

FHWA-Indiana Environmental Document
CATEGORICAL EXCLUSION / ENVIRONMENTAL ASSESSMENT FORM
GENERAL PROJECT INFORMATION

Road No./County:	State Road (SR) 26 / Jay County
Designation Number(s):	2200586
Project Description/Termini:	Bridge Replacement / approximately 1.84 Miles East of United States Highway (US) 27 to 1.96 Miles East of US 27


X	Categorical Exclusion, Level 2 – Required Signatories: INDOT DE and/or INDOT ESD
	Categorical Exclusion, Level 3 – Required Signatories: INDOT ESD
	Categorical Exclusion, Level 4 – Required Signatories: INDOT ESD and FHWA
	Environmental Assessment (EA) – Required Signatories: INDOT ESD and FHWA
	Additional Investigation (AI) – The proposed action included a design change from the original approved environmental document. Required Signatories must include the appropriate environmental approval authority

Approval

_____	_____
INDOT DE Signature and Date	INDOT ESD Signature and Date

FHWA Signature and Date	

Release for Public Involvement

 5/29/25	N/A
INDOT DE Initials and Date	INDOT ESD Initials and Date

Certification of Public Involvement

INDOT Consultant Services Signature and Date

INDOT DE/ESD Reviewer Signature and Date:

Name and Organization of CE/EA Preparer:

Nakayla J. Krahn, Burgess & Niple, Inc.

Note: Refer to the most current INDOT CE Manual, guidance language, and other ESD resources for further guidance regarding any section of this form.

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Part I – Public Involvement

Every Federal action requires some level of public involvement, providing for early and continuous opportunities throughout the project development process. **The level of public involvement should be commensurate with the proposed action.**

	Yes	No
Does the project have a historic bridge processed under the Historic Bridges PA*?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
If No, then:		
Opportunity for a Public Hearing Required?	<input checked="" type="checkbox"/>	<input type="checkbox"/>

*A public hearing is required for all historic bridges processed under the Historic Bridges Programmatic Agreement between INDOT, FHWA, SHPO, and the ACHP.

Discuss what public involvement activities (legal notices, letters to affected property owners and residents (i.e. notice of entry), meetings, special purpose meetings, newspaper articles, etc.) have occurred for this project.

Notice of Survey letters were mailed to potentially affected property owners near the project area on May 26, 2023, notifying them about the project and that individuals responsible for land surveying and field activities may be seen in the area. A sample copy of the Notice of Survey letter is included in Appendix G, G-1.

Project Does Meet

The project will meet the minimum requirements described in the current *Indiana Department of Transportation (INDOT) Project Development Public Involvement Procedures Manual* which requires the project sponsor to offer the public an opportunity to submit comments and/or request a public hearing. Therefore, a legal notice will appear in a local publication contingent upon the release of this document for public involvement. This document will be revised after the public involvement requirements are fulfilled.

Public Controversy on Environmental Grounds

Discuss public controversy concerning community and/or natural resource impacts, including what is being done during the project to minimize impacts.

At this time, there is no substantial public controversy concerning impacts to the community or to natural resources.

Part II - General Project Identification, Description, and Design Information

Sponsor of the Project: INDOT INDOT District: GreenfieldLocal Name of the Facility: SR 26Funding Source (mark all that apply): Federal ☒ State ☒ Local ☐ Other* ☐

*If other is selected, please identify the funding source: _____

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PURPOSE AND NEED:

The need should describe the specific transportation problem or deficiency that the project will address. The purpose should describe the goal or objective of the project. The solution to the traffic problem should NOT be discussed in this section.

Need

The need for the project stems from the deteriorated condition of the existing structure (Bridge No. 026-38-03431, National Bridge Inventory (NBI) 007050) that carries SR 26 over Race Run (also known as Bolen Ditch). The existing structure exhibits cracking, transverse cracks, and heavy spalling/delaminations with exposed rebar. The overall bridge condition rating noted in the *October 10, 2024, Indiana Department of Transportation (INDOT) Bridge Inspection Report* (Appendix I, I-1 to I-10) is 4 (Poor) out of 9 (Excellent) on the INDOT Bridge Condition Rating Categories Table.

Purpose

The purpose of the project is to address the bridge deficiencies and provide an overall bridge condition rating of at least 7 (Good) out of 9 (Excellent).

PROJECT DESCRIPTION (PREFERRED ALTERNATIVE):

County: JayMunicipality: N/ALimits of Proposed Work: SR 26, from approximately 1.84 miles east of US 27 to 1.96 miles east of US 27Total Work Length: 0.12 Mile(s)Total Work Area: 1.95 Acre(s)Is an Interstate Access Document (IAD)¹ required?

If yes, when did the FHWA provide a Determination of Engineering and Operational Acceptability?

¹If an IAD is required; a copy of the approved CE/EA document must be submitted to the FHWA with a request for final approval of the IAD.

Yes¹

No

☐☒

Date:

Describe location of project including township, range, city, county, roads, etc. Existing conditions should include current conditions, current deficiencies, roadway description, surrounding features, etc. Preferred alternative should include the scope of work, anticipated impacts, and how the project will meet the Purpose and Need. Logical termini and independent utility also need discussed.

The INDOT Greenfield District with funding from the Federal Highway Administration (FHWA) intends to proceed with the SR 26 Bridge Replacement project.

Location

The project is located along SR 26, 1.84 miles east of US 27, in Jay County, Indiana. More specifically, the project is located on the Portland, Indiana United States Geological Survey (USGS) Quadrangle Map in Section 22, Township 23 North, Range 14 East. Various maps and photographs can be referenced in Appendix B, B-1 to B-5.

Existing Conditions

SR 26 is a two-lane, Rural Major Collector, with a posted speed limit of 55 miles per hour (mph). The existing SR 26 approach consists of two 12-foot-wide travel lanes (one in each direction) with 2-foot paved shoulders. Guardrails exist on both sides of the roadway and include substandard aluminum tube rail with no curb (Appendix B, B-6 to B-14). The SR 26 bridge has a clear roadway of 44 feet. The existing continuous reinforced concrete bridge slab (Bridge No. 026-38-03431, NBI 007050) was built in 1946 with rehabilitations completed in 1982. The existing bridge is a two span structure with a total structure length of 44-feet, 2 inches with an out-to-out width of 46-feet, 6 inches. The bridge typical cross section consists of two 12-foot-wide travel lanes (one in each direction) bordered by 11-foot paved shoulders. The existing bridge rail is a substandard aluminum tube rail with no curb. No riprap is present. A 24-inch driveway high-density polyethylene (HPDE) pipe is located under a field entrance in the southwest quadrant and a 12-inch reinforced concrete pipe (RCP) is located under a field entrance in the southeast quadrant (Appendix B, B-3). Additionally, a 10-inch corrugated metal pipe (CMP) is located upstream of the bridge. Land use in the vicinity of the project area is primarily maintained grass, agricultural, and forested habitat.

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The overall bridge condition rating noted in the *October 10, 2024, INDOT Bridge Inspection Report* (Appendix I, I-1 to I-10) is 4 (Poor) out of 9 (Excellent). Specific deficiencies noted in the report include advanced deterioration, heavy spalling/delaminations with exposed rebar, widely-spaced hairline longitudinal cracks on the deck and superstructure, cracks on the wearing surface and approach slabs, and cracks and efflorescence on the abutments. There is exposed rebar and spalling on the substructure. A general condition rating of 6 (Satisfactory) out of 9 (Excellent) for the channel is due to slumping banks, restriction from debris, and minor streambed movement.

Apparent existing ROW in the northern quadrants and southeast quadrant is the edge of pavement. Apparent existing ROW in the southwest quadrant is approximately 40 feet from the edge of pavement.

Preferred Alternative

The project includes a full bridge replacement. The proposed bridge will receive a new bridge inventory number (Bridge 026-38-10835). The new structure will be a 83-foot single span, continuous prestressed concrete bulb-tee beam bridge with an out-to-out width of 35 feet, and a clear roadway width of 32 feet. On the structure, the typical section will be two 12-foot travel lanes, 4-foot paved shoulders, and concrete bridge railing.

Roadway approach work west of the bridge includes 274 feet of full-depth pavement replacement with guardrail and guardrail transition, followed by 90 feet of transition milling, totaling 364 feet including all incidental work. Roadway approach work east of the bridge includes 206 feet of full-depth pavement with guardrail and guardrail transition, followed by 90 feet of transition milling, for a total of 296 feet, including incidental work. The approach roadway typical section will include 12-foot travel lanes (one in each direction) and will taper the proposed 4-foot shoulders to the existing 2-foot shoulders. The existing guardrail will be replaced in all four quadrants. To accommodate the structure depth, the existing profile grade will be raised by 11 inches. Additionally, revetment riprap will be placed under the structure with riprap turnouts being constructed in all four quadrants. The southwest field entrance or the 10-inch CMP will not be reconstructed or relocated. The southeast field entrance will be reconstructed and relocated slightly to the east.

Approximately 0.40 acre of tree clearing will occur less than 100 feet from the edge of pavement for construction access and placement of riprap turnouts. No permanent lighting will be constructed but temporary lighting may be used during construction. Approximately 136 feet (0.03 acre) of Race Run (Bolen Ditch) will be impacted by the project due to placement of riprap and dewatering measures. Avoidance is not practicable and no mitigation is anticipated. Impacts have been minimized to the greatest extent possible by utilizing the minimum amount of riprap necessary, installing temporary erosion control measures around construction limits, revegetating bare areas after project completion, and directing temporary lighting away from suitable bat habitat.

The project will require approximately 1.54 acres of permanent ROW north and south of the structure and approximately 0.03 acre of temporary ROW in the southeast quadrant from agricultural and forested habitat. Proposed ROW varies from 35 to 41 feet from the center SR 26, with a max distance of 66 feet south of the roadway. For more details concerning impacts, see the appropriate sections below. Project plans are included in Appendix B, B-6 to B-14.

Maintenance of Traffic (MOT)

MOT will include a full road closure of SR 26 with a detour utilizing US 27, SR 27, and Ohio SR 49 (Appendix B, B-10). For additional details regarding MOT, see the *Maintenance of Traffic (MOT)* section below.

Logical Termini/Independent Utility

The project termini represent the minimum area necessary to fulfill the purpose and need of the project. From the center of the existing bridge, project termini extend approximately 430 feet west and 360 feet east along SR 26. This project has independent utility as it can function on its own and does not rely on any other project to meet the purpose and need.

The preferred alternative meets the purpose and need for the project as it provides the bridge a condition rating of at least 7 (Good) out of 9 (Excellent).

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OTHER ALTERNATIVES CONSIDERED:

Provide a header for each alternative. Describe all discarded alternatives, including the No Build Alternative. Explain why each discarded alternative was not selected. Make sure to state how each alternative meets or does not meet the Purpose and Need and why.

Do Nothing

This alternative would leave the bridge as it currently exists. While this alternative would eliminate cost and environmental impacts, it does not address the purpose and need of the project. If no improvements are made, the bridge will continue to deteriorate and likely result in a more costly repair. Therefore, this alternative was eliminated from further consideration.

Rehabilitation of Existing Structure

While this alternative may help eliminate costs and some environmental impacts, it does not fully address the deficiencies and therefore would not meet the purpose and need of the project. If the bridge is not replaced, it will continue to deteriorate and likely result in a more costly repair. Therefore, this alternative was eliminated from further consideration.

The No Build Alternative is not feasible, prudent or practicable because (Mark all that apply)

It would not correct existing capacity deficiencies;

It would not correct existing safety hazards;

It would not correct the existing roadway geometric deficiencies;

It would not correct existing deteriorated conditions and maintenance problems; or

It would result in serious impacts to the motoring public and general welfare of the economy.

Other (Describe):

X

ROADWAY CHARACTER:

If the proposed action includes multiple roadways, complete and duplicate for each roadway.

Name of Roadway:	<u>SR 26</u>			
Functional Classification:	<u>Rural Major Collector</u>			
Current ADT:	<u>2,494</u>	<u>VPD (2026)</u>	Design Year ADT:	<u>2,756</u> <u>VPD (2046)</u>
Design Hour Volume (DHV):	<u>328</u>	Truck Percentage (%)	<u>15.93</u>	
Designed Speed (mph):	<u>55</u>	Legal Speed (mph):	<u>55</u>	

	Existing		Proposed	
Number of Lanes:	2		2	
Type of Lanes:	Travel Lanes		Travel Lanes	
Pavement Width:	28	ft.	32 feet, 8 inches	ft.
Shoulder Width:	2 (paved)	ft.	2-foot to 4-foot, 4-inches (paved)	ft.
Median Width:	N/A	ft.	N/A	ft.
Sidewalk Width:	N/A	ft.	N/A	ft.

Setting: ☐ Urban
Topography: ☒ Level

☐ Suburban
☐ Rolling

☒ Rural
☐ Hilly

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BRIDGES AND/OR SMALL STRUCTURE(S):

If the proposed action includes multiple structures, complete and duplicate for each bridge and/or small structure. Include both existing and proposed bridge(s) and/or small structure(s) in this section.

Structure/NBI Number(s): 026-38-03431 / 007050 Sufficiency Rating: 4 (Poor), October 10, 2024, INDOT
(Proposed 026-38-10835) Bridge Inspection Report
 (Rating, Source of Information)

	Existing	Proposed
Bridge/Structure Type:	Continuous Reinforced Concrete Slab Bridge	Continuous Prestressed Concrete Bulb-Tee Beam Bridge
Number of Spans:	2	1
Weight Restrictions:	N/A ton	N/A ton
Height Restrictions:	N/A ft.	N/A ft.
Curb to Curb Width:	44 ft.	32 ft.
Outside to Outside Width:	46-feet, 8-inches ft.	35 ft.
Shoulder Width:	11 (paved) ft.	4-feet, 4-inches (paved) ft.

Describe impacts and work involving bridge(s), culvert(s), pipe(s), and small structure(s). Provide details for small structure(s): structure number, type, size (length and dia.), location and impacts to water. Use a table if the number of small structures becomes large. If the table exceeds a complete page, put it in the appendix and summarize the information below with a citation to the table.

The structure, Bridge 026-38-03431 (NBI 007050), is a 44-foot 2-inch long continuous reinforced concrete slab bridge with two spans, built in 1946 with rehabilitations completed in 1982. The bridge carries SR 26 over Race Run. The project will replace the existing structure with a single-span continuous prestressed concrete bulb-tee beam bridge (Bridge No. 026-38-10835) (Appendix B, B-6 to B-14).

Since the rehabilitations were completed less than 50 years ago, it therefore does not qualify for listing in the National Register for Historic Places (NHRP) as defined in the Indiana Historic Bridge Inventory.

A 24-inch driveway HPDE pipe is located in the southwest quadrant and a 12-inch driveway RCP is in the southeast quadrant. Additionally, a 10-inch CMP is located upstream of the bridge. No reconstruction or relocation is planned for the southwest field entrance or the 10-inch CMP. However, the southeast field entrance will be reconstructed and relocated slightly to the east. No other bridge or small structures are located within the project area.

MAINTENANCE OF TRAFFIC (MOT) DURING CONSTRUCTION:

	Yes	No
Is a temporary bridge proposed?		<input checked="" type="checkbox"/>
Is a temporary roadway proposed?		<input checked="" type="checkbox"/>
Will the project involve the use of a detour or require a ramp closure? (describe below)	<input checked="" type="checkbox"/>	
Provisions will be made for access by local traffic and so posted.	<input checked="" type="checkbox"/>	
Provisions will be made for through-traffic dependent businesses.	<input checked="" type="checkbox"/>	
Provisions will be made to accommodate any local special events or festivals.	<input checked="" type="checkbox"/>	
Will the proposed MOT substantially change the environmental consequences of the action?		<input checked="" type="checkbox"/>
Is there substantial controversy associated with the proposed method for MOT?		<input checked="" type="checkbox"/>
Will the project require a sidewalk, curb ramp, and/or bicycle lane closure? (describe below)		<input checked="" type="checkbox"/>
Provisions will be made for access by pedestrians and/or bicyclist and so posted (describe below).		

Discuss closures, detours, and/or facilities (if any) that will be provided for maintenance of traffic. Any known impacts from these temporary measures should be quantified to the extent possible, particularly with respect to properties such as Section 4(f) resources

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and wetlands. Discuss any pedestrian/bicycle closures. Any local concerns about access and traffic flow should be detailed as well.

The MOT for the project will require a full road closure of SR 26 and a detour utilizing US 27, SR 67, and Ohio SR 49. The detour will remain in place for 4 months and will add approximately 38 miles and 45 minutes of travel time (Appendix B, B-10).

The closures/lane restrictions will pose a temporary inconvenience to traveling motorists (including school buses and emergency services); however, no significant delays are anticipated, and all inconveniences and delays will cease upon project completion.

ESTIMATED PROJECT COST AND SCHEDULE:

Engineering: \$ 157,000 (2025) Right-of-Way: \$ 50,000 (2026) Construction: \$ 1,354,000 (2027)

Anticipated Start Date of Construction: Spring 2027

RIGHT OF WAY:

Land Use Impacts	Amount (acres)	
	Permanent	Temporary
Residential	0.00	0.00
Commercial	0.00	0.00
Agricultural	0.65	0.03
Forest	0.89	0.00
Wetlands	0.00	0.00
Other:	0.00	0.00
Other:	0.00	0.00
TOTAL	1.54	0.03

Describe both Permanent and Temporary right-of-way and describe their current use. Typical and Maximum right-of-way widths (existing and proposed) should also be discussed. Any advance acquisition, reacquisition or easements, either known or suspected, and their impacts on the environmental analysis should be discussed.

Apparent existing ROW in the northern quadrants and southeast quadrant is the edge of pavement. Apparent existing ROW in the southwest quadrant is approximately 40 feet from the edge of pavement. The current use of ROW is roadway, maintained ROW, unmaintained ROW, forested habitat, and agricultural land.

The project requires approximately 1.54 acres of permanent ROW from the forested habitat north of SR 26 and agricultural land south of SR 26 for construction access, placement of riprap turnouts, and replacement of the structure. The project also requires approximately 0.03 acre of temporary ROW from the agricultural land in the southeast quadrant for reconstruction of the field entrance.

If the scope of work or permanent or temporary right-of-way amounts change, the INDOT Environmental Services Division (ESD) and the INDOT District Environmental Section will be contacted immediately.

Part III – Identification and Evaluation of Impacts of the Proposed Action

SECTION A - EARLY COORDINATION:

List the date(s) coordination was sent and all resource agencies that were contacted as a part of the development of this Environmental Study. Also, include the date of their response or indicate that no response was received.

Early coordination letters were sent on July 17, 2024, and March 3, 2025 (Appendix C, C-1 to C-3).

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<u>Agency</u>	<u>Date Sent</u>	<u>Date Response Received</u>	<u>Appendix</u>
Jay County Highway Department	07/17/2024	07/17/2024	C-4
Jay County Emergency Management Agency	07/17/2024	07/30/2024	C-5
Indiana Department of Natural Resources – Division of Fish and Wildlife (IDNR-DFW)	07/17/2024	08/16/2024	C-6 to C-8
Natural Resources Conservation Service (NRCS)	07/17/2024	08/21/2024	C-9 to C-10
Indiana Geological and Water Survey (IGWS)	07/17/2024 (online coordination)	03/03/2025 (automated response)	C-11
INDOT Aviation	03/03/2025	03/03/2025	C-12
Federal Highway Administration, Greenfield District	07/17/2024	No response	N/A
US Department of Housing and Urban Development	07/17/2024	No response	N/A
INDOT Greenfield District – Environmental Section Manager	07/17/2024	No response	N/A
INDOT Greenfield District – Project Manager	07/17/2024	No response	N/A
Indiana Department of Environmental Management (IDEM) – Wetlands and Stormwater Programs	07/17/2024	No response	N/A
Eastern Indiana Regional Planning Commission	07/17/2024	No response	N/A
Eighth Coast Guard District	07/17/2024	No response	N/A
US Army Corps of Engineers (USACE), Louisville District	07/17/2024	No response	N/A
Jay County Commissioner	07/17/2024	No response	N/A
Jay County Surveyor	07/17/2024	No response	N/A
Jay School Corporation	07/17/2024	No response	N/A
Jay County Floodplain Administrator	07/17/2024	No response	N/A

All applicable recommendations are included in the *Environmental Commitments* section of this CE document.

SECTION B – ECOLOGICAL RESOURCES:

Streams, Rivers, Watercourses & Other Jurisdictional Features

Federal Wild and Scenic Rivers
 State Natural, Scenic or Recreational Rivers
 Nationwide Rivers Inventory (NRI) listed
 Outstanding Rivers List for Indiana
 Navigable Waterways

Presence

X

Impacts

Yes

X

No

Total stream(s) in project area: 136 Linear feet Total impacted stream(s): 136 Linear feet

Stream Name	Classification	Total Size in Project Area (linear feet)	Impacted linear feet	Comments (i.e. location, flow direction, likely Water of the US, appendix reference)
Race Run	Intermittent	136	136	Flows south-north through the project area below SR 26, and is likely a Waters of the U.S. due to its hydrological connection to the Wabash River, which is a Traditionally Navigable Waterway (TNW) via Salamonie River (Appendix F, F-2 to F-26).

Describe all streams, rivers, watercourses and other jurisdictional features adjacent or within the project area. Include whether or not impacts (both permanent and temporary) will occur to the features identified. Include if the streams or rivers are listed on any federal

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or state lists for Indiana. Include if features are likely subject to federal or state jurisdiction. Discuss measures to avoid, minimize, and mitigate if impacts will occur.

Based on the desktop review, the aerial map of the project area (Appendix B, B-3), and the RFI report (Appendix E, E-1 to E-9) there are thirty-four rivers watercourse or other jurisdictional features within the 0.5-mile search radius. There is one stream (two segments) located within the project area. That number was confirmed by the site visit on May 29, 2024, by Burgess and Niple (B&N), Inc. staff.

A *Waters of the U.S. Determination / Wetland Delineation Report* was approved by INDOT Ecology, Waterway, Permitting, and Stormwater Office (EWPSO) on December 9, 2024. Please refer to Appendix F, F-2 to F-26 for the *Waters of the U.S. Determination / Wetland Delineation Report*. It was determined that one stream, Race Run (also known as Bolen Ditch), is located within the project area and would likely be considered a Waters of the U.S. The USACE makes all final determinations regarding jurisdiction.

Following the approval of the *Waters of the U.S. Determination / Wetland Delineation Report*, it was identified that the project limits extend outside the area previously investigated. Coordination with INDOT EWPSO occurred on March 11, 2025, and confirmed that no additional investigations were necessary (Appendix F, F-1).

The Federal, Wild and Scenic Rivers List; State Natural Scenic and Recreational Rivers List; Outstanding Rivers for Indiana; Navigable Waterways List; and National Rivers Inventory List were researched by B&N, Inc. staff to determine possible presence of protected waterways in the project area. No listed waterways were identified within or adjacent to the project area.

Race Run is classified as a poor intermittent stream that flows in a south-north direction through the project area below SR 26, under Bridge No. 026-38-03431. The stream has an upstream drainage area of 1.23 square miles. The ordinary high-water mark (OHWM) is 11-feet, 2-inches wide and 1 foot deep.

It is anticipated that approximately 136 feet (0.03 acre) of impacts to Race Run will occur due to the placement of riprap and dewatering measures. Impacts to Race Run will likely require the issuance of an IDEM Section 401 Water Quality Certification (WQC) and a USACE Section 404 Nationwide Permit (NWP). Stream mitigation is not anticipated to be required as permanent impacts will be less than 300 linear feet and/or 0.10 acre. This will be determined during the permitting process.

Stream impacts are unavoidable as the bridge replacement work is necessary to address the deterioration of the structure in order to achieve the purpose and need of the project. Stream impacts have been minimized to the greatest extent possible by using the minimum amount of riprap necessary, installing temporary erosion control measures around the stream, and revegetating bare areas after project completion.

IDNR-DFW responded on August 16, 2024, with recommendations to avoid or minimize impacts to streams. The response included recommendations regarding wildlife passage, streambank stabilization, placement of riprap and fill, riparian habitat, in-channel disturbance, timing restrictions on work in waterways, low-flow areas, construction access, dewatering, revegetation of bare areas with native species, and erosion and sediment control measures (Appendix C, C-6 to C-8).

All applicable recommendations are included in the *Environmental Commitments* section of this CE document.

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Open Water Feature(s)

Reservoirs
Lakes
Farm Ponds
Retention/Detention Basin
Storm Water Management Facilities
Other: _____

Presence

Impacts

Yes	No

Describe all open water feature(s) identified adjacent or within the project area. Include whether or not impacts (both permanent and temporary) will occur to the features identified. Include if features are likely subject to federal or state jurisdiction. Discuss measures to avoid, minimize, and mitigate if impacts will occur.

Based on the desktop review, the aerial map of the project area (Appendix B, B-3), and the RFI report (Appendix E, E-1 to E-9) there are three open water features within the 0.5-mile search radius. There are no open water features within or adjacent to the project area. That number was confirmed by the site visit on May 29, 2024, by B&N, Inc. staff.

A *Waters of the U.S. Determination / Wetland Delineation Report* was approved by INDOT EWPSO on December 9, 2024. Please refer to Appendix F, F-2 to F-26 for the *Waters of the U.S. Determination / Wetland Delineation Report*. It was determined that no open water features are located within the project area. The USACE makes all final determinations regarding jurisdiction.

Wetlands

Presence

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Impacts

Yes	No

Total wetland area: N/A Acre(s) Total wetland area impacted: N/A Acre(s)

(If a determination has not been made for non-isolated/isolated wetlands, fill in the total wetland area impacted above.)

Wetland No.	Classification	Total Size (Acres)	Impacted Acres	Comments (i.e. location, likely Water of the US, appendix reference)
N/A	N/A	N/A	N/A	N/A

Documentation

ESD Approval Dates

Wetlands (Mark all that apply)

Wetland Determination
Wetland Delineation
USACE Isolated Waters Determination

X

December 9, 2024

Improvements that will not result in any wetland impacts are not practicable because such avoidance would result in (Mark all that apply and explain):

Substantial adverse impacts to adjacent homes, business or other improved properties;
Substantially increased project costs;
Unique engineering, traffic, maintenance, or safety problems;
Substantial adverse social, economic, or environmental impacts, or
The project not meeting the identified needs.

Describe all wetlands identified adjacent or within the project area. Include whether or not impacts (both permanent and temporary) will occur to the features identified. Include if features are likely subject to federal or state jurisdiction. Discuss measures to avoid, minimize, and mitigate if impacts will occur.

Based on the desktop review, the aerial map of the project area (Appendix B, B-3), and the RFI report (Appendix E, E-1 to E-9) there are sixteen wetlands within the 0.5-mile search radius. There are two wetlands within or adjacent to the project area. That number was updated to zero during the site visit on May 29, 2024, by B&N, Inc. staff. Therefore, no impacts are expected.

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A *Waters of the U.S. Determination / Wetland Delineation Report* was approved by INDOT EWPSO on December 9, 2024. Please refer to Appendix F, F-2 to F-26 for the *Waters of the U.S. Determination / Wetland Delineation Report*. It was determined that no wetlands are located within the project area. The USACE makes all final determinations regarding jurisdiction.

Terrestrial Habitat

Presence

☒ X

Impacts

Yes ☒ X NO ☐

Total terrestrial habitat in project area: 1.30 Acre(s) Total tree clearing: 0.40 Acre(s)

Describe types of terrestrial habitat (i.e. forested, grassland, farmland, lawn, etc) adjacent or within the project area. Include whether or not impacts will occur to habitat identified. Include total terrestrial habitat impacted and total tree clearing that will occur. Discuss measure to avoid, minimize, and mitigate if impacts will occur.

Based on a desktop review, a site visit on May 29, 2024, by B&N, Inc. staff, and the aerial map of the project area (Appendix B, B-3), there is maintained grass, agricultural, and forested habitat within the project area. Dominant species noted during the site visit include red mulberry (*Morus rubra*), bush honeysuckle (*Lonicera maackii*), white avens (*Geum canadense*), Davis' sedge (*Carex davisii*), reed canary grass (*Phalaris arundinacea*), honey locust (*Gleditsia triacanthos*), common pawpaw (*Asimina triloba*), and creeping jenny (*Lysimachia nummularia*). The project will impact a total of approximately 1.30 acres of terrestrial habitat for replacement of the bridge. Approximately 0.40 acre of tree clearing (dominant species including *Morus rubra*, black walnut (*Juglans nigra*), *Gleditsia triacanthos*, hackberry (*Celtis occidentalis*), and *Asimina triloba*) will occur less than 100 feet from the edge of pavement, for construction access and placement of riprap turnouts. All tree clearing will occur during the bat inactive season. Terrestrial habitat impacts have been minimized to the greatest extent possible by minimizing the work area to that only needed to complete construction and meet the purpose and need of the project. Implementation of standard INDOT specifications for revegetation of disturbed areas will promote similar ground cover in areas temporarily impacted by site access. Mitigation for impacts to terrestrial habitat is not anticipated.

Avoidance and Minimization Measures (AMMs) generated from the U.S. Fish and Wildlife Service (USFWS) Information for Planning and Consultation (IPaC) portal included the following terrestrial AMM's: Tree Removal AMM 1, Tree Removal AMM 2, Tree Removal AMM 3, and Tree Removal AMM 4.

IDNR-DFW responded on August 16, 2024, with recommendations to avoid or minimize impacts to terrestrial habitat. The response included recommendations regarding wildlife passage, riparian habitat, revegetation of disturbed areas, tree clearing restrictions, and erosion and sediment control measures (Appendix C, C-6 to C-8).

All applicable recommendations are included in the *Environmental Commitments* section of this CE document.

Protected Species

Federally Listed Bats

Information for Planning and Consultation (IPaC) determination key completed
 Section 7 informal consultation completed (IPaC cannot be completed)
 Section 7 formal consultation Biological Assessment (BA) required

Yes

☒ X

No

☒ X
☒ X

Determination Received for Listed Bats from USFWS:

NE ☐

NLAA ☒ X

LAA ☐

Other Species not included in IPaC

Additional federal species found in project area (based on IPaC species list)
 State species (not bird) found in project area (based upon consultation with IDNR)

Yes

☒ X

No

☒ X

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Migratory Birds

Known usage or presence of birds (i.e. nests)

State bird species based upon coordination with IDNR

Yes

X

No

X

Discuss IDNR coordination and species identified. Describe USFWS Section 7 consultation and determination received for Indiana bat and northern long-eared bat impacts. Discuss if other federally listed species were identified. If so, include consultation that has occurred and the determination that was received. Discuss if migratory birds have been observed and any impacts.

Based on a desktop review and the RFI report (Appendix E, E-1 to E-9), completed by B&N, Inc. staff on July 29, 2024, the IDNR Jay County Endangered, Threatened and Rare (ETR) Species List has been checked. According to the IDNR-DFW early coordination response letter dated August 16, 2024 (Appendix C, C-6 to C-8), the Natural Heritage Program's Database has been checked and no plant or animal species listed as state or federally threatened, endangered, or rare have been documented within 0.5 mile of the project area. An INDOT 0.5-mile bat review occurred on March 21, 2024, and did not indicate the presence of endangered bat species in or within the project vicinity.

Project information was submitted through the USFWS's IPaC portal, and an official species list was generated (Appendix C, C-14 to C-23). The project is within range of the federally endangered Indiana bat (*Myotis sodalis*). Two other species were generated in the IPaC species list along with the Indiana bat. Refer to paragraph below.

The official species list generated from IPaC indicated two other species present within the project area; the whooping crane (*Grus americana*) and monarch butterfly (*Danaus Plexippus*). The whooping crane is listed as endangered wherever found, except where listed as an experimental population according to the Environmental Conservation Online System (<https://ecos.fws.gov/ecp/species/758>). The whooping crane is listed as an experimental population in this location. The monarch butterfly is identified as a proposed threatened species and is not yet listed. No further coordination is needed with USFWS.

The project qualifies for the *Range-wide Programmatic Informal Consultation for the Indiana bat and northern long-eared bat (NLEB)*, dated May 2016 (updated March 2023), between FHWA, Federal Railroad Administration (FRA), Federal Transit Administration (FTA), and USFWS. An INDOT Bridge Inspection occurred on October 10, 2024, (Appendix I, I-1 to I-10) and a bat assessment occurred on May 29, 2024 (Appendix C, C-40); no evidence of bats was seen or heard under the bridge. An effect determination key was completed on July 18, 2024, and based on the responses provided, the project was found to "*May Affect – Not Likely to Adversely Affect*" the Indiana bat and/or the NLEB (Appendix C, C-24 to C-39). INDOT reviewed and verified the effect finding on July 22, 2024, and requested USFWS's review of the finding. No response was received from USFWS within the 14-day review period; therefore, it was concluded they concur with the finding.

AMMs are included as firm commitments in the *Environmental Commitments* section of this document. The AMM's included are General AMM 1, Lighting AMM 1, Tree Removal AMM 1, Tree Removal AMM 2, Tree Removal AMM 3, and Tree Removal AMM 4.

A bridge inspection occurred on May 29, 2024, and no bats or evidence of bats was seen or heard under the bridge (Appendix C, C-40). USFWS Bridge/Structure Assessment are only valid for two years. If construction will begin after May 29, 2026, an inspection of the structure by a qualified individual, must be performed. Inspection of the structure should check for presence of bats/bat indicators and/or presence of birds. The results of the inspection must indicate no signs of bats or birds. If signs of bats or birds are documented during this inspection, the INDOT District Environmental Manager must be contacted immediately. This firm commitment is included in the *Environmental Commitments* of this document.

Bridge No. 026-38-03431, located 1.84 miles east of US 27, has shown evidence of use (i.e. nests) by a bird species protected under the Migratory Bird Treaty Act (MBTA) during the May 29, 2024, inspection. Avoidance and minimization measures must be implemented prior to the start of and during the nesting season. Nests without eggs or young should be removed prior to construction during the non-nesting season (September 8 – April 30) and during the nesting season if no eggs or young are present. Nests with eggs or young cannot be removed or disturbed during the nesting season (May 1 – September 7). Nests with eggs or young should be screened or buffered from active construction. Details of the required procedures are outlined in the "107-C-273 Migratory Bird Protection" Recurring Special Provision (RSP). This

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firm commitment is included in the *Environmental Commitments* of this document.

This precludes the need for further consultation on this project as required under Section 7 of the Endangered Species Act, as amended. If new information on endangered species at the site becomes available, or if project plans are changed, USFWS will be contacted for consultation.

Geological and Mineral Resources

- Project located within the Indiana Karst Region
- Karst features identified within or adjacent to the project area
- Oil/gas or exploration/abandoned wells identified in the project area

Yes

No

X
X
X

Date Karst Evaluation reviewed by INDOT EWPO (if applicable): _____

Discuss if project is located in the Indiana Karst Region and if any karst features have been identified in the project area (from RFI). Discuss response received from IGWS coordination. Discuss if any mines, oil/gas, or exploration/abandoned wells were identified and if impacts will occur. Include discussion of karst study/report was completed and results. (Karst investigation must comply with the current Protection of Karst Features during Planning and Construction guidance and coordinated and reviewed by INDOT EWPO)

Based on a desktop review and the Indiana Karst Region map, the project is located outside the designated Indiana Karst Region as outlined in the most current *Protection of Karst Features during Project Development and Construction*. According to the topo map of the project area (Appendix B, B-2) and the RFI report (Appendix E, E-1 to E-9), there are no karst features identified within or adjacent to the project area.

In the early coordination response dated March 3, 2025, the IGWS did not indicate that karst features exist in the project area (Appendix C, C-11). The response indicated that the project is located in a floodway and has a moderate liquefaction potential. Mineral resources exist within the project area; bedrock resources are classified as high potential and sand and gravel resources are classified as low potential.

The features will not be affected because all construction is within 100 feet of edge of pavement where these resources are not present. Response from IGWS has been communicated with the designer on March 31, 2025. No impacts are expected.

SECTION C – OTHER RESOURCES

Drinking Water Resources

- Wellhead Protection Area(s)
- Source Water Protection Area(s)
- Water Well(s)
- Urbanized Area Boundary
- Public Water System(s)

Presence

Impacts

Yes

No

Is the project located in the St. Joseph Sole Source Aquifer (SSA):

If Yes, is the FHWA/EPA SSA MOU Applicable?

If Yes, is a Groundwater Assessment Required?

Yes

No

X

Check the appropriate boxes and discuss each topic below. Provide details about impacts and summarize resource-specific coordination responses and any mitigation commitments. Reference responses in the Appendix.

Sole Source Aquifer (SSA)

The project is located in Jay County, which is not located within the area of the St. Joseph SSA, the only legally designated sole source aquifer in the state of Indiana. Therefore, the FHWA/EPA/INDOT SSA Memorandum of Understanding (MOU) is not applicable to this project, a detailed groundwater assessment is not needed, and no impacts are expected.

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Wellhead Protection Area and Source Water

The Indiana Department of Environmental Management's Wellhead Proximity Determinator website (<http://www.in.gov/idem/cleanwater/pages/wellhead/>) was accessed on June 10, 2024, by B&N, Inc. staff. This project is not located within a Wellhead Protection Area or Source Water Area. No impacts are expected.

Water Wells

The Indiana Department of Natural Resources Water Well Record Database website (<https://www.in.gov/dnr/water/3595.htm>) was accessed on March 13, 2025, by B&N, Inc. staff. No wells are located near this project. Therefore, no impacts are expected.

Urban Area Boundary

Based on a desktop review of IDEM MS4 map (<https://www.in.gov/idem/cleanwater/ms4s-boundaries-map-for-indiana/>) by B&N, Inc. staff on March 26, 2025, this project is not located in an Urban Area Boundary. No impacts are expected.

Public Water System

Based on a desktop review, a site visit on May 29, 2024, by B&N, Inc. staff, and the aerial map of the project area (Appendix B, B-3), no public water systems were identified. Therefore, no impacts are expected.

Floodplains

Project located within a regulated floodplain

Longitudinal encroachment

Transverse encroachment

Homes located in floodplain within 1000' up/downstream from project

Presence

<input checked="" type="checkbox"/>
<input checked="" type="checkbox"/>
<input checked="" type="checkbox"/>
<input type="checkbox"/>

Impacts

<u>Yes</u>	<u>No</u>
<input checked="" type="checkbox"/>	<input type="checkbox"/>
<input checked="" type="checkbox"/>	<input type="checkbox"/>
<input checked="" type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>

If applicable, indicate the Floodplain Level?

Level 1 ☐

Level 2 ☐

Level 3 ☐

Level 4 ☒

Level 5 ☐

Use the IDNR Floodway Information Portal to help determine potential impacts. Include floodplain map in appendix. Discuss impacts according to the classification system. If encroachment on a flood plain will occur, coordinate with the Local Flood Plain Administrator during design to insure consistency with the local flood plain planning.

Based on a desktop review of The Indiana Department of Natural Resources Indiana Floodway Information Portal website (<https://hazards-fema.maps.arcgis.com/apps/webappviewer/index.html>) on July 10, 2024, by B&N, Inc. staff, and the RFI report, this project is located in a regulatory floodplain as determined from approved IDNR floodplain map (Appendix F, F-9). An early coordination letter was sent on July 17, 2024, to the local Floodplain Administrator. The floodplain administrator did not respond within the 30-day time frame.

In an early coordination response dated August 16, 2024, the IDNR-DFW stated that formal approval for construction in a floodway, pursuant to the Flood Control Act (IC 14-28-1), unless it qualifies for a bridge exemption or qualifies under the INDOT and IDNR Memorandum of Understanding for Maintenance Activity Exemption, dated March 2023. This project qualifies for a rural bridge exemption and will not require a construction in a floodway permit.

