

FHWA-Indiana Environmental Document
CATEGORICAL EXCLUSION / ENVIRONMENTAL ASSESSMENT FORM
GENERAL PROJECT INFORMATION

Road No./County:

State Road (SR) 2 and County Road (CR) 500 West (W) / LaPorte County

Designation Number(s):

2200043

Project

Description/Termini:

Intersection Improvement Project / 3.27 Miles West of SR 39

| | |
|----------|---|
| X | Categorical Exclusion, Level 2 – Required Signatories: INDOT DE and/or INDOT ESD |
| | Categorical Exclusion, Level 3 – Required Signatories: INDOT ESD |
| | Categorical Exclusion, Level 4 – Required Signatories: INDOT ESD and FHWA |
| | Environmental Assessment (EA) – Required Signatories: INDOT ESD and FHWA |
| | Additional Investigation (AI) – The proposed action included a design change from the original approved environmental document. Required Signatories must include the appropriate environmental approval authority |

Approval

INDOT DE Signature and Date

INDOT ESD Signature and Date

FHWA Signature and Date

Release for Public Involvement

SFM 10/22/2025
INDOT DE Initials and Date

INDOT ESD Initials and Date

Certification of Public Involvement

INDOT Consultant Services Signature and Date

INDOT DE/ESD Reviewer Signature and Date:

Name and Organization of CE/EA Preparer:

Samantha Wickizer, Metric Environmental, LLC

Indiana Department of Transportation

County LaPorteRoute SR 2 and CR 500 WestDes. No. 2200043

Part I – Public Involvement

Every Federal action requires some level of public involvement, providing for early and continuous opportunities throughout the project development process. **The level of public involvement should be commensurate with the proposed action.**

Does the project have a historic bridge processed under the Historic Bridges PA*?

Yes

☐

No

☒

If No, then:

Opportunity for a Public Hearing Required?

☒

*A public hearing is required for all historic bridges processed under the Historic Bridges Programmatic Agreement between INDOT, FHWA, SHPO, and the ACHP.

Discuss what public involvement activities (legal notices, letters to affected property owners and residents (i.e. notice of entry), meetings, special purpose meetings, newspaper articles, etc.) have occurred for this project.

Notice of Entry letters were mailed to potentially affected property owners near the project area on July 27, 2023, notifying them about the project and that individuals responsible for land surveying and field activities may be seen in the area. A sample copy of the Notice of Entry letter is provided in Appendix G, page G-1.

To meet the public involvement requirements of Section 106, a legal notice of Federal Highway Administration-Indiana Division's (FHWA's) finding of "No Historic Properties Affected" was published in the *LaPorte County Herald-Dispatch* newspaper on July 9, 2025, offering the public an opportunity to submit comment pursuant to 36 CFR 800.2(d), 800.3(e), and 800.6(a)(4). The public comment period closed after 30 days on August 8, 2025. No comments were received. The text of the public notice and the affidavit of publication appear in Appendix D, page D-105.

The project will meet the minimum requirements described in the current *Indiana Department of Transportation (INDOT) Project Development Public Involvement Procedures Manual* which requires the project sponsor to offer the public an opportunity to submit comments and/or request a public hearing. INDOT has determined that it is in the public's interest to hold a public hearing for this project. Therefore, a legal notice of public hearing will appear in a local publication contingent upon the release of this document for public involvement. This document will be revised after the public involvement requirements are fulfilled.

Public Controversy on Environmental Grounds

Discuss public controversy concerning community and/or natural resource impacts, including what is being done during the project to minimize impacts.

At this time, there is no substantial public controversy concerning impacts to the community or to natural resources.

Part II - General Project Identification, Description, and Design Information

Sponsor of the Project: INDOTINDOT District: LaPorteLocal Name of the Facility: SR 2 and CR 500 W

Funding Source (mark all that apply):

Federal ☒State ☒Local ☐Other* ☐

*If other is selected, please identify the funding source: _____

PURPOSE AND NEED:

The need should describe the specific transportation problem or deficiency that the project will address. The purpose should describe the goal or objective of the project. The solution to the traffic problem should NOT be discussed in this section.

The need for this project is evidenced by the high frequency of car crashes at this intersection. A total of 22 crashes occurred at the intersection during the study period of 2017 to 2019. Of these, 4 crashes (18.18%) involved collision with an animal (including deer), 1 crash (4.55%) was a head on collision, 3 crashes (13.64%) were opposite direction sideswipes, 1 crash involved a car running off the road (4.55%), 3 were rear end crashes (13.64%), 1 crash (4.55%) was a same direction sideswipe, and 1 crash (4.55%) was listed as other. The highest frequency of crash type involved right and left turn or right-angle crashes where 8 (36.36%) occurred during the study period and these crashes often resulted in injuries. The Index of Crash Frequency (ICF) compares the crash frequency data of this intersection with intersections of similar volumes, roadway classification, and control type throughout Indiana. An ICF of 0 indicates a roadway is performing as expected and an ICF of 2 or higher indicates the roadway may be considered a high crash location. Crash data for this intersection resulted in an ICF of 2.96, which indicates the existing stop signs on CR 500 W are not safely controlling traffic at this intersection.

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Level of Service (LOS) is a term used to qualitatively describe the operating conditions of a transportation facility. It is based on factors such as speed, travel time, mobility and delay. There are six criteria to determine LOS and each level is assigned a letter designation from A to F, with LOS A representing the best operating conditions (free flow), and LOS F the worst operating conditions (breakdown flow). CR 500 W currently experiences a LOS of C (stable flow, at or near free flow) during the AM peak travel hour and a LOS of D (approaching unstable flow) during the PM peak travel hour. The LOS on southbound CR 500 W is expected to reach a LOS of F prior to the design year (2046). Excerpts relating to the crash data and LOS from the Engineer's Report, dated November 5, 2021, are provided in Appendix I, pages I-1 to I-13.

The purpose of this project is to improve the safety and efficiency of the SR 2 and CR 500 W intersection by reducing the ICF to a level less than 2 by reducing the number of right-angle crashes and improving the LOS to a level A.

PROJECT DESCRIPTION (PREFERRED ALTERNATIVE):

County: LaPorte Municipality: N/A

Limits of Proposed Work: Project limits will extend approximately 1,640 feet (ft.) west and 820 ft. east along SR 2 and 297 ft. north and 389 ft. south along CR 500 W

Total Work Length: 0.60 Mile(s) Total Work Area: 5.64 Acre(s)

Is an Interstate Access Document (IAD)¹ required?

| | |
|--------------------------|-------------------------------------|
| Yes ¹ | No |
| <input type="checkbox"/> | <input checked="" type="checkbox"/> |

If yes, when did the FHWA provide a Determination of Engineering and Operational Acceptability? Date:

¹If an IAD is required; a copy of the approved CE/EA document must be submitted to the FHWA with a request for final approval of the IAD.

Describe location of project including township, range, city, county, roads, etc. Existing conditions should include current conditions, current deficiencies, roadway description, surrounding features, etc. Preferred alternative should include the scope of work, anticipated impacts, and how the project will meet the Purpose and Need. Logical termini and independent utility also need discussed.

INDOT and FHWA intend to proceed with a proposed intersection improvement project in LaPorte County, Indiana.

Location

The project is located at the intersection of SR 2 and CR 500 W, approximately 3.27 miles west of SR 39 (Appendix B, page B-1). Specifically, the project is located within Sections 5, 6, 7, and 8, Township 36 North, Range 3 West as illustrated on the LaPorte West, Indiana 7.5-minute United States Geological Survey (USGS) topographic quadrangle (Appendix B, page B-2).

Existing Conditions

SR 2 and CR 500 W are both two-lane roadways. SR 2 is classified as a Rural Principal Arterial roadway that provides one east and west 12 ft. wide travel lane. The travel lanes are bordered by 6 ft. wide gravel shoulders, and there are dedicated right-turn lanes at the intersection with CR 500 W. CR 500 W is classified as a Rural Local Agency Collector roadway that provides one north and south 11 ft. wide travel lane with no usable shoulders. Traffic on SR 2 is free-flowing. There is a two-way stop at the intersection for traffic traveling on CR 500 W. This configuration increases the potential for right angle crashes caused during left turn movements and limits capacity especially during peak travel periods. A total of 22 recorded crashes took place within the project limits during the three-year crash study period (2017 through 2019) with the highest frequency of crashes being left turn or angle crashes. There are no sidewalks or other pedestrian amenities present in the project area. The posted speed limit on SR 2 within the project area is 55 miles-per-hour (mph) and the posted speed limit on CR 500 W is 30 and 45 mph. Land use within the project area is predominantly agricultural and residential dwellings with a commercial gas station to the northeast of the project.

Preferred Alternative

The preferred alternative will modify the existing intersection by constructing a single-lane roundabout with a truck apron. The roundabout will consist of a 140 ft. diameter roundabout with one 19 ft. circulatory travel lane and an inscribed diameter of 51 ft. providing a 38 ft. interior grass area and a 13 ft. truck apron bordered by concrete curb and gutter. The roadway cross-section will provide 12 ft. entry and exit lanes from the approaches of SR 2 and CR 500 W. The approach roads along SR 2 will have approximately 3 ft. paved shoulders with an additional 8 ft. gravel shoulder in a few locations. The approach roads along CR 500 W will have approximately 2 ft. paved shoulders. The proposed drainage infrastructure includes installation of new roadside ditches along with enclosed drainage and underdrains that will direct runoff into a detention basin that will be constructed in the southwest quadrant of the intersection. Other features of the roundabout will include raised medians on each leg of the roundabout of varying width up to 5 ft. Specialized pavement markings will include directional and yield arrows to assist motorists with lane direction.

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Seven 160-watt type II LED lights will be installed throughout the interchange. The lights will be installed in a staggered orientation and will extend approximately 230 ft. along the east approach and 175 ft. along the west approach of SR 2 and 80 ft. along the north approach and 30 ft. along the south approach of CR 500 W. Project plan sheets are provided in Appendix B, pages B-22 to B-43.

It is anticipated that the project will require 2.35 acres of reacquired right of way (ROW) and 2.02 acres of permanent ROW with 2.22 acres planned to be converted from prime farmland. The project will also require temporary ROW for driveway construction consisting of 0.023 acre of residential land and 0.096 acre of commercial land. Tree clearing is also planned as a part of the project and will include 0.014 acre of tree removal. The project will result in 5.63 acres of land disturbance and will require a Construction Stormwater General Permit (CSGP).

The project termini are logical because they encompass only the area necessary to reconstruct the intersection with the single-lane roundabout and tie the improvements into the existing roadways for a smooth transition. The project limits will extend approximately 1,640 ft. west and 820 ft. east along SR 2 and 297 ft. north and 389 ft. south along CR 500 W. The project has independent utility as it does not depend on the construction of a secondary project to be considered complete.

The maintenance of traffic (MOT) plan will require a detour as the intersection will be closed during construction. The detour will utilize SR 39, US 6, and US 421 (Appendix B, B-31 to B-32). Additional details are provided in the MOT During Construction section of this document. The preferred alternative will meet the purpose and need of the project by improving the safety and efficiency of the SR 2 and CR 500 W intersection by reducing the ICF to a level less than 2 by reducing the number of right-angle crashes and improving the LOS to a level A.

OTHER ALTERNATIVES CONSIDERED:

Provide a header for each alternative. Describe all discarded alternatives, including the No Build Alternative. Explain why each discarded alternative was not selected. Make sure to state how each alternative meets or does not meet the Purpose and Need and why.

No Build

The "No Build" alternative would allow the existing intersection to remain in place with no improvements and would not require the expenditure of funds. Although this alternative is feasible it would not be prudent to allow the existing conditions to persist and contribute to high frequency of right-angle crashes and low LOS for the surrounding community. This alternative does not meet the purpose and need of the project and was discarded from further consideration. This alternative is discussed in the Engineer's Report provided in Appendix I, pages I-6 to I-7 and I-14.

Signalized Intersection

The signalized intersection alternative consists of installing a traffic signal at the SR 2 and CR 500 W intersection. An eastbound and westbound left turn auxiliary lane would be constructed. SR 2 would require approximately 6 ft. of pavement widening on each side of the existing pavement. This alternative is predicted to improve the intersection to LOS B or greater for all approach movements and improve safety by reducing right angle crashes. However, the installation of traffic signals has the potential to increase rear-end and red-light running accidents. Although this alternative was deemed a viable option, further analysis identified that the preferred alternative performed more proficiently in regards to both safety and efficiency. This alternative does not address the purpose and need of the project by improving the intersection to LOS A for all approach movements. For these reasons, the signalized intersection alternative was discarded from further consideration. This alternative is discussed in the Engineer's Report provided in Appendix I, page I-7. Further discussion of this alternative is provided in Appendix I, page I-14.

Median U-turn (J-turn)

A median U-turn (J-turn) intersection requires installation of a median along SR 2 with dedicated left turn lanes for through traffic and rerouting of cross traffic from CR 500 W to a U-turn. This alternative requires extensive reconstruction of both roadways to accommodate a concrete median and widened roadways. This alternative would require expansion of the project footprint in relation to the preferred alternative, likely to result in the need for additional permanent ROW and increased expenditure of funds. This alternative would likely meet the purpose and need by reducing the potential for right angle crashes and improving the LOS; however, due to the expanded footprint, potential increase in expenditure of funds and environmental impacts it was determined to be a less desirable option. Therefore, it was dismissed from further consideration. This alternative is discussed in the Engineer's Report provided in Appendix I, page I-8.

Displaced Left-turn (DLT) intersection

A DLT intersection is a type of intersection in which all left-turns from the mainline road and sometimes the minor roadway are made in a two-stage process. Vehicles first cross from the center to the opposite side of the roadway at an initial signalized

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intersection and then proceed to the primary signal via a special roadway parallel to the main roadway. At the primary signal, vehicles complete the left turn at the same time as through traffic since they are no longer in conflict with one another. Installation of a DLT at the SR 2 and CR 500 W intersection involves installation of 3 traffic signals and requires extensive alteration of the existing intersection to construct the special parallel roadway. This alternative would require expansion of the project footprint in relation to the preferred alternative, likely to result in the need for additional permanent ROW and increased expenditure of funds. This alternative would likely meet the purpose and need by reducing the potential for right angle crashes and improving the LOS; however, due to the expanded footprint, potential increase in expenditure of funds and environmental impacts it was determined to be a less desirable option. Therefore, it was dismissed from further consideration. This alternative is discussed in the Engineer's Report provided in Appendix I, page I-8.

Jughandle Intersection

A Jughandle intersection removes the left turns from SR 2 by constructing left turning ramps to CR 500 W. This configuration removes the left turn movements from the intersection and requires extensive reconstruction of the intersection to accommodate the left turning ramps. This alternative would require expansion of the project footprint in relation to the preferred alternative, likely to result in the need for additional permanent ROW and increased expenditure of funds. This alternative would likely meet the purpose and need by reducing the potential for right angle crashes and improving the LOS; however, due to the expanded footprint, potential increase in expenditure of funds and environmental impacts it was determined to be a less desirable option. Therefore, it was dismissed from further consideration. This alternative is discussed in the Engineer's Report provided in Appendix I, pages I-8 to I-9.

Offset "T" Intersection

The offset "T" intersection creates a staggered or offset configuration where CR 500 W northbound and southbound traffic would intersect SR 2 at two separate points, rather than one continuous intersection. This alternative would require extensive reconstruction of the intersection to allow for the north leg of CR 500 W to tie into SR 2 approximately 700 ft. west of the intersection and the realignment of approximately 2,000 ft. of CR 500 W. This alternative would require expansion of the project footprint in relation to the preferred alternative, likely to result in the need for additional permanent ROW and increased expenditure of funds. This alternative would likely meet the purpose and need by reducing the potential for left and right angle crashes and improving the LOS; however, due to the expanded footprint, potential increase in expenditure of funds and environmental impacts it was determined to be a less desirable option. Therefore, it was dismissed from further consideration. This alternative is discussed in the Engineer's Report provided in Appendix I, page I-9.

Continuous Green "T" Intersection (CGT)

A CGT intersection consists of a 3-leg intersection in which through traffic does not have to stop and cross traffic is controlled via a stop signal. Left turn movements for both through traffic and cross traffic consist of dedicated deceleration and acceleration lanes. This type of intersection is only applicable to an existing "T" intersection. Since the existing intersection is not a "T" intersection, this alternative was removed from consideration. This alternative is discussed in the Engineer's Report provided in Appendix I, page I-9.

Quadrant Roadway Intersection

A quadrant roadway intersection consists of rerouting all four left-turn movements at the intersection onto a new road that would connect the intersection of CR 500 W and SR 2 to another intersection. This alternative would require expansion of the project footprint in relation to the preferred alternative, likely to result in the need for additional permanent ROW and increased expenditure of funds. This alternative would likely meet the purpose and need by reducing the potential for right angle crashes and improving the LOS; however, due to the expanded footprint, potential increase in expenditure of funds and environmental impacts it was determined to be a less desirable option. Therefore, it was dismissed from further consideration. This alternative is discussed in the Engineer's Report provided in Appendix I, page I-9.

Grade Separation

A grade separation requires the construction of a bridge to carry CR 500 W over SR 2. This alternative would eliminate access between CR 500 W and SR 2, causing traffic to travel further to other intersections to get to SR 2. This alternative would require expansion of the project footprint in relation to the preferred alternative, likely to result in the need for additional permanent ROW and increased expenditure of funds. This alternative would remove right angle crashes entirely but does not meet the purpose and need due to elimination of the intersection and LOS rating. Therefore, this alternative was dismissed from further consideration. This alternative is discussed in the Engineer's Report provided in Appendix I, page I-9.

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The No Build Alternative is not feasible, prudent or practicable because (Mark all that apply):

It would not correct existing capacity deficiencies;
 It would not correct existing safety hazards;
 It would not correct the existing roadway geometric deficiencies;
 It would not correct existing deteriorated conditions and maintenance problems; or
 It would result in serious impacts to the motoring public and general welfare of the economy.
 Other (Describe):

| |
|---|
| X |
| X |
| |
| |
| X |
| |

ROADWAY CHARACTER:

If the proposed action includes multiple roadways, complete and duplicate for each roadway.

Name of Roadway SR 2
 Functional Classification: Rural Principal Arterial
 Current ADT: 9,008 VPD (2027) Design Year ADT: 9,668 VPD (2047)
 Design Hour Volume (DHV): 1,001 Truck Percentage (%) 1.97
 Designed Speed (mph): 55 Legal Speed (mph): 55

| | Existing | | Proposed | |
|------------------|---------------------|-----|--|-----|
| Number of Lanes: | 2 | | 2 | |
| Type of Lanes: | 12 ft. travel lanes | | 12-15 ft. entry/exit lanes 19 ft. circulatory travel lane | |
| Pavement Width: | 24 | ft. | 30 | ft. |
| Shoulder Width: | 6 | ft. | 3-8 | ft. |
| Median Width: | N/A | ft. | 5-10 | ft. |
| Sidewalk Width: | N/A | ft. | N/A | ft. |

Name of Roadway CR 500 W
 Functional Classification: Rural Local Agency Collector
 Current ADT: 2,249 VPD (2027) Design Year ADT: 2,249 VPD (2047)
 Design Hour Volume (DHV): 224 Truck Percentage (%) 1.34
 Designed Speed (mph): 30 & 45 Legal Speed (mph): 30 & 45

| | Existing | | Proposed | |
|------------------|---------------------|-----|--|-----|
| Number of Lanes: | 2 | | 2 | |
| Type of Lanes: | 11 ft. travel lanes | | 12-15 ft. entry/exit lanes 19 ft. circulatory travel lane | |
| Pavement Width: | 22 | ft. | 24-30 | ft. |
| Shoulder Width: | 0 | ft. | 2-1 | ft. |
| Median Width: | N/A | ft. | 5-10 | ft. |
| Sidewalk Width: | N/A | ft. | N/A | ft. |

Setting: ☐ Urban ☐ Suburban ☒ Rural
 Topography: ☒ Level ☐ Rolling ☐ Hilly

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BRIDGES AND/OR SMALL STRUCTURE(S):

If the proposed action includes multiple structures, complete and duplicate for each bridge and/or small structure. Include both existing and proposed bridge(s) and/or small structure(s) in this section.

Structure/NBI Number(s): N/A Sufficiency Rating: N/A

| | Existing | Proposed |
|---------------------------|----------|----------|
| Bridge/Structure Type: | N/A | N/A |
| Number of Spans: | N/A | N/A |
| Weight Restrictions: | ton | ton |
| Height Restrictions: | N/A ft. | N/A ft. |
| Curb to Curb Width: | N/A ft. | N/A ft. |
| Outside to Outside Width: | N/A ft. | N/A ft. |
| Shoulder Width: | N/A ft. | N/A ft. |

Describe impacts and work involving bridge(s), culvert(s), pipe(s), and small structure(s). Provide details for small structure(s): structure number, type, size (length and dia.), location and impacts to water. Use a table if the number of small structures becomes large. If the table exceeds a complete page, put it in the appendix and summarize the information below with a citation to the table.

No bridges are located within the project area. Pipes will be installed for drainage improvements and are detailed in the table below.

| Structure Number | Size (linear ft. (lft.) x inches (in.)) | Type | Appendix B |
|------------------|---|----------------------------|------------|
| 11 | 33 lft. x 15 in. | New pipe | B-34 |
| 12 | 32 lft. x 15 in. | New pipe | B-34 |
| 13 | 27 lft. x 15 in. | Replaced in-kind pipe | B-35 |
| 15 | 99 lft. x 18 in. | New pipe | B-37 |
| 16 | 86 lft. x 24 in. | New pipe | B-37 |
| 18 | 23 lft. x 15 in. | Replaced in-kind pipe | B-39 |
| 17 | 118 lft. x 24 in. | New pipe | B-37 & 41 |
| 19 | 55 lft. x 12 in. | New inlet with pipe | B-37 & 41 |
| 20 | 11 lft x 15 in. and 6ft. x 12 in. | New structure with 2 pipes | B-33 |
| 21 | 37 lft. x 15 in. and 25 lft x 6 in. | New structure with 2 pipes | B-36 |

MAINTENANCE OF TRAFFIC (MOT) DURING CONSTRUCTION:

| | Yes | No |
|--|-----|----|
| Is a temporary bridge proposed? | | X |
| Is a temporary roadway proposed? | | X |
| Will the project involve the use of a detour or require a ramp closure? (describe below) | X | |
| Provisions will be made for access by local traffic and so posted. | X | |
| Provisions will be made for through-traffic dependent businesses. | X | |
| Provisions will be made to accommodate any local special events or festivals. | X | |
| Will the proposed MOT substantially change the environmental consequences of the action? | | X |
| Is there substantial controversy associated with the proposed method for MOT? | | X |
| Will the project require a sidewalk, curb ramp, and/or bicycle lane closure? (describe below) | | X |
| Provisions will be made for access by pedestrians and/or bicyclist and so posted (describe below). | | X |

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Discuss closures, detours, and/or facilities (if any) that will be provided for maintenance of traffic. Any known impacts from these temporary measures should be quantified to the extent possible, particularly with respect to properties such as Section 4(f) resources and wetlands. Discuss any pedestrian/bicycle closures. Any local concerns about access and traffic flow should be detailed as well.

The MOT for the project will require the closure of the intersection and a detour will be necessary. Road closure barricades and signage will be placed along all 4 quadrants of the intersection with detour signage along the official detour route. Westbound traffic will be routed to SR 39 S to US 6 West and ultimately US 421 North. Eastbound traffic will be routed to US 421 South to US 6 East, and ultimately SR 39 North.

The total detour length is approximately 15.6 miles with approximately 6.5 miles of additional travel length resulting in approximately 9-10 minutes of additional travel time. The detour will be in place for approximately 9-12 months. Access to all properties will be maintained for the duration of construction. The MOT sheets are provided in Appendix B, pages B-31 to B-32.

The LaPorte County Fair is located approximately 2.5 miles east of the project area. An unofficial local detour will be coordinated with the LaPorte County Engineer prior to construction.

The closure will pose a temporary inconvenience to traveling motorists (including school buses and emergency services); however, no significant delays are anticipated, and all inconveniences and delays will cease upon project completion.

ESTIMATED PROJECT COST AND SCHEDULE:

Engineering: \$300,000 (2023) Right-of-Way: \$250,000 (2025) Construction: \$2,150,000 (2027)

Note: The ROW amount may need updated, prior to the CE approval, based on the updated TIP and STIP.

Anticipated Start Date of Construction: Fall 2027

RIGHT OF WAY:

| Land Use Impacts | Amount (acres) | |
|-------------------|----------------|-----------|
| | Permanent | Temporary |
| Residential | 0.57 | 0.023 |
| Commercial | 0.26 | 0.096 |
| Agricultural Land | 1.19 | 0.00 |
| Forest | 0.00 | 0.00 |
| Wetlands | 0.00 | 0.00 |
| Other: | 0.00 | 0.00 |
| TOTAL | 2.02 | 0.119 |

Describe both Permanent and Temporary right-of-way and describe their current use. Typical and Maximum right-of-way widths (existing and proposed) should also be discussed. Any advance acquisition, reacquisition or easements, either known or suspected, and their impacts on the environmental analysis should be discussed.

The existing ROW along SR 2 is approximately 30 ft. north to south (full width) and approximately 40 ft. east to west (full width) along CR 500 W. The existing ROW consists of agricultural properties, maintained lawns, and roadside drainage.

The project requires a total of approximately 2.02 acres of new permanent ROW and reacquiring 2.35 acres of ROW as a result of the expanded roadway footprint for installation of the proposed roundabout, paved and gravel shoulders, medians and drainage improvements. The proposed new permanent ROW consists of 0.57 acres of residential land, 0.26 acre of commercial land, and 1.19 acres of agricultural vacant land. The reacquired ROW will consist of 1.17 acres of residential land, 0.50 acre of commercial land, and 0.68 acre of agricultural vacant land. The temporary ROW for the project will require 0.023 acre of residential land and 0.096 acre of commercial land for driveway construction. The new ROW acreage varies along both sides of each roadway to construct the roundabout and detention basin. The proposed ROW along SR 2 will extend approximately 100 ft. to 357 ft. north to south (full width). The permanent ROW along CR 500 W will extend approximately 40 ft. to 422 ft. east to west (full width).

If the scope of work or permanent or temporary ROW amounts change, the INDOT Environmental Services Division (ESD) and the INDOT District Environmental Section will be contacted immediately.

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Part III – Identification and Evaluation of Impacts of the Proposed Action

SECTION A - EARLY COORDINATION:

List the date(s) coordination was sent and all resource agencies that were contacted as a part of the development of this Environmental Study. Also, include the date of their response or indicate that no response was received.

Early coordination letters (ECL) were sent on April 1, 2024, January 29, 2025, and May 7, 2025, to the agencies listed below. A copy of the ECL is provided in Appendix C, pages C-1 to C-2.

| Agency | Date Sent | Response Received | Appendix C |
|--|---|-------------------------|--------------------|
| FHWA | April 1, 2024 and January 29, 2025 | No Response | N/A |
| US Department of Housing and Urban Development (HUD) | April 1, 2024 and January 29, 2025 | No Response | N/A |
| INDOT LaPorte District | April 1, 2024 and January 29, 2025 | No Response | N/A |
| INDOT Project Manager | April 1, 2024 and January 29, 2025 | No Response | N/A |
| Northwestern Indiana Regional Planning Commission (NIRPC) | April 1, 2024 and January 29, 2025 | No Response | N/A |
| Indiana Department of Natural Resources Division of Fish and Wildlife (IDNR-DFW) | April 1, 2024 and January 29, 2025 | April 17, 2024 | Pages C-4 to C-5 |
| US Fish and Wildlife Service (USFWS) Northern Indiana Field Office | April 1, 2024 and January 29, 2025 | April 1, 2024 | Pages C-6 to C-7 |
| INDOT, Office of Aviation | April 1, 2024 and January 29, 2025 | April 1, 2024 | Page C-31 |
| Indiana Geological and Water Survey (IGWS) | May 7, 2025 (automated) | May 7, 2025 (automated) | Pages C-26 to C-28 |
| LaPorte County Surveyor's Office | April 1, 2024 and January 29, 2025 | No Response | N/A |
| LaPorte County Highway Department | April 1, 2024 and January 29, 2025 | No Response | N/A |
| LaPorte County Board of Commissioners | April 1, 2024 and January 29, 2025 | No Response | N/A |
| LaPorte County Emergency Management Agency | April 1, 2024 and January 29, 2025 | No Response | N/A |
| LaPorte County Emergency Medical Services | April 1, 2024 and January 29, 2025 | No Response | N/A |
| LaPorte Fire Department | April 1, 2024 and January 29, 2025 | No Response | N/A |
| LaPorte Police Department | April 1, 2024 and January 29, 2025 | No Response | N/A |
| Natural Resources Conservation Service (NRCS) | April 1, 2024, January 29, 2025 and May 7, 2025 | May 13, 2025 | Pages C-29 to C-30 |
| | | | |

All applicable recommendations are included in the *Environmental Commitments* section of this Categorical Exclusion (CE) document.

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SECTION B – ECOLOGICAL RESOURCES:

| | <u>Presence</u> | <u>Impacts</u> | |
|--|-----------------|----------------|----|
| | | Yes | No |
| Streams, Rivers, Watercourses & Other Jurisdictional Features | | | |
| Federal Wild and Scenic Rivers | | | |
| State Natural, Scenic or Recreational Rivers | | | |
| Nationwide Rivers Inventory (NRI) listed | | | |
| Outstanding Rivers List for Indiana | | | |
| Navigable Waterways | | | |

Total stream(s) in project area: 0.0 Linear feet Total impacted stream(s): 0.0 Linear feet

| Stream Name | Classification | Total Size in Project Area (linear feet) | Impacted linear feet | Comments (i.e. location, flow direction, likely Water of the US, appendix reference) |
|-------------|----------------|--|----------------------|--|
| N/A | N/A | N/A | N/A | N/A |

Describe all streams, rivers, watercourses and other jurisdictional features adjacent or within the project area. Include whether or not impacts (both permanent and temporary) will occur to the features identified. Include if the streams or rivers are listed on any federal or state lists for Indiana. Include if features are likely subject to federal or state jurisdiction. Discuss measures to avoid, minimize, and mitigate if impacts will occur.

Based on the desktop review, the aerial map of the project area (Appendix B, page B-3) and the Red Flag Investigation (RFI) report (Appendix E, page E-3), there are no streams, rivers, watercourses, or other jurisdictional features within the 0.5-mile search radius. There are no streams, rivers, watercourses, or other jurisdictional features within or adjacent to the project area, which was confirmed by the site visit on August 2, 2023, conducted by Metric Environmental. Therefore, no impacts are expected.

A *Waters of the U.S. Determination/Wetland Delineation Report* was completed for the project on March 5, 2024. Please refer to Appendix F for the *Waters of the U.S. Determination/Wetland Delineation Report*. It was determined that no streams, rivers, watercourses, or other jurisdictional features are located within the project area. The US Army Corps of Engineers (USACE) makes all final determinations regarding jurisdiction.

| | <u>Presence</u> | <u>Impacts</u> | |
|-----------------------------------|-----------------|----------------|----|
| | | Yes | No |
| Open Water Feature(s) | | | |
| Reservoirs | | | |
| Lakes | | | |
| Farm Ponds | | | |
| Retention/Detention Basin | | | |
| Storm Water Management Facilities | | | |
| Other: _____ | | | |

Describe all open water feature(s) identified adjacent or within the project area. Include whether or not impacts (both permanent and temporary) will occur to the features identified. Include if features are likely subject to federal or state jurisdiction. Discuss measures to avoid, minimize, and mitigate if impacts will occur.

Based on the desktop review, the aerial map of the project area (Appendix B, page B-3), and the RFI report (Appendix E, page E-3), there is one open water feature within the 0.5-mile search radius. There are no open water features within or adjacent to the project area, which was confirmed by the site visit on August 2, 2023, conducted by Metric Environmental. Therefore, no impacts are expected.

A *Waters of the U.S. Determination/Wetland Delineation Report* was completed for the project on March 5, 2024. Please refer to Appendix F for the *Waters of the U.S. Determination/Wetland Delineation Report*. It was determined that no open water features are located within the project area. USACE makes all final determinations regarding jurisdiction.

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| | <u>Presence</u> | <u>Impacts</u> | Yes | No |
|-----------------|--|----------------|---|--|
| Wetlands | <div style="border: 1px solid black; padding: 2px 10px;">X</div> | | <div style="border: 1px solid black; padding: 2px 10px;"></div> | <div style="border: 1px solid black; padding: 2px 10px;">X</div> |

Total wetland area: 0.012 Acre(s) Total wetland area impacted: 0.00 Acre(s)

| Wetland No. | Classification | Total Size (Acres) | Impacted Acres | Comments (i.e. location, likely Water of the US, appendix reference) |
|-------------|----------------|--------------------|----------------|--|
| A | PEM1A | 0.012 | 0.00 | Located within a concave depression on the western edge of the Investigated Area (IA) along the south side of SR 2. Likely Water of the US (Appendix F, page F-3 to F-4) |

| | <u>Documentation</u> | <u>ESD Approval Dates</u> |
|---------------------------------------|--|---|
| Wetlands (Mark all that apply) | | |
| Wetland Determination | <div style="border: 1px solid black; padding: 2px 10px;">X</div> | <div style="border: 1px solid black; padding: 2px 10px;">3/6/2024</div> |
| Wetland Delineation | <div style="border: 1px solid black; padding: 2px 10px;">X</div> | <div style="border: 1px solid black; padding: 2px 10px;">3/6/2024</div> |
| USACE Isolated Waters Determination | <div style="border: 1px solid black; padding: 2px 10px;"></div> | <div style="border: 1px solid black; padding: 2px 10px;"></div> |

Improvements that will not result in any wetland impacts are not practicable because such avoidance would result in (Mark all that apply and explain):

- ☐ Substantial adverse impacts to adjacent homes, business or other improved properties;
- ☐ Substantially increased project costs;
- ☐ Unique engineering, traffic, maintenance, or safety problems;
- ☐ Substantial adverse social, economic, or environmental impacts, or
- ☐ The project not meeting the identified needs.

Describe all wetlands identified adjacent or within the project area. Include whether or not impacts (both permanent and temporary) will occur to the features identified. Include if features are likely subject to federal or state jurisdiction. Discuss measures to avoid, minimize, and mitigate if impacts will occur.

Based on the desktop review, the aerial map of the project area (Appendix B, page B-3), and the RFI report (Appendix E, page E-3), there are three wetlands within the 0.5-mile search radius. There is one wetland within or adjacent to the project area, which was confirmed by the site visit conducted on August 2, 2023, by Metric Environmental. One wetland, Wetland A, is located adjacent to the project area, west of the westernmost construction limits along the south side of SR 2. Wetland A is classified as a Palustrine, Emergent, Persistent, Temporarily Flooded (PEM1A) and is approximately 0.012 acre in size. The wetland is beyond the construction limits; thus, it will not be impacted. The wetland will be labeled "Do Not Disturb" on the project plans and has been included as a firm commitment in the Environmental Commitments section of this document.

A *Waters of the U.S. Determination/Wetland Delineation Report* was completed for the project on March 5, 2024. Please refer to Appendix F for the *Waters of the U.S. Determination/Wetland Delineation Report*. It was determined that there is one wetland located within or adjacent to the project area. USACE makes all final determinations regarding jurisdiction.

| | <u>Presence</u> | <u>Impacts</u> | Yes | No |
|----------------------------|--|----------------|--|---|
| Terrestrial Habitat | <div style="border: 1px solid black; padding: 2px 10px;">X</div> | | <div style="border: 1px solid black; padding: 2px 10px;">X</div> | <div style="border: 1px solid black; padding: 2px 10px;"></div> |

Total terrestrial habitat in project area: 2.22 Acre(s) Total tree clearing: 0.0014 Acre(s)

Describe types of terrestrial habitat (i.e. forested, grassland, farmland, lawn, etc) adjacent or within the project area. Include whether or not impacts will occur to habitat identified. Include total terrestrial habitat impacted and total tree clearing that will occur. Discuss measure to avoid, minimize, and mitigate if impacts will occur.

Based on a desktop review, a site visit on August 2, 2023, by Metric Environmental, and a review of the aerial map of the project area (Appendix B, page B-3), the predominant land use in the project area consists of residential dwellings with maintained lawns and agricultural/pasture fields. The dominant vegetation along the project roadway consists of red mulberry (*Morus rubra*) in the tree stratum, and red fescue (*Festuca rubra*) and common milkweed (*Asclepias syriaca*) in the herb stratum.

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Approximately 2.22 acres of terrestrial habitat consisting of residential maintained lawns, scrub shrub, mature trees, and cultivated agricultural land will be impacted to reconstruct the existing intersection.

Approximately 0.0014 acres of trees and brush will be removed from the northwest, northeast and southwest quadrants of the intersection. The dominant tree species to be removed is red mulberry. All efforts to minimize terrestrial impacts were considered during the design phase of the project. The construction limits have been reduced to the extent that is practical to build the project while limiting terrestrial disturbance. All disturbed areas will be stabilized, graded and re-seeded per INDOT standard specifications. No terrestrial habitat restoration or mitigation will be necessary.

The IDNR-DFW responded on April 17, 2024, with recommendations to minimize terrestrial impacts including revegetating all bare and disturbed areas that are not currently mowed and maintained with a mixture of grasses (Appendix C, pages C-4 to C-5). All applicable recommendations are included in the Environmental Commitments section of this CE document.

Protected Species

Federally Listed Bats

Information for Planning and Consultation (IPaC) determination key completed
 Section 7 informal consultation completed (IPaC cannot be completed)
 Section 7 formal consultation Biological Assessment (BA) required

| Yes | No |
|-------------------------------------|-------------------------------------|
| <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| <input type="checkbox"/> | <input checked="" type="checkbox"/> |

Determination Received for Listed Bats from USFWS: NE ☐ NLAA ☒ LAA ☐

Other Species not included in IPaC

Additional federal species found in project area (based on IPaC species list)
 State species (not bird) found in project area (based upon consultation with IDNR)

| Yes | No |
|--------------------------|-------------------------------------|
| <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| <input type="checkbox"/> | <input checked="" type="checkbox"/> |

Migratory Birds

Known usage or presence of birds (i.e. nests)
 State bird species based upon coordination with IDNR

| Yes | No |
|--------------------------|-------------------------------------|
| <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| <input type="checkbox"/> | <input checked="" type="checkbox"/> |

Discuss IDNR coordination and species identified. Describe USFWS Section 7 consultation and determination received for Indiana bat and northern long-eared bat impacts. Discuss if other federally listed species were identified. If so, include consultation that has occurred and the determination that was received. Discuss if migratory birds have been observed and any impacts.

Based on a desktop review and the RFI report (Appendix E, page E-4) completed by Metric Environmental on March 7, 2024, the IDNR LaPorte County Endangered, Threatened and Rare (ETR) Species List has been checked. According to the IDNR-DFW early coordination response letter dated April 17, 2024 (Appendix C, pages C-4 to C-5), the Natural Heritage Program's Database has been checked and no plant or animal species listed as state or federally threatened, endangered, or rare have been reported to occur in the project vicinity. An INDOT 0.5-mile bat review occurred on January 3, 2024, and no reports of the Indiana Bat or the Northern Long-Eared Bat (NLEB) have been documented within 0.5 mile of the project site.

Project information was submitted through the USFWS's Information for Planning and Consultation (IPaC) portal and an official species list was generated (Appendix C, pages C-16 to C-23). The project is within range of the federally endangered Indiana Bat (*Myotis sodalis*). Other species were generated in the IPaC species list along with the Indiana Bat.

The official species list generated from IPaC indicated three other species present within the project area: the Whooping Crane (*Grus americana*), the Salamander Mussel (*Simpsonaias ambigua*), and the Monarch Butterfly (*Danaus plexippus*).

The Whooping Crane is listed as endangered wherever found, except where listed as an experimental population according to the Environmental Conservation Online System (<https://ecos.fws.gov/ecp/species/758>). The Whooping Crane is listed as an experimental population in this location. The Salamander Mussel is proposed for future listing as an endangered species and the Monarch Butterfly is proposed for future listing as a threatened species. There are proposed critical habitats designated for these species; however, the project area does not overlap the designated critical habitat. The project will not jeopardize their continued existence and no further coordination with USFWS is required.

The project qualifies under the Programmatic Biological Opinion/Conference Opinion for Transportation Projects in the Range of the Indiana Bat, Northern Long-Eared Bat, and Tricolored Bat dated December 2024 between FHWA, Federal Railroad Administration (FRA), Federal Transit Administration (FTA), and USFWS.

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The effect determination key was completed using the Project Submittal Form on March 25, 2025, and based on the responses provided, the project was found to “May Affect/Not Likely to Adversely Affect” (NLAA) the Indiana Bat, the NLEB, and TCB (Appendix C, pages C-8 to C-15). INDOT reviewed and verified the effect finding on March 26, 2025, and requested USFWS’s review of the finding. On March 27, 2025, the USFWS concurred with the effect finding (Appendix C, page C-24).

Avoidance and Minimization Measures (AMMs) include directing temporary lighting away from suitable habitat, restricting tree clearing to what is specified in the plans, applying time-of-year restrictions for tree removal and ensuring all operators and contractors are aware of all environmental commitments and AMMs. The AMMs are included as firm commitments in the Environmental Commitments section of this document.

This precludes the need for further consultation on this project as required under Section 7 of the Endangered Species Act, as amended. If new information on endangered species at the site becomes available, or if project plans are changed, USFWS will be contacted for consultation.

Geological and Mineral Resources

Project located within the Indiana Karst Region
 Karst features identified within or adjacent to the project area
 Oil/gas or exploration/abandoned wells identified in the project area

| Yes | No |
|--------------------------|-------------------------------------|
| <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| <input type="checkbox"/> | <input checked="" type="checkbox"/> |

Date Karst Evaluation reviewed by INDOT EWPO (if applicable): N/A

Discuss if project is located in the Indiana Karst Region and if any karst features have been identified in the project area (from RFI). Discuss response received from IGWS coordination. Discuss if any mines, oil/gas, or exploration/abandoned wells were identified and if impacts will occur. Include discussion of karst study/report was completed and results. (Karst investigation must comply with the current Protection of Karst Features during Planning and Construction guidance and coordinated and reviewed by INDOT EWPO)

Based on a desktop review and the Indiana Karst Region map, the project is located outside the designated Indiana Karst Region as outlined in the most current *Protection of Karst Features during Project Development and Construction*. According to the topo map of the project area (Appendix B, page B-2) and the RFI report (Appendix E, page E-3), there are no karst features identified within or adjacent to the project area.

In the early coordination response dated May 7, 2025, IGWS did not indicate that karst features exist in the project area (Appendix C, pages C-24 to C-25). IGWS indicated there is a moderate liquefaction potential and a high potential for sand and gravel resources. The aforementioned geological features will not be affected because the scope of work will not involve deep excavation (i.e., greater than 5 ft. below ground surface). The response from IGWS has been communicated with the designer on June 5, 2025. No impacts are expected.

SECTION C – OTHER RESOURCES

Drinking Water Resources

Wellhead Protection Area(s)
 Source Water Protection Area(s)
 Water Well(s)
 Urbanized Area Boundary
 Public Water System(s)

Presence

| |
|----------|
| |
| |
| X |
| |
| |

Impacts

| Yes | No |
|-----|----------|
| | |
| | |
| | X |
| | |
| | |

Is the project located in the St. Joseph Sole Source Aquifer (SSA):

If Yes, is the FHWA/EPA SSA MOU Applicable?

