATION	
DETAIL SHEET - CONSTRUCTION DETAILS MAINLINE STA. 9+67 TO STA. 59+10	

SCALE 1" = 80'	BRIDGE FILE
	DESIGNATION 2001905
SURVEY BOOK	SHEETS 5 of 7
CONTRACT R-43370	PROJECT 2001905

Plot: 1/30/2025 4:10 PM

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Model: 07-Detail 02



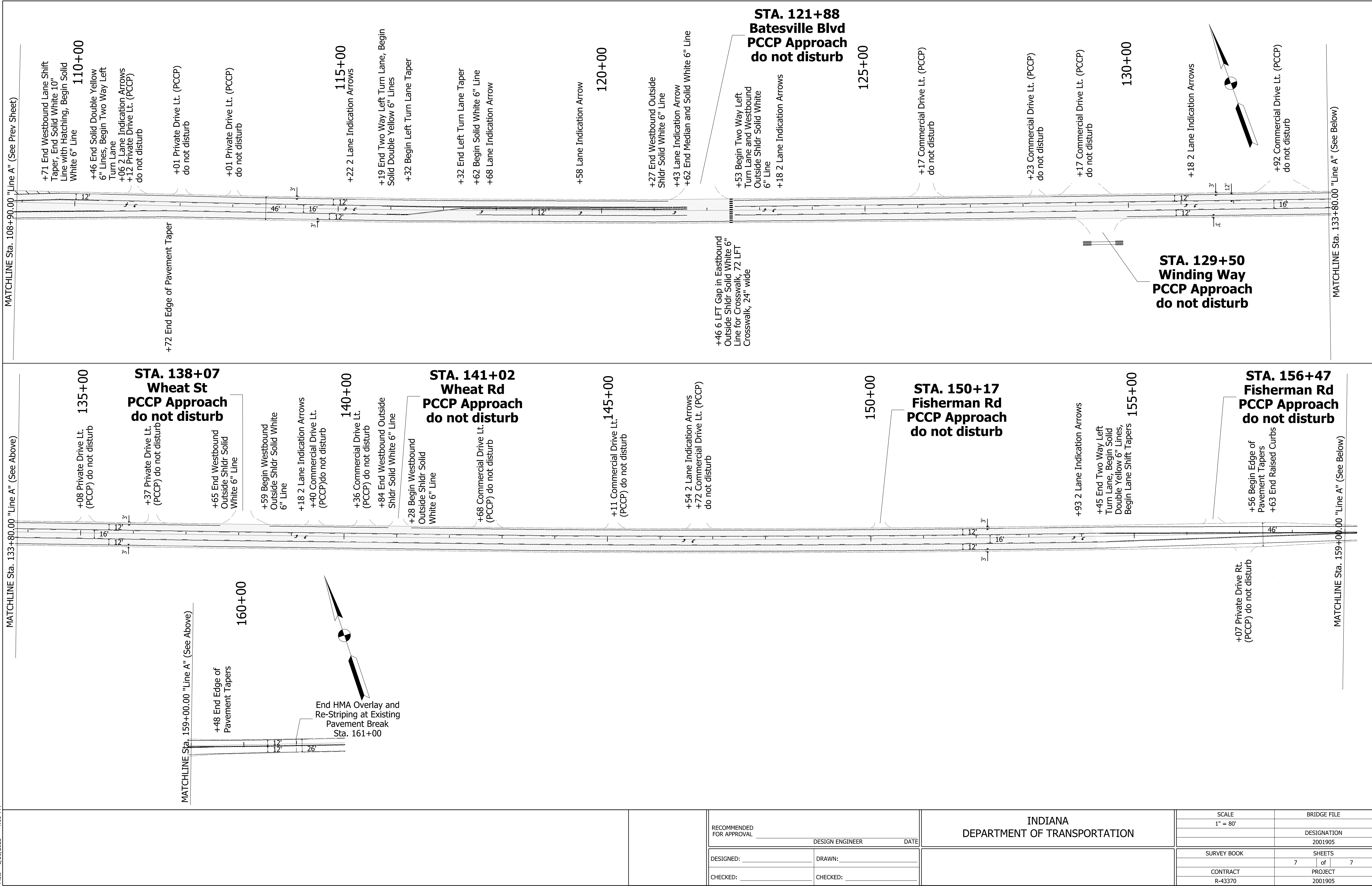
RECOMMENDED FOR APPROVAL _____	DESIGN ENGINEER _____	DATE _____
DESIGNED: _____	DRAWN: _____	
CHECKED: _____	CHECKED: _____	

INDIANA DEPARTMENT OF TRANSPORTATION
DETAIL SHEET - CONSTRUCTION DETAILS MAINLINE STA. 59+10 TO STA. 108+90

SCALE 1" = 80'	BRIDGE FILE
	DESIGNATION 2001905
SURVEY BOOK	SHEETS 6 of 7
CONTRACT R-43370	PROJECT 2001905

Plot: 1/30/2025 4:10 PM

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Model: 08-Detail 03



RECOMMENDED FOR APPROVAL _____	
DESIGNED: _____	DRAWN: _____
CHECKED: _____	CHECKED: _____

INDIANA DEPARTMENT OF TRANSPORTATION	
SURVEY BOOK _____	
CONTRACT R-43370	

SCALE 1" = 80'	BRIDGE FILE	
	DESIGNATION 2001905	
	SURVEY BOOK	SHEETS
	7	of 7
	PROJECT	2001905

Appendix C

Coordination



United States Department of the Interior

FISH AND WILDLIFE SERVICE
Indiana Ecological Services Field Office
620 South Walker Street
Bloomington, IN 47403-2121
Phone: (812) 334-4261 Fax: (812) 334-4273



In Reply Refer To:

03/07/2025 13:09:25 UTC

Project Code: 2023-0035584

Project Name: Des. No 2001905 HMA Overlay SR 46

Subject: List of threatened and endangered species that may occur in your proposed project location or may be affected by your proposed project

To Whom It May Concern:

The enclosed species list identifies threatened, endangered, proposed and candidate species, as well as proposed and final designated critical habitat, that may occur within the boundary of your proposed project and/or may be affected by your proposed project. The species list fulfills the requirements of the U.S. Fish and Wildlife Service (Service) under section 7(c) of the Endangered Species Act (Act) of 1973, as amended (16 U.S.C. 1531 *et seq.*).

New information based on updated surveys, changes in the abundance and distribution of species, changed habitat conditions, or other factors could change this list. Please feel free to contact us if you need more current information or assistance regarding the potential impacts to federally proposed, listed, and candidate species and federally designated and proposed critical habitat. Please note that under 50 CFR 402.12(e) of the regulations implementing section 7 of the Act, the accuracy of this species list should be verified after 90 days. This verification can be completed formally or informally as desired. The Service recommends that verification be completed by visiting the IPaC website at regular intervals during project planning and implementation for updates to species lists and information. An updated list may be requested through the IPaC system by completing the same process used to receive the enclosed list.

The purpose of the Act is to provide a means whereby threatened and endangered species and the ecosystems upon which they depend may be conserved. Under sections 7(a)(1) and 7(a)(2) of the Act and its implementing regulations (50 CFR 402 *et seq.*), Federal agencies are required to utilize their authorities to carry out programs for the conservation of threatened and endangered species and to determine whether projects may affect threatened and endangered species and/or designated critical habitat.

Please use the species list provided and visit the U.S. Fish and Wildlife Service's Region 3 Section 7 Technical Assistance website at - <http://www.fws.gov/midwest/endangered/section7/s7process/index.html>. This website contains step-by-step instructions which will help you

determine if your project will have an adverse effect on listed species and will help lead you through the Section 7 process. For all **wind energy projects and projects that include installing towers that use guy wires or are over 200 feet in height**, please contact this field office directly for assistance, even if no federally listed plants, animals or critical habitat are present within your proposed project or may be affected by your proposed project.

A Biological Assessment is required for construction projects (or other undertakings having similar physical impacts) that are major Federal actions significantly affecting the quality of the human environment as defined in the National Environmental Policy Act (42 U.S.C. 4332(2)(c)). For projects other than major construction activities, the Service suggests that a biological evaluation similar to a Biological Assessment be prepared to determine whether the project may affect listed or proposed species and/or designated or proposed critical habitat. Recommended contents of a Biological Assessment are described at 50 CFR 402.12.

If a Federal agency determines, based on the Biological Assessment or biological evaluation, that listed species and/or designated critical habitat may be affected by the proposed project, the agency is required to consult with the Service pursuant to 50 CFR 402. In addition, the Service recommends that candidate species, proposed species and proposed critical habitat be addressed within the consultation. More information on the regulations and procedures for section 7 consultation, including the role of permit or license applicants, can be found in the "Endangered Species Consultation Handbook" at:

<https://www.fws.gov/sites/default/files/documents/endangered-species-consultation-handbook.pdf>

Migratory Birds: In addition to responsibilities to protect threatened and endangered species under the Endangered Species Act (ESA), there are additional responsibilities under the Migratory Bird Treaty Act (MBTA) and the Bald and Golden Eagle Protection Act (BGEPA) to protect native birds from project-related impacts. Any activity, intentional or unintentional, resulting in take of migratory birds, including eagles, is prohibited unless otherwise permitted by the U.S. Fish and Wildlife Service (50 C.F.R. Sec. 10.12 and 16 U.S.C. Sec. 668(a)). For more information regarding these Acts, see <https://www.fws.gov/program/migratory-bird-permit/what-we-do>.

The MBTA has no provision for allowing take of migratory birds that may be unintentionally killed or injured by otherwise lawful activities. It is the responsibility of the project proponent to comply with these Acts by identifying potential impacts to migratory birds and eagles within applicable NEPA documents (when there is a federal nexus) or a Bird/Eagle Conservation Plan (when there is no federal nexus). Proponents should implement conservation measures to avoid or minimize the production of project-related stressors or minimize the exposure of birds and their resources to the project-related stressors. For more information on avian stressors and recommended conservation measures, see <https://www.fws.gov/library/collections/threats-birds>.

In addition to MBTA and BGEPA, Executive Order 13186: *Responsibilities of Federal Agencies to Protect Migratory Birds*, obligates all Federal agencies that engage in or authorize activities that might affect migratory birds, to minimize those effects and encourage conservation measures that will improve bird populations. Executive Order 13186 provides for the protection of both migratory birds and migratory bird habitat. For information regarding the implementation of

Executive Order 13186, please visit <https://www.fws.gov/partner/council-conservation-migratory-birds>.

We appreciate your concern for threatened and endangered species. The Service encourages Federal agencies to include conservation of threatened and endangered species into their project planning to further the purposes of the Act. **Please include the Consultation Code in the header of this letter with any request for consultation or correspondence about your project that you submit to our office.**

Attachment(s):

- Official Species List
- Bald & Golden Eagles
- Migratory Birds
- Wetlands

OFFICIAL SPECIES LIST

This list is provided pursuant to Section 7 of the Endangered Species Act, and fulfills the requirement for Federal agencies to "request of the Secretary of the Interior information whether any species which is listed or proposed to be listed may be present in the area of a proposed action".

This species list is provided by:

Indiana Ecological Services Field Office

620 South Walker Street
Bloomington, IN 47403-2121
(812) 334-4261

PROJECT SUMMARY

Project Code: 2023-0035584

Project Name: Des. No 2001905 HMA Overlay SR 46

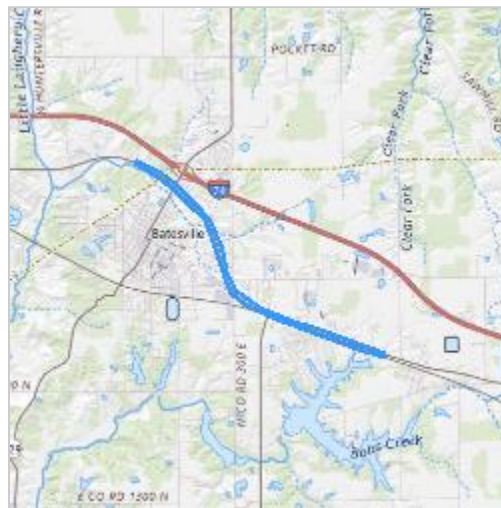
Project Type: Road/Hwy - Maintenance/Modification

Project Description: This section of SR 46 is an undivided 4-lane principal arterial roadway. The lanes are 12 feet wide with a curb ramp shoulder. Motorists regularly speed through this area, causing safety concerns for property owners, pedestrians, and other motorists. This project is to reduce the number of lanes, and convert the median to a two-way center turn lane. This will include reducing 2 lanes in each direction down to 1, and creating a 16 foot wide center left turn lane. The street will also have partial depth patching in areas of concern. At the intersection of SR 229 and SR 46, SR 229 will receive a hot mixed asphalt (HMA) overlay that will extend 615 feet south, as this area was missed during the last HMA overlay of SR 229. No excavation or cabinet work will take place. All work is confined to the roadway, no permanent or temporary Right-of-Way (ROW) will be needed.

No bats were found within a 0.5 mile radius of the project area. No trees will be trimmed. No excavation will occur.

Project Location:

The approximate location of the project can be viewed in Google Maps: <https://www.google.com/maps/@39.296854749999994,-85.20994954625459,14z>



Counties: Franklin and Ripley counties, Indiana

ENDANGERED SPECIES ACT SPECIES

There is a total of 3 threatened, endangered, or candidate species on this species list.