This project qualifies as a Category 4 per the current INDOT CE Manual, which applies to projects involving the replacement of existing drainage structures on or near the same alignment. Category 4 states: Zero homes are located within the base floodplain within 1,000 feet upstream and zero homes are located within the base floodplain within 1,000 feet downstream. The proposed structure will have an effective capacity such that backwater surface elevations are not expected to substantially increase. As a result, there will be no substantial adverse impacts on natural and beneficial floodplain values; there will be no substantial change in flood risks; and there will be no substantial increase in potential for interruption or termination of emergency service or emergency evacuation routes; therefore, it has been determined that this encroachment is not substantial.

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Farmland

Agricultural Lands

Prime Farmland (per NRCS)

Presence

X
X

Impacts

Yes

No

X
X

Total Points (from Section VII of CPA-106/AD-1006*) 150

**If 160 or greater, see CE Manual for guidance.*

Discuss existing farmland resources in the project area, impacts that will occur to farmland, and mitigation and minimization measures considered.

Based on a desktop review, a site visit on May 29, 2024, by B&N, Inc. staff, and the aerial map of the project area (Appendix B, B-3), the project will convert 0.25 acre of farmland as defined by the Farmland Protection Policy Act. As design progressed, it was identified that 0.65 acre of agricultural land is present within the project limits. An early coordination letter was sent on July 17, 2024, to NRCS. Coordination with NRCS resulted in a score of 150 on the AD 1006 Form (Appendix C, C-10).

NRCS's threshold score for significant impacts to farmland that result in the consideration of alternatives is 160. Since this project score is less than the threshold, no significant loss of prime, unique, statewide, or local important farmland will result from this project. No alternatives other than those previously discussed in this document will be investigated without reevaluating impacts to prime farmland.

SECTION D – CULTURAL RESOURCES

Minor Projects PA

Category(ies) and Type(s)

A4, B-10, and B-12

INDOT Approval Date(s)

May 20, 2025

N/A

Full 106 Effect Finding

No Historic Properties Affected

☐

No Adverse Effect

☐

Adverse Effect

☐

Eligible and/or Listed Resources Present

NRHP Building/Site/District(s)

☐

Archaeology

☐

NRHP Bridge(s)

☐

Documentation Prepared (mark all that apply)

APE, Eligibility and Effect Determination

800.11 Documentation

Historic Properties Report or Short Report

Archaeological Records Check and Assessment

Archaeological Phase Ia Survey Report

Archaeological Phase Ic Survey Report

Other:

X

ESD Approval Date(s)

May 20, 2025

SHPO Approval Date(s)

Memorandum of Agreement (MOA)

☐

MOA Signature Dates (List all signatories)

--

If the project falls under the MPPA, describe the category(ies) that the project falls under and any approval dates. If the project requires full Section 106, use the headings provided. The completion of the Section 106 process requires that a Legal Notice be published in local newspapers. Please indicate the publication date, name of the paper(s) and the comment period deadline. Include any further Section 106 work which must be completed at a later date, such as mitigation from a MOA or avoidance commitments.

This is page 15 of 22 Project name: SR 26 over Race Run, Bridge Replacement Date: May 27, 2025

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On May 20, 2025, the INDOT Cultural Resource Office (CRO) determined that this project falls within the guidelines of Category A, Type 4, Category B, Type 10, and 12 under the Minor Projects Programmatic Agreement (MPPA) (Appendix D, D-1 to D-6). The type of work covered under the MPPA are listed below:

Category A, Type 4: Roadway work associated with surface replacement, reconstruction, rehabilitation, or resurfacing, projects, including overlays, shoulder treatments, pavement repair, seal coating, pavement grinding, and pavement marking within previously disturbed soils where replacement, repair, or installation of curbs, curb ramps, or sidewalks will not be required.

Category B, Type 10: Slide corrections, slope repairs, and other erosion control measures, in undisturbed soils.

Category B, Type 12: Replacement, widening, or raising the elevation of the superstructure on existing bridges, and bridge replacement projects (when both the superstructure and substructure are removed).

Archaeological investigations occurred on February 19, 2025, by a qualified professional archaeologist from Cultural Resources Analysts (CRA), Inc. It was determined that no archaeological materials were found and a Archaeological Short Report was prepared and submitted with the MPPA. This information is summarized in the MPPA Form (Appendix D, D-1 to D-6).

No further consultation is required. This completes the Section 106 process and the responsibilities of the FHWA under Section 106 have been fulfilled.

SECTION E – SECTION 4(f) RESOURCES/ SECTION 6(f) RESOURCES

	<u>Presence</u>	<u>Use</u>	
		Yes	No
Parks and Other Recreational Land			
Publicly owned park	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Publicly owned recreation area	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Other (school, state/national forest, bikeway, etc.)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Wildlife and Waterfowl Refuges			
National Wildlife Refuge	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
National Natural Landmark	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
State Wildlife Area	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
State Nature Preserve	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Historic Properties			
Site eligible and/or listed on the NRHP	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
<u>Evaluations</u>			
<u>Prepared</u>			
Programmatic Section 4(f)	<input type="checkbox"/>		
"De minimis" Impact	<input type="checkbox"/>		
Individual Section 4(f)	<input type="checkbox"/>		
Any exception included in 23 CFR 774.13	<input type="checkbox"/>		

Discuss Programmatic Section 4(f) and "de minimis" Section 4(f) impacts in the discussion below. Individual Section 4(f) documentation must be included in the appendix and summarized below. Discuss proposed alternatives that satisfy the requirements of Section 4(f). FHWA has identified various exceptions to the requirement for Section 4(f) approval. Refer to 23 CFR § 774.13 - Exceptions.

Section 4(f) of the U.S. Department of Transportation Act of 1966 prohibits the use of certain public and historic lands for federally funded transportation facilities unless there is no feasible and prudent alternative. The law applies to significant publicly owned parks, recreation areas, wildlife / waterfowl refuges, and NRHP eligible or listed historic properties regardless of ownership. Lands subject to this law are considered Section 4(f) resources.

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Based on a desktop review, the aerial map of the project area (Appendix B, B-3), and the RFI report (Appendix E, E-1 to E-9) there are no potential 4(f) resources located within the 0.5-mile search radius. According to additional research using the Division of Historic Preservation and Archaeology (DHPA) and Indiana Historic Buildings, Bridges, and Cemeteries (IHBBC) public map (<https://gisdata.in.gov/portal/apps/webappviewer/index.html>), no potential Section 4(f) resources are located within or adjacent to the project area. On May 20, 2025, the INDOT CRO determined that this project falls within the guidelines of Category A, Type 4, Category B, Type 10 and 12 under the MPPA (Appendix D, D-1 to D-6). Therefore, no 4(f) use is expected.

Section 6(f) Involvement**Presence****Use****Yes****No****Section 6(f) Property**☐☐☐

Discuss Section 6(f) resources present or not present. Discuss if any conversion would occur as a result of this project. If conversion will occur, discuss the conversion approval.

The U.S. Land and Water Conservation Fund Act of 1965 established the Land and Water Conservation Fund (LWCF), which was created to preserve, develop, and assure accessibility to outdoor recreation resources. Section 6(f) of this Act prohibits conversion of lands purchased with LWCF monies to a non-recreation use.

A review of 6(f) properties on the INDOT ESD website revealed a total of two properties in Jay County (Appendix I, I-11). None of these properties are located within or adjacent to the project area. Therefore, there will be no impacts to 6(f) resources.

SECTION F – Air Quality**STIP/TIP and Conformity Status of the Project**

Is the project in the most current STIP/TIP?

Is the project located in an MPO Area?

Is the project in an air quality non-attainment or maintenance area?

If Yes, then:

Is the project in the most current MPO TIP?

Is the project exempt from conformity?

If No, then:

Is the project in the Transportation Plan (TP)?

Is a hot spot analysis required (CO/PM)?

Yes**No**

X

X
X

Location in STIP:

Name of MPO (if applicable):

Location in TIP (if applicable):

Level of MSAT Analysis required?

Level 1a

☒

Level 1b

☐

Level 2

☐

Level 3

☐

Level 4

☐

Level 5

☐FY 2024-2028, STIP, Updated Rural Project
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N/A

N/A

Describe if the project is listed in the STIP and if it is in a TIP. Describe the attainment status of the county(ies) where the project is located. Indicate whether the project is exempt from a conformity determination. If the project is not exempt, include information about the TP and TIP. Describe if a hot spot analysis is required and the MSAT Level.

Statewide Transportation Improvement Program (STIP)/Transportation Improvement Program (TIP)

This project is included in the Fiscal Year (FY) 2024-2028 STIP (Appendix H, H-1 to H-5).

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Attainment Status

This project is located in Jay County, which is currently in attainment for all criteria pollutants according to the Indiana Current Status and Nonattainment History, by County (https://www.in.gov/idem/sips/files/nonattainment_county_list.pdf). Therefore, the conformity procedures of 40 CFR Part 93 do not apply.

MSAT

This project is of a type qualifying as a categorical exclusion (Group 1) under 23 CFR 771.117(c), or exempt under the Clean Air Act conformity rule under 40 CFR 93.126, and as such, a Mobile Source Air Toxics analysis is not required.

SECTION G - NOISE

Noise

Yes

No

Is a noise analysis required in accordance with FHWA regulations and INDOT's traffic noise policy? ☐

☒

Date Noise Analysis was approved/technically sufficient by INDOT ESD: _____

Describe if the project is a Type I or Type III project. If it is a Type I project, describe the studies completed to date and if noise impacts were identified. If noise impacts were identified, describe if abatement is feasible and reasonable and include a statement of likelihood.

Type III Project

This project is a Type III project. In accordance with 23 CFR 772 and the current *Indiana Department of Transportation Traffic Noise Analysis Procedure*, this action does not require a formal noise analysis.

SECTION H - COMMUNITY IMPACTS

Regional, Community & Neighborhood Factors

Will the proposed action comply with the local/regional development patterns for the area?

Yes

No

☒☐

Will the proposed action result in substantial impacts to community cohesion?

☐☒

Will the proposed action result in substantial impacts to local tax base or property values?

☐☒

Will construction activities impact community events (festivals, fairs, etc.)?

☐☒

Does the community have an approved transition plan?

☒☐

If No, are steps being made to advance the community's transition plan?

☐☐

Does the project comply with the transition plan? (explain in the discussion below)

☒☐

Discuss how the project complies with the area's local/regional development patterns; whether the project will impact community cohesion; and impact community events. Discuss how the project conforms with the ADA Transition Plan.

The project is not anticipated to result in substantial impacts to community cohesion because it will not change access to properties within the area or divide existing communities. Though the project may cause delays to the motoring public during construction, it is not expected to impact the surrounding community or cause economic impacts to the surrounding area. Therefore, the project will have minimal or no negative impacts to the community or local economy.

Visit Jay County (<https://visitjaycounty.com/>) was accessed on April 2, 2025, by B&N, Inc. staff to identify events or festivals occurring during construction. To date, no events are listed for spring 2027. However, if an event occurs during the construction period, accommodations will be made to maintain access to local special events and/or festivals.

The bridge is located in a rural part of Jay County, and there are no commercial areas, sidewalks, or trails in or near the project area. The nearest urbanized area, Portland, is located approximately 1 mile away; therefore, the project will have no involvement with sidewalks or other facilities regulated under the Americans with Disabilities Act (ADA). The project complies with the Jay County, 2019-2023 Economic Development Plan.

Public Facilities and Services

Discuss what public facilities and services are present in the project area and impacts (such as MOT) that will occur to them. Include how the impacts have been minimized and what coordination has occurred. Some examples of public facilities and services include health facilities, educational facilities, public and private utilities, emergency services, religious institutions, airports, transportation or

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public pedestrian and bicycle facilities.

Based on a desktop review, the aerial map of the project area (Appendix B, B-3), and the RFI report (Appendix E, E-1 to E-9) there is one public airport within the 0.5-mile search radius. There are no public facilities within or adjacent to the project area, which was confirmed by the site visit on May 29, 2024, by B&N, Inc. staff, and google imagery. Therefore, no impacts are expected. Access to all properties will be maintained during construction.

Based on the approved RFI (Appendix E, E-1 to E-9), the project is located within 20,000 feet (3.8 miles) of a public-use airport (Portland Municipal Airport). Coordination with INDOT Aviation occurred on March 3, 2025, and they responded on March 3, 2025, stating the elevation of the project site is slightly lower than the airport. If no construction equipment is over 125 feet in height, there is no need for a tall structure permit (Appendix C, C-12). This is included as a firm commitment in the *Environmental Commitments* section of this document.

It is the responsibility of the project sponsor to notify school corporations and emergency services at least two weeks prior to any construction that would block or limit access.

Environmental Justice (EJ) (Presidential EO 12898)

During the development of the project were EJ issues identified?

Yes	No
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>

Does the project require an EJ analysis?

If YES, then:

Are any EJ populations located within the project area?

<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>

Will the project result in adversely high and disproportionate impacts to EJ populations?

Indicate if EJ issues were identified during project development. If an EJ analysis was not required, discuss why. If an EJ analysis was required, describe how the EJ population was identified. Include if the project has a disproportionately high or adverse effect on EJ populations and explain your reasoning. If yes, describe actions to avoid, minimize and mitigate these effects.

Due to the issuance of recent federal Executive Orders (EO) from January 2025, including EO 14154, EO 14148, and EO 14173, EO 12898 has been rescinded and this section is no longer applicable.

Relocation of People, Businesses or Farms

Will the proposed action result in the relocation of people, businesses or farms?

Yes	No
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>

Is a BIS or CSRS required?

Number of relocations: Residences: _____ Businesses: _____ Farms: _____ Other: _____

Discuss any relocations that will occur due to the project. If a BIS or CSRS is required, discuss the results in the discussion below.

No relocations of people, businesses, or farms will take place as a result of this project.

SECTION I – HAZARDOUS MATERIALS & REGULATED SUBSTANCES

Hazardous Materials & Regulated Substances (Mark all that apply)

Red Flag Investigation (RFI)

Phase I Environmental Site Assessment (Phase I ESA)

Phase II Environmental Site Assessment (Phase II ESA)

Design/Specifications for Remediation required?

Documentation

<input checked="" type="checkbox"/>
<input type="checkbox"/>
<input type="checkbox"/>
<input type="checkbox"/>

Date RFI concurrence by INDOT SAM (if applicable): July 29, 2024

Include a summary of the potential hazardous material concerns found during review. Discuss in depth sites found within, directly adjacent to, or ones that could impact the project area. Refer to current INDOT SAM guidance. If additional documentation (special provisions, pay quantities, etc.) will be needed, include in discussion. Include applicable commitments.

Based on a review of GIS and available public records, the RFI was completed on July 29, 2024, by B&N, Inc. staff and INDOT SAM provided their concurrence on July 29, 2024 (Appendix E, E-1 to E-9). One confined feeding operation (CFO)

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is located within 0.5 mile of the project area. None of the hazmat sites identified will impact the project. Further investigation for hazardous material concerns is not required at this time.

Part IV – Permits and Commitments

PERMITS CHECKLIST

Permits (mark all that apply)

Likely Required**Army Corps of Engineers (404/Section10 Permit)**

Nationwide Permit (NWP)

☒

Regional General Permit (RGP)

☐

Individual Permit (IP)

☐

Other

☐**IN Department of Environmental Management
(401/CSGP)**

Water Quality Certification (WQC)

☒

Regional General Permit (RGP)

☐

Individual Permit (IP)

☐

Isolated Wetlands

☐

Construction Stormwater General Permit (CSGP)

☒

Other

☐**IN Department of Natural Resources**

Construction in a Floodway

☐

Navigable Waterway Permit

☐

Other

☐**Mitigation Required****US Coast Guard Section 9 Bridge Permit**☐**Others (Please discuss in the discussion below)**☐

List the permits likely required for the project and summarize why the permits are needed, including permits designated as "Other."

Due to the anticipated impacts to Race Run, an IDEM 401 WQC and a USACE 404 NWP will likely be required. Mitigation is not anticipated to be required and will be confirmed during the permitting process.

Although the project is located within a floodway, it qualifies for the rural bridge exemption and will not require a Construction in a Floodway permit.

A CSGP will be required because soil disturbance will be over the 1-acre threshold.

Applicable recommendations provided by resource agencies are included in the *Environmental Commitments* section of this document. If permits are found to be necessary, the conditions of the permit will be requirements of the project and will supersede these recommendations.

It is the responsibility of the project sponsor to identify and obtain all required permits.

ENVIRONMENTAL COMMITMENTS

List all commitments and include the name of agency/organization requesting/requiring the commitment(s). Listed commitments should be numbered.

FIRM

- 1) If the scope of work or permanent or temporary right-of-way amounts change, the INDOT ESD and the INDOT District Environmental Section will be contacted immediately. (INDOT ESD and INDOT Greenfield District)

Indiana Department of Transportation

County Jay

Route SR 26

Des. No. 2200586

- 2) It is the responsibility of the project sponsor to notify school corporations and emergency services at least two weeks prior to any construction that would block or limit access. (INDOT ESD)
- 3) **General AMM 1:** Ensure all operators, employees, and contractors working in areas of known or presumed bat habitat are aware of all FHWA/FRA/FTA (Transportation Agencies) environmental commitments, including all applicable AMMs. (USFWS)
- 4) **Lighting AMM 1:** Direct temporary lighting away from suitable habitat during the active season. (USFWS)
- 5) **Tree Removal AMM 1:** Modify all phases/aspects of the project (e.g., temporary work areas, alignments) to avoid tree removal. (USFWS)
- 6) **Tree Removal AMM 2:** Apply time of year restrictions for tree removal when bats are not likely to be present, or limit tree removal to 10 or fewer trees per project at any time of year within 100 feet of existing road/rail surface and outside of documented roosting/foraging habitat or travel corridors; visual emergence survey must be conducted with no bats observed. No tree removal from April 1 through November 14. (USFWS & IDNR-DFW)
- 7) **Tree Removal AMM 3:** Ensure tree removal is limited to that specified in project plans and ensure that contractors understand clearing limits and how they are marked in the field (e.g., install bright colored flagging/fencing prior to any tree clearing to ensure contractors stay within clearing limits). (USFWS)
- 8) **Tree Removal AMM 4:** Do not remove documented Indiana bat or NLEB roosts that are still suitable for roosting, or trees within 0.25 miles of roosts, or documented foraging habitat any time of year. (USFWS)
- 9) USFWS Bridge/Structure Assessments shall take place no earlier than two (2) years prior to the start of construction. If construction will begin after May 29, 2026, an inspection of the structure by a qualified individual, must be performed. Inspection of the structure should check for presence of bats/bat indicators and/or presence of birds. The results of the inspection must indicate no signs of bats or birds. If signs of bats or birds are documented during this inspection, the INDOT District Environmental Manager must be contacted immediately. (INDOT ESD)
- 10) Bridge No. 026-38-03431, located 1.84 miles east of US 27, has shown evidence of use (i.e. nests) by a bird species protected under the Migratory Bird Treaty Act (MBTA) during the May 29, 2024, inspection. Avoidance and minimization measures must be implemented prior to the start of and during the nesting season. Nests without eggs or young should be removed prior to construction during the non-nesting season (September 8 – April 30) and during the nesting season if no eggs or young are present. Nests with eggs or young cannot be removed or disturbed during the nesting season (May 1 – September 7). Nests with eggs or young should be screened or buffered from active construction. Details of the required procedures are outlined in the “107-C-273 Migratory Bird Protection” RSP. (INDOT ESD)
- 11) If construction equipment is over 125 feet in height, a tall structure permit will be required. (INDOT Aviation)

For Consideration

- 1) For crossing replacements, the new structure must include wildlife passage appropriate for the type of replacement structure being proposed. If the existing structure is sized to accommodate white-tailed deer passage, then it should be included in the design of the new structure. If white-tailed deer passage is not possible with the existing structure, deer passage still needs to be considered in the design and at minimum the bank lines must be restored within structures to allow for smaller wildlife passage above the OHWM. Wildlife passage designs should include a smooth level pathway preferably 3 feet wide but a minimum of 1-2 feet in width composed of natural substrate (soil, sand, gravel, etc.) or compacted aggregate fill over riprap (#2, #53, #73, etc.) tied into existing elevations both upstream and downstream. The stream crossing repairs or modifications, and any bank stabilization under or around the structure, must not create conditions that are less favorable for wildlife passage when compared to existing conditions. Upgrading wildlife passage for rehabilitated/modified structures is encouraged whenever possible to improve wildlife/vehicle safety. (IDNR-DFW)
- 2) Impacts to non-wetland forest of one (1) acre or more in a rural or urban area should be mitigated at a minimum 2:1 ratio based on area of impact. Impacts to non-wetland forest under one (1) acre but at least 0.10 acre in a rural or urban area should be mitigated at a minimum 1:1 ratio based on area of impact. Impacts under 0.10 acre in a rural area typically do not require mitigation or additional plantings beyond seeding and stabilizing disturbed areas, though there are exceptions for high quality habitat sites. Impacts under 0.10 acre in an urban area should be mitigated by replacing each mature tree removed (trees that are 10" dbh) with two trees of 3-gallon stock or larger. (IDNR-DFW)

Indiana Department of Transportation

County Jay

Route SR 26

Des. No. 2200586

- 3) Do not cut any trees suitable for Indiana Bat or Northern Long-eared Bat roosting (3 inches or greater diameter-at-breast height, living or dead, with loose hanging bark, or with cracks, crevices, or cavities) from April 1 through September 30. (IDNR-DFW)
- 4) Do not excavate in the low flow area except for the placement of piers, foundations, and riprap, or the removal of the old structure. (IDNR-DFW)
- 5) Do not construct any temporary runarounds, access bridges, causeways, cofferdams, diversions, or pumparounds. (IDNR-DFW)
- 6) Use minimum average 6-inch graded riprap stone extended below the normal water level to provide habitat for aquatic organisms in the voids. (IDNR-DFW)

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STATE ROAD 26 OVER RACE RUN, BRIDGE REPLACEMENT, Des. No. 2200586

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Appendix A

INDOT Supporting Documentation

Categorical Exclusion Level Thresholds

	PCE	Level 1	Level 2	Level 3	Level 4 ¹
Section 106	Falls within guidelines of Minor Projects PA	"No Historic Properties Affected"	"No Adverse Effect"	-	"Adverse Effect" Or Historic Bridge involvement ²
Stream Impacts³	No construction in waterways or water bodies	< 300 linear feet of stream impacts	≥ 300 linear feet of stream impacts	-	USACE Individual 404 Permit ⁴
Wetland Impacts³	No adverse impacts to wetlands	< 0.1 acre	-	< 1.0 acre	≥ 1.0 acre
Right-of-way⁵	Property acquisition for preservation only or none	< 0.5 acre	≥ 0.5 acre	-	-
Relocations⁶	None	-	-	< 5	≥ 5
Threatened/Endangered Species (Species Specific Programmatic for Indiana bat & northern long eared bat)*	"No Effect", "Not likely to Adversely Affect" (With select AMMs ⁷)	"Not likely to Adversely Affect" (With any AMMs or commitments)	-	"Likely to Adversely Affect"	Project does not fall under Species Specific Programmatic ⁸
Threatened/Endangered Species (Any other species)*	Falls within guidelines of USFWS 2013 Interim Policy or "No Effect"	"Not likely to Adversely Affect"	-	-	"Likely to Adversely Affect"
Environmental Justice	No disproportionately high and adverse impacts	-	-	-	Potential ⁹
Sole Source Aquifer	No Detailed Groundwater Assessment	-	-	-	Detailed Groundwater Assessment
Floodplain	No Substantial Impacts	-	-	-	Substantial Impacts
Section 4(f) Impacts	None	-	-	-	Any ¹⁰
Section 6(f) Impacts	None	-	-	-	Any
Permanent Traffic Alteration	None	-	-	-	Any
Noise Analysis Required	No	-	-	-	Yes
Air Quality Analysis Required	No	-	-	-	Yes ¹¹
Approval Level <ul style="list-style-type: none"> District Env. (DE) Env. Serv. Div. (ESD) FHWA 	Concurrence by DE or ESD	DE or ESD	DE or ESD	DE and/or ESD	DE and/or ESD; and FHWA

¹ Coordinate with INDOT Environmental Services Division. INDOT will then coordinate with the appropriate FHWA Environmental Specialist.

² Any involvement with a bridge processed under the Historic Bridge Programmatic Agreement.

³ Total permanent impacts to streams (linear feet) and wetlands (acres).

⁴ US Army Corps of Engineers Individual 404 Permit

⁵ Total permanent and temporary right-of-way. This does not include reacquisition of existing apparent right-of-way.

⁶ If any relocations are within an area with a known or suspected Environmental Justice (EJ) or disadvantaged population, or has greater than 5 relocations, a conversation with FHWA, through INDOT ESD, is needed to confirm NEPA classification and outreach plan for the project.

⁷ Avoidance and Mitigation Measures (AMMs) determined by the IPAC determination key to be required that are not tree AMMs, bridge AMMs, or structure AMMs.

⁸ Projects that do not fall under a Species Specific Programmatic and results in a "Likely to Adversely Affect". Other findings can be processed as a lower level CE.

⁹ Potential for causing a disproportionately high and adverse impact.

¹⁰ Section 4(f) use resulting in an Individual, Programmatic, or *de minimis* evaluation. The only exception is a *de minimis* evaluation for historic properties (Effective January 2, 2020). If a historic property *de minimis* and no other use, mark the *None* column.

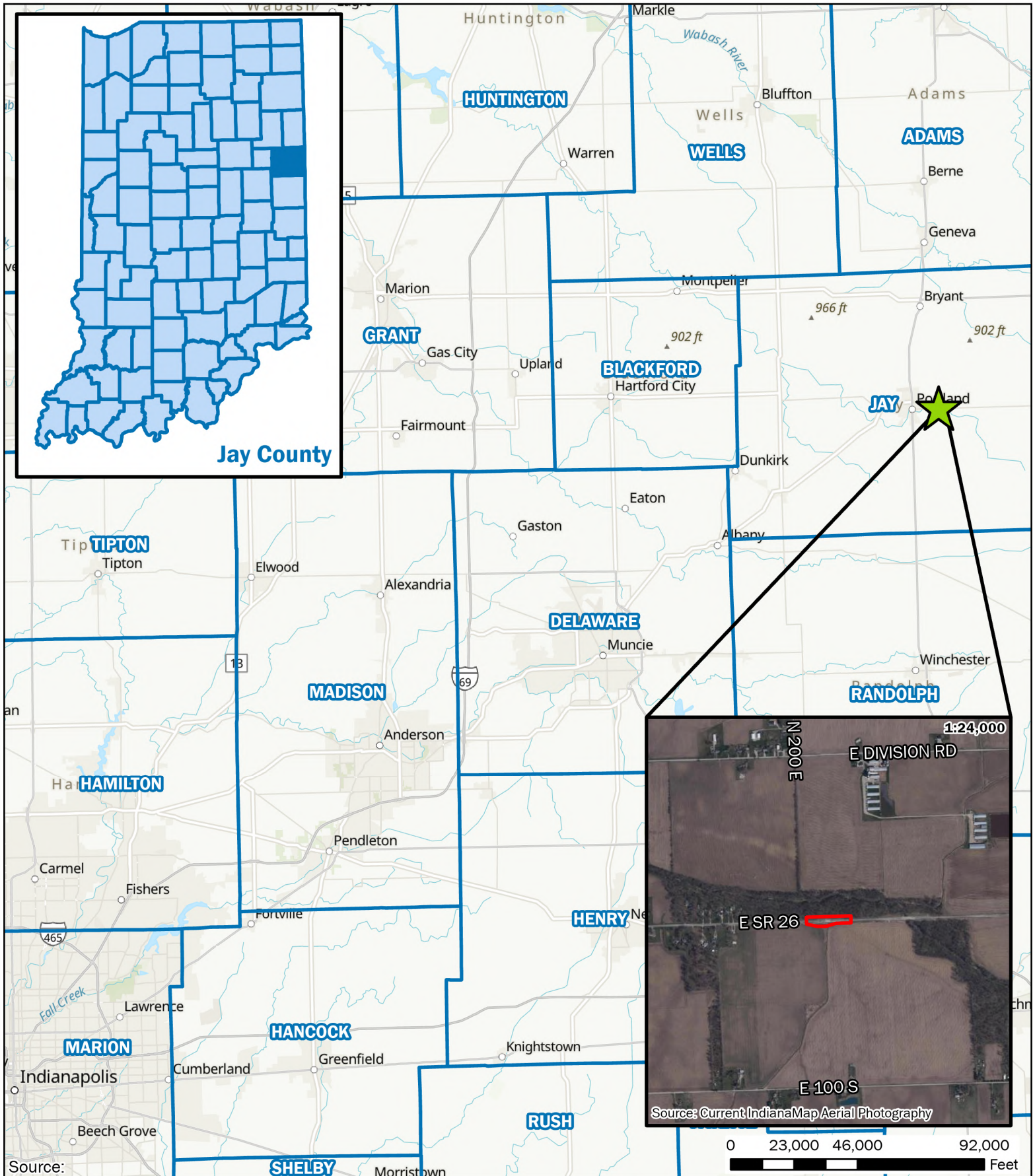
¹¹ Hot Spot Analysis and/or MSAT Quantitative Emission Analysis.

* Includes the threatened/endangered species critical habitat

Note: Substantial public or agency controversy may require a higher-level NEPA document.

Appendix B

Graphics

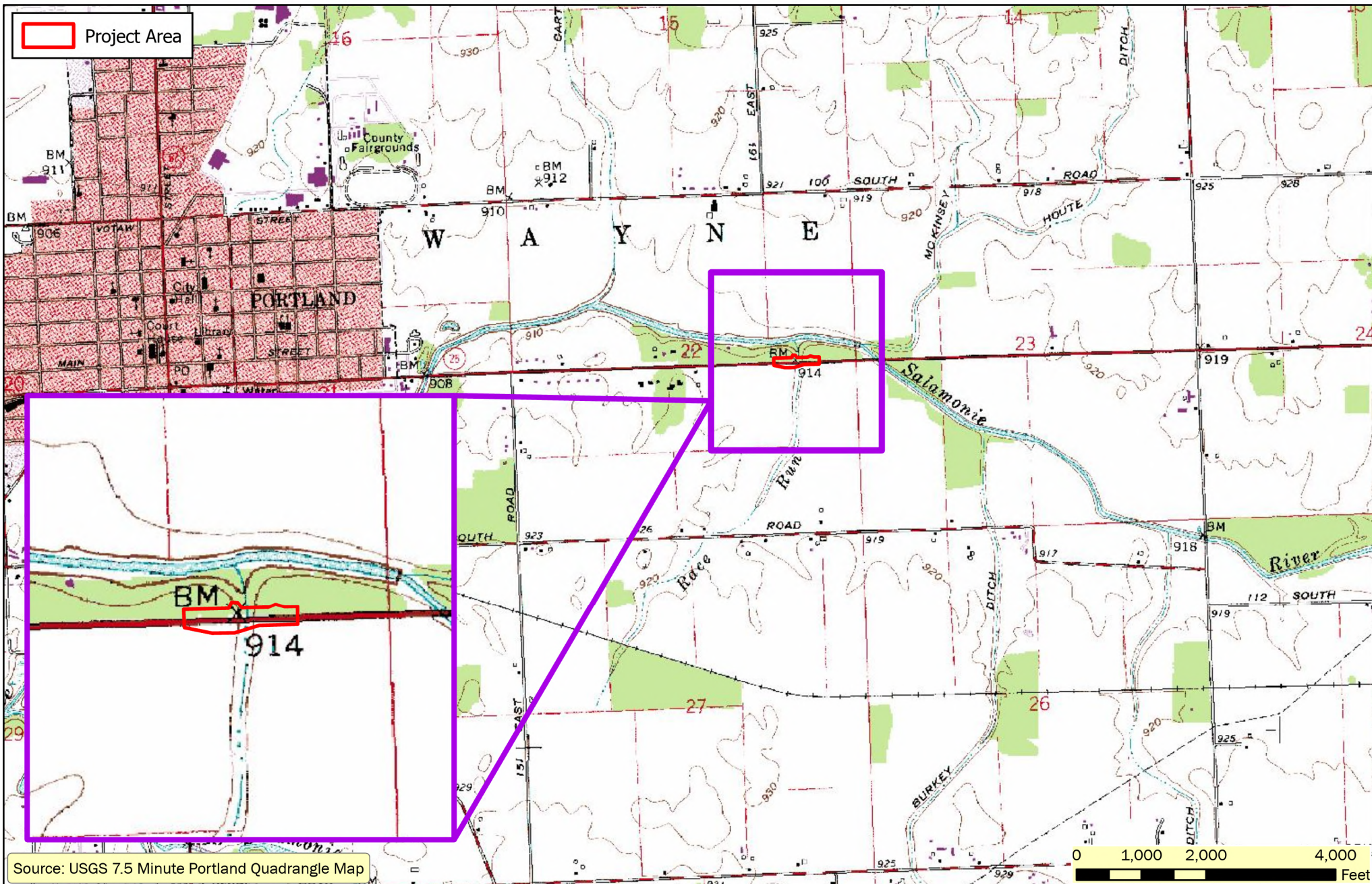


Project Location Map

BURGESS & NIPLE
Engineers ■ Environmental Scientists

SR 26 over Race Run, Bridge Replacement
Des No. 2200586
near Portland, Jay Co., Indiana





USGS Topographic Map

BURGESS & NIPLÉ
Engineers ■ Environmental Scientists

SR 26 over Race Run, Bridge Replacement
Des No. 2200586
near Portland, Jay Co., Indiana

6/25/2024

N

M

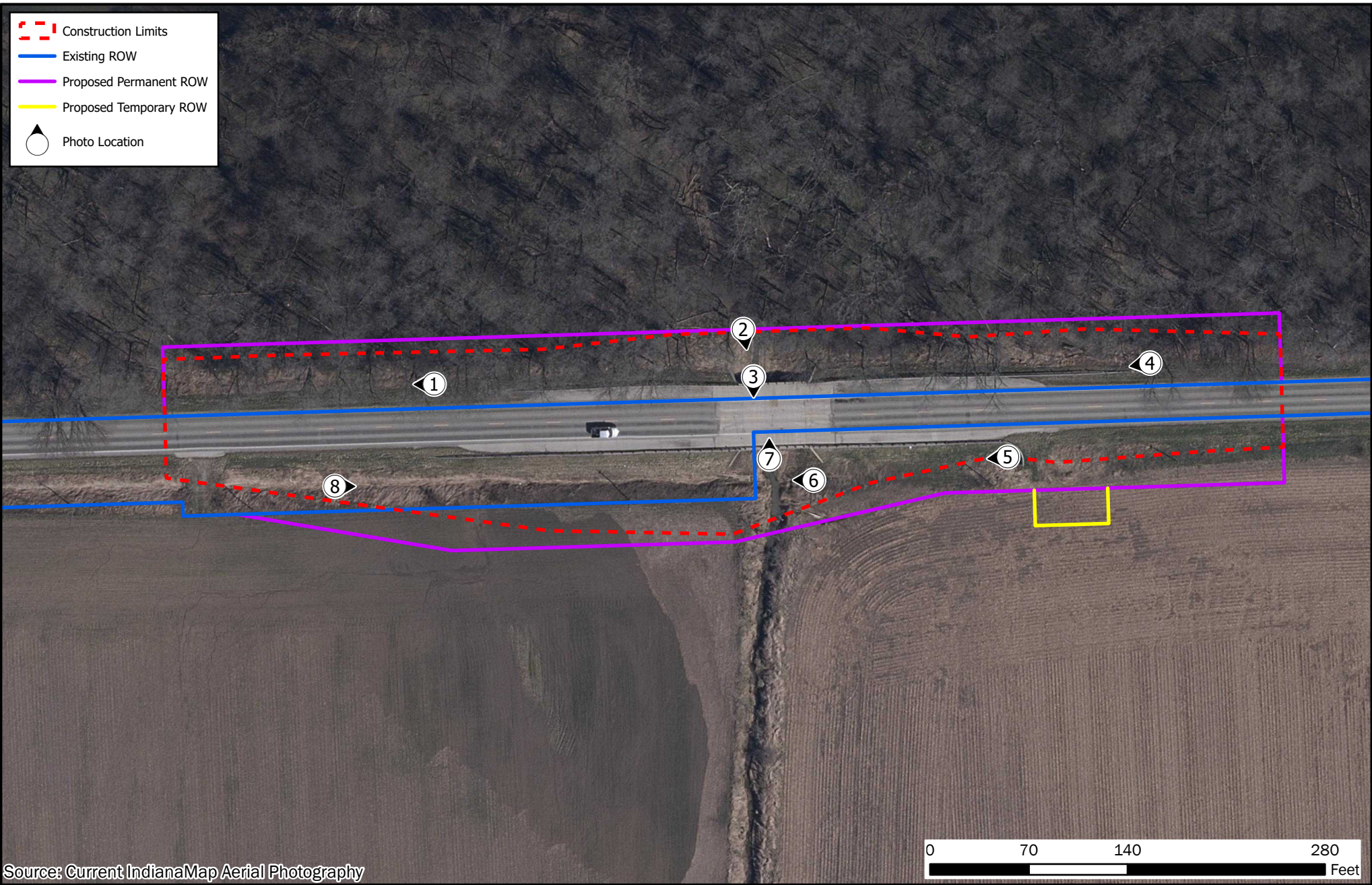
Construction Limits

Existing ROW

Proposed Permanent ROW

Proposed Temporary ROW

Photo Location



Project Aerial and Photo Location Map

<div> <div>4/2/2025</div> <div> <div>BURGESS & NIPLE</div> <div>Engineers ■ Environmental Scientists</div> </div> </div>	<div>SR 26 over Race Run, Bridge Replacement</div> <div>Des No. 2200586</div> <div>near Portland, Jay Co., Indiana</div>	<div> <div>N</div> </div>
	<div>Author: krahn Des No. 2200586</div>	



P1 - Looking west along the westbound lane of SR 26 in the NW quad.



P2 - Looking southeast (upstream) along Race Run at BR 026-38-03431.



P3 - Looking south (upstream) along Race Run under BR 026-38-03431.



P4 - Looking west along the westbound lane of SR 26 in the NE quad.



P5 - Looking west along the eastbound lane of SR 26 in the SE quad.



P6 - Looking west along the eastbound lane of SR 26 in the SE quad, towards Race Run.



P7 - Looking north (downstream) along Race Run under BR 026-38-03431.



P8 - Looking east along the eastbound lane of SR 26 in the SW quad.

PROJECT	DESIGNATION
2200586	2200586
CONTRACT	BRIDGE FILE
B-44619	026-38-XXXX

STRUCTURE INFORMATION				
STRUCTURE	TYPE	SPAN AND SKEW	OVER	STATION
026-38-XXXX	Continuous Prestressed Concrete Bulb-Tee Beam Bridge	1 Span; 83'-0" Skew: Square	Bolen Ditch	52+79.00 Line "BrA"

NOTE TO REVIEWER: A NEW BRIDGE FILE NUMBER
HAS BEEN REQUESTED BUT NOT YET RECEIVED

INDIANA DEPARTMENT OF TRANSPORTATION



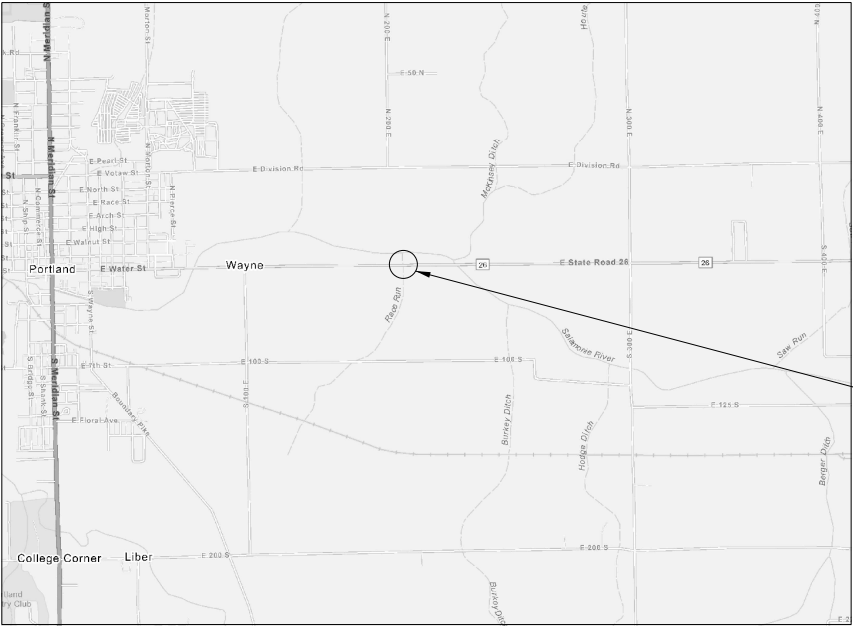
BRIDGE PLANS

FOR SPANS OVER 20 FEET

ROUTE: SR 26 AT: RP 142+24

PROJECT NO. 2200586 P.E.
2200586 R/W
2200586 CONST.