If Yes, is a Groundwater Assessment Required?

| Yes | No |
|-----|----------|
| | X |
| | |
| | |

Check the appropriate boxes and discuss each topic below. Provide details about impacts and summarize resource-specific coordination responses and any mitigation commitments. Reference responses in the Appendix.

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The project is located in LaPorte County, which is not located within the area of the St. Joseph Sole Source Aquifer, the only legally designated sole source aquifer in the state of Indiana. Therefore, the FHWA/Environmental Protection Agency (EPA)/INDOT Sole Source Aquifer Memorandum of Understanding (MOU) is not applicable to this project. Therefore, a detailed groundwater assessment is not needed, and no impacts are expected.

The Indiana Department of Environmental Management's Wellhead Proximity Determinator website (<http://www.in.gov/idem/cleanwater/pages/wellhead/>) was accessed on May 6, 2025, by Metric Environmental. This project is not located within a Wellhead Protection Area or Source Water Area. No impacts are expected.

The IDNR Water Well Record Database website (<https://www.in.gov/dnr/water/3595.htm>) was accessed on May 7, 2025, by Metric Environmental. One well is located south of the project area. The feature will not be affected because the well is located outside of the construction limits. Therefore, no impacts are expected. Should it be determined during the right-of-way phase that this well will be affected, a cost to cure will likely be included in the appraisal to restore the well.

Based on a desktop review of the INDOT Municipal Separate Storm Sewer System (MS4) website (<https://www.in.gov/idem/cleanwater/ms4s-boundaries-map-for-indiana/>) by Metric Environmental on May 27, 2025, this project is not located within an Urban Area Boundary. No impact is expected.

Based on a desktop review, a site visit on August 2, 2023 by Metric Environmental, and the aerial map of the project area (Appendix B, page B-3), no public water systems were identified. Therefore, no impacts are expected.

| | <u>Presence</u> | <u>Impacts</u> | |
|---|--------------------------|--------------------------|--------------------------|
| | | <u>Yes</u> | <u>No</u> |
| Floodplains | | | |
| Project located within a regulated floodplain | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |
| Longitudinal encroachment | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |
| Transverse encroachment | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |
| Homes located in floodplain within 1000' up/downstream from project | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |

If applicable, indicate the Floodplain Level?

Level 1 ☐ Level 2 ☐ Level 3 ☐ Level 4 ☐ Level 5 ☐

Use the IDNR Floodway Information Portal to help determine potential impacts. Include floodplain map in appendix. Discuss impacts according to the classification system. If encroachment on a flood plain will occur, coordinate with the Local Flood Plain Administrator during design to insure consistency with the local flood plain planning.

The IDNR Indiana Floodway Information Portal website (<https://indnr.maps.arcgis.com/apps/webappviewer/index.html?id=05026dabc2e8461983e196d56a213c1e>) was accessed on December 8, 2023, by Metric Environmental. This project is not located in a regulatory floodplain as determined from approved IDNR floodplain maps (Appendix F, page F-10). Therefore, it does not fall within the guidelines for the implementation of 23 CFR 650, 23 CFR 771, and 44 CFR. No impacts are expected.

| | <u>Presence</u> | <u>Impacts</u> | |
|---------------------------|-------------------------------------|-------------------------------------|--------------------------|
| | | <u>Yes</u> | <u>No</u> |
| Farmland | | | |
| Agricultural Lands | <input checked="" type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| Prime Farmland (per NRCS) | <input checked="" type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |

Total Points (from Section VII of CPA-106/AD-1006*)

111

**If 160 or greater, see CE Manual for guidance.*

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Discuss existing farmland resources in the project area, impacts that will occur to farmland, and mitigation and minimization measures considered.

Based on a desktop review, a site visit on August 2, 2023, by Metric Environmental, and the aerial map of the project area (Appendix B, page B-3) the project will convert 2.22 acres of prime farmland as defined by the Farmland Protection Policy Act. An ECL was sent on May 7, 2025, to the NRCS. Coordination with NRCS resulted in a score of 111 on the AD- 1006 Form (Appendix C, page C-30). NRCS's threshold score for significant impacts to farmland that result in the consideration of alternatives is 160. Since this project score is less than the threshold, no significant loss of prime, unique, statewide, or local important farmland will result from this project. No alternatives other than those previously discussed in this document will be investigated without reevaluating impacts to prime farmland.

SECTION D – CULTURAL RESOURCES

| | Category(ies) and Type(s) | INDOT Approval Date(s) | N/A |
|--------------------------|---------------------------|------------------------|-----|
| Minor Projects PA | | | X |

Full 106 Effect Finding

No Historic Properties Affected ☒ No Adverse Effect ☐ Adverse Effect ☐

Eligible and/or Listed Resources Present

NRHP Building/Site/District(s) ☐ Archaeology ☐ NRHP Bridge(s) ☐

Documentation Prepared (mark all that apply)

APE, Eligibility and Effect Determination
 800.11 Documentation
 Historic Properties Report or Short Report
 Archaeological Records Check and Assessment
 Archaeological Phase Ia Survey Report
 Archaeological Phase Ic Survey Report
 Other:

| | ESD Approval Date(s) | SHPO Approval Date(s) |
|-------------------------------------|----------------------|-----------------------|
| <input checked="" type="checkbox"/> | September 10, 2024 | October 10, 2024 |
| <input checked="" type="checkbox"/> | July 2, 2025 | July 30, 2025 |
| <input checked="" type="checkbox"/> | September 10, 2024 | October 10, 2024 |
| <input checked="" type="checkbox"/> | March 12, 2025 | April 11, 2025 |
| <input checked="" type="checkbox"/> | March 12, 2025 | April 11, 2025 |
| <input type="checkbox"/> | | |
| <input type="checkbox"/> | | |

Memorandum of Agreement (MOA)

MOA Signature Dates (List all signatories)

If the project falls under the MPPA, describe the category(ies) that the project falls under and any approval dates. If the project requires full Section 106, use the headings provided. The completion of the Section 106 process requires that a Legal Notice be published in local newspapers. Please indicate the publication date, name of the paper(s) and the comment period deadline. Include any further Section 106 work which must be completed at a later date, such as mitigation from a MOA or avoidance commitments.

Area of Potential Effect:

Qualified professionals working for Metric Environmental and meeting the Secretary of the Interior's Professional Qualifications Standards defined an Area of Potential Effect (APE). The APE is "the geographic area or areas within which an undertaking may directly or indirectly cause alterations in the character or use of historic properties, if any such properties exist. The APE effect is influenced by the scale and nature of an undertaking and may be different for different kinds of effects caused by the undertaking" [36 CFR § 800.16(d)]. The APE includes all locations where the project may result in disturbance of the ground; all locations from which elements of the project may be visible or audible; all locations where activity may result in changes in traffic patterns, land use, or public access. For above-ground structures, the APE extends at least 500 ft. beyond the project end points, and at least one property deep on each side of SR 2 and CR 500 W. The APE for archaeology was the project footprint, including all proposed ROW, which was defined as encompassing 3.9 hectares (9.64 acres). (Appendix D, page D-11).

Coordination with Consulting Parties:

Section 106 of the National Historic Preservation Act (NHPA) requires federal agencies to take into account the effects of their undertakings on historic properties. In accordance with 36 CFR 800.2(c), individuals and groups with a demonstrated interest in the undertaking were invited to participate in efforts to identify historic properties potentially affected by the undertaking, assess its effects, and seek ways to avoid, minimize or mitigate any adverse effects on historic properties.

On December 8, 2023, an ECL was sent via letter or email to the State Historic Preservation Office (SHPO) and to other

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stakeholders and interested parties (Appendix D, pages D-39 to D-41). On February 12, 2024, INDOT-Cultural Resource Office (CRO) announced effective immediately, that the Turtle Mountain Band of Chippewa Indians (MBCI) will be added as a Tribal Consulting Party for projects in all Indiana counties, and they were then invited to serve as a consulting part to this undertaking. The ECL provided project information, project location maps, and invited recipients to become consulting parties to the undertaking. The parties were requested to indicate whether they agreed or did not agree to participate as a consulting party within 30 days of receipt of the invitation. It was noted that if the invited party did not reply, they would not be considered a consulting party and would not receive further information about the undertaking unless the scope changed. Below is a list of invited consulting parties for this undertaking:

| Invited Organization | Accepted/Declined |
|---|-------------------|
| Indiana State Historic Preservation Office | Accepted |
| NIRPC | No Response |
| LaPorte County Commissioners | No Response |
| LaPorte County Highway Superintendent | No Response |
| LaPorte County Highway Engineer | No Response |
| LaPorte County Historian | No Response |
| LaPorte County Genealogical Society | No Response |
| LaPorte County Historical Society | No Response |
| Westville Community Historical Society | No Response |
| Indiana Landmarks – Northern Regional Office | No Response |
| Indiana Lincoln Highway Association, Inc. | No Response |
| Preserve Historic La Porte | No Response |
| Eastern Shawnee Tribe of Oklahoma | No Response |
| Shawnee Tribe | Accepted |
| Miami Tribe of Oklahoma | No Response |
| Peoria Tribe of Indians of Oklahoma | No Response |
| Forest County Potawatomi Community | Accepted |
| Pokagon Band of Potawatomi Indians | No Response |
| MBCI | Accepted |

In an email dated December 11, 2023, the Forest County Potawatomi Community (FCPC) accepted the invitation to serve as a consulting party and offered a finding of No Historic Properties affected of significance to the FCPC. They issued their “standard caveat sent with each proposed project reviewed by the FCPC Tribal Historic Preservation Office (THPO)” which requested that work cease immediately and to be consulted if an inadvertent discovery occurred at any phase of the project (Appendix D, page D-43).

In an email dated December 20, 2023, Shawnee Tribe accepted the invitation to serve as a consulting party and concurred “that no known historic properties will be negatively impacted by this project.” They requested immediate consultation “in the event archaeological materials are encountered during construction, use, or maintenance of this location” (Appendix D, page D-44).

In a letter dated January 4, 2024, the SHPO acknowledged receipt of the ECL and noted they were not aware of any parties who should be invited to participate, “beyond those whom INDOT already has invited” (Appendix D, pages D-45 to D-46).

No other replies were received in response to the ECL.

Archaeology:

Pursuant to 36 CFR § 800.4(b), a Qualified Professional Archaeologist with Metric Environmental conducted an archaeological records check which involved a review the State Historical Architectural and Archaeological Research Database (SHAARD), site maps on file with the IDNR-Division of Historic Preservation and Archaeology, cultural resource management reports, cemetery records, and historical data. A Phase Ia Archaeological Survey was conducted on August 29 and September 19, 2024. No archaeological resources were identified in the project area. An Archaeological Short Report (ASR) was prepared by QP Samuel Snell of Metric Environmental (Snell, 3/12/2025). The report recommended the project proceed as planned. Excerpts of the ASR are provided in Appendix D, pages D-29 to D-32.

The ASR was submitted to INDOT-CRO for review and concurrence to the INDOT-CRO on March 12, 2025. INDOT-CRO

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responded with minor comments and indicated the report should be submitted once the comments were addressed. The ASR, and the document was uploaded to INSCOPE and distributed to the SHPO and tribes only for review and comment on March 12, 2025 (Appendix D, page D-58 to D-63).

In a letter dated April 11, 2025, SHPO acknowledged receipt of the ASR and concurred with the archaeological report's conclusions that there are no National Register of Historic Places (NRHP) eligible archaeological sites within the project's APE (Appendix D, pages D- 64 to D- 65).

Historic Properties:

Pursuant to 36 CFR § 800.4(b), personnel with Metric Environmental, who meets the Secretary of the Interior's Professional Qualification Standards as per 36 CFR Part 61, reviewed the Indiana SHAARD, the 1989 *LaPorte County Interim Report: Indiana Historic Sites and Structures Inventory*, the 2009 INDOT-sponsored Indiana Historic Bridge Inventory (IHBI; Mead & Hunt), the NRHP Database, the Indiana Historic Buildings, Bridges, and Cemeteries Map (IHBBCM), and the CRO Public Web Map Application. These resources revealed that the APE does not contain any previously surveyed or previously determined NRHP eligible sites. The results of field surveys were reported in a Historic Property Short Report (HPSR) (Vorndran/Receveur, 9/10/2024). The HPSR recommended no resources within the APE as eligible for listing in the NRHP. Excerpts of the HPSR are provided in Appendix D, pages D- 25 to D- 27.

The HPSR was submitted to INDOT-CRO for review and concurrence and the HPSR was distributed to consulting parties for review and comment on September 10, 2024 (Appendix D, pages D-51 to D-54).

In an email dated September 11, 2024, the MBCI stated, "MBCI THPO office defer to the nearest THPO/SHPO office to the APE." (Appendix D, page D-55).

SHPO concurred with the limits of the established APE and with the findings of the HPSR in a letter dated October 10, 2024 (Appendix D, page D-56).

In an email dated November 12, 2024, the Shawnee Tribe concurred "that no historic properties will be negatively impacted by this project," though they noted that "there is still potential for the discovery of unknown resources" (Appendix D, page D-57).

No other responses were received.

Documentation, Findings:

On July 2, 2025, INDOT approved the APE and issued a "No Historic Properties Affected" finding (Appendix D, page D-3).

On July 3, 2025 the finding of "No Historic Properties Affected" and 800.11 documentation was provided to the SHPO and other consulting parties for a 30-day review and comment period (Appendix D, pages D-68 to D-72).

On July 9, 2025 the Shawnee Tribe responded that they have no issues or concerns at this time and to please continue with the project as planned. They request, in the event archaeological materials are encountered during construction, to please re-notify them (Appendix D, pages D-73 to D-74).

On July 30, 2025 SHPO responded and concurred with the "No Historic Properties Affected" finding (Appendix D, pages D-1 to D-2).

On August 6, 2025, the Forest County Potawatomi Community responded that they offer a finding of "No Historic Properties Affected" and they wish to remain as a consulting party for this project. In the event an inadvertent discovery occurs at any time please re-notify them (Appendix D, pages D-75 to D-76).

No other responses were received.

Public Involvement:

To meet the public involvement requirements of Section 106, a legal notice of FHWA Indiana Division's finding of "No Historic Properties Affected" was published in the *La Porte County Herald-Dispatch* on July 9, 2025 offering the public an opportunity to submit comment pursuant to 36 CFR 800.2(d), 800.3(e), and 800.6(a)(4). The public comment period closed after 30 days on August 8, 2025. No comments were received.

The legal notice and the affidavit of publication are provided in Appendix D, page D-105.

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No further consultation is required. This completes the Section 106 process and the responsibilities of the FHWA under Section 106 have been fulfilled.

SECTION E – SECTION 4(f) RESOURCES/ SECTION 6(f) RESOURCES

| | <u>Presence</u> | <u>Use</u> | |
|--|----------------------|----------------------|----------------------|
| | | <u>Yes</u> | <u>No</u> |
| Parks and Other Recreational Land | | | |
| Publicly owned park | <input type="text"/> | <input type="text"/> | <input type="text"/> |
| Publicly owned recreation area | <input type="text"/> | <input type="text"/> | <input type="text"/> |
| Other (school, state/national forest, bikeway, etc.) | <input type="text"/> | <input type="text"/> | <input type="text"/> |
| Wildlife and Waterfowl Refuges | | | |
| National Wildlife Refuge | <input type="text"/> | <input type="text"/> | <input type="text"/> |
| National Natural Landmark | <input type="text"/> | <input type="text"/> | <input type="text"/> |
| State Wildlife Area | <input type="text"/> | <input type="text"/> | <input type="text"/> |
| State Nature Preserve | <input type="text"/> | <input type="text"/> | <input type="text"/> |
| Historic Properties | | | |
| Site eligible and/or listed on the NRHP | <input type="text"/> | <input type="text"/> | <input type="text"/> |

Evaluations
Prepared

| | |
|---|----------------------|
| Programmatic Section 4(f) | <input type="text"/> |
| “De minimis” Impact | <input type="text"/> |
| Individual Section 4(f) | <input type="text"/> |
| Any exception included in 23 CFR 774.13 | <input type="text"/> |

Discuss Programmatic Section 4(f) and “de minimis” Section 4(f) impacts in the discussion below. Individual Section 4(f) documentation must be included in the appendix and summarized below. Discuss proposed alternatives that satisfy the requirements of Section 4(f). FHWA has identified various exceptions to the requirement for Section 4(f) approval. Refer to 23 CFR § 774.13 - Exceptions.

Section 4(f) of the U.S. Department of Transportation Act of 1966 prohibits the use of certain public and historic lands for federally funded transportation facilities unless there is no feasible and prudent alternative. The law applies to significant publicly owned parks, recreation areas, wildlife / waterfowl refuges, and NRHP eligible or listed historic properties regardless of ownership. Lands subject to this law are considered Section 4(f) resources.

Based on a desktop review, the aerial map of the project area (Appendix B, page B-3), and the RFI report (Appendix E, page E-2), one trail segment was identified within the 0.5-mile search radius. The trail segment, identified as *Porter County Line to LaPorte*, is a planned trail by NIRPC that will extend the SR 2/Westville Trail Corridor and is planned as a future project. Since the trail has not been constructed yet, it is not currently a Section 4(f) resource. If the trail does get constructed, it will qualify as a Section 4(f) resource since it will be publicly owned and provide a public recreational use. The construction of the project will not preclude construction of the trail in the future. Based on the site visit conducted on August 2, 2023, by Metric Environmental, no trails were observed and there are no Section 4(f) resources located within or adjacent to the project area.

Therefore, no Section 4 (f) use is expected.

Section 6(f) Involvement

| | <u>Presence</u> | <u>Use</u> | |
|------------------------------|----------------------|----------------------|----------------------|
| | | <u>Yes</u> | <u>No</u> |
| Section 6(f) Property | <input type="text"/> | <input type="text"/> | <input type="text"/> |

Discuss Section 6(f) resources present or not present. Discuss if any conversion would occur as a result of this project. If conversion will occur, discuss the conversion approval.

The U.S. Land and Water Conservation Fund Act of 1965 established the Land and Water Conservation Fund (LWCF), which was created to preserve, develop, and assure accessibility to outdoor recreation resources. Section 6(f) of this Act prohibits conversion of lands purchased with LWCF monies to a non-recreation use.

A review of Section 6(f) properties on the INDOT ESD website revealed a total of twelve properties in LaPorte County (Appendix I, page I-15). None of these properties are located within or adjacent to the project area. Therefore, there will be no impacts to 6(f)

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resources.

SECTION F – Air Quality

STIP/TIP and Conformity Status of the Project

Is the project in the most current STIP/TIP?

Is the project located in an MPO Area?

Is the project in an air quality non-attainment or maintenance area?

If Yes, then:

Is the project in the most current MPO TIP?

Is the project exempt from conformity?

If No, then:

Is the project in the Transportation Plan (TP)?

Is a hot spot analysis required (CO/PM)?

| Yes | No |
|-------------------------------------|--------------------------|
| <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| <input type="checkbox"/> | <input type="checkbox"/> |
| <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| <input type="checkbox"/> | <input type="checkbox"/> |
| <input type="checkbox"/> | <input type="checkbox"/> |
| <input type="checkbox"/> | <input type="checkbox"/> |

Location in STIP:

Name of MPO (if applicable):

Location in TIP (if applicable):

Fiscal Year (FY) 2026-2030

NIRPC

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Level of MSAT Analysis required?

Level 1a ☒ Level 1b ☐ Level 2 ☐ Level 3 ☐ Level 4 ☐ Level 5 ☐

Describe if the project is listed in the STIP and if it is in a TIP. Describe the attainment status of the county(ies) where the project is located. Indicate whether the project is exempt from a conformity determination. If the project is not exempt, include information about the TP and TIP. Describe if a hot spot analysis is required and the MSAT Level.

This project is included in the FY 2026-2030 NIRPC Metropolitan Planning Organization (MPO) Transportation Improvement Plan (TIP), which has been incorporated by reference into the FY 2026-2030 Statewide Transportation Improvement Program (STIP) (Appendix H, pages H-1 to H-5). The ROW funds are in the process of being incorporated into the STIP/TIP. The updated STIP/TIP will be incorporated into this document once it has been approved.

This project is located in LaPorte County, which is currently a maintenance area for 8-Hour Ozone (2007) under the EPA Nonattainment/Maintenance Status List located at (https://www3.epa.gov/airquality/greenbook/anayo_in.html).

This project has been identified as being exempt from air quality analysis in accordance with 40 CFR Part 93.126 and this project is not a project of air quality concern (40 CFR Part 93.123). Therefore, the project will have no significant impact on air quality.

This project is of a type qualifying as a CE (Group 1) under 23 CFR 771.117(c) or exempt under the Clean Air Act conformity rule under 40 CFR 93.126, and as such, a Mobile Source Air Toxics analysis is not required.

SECTION G - NOISE

Noise

Is a noise analysis required in accordance with FHWA regulations and INDOT's traffic noise policy?

Yes

No

☐
☒

Date Noise Analysis was approved/technically sufficient by INDOT ESD: N/A

Describe if the project is a Type I or Type III project. If it is a Type I project, describe the studies completed to date and if noise impacts were identified. If noise impacts were identified, describe if abatement is feasible and reasonable and include a statement of likelihood.

This project is a Type III project. In accordance with 23 CFR 772 and the current *Indiana Department of Transportation Traffic Noise Analysis Procedure*, this action does not require a formal noise analysis.

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SECTION H – COMMUNITY IMPACTS

Regional, Community & Neighborhood Factors

Will the proposed action comply with the local/regional development patterns for the area?

Yes

☒

No

☐

Will the proposed action result in substantial impacts to community cohesion?

☒

Will the proposed action result in substantial impacts to local tax base or property values?

☒

Will construction activities impact community events (festivals, fairs, etc.)?

☒

Does the community have an approved transition plan?

☒

☐

If No, are steps being made to advance the community's transition plan?

☐

☐

Does the project comply with the transition plan? (explain in the discussion below)

☒

☐

Discuss how the project complies with the area's local/regional development patterns; whether the project will impact community cohesion; and impact community events. Discuss how the project conforms with the ADA Transition Plan.

The HUD was consulted on April 1, 2024 and January 29, 2025 as part of the early coordination process regarding possible regional, community or neighborhood factors associated with this project. No response was received.

On May 27, 2025, Metric conducted an on-line review of the Indiana Festivals website (<http://www.indianafestivals.org>). The LaPorte County Fair is located approximately 2.5 miles east of the project area. If an event is planned during construction, the detour will provide access to the fairgrounds.

The Americans with Disabilities Act (ADA) requires a transition plan by local and state governments. Such a plan includes how the government will remove barriers to accessibility over time for persons with disabilities, such as installing curb ramps at intersections, making a web site accessible for persons with low vision, ensuring public meetings are fully accessible to persons with disabilities and other related issues. LaPorte County has an approved ADA transition plan; however, there are no ADA applicable structures that are to be installed as a part of the preferred alternative.

This project will not change the general development patterns, population density, or residential or commercial growth rate of the community. Furthermore, there will be no permanent impacts to community cohesion, local mobility, access, pedestrian or motorist safety or emergency services as a result of the project. The project will not have any adverse impacts to the local tax base or property values.

Public Facilities and Services

Discuss what public facilities and services are present in the project area and impacts (such as MOT) that will occur to them. Include how the impacts have been minimized and what coordination has occurred. Some examples of public facilities and services include health facilities, educational facilities, public and private utilities, emergency services, religious institutions, airports, transportation or public pedestrian and bicycle facilities.

Based on a desktop review, the aerial map of the project area (Appendix B, page B-3), and the RFI report (Appendix E, page E-2) one trail segment was identified within the 0.5-mile search radius. The trail segment, identified as *Porter County Line to LaPorte*, is a potential extension of the SR 2/Westville Trail Corridor that will cross through the project area in the future. The project will not impact the trail because the trail hasn't been constructed yet. An ECL was sent to the NIRPC on April 1, 2024 and January 29, 2025 and no response was received. One public-use airport, the LaPorte Municipal Airport, is located approximately 3.05 miles southeast of the project area.

The INDOT Office of Aviation responded on April 1, 2024, stating that no tall structure permit is required for the project if all equipment being used is under 147 ft. in height (Appendix C, C-31). The scope of this project (intersection improvement) will not require the utilization of any tall structure equipment.

There are no other public facilities within or adjacent to the project area, this was confirmed by the site visit on August 2, 2023 conducted by Metric Environmental. Therefore, no impacts are expected. Access to all properties will be maintained during construction.

It is the responsibility of the project sponsor to notify school corporations and emergency services at least two weeks prior to any construction that would block or limit access.

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Environmental Justice (EJ)

Environmental Justice (EJ) (Presidential EO 12898)

During the development of the project were EJ issues identified?

Does the project require an EJ analysis?

If YES, then:

Are any EJ populations located within the project area?

Will the project result in adversely high and disproportionate impacts to EJ populations?

| Yes | No |
|--------------------------|--------------------------|
| <input type="checkbox"/> | <input type="checkbox"/> |
| <input type="checkbox"/> | <input type="checkbox"/> |
| <input type="checkbox"/> | <input type="checkbox"/> |
| <input type="checkbox"/> | <input type="checkbox"/> |

Indicate if EJ issues were identified during project development. If an EJ analysis was not required, discuss why. If an EJ analysis was required, describe how the EJ population was identified. Include if the project has a disproportionately high or adverse effect on EJ populations and explain your reasoning. If yes, describe actions to avoid, minimize and mitigate these effects.

Due to the issuance of recent federal Executive Orders (EO) from January 2025, including EO 14154, EO 14148, and EO 14173, EO 12898 has been rescinded and this section is no longer applicable.

Relocation of People, Businesses or Farms

Will the proposed action result in the relocation of people, businesses or farms?

Is a BIS or CSRS required?

| Yes | No |
|--------------------------|-------------------------------------|
| <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| <input type="checkbox"/> | <input checked="" type="checkbox"/> |

Number of relocations: Residences: 0 Businesses: 0 Farms: 0 Other: 0

Discuss any relocations that will occur due to the project. If a BIS or CSRS is required, discuss the results in the discussion below.

No relocations of people, businesses or farms will take place as a result of this project.

SECTION I – HAZARDOUS MATERIALS & REGULATED SUBSTANCES

Documentation

Hazardous Materials & Regulated Substances (Mark all that apply)

Red Flag Investigation (RFI)

Phase I Environmental Site Assessment (Phase I ESA)

Phase II Environmental Site Assessment (Phase II ESA)

Design/Specifications for Remediation required?

| |
|-------------------------------------|
| <input checked="" type="checkbox"/> |
| <input type="checkbox"/> |
| <input type="checkbox"/> |
| <input type="checkbox"/> |

Date RFI concurrence by INDOT SAM (if applicable): March 7, 2024

Include a summary of the potential hazardous material concerns found during review. Discuss in depth sites found within, directly adjacent to, or ones that could impact the project area. Refer to current INDOT SAM guidance. If additional documentation (special provisions, pay quantities, etc.) will be needed, include in discussion. Include applicable commitments.

Based on a review of Geographic Information System (GIS) and available public records, an RFI was completed by Metric Environmental on March 7, 2024, and INDOT-Site Assessment and Management (SAM) provided their concurrence on March 7, 2024. One Underground Storage Tank (UST) and one National Pollutant Discharge Elimination System (NPDES) facility were identified in or within 0.5-mile of the project area (Appendix E, pages E-4 to E-5).

None of the hazmat sites identified will impact the project. Further investigation for hazardous material concerns is not required at this time.

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Part IV – Permits and Commitments

PERMITS CHECKLIST

Permits (mark all that apply)

Likely Required

Army Corps of Engineers (404/Section10 Permit)

Nationwide Permit (NWP)

Regional General Permit (RGP)

Individual Permit (IP)

Other

IN Department of Environmental Management (401/Rule 5)

Nationwide Permit (NWP)

Regional General Permit (RGP)

Individual Permit (IP)

Isolated Wetlands

Rule 5

Other

IN Department of Natural Resources

Construction in a Floodway

Navigable Waterway Permit

Other

Mitigation Required

US Coast Guard Section 9 Bridge Permit

Others (Please discuss in the discussion below)

| |
|--|
| |
| |
| |
| |

| |
|---|
| |
| |
| |
| |
| X |
| |

| |
|--|
| |
| |
| |
| |
| |
| |

List the permits likely required for the project and summarize why the permits are needed, including permits designated as "Other."

The project will require a CSGP due to the disturbance of more than 1.0 acre of land.

Applicable recommendations provided by resource agencies are included in the Environmental Commitments section of this document. If permits are found to be necessary, the conditions of the permit will be requirements of the project and will supersede these recommendations.

It is the responsibility of the project sponsor to identify and obtain all required permits.

ENVIRONMENTAL COMMITMENTS

List all commitments and include the name of agency/organization requesting/requiring the commitment(s). Listed commitments should be numbered.

Firm:

1. If the scope of work or permanent or temporary right-of-way amounts change, the INDOT Environmental Services Division (ESD) and the INDOT District Environmental Section will be contacted immediately. (INDOT ESD)
2. It is the responsibility of the project sponsor to notify school corporations and emergency services at least two weeks prior to any construction that would block or limit access. (INDOT ESD)
3. Any work in a wetland area within INDOT's right of way or in borrow/waste areas is prohibited unless specifically allowed in the US Army Corps of Engineers or IDEM permit. (INDOT EWPSO)
4. GAMM 1: Ensure all operators, employees, and contractors working in areas of Indiana Bat, NLEB, or TCB suitable habitat are aware of all Transportation Agency environmental commitments, including all applicable AMMs. (USFWS)
5. LAMM 1: Direct temporary lighting away from suitable habitat during the active season. (USFWS)
6. LAMM 2: When installing new/additional permanent lighting or replacing existing permanent lights, use downward-facing, full cut-off lens lights (with same intensity or less for replacement lighting); or for those transportation agencies

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using the Backlight Uplight and Glare (BUG) system developed by the Illuminating Engineering Society, the project should be as close to 0 for all three ratings with a priority of "uplight" of 0 and "backlight" as low as practicable. (USFWS)

7. TRTAMM 1: Modify all phases/aspects of the project (e.g., temporary work areas, alignments) to the extent practicable to avoid tree removal/trimming in excess of what is required to implement the project safely. (USFWS)
8. TRTAMM 2: Ensure tree removal/trimming is limited to that specified in project plans and ensure that contractors understand clearing limits and how they are marked in the field (e.g. install bright colored flagging/fencing prior to any tree removal/trimming to ensure contractors stay within clearing limits.) (USFWS)
9. TRTAMM 3: Ensure tree removal/trimming is limited to the inactive season, occurs within 100 ft. of the road/rail surface, and is outside of documented habitat for the Indiana bat, NLEB, and TCB. (USFWS)
10. Wetland A will be labeled "Do Not Disturb" on the project plans. (INDOT ESD).

For Further Consideration:

1. Plant five trees, 1 inch to 2 inches in diameter-at-breast height, for each tree which is removed that is 10 inches or greater in diameter-at-breast height. (IDNR-DFW).

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APPENDIX A

INDOT Supporting Documentation

Categorical Exclusion Level Thresholds

| | PCE | Level 1 | Level 2 | Level 3 | Level 4 ¹ |
|--|--|---|-------------------------------------|------------------------------|--|
| Section 106 | Falls within guidelines of Minor Projects PA | “No Historic Properties Affected” | “No Adverse Effect” | - | “Adverse Effect” Or Historic Bridge involvement ² |
| Stream Impacts³ | No construction in waterways or water bodies | < 300 linear feet of stream impacts | ≥ 300 linear feet of stream impacts | - | USACE Individual 404 Permit ⁴ |
| Wetland Impacts³ | No adverse impacts to wetlands | < 0.1 acre | - | < 1.0 acre | ≥ 1.0 acre |
| Right-of-way⁵ | Property acquisition for preservation only or none | < 0.5 acre | ≥ 0.5 acre | - | - |
| Relocations⁶ | None | - | - | < 5 | ≥ 5 |
| Threatened/Endangered Species (Species Specific Programmatic for Indiana bat & northern long eared bat)* | “No Effect”, “Not likely to Adversely Affect” (With select AMMs ⁷) | “Not likely to Adversely Affect” (With any AMMs or commitments) | - | “Likely to Adversely Affect” | Project does not fall under Species Specific Programmatic ⁸ |
| Threatened/Endangered Species (Any other species)* | Falls within guidelines of USFWS 2013 Interim Policy or “No Effect” | “Not likely to Adversely Affect” | - | - | “Likely to Adversely Affect” |
| Environmental Justice | No disproportionately high and adverse impacts | - | - | - | Potential ⁹ |
| Sole Source Aquifer | No Detailed Groundwater Assessment | - | - | - | Detailed Groundwater Assessment |
| Floodplain | No Substantial Impacts | - | - | - | Substantial Impacts |
| Section 4(f) Impacts | None | - | - | - | Any ¹⁰ |
| Section 6(f) Impacts | None | - | - | - | Any |
| Permanent Traffic Alteration | None | - | - | - | Any |
| Noise Analysis Required | No | - | - | - | Yes |
| Air Quality Analysis Required | No | - | - | - | Yes ¹¹ |
| Approval Level <ul style="list-style-type: none"> • District Env. (DE) • Env. Serv. Div. (ESD) • FHWA | Concurrence by DE or ESD | DE or ESD | DE or ESD | DE and/or ESD | DE and/or ESD; and FHWA |

¹ Coordinate with INDOT Environmental Services Division. INDOT will then coordinate with the appropriate FHWA Environmental Specialist.

² Any involvement with a bridge processed under the Historic Bridge Programmatic Agreement.

³ Total permanent impacts to streams (linear feet) and wetlands (acres).

⁴ US Army Corps of Engineers Individual 404 Permit

⁵ Total permanent and temporary right-of-way. This does not include reacquisition of existing apparent right-of-way.

⁶ If any relocations are within an area with a known or suspected Environmental Justice (EJ) or disadvantaged population, or has greater than 5 relocations, a conversation with FHWA, through INDOT ESD, is needed to confirm NEPA classification and outreach plan for the project.

⁷ Avoidance and Mitigation Measures (AMMs) determined by the IPAC determination key to be required that are not tree AMMs, bridge AMMs, or structure AMMs.

⁸ Projects that do not fall under a Species Specific Programmatic and results in a “Likely to Adversely Affect”. Other findings can be processed as a lower-level CE.

⁹ Potential for causing a disproportionately high and adverse impact.

¹⁰ Section 4(f) use resulting in an Individual, Programmatic, or *de minimis* evaluation. The only exception is a *de minimis* evaluation for historic properties (Effective January 2, 2020). If a historic property *de minimis* and no other use, mark the *None* column.

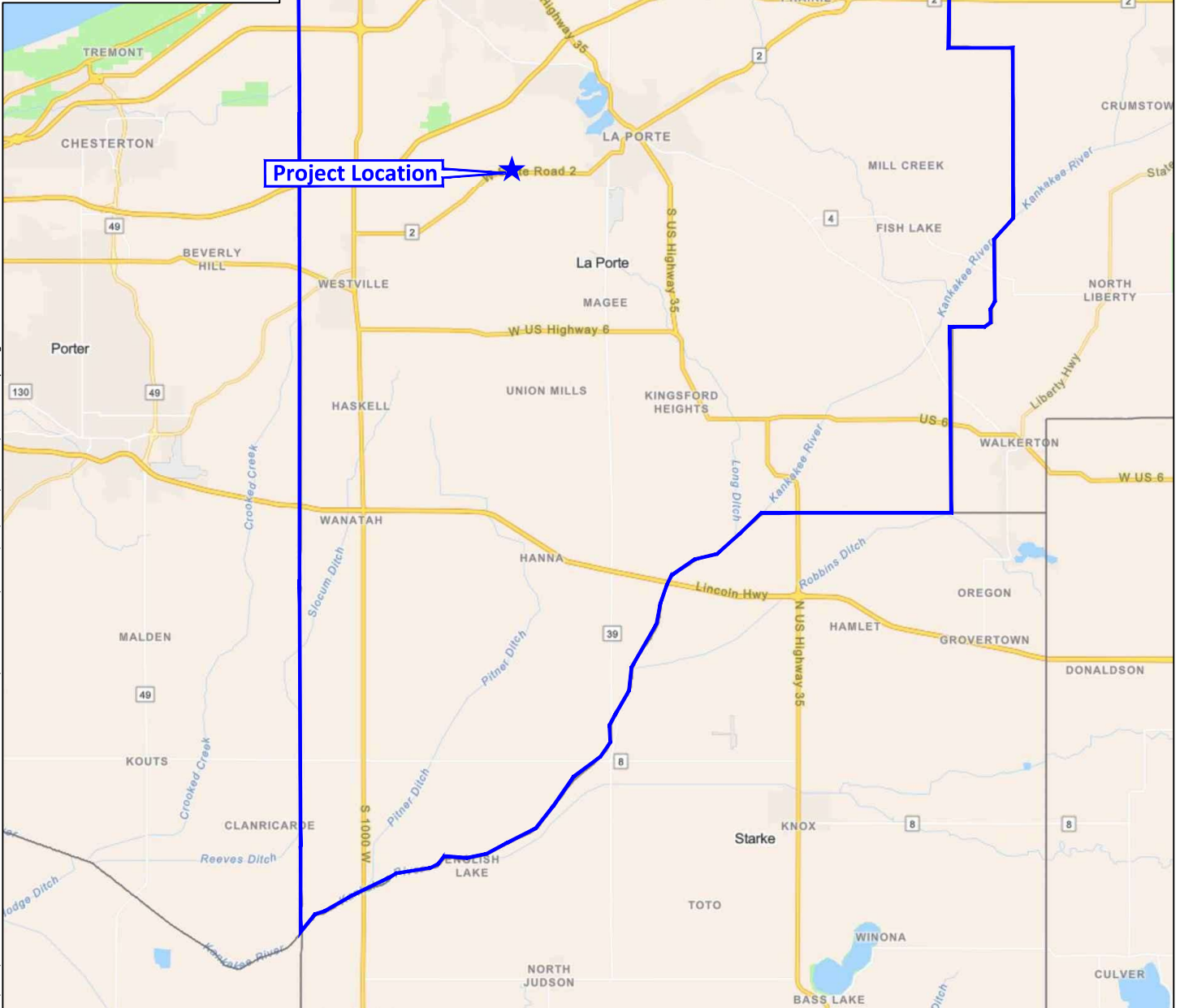
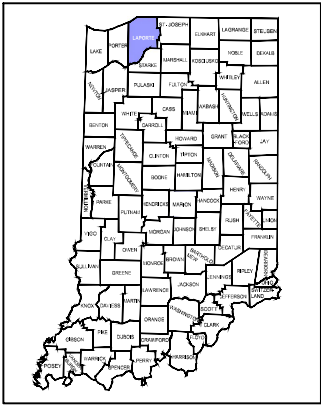
¹¹ Hot Spot Analysis and/or MSAT Quantitative Emission Analysis.

* Includes the threatened/endangered species critical habitat

Note: Substantial public or agency controversy may require a higher-level NEPA document.