Species on this list should be considered in an effects analysis for your project and could include species that exist in another geographic area. For example, certain fish may appear on the species list because a project could affect downstream species.

IPaC does not display listed species or critical habitats under the sole jurisdiction of NOAA Fisheries¹, as USFWS does not have the authority to speak on behalf of NOAA and the Department of Commerce.

See the "Critical habitats" section below for those critical habitats that lie wholly or partially within your project area under this office's jurisdiction. Please contact the designated FWS office if you have questions.

-
1. [NOAA Fisheries](#), also known as the National Marine Fisheries Service (NMFS), is an office of the National Oceanic and Atmospheric Administration within the Department of Commerce.

MAMMALS

NAME	STATUS
Indiana Bat <i>Myotis sodalis</i> There is final critical habitat for this species. Your location does not overlap the critical habitat. Species profile: https://ecos.fws.gov/ecp/species/5949	Endangered

CLAMS

NAME	STATUS
Salamander Mussel <i>Simpsonaias ambigua</i> There is proposed critical habitat for this species. Your location does not overlap the critical habitat. Species profile: https://ecos.fws.gov/ecp/species/6208	Proposed Endangered

INSECTS

NAME	STATUS
Monarch Butterfly <i>Danaus plexippus</i> There is proposed critical habitat for this species. Your location does not overlap the critical habitat. Species profile: https://ecos.fws.gov/ecp/species/9743	Proposed Threatened

CRITICAL HABITATS

THERE ARE NO CRITICAL HABITATS WITHIN YOUR PROJECT AREA UNDER THIS OFFICE'S JURISDICTION.

YOU ARE STILL REQUIRED TO DETERMINE IF YOUR PROJECT(S) MAY HAVE EFFECTS ON ALL ABOVE LISTED SPECIES.

BALD & GOLDEN EAGLES

Bald and Golden Eagles are protected under the Bald and Golden Eagle Protection Act ² and the Migratory Bird Treaty Act (MBTA) ¹. Any person or organization who plans or conducts activities that may result in impacts to Bald or Golden Eagles, or their habitats, should follow appropriate regulations and consider implementing appropriate avoidance and minimization measures, as described in the various links on this page.

-
1. The [Bald and Golden Eagle Protection Act](#) of 1940.
 2. The [Migratory Birds Treaty Act](#) of 1918.
 3. 50 C.F.R. Sec. 10.12 and 16 U.S.C. Sec. 668(a)

There are Bald Eagles and/or Golden Eagles in your [project](#) area.

Measures for Proactively Minimizing Eagle Impacts

For information on how to best avoid and minimize disturbance to nesting bald eagles, please review the [National Bald Eagle Management Guidelines](#). You may employ the timing and activity-specific distance recommendations in this document when designing your project/activity to avoid and minimize eagle impacts. For bald eagle information specific to Alaska, please refer to [Bald Eagle Nesting and Sensitivity to Human Activity](#).

The FWS does not currently have guidelines for avoiding and minimizing disturbance to nesting Golden Eagles. For site-specific recommendations regarding nesting Golden Eagles, please consult with the appropriate Regional [Migratory Bird Office](#) or [Ecological Services Field Office](#).

If disturbance or take of eagles cannot be avoided, an [incidental take permit](#) may be available to authorize any take that results from, but is not the purpose of, an otherwise lawful activity. For assistance making this determination for Bald Eagles, visit the [Do I Need A Permit Tool](#). For assistance making this determination for golden eagles, please consult with the appropriate Regional [Migratory Bird Office](#) or [Ecological Services Field Office](#).

Ensure Your Eagle List is Accurate and Complete

If your project area is in a poorly surveyed area in IPaC, your list may not be complete and you may need to rely on other resources to determine what species may be present (e.g. your local FWS field office, state surveys, your own surveys). Please review the [Supplemental Information on Migratory Birds and Eagles](#), to help you properly interpret the report for your specified location, including determining if there is sufficient data to ensure your list is accurate.

For guidance on when to schedule activities or implement avoidance and minimization measures to reduce impacts to bald or golden eagles on your list, see the "Probability of Presence Summary" below to see when these bald or golden eagles are most likely to be present and breeding in your project area.

NAME	BREEDING SEASON
Bald Eagle <i>Haliaeetus leucocephalus</i> This is not a Bird of Conservation Concern (BCC) in this area, but warrants attention because of the Eagle Act or for potential susceptibilities in offshore areas from certain types of development or activities. https://ecos.fws.gov/ecp/species/1626	Breeds Sep 1 to Jul 31

PROBABILITY OF PRESENCE SUMMARY

The graphs below provide our best understanding of when birds of concern are most likely to be present in your project area. This information can be used to tailor and schedule your project activities to avoid or minimize impacts to birds. Please make sure you read ["Supplemental Information on Migratory Birds and Eagles"](#), specifically the FAQ section titled "Proper Interpretation and Use of Your Migratory Bird Report" before using or attempting to interpret this report.

Probability of Presence (■)

Green bars; the bird's relative probability of presence in the 10km grid cell(s) your project overlaps during that week of the year.

Breeding Season (■)

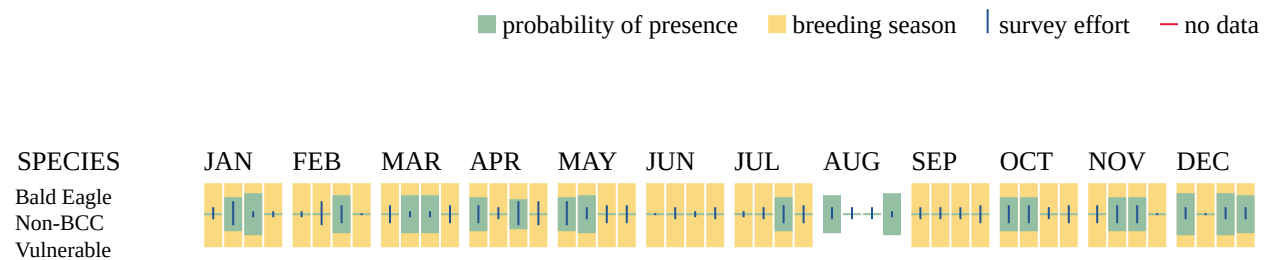
Yellow bars; liberal estimate of the timeframe inside which the bird breeds across its entire range.

Survey Effort (|)

Vertical black lines; the number of surveys performed for that species in the 10km grid cell(s) your project area overlaps.

No Data (—)

A week is marked as having no data if there were no survey events for that week.



Additional information can be found using the following links:

- Eagle Management <https://www.fws.gov/program/eagle-management>
- Measures for avoiding and minimizing impacts to birds <https://www.fws.gov/library/collections/avoiding-and-minimizing-incidental-take-migratory-birds>
- Nationwide avoidance and minimization measures for birds <https://www.fws.gov/sites/default/files/documents/nationwide-standard-conservation-measures.pdf>
- Supplemental Information for Migratory Birds and Eagles in IPaC <https://www.fws.gov/media/supplemental-information-migratory-birds-and-bald-and-golden-eagles-may-occur-project-action>

MIGRATORY BIRDS

The Migratory Bird Treaty Act (MBTA) ¹ prohibits the take (including killing, capturing, selling, trading, and transport) of protected migratory bird species without prior authorization by the Department of Interior U.S. Fish and Wildlife Service (Service). The incidental take of migratory birds is the injury or death of birds that results from, but is not the purpose, of an activity. The Service interprets the MBTA to prohibit incidental take.

-
1. The [Migratory Birds Treaty Act](#) of 1918.
 2. The [Bald and Golden Eagle Protection Act](#) of 1940.
 3. 50 C.F.R. Sec. 10.12 and 16 U.S.C. Sec. 668(a)

For guidance on when to schedule activities or implement avoidance and minimization measures to reduce impacts to migratory birds on your list, see the "Probability of Presence Summary" below to see when these birds are most likely to be present and breeding in your project area.

NAME	BREEDING SEASON
Bald Eagle <i>Haliaeetus leucocephalus</i> This is not a Bird of Conservation Concern (BCC) in this area, but warrants attention because of the Eagle Act or for potential susceptibilities in offshore areas from certain types of development or activities. https://ecos.fws.gov/ecp/species/1626	Breeds Sep 1 to Jul 31
Cerulean Warbler <i>Setophaga cerulea</i> This is a Bird of Conservation Concern (BCC) throughout its range in the continental USA and Alaska. https://ecos.fws.gov/ecp/species/2974	Breeds Apr 23 to Jul 20
Chimney Swift <i>Chaetura pelagica</i> This is a Bird of Conservation Concern (BCC) throughout its range in the continental USA and Alaska. https://ecos.fws.gov/ecp/species/9406	Breeds Mar 15 to Aug 25
Field Sparrow <i>Spizella pusilla</i> This is a Bird of Conservation Concern (BCC) only in particular Bird Conservation Regions (BCRs) in the continental USA https://ecos.fws.gov/ecp/species/9446	Breeds Mar 1 to Aug 15
Grasshopper Sparrow <i>Ammodramus savannarum perpallidus</i> This is a Bird of Conservation Concern (BCC) only in particular Bird Conservation Regions (BCRs) in the continental USA https://ecos.fws.gov/ecp/species/8329	Breeds Jun 1 to Aug 20
Kentucky Warbler <i>Geothlypis formosa</i> This is a Bird of Conservation Concern (BCC) throughout its range in the continental USA and Alaska. https://ecos.fws.gov/ecp/species/9443	Breeds Apr 20 to Aug 20
Prairie Warbler <i>Setophaga discolor</i> This is a Bird of Conservation Concern (BCC) throughout its range in the continental USA and Alaska. https://ecos.fws.gov/ecp/species/9513	Breeds May 1 to Jul 31
Prothonotary Warbler <i>Protonotaria citrea</i> This is a Bird of Conservation Concern (BCC) throughout its range in the continental USA and Alaska. https://ecos.fws.gov/ecp/species/9439	Breeds Apr 1 to Jul 31
Red-headed Woodpecker <i>Melanerpes erythrocephalus</i> This is a Bird of Conservation Concern (BCC) throughout its range in the continental USA and Alaska. https://ecos.fws.gov/ecp/species/9398	Breeds May 10 to Sep 10

NAME	BREEDING SEASON
Rusty Blackbird <i>Euphagus carolinus</i> This is a Bird of Conservation Concern (BCC) only in particular Bird Conservation Regions (BCRs) in the continental USA https://ecos.fws.gov/ecp/species/9478	Breeds elsewhere
Wood Thrush <i>Hylocichla mustelina</i> This is a Bird of Conservation Concern (BCC) throughout its range in the continental USA and Alaska. https://ecos.fws.gov/ecp/species/9431	Breeds May 10 to Aug 31

PROBABILITY OF PRESENCE SUMMARY

The graphs below provide our best understanding of when birds of concern are most likely to be present in your project area. This information can be used to tailor and schedule your project activities to avoid or minimize impacts to birds. Please make sure you read "[Supplemental Information on Migratory Birds and Eagles](#)", specifically the FAQ section titled "Proper Interpretation and Use of Your Migratory Bird Report" before using or attempting to interpret this report.

Probability of Presence (■)

Green bars; the bird's relative probability of presence in the 10km grid cell(s) your project overlaps during that week of the year.

Breeding Season (■)

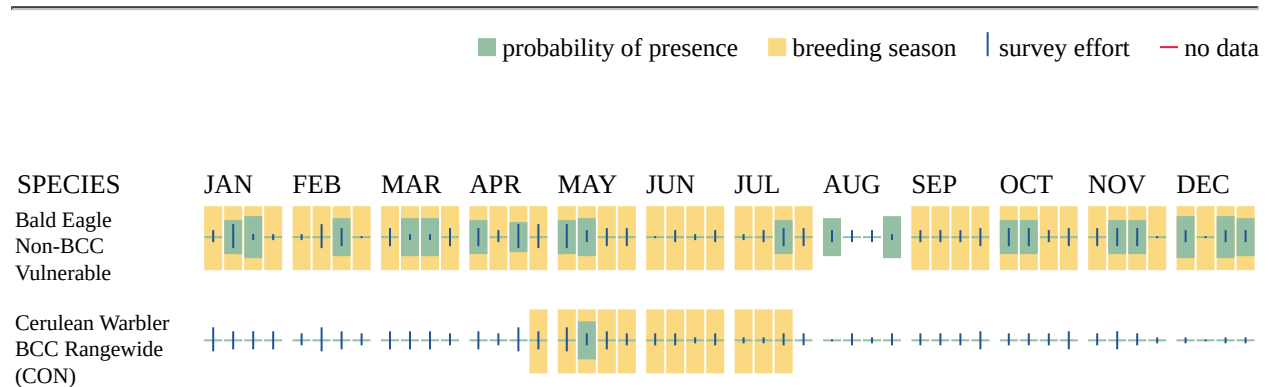
Yellow bars; liberal estimate of the timeframe inside which the bird breeds across its entire range.