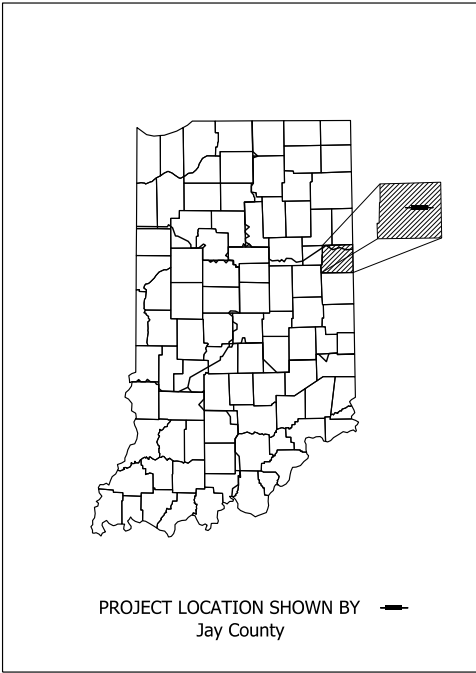
Bridge Replacement on SR26 over Bolen Ditch
Located 1.84 Miles East of US 27
Section 22, T-23-N, R-14-E, Wayne of Township, Jay County, Indiana



SCALE: 1" = 2000'

Project Location
Str. 026-38-XXXX
Sta. 52+79.00 Line "BrA" over Bolen Ditch
Begin Sta. 49+40 Line "BrA"
End Sta. 55+50 Line "BrA"

TRAFFIC DATA		
A.A.D.T.	(2026)	2,494 V.P.D.
A.A.D.T.	(2046)	2,756 V.P.D.
D.H.V	(2046)	328 V.P.H.
DIRECTIONAL DISTRIBUTION		32.56 %
TRUCKS		15.93 % A.A.D.T. 12.07 % D.H.V.
DESIGN DATA		
DESIGN SPEED		55 M.P.H.
PROJECT DESIGN CRITERIA		3R (NON-FREEWAY)
FUNCTIONAL CLASSIFICATION		MAJOR COLLECTOR
RURAL/URBAN		RURAL
TERRAIN		LEVEL
ACCESS CONTROL		N/A



LATITUDE: N 40°25'58" LONGITUDE: W 84°56'35"

BRIDGE LENGTH: 0.016 MI.
ROADWAY LENGTH: 0.100 MI.
TOTAL LENGTH: 0.116 MI.
MAX. GRADE: 1.35 %

HUC: 051201020105

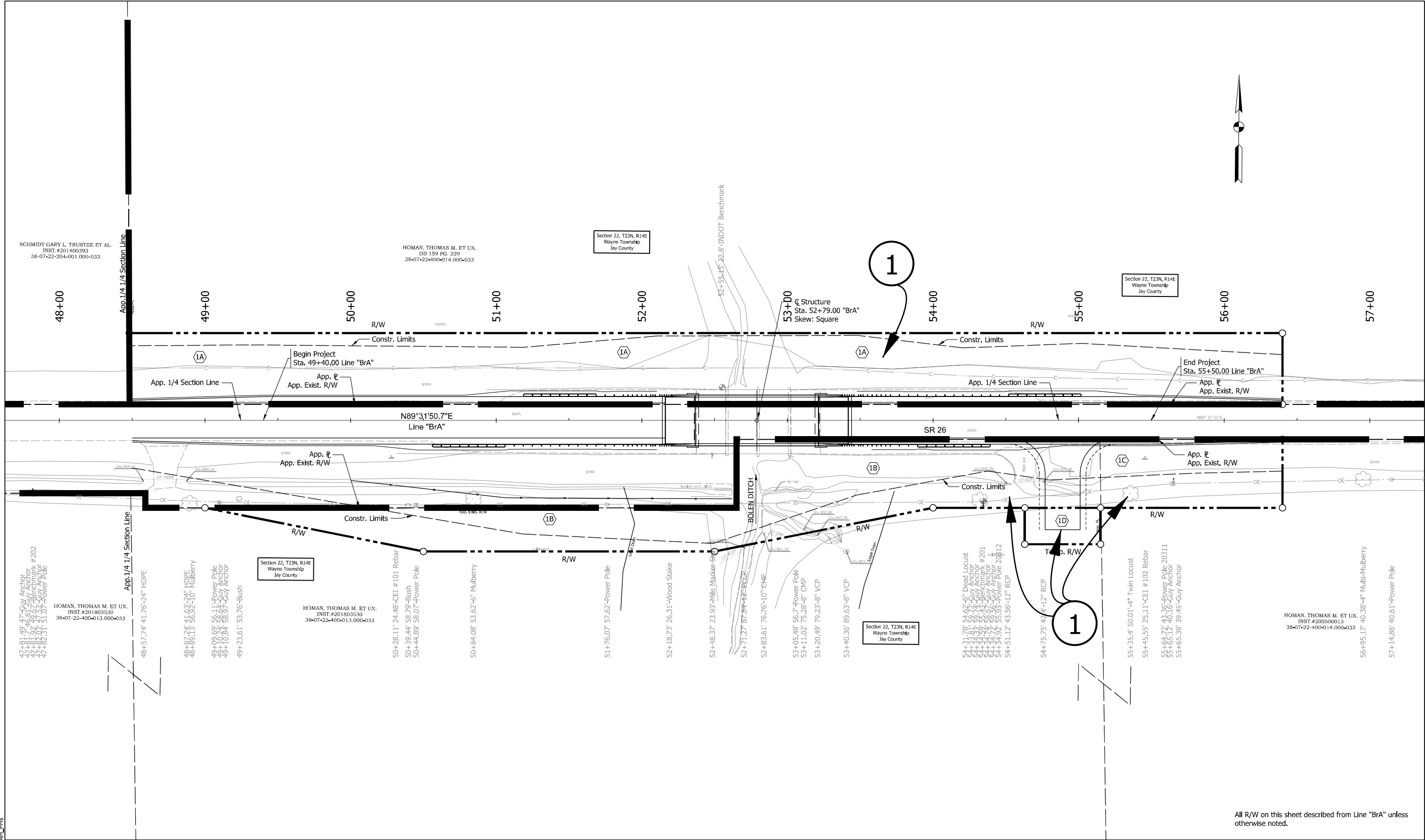
INDIANA DEPARTMENT OF TRANSPORTATION
STANDARD SPECIFICATIONS DATED 2024
TO BE USED WITH THESE PLANS.

PLANS
PREPARED BY: _____ PHONE NUMBER _____
CERTIFIED BY: _____ DATE _____
RECOMMENDED
FOR LETTING: _____ DATE _____
INDIANA DEPARTMENT OF TRANSPORTATION

BRIDGE FILE		
026-38-XXXX		
DESIGNATION		
2200586		
SHEETS		
1	of	20
CONTRACT	PROJECT	
B-44619	2200586	

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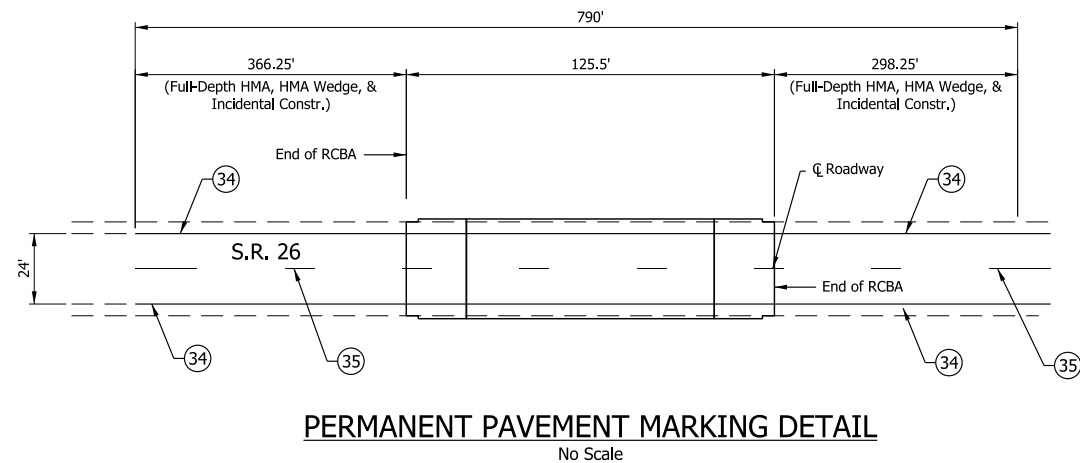
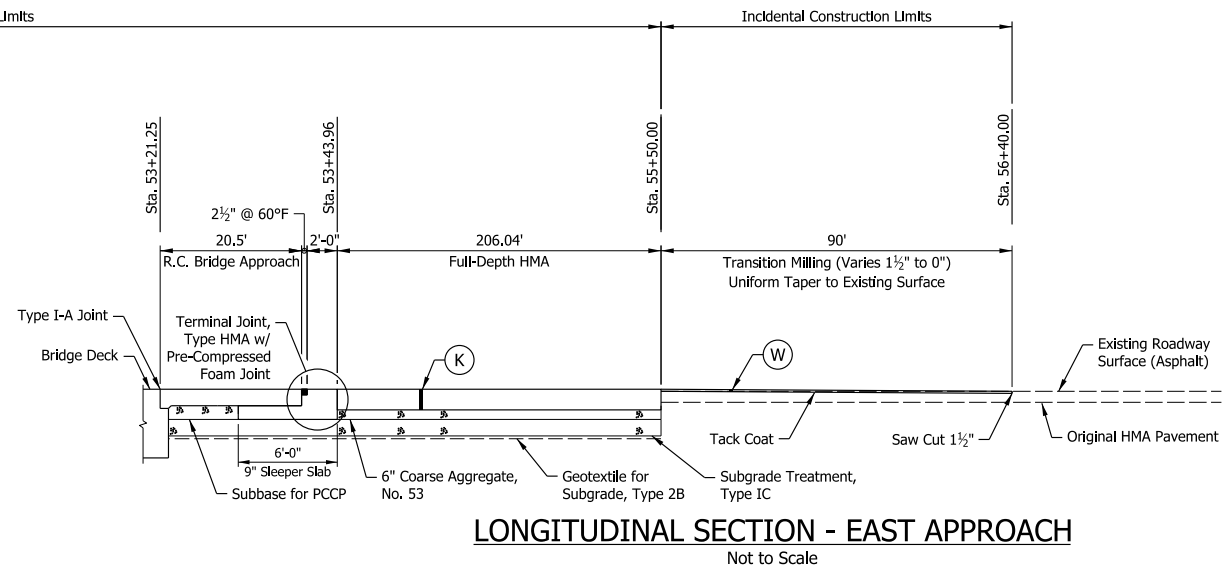
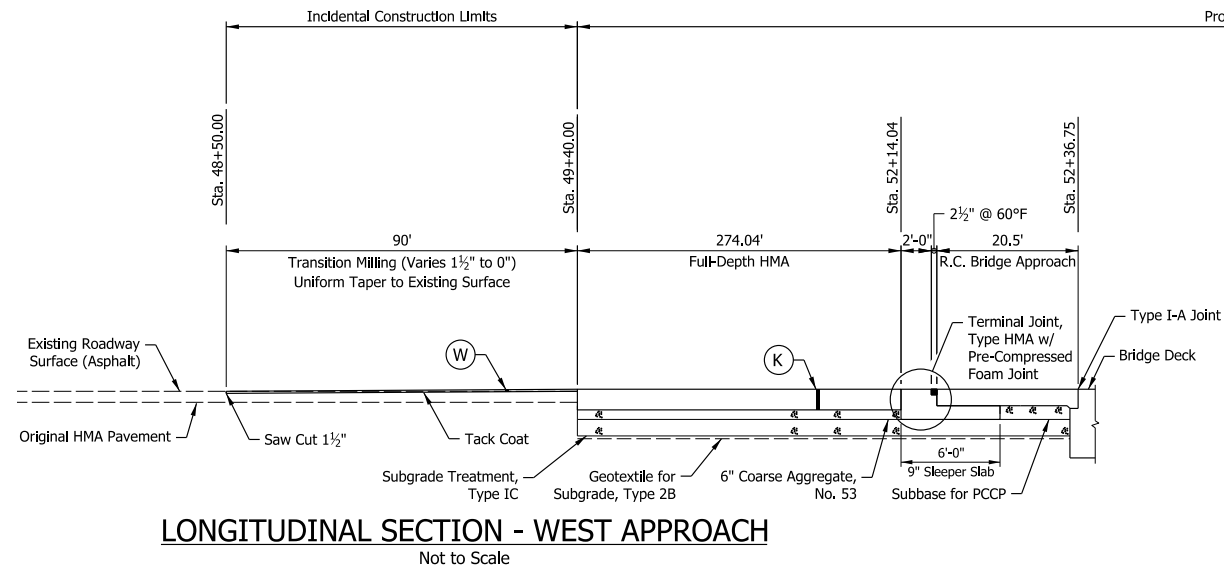
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RECOMMENDED FOR APPROVAL		DESIGN ENGINEER		DATE	INDIANA DEPARTMENT OF TRANSPORTATION		SCALE	BRIDGE FILE
DESIGNED: MCB		05/2024		05/2024	PLAT NO. 1		1" = 30'	026-38-10835
CHECKED: CRF		05/2024		05/2024	LINE "BrA"		SURVEY BOOK	DESIGNATION
							CONTRACT	2200586
							B-44619	2200586

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Model: \$MODEL_NAMES

PROSLOT: LARSEN, TIMES, SAM, PHIS



PAVEMENT MARKING LEGEND & QUANTITIES

- | | | |
|----|-------------------------------------|-----------|
| 34 | Line, Paint, Solid, White, 6 inch | 1580 Lft. |
| 35 | Line, Paint, Broken, Yellow, 6 inch | 198 Lft. |

DATE	REVISION					INDIANA DEPARTMENT OF TRANSPORTATION		SCALE AS NOTED	BRIDGE FILE 026-38-XXXXX
									DESIGNATION 2200586
						ROADWAY DETAILS		SHEETS 4 of 20	
								CONTRACT B-44619	PROJECT 2200586

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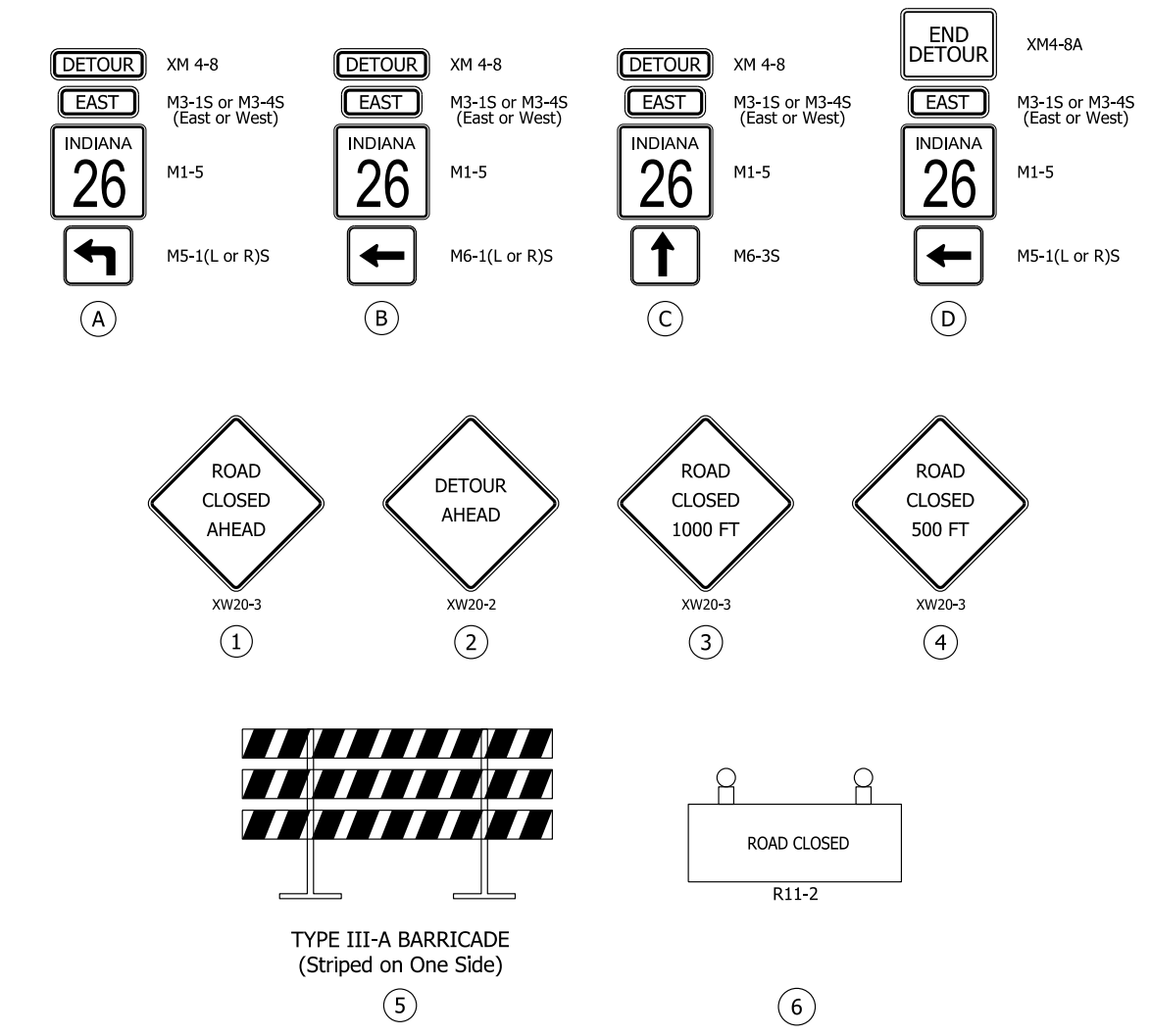
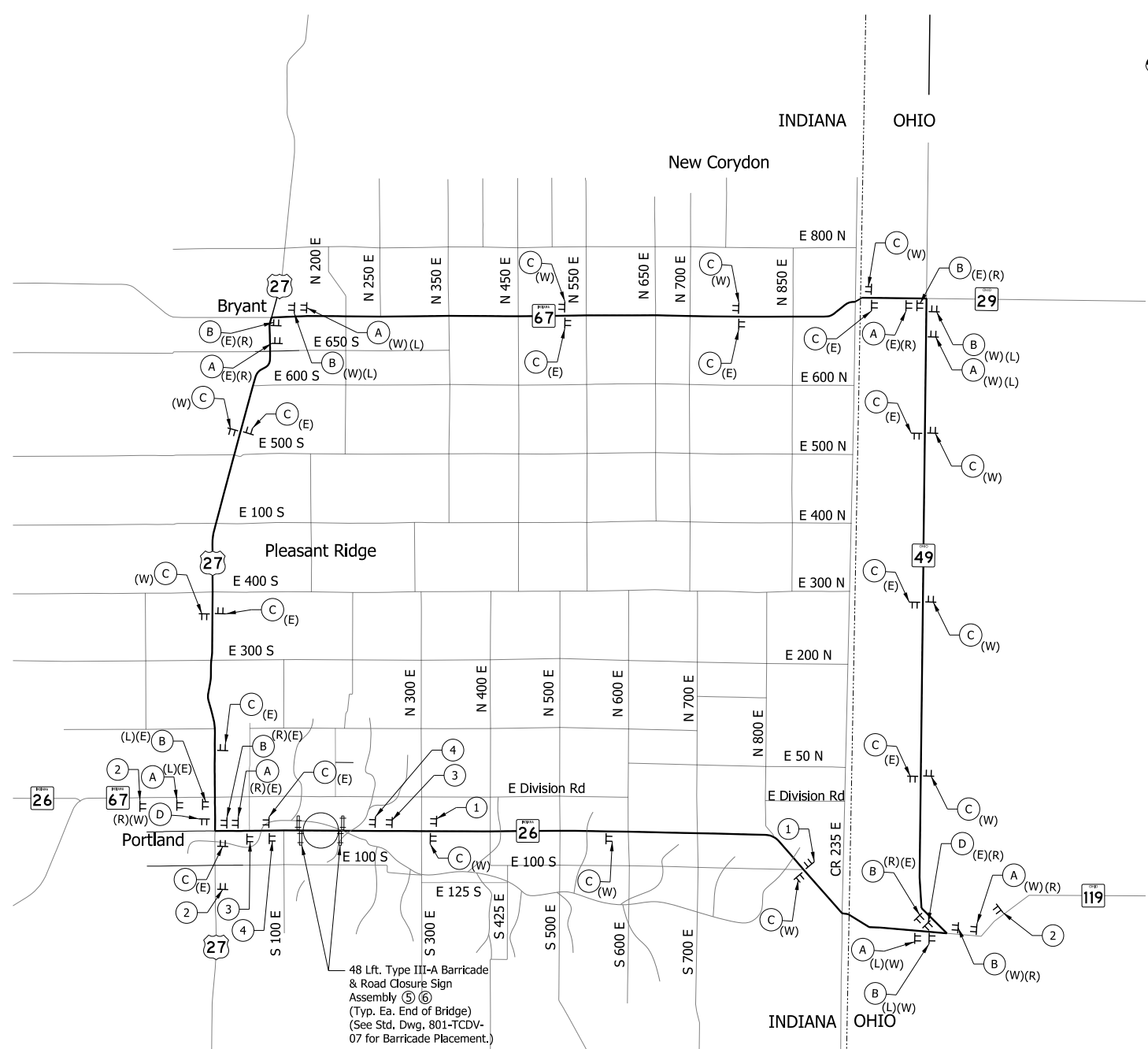
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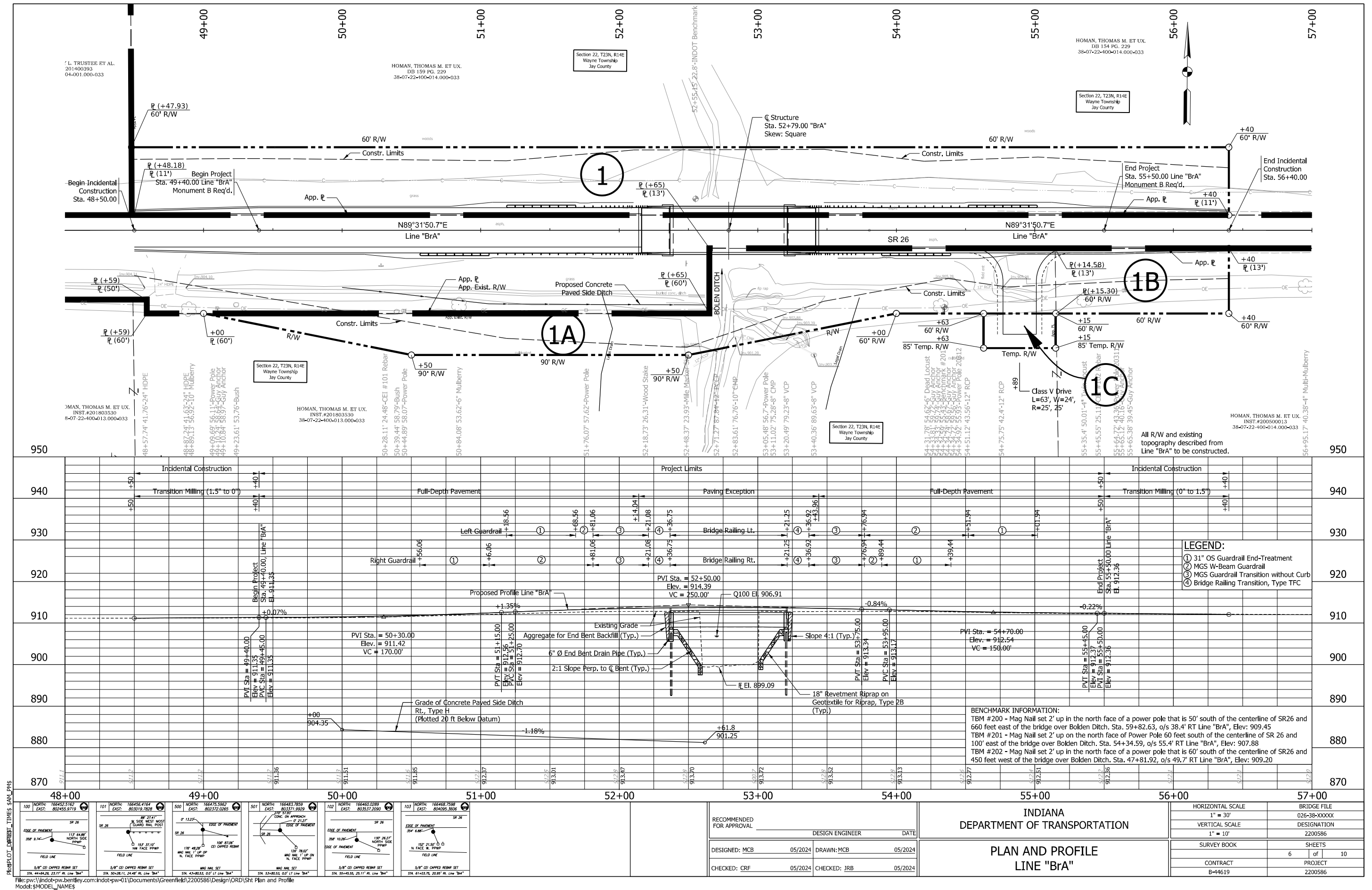
DATE	REVISION

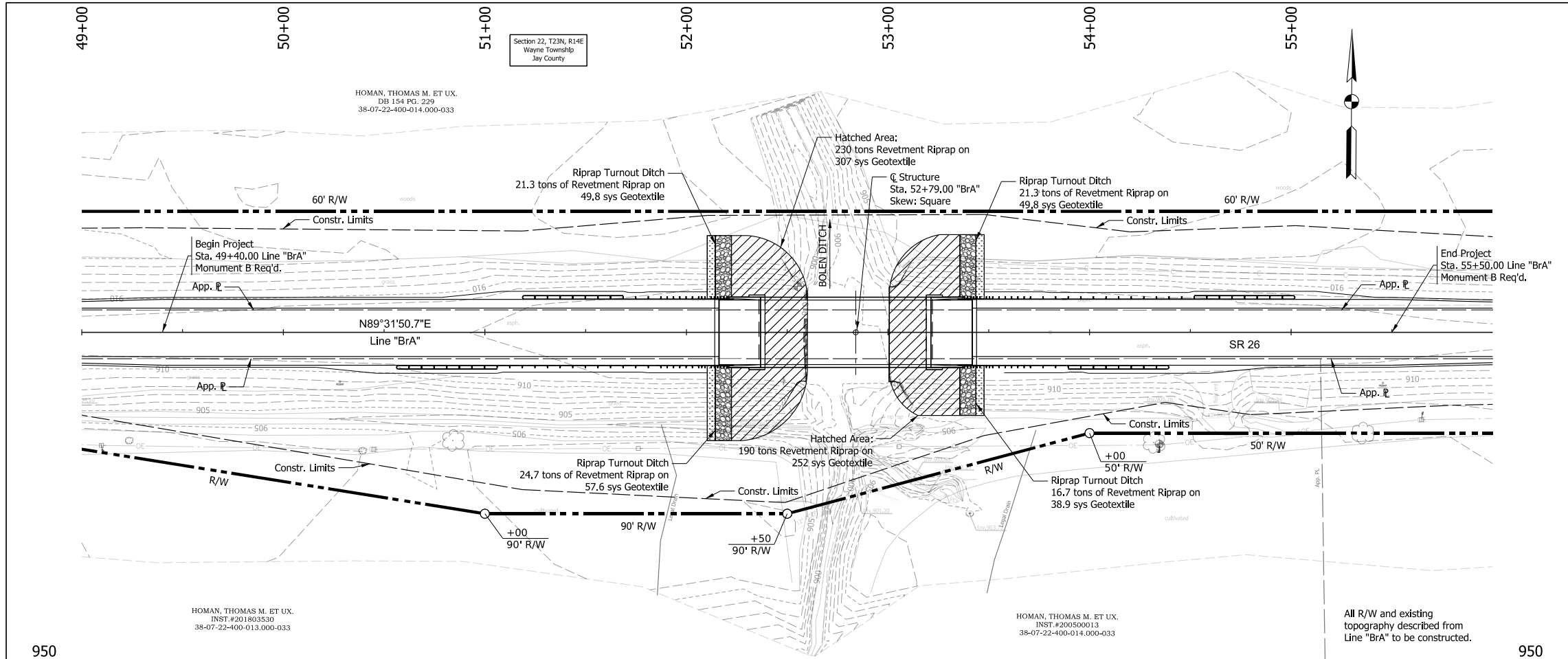
RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: MAS	04/2024	DRAWN: MGA
		04/2024
CHECKED: MCB	05/2024	CHECKED: MCB
		05/2024

INDIANA DEPARTMENT OF TRANSPORTATION

SCALE AS NOTED	BRIDGE FILE 026-38-XXXX
	DESIGNATION 2200586





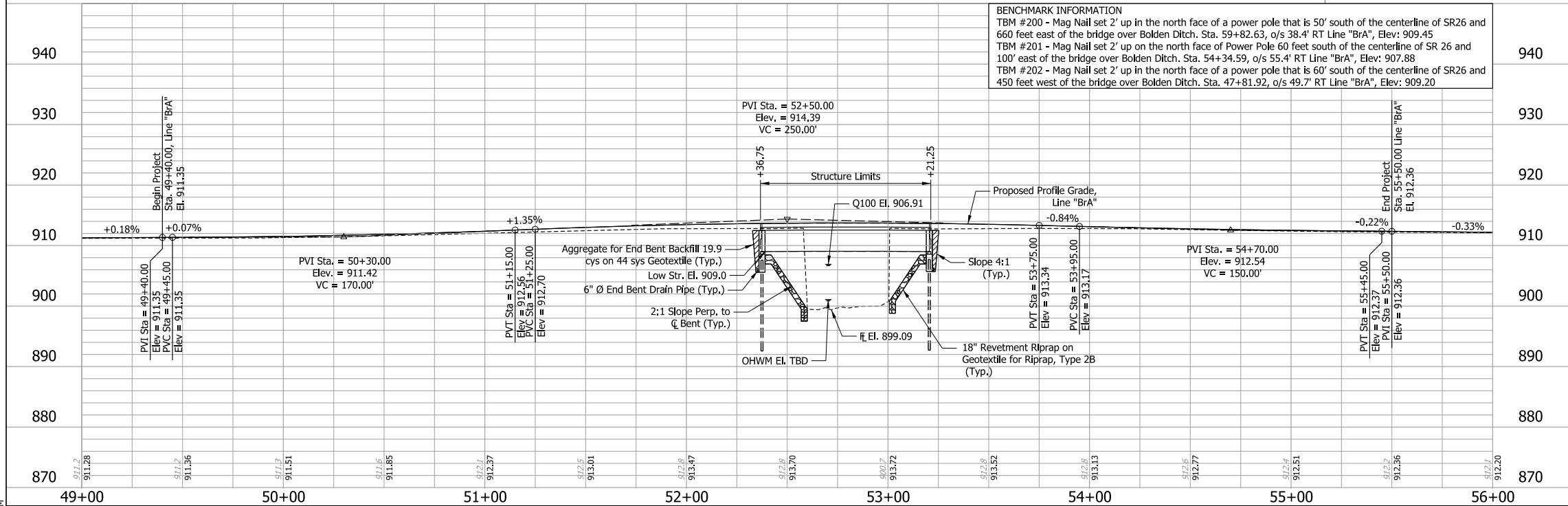


EXISTING STRUCTURE
The existing structure (026-38-03431-A) is a Continuous Reinforced Concrete Slab Bridge (2 @ 21'-7½") with a 44'-0" Clear Roadway. (Entire structure to be removed.)

HYDRAULIC DATA		
Waterway Opening Required	318.4	sq. ft.
Waterway Opening Provided	384	sq. ft.
Drainage Area	1.31	sq. mi.
Q100 Design Discharge	400	cfs.
Q100 Velocity	2.31	ft./sec.
Q100 Elev.	906.08	ft.
Estimated Scour Elev.	897.75	ft.
Backwater at Q100	0.53	ft.
Existing Waterway Opening	250.6	sq. ft.
Existing Backwater	0.56	ft.
Low Structure Elev.	909.0	ft.
Existing Low Structure Elev.	911.35	ft.
Flowline Elevation	899.09	ft.

HYDRAULIC SCOUR DATA		
Q100 Discharge	400	cfs.
Q100 Elevation	906.08	ft.
Q100 Max Velocity	2.71	ft./sec.
Scour Depth (Contraction)	1.34	ft.
Scour Depth (Total)	1.34	ft.
Low Scour Elevation	897.75	ft.
Q500 Discharge	520	cfs.
Q500 Elevation	907.45	ft.
Q500 Max Velocity	2.76	ft./sec.
Scour Depth (Contraction)	2.80	ft.
Scour Depth (Total)	2.80	ft.
Low Scour Elevation	896.29	ft.

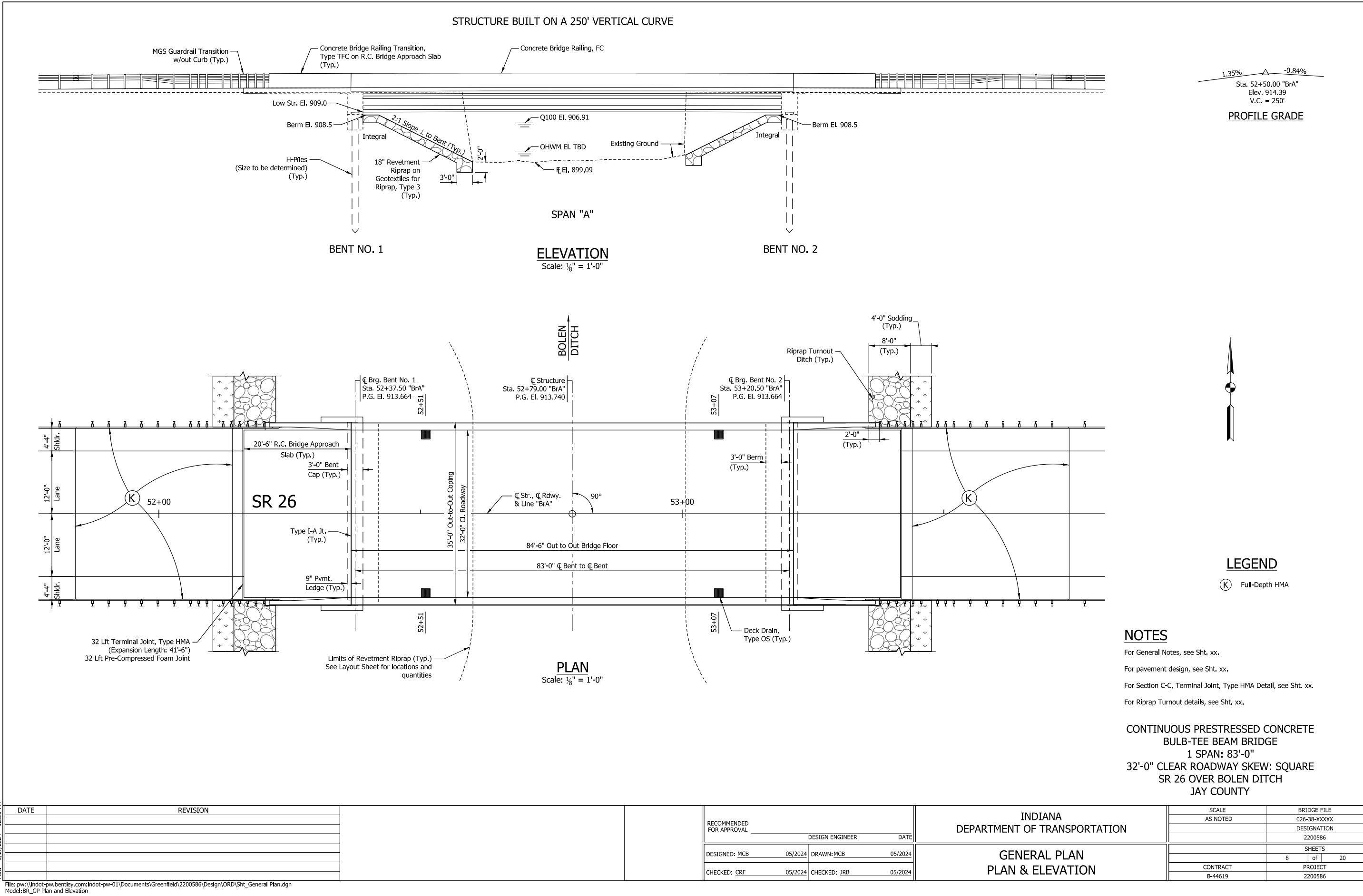
EARTHWORK TABULATION		
Fill + 20%	1,214	Cys.
Common Excavation	2,259	Cys.
Usable Waterway Excavation (70%)	101	Cys.
Surplus Foundation Excavation	535	Cys.
Waste	1,681	Cys.
Total Waterway Excavation	145	Cys.
* Benching (Estimated)	TBD	Cys.
* No direct payment. Benching will not be paid for as Common Excavation.		



**CONTINUOUS PRESTRESSED CONCRETE
BULB-TEE BEAM BRIDGE
1 SPAN: 83'-0"
32'-0" CLEAR ROADWAY SKEW: SQUARE
SR 26 OVER BOLEN DITCH
JAY COUNTY**

INDIANA DEPARTMENT OF TRANSPORTATION				BRIDGE FILE 026-38-XXXXX	
DESIGN ENGINEER				DESIGNATION 2200586	
DESIGNED: MCB 05/2024				SHEETS 7 of 20	
DRAWN: MCB 05/2024				PROJECT 2200586	
CHECKED: JRB 05/2024					
LAYOUT LINE "BrA"					

File: pw:\Indot-pw.bentley.com\Indot-pw-01\Documents\Greenfield\2200586\Design\ORD\ShT Layout
Model: BrA - Plan 1 [Sheet]



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GENERAL NOTES

Plans for the existing structure are on file and available upon request from the Research & Documents Library, Indiana Department of Transportation as; Str. No. 026-38-03431 and 026-38-03431 A.

Reinforcing steel covering shall be $2\frac{1}{2}$ " minimum in the top and 1" minimum in the bottom of the slab and 2" in all other parts unless otherwise noted.

The following surfaces shall be Surface Sealed:
The exposed faces of the concrete bridge railings.
(Estimated Quantity = 1,074 Sft)

All disturbed areas not sodded or covered with riprap will be reseeded using Mulched Seeding, R.

DESIGN DATA

Live Load: Designed for HL-93 Loading in accordance with the AASHTO LRFD Bridge Design Specifications, 9th Edition, 2020.

Dead Load: Designed for actual dead load plus 35 lbs/sft future wearing surface and 15 lbs/sft to permit use of Permanent Metal Deck Forms.

Floor Slab: Designed with a 7½" structural depth and ½" integral wearing course.