APPENDIX B

Graphics

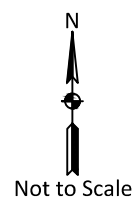


Source: <https://viewer.indianamap.org/>

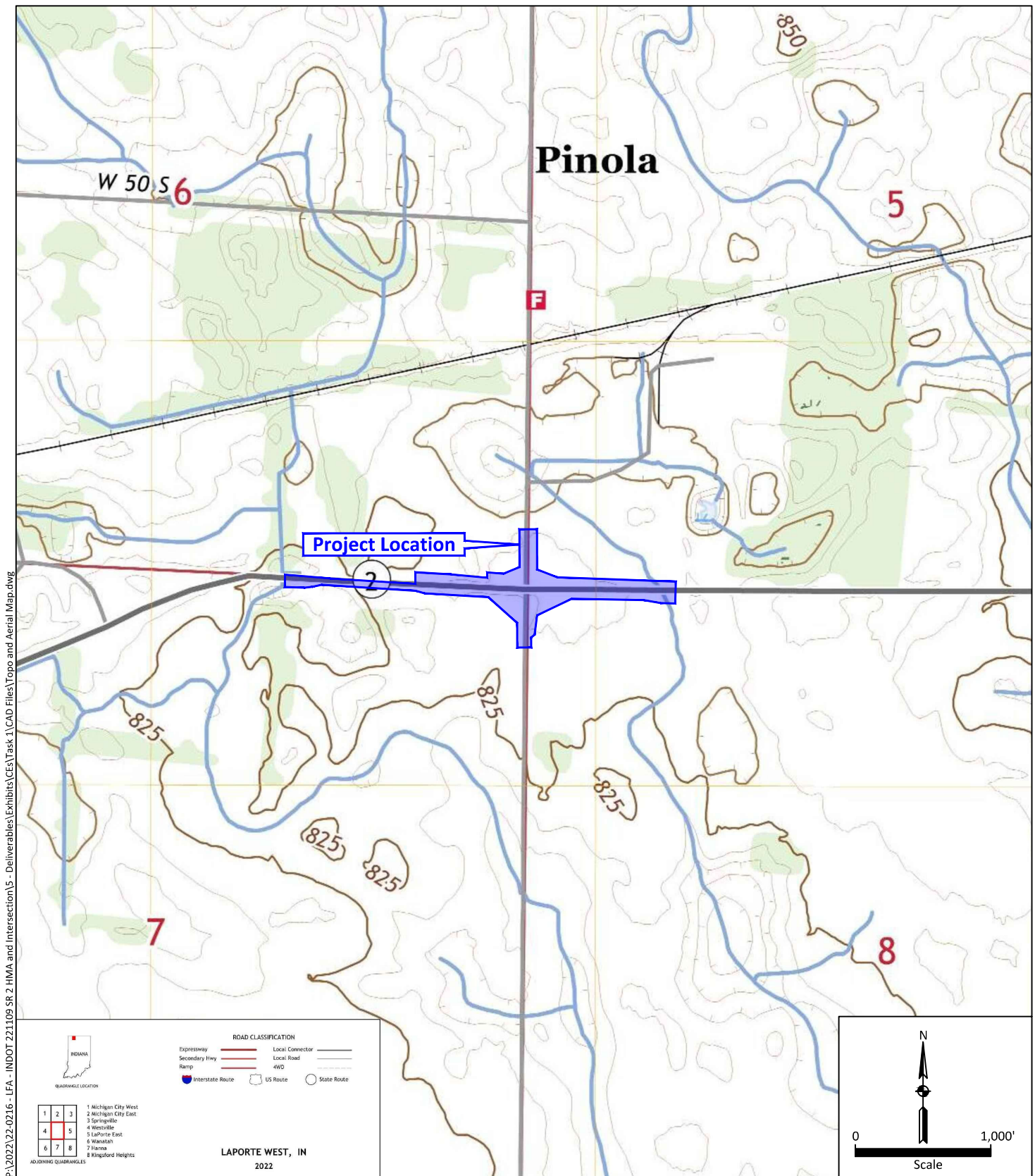
Location Map

Intersection Improvement Project
SR 2 and CR 500 West
Scipio Township, LaPorte County, IN
Des. No. 2200043
Metric Project No. 22-0216-1

All locations approximate



Drawn by: ILJ
Checked by: SW
Approved by: SC
Date: May, 2025



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Source: <https://ngmdb.usgs.gov/topoview/viewer/#13/41.5998/-86.8111>

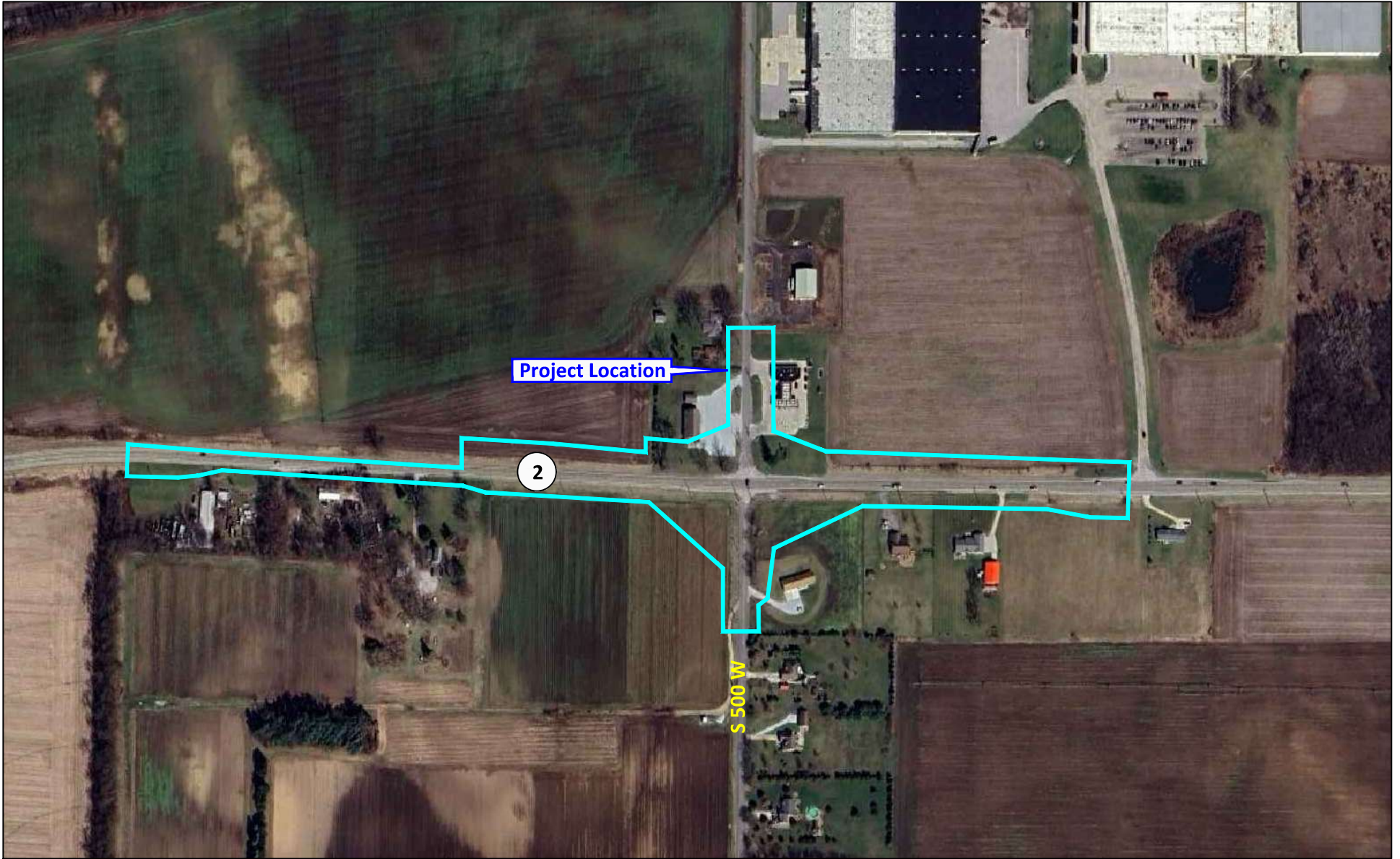
USGS Topographic Map

Intersection Improvement Project
SR 2 and CR 500 West
Scipio Township, LaPorte County, IN
Des. No. 2200043
Metric Project No. 22-0216-1

All locations approximate

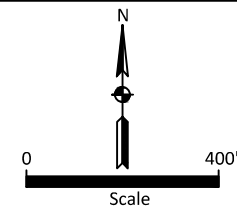


Drawn by: ILJ
Checked by: SW
Approved by: LBH
Date: May, 2025

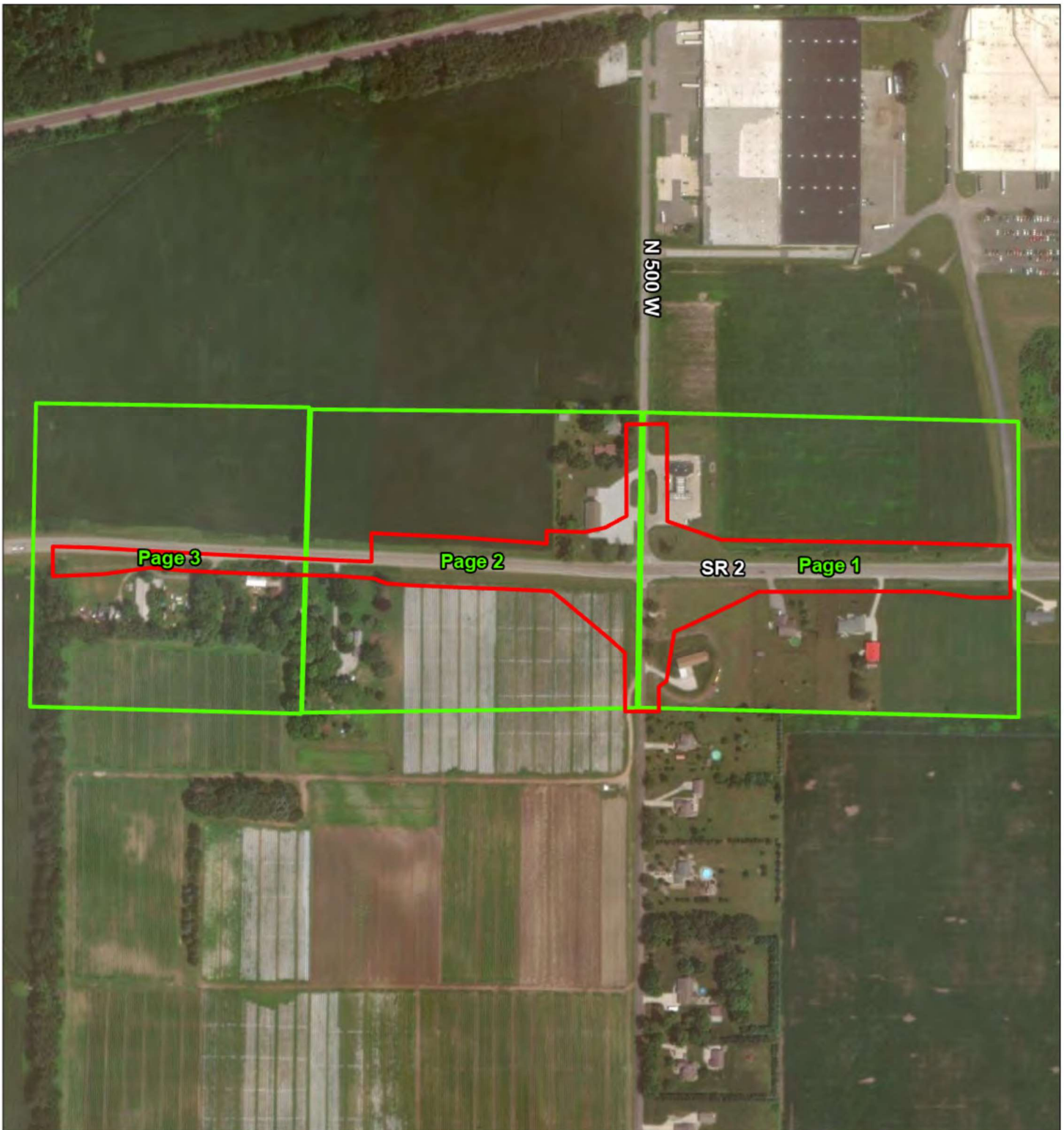


2024 Aerial Photograph
 Intersection Improvement Project
 SR 2 and CR 500 West
 Scipio Township, LaPorte County, IN
 Des. No. 2200043
 Metric Project No. 22-0216-1

Note: All locations are approximate



Drawn by: ILJ
 Checked by: SW
 Approved by: LBH
 Date: May, 2025



 Investigated Area (IA) Page Number

Exhibit 5 & Exhibit 6 - Reference Map
 SR 2 - Intersection Improvement
 Roundabout Installation
 LaPorte County, Indiana
 Des. No. 2200043
 Metric Project No. 22-0216-1
 Map Date: 7/31/2023
 Map Author: Seth Snyder

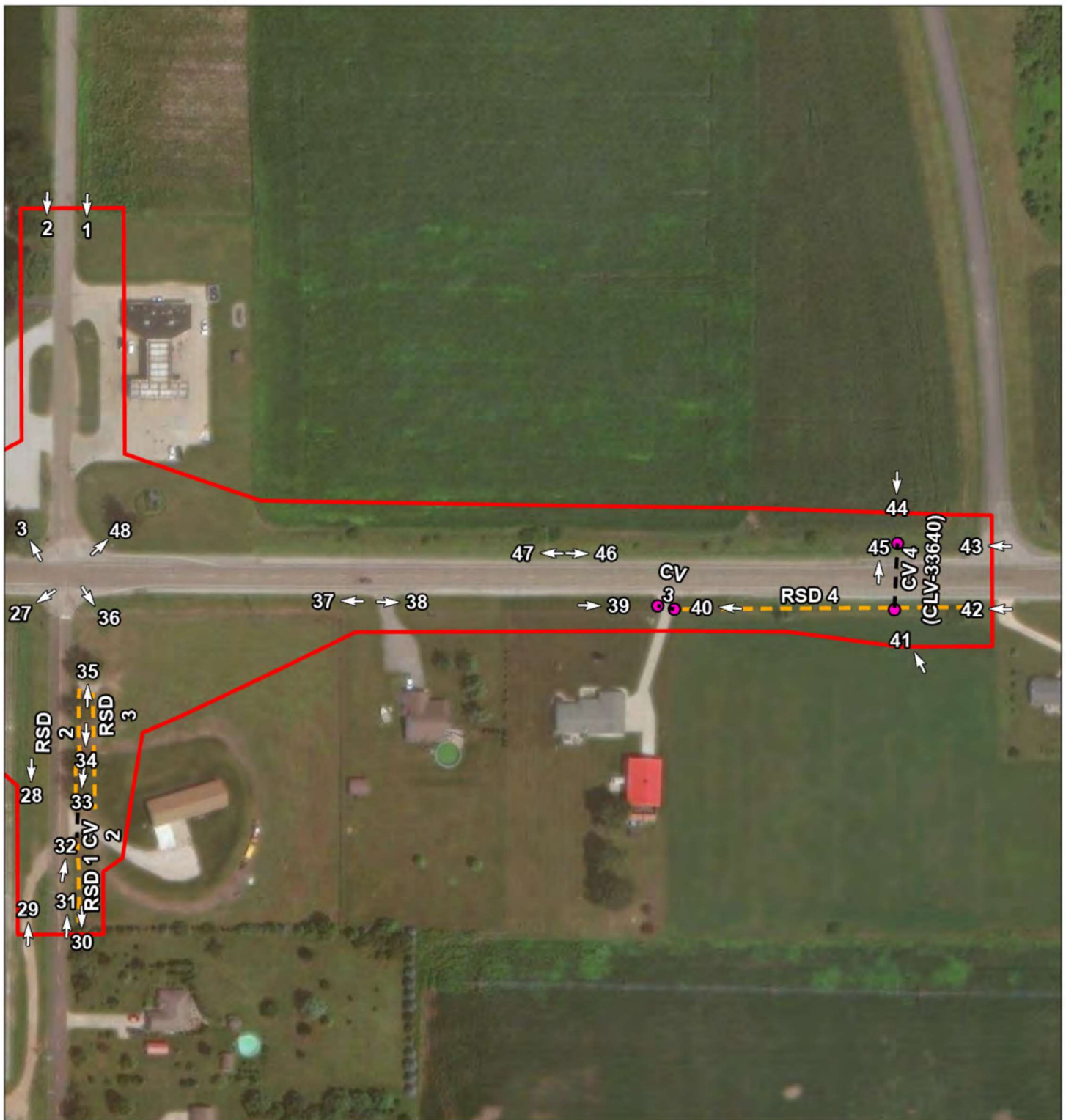
All locations approximate
 Source: Indiana Spatial Data Portal (2018)



0 250 500 1,000
 Feet



Exh. 5 & Exh. 6
 Reference Map



- Investigated Area (IA)
- Culvert Opening (CV)
- Culvert (CV)
- - Roadside Ditch (RSD)

Exhibit 6 - Photograph Location Map
 SR 2 - Intersection Improvement
 Roundabout Installation
 LaPorte County, Indiana
 Des. No. 2200043
 Metric Project No. 22-0216-1
 Map Date: 7/31/2023
 Map Author: Seth Snyder

All locations approximate
 Source: ESRI World Imagery Clarity (2022)



0 100 200 400
 Feet



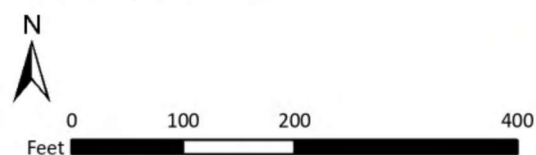
Exh. 6
 Page 1 of 3



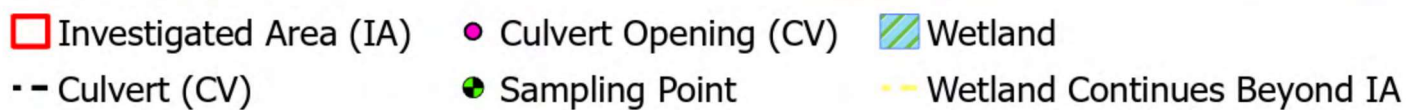
- Investigated Area (IA) ● Culvert Opening (CV)
- Culvert (CV) - - Roadside Ditch (RSD)

Exhibit 6 - Photograph Location Map
 SR 2 - Intersection Improvement
 Roundabout Installation
 LaPorte County, Indiana
 Des. No. 2200043
 Metric Project No. 22-0216-1
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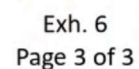
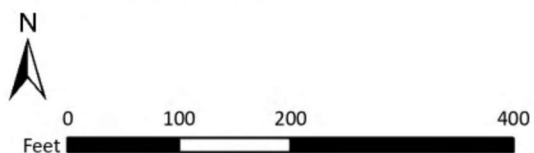
All locations approximate
 Source: ESRI World Imagery Clarity (2022)



Exh. 6
 Page 2 of 3



All locations approximate
Source: ESRI World Imagery Clarity (2022)





1. View of upland area from the northern terminus of the IA east of S 500 W, looking south.



2. View of upland area from the northern terminus of the IA west of S 500 W, looking south.



3. Panoramic view at the intersection of SR 2 and S 500 W, looking northwest.

SITE PHOTOGRAPHS—8/2/2023

SR 2 Intersection Improvement
Roundabout Installation
Scipio Township, LaPorte County, Indiana
Des. No. 2200043





4. View of upland area on north side of SR 2, looking east.



5. View of upland area from north side of SR 2, looking west



6. View of upland area from north side of SR 2, looking east.



7. View of upland area and Wetland A from the western terminus of the IA on the south side of SR 2, looking east.

SITE PHOTOGRAPHS—8/2/2023

SR 2 Intersection Improvement
Roundabout Installation
Scipio Township, LaPorte County, Indiana
Des. No. 2200043





8. View of Wetland A from the south side of SR 2, looking south. The red dashed line represents the western terminus of the IA. Vegetation observed included curly dock (*Rumex crispus*, FAC), giant ragweed (*Ambrosia trifida*, FAC), black elder (*Sambucus nigra*, FAC), and more.



10. View of NHD flowline (unobserved) from the south side of SR 2, looking south. Vegetation observed included red fescue (*Festuca rubra*, FACU), tall false rye grass (*Schedonorus arundinaceus*, FACU), ground ivy (*Glechoma hederacea*, FACU), and more.



9. View of NHD flowline (unobserved) from the south side of SR 2, looking north. No culvert was observed during the field reconnaissance.



11. View of Wetland A Sampling Point (A) 1, looking southeast.

SITE PHOTOGRAPHS—8/2/2023

SR 2 Intersection Improvement
Roundabout Installation
Scipio Township, LaPorte County, Indiana
Des. No. 2200043





12. View of A1, looking northwest.



13. View of A1, soil profile.



14. View of A2, looking south.



15. View of A2, looking north.

SITE PHOTOGRAPHS—8/2/2023

SR 2 Intersection Improvement
Roundabout Installation
Scipio Township, LaPorte County, Indiana
Des. No. 2200043





16. View of A2, soil profile.



17. View of NHD flowline (unobserved), from the south side of SR 2 looking south. Vegetation observed included red fescue (*Festuca rubra*, FACU).



18. View of NHD flowline (unobserved) from the south side of SR 2, looking west.



19. View of upland forested area on the south side of SR 2, looking east. Vegetation pictured includes sugar maple (*Acer Saccharum*, FACU) and amur honeysuckle (*Lonicera maackii*).

SITE PHOTOGRAPHS—8/2/2023

SR 2 Intersection Improvement
Roundabout Installation
Scipio Township, LaPorte County, Indiana
Des. No. 2200043





20. View of Culvert (CV) 1 from the south side of SR 2, looking east.



21. View of CV 1 from the south side of SR 2, looking west.



22. View of CV 1 and upland area from the south side of SR 2, looking west.



23. View of upland area from the south side of SR 2, looking east.

SITE PHOTOGRAPHS—8/2/2023

SR 2 Intersection Improvement
Roundabout Installation
Scipio Township, LaPorte County, Indiana
Des. No. 2200043





24. View of upland area from the south side of SR 2, looking west.



25. View of upland area from the south side of SR 2, looking east.



26. View of row crop towards SR 2 and S 500 W intersection, looking northeast.

SITE PHOTOGRAPHS—8/2/2023

SR 2 Intersection Improvement
Roundabout Installation
Scipio Township, LaPorte County, Indiana
Des. No. 2200043





27. Panoramic view at the intersection of SR 2 and S 500 W, looking southwest.



28. View of upland area, row crop and NHD Flowline (unobserved) from the west side of S 500 W, looking south.



29. View of upland area, row crop, and NHD Flowline (unobserved) along the west side of S 500 W, looking north.

SITE PHOTOGRAPHS—8/2/2023

SR 2 Intersection Improvement
Roundabout Installation
Scipio Township, LaPorte County, Indiana
Des. No. 2200043





30. View of RSD 1 and upland area from the east side of S 500 W, looking south. Vegetation observed within RSD 1 included field brome (*bromus arvensis*, FACU) and tall false rye grass (*Schedonorus arundinaceus*, FACU).



31. View of RSD 1 and upland area from the east side of S 500 W, looking north.



32. View of CV 2 and RSD 1 from the east side of S 500 W, looking north.



33. View of CV 2 and RSD 2 from the east side of S 500 W, looking south.

SITE PHOTOGRAPHS—8/2/2023

SR 2 Intersection Improvement
Roundabout Installation
Scipio Township, LaPorte County, Indiana
Des. No. 2200043





34. View of RSD 2, RSD 3, and upland area from the east side of S 500 W, looking south.



35. View of RSD 2, RSD 3, and upland area from the east side of S 500 W, looking north.



36. Panoramic view at the intersection of SR 2 and S CR 500 W, looking southeast.

SITE PHOTOGRAPHS—8/2/2023

SR 2 Intersection Improvement
Roundabout Installation
Scipio Township, LaPorte County, Indiana
Des. No. 2200043





37. View of upland area from the south side of SR 2, looking west.



38. View of upland area from the south side of SR 2, looking east.



39. View of CV 3 from the south side of SR 2, looking east.



40. View of CV 3 from the south side of SR 2, looking west.

SITE PHOTOGRAPHS—8/2/2023

SR 2 Intersection Improvement
Roundabout Installation
Scipio Township, LaPorte County, Indiana
Des. No. 2200043





41. View of CV 4 from the south side of SR 2, looking northwest.



42. View of upland area from the eastern terminus of the IA south of SR 2, looking west. Vegetation observed included common milkweed (*Asclepias syriaca*, FACU) and amur honeysuckle (*Lonicera maackii*).



43. View of upland area from the eastern terminus of the IA north of SR 2, looking west.



44. View of CV 5 from the north side of SR 2, looking south.

SITE PHOTOGRAPHS—8/2/2023

SR 2 Intersection Improvement
Roundabout Installation
Scipio Township, LaPorte County, Indiana
Des. No. 2200043





45. View of CV 5 and NHD flowline from the north side of SR 2, looking north. No OHWM was observed.



46. View of upland area from the north side of SR 2, looking east.



47. View of upland area from the north side of SR 2, looking west.

SITE PHOTOGRAPHS—8/2/2023

SR 2 Intersection Improvement
Roundabout Installation
Scipio Township, LaPorte County, Indiana
Des. No. 2200043





48. Panoramic view at the intersection of SR 2 and S 500 W, looking northeast.

SITE PHOTOGRAPHS—8/2/2023

SR 2 Intersection Improvement
Roundabout Installation
Scipio Township, LaPorte County, Indiana
Des. No. 2200043



| PROJECT | DESIGNATION |
|----------|-------------|
| 2200043 | 2200043 |
| CONTRACT | |
| R-44648 | |

INDIANA DEPARTMENT OF TRANSPORTATION

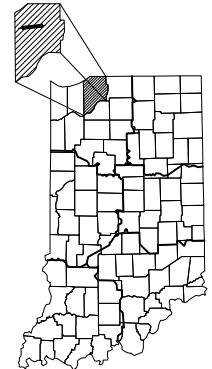


| CULVERT ASSETS | | |
|----------------|-----------------------|-------------|
| DES. NO. | CULVERT ASSET ID | WORK TYPE |
| 2200043 | Sta. 926+50.00 "A" | New Culvert |
| 2200043 | Sta. 48+75.00 "S-1-A" | New Culvert |
| 2200043 | Sta. 929+60.00 "A" | New Culvert |

ROAD PLANS

ROUTE: S.R. 2 AT: 55+04
PROJECT NO. 2200043 P.E.
 2200043 R/W
 2200043 CONST.

| TRAFFIC DATA | S.R. 2 | CR 500 W |
|---------------------------|----------------------------------|----------------------------------|
| A.A.D.T. (2027) | 9,008 V.P.D. | 2,249 V.P.D. |
| A.A.D.T. (2047) | 9,658 V.P.D. | 2,249 V.P.D. |
| D.H.V. (2047) | 1,001 V.P.H. | 224 V.P.H. |
| DIRECTIONAL DISTRIBUTION | 48.90 % | 51.49 % |
| TRUCKS | 5.08 % A.A.D.T. 1.97 % D.H.V. | 1.73 % A.A.D.T. 1.34 % D.H.V. |
| DESIGN DATA | | |
| DESIGN SPEED | 55 MPH | 30 AND 40 MPH |
| PROJECT DESIGN CRITERIA | RECONSTRUCTION (NON-FREEWAY) | RECONSTRUCTION (NON-FREEWAY) |
| FUNCTIONAL CLASSIFICATION | PRINCIPAL ARTERIAL | LOCAL AGENCY COLLECTOR |
| RURAL/URBAN | RURAL | RURAL |
| TERRAIN | LEVEL | LEVEL |
| ACCESS CONTROL | NONE | NONE |



PROJECT LOCATION SHOWN BY
LA PORTE COUNTY

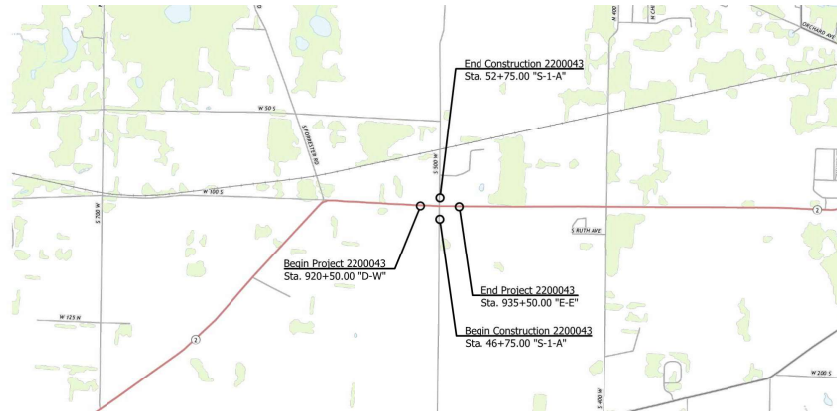
LATITUDE: 41°35'29" N LONGITUDE: 86°47'53" W

GROSS LENGTH: 0.284 MI.
NET LENGTH: 0.284 MI.
MAX. GRADE: 1.68 %

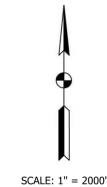
HYDROLOGIC UNIT CODE (S): 071200010406

STAGE 2 PLANS
SEPTEMBER 16, 2024

Roundabout Intersection Improvement on S.R. 2 at CR 500 W, 3.27 Miles West of S.R. 39
Sections 5, 6, 7, & 8, T-36-N, R-3-W, Scipio Township, La Porte County, Indiana



LOCATION MAP
LA PORTE COUNTY



LFA
LAWSON-FISHER ASSOCIATES P.C.
525 W. WASHINGTON AVENUE
SOUTH BEND, INDIANA 46601
PH. (574) 234-3167

NOT FOR
CONSTRUCTION

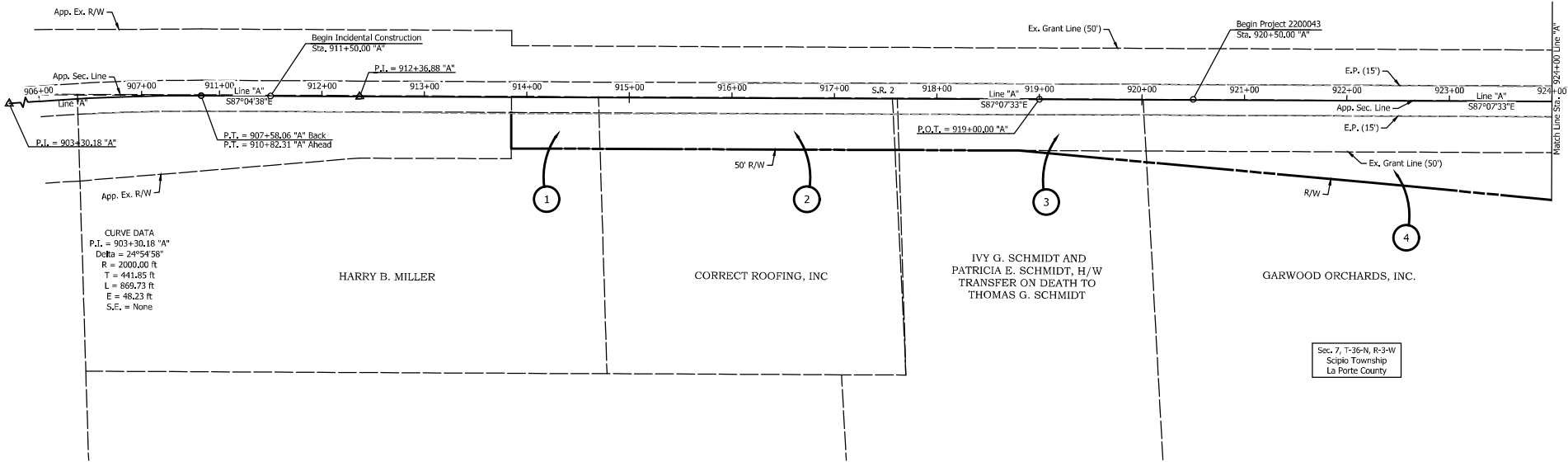
PLANS PREPARED BY: LAWSON-FISHER ASSOC. P.C. 574-234-3167
PHONE NUMBER
CERTIFIED BY: DATE
APPROVED FOR LETTING: INDIANA DEPARTMENT OF TRANSPORTATION DATE

| DESIGNATION | |
|-------------|---------|
| 2200043 | |
| SHEETS | |
| SURVEY BOOK | 1 of 70 |
| PROJECT | |
| CONTRACT | 2200043 |
| R-44648 | |



MARTIN R. BARR TRUST NO. 4 (35% INTEREST),
MARSHA H. BARR TRUST NO. 54 (35% INTEREST),
AND EVAN S. BARR (30% INTEREST)

Sec. 6, T-36-N, R-3-W
Scipio Township
La Porte County



CURVE DATA
P.I. = 903+30.18 "A"
Delta = 24°54'58"
R = 2000.00 ft
L = 441.85 ft
E = 48.23 ft
S.E. = None

HARRY B. MILLER

CORRECT ROOFING, INC

IVY G. SCHMIDT AND
PATRICIA E. SCHMIDT, H/W
TRANSFER ON DEATH TO
THOMAS G. SCHMIDT

GARWOOD ORCHARDS, INC.

Sec. 7, T-36-N, R-3-W
Scipio Township
La Porte County

NOT FOR
CONSTRUCTION

| | | |
|-----------------------------------|--------------|------------|
| RECOMMENDED FOR APPROVAL _____ | | |
| DESIGNED: DDE | DRAWN: GDH | DATE _____ |
| CHECKED: DGA | CHECKED: DGA | |

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| INDIANA DEPARTMENT OF TRANSPORTATION | |
| PLAT NO. 1 | |

| | |
|---------------------|------------------------|
| SCALE 1" = 50' | BRIDGE FILE |
| | DESIGNATION 2200043 |
| SURVEY BOOK | SHEETS |
| 6 | of 31 |
| CONTRACT S-44548 | PROJECT 2200043 |

MARTIN R. BARR TRUST
NO. 4 (35% INTEREST),
MARSHA H. BARR TRUST
NO. 54 (35% INTEREST),
AND EVAN S. BARR
(30% INTEREST)

Sec. 6, T-36-N, R-3-W
Scipio Township
La Porte County

MIKE GILLESPIE,
KERRY DEARTH, & DAN MELVIN,
AS TRUSTEES FOR TEAMSTER
LOCAL UNION NO. 135 30' R/W

FAMILY EXPRESS
CORPORATION

LAKE COUNTY TRUST
COMPANY-TRUST #5148

Sec. 5, T-36-N, R-3-W
Scipio Township
La Porte County

AGNL CROWN,
L.L.C.

GARWOOD ORCHARDS, INC.

Sec. 7, T-36-N, R-3-W
Scipio Township
La Porte County

JOHN K. BECKER AND
MELLISSA D. BECKER

DANIEL E. HAMMOND AND
DORCAS E. HAMMOND, H/W
TRANSFER ON DEATH TO
ANTOINETTE HAMMOND,
SHARON WROBLEWSKI,
HEATHER HAMMOND AND
DANIEL E. HAMMOND

DOMINIC E. SEMENTO JR. AND
PAMELA A. SEMENTO, H/W

Sec. 8, T-36-N, R-3-W
Scipio Township
La Porte County

JOHN E. SHEBEL AND
ELYSIA D. SHEBEL, H/W

JOHN E. SHEBEL AND
ELYSIA D. SHEBEL, H/W

BRIAN L. ROSEMAN AND
WENDY S. ROSEMAN, H/W
TRANSFER ON DEATH TO
TONJA WORKMAN, KIMBER L. CORNELL AND
MICHAEL A. CORNELL

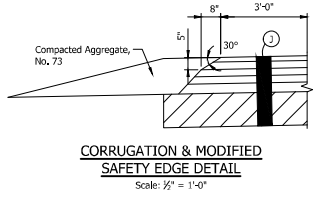
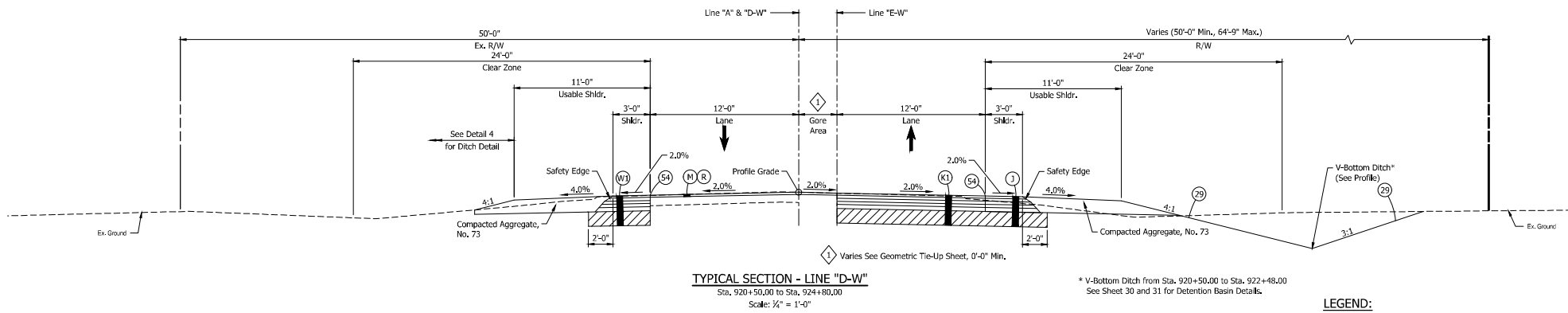
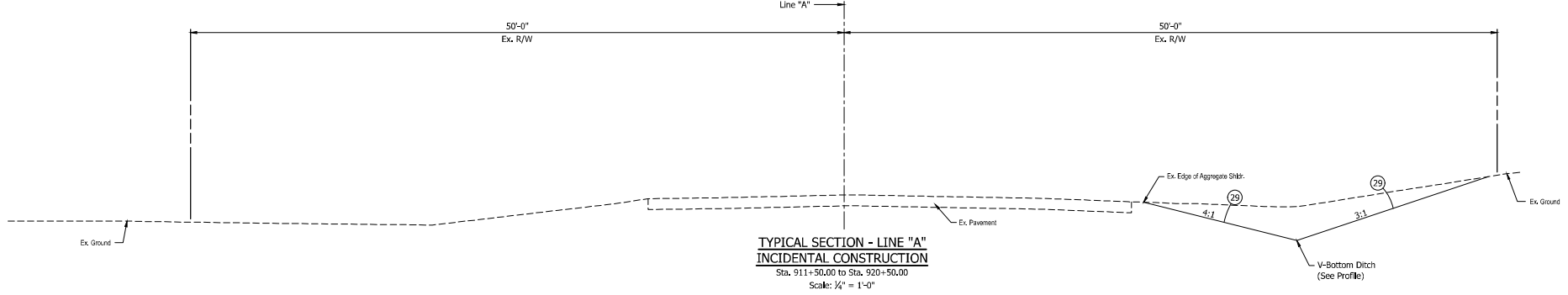
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| DESIGNED: DDE | DRAWN: GDM | |
| CHECKED: DGA | CHECKED: DGA | |

INDIANA
DEPARTMENT OF TRANSPORTATION

PLAT NO. 1

| | |
|-------------|-------------|
| SCALE | BRIDGE FILE |
| 1" = 50' | DESIGNATION |
| | 2200043 |
| SURVEY BOOK | SHEETS |
| | 7 of 31 |
| CONTRACT | PROJECT |
| R-44648 | 2200043 |



- LEGEND:**
- (J) 220 #/sys QC/QA-HMA, 2, 58S, Surface, 9.5mm on 330 #/sys QC/QA-HMA, 2, 58S, Intermediate, 19.0mm on 330 #/sys QC/QA-HMA, 2, 58S, Base, 19.0mm on 4 in. Compacted Aggregate No. 53 on Subgrade Treatment, Type IBC
 - (K) 220 #/sys QC/QA-HMA, 2, 58S, Surface, 9.5mm on 330 #/sys QC/QA-HMA, 2, 58S, Intermediate, 19.0mm on 330 #/sys QC/QA-HMA, 2, 58S, Base, 19.0mm on 4 in. Compacted Aggregate No. 53 on Subgrade Treatment, Type IBC
 - (M) Milling, Profile (0" - 2")
 - (R) 220 #/sys QC/QA-HMA, 2, 58S, Surface, 9.5mm on 330 #/sys HMA Intermediate, Type B on 330 #/sys HMA Base, Type B on 4 in. Compacted Aggregate No. 53 on Subgrade Treatment, Type IBC
 - (W) 220 #/sys QC/QA-HMA, 2, 58S, Surface, 9.5mm on 330 #/sys HMA Intermediate, Type B on 330 #/sys HMA Base, Type B on 4 in. Compacted Aggregate No. 53 on Subgrade Treatment, Type IBC
 - (29) Seed Mixture, R Mulching Fertilizer
 - (SA) Milled HMA Corrugations, Sinusoidal (Edgeline)

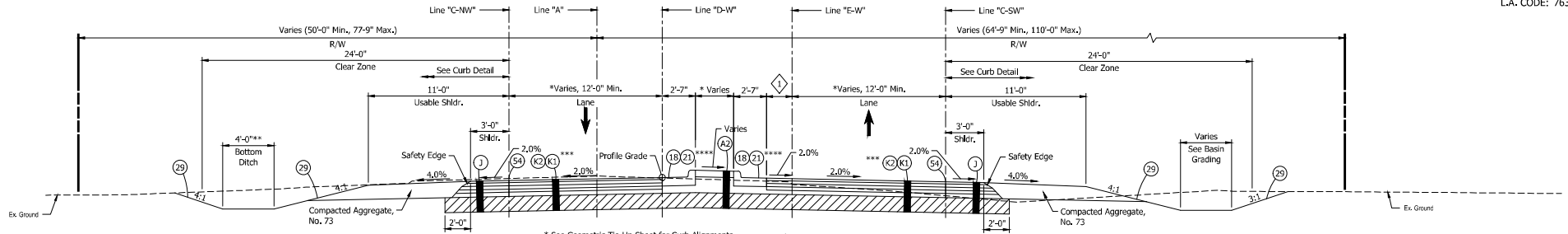
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| DESIGNED: DDE | DRAWN: GDH |
| CHECKED: DGA | CHECKED: DGA |

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| INDIANA DEPARTMENT OF TRANSPORTATION | |
| TYPICAL SECTIONS LINE "A" AND "D-W" | |

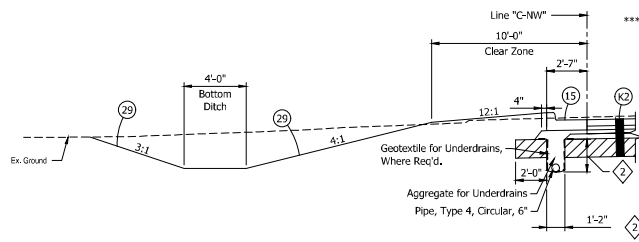
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| SCALE | BRIDGE FILE |
| AS NOTED | DESIGNATION |
| 2200043 | SHEETS |
| 8 | of 31 |
| CONTRACT | PROJECT |
| 5-44548 | 2200043 |

U:\2023\202318 INDOT SR2 Roundabout\Coord\Plan Set\Gm\2318DOTR_TSD1.dwg [TYP. SECTION 01]

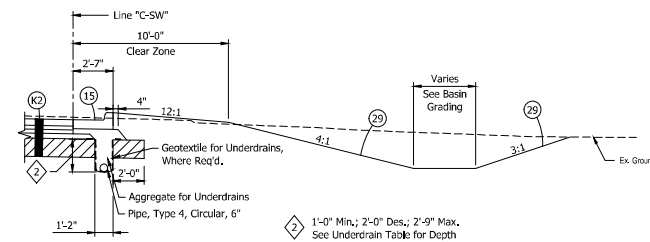


* See Geometric Tie-Up Sheet for Curb Alignments
 ** Ditch Begins at Sta. 924+28.00
 *** (K1) From Sta. 924+80.00 to Sta. 926+56.00
 (K2) From Sta. 926+56.00 to Sta. 927+40.65
 **** (21) From Sta. 924+80.00 to Sta. 925+80.00
 (18) From Sta. 925+80.00 to Sta. 927+40.65

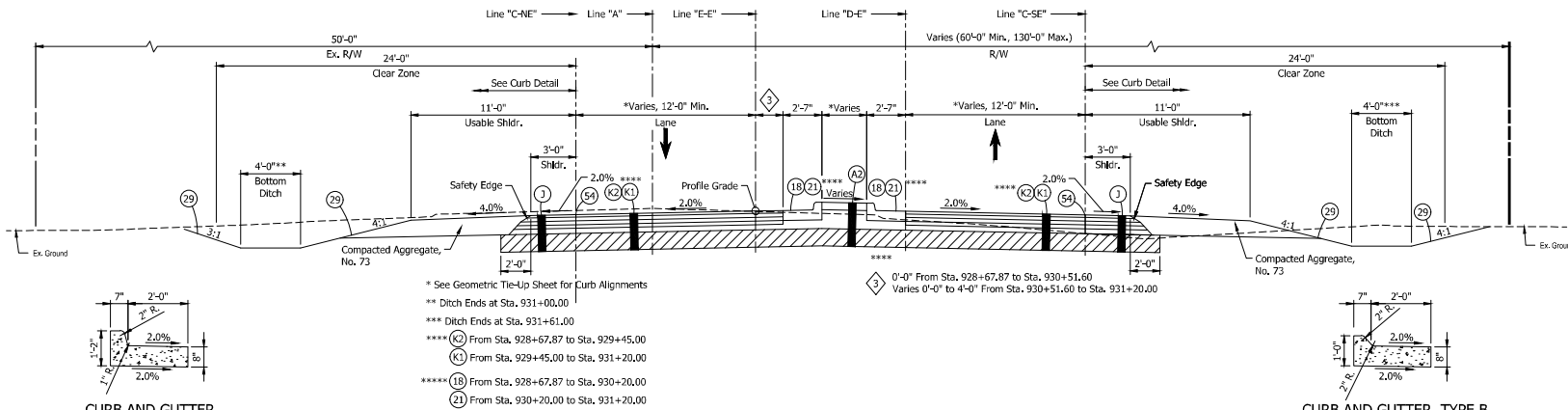
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 Scale: 1/4" = 1'-0"



CURB DETAIL
 Sta. 926+55.00 to Sta. 928+46.00 Line "C-W"
 Sta. 927+44.00 to Sta. 929+45.00 Line "C-NE"
 Scale: 1/4" = 1'-0"

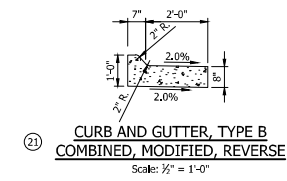
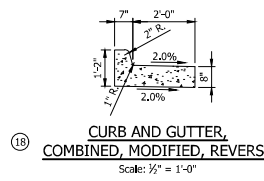


CURB DETAIL
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 Sta. 927+33.00 to Sta. 929+47.00 Line "C-SE"
 Scale: 1/4" = 1'-0"



* See Geometric Tie-Up Sheet for Curb Alignments
 ** Ditch Ends at Sta. 931+00.00
 *** Ditch Ends at Sta. 931+61.00
 **** (K2) From Sta. 928+67.87 to Sta. 929+45.00
 (K1) From Sta. 929+45.00 to Sta. 931+20.00
 ***** (18) From Sta. 928+67.87 to Sta. 930+20.00
 (21) From Sta. 930+20.00 to Sta. 931+20.00

TYPICAL SECTION - LINE "E-E"
 Sta. 928+67.87 to Sta. 931+20.00
 Scale: 1/4" = 1'-0"



LEGEND:

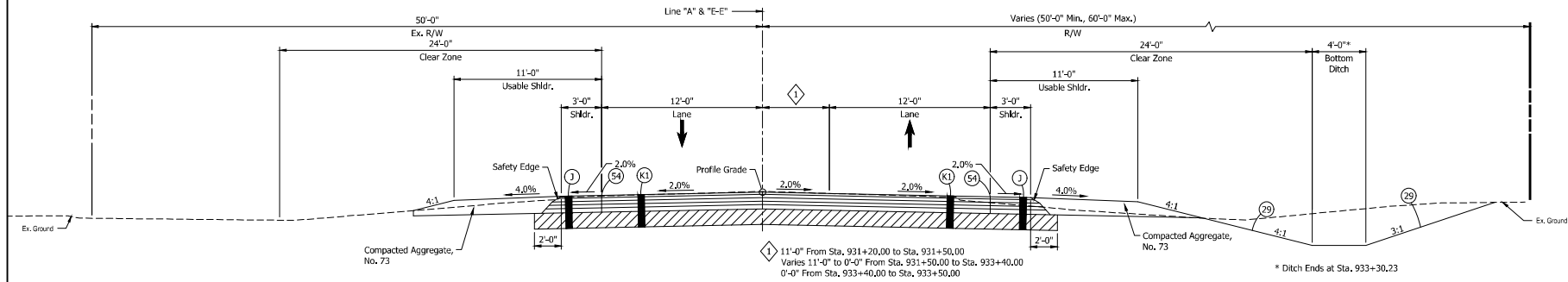
- (A2) Color PCOP, 6" on 13" - 15" Compacted Aggregate, No. 53 on (To Match Subgrade Treatment Elevation) Geotextile for Pavement, Type 2A on Subgrade Treatment, Type IBC
- (1) 220 #/sys QC/QA-HMA, 2, SBS, Surface, 9.5mm on 330 #/sys QC/QA-HMA, 2, SBS, Intermediate, 19.0mm on 330 #/sys QC/QA-HMA, 2, SBS, Base, 19.0mm on 330 #/sys QC/QA-HMA, 2, SBS, Base, 19.0mm on 4 in. Compacted Aggregate No. 53 on Subgrade Treatment, Type IBC
- (K1) 220 #/sys QC/QA-HMA, 2, SBS, Surface, 9.5mm on 330 #/sys QC/QA-HMA, 2, SBS, Intermediate, 19.0mm on 330 #/sys QC/QA-HMA, 2, SBS, Base, 19.0mm on 330 #/sys QC/QA-HMA, 2, SBS, Base, 19.0mm on 4 in. Compacted Aggregate No. 53 on Subgrade Treatment, Type IBC
- (K2) 220 #/sys QC/QA-HMA, 2, SBS, Surface, 9.5mm on 330 #/sys QC/QA-HMA, 2, SBS, Intermediate, 19.0mm on 330 #/sys QC/QA-HMA, 2, SBS, Base, 19.0mm on 330 #/sys QC/QA-HMA, 2, SBS, Base, 19.0mm on 4 in. Compacted Aggregate No. 53 on Subgrade Treatment, Type IBC
- (15) Curb and Gutter, Combined
- (18) Curb and Gutter, Combined Modified, Reverse
- (21) Curb and Gutter, Type B, Combined Modified, Reverse
- (29) Seed Mixture, R Mulching Fertilizer
- (54) Milled HMA Corrugations, Sinusoidal (Edgetine)

NOT FOR CONSTRUCTION

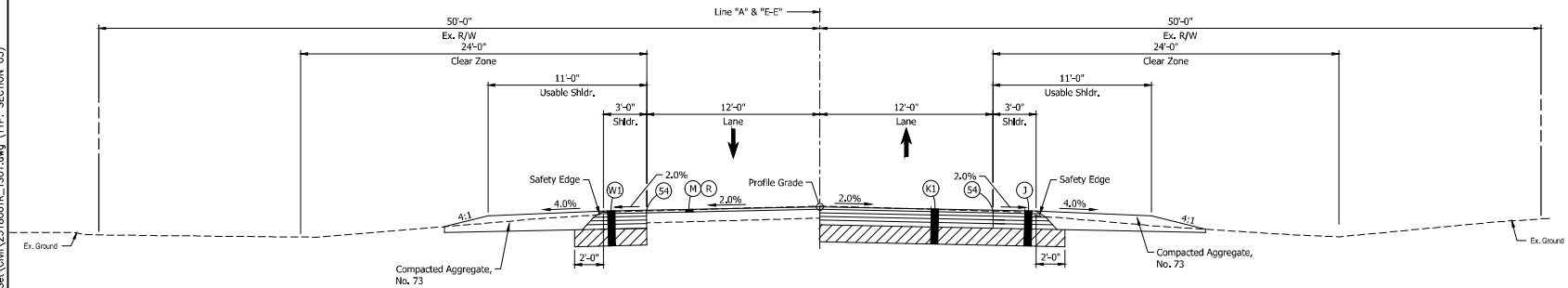
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| DESIGNED: DDE | DRAWN: GGH | |
| CHECKED: DGA | CHECKED: DGA | |

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|--------------------------------------|
| INDIANA DEPARTMENT OF TRANSPORTATION |
| TYPICAL SECTIONS LINE "D-W" & "E-E" |

| | |
|-------------|-------------|
| SCALE | BRIDGE FILE |
| AS NOTED | DESIGNATION |
| | 2200043 |
| SURVEY BOOK | SHEETS |
| | 9 of 31 |
| CONTRACT | PROJECT |
| S-44548 | 2200043 |



TYPICAL SECTION - LINE "E-E"
Sta. 931+20.00 to Sta. 933+50.00
Scale: 1/4" = 1'-0"



TYPICAL SECTION - LINE "E-E"
Sta. 933+50.00 to Sta. 935+50.00
Scale: 1/4" = 1'-0"

- LEGEND:**
- (1) 220 #/sys QC/QA-HMA, 2, SRS, Surface, 9.5mm on
330 #/sys QC/QA-HMA, 2, SRS, Intermediate, 19.0mm on
330 #/sys QC/QA-HMA, 2, SRS, Base, 19.0mm on
4 in. Compacted Aggregate No. 53 on
Subgrade Treatment, Type IBC
 - (K1) 220 #/sys QC/QA-HMA, 2, SRS, Surface, 9.5mm on
330 #/sys QC/QA-HMA, 2, SRS, Intermediate, 19.0mm on
330 #/sys QC/QA-HMA, 2, SRS, Base, 19.0mm on
4 in. Compacted Aggregate No. 53 on
Subgrade Treatment, Type IBC
 - (H) Milling, Profile (0" - 2")
 - (R) 220 #/sys QC/QA-HMA, 2, SRS, Surface, 9.5 mm
 - (W) 220 #/sys QC/QA-HMA, 2, SRS, Surface, 9.5mm on
330 #/sys HMA Intermediate, Type B on
330 #/sys HMA Base, Type B on
4 in. Compacted Aggregate No. 53 on
Subgrade Treatment, Type IBC
 - (29) Seed Mixture, R
Mulching
Fertilizer
 - (S4) Milled HMA Corrugations, Sinusoidal (Edgeline)

NOT FOR
CONSTRUCTION

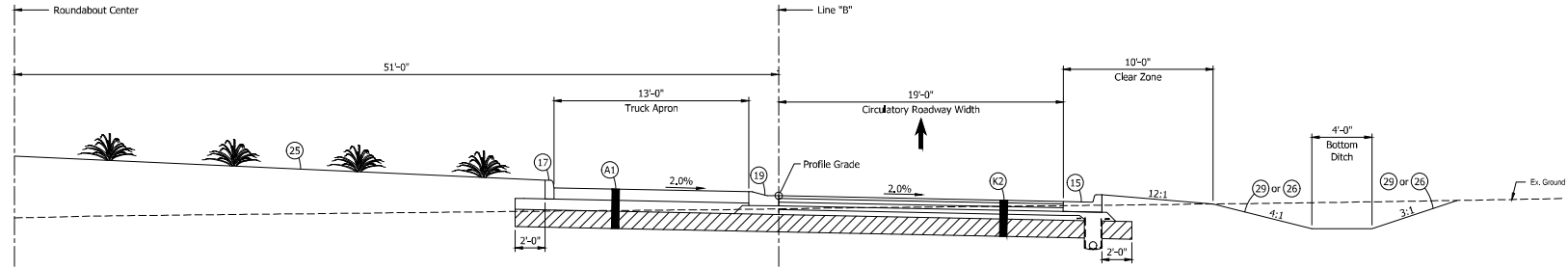
| | | | | | |
|--------------------------------|--|-----------------------|--|------------|--|
| RECOMMENDED FOR APPROVAL _____ | | DESIGN ENGINEER _____ | | DATE _____ | |
| DESIGNED: DDE | | DRAWN: GDH | | | |
| CHECKED: DGA | | CHECKED: DGA | | | |

INDIANA
DEPARTMENT OF TRANSPORTATION

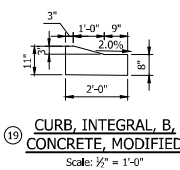
TYPICAL SECTIONS
LINE "E-E"

| | |
|-------------|-------------|
| SCALE | BRIDGE FILE |
| AS NOTED | |
| DESIGNATION | 2200043 |
| SURVEY BOOK | SHEETS |
| 10 | of 31 |
| CONTRACT | PROJECT |
| S-44548 | 2200043 |

U:\2023\202318 INDOT SR2 Roundabout\Coord\Plan Set\GWT\231800TR_TSD1.dwg [TYP. SECTION 03]



TYPICAL SECTION - LINE "B"
Scale: 1/4" = 1'-0"

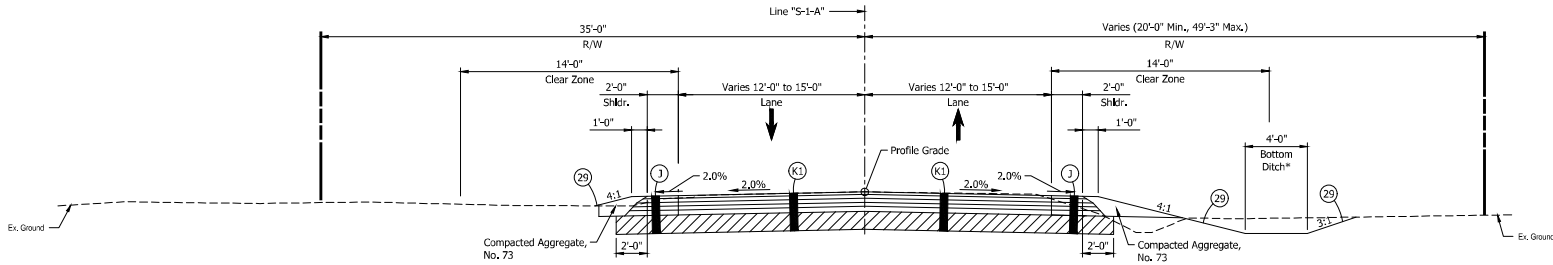


- LEGEND:**
- (A1) Color-PCCP 9" on 9" Compacted Aggregate, No. 53 on Geotextile for Pavement, Type 2A on Subgrade Treatment, Type 1BC
 - (K2) 220 #/sys QC/QA-HMA, 2, 58S, Surface, 9.5mm on 330 #/sys QC/QA-HMA, 2, 58S, Intermediate, 19.0mm on 330 #/sys QC/QA-HMA, 2, 58S, Base, 19.0mm on 300 #/sys QC/QA-HMA, 4, 58E, Intermediate, OG, 19.0mm on 4 in. Compacted Aggregate No. 53 on Geotextile for Pavement, Type 2A on Subgrade Treatment, Type 1BC
 - (15) Curb and Gutter, Combined
 - (17) Integral Curb
 - (19) Curb and Gutter, Type B, Modified, 3" Reveal, Reverse
 - (25) Native Seed Mixture
 - (26) Sodding
 - (29) Seed Mixture, R Mulching Fertilizer

U:\2023\202318 INDIAN SR2 Roundabout\Cad\Plan Set\Civil\231802TR_TSD1.dwg (TYP. SECTION 06)

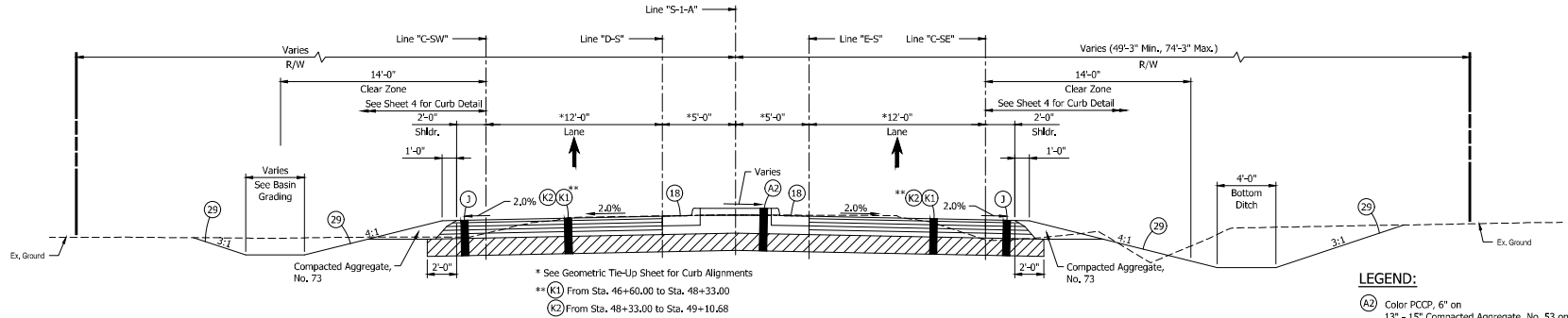
NOT FOR CONSTRUCTION

| | | | | | | | | | | | |
|--------------------------------|--|-----------------------|--|--------------|--|--------------------------------------|--|----------------|--|-------------|--|
| RECOMMENDED FOR APPROVAL _____ | | DESIGN ENGINEER _____ | | DATE _____ | | INDIANA DEPARTMENT OF TRANSPORTATION | | SCALE AS NOTED | | BRIDGE FILE | |
| DESIGNED: JDD | | DRAWN: GEH | | CHECKED: DGA | | TYPICAL SECTION LINE "B" | | SURVEY BOOK | | SHEETS | |
| | | | | | | | | 11 | | of 31 | |
| | | | | | | | | CONTRACT | | PROJECT | |
| | | | | | | | | S-44548 | | 2200043 | |



TYPICAL SECTION - LINE "S-1-A"
Sta. 46+75.00 to Sta. 47+75.00
Scale: 1/4" = 1'-0"

* Ditch Start 47+15.00



TYPICAL SECTION - LINE "S-1-A"
Sta. 47+75.00 to Sta. 49+10.68
Scale: 1/4" = 1'-0"

LEGEND:

- (A2) Color PCCP, 6" on 13" - 15" Compacted Aggregate, No. 53 on (To Match Subgrade Treatment Elevation) Geotextile for Pavement, Type 2A on Subgrade Treatment, Type IBC
- (J) 220 #/sys QC/QA-HMA, 2, 58S, Surface, 9.5mm on 330 #/sys QC/QA-HMA, 2, 58S, Intermediate, 19.0mm on 330 #/sys QC/QA-HMA, 2, 58S, Base, 19.0mm on 4 in. Compacted Aggregate No. 53 on Subgrade Treatment, Type IBC
- (K1) 220 #/sys QC/QA-HMA, 2, 58S, Surface, 9.5mm on 330 #/sys QC/QA-HMA, 2, 58S, Intermediate, 19.0mm on 330 #/sys QC/QA-HMA, 2, 58S, Base, 19.0mm on 4 in. Compacted Aggregate No. 53 on Subgrade Treatment, Type IBC
- (K2) 220 #/sys QC/QA-HMA, 2, 58S, Surface, 9.5mm on 330 #/sys QC/QA-HMA, 2, 58S, Intermediate, 19.0mm on 330 #/sys QC/QA-HMA, 2, 58S, Base, 19.0mm on 4 in. Compacted Aggregate No. 53 on Subgrade Treatment, Type IBC
- (18) Curb and Gutter, Combined Modified, Reverse
- (29) Seed Mixture, R Mulching Fertilizer

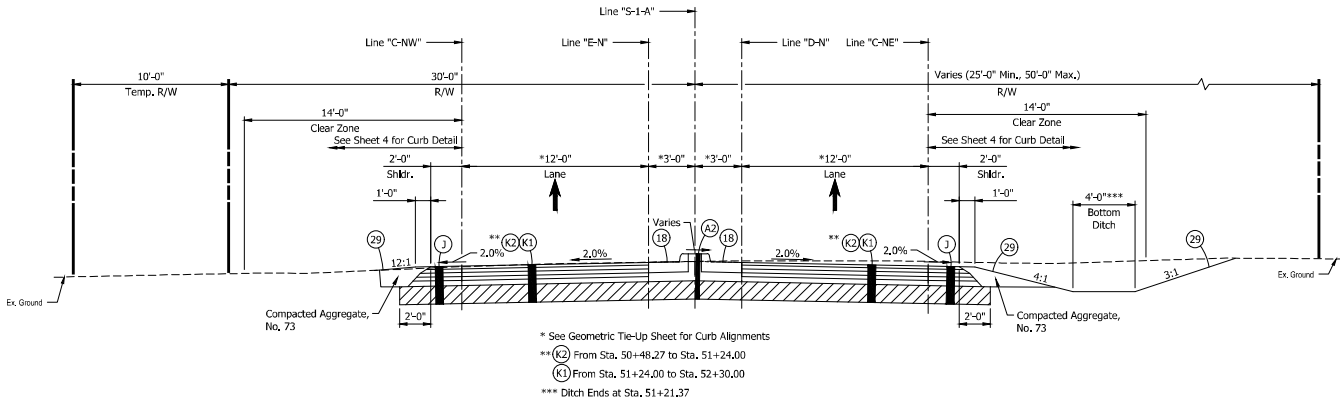
NOT FOR CONSTRUCTION

| | | |
|--------------------------|-----------------|------|
| RECOMMENDED FOR APPROVAL | DESIGN ENGINEER | DATE |
| DESIGNED: JDD | DRAWN: GEH | |
| CHECKED: DGA | CHECKED: DGA | |

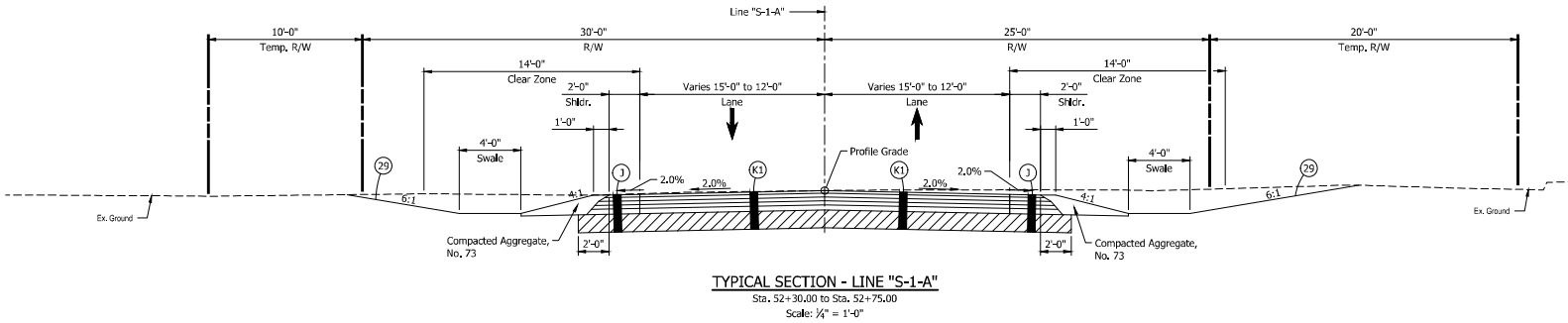
INDIANA
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS
LINE "S-1-A"

| | |
|--------------|-------------|
| SCALE | BRIDGE FILE |
| 1/4" = 1'-0" | |
| DESIGNATION | |
| 2200043 | |
| SURVEY BOOK | SHEETS |
| | 12 of 31 |
| CONTRACT | PROJECT |
| S-44548 | 2200043 |



TYPICAL SECTION - LINE "S-1-A"
Sta. 50+48.27 to Sta. 52+30.00
Scale: 1/4" = 1'-0"



TYPICAL SECTION - LINE "S-1-A"
Sta. 52+30.00 to Sta. 52+75.00
Scale: 1/4" = 1'-0"

LEGEND:

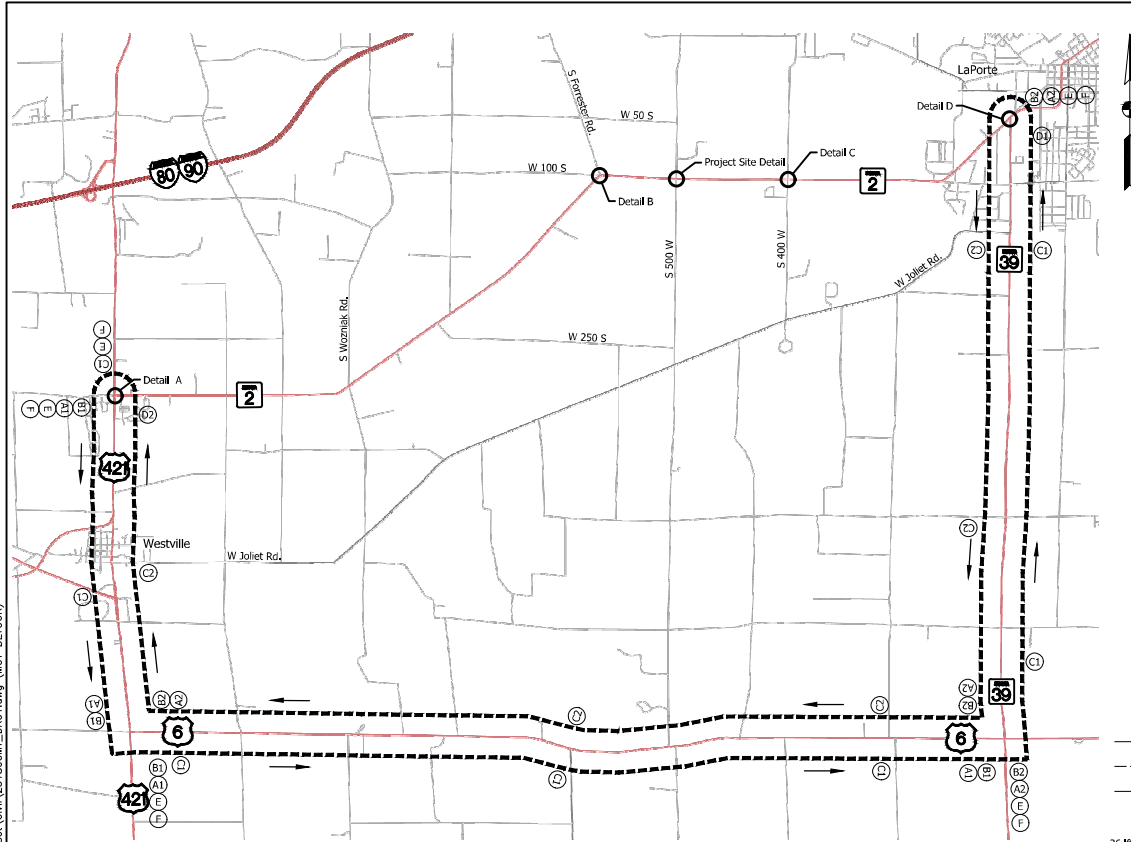
- (A2) Color PCCP, 6" on 13" - 15" Compacted Aggregate, No. 53 on (To Match Subgrade Treatment Elevation) Geotextile for Pavement, Type 2A on Subgrade Treatment, Type 1BC
- (J) 220 #/sys QC/QA-HMA, 2, 58S, Surface, 9.5mm on 330 #/sys QC/QA-HMA, 2, 58S, Intermediate, 19.0mm on 330 #/sys QC/QA-HMA, 2, 58S, Base, 19.0mm on 4 in. Compacted Aggregate No. 53 on Subgrade Treatment, Type 1BC
- (K1) 220 #/sys QC/QA-HMA, 2, 58S, Surface, 9.5mm on 330 #/sys QC/QA-HMA, 2, 58S, Intermediate, 19.0mm on 330 #/sys QC/QA-HMA, 2, 58S, Base, 19.0mm on 4 in. Compacted Aggregate No. 53 on Subgrade Treatment, Type 1BC
- (K2) 220 #/sys QC/QA-HMA, 2, 58S, Surface, 9.5mm on 330 #/sys QC/QA-HMA, 2, 58S, Intermediate, 19.0mm on 330 #/sys QC/QA-HMA, 2, 58S, Base, 19.0mm on 4 in. Compacted Aggregate No. 53 on Geotextile for Pavement, Type 2A on Subgrade Treatment, Type 1BC
- (18) Curb and Gutter, Combined Modified, Reverse
- (29) Seed Mixture, R Mulching Fertilizer

NOT FOR
CONSTRUCTION

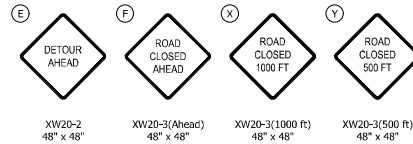
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| RECOMMENDED FOR APPROVAL | |
| DESIGNED: JDD | DRAWN: GEH |
| CHECKED: DGA | CHECKED: DGA |

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|--------------------------------------|--|
| INDIANA DEPARTMENT OF TRANSPORTATION | |
| TYPICAL SECTIONS LINE "S-1-A" | |

| | |
|--------------|-------------|
| SCALE | BRIDGE FILE |
| 1/4" = 1'-0" | DESIGNATION |
| | 2200043 |
| SURVEY BOOK | SHEETS |
| | 13 of 31 |
| CONTRACT | PROJECT |
| S-44548 | 2200043 |



DETOUR ROUTE
Scale: 1" = 300'



NOT FOR
CONSTRUCTION

| | | |
|--------------------------|-----------------|------|
| RECOMMENDED FOR APPROVAL | DESIGN ENGINEER | DATE |
| DESIGNED: DOT | DRAWN: PJV | |
| CHECKED: DGA | CHECKED: DGA | |

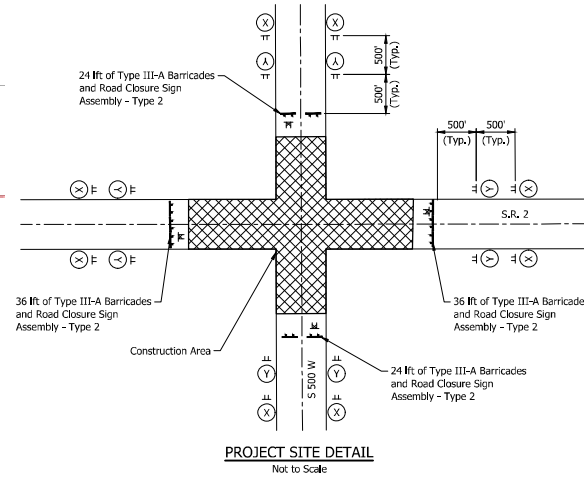
INDIANA
DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC
DETOUR ROUTE

| | |
|---------------------|------------------------|
| SCALE 1" = 300' | BRIDGE FILE |
| | DESIGNATION 2200043 |
| SURVEY BOOK | SHEETS 13 of 70 |
| CONTRACT S-44548 | PROJECT 2200043 |

DETOUR ROUTE MARKER ASSEMBLIES

| | | | |
|--|---|--|---|
| A1 DETOUR EAST INDIANA 2 ADVANCE TURN DETOUR MARKER ASSEMBLY | B1 DETOUR EAST INDIANA 2 DIRECTIONAL DETOUR MARKER ASSEMBLY | C1 DETOUR EAST INDIANA 2 CONFIRMING DETOUR ROUTE MARKER ASSEMBLY | D1 END DETOUR EAST INDIANA 2 END DETOUR ROUTE MARKER ASSEMBLY |
| A2 DETOUR WEST INDIANA 2 ADVANCE TURN DETOUR MARKER ASSEMBLY | B2 DETOUR WEST INDIANA 2 DIRECTIONAL DETOUR MARKER ASSEMBLY | C2 DETOUR WEST INDIANA 2 CONFIRMING DETOUR ROUTE MARKER ASSEMBLY | D2 END DETOUR WEST INDIANA 2 END DETOUR ROUTE MARKER ASSEMBLY |



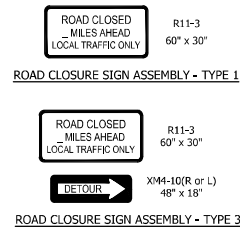
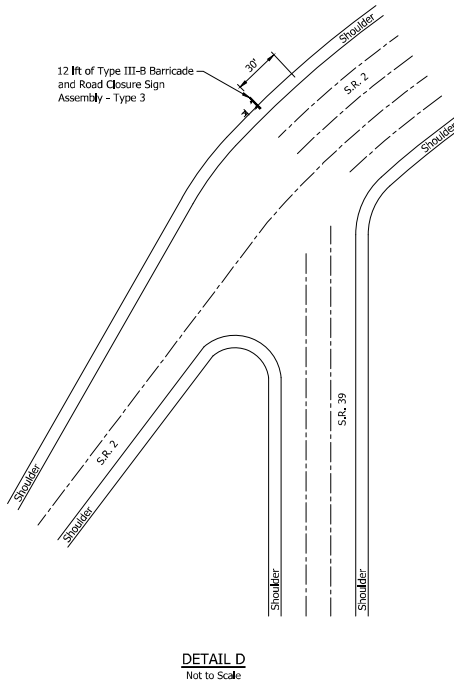
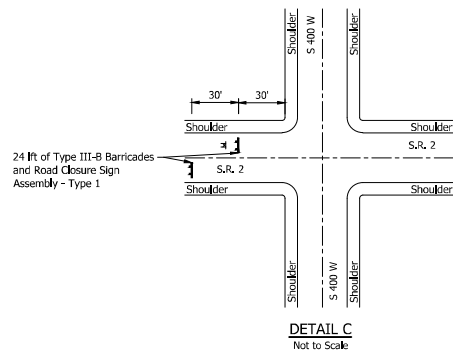
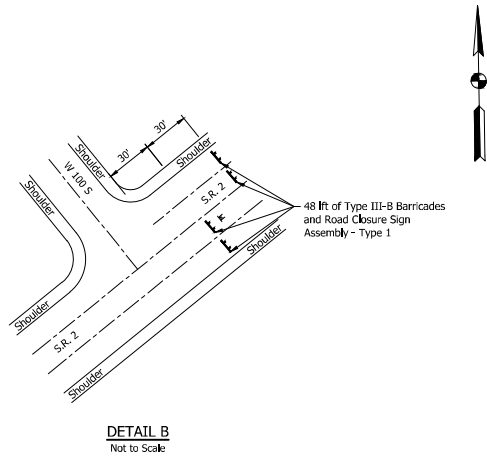
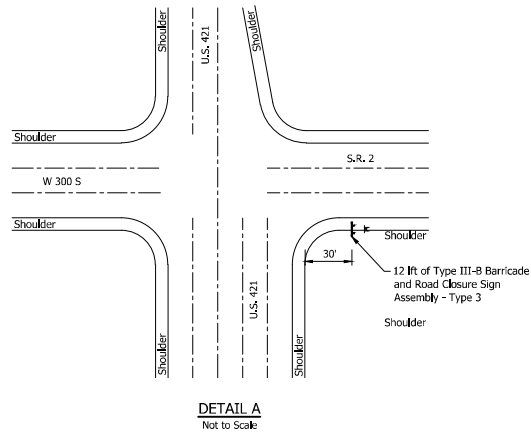
DETOUR ROUTE MARKER ASSEMBLY:

| | |
|------------------|------------------|
| A1 4 Each | A2 4 Each |
| B1 4 Each | B2 4 Each |
| C1 7 Each | C2 5 Each |
| D1 1 Each | D2 1 Each |

LEGEND:

| | |
|-----------|---|
| | Direction of Traffic |
| A1 | Advance Turn Detour Route Marker Assembly |
| B1 | Directional Detour Route Marker Assembly |
| C1 | Confirming Detour Route Marker Assembly |
| D1 | End Detour Route Marker Assembly |
| | Construction Sign and Supports |
| | Road Closure Assembly |
| | Barricade |
| | Detour Route |
| | Construction Area |

DDE - 9/16/2024 9:51 AM - U:\2023\202318 INDOT SP2 Roundabout\Con\Plan Set\Civil\231800MT_DR02.dwg (NOT GEN. NOTES)



| ESTIMATE OF QUANTITIES | | |
|-------------------------------------|----------|----------|
| ITEM | QUANTITY | UNIT |
| Maintaining Traffic | 1 | Lump Sum |
| Construction Sign, Type A | 26 | Each |
| Barricade Type III-A | 120 | Lft |
| Barricade Type III-B | 96 | Lft |
| Detour Route Sign Assemblies | 30 | Each |
| Road Closure Sign Assemblies Type 1 | 2 | Each |
| Road Closure Sign Assemblies Type 2 | 4 | Each |
| Road Closure Sign Assemblies Type 3 | 2 | Each |

- GENERAL NOTES:**
1. Install construction signs as shown on plans and as directed.
 2. Traffic shall be detoured along U.S. 421, U.S. 6 and S.R. 39.
 3. All type 'A' construction signs to have low intensity flashing yellow light, type 'A'.
 4. All materials, procedures, signs, markings, and miscellaneous items shall conform to the requirements of the INDOT standard specifications and the Indiana Manual on Uniform Traffic Control Devices, and revisions there to.
 5. Contractor shall notify local fire department, police, ambulance services, and schools of the work schedule and temporary traffic layouts.

| LEGEND: | |
|---------|-----------------------|
| | Road Closure Assembly |
| | Type III-B Barricade |

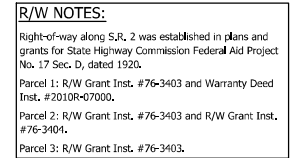
NOT FOR
CONSTRUCTION

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|---------------------------------|--------------|
| RECOMMENDED FOR APPROVAL: _____ | |
| DESIGNED: JDD | DRAWN: PJV |
| CHECKED: DGA | CHECKED: DGA |

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| INDIANA DEPARTMENT OF TRANSPORTATION | |
| MAINTENANCE OF TRAFFIC GENERAL NOTES AND DETAILS | |

| | |
|-----------------------|---------------------------------------|
| SCALE NOT TO SCALE | BRIDGE FILE DESIGNATION 2200043 |
| SURVEY BOOK | SHEETS 14 of 70 |
| CONTRACT R-44648 | PROJECT 2200043 |

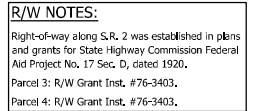




- 11 Saw Cut
- 26 Sodding
- 29 Seed Mixture, R
Mulching
Fertilizer

NOT FOR
CONSTRUCTION

PLAN AND PROFILE - LINE "A"
STA. 914+00 TO STA. 918+00



| | |
|------|---|
| (D) | HMA for Approaches, Type B 165 #/ys HMA Surface, Type B, on 275 #/ys HMA Intermediate, Type B, on 6" Compacted Aggregate, No. 53 on Subgrade Treatment, Type IBC |
| (J) | 220 #/ys QC/QA-HMA, 2, 585, Surface, 9.5mm on 330 #/ys QC/QA-HMA, 2, 585, Intermediate, 13.0mm on 330 #/ys QC/QA-HMA, 2, 585, Base, 19.0mm on 330 #/ys QC/QA-HMA, 2, 585, Base, 19.0mm on 4 in. Compacted Aggregate No. 53 on Subgrade Treatment, Type IBC |
| (K) | 220 #/ys QC/QA-HMA, 2, 585, Surface, 9.5mm on 330 #/ys QC/QA-HMA, 2, 585, Intermediate, 13.0mm on 330 #/ys QC/QA-HMA, 2, 585, Base, 19.0mm on 330 #/ys QC/QA-HMA, 2, 585, Base, 19.0mm on 4 in. Compacted Aggregate No. 53 on Subgrade Treatment, Type IBC |
| (M) | Milling, Profile (0" - 2") |
| (R) | 220 #/ys QC/QA-HMA, 2, 585, Surface, 9.5 mm |
| (W) | 220 #/ys QC/QA-HMA, 2, 585, Surface, 9.5mm on Transition Milling |
| (W) | 220 #/ys QC/QA-HMA, 2, 585, Surface, 9.5mm on 330 #/ys HMA Intermediate, Type B on 330 #/ys HMA Base, Type B on 330 #/ys HMA Base, Type B on 4 in. Compacted Aggregate No. 53 on Subgrade Treatment, Type IBC |
| (11) | Saw Cut |
| (29) | Seed Mixture, R Mulching Fertilizer |
| (99) | Remove |

NOT FOR
CONSTRUCTION

INDIANA
DEPARTMENT OF TRANSPORTATION

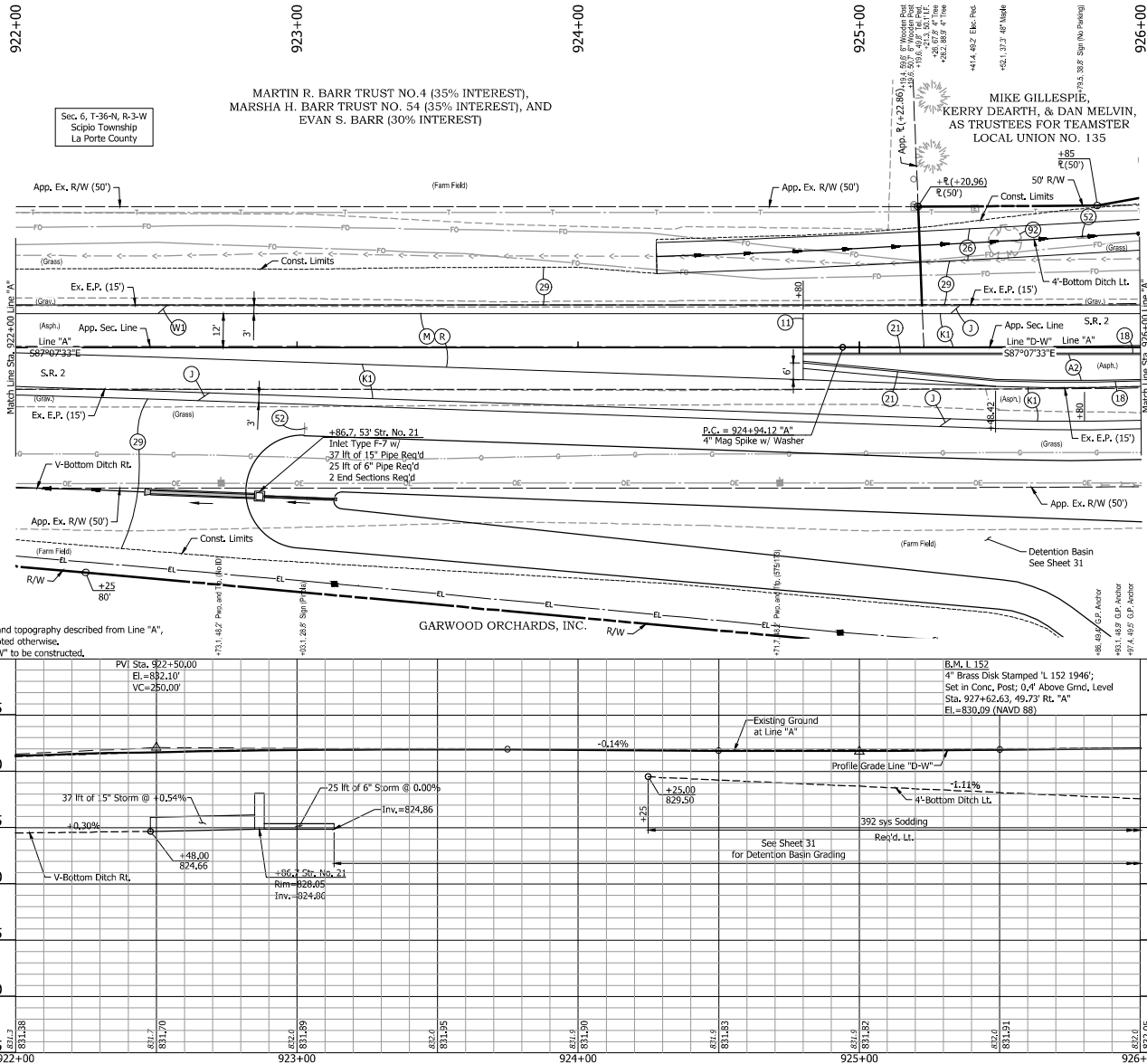
PLAN AND PROFILE - LINE "A"
STA. 918+00 TO STA. 922+00

INDIANA
DEPARTMENT OF TRANSPORTATION

PLAN AND PROFILE - LINE "A"
STA. 918+00 TO STA. 922+00

B-35

FTP - 8/19/2025 1:46 PM - U:\2023\202318 INDOT SR2 Roundabout\Coa\Plan Set\Civil\231801R_PP02.dwg (P & P - 922-926 - LINE A)



Sec. 6, T-36-N, R-3-W
Scipio Township
La Porte County

MARTIN R. BARR TRUST NO. 4 (35% INTEREST),
MARSHA H. BARR TRUST NO. 54 (35% INTEREST), AND
EVAN S. BARR (30% INTEREST)

MIKE GILLESPIE,
KERRY DEARTH, & DAN MELVIN,
AS TRUSTEES FOR TEAMSTER
LOCAL UNION NO. 135

Sec. 7, T-36-N, R-3-W
Scipio Township
La Porte County

LEGEND:

- (A) Color PCCP, 6" on 13" - 15" Compacted Aggregate, No. 53 on (To Match Subgrade Treatment Elevation) Geotextile for Pavement, Type 2A on Subgrade Treatment, Type IBC
- (2) 220 #/sys QC/QA-HMA, 2, 58S, Surface, 9.5mm on 330 #/sys QC/QA-HMA, 2, 58S, Intermediate, 19.0mm on 330 #/sys QC/QA-HMA, 2, 58S, Base, 19.0mm on 4 in. Compacted Aggregate No. 53 on Subgrade Treatment, Type IBC
- (K) 220 #/sys QC/QA-HMA, 2, 58S, Surface, 9.5mm on 330 #/sys QC/QA-HMA, 2, 58S, Intermediate, 19.0mm on 330 #/sys QC/QA-HMA, 2, 58S, Base, 19.0mm on 4 in. Compacted Aggregate No. 53 on Subgrade Treatment, Type IBC
- (M) Milling, Profile (0" - 2")
- (R) 220 #/sys QC/QA-HMA, 2, 58S, Surface, 9.5 mm
- (W) 220 #/sys QC/QA-HMA, 2, 58S, Surface, 9.5mm on Widening with HMA, Type B:
330 #/sys HMA Intermediate, Type B on 330 #/sys HMA Base, Type B on 330 #/sys HMA Base, Type B on 4 in. Compacted Aggregate No. 53 on Subgrade Treatment, Type IBC
- (11) Saw Cut
- (18) Curb and Gutter, Combined Modified, Reverse
- (21) Curb and Gutter, Type B, Combined Modified, Reverse
- (26) Sodding
- (29) Seed Mixture, R Mulching Fertilizer
- (52) Remove Existing Sign & Supports
- (92) Tree Removal

Scale: 1" = 20'

North Arrow

Sign (No) (11E) 55.9' AL=30'

Line A

Sta. 924+94.12 "A"

Mag Spike w/ "Lawson-Fisher" Washer

NOT FOR CONSTRUCTION

| | |
|--------------------------|--------------|
| RECOMMENDED FOR APPROVAL | |
| DESIGNED: DDE | DRAWN: GDH |
| CHECKED: DGA | CHECKED: DGA |

| | |
|--------------------------------------|--|
| INDIANA DEPARTMENT OF TRANSPORTATION | |
| PLAN AND PROFILE - LINE "A" | |
| STA. 922+00 TO STA. 926+00 | |

| | |
|------------------|-------------|
| HORIZONTAL SCALE | BRIDGE FILE |
| 1" = 20' | |
| VERTICAL SCALE | DESIGNATION |
| 1" = 5' | 2200043 |
| | SHEETS |
| 18 | of 70 |
| | CONTRACT |
| | R-44648 |

R/W NOTES:

Right-of-way along S.R. 2 was established in plans and grants for State Highway Commission Federal Aid Project No. 17 Sec. D, dated 1920.