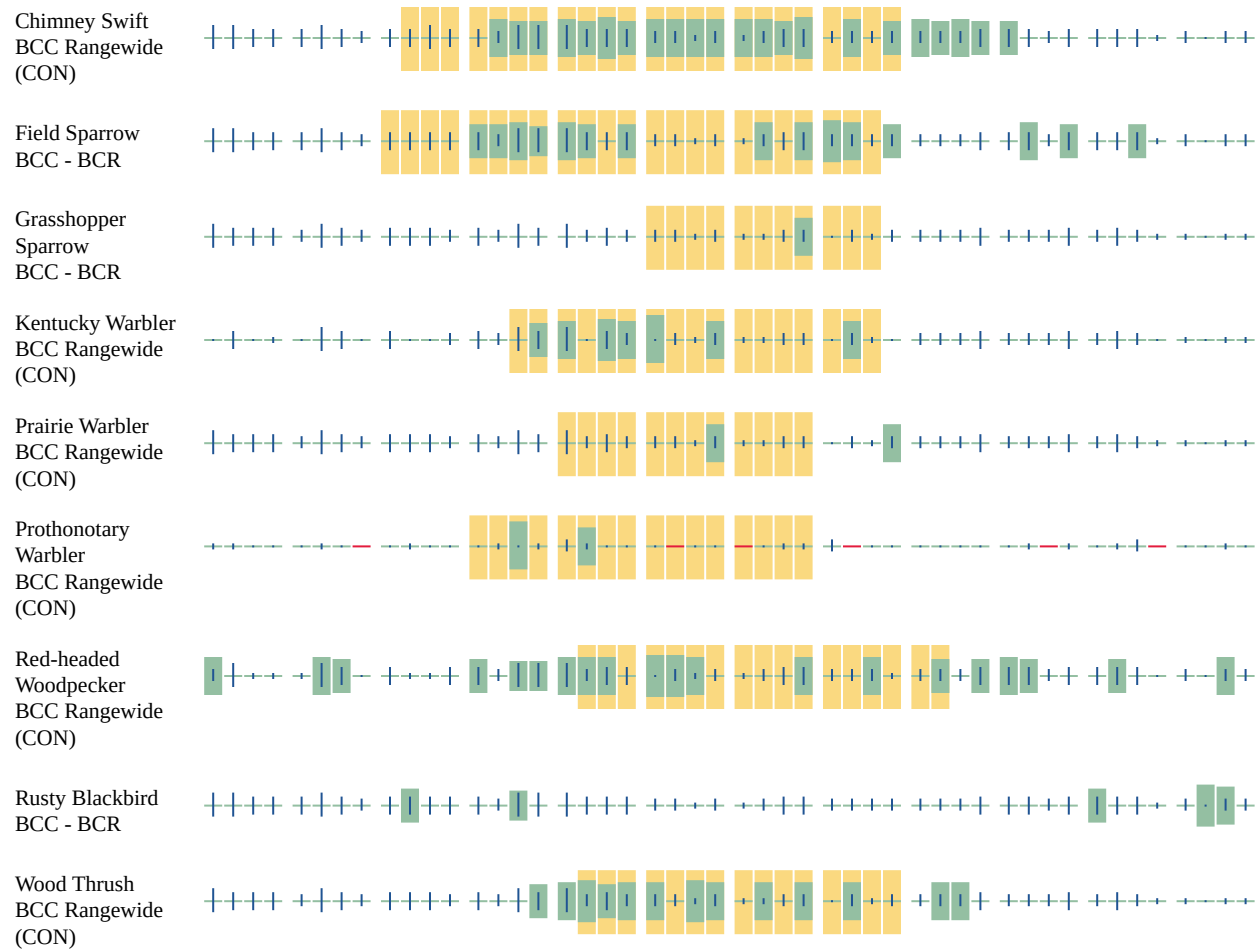
Survey Effort (|)

Vertical black lines; the number of surveys performed for that species in the 10km grid cell(s) your project area overlaps.

No Data (—)

A week is marked as having no data if there were no survey events for that week.





Additional information can be found using the following links:

- Eagle Management <https://www.fws.gov/program/eagle-management>
- Measures for avoiding and minimizing impacts to birds <https://www.fws.gov/library/collections/avoiding-and-minimizing-incidental-take-migratory-birds>
- Nationwide avoidance and minimization measures for birds
- Supplemental Information for Migratory Birds and Eagles in IPaC <https://www.fws.gov/media/supplemental-information-migratory-birds-and-bald-and-golden-eagles-may-occur-project-action>

WETLANDS

Impacts to [NWJ wetlands](#) and other aquatic habitats may be subject to regulation under Section 404 of the Clean Water Act, or other State/Federal statutes.

For more information please contact the Regulatory Program of the local [U.S. Army Corps of Engineers District](#).

Please note that the NWI data being shown may be out of date. We are currently working to update our NWI data set. We recommend you verify these results with a site visit to determine the actual extent of wetlands on site.

FRESHWATER POND

- PUBGx

RIVERINE

- R2UBH
- R4SBC

FRESHWATER FORESTED/SHRUB WETLAND

- PFO1A
- PFO1C

IPAC USER CONTACT INFORMATION

Agency: Indiana Department of Transportation

Name: Erin Carleton

Address: 185 Agrico Ln

City: Seymour

State: IN

Zip: 47274

Email: ecarleton@indot.in.gov

Phone: 8125243988

LEAD AGENCY CONTACT INFORMATION

Lead Agency: Federal Highway Administration

You have indicated that your project falls under or receives funding through the following special project authorities:

- BIPARTISAN INFRASTRUCTURE LAW (BIL) (OTHER)

Federal Highway Administration (FHWA), Federal Railroad
Administration (FRA), and Federal Transit Administration (FTA)

Programmatic Biological Opinion/Conference Opinion for
Transportation Projects in the Range of the Indiana Bat, Northern
Long-Eared Bat, and Tricolored Bat

Appendix B: Project Submittal Form

Updated December 2024

The use of the Assisted Determination Key in the U.S. Fish and Wildlife Service (Service) Information for Planning and Conservation (IPaC) System is strongly recommended for submitting project-level information to the Service for use of the range-wide programmatic consultation covering actions that may affect the Indiana bat, northern long-eared bat (NLEB), or tricolored bat (TCB). However, if not using the assisted determination key, transportation agencies must provide this submittal form (or a comparable Service approved form) with project-level information to the Service. The completed form should be submitted to the appropriate Service Field Office prior to project commencement. For more information, see the Standard Operating Procedure for Site Specific Project(s) Submission in the User's Guide (Section 3).

By submitting this form, the transportation agency ensures that each component of the proposed project(s) adheres to the criteria and conditions of the 2024 range-wide programmatic biological opinion (PBO). Upon submittal of this form, the appropriate Service Field Office may review the project-specific information provided and request additional information. For projects that may affect but are not likely to adversely affect (NLAA) the Indiana bat, NLEB, or TCB, if the applying transportation agency is not contacted by the Service with any questions or concerns within 14 calendar days of form submittal, it may proceed under the range-wide programmatic consultation and assume concurrence of the NLAA determination made by the Service in the PBO. For projects that may affect and are likely to adversely affect (LAA) the Indiana bat, NLEB, or TCB, the appropriate Service Field Office will respond¹ within 30 calendar days of receiving a complete project-level submission, which includes, but may not be limited to this completed form.

1. Date: 3/11/25

2. Lead agency: FHWA

*This refers to the **Federal** governmental lead action agency initiating consultation; select **FHWA, FRA, FTA, or FHWA/FRA Program Assignment State or Categorical Exclusion Assignment State** as appropriate.*

¹ Service Field Offices should use the LAA verification letter template for projects that may affect, and are likely to adversely affect the Indiana bat, NLEB, or TCB.

3. Requesting agency: INDOT

This refers to the transportation agency completing the form (it may or may not be the same as the Lead Agency).

- Name: Erin Carleton
- Title: Environmental Manager 2
- Phone: 812-524-3988
- Email: ecarleton@indot.in.gov



4. Project code:² 2023-0035584

5. Project name(s): Des. No 2001905 Lane Reduction SR 46 Batesville

6. Project description:

Please attach additional documentation or explanatory text if necessary.

This section of SR 46 is an undivided 4-lane principal arterial roadway. The lanes are 12 feet wide with a curb ramp shoulder. Motorists regularly speed through this area, causing safety concerns for property owners, pedestrians, and other motorists. This project is to reduce the number of lanes, and convert the median to a two-way center turn lane. This will include reducing 2 lanes in each direction down to 1, and creating a 16 foot wide center left turn lane. The street will also have partial depth patching in areas of concern. At the intersection of SR 229 and SR 46, SR 229 will receive a hot mixed asphalt (HMA) overlay that will extend 615 feet south, as this area was missed during the last HMA overlay of SR 229. No cabinet work will take place. All work is confined to the roadway, no permanent or temporary Right-of-Way (ROW) will be needed.

New signals will be placed at Tekulve Rd, and signals will be replaced in kind at SR 229, and shifted a few feet at SR 129. Permanent and temporary lighting is anticipated.

No bats were found within a 0.5 mile radius of the project area. No trees will be trimmed. No excavation will occur.

7. Project location (county, state): Batesville, Ripley and Franklin County, Indiana

If not delineated in IPaC, attach shape files.

8. For species **other than** Indiana bat, NLEB, and TCB (from IPaC official species list):

- ☒ No effect – project(s) are inside the species range, but no suitable habitat (see additional information attached).
- ☐ May affect – see additional information provided for those species (see attached or forthcoming).

² Available through IPaC System Official Species List: <https://ipac.ecosphere.fws.gov/>

Please confirm and identify how each component of the proposed project(s) adheres to the criteria of the PBO by completing the following (see User Guide Section 2.0):

NO EFFECT

9. For Indiana bat/NLEB/TCB, if applicable, select your no effect determination:

- ☐ No effect – project(s) are outside the species' range.
- ☐ No effect – project(s) are inside the species range with no suitable habitat³ within the project action area⁴; project(s) must also be greater than 0.5 miles from any hibernaculum.
- ☐ No effect – project(s) do not cause any stressors⁵ to the covered bat species, such as those that do not involve ground disturbance, vibrations, noise above background levels (including general traffic), temporary or new/additional permanent lighting, tree removal/trimming, nor bridge, culvert, and structure activities.
- ☐ No effect – project(s) includes percussive activities in suitable habitat (not related to tree removal and/or bridge, culvert, or structure work) that involve noise/vibration above existing background levels and are conducted greater than 0.5 miles (0.8 km) of a hibernaculum during the inactive season.
- ☐ No effect – project(s) includes the removal, replacement, or alteration of bridge, culvert, or structure that does not meet the minimum culvert dimensions (see the Service's current survey guidance).

Proceed with this form to identify how other components of the proposed project adhere to the criteria of the PBO.

MAY AFFECT, NOT LIKELY TO ADVERSELY EFFECT – W/O AMMS

10. For Indiana bat/NLEB/TCB, if applicable, select your may affect, NLAA determination (without implementation of AMMs):

- ☐ NLAA – project(s) are inside the species range and within suitable bat habitat, but have **negative** bat presence/absence (P/A) surveys; must also

³ Refer to the Service's Range-wide Bat Survey Guidelines at <https://www.fws.gov/library/collections/range-wide-indiana-bat-and-northern-long-eared-bat-survey-guidelines>.

⁴ The "action area" is defined as "all areas to be affected directly or indirectly by the Federal action and not merely the immediate area involved in the action." Further clarification is provided by the national consultation FAQs at: <https://www.fws.gov/glossary/action-area>.

⁵ Examples of activities that **do not cause stressors** may include striping roadways, unlighted road signage, railroad crossing signals, signal lighting, minor road repair such as asphalt fill of potholes, among others.

be greater than 0.5 miles from any hibernaculum.

- ☐ NLAA – project(s) include percussive activities within suitable habitat (not related to tree removal and/or bridge, culvert, structure work) that cause noise/vibration above existing background levels; and are conducted greater than 0.5 miles (0.8 km) of a hibernaculum; no further than 100 ft (30.5 m) of the road/rail surface during the pup season; and not carried out between December 15 and February 15 in Zone 1 of the NLEB and TCB YR active ranges.
- ☐ NLAA – project(s) includes the removal, replacement, or alteration of bridges, culverts, or structures with no signs of bat use [e.g., bats, guano], and does not impact suitable habitat within the project action area.

Proceed with this form to identify how other components of the proposed project adhere to the criteria of the PBO.

MAY EFFECT, NOT LIKELY TO ADVERSELY AFFECT – WITH AMMs

11. For Indiana bat/NLEB/TCB, if applicable, document your may affect, NLAA determination (**with implementation of AMMs**) by completing the following section; use #13 to document AMMs):

a. Tree Removal/Trimming Activities

- ☐ NLAA – project(s) includes the removal/trimming of trees outside documented habitat⁶ within 100 ft (30.5m) from the road/rail surface during the inactive season; and all applicable lighting minimization measures will be implemented.

b. Bridge/Culvert/Structure Activities

Projects Proposed work:

Timing of work:

- ☐ NLAA – project(s) includes the removal, replacement, or alteration of bridges, culverts, or structures with a large number of bats (>5) observed or assuming bat use when conducted during the inactive season, so long as no hibernating bats are using the bridge, culvert, or structure.

⁶ See glossary in Appendix A for definition of documented habitat.