Design Stresses:

Concrete, Class A	$f_c = 3,500$ psi
Concrete, Class B	$f_c = 3,000$ psi
Concrete, Class C	$f_c = 4,000$ psi
Reinforcing Steel (Grade 60)	$f_y = 60,000$ psi

Design Stresses:	
Seismic Performance Zone	TBD
Acceleration Coefficient	TBD
Seismic Soil Profile Type	TBD

CONSTRUCTION LOADING

The exterior girder has been checked for strength, deflection, and overturning using the construction loads shown below. Cantilever overhang brackets were assumed for support of the deck overhang past the edge of the exterior girder. The finishing machine was assumed to be supported 6 in. outside the vertical coping form. The top overhang brackets were assumed to be located 6 in. past the edge of the vertical coping form. The bottom overhang brackets were assumed to be braced against the intersection of the girder bottom flange and web.

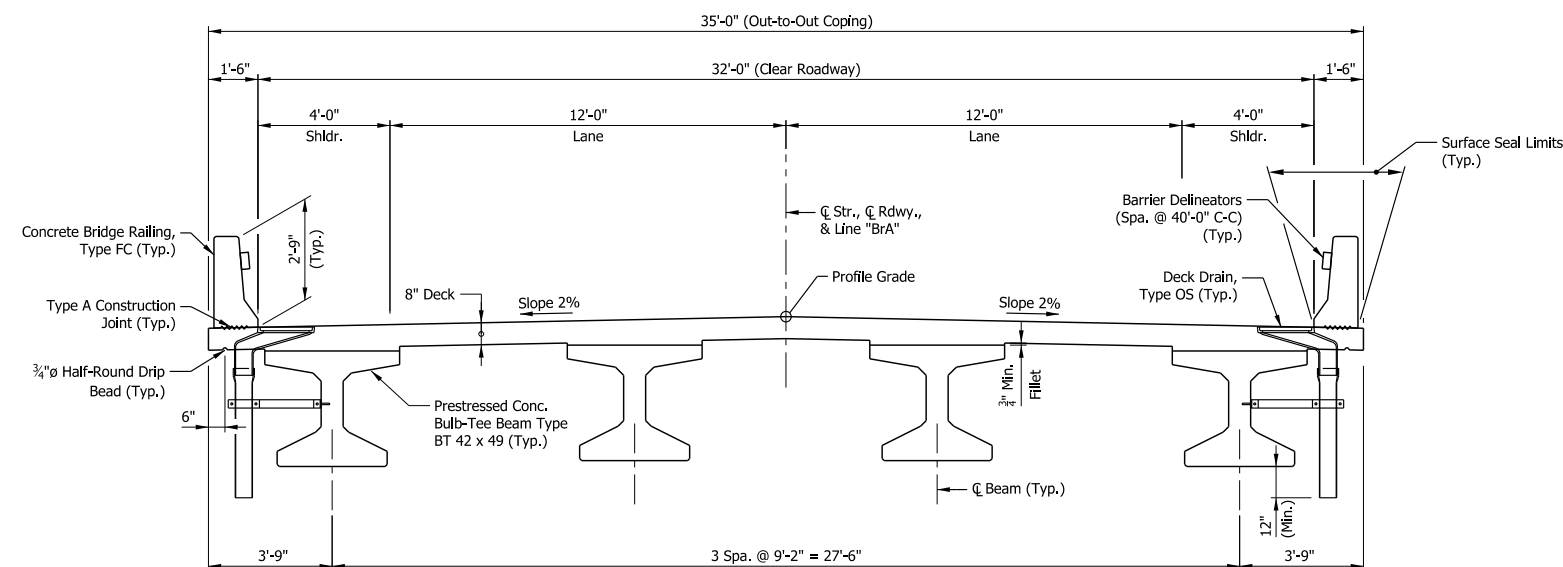
Deck Falsework Loads: Designed for 15 lb/ft² for permanent metal stay-in-place deck forms, removable deck forms, and 2-ft exterior walkway.

Construction Live Load: Designed for 20 lb/ft² extending 2 ft past the edge of coping and 75 lb/ft vertical force applied at a distance of 6 in, outside the face of coping over a 30-ft length of the deck centered with the finishing machine.

Finishing Machine Load: 4500 lb distributed over 10 ft applied 6 in. outside the bridge coping.

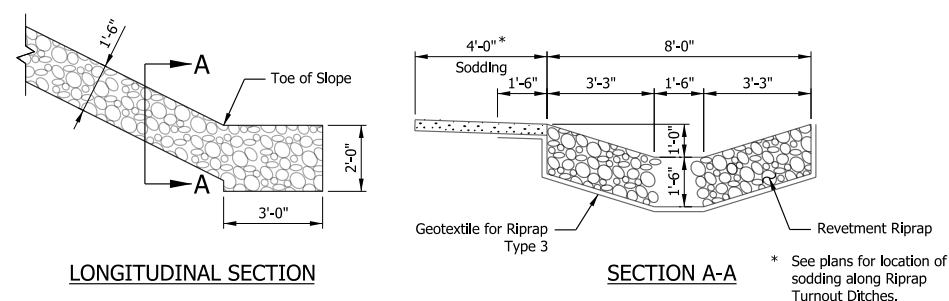
Wind Load: Structure designed for 70 mph horizontal wind loading in accordance with LRFD 3.8.1.

CONTINUOUS PRESTRESSED CONCRETE
BULB-TEE BEAM BRIDGE
1 SPAN: 83'-0"
32'-0" CLEAR ROADWAY SKEW: SQUARE
SR 26 OVER BOLEN DITCH
JAY COUNTY



PROPOSED STRUCTURE SECTION

Scale: $\frac{3}{8}" = 1'-0"$



RIPRAP TURNOUT DITCH DETAIL

Scale: $\frac{3}{8}" = 1'-0"$

[illegible]

Appendix C

Early Coordination



INDIANA DEPARTMENT OF TRANSPORTATION

100 North Senate Avenue
Room N758-ES
Indianapolis, Indiana 46204

PHONE: (317)-694-8284

Eric Holcomb, Governor
Michael Smith, Commissioner

July 17, 2024

Sample Early Coordination Letter

Re: Early Coordination Letter, Des. No. 2200586, State Road (SR) 26 over Race Run, Bridge Replacement, 1.84 Miles East of United States Highway (US) 27, Jay County, IN

Dear Mr. Swiderski:

The Indiana Department of Transportation (INDOT), with federal funding intends to proceed with a project involving the aforementioned bridge replacement of Bridge 026-38-03431 in Jay County, Indiana. This letter is part of the early coordination phase of the environmental review process. We are requesting comments from your area of expertise regarding any possible environmental effects associated with this project. **Please use the above designation numbers and description in your reply.** We will incorporate your comments into a study of the project's environmental impacts.

This project is located on SR 26 over Race Run (also referred to as Bolen Ditch), 1.84 miles east of US 27, near Portland, Jay County, Indiana. This section of SR 26 is a two lane, rural Major Collector, with a speed limit of 55 mph. The existing SR 26 approach cross section consists of two, 12-foot-wide travel lanes (one eastbound, one westbound) bordered by 11-foot paved usable shoulders. The existing bridge section consists of two 12-foot lanes and 10-foot shoulders. Guardrails existing along both sides of the roadway within the project area include a substandard aluminum tube rail with no curb. The existing bridge was built in 1946, with rehabilitations in 1982, and is a 44-foot-long continuous concrete slab bridge. The draft need is evidenced by the deteriorating condition of the bridge. Specific condition ratings noted in the *October 6th, 2023, INDOT Bridge Inspection Report* for Bridge No. 026-38-03431A include a 4 (poor, advanced deterioration) out of 9 (excellent) for the deck, a 5 (fair condition) out of 9 (excellent) for the wearing surface, a 5 (fair condition) out of 9 (excellent) for the approach slabs, a 4 (poor, advanced deterioration) out of 9 (excellent) for the superstructure, and a 5 (fair condition) out of 9 (excellent) for the substructure. Deficiencies for the deck and superstructure include heavy spalling/delamination with exposed rebar at the copings of both spans, widely spaced hairline longitudinal cracks, and delamination at the center line of Span B. Deficiencies noted for the approach slabs include wide random cracks and small spalls in the east bound lane, irregular and wide transverse cracks, and wide longitudinal cracks. Deficiencies noted in the substructure include vertical cracks, efflorescence, patched reconstruction, and horizontal cracking in the abutments; a surface spall at the south construction joint; spall with exposed rebar at the northeast corner; spalling with minor rebar exposure at the southeast wingwall; and light scaling and vertical cracks on pier 2. The draft purpose of this project is to improve the condition ratings of the deck and wearing surface to at least a 7 (good) out of 9 (excellent). The apparent existing right-of-way (ROW) is the edge of pavement.

The proposed project would replace the existing bridge with a four-beam single-span, square, prestressed concrete bulb-tee beam bridge with integral end bents. The beams are bulb-tee (BT) type 42-inches by 49-inches. The proposed span length is 83-feet with 2:1 spillslopes. The proposed bridge section would consist of two 12-foot lanes bordered by 4-foot, 4-inch paved shoulders. The existing guardrail at each bridge corner will be replaced with Midwest Guardrail System (MGS) compliant guardrail. The proposed project will require a greater grade raise of approximately 11 inches. Approximately 0.4 acres (0.2 acres NW quadrant, 0.2 acres NE quadrant) of tree clearing is anticipated as part of this project. The project will require more than 0.5 acre of permanent ROW. Maintenance of Traffic (MOT) is expected to include a full road closure with a detour utilizing official and local routes US 27, SR 67, SR 49, SR 119, Ohio SR 29, Ohio SR 49, and Ohio SR 119. The project is anticipated to begin construction in summer 2026.

Land use in the vicinity of the project is primarily forested habitat surrounded by agricultural land. Burgess & Niple, Inc. will prepare a Waters of the U.S. Report (WOUSR) that will be reviewed by the INDOT Ecology and Waterway Permitting Office. The project is anticipated to qualify for the Range-wide Programmatic Agreement for the Indiana bat (*Myotis sodalis*) and northern long-eared bat (*M. septentrionalis*) by completing the U.S. Fish and Wildlife Service (USFWS) Information for Planning and Consultation (IPaC). Coordination will occur with INDOT Cultural Resources (CRO) to evaluate the project area for archaeological and historic resources and for Section 106 compliance. The results of this investigation will be forwarded to State Historic Preservation Officer (SHPO) for review and concurrence as appropriate.

Please provide your response to Nakayla Krahn within thirty (30) calendar days from the date of this letter. However, should you find an extension to the response time is necessary, a reasonable amount may be granted upon request. If you have any questions regarding this matter, please feel free to contact Nakayla Krahn, Burgess & Niple, Inc, Environmental Scientist, by mail at 251 N. Illinois Street, Suite 920 Indianapolis, Indiana 46204 or email at Nakayla.krahn@burgessniple.com, or Mark Swiderski, INDOT Project Manager, by email at mswiderski@indot.in.gov.

Sincerely,



Environmental Scientist
Burgess & Niple, Inc.

ATTACHMENTS:

Location Map
Topographic Map
Aerial Map
Photographs

Duplicate attachments have been removed to reduce file size and avoid redundancy. Duplicate attachments can be found in Appendix B.

cc: Federal Highway Administration, Greenfield District
Indiana Department of Environmental Management – Groundwater (Wellhead Proximity Website)
Indiana Geological Survey (Electronic Submittal)
Indiana Department of Natural Resources, Division of Fish & Wildlife
US Department of Housing & Urban Development
INDOT, Greenfield District - Environmental Section Manager
INDOT, Greenfield District - Project Manager
Indiana Department of Environmental Management - Wetlands and Stormwater Programs
Natural Resources Conservation Service

East Central IARC
Eighth Coast Guard District
US Army Corps of Engineers, Louisville District
Jay County Highway Department
Jay County Commissioner, Middle District
Jay County EMA
Jay County Surveyor
Jay School Corporation
Jay County Floodplain Administrator

Nakayla Krahn

From: highway <highway@co.jay.in.us>
Sent: Wednesday, July 17, 2024 1:56 PM
To: Nakayla Krahn
Subject: Re: ECL - SR 26 over Race Run (Des No. 2200636)

No concerns on our end as of now.

Thank you,

Robert Howell
Jay County Highway Department
Superintendent
1035 E 200 N
Portland, IN 47371

From: Nakayla Krahn <Nakayla.Krahn@burgessniple.com>
Sent: Wednesday, July 17, 2024 1:44 PM
To: highway <highway@co.jay.in.us>
Subject: ECL - SR 26 over Race Run (Des No. 2200636)

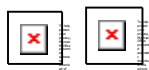
Good afternoon,

Please find the attached Early Coordination Letter (ECL) for the SR 26 over Race Run, Bridge Replacement (Des No. 2200586) located near Portland, Jay County, Indiana. Please review the ECL and supply our office with any comments or concerns you may have regarding the proposed project.

Sincerely,

Nakayla Krahn (she/her)
Environmental Scientist

Burgess & Niple, Inc.
317.237.2760 x7507
Capital Center | 251 N. Illinois Street | Suite 920
Indianapolis, IN 46204
burgessniple.com



Note:
These electronic documents are provided by Burgess & Niple (B&N) as a convenience to our clients. It is our professional opinion that this electronic information provides information current as of the date of its release. Any use of this information is at the sole risk and liability of the user. The user is responsible for updating information to reflect any changes in the information following the preparation date of this transmittal.
The delivery of this information in electronic format is for the benefit of the owner for whom the services have been performed. Nothing in the transfer should be construed to provide any right to third parties to rely on the information provided, or that the use of this information implies the review and approval of Burgess & Niple.

Nakayla Krahn

From: Samantha Rhodehamel <srhodehamel@co.jay.in.us>
Sent: Tuesday, July 30, 2024 11:12 AM
To: Nakayla Krahn
Subject: Re: ECL - SR 26 over Race Run (Des No. 2200636)

Hello,

I have no comments or concerns relating to environmental effects of this project.

Thanks,
Sam

Samantha Rhodehamel | Director

Jay County Emergency Management Agency
P. 260-726-6908 | C. 260-729-2882 |



From: Nakayla Krahn <Nakayla.Krahn@burgessniple.com>
Sent: Wednesday, July 17, 2024 1:47 PM
To: Samantha Rhodehamel <srhodehamel@co.jay.in.us>
Subject: ECL - SR 26 over Race Run (Des No. 2200636)

Good afternoon,

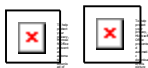
Please find the attached Early Coordination Letter (ECL) for the SR 26 over Race Run, Bridge Replacement (Des No. 2200586) located near Portland, Jay County, Indiana. Please review the ECL and supply our office with any comments or concerns you may have regarding the proposed project.

Sincerely,

Nakayla Krahn (she/her)
Environmental Scientist

Burgess & Niple, Inc.

317.237.2760 x7507
Capital Center | 251 N. Illinois Street | Suite 920
Indianapolis, IN 46204
burgessniple.com



Note:
These electronic documents are provided by Burgess & Niple (B&N) as a convenience to our clients.
It is our professional opinion that this electronic information provides information current as of the date of its release. Any use of this information is at the sole risk and liability of the user. The user is responsible for updating information to reflect any changes in the information following the preparation date of this transmittal.
The delivery of this information in electronic format is for the benefit of the owner for whom the services have been performed. Nothing in the transfer should

**State of Indiana
DEPARTMENT OF NATURAL RESOURCES
Division of Fish and Wildlife
Early Coordination/Environmental Assessment**

DNR#: ER-26697

Request Received: July 17, 2024

Requestor:

Nakayla Krahn
Burgess & Niple, Inc.
251 North Illinois Street, Suite 920
Indianapolis, IN 46204

Project:

SR 26 bridge (#026-38-03431A) replacement over Race Run (Bolen Ditch) with a four-beam single-span, square, prestressed concrete bulb-tee beam bridge with integral end bents, 1.84 miles east of US 27, Portland; Des #2200586

County/Site Info: Jay County

The Indiana Department of Natural Resources has reviewed the above referenced project per your request. Our agency offers the following comments for your information and in accordance with the National Environmental Policy Act of 1969.

If our agency has regulatory jurisdiction over the project, the recommendations contained in this letter may become requirements of any permit issued. If we do not have permitting authority, all recommendations are voluntary.

Regulatory Assessment:

This proposal will require the formal approval of our agency for construction in a floodway, pursuant to the Flood Control Act (IC 14-28-1), unless it qualifies for a bridge exemption (see enclosure) or qualifies under the INDOT and IDNR Memorandum of Understanding for Maintenance Activity Exemption, dated March 2023. Please include a copy of this letter with the permit application, if required.

Natural Heritage Database:

The Natural Heritage Program's data have been checked. To date, no plant or animal species listed as state or federally threatened, endangered, or rare have been reported to occur in the project vicinity.

Fish and Wildlife Comments:

Avoid and minimize impacts to fish, wildlife, and botanical resources to the greatest extent possible, and compensate for impacts. The following are recommendations that address potential impacts identified in the proposed project area:

A) Wildlife Passage

Maintaining or improving fish and wildlife passage at existing and proposed crossings is a priority for the Division of Fish and Wildlife (DFW) to reduce wildlife mortality along roadways. The DFW has outlined different requirements for different types of crossing structure impacts. For crossing replacements, the new structure must include wildlife passage appropriate for the type of replacement structure being proposed. If the existing structure is sized to accommodate white-tailed deer passage, then it should be included in the design of the new structure. If white-tailed deer passage is not possible with the existing structure, deer passage still needs to be considered in the design and at minimum the bank lines must be restored within structures to allow for smaller wildlife passage above the ordinary high-water mark (OHWM). Wildlife passage designs should include

a smooth level pathway preferably 3 feet wide but a minimum of 1-2 feet in width composed of natural substrate (soil, sand, gravel, etc.) or compacted aggregate fill over riprap (#2, #53, #73, etc.) tied into existing elevations both upstream and downstream. The stream crossing repairs or modifications, and any bank stabilization under or around the structure, must not create conditions that are less favorable for wildlife passage when compared to existing conditions. Upgrading wildlife passage for rehabilitated/modified structures is encouraged whenever possible to improve wildlife/vehicle safety.

There are several techniques and materials for incorporating wildlife passage into the design of a crossing structure. Coordination with a Regional Environmental Biologist to address wildlife passage issues before submitting a permit application (if required) is encouraged to avoid delays in the permitting process. The following links are good resources to consider in the design of stream crossing structures to maintain fish and wildlife passage:

<https://www.fs.usda.gov/ccrc/tool/fishxing-fish-passage-learning-systems>
<https://www.fs.usda.gov/wildlifecrossings/library/index.php>
https://www.fhwa.dot.gov/clas/ctip/wildlife_crossing_structures/
<https://www.fhwa.dot.gov/engineering/hydraulics/pubs/11008/hif11008.pdf>

B) Streambank Stabilization

Some form of bank stabilization is almost always needed with the construction, repair, replacement, or modification of a stream channel or crossing structure. For streambank stabilization and erosion control, regrading to a stable slope (2:1 or shallower) and establishing native vegetation along the banks are typically the most effective techniques and allow a vegetated stream bank to develop. A variety of methods to accomplish this include planting plugs, whips, container stock, seeding, and live stakes. In addition to vegetation establishment, some additional level of bioengineered bank stabilization may be needed under certain circumstances (inability to regrade to a stable slope, flow velocities that exceed the limits of vegetation alone, etc.). Combining vegetation with any of the following bank stabilization methods can provide additional bank protection while not compromising benefits to fish, wildlife, and botanical resources:

- Geotextiles (erosion control blankets and/or turf reinforcement mats that are heavy-duty, biodegradable, and net free or that use loose-woven / Leno-woven netting to minimize the entrapment and snaring of small-bodied wildlife such as snakes and turtles)
- Vegetated geogrids or soil lifts, fiber rolls, glacial stone, or riprap.

Riprap or other hard bank stabilization materials should be used only at the toe of the sideslopes up to the OHWM with the exception of areas directly under bridges for instance. The banks above the OHWM should be restored, stabilized, and revegetated using geotextiles and a mixture of grasses, sedges, wildflowers, shrubs, and trees native to Central Indiana and specifically for stream bank/floodway stabilization purposes as soon as possible upon completion. Information about bioengineering techniques can be found at the following link to a USDA/NRCS document that outlines many different bioengineering techniques for streambank stabilization: https://irrigationtoolbox.com/NEH/Part650_EngineeringFieldHandbook/H_210_650_16.pdf.

C) Riparian Habitat

We recommend a mitigation plan be developed (and submitted with the permit application, if required) for any unavoidable habitat impacts that will occur. The DNR's Habitat Mitigation Guidelines (and plant lists) can be found online at: <https://www.in.gov/nrc/files/IB-17.pdf>.

Impacts to non-wetland forest of one (1) acre or more in a rural or urban area should be mitigated at a minimum 2:1 ratio based on area of impact. Impacts to non-wetland forest under one (1) acre but at least 0.10 acre in a rural or urban area should be mitigated at a minimum 1:1 ratio based on area of impact. Impacts under 0.10 acre in a rural area typically do not require mitigation or additional plantings beyond seeding and stabilizing disturbed areas, though there are exceptions for high quality habitat sites. Impacts under 0.10 acre in an urban area should be mitigated by replacing each mature tree removed (trees that are 10" diameter-at-breast height (dbh)) with two trees of 3-gallon stock or larger. Seeding and stabilizing disturbed areas is required regardless of the impact amount and location.

The mitigation site should be located in the floodway, downstream of the one (1) square mile drainage area of that stream (or another stream within the 8-digit HUC, preferably as close to the impact site as possible) and adjacent to existing forested riparian habitat.

The additional measures listed below should be implemented to avoid, minimize, or compensate for impacts to fish, wildlife, and botanical resources:

1. Revegetate all bare and disturbed areas that will not be mowed and maintained with a mixture of grasses, sedges, and wildflowers native to Central Indiana and specifically for stream bank/floodway stabilization purposes as soon as possible upon completion. Turf-type grasses (including low-endophyte, friendly endophyte, and endophyte free tall fescue but excluding all other varieties of tall fescue) may be used in regularly mowed areas only.
2. Minimize and contain within the project limits inchannel disturbance and the clearing of trees and brush.
3. Do not work in the waterway from April 1 through June 30 without the prior written approval of the Division of Fish and Wildlife.
4. Do not cut any trees suitable for Indiana Bat or Northern Long-eared Bat roosting (3 inches or greater diameter-at-breast height, living or dead, with loose hanging bark, or with cracks, crevices, or cavities) from April 1 through September 30.
5. Do not excavate in the low flow area except for the placement of the replacement structure and riprap, or removal of the old structure.
6. Do not construct any temporary runarounds, access bridges, causeways, diversions, or pumparounds.
7. Use minimum average 6-inch graded riprap stone extended below the normal water level to provide habitat for aquatic organisms in the voids.
8. Do not use broken concrete as riprap.
9. Underlay the riprap with a bedding layer of well graded aggregate or a geotextile to prevent piping of soil underneath the riprap.
10. Minimize the movement of resuspended bottom sediment from the immediate project area.
11. Appropriately designed measures for controlling erosion and sediment must be implemented to prevent sediment from entering the waterbody or leaving the construction site; maintain these measures until construction is complete and all disturbed areas are stabilized.
12. Seed and protect all disturbed streambanks and slopes not protected by other methods that are 3:1 or steeper with erosion control blankets that are heavy-duty, biodegradable, and net free or that use loose-woven / Leno-woven netting to minimize the entrapment and snaring of small-bodied wildlife such as snakes and turtles (follow manufacturer's recommendations for selection and installation); seed and apply mulch on all other disturbed areas.
13. Do not excavate or place fill in any riparian wetland.

Contact Staff:

Our agency appreciates this opportunity to be of service. Please contact me at RVanVoorhis@dnr.IN.gov or (317) 232-8163 if we can be of further assistance.

Rachel Van Voorhis
Rachel Van Voorhis
Environmental Coordinator
Division of Fish and Wildlife

Date: August 16, 2024

August 21, 2024

Nakayla Krahn
251 N. Illinois Street, Suite 920
Indianapolis, Indiana 46204

Dear Nakayla Krahn:

The proposed Bridge Replacement Project for State Road (SR) 26 over Race Run, 1.84 Miles East of United States Highway (US) 27 in Jay County, Indiana (Des. No. 2200586), as referred to in your letter received July 17, 2024, will cause a conversion of prime farmland.

The attached packet of information is for your use competing Parts VI and VII of the AD-1006. After completion, the federal funding agency needs to forward one copy to NRCS for our records.

If you need additional information, please contact John Allen at 317-295-5859 or john.allen@usda.gov.

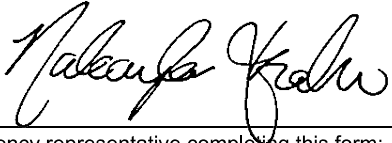
Sincerely,

JOHN ALLEN

JOHN ALLEN
State Soil Scientist

Digitally signed by JOHN ALLEN
Date: 2024.08.21 14:44:40 -04'00'

FARMLAND CONVERSION IMPACT RATING

PART I (To be completed by Federal Agency)		Date Of Land Evaluation Request 08/23/2024				
Name of Project DES2200636 SR26 over Race Run		Federal Agency Involved FHWA				
Proposed Land Use Transportation		County and State Jay County, IN				
PART II (To be completed by NRCS)		Date Request Received By NRCS 8/21/2024		Person Completing Form: JRA		
Does the site contain Prime, Unique, Statewide or Local Important Farmland? (If no, the FPPA does not apply - do not complete additional parts of this form)		YES <input checked="" type="checkbox"/> NO <input type="checkbox"/>		Acres Irrigated 270 ac		
Major Crop(s) Corn	Farmable Land In Govt. Jurisdiction Acres: 242136 % 99		Amount of Farmland As Defined in FPPA Acres: 22607 % 92			
Name of Land Evaluation System Used LESA	Name of State or Local Site Assessment System		Date Land Evaluation Returned by NRCS			
PART III (To be completed by Federal Agency)		Alternative Site Rating				
		Site A	Site B	Site C	Site D	
A. Total Acres To Be Converted Directly		0				
B. Total Acres To Be Converted Indirectly		0.25				
C. Total Acres In Site		0.25				
PART IV (To be completed by NRCS) Land Evaluation Information						
A. Total Acres Prime And Unique Farmland		1.98				
B. Total Acres Statewide Important or Local Important Farmland		0.00				
C. Percentage Of Farmland in County Or Local Govt. Unit To Be Converted		<0.001				
D. Percentage Of Farmland in Govt. Jurisdiction With Same Or Higher Relative Value		25				
PART V (To be completed by NRCS) Land Evaluation Criterion Relative Value of Farmland To Be Converted (Scale of 0 to 100 Points)		92				
PART VI (To be completed by Federal Agency) Site Assessment Criteria (Criteria are explained in 7 CFR 658.5 b. For Corridor project use form NRCS-CPA-106)		Maximum Points	Site A	Site B	Site C	Site D
1. Area In Non-urban Use	(15)	14				
2. Perimeter In Non-urban Use	(10)	10				
3. Percent Of Site Being Farmed	(20)	0				
4. Protection Provided By State and Local Government	(20)	0				
5. Distance From Urban Built-up Area	(15)	15				
6. Distance To Urban Support Services	(15)	10				
7. Size Of Present Farm Unit Compared To Average	(10)	0				
8. Creation Of Non-farmable Farmland	(10)	0				
9. Availability Of Farm Support Services	(5)	3				
10. On-Farm Investments	(20)	6				
11. Effects Of Conversion On Farm Support Services	(10)	0				
12. Compatibility With Existing Agricultural Use	(10)	0				
TOTAL SITE ASSESSMENT POINTS		160	58	0	0	0
PART VII (To be completed by Federal Agency)						
Relative Value Of Farmland (From Part V)		100	92	0	0	0
Total Site Assessment (From Part VI above or local site assessment)		160	58	0	0	0
TOTAL POINTS (Total of above 2 lines)		260	150	0	0	0
Site Selected:	Date Of Selection		Was A Local Site Assessment Used? YES <input type="checkbox"/> NO <input checked="" type="checkbox"/>			
Reason For Selection: 						
Name of Federal agency representative completing this form: Nakayla Krahn					Date: 03/06/2025	

(See Instructions on reverse side)

Form AD-1006 (03-02)



Organization and Project Information

Organization Name: Burgess & Niple, Inc.

First Name: Nakayla

Last Name: Krahn

Phone: (317) 550-6928

Email: nakayla.krahn@burgessniple.com

Address Line 1: 251 N. Illinois Street, Suite 920

City: Indianapolis

State: IN

Zip: 46204

Project ID: 61251, 10

Project Title: SR 26 over Race Run, Bridge Replacement

Destination ID: 2200586

Project Description: The proposed project would replace the existing bridge with a four-beam single-span, square, prestressed concrete bulb-tee beam bridge with integral end bents. The beams are bulb-tee (BT) type 42-inches by 49-inches. The proposed span length is 83-feet with 2:1 spillslopes. The proposed bridge section would consist of two 12-foot lanes bordered by 4-foot, 4-inch paved shoulders. The existing guard

Environmental Assessment Report

Geological Hazards:

1. Floodway
2. Moderate liquefaction potential

Mineral Resources:

1. Bedrock Resource: High Potential
2. Sand and Gravel Resource: Low Potential

Disclaimer:

This document was compiled by Indiana University, Indiana Geological Survey, using data believed to be accurate; however, a degree of error is inherent in all data. This product is distributed "AS-IS" without warranties of any kind, either expressed or implied, including but not limited to warranties of suitability to a particular purpose or use. No attempt has been made in either the design or production of these data and document to define the limits or jurisdiction of any federal, state, or local government. The data used to assemble this document are intended for use only at the published scale of the source data or smaller (see the metadata links below) and are for reference purposes only. They are not to be construed as a legal document or survey instrument. A detailed on-the-ground survey and historical analysis of a single site may differ from these data and this document.

This information was furnished by Indiana Geological Survey

Address: 1001 E. 10th St., Bloomington, IN 47405

Email: IGSEnvir@indiana.edu

Phone: (812) 855-7428

Nakayla Krahn

From: Dial, Marcus <MDial@indot.IN.gov>
Sent: Monday, March 3, 2025 1:29 PM
To: Nakayla Krahn
Cc: Nieman, Cora
Subject: RE: ECL - SR 26 over Race Run (Des No. 2200636)

Nakayla,

Thanks for sending in the early coordination project east of Portland in Jay County. After review, it looks like the structure is roughly 12,700 feet southeast of Portland Municipal Airport. The elevation at the project sight is slightly lower than the at the airport as well. If no construction equipment is over 125 feet in height, there shouldn't be a need for a tall structure permit.

Thanks,

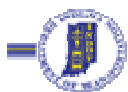
Marcus A. Dial

State Aviation Director

Indiana Department of Transportation
100 North Senate Ave., N758 — Multimodal
Indianapolis, IN 46204

Cell: 317-407-9511

Email: mdial@indot.in.gov



From: Nieman, Cora <cnieman@indot.IN.gov>
Sent: Monday, March 3, 2025 12:19 PM
To: Dial, Marcus <MDial@indot.IN.gov>
Subject: FW: ECL - SR 26 over Race Run (Des No. 2200636)

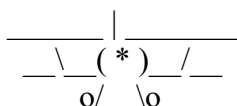
Cora Nieman

Aviation Specialist

Office of Aviation
100 N. Senate Ave. Rm N758-MM
Indianapolis, IN 46204

Email: cnieman@indot.in.gov

Work: 317-234-0795



From: Nakayla Krahn <Nakayla.Krahn@burgessniple.com>
Sent: Monday, March 3, 2025 10:54 AM
To: Nieman, Cora <cnieman@indot.IN.gov>
Subject: ECL - SR 26 over Race Run (Des No. 2200636)



United States Department of the Interior

FISH AND WILDLIFE SERVICE
Indiana Ecological Services Field Office
620 South Walker Street
Bloomington, IN 47403-2121
Phone: (812) 334-4261 Fax: (812) 334-4273



In Reply Refer To:

02/05/2025 18:19:04 UTC

Project Code: 2024-0118103

Project Name: SR 26 over Race Run Bridge Replacement (Des No. 2200586)

Subject: List of threatened and endangered species that may occur in your proposed project location or may be affected by your proposed project

To Whom It May Concern:

The enclosed species list identifies threatened, endangered, proposed and candidate species, as well as proposed and final designated critical habitat, that may occur within the boundary of your proposed project and/or may be affected by your proposed project. The species list fulfills the requirements of the U.S. Fish and Wildlife Service (Service) under section 7(c) of the Endangered Species Act (Act) of 1973, as amended (16 U.S.C. 1531 *et seq.*).

New information based on updated surveys, changes in the abundance and distribution of species, changed habitat conditions, or other factors could change this list. Please feel free to contact us if you need more current information or assistance regarding the potential impacts to federally proposed, listed, and candidate species and federally designated and proposed critical habitat. Please note that under 50 CFR 402.12(e) of the regulations implementing section 7 of the Act, the accuracy of this species list should be verified after 90 days. This verification can be completed formally or informally as desired. The Service recommends that verification be completed by visiting the IPaC website at regular intervals during project planning and implementation for updates to species lists and information. An updated list may be requested through the IPaC system by completing the same process used to receive the enclosed list.

The purpose of the Act is to provide a means whereby threatened and endangered species and the ecosystems upon which they depend may be conserved. Under sections 7(a)(1) and 7(a)(2) of the Act and its implementing regulations (50 CFR 402 *et seq.*), Federal agencies are required to utilize their authorities to carry out programs for the conservation of threatened and endangered species and to determine whether projects may affect threatened and endangered species and/or designated critical habitat.

Please use the species list provided and visit the U.S. Fish and Wildlife Service's Region 3 Section 7 Technical Assistance website at - <http://www.fws.gov/midwest/endangered/section7/s7process/index.html>. This website contains step-by-step instructions which will help you

determine if your project will have an adverse effect on listed species and will help lead you through the Section 7 process. For all **wind energy projects and projects that include installing towers that use guy wires or are over 200 feet in height**, please contact this field office directly for assistance, even if no federally listed plants, animals or critical habitat are present within your proposed project or may be affected by your proposed project.

A Biological Assessment is required for construction projects (or other undertakings having similar physical impacts) that are major Federal actions significantly affecting the quality of the human environment as defined in the National Environmental Policy Act (42 U.S.C. 4332(2)(c)). For projects other than major construction activities, the Service suggests that a biological evaluation similar to a Biological Assessment be prepared to determine whether the project may affect listed or proposed species and/or designated or proposed critical habitat. Recommended contents of a Biological Assessment are described at 50 CFR 402.12.

If a Federal agency determines, based on the Biological Assessment or biological evaluation, that listed species and/or designated critical habitat may be affected by the proposed project, the agency is required to consult with the Service pursuant to 50 CFR 402. In addition, the Service recommends that candidate species, proposed species and proposed critical habitat be addressed within the consultation. More information on the regulations and procedures for section 7 consultation, including the role of permit or license applicants, can be found in the "Endangered Species Consultation Handbook" at:

<https://www.fws.gov/sites/default/files/documents/endangered-species-consultation-handbook.pdf>

Migratory Birds: In addition to responsibilities to protect threatened and endangered species under the Endangered Species Act (ESA), there are additional responsibilities under the Migratory Bird Treaty Act (MBTA) and the Bald and Golden Eagle Protection Act (BGEPA) to protect native birds from project-related impacts. Any activity, intentional or unintentional, resulting in take of migratory birds, including eagles, is prohibited unless otherwise permitted by the U.S. Fish and Wildlife Service (50 C.F.R. Sec. 10.12 and 16 U.S.C. Sec. 668(a)). For more information regarding these Acts, see <https://www.fws.gov/program/migratory-bird-permit/what-we-do>.

The MBTA has no provision for allowing take of migratory birds that may be unintentionally killed or injured by otherwise lawful activities. It is the responsibility of the project proponent to comply with these Acts by identifying potential impacts to migratory birds and eagles within applicable NEPA documents (when there is a federal nexus) or a Bird/Eagle Conservation Plan (when there is no federal nexus). Proponents should implement conservation measures to avoid or minimize the production of project-related stressors or minimize the exposure of birds and their resources to the project-related stressors. For more information on avian stressors and recommended conservation measures, see <https://www.fws.gov/library/collections/threats-birds>.

In addition to MBTA and BGEPA, Executive Order 13186: *Responsibilities of Federal Agencies to Protect Migratory Birds*, obligates all Federal agencies that engage in or authorize activities that might affect migratory birds, to minimize those effects and encourage conservation measures that will improve bird populations. Executive Order 13186 provides for the protection of both migratory birds and migratory bird habitat. For information regarding the implementation of

Executive Order 13186, please visit <https://www.fws.gov/partner/council-conservation-migratory-birds>.

We appreciate your concern for threatened and endangered species. The Service encourages Federal agencies to include conservation of threatened and endangered species into their project planning to further the purposes of the Act. **Please include the Consultation Code in the header of this letter with any request for consultation or correspondence about your project that you submit to our office.**

Attachment(s):

- Official Species List
- Bald & Golden Eagles
- Migratory Birds
- Wetlands

OFFICIAL SPECIES LIST

This list is provided pursuant to Section 7 of the Endangered Species Act, and fulfills the requirement for Federal agencies to "request of the Secretary of the Interior information whether any species which is listed or proposed to be listed may be present in the area of a proposed action".

This species list is provided by:

Indiana Ecological Services Field Office

620 South Walker Street
Bloomington, IN 47403-2121
(812) 334-4261

PROJECT SUMMARY

Project Code: 2024-0118103
Project Name: SR 26 over Race Run Bridge Replacement (Des No. 2200586)
Project Type: Bridge - Replacement
Project Description: This project is located on SR 26 over Race Run (also referred to as Bolen Ditch), 1.84 miles east of US 27, near Portland, Jay County, Indiana. This section of SR 26 is a two lane, rural Major Collector, with a speed limit of 55 mph. The existing SR 26 approach cross section consists of two, 12-foot-wide travel lanes (one eastbound, one westbound) bordered by 11-foot paved usable shoulders. The existing bridge section consists of two 12-foot lanes and 10-foot shoulders. Guardrails existing along both sides of the roadway within the project area include a substandard aluminum tube rail with no curb. The existing bridge was built in 1946, with rehabilitations in 1982, and is a 44-foot-long continuous concrete slab bridge. The draft need is evidenced by the deteriorating condition of the bridge. Specific condition ratings noted in the October 6th, 2023, INDOT Bridge Inspection Report for Bridge No. 026-38-03431A (NBI 007050) include a 4 (poor, advanced deterioration) out of 9 (excellent) for the deck, a 5 (fair condition) out of 9 (excellent) for the wearing surface, a 5 (fair condition) out of 9 (excellent) for the approach slabs, a 4 (poor, advanced deterioration) out of 9 (excellent) for the superstructure, and a 5 (fair condition) out of 9 (excellent) for the substructure. Deficiencies for the deck and superstructure include heavy spalling/delamination with exposed rebar at the copings of both spans, widely spaced hairline longitudinal cracks, and delamination at the center line of Span B. Deficiencies noted for the approach slabs include wide random cracks and small spalls in the east bound lane, irregular and wide transverse cracks, and wide longitudinal cracks. Deficiencies noted in the substructure include vertical cracks, efflorescence, patched reconstruction, and horizontal cracking in the abutments; a surface spall at the south construction joint; spall with exposed rebar at the northeast corner; spalling with minor rebar exposure at the southeast wingwall; and light scaling and vertical cracks on pier 2. The draft purpose of this project is to improve the condition ratings of the deck and wearing surface to at least a 7 (good) out of 9 (excellent). The apparent existing right-of-way (ROW) is the edge of pavement.