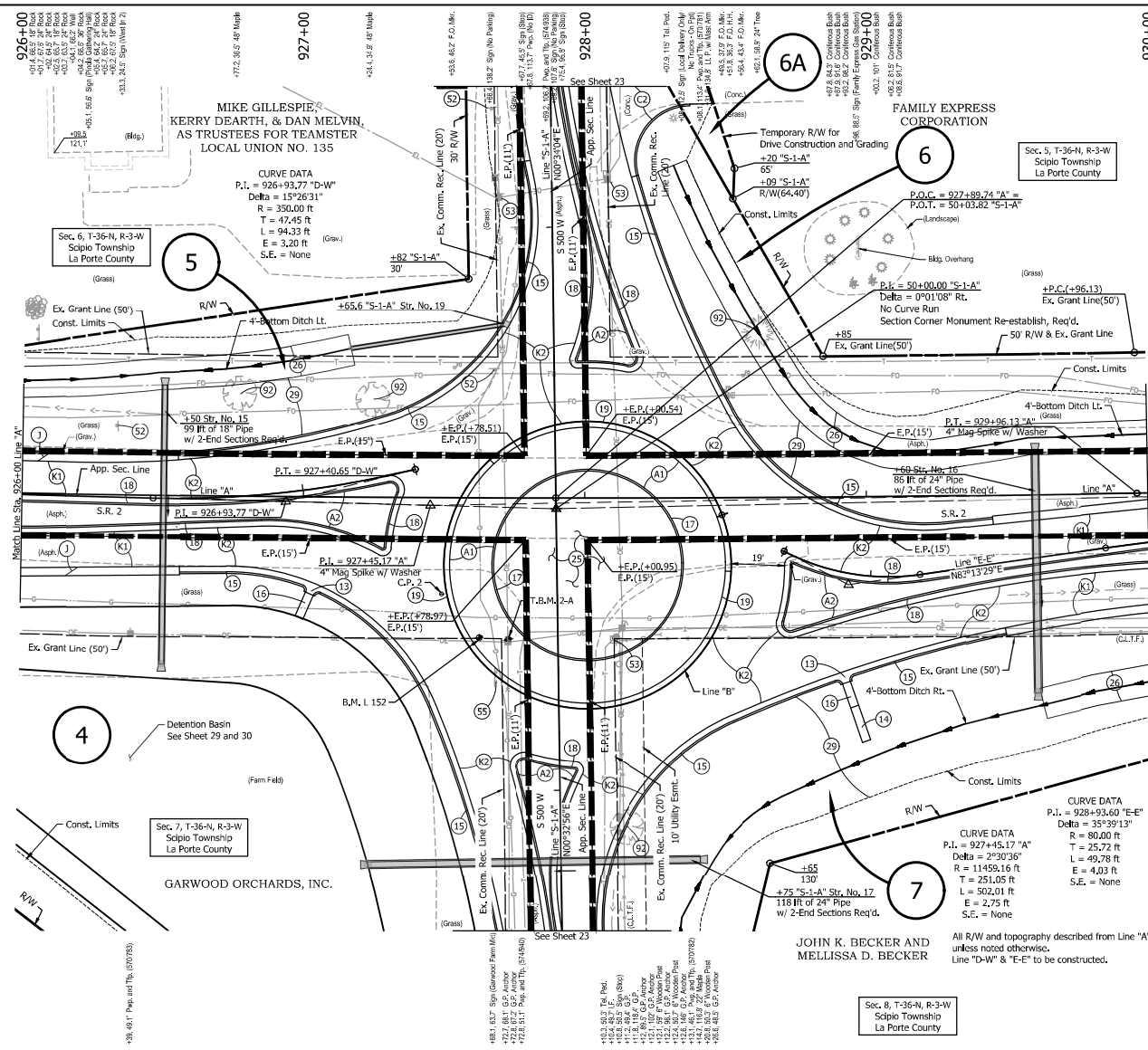
Right-of-way along C.R. 500 W was established by Commissioner's Record Book B, Pages 226, 227, and 534, dated 1941.

Parcel 4: R/W Grant Inst. #76-3403 and Commissioner's Record Book B, Pages 226, 227, and 534.

Parcel 5: R/W Grant Book 199, Page 362 and Commissioner's Record Book B, Pages 226, 227, and 534.

Parcel 6: R/W Grant Inst. #76-3405 and Commissioner's Record Book B, Pages 226, 227, and 534.

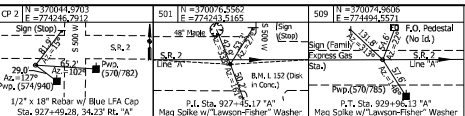
Parcel 7: R/W Grant Inst. #76-3406 and Commissioner's Record Book B, Pages 226, 227, and 534.



LEGEND:

- A1 Color PCP 9" on 9" Compacted Aggregate, No. 53 on Geotextile for Pavement, Type 2A on Subgrade Treatment, Type IBC
- A2 Color PCP, 6" on 13" - 15" Compacted Aggregate, No. 53 on (To Match Subgrade Treatment Elevation) Geotextile for Pavement, Type 2A on Subgrade Treatment, Type IBC
- C1 PCP for Approaches, 9" on Dense Graded Subbase, 6" on Geogrid, Type IB on Subgrade Treatment, Type II
- J 220 #/sys QC/QA-HMA, 2, 58S, Surface, 9.5mm on 330 #/sys QC/QA-HMA, 2, 58S, Intermediate, 19.0mm on 330 #/sys QC/QA-HMA, 2, 58S, Base, 19.0mm on 4 in. Compacted Aggregate No. 53 on Subgrade Treatment, Type IBC
- K1 220 #/sys QC/QA-HMA, 2, 58S, Surface, 9.5mm on 330 #/sys QC/QA-HMA, 2, 58S, Intermediate, 19.0mm on 330 #/sys QC/QA-HMA, 2, 58S, Base, 19.0mm on 4 in. Compacted Aggregate No. 53 on Subgrade Treatment, Type IBC
- K2 220 #/sys QC/QA-HMA, 2, 58S, Surface, 9.5mm on 330 #/sys QC/QA-HMA, 2, 58S, Intermediate, 19.0mm on 330 #/sys QC/QA-HMA, 2, 58S, Base, 19.0mm on 300 #/sys QC/QA-HMA, 4, 58E, Intermediate, OG, 19.0mm on 4 in. Compacted Aggregate No. 53 on Subgrade Treatment, Type IBC
- 13 Curb and Gutter, Turnout Combined
- 14 Paved Side Ditch, Type A
- 15 Curb and Gutter, Combined
- 16 Concrete Curb, A
- 17 Integral Curb
- 18 Curb and Gutter, Combined Modified, Reverse
- 19 Curb and Gutter, Type B, Modified, 3" Reveal, Reverse
- 25 Native Seed Mixture
- 26 Sodding
- 29 Seed Mixture, R Mulching Fertilizer
- 52 Remove Existing Sign & Supports
- 53 Remove Existing Sign From Existing Pole
- 55 Sign Relocate
- 92 Tree Removal

JAN - 12/17/2024 1:51 PM - U:\2023\202318 INDI SR2 Roundabout\Con Plan Set\GWH\231800TR_P103.dwg [PLAN - 926+00 - 930+00 - LINE A]



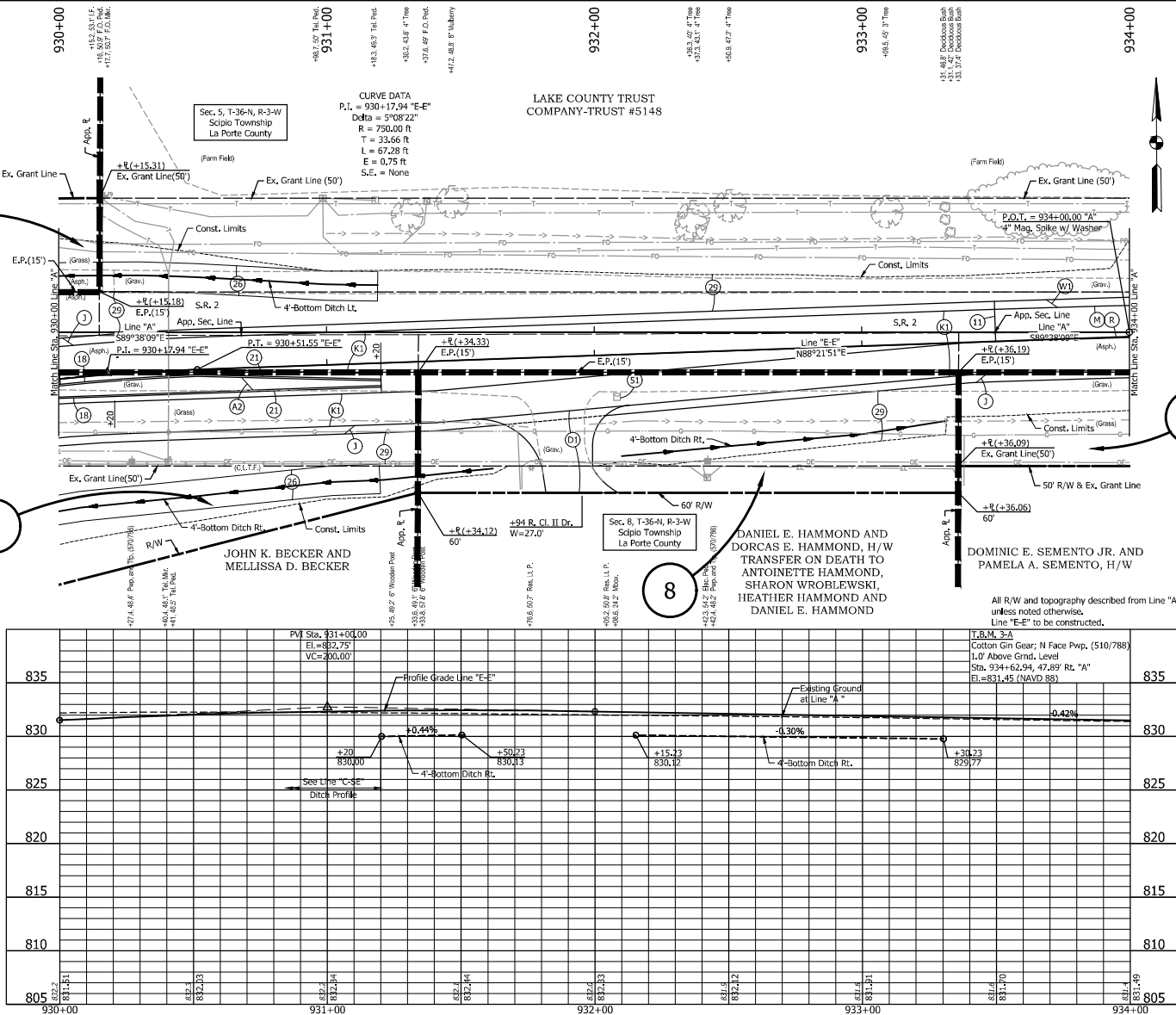
NOT FOR CONSTRUCTION

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|--------------------------|--------------|
| RECOMMENDED FOR APPROVAL | |
| DESIGNED: DDE | DRAWN: GDH |
| CHECKED: DGA | CHECKED: DGA |

| | |
|--------------------------------------|--|
| INDIANA DEPARTMENT OF TRANSPORTATION | |
| PLAN - LINE "A" | |
| STA. 926+00 TO STA. 930+00 | |

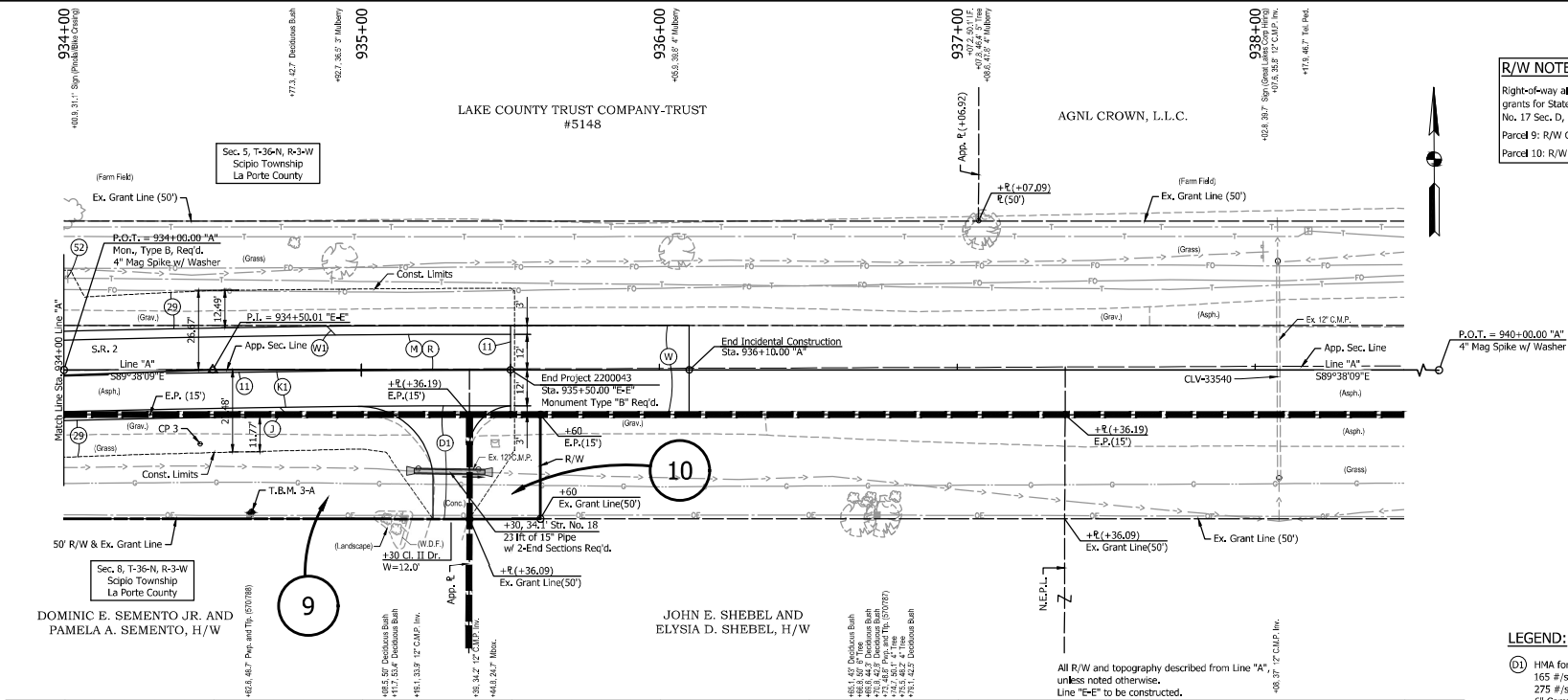
| | |
|-------------|-------------|
| SCALE | BRIDGE FILE |
| 1" = 20' | DESIGNATION |
| | 2200043 |
| SURVEY BOOK | SHEETS |
| | 18 of 31 |
| CONTRACT | PROJECT |
| S-44548 | 2200043 |

R/W NOTES:
 Right-of-way along S.R. 2 was established in plans and grants for State Highway Commission Federal Aid Project No. 17 Sec. D, dated 1920.
 Parcel 6: R/W Grant Inst. #76-3405.
 Parcel 7: R/W Grant Inst. #76-3406.
 Parcel 8: R/W Grant Inst. #76-3406.
 Parcel 9: R/W Grant Inst. #76-3406.

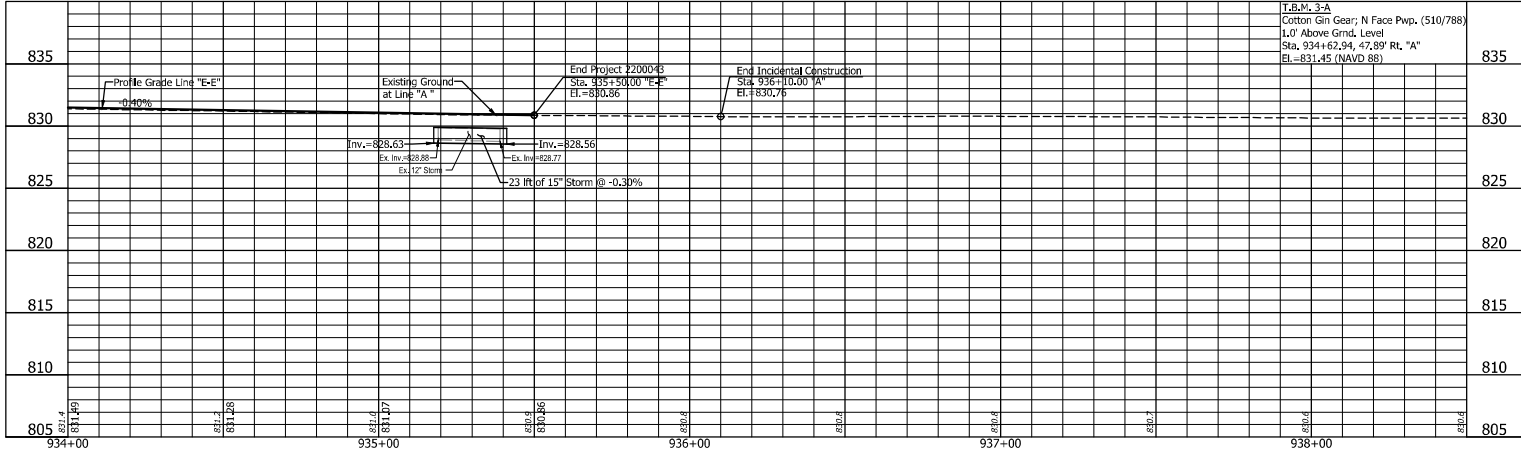


- LEGEND:**
- (A2) Color PCPP, 6\"/>

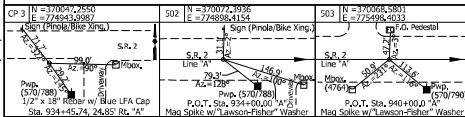
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| NOT FOR CONSTRUCTION | | RECOMMENDED FOR APPROVAL _____ DESIGN ENGINEER _____ DATE _____ | | INDIANA DEPARTMENT OF TRANSPORTATION | |
| DESIGNED: DDE | DRAWN: GDH | PLAN AND PROFILE - LINE "A" STA. 930+00 TO STA. 934+00 | | SURVEY BOOK _____ CONTRACT R-44548 | |
| CHECKED: DGA | CHECKED: DGA | | | SHEETS 20 of 31 PROJECT 2200043 | |



R/W NOTES:
 Right-of-way along S.R. 2 was established in plans and grants for State Highway Commission Federal Aid Project No. 17 Sec. D, dated 1920.
 Parcel 9: R/W Grant Inst. #76-3406.
 Parcel 10: R/W Grant Inst. #76-3406.



- LEGEND:**
- (D1) HMA for Approaches, Type B
 - 165 #/sys HMA Surface, Type B, on
 - 275 #/sys HMA Intermediate, Type B, on
 - 6" Compacted Aggregate, No. 53 on
 - Subgrade Treatment, Type II
 - 220 #/sys QC/QA-HMA, 2, 585, Surface, 9.5mm on
 - 330 #/sys QC/QA-HMA, 2, 585, Intermediate, 15.0mm on
 - 330 #/sys QC/QA-HMA, 2, 585, Base, 19.0mm on
 - 330 #/sys QC/QA-HMA, 2, 585, Base, 19.0mm on
 - 4 in. Compacted Aggregate No. 53 on
 - Subgrade Treatment, Type IBC
 - 220 #/sys QC/QA-HMA, 2, 585, Surface, 9.5mm on
 - 330 #/sys QC/QA-HMA, 2, 585, Intermediate, 19.0mm on
 - 330 #/sys QC/QA-HMA, 2, 585, Base, 19.0mm on
 - 330 #/sys QC/QA-HMA, 2, 585, Base, 19.0mm on
 - 4 in. Compacted Aggregate No. 53 on
 - Subgrade Treatment, Type IBC
 - (M) Milling, Profile (0" - 2")
 - (R) 220 #/sys QC/QA-HMA, 2, 585, Surface, 9.5 mm
 - (W) 220 #/sys QC/QA-HMA, 2, 585, Surface, 9.5mm on
 - Transition Milling
 - (W1) 220 #/sys QC/QA-HMA, 2, 585, Surface, 9.5mm on
 - 330 #/sys HMA Intermediate, Type B on
 - 330 #/sys HMA Base, Type B on
 - 330 #/sys HMA Base, Type B on
 - 330 #/sys HMA Base, Type B on
 - 4 in. Compacted Aggregate No. 53 on
 - Subgrade Treatment, Type IBC
 - (11) Saw Cut
 - (29) Seed Mixture, R
 - Mulching
 - Fertilizer
 - (52) Remove Existing Sign & Supports



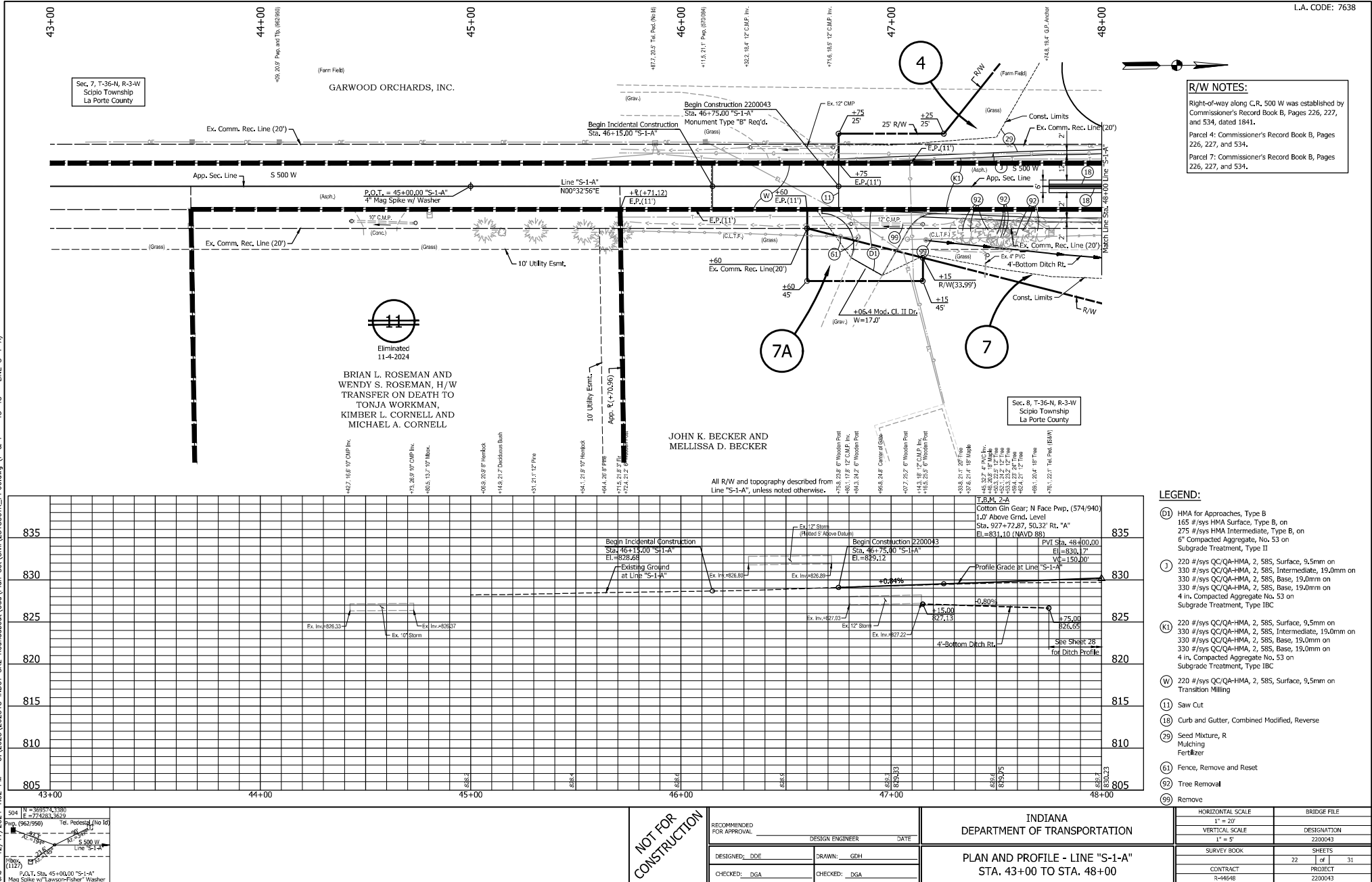
NOT FOR CONSTRUCTION

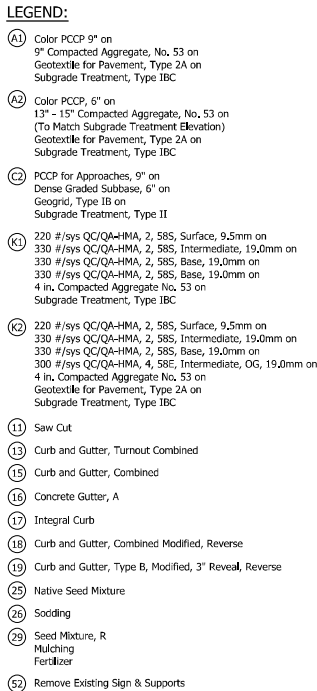
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| DESIGNED: DDE | DRAWN: GWH | | |
| CHECKED: DGA | CHECKED: DGA | | |

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| INDIANA DEPARTMENT OF TRANSPORTATION | |
| PLAN AND PROFILE - LINE "A" | |
| STA. 934+00 TO STA. 938+50 | |

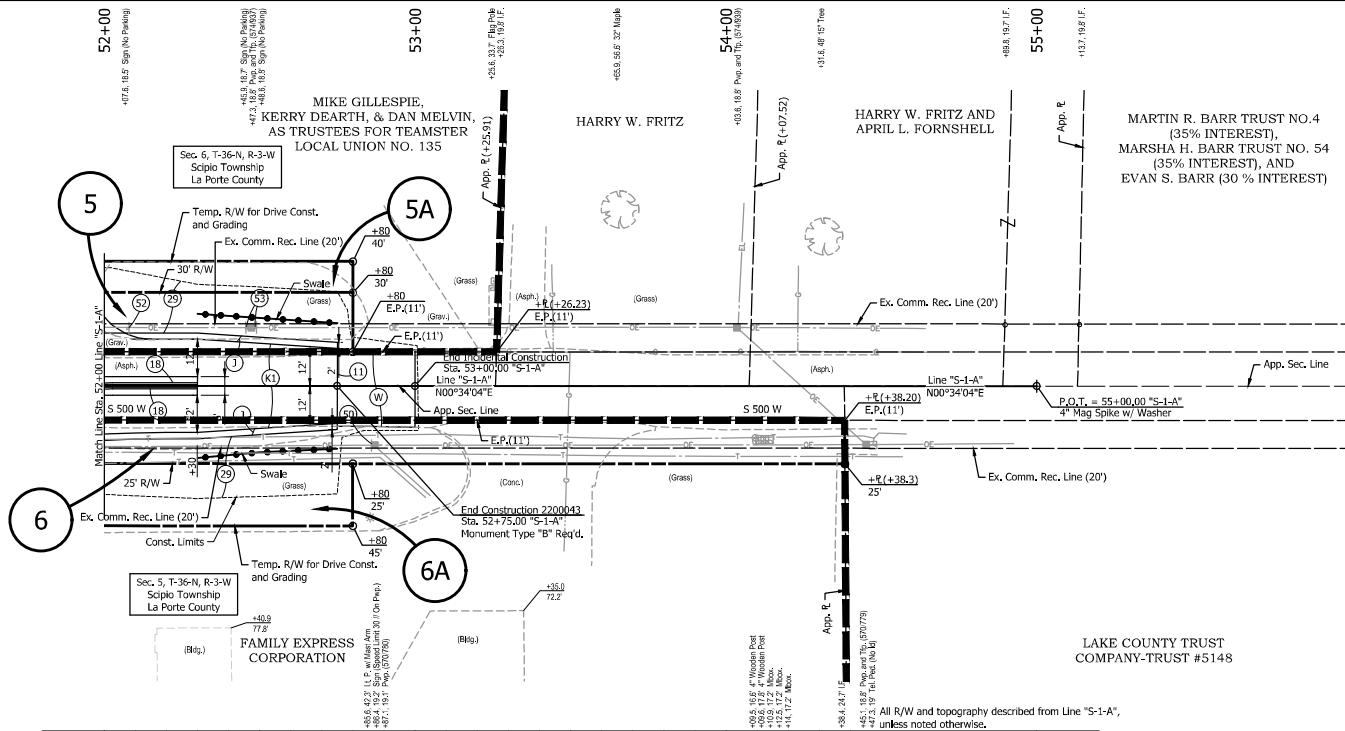
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|------------------|-------------|
| HORIZONTAL SCALE | BRIDGE FILE |
| 1" = 20' | |
| VERTICAL SCALE | DESIGNATION |
| 1" = 5' | 2200043 |
| SURVEY BOOK | SHEETS |
| | 21 of 31 |
| CONTRACT | PROJECT |
| R-44648 | 2200043 |

\\JAN - 12/17/2024 1:52 PM - U:\2023\202318 INDOT SR2 Roundabout\Con\Plan Set\GWH\231800TR_PP06.dwg (P & P - 43-48 - LINE S-1-A)

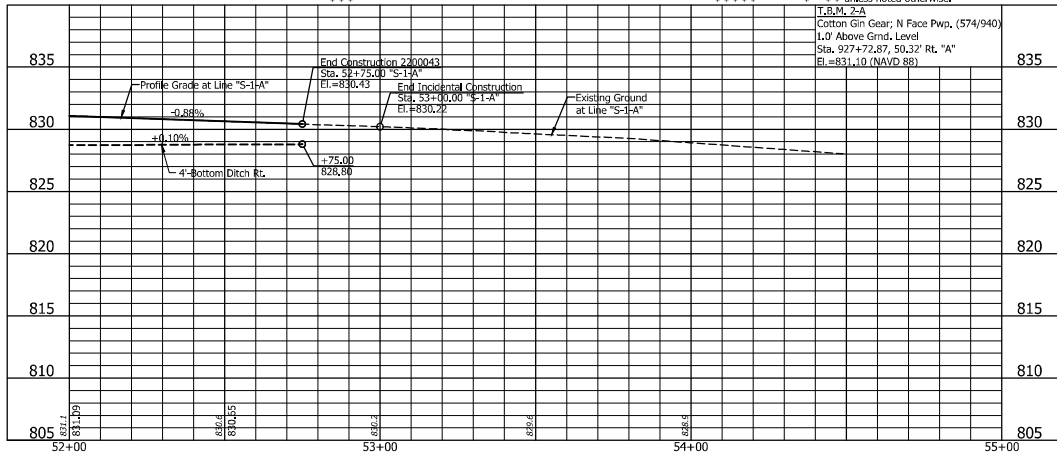


B-41

NOT FOR
CONSTRUCTION



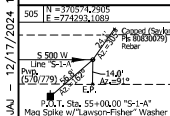
R/W NOTES:
Right-of-way along C.R. 500 W was established by Commissioner's Record Book B, Pages 226, 227, and 534, dated 1841.
Parcel 5: Commissioner's Record Book B, Pages 226, 227, and 534.
Parcel 6: Commissioner's Record Book B, Pages 226, 227, and 534.



LEGEND:

- ① 220 #1/2s QC/QA-HMA, 2, 58S, Surface, 9.5mm on 330 #1/2s QC/QA-HMA, 2, 58S, Intermediate, 19.0mm on 330 #1/2s QC/QA-HMA, 2, 58S, Base, 19.0mm on 4 in. Compacted Aggregate No. 53 on Subgrade Treatment, Type IBC
- (K) 220 #1/2s QC/QA-HMA, 2, 58S, Surface, 9.5mm on 330 #1/2s QC/QA-HMA, 2, 58S, Intermediate, 19.0mm on 330 #1/2s QC/QA-HMA, 2, 58S, Base, 19.0mm on 4 in. Compacted Aggregate No. 53 on Subgrade Treatment, Type IBC
- (W) 220 #1/2s QC/QA-HMA, 2, 58S, Surface, 9.5mm on Transition Milling
- (11) Saw Cut
- (18) Curb and Gutter, Combined Modified, Reverse
- (29) Seed Mixture, R Mulching Fertilizer
- (50) No Change Required to Existing Sign and Supports
- (52) Remove Existing Sign & Supports
- (53) Remove Existing Sign From Existing Pole

LAJ - 12/17/2024 1:52 PM - U:\2023\202318 INDOT SR2 Roundabout\Con\Plan Set\G:\231800TR_PP08.dwg (P & P - 52-55 - LINE S-1-A)

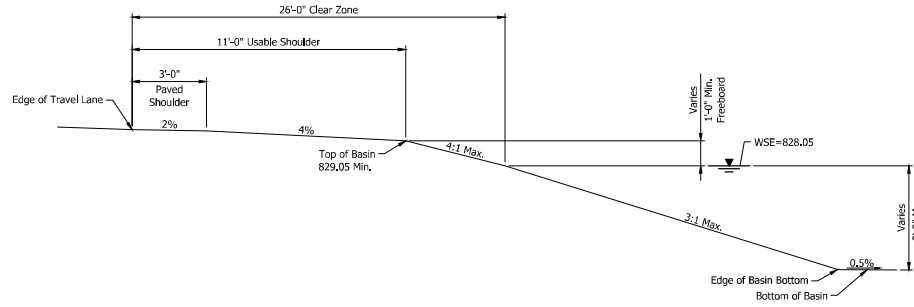


NOT FOR CONSTRUCTION

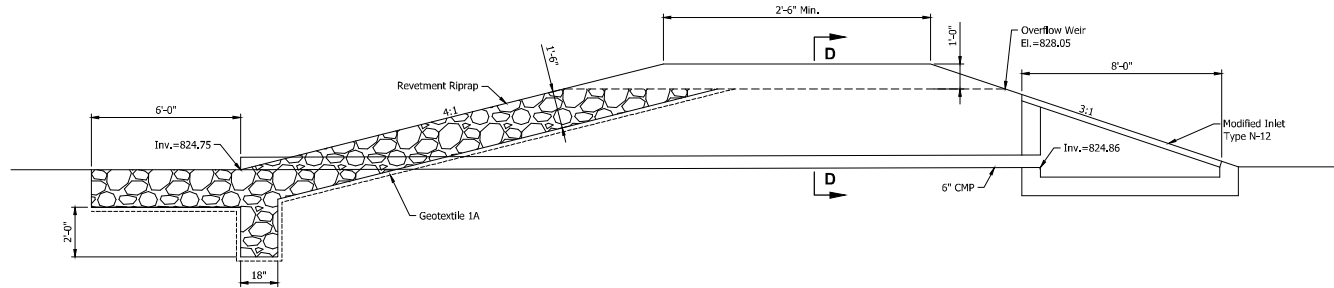
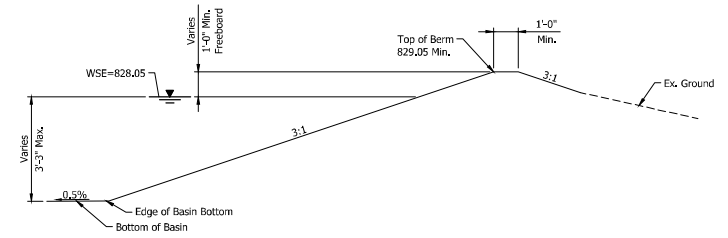
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| CHECKED: DGA | CHECKED: DGA |

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| PLAN AND PROFILE - LINE "S-1-A" STA. 52+00 TO STA. 55+00 |

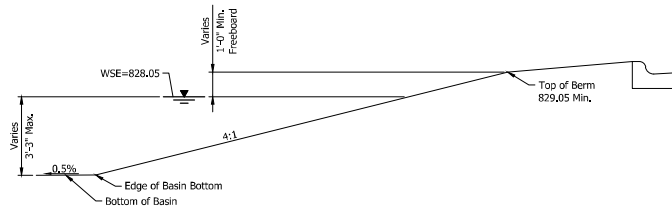
| | |
|------------------------------|------------------------|
| HORIZONTAL SCALE 1" = 20' | BRIDGE FILE |
| VERTICAL SCALE 1" = 5' | DESIGNATION 2200043 |
| SURVEY BOOK | SHEETS 24 of 31 |
| CONTRACT S-44548 | PROJECT 2200043 |



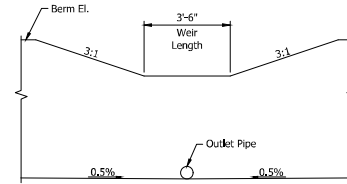
SECTION A-A
Not to Scale



SECTION B-B
Not to Scale



SECTION C-C
Not to Scale



SECTION D-D
Not to Scale

NOT FOR
CONSTRUCTION

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| RECOMMENDED FOR APPROVAL | DESIGN ENGINEER | DATE |
| DESIGNED: DJT | DRAWN: PJV | |
| CHECKED: DGA | CHECKED: DGA | |

INDIANA
DEPARTMENT OF TRANSPORTATION
DETENTION BASIN SECTIONS

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| SCALE | BRIDGE FILE |
| NOT TO SCALE | DESIGNATION |
| | 2200043 |
| SURVEY BOOK | SHEETS |
| 30 | of 31 |
| CONTRACT | PROJECT |
| S-14618 | 2200043 |

APPENDIX C

Early Coordination



INDIANA DEPARTMENT OF TRANSPORTATION

100 North Senate Avenue
Room N642
Indianapolis, Indiana 46204

855-INDOT4U

Mike Braun, Governor
Kent Abernathy, Commissioner

Sample Early Coordination Letter

January 29, 2025

Re: Early Coordination
Des. No. 2200043
Intersection Improvement Project
SR 2 and CR 500 West
Scipio Township, LaPorte County, Indiana

Dear Agency:

The Indiana Department of Transportation (INDOT), with partial funding and oversight from the Federal Highway Administration (FHWA), intends to proceed with an intersection improvement project in the aforementioned area. This letter is part of the early coordination phase of the environmental review process. We are requesting comments from your area of expertise regarding any possible environmental effects associated with this project. Please use the above designation number (Des. No. 2200043) and description in your reply. We will incorporate your comments into a study of the project's environmental impacts.

The project is located at the intersection of SR 2 and CR 500 W in LaPorte County, Indiana, southwest of the City of LaPorte, approximately 3.27 miles West of SR 39. Specifically, the project is located within Sections 5, 6, 7, 8, Township 36 North, Range 3 West of the LaPorte West, Indiana 7.5-minute United States Geological Survey (USGS) topographic quadrangles. Land use within the project area is predominantly rural, residential, and agricultural.

SR 2 and CR 500 W are both two-lane roadways. SR 2 is classified as a Rural Principal Arterial Other with two 12-foot-wide travel lanes, narrow shoulders, and right turn lanes onto CR 500 W at the intersection. CR 500 W is classified as a Rural Major Collector with two 11-foot-wide travel lanes and no shoulders in the project area. The intersection is controlled with stop signs on CR 500 W. There are no sidewalks present. The posted speed limit is 35 to 45 miles-per-hour (mph) in the project area.

The need for this project is due to the high frequency of crashes at this intersection. A total of 22 crashes occurred at the intersection during the study period from 2017 to 2019: the majority involving right and left turns. The purpose of this project is to address the high crash rate and operational characteristics of the intersection.

The preferred alternative will consist of constructing a single-lane roundabout at the intersection. Project limits are preliminarily estimated to extend approximately 500 feet to the east and west along SR 2 and approximately 400 feet north and south along CR 500 W. Curb and gutter will be installed within the limits of the roundabout to drain excess runoff into a new detention basin to be constructed in the southwest quadrant of the roundabout. New overhead lighting will be installed along each leg of the roundabout per design requirements. The project letting date is in July 2027 and construction is anticipated to begin in Fall 2027.

Approximately 2.02 acres of additional permanent right-of-way will be required to construct the project.

If we do not receive your response within thirty (30) calendar days from the date of this letter, it will be assumed that your agency believes that there will be no adverse effects incurred as a result of the proposed project. However, if you find that an extension to the response time is necessary, a reasonable amount may be granted upon request.

If you have any questions regarding this matter, please feel free to contact Elayna Stoner, Project Manager, at Metric Environmental, by telephone at 317.315.3322, or by email at elaynas@metricenv.com.

Sincerely,

Elayna Stoner

Project Manager
Metric Environmental, LLC

cc: Metric File No. 22-0216-1
Dimitri Adams, Lawson-Fisher Associates (LFA) Project Manager, dadams@lawson-fisher.com
Dan Delgado, LFA, ddelgado@lawson-fisher.com
Michael Grylewicz, INDOT LaPorte District Senior Project Manager, mgrylewicz@indot.in.gov

Graphics Provided with this Letter are Located in Appendix B of this Document



INDIANA DEPARTMENT OF TRANSPORTATION

100 North Senate Avenue
Room N758-ES
Indianapolis, Indiana 46204

PHONE: (855) 463-6848

Eric Holcomb, Governor
Michael Smith, Commissioner

The following agencies received Early Coordination Letters:

Federal Highway Administration
k.carmanygeorge@dot.gov

Indiana Geological and Water Survey
<https://igws.indiana.edu/eAssessment>

Indiana Department of Natural Resources
Division of Fish and Wildlife
environmentalreview@dnr.in.gov

US Department of Housing & Urban Development
Chicago Regional Office
erik.r.sandstedt@hud.gov

Indiana Department of Transportation
LaPorte District
SMichels@indot.in.gov
mgrylewicz@indot.in.gov

Indiana Department of Transportation
Office of Aviation
TLewandowski@indot.in.gov

Natural Resources Conservation Service
State Conservationist
john.allen@usda.gov

Northwestern Indiana Regional Planning Commission
twarner@nirpc.org

US Fish and Wildlife Service
Northern Indiana Suboffice
elizabeth_mccloskey@fws.gov

LaPorte County Board of Commissioners
cgramarossa@laporteco.in.gov

LaPorte County Emergency Management Agency
rsabie@laporteco.in.gov

LaPorte County Emergency Medical Services
EFenstermaker@laporteco.in.gov

LaPorte County Highway Department
cglaser@laporteco.in.gov

LaPorte County Surveyor's Office
ahendricks@laporteco.in.gov

LaPorte Fire Department
andy.snyder@laportefire.com

LaPorte Police Department
dbuell@lpcitypd.com

THIS IS NOT A PERMIT

State of Indiana
DEPARTMENT OF NATURAL RESOURCES
Division of Fish and Wildlife
Early Coordination/Environmental Assessment

DNR#: ER-26412

Request Received: April 1, 2024

Requestor:

Joshua Netherton
Metric Environmental, LLC
6958 Hillsdale Court
Indianapolis, IN 46250

Project:

SR 2 & CR 500 West single-lane roundabout construction with a new detention basin in the southwest quadrant, 3.27 miles west of SR 39; Des #2200043

County/Site Info: LaPorte County

The Indiana Department of Natural Resources has reviewed the above referenced project per your request. Our agency offers the following comments for your information and in accordance with the National Environmental Policy Act of 1969.

If our agency has regulatory jurisdiction over the project, the recommendations contained in this letter may become requirements of any permit issued. If we do not have permitting authority, all recommendations are voluntary.

Regulatory Assessment:

Formal approval by the Department of Natural Resources under the regulatory programs administered by the Division of Water is not required for this project.