- ☐ NLAA – project(s) includes the removal, replacement, or alteration of bridges, culverts, or structures with a large number of bats (>5) observed or assuming bat use when conducted during the active season, so long as the covered bats species are **not likely to be disturbed/killed** and suitable roosting habitat is still available within the bridge, culvert, or structure.
- ☐ NLAA – project(s) includes the removal, replacement, or alteration of bridges, culverts, or structures with a small number of bats (≤ 5) observed when conducted during the inactive season, so long as no hibernating bats are using the bridge, culvert, or structure.
- ☐ NLAA – project(s) includes the removal, replacement, or alteration of bridges, culverts, or structures with a small number of bats (≤ 5) observed when conducted during the active season, so long as the covered bats species are **not likely to be disturbed/killed**.

c. Lighting

- ☒ Verify that all applicable lighting minimization measures will be implemented.

Proceed with this form to identify how other components of the proposed project adhere to the criteria of the PBO.

MAY AFFECT, LIKELY TO ADVERSELY AFFECT

12. For Indiana bat/NLEB/TCB, if applicable, document your may affect, LAA determination (**with implementation of AMMs**) by completing the following section (use #13 to document AMMs):

a. Tree Removal/Trimming Activities

Tree Removal/Trimming Activities in the Hibernating Range of the Indiana bat, NLEB, and TCB.

- ☐ LAA – project(s) includes the removal/trimming of trees **outside documented habitat** for the Indiana bat or NLEB or TCB within 100 ft (30.5m) from the road/rail surface during the pup season; all cleared/trimmed trees must be <9 in DBH.
- ☐ LAA – project(s) includes the removal/trimming of trees **outside documented habitat** for the Indiana bat or NLEB or TCB during the active season; excluding the pup season.
- ☐ LAA – project(s) includes the removal/trimming of trees **outside documented habitat** for the Indiana bat or NLEB or TCB beyond 100 ft (30.5m) from the road/rail surface during the inactive season.
- ☐ LAA – project(s) includes the removal/trimming of trees **within documented habitat** for the Indiana bat or NLEB or TCB during the inactive season.
- ☐ LAA – project(s) includes the removal/trimming of trees **within documented habitat** for the Indiana bat or NLEB or TCB during the active season; excluding the pup season.

Tree Removal/Trimming Activities in the **YR Active Ranges** of the NLEB and TCB.

- ☐ LAA – project(s) includes the removal/trimming of trees **outside documented habitat** for the NLEB or TCB within 100 ft (30.5m) from the road/rail surface during the pup season; all cleared/trimmed trees must be <9 in DBH.
- ☐ LAA – project(s) includes the removal/trimming of trees **outside documented habitat** for the NLEB or TCB anytime, excluding the pup season and Dec 15 – Feb 15*.
- ☐ LAA – project(s) includes the removal/trimming of trees **within documented habitat** for the NLEB or TCB anytime, excluding the pup season and Dec 15 – Feb 15*.

*For the YR active ranges of the NLEB and TCB, winter tree clearing restrictions from Dec. 15 – Feb. 15 do not apply in areas where the mean minimum temperature is above 40° F throughout the winter months (depicted as Zone 2 in Figure 9 of the PBO):

- ☐ Number of acres of trees 0-100 feet of existing road/rail surface proposed for removal/trimming: [Click or tap here to enter text.](#)
- ☐ Number of acres of trees 100-300 feet from edge of existing road/rail surface proposed for removal/trimming. [Click or tap here to enter text.](#)
- ☐ Number of acres of trees beyond 300 feet from edge of existing road/rail surface proposed for removal/trimming. [Click or tap here to enter text.](#)
- ☐ Verify that all tree removal/trimming occurs greater than 0.5 mile from any hibernaculum
- ☐ Verify trees removed/trimmed outside documented habitat for the Indiana bat or NLEB or TCB within 100 feet of the road/rail surface during the pup season are <9 in DBH,
- ☐ Verify no tree removal/trimming outside documented habitat for the Indiana bat, NLEB, or TCB beyond 100 feet of the road/rail surface during the pup season,
- ☐ Verify no tree removal/trimming of documented habitat for the Indiana bat, NLEB, or TCB during the pup season,
- ☐ Verify no tree removal/trimming of suitable habitat for the NLEB and/or TCB in Zone 1 of their YR active ranges between December 15 – February 15.

b. Bridge/Culvert/Structure Activities
Projects Proposed work:

Timing of work:

Date of Bridge/Culvert/Structure Assessment (if completed):

- ☐ Verify a small number of bats were observed (≤ 5).
- ☐ LAA – project(s) includes the removal, replacement, or alteration of bridges, culverts, or structures with a small number of bats (≤ 5) observed when conducted during the active season and the covered bats species are **likely to be disturbed/killed**.

c. Lighting

- ☐ Verify that all applicable lighting minimization measures will be implemented.

13. For Indiana bat/NLEB/TCB, if applicable to the action type, the following AMMs⁷ will be implemented:

- ☒ General AMM 1 (required for all projects)
- ☐ Tree Removal AMM 1 (required for all projects)
- ☐ Tree Removal AMM 2 (required for all projects)
- ☐ Tree Removal AMM 3 (required for NLAA)
- ☐ Tree Removal AMM 4 (required for LAA)
- ☐ Tree Removal AMM 5 (required for LAA)
- ☐ Tree Removal AMM 6 (required for LAA)
- ☐ Tree Removal AMM 7 (required for LAA)
- ☐ Bridge/Culvert/Structure AMM 1a (required for NLAA)
- ☐ Bridge/Culvert/Structure AMM 1b (required for NLAA)
- ☐ Bridge/Culvert/Structure AMM 2 (required for NLAA)
- ☐ Bridge/Culvert/Structure AMM 3a (required for NLAA)
- ☐ Bridge/Culvert/Structure AMM 3b (required for NLAA)
- ☐ Bridge/Culvert/Structure AMM 4 (required for NLAA)
- ☐ Bridge/Culvert/Structure AMM 5 (required for all projects)
- ☐ Bridge/Culvert/Structure AMM 6 (required for NLAA)
- ☒ Lighting AMM 1 (required for all projects during the active season)
- ☒ Lighting AMM 2 (required for all projects)

⁷ See AMMs (Appendix C) for more information on AMMs.

☒ Hibernacula AMM 1 (required for all projects)

14. If applicable, compensatory mitigation measures will also be required to offset adverse effects to the Indiana bat and/or NLEB. Select what type of program will be used to mitigate for the Indiana bat and/or NLEB:

☐ In-Lieu Fee Program, The Conservation Fund

☐ State, Regional, Recovery Unit-Specific In-Lieu Fee Program:

Program Name:

☐ Conservation Bank:

Bank Name:

Location:

☐ Local Conservation Site:

Site(s) Name:

Location:

Description:

From: [McWilliams, Robin](#)
To: [Carleton, Erin](#)
Subject: Re: [EXTERNAL] FW: Des no 2001905 IPaC Reevaluation
Date: Monday, March 24, 2025 3:18:43 PM
Attachments: [image001.png](#)

EXTERNAL EMAIL: This email was sent from outside your organization. Exercise caution when clicking links, opening attachments or taking further action, before validating its authenticity.

Hi Erin,

I did look at it but I guess I did not respond. See below:

We have reviewed this and concur the project may proceed under the new December 2024 range-wide programmatic consultation and assume concurrence of the NLAA determinations made by the Service in the PBO.

Thanks!

Robin

Robin McWilliams Munson
Fish and Wildlife Biologist/Transportation Liaison
U.S. Fish and Wildlife Service
Indiana Ecological Services Field Office
620 South Walker Street
Bloomington, IN 47403
Robin_McWilliams@fws.gov

***NEW* 812-902-1752**

[Mon-Thurs 8:30-4:30p](#)

From: Carleton, Erin <ECarleton@indot.IN.gov>
Sent: Monday, March 24, 2025 1:51 PM
To: McWilliams, Robin <robin_mcwilliams@fws.gov>
Subject: [EXTERNAL] FW: Des no 2001905 IPaC Reevaluation

This email has been received from outside of DOI - Use caution before clicking on links, opening attachments, or responding.

Hi Robin,

I sent this to you and Susan while you were on medical leave. Have you had a chance to review it

yet? It's a resubmittal of a project that now includes new lighting.

Erin Carleton

Environmental Manager 2

Indiana Department of Transportation

Seymour District

Phone: 812-524-3988

Email: ecarleton@indot.in.gov

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From: Carleton, Erin

Sent: Tuesday, March 11, 2025 3:43 PM

To: McWilliams, Robin <robin_mcwilliams@fws.gov>

Cc: Schwering, Taylor <TSchwering@indot.IN.gov>; Dye, David <DDYE@indot.IN.gov>;
san_e_cooper@fws.gov

Subject: Des no 2001905 IPaC Reevaluation

Hi Robin,

We have an in-house project requiring a reevaluation. This Programmatic Biological Opinion/Conference Opinion Project Submittal Form for Transportation Projects in the Range of the Indiana bat for Des 2001905. This is a resubmittal, as the original project was in a different area, and no permanent lighting was initially anticipated. INDOT concurs with the NLAA finding with General AMM 1, Lighting AMM 1 and 2, and Hibernacula AMM 1, with No Effect for other species. The species list and project description have been uploaded to the IPaC. We are submitting this to USFWS for your review. Please let us know within 14 calendar days if you have any questions or concerns.

Thanks, hope you are feeling better soon.

Erin Carleton

Environmental Manager 2

Indiana Department of Transportation

Seymour District

Phone: 812-524-3988

Email: ecarleton@indot.in.gov

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Organization and Project Information

Organization Name: INDOT - Seymour District

First Name: Erin

Last Name: Carleton

Phone: (812) 524-3988

Email: ecarleton@indot.in.gov

Customer Id: 2001905

Project Title: Roadway Lane Reduction SR 46 in
Batesville, 2001905

Project Description: Based on traffic counts and the size of the road, the preferred alternative is to reduce the number of lanes, and convert the median to a two-way center turn lane. This will include reducing 2 lanes in each direction down to 1, and creating a 16 foot wide center left turn lane. The street will also have partial depth patching in areas of concern. Signal heads at SR 129 will need to be shifted.

Environmental Assessment Report

Geological Hazards:

1. High liquefaction potential

Mineral Resources:

Active or abandoned mineral resources extraction sites:

1. Abandoned Industrial Minerals Quarries

Disclaimer:

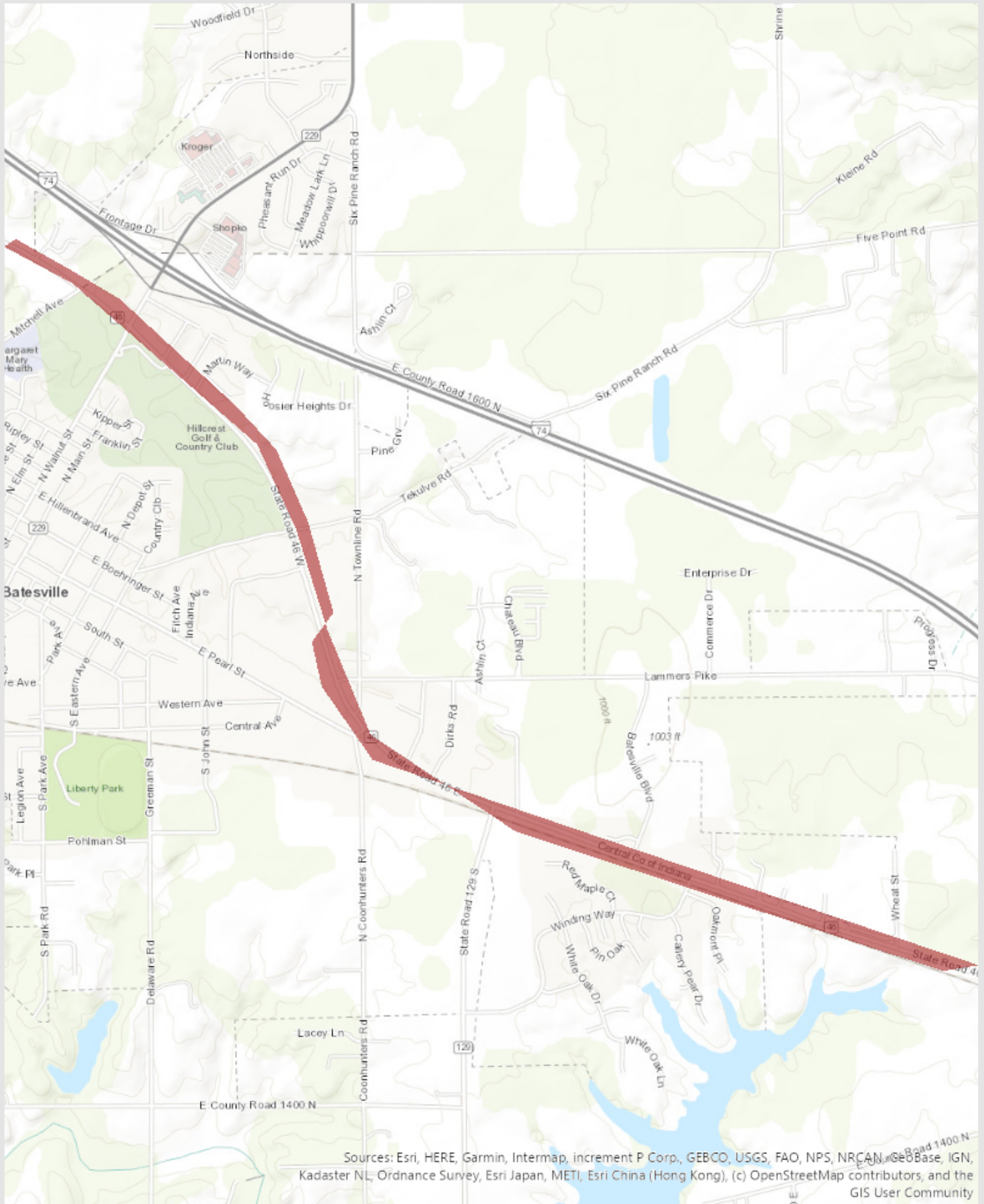
This document was compiled by Indiana University, Indiana Geological Survey, using data believed to be accurate; however, a degree of error is inherent in all data. This product is distributed "AS-IS" without warranties of any kind, either expressed or implied, including but not limited to warranties of suitability to a particular purpose or use. No attempt has been made in either the design or production of these data and document to define the limits or jurisdiction of any federal, state, or local government. The data used to assemble this document are intended for use only at the published scale of the source data or smaller (see the metadata links below) and are for reference purposes only. They are not to be construed as a legal document or survey instrument. A detailed on-the-ground survey and historical analysis of a single site may differ from these data and this document.