The proposed project would replace the existing bridge with a four-beam single-span, square, prestressed concrete bulb-tee beam bridge with integral end bents. The beams are bulb-tee (BT) type 42-inches by 49-inches. The proposed span length is 83-feet with 2:1 spillslopes. The proposed bridge section would consist of two 12-foot lanes bordered by 4-foot, 4-inch paved shoulders. The existing guardrail at each bridge corner will be replaced with Midwest Guardrail System (MGS) compliant

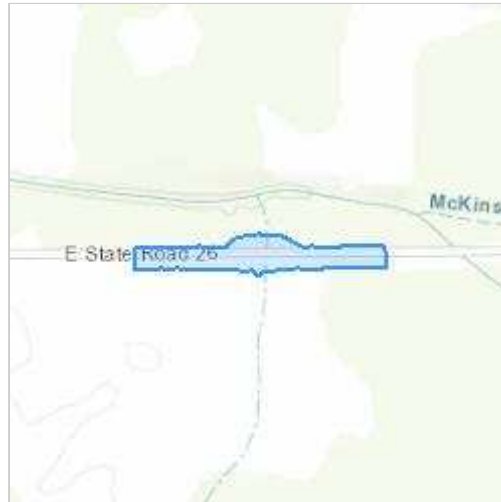
guardrail. The proposed project will require a greater grade raise of approximately 11 inches. Approximately 0.4 acres (0.2 acres NW quadrant, 0.2 acres NE quadrant) of tree clearing is anticipated as part of this project. The project will require more than 0.5 acre of permanent ROW. Maintenance of Traffic (MOT) is expected to include a full road closure with a detour utilizing official and local routes US 27, SR 67, SR 49, SR 119, Ohio SR 29, Ohio SR 49, and Ohio SR 119.

Some suitable bat habitat is within and adjacent to the project area. A review of the USFWS database conducted on March 21, 2024, did not indicate the presence of endangered bat species in or within half mile of the project area. The October 26, 2023, INDOT Bridge Inspection and the May 29, 2024, bat assessment states there was no evidence of bats using the bridge.

Construction is anticipated to occur in Spring 2026. Temporary lighting may be used during construction, but all lighting will be directed away from potential bat roosts. No permanent lighting will be added or changed.

Project Location:

The approximate location of the project can be viewed in Google Maps: <https://www.google.com/maps/@40.43271215,-84.94297375806245,14z>



Counties: Jay County, Indiana

ENDANGERED SPECIES ACT SPECIES

There is a total of 3 threatened, endangered, or candidate species on this species list.

Species on this list should be considered in an effects analysis for your project and could include species that exist in another geographic area. For example, certain fish may appear on the species list because a project could affect downstream species.

IPaC does not display listed species or critical habitats under the sole jurisdiction of NOAA Fisheries¹, as USFWS does not have the authority to speak on behalf of NOAA and the Department of Commerce.

See the "Critical habitats" section below for those critical habitats that lie wholly or partially within your project area under this office's jurisdiction. Please contact the designated FWS office if you have questions.

-
1. [NOAA Fisheries](#), also known as the National Marine Fisheries Service (NMFS), is an office of the National Oceanic and Atmospheric Administration within the Department of Commerce.

MAMMALS

NAME	STATUS
Indiana Bat <i>Myotis sodalis</i> There is final critical habitat for this species. Your location does not overlap the critical habitat. Species profile: https://ecos.fws.gov/ecp/species/5949	Endangered

BIRDS

NAME	STATUS
Whooping Crane <i>Grus americana</i> Population: U.S.A. (AL, AR, CO, FL, GA, ID, IL, IN, IA, KY, LA, MI, MN, MS, MO, NC, NM, OH, SC, TN, UT, VA, WI, WV, western half of WY) No critical habitat has been designated for this species. Species profile: https://ecos.fws.gov/ecp/species/758	Experimental Population, Non- Essential

INSECTS

NAME	STATUS
Monarch Butterfly <i>Danaus plexippus</i> There is proposed critical habitat for this species. Your location does not overlap the critical habitat. Species profile: https://ecos.fws.gov/ecp/species/9743	Proposed Threatened

CRITICAL HABITATS

THERE ARE NO CRITICAL HABITATS WITHIN YOUR PROJECT AREA UNDER THIS OFFICE'S JURISDICTION.

YOU ARE STILL REQUIRED TO DETERMINE IF YOUR PROJECT(S) MAY HAVE EFFECTS ON ALL ABOVE LISTED SPECIES.

BALD & GOLDEN EAGLES

Bald and Golden Eagles are protected under the Bald and Golden Eagle Protection Act ² and the Migratory Bird Treaty Act (MBTA) ¹. Any person or organization who plans or conducts activities that may result in impacts to Bald or Golden Eagles, or their habitats, should follow appropriate regulations and consider implementing appropriate avoidance and minimization measures, as described in the various links on this page.

1. The [Bald and Golden Eagle Protection Act](#) of 1940.
2. The [Migratory Birds Treaty Act](#) of 1918.
3. 50 C.F.R. Sec. 10.12 and 16 U.S.C. Sec. 668(a)

THERE ARE NO BALD AND GOLDEN EAGLES WITHIN THE VICINITY OF YOUR PROJECT AREA.

MIGRATORY BIRDS

The Migratory Bird Treaty Act (MBTA) ¹ prohibits the take (including killing, capturing, selling, trading, and transport) of protected migratory bird species without prior authorization by the Department of Interior U.S. Fish and Wildlife Service (Service). The incidental take of migratory birds is the injury or death of birds that results from, but is not the purpose, of an activity. The Service interprets the MBTA to prohibit incidental take.

1.

The [Migratory Birds Treaty Act](#) of 1918.
2.

The [Bald and Golden Eagle Protection Act](#) of 1940.
3.

50 C.F.R. Sec. 10.12 and 16 U.S.C. Sec. 668(a)

For guidance on when to schedule activities or implement avoidance and minimization measures to reduce impacts to migratory birds on your list, see the "Probability of Presence Summary" below to see when these birds are most likely to be present and breeding in your project area.

NAME	BREEDING SEASON
<div>Chimney Swift <i>Chaetura pelagica</i></div> <div>This is a Bird of Conservation Concern (BCC) throughout its range in the continental USA and Alaska.</div> <div>https://ecos.fws.gov/ecp/species/9406</div>	Breeds Mar 15 to Aug 25
<div>Lesser Yellowlegs <i>Tringa flavipes</i></div> <div>This is a Bird of Conservation Concern (BCC) throughout its range in the continental USA and Alaska.</div> <div>https://ecos.fws.gov/ecp/species/9679</div>	Breeds elsewhere

PROBABILITY OF PRESENCE SUMMARY

The graphs below provide our best understanding of when birds of concern are most likely to be present in your project area. This information can be used to tailor and schedule your project activities to avoid or minimize impacts to birds. Please make sure you read ["Supplemental Information on Migratory Birds and Eagles"](#), specifically the FAQ section titled "Proper Interpretation and Use of Your Migratory Bird Report" before using or attempting to interpret this report.

Probability of Presence (■)

Green bars; the bird's relative probability of presence in the 10km grid cell(s) your project overlaps during that week of the year.

Breeding Season (■)

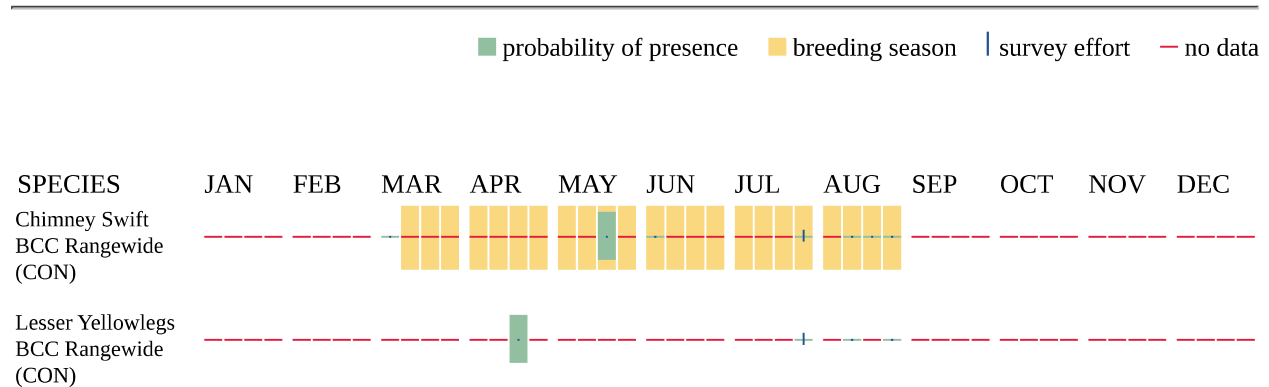
Yellow bars; liberal estimate of the timeframe inside which the bird breeds across its entire range.

Survey Effort (|)

Vertical black lines; the number of surveys performed for that species in the 10km grid cell(s) your project area overlaps.

No Data (—)

A week is marked as having no data if there were no survey events for that week.



Additional information can be found using the following links:

- Eagle Management <https://www.fws.gov/program/eagle-management>
- Measures for avoiding and minimizing impacts to birds <https://www.fws.gov/library/collections/avoiding-and-minimizing-incident-take-migratory-birds>
- Nationwide avoidance and minimization measures for birds
- Supplemental Information for Migratory Birds and Eagles in IPaC <https://www.fws.gov/media/supplemental-information-migratory-birds-and-bald-and-golden-eagles-may-occur-project-action>

WETLANDS

Impacts to [NWI wetlands](#) and other aquatic habitats may be subject to regulation under Section 404 of the Clean Water Act, or other State/Federal statutes.

For more information please contact the Regulatory Program of the local [U.S. Army Corps of Engineers District](#).

Please note that the NWI data being shown may be out of date. We are currently working to update our NWI data set. We recommend you verify these results with a site visit to determine the actual extent of wetlands on site.

FRESHWATER FORESTED/SHRUB WETLAND

- PSS1Ax
- PFO1A

IPAC USER CONTACT INFORMATION

Agency: Department of Transportation
Name: Nakayla Krahn
Address: 251 N. Illinois Street
City: Indianapolis
State: IN
Zip: 46204
Email: nakayla.krahn@burgessniple.com
Phone: 3175506928

LEAD AGENCY CONTACT INFORMATION

Lead Agency: Federal Highway Administration

You have indicated that your project falls under or receives funding through the following special project authorities:

- BIPARTISAN INFRASTRUCTURE LAW (BIL) (OTHER)



United States Department of the Interior

FISH AND WILDLIFE SERVICE
Indiana Ecological Services Field Office
620 South Walker Street
Bloomington, IN 47403-2121
Phone: (812) 334-4261 Fax: (812) 334-4273



In Reply Refer To:

07/22/2024 16:59:27 UTC

Project code: 2024-0118103

Project Name: SR 26 over Race Run Bridge Replacement (Des No. 2200586)

Subject: Concurrence verification letter for the 'SR 26 over Race Run Bridge Replacement (Des No. 2200586)' project under the amended February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion (dated March 23, 2023) for Transportation Projects within the Range of the Indiana Bat and Northern Long-eared Bat (NLEB).

To whom it may concern:

The U.S. Fish and Wildlife Service (Service) has received your request dated July 22, 2024 to verify that the **SR 26 over Race Run Bridge Replacement (Des No. 2200586)** (Proposed Action) may rely on the concurrence provided in the amended February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion (dated March 23, 2023) for Transportation Projects within the Range of the Indiana Bat and Northern Long-eared Bat (PBO) to satisfy requirements under Section 7(a)(2) of the Endangered Species Act of 1973 (ESA) (87 Stat.884, as amended; 16 U.S.C. 1531 *et seq.*).

Based on the information you provided (Project Description shown below), you have determined that the Proposed Action is within the scope and adheres to the criteria of the PBO, including the adoption of applicable avoidance and minimization measures. **At least one of the qualification interview questions indicated an activity or portion of your project is consistent with a not likely to adversely affect determination therefore, the overall determination for your project is, may affect, and is not likely to adversely affect (NLAA) the endangered Indiana bat (*Myotis sodalis*) and/or the endangered northern long-eared bat (*Myotis septentrionalis*).** Consultation with the Service pursuant to section 7(a)(2) of ESA (87 Stat. 884, as amended; 16 U.S.C. 1531 *et seq.*) is required.

The Service has 14 calendar days to notify the lead Federal action agency or designated non-federal representative if we determine that the Proposed Action does not meet the criteria for a NLAA determination under the PBO. If we do not notify the lead Federal action agency or designated non-federal representative within that timeframe, you may proceed with the Proposed Action under the terms of the NLAA concurrence provided in the PBO. This verification period allows Service Field Offices to apply local knowledge to implementation of the PBO, as we may

identify a small subset of actions having impacts that were unanticipated. In such instances, Service Field Offices may request additional information that is necessary to verify inclusion of the proposed action under the PBO.

For Proposed Actions that include bridge/culvert or structure removal, replacement, and/or maintenance activities: If your initial bridge/culvert or structure assessment documented signs of bat use or occupancy, or an assessment failed to detect Indiana bats and/or NLEBs, yet are later detected prior to, or during construction, please submit the Post Assessment Discovery of Bats at Bridge/Culvert or Structure Form (User Guide Appendix E) to this Service Office within 2 working days of any potential take. In these instances, potential incidental take of Indiana bats and/or NLEBs is covered under the Incidental Take Statement in the 2018 FHWA, FRA, FTA PBO (provided that the take is reported to the Service).

If the Proposed Action is modified, or new information reveals that it may affect the Indiana bat and/or northern long-eared bat in a manner or to an extent not considered in the PBO, further review to conclude the requirements of ESA Section 7(a)(2) may be required.

For Proposed Actions that include bridge/culvert or structure removal, replacement, and/or maintenance activities:

If your initial bridge/culvert or structure assessments failed to detect Indiana bats and/or NLEB use or occupancy, yet bats are later detected prior to, or during construction, please submit the Post Assessment Discovery of Bats at Bridge/Culvert or Structure Form (User Guide Appendix E) to this Service Office within 2 working days of the incident. In these instances, potential incidental take of Indiana bats and/or NLEBs may be exempted provided that the take is reported to the Service.

If the Proposed Action may affect any other federally-listed or proposed species, and/or any designated critical habitat, additional consultation between the lead Federal action agency and this Service Office is required. If the proposed action has the potential to take bald or golden eagles, additional coordination with the Service under the Bald and Golden Eagle Protection Act may also be required. In either of these circumstances, please contact this Service Office.

The following species may occur in your project area and **are not** covered by this determination:

- Monarch Butterfly *Danaus plexippus* Candidate
- Whooping Crane *Grus americana* Experimental Population, Non-Essential

PROJECT DESCRIPTION

The following project name and description was collected in IPaC as part of the endangered species review process.

NAME

SR 26 over Race Run Bridge Replacement (Des No. 2200586)

DESCRIPTION

This project is located on SR 26 over Race Run (also referred to as Bolen Ditch), 1.84 miles east of US 27, near Portland, Jay County, Indiana. This section of SR 26 is a two lane, rural Major Collector, with a speed limit of 55 mph. The existing SR 26 approach cross section consists of two, 12-foot-wide travel lanes (one eastbound, one westbound) bordered by 11-foot paved usable shoulders. The existing bridge section consists of two 12-foot lanes and 10-foot shoulders. Guardrails existing along both sides of the roadway within the project area include a substandard aluminum tube rail with no curb. The existing bridge was built in 1946, with rehabilitations in 1982, and is a 44-foot-long continuous concrete slab bridge. The draft need is evidenced by the deteriorating condition of the bridge. Specific condition ratings noted in the October 6th, 2023, INDOT Bridge Inspection Report for Bridge No. 026-38-03431A (NBI 007050) include a 4 (poor, advanced deterioration) out of 9 (excellent) for the deck, a 5 (fair condition) out of 9 (excellent) for the wearing surface, a 5 (fair condition) out of 9 (excellent) for the approach slabs, a 4 (poor, advanced deterioration) out of 9 (excellent) for the superstructure, and a 5 (fair condition) out of 9 (excellent) for the substructure. Deficiencies for the deck and superstructure include heavy spalling/delamination with exposed rebar at the copings of both spans, widely spaced hairline longitudinal cracks, and delamination at the center line of Span B. Deficiencies noted for the approach slabs include wide random cracks and small spalls in the east bound lane, irregular and wide transverse cracks, and wide longitudinal cracks. Deficiencies noted in the substructure include vertical cracks, efflorescence, patched reconstruction, and horizontal cracking in the abutments; a surface spall at the south construction joint; spall with exposed rebar at the northeast corner; spalling with minor rebar exposure at the southeast wingwall; and light scaling and vertical cracks on pier 2. The draft purpose of this project is to improve the condition ratings of the deck and wearing surface to at least a 7 (good) out of 9 (excellent). The apparent existing right-of-way (ROW) is the edge of pavement.

The proposed project would replace the existing bridge with a four-beam single-span, square, prestressed concrete bulb-tee beam bridge with integral end bents. The beams are bulb-tee (BT) type 42-inches by 49-inches. The proposed span length is 83-feet with 2:1 spillslopes. The proposed bridge section would consist of two 12-foot lanes bordered by 4-foot, 4-inch paved shoulders. The existing guardrail at each bridge corner will be replaced with Midwest Guardrail System (MGS) compliant guardrail. The proposed project will require a greater grade raise of approximately 11 inches. Approximately 0.4 acres (0.2 acres NW quadrant, 0.2 acres NE quadrant) of tree clearing is anticipated as part of this project. The project will require more than 0.5 acre of permanent ROW. Maintenance of Traffic (MOT) is expected to include a full road closure with a detour utilizing official and local routes US 27, SR 67, SR

49, SR 119, Ohio SR 29, Ohio SR 49, and Ohio SR 119.

Some suitable bat habitat is within and adjacent to the project area. A review of the USFWS database conducted on March 21, 2024, did not indicate the presence of endangered bat species in or within half mile of the project area. The October 26, 2023, INDOT Bridge Inspection and the May 29, 2024, bat assessment states there was no evidence of bats using the bridge.

Construction is anticipated to occur in Spring 2026. Temporary lightning may be used during construction, but all lighting will be directed away from potential bat roosts. No permanent lighting will be added or changed.

The approximate location of the project can be viewed in Google Maps: <https://www.google.com/maps/@40.43269715,-84.94297338304422,14z>



DETERMINATION KEY RESULT

Based on your answers provided, this project(s) may affect, but is not likely to adversely affect the endangered Indiana bat and/or the endangered northern long-eared bat, therefore, consultation with the U.S. Fish and Wildlife Service pursuant to Section 7(a)(2) of the Endangered Species Act of 1973 (ESA) (87 Stat. 884, as amended 16 U.S.C. 1531 *et seq.*) is required. However, also based on your answers provided, this project may rely on the concurrence provided in the amended February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion (dated March 23, 2023) for Transportation Projects within the Range of the Indiana Bat and Northern Long-eared Bat.

QUALIFICATION INTERVIEW

1. Is the project within the range of the Indiana bat^[1]?

[1] See [Indiana bat species profile](#)

Automatically answered

Yes

2. Is the project within the range of the northern long-eared bat^[1]?

[1] See [northern long-eared bat species profile](#)

Automatically answered

Yes

3. Which Federal Agency is the lead for the action?

A) Federal Highway Administration (FHWA)

4. Are *all* project activities limited to non-construction^[1] activities only? (examples of non-construction activities include: bridge/abandoned structure assessments, surveys, planning and technical studies, property inspections, and property sales)

[1] Construction refers to activities involving ground disturbance, percussive noise, and/or lighting.

No

5. Does the project include *any* activities that are **greater than** 300 feet from existing road/rail surfaces^[1]?

[1] Road surface is defined as the actively used [e.g. motorized vehicles] driving surface and shoulders [may be pavement, gravel, etc.] and rail surface is defined as the edge of the actively used rail ballast.

No

6. Does the project include *any* activities **within** 0.5 miles of a known Indiana bat and/or NLEB hibernaculum^[1]?

[1] For the purpose of this consultation, a hibernaculum is a site, most often a cave or mine, where bats hibernate during the winter (see suitable habitat), but could also include bridges and structures if bats are found to be hibernating there during the winter.

No

7. Is the project located **within** a karst area?

No

8. Is there *any* suitable^[1] summer habitat for Indiana Bat or NLEB **within** the project action area^[2]? (includes any trees suitable for maternity, roosting, foraging, or travelling habitat)

[1] See the Service's [summer survey guidance](#) for our current definitions of suitable habitat.

[2] The action area is defined as all areas to be affected directly or indirectly by the Federal action and not merely the immediate area involved in the action (50 CFR Section 402.02). Further clarification is provided by the [User's Guide for the Range-wide Programmatic Consultation for Indiana Bat and Northern Long-eared Bat](#).

Yes

9. Will the project remove *any* suitable summer habitat^[1] and/or remove/trim any existing trees **within** suitable summer habitat?

[1] See the Service's [summer survey guidance](#) for our current definitions of suitable habitat.

Yes

10. Will the project clear more than 20 acres of suitable habitat per 5-mile section of road/rail?

No

11. Have presence/probable absence (P/A) summer surveys^{[1][2]} been conducted^{[3][4]} **within** the suitable habitat located within your project action area?

[1] See the Service's [summer survey guidance](#) for our current definitions of suitable habitat.

[2] Presence/probable absence summer surveys conducted within the fall swarming/spring emergence home range of a documented Indiana bat hibernaculum (contact local Service Field Office for appropriate distance from hibernacula) that result in a negative finding requires additional consultation with the local Service Field Office to determine if clearing of forested habitat is appropriate and/or if seasonal clearing restrictions are needed to avoid and minimize potential adverse effects on fall swarming and spring emerging Indiana bats.

[3] For projects within the range of either the Indiana bat or NLEB in which suitable habitat is present, and no bat surveys have been conducted, the transportation agency will assume presence of the appropriate species. This assumption of presence should be based upon the presence of suitable habitat and the capability of bats to occupy it because of their mobility.

[4] Negative presence/probable absence survey results obtained using the [summer survey guidance](#) are valid for a minimum of two years from the completion of the survey unless new information (e.g., other nearby surveys) suggest otherwise.

No

12. Does the project include activities **within documented Indiana bat habitat**^{[1][2]}?

[1] Documented roosting or foraging habitat – for the purposes of this consultation, we are considering documented habitat as that where Indiana bats and/or NLEB have actually been captured and tracked using (1) radio telemetry to roosts; (2) radio telemetry biangulation/triangulation to estimate foraging areas; or (3) foraging areas with repeated use documented using acoustics. Documented roosting habitat is also considered as suitable summer habitat within 0.25 miles of documented roosts.)

[2] For the purposes of this key, we are considering documented corridors as that where Indiana bats and/or NLEB have actually been captured and tracked to using (1) radio telemetry; or (2) treed corridors located directly between documented roosting and foraging habitat.

No

13. Will the removal or trimming of habitat or trees occur **within** suitable but **undocumented Indiana bat** roosting/foraging habitat or travel corridors?

Yes

14. What time of year will the removal or trimming of habitat or trees **within** suitable but **undocumented Indiana bat** roosting/foraging habitat or travel corridors occur^[1]?

[1] Coordinate with the local Service Field Office for appropriate dates.

B) During the inactive season

15. Does the project include activities **within documented NLEB habitat**^{[1][2]}?

[1] Documented roosting or foraging habitat – for the purposes of this consultation, we are considering documented habitat as that where Indiana bats and/or NLEB have actually been captured and tracked using (1) radio telemetry to roosts; (2) radio telemetry biangulation/triangulation to estimate foraging areas; or (3) foraging areas with repeated use documented using acoustics. Documented roosting habitat is also considered as suitable summer habitat within 0.25 miles of documented roosts.)

[2] For the purposes of this key, we are considering documented corridors as that where Indiana bats and/or NLEB have actually been captured and tracked to using (1) radio telemetry; or (2) treed corridors located directly between documented roosting and foraging habitat.

No

16. Will the removal or trimming of habitat or trees occur **within** suitable but **undocumented NLEB** roosting/foraging habitat or travel corridors?

Yes

17. What time of year will the removal or trimming of habitat or trees **within** suitable but **undocumented NLEB** roosting/foraging habitat or travel corridors occur?

B) During the inactive season

18. Will *any* tree trimming or removal occur **within** 100 feet of existing road/rail surfaces?

Yes

19. Will *any* tree trimming or removal occur **between** 100-300 feet of existing road/rail surfaces?

No

20. Are *all* trees that are being removed clearly demarcated?

Yes

21. Will the removal of habitat or the removal/trimming of trees include installing new or replacing existing **permanent** lighting?

No

22. Does the project include wetland or stream protection activities associated with compensatory wetland mitigation?

No

23. Does the project include slash pile burning?

No

24. Does the project include *any* bridge removal, replacement, and/or maintenance activities (e.g., any bridge repair, retrofit, maintenance, and/or rehabilitation work)?

Yes

25. Is there *any* suitable habitat^[1] for Indiana bat or NLEB **within** 1,000 feet of the bridge? (includes any trees suitable for maternity, roosting, foraging, or travelling habitat)

[1] See the Service's current [summer survey guidance](#) for our current definitions of suitable habitat.

Yes

26. Has a bridge assessment^[1] been conducted **within** the last 24 months^[2] to determine if the bridge is being used by bats?

[1] See [User Guide Appendix D](#) for bridge/structure assessment guidance

[2] Assessments must be completed no more than 2 years prior to conducting any work below the deck surface on all bridges that meet the physical characteristics described in the Programmatic Consultation, regardless of whether assessments have been conducted in the past. Due to the transitory nature of bat use, a negative result in one year does not guarantee that bats will not use that bridge/structure in subsequent years.

Yes

SUBMITTED DOCUMENTS

- USFWS Bat Assessment Form_signed_flattened.pdf <https://ipac.ecosphere.fws.gov/project/SKSDTTRXBF4XENJ45UWXT6VE/projectDocuments/146545749>

27. Did the bridge assessment detect *any* signs of Indiana bats and/or NLEBs roosting in/under the bridge (bats, guano, etc.)^[1]?

[1] If bridge assessment detects signs of *any* species of bats, coordination with the local FWS office is needed to identify potential threatened or endangered bat species. Additional studies may be undertaken to try to identify which bat species may be utilizing the bridge prior to allowing *any* work to proceed.

Note: There is a small chance bridge assessments for bat occupancy do not detect bats. Should a small number of bats be observed roosting on a bridge just prior to or during construction, such that take is likely to occur or does occur in the form of harassment, injury or death, the PBO requires the action agency to report the take. Report all unanticipated take within 2 working days of the incident to the USFWS. Construction activities may continue without delay provided the take is reported to the USFWS and is limited to 5 bats per project.

No

28. Will the bridge removal, replacement, and/or maintenance activities include installing new or replacing existing **permanent** lighting?

No

29. Does the project include the removal, replacement, and/or maintenance of *any* structure other than a bridge? (e.g., rest areas, offices, sheds, outbuildings, barns, parking garages, etc.)

No

30. Will the project involve the use of **temporary** lighting *during* the active season?

Yes

31. Is there *any* suitable habitat **within** 1,000 feet of the location(s) where **temporary** lighting will be used?

Yes

32. Will the project install new or replace existing **permanent** lighting?

No

33. Does the project include percussives or other activities (**not including tree removal/trimming or bridge/structure work**) that will increase noise levels above existing traffic/background levels?

No

34. Are *all* project activities that are **not associated with** habitat removal, tree removal/trimming, bridge and/or structure activities, temporary or permanent lighting, or use of percussives, limited to actions that DO NOT cause any additional stressors to the bat species?

Examples: lining roadways, unlighted signage, rail road crossing signals, signal lighting, and minor road repair such as asphalt fill of potholes, etc.

Yes

35. Will the project raise the road profile **above the tree canopy**?

No

36. Are the project activities that are not associated with habitat removal, tree removal/trimming, bridge and/or structure activities, temporary or permanent lighting, or use of percussives consistent with a No Effect determination in this key?

Automatically answered

Yes, other project activities are limited to actions that DO NOT cause any additional stressors to the bat species as described in the BA/BO

37. Is the habitat removal portion of this project consistent with a Not Likely to Adversely Affect determination in this key?

Automatically answered

Yes, because the tree removal/trimming that occurs outside of the Indiana bat's active season occurs greater than 0.5 miles from the nearest hibernaculum, is less than 100 feet from the existing road/rail surface, includes clear demarcation of the trees that are to be removed, and does not alter documented roosts and/or surrounding summer habitat within 0.25 miles of a documented roost.

38. Is the habitat removal portion of this project consistent with a Not Likely to Adversely Affect determination in this key?

Automatically answered

Yes, because the tree removal/trimming that occurs outside of the NLEB's active season occurs greater than 0.5 miles from the nearest hibernaculum, is less than 100 feet from the existing road/rail surface, includes clear demarcation of the trees that are to be removed, and does not alter documented roosts and/or surrounding summer habitat within 0.25 miles of a documented roost.

39. Is the bridge removal, replacement, or maintenance activities portion of this project consistent with a No Effect determination in this key?

Automatically answered

Yes, because the bridge has been assessed using the criteria documented in the BA and no signs of bats were detected

40. **General AMM 1**

Will the project ensure *all* operators, employees, and contractors working in areas of known or presumed bat habitat are aware of *all* FHWA/FRA/FTA (Transportation Agencies) environmental commitments, including all applicable Avoidance and Minimization Measures?

Yes

41. Tree Removal AMM 1

Can *all* phases/aspects of the project (e.g., temporary work areas, alignments) be modified, to the extent practicable, to avoid tree removal^[1] in excess of what is required to implement the project safely?

Note: Tree Removal AMM 1 is a minimization measure, the full implementation of which may not always be practicable. Projects may still be NLAA as long as Tree Removal AMMs 2, 3, and 4 are implemented and LAA as long as Tree Removal AMMs 3, 5, 6, and 7 are implemented.

[1] The word “trees” as used in the AMMs refers to trees that are suitable habitat for each species within their range. See the USFWS’ current summer survey guidance for our latest definitions of suitable habitat.

Yes

42. Tree Removal AMM 3

Can tree removal be limited to that specified in project plans and ensure that contractors understand clearing limits and how they are marked in the field (e.g., install bright colored flagging/fencing prior to any tree clearing to ensure contractors stay within clearing limits)?

Yes

43. Tree Removal AMM 4

Can the project avoid cutting down/removal of *all* (1) **documented**^[1] Indiana bat or NLEB roosts^[2] (that are still suitable for roosting), (2) trees **within** 0.25 miles of roosts, and (3) documented foraging habitat any time of year?

[1] The word documented means habitat where bats have actually been captured and/or tracked.

[2] Documented roosting or foraging habitat – for the purposes of this consultation, we are considering documented habitat as that where Indiana bats and/or NLEB have actually been captured and tracked using (1) radio telemetry to roosts; (2) radio telemetry biangulation/triangulation to estimate foraging areas; or (3) foraging areas with repeated use documented using acoustics. Documented roosting habitat is also considered as suitable summer habitat within 0.25 miles of documented roosts.)

Yes

44. Lighting AMM 1

Will *all* **temporary** lighting be directed away from suitable habitat during the active season?

Yes

PROJECT QUESTIONNAIRE

1. Please describe the proposed bridge work:

The proposed project would replace the existing bridge with a four-beam single-span, square, prestressed concrete bulb-tee beam bridge with integral end bents. The beams are bulb-tee (BT) type 42-inches by 49-inches. The proposed span length is 83-feet with 2:1 spillslopes. The proposed bridge section would consist of two 12-foot lanes bordered by 4-foot, 4-inch paved shoulders. The existing guardrail at each bridge corner will be replaced with Midwest Guardrail System (MGS) compliant guardrail. The proposed project will

require a greater grade raise of approximately 11 inches. Approximately 0.4 acres (0.2 acres NW quadrant, 0.2 acres NE quadrant) of tree clearing is anticipated as part of this project.

2. Please state the timing of all proposed bridge work:

Summer 2026

3. Please enter the date of the bridge assessment:

May 29, 2024

4. Have you made a No Effect determination for *all* other species indicated on the FWS IPaC generated species list?

N/A

5. Have you made a May Affect determination for *any* other species on the FWS IPaC generated species list?

N/A

6. How many acres^[1] of trees are proposed for removal between 0-100 feet of the existing road/rail surface?

[1] If described as number of trees, multiply by 0.09 to convert to acreage and enter that number.

0.4

AVOIDANCE AND MINIMIZATION MEASURES (AMMS)

This determination key result includes the commitment to implement the following Avoidance and Minimization Measures (AMMs):

TREE REMOVAL AMM 1

Modify all phases/aspects of the project (e.g., temporary work areas, alignments) to avoid tree removal.

LIGHTING AMM 1

Direct temporary lighting away from suitable habitat during the active season.

TREE REMOVAL AMM 2

Apply time of year restrictions for tree removal when bats are not likely to be present, or limit tree removal to 10 or fewer trees per project at any time of year within 100 feet of existing road/rail surface and **outside of documented** roosting/foraging habitat or travel corridors; visual emergence survey must be conducted with no bats observed.

TREE REMOVAL AMM 3

Ensure tree removal is limited to that specified in project plans and ensure that contractors understand clearing limits and how they are marked in the field (e.g., install bright colored flagging/fencing prior to any tree clearing to ensure contractors stay within clearing limits).

TREE REMOVAL AMM 4

Do not remove **documented** Indiana bat or NLEB roosts that are still suitable for roosting, or trees within 0.25 miles of roosts, or **documented** foraging habitat any time of year.

GENERAL AMM 1

Ensure all operators, employees, and contractors working in areas of known or presumed bat habitat are aware of all FHWA/FRA/FTA (Transportation Agencies) environmental commitments, including all applicable AMMs.

DETERMINATION KEY DESCRIPTION: FHWA, FRA, FTA PROGRAMMATIC CONSULTATION FOR TRANSPORTATION PROJECTS AFFECTING NLEB OR INDIANA BAT

This key was last updated in IPaC on October 30, 2023. Keys are subject to periodic revision.

This decision key is intended for projects/activities funded or authorized by the Federal Highway Administration (FHWA), Federal Railroad Administration (FRA), and/or Federal Transit Administration (FTA), which may require consultation with the U.S. Fish and Wildlife Service (Service) under Section 7 of the Endangered Species Act (ESA) for the endangered **Indiana bat** (*Myotis sodalis*) and the endangered **northern long-eared bat** (NLEB) (*Myotis septentrionalis*).

This decision key should only be used to verify project applicability with the Service's [amended February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion \(dated March 23, 2023\) for Transportation Projects](#). The programmatic biological opinion covers limited transportation activities that may affect either bat species, and addresses situations that are both likely and not likely to adversely affect either bat species. This decision key will assist in identifying the effect of a specific project/activity and applicability of the programmatic consultation. The programmatic biological opinion is not intended to cover all types of transportation actions. Activities outside the scope of the programmatic biological opinion, or that may affect ESA-listed species other than the Indiana bat or NLEB, or any designated critical habitat, may require additional ESA Section 7 consultation.

IPAC USER CONTACT INFORMATION

Agency: Department of Transportation

Name: Ron Bales

Address: 32 South Broadway Street

City: Greenfield

State: IN

Zip: 46140




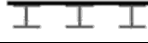



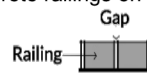

Email: rbales@indot.in.gov

Phone: 3175157908

You have indicated that your project falls under or receives funding through the following special project authorities:

- BIPARTISAN INFRASTRUCTURE LAW (BIL) (OTHER)

Bridge/Structure Bat Assessment Form

Date & Time of Assessment 2024/05/29 - morning		DOT Project Number 2200586		Route/Facility Carried SR 26		County Jay	
Federal Structure ID 026-38-03431 (NBI 007050)		Structure Coordinates 40.432760, (latitude and longitude) -84.942960		Structure Height (approximate) 100 feet		Structure Length 44.2 feet	
Structure Type (check one)				Structure Material (check all that apply)			
Bridge Construction Style				Deck Material		Beam Material	
<input checked="" type="radio"/> Cast-in-place 		<input type="radio"/> Pre-stressed Girder 		<input type="checkbox"/> Metal		<input type="checkbox"/> None	
<input type="radio"/> Flat Slab/Box 		<input type="radio"/> Steel I-beam 		<input checked="" type="checkbox"/> Concrete		<input checked="" type="checkbox"/> Concrete	
<input type="radio"/> Truss 		<input type="radio"/> Covered 		<input type="checkbox"/> Timber		<input type="checkbox"/> Steel	
<input type="radio"/> Parallel Box Beam 		<input type="radio"/> Other:		<input type="checkbox"/> Open grid		<input type="checkbox"/> Timber	
				<input type="checkbox"/> Other:		<input type="checkbox"/> Other:	
Culvert Type				Culvert Material		Creosote Evidence	
<input type="radio"/> Box		<input type="radio"/> Other Structure		<input type="checkbox"/> Metal		<input checked="" type="radio"/> Yes <input type="radio"/> No	
<input type="radio"/> Pipe/Round				<input type="checkbox"/> Concrete		<input type="radio"/> Unknown	
<input type="radio"/> Other:				<input type="checkbox"/> Plastic		Notes:	
				<input type="checkbox"/> Stone/Masonry			
				<input type="checkbox"/> Other:			
Crossings Traversed (check all that apply)				Surrounding Habitat (check all that apply)			
<input type="checkbox"/> Bare ground		<input checked="" type="checkbox"/> Open vegetation		<input checked="" type="checkbox"/> Agricultural		<input type="checkbox"/> Grassland	
<input type="checkbox"/> Rip-rap		<input type="checkbox"/> Closed vegetation		<input type="checkbox"/> Commercial		<input type="checkbox"/> Ranching	
<input checked="" type="checkbox"/> Flowing water		<input type="checkbox"/> Railroad		<input type="checkbox"/> Residential-urban		<input checked="" type="checkbox"/> Riparian/wetland	
<input type="checkbox"/> Standing water		<input type="checkbox"/> Road/trail - Type:		<input type="checkbox"/> Residential-rural		<input type="checkbox"/> Mixed use	
<input type="checkbox"/> Seasonal water		<input type="checkbox"/> Other:		<input checked="" type="checkbox"/> Woodland/forested		<input type="checkbox"/> Other:	
Areas Assessed (check all that apply)							
Check all areas that apply. If an area is not present in the structure, check the "not present" box.							
Document all bat indicators observed during the assessment. Include the species present, if known, and provide photo documentation as indicated.							
Area (check if assessed)		Assessment Notes		Evidence of Bats (include photos if present)			
<input type="checkbox"/> All crevices and cracks:		<input type="checkbox"/> Not present		<input type="checkbox"/> Visual - live # dead #		<input type="checkbox"/> Audible <input type="checkbox"/> Species	
<input checked="" type="checkbox"/> Bridges/culverts: rough surfaces or imperfections in concrete		No evidence of bats present.		<input type="checkbox"/> Guano		<input type="checkbox"/> Odor	
<input type="checkbox"/> Other structures: soffits, rafters, attic areas				<input type="checkbox"/> Photos			
				<input type="checkbox"/> Staining			
<input checked="" type="checkbox"/> Concrete surfaces (open roosting on concrete)		No evidence of bats present.		<input type="checkbox"/> Visual - live # dead #		<input type="checkbox"/> Audible <input type="checkbox"/> Species	
				<input type="checkbox"/> Guano		<input type="checkbox"/> Odor	
				<input type="checkbox"/> Photos			
				<input type="checkbox"/> Staining			
<input checked="" type="checkbox"/> Spaces between concrete end walls and the bridge deck		No evidence of bats present.		<input type="checkbox"/> Visual - live # dead #		<input type="checkbox"/> Audible <input type="checkbox"/> Species	
				<input type="checkbox"/> Guano		<input type="checkbox"/> Odor	
				<input type="checkbox"/> Photos			
				<input type="checkbox"/> Staining			
<input checked="" type="checkbox"/> Crack between concrete railings on top of the bridge deck 		No evidence of bats present.		<input type="checkbox"/> Visual - live # dead #		<input type="checkbox"/> Audible <input type="checkbox"/> Species	
				<input type="checkbox"/> Guano		<input type="checkbox"/> Odor	
				<input type="checkbox"/> Photos			
				<input type="checkbox"/> Staining			
<input checked="" type="checkbox"/> Vertical surfaces on concrete I-beams		No evidence of bats present.		<input type="checkbox"/> Visual - live # dead #		<input type="checkbox"/> Audible <input type="checkbox"/> Species	
				<input type="checkbox"/> Guano		<input type="checkbox"/> Odor	
				<input type="checkbox"/> Photos			
				<input type="checkbox"/> Staining			
<input checked="" type="checkbox"/> Spaces between walls, ceiling joists		No evidence of bats present.		<input type="checkbox"/> Visual - live # dead #		<input type="checkbox"/> Audible <input type="checkbox"/> Species	
				<input type="checkbox"/> Guano		<input type="checkbox"/> Odor	
				<input type="checkbox"/> Photos			
				<input type="checkbox"/> Staining			
<input checked="" type="checkbox"/> Weep holes, scupper drains, and inlets/pipes		No evidence of bats present.		<input type="checkbox"/> Visual - live # dead #		<input type="checkbox"/> Audible <input type="checkbox"/> Species	
				<input type="checkbox"/> Guano		<input type="checkbox"/> Odor	
				<input type="checkbox"/> Photos			
				<input type="checkbox"/> Staining			
<input checked="" type="checkbox"/> All guiderails		No evidence of bats present.		<input type="checkbox"/> Visual - live # dead #		<input type="checkbox"/> Audible <input type="checkbox"/> Species	
				<input type="checkbox"/> Guano		<input type="checkbox"/> Odor	
				<input type="checkbox"/> Photos			
				<input type="checkbox"/> Staining			
<input checked="" type="checkbox"/> All expansion joints		No evidence of bats present.		<input type="checkbox"/> Visual - live # dead #		<input type="checkbox"/> Audible <input type="checkbox"/> Species	
				<input type="checkbox"/> Guano		<input type="checkbox"/> Odor	
				<input type="checkbox"/> Photos			
				<input type="checkbox"/> Staining			
Name: Nakayla J. Krahn				Signature: 			

Appendix D

Section 106 of the NHPA

Minor Projects PA Project Submittal and Assessment Form

SECTION 1

Submittal of this form is only required for projects where Category B applies. Projects qualifying under Category A do not require submittal of this form. SECTION 2 (for Conditions of Category B-1 for curb/sidewalk) or SECTION 3 (for Conditions of Category B-9 for drainage structures) may be required as determined by INDOT-Cultural Resources Office (INDOT-CRO) review. INDOT-CRO will notify applicant if the Minor Projects PA does not apply.