Natural Heritage Database:

The Natural Heritage Program's data have been checked. To date, no plant or animal species listed as state or federally threatened, endangered, or rare have been reported to occur in the project vicinity.

Fish and Wildlife Comments:

Avoid and minimize impacts to fish, wildlife, and botanical resources to the greatest extent possible, and compensate for impacts. The following are recommendations that address potential impacts identified in the proposed project area:

A) Street Trees

The Division of Fish and Wildlife recommends avoiding removing trees to the greatest extent possible and replacing trees that must be removed. Indiana's street trees provide millions of dollars of tangible benefits to Indiana communities by their presence in the urban and rural environment. Their shade and beauty contribute to the quality of life. They provide significant increases in real estate values, create attractive settings for commercial businesses, and improve community neighborhood appeal. Trees decrease energy consumption by providing shade and acting as windbreaks. They reduce water treatment costs and impede soil erosion by slowing the runoff of stormwater. Trees also cool the air temperature, cleanse pollutants from the air, and produce oxygen while absorbing carbon dioxide. Trees are an integral component of the urban environment. Proactively managing and maintaining a street tree population will ultimately maximize the benefits afforded by their aesthetic and ecological functions.

The following links give a good overview of the benefits of a street tree program and how to select the right species to avoid the negative impacts of non-native invasive species such as the common and popular Bradford pear: <https://www.in.gov/dnr/forestry/forestry-publications-and-presentations/> (scroll down to the Community & Urban Forestry section).

B) Lighting

Most transportation corridor designers and municipalities are trending toward LED lighting. Certain types of LED lighting can have negative impacts on both human and wildlife health and safety. The Division of Fish and Wildlife strongly encourages visiting the International Dark-Sky Association's website to learn more about the potential negative impacts of improperly selected LED lighting systems, if required: <https://www.darksky.org/our-work/lighting/lighting-for-citizens/led-guide/>.

The additional measures listed below should be implemented to avoid, minimize, or compensate for impacts to fish, wildlife, and botanical resources:

1. Revegetate all bare and disturbed areas that are not currently mowed and maintained with a mixture of grasses (excluding all varieties of tall fescue) and legumes as soon as possible upon completion; turf-type grasses (including low-endophyte, friendly endophyte, and endophyte free tall fescue but excluding all other varieties of tall fescue) may be used in currently mowed areas only.
2. Minimize and contain within the project limits all tree and brush clearing.
3. Do not cut any trees suitable for Indiana Bat or Northern Long-eared Bat roosting (3 inches or greater diameter-at-breast height, living or dead, with loose hanging bark, or with cracks, crevices, or cavities) from April 1 through September 30.
4. Appropriately designed measures for controlling erosion and sediment must be implemented to prevent sediment from entering the waterbody or leaving the construction site; maintain these measures until construction is complete and all disturbed areas are stabilized.
5. If erosion control blankets are used, they shall be heavy-duty, biodegradable, and net free or use loose-woven/Leno-woven netting to minimize the entrapment and snaring of small-bodied wildlife such as snakes and turtles (follow manufacturer's recommendations for selection and installation); seed and apply mulch on all other disturbed areas.
6. Plant five trees, 1 inch to 2 inches in diameter-at-breast height, for each tree which is removed that is 10 inches or greater in diameter-at-breast height.

Contact Staff:

Our agency appreciates this opportunity to be of service. Please contact me at RVanVoorhis@dnr.IN.gov or (317) 232-8163 if we can be of further assistance.

Rachel Van Voorhis
Rachel Van Voorhis
Environmental Coordinator
Division of Fish and Wildlife

Date: April 17, 2024

From: [McCloskey, Elizabeth](#)
To: [Joshua Netherton](#)
Cc: [Elayna Stoner](#); [Dmitri Adams](#); [Dan Delgado](#)
Subject: Re: [EXTERNAL] Early Coordination Letter - (Des. No. 2200043) SR 2 and CR 500 West Intersection Improvement Project, 3.27 Miles West of SR 39, LaPorte County
Date: Monday, April 1, 2024 12:49:55 PM
Attachments: [image001.png](#)

Good afternoon, because the proposed project will have minor impacts on natural resources, and no Federally listed endangered species are known to be present, the U.S. Fish and Wildlife Service will not be providing a comment letter.

I will be retiring at the end of April and no one will be in the Northern Indiana Suboffice and this email address will no longer exist. So it will be necessary to send all early coordination letters and other correspondence to Robin McWilliams Munson at the Indiana Field Office in Bloomington at robin_mcwilliams@fws.gov. Please pass this information on to your co-workers so that they send inquiries to Robin.

Thank you.

Elizabeth McCloskey
U.S. Fish and Wildlife Service
Northern Indiana Suboffice
Ecological Services
Chesterton, Indiana

From: Joshua Netherton <joshuan@metricenv.com>
Sent: Monday, April 1, 2024 8:29 AM
Subject: [EXTERNAL] Early Coordination Letter - (Des. No. 2200043) SR 2 and CR 500 West Intersection Improvement Project, 3.27 Miles West of SR 39, LaPorte County

Des. No.: 2200043, State Project
Project Description: State Road (SR) 2 County Road (CR) 500 West Intersection Improvement Project, 3.27 miles West of SR 39
Project Location: Scipio Township, LaPorte County, Indiana

The Indiana Department of Transportation (INDOT), with partial funding from the Federal Highway Administration, intends to proceed with an intersection improvement project at the intersection of SR 2 and CR 500 West, approximately 3.27 miles west of SR 39, in LaPorte County. Please see the attached early coordination letter for a full description of the proposed project.

This letter is part of the early coordination phase of the environmental review process requesting

comments associated with this project. We are requesting comments from your area of expertise regarding any possible environmental effects associated with this project. Please review the attached letter and respond with your comments on any possible impacts to be incurred as a result of this project so that an environmental report can be completed. We also welcome your related opinions and other input to be considered in the preparation of the environmental document. If a hard copy of the materials is needed, please respond to this email with your request as soon as you can.

Recipients have thirty (30) calendar days from receipt of this information to review and provide comments. If we do not receive a response from a recipient within the time allotted, the project will proceed consistent with the proposed design.

If any further information is needed, you may contact Josh Netherton, Metric Environmental, by replying directly to this email or by telephone at 317.981.3093, or contact Michael Grylewicz, INDOT LaPorte District Senior Project Manager, at mgrylewicz@indot.in.gov or by telephone at 219.851.0169.

Thank you in advance for your input,



Josh Netherton

Assistant Project Manager

NEPA Compliance

O 317.400.1633 ext.157

M 317.981.3093

6958 Hillside Court
Indianapolis, IN 46250

www.metricenv.com

Certified DBE/MBE/SBE Company
Indiana | Kentucky | Ohio | West Virginia

Federal Highway Administration (FHWA), Federal Railroad
Administration (FRA), and Federal Transit Administration (FTA)

Programmatic Biological Opinion/Conference Opinion for
Transportation Projects in the Range of the Indiana Bat, Northern
Long-Eared Bat, and Tricolored Bat

Appendix B: Project Submittal Form

Updated December 2024

The use of the Assisted Determination Key in the U.S. Fish and Wildlife Service (Service) Information for Planning and Conservation (IPaC) System is strongly recommended for submitting project-level information to the Service for use of the range-wide programmatic consultation covering actions that may affect the Indiana bat, northern long-eared bat (NLEB), or tricolored bat (TCB). However, if not using the assisted determination key, transportation agencies must provide this submittal form (or a comparable Service approved form) with project-level information to the Service. The completed form should be submitted to the appropriate Service Field Office prior to project commencement. For more information, see the Standard Operating Procedure for Site Specific Project(s) Submission in the User's Guide (Section 3).

By submitting this form, the transportation agency ensures that each component of the proposed project(s) adheres to the criteria and conditions of the 2024 range-wide programmatic biological opinion (PBO). Upon submittal of this form, the appropriate Service Field Office may review the project-specific information provided and request additional information. For projects that may affect but are not likely to adversely affect (NLAA) the Indiana bat, NLEB, or TCB, if the applying transportation agency is not contacted by the Service with any questions or concerns within 14 calendar days of form submittal, it may proceed under the range-wide programmatic consultation and assume concurrence of the NLAA determination made by the Service in the PBO. For projects that may affect and are likely to adversely affect (LAA) the Indiana bat, NLEB, or TCB, the appropriate Service Field Office will respond¹ within 30 calendar days of receiving a complete project-level submission, which includes, but may not be limited to this completed form.

1. Date: 3/26/20

2. Lead agency: FHWA



*This refers to the **Federal** governmental lead action agency initiating consultation; select **FHWA**, **FRA**, **FTA**, or **FHWA/FRA Program Assignment State or Categorical Exclusion Assignment State** as appropriate.*

¹ Service Field Offices should use the LAA verification letter template for projects that may affect, and are likely to adversely affect the Indiana bat, NLEB, or TCB.

3. Requesting agency: Indiana Department of Transportation

This refers to the transportation agency completing the form (it may or may not be the same as the Lead Agency).

- Name: Jason Damm
- Title: Senior Project Manager
- Phone: 317.605.2392
- Email: jasond@metricenv.com

4. Project code:² 2025-0073896

5. Project name(s): Des 2200043, Intersection Project, SR 2 and CR 500W, LaPorte County, Indiana

6. Project description:

Please attach additional documentation or explanatory text if necessary.

7. Project location (county, state): LaPorte County, Indiana

If not delineated in IPaC, attach shape files.

8. For species **other than** Indiana bat, NLEB, and TCB (from IPaC official species list):

- ☒ No effect – project(s) are inside the species range, but no suitable habitat (see additional information attached).
- ☐ May affect – see additional information provided for those species (see attached or forthcoming).

² Available through IPaC System Official Species List: <https://ipac.ecosphere.fws.gov/>

Please confirm and identify how each component of the proposed project(s) adheres to the criteria of the PBO by completing the following (see User Guide Section 2.0):

NO EFFECT

9. For Indiana bat/NLEB/TCB, if applicable, select your no effect determination:

- ☐ No effect – project(s) are outside the species' range.
- ☐ No effect – project(s) are inside the species range with no suitable habitat³ within the project action area⁴; project(s) must also be greater than 0.5 miles from any hibernaculum.
- ☐ No effect – project(s) do not cause any stressors⁵ to the covered bat species, such as those that do not involve ground disturbance, vibrations, noise above background levels (including general traffic), temporary or new/additional permanent lighting, tree removal/trimming, nor bridge, culvert, and structure activities.
- ☐ No effect – project(s) includes percussive activities in suitable habitat (not related to tree removal and/or bridge, culvert, or structure work) that involve noise/vibration above existing background levels and are conducted greater than 0.5 miles (0.8 km) of a hibernaculum during the inactive season.
- ☐ No effect – project(s) includes the removal, replacement, or alteration of bridge, culvert, or structure that does not meet the minimum culvert dimensions (see the Service's current survey guidance).

Proceed with this form to identify how other components of the proposed project adhere to the criteria of the PBO.

MAY AFFECT, NOT LIKELY TO ADVERSELY EFFECT – W/O AMMS

10. For Indiana bat/NLEB/TCB, if applicable, select your may affect, NLAA determination (without implementation of AMMs):

- ☐ NLAA – project(s) are inside the species range and within suitable bat habitat, but have **negative** bat presence/absence (P/A) surveys; must also

³ Refer to the Service's Range-wide Bat Survey Guidelines at <https://www.fws.gov/library/collections/range-wide-indiana-bat-and-northern-long-eared-bat-survey-guidelines>.

⁴ The "action area" is defined as "all areas to be affected directly or indirectly by the Federal action and not merely the immediate area involved in the action." Further clarification is provided by the national consultation FAQs at: <https://www.fws.gov/glossary/action-area>.

⁵ Examples of activities that **do not cause stressors** may include striping roadways, unlighted road signage, railroad crossing signals, signal lighting, minor road repair such as asphalt fill of potholes, among others.

be greater than 0.5 miles from any hibernaculum.

- ☐ NLAA – project(s) include percussive activities within suitable habitat (not related to tree removal and/or bridge, culvert, structure work) that cause noise/vibration above existing background levels; and are conducted greater than 0.5 miles (0.8 km) of a hibernaculum; no further than 100 ft (30.5 m) of the road/rail surface during the pup season; and not carried out between December 15 and February 15 in Zone 1 of the NLEB and TCB YR active ranges.
- ☐ NLAA – project(s) includes the removal, replacement, or alteration of bridges, culverts, or structures with no signs of bat use [e.g., bats, guano], and does not impact suitable habitat within the project action area.

Proceed with this form to identify how other components of the proposed project adhere to the criteria of the PBO.

MAY EFFECT, NOT LIKELY TO ADVERSELY AFFECT – WITH AMMs

11. For Indiana bat/NLEB/TCB, if applicable, document your may affect, NLAA determination (**with implementation of AMMs**) by completing the following section; use #13 to document AMMs):

a. Tree Removal/Trimming Activities

- ☒ NLAA – project(s) includes the removal/trimming of trees outside documented habitat⁶ within 100 ft (30.5m) from the road/rail surface during the inactive season; and all applicable lighting minimization measures will be implemented.

b. Bridge/Culvert/Structure Activities

Projects Proposed work:

Timing of work:

- ☐ NLAA – project(s) includes the removal, replacement, or alteration of bridges, culverts, or structures with a large number of bats (>5) observed or assuming bat use when conducted during the inactive season, so long as no hibernating bats are using the bridge, culvert, or structure.

⁶ See glossary in Appendix A for definition of documented habitat.

- ☐ NLAA – project(s) includes the removal, replacement, or alteration of bridges, culverts, or structures with a large number of bats (>5) observed or assuming bat use when conducted during the active season, so long as the covered bats species are **not likely to be disturbed/killed** and suitable roosting habitat is still available within the bridge, culvert, or structure.
- ☐ NLAA – project(s) includes the removal, replacement, or alteration of bridges, culverts, or structures with a small number of bats (≤ 5) observed when conducted during the inactive season, so long as no hibernating bats are using the bridge, culvert, or structure.
- ☐ NLAA – project(s) includes the removal, replacement, or alteration of bridges, culverts, or structures with a small number of bats (≤ 5) observed when conducted during the active season, so long as the covered bats species are **not likely to be disturbed/killed**.

c. Lighting

- ☒ Verify that all applicable lighting minimization measures will be implemented.

Proceed with this form to identify how other components of the proposed project adhere to the criteria of the PBO.

MAY AFFECT, LIKELY TO ADVERSELY AFFECT

12. For Indiana bat/NLEB/TCB, if applicable, document your may affect, LAA determination (**with implementation of AMMs**) by completing the following section (use #13 to document AMMs):

a. Tree Removal/Trimming Activities

Tree Removal/Trimming Activities in the Hibernating Range of the Indiana bat, NLEB, and TCB.

- ☐ LAA – project(s) includes the removal/trimming of trees **outside documented habitat** for the Indiana bat or NLEB or TCB within 100 ft (30.5m) from the road/rail surface during the pup season; all cleared/trimmed trees must be <9 in DBH.
- ☐ LAA – project(s) includes the removal/trimming of trees **outside documented habitat** for the Indiana bat or NLEB or TCB during the active season; excluding the pup season.
- ☐ LAA – project(s) includes the removal/trimming of trees **outside documented habitat** for the Indiana bat or NLEB or TCB beyond 100 ft (30.5m) from the road/rail surface during the inactive season.
- ☐ LAA – project(s) includes the removal/trimming of trees **within documented habitat** for the Indiana bat or NLEB or TCB during the inactive season.
- ☐ LAA – project(s) includes the removal/trimming of trees **within documented habitat** for the Indiana bat or NLEB or TCB during the active season; excluding the pup season.

Tree Removal/Trimming Activities in the YR Active Ranges of the NLEB and TCB.

- ☐ LAA – project(s) includes the removal/trimming of trees **outside documented habitat** for the NLEB or TCB within 100 ft (30.5m) from the road/rail surface during the pup season; all cleared/trimmed trees must be <9 in DBH.
- ☐ LAA – project(s) includes the removal/trimming of trees **outside documented habitat** for the NLEB or TCB anytime, excluding the pup season and Dec 15 – Feb 15*.
- ☐ LAA – project(s) includes the removal/trimming of trees **within documented habitat** for the NLEB or TCB anytime, excluding the pup season and Dec 15 – Feb 15*.

*For the YR active ranges of the NLEB and TCB, winter tree clearing restrictions from Dec. 15 – Feb. 15 do not apply in areas where the mean minimum temperature is above 40° F throughout the winter months (depicted as Zone 2 in Figure 9 of the PBO):

- ☐ Number of acres of trees 0-100 feet of existing road/rail surface proposed for removal/trimming: [Click or tap here to enter text.](#)
- ☐ Number of acres of trees 100-300 feet from edge of existing road/rail surface proposed for removal/trimming. [Click or tap here to enter text.](#)
- ☐ Number of acres of trees beyond 300 feet from edge of existing road/rail surface proposed for removal/trimming. [Click or tap here to enter text.](#)
- ☐ Verify that all tree removal/trimming occurs greater than 0.5 mile from any hibernaculum
- ☐ Verify trees removed/trimmed outside documented habitat for the Indiana bat or NLEB or TCB within 100 feet of the road/rail surface during the pup season are <9 in DBH,
- ☐ Verify no tree removal/trimming outside documented habitat for the Indiana bat, NLEB, or TCB beyond 100 feet of the road/rail surface during the pup season,
- ☐ Verify no tree removal/trimming of documented habitat for the Indiana bat, NLEB, or TCB during the pup season,
- ☐ Verify no tree removal/trimming of suitable habitat for the NLEB and/or TCB in Zone 1 of their YR active ranges between December 15 – February 15.

b. Bridge/Culvert/Structure Activities
Projects Proposed work:

Timing of work:

Date of Bridge/Culvert/Structure Assessment (if completed):

- ☐ Verify a small number of bats were observed (≤ 5).
- ☐ LAA – project(s) includes the removal, replacement, or alteration of bridges, culverts, or structures with a small number of bats (≤ 5) observed when conducted during the active season and the covered bats species are **likely to be disturbed/killed**.

c. Lighting

- ☐ Verify that all applicable lighting minimization measures will be implemented.

13. For Indiana bat/NLEB/TCB, if applicable to the action type, the following AMMs⁷ will be implemented:

- ☒ General AMM 1 (required for all projects)
- ☒ Tree Removal AMM 1 (required for all projects)
- ☒ Tree Removal AMM 2 (required for all projects)
- ☒ Tree Removal AMM 3 (required for NLAA)
- ☐ Tree Removal AMM 4 (required for LAA)
- ☐ Tree Removal AMM 5 (required for LAA)
- ☐ Tree Removal AMM 6 (required for LAA)
- ☐ Tree Removal AMM 7 (required for LAA)
- ☐ Bridge/Culvert/Structure AMM 1a (required for NLAA)
- ☐ Bridge/Culvert/Structure AMM 1b (required for NLAA)
- ☐ Bridge/Culvert/Structure AMM 2 (required for NLAA)
- ☐ Bridge/Culvert/Structure AMM 3a (required for NLAA)
- ☐ Bridge/Culvert/Structure AMM 3b (required for NLAA)
- ☐ Bridge/Culvert/Structure AMM 4 (required for NLAA)
- ☐ Bridge/Culvert/Structure AMM 5 (required for all projects)
- ☐ Bridge/Culvert/Structure AMM 6 (required for NLAA)
- ☒ Lighting AMM 1 (required for all projects during the active season)
- ☒ Lighting AMM 2 (required for all projects)

⁷ See AMMs (Appendix C) for more information on AMMs.

☐ Hibernacula AMM 1 (required for all projects)

14. If applicable, compensatory mitigation measures will also be required to offset adverse effects to the Indiana bat and/or NLEB. Select what type of program will be used to mitigate for the Indiana bat and/or NLEB:

☐ In-Lieu Fee Program, The Conservation Fund

☐ State, Regional, Recovery Unit-Specific In-Lieu Fee Program:

Program Name:

☐ Conservation Bank:

Bank Name:

Location:

☒ Local Conservation Site:

Site(s) Name:

Location:

Description:



United States Department of the Interior

FISH AND WILDLIFE SERVICE
Indiana Ecological Services Field Office
620 South Walker Street
Bloomington, IN 47403-2121
Phone: (812) 334-4261 Fax: (812) 334-4273



In Reply Refer To:

06/29/2025 17:31:58 UTC

Project Code: 2025-0073896

Project Name: Des. 2200043, Intersection Project, SR 2 and CR 500W, LaPorte County

Subject: List of threatened and endangered species that may occur in your proposed project location or may be affected by your proposed project

To Whom It May Concern:

To Whom It May Concern:

The purpose of the Endangered Species Act (Act) of 1973, as amended (16 U.S.C. 1531 et seq.), is to provide a means whereby threatened and endangered species and the ecosystems upon which they depend may be conserved. Under sections 7(a)(1) and 7(a)(2) of the Act and its implementing regulations (50 CFR 402 et seq.), Federal agencies are required to utilize their authorities to carry out programs for the conservation of threatened and endangered species and to determine whether projects may affect threatened and endangered species and/or designated critical habitat.

The enclosed species list identifies threatened, endangered, proposed, and candidate species, as well as proposed and final designated critical habitat, that may occur within the boundary of your proposed project and may be affected by your proposed project. The species list fulfills the requirements of the U.S. Fish and Wildlife Service (Service) under section 7(c) of the Act.

New information based on updated surveys, changes in the abundance and distribution of species, changed habitat conditions, or other factors could change this list. Please note that under 50 CFR 402.12(e) of the regulations implementing section 7 of the Act, the accuracy of this species list should be verified after 90 days. You can complete this verification formally or informally or request an updated list by visiting the IPaC website at regular intervals during project planning and implementation.

Please use the species list provided and visit the U.S. Fish and Wildlife Service's Region 3 Section 7 Technical Assistance website at [Midwest Section 7 Technical Assistance | U.S. Fish & Wildlife Service](#). This website contains step-by-step instructions to help you determine if your project will have an adverse effect on listed species and to lead you through the Section 7 process.

We appreciate your concern for threatened and endangered species. Federal agencies should include conservation of threatened and endangered species into their project planning to further the purposes of the Act. **Please include the Consultation Code in the header of this letter with any request for consultation or correspondence about your project that you submit to our office.**

Other Trust Resources and Activities

Bald and Golden Eagles - Although the bald eagle has been removed from the endangered species list, this species and the golden eagle are protected by the Bald and Golden Eagle Act and the Migratory Bird Treaty Act. It is the responsibility of the project proponent to survey the area for any migratory bird nests. If there is an eagle nest on-site while work is on-going, eagles may be disturbed. We recommend avoiding and minimizing disturbance to eagles whenever practicable. If you cannot avoid eagle disturbance, you may seek a [permit](#). A [nest take permit](#) is always required for removal, relocation, or obstruction of an eagle nest. For communication and wind energy projects, please refer to additional guidelines below.

Migratory Birds - The Migratory Bird Treaty Act (MBTA) prohibits the taking, killing, possession, transportation, and importation of migratory birds, their eggs, parts, and nests, except when specifically authorized by the Service. The Service has the responsibility under the MBTA to proactively prevent the mortality of migratory birds whenever possible and we encourage implementation of [recommendations that minimize potential impacts to migratory birds](#). Such measures include clearing forested habitat outside the nesting season (generally March 1 to August 31) or conducting nest surveys prior to clearing to avoid injury to eggs or nestlings.

Communication Towers - Construction of new communications towers (including radio, television, cellular, and microwave) creates a potentially significant impact on migratory birds, especially some 350 species of night-migrating birds. However, the Service has developed [voluntary guidelines for minimizing impacts](#).

Transmission Lines - Migratory birds, especially large species with long wingspans, heavy bodies, and poor maneuverability can also collide with power lines. In addition, mortality can occur when birds, particularly hawks, eagles, kites, falcons, and owls, attempt to perch on uninsulated or unguarded power poles. To minimize these risks, please refer to [guidelines](#) developed by the Avian Power Line Interaction Committee and the Service. Implementation of these measures is especially important along sections of lines adjacent to wetlands or other areas that support large numbers of raptors and migratory birds.

Wind Energy - To minimize impacts to migratory birds and bats, wind energy projects should follow the Service's [Wind Energy Guidelines](#). In addition, please refer to the Service's [Eagle Conservation Plan Guidance](#), which provides guidance for conserving bald and golden eagles in the course of siting, constructing, and operating wind energy facilities.

Attachment(s):

- Official Species List

OFFICIAL SPECIES LIST

This list is provided pursuant to Section 7 of the Endangered Species Act, and fulfills the requirement for Federal agencies to "request of the Secretary of the Interior information whether any species which is listed or proposed to be listed may be present in the area of a proposed action".

This species list is provided by:

Indiana Ecological Services Field Office

620 South Walker Street

Bloomington, IN 47403-2121

(812) 334-4261

PROJECT SUMMARY

Project Code: 2025-0073896
Project Name: Des. 2200043, Intersection Project, SR 2 and CR 500W, LaPorte County
Project Type: Road/Hwy - Maintenance/Modification
Project Description: The Indiana Department of Transportation, with oversight and partial funding from the Federal Highway Administration, proposes to proceed with an intersection improvement project located at State Road (SR) 2 and County Road (CR) 500 W in LaPorte County, Indiana (Des. No. 2200043).

The proposed preferred alternative consists of constructing a single-lane roundabout at the intersection.

The need for the project is a result of the higher-than-expected crash frequency. Currently, the intersection is stop-controlled with signage at the CR 500 W approaches with SR 2 traffic having the right-of-way. In addition, the traffic mobility observed at the CR 500 W approaches are less than desirable and are considered inadequate for current and future projected traffic growth. The purpose of the project is to address the high crash frequency and less than desirable traffic mobility.

The project design includes enclosed drainage which will outlet into a detention basin to be constructed in the southwest quadrant of the intersection. Maintenance of Traffic is expected to include phased lane closures and a temporary detour. Traffic will be maintained on each roadway using alternating lane closures as allowable; however, a temporary detour will likely be necessary to construct the roundabout. Approximately 2.02 acres of new permanent right-of-way will be required to build the project. In addition, approximately 0.18 acre of temporary right-of-way will be required for construction access, roadside ditch/side slope re-grading and driveway reconstruction.

There is suitable summer habitat located within the project area. Fifteen (15) trees totaling 0.00139 acre will be removed as part of this project (see attached tree removal document for details). All tree impacts will occur during the inactive season for bats. No mitigation is anticipated. Temporary lighting may be required during construction. New overhead lighting will be installed for approximately 400 feet along each leg of the roundabout per design requirements.

Based on consultation with INDOT LaPorte District, a January 3, 2024, review of the U.S. Fish and Wildlife Service database did not indicate the presence of endangered bat species in or within 0.5 mile of the project area.

The project letting is scheduled for summer 2027 and the project should be completed by spring 2028.

Project Location:

The approximate location of the project can be viewed in Google Maps: <https://www.google.com/maps/@41.5915142,-86.79795369062863,14z>



Counties: LaPorte County, Indiana

ENDANGERED SPECIES ACT SPECIES

There is a total of 4 threatened, endangered, or candidate species on this species list.

Species on this list should be considered in an effects analysis for your project and could include species that exist in another geographic area. For example, certain fish may appear on the species list because a project could affect downstream species.

IPaC does not display listed species or critical habitats under the sole jurisdiction of NOAA Fisheries¹, as USFWS does not have the authority to speak on behalf of NOAA and the Department of Commerce.

See the "Critical habitats" section below for those critical habitats that lie wholly or partially within your project area under this office's jurisdiction. Please contact the designated FWS office if you have questions.

-
1. [NOAA Fisheries](#), also known as the National Marine Fisheries Service (NMFS), is an office of the National Oceanic and Atmospheric Administration within the Department of Commerce.

MAMMALS

| NAME | STATUS |
|--|------------|
| Indiana Bat <i>Myotis sodalis</i> There is final critical habitat for this species. Your location does not overlap the critical habitat. Species profile: https://ecos.fws.gov/ecp/species/5949 | Endangered |

BIRDS

| NAME | STATUS |
|--|--|
| Whooping Crane <i>Grus americana</i> Population: U.S.A. (AL, AR, CO, FL, GA, ID, IL, IN, IA, KY, LA, MI, MN, MS, MO, NC, NM, OH, SC, TN, UT, VA, WI, WV, western half of WY) No critical habitat has been designated for this species. Species profile: https://ecos.fws.gov/ecp/species/758 | Experimental Population, Non- Essential |

CLAMS

| NAME | STATUS |
|--|------------------------|
| Salamander Mussel <i>Simpsonaias ambigua</i> There is proposed critical habitat for this species. Your location does not overlap the critical habitat. Species profile: https://ecos.fws.gov/ecp/species/6208 | Proposed Endangered |

INSECTS

| NAME | STATUS |
|---|------------------------|
| Monarch Butterfly <i>Danaus plexippus</i> There is proposed critical habitat for this species. Your location does not overlap the critical habitat. Species profile: https://ecos.fws.gov/ecp/species/9743 | Proposed Threatened |

CRITICAL HABITATS

THERE ARE NO CRITICAL HABITATS WITHIN YOUR PROJECT AREA UNDER THIS OFFICE'S JURISDICTION.

YOU ARE STILL REQUIRED TO DETERMINE IF YOUR PROJECT(S) MAY HAVE EFFECTS ON ALL ABOVE LISTED SPECIES.

IPAC USER CONTACT INFORMATION

Agency: Metric Environmental, LLC

Name: Susan Castle

Address: 6958 Hillsdale Court

City: Indianapolis

State: IN

Zip: 46250

Email: susanc@metricenv.com

Phone: 3176082730

LEAD AGENCY CONTACT INFORMATION

Lead Agency: Federal Highway Administration

From: [McWilliams, Robin](#)
To: [Wahl, Cassie](#)
Cc: [Michels, Stewart](#)
Subject: Re: [EXTERNAL] Des 2200043 NLAA Determination Review Request
Date: Thursday, March 27, 2025 4:02:23 PM
Attachments: [image001.png](#)

EXTERNAL EMAIL: This email was sent from outside your organization. Exercise caution when clicking links, opening attachments or taking further action, before validating its authenticity.

Thanks Cassie.

Your project may proceed under the range-wide programmatic consultation and assume concurrence of the NLAA determination(s) made by the Service in the 2024 PBO. If you have any other questions or concerns, please let me know.

Sincerely,
Robin

Robin McWilliams Munson
Fish and Wildlife Biologist/Transportation Liaison
U.S. Fish and Wildlife Service
Indiana Ecological Services Field Office
620 South Walker Street
Bloomington, IN 47403
Robin_McWilliams@fws.gov
***NEW* 812-902-1752**

Mon-Thurs 8:30-4:30p

From: Wahl, Cassie <CWahl@indot.IN.gov>
Sent: Wednesday, March 26, 2025 1:39 PM
To: McWilliams, Robin <robin_mcwiliams@fws.gov>
Cc: Michels, Stewart <SMichels@indot.IN.gov>
Subject: [EXTERNAL] Des 2200043 NLAA Determination Review Request

This email has been received from outside of DOI - Use caution before clicking on links, opening attachments, or responding.

Hello Robin,

Please see the attached submittal form and species list for Des 2200043 in LaPorte County. The consultant made a determination of NLAA. Please let me know if you need anything else from me or if corrections need to be made.

Thank you,

Cassie Wahl

Environmental Manager

Indiana Department of Transportation

LaPorte District

Cell: (219) 809-7566

Email: cwahl@indot.in.gov

[Find us on social media!](#)





Organization and Project Information

Organization Name: Metric Environmental, LLC.

First Name: Joshua

Last Name: Netherton

Phone: (765) 810-3867

Email: joshuan@metricenv.com

Address Line 1: 6958 Hillsdale Court

City: Indianapolis

State: IN

Zip: 46250

Customer Id: 22-0216-1

Destination Id: 2200043

Project Title: Roundabout Intersection Improvement on S.R. 2 at CR 500 W, 3.27 Miles West

Project Description: Roundabout Intersection Improvement on S.R. 2 at CR 500 W, 3.27 Miles West of S.R. 39

Environmental Assessment Report

Geological Hazards:

1. Moderate liquefaction potential

Mineral Resources:

1. Sand and Gravel Resource: High Potential

Disclaimer:

This document was compiled by Indiana University, Indiana Geological Survey, using data believed to be accurate; however, a degree of error is inherent in all data. This product is distributed "AS-IS" without warranties of any kind, either expressed or implied, including but not limited to warranties of suitability to a particular purpose or use. No attempt has been made in either the design or production of these data and document to define the limits or jurisdiction of any federal, state, or local government. The data used to assemble this document are intended for use only at the published scale of the source data or smaller (see the metadata links below) and are for reference purposes only. They are not to be construed as a legal document or survey instrument. A detailed on-the-ground survey and historical analysis of a single site may differ from these data and this document.

This information was furnished by Indiana Geological Survey

Address: 1001 E. 10th St., Bloomington, IN 47405

Email: IGSEnvir@indiana.edu

Phone: (812) 855-7428





Metadata:

https://portal.igs.indiana.edu/arcgis/rest/services/Seismic_Earthquake_Liquefaction_Potential/MapServer/info/metadata/

https://portal.igs.indiana.edu/arcgis/rest/services/Industrial_Minerals_SandAndGravel_Resources/MapServer/info/metadata/

May 13, 2025

Samantha Wickizer
Metric Environmental
6958 Hillsdale Court
Indianapolis, Indiana 46250

Dear Samantha Wickizer:

The Intersection Improvement SR 2 CR 500 W project in LaPorte County, Indiana, (Des. No. 2200043) as referred to in your letter received on March 7, 2025, will cause a conversion of prime farmland.

'March' is incorrect the letter
was sent on May 7

The attached packet of information is for your use competing Parts VI and VII of the AD-1006. After completion, the federal funding agency needs to forward one copy to NRCS for our records.

If you need additional information, please contact John Allen at 317-295-5859 or john.allen@usda.gov.

Sincerely,

JOHN ALLEN

JOHN ALLEN
State Soil Scientist

Digitally signed by JOHN ALLEN
Date: 2025.05.13 14:15:23 -04'00'

Enclosers

FARMLAND CONVERSION IMPACT RATING

| | | | | | | | | | |
|--|--|--|--|--|---|--|-----------------|------------------------------------|---------------------------------|
| PART I <i>(To be completed by Federal Agency)</i> | | | | | Date Of Land Evaluation Request 5/12/2025 | | | | |
| Name of Project DES2200043 Intersect Improv SR 2_CRS | | | | | Federal Agency Involved | | | | |
| Proposed Land Use | | | | | County and State LaPorte County, IN | | | | |
| PART II <i>(To be completed by NRCS)</i> | | | | | Date Request Received By NRCS | | | Person Completing Form: JRA | |
| Does the site contain Prime, Unique, Statewide or Local Important Farmland? <i>(If no, the FPPA does not apply - do not complete additional parts of this form)</i> | | | | | YES <input checked="" type="checkbox"/> NO <input type="checkbox"/> | | Acres Irrigated | | Average Farm Size 410 ac |
| Major Crop(s) Corn | | | Farmable Land In Govt. Jurisdiction Acres: 351560 % 91 | | | Amount of Farmland As Defined in FPPA Acres: 28329 % 73 | | | |
| Name of Land Evaluation System Used LESA | | | Name of State or Local Site Assessment System | | | Date Land Evaluation Returned by NRCS 5/13/2025 | | | |
| PART III <i>(To be completed by Federal Agency)</i> | | | | | Alternative Site Rating | | | | |
| | | | | | Site A | | Site B | | Site C |
| A. Total Acres To Be Converted Directly | | | | | 2.22 | | | | |
| B. Total Acres To Be Converted Indirectly | | | | | | | | | |
| C. Total Acres In Site | | | | | 2.22 | | | | |
| PART IV <i>(To be completed by NRCS) Land Evaluation Information</i> | | | | | | | | | |
| A. Total Acres Prime And Unique Farmland | | | | | 0.41 | | | | |
| B. Total Acres Statewide Important or Local Important Farmland | | | | | 0.00 | | | | |
| C. Percentage Of Farmland in County Or Local Govt. Unit To Be Converted | | | | | <0.001 | | | | |
| D. Percentage Of Farmland in Govt. Jurisdiction With Same Or Higher Relative Value | | | | | 67 | | | | |
| PART V <i>(To be completed by NRCS) Land Evaluation Criterion Relative Value of Farmland To Be Converted (Scale of 0 to 100 Points)</i> | | | | | 70 | | | | |
| PART VI <i>(To be completed by Federal Agency) Site Assessment Criteria (Criteria are explained in 7 CFR 658.5 b. For Corridor project use form NRCS-CPA-106)</i> | | | | | Maximum Points | Site A | Site B | Site C | Site D |
| 1. Area In Non-urban Use | | | | | (15) | 5 | | | |
| 2. Perimeter In Non-urban Use | | | | | (10) | 5 | | | |
| 3. Percent Of Site Being Farmed | | | | | (20) | 0 | | | |
| 4. Protection Provided By State and Local Government | | | | | (20) | 10 | | | |
| 5. Distance From Urban Built-up Area | | | | | (15) | 5 | | | |
| 6. Distance To Urban Support Services | | | | | (15) | 5 | | | |
| 7. Size Of Present Farm Unit Compared To Average | | | | | (10) | 5 | | | |
| 8. Creation Of Non-farmable Farmland | | | | | (10) | 0 | | | |
| 9. Availability Of Farm Support Services | | | | | (5) | 1 | | | |
| 10. On-Farm Investments | | | | | (20) | 0 | | | |
| 11. Effects Of Conversion On Farm Support Services | | | | | (10) | 0 | | | |
| 12. Compatibility With Existing Agricultural Use | | | | | (10) | 5 | | | |
| TOTAL SITE ASSESSMENT POINTS | | | | | 160 | 41 | 0 | 0 | 0 |
| PART VII <i>(To be completed by Federal Agency)</i> | | | | | | | | | |
| Relative Value Of Farmland <i>(From Part V)</i> | | | | | 100 | 70 | 0 | 0 | 0 |
| Total Site Assessment <i>(From Part VI above or local site assessment)</i> | | | | | 160 | 41 | 0 | 0 | 0 |
| TOTAL POINTS (Total of above 2 lines) | | | | | 260 | 111 | 0 | 0 | 0 |
| Site Selected: A | | | Date Of Selection | | | Was A Local Site Assessment Used? YES <input type="checkbox"/> NO <input checked="" type="checkbox"/> | | | |
| Reason For Selection: Intersection Improvement | | | | | | | | | |
| Name of Federal agency representative completing this form: Metric Environmental, LLC | | | | | | | | Date: 7/2/2025 | |

(See Instructions on reverse side)

Form AD-1006 (03-02)

From: [Joshua Netherton](#)
To: [Elayna Stoner](#)
Subject: FW: Early Coordination Letter - (Des. No. 2200043) SR 2 and CR 500 West Intersection Improvement Project, 3.27 Miles West of SR 39, LaPorte County
Date: Monday, April 1, 2024 4:04:18 PM
Attachments: [image002.png](#)
[image003.png](#)



Josh Netherton

Assistant Project Manager

NEPA Compliance

O 317.400.1633 ext.157
M 317.981.3093

6958 Hillsdale Court
Indianapolis, IN 46250
www.metricenv.com

Certified DBE/MBE/SBE Company
Indiana | Kentucky | Ohio | West Virginia

From: Lewandowski, Tyler <TLewandowski@indot.IN.gov>
Sent: Monday, April 1, 2024 1:58 PM
To: Joshua Netherton <joshuan@metricenv.com>
Subject: RE: Early Coordination Letter - (Des. No. 2200043) SR 2 and CR 500 West Intersection Improvement Project, 3.27 Miles West of SR 39, LaPorte County

Good afternoon,

After review, no tall structure permit is required for the project if all equipment being used is under 147 feet in height. Please let our office know if you have any further questions.

Thank you,

Tyler Lewandowski
Project Manager
INDOT Office of Aviation
(317) 495-4875
tlewandowski@indot.in.gov
www.aviation.indot.in.gov



APPENDIX D
**Section 106 of the National Historic
Preservation Act**

Division of Historic Preservation & Archaeology · 402 W. Washington Street, W274 · Indianapolis, IN 46204-2739
Phone 317-232-1646 · Fax 317-232-0693 · dhpa@dnr.IN.gov

July 30, 2025

Zoe Vorndran
Metric Environmental, LLC
6958 Hillsdale Court
Indianapolis, Indiana 46250

Federal Agency: Indiana Department of Transportation (“INDOT”),
on behalf of Federal Highway Administration, Indiana Division (“FHWA”)

Re: Indiana Department of Transportation’s finding of “no historic properties affected.” on behalf of
the Federal Highway Administration. for the State Route 2 and County Road 500 West
Intersection Improvement Project (Des. No. 2200043; DHPA No. 31664)

Dear Ms. Vorndran:

Pursuant to Section 106 of the National Historic Preservation Act of 1966, as amended (54 U.S.C. § 306108), 36 C.F.R. Part 800, and the “Programmatic Agreement (PA) Among the Federal Highway Administration, the Indiana Department of Transportation, the Advisory Council on Historic Preservation and the Indiana State Historic Preservation Officer Regarding the Implementation of the Federal Aid Highway Program In the State of Indiana,” the staff of the Indiana State Historic Preservation Officer (“Indiana SHPO”) has reviewed your July 3, 2025, submission which enclosed INDOT’s finding and supporting documentation, and which was received by our office July 3, 2025, for the above-indicated project, which is proposed for areas located at Scipio Township, in LaPorte County, Indiana.

As we previously stated, the area of potential effects (“APE”) proposed in the HPSR appears to be of adequate size to encompass the geographic area in which direct and indirect effects of a project of this nature could occur.

Additionally, our previous comment was, for the purposes of the Section 106 review of this federal undertaking, we agree with the conclusions in the HPSR that there are no previously recorded National Register of Historic Places (“NRHP”) listed or eligible resources located within the APE. Furthermore, we also agree with the conclusions in the HPSR of the Contributing-rated status of the other historic-aged resources documented during survey and that there are no resources listed or eligible for the NRHP within the APE.

In terms of impacts of proposed project-related ground-disturbing activities to archaeological resources, as previously indicated, based on the submitted information and the documentation available to the staff of the Indiana DNR-DHPA, we have not identified any currently known archaeological resources listed in or eligible for inclusion in the NRHP within the proposed project area; and we concur with the opinion of the archaeologist, as expressed in the Indiana Archaeological Short Report (Peters et al., 03/12/2025), that no further archaeological investigations appear necessary at the proposed project area.

If any prehistoric or historic archaeological artifacts or human remains are uncovered during construction, demolition, or earthmoving activities, state law (Indiana Code 14-21-1-27 and Indiana Code 14-21-1-29) requires that the discovery be reported to the Indiana Department of Natural Resources, Division of Historic Preservation and Archaeology within two (2) business days. In that event, please call (317) 232-1646. Be advised that adherence to Indiana Code 14-21-1-27 and Indiana Code 14-21-1-29 does not obviate the need to adhere to applicable federal statutes and regulations, including but not limited to 36 C.F.R. Part 800.

Accordingly, we concur with INDOT’s July 2, 2025, Section 106 finding of “No Historic Properties Affected” on behalf of FHWA for this federal undertaking.

The Indiana SHPO staff's archaeological reviewer for this project is Wade T. Tharp, and the structures reviewer is Toni Lynn Giffin. However, if you have a question about the Section 106 process, please contact initially the INDOT Cultural Resources staff members who are assigned to this project.

In all future correspondence about the State Road 2 and County Road 500 West Intersection Improvement Project (Des. No. 2200043), which is proposed for areas located at Scipio Township, in LaPorte County, Indiana, please refer to DHPA No. 31664.