This information was furnished by Indiana Geological Survey

Address: 1001 E. 10th St., Bloomington, IN 47405

Email: IGSEnvir@indiana.edu

Phone: (812) 855-7428





Metadata:

https://portal.igs.indiana.edu/arcgis/rest/services/Seismic_Earthquake_Liquefaction_Potential/MapServer/info/metadata/

https://portal.igs.indiana.edu/arcgis/rest/services/Industrial_Minerals_Quarries_Abandoned/MapServer/info/metadata/

Appendix D

Cultural Resources

Minor Projects PA Project Submittal and Assessment Form

INDOT

SECTION 1

Submittal of this form is only required for projects where Category B applies. Projects qualifying under Category A do not require submittal of this form. SECTION 2 (for Conditions of Category B.1 for curb/sidewalk) or SECTION 3 (for Conditions of Category B.9 for drainage structures) may be required as determined by INDOT-Cultural Resources Office (INDOT-CRO) review. INDOT-CRO will notify applicant if the Minor Projects PA does not apply.

Part I: Project Information-Completed by Applicant (Consultant/PM/Project Sponsor/INDOT District Staff) *

**A qualified professional historian (QP) is not required to complete Part I INDOT-Cultural Resources Office (INDOT-CRO) staff will be responsible for completion of Part II.*

Original Submission Date: 1/9/2023

Amended Submission Date*: 1/20/2023

**Consult with INDOT-CRO to determine whether an amendment is required. For revisions/updates to original form, please detail in applicable sections below. Please use red font to distinguish the revisions/updates.*

Submitted By (Provide Name and Firm/Organization): Erin Carleton / INDOT – Seymour District

Project Designation Number: 2001905

Route Number: SR 46

Feature crossed (if applicable): N/A

City/Township: City of Batesville/Laughery and Adams Townships (Ripley County) Ray Township (Franklin County)

County: Ripley/Franklin Counties

Project Description: * Previous scope

This project is an HMA overlay on SR 46 starting at 0.3 mile west of SR 229 in Franklin County and ending on 1.07 miles east of SR 129 in Ripley County. There will be curb ramp upgrades in three locations:

- Southeast and southwest quadrants of the intersection of SR 46 and SR 129
- Northeast and Southeast quadrants of the intersection of SR 46 and Batesville Blvd.
- Southwest quadrant of the intersection of SR 46 and Winding Way

The curb ramp upgrade of SR 129/ SR 46 will feature pedestrian push buttons, APS and pedestrian signals with wiring. Batesville Blvd will receive two new curb ramps. Winding Way will receive one new curb ramp. Curb ramp upgrades will require a maximum excavation depth of 1 foot. Installation of the pedestrian signals at SR 129 will require a maximum excavation depth of 3 feet for the poles. Traffic will be maintained via arrow boards, single lane closures, sidewalk closures, and flagging traffic (if needed).

If the project includes any curb, curb ramp, or sidewalk work, please specify the location(s) of such work:

There will be curb ramp upgrades in three locations:

- Southeast and southwest quadrants of the intersection of SR 46 and SR 129
- Northeast and Southeast quadrants of the intersection of SR 46 and Batesville Blvd.
- Southwest quadrant of the intersection of SR 46 and Winding Way

For bridge or small structure projects, please list feature crossed, structure number, NBI number, and structure type: N/A

Minor Projects PA Project Submittal and Assessment Form

For bridge projects, is the bridge included in INDOT's Historic Bridge Inventory (<https://www.in.gov/indot/2531.htm>)?

☐ Yes ☐ No

If yes, did the inventory determine the bridge eligible for or listed in the National Register of Historic Places? Please provide page # of entry in Historic Bridge Inventory.

☐ Yes ☐ No

Inventory Page # _____

Will there be right-of-way acquisition as part of this project?

☐ Yes ☒ No

If yes was checked above, please check all that apply:

☐ Permanent ☐ Temporary ☐ Reacquisition

If applicable, identify right-of-way acquisition locations in text below and in attached mapping. Please specify how much (both temporary and permanent) and indicate what activities are included in the proposed right-of-way: N/A

Is there any potential for additional temporary right-of-way to be needed later for purposes such as access, staging, etc.?

☐ Yes ☒ No

Archaeology (check one):

- ☒ All proposed activities are presumed to occur in previously disturbed soils*
**INDOT-CRO will notify you if project area includes undisturbed soils and requires an archaeological reconnaissance.*
- ☐ Project takes place in undisturbed soils and the archaeology report is included in submission or will be forthcoming*
**If an archaeology report is required, the Minor Projects PA Form will not be finalized until the report is reviewed and approved by INDOT-CRO. For INDOT-sponsored projects, INDOT-CRO may be able to complete the archaeological investigation. If you would like to request that INDOT-CRO complete an archaeological investigation, please contact the INDOT-CRO archaeology team lead. See CRM Pt. 1 Ch. 3 for current contact information.*

Please specify all applicable categories and condition(s) (highlight applicable conditions in yellow)*:

**Include full category text, including any conditions. INDOT-CRO will finalize categories upon their review.*

B-1. Replacement, repair, or installation of curbs, curb ramps, or sidewalks, including when such projects are associated with roadway work such as surface replacement, reconstruction, rehabilitation, or resurfacing, including overlays, shoulder treatments, pavement repair, seal coating, pavement grinding, and pavement marking, under the following conditions **[BOTH Condition A, which pertains to Archaeological Resources, and Condition B, which pertains to Above-Ground Resources, must be satisfied]**:

Condition A (Archaeological Resources)

One of the two conditions listed below must be satisfied (*EITHER Condition i or Condition ii must be satisfied*):

i. Work occurs in previously disturbed soils; OR

ii. Work occurs in undisturbed soils and an archaeological investigation conducted by the applicant and reviewed by INDOT Cultural Resources Office determines that no National

Minor Projects PA Project Submittal and Assessment Form

Register-listed or potentially National Register-eligible archaeological resources are present within the project area. If the archaeological investigation locates National Register-listed or potentially National Register-eligible archaeological resources, then full Section 106 review will be required. Copies of any archaeological reports prepared for the project will be provided to the Division of Historic Preservation and Archaeology (DHPA) and any archaeological site form information will be entered directly into the State Historic Architectural and Archaeological Database (SHAARD) by the applicant. The archaeological reports will also be available for viewing (by Tribes only) on INSCOPE.

Condition B (Above-Ground Resources)

One of the two conditions listed below must be satisfied (*EITHER Condition i or Condition ii must be satisfied*):

i. Work does not occur adjacent to or within a National Register-listed or National Register-eligible district or individual above-ground resource; *OR*

ii. Work occurs adjacent to or within a National Register-listed or National Register-eligible district or individual above-ground resource under one of the two additional conditions listed below (*EITHER Condition a OR Condition b must be met and field work and documentation must be completed as described below*):

a. No unusual features, including but not limited to historic brick or stone sidewalks, curbs, or curb ramps, stepped or elevated sidewalks and historic brick or stone retaining walls are present in the project area adjacent to or within a National Register-listed or National Register-eligible district or individual above-ground resource; *OR*

b. Unusual features, including but not limited to historic brick or stone sidewalks, curbs or curb ramps, stepped or elevated sidewalks and historic brick or stone retaining walls are present in the project area adjacent to or within a National Register-listed or National Register-eligible individual above-ground resource or district and ANY ONE of the conditions (1, 2, or 3) listed below must be fulfilled:

1. Unusual features described above will not be impacted by the project. Firm commitments regarding the avoidance of these features must be listed in the MPPA determination form and the NEPA document and must be entered into the INDOT Project Commitments Database. These projects will also be flagged for quality assurance reviews by INDOT Cultural Resources Office during/after project construction.

2. Unusual features described above have been determined not to contribute to the significance of the historic resource by INDOT Cultural Resources Office in consultation with the SHPO based on an analysis and justification prepared by their staff or review of such information from other qualified professional historians.

3. Impacts to unusual features described above have been determined by INDOT Cultural Resources Office to be so minimal that they do not diminish any of the characteristics that contribute to the significance of the historic resource, based on an analysis and justification prepared by their staff or review of such information from other qualified professional historians.

- B-14.** Installation of new traffic signal controller cabinets, handholes, traffic detection loops and housing, and ADA-complaint accessible pedestrian pushbutton pedestal poles in close proximity to existing traffic signals adjacent to or within a National Register-listed or National Register-eligible district or individual above-ground resource under the following conditions [***BOTH Condition A, which pertains to Archaeological Resources, and Condition B, which pertains to Above-Ground Resources, must be satisfied***]:

Condition A (Archaeological Resources)

One of the two conditions listed below must be met (*EITHER Condition i or Condition ii must be satisfied*):

Minor Projects PA Project Submittal and Assessment Form

i. Work occurs in previously disturbed soils; *OR*

ii. Work occurs in undisturbed soils and an archaeological investigation conducted by the applicant and reviewed by INDOT Cultural Resources Office determines that no National Register-listed or potentially National Register-eligible archaeological resources are present within the project area. If the archaeological investigation locates National Register-listed or potentially National Register eligible archaeological resources, then full Section 106 review will be required. Copies of any archaeological reports prepared for the project will be provided to the DHPA and any archaeological site form information will be entered directly into the SHAARD by the applicant. The archaeological reports will also be available for viewing (by Tribes only) on INSCOPE.

Condition B (Above-Ground Resources)

No unusual features, including but not limited to historic brick or stone sidewalks, curbs or curb ramps are present at the location where such work will occur.

Check ☐ if SECTION 2: Minor Projects PA Category B-1, Condition B-ii Submission is included

Check ☐ if SECTION 3: Minor Projects PA Category B-9, Condition B-i-c-2 or B-ii-b-3 Submission is included

Part II: Completed by INDOT-CRO

Amendments will be shown in red font.

Information reviewed (please check all that apply):

General project location map ☒ USGS map ☒ Aerial photograph ☒ Soil survey data ☒

General project area photos ☒ Archaeology Reports ☐ Historic Property Reports ☐

Indiana Historic Buildings, Bridges, and Cemeteries Map/Interim Report ☒

Bridge inspection information/BIAS ☐ Historic Bridge Inventory Database ☐

SHAARD ☒ SHAARD GIS ☒ Streetview Imagery ☒ County GIS Data/Property Cards ☒

Other (please specify): Project information, photos, and maps provided by INDOT-Seymour District on January 9, 2023, and on file at INDOT-CRO.

Are there any commitments associated with this project? If yes, please explain and include in the Additional Comments Section below. yes ☐ no ☒

Does the project result in a de minimis impact to a Section 4(f) protected historic resource? If yes, please explain in the Additional Comments Section below. yes ☐ no ☒

Additional Comments:

Above-ground Resources

An INDOT-Cultural Resources Office (CRO) historian who meets the Secretary of the Interior's Professional Qualification Standards as per 36 CFR Part 61 first performed a desktop review, checking the Indiana Register of Historic Sites and Structures (State Register) and National Register of Historic Places (National Register) lists for

Minor Projects PA Project Submittal and Assessment Form

Ripley and Franklin Counties. No listed resources are present immediately adjacent to the project area, a distance that serves as an adequate area of potential effects given the project scope and terrain.

The National Register & IHSSI information is available in the Indiana State Historic Architectural and Archaeological Research Database (SHAARD) and the Indiana Historic Buildings, Bridges, and Cemeteries Map (IHBBCM). The *Ripley County Interim Report* (1986; Laughery and Adams Townships) and the *Franklin County Interim Report* (2011; Ray Township) of the Indiana Historic Sites and Structures Inventory (IHSSI) was also consulted. The SHAARD information was checked against the Interim Report hard copy maps. The IHBBCM contains the most up to date IHSSI information. No IHSSI properties rated above “Contributing” are immediately adjacent to the project area.

According to the IHSSI rating system, generally properties rated "Contributing" do not possess the level of historical or architectural significance necessary to be considered individually National Register eligible, although they would contribute to a historic district. If they retain material integrity, properties rated “Notable” might possess the necessary level of significance after further research. Properties rated “Outstanding” usually possess the necessary level of significance to be considered National Register eligible if they retain material integrity. Historic districts identified in the IHSSI are usually considered eligible for the National Register.