Part I: Project Information-Completed by Applicant (Consultant/PM/Project Sponsor/INDOT District Staff)*

**A qualified professional historian (QP) is not required to complete Part I. INDOT-CRO staff will be responsible for completion of Part II.*

Original Submission Date: 3/26/25 Amended Submission Date*:

Consult with INDOT-CRO to determine whether an amendment is required. For revisions/updates to original form, please detail in applicable sections below. **Please use red font to distinguish the revisions/updates.*

Submitted By (Provide Name and Firm/Organization):

Alyssa Reynolds
Cultural Resource Analysts, Inc.
201 NW 4th Street, Suite 204
Evansville, IN 47708
812.549.4503
adreynolds@crai-ky.com

Project Designation Number: 2200586

Route Number: State Road (SR) 26

Feature crossed (if applicable): Race Run (also known as Bolen Ditch)

City/Township: Wayne Township

County: Jay County

Project Description:*

The SR 26 bridge project is located along SR 26, 1.84 miles east of US 27 in Wayne Township, Jay County, Indiana. The need for the project stems from the deteriorated condition of the existing structure, which has an overall condition rating of 4 (poor) out of 9 (excellent) with heavy spalling and delamination with exposed rebar identified on the superstructure and spalling, cracks, and efflorescence present on the substructure. The purpose of the project is address deficiencies and to improve the overall condition of the structure to at least a condition rating of 7 (good) out of 9 (excellent).

The proposed project includes a full bridge replacement. The existing bridge (Bridge No. 026-38-03431A; NBI No. 007050) is a 44.3' x 46.6,' reinforced concrete slab bridge constructed in 1946. The slab superstructure was replaced with a widened slab and the substructure was widened to accommodate the widened slab in 1982. The existing bridge will be replaced with a is 83' x 35,' continuous prestressed concrete bulb tee beam bridge.

The existing approach roadway has 12-foot lanes with paved shoulders varying from 2-foot to 11-foot. The existing bridge section consists of two 12-foot lanes and 11-foot shoulders. The existing bridge rail is a substandard aluminum tube rail with no curb. Approach guardrail is present at all four corners of the bridge. The proposed bridge section will have two 12-foot lanes and 4-foot shoulders. The proposed approach roadway will have 12-foot lanes with 4-foot 4-inch paved shoulders. The existing 11-foot shoulders are oversized when compared with the typical

Minor Projects PA Project Submittal and Assessment Form

roadway sections on SR 26, and the widened shoulder is only provided for 400 feet before reverting to the typical 2-foot paved and 3-foot usable shoulders.

All existing guardrail within the project limits will be replaced with Midwest Guardrail System (MGS) compliant guardrail. It is anticipated that the existing profile grade will be raised a maximum of 11 inches to accommodate the structure depth.

Approximately 0.03 acre of temporary right-of-way (ROW) and approximately 1.54 acres of permanent ROW acquisition is anticipated for the proposed project.

The proposed maintenance of traffic (MOT) is anticipated to include the full closure of SR 26 with a detour utilizing US 27 and SR 67. SR 49, located in Ohio, will also be utilized.

If the project includes any curb, curb ramp, or sidewalk work, please specify the location(s) of such work:
N/A

For bridge or small structure projects, please list feature crossed, structure number, NBI number, and structure type: The existing bridge (Bridge No. 026-38-03431A; NBI No. 007050) is a 44.3' x 46.6,' reinforced concrete slab bridge that carries SR 26 over Bolen Ditch.

For bridge projects, is the bridge included in INDOT's Historic Bridge Inventory (<https://www.in.gov/indot/2531.htm>)?

☐ Yes ☒ No

If yes, did the inventory determine the bridge eligible for or listed in the National Register of Historic Places? Please provide page # of entry in Historic Bridge Inventory.

☐ Yes ☐ No

Inventory Page # _____

Will there be right-of-way acquisition as part of this project?

☒ Yes ☐ No

If yes was checked above, please check all that apply:

☒ Permanent ☒ Temporary ☐ Reacquisition

If applicable, identify right-of-way acquisition locations in text below and in attached mapping. Please specify how much (both temporary and permanent) and indicate what activities are included in the proposed right-of-way:

Approximately 0.03 acre of temporary ROW acquisition and approximately 1.54 acres of permanent ROW acquisition is anticipated for the proposed project. This will be used for field entrance reconstruction, construction access, riprap turnouts, and reconstruction of the bridge.

Is there any potential for additional temporary right-of-way to be needed later for purposes such as access, staging, etc.?

☐ Yes ☒ No

Archaeology (check one):

☐ All proposed activities are presumed to occur in previously disturbed soils.*
**INDOT-CRO will notify you if project area includes undisturbed soils and requires an archaeological reconnaissance.*

☒ Project takes place in undisturbed soils and the archaeology report is included with the submission.*

Minor Projects PA Project Submittal and Assessment Form

** If an archaeology report is required, the Minor Projects PA Form will not be finalized until the report is reviewed and approved by INDOT-CRO. For INDOT-sponsored projects, INDOT-CRO may be able to complete the archaeological investigation. If you would like to request that INDOT-CRO complete an archaeological investigation, please contact the INDOT-CRO Archaeology Team Lead. See CRM Pt. 1 Ch. 3 for current contact information.*

Please specify all applicable categories and condition(s) (INDOT will highlight applicable conditions in yellow):

{Include full category text(s) verbatim, including any conditions. INDOT-CRO will finalize categories upon their review.}

- B-12.** Replacement, widening, or raising the elevation of the superstructure on existing bridges, and bridge replacement projects (when both the superstructure and substructure are removed), under the following conditions **[BOTH Condition A, which pertains to Archaeological Resources, and Condition B, which pertains to Above-Ground Resources, must be satisfied]:**

Condition A (Archaeological Resources)

One of the two conditions listed below must be met (*EITHER Condition i or Condition ii must be satisfied*):

- i. Work occurs in previously disturbed soils; *OR*
- ii. Work occurs in undisturbed soils and an archaeological investigation conducted by the applicant and reviewed by INDOT Cultural Resources Office determines that no National Register-listed or potentially National Register-eligible archaeological resources are present within the project area. If the archaeological investigation locates National Register-listed or potentially National Register-eligible archaeological resources, then full Section 106 review will be required. Copies of any archaeological reports prepared for the project will be provided to the DHPA and any archaeological site form information will be entered directly into the SHAARD by the applicant. The archaeological reports will also be available for viewing (by Tribes only) on IN SCOPE.

Condition B (Above-Ground Resources)

The conditions listed below must be met (*BOTH Condition i and Condition ii must be satisfied*)

- i. Work does not occur adjacent to or within a National Register-listed or National Register-eligible district or individual above-ground resource; *AND*
- ii. With regard to the subject bridge, at least one of the conditions listed below is satisfied (*AT LEAST one of the conditions a, b or c, must be fulfilled*):
 - a. The latest Historic Bridge Inventory identified the bridge as non-historic (see <http://www.in.gov/indot/2531.htm>);
 - b. The bridge was built after 1945, and is a common type as defined in Section V. of the *Program Comment Issued for Streamlining Section 106 Review for Actions Affecting Post-1945 Concrete and Steel Bridges* issued by the Advisory Council on Historic Preservation on November 2, 2012 for so long as that Program Comment remains in effect *AND* the considerations listed in Section IV of the Program Comment do not apply;
 - c. The bridge is part of the Interstate system and was determined not eligible for the National Register under the Section 106 Exemption Regarding Effects to the Interstate Highway System adopted by the Advisory Council on Historic Preservation on March 10, 2005, for so long as that Exemption remains in effect.

Check ☐ if SECTION 2: Minor Projects PA Category B-1, Condition B-ii Submission is included.

Check ☐ if SECTION 3: Minor Projects PA Category B-9, Condition B-i-c-2 or B-ii-b-3 Submission is included.

Minor Projects PA Project Submittal and Assessment Form

Part II: Completed by INDOT-CRO

Information reviewed (please check all that apply):

General project location map ☒ USGS map ☒ Aerial photograph ☒ Soil survey data ☒

General project area photos ☒ Archaeology Reports ☒ Historic Property Reports ☐

Indiana Historic Buildings, Bridges, and Cemeteries Map/Interim Report ☒

Bridge inspection information/iTAMS ☒ Historic Bridge Inventory Database ☒

SHAARD ☒ SHAARD GIS ☒ Streetview Imagery ☒ County GIS Data/Property Cards ☒

Other (please specify):

Lawhorn, Ashley

2025 A Phase Ia Archaeological Survey for the Proposed SR 26 Bridge Project Over Race Run (Bolen Ditch), 1.84 Miles East of US 27 in Jay County, Indiana (INDOT Des. No. 2200586). Report on file, Indiana Department of Transportation, Cultural Resources Office, Indianapolis, IN.

Are there any commitments associated with this project? If yes, please explain and include in the Additional Comments Section below. Yes ☐ No ☒

Does the project result in a de minimis impact to a Section 4(f) protected historic resource? If yes, please explain in the Additional Comments Section below. Yes ☐ No ☒

Additional Comments:

Above-ground Resources

An INDOT-Cultural Resources Office (CRO) historian who meets the Secretary of the Interior's Professional Qualification Standards as per 36 CFR Part 61 first performed a desktop review, checking the Indiana Register of Historic Sites and Structures (State Register) and National Register of Historic Places (National Register) lists for Jay County. No listed resources are present within 0.25 mile of the project area, a distance that serves as an adequate area of potential effects given the project scope and terrain.

The National Register & IHSSI information for Jay County is available in the Indiana State Historic Architectural and Archaeological Research Database (SHAARD) and the Indiana Historic Buildings, Bridges, and Cemeteries Map (IHBBCM). The *Jay County Interim Report* (1985; Wayne Township) of the Indiana Historic Sites and Structures Inventory (IHSSI) was also consulted. The SHAARD information was checked against the Interim Report hard copy maps. The IHBBCM contains the most up to date IHSSI information. No IHSSI documented properties are located within 0.25 mile of the project area.

According to the IHSSI rating system, generally properties rated "Contributing" do not possess the level of historical or architectural significance necessary to be considered individually National Register eligible, although they would contribute to a historic district. If they retain material integrity, properties rated "Notable" might possess the necessary level of significance after further research. Properties rated "Outstanding" usually possess the necessary level of significance to be considered National Register eligible if they retain material integrity. Historic districts identified in the IHSSI are usually considered eligible for the National Register.

The INDOT-CRO historian reviewed structures adjacent to the project area utilizing online aerial, street-view photography, and the Jay County GIS website. The subject structure is located in a rural setting with agricultural

Minor Projects PA Project Submittal and Assessment Form

fields to the south and a thick swath of trees to the north of the project area. The building stock within 0.25 mile of the project area is comprised primarily of early to late twentieth century residential and agricultural structures. Of the seven (7) parcels with structures located within, two (2) residences were constructed in 1978, meaning that they will not be fifty years old or older at the time of letting (2026). These two residences do not appear to possess the significance necessary for National Register eligibility under Criteria Consideration G. The remaining five (5) properties consist of two (2) heavily altered early twentieth century residences with associated outbuildings and three (3) midcentury residences. As stated above, the two (2) early twentieth century residences have both been heavily altered with those alterations including vinyl siding, replacement vinyl windows, and additions that alter the form of the building. Neither appear to possess the integrity and significance to be considered eligible for the National Register. The three (3) midcentury residences possess integrity; however, they do not appear to possess the necessary significance for individual eligibility according to the standards set in the multiple property documentation form (MPDF) *Residential Planning and Development in Indiana, 1940-1973*, and they do not appear to be part of a district.

The most recent inspection report (B. D. Harvey; 10/10/2024) from the INDOT Total Assets Management System (iTAMS) was referenced to review the structure. The subject structure (INDOT Bridge No. 026-38-03431 A; NBI No. 007050) carries SR 26 over Bolen Ditch. The bridge is a 2-span reinforced concrete slab bridge and was constructed in 1946. It was reconstructed in 1982. Structures built after 1965 were not included in the data-gathering conducted for the 2009 INDOT-sponsored *Indiana Historic Bridge Inventory* (HBI).

On November 12, 2012, the Advisory Council on Historic Preservation (ACHP) issued the Program Comment for Streamlining Section 106 Review for Actions Affecting Post-1945 Concrete and Steel Bridges (*Program Comment*). The *Program Comment* relieves federal agencies from the Section 106 requirement to consider the effects of undertakings on most concrete and steel bridges built after 1945. On March 19, 2013, federal agencies were approved to use the *Program Comment* for Indiana projects.

The *Program Comment* applies for Bridge No. 026-38-03431 A /NBI No. 007050 because it has not been previously listed in or determined eligible for listing in the National Register of Historic Places and are not located in or adjacent to a historic district (Section IV.A of the *Program Comment*). As an example of a reinforced concrete slab structure, the bridge was also not one of the types exempted from the *Program Comment* (arch bridges, truss bridges, bridges with movable spans, suspension bridges, cable-stayed bridges, or covered bridges [Section IV.B]). Additionally, the bridge has not been identified as having exceptional significance for association with a person or event, being a very early or particularly important example of its type in the state or the nation, having distinctive engineering or architectural features that depart from standard designs, or displaying other elements that were engineered to respond to a unique environmental context (Section IV.C). This bridge also has not been identified as having some exceptional quality. Based on consultation between FHWA, INDOT, SHPO and interested parties, no bridges with exceptional significance were identified in Indiana (Section IV.C). Because the above criteria from the *Program Comment* have been met, no individual consideration under Section 106 is required for Bridge No. 026-38-03431 A /NBI No. 007050.

There are no above-ground concerns at this time so long as the project scope remains unchanged.

Archaeological Resources

An INDOT-CRO archaeologist who meets the Secretary of the Interior's Professional Qualification Standards as per 36 CFR Part 61 reviewed the Phase Ia archaeological reconnaissance submitted by Cultural Resources Analysts on behalf of Burgess and Niple (Lawhorn 2025).

A 4.08-acre survey area was examined through a combination of systematic shovel probing (n=18), bucket auguring (n=1), pedestrian survey, and visual inspection of disturbed areas. The area encompassing SR 26 has been previously disturbed from the construction of the state road, road grade and fill, existing bridge with associated drainage, embankments, concrete ditch, and buried utilities. Shovel test probes were placed on the

Minor Projects PA Project Submittal and Assessment Form

northside of SR 26 in 15 m intervals in a wooded area with less than 30 percent visibility. Pedestrian survey was utilized on the southside of SR 26 in 2 m intervals in agricultural fields with greater than 40 percent visibility. One bucket auger was placed in alluvial soil on the northside of SR 26. All shovel probes and auger were negative. No archaeological sites were documented as a result of the survey and no further investigation is recommended (Lawhorn 2025).

Therefore, there are no archaeological concerns as long as the project scope and footprint do not change.

Accidental Discovery: If any archaeological artifacts or human remains are uncovered during construction, demolition, or earth moving activities, construction within 100 feet of the discovery will be stopped, and INDOT-CRO and the Indiana Department of Natural Resources-Division of Historic Preservation and Archaeology (IDNR-DHPA) will be notified immediately.

INDOT-CRO staff reviewer(s): Haley Brinker and KayLee Blum

INDOT Approval Date: 5/20/2025

Amendment Approval Date (if applicable):

****Be sure to attach this form to the National Environmental Policy Act documentation for this project. Also, the NEPA documentation shall reference and include the description of the specific stipulation in the PA that qualifies the project as exempt from further Section 106 review.*

Please attach the following to this form:

- **General Location Map.** This map should allow the INDOT-CRO reviewer to quickly locate the project.
- **Aerial photography map(s) of project area.** This map must include project limits. It may also include SHAARD data, but SHAARD data is not required.
- **Map depicting potential temporary and/or permanent right-of-way acquisitions.**
- **Project plans, if available.**
- **If bridge or small structure project, please attach photographs of bridge or small structure.** Photographs can be found in inspection reports located in INDOT's Bridge Inspection Application System (BIAS), as well as other project documents, such as engineering assessments or mini-scopes.

In the email submission to INDOT-CRO, please also include:

- **A GIS polygon shapefile or KMZ file of the project area** (shapefiles are preferred). Shapefiles should depict the project area, including all existing and proposed right-of-way and construction limits, and should use the "NAD_1983_UTM" projected coordinate system. In addition, these files should contain the following *text* attribute field: DES_NO. The project designation number should be entered in this field.
- **If the project takes place in undisturbed soils, attach the results of the archaeological investigation.**

Appendix E

Red Flag and Hazardous Materials



INDIANA DEPARTMENT OF TRANSPORTATION

100 North Senate Avenue
Room N758-ES
Indianapolis, Indiana 46204

PHONE: (855) 463-6848
(855) INDOT4U

Eric Holcomb, Governor
Michael Smith, Commissioner

Date: July 29, 2024

To: Site Assessment & Management (SAM)
Environmental Policy Office - Environmental Services Division (ESD)
Indiana Department of Transportation (INDOT)
100 N Senate Avenue, Room N758-ES
Indianapolis, IN 46204

From: Nakayla J. Krahn
Burgess & Niple, Inc.
251. N. Illinois Street, Suite 920
Indianapolis, IN 46204
Nakayla.krahn@burgessniple.com

Re: RED FLAG INVESTIGATION
DES #2200586, State Project
Bridge Replacement
State Road (SR) 26, 1.84 Miles East of United States Highway (US) 27
Jay County, Indiana

PROJECT DESCRIPTION

The proposed project is located at the SR 26 bridge over Race Run (INDOT Bridge No. 026-38-03431), approximately 1.84 miles east of US 27, near Portland, Jay County, Indiana. The project will include a full bridge replacement, shoulder reconstruction, and placement of riprap on the spillslopes. Additionally, riprap turnouts will be placed on the sideslopes at each end of the bridge.

Bridge Work Included in Project: Yes ☒ No ☐ Structure #(s) 026-38-03431

Is the bridge Historical? Yes ☐ No ☒ , Select ☐ Non-Select ☐

(Note: If the project involves a historical bridge, please include the bridge information in the Recommendations Section of the report).

Culvert Work Included in Project: Yes ☐ No ☒ Structure #(s) _____

Proposed right of way: Temporary ☒ # Acres <0.5 Permanent ☒ # Acres >0.5, Not Applicable ☐

Type and proposed depth of excavation: It is anticipated that the max depth of excavation would be approximately 17 feet to remove the existing bridge abutments.

Maintenance of traffic (MOT): The anticipated MOT will require a full closure with the use of a detour. The detour will utilize US 27 and SR 67.

Work in waterway: Yes ☒ No ☐ Below ordinary high water mark: Yes ☒ No ☐

State Project: ☒ LPA: ☐

Any other factors influencing recommendations: N/A

INFRASTRUCTURE TABLE AND SUMMARY

Infrastructure Indicate the number of items of concern found within the 0.5 mile search radius. If there are no items, please indicate N/A:			
Religious Facilities	N/A	Recreational Facilities	N/A
Airports ¹	1	Pipelines	N/A
Cemeteries	N/A	Railroads	N/A
Hospitals	N/A	Trails	N/A
Schools	N/A	Managed Lands	N/A

¹In order to complete the required airport review, a review of public-use airports within 3.8 miles (20,000 feet) is required.

Explanation:

Airports: Although not located within the 0.5 mile search radius, one (1) public use airport, Portland Municipal Airport, is located within 3.8 miles (20,000 feet) of the project area. The public use airport is located approximately 2.43 miles northwest of the project area; therefore, early coordination with INDOT Aviation will occur.

WATER RESOURCES TABLE AND SUMMARY

Water Resources Indicate the number of items of concern found within the 0.5 mile search radius. If there are no items, please indicate N/A:			
NWI - Points	N/A	Canal Routes - Historic	N/A
Karst Springs	N/A	NWI - Wetlands	16
Canal Structures – Historic	N/A	Lakes	3
NPS NRI Listed	N/A	Floodplain - DFIRM	6
NWI-Lines	N/A	Cave Entrance Density	N/A
IDEM 303d Listed Streams and Lakes (Impaired)	N/A	Sinkhole Areas	N/A
Rivers and Streams	34	Sinking-Stream Basins	N/A

If unmapped water features are identified that might impact the project area, direct coordination with INDOT ESD Ecology, Waterway Permitting, and Stormwater Office (EWPSO) will occur.

Explanation:

Rivers and Streams: Thirty-four (34) stream segments are located within the 0.5 mile search radius. Two (2) stream segments, Race Run (also known as Bolen Ditch), are located within the project area. A Waters of the US Report is recommended based on mapped features, and coordination with INDOT ESD and EWPSO will occur.

NWI: Sixteen (16) wetland polygons are located within the 0.5 mile search radius. Two (2) wetland polygons are located within the project area. A Waters of the US Report is recommended based on mapped features, and coordination with INDOT ESD and EWPSO will occur.

Lakes: Three (3) lake polygons are located within the 0.5 mile search radius. The nearest lake polygon is located approximately 0.31 mile north of the project area. No impact is expected.

Floodplains: Six (6) floodplain polygons are located within the 0.5 mile search radius. The project area is located within two (2) floodplain polygons. Coordination with INDOT ESD and EWPSO will occur.

MINING AND MINERAL EXPLORATION TABLE AND SUMMARY

Mining/Mineral Exploration Indicate the number of items of concern found within the 0.5 mile search radius. If there are no items, please indicate N/A:			
Petroleum Wells	1	Mineral Resources	N/A
Mines – Surface	N/A	Mines – Underground	N/A

Explanation:

Petroleum Wells: One (1) petroleum well is located within the 0.5 mile search radius. The petroleum well is located approximately 0.48 mile southeast of the project area. No impact is expected.

HAZARDOUS MATERIAL CONCERNS TABLE AND SUMMARY

Hazardous Material Concerns Indicate the number of items of concern found within the 0.5 mile search radius. If there are no items, please indicate N/A:			
Superfund	N/A	Manufactured Gas Plant Sites	N/A
RCRA Generator/ TSD	N/A	Open Dump Waste Sites	N/A
RCRA Corrective Action Sites	N/A	Restricted Waste Sites	N/A
State Cleanup Sites	N/A	Waste Transfer Stations	N/A
Septage Waste Sites	N/A	Tire Waste Sites	N/A
Underground Storage Tank (UST) Sites	N/A	Confined Feeding Operations (CFO)	1
Voluntary Remediation Program	N/A	Brownfields	N/A
Construction Demolition Waste	N/A	Institutional Controls	N/A
Solid Waste Landfill	N/A	NPDES Facilities	N/A
Infectious/Medical Waste Sites	N/A	NPDES Pipe Locations	N/A
Leaking Underground Storage (LUST) Sites	N/A	Notice of Contamination Sites	N/A

Unless otherwise noted, site specific details presented in this section were obtained from documents reviewed on the Indiana Department of Environmental Management (IDEM) Virtual File Cabinet (VFC).

Explanation:

Confined Feeding Operations (CFO): One (1) CFO facility is located within the 0.5 mile search radius. The facility, Thomas M Homan, 1947 E. Division Road, AI ID 23718, is located approximately 0.29 mile north of the project area. No impact is expected.

ECOLOGICAL INFORMATION SUMMARY

The Jay County listing of the Indiana Natural Heritage Data Center information on endangered, threatened, or rare (ETR) species and high quality natural communities are provided at https://www.in.gov/dnr/nature-preserves/files/np_jay.pdf. A preliminary review of the Indiana Natural Heritage Database by INDOT ESD did not indicate the presence of ETR species within the 0.5 mile search radius. Coordination with USFWS and IDNR will occur.

A review of the USFWS database did not indicate the presence of endangered bat species in or within 0.5 mile of the project area. The project area is located in a rural area surrounded primarily by agricultural and forested land. The October 6, 2023, INDOT Inspection Report for Bridge 026-38-03431 states that no evidence of bats was seen or heard under the bridge. The range-wide programmatic consultation for the Indiana Bat and Northern Long-eared Bat will be completed according to the most recent "Using the USFWS's IPaC System for Listed Bat Consultation for INDOT Projects".

RECOMMENDATIONS SECTION

Include recommendations from each section. If there are no recommendations, please indicate N/A:

INFRASTRUCTURE:

Airports: The public use airport, Portland Municipal Airport, is located approximately 2.43 miles northwest of the project area; therefore, early coordination with INDOT Aviation will occur.

WATER RESOURCES: A Waters of the US Report is recommended based on the presence of mapped features, and coordination with INDOT ESD and EWPSO will occur for the following resources:

- Two (2) stream segments, Race Run (also known as Bolen Ditch), are located within the project area.
- Two (2) wetland polygons are located within the project area.
- The project area is located within two (2) floodplain polygons (coordination only).

MINING/MINERAL EXPLORATION: N/A

HAZARDOUS MATERIAL CONCERNS: N/A

ECOLOGICAL INFORMATION: Coordination with USFWS and IDNR will occur. The range-wide programmatic consultation for the Indiana Bat and Northern Long-eared Bat will be completed according to the most recent "Using the USFWS's IPaC System for Listed Bat Consultation for INDOT Projects".

INDOT ESD concurrence: **Darlane Davis**  Digitally signed by Darlane Davis
Date: 2024.07.29 13:28:25 -04'00' (Signature)

Prepared by:
Nakayla J. Krahn
Environmental Scientist
Burgess & Niple, Inc.

Graphics:

A map for each report section with a 0.5 mile search radius buffer around all project area(s) showing all items identified as possible items of concern is attached. If there is not a section map included, please change the YES to N/A:

SITE LOCATION: YES

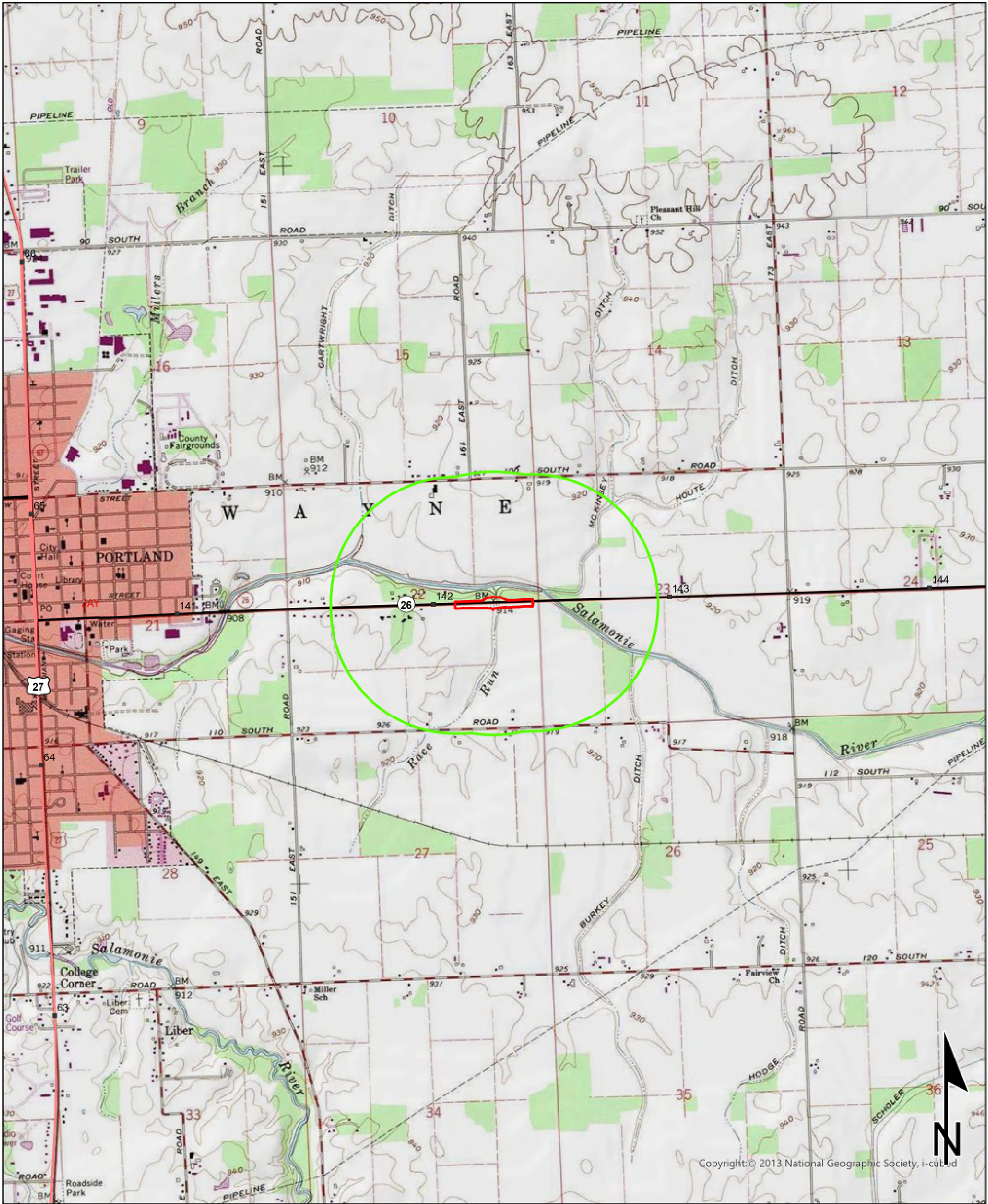
INFRASTRUCTURE: N/A

WATER RESOURCES: YES

MINING/MINERAL EXPLORATION: YES

HAZARDOUS MATERIAL CONCERNS: YES

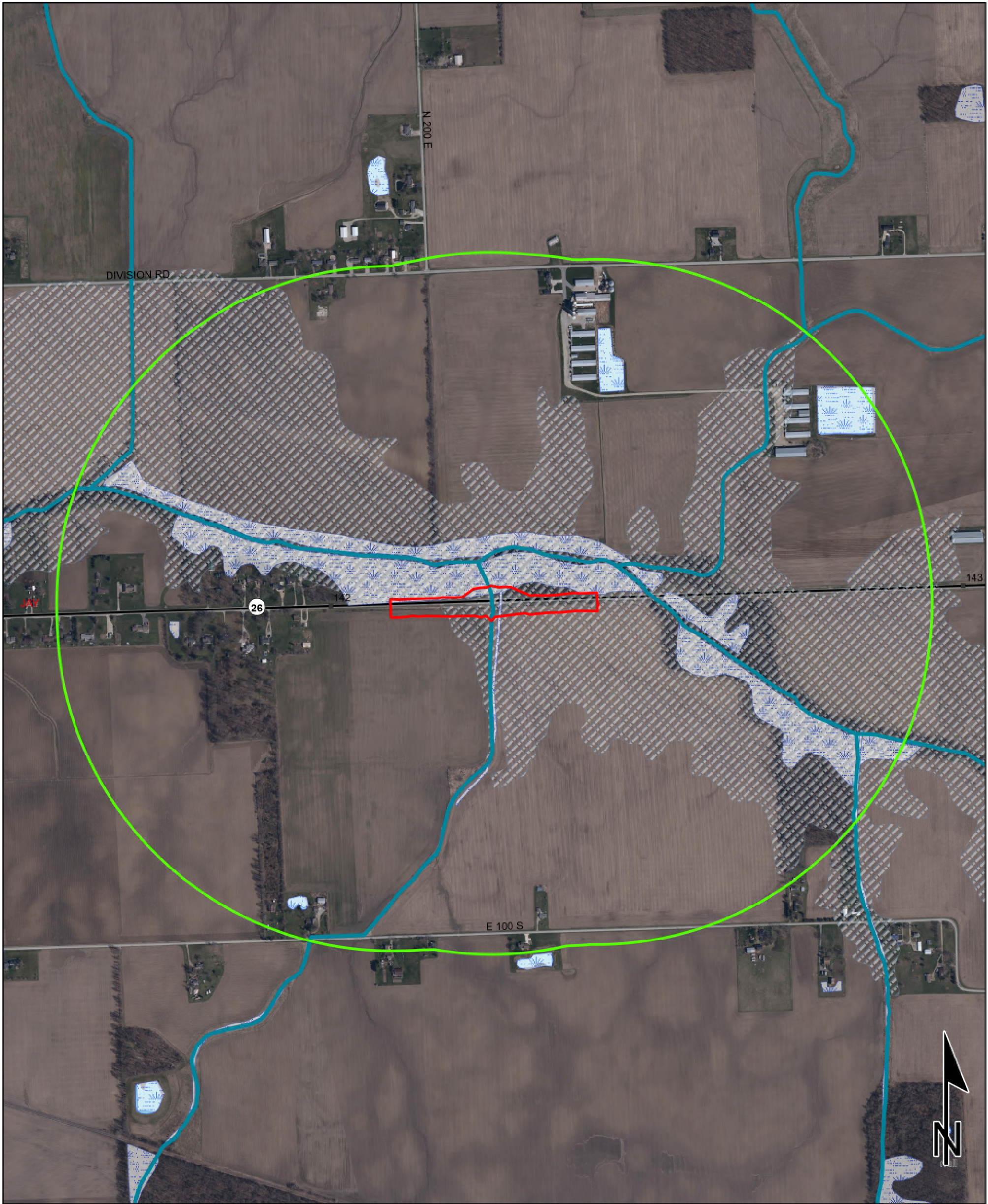
Red Flag Investigation - Site Location
SR 26, 1.84 Miles East of US 27
Des. No. 2200586, Bridge Replacement
Jay County, Indiana



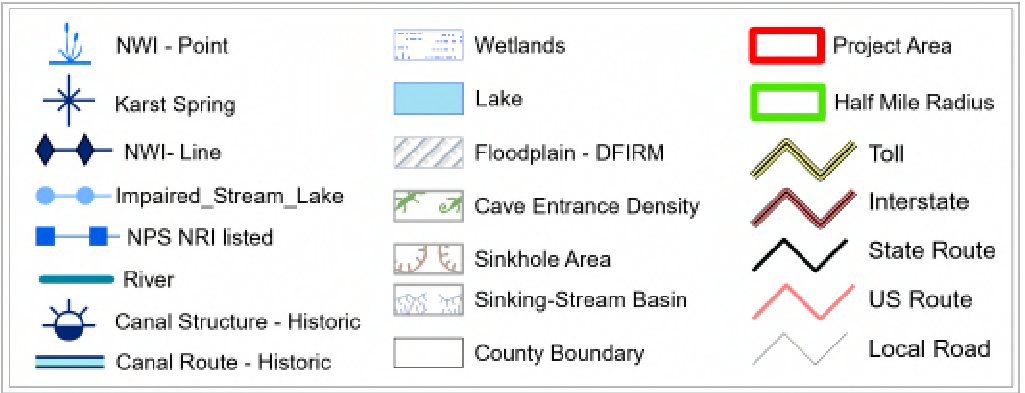
Sources: 0.45 0.23 0 0.45 Miles
Non Orthophotography
Data - Obtained from the State of Indiana Geographical Information Office Library
Orthophotography - Obtained from Indiana Map Framework Data (www.indianamap.org)
Map Projection: UTM Zone 16 N **Map Datum:** NAD83
This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.

PORTLAND QUADRANGLE
INDIANA
7.5 MINUTE SERIES
(TOPOGRAPHIC)

Red Flag Investigation - Water Resources
SR 26, 1.84 Miles East of US 27
Des. No. 2200586, Bridge Replacement
Jay County, Indiana



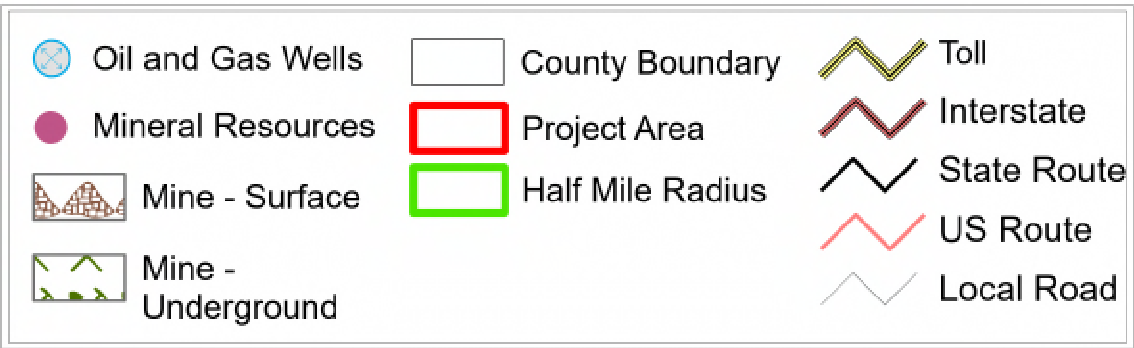
Sources: 0.15 0.07 0 0.15 Miles
Non Orthophotography
Data - Obtained from the State of Indiana Geographical Information Office Library
Orthophotography - Obtained from Indiana Map Framework Data (www.indianamap.org)
Map Projection: UTM Zone 16 N Map Datum: NAD83
This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.



Red Flag Investigation - Mining and Mineral Exploration
SR 26, 1.84 Miles East of US 27
Des. No. 2200586, Bridge Replacement
Jay County, Indiana



Sources:
Non Orthophotography
Data - Obtained from the State of Indiana Geographical Information Office Library
Orthophotography - Obtained from Indiana Map Framework Data (www.indianamap.org)
Map Projection: UTM Zone 16 N **Map Datum:** NAD83
This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.