Very truly yours,



Beth K. McCord
Deputy State Historic Preservation Officer

BKM:TLG:WTT:wt

emc: Erica Tait, Federal Highway Administration
Matt Coon, Ph.D., Indiana Department of Transportation
Susan Branigin, Indiana Department of Transportation
Clint Kelly, Indiana Department of Transportation
Zoe Vorndran, Metric Environmental, LLC
Toni Lynn Giffin, Indiana DNR-DHPA
Wade T. Tharp, Indiana DNR-DHPA

**FEDERAL HIGHWAY ADMINISTRATION'S
SECTION 4(F) COMPLIANCE REQUIREMENTS (for historic properties) AND
SECTION 106 FINDINGS AND DETERMINATIONS
AREA OF POTENTIAL EFFECT
ELIGIBILITY DETERMINATIONS
EFFECT FINDING**

**STATE ROAD 2 AND COUNTY ROAD 500 WEST INTERSECTION IMPROVEMENT PROJECT
DES. NO.: 2200043; DHPA NO. 31664**

AREA OF POTENTIAL EFFECTS

(Pursuant to 36 CFR Section 800.4(a)(1))

The APE for this project extends at least 500 feet beyond the project end points, and at least one property deep on each side of State Road (SR) 2 and County Road (CR) 500 West (W). The APE was determined based upon the anticipated amount of ground disturbance, the sightlines to the project area, potential direct audio and visual impacts within the project's setting, and the scale of the project. The archaeological APE is the project footprint including all proposed right-of-way, which was defined as encompassing 3.9 hectares (9.64 acres). Refer to Appendix A: Figures 1 and 2 for maps of the project area and the APE.

ELIGIBILITY DETERMINATIONS

(Pursuant to 36 CFR 800.4(c)(2))

The APE contains no properties listed, or eligible for listing, in the National Register of Historic Places (NRHP).

As a result of this investigation no properties were found to be eligible for listing in the NRHP.

EFFECT FINDING

INDOT, acting on FHWA's behalf, has determined a "No Historic Properties Affected" finding is appropriate for this undertaking. INDOT respectfully requests the Indiana State Historic Preservation Officer provide written concurrence with the Section 106 determination of effect.

SECTION 4(F) COMPLIANCE REQUIREMENTS (for historic properties)

This undertaking will not convert property from any Section 4(f) historic property to a transportation use; the INDOT, acting on FHWA's behalf, has determined the appropriate Section 106 finding is "No Historic Properties Affected"; therefore no Section 4(f) evaluation is required.

Susan R. Branigin for

Digitally signed by Susan R.
Branigin for
Date: 2025.07.02 14:34:14 -04'00'

Matthew S. Coon, for FHWA
Manager
INDOT Cultural Resources

Approved Date

**FEDERAL HIGHWAY ADMINISTRATION
DOCUMENTATION OF SECTION 106 FINDING OF
NO HISTORIC PROPERTIES AFFECTED
SUBMITTED TO THE STATE HISTORIC PRESERVATION OFFICER
PURSUANT TO 36 CFR Section 800.4(d)(1)
STATE ROAD 2 AND COUNTY ROAD 500 WEST INTERSECTION IMPROVEMENT PROJECT
DES. NO.: 2200043; DHPA NO. 31664**

1. DESCRIPTION OF THE UNDERTAKING

The Indiana Department of Transportation (INDOT), with funding from the Federal Highway Administration (FHWA), proposes to proceed with the State Road (SR) 2 and County Road (CR) 500 West (W) Intersection Improvement Project, Des. No. 2200043. The proposed undertaking is located at the intersection of SR 2 and CR 500 W in LaPorte County, Indiana, southwest of the City of La Porte between United States Highway (US) 421 and US 35. It is within Scipio Township, on the LaPorte West, IN, USGS Topographic Quadrangle, in Sections 5, 6, 7, 8, Township 36 North, Range 3 West. Refer to Appendix A for maps of the project location.

State Road 2 and CR 500 W are both two-lane roadways. State Road 2 is classified as a Rural Principal Arterial Other – a rural route intended to accommodate high volumes of traffic for greater distances – with 12-foot lanes, narrow shoulders, and right turn lanes onto CR 500 W at the intersection. County Road 500 W is classified as a Rural Minor Collector – a roadway intended to connect local and rural roadways and to provide service to smaller communities – with 11-foot lanes and no shoulders in the project area. The intersection is controlled with stop signs on S CR 500 W.

The need for this project is evidenced by the high frequency of car crashes at this intersection. A total of 22 crashes occurred at the intersection during the study period 2017 to 2019, the majority being right- and left-hand turns. A RoadHAT crash analysis (traffic engineering safety modeling software) resulted in an Index of Crash Frequency of 2.96. The Index of Crash Frequency (ICF) compares the crash frequency data of this intersection with intersections of similar volumes, roadway classification, and control type throughout Indiana. Crash data for this intersection resulted in an ICF of 2.96, putting it in the 99th percentile compared with similar intersections, which indicates the existing stop signs on CR 500 W are not safely controlling traffic at this intersection. Additionally, southbound CR 500 W experiences a high volume of traffic and its Level of Service (LOS) is expected to reach F (breakdown flow) prior to the design year. The LOS is on a scale from A (free flow) to F (breakdown flow). The purpose of this project is to improve safety of the SR 2 and CR 500 W intersection and provide a long-term solution that ensures safe operation and reduced congestion at this intersection.

The proposed preferred alternative will modify the existing intersection by constructing a single-lane roundabout with a truck apron. The roundabout's location will be closely centered at the existing intersection to avoid property impacts and traffic volume. Along SR 2, the approach work is estimated to begin approximately 1,640 feet west of CR 500 W and terminate approximately 760 feet east of CR 500 W. Along CR 500 W, the approach work is estimated to begin approximately 300 feet north of SR 2 and

terminate approximately 435 feet south of SR 2. Curb and gutter will be installed within the limits of the roundabout to drain excess runoff into a new detention basin to be constructed in the southwest quadrant of the roundabout. Ancillary work involves removing existing trees along the roadway, relocating overhead utilities, lighting installation, and reconstructing a driveway within the project area.

No properties will be relocated as a result of this undertaking. The project will require 2.020 acres of newly acquired permanent right-of-way and 3.619 acres of reacquired permanent right-of-way, amounting to a total acquisition of 5.639 acres of permanent right-of-way, as well as 0.119 acre of temporary right-of-way. During construction, traffic will be maintained using lane closures and a detour.

The APE is the “geographic area or areas within which an undertaking may directly or indirectly cause alterations in the character or use of historic properties, if any such properties exist. The area of potential effects is influenced by the scale and nature of an undertaking and may be different for different kinds of effects caused by the undertaking” (36 CFR 800.16(d)). The APE includes all locations where the project may result in disturbance of the ground; all locations from which elements of the project may be visible or audible; all locations where activity may result in changes in traffic patterns, land use, or public access. For above-ground structures, the APE was defined as extending at least 500 feet beyond the project end points, and at least one property deep on each side of SR 2 and CR 500 W. The archaeological APE was the project footprint, including all proposed right-of-way, which was defined as encompassing 3.9 hectares (9.64 acres).

Please refer to Appendix A for maps of the project area and the APE, Appendix B for project site photographs, and Appendix F for project plans.

2. EFFORTS TO IDENTIFY HISTORIC PROPERTIES

Per the National Historic Preservation Act of 1966, the Indiana State Historic Preservation Officer (SHPO) is automatically invited to participate in the Section 106 process as a consulting party. An early coordination letter was sent to the following other potential consulting parties by letter or email dated December 8, 2023 (refer to Appendix E, 33-40).

Indiana State Historic Preservation Office
LaPorte County Historian
LaPorte County Genealogical Society
LaPorte County Historical Society
Westville Community Historical Society
Indiana Landmarks – Northern Regional Office
Indiana Lincoln Highway Association, Inc.
Preserve Historic La Porte
Northwestern Indiana Regional Planning Commission
LaPorte County Commissioners
LaPorte County Highway Superintendent
LaPorte County Highway Engineer

Eastern Shawnee Tribe of Oklahoma
Miami Tribe of Oklahoma
Peoria Tribe of Indians of Oklahoma
Pokagon Band of Potawatomi Indians
Shawnee Tribe
Forest County Potawatomi County

In an email dated December 11, 2023, the Forest County Potawatomi Community (FCPC) accepted the invitation to serve as a consulting party and offered a finding of No Historic Properties affected of significance to the FCPC. They issued their “standard caveat sent with each proposed project reviewed by the FCPC THPO” which requested that work cease immediately and to be consulted if an inadvertent discovery occurred at any phase of the project (refer to Appendix E, 41).

In an email dated December 20, 2023, Shawnee Tribe accepted the invitation to serve as a consulting party and concurred “that no known historic properties will be negatively impacted by this project.” They requested immediate consultation “in the event archaeological materials are encountered during construction, use, or maintenance of this location” (refer to Appendix E, 42).

In a letter dated January 4, 2024, the SHPO acknowledged receipt of the early coordination letter and noted they were not aware of any parties who should be invited to participate, “beyond those whom INDOT already has invited” (refer to Appendix E, 43-44).

No other replies were received in response to the early coordination letter.

Efforts to identify historic properties in the APE included communication with consulting parties and a literature review of the following:

- the 1989 *LaPorte County Interim Report: Indiana Historic Sites and Structures Inventory* (IHSSI; Historic Landmarks Foundation of Indiana)
- the 2009 INDOT-sponsored Indiana Historic Bridge Inventory (IHBI; Mead & Hunt)
- the NRHP Database
- the Indiana State Historic Architectural and Archaeological Research Database (SHAARD), which includes resources listed in the Indiana Register of Historic Sites and Structures (State Register)
- the Indiana Historic Buildings, Bridges, and Cemeteries Map (IHBBCM)
- the Indiana Department of Transportation Cultural Resources Office’s Public Web Map App¹

¹ These databases are accessible at the following:

for the NRHP database, <https://www.nps.gov/subjects/nationalregister/database-research.htm>;

for SHAARD, <https://secure.in.gov/apps/dnr/shaard/welcome.html>;

for the IHBBCM, <https://www.arcgis.com/home/item.html?id=1593429c17c34942a0d1d3fac03c4a80>;

for the INDOT-CRO Public Web Map App,

<https://www.arcgis.com/apps/webappviewer/index.html?id=77a6d13b2a9f4785ab3139daf287ddac&extent=-9897967.2983%2C4665077.6793%2C-9338448.2513%2C4998343.1226%2C102100>.

- historical maps and aerial photographs
- archaeological site maps

The literature review determined there are no NRHP-listed properties within the APE. No previously inventoried archaeological sites are located within the project area.

The results of field surveys were reported in a Historic Property Short Report (HPSR) (Vorndran/Receveur, 9/10/2024) and an Archaeological Short Report (ASR) (Snell, 3/12/2025). The principal investigators for these reports meet the Secretary of the Interior's Professional Qualification Standards as per 36 CFR Part 61. The HPSR recommended no resources within the APE as eligible for listing in the NRHP. The ASR found no sites and recommended the project be allowed to proceed as planned.

A letter distributed on September 10, 2024, notified consulting parties that an HPSR was available for review and comment via INDOT's Section 106 document posting website IN SCOPE at <http://erms12c.indot.in.gov/Section106Documents> (refer to Appendix E, 45-51). The full HPSR document may also be downloaded from IN SCOPE (the Des. No. is the most efficient search term, once in IN SCOPE). A copy of the management summary and conclusion for the HPSR is included in Appendix C.

On February 12, 2024, INDOT-CRO announced, effective immediately, that the Turtle Mountain Band of Chippewa Indians will be added as Tribal Consulting Parties for projects in all Indiana counties, and they were invited to serve as a consulting party as part of the distribution of the HPSR for this undertaking.

In an email dated September 11, 2024, the Turtle Mountain Band of Chippewa Indians stated, "MBCI THPO office defer to the nearest THPO/SHPO office to the APE" (refer to Appendix E, 52).

In a letter dated October 10, 2024, the SHPO acknowledged receipt of the HPSR. The SHPO concurred that the APE proposed in the HPSR appears to be of adequate size to encompass the effects of the undertaking. The SHPO also agreed with the conclusions in the HPSR that there are no properties that have been listed, or are eligible for listing, in the NRHP (refer to Appendix E, 53).

In an email dated November 12, 2024, the Shawnee Tribe concurred "that no known historic properties will be negatively impacted by this project," though they noted that "there is still potential for the discovery of unknown resources" (refer to Appendix E, 54).

The ASR was distributed without a distribution letter on March 12, 2025 only to SHPO and Tribal consulting parties. The results and recommendations for the ASR are included in Appendix D.

In a letter dated April 11, 2025, the SHPO acknowledged receipt of the ASR (refer to Appendix E, 55). The SHPO concurred with the ASR that no further archaeological work is necessary for this project.

No other replies were received in response to the HPSR and ASR. Copies of consulting parties' responses are located in Appendix E.

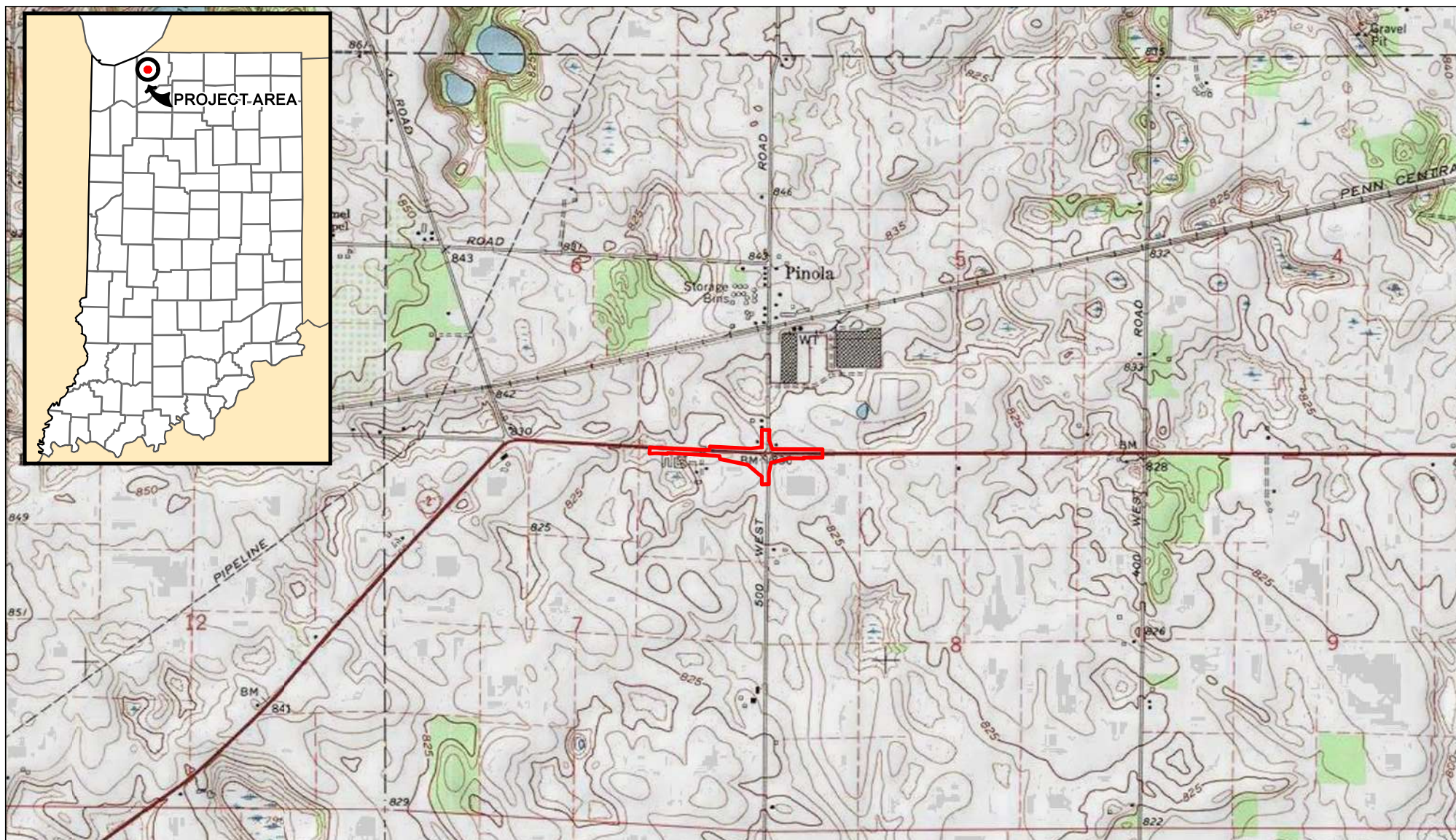
3. BASIS FOR FINDING

A finding of “No Historic Properties Affected” is appropriate for this undertaking because no historic properties are present within the APE. INDOT’s Finding, made on behalf of the FHWA, and supporting 800.11(d) documentation are hereby provided to the SHPO for a final 30-day comment period. A public notice will be published in the *La Porte County Herald-Dispatch* newspaper and the public will be afforded thirty (30) days to respond to INDOT’s “No Historic Properties Affected” finding. If appropriate, this document will be revised after the expiration of the public comment period to include any substantive comments received.

APPENDIX

- A. Project Location and APE Maps
- B. Project Site Photographs and Key Maps
- C. Historic Property Short Report Management Summary and Conclusions
- D. Archaeological Short Report Results and Recommendations
- E. Consulting Parties’ List and Correspondence
- F. Project Plans
- G. Affidavit of Publication and Public Notice

Appendix A. Project Location and APE Maps



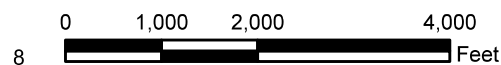
— Project Area

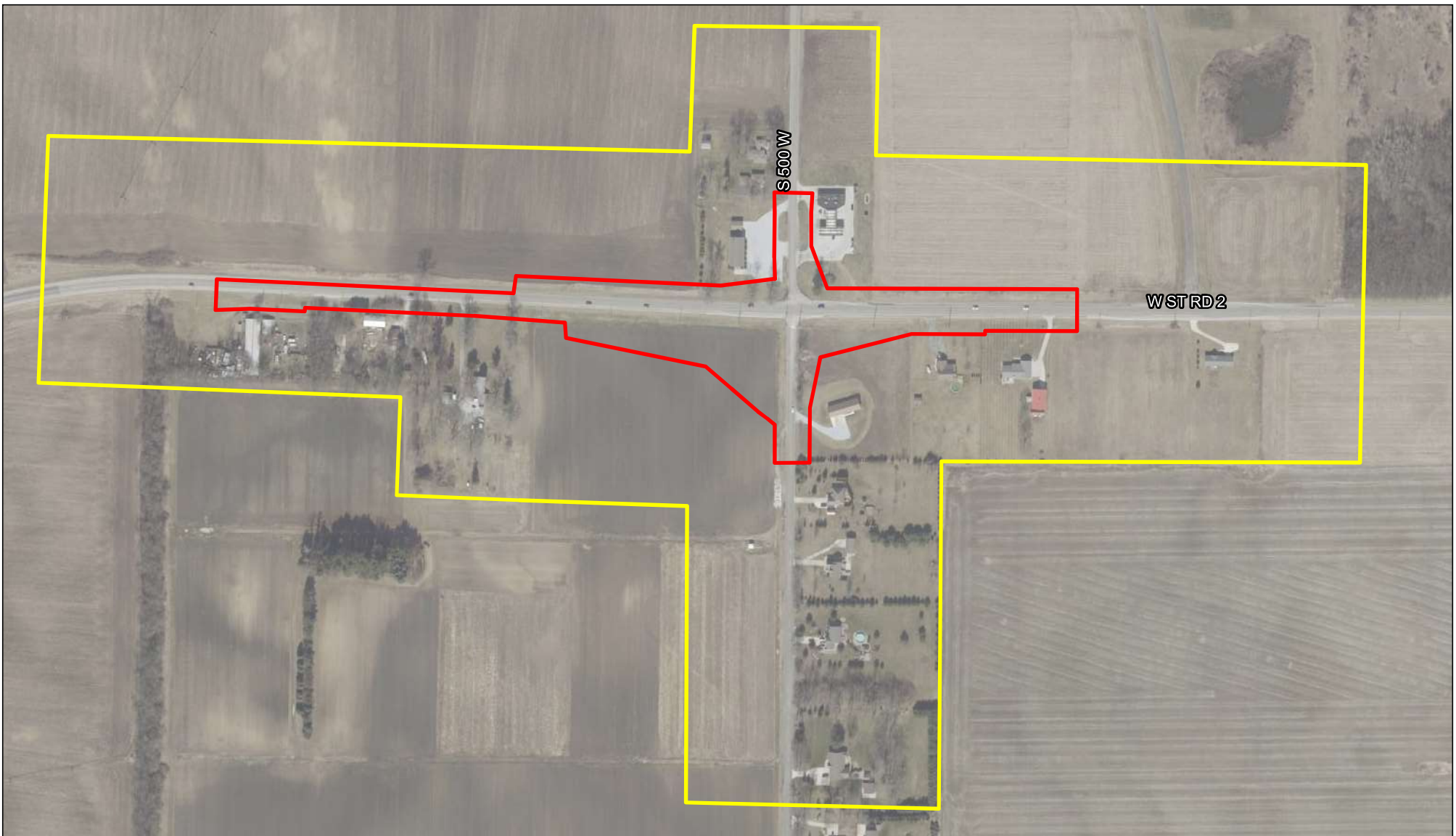
Figure 1. Project Area on a Portion of the 1969
LaPorte West, IN, 7.5-minute USGS Topographic Quadrangle
in LaPorte County, Indiana
SR 2 and CR 500 W Intersection Improvement Project
Scipio Township, LaPorte County, IN
Des. No. 2200043
Metric Project No. 22-0216-1
Map Date: 04/09/2025

All locations approximate



1 inch = 2,000 feet

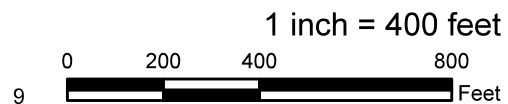




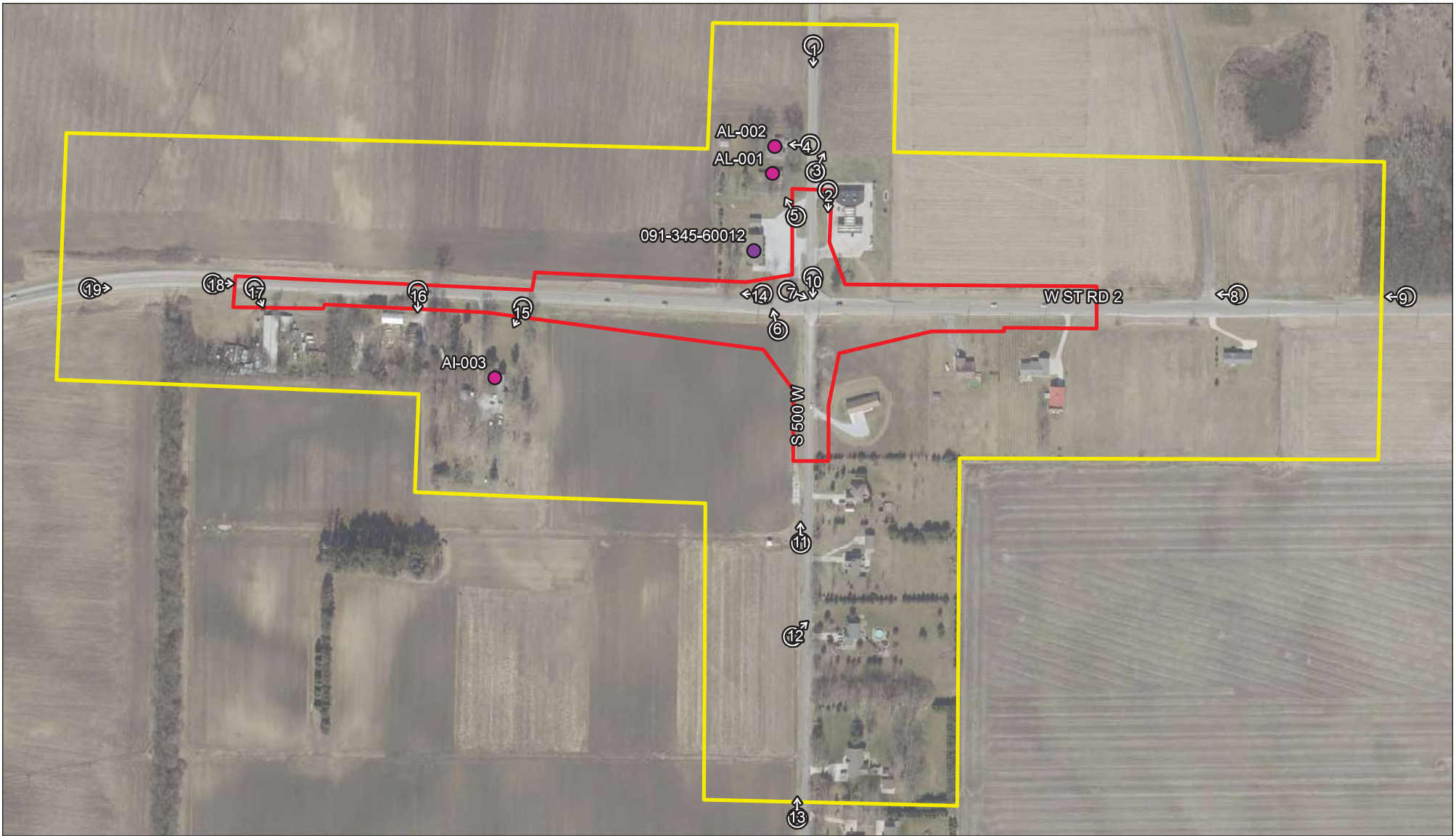
- Project Area
- Area of Potential Effects (APE)

Figure 2. Project Area and APE
on a 2023 Aerial Photograph
SR 2 and CR 500 W Intersection Improvement Project
Scipio Township, LaPorte County, IN
Des. No. 2200043
Metric Project No. 22-0216-1
Map Date: 04/09/2025

All locations approximate
Service Layer Credits: Indiana Current Imagery: Indiana
Geographic Information Office (IGIO)



Appendix B. Project Site Photographs and Key Maps



- Area of Potential Effects (APE)
 ● IHSSI Contributing
 Photo Location
● Architectural Location

Figure 4. Photo Location Map
 SR 2 and CR 500 W Intersection Improvement Project
 Scipio Township, LaPorte County, IN
 Des. No. 2200043
 Metric Project No. 22-0216-1
 Map Date: 07/18/2024

All locations approximate
 Service Layer Credits: Indiana Geographic Information Office
 (IGIO)



1 inch = 400 feet





Photo 1. Streetscape view facing south showing S CR 500 W from the north end of the APE; 999 S CR 500 (ca. 2000) is visible on the left

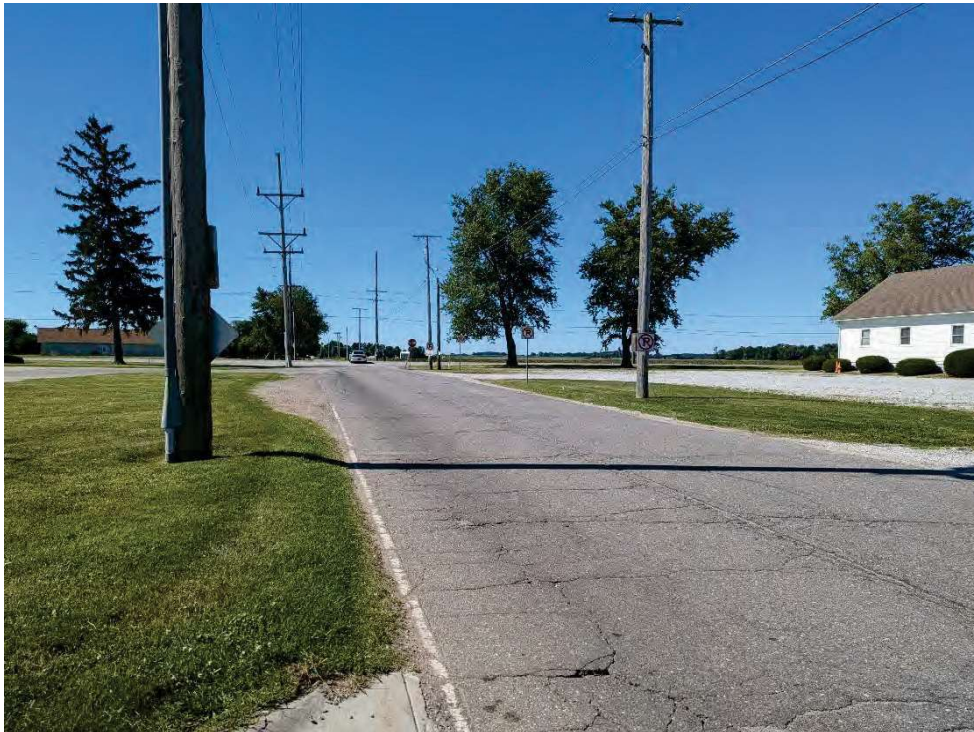


Photo 2. Streetscape view facing south showing S CR 500 W from the north end of the project area



Photo 3. Streetscape view facing northeast showing (right to left) the building at 965 S CR 500 W (ca. 2023) and the industrial complex at 4749 SR 2 from S CR 500 W



Photo 4. View facing west of 984 S CR 500 W (AL-002, rated Contributing [C], ca. 1949) from S CR 500 W



Photo 5. View facing northwest of 988 S CR 500 W (AL-001, rated C, ca. 1949) from S CR 500 W



Photo 6. View facing northwest of the Pinola Grange Hall No. 2204 at 5005 SR 2 (IHSSI No. 091-345-60012, rated C, ca. 1890) from SR 2



Photo 7. Streetscape view facing southeast showing (right to left) the garage at 4762 SR 2 (ca. 2010), the house at 4878 SR 2 (ca. 2012), the house at 4912 SR 2 (ca. 2013), and the house at 1111 S CR 500 W (ca. 2019) from the intersection of SR 2 and S CR 500 W



Photo 8. Streetscape view facing west showing SR 2 toward the east end of the project area



Photo 9. Streetscape view facing west showing SR 2 from the east end of the APE



Photo 10. Streetscape view facing south of S CR 500 W from the intersection of SR 2 and S CR 500 W



Photo 11. Streetscape view facing north showing S CR 500 W toward the south end of the project area



Photo 12. Streetscape view facing northeast showing (left to right) the house at 1139 S CR 500 W (ca. 2004) and the house at 1171 S CR 500 W (ca. 2006) from S CR 500 W



Photo 13. Streetscape view facing north showing S CR 500 W from the south end of the APE; 1197 S CR 500 W (ca. 2004) is slightly visible on the right



Photo 14. Streetscape view facing west showing SR 2 from the intersection of S CR 500 W and SR 2



Photo 15. View facing southwest of the Wall House at 5152 SR 2 (AL-003, rated C, ca. 1859) from SR 2



Photo 16. View facing south of 5172 SR 2 (rated Non-Contributing [NC], ca. 1951) from SR 2



Photo 17. View facing southeast of 5180 SR 2 (rated NC, ca. 1951) from SR 2



Photo 18. Streetscape view facing east showing SR 2 from the west end of the project area



Photo 19. Streetscape view facing east showing SR 2 from the west end of the APE

Appendix C. Historic Property Short Report Management Summary and Conclusions

HISTORIC PROPERTY SHORT REPORT

STATE ROAD 2 AND COUNTY ROAD 500 WEST INTERSECTION
IMPROVEMENT PROJECT
DES NO. 2200043/DHPA NO. 31664
SCIPPIO TOWNSHIP, LAPORTE COUNTY, INDIANA

PREPARED FOR:

LAWSON-FISHER ASSOCIATES P.C.
525 WEST WASHINGTON AVE.
SOUTH BEND, IN 46601
Dmitri Adams
574-234-3167

LEAD AGENCY:

FEDERAL HIGHWAY ADMINISTRATION
Prepared by:



Complex Environment. Creative Solutions.

6958 Hillsdale Court
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Telephone: 317.400.1633
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A handwritten signature in black ink, appearing to read 'Zoe Vorndran'.

Zoe Vorndran, QP
Principal Investigator
zoev@metricenv.com

A handwritten signature in black ink, appearing to read 'Haley Receveur'.

Haley Receveur, QP
Associate Investigator
haleyr@metricenv.com

September 10, 2024

MANAGEMENT SUMMARY

This report documents the identification and evaluation efforts for properties included in the Area of Potential Effects (APE) for the proposed State Road (SR) 2 and County Road (CR) 500 West (W) Intersection Improvement Project in Scipio Township, LaPorte County, Indiana. Above-ground resources located within the project APE were identified and evaluated in accordance with Section 106, National Historic Preservation Act (NHPA) of 1966, as amended, and the regulations implementing Section 106 (36 CFR Part 800).

As a result of the NHPA, as amended, and CFR Part 800, federal agencies are required to take into account the impact of federal undertakings upon historic properties in the area of the undertaking. Historic properties include buildings, structures, sites, objects, and/or districts that are eligible for or listed in the National Register of Historic Places (NRHP). As this project is receiving funding from the Federal Highway Administration (FHWA), it is subject to a Section 106 review.

The APE contains no properties listed in the NRHP.

The APE contains no properties that are recommended eligible for listing in the NRHP.

CONCLUSIONS

The APE contains no properties listed in the NRHP.

As a result of identification and evaluation efforts for this project, no properties are recommended eligible for listing in the NRHP.

Appendix D. Archaeological Short Report Results and Recommendations

ARCHAEOLOGICAL SHORT REPORT

PHASE IA ARCHAEOLOGICAL RECONNAISSANCE SURVEY FOR THE PROPOSED SR 2 AND CR 500 W INTERSECTION IMPROVEMENT PROJECT, 1.99 MILES SOUTH OF I-90, SCIPIO TOWNSHIP, LAPORTE COUNTY, INDIANA (INDOT DES. NO. 2200043)

PREPARED FOR:

**LAWSON-FISHER ASSOCIATES P.C.
525 WEST WASHINGTON AVENUE,
SOUTH BEND, IN 46001
(574) 234-3167**

LEAD AGENCY:

INDIANA DEPARTMENT OF TRANSPORTATION

Prepared by:

Clara Peters, Jacob Overstreet, and Virginia K. Carter



Complex Environment. Creative Solutions.

6958 Hillsdale Court
Indianapolis, IN 46250
Telephone: 317.400.1633
www.metricenv.com

A handwritten signature in black ink that reads "Samuel P. Snell".

Samuel P. Snell, MS, RPA
Archaeological Principal Investigator
sams@metricenv.com

March 12, 2025



INDIANA ARCHAEOLOGICAL SHORT REPORT

State Form 54566 (R3 / 3-22)

INDIANA DEPARTMENT OF NATURAL RESOURCES
DIVISION OF HISTORIC PRESERVATION AND ARCHAEOLOGY
402 West Washington Street, Room W274
Indianapolis, Indiana 46204-2739
Telephone Number: (317) 232-1646
Fax Number: (317) 232-0693
E-mail: dhpa@dnr.IN.gov

Where applicable, the use of this form is recommended but not required by the Division of Historic Preservation and Archaeology (DHPA).

| | | |
|---|-------------------|---|
| Name(s) of author(s) Clara Peters, Jacob Overstreet, and Virginia K. Carter | | Date (month, day, year) March 12, 2025 |
| Title of project Phase Ia Archaeological Reconnaissance Survey for the Proposed SR 2 and CR 500 W Intersection Improvement Project, 1.99 Miles South of I-90, Scipio Township, LaPorte County, Indiana (INDOT Des. No. 2200043) | | |
| This document is being used to report on the results of: <input type="checkbox"/> Records check only <input checked="" type="checkbox"/> Records check and Phase 1a archaeological reconnaissance <input type="checkbox"/> An addendum to a previous archaeological report. For an addendum, provide the following information. | | |
| Name(s) of author(s) of previous report NA | | |
| Title of previous report NA | | |
| Date of previous report (month, day, year) NA | DHPA number NA | |

| PROJECT OVERVIEW | | | |
|---|-----------------------------|--|------------------------|
| Description of project The proposed project involves improvements at the intersection of State Road (SR) 2 and County Road (CR) 500 West (W) in Scipio Township, LaPorte County, Indiana (Figure 1). The project area will extend approximately 499.87 meters (m) (1,640 feet [ft]) west and 231.65 m (760 ft) east of CR 500 W along SR 2 as well as 91.44 m (300 ft) north and 132.56 m (435 ft) south of SR 2 along CR 500 W. The distance from the edge of pavement along the existing roads will vary from 7.62 to 27.43 m (25 to 90 ft). The existing intersection has exhibited a high frequency of car crashes, falling within the 99th percentile of crash occurrences with similar intersections. County Road 500 West's level of service is also expected to reach a "F", or breakdown flow, rating due to increasingly high volume traffic. The proposed preferred alternative will modify the existing intersection by constructing a single-lane roundabout with a truck apron. The roundabout's location will be closely centered at the existing intersection to avoid property impacts and traffic volume. The approaches will be reconstructed along CR 500 W. Curb and gutter will be installed within the limits of the roundabout to drain excess runoff into a new detention basin to be constructed in the southwest quadrant of the roundabout. Ancillary work involves removing existing trees along the roadway, relocating overhead utilities, and lighting installation within the project. Preliminary right-of-way (ROW) amounts are 0.76 hectares (ha) (1.88 acres [ac]) of permanent ROW and 0.07 ha (0.18 ac) of temporary ROW. The purpose of this project is to ensure greater safety of the SR 2 and CR 500 W intersection and provide a long-term solution to safer operations and reduced congestion. The need for this project derives from the high frequency of crashes occurring at the intersection and its predicted Level of Service F grading. The proposed project encompasses an area of 3.9 ha (9.64 ac), which corresponds to the Phase Ia survey area. | | | |
| INDOT designation number(s) 2200043 | Project number 22-0216-1 | DHPA number NA | DHPA plan number NA |
| Prepared for: (Company / Institution / Agency) Lawson-Fisher Associates P.C. | | | |
| Name of contact Dan Delgado | | | |
| Address (number and street, city, state, and ZIP code) 525 West Washington Ave, South Bend, IN 46001 | | | |
| Telephone number (574) 234-3167 | | E-mail address ddelgado@lawson-fisher.com | |
| Name of principal investigator Sam P. Snell, MS, RPA | | | |
| Name of company / institution Metric Environmental, LLC | | | |
| Address (number and street, city, state, and ZIP code) 6958 Hillsdale Court, Indianapolis, IN 46250 | | | |
| Telephone number (317) 912-3499 | | E-mail address sams@metricenv.com | |

Additional field investigation comments

The survey area was investigated in accordance with Indiana Department of Natural Resources, Division of Historic Preservation and Archaeology Guidebook (IDNR, DHPA 2022) and the Indiana Department of Transportation Cultural Resources Manual (INDOT, CRO 2024). The survey area was subject to visual walkover and shovel testing. Any specific changes to methodology were based upon conditions encountered in the field and are further described within the following section.

RESULTS

Summary of relevant regional culture background

Cultural manifestations near the project reflect the general sequence from Paleoindian through Historic cultural periods. Within LaPorte County, based on the Indiana State Historic Architecture and Archaeology Research Database (SHAARD) (IDNR, DHPA 2024), there are four Paleoindian, one Early Paleoindian, two Late Paleoindian, two Late Paleoindian/Early Archaic, 15 Archaic, 19 Early Archaic, eight Middle Archaic, four Middle/Late Archaic, 17 Late Archaic, four Terminal Late Archaic, four Late Archaic/Early Woodland, 33 Woodland, seven Early Woodland, 50 Middle Woodland, one Terminal Middle Woodland, 27 Late Woodland, four Late Woodland/Mississippian, seven Mississippian, one Middle Mississippian, eight Upper Mississippian, two Contact, two Protohistoric, 253 Unidentified Precontact, and 190 Historic period sites.

The following resources were investigated: the 1961, 1969, 1983, 2010, 2013, 2016, 2019, and 2022 LaPorte County, IN United States Geological Survey (USGS) 7.5' quadrangle topographic maps (USGS 2022); Illustrated Historical Atlas of LaPorte Co., Indiana (Higgins, Belden & Company 1874); Atlas of LaPorte County, Indiana (W. Wangersheim 1907); Map of LaPorte County (Geo A Ogle & Company 1921), Map of LaPorte County Indiana Showing Rural Delivery Service (United States Post Office Department 1910); LaPorte County, Indiana Standard Atlas (W.W. Hixson & Company 1925-1940); Map of LaPorte County, Road (Indiana Highway Survey Commission 1936); aerial imagery from 1951, 1969, 1981, 1998, 2010, 2014, 2018, and 2020 (Historic Aerials 2024); and satellite imagery from 1985 to 2022 (Google Earth 2024).

The 1921 and 1925 historic atlases display illustrations of standing structures in the vicinity of the SR 2 and CR 500 W intersection during this time period. Structures closest to the project include a schoolhouse and LaPorte RFD (rural free delivery) on the north side of SR 2 and a residence located within the southwest quadrant of the intersection. Only the schoolhouse building is currently extant in the northwest quadrant, recorded as Indiana Historic Sites and Structures No. 091-345-50012 and ranked as Contributing (IDNR, DHPA 2022). A gas station was built after 1998 at the location of the LaPorte RFD, while the southwest quadrant of the intersection consists of an agricultural field. Additionally, beginning from 2010, residential housing has increased in the plot of land southeast of the survey area along SR 2 and CR 500 W.

Records check (Check all that apply)

- ☐ The project area does not have the potential to contain archaeological resources. *Provide explanation / justification.*
- ☐ There are previously recorded archaeological resources within the project area, but those resources do not warrant additional archaeological investigation. *Provide explanation / justification.*
- ☒ The project area contains previously recorded archaeological resources that warrant additional investigation and/or the project area has the potential to contain archaeological resources. *Provide explanation / justification.*
- ☒ Based upon the records check results, a reconnaissance has been conducted.
- ☐ A cemetery is located within or adjacent to the project area.

Explanation / justification

The survey area has the potential to contain intact soils and thus has the potential to contain previously unidentified archaeological resources.

Phase 1a archaeological reconnaissance (Check all that apply)

- ☐ No Phase 1a reconnaissance was conducted.
- ☒ Phase 1a reconnaissance located no archaeological resources.
- ☐ Previously recorded sites were in the project area.
- ☐ Artifacts and/or features at a previously recorded site(s) within the project area were not discovered. *List the site(s) below.*
- ☐ Phase 1a reconnaissance has identified landforms conducive to buried archaeological deposits. *Describe below.*

List sites.

None

Describe landforms.

NA

Number of shovel probes excavated

113

Number of cores / auger probes

0

Describe disturbances. Attach photographs documenting disturbances.

Landscaping, road grade slope, drainage ditches, buried utilities, parking lots, crop vegetation, gravel areas, utility boxes

Records check (Check all that apply)

- ☐ No archaeological investigation is recommended before the project is allowed to proceed because the records check has determined that the project area does not have the potential to contain archaeological resources.
- ☐ A Phase 1a archaeological reconnaissance is recommended.
- ☒ Based upon the records check results, a Phase 1a archaeological reconnaissance was recommended and has been conducted.
- ☐ A cemetery development plan may be required under Indiana Code 14-21-1-26.5 because project ground disturbance will be within 100 feet of a cemetery.

Phase 1a archaeological reconnaissance (Check all that apply)

- ☒ It is recommended that the project be allowed to proceed as planned because the Phase 1a archaeological reconnaissance has located no archaeological sites within the project area and/or previously recorded sites that were investigated warrant no additional investigation.
- ☐ It is recommended that Phase 1c archaeological subsurface reconnaissance be conducted before the project is allowed to proceed. The Phase 1a archaeological reconnaissance has determined that the project area includes landforms which have the potential to contain buried archaeological deposits.