Because the location of the project is not adjacent to a National Register-listed or eligible resource, a field visit by a Qualified Professional historian is not required. It should be noted that this review is focused only on the B-1 portion of the project area. The HMA portion of the project was not reviewed in detail as it is limited to A-4 work within the existing roadway.

The INDOT-CRO historian reviewed structures adjacent to the three (3) B-1 curb ramp updates utilizing online aerial, street-view photography, and the Ripley County GIS website. The project is located along a two-lane highway that runs through a commercial area. The nearby building stock consists of mid-twentieth to early-twenty-first century commercial structures, as well as a late twentieth century educational structure. None of the properties adjacent to the ADA updates appear to possess the integrity or significance necessary to be considered National Register-eligible. No unusual features appeared to be present.

Based on the available information, as summarized above, no above ground concerns exist if the project scope does not change.

Archaeological Resources

An INDOT-CRO archaeologist who meets the Secretary of the Interior’s Professional Qualification Standards as per 36 CFR Part 61 reviewed the MPPA request submitted by INDOT-Seymour District and conducted a desktop review of the project area and completed an archaeological assessment.

The proposed HMA overlay is limited to the existing SR 46, which consists of the four-lane paved road flanked by side ditches, residential and business infrastructure, and buried utilities. The proposed curb ramp upgrade locations have been previously disturbed by the installation of the existing curb ramps, sidewalks, and storm sewers. The existing R/W has been completely disturbed by previous construction, and no intact soils remain. There are a few sites within a mile of the project area, however, they are far away enough to not be impacted by construction activities.

Therefore, there are no archaeological concerns as long as the project scope does not change.

Accidental Discovery: If any archaeological artifacts or human remains are uncovered during construction, demolition, or earth moving activities, construction within 100 feet of the discovery will be stopped, and INDOT-CRO and the Indiana Department of Natural Resources-Division of Historic Preservation and Archaeology (IDNR-DHPA) will be notified immediately.

Minor Projects PA Project Submittal and Assessment Form

INDOT-CRO staff reviewer(s): Haley Brinker, Clint Kelly, Matt Coon, and KayLee Blum

INDOT Approval Date: 3/28/2023

Amendment Approval Date (if applicable):

****Be sure to attach this form to the National Environmental Policy Act documentation for this project. Also, the NEPA documentation shall reference and include the description of the specific stipulation in the PA that qualifies the project as exempt from further Section 106 review.*

Please attach the following to this form:

- **General Location Map.** This map should allow the INDOT-CRO reviewer to quickly locate the project.
- **Aerial photography map(s) of project area.** This map must include project limits. It may also include SHAARD data, but SHAARD data is not required.
- **If bridge or small structure project, please attach photographs of bridge or small structure.** Photographs can be found in inspection reports located in INDOT's Bridge Inspection Application System (BIAS), as well as other project documents, such as engineering assessments or mini-scopes.

Map depicting potential temporary and/or permanent right-of-way acquisitions. In the email submission to INDOT-CRO, please also include:

- **A GIS polygon shapefile or KMZ file of the project area** (shapefiles are preferred). Shapefiles should use "NAD_1983_UTM" projected coordinate system. In addition, these files should contain the following *text* attribute field: DES_NO. The project designation number should be entered in this field.
- **If the project takes place in undisturbed soils, attach the results of the archaeological investigation, if completed.** *Note: The MPPA Submission Form may be submitted before the archaeology report. INDOT-CRO staff will process the above-ground portion of the form in advance of the archaeological portion of the form. However, a completed determination form will not be returned to the applicant until after the archaeology report has been reviewed and approved by INDOT-CRO.*

APPENDIX A

Category A Minor Projects

Requires No Review by INDOT Cultural Resources Office

Category A consists of projects that, by their nature, have no effect on properties listed in or eligible for inclusion in the National Register of Historic Places (hereinafter referred to as the “National Register”) and do not require review by INDOT Cultural Resources Office. All of the work under this Category must occur in previously disturbed soils, which are defined as soils that have been completely altered or displaced by earthmoving or other modern manipulation.

1. Any work on bridges limited to substructure or superstructure elements without replacing, widening, or elevating the superstructure under the conditions listed below (***BOTH Conditions A and B must be met***). This category **does not** include bridge replacement projects (when both superstructure and substructure are removed):
 - A. The project takes place in previously disturbed soils; *AND*
 - B. With regard to the bridges, at least one of the conditions (i, ii or iii) listed below must be satisfied:
 - i. The latest Historic Bridge Inventory identified the bridge as non-historic (see <http://www.in.gov/indot/2531.htm>);
 - ii. The bridge was built after 1945, and is a common type as defined in Section V. of the *Program Comment Issued for Streamlining Section 106 Review for Actions Affecting Post-1945 Concrete and Steel Bridges* issued by the Advisory Council on Historic Preservation on November 2, 2012 for so long as that Program Comment remains in effect AND the considerations listed in Section IV of the Program Comment do not apply;
 - iii. The bridge is part of the Interstate system and was determined not eligible for the National Register under the Section 106 Exemption Regarding Effects to the Interstate Highway System adopted by the Advisory Council on Historic Preservation on March 10, 2005, for so long as that Exemption remains in effect.
2. All work within interchanges and within medians of divided highways in previously disturbed soils.
3. Replacement, repair, lining, or extension of culverts and other drainage structures that do not exhibit wood, stone or brick structures or parts therein and are in previously disturbed soils.
4. Roadway work associated with surface replacement, reconstruction, rehabilitation, or resurfacing projects, including overlays, shoulder treatments, pavement repair, seal coating, pavement grinding, and pavement marking within previously disturbed soils where replacement, repair, or installation of curbs, curb ramps or sidewalks will not be required.
5. Repair, in-kind replacement or upgrade of existing lighting, signals, signage, and other traffic control devices in previously disturbed soils.
6. Repair, replacement, or upgrade of existing safety appurtenances such as guardrails, barriers, glare screens, and crash attenuators in previously disturbed soils.
7. Repair or in-kind replacement of fencing and hardscape landscaping elements and/or replacement of existing plant materials in previously disturbed soils and installation of new fencing and hardscape landscaping elements and plant materials limited to locations within interstate right-of way within previously disturbed soils.
8. Installation of new or modification of existing traffic control devices and systems, including signs, signals, markings, illumination, other warning devices and their supports, to improve safety at railway crossings in previously disturbed soils.
9. Installation, repair, or replacement of erosion control measures along roadways, waterways and bridge piers within previously disturbed soils.

10. Routine roadside maintenance activities necessary to preserve existing infrastructure or maintain roadway safety in previously disturbed soils.
11. Rehabilitation of existing rest areas and truck weigh stations within previously disturbed soils.
12. Removal and disposal of hazardous waste.
13. Work on concrete and asphalt decks of bridges identified in the Historic Bridge Inventory as National Register-listed or National Register-eligible (see <http://www.in.gov/indot/2531.htm>), which is limited to pavement resurfacing, overlay, pavement repair, pavement grinding, pavement marking, seal coating, joint repair, and in-kind replacement or repair of existing concrete curbs, curb ramps or sidewalks in previously disturbed soils, provided none of these actions impact structural members of the bridge.
14. Repair and/or replace existing MSE walls, retaining walls and noise walls in previously disturbed soils, using similar design, dimensions and materials.

Appendix E

Red Flag and Hazardous Materials



INDIANA DEPARTMENT OF TRANSPORTATION

100 North Senate Avenue
Room N758-ES
Indianapolis, Indiana 46204

PHONE: (855) 463-6848
(855) INDOT4U

Eric Holcomb, Governor
Michael Smith, Commissioner

Date: January 18, 2023

To: Site Assessment & Management (SAM)
Environmental Policy Office - Environmental Services Division (ESD)
Indiana Department of Transportation (INDOT)
100 N Senate Avenue, Room N758-ES
Indianapolis, IN 46204

From: Erin Carleton
INDOT-Seymour District
185 Agrico Ln
Seymour, IN
ecarleton@indot.in.gov

Re: LIMITED RED FLAG INVESTIGATION
Des. No. 2001905 State Project
HMA Overlay with Curb Ramp Upgrades
Curb Ramps at Intersections of SR 46 / SR 129, SR 46 / Batesville Blvd, and SR 46/ Winding Way.
Ripley and Dearborn County, Indiana

PROJECT DESCRIPTION

Previous scope

Brief Description of Project: This project is an HMA overlay on SR 46 starting at 0.29 miles west of SR 229 in Ripley County and ending on SR 1 in Dearborn County. There will be curb ramp upgrades in three locations to meet ADA (Americans with Disabilities Act) standards:

Intersection:	Quadrants:	Depth of Excavation:
SR 46 and SR 129	SE and SW	3ft below ground surface
SR 46 and Batesville Blvd	NE and SE	1ft below ground surface
SR 46 and Winding Way	SW	1ft below ground surface

The curb ramp upgrades of SR 129 at SR 46 will feature pedestrian push buttons, APS and pedestrian signals with wiring. Batesville Blvd will receive two new curb ramps. Winding Way will receive one new curb ramp.

Bridge Work Included in Project: Yes ☐ No ☒ Structure #(s) _____

If this is a bridge project, is the bridge Historical? Yes ☐ No ☐ , Select ☐ Non-Select ☐

(Note: If the project involves a historical bridge, please include the bridge information in the Recommendations Section of the report).

Culvert Work Included in Project: Yes ☐ No ☒ Structure #(s) _____

Proposed right of way: Temporary ☐ # Acres _____ Permanent ☐ # Acres _____, Not Applicable ☒

Type and proposed depth of excavation: Curb ramp upgrades will require a maximum excavation depth of 1 ft below ground surface (bgs). Installation of the pedestrian signals at SR 129 will require a maximum excavation depth of 3 ft-bgs for the poles.

Maintenance of traffic (MOT): Traffic will be maintained via arrow boards, single lane closures, sidewalk closures, and flagging traffic (if needed).

Work in waterway: Yes ☐ No ☒ Below ordinary high water mark: Yes ☐ No ☐

State Project: ☒ LPA: ☐

Any other factors influencing recommendations: N/A

HAZARDOUS MATERIAL CONCERNS TABLE AND SUMMARY

Hazardous Material Concerns			
Indicate the number of items of concern found within the 0.5 mile search radius. If there are no items, please indicate N/A:			
Superfund	N/A	Manufactured Gas Plant Sites	N/A
RCRA Generator/TSD	N/A	Open Dump Waste Sites	N/A
RCRA Corrective Action Sites	N/A	Restricted Waste Sites	N/A
State Cleanup Sites	N/A	Waste Transfer Stations	N/A
Septage Waste Sites	N/A	Tire Waste Sites	N/A
Underground Storage Tank (UST) Sites	4	Confined Feeding Operations (CFO)	N/A
Voluntary Remediation Program	N/A	Brownfields	N/A
Construction Demolition Waste	N/A	Institutional Controls	N/A
Solid Waste Landfill	N/A	NPDES Facilities	3
Infectious/Medical Waste Sites	N/A	NPDES Pipe Locations	3
Leaking Underground Storage (LUST) Sites	2	Notice of Contamination Sites	N/A

Unless otherwise noted, site specific details presented in this section were obtained from documents reviewed on the Indiana Department of Environmental Management (IDEM) Virtual File Cabinet (VFC).

Explanation:

SR 46 and SR 129-

Leaking Underground Storage Tank (LUST) Sites: Day-Nite Food Mart (also referred to as Day-Night Food Mart), 1340 SR 46, AI ID # 60104, is a LUST Site located 0.08 mile northwest of the intersection project area. IDEM granted No Further Action (NFA) status for Incident 199804522 on June 25, 1999. More recently, the IDEM issued a Violation Letter, dated December 2, 2021, regarding UST paperwork and lack of annual testing. A response has not yet been received; however, due to the location and depth of proposed excavation (i.e. <1 ft-bgs), no impact is expected.

Underground Storage Tank (UST) Sites: One (1) UST site was found, Weberdings Carving Shop Incorporated, 1230 SR 46 E, AI ID # 58826. Although the icon is mapped 0.12 mile west of the project area, the site is actually located 0.15 mile west of the project area. No impacts are expected.

SR 46 and Batesville Blvd-

There do not appear to be any hazardous material concern sites mapped adjacent to or in close proximity to the curb ramp location.

SR 46 and Winding Way-

NPDES Facilities: One (1) NPDES Facility owned by Batesville Logistics Incorporated, 1674 SR 46, is located adjacent to the north side of SR 46, across from the project area. The permit is in effect until June 22, 2024; however, based on the scope of work and that excavation is not expected to exceed 1 ft-bgs, no impact is expected.

NPDES Pipe Location: One (1) NPDES Pipe Location is mapped 0.06 mile east of the project area. Due to the location of the mapped outfall and the proposed project activities, excavation <1 ft-bgs, no impact is expected.

UST Site: Batesville Casket Company Fleet Garage, 1069 SR 46, AI ID # 11720, is located adjacent to the north side of SR 46, across from the project area. IDEM issued a No Further Action Approval Determination Pursuant to Remediation Closure Guide, dated September 12, 2017, following UST Closure activities completed in September of 2016. More recently, IDEM issued a Violation Letter, dated August 18, 2022, requesting additional documentation on tank and line tightness and records. The facility has submitted requested documentation. No impact is expected.