Red Flag Investigation - Hazardous Material Concerns

SR 26, 1.84 Miles East of US 27

Des. No. 2200586, Bridge Replacement

Jay County, Indiana



	Brownfield		RCRA Generator/TSD		Institutional Controls
	RCRA Corrective Action Sites		Restricted Waste Site		County Boundary
	Confined Feeding Operation		Septage Waste Site		Project Area
	Notice_Of_Contamination		Solid Waste Landfill		Half Mile Radius
	Construction/Demolition Site		State Cleanup Site		Toll
	Infectious/Medical Waste Site		Superfund		Interstate
	Leaking Underground Storage Tank		Tire Waste Site		State Route
	Manufactured Gas Plant		Underground Storage Tank		US Route
	NPDES Facilities		Voluntary Remediation Program		Local Road
	NPDES Pipe Locations		Waste Transfer Station		
	Open Dump Waste Site				

Appendix F

Water Resources

Nakayla Krahn

From: Geissler, Aidan <AGeissler@indot.IN.gov>
Sent: Tuesday, March 11, 2025 8:02 AM
To: Nakayla Krahn
Subject: RE: Waters Report Approved -- DES 2200586 --SR 26 over Race Run (Bolden Ditch)

Hi Nakayla,

Thanks for touching base about this and sharing that map. Yes, you are good to proceed with the existing waters report!

Take care,

Aidan Geissler

Greenfield Permitting Specialist
Ecology, Waterway Permitting, & Stormwater Office
Indiana Department of Transportation
Central Office
Cell: (317)-694-7134
[Find us on social media!](#)



From: Nakayla Krahn <Nakayla.Krahn@burgessniple.com>
Sent: Monday, March 10, 2025 12:00 PM
To: Geissler, Aidan <AGeissler@indot.IN.gov>
Subject: RE: Waters Report Approved -- DES 2200586 --SR 26 over Race Run (Bolden Ditch)

EXTERNAL EMAIL: This email was sent from outside your organization. Exercise caution when clicking links, opening attachments or taking further action, before validating its authenticity.

Hey Aidan,

While developing the CE, I noticed that the construction limits extend slightly beyond what was originally anticipated. I've attached a map with photos for reference.

As you can see from the photos, the area was visually inspected and is just a vegetated slope, but the footprint of the investigated area on our maps doesn't reflect this. Could you confirm that we can proceed with the existing waters report? If so, I can attach your concurrence to the CE.

Thanks,

Nakayla Krahn (she/her)
Environmental Scientist
Burgess & Niple, Inc.

From: Geissler, Aidan <AGeissler@indot.IN.gov>
Sent: Monday, December 9, 2024 2:28 PM
To: Nakayla Krahn <Nakayla.Krahn@burgessniple.com>
Cc: Burskey, Jacob L <JBurskey@indot.IN.gov>; Darrah, Taylor N <TDarrah@indot.IN.gov>
Subject: Waters Report Approved -- DES 2200586 --SR 26 over Race Run (Bolden Ditch)

**WATERS REPORT
STATE ROAD (SR) 26 OVER RACE RUN
JAY COUNTY, INDIANA
BRIDGE REPLACEMENT PROJECT
DES. NO.: 2200586
ASSET ID #: 026-38-03431**

Prepared by:
Nakayla Krahn
Nakayla.krahn@burgessniple.com
317-237-2760 ext. 7507
Burgess & Niple, Inc.

Completed Date: 11/21/2024

Date of Field Reconnaissance: 05/29/2024

Location:

Section 22, Township 23N, Range 14E
Portland, Indiana Quadrangle
Jay County, Indiana
HUC 12: 051201020103 (Berger Ditch-Salamonie River)
40.432784, -84.942976

PROJECT DESCRIPTION

The proposed project is located at the SR 26 bridge over Race Run (also known as Bolen Ditch), approximately 1.84 miles east of US 27, near Portland, Jay County, Indiana. The project will include a full bridge replacement, shoulder reconstruction, and placement of riprap on the spill slopes. Additionally, riprap turnouts will be placed on the side slopes at each end of the bridge.

1.0 DESKTOP RECONNAISSANCE

The literature review for this report included review of proposed project plans, U.S. Geological Survey (USGS) topographic maps, current aerial photography, National Hydrography Database (NHD), National Wetlands Inventory (NWI) maps, Natural Resources Conservation (NRCS) soil map, Indiana Department of Natural Resources (IDNR) Flood Hazard map, and Indiana Department of Environmental Management (IDEM) water quality and use designation information, as applicable. Findings of the literature review are summarized below.

1.1 USGS Topography, Aerial Photography, and NHD Mapping

The project location is depicted on the Portland, Indiana 7.5-Minute Series USGS topographic quadrangle. Aerial photography was evaluated from imagery obtained from Indiana Map (<https://www.indianamap.org/>).

The investigated area is located in a rural setting along SR 26. The USGS topographic map depicts an intermittent stream, identified as Race Run (dashed blue line) flowing through the investigated area. The NHD map features one classified flowline (corresponding to Race Run) within the investigated area. One unclassified flowline is located south of the investigated area and appears to be a drainage tile. Race Run and roadside ditch (RSD) 1 are visible on the aerial photography map.

1.2 Soils

According to the Soil Survey Geographic (SSURGO) Database for Jay County, Indiana, the investigated area contains three soil areas with nationally listed hydric soils.

Review results for soil mapping and unit descriptions obtained from the NRCS Web Soil Survey (<https://websoilsurvey.nrcs.usda.gov/app/>) are summarized in **Table 1** below.

Table 1
Soil Survey

Map Abbreviation	Soil Unit Name	Description	Hydric Range	Hydric Classification
BIA	Blount-Glynwood	Thin solum complex, 0 to 3 percent slopes	5	Predominantly non-hydric
Ee	Eel clay loam	Frequently flooded	5	Predominantly non-hydric
Pm	Pewamo silty clay	0 to 2 percent slopes	91	Predominantly hydric

1.3 NWI Information

A Palustrine, Scrub-Shrub wetland (PSS1Ax) is located in the center/eastern portion of the investigated area, likely corresponding to Race Run. Additionally, a Palustrine, Forested wetland (PFO1A) is located within the northern portion of the investigated area.

NWI map review results obtained from the U.S. Fish & Wildlife Service's Wetlands Mapper application (<https://www.fws.gov/program/national-wetlands-inventory/wetlands-mapper>), are summarized in **Table 2** below.

Table 2
NWI Mapped Features

Abbreviation	Classification	Description	Location
PFO1A	Palustrine/Forested/Broad-Leaved Deciduous/ Temporary Flooded	Freshwater Forested/Shrub Wetland	Northern portion of the investigated area
PSS1Ax	Palustrine/Scrub-Shrub/Broad-Leaved Deciduous/Temporary Flooded/ Excavated	Freshwater Forested/Shrub Wetland	Runs through the center of the investigated area

1.4 Flood Hazard Mapping

The project is located within the mapped IDNR Floodplain.

2.0 FIELD RECONNAISSANCE

The investigated area was visited by Brooke Fox and Nakayla Krahn, Environmental Scientists of B&N on May 29, 2024, to observe and document existing conditions, and to identify and evaluate potentially jurisdictional “Waters of the U.S.” (WOTUS) and other aquatic resources. Weather conditions were a high of 70°F and the last recorded precipitation was 0.35 in. on May 27, 2024. Findings of the field investigation are summarized below.

2.1 Streams

One stream was identified within the investigated area: Race Run. It displayed a bed, bank, an ordinary high-water mark (OHWM), and intermittent flow; therefore, meeting each of the criteria which define a potentially jurisdictional tributary. Stream characteristics are summarized below:

Race Run: Race Run is an intermittent stream that flows approximately 178 ft. in a south-north direction through the investigated area. StreamStats reports the estimated upstream drainage area of Race Run as 1.23 mi.². Based on field observations, Race Run appears to be intermittent, as it has a defined bed and bank, prominent OHWM, and appears to experience seasonal flow. It is dominated by mud and silt substrates, has moderate instream cover, and has riffles and pools. The OHWM of Race Run (40.433010, -84.943080) was measured downstream and away from the influence of the bridge, measuring 11.1 feet in width and 1 foot in depth. The quality of Race Run within the investigated area would be considered poor due to its intermittent flow, receives runoff from SR 26, lack of sinuosity, uniform substrate, and presence of field tile drainage. Characteristics of Race Run are summarized in **Table 3**.

Given its hydrological connection to the Wabash River via Salamonie River, a Traditional Navigable Waterway (TNW), it is likely a jurisdictional WOTUS.

Table 3
Stream Summary Table

Water Feature Name	Photos	Lat / Long	OHWM Width/ Depth (ft.)	USGS Blue-line? Type?	Upstream Drainage Area	Linear Feet in Investigated Area	Riffles? Pools?	Quality	Substrate	Likely Water of the U.S.?
Race Run	9-12, 25-28	40.433010, -84.943080	11.1 / 1	Yes, Intermittent	1.23 mi ²	178	Yes	Poor	Mud/Silt	Yes

2.2 Wetlands

The investigated area was examined for the presence of wetlands. Data points (DPs) were established to characterize and delineate potential wetlands, and adjacent upland communities. Vegetation, hydrology, and soil data were collected at each sample point in accordance with applicable U.S. Army Corps of Engineers (USACE) Regional Supplement delineation protocols (*Midwest Regional Supplement*).

Four DPs were collected based on the presence of hydrophytic vegetation and are summarized in **Table 4**. However, none of the DPs met all three criteria necessary to be classified as wetlands. Therefore, no wetlands were identified within the investigated area.

Table 4
Wetland Summary Table

Wetland ID	Type	Acreage	Quality	Photo ID	DP ID	Lat/Long	Dom. Vegetation	Hydric Soil Indicator(s)	Hydrology Indicators	Within Wetland?	Notes
N/A	N/A	N/A	N/A	2-3	DP1	40.432879, -84.943677	<i>Morus rubra</i> , <i>Celtis occidentalis</i> , <i>Juglans nigra</i> , <i>Lonicera maacki</i> , <i>Aesculus glabra</i> , <i>Geum canadense</i> , & <i>Carex davisii</i>	N/A	N/A	No	N/A
N/A	N/A	N/A	N/A	6-7	DP2	40.432885, -84.943217	<i>Phalaris arundinacea</i>	N/A	D5	No	N/A
N/A	N/A	N/A	N/A	15-16	DP3	40.432952, -84.942205	<i>Gleditsia triacanthos</i> , <i>Asimina triloba</i> , <i>Celtis occidentalis</i> , <i>Lysimachia nummularia</i> , <i>Phalaris arundinacea</i> , & <i>Toxicodendron radicans</i>	N/A	D2 & D5	No	N/A
N/A	N/A	N/A	N/A	21-22	DP4	40.432655, -84.942125	<i>Carex lacustris</i> , <i>Cirsium altissimum</i> , <i>Ipomoea hederacea</i> , & <i>Phalaris arundinacea</i>	F6	D5	No	N/A

2.3 Open Waters

No ponds, lakes, or other open water features were observed in the investigated area.

2.4 Other Features

One Roadside Ditch (RSD1) is located within the investigated area. Ditch characteristics and locations are discussed below:

RSD1: RSD1 is a partially concrete lined ditch located in the southwest quadrant of the bridge and is approximately 192 ft. The ditch conveys drainage from SR 26 to Race Run. Since RSD1 lacks a defined bed, bank, and OHWM, it is classified as a ditch and is likely non-jurisdictional.

2.5 Wildlife Evidence and Concerns

The bridge was surveyed for use or potential use for wildlife crossings. Sediment buildup was present along the bridge (Photo 11 & 28) and likely serves as wildlife passage under SR 26 for both large and small mammals. While no evidence of bats using the bridge was found, there was evidence of bird activity.

3.0 CONCLUSION

Based on the findings of this investigation, B&N concludes that there is one potentially jurisdictional stream (Race Run) located within the investigated area. The onsite roadside ditch is likely non-jurisdictional. No wetlands, ponds, lakes, or other water features were observed in the investigated area.

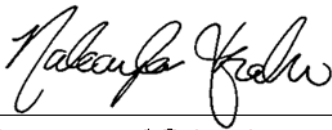
These waterways and wetlands are likely *Waters of the U.S.* Every effort should be taken to avoid and minimize impacts to the waterway and wetlands. If impacts are necessary, then mitigation may be required. The INDOT Environmental Services Division should be contacted immediately if impacts will occur. The final determination of jurisdictional waters is ultimately made by the U.S. Army Corps of Engineers. This report is our best judgement based on the guidelines set forth by the Corps.

4.0 ACKNOWLEDGEMENT

The waters determination has been prepared based on the best available information interpreted in the light of the investigator's training, experience, and professional judgement in conformance with the 1987 Corps of Engineers Wetlands Delineation Manual, the appropriate regional supplement, the USACE Jurisdictional Determination Form Instructional Guidebook, and other appropriate agency guidelines.

Respectfully,

Nakayla Krahn



11/21/2024

Environmental Scientist
Burgess & Niple, Inc.

ATTACHMENTS

Appendix A

Attachment 1	Project Location Map
Attachment 2	USGS Topographic Map
Attachment 3	NHD & NWI Map
Attachment 4	IDNR Floodway Map
Attachment 5	USGS StreamStats
Attachment 6	NRCS Hydric Soil Map
Attachment 7	Aerial & Water Resource Map
Attachment 8	Feature & Photo Location Map

Duplicate attachments and datasheets were removed to reduce file size and avoid redundancy. Duplicate attachments can be found in Appendix B.

Appendix B

Project Photographs

Appendix C

Wetland Determination Data Forms

Appendix D

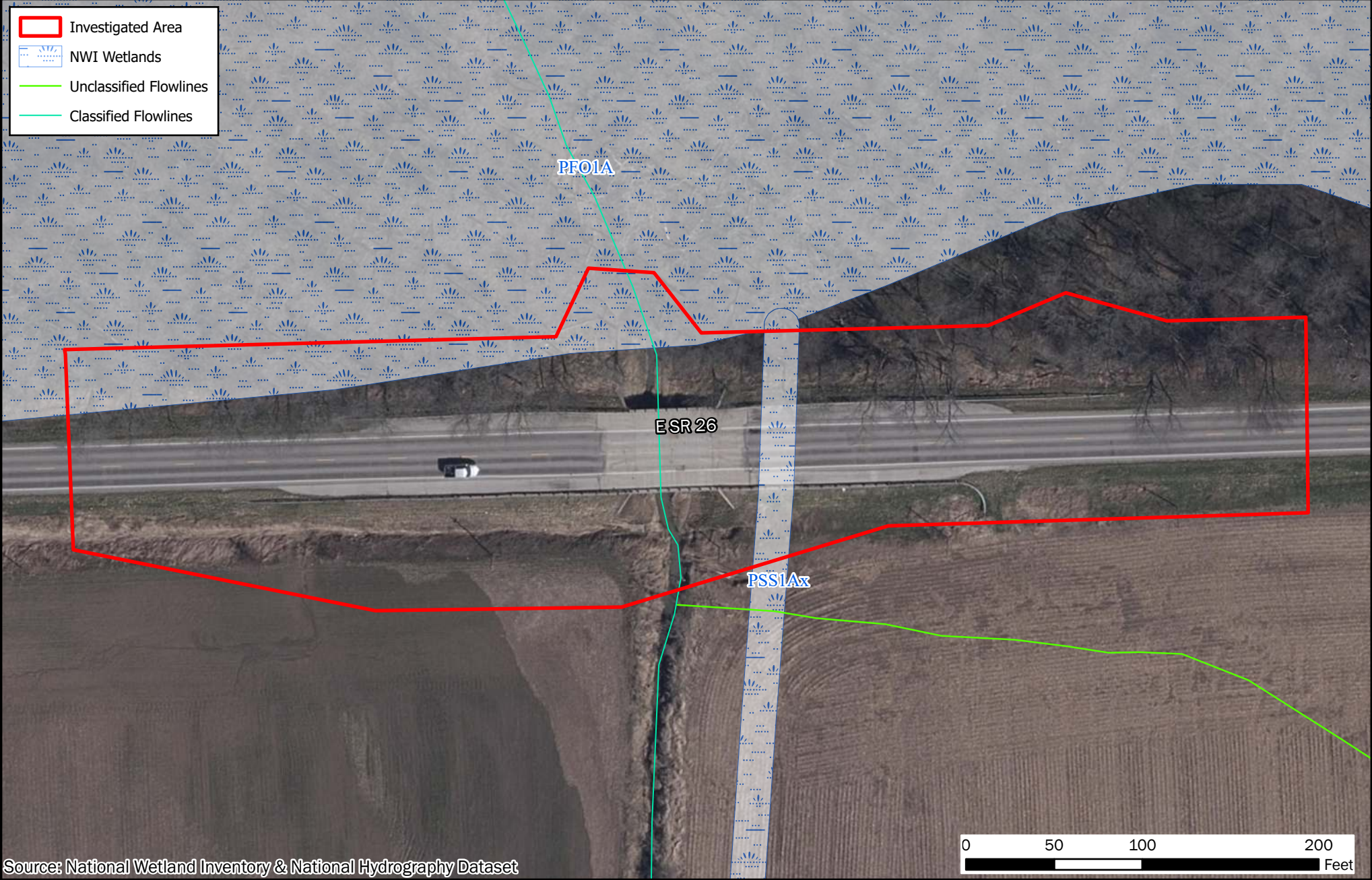
Preliminary Jurisdictional Determination Form

Investigated Area

NWI Wetlands

Unclassified Flowlines

Classified Flowlines



A3 - NHD & NWI Map

11/21/2024

BURGESS & NIPLE

Engineers ■ Environmental Scientists

Author: krah

Des No. 2200586

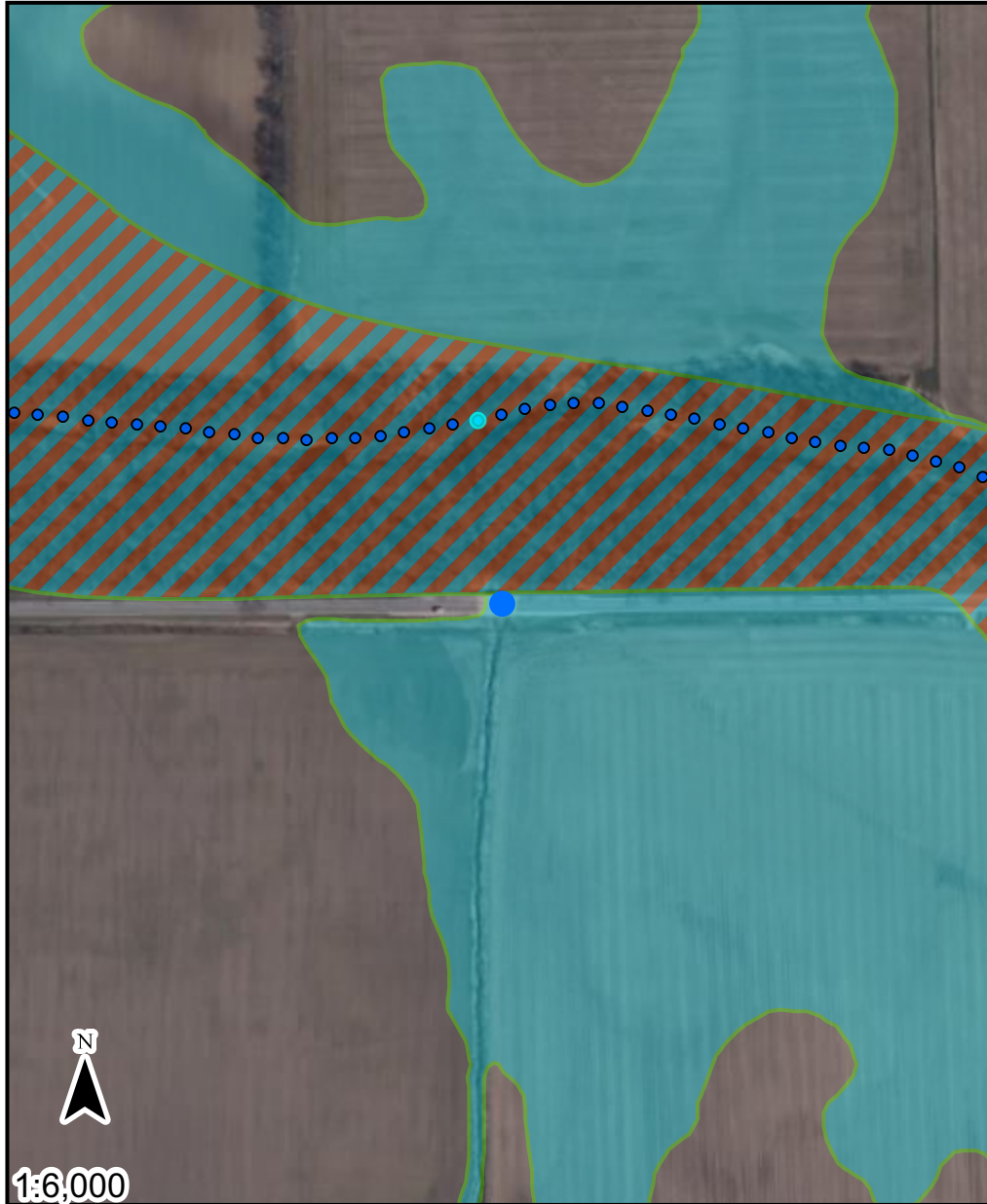
SR 26 over Race Run, Bridge Replacement

Des No. 2200586

near Portland, Jay Co., Indiana

N

F-8



- Point of Interest
- Base Flood Elevation Point
- POI
- 1.0
- FEMA Zone AE Floodway; FEMA Administrative Floodway
- FEMA Zone AE
- Not Mapped

Long: -84.94295467393039

Lat: 40.432767186218406

The information provided below is based on the point of interest shown in the map above.

County: **Jay**

Stream Name:

Salamonie River

Approximate Ground Elevation: **900.5 feet (NAVD88)**

Base Flood Elevation: **909.4 Feet (NAVD88)**

Drainage Area: **Not Available**

Best Available Flood Hazard Zone: **FEMA Zone AE**

National Flood Hazard Zone: **FEMA Zone AE**

Is a Flood Control Act permit from the DNR needed for this location? **See following pages**

Is a local floodplain permit needed for this location? **yes-**

Floodplain Administrator: **John Hemmelgarn, Director Building/Planning**

Community Jurisdiction: **Jay County, County proper**

Phone: **(260) 726-6904**

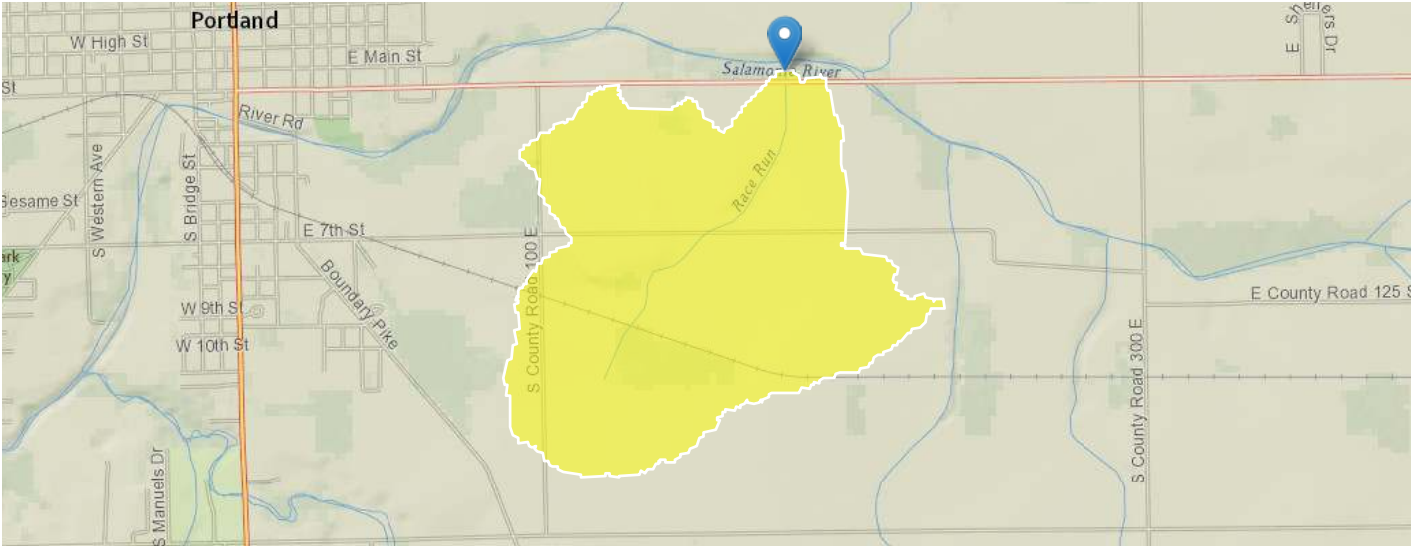
Email: **jpbp.jhemmel@gmail.com**

US Army Corps of Engineers District: **Louisville**

Date Generated: 10/14/2024

A5 - SR 26 over Race Run - StreamStats Report

Region ID: IN
Workspace ID: IN20240909155129802000
Clicked Point (Latitude, Longitude): 40.43321, -84.94322
Time: 2024-09-09 11:52:03 -0400



Collapse All

Basin Characteristics

Parameter Code	Parameter Description	Value	Unit
DRNAREA	Area that drains to a point on a stream	1.233	square miles

USGS Data Disclaimer: Unless otherwise stated, all data, metadata and related materials are considered to satisfy the quality standards relative to the purpose for which the data were collected. Although these data and associated metadata have been reviewed for accuracy and completeness and approved for release by the U.S. Geological Survey (USGS), no warranty expressed or implied is made regarding the display or utility of the data for other purposes, nor on all computer systems, nor shall the act of distribution constitute any such warranty.

USGS Software Disclaimer: This software has been approved for release by the U.S. Geological Survey (USGS). Although the software has been subjected to rigorous review, the USGS reserves the right to update the software as needed pursuant to further analysis and review. No warranty, expressed or implied, is made by the USGS or the U.S. Government as to the functionality of the software and related material nor shall the fact of release constitute any such warranty. Furthermore, the software is released on condition that neither the USGS nor the U.S. Government shall be held liable for any damages resulting from its authorized or unauthorized use.

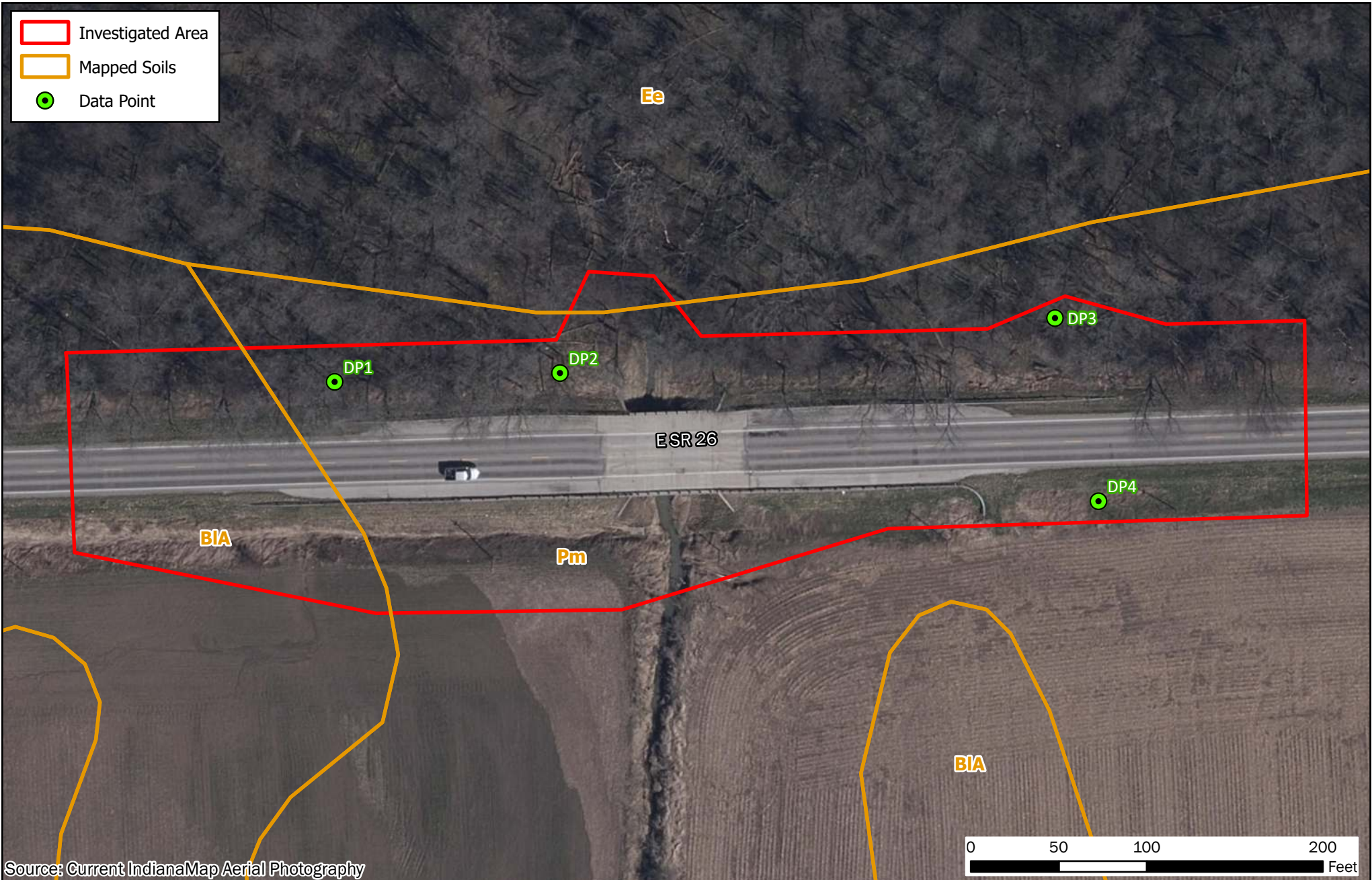
USGS Product Names Disclaimer: Any use of trade, firm, or product names is for descriptive purposes only and does not imply endorsement by the U.S. Government.

Application Version: 4.23.0
StreamStats Services Version: 1.2.22
NSS Services Version: 2.2.1

Investigated Area

Mapped Soils

Data Point



A6 - NRCS Hydric Soil Map

11/21/2024

BURGESS & NIPLE

Engineers ■ Environmental Scientists

SR 26 over Race Run, Bridge Replacement

Des No. 2200586

near Portland, Jay Co., Indiana

N

F-11

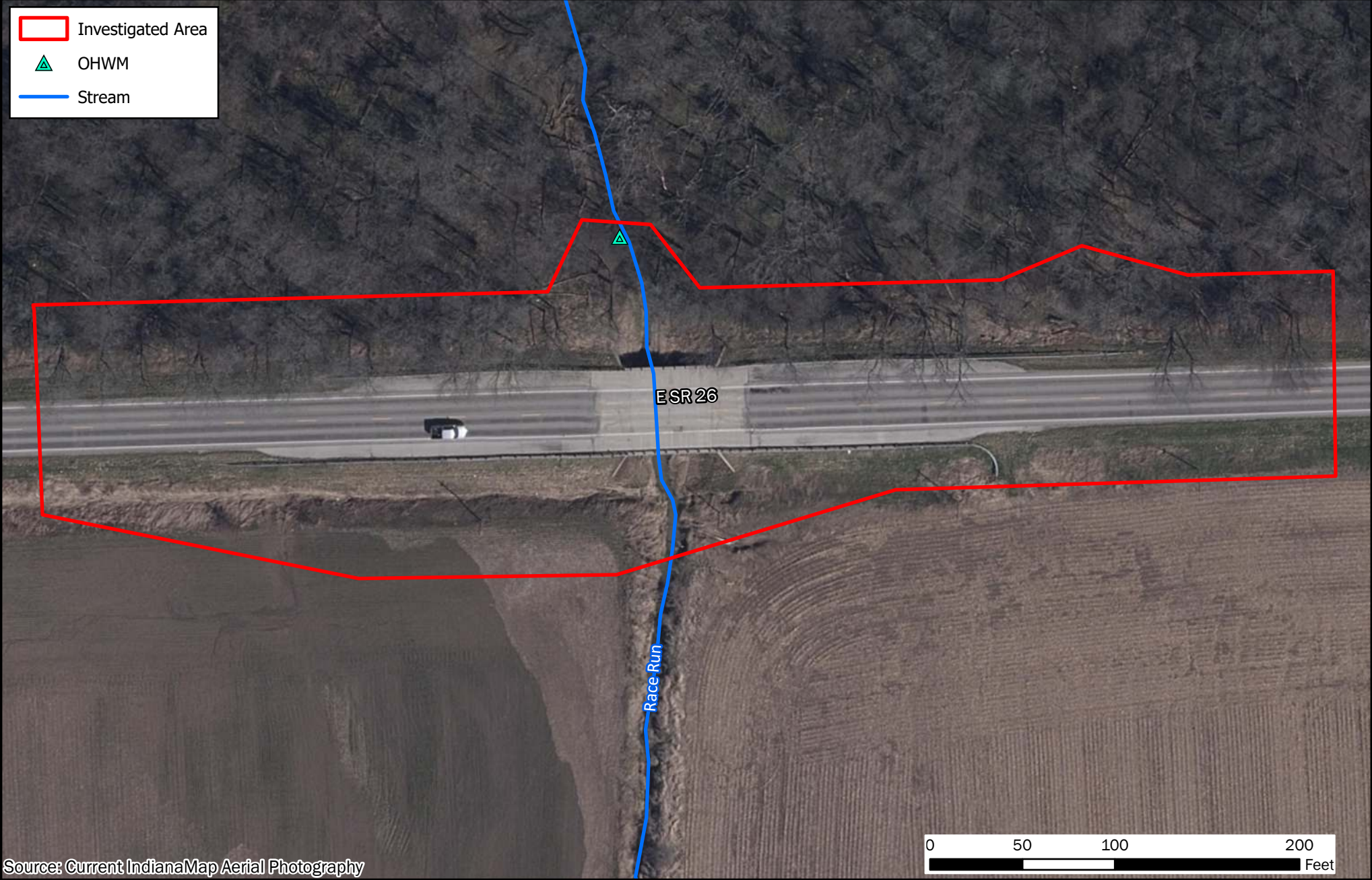
Hydric Rating by Map Unit

Map unit symbol	Map unit name	Rating	Acres in AOI	Percent of AOI
BIA	Blount-Glynwood, thin solum complex, 0 to 3 percent slopes	5	0.4	19.9%
Ee	Eel clay loam, frequently flooded	5	0.0	0.9%
Pm	Pewamo silty clay, 0 to 2 percent slopes	91	1.7	79.2%
Totals for Area of Interest			2.2	100.0%

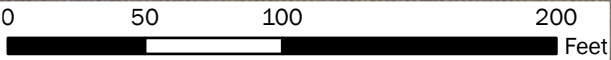
Investigated Area

OHWM

Stream



Source: Current IndianaMap Aerial Photography



A7 - Aerial & Water Resource Map

<div> <div>11/21/2024</div> <div> <div>BURGESS & NIPLE</div> <div>Engineers ■ Environmental Scientists</div> </div> </div>	<div>SR 26 over Race Run, Bridge Replacement</div> <div>Des No. 2200586</div> <div>near Portland, Jay Co., Indiana</div>	<div> <div>N</div> <div>▲</div> </div>
	<div> <div>Author: krahn Des No. 2200586</div> <div>Appendix F - Water Resources</div> </div>	

Investigated Area

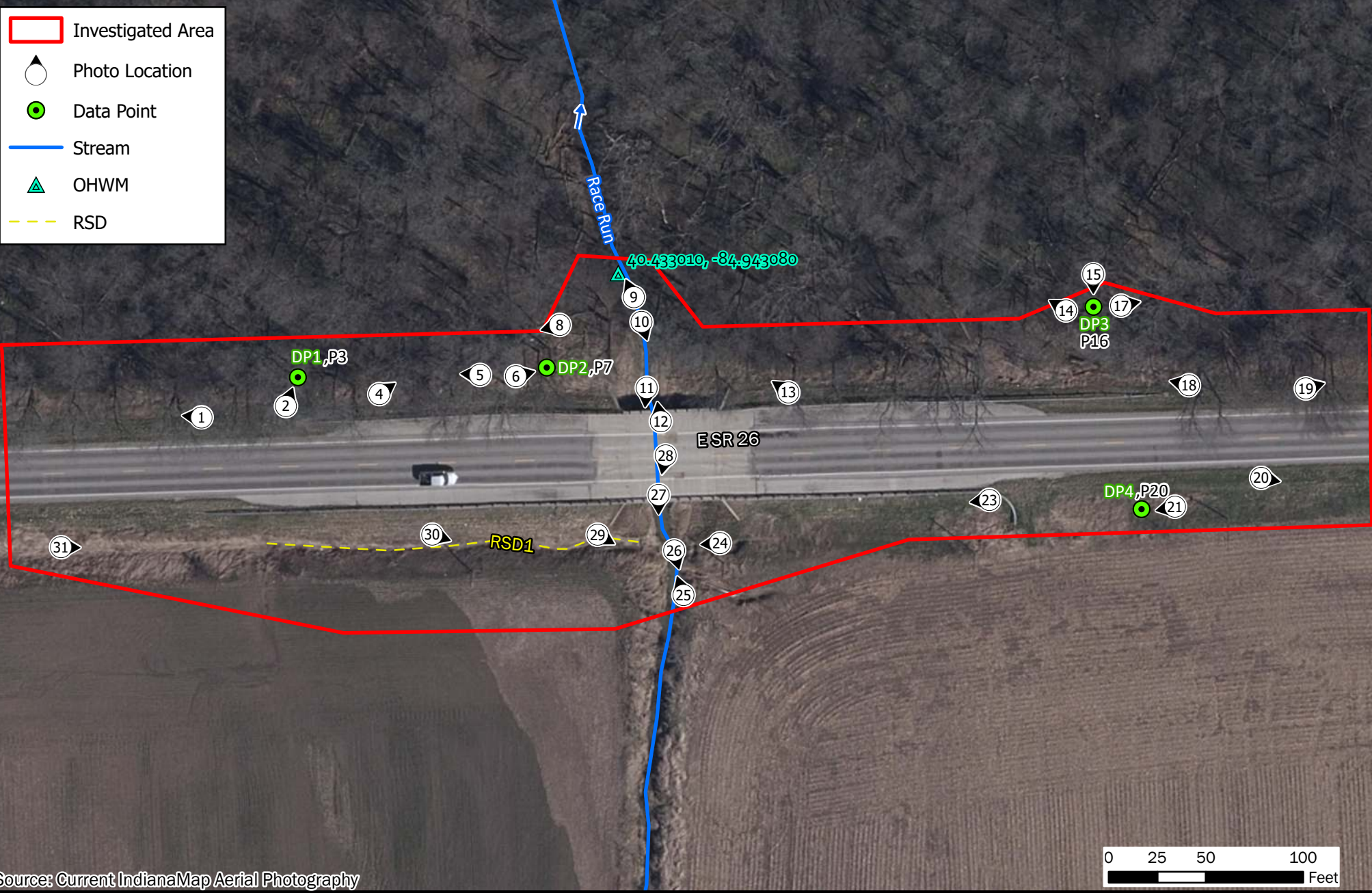
Photo Location

Data Point

Stream

OHWM

RSD



A8 - Feature & Photo Location Map

<div> <div>BURGESS & NIPLE</div> <div>Engineers ■ Environmental Scientists</div> </div>	<div>SR 26 over Race Run, Bridge Replacement</div> <div>Des No. 2200586</div> <div>near Portland, Jay Co., Indiana</div>	<div>N</div> <div>▲</div>



P1 - Looking west along the westbound lane of SR 26 in the NW quad.



P2 - Looking northeast at the upland area surrounding DP1 in the NW quad.



P3 - Looking at the DP1 soil profile.



P4 - Looking northeast along the westbound lane of SR 26 in the NW quad.



P5 - Looking west along the westbound lane of SR 26 in the NW quad.