Other recommendations / commitments

Pursuant to IC-14-21-1, if any archaeological artifacts or human remains are uncovered during construction, demolition, or earthmoving activities, state law (Indiana Code 14-21-1-27 and 29) requires that the discovery must be reported to the Department of Natural Resources within two (2) business days. In that event, please call (317) 232-1646.

REQUIRED ATTACHMENTS

- ☒ Figure showing project location within Indiana
- ☒ USGS topographic map showing the project area (1:24,000 scale)
- ☒ Aerial photograph showing the project area, land use and survey methods
- ☒ Photographs of the project area, including, if applicable, photographs documenting disturbances
- ☐ Project plans (if available)

Other attachments

References cited (See short report instructions for required references to be consulted)

Google Earth

2024 Google Earth Pro. Desktop software, <https://www.google.com/earth/versions/#earth-pro>, accessed August 15, 2024.

Geo. A. Ogle & Company

1921 LaPorte County, Indiana Standard Atlas, Geo. A. Ogle.

Higgins, Belden & Company

1874 Illustrated Historical Atlas of LaPorte Co., Indiana, Higgins, Belden.

Historic Aerials

2024 Historic Aerials. Netronline, <<https://www.historicaerials.com/viewer>>, accessed July 26, 2023.

Indiana Department of Natural Resources, Division of Historic Preservation and Archaeology (INDR, DHPA)

2022 Guidebook for Indiana Historic Sites and Structures Inventory – Archaeological Sites. Manuscript on file, Division of Historic Preservation and Archaeology, Indiana Department of Natural Resources, Indianapolis.

2024 Indiana State Historic Architectural and Archaeological Research Database (SHAARD). Electronic database, <https://secure.in.gov/apps/dnr/shaard/welcome.html>, accessed June 13, 2024.

Indiana Department of Transportation, Cultural Resources Office (INDOT, CRO)

2024 Cultural Resources Manual. Electronic document, <<https://www.in.gov/indot/crm/INDOT>>, accessed June 13, 2024.

Indiana Geographic Information Office (IGIO)

2024a Physiography Natural Regions. IndianaMap, <https://www.indianamap.org/datasets/INMap::physiography-natural-regions/about>, accessed June 13, 2024.

2024b HUC8 Boundaries. IndianaMap, <https://www.indianamap.org/datasets/INMap::huc8-boundaries/about>, accessed June 13, 2024.

Indiana Highway Survey Commission

1936 Map of LaPorte County, Roads. Indiana Highway Survey Commission, Indianapolis.

Appendix E. Consulting Parties' List and Correspondence

The Chamber of Commerce and Mayor/Town Council President fields were incorporated into the Effect Finding document by mistake. These fields are not applicable due to this being a county project.

| Consulting Parties List | | |
|---|-------------------|--------------------------|
| Consulting Party | Contact Name | Accepted Invitation: Y/N |
| IN State Historic Preservation Office | Beth McCord | Yes |
| Northwestern Indiana Regional Planning Commission | Ty Warner | |
| LaPorte County Commissioner | Connie Gramarossa | |
| LaPorte County Commissioner | Richard Mrozinski | |
| LaPorte County Commissioner | Joe Haney | |
| LaPorte County Highway Superintendent | | |
| Chamber of Commerce (if in a downtown) | | |
| Historic Preservation Commission or Board | | |
| Mayor/Town Council President | | |
| LaPorte County Historical Society | | |
| LaPorte County Historian | Bruce Johnson | |
| LaPorte County Genealogical Society | | |
| Indiana Landmarks - Northern Regional Office | Todd Zieger | |
| Westville Community Historical Society | | |
| Indiana Lincoln Highway Association, Inc. | Debra Parcell | |
| Preserve Historic La Porte | | |
| Eastern Shawnee Tribe of Oklahoma | | |
| Miami Tribe of Oklahoma | | |
| Peoria Tribe of Indians of Oklahoma | | |
| Pokagon Band of Potawatomi Indians | | |
| Shawnee Tribe | | Yes |
| Forest County Potawatomi Community | Ben Rhodd | Yes |
| Turtle Mountain Band of Chippewa Indians | | Yes |

From: [Zoe Vorndran](#)
To: [BMCcord@dnr.IN.gov](#); [mrjsc@csinet.net](#); [carolfloyd@yahoo.com](#); [info@laportecountyhistory.org](#); [TZeiger@indianalandmarks.org](#); [north@indianalandmarks.org](#); [Lincolnhighwayassoc.office@yahoo.com](#); [preservehistorilaporte@gmail.com](#); [nirpc@nirpc.org](#); [cgramarossa@laporteco.in.gov](#); [rmrozinski@laportecounty.org](#); [jhaney@laporteco.in.gov](#); [jgsullivan@laportecounty.org](#)
Cc: [Kelly, Clint](#); [Candace Hudziak](#); [Sam Snell](#); [Dmitri Adams](#)
Subject: FHWA Project: Des. No. 2200043; Early Coordination; SR 2 and CR 500 W Intersection Improvement Project, LaPorte County, Indiana
Date: Friday, December 8, 2023 1:10:00 PM
Attachments: [SR2andCR500WIntersectionProject_Des2200043_ECL_2023-12-08.pdf](#)
[image001.png](#)

Des. No.: 2200043

Project Description: SR 2 and CR 500 W Intersection Improvement Project

Location: Scipio Township, LaPorte County, Indiana

The Indiana Department of Transportation, with funding from the Federal Highway Administration, proposes to proceed with the State Road (SR) 2 and County Road (CR) 500 West (W) Intersection Improvement Project, Des. No. 2200043.

Section 106 of the National Historic Preservation Act requires federal agencies to take into account the effects of their undertakings on historic properties. The following agencies/individuals are being invited to become consulting parties:

Indiana State Historic Preservation Office
LaPorte County Historian
LaPorte County Genealogical Society
LaPorte County Historical Society
Westville Community Historical Society
Indiana Landmarks, Northern Regional Office
Indiana Lincoln Highway Association, Inc.
Preserve Historic La Porte
Northwestern Indiana Regional Planning Commission
LaPorte County Commissioners – Connie Gramarossa, Richard Mrozinski, Joe Haney
LaPorte County Highway Superintendent
LaPorte County Highway Engineer
Eastern Shawnee Tribe of Oklahoma
Miami Tribe of Oklahoma
Peoria Tribe of Indians of Oklahoma
Pokagon Band of Potawatomi Indians
Shawnee Tribe
Forest County Potawatomi Community

This letter is part of the early coordination phase of the environmental review process requesting comments associated with this project. We are requesting comments from your area of expertise regarding any possible environmental effects associated with this project. Please use the above Des. Number and project description in your reply and your comments will be incorporated into the formal environmental study.

Please review the attached letter, which is also located in IN SCOPE at

<http://erms12c.indot.in.gov/Section106Documents/> (the Des. No. is the most efficient search term, once in IN SCOPE), and respond with your comments on any historic resource impacts incurred as a result of this project so that an environmental report can be completed. We also welcome your related opinions and other input to be considered in the preparation of the environmental document. If a hard copy of the materials is needed, please respond to this email with your request within seven (7) days.

Consulting parties have thirty (30) calendar days from receipt of this information to review and provide comment. If we do not receive a response from an invited consulting party within the time allotted, the project will proceed consistent with the proposed design. Therefore, if we do not receive a response within thirty (30) days, your agency or organization will not receive any further information on the project unless the scope of work changes.

Tribal Contacts please respond to INDOT's Acting Tribal Liaison, Matt Coon, at

mcoon@indot.in.gov (317-697-9752) with any responses pertaining to this project including to provide INDOT/Indiana FHWA additional information about Tribal resources/concerns and questions/comments regarding cultural resources. The FHWA point of contact is Kari Carmany-George at K.CarmanyGeorge@dot.gov (317-226-5629).

Thank you in advance for your input,



Zoe Vorndran

Cultural Resources Staff Scientist

○ 317.504.4296

6958 Hillsdale Court
Indianapolis, IN 46250
www.metricenv.com

Certified DBE/MBE/SBE Company
Indiana | Kentucky | Ohio | West Virginia | Pennsylvania

From: [Kelly, Clint](#)
To: [THPO@MiamiNation.com](#); [thpo@estoo.net](#); [bfletcher@peoriatribes.com](#); [Matthew.Bussler@pokagonband-nsn.gov](#); [Section106@shawnee-tribe.com](#); [benjamin.rhodd@fcp-nsn.gov](#)
Cc: [Coon, Matthew](#); [Branigin, Susan](#); [Alexander, Dawn](#); [Candace Hudziak](#); [Zoe Vomdran](#); [Grylewicz, Michael J](#); [Carmany-George, Karstin \(FHWA\)](#)
Subject: FHWA Project: Des. No. 2200043; Early Coordination; SR 2 and CR 500 W Intersection Improvement Project, LaPorte County, Indiana
Date: Friday, December 8, 2023 1:38:19 PM
Attachments: [image001.png](#)
[image002.png](#)
[image003.png](#)
[image004.png](#)
[image006.png](#)
[SR2andCR500WIntersectionProject_Des2200043_ECL_2023-12-08.pdf](#)

External Message: *This message originated outside of Metric Environmental.
Do not click links or open attachments unless you recognize the sender and know the content is safe.*

Des. No.: 2200043

Project Description: SR 2 and CR 500 W Intersection Improvement Project

Location: Scipio Township, LaPorte County, Indiana

The Indiana Department of Transportation, with funding from the Federal Highway Administration, proposes to proceed with the State Road (SR) 2 and County Road (CR) 500 West (W) Intersection Improvement Project, Des. No. 2200043.

Section 106 of the National Historic Preservation Act requires federal agencies to take into account the effects of their undertakings on historic properties. The following agencies/individuals are being invited to become consulting parties:

Indiana State Historic Preservation Office
LaPorte County Historian
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LaPorte County Historical Society
Westville Community Historical Society
Indiana Landmarks, Northern Regional Office
Indiana Lincoln Highway Association, Inc.
Preserve Historic La Porte
Northwestern Indiana Regional Planning Commission
LaPorte County Commissioners – Connie Gramarossa, Richard Mrozinski, Joe Haney
LaPorte County Highway Superintendent
LaPorte County Highway Engineer
Eastern Shawnee Tribe of Oklahoma
Miami Tribe of Oklahoma
Peoria Tribe of Indians of Oklahoma
Pokagon Band of Potawatomi Indians
Shawnee Tribe
Forest County Potawatomi Community

This letter is part of the early coordination phase of the environmental review process requesting comments associated with this project. We are requesting comments from your area of expertise regarding any possible environmental effects associated with this project. Please use the above Des. Number and project description in your reply and your comments will be incorporated into the formal environmental study.

Please review the attached letter, which is also located in IN SCOPE at <http://erms12c.indot.in.gov/Section106Documents/> (the Des. No. is the most efficient search term, once in IN SCOPE), and respond with your comments on any historic resource impacts incurred as a result of this project so that an environmental report can be completed. We also welcome your related opinions and other input to be

considered in the preparation of the environmental document. If a hard copy of the materials is needed, please respond to this email with your request within seven (7) days.

Consulting parties have thirty (30) calendar days from receipt of this information to review and provide comment. If we do not receive a response from an invited consulting party within the time allotted, the project will proceed consistent with the proposed design. Therefore, if we do not receive a response within thirty (30) days, your agency or organization will not receive any further information on the project unless the scope of work changes.

Tribal Contacts please respond to INDOT's Acting Tribal Liaison, Matt Coon, at mcoon@indot.in.gov (317-697-9752) with any responses pertaining to this project including to provide INDOT/Indiana FHWA additional information about Tribal resources/concerns and questions/comments regarding cultural resources. The FHWA point of contact is Kari Carmany-George at K.CarmanyGeorge@dot.gov (317-226-5629).

Thank you in advance for your input,

Clint Kelly

Section 106 Specialist/Historian

Cultural Resources Office

Environmental Services

100 N. Senate Ave., Rm. N758-ES

Indianapolis, IN 46204

Office: (317) 447-8707

Email: ckelly1@indot.in.gov

Core Office Hours: M-F 7:30-3:30





INDIANA DEPARTMENT OF TRANSPORTATION

100 North Senate Avenue
Room N758-ES
Indianapolis, Indiana 46204

PHONE: (855) 463-6848

Eric Holcomb, Governor
Michael Smith, Commissioner

December 8, 2023

This letter was sent to the listed parties.

RE: State Road (SR) 2 and County Road (CR) 500 West (W) Intersection Improvement Project
Des. No. 2200043
Scipio Township, LaPorte County, Indiana

Dear Consulting Party (see attached list),

The Indiana Department of Transportation (INDOT), with funding from the Federal Highway Administration, proposes to proceed with the State Road (SR) 2 and County Road (CR) 500 West (W) Intersection Improvement Project, Des. No. 2200043. Metric Environmental, LLC, is under contract with INDOT to advance the environmental documentation for the referenced project.

This letter is part of the early coordination phase of the environmental review process requesting comments associated with this project. We are requesting comments from your area of expertise regarding any possible environmental effects associated with this project. Please use the above Des. Number and project description in your reply and your comments will be incorporated into the formal environmental study.

The proposed undertaking is at the intersection of SR 2 and CR 500 W in LaPorte County, Indiana, southwest of the City of La Porte between US 421 and US 35. It is within Scipio Township, on the LaPorte West, IN, USGS Topographic Quadrangle, in Sections 5, 6, 7, 8, Township 36 North, Range 3 West. The project area can be viewed online at <https://arcg.is/jqueP> (the Des. No. is the most efficient search term once in the CRO - Public Web Map App).

State Road 2 and CR 500 W are both two-lane roadways. SR 2 is classified as a Rural Principal Arterial Other with 12-foot lanes, narrow shoulders, and right turn lanes onto CR 500 W at the intersection. CR 500 W is classified as a Rural Minor Collector with 11-foot lanes and no shoulders in the project area. The intersection is controlled with stop signs on CR 500 W.

The need for this project is evidenced by the high frequency of car crashes at this intersection. A total of 22 crashes occurred at the intersection during the study period 2017 to 2019, the majority being right and left hand turns. A RoadHAT crash analysis (traffic engineering safety modeling software) resulted in an Index of Crash Frequency of 2.96. The Index of Crash Frequency compares the crash frequency data of this intersection with intersections of similar volumes, roadway classification, and control type throughout Indiana. Crash data for this intersection resulted in an ICF of 2.96, putting it in the 99th percentile compared with similar intersections, which indicates the existing stop signs on CR 500 W are not safely controlling traffic at this intersection. Additionally, southbound CR 500 W experiences a high volume of traffic and its Level of Service (LOS) is expected to reach F (breakdown flow) prior to the design year. The LOS is on a scale from A (free flow) to F (breakdown flow). The purpose of this project is to improve safety of the SR 2 and CR 500 W intersection and provide a long-term solution that ensures safe operation and reduced congestion at this intersection.

The proposed preferred alternative will modify the existing intersection by constructing a single-lane roundabout with a truck apron. The roundabout's location will be closely centered at the existing intersection to avoid property impacts and traffic volume. The approach work is estimated to be 420 feet east and west of SR 2 and 380 feet north and south along CR 500 W. Curb and gutter will be installed within the limits of the roundabout to drain excess runoff into a new detention basin to be constructed in the southwest quadrant of the roundabout. Ancillary work involves removing existing trees along the roadway, relocating overhead utilities, and lighting installation within the project area.

Preliminary right-of-way amounts are 2.36 acres of permanent and 0.003 acre of temporary. No relocations of properties are anticipated for this project. During construction, traffic will be maintained using lane closures and a detour. A minimum of one lane in each direction will be maintained on SR 2, and a short closure of each leg of CR 500 W is anticipated for the reconstruction of each approach. Plans for a detour will be available at a later date.

Section 106 of the National Historic Preservation Act requires federal agencies to take into account the effects of their undertakings on historic properties. In accordance with 36 CFR 800.2 (c), you are hereby requested to be a consulting party to participate in the Section 106 process. Entities that have been invited to participate in the Section 106 consultation process for this project are identified in the attached list. Per 36 CFR 800.3(f), we hereby request that the Indiana State Historic Preservation Officer (SHPO) notify this office if the SHPO staff is aware of any other parties that may be entitled to be consulting parties or should be contacted as potential consulting parties for the project.

The Section 106 process involves efforts to identify historic properties potentially affected by the undertaking, assess its effects and seek ways to avoid, minimize or mitigate any adverse effects on historic properties. For more information regarding the protection of historic resources, please see the Advisory Council on Historic Preservation's guide: *Protecting Historic Properties: A Citizen's Guide to Section 106 Review* available online at <https://www.achp.gov/sites/default/files/documents/2017-01/CitizenGuide.pdf>.

The Area of Potential Effects (APE) is the area in which the proposed project may cause alterations in the character or use of historic resources. At this time, no cultural resource investigations have occurred; however, the results of cultural resource identification and evaluation efforts, both above-ground and archaeological, will be forthcoming. Consulting parties will receive notification when these reports are completed.

Please review the information and comment within thirty (30) calendar days of receipt. If you indicate that you do not desire to be a consulting party, or if you do not respond, you will not be included on the list of consulting parties for this project. If we do not receive your response in the time allotted, the project will proceed consistent with the proposed design and you will not receive further information about the project unless the design changes.

For questions concerning specific project details, you may contact Candace Hudziak of Metric Environmental, LLC, at 317-443-4123 or candaceh@metricenv.com. All future responses regarding the proposed project should be forwarded to Metric Environmental, LLC, at the following address:

Candace Hudziak
Architectural Historian
Metric Environmental, LLC
6958 Hillsdale Court
Indianapolis, Indiana, 46250
candaceh@metricenv.com

Tribal Contacts please respond to INDOT's Acting Tribal Liaison, Matt Coon, at mcoon@indot.in.gov (317-697-9752) with any responses pertaining to this project including to provide INDOT/Indiana FHWA additional information about Tribal

resources/concerns and questions/comments regarding cultural resources. The FHWA point of contact is Kari Carmany-George at K.CarmanyGeorge@dot.gov (317-226-5629).

Sincerely,

A handwritten signature in black ink, appearing to read 'Matthew S. Coon', with a stylized flourish at the end.

Matthew S. Coon, Manager
Cultural Resources Office
Environmental Services

Enclosures:

Distribution List

Project Location Maps

Distribution List:

Beth McCord, Deputy SHPO
Department of Natural Resources
Division of Historic Preservation and Archaeology
402 West Washington Street, Room W274
Indianapolis, IN 46204-2739
BMCCord@dnr.IN.gov

LaPorte County Historian
Bruce Johnson
2012 South Village Road
La Porte, IN 46350
mrjsc@csinet.net

LaPorte County Genealogical Society
PO Box 365
La Porte, IN 46352
carolfloyd@yahoo.com

LaPorte County Historical Society
2405 Indiana Avenue, Suite 1
La Porte, IN 46350-6063
info@laportecountyhistory.org

Westville Community Historical Society
PO Box 395
Westville, IN 46391-0395

Indiana Landmarks – Northern Regional Office
Todd Zeiger, Director
801 West Washington Street
South Bend, IN 46601
tzeiger@indianalandmarks.org
north@indianalandmarks.org

Indiana Lincoln Highway Association, Inc.
Debra Parcell
402 West Washington Street
South Bend, IN 46601
Lincolnhighwayassoc.office@yahoo.com

Preserve Historic La Porte
PO Box 853
La Porte, IN 46352
Preservehistoriclaporte@gmail.com

Northwestern Indiana Regional Planning Commission
Ty Warner, Executive Director
6100 Southport Road
Portage, IN 46368
nirpc@nirpc.org

LaPorte County Commissioners
555 Michigan Avenue, Suite 202
La Porte, IN 46350
Connie Gramarossa
• cgramarossa@laporteco.in.gov
Richard Mrozinski
• rmrozinski@laportecounty.org
Joe Haney
• jhaney@laporteco.in.gov

LaPorte County Highway Superintendent
1805 West 5th Street
La Porte, IN 46350

LaPorte County Highway Engineer
Jerry Sullivan
1805 West 5th Street
La Porte, IN 46350-8380
jgsullivan@laportecounty.org

Tribal Consulting Party Contacts:
Eastern Shawnee Tribe of Oklahoma

Miami Tribe of Oklahoma

Peoria Tribe of Indians of Oklahoma

Pokagon Band of Potawatomi Indians

Shawnee Tribe

Forest County Potawatomi Community

From: [Zoe Vorndran](#)
To: [Zoe Vorndran](#)
Subject: FW: FHWA Project: Des. No. 2200043; Early Coordination; SR 2 and CR 500 W Intersection Improvement Project, LaPorte County, Indiana
Date: Tuesday, December 19, 2023 11:49:00 AM

From: Benjamin Rhodd <Benjamin.Rhodd@fcp-nsn.gov>
Sent: Monday, December 11, 2023 10:14 AM
To: Kelly, Clint <CKelly1@indot.IN.gov>
Subject: RE: FHWA Project: Des. No. 2200043; Early Coordination; SR 2 and CR 500 W Intersection Improvement Project, LaPorte County, Indiana

**** This is an EXTERNAL email. Exercise caution. DO NOT open attachments or click links from unknown senders or unexpected email. ****

Mr. Kelly,

Pursuant to consultation under Section 106 of the National Historic Preservation Act (1966 as amended) the Forest County Potawatomi Community (FCPC), a Federally Recognized Native American Tribe, reserves the right to comment on Federal undertakings, as defined under the act inclusive of licensing, permitting or use of federal funds by a delegated agency.

The Tribal Historic Preservation Office (THPO) staff has reviewed the information you provided for this project. Upon review of site data and supplemental cultural history within our Office, the FCPC THPO is pleased to offer a finding of No Historic Properties affected of significance to the FCPC, however, we request to remain as a consulting party for this project.

As a standard caveat sent with each proposed project reviewed by the FCPC THPO, the following applies. In the event an Inadvertent Discovery (ID) occurs at any phase of a project or undertaking as defined, and human remains or archaeologically significant materials are exposed as a result of project activities, work should cease immediately. The Tribe(s) must be included with the SHPO in any consultation regarding treatment and disposition of an ID find.

Thank you for protecting cultural and historic properties and if you have any questions or concerns, please contact me at the email or number listed below.

Respectfully,

Ben Rhodd, MS, RPA, Tribal Historic Preservation Officer
Forest County Potawatomi
Historic Preservation Office
8130 Mish ko Swen Drive, P.O. Box 340, Crandon, Wisconsin 54520
P: 715-478-7354 C: 715-889-0202 Main: 715-478-7474
Email: Benjamin.Rhodd@fcp-nsn.gov
www.fcpotawatomi.com

From: [Laserfiche Notification](#)
To: [Coon, Matthew](#)
Subject: Section 106 Consultation - Des. No. 2200043
Date: Wednesday, December 20, 2023 1:56:53 PM

**** This is an EXTERNAL email. Exercise caution. DO NOT open attachments or click links from unknown senders or unexpected email. ****

This email is in response to Des. No. 2200043.

The Shawnee Tribe's Tribal Historic Preservation Department concurs that no known historic properties will be negatively impacted by this project. However, there is still potential for the discovery of unknown resources. We would like to accept your invitation for consultation.

We have no issues or concerns at this time, but in the event archaeological materials are encountered during construction, use, or maintenance of this location, please re-notify us at that time as we would like to resume immediate consultation under such a circumstance.

If you have any questions, you may contact me via email at Section106@shawnee-tribe.com





Division of Historic Preservation & Archaeology · 402 W. Washington Street, W274 · Indianapolis, IN 46204-2739
Phone 317-232-1646 · Fax 317-232-0693 · dhpa@dnr.IN.gov ·



January 4, 2024

Candace Hudziak
Architectural Historian
Metric Environmental, LLC
6958 Hillsdale Court
Indianapolis, Indiana 46250

Federal Agency: Indiana Department of Transportation (“INDOT”),
on behalf of Federal Highway Administration, Indiana Division
 (“FHWA”)

Re: Early coordination letter for the SR 2 and CR 500 W Intersection Improvement
Project (Des. No. 2200043; DHPA No. 31664)

Dear Ms. Hudziak:

Pursuant to Section 106 of the National Historic Preservation Act of 1966, as amended (54 U.S.C. § 306108), 36 C.F.R. Part 800, and the “Programmatic Agreement (PA) Among the Federal Highway Administration, the Indiana Department of Transportation, the Advisory Council on Historic Preservation and the Indiana State Historic Preservation Officer Regarding the Implementation of the Federal Aid Highway Program In the State of Indiana,” the staff of the Indiana State Historic Preservation Officer (“Indiana SHPO”) has reviewed your December 8, 2023, submission which enclosed INDOT’s early coordination letter, received by our office December 8, 2023, for this project in Scipio Township, LaPorte County, Indiana.

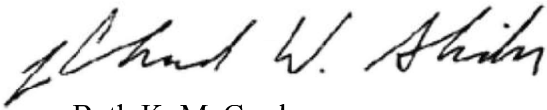
We are not aware of any parties who should be invited to participate in the Section 106 consultation on this federal undertaking, beyond those whom INDOT already has invited. In your next regular correspondence on this project, please advise us as to which of the invited consulting parties has accepted the invitation.

We look forward to reviewing the proposed area of potential effects and the reports on investigations of above-ground cultural resources and archaeological resources that the early coordination letter indicated will be forthcoming.

The Indiana SHPO staff's archaeological reviewer for this project is Wade T. Tharp, and the structures reviewer is Toni Lynn Giffin. However, if you have a question about the Section 106 process, please contact initially the INDOT Cultural Resources staff members who are assigned to this project.

In all future correspondence about the SR 2 and CR 500 W intersection improvement project in Scipio Township, LaPorte County, Indiana (Des. No. 2200043), please refer to DHPA No. 31664.

Very truly yours,

A handwritten signature in black ink, appearing to read "Beth K. McCord". The signature is fluid and cursive, with the first name "Beth" being more prominent.

Beth K. McCord
Deputy State Historic Preservation Officer

BKM:TLG:WTT:wt

emc: Matt Coon, Ph.D., Indiana Department of Transportation
Susan Branigin, Indiana Department of Transportation
Candace Hudziak, Metric Environmental, LLC
Toni Lynn Giffin, Indiana DNR-DHPA
Wade T. Tharp, Indiana DNR-DHPA

From: [Zoe Vorndran](#)
To: bmccord@dnr.in.gov
Cc: [Kelly, Clint](#); [Branigin, Susan](#); [Coon, Matthew](#); [Candace Hudziak](#); [Sam Snell](#); [Elayna Stoner](#); [Alex Gray](#); [Dmitri Adams](#); [Dan Delgado](#)
Subject: FHWA Project: Des. No. 2200043; HPSR; SR 2 and CR 500 W Intersection Improvement Project, LaPorte County, Indiana
Date: Tuesday, September 10, 2024 3:57:00 PM

Des. No.: 2200043

Project Description: SR 2 and CR 500 W Intersection Improvement Project

Location: Scipio Township, LaPorte County, Indiana

The Indiana Department of Transportation, with funding from the Federal Highway Administration, proposes to proceed with the State Road (SR) 2 and County Road (CR) 500 West (W) Intersection Improvement Project, Des. No. 2200043.

As part of Section 106 of the National Historic Preservation Act, a Historic Property Short Report has been prepared and is ready for review and comment by consulting parties.

Please review this documentation located in IN SCOPE at <https://erms12c.indot.in.gov/Section106Documents/> (the Des. No. is the most efficient search term, once in IN SCOPE), and respond with any comments that you may have. If a hard copy of the materials is needed, please respond to this email with your request within seven (7) days.

Please review the information and comment within thirty (30) calendar days of receipt. If you indicate that you do not desire to be a consulting party or if you have not previously accepted consulting party status and you do not respond, you will not be included on the list of consulting parties for this project and will not receive further information about the project unless the design changes.

Tribal Contacts please respond to INDOT's Acting Tribal Liaison, Matt Coon

mcoon@indot.in.gov; (317-697-9752) with any responses pertaining to this project including to provide INDOT/Indiana FHWA additional information about Tribal resources/concerns and questions/comments regarding cultural resources. The FHWA point of contact is Kari Carmany-George at K.CarmanyGeorge@dot.gov (317-226-5629).

Thank you in advance for your input,



Zoe Vorndran, MA, QP

Staff Scientist / Historian

○ 317.504.4296

6958 Hillside Court
Indianapolis, IN 46250
www.metricenv.com

Certified DBE/MBE/SBE Company
Indiana | Kentucky | Ohio | West Virginia | Pennsylvania

From: [Kelly, Clint](#)
To: Section106@shawnee-tribe.com; Luke.Heider@fcp-nsn.gov
Cc: [Coon, Matthew](#); [Branigin, Susan](#); [Walton, David](#); [Zoe Vorndran](#); [Carmany-George, Karstin \(FHWA\)](#)
Subject: FHWA Project: Des. No. 2200043; HPSR; SR 2 and CR 500 W Intersection Improvement Project, LaPorte County, Indiana
Date: Tuesday, September 10, 2024 4:05:33 PM
Attachments: [image001.png](#)
[image002.png](#)
[image003.png](#)
[image004.png](#)
[image006.png](#)

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Des. No.: 2200043
Project Description: SR 2 and CR 500 W Intersection Improvement Project
Location: Scipio Township, LaPorte County, Indiana

The Indiana Department of Transportation, with funding from the Federal Highway Administration, proposes to proceed with the State Road (SR) 2 and County Road (CR) 500 West (W) Intersection Improvement Project, Des. No. 2200043.

As part of Section 106 of the National Historic Preservation Act, a Historic Property Short Report has been prepared and is ready for review and comment by consulting parties.

Please review this documentation located in IN SCOPE at <https://erms12c.indot.in.gov/Section106Documents/> (the Des. No. is the most efficient search term, once in IN SCOPE), and respond with any comments that you may have. If a hard copy of the materials is needed, please respond to this email with your request within seven (7) days.

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Tribal Contacts please respond to INDOT's Acting Tribal Liaison, Matt Coon mcoon@indot.in.gov; (317-697-9752) with any responses pertaining to this project including to provide INDOT/Indiana FHWA additional information about Tribal resources/concerns and questions/comments regarding cultural resources. The FHWA point of contact is Kari Carmany-George at K.CarmanyGeorge@dot.gov (317-226-5629).

Thank you in advance for your input,

Clint Kelly
Section 106 Specialist/Historian
Cultural Resources Office
Environmental Services
100 N. Senate Ave., Rm. N758-ES
Indianapolis, IN 46204
Office: (317) 447-8707
Email: ckelly1@indot.in.gov
Core Office Hours: M-F 7:30-3:30



From: [Kelly, Clint](#)
To: [Larus Longie](#)
Cc: [Coon, Matthew](#); [Branigin, Susan](#); [Walton, David](#); [Zoe Vorndran](#); [Carmany-George, Karstin \(FHWA\)](#)
Subject: FHWA Project: Des. No. 2200043; HPSR; SR 2 and CR 500 W Intersection Improvement Project, LaPorte County, Indiana
Date: Tuesday, September 10, 2024 4:07:08 PM
Attachments: [image001.png](#)
[image002.png](#)
[image003.png](#)
[image004.png](#)
[image006.png](#)

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Des. No.: 2200043

Project Description: SR 2 and CR 500 W Intersection Improvement Project

Location: Scipio Township, LaPorte County, Indiana

The Indiana Department of Transportation, with funding from the Federal Highway Administration, proposes to proceed with the State Road (SR) 2 and County Road (CR) 500 West (W) Intersection Improvement Project, Des. No. 2200043.

As part of Section 106 of the National Historic Preservation Act, a Historic Property Short Report has been prepared and is ready for review and comment by consulting parties.

Please review this documentation located in IN SCOPE at <https://erms12c.indot.in.gov/Section106Documents/> (the Des. No. is the most efficient search term, once in IN SCOPE), and respond with any comments that you may have. If a hard copy of the materials is needed, please respond to this email with your request within seven (7) days.

Please review the information and comment within thirty (30) calendar days of receipt. If you indicate that you do not desire to be a consulting party or if you have not previously accepted consulting party status and you do not respond, you will not be included on the list of consulting parties for this project and will not receive further information about the project unless the design changes.

Tribal Contacts please respond to INDOT's Acting Tribal Liaison, Matt Coon mcoon@indot.in.gov; (317-697-9752) with any responses pertaining to this project including to provide INDOT/Indiana FHWA additional information about Tribal resources/concerns and questions/comments regarding cultural resources. The FHWA point of contact is Kari Carmany-George at K.CarmanyGeorge@dot.gov (317-226-5629).

Thank you in advance for your input,

Clint Kelly

Section 106 Specialist/Historian

Cultural Resources Office

Environmental Services

100 N. Senate Ave., Rm. N758-ES

Indianapolis, IN 46204

Office: (317) 447-8707

Email: ckelly1@indot.in.gov

Core Office Hours: M-F 7:30-3:30





INDIANA DEPARTMENT OF TRANSPORTATION

100 North Senate Avenue
Room N758-ES
Indianapolis, Indiana 46204

PHONE: (855) 463-6848

Eric Holcomb, Governor
Michael Smith, Commissioner

September 10, 2024

This letter was sent to the listed parties.

RE: State Road 2 and County Road 500 West Intersection Improvement Project
Des. No. 2200043
Scipio Township, LaPorte County, Indiana

Dear Consulting Party,

The Indiana Department of Transportation (INDOT), with funding from the Federal Highway Administration (FHWA), proposes to proceed with the State Road (SR) 2 and County Road (CR) 500 West (W) Intersection Improvement Project, Des. No. 2200043.

This letter is part of the Section 106 review process for this project. Section 106 of the National Historic Preservation Act requires federal agencies to take into account the effects of their undertakings on historic properties. We are requesting comments from you regarding the possible effects of this project. Please use the above Des. Number and project description in your reply and your comments will be incorporated into the formal environmental study.

A Section 106 early coordination letter (ECL) was distributed on December 8, 2023.

The proposed undertaking is at the intersection of SR 2 and CR 500 W in LaPorte County, Indiana, southwest of the City of La Porte between US 421 and US 35. It is within Scipio Township, on the LaPorte West, IN, USGS Topographic Quadrangle, in Sections 5, 6, 7, 8, Township 36 North, Range 3 West. The project area can be viewed online at <https://arcg.is/jqueP> (the Des. No. is the most efficient search term once in the CRO - Public Web Map App).

State Road 2 and CR 500 W are both two-lane roadways. State Road 2 is classified as a Rural Principal Arterial Other – a rural route intended to accommodate high volumes of traffic for greater distances – with 12-foot lanes, narrow shoulders, and right turn lanes onto CR 500 W at the intersection. County Road 500 W is classified as a Rural Minor Collector – a roadway intended to connect local and rural roadways and to provide service to smaller communities – with 11-foot lanes and no shoulders in the project area. The intersection is controlled with stop signs on CR 500 W.

The need for this project is evidenced by the high frequency of car crashes at this intersection. A total of 22 crashes occurred at the intersection during the study period 2017 to 2019, the majority being right and left hand turns. A RoadHAT crash analysis (traffic engineering safety modeling software) resulted in an Index of Crash Frequency of 2.96. The Index of Crash Frequency compares the crash frequency data of this intersection with intersections of similar volumes, roadway classification, and control type throughout Indiana. Crash data for this intersection resulted in an ICF of 2.96, putting it in the 99th percentile compared with similar intersections, which indicates the existing stop signs on CR 500 W are not safely controlling traffic at this intersection. Additionally, southbound CR 500 W experiences a high volume of traffic and its Level of Service (LOS) is expected to reach F (breakdown flow) prior to the design year. The LOS is on a scale from A (free flow) to F (breakdown flow). The purpose of this project is to improve safety of the SR 2 and CR 500 W intersection and provide a long-term solution that ensures safe operation and reduced congestion at this intersection.

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The proposed preferred alternative will modify the existing intersection by constructing a single-lane roundabout with a truck apron. The roundabout's location will be closely centered at the existing intersection to avoid property impacts and traffic volume. Since the distribution of the ECL, which stated the approach work is estimated to be 420 feet east and west of SR 2 and 380 feet north and south along CR 500 W, the approach work has been refined to begin approximately 1,640 feet west and terminate approximately 760 feet east of CR 500 W along SR 2, and begin approximately 300 feet north and 435 feet south of SR 2 along CR 500 W (please refer to the attached figures for maps of the new project area). Curb and gutter will be installed within the limits of the roundabout to drain excess runoff into a new detention basin to be constructed in the southwest quadrant of the roundabout. Ancillary work involves removing existing trees along the roadway, relocating overhead utilities, and lighting installation within the project area.

During the distribution of the ECL, the preliminary right-of-way amounts were 2.36 acres of permanent and 0.003 acre of temporary; however, the preliminary right-of-way amounts have changed to 1.88 acres of permanent and 0.18 acre of temporary. No relocations of properties are anticipated for this project. During construction, traffic will be maintained using lane closures and a detour. A minimum of one lane in each direction will be maintained on SR 2, and a short closure of each leg of CR 500 W is anticipated for the reconstruction of each approach. Plans for a detour will be available at a later date.

Lawson-Fisher Associates P.C. is under contract with INDOT to advance the environmental documentation for the referenced project. Metric Environmental, LLC, has been subcontracted to complete the Section 106 documentation for the project.

In accordance with 36 CFR 800.2 (c), you were invited to become a consulting party as part of the Section 106 process, or you are hereby invited to become a consulting party as part of the Section 106 process. Entities that have previously accepted consulting party status--as well as additional entities that are currently being invited to become consulting parties--are identified in the attached list.

The Section 106 process involves efforts to identify historic properties potentially affected by the undertaking, to assess the undertaking's effects and to seek ways to avoid, minimize, or mitigate any adverse effects on historic properties. For more information regarding the protection of historic resources, please see the Advisory Council on Historic Preservation's guide: *Protecting Historic Properties: A Citizen's Guide to Section 106 Review* available online at <https://www.achp.gov/sites/default/files/documents/2017-01/CitizenGuide.pdf>.

The Area of Potential Effects (APE) is the area in which the proposed project may cause alterations in the character or use of historic resources. The APE contains no resources listed in the National Register of Historic Places (NRHP).

A historian who meets the Secretary of the Interior's Professional Qualification Standards identified and evaluated above-ground resources within the APE for potential eligibility for the NRHP. As a result of the historic property identification and evaluation efforts, no above-ground resources are recommended as eligible for listing in the NRHP.

An archaeologist who meets the Secretary of the Interior's Professional Qualification Standards is conducting a survey of archaeological resources within the APE for potential eligibility for listing in the NRHP. A report of that investigation is forthcoming and will be distributed to the appropriate consulting parties for review at a later date.

In an email dated December 11, 2023, the Forest County Potawatomi Community (FCPC) accepted the invitation to become a consulting party, stated that there are No Historic Properties affected of significance to the FCPC, and asked to be contacted in the event of an Inadvertent Discovery.

In an email dated December 20, 2023, the Shawnee Tribe accepted the invitation to become a consulting party and concurred that “no known historic properties will be impacted by this project.”

In a letter dated January 4, 2024, the Indiana Deputy State Historic Preservation Officer stated that they are not aware of any additional parties who should be invited to participate in Section 106 consultation on this undertaking.

On February 12, 2024, INDOT-CRO announced, effective immediately, that the Turtle Mountain Band of Chippewa Indians will be added as Tribal Consulting Parties for projects in all Indiana counties, and they are hereby invited to serve as a consulting party to this undertaking.

The Historic Property Short Report is available for review in IN SCOPE at <https://erms12c.indot.in.gov/Section106Documents/> (the Des. No. is the most efficient search term, once in IN SCOPE). You are invited to review these documents and to respond with comments on any historic resource impacts incurred as a result of this project so that an environmental report can be completed. We also welcome your related opinions and other input to be considered in the preparation of the environmental document. If a hard copy of the materials is needed, please respond to this email with your request within seven (7) days

Please review the information and comment within thirty (30) calendar days of receipt. If you indicate that you do not desire to be a consulting party or if you have not previously accepted consulting party status and you do not respond to this letter, you will not be included on the list of consulting parties for this project and will not receive further information about the project unless the design changes.

For questions concerning specific project details, you may contact Dan Delgado of Lawson-Fisher Associates P.C., at 574-234-3167 or ddelgado@lawson-fisher.com. All future responses regarding the proposed project should be forwarded to Metric Environmental, LLC, at the following address:

Candace Hudziak
Architectural Historian
Metric Environmental, LLC
6958 Hillside Court
Indianapolis, Indiana, 46250
candaceh@metricenv.com

Tribal Contacts please respond to INDOT's Acting Tribal Liaison, Matt Coon, at mcoon@indot.in.gov (317-697-9752) with any responses pertaining to this project including to provide INDOT/Indiana FHWA additional information about Tribal resources/concerns and questions/comments regarding cultural resources. The FHWA point of contact is Kari Carmany-George at K.CarmanyGeorge@dot.gov (317-226-5629).

Sincerely,

Matthew S. Coon, Manager
Cultural Resources Office
Environmental Services

Enclosures:

Distribution List

Updated Topographic Map and Aerial Map of the Project Area

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Distribution List

New Invitees

Turtle Mountain Band of Chippewa Indians

Previous Invitees (accepted CP status)

Beth McCord
Deputy State Historic Preservation Officer
Department of Natural Resources
Division of Historic Preservation and Archaeology
402 West Washington Street, Room W274
Indianapolis, IN 46204-2739
BMCCord@dnr.IN.gov

Tribal Contacts (accepted CP status):

Forest County Potawatomi Community

Shawnee Tribe

From: Larus Longie <larus.longie@tmbci.org>

Sent: Wednesday, September 11, 2024 10:52 AM

To: Kelly, Clint <CKelly1@indot.IN.gov>

Subject: RE: FHWA Project: Des. No. 2200043; HPSR; SR 2 and CR 500 W Intersection Improvement Project, LaPorte County, Indiana

EXTERNAL EMAIL: This email was sent from outside your organization. Exercise caution when clicking links, opening attachments or taking further action, before validating its authenticity.

MBCI THPO office defer to the nearest THPO/SHPO office to the APE.

Division of Historic Preservation & Archaeology · 402 W. Washington Street, W274 · Indianapolis, IN 46204-2739
Phone 317-232-1646 · Fax 317-232-0693 · dhpa@dnr.IN.gov ·

October 10, 2024

Candace Hudziak
Architectural Historian
Metric Environmental, LLC
6958 Hillsdale Court
Indianapolis, Indiana 46250

Federal Agency: Indiana Department of Transportation ("INDOT"),
on behalf of Federal Highway Administration, Indiana Division ("FHWA")

Re: Historic property short report (Vorndran/Receveur, 9/10/24) for the State Road 2 and County Road 500 West
Intersection Improvement Project (Des. No. 2200043; DHPA No. 31664)

Dear Ms. Hudziak:

Pursuant to Section 106 of the National Historic Preservation Act of 1966, as amended (54 U.S.C. § 306108), 36 C.F.R. Part 800, and the "Programmatic Agreement (PA) Among the Federal Highway Administration, the Indiana Department of Transportation, the Advisory Council on Historic Preservation and the Indiana State Historic Preservation Officer Regarding the Implementation of the Federal Aid Highway Program In the State of Indiana," the staff of the Indiana State Historic Preservation Officer ("Indiana SHPO") has reviewed your September 10, 2024, submission which enclosed the Historic property short report ("HPSR", Vorndran/Receveur, 9/10/24), a; of which was received by our office September 10, 2024, for the above-referenced project, which has been proposed for areas located in Scipio Township, LaPorte County, Indiana.

The area of potential effects ("APE") proposed in the HPSR appears to be of adequate size to encompass the geographic area in which direct and indirect