ECOLOGICAL INFORMATION SUMMARY

The Ripley County listing of the Indiana Natural Heritage Data Center information on endangered, threatened, or rare (ETR) species and high quality natural communities is provided at https://www.in.gov/dnr/nature-preserves/files/np_ripley.pdf. A preliminary review of the Indiana Natural Heritage Database by INDOT ESD did not indicate the presence of ETR species within the 0.5 mile search radius. Due to the nature of project activities, this project will fall under the guidelines set forth under USFWS Interim Policy for the Review of Highway Transportation Projects in Indiana dated May 29, 2013. No further coordination is necessary.

A review of the USFWS database did not indicate the presence of endangered bat species in or within 0.5 mile of the project area. The range-wide programmatic consultation for the Indiana Bat and Northern Long-eared Bat will be completed according to the most recent "Using the USFWS's IPaC System for Listed Bat Consultation for INDOT Projects".

RECOMMENDATIONS SECTION

Include recommendations from each section. If there are no recommendations, please indicate N/A:

HAZARDOUS MATERIAL CONCERNS: N/A

ECOLOGICAL INFORMATION: The range-wide programmatic consultation for the Indiana Bat and Northern Long-eared Bat will be completed according to the most recent "Using the USFWS's IPaC System for Listed Bat Consultation INDOT Projects".

INDOT ESD concurrence: Darlane Davis (Signature)
Digitally signed by Darlane Davis
Date: 2023.01.18 15:16:53 -05'00'

Prepared by:
Erin Carleton
Environmental Manager 2
INDOT – Seymour District

Graphics:

SITE LOCATION: YES

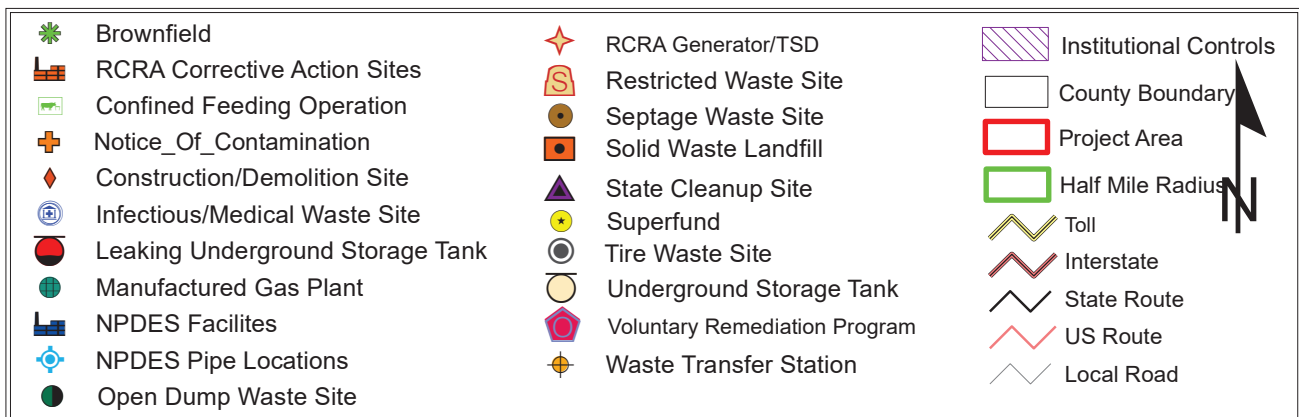
HAZARDOUS MATERIAL CONCERNS: YES

Limited Red Flag Investigation - Site Location
SR 46 from SR 229 to SR 1
Des No. 2001905, HMA Overlay with Curb Ramp Upgrades
Ripley and Dearborn County, Indiana



Sources:
Non Orthophotography
Data - Obtained from the State of Indiana Geographical Information Office Library
Orthophotography - Obtained from Indiana Map Framework Data (www.indianamap.org)
Map Projection: UTM Zone 16 N **Map Datum:** NAD83
This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.

**BATESVILLE/SPADES/CEDAR
GROVE QUADRANGLE
INDIANA
7.5 MINUTE SERIES**

[illegible]

This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.

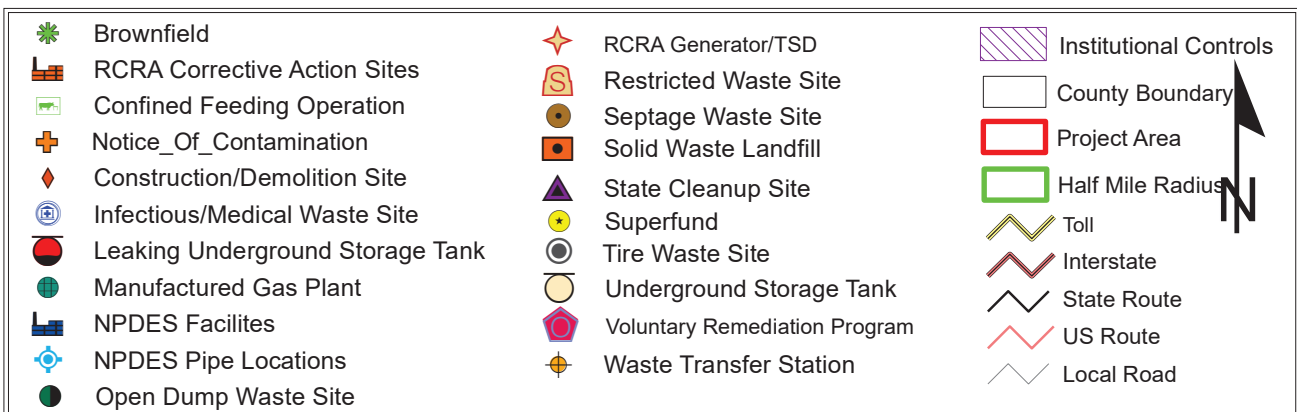
Map Projection: UTM Zone 16 N **Map Datum:** NAD83

Limited Red Flag Investigation - Hazardous Material Concerns

SR 46 at SR 129

Des. No. 2001905 Curb Ramp Upgrades

Ripley County, Indiana



0.08 0.04 0 0.08 Miles

This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.

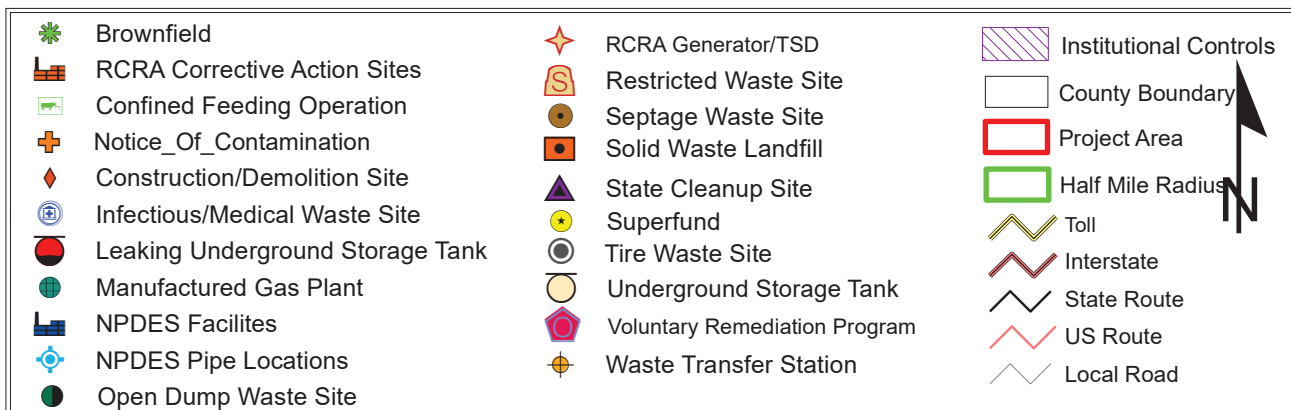
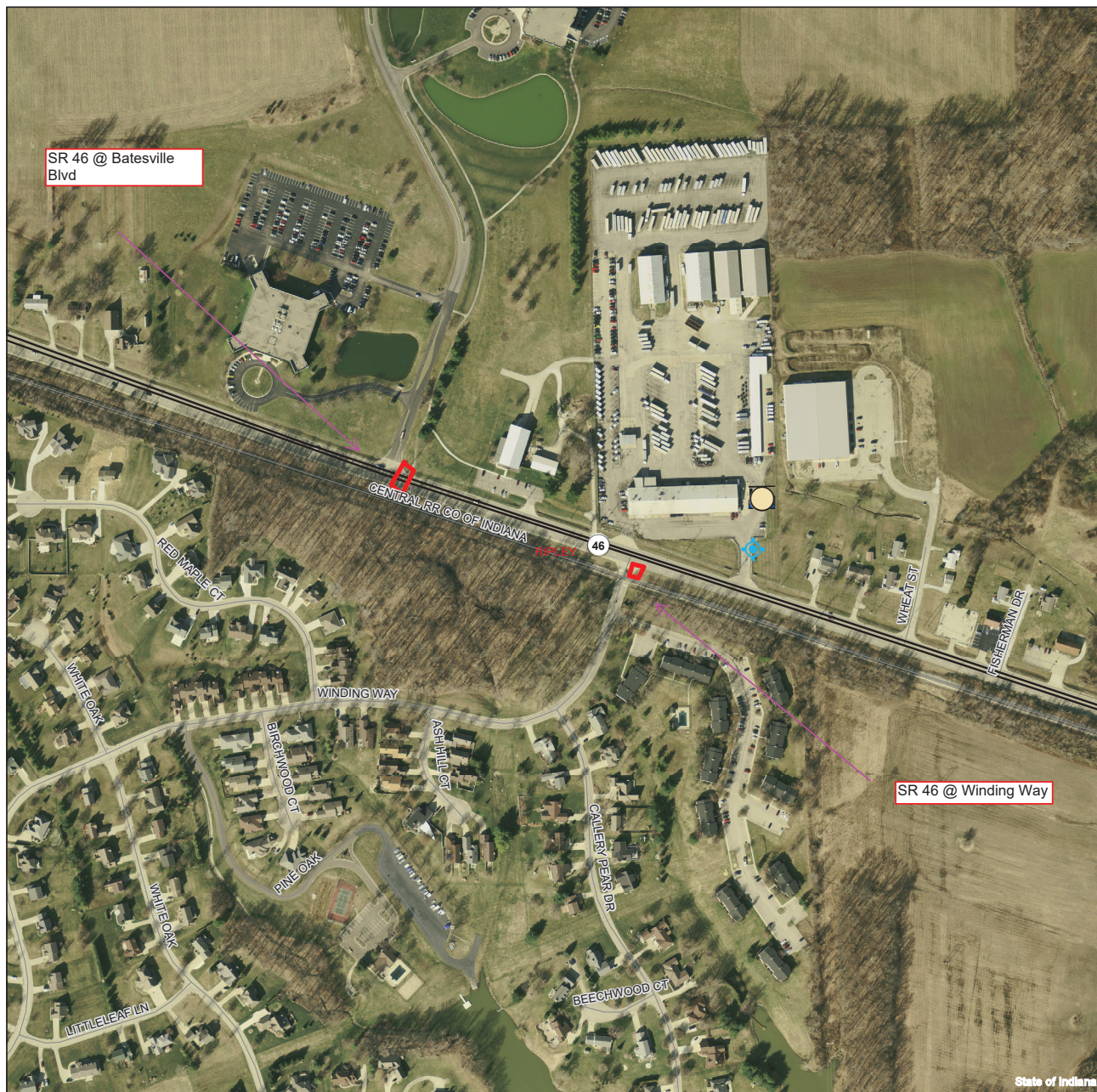
Sources:
Non Orthophotography
Data - Obtained from the State of Indiana Geographical Information Office Library
Orthophotography - Obtained from Indiana Map Framework Data (www.indianamap.org)
Map Projection: UTM Zone 16 N **Map Datum:** NAD83

Limited Red Flag Investigation - Hazardous Material Concerns

SR 46 at Batesville Blvd, and SR 46 at Winding Way

Des. No. 2001905 Curb Ramp Upgrades

Ripley County, Indiana



0.08 0.04 0 0.08 Miles

This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.