P6 - Looking east at the upland area surrounding DP2 in the NW quad.



P7 - Looking at the DP2 soil pit.



P8 - Looking west towards the forested area located adjacent to the westbound lane of SR 26 in the NW quad.



P9 - Looking northwest (downstream) along Race Run.



P10 - Looking southeast (upstream) along Race Run at BR 026-38-03431.



P11 - Looking south (upstream) along Race Run under BR 026-38-03431.



P12 - Looking north (downstream) along Race Run from BR 026-38-03431.



P13 - Looking northwest along the westbound lane of SR 26 in the NE quad, towards Race Run.



P14 - Looking northwest of where DP3 was taken in the NE quad.



P15 - Looking south at the upland area surrounding DP3 in the NE quad.



P16 – Looking at the DP3 soil pit.



P17 - Looking east of where DP3 was taken in the NE quad.



P18 - Looking west along the westbound lane of SR 26 in the NE quad.



P19 - Looking east along the westbound lane of SR 26 in the NE quad.



P20 - Looking east along the eastbound lane of SR 26 in the SE quad.



P21 - Looking west at the upland area surrounding DP4 in the SE quad.



P22 - Looking at the DP4 soil pit.



P23 - Looking west along the eastbound lane of SR 26 in the SE quad.



P24 - Looking west along the eastbound lane of SR 26 in the SE quad, towards Race Run.



P25 - Looking northwest (downstream) along Race Run at BR 026-38-03431.



P26 - Looking south (upstream) along Race Run.



P27 - Looking south (upstream) along Race Run from BR 026-38-03431.



P28 - Looking south (upstream) along Race Run under BR 026-38-03431.



P29 - Looking east at the end of RSD1 along the eastbound lane of SR 26 in the SW quad.



P30 - Looking east along RSD1, along the eastbound lane of SR 26 in the SW quad.



P31 - Looking east along the eastbound lane of SR 26 in the SW quad.

Appendix 2 - PRELIMINARY JURISDICTIONAL DETERMINATION (PJD) FORM

BACKGROUND INFORMATION

A. REPORT COMPLETION DATE FOR PJD: 11/5/2024

B. NAME AND ADDRESS OF PERSON REQUESTING PJD: Nakayla Krahn, Burgess & Niple, Inc., 251 N Illinois St # 920, Indianapolis, IN 46204

C. DISTRICT OFFICE, FILE NAME, AND NUMBER:

D. PROJECT LOCATION(S) AND BACKGROUND INFORMATION:

The proposed project (Des. No. 2200586) is located on SR 26, over Race Run (also known as Bolen Ditch) approximately 1.84 miles east of US 27, near the City of Portland, Jay County, Indiana. The project will include a full bridge replacement, shoulder reconstruction, and placement of riprap on the spill slopes. Additionally, riprap turnouts will be placed on the side slopes at each end of the bridge.

(USE THE TABLE BELOW TO DOCUMENT MULTIPLE AQUATIC RESOURCES AND/OR AQUATIC RESOURCES AT DIFFERENT SITES)

State: Indiana

County/parish/borough: Jay

City: Portland, IN

Center coordinates of site (lat/long in degree decimal format):

Lat.: 40.432784

Long.: -84.942976

Universal Transverse Mercator: 16T 674479 4477825

Name of nearest waterbody: Race Run

E. REVIEW PERFORMED FOR SITE EVALUATION (CHECK ALL THAT APPLY):

☐ Office (Desk) Determination. Date:

☐ Field Determination. Date(s):

**TABLE OF AQUATIC RESOURCES IN REVIEW AREA WHICH “MAY BE” SUBJECT TO
REGULATORY
JURISDICTION.**

Site number	Latitude (decimal degrees)	Longitude (decimal degrees)	Estimated amount of aquatic resource in review area (acreage and linear feet, if applicable)	Type of aquatic resource (i.e., wetland vs. non- wetland waters)	Geographic authority to which the aquatic resource “may be” subject (i.e., Section 404 or Section 10/404)
Race Run	40.433010	-84.943080	178 LF / 0.045 Acre	Non-wetland	Section 404

- 1) The Corps of Engineers believes that there may be jurisdictional aquatic resources in the review area, and the requestor of this PJD is hereby advised of his or her option to request and obtain an approved JD (AJD) for that review area based on an informed decision after having discussed the various types of JDs and their characteristics and circumstances when they may be appropriate.
- 2) In any circumstance where a permit applicant obtains an individual permit, or a Nationwide General Permit (NWP) or other general permit verification requiring “pre- construction notification” (PCN), or requests verification for a non-reporting NWP or other general permit, and the permit applicant has not requested an AJD for the activity, the permit applicant is hereby made aware that: (1) the permit applicant has elected to seek a permit authorization based on a PJD, which does not make an official determination of jurisdictional aquatic resources; (2) the applicant has the option to request an AJD before accepting the terms and conditions of the permit authorization, and that basing a permit authorization on an AJD could possibly result in less compensatory mitigation being required or different special conditions; (3) the applicant has the right to request an individual permit rather than accepting the terms and conditions of the NWP or other general permit authorization; (4) the applicant can accept a permit authorization and thereby agree to comply with all the terms and conditions of that permit, including whatever mitigation requirements the Corps has determined to be necessary; (5) undertaking any activity in reliance upon the subject permit authorization without requesting an AJD constitutes the applicant’s acceptance of the use of the PJD; (6) accepting a permit authorization (e.g., signing a proffered individual permit) or undertaking any activity in reliance on any form of Corps permit authorization based on a PJD constitutes agreement that all aquatic resources in the review area affected in any way by that activity will be treated as jurisdictional, and waives any challenge to such jurisdiction in any administrative or judicial compliance or enforcement action, or in any administrative appeal or in any Federal court; and (7) whether the applicant elects to use either an AJD or a PJD, the JD will be processed as soon as practicable. Further, an AJD, a proffered individual permit (and all terms and conditions contained therein), or individual permit denial can be administratively appealed pursuant to 33 C.F.R. Part 331. If, during an administrative appeal, it becomes appropriate to make an official determination whether geographic jurisdiction exists over aquatic resources in the review area, or to provide an official delineation of jurisdictional aquatic resources in the review area, the Corps will provide an AJD to accomplish that result, as soon as is practicable. This PJD finds that there “*may be*” waters of the U.S. and/or that there “*may be*” navigable waters of the U.S. on the subject review area, and identifies all aquatic features in the review area that could be affected by the proposed activity, based on the following information:

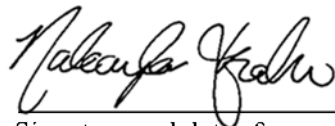
SUPPORTING DATA. Data reviewed for PJD (check all that apply)

Checked items should be included in subject file. Appropriately reference sources below where indicated for all checked items:

- ☒ Maps, plans, plots or plat submitted by or on behalf of the PJD requestor:
Maps: Indiana GIO Library, IndianaMap, USGS, NWI
- ☒ Data sheets prepared/submitted by or on behalf of the PJD requestor.
☐ Office concurs with data sheets/delineation report.
☐ Office does not concur with data sheets/delineation report. Rationale: _____.
- ☐ Data sheets prepared by the Corps: _____.
- ☐ Corps navigable waters' study: _____.
- ☒ U.S. Geological Survey Hydrologic Atlas: USGS TNM-NHD: Data Refreshed July 2024.
☒ USGS NHD data.
☐ USGS 8 and 12 digit HUC maps.
- ☒ U.S. Geological Survey map(s). Cite scale & quad name: Portland / 1:24,000.
- ☒ Natural Resources Conservation Service Soil Survey. Citation: NRCS Web Soil Survey: Jay County.
- ☒ National wetlands inventory map(s). Cite name: USFWS NWI data: Jay County.
- ☐ State/local wetland inventory map(s): _____.
- ☒ FEMA/FIRM maps: FEMA/FIRM Jay County, Indiana.
- ☐ 100-year Floodplain Elevation is: _____. (National Geodetic Vertical Datum of 1929)
- ☒ Photographs: ☒ Aerial (Name & Date): Jay County / NAIP Imagery 2024.
or ☒ Other (Name & Date): Photos taken: May 29, 2024.
- ☐ Previous determination(s). File no. and date of response letter: _____.
- ☐ Other information (please specify): _____.

IMPORTANT NOTE: The information recorded on this form has not necessarily been verified by the Corps and should not be relied upon for later jurisdictional determinations.

Signature and date of
Regulatory staff member
completing PJD

 11/5/2024

Signature and date of
person requesting PJD
(REQUIRED, unless obtaining

¹ Districts may establish timeframes for requestor to return signed PJD forms. If the requestor does not respond within the established time frame, the district may presume concurrence and no additional follow up is necessary prior to finalizing an action.

Appendix G

Public Involvement

May 26, 2023

Notice of Survey

RE: SR 26 over Bolden Ditch, Des. No. 2200586 in Jay County

Dear Property Owner,

Certified Engineering, Inc. has been selected by INDOT for field survey of the above referenced project. Our information indicates that you own property near the above proposed project. Certified Engineering, Inc. will be performing a survey of the project area in the near future. It may be necessary for representatives from Certified Engineering, Inc. to enter your property to complete this work. This is permitted by law per Indiana Code (IC) 8-23-7-26. Anyone performing this type of work has been instructed to identify him or herself, if you are available, before they enter your property. If you no longer own this property or it is currently occupied by someone else, please let us know the name of the new owner or occupant so that we can contact them about the survey.


At this stage, we generally do not know what effect, if any, the project may eventually have on your property. If we later determine that your property is involved, you will be contacted with additional information.

The survey is needed for this project. Please be assured of our sincere desire to cause you as little inconvenience as possible during this survey.

If any problems do occur, please contact Jason Hesler of Certified Engineering, Inc. at (317) 546-1599 or at 3939 Millersville Road, Indianapolis, Indiana 46205. Thank you in advance for your cooperation.

Sincerely,

Certified Engineering, Inc.



Jason R. Hesler, PE, PLS

Appendix H

Air Quality

Indiana Department of Transportation (INDOT)
State Preservation and Local Initiated Projects FY 2024 - 2028

SPONSOR	CONTR ACT # / LEAD DES	STIP NAME	ROUTE	WORK TYPE	DISTRICT	MILES	FEDERAL CATEGORY	Total Cost of Project*	PROGRAM	PHASE	FEDERAL	MATCH	2024	2025	2026	2027	2028
Indiana Department of Transportation	44590 / 2200520	Init.	SR 1	Small Structures & Drains Construction	Greenfield	8.972	STBG	\$6,452,000.00	Bridge Construction	CN	\$4,248,000.00	\$1,062,000.00				\$5,310,000.00	
									Bridge Consulting	PE	\$849,600.00	\$212,400.00	\$1,062,000.00				
									Bridge ROW	RW	\$64,000.00	\$16,000.00		\$80,000.00			
Performance Measure Impacted: Safety																	
Location: 0.19 mi N of SR 67 to 2.66 mi N of SR 26 N junct (Br over Salamonie River)																	
Comments:Include DES 2200520																	
Indiana Department of Transportation	44590 / 2200520	A 09	SR 1	Small Structures & Drains Construction	Greenfield	8.972	STBG	\$8,195,783.00	Bridge Construction	CN	\$381,600.00	\$95,400.00				\$477,000.00	
									Bridge Consulting	PE	\$1,862,984.00	\$465,746.00	\$1,062,000.00	\$1,266,730.00			
Performance Measure Impacted: Safety																	
Location: 0.19 mi N of SR 67 to 2.66 mi N of SR 26 N junct (Br over Salamonie River)																	
Comments:Add PE and add CN.																	
Indiana Department of Transportation	44590 / 2200520	M 45	SR 1	Small Structures & Drains Construction	Greenfield	8.972	STBG	\$6,452,000.00	Bridge ROW	RW	\$0.00	\$0.00		(\$80,000.00)	\$80,000.00		
Performance Measure Impacted: Safety																	
Location: 0.19 mi N of SR 67 to 2.66 mi N of SR 26 N junct (Br over Salamonie River)																	
Comments:Move RW from FY 25 to FY 26																	
Indiana Department of Transportation	44619 / 2200586	Init.	SR 26	Bridge Replacement	Greenfield	0	STBG	\$1,540,000.00	Bridge Construction	CN	\$1,083,200.00	\$270,800.00				\$1,354,000.00	
									Bridge ROW	RW	\$40,000.00	\$10,000.00		\$50,000.00			
Performance Measure Impacted: Bridge Condition																	
Location: Bridge OVER Bolden Ditch, 1.84 miles East of US 27																	
Comments:Include DES 2200586																	
Indiana Department of Transportation	44619 / 2200586	M 45	SR 26	Bridge Replacement	Greenfield	0	STBG	\$1,540,000.00	Bridge ROW	RW	\$0.00	\$0.00		(\$50,000.00)	\$50,000.00		
Performance Measure Impacted: Bridge Condition																	
Location: Bridge OVER Bolden Ditch, 1.84 miles East of US 27																	
Comments:Move RW from FY 25 to FY 26																	
Indiana Department of Transportation	45196 / 2300745	A 06	SR 26	Bridge Thin Deck Overlay	Greenfield	0	STBG	\$1,555,239.00	Bridge Consulting	PE	\$360,000.00	\$90,000.00		\$450,000.00			
Performance Measure Impacted: Bridge Condition																	
Location: Bridge replacement in Jay and Randolph County																	
Comments:DES includes 2300745, 2300748, and 2300817 Add PE.																	

*Estimated Costs left to Complete Project column is for costs that may extend beyond the four years of a STIP. This column is not fiscally constrained and is for information purposes.

**Federal Transit
Administration**
Region V
200 West Adams St., Suite 320
Chicago, IL 60606-5253



**U.S. Department
of Transportation**

Federal Highway Administration
Indiana Division
575 N. Pennsylvania St., Rm 254
Indianapolis, IN 46204-1576

September 1, 2023

Mr. Michael Smith
Commissioner
Indiana Department of Transportation
100 N Senate Ave. N955
Indianapolis, IN 46204

SUBJECT: Indiana FY2024-2028 STIP Approval and Associated Federal Planning Finding

Dear Mr. Smith:

The Federal Highway Administration (FHWA) and the Federal Transit Administration (FTA) have completed our review of the FY2024-2028 Indiana Statewide Transportation Improvement Program (INSTIP), which was submitted by the Indiana Department of Transportation (INDOT) request letter dated August 23, 2023.

Based on our review of the information provided, certifications of the Statewide and Metropolitan transportation planning processes for and within the state of Indiana, and our participation in those transportation planning processes (including planning certification reviews conducted in Transportation Management Areas), FHWA and FTA are jointly approving the FY2024-2028 STIP, including the Metropolitan Planning Organization (MPO) Transportation Improvement Programs (TIPs) incorporated into the STIP by reference, subject to the corrective actions identified in the attached Federal Planning Finding (FPF) report. FHWA and FTA consider the projects in the 5th year for informational purposes only, and our approval does not exceed four years per 23 CFR 450.220(c).

FHWA and FTA are required under 23 CFR 450.220(b) to document and issue an FPF in conjunction with the approval of the FY2024-2028 STIP. At a minimum, the FPF verifies that the development of the STIP is consistent with the provisions of both the Statewide and Metropolitan transportation planning requirements. FHWA and FTA find that the Indiana FY2024-2028 STIP substantially meets the transportation planning requirements and are approving the STIP subject to the corrective actions outlined in the FPF. This approval is effective September 1, 2023 and is given with the understanding that an eligibility determination of individual projects for funding must be met, and INDOT must ensure the satisfaction of all administrative and statutory requirements, as well as address the corrective actions outlined in the attached report.

If you have questions or need additional information concerning our approval and the FPF, please contact Ms. Erica Tait of the FHWA Indiana Division at (317) 226-7481, or by email at erica.tait@dot.gov, or Mr. Tony Greep of the FTA Region 5 Office at (312) 353-1646, or by email at anthony.greep@dot.gov.

Sincerely,

**KELLEY
BROOKINS** Digitally signed by
KELLEY BROOKINS
Date: 2023.08.31
17:33:15 -05'00'

Kelley Brookins
Regional Administrator
FTA Region V

Sincerely,

**JERMAINE
R HANNON** Digitally signed by
JERMAINE R HANNON
Date: 2023.09.01
11:46:31 -04'00'

Jermaine R. Hannon
Division Administrator
FHWA Indiana Division



INDIANA DEPARTMENT OF TRANSPORTATION

100 North Senate Avenue
Room N758-Executive Office
Indianapolis, Indiana 46204

PHONE: (855) 463-6848

Eric Holcomb, Governor
Michael Smith, Commissioner

August 28, 2023

Mr. Jermaine R. Hannon, Division Administrator
FHWA Indiana Division
575 North Pennsylvania St., Room 254
Indianapolis, IN 46204

Ms. Kelley Brookins, Regional Administrator
FTA Region 5
200 West Adams St.
Suite 320
Chicago, IL 60606-5253

Dear Mr. Hannon /Ms. Brookins:

The Indiana Department of Transportation is pleased to submit its FY 2024-2028 Statewide Transportation Improvement Program (STIP) for review and approval by your offices.

Included in the final submitted document is a listing of the state's expansion/preservation and local small urban and rural and rural transit projects. The following Metropolitan Planning Organization TIPs will be included in the FY 2024-2028 STIP by reference.

Area Plan Commission of Tippecanoe County (APCTC)	FY 2024-2028
<ul style="list-style-type: none">https://www.tippecanoe.in.gov/DocumentCenter/View/40728/FY-2024-2028-TIP-including-0-amendments	
Bloomington-Monroe County Metropolitan Planning Organization (BMCMPPO)	FY 2024-2028
<ul style="list-style-type: none">https://bloomington.in.gov/sites/default/files/2023-08/BMCMPPO%20FY%202024%20-%202028%20TIP%20-%202006-30-23%20-%20ADOPTED%20FINAL.pdf	
Columbus Area Metropolitan Planning Organization (CAMPO)	FY 2024-2028
<ul style="list-style-type: none">https://www.columbus.in.gov/planning/tip/	
Delaware-Muncie Metropolitan Plan Commission (DMMPC)	FY 2022-2025
<ul style="list-style-type: none"><i>Including Amendments/modifications through 2/14/23</i>https://www.co.delaware.in.us/egov/documents/1692987897_47263.pdf	
Evansville Metropolitan Planning Organization (EMPO)	FY 2024-2028
<ul style="list-style-type: none">http://www.evansvillempo.com/Docs/TIP/TIP_2024-2028/TIP_2024-2028.pdf	
Kokomo-Howard County Governmental Coordinating Council (KHCGCC)	FY 2022-2026
<ul style="list-style-type: none"><i>Including Amendments/modification through 7/28/23</i>https://www.kokomompo.com/project/tip-2020-2024/	

www.in.gov/dot/

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Kentuckiana Regional Planning and Development Agency (KIPDA)	FY 2023-2026
• https://www.kipda.org/wp-content/uploads/2023/05/FY2023-TIP-FINAL-5-25.pdf	
Indianapolis Metropolitan Planning Organization (IMPO)	FY 2024-2027
• https://www.indympo.org/whats-underway/irtip	
Michiana Area Council of Governments (MACOG)	FY 2024-2028
• http://www.macog.com/docs/transportation/tip/approved/fy2028tip_projects.pdf	
Madison County Council of Governments (MCCOG)	FY 2022-2026
• <i>Including Amendments/modifications through 7/28/23</i>	
• https://irp.cdn-website.com/65a760a0/files/uploaded/TIP%202022-2026%20-%20updated%205-1-23.pdf	
Northeastern Indiana Regional Coordinating Council (NIRCC)	FY 2024-2028
• https://www.nircc.com/uploads/1/2/9/8/129837621/final_2024-2028_tip_5-25-23.pdf	
Northwestern Indiana Regional Planning Commission (NIRPC)	FY 2022-2026
• <i>Including Amendments/modifications through 7/25/23</i>	
• https://nirpc.org/2040-plan/mobility/transportation-improvement-program/	
Ohio-Kentucky-Indiana Regional Council of Governments (OKI)	FY 2024-2027
• https://www.oki.org/transportation-planning/transportation-improvement-program-tip/	
Terre Haute Area Metropolitan Planning Organization (THAMPO)	FY 2024-2028
• https://www.terrehautempo.com/images/THAMPO_2024_2028_AdoptionTIP.pdf	

In addition, INDOT has expanded our public involvement process by taking advantage of virtual meeting techniques and allowing accessibility to online documents, materials, virtual meeting registration, recorded virtual meetings, and comment forms. INDOT also leveraged our planning partner contacts (MPOs, RPOs, LTAP), social media, and notifications sent to local libraries, housing authorities, senior aging centers, and local newspapers across the state.

We greatly appreciate FHWA/FTA support in the development of the STIP 2024-2028 and look forward to working together to achieve our mutual goals. Should you have any questions pertaining to this amendment, please contact April Leckie, STIP Administration at 317-232-5466 or at aleckie@indot.in.gov.

Sincerely,



Michael Smith, Commissioner
Indiana Department of Transportation

cc: (w/enclosure): Angelica Salgado, FTA
Cecilia Crenshaw, FTA
Erica Tait, FHWA
Lyndsay Quist, INDOT
Kristin Brier, INDOT
Kathy Eaton-McKalip, INDOT
Louis Feagans, INDOT

April Leckie, INDOT
Roy Nunnally, INDOT
Larry Buckel, INDOT
Jay Mitchell, INDOT
Jason Casteel, INDOT
Michael McNeil, INDOT

Appendix I

Additional Information

Routine Bridge Inspection Report



Structure Information

Structure:	026-38-03431 A	Facility Carried:	SR 26
NBI Number:	007050	Features Intersected:	BOLEN DITCH

Inspection Information

Inspection Date:	10/10/2024	Lead Inspector:	Brian D. Harvey
Inspection Type:	Routine	Additional Inspectors:	

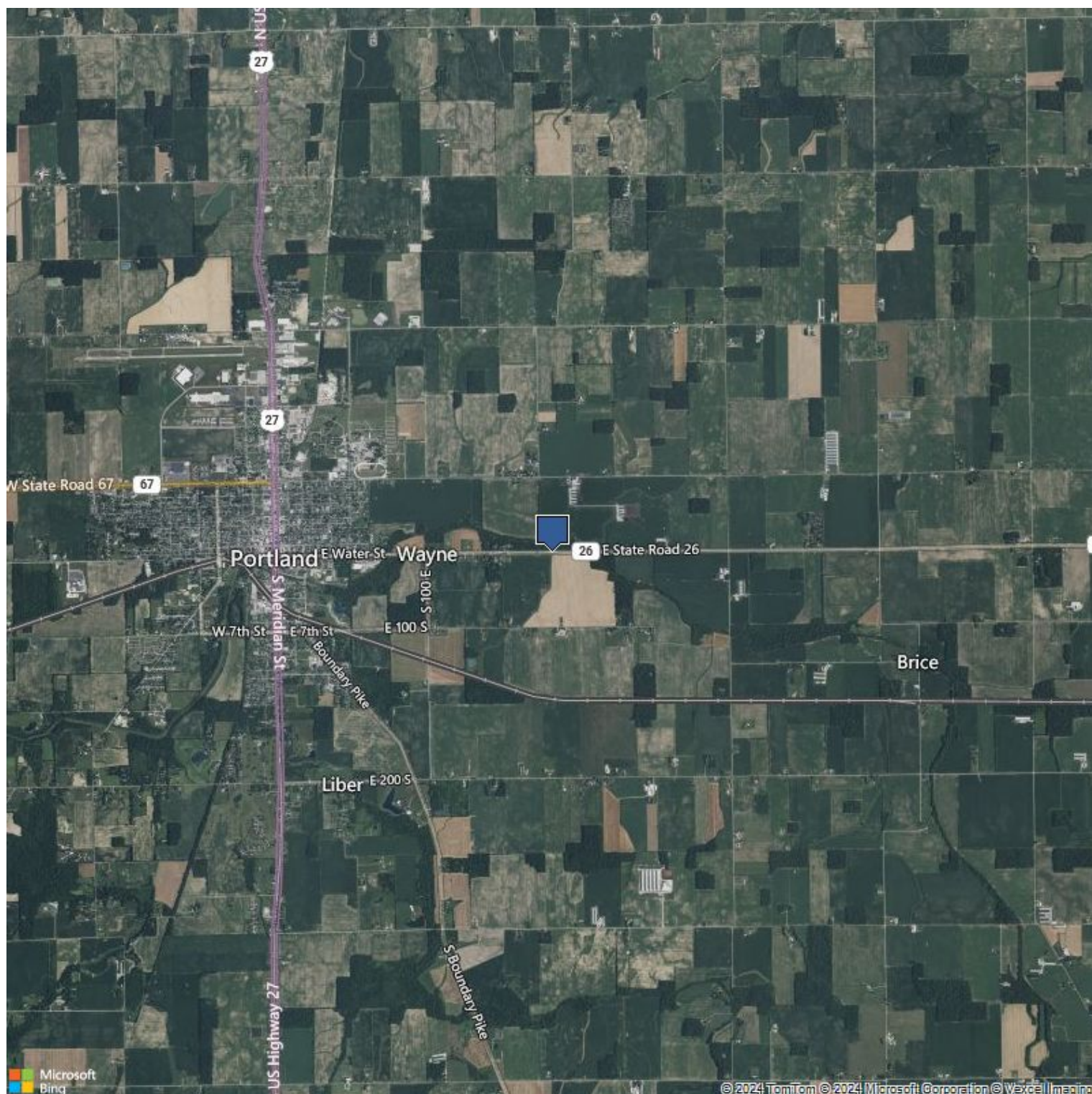
Condition Ratings Summary

(58) Deck:	4	(60) Substructure:	5
(58.01) Wearing Surface:	5	(61) Channel / Channel Protection:	6
(58.02) Joints:	N	(62) Culverts:	N
(58.05) Approach Slabs:	5	(71) Waterway Adequacy:	9
(59) Superstructure:	4	(72) Approach Roadway Alignment:	8
(59.01) Paint:	N	(113) Scour Critical Bridge:	3



Structure:	026-38-03431 A	Facility Carried:	SR 26	Inspector:	Brian D. Harvey
NBI Number:	007050	Features Intersected:	BOLEN DITCH	Inspection Date:	10/10/2024

Location Map



Location:	01.84 E US 27
County:	Jay

Latitude:	40.43276
Longitude:	-84.94296

Structure:	026-38-03431 A	Facility Carried:	SR 26	Inspector:	Brian D. Harvey
NBI Number:	007050	Features Intersected:	BOLEN DITCH	Inspection Date:	10/10/2024

Routine Inspection Summary

General Notes:

In July of 2015, the State Bridge Inspection Manager attempted to level out the workload between districts by having the Fort Wayne District inspect over 40 of the bridges in the furthest Northern portions of the Greenfield District. These bridges would still be owned by Greenfield, they would just be inspected by Fort Wayne. Str. #26-38-03431 A was one of those bridges. In March of 2020, these structures were taken back by the Greenfield District.

***** Bridge on annual inspection frequency due to poor condition of the SUPERSTRUCTURE *****

SR 26 over Bolen Ditch (RP 142+29)

Abutment #1 is WEST.

The Bridge was built in 1946, under contract B-2639.

'A' Rehab (Replaced Superstructure & Widened, 1.5" Bridge Deck Surface) in 1982, B-13450.

DES. #1702876 - Replacement project eliminated (recommended replacement to Bridge Asset Engineer again in January, 2021).

DES# 2200586 - Programmed for Bridge Replacement in 2027, Contract B-44619.

Summary Condition Statement: Overall, the bridge is in POOR condition.

- The Bridge Deck surface has fairly wide random cracks, several fairly large patches, and some areas starting to break-up
- The bottom of the Slab superstructure has very heavy delaminations & spalls with 4-5 rebar exposed with section loss at copings.

Structure:	026-38-03431 A	Facility Carried:	SR 26	Inspector:	Brian D. Harvey
NBI Number:	007050	Features Intersected:	BOLEN DITCH	Inspection Date:	10/10/2024

Identification

(1) State Code:	185 - Indiana	(12) Base Highway Network:	0
(8) Structure:	007050	(13A) Inventory Route:	
(5) Inv. Route:	1 - 3 - 1 - 00026 - 0	(13B) Subroute Number:	
(2) Highway Agency District:	3 - Greenfield	(16) Latitude:	40.43276
(3) County Code:	038 - Jay	(17) Longitude:	-84.94296
(4) Place Code:	00000 - N/A	(98) Border	
(6) Features Intersected:	BOLEN DITCH	(A) State Name:	
(7) Facility Carried:	SR 26	(B) Percent:	
(9) Location:	01.84 E US 27	(99) Border Bridge Struct. No:	
(11) Milepoint:	0015.360		

Age Of Service

(27) Year Built:	1946	(19) Bypass Detour Length:	005
(106) Year Reconstructed:	1982	(29) ADT:	002127
(42) Type Of Service		(30) Year Of ADT:	2021
(A) On Bridge:	1 - Highway	(109) ADTT:	20
(B) Under Bridge:	5 - Waterway	(114) Future ADT:	002800
(28) Lanes		(115) Year Of Future ADT:	2035
(A) On Bridge:	02		
(B) Under Bridge:	00		

Structure Type And Material

(43) Main Spans:		(45) No. Of Spans In Main Unit:	002
(A) Kind Of Material:	2 - Concrete Continuous	(46) No. Of Approach Spans:	0000
(B) Type Of Design:	1 - Slab	(107) Deck Structure Type:	1 - Concrete Cast-In-Place
(44) Approach Spans		(108) Wearing Surface	
(A) Kind Of Material:	0 - Other	A) Wearing Surface:	3 - Latex Concrete or similar additive
(B) Type Of Design:	00 - Other	B) Deck Membrane:	0 - None
		C) Deck Protection:	0 - None

Structure:	026-38-03431 A	Facility Carried:	SR 26	Inspector:	Brian D. Harvey
NBI Number:	007050	Features Intersected:	BOLEN DITCH	Inspection Date:	10/10/2024

Classification

(20) Toll:	3 - On Free Road. The structure is toll-free and carries a toll-free highway.	(21) Maint Responsibility:	01
(22) Owner:	01 - State Highway Agency	(26) Functional Class:	07
(37) Historical Significance:	5 - Not eligible	(100) Strahnet Highway:	0 - The inventory route is not a STRAHNET route.
(101) Parallel Structure:	N - No parallel structure exists.	(102) Direction Of Traffic:	2 - 2-Way Traffic
(103) Temporary Structure:		(104) NHS Inventory:	0 - Inventory Route is not on the NHS
(105) Federal Lands Highways:	0 - Not Applicable	(110) DES National Network:	0 - Inventory route not on network
(112) NBIS Bride Length:	Y - Yes		

Geometric Data

(48) Length Of Max Span:	00021.6	(35) Structure Flared:	0 - No Flare
(49) Structure Length:	00044.2	(10) Inv Rte, Min Vert Clearance:	99.99
(50) Curb/Sidewalk Widths		(47) Tot Horiz Clearance:	044.0
(A) Left:	00.0	(53) Vert Clear Over Br Rdwy:	99.99
(B) Right:	00.0	(54) Min Vertical Underclearance:	
(51) Brdg Rdwy Width Curb- To-Curb:	044.0	A) Reference Feature:	N
(52) Deck Width, Out-To-Out:	046.5	B) Min Vert Underclear:	00.00
(32) Approach Roadway:	028.0	(55) Lateral Underclearance Right:	
(33) Bridge Median:	0 - No Median	A) Reference Feature:	N
(34) Skew:	00	B) Min Lateral Underclear:	000.0
		(56) Min Lateral Underclear On Left:	000.0

Structure:	026-38-03431 A	Facility Carried:	SR 26	Inspector:	Brian D. Harvey
NBI Number:	007050	Features Intersected:	BOLEN DITCH	Inspection Date:	10/10/2024

Inspections

(90) Inspection Date:	Oct 6 2023 12:00AM	(91) Designated Inspection Frequency:	12
(92) Critical Feature Inspection		(93) Critical Feature Inspection Date	
A) NSTM Insp Req / Freq:	N	A) NSTM Date:	
B) Underwater Insp Req / Freq:	N	B) Underwater Insp Date:	
C) Special Insp Req / Freq:	N	C) Special Insp Date:	

Proposed Improvements

(75A) Type Of Work:		(94) Bridge Improvement Cost:	000000
(75B) Work Done By:		(95) Roadway Improvement Cost:	000000
(76) Length Of Improvement:	00000.0	(96) Total Project Cost:	000000
(97) Year Of Improvement Cost Estimate:			
Comments:			

Navigation Data

(38) Navigation Control:	0	(39) Navigation Vertical Clear:	000.0
(111) Pier Or Abutment Protection:		(116) Minimum Navigation Verti.Clearance, Vert. Lift Bridge:	
		(40) Nav Horizontal Clearance:	0000.0

Structure:	026-38-03431 A	Facility Carried:	SR 26	Inspector:	Brian D. Harvey
NBI Number:	007050	Features Intersected:	BOLEN DITCH	Inspection Date:	10/10/2024

Load Rating & Posting

5.1 – Loads And Load Ratings		Legacy Coding	
B.LR.01 - Design Load	HS20	(65) Inventory Rating Method	8
B.LR.02 - Design Method	LFD	(66) Inventory Rating	0.81
B.LR.03 - Load Rating Date		(63) Operating Rating Method	8
B.LR.04 - Load Rating Method	LRFR	(64) Operating Rating	1.05
B.LR.05 - Inventory Load Rating Factor	0.81	(31) Design Load	5
B.LR.06 - Operating Load Rating Factor	1.05	(70) Bridge Posting	5
B.LR.07 - Controlling Legal Load Rating Factor	1.28	(41) Structure Open/Posted/Closed	A
B.LR.08 - Routine Permit Loads	C	Tons Posted	
		Date Posted/Closed	

5.2 – Load Posting Status		Posting – Emergency Vehicles (Ton)	
B.PS.01 - Load Posting Status	PO	Emergency Vehicle Sign	
B.PS.02 - Posting Status Change Date		Posted Tonnage (Single Axle) EV	
		Posted Tonnage (Tandem) EV	
		Posted Tonnage (Gross) EV	

Posting – Commercial Vehicle (Ton)	Maximum Allowable Tonnages
------------------------------------	----------------------------

*Actual posted values may not exceed those as shown below

Commercial Vehicle Sign
Posted Tonnage (Single Axle) CV
Posted Tonnage (Gross) CV
Posted Tonnage (2-axle) CV
Posted Tonnage (3-axle) CV
Posted Tonnage (4-axle) CV
Posted Tonnage (5-axle) CV
Posted Tonnage (6-axle) CV

**Weight
Limit

Tons**

**Emergency
Vehicle
Weight Limit
Single Axle T
Tandem T
Gross T**

Structure:	026-38-03431 A	Facility Carried:	SR 26	Inspector:	Brian D. Harvey
NBI Number:	007050	Features Intersected:	BOLEN DITCH	Inspection Date:	10/10/2024

National Bridge Inventory Condition Ratings

(58) Deck:	4 - Poor Condition (advanced deterioration)		
See Superstructure			
(58.01) Wearing Surface:	5 - Fair Condition		
Wearing surface: fairly wide random cracks; several fairly large patches; some areas starting to break-up, esp. near one of the patches over Pier 2.			
(58.02) Joints:	N - ONLY to remove other value that is no longer present.		
Joint Type:	N - ONLY to remove other value that is no longer present	Joint Location:	No Joints Present
(58.05) Approach Slabs:	5 - Fair condition, no settlement, moderate cracking and spalls, crack spacing > .5'		
West: wide random cracks, partially sealed; 2 small spalls EB lane.			
East: wide, irregular transverse crack {4' off deck joint}; wide longitudinal cracks.			
(58.06) Terminal Joints:	N - No terminal joint		
(59) Superstructure:	4 - Poor Condition (advanced deterioration)		
2-span Continuous Reinforced Concrete Slab: heavy spalling/delamination with exposed rebar in the outer 3' at copings of both spans (4-5 rebar exposed with section loss); widely-spaced, hairline longitudinal cracks; Span B - longitudinal crack with delamination at center line.			
(59.01) Paint:	N - Not Rated / N/A	Paint Year:	
(59.02) Bearings:		Bearing Type:	N
(60) Substructure:	5 - Fair Condition (minor section loss)		
Abutments: original portions - vertical cracks & efflorescence, areas reconstructed/patched (mostly East); widened and new caps w/ some horizontal cracking; East - surface spall at South construction joint; spall with exposed rebar at the top NE corner.			
SE wingwall: spalling with minor rebar exposed at top.			
Pier 2: light scaling to older section; a few vertical cracks - 2 are wide near centerline; cap has a vertical crack at South end.			
(61) Channel / Channel Protection:	6 - Bank is beginning to slump. River control devices and embankment protection have widespread minor damage. There is minor stream bed movement evident. Debris is restricting the channel slightly.		
Bridge located over an overflow area; some water may flow south-to-north {toward river}. Mud under Span A; Some water under Span B; Upstream is a shallow swale surrounded by corn fields. 100' downstream is a dry, gravelly swale through a forest (leading to nearby river). Channel is well vegetated at structure - fairly heavy bank erosion downstream outside ROW. Previous debris has caught on bottom of deck indicating water level has reached bottom of superstructure.			
(62) Culverts:	N - Not applicable. Use if structure is not a culvert.		

INDOT Defined Condition Ratings

Concrete Slopewall:	N - No concrete slopewall
Birds Present?:	No

Structure:	026-38-03431 A	Facility Carried:	SR 26	Inspector:	Brian D. Harvey
NBI Number:	007050	Features Intersected:	BOLEN DITCH	Inspection Date:	10/10/2024
Bats Present?:	No				

Structure:	026-38-03431 A	Facility Carried:	SR 26	Inspector:	Brian D. Harvey
NBI Number:	007050	Features Intersected:	BOLEN DITCH	Inspection Date:	10/10/2024

Appraisal

(71) Water Adequacy: ~1' H.W. to W. Appr. P.G.	9 - Bridge above flood water elevations		
(72) Approach Roadway Alignment:	8 - Equal to present desirable criteria		
36A) Bridge Rails:	0 - Does not meet acceptable standards/safety feature is required		
36B) Transitions:	0 - Does not meet acceptable standards/safety feature is required		
36D) Approach Guardrail Ends:	0 - Does not meet acceptable standards/safety feature is required		
Sufficiency Rating:	61.1	(67) Structural Evaluation:	4
Status:	1	(68) Deck Geometry:	7
		(69) Underclearances, Vertical & Horizontal	N

Scour Critical Bridge Appraisal

Bridge Inspection Scour Appraisal

(113) Scour Critical Bridges:	3 - Foundations unstable for scour conditions
2/18/2022 Scour memo determined structure scour critical. Submitted deficiency to place riprap per 2/18/22 Scour Memo. Plan-of Action written to monitor until Scour Critical determination changes. To be addressed through proposed replacement in 2027.	
Scour Critical Safety Status:	B - Bridge IS scour critical based on analysis findings, Scour POA or Countermeasures REQUIRE
Countermeasures Placed/Verified:	
Bridge Inspection Comments:	2/18/2022 Scour memo determined structure scour critical. Submitted deficiency to place riprap per 2/18/22 Scour Memo. Plan-of Action written to monitor until Scour Critical determination changes. To be addressed through proposed replacement in 2027.

Hydraulic Scour Analysis Determination

Scour Analysis Status:	A - Scour Analysis on file
Scour Analysis Date:	Feb 18 2022 12:00AM
Scour Analysis Determination:	B
Hydraulics Comments:	

Land and Water Conservation Fund (LWCF) County Property List for Indiana (Last Updated March 2022)

ProjectNumber	SubProjectCode	County	Property
1800187	1800187	Jay	Sportland Park
1800243	1800243	Jay	North End Park (Milton Miller Memorial Park)

*Park names may have changed. If acquisition of publically owned land or impacts to publically owned land is anticipated, coordination with IDNR, Division of Outdoor Recreation, should occur.