Sources:

Non Orthophotography

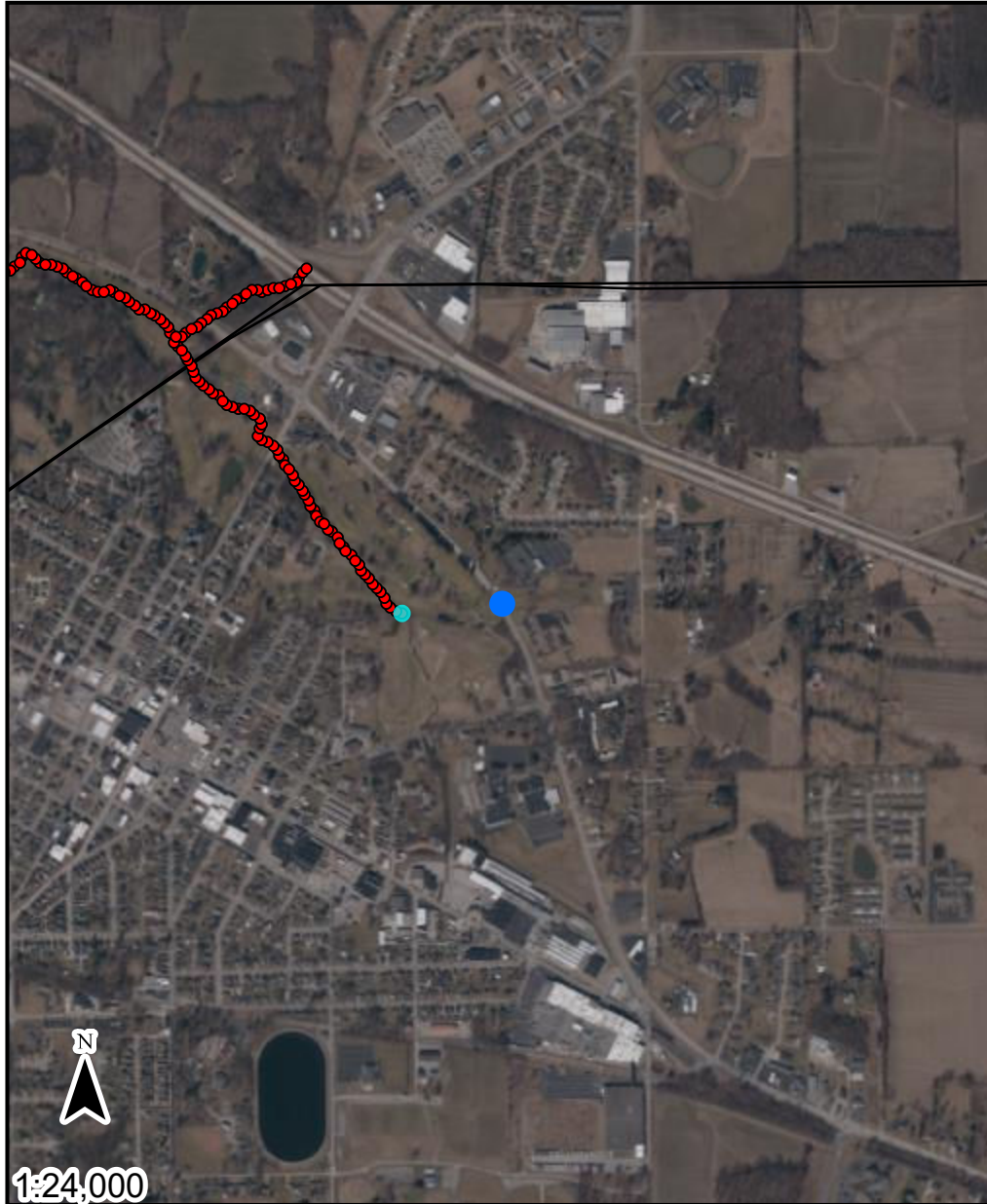
Data - Obtained from the State of Indiana Geographical Information Office Library

Orthophotography - Obtained from Indiana Map Framework Data (www.indianamap.org)

Map Projection: UTM Zone 16 N Map Datum: NAD83

Appendix F

Water Resources



- Point of Interest
- Base Flood Elevation Point
- POI
- 1.5
- Not Mapped
- FPA Jurisdictions
- RGB
 - Red: Band_1
 - Green: Band_2
 - Blue: Band_3

Long: -85.21217026646787

Lat: 39.30143173641058

The information provided below is based on the point of interest shown in the map above.

County: Ripley	Approximate Ground Elevation: 948.0 feet (NAVD88)
Stream Name: Unnamed Tributary	Base Flood Elevation: Not Available
	Drainage Area: Not Available

Best Available Flood Hazard Zone: **Not Mapped**

National Flood Hazard Zone: **Not Mapped**

Is a Flood Control Act permit from the DNR needed for this location? **See following pages**

Is a local floodplain permit needed for this location? **Contact your local Floodplain Administrator-**

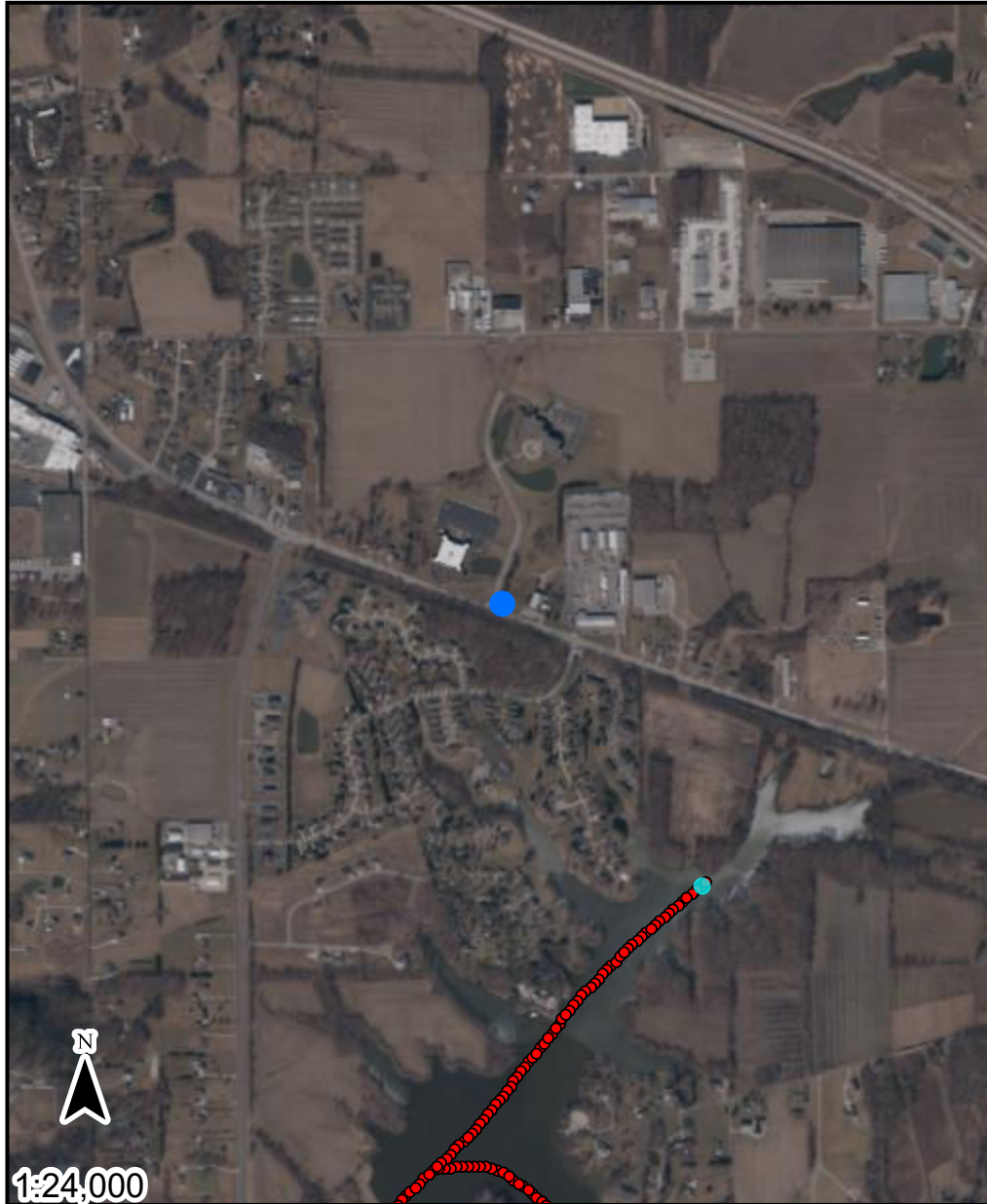
Floodplain Administrator: **John Kuntz, City Engineer**

Community Jurisdiction: **City Of Batesville, City proper**

Phone: **(812) 933-6103**

Email: **jkuntz@batesville.in.gov**

US Army Corps of Engineers District: **Louisville**



- Point of Interest
- Base Flood Elevation Point
- POI
- 1.5
- Not Mapped
- FPA Jurisdictions
- RGB
 - Red: Band_1
 - Green: Band_2
 - Blue: Band_3

Long: -85.1956907742804
Lat: 39.28801403904641

The information provided below is based on the point of interest shown in the map above.

County: Ripley	Approximate Ground Elevation: 991.3 feet (NAVD88)
Stream Name: Unnamed Tributary	Base Flood Elevation: Not Available
	Drainage Area: Not Available

Best Available Flood Hazard Zone: **Not Mapped**
National Flood Hazard Zone: **Not Mapped**

Is a Flood Control Act permit from the DNR needed for this location? **See following pages**
Is a local floodplain permit needed for this location? **Contact your local Floodplain Administrator-**
Floodplain Administrator: **John Kuntz, City Engineer**

Community Jurisdiction: **City Of Batesville, City proper**
Phone: **(812) 933-6103**
Email: **jkuntz@batesville.in.gov**

Appendix G

Air Quality

Indiana Department of Transportation (INDOT)
State Preservation and Local Initiated Projects FY 2024 - 2028

SPONSOR	CONTR ACT # / LEAD DES	STIP NAME	ROUTE	WORK TYPE	DISTRICT	MILES	FEDERAL CATEGORY	Total Cost of Project*	PROGRAM	PHASE	FEDERAL	MATCH	2024	2025	2026	2027	2028
Indiana Department of Transportation	43328 / 2002066	A 17	SR 129	Scour Protection (Erosion)	Seymour	0	STBG	\$368,611.00	Bridge Construction	CN	-\$520,000.00	-\$130,000.00		(\$650,000.00)			
Performance Measure Impacted: Safety																	
Location: Over Ripley Creek's, 01.06 mi N of SR 48																	
Comments:Decrease CN funding by \$650,000 for FY 25.																	
Indiana Department of Transportation	43361 / 2001970	Init.	SR 129	Small Structure Replacement	Seymour	0	STBG	\$1,632,472.11	Bridge Construction	CN	\$810,400.00	\$202,600.00		\$1,013,000.00			
									Bridge ROW	RW	\$24,000.00	\$6,000.00	\$30,000.00				
Performance Measure Impacted: Bridge Condition																	
Location: 00.55 N of SR 350																	
Comments:Include DES 2001952, 2001970, 2002329																	
Indiana Department of Transportation	43361 / 2001970	A 06	SR 129	Small Structure Replacement	Seymour	0	STBG	\$3,645,827.11	Bridge Construction	CN	\$1,520,800.00	\$380,200.00		\$1,901,000.00			
Performance Measure Impacted: Bridge Condition																	
Location: 00.55 N of SR 350																	
Comments:Add \$1,900,854 of CN funding in FY2025																	
Indiana Department of Transportation	43370 / 2001905	A 06	SR 46	HMA Overlay, Preventive Maintenance	Seymour	2.833	STBG	\$2,815,579.00	Road Construction	CN	\$2,252,800.00	\$563,200.00			\$2,816,000.00		
Performance Measure Impacted: Pavement Condition																	
Location: SR 101 to SR 1																	
Comments:Add CN																	
Indiana Department of Transportation	43370 / 2001905	A 14	SR 46	HMA Overlay, Preventive Maintenance	Seymour	2.833	STBG	\$2,845,579.00	Road Consulting	PE	\$24,000.00	\$6,000.00		\$30,000.00			
Performance Measure Impacted: Pavement Condition																	
Location: 0.29 miles W of SR 229 to 1.07 miles E of SR 129																	
Comments:Add PE (new phase) \$30,000 FY2025.																	
Indiana Department of Transportation	43370 / 2001905	M 109	SR 46	HMA Overlay, Preventive Maintenance	Seymour	2.833	STBG	\$3,593,336.00	Road Construction	CN	\$597,600.00	\$149,400.00			\$747,000.00		
Performance Measure Impacted: Pavement Condition																	
Location: SR 101 to SR 1																	
Comments:Add \$747,336 to CN for FY 26																	
Indiana Department of Transportation	43592 / 1900089	Init.	SR 129	Bridge Replacement	Seymour	0	STBG	\$3,272,000.00	Bridge Construction	CN	\$2,312,000.00	\$578,000.00		\$2,890,000.00			
									Bridge Consulting	PE	\$50,400.00	\$12,600.00		\$63,000.00			
Performance Measure Impacted: Bridge Condition																	
Location: 01.86 mile N of SR 62 @ Raccoon Creek																	

*Estimated Costs left to Complete Project column is for costs that may extend beyond the four years of a STIP. This column is not fiscally constrained and is for information purposes.

Appendix H

Additional Studies / Reports

Project Number	Subproject Code	County	Property
1800031	1800031	Franklin	Franklin County Park
1800176	1800176	Franklin	Whitewater Canal State Historic Site
1800225	1800225	Franklin	Fairfield Marina, Brookville Lake
1800324	1800324	Franklin	Mounds State Recreation Area
1800331	1800331	Franklin	Batesville Community Park
1800363	1800363B	Franklin	Brookville Reservoir
1800111	1800111	Ripley	Liberty Park & Park Reservoir
1800116	1800116	Ripley	Batesville Memorial Pool
1800171	1800171Q	Ripley	Versailles State Park
1800178	1800178	Ripley	Versailles State Park
1800181	1800181	Ripley	Versailles State Park
1800312	1800312S	Ripley	Versailles State Park
1800327	1800327M	Ripley	Versailles State Park
1800363	1800363HH	Ripley	Versailles State Park
1800378	1800378H	Ripley	Versailles State Park
1800413	1800413W	Ripley	Versailles State Park
1800471	1800471	Ripley	Milan Community Park
1800594	1800594D	Ripley	Versailles State Park
1800597	1800597	Ripley	Six Pines Ranch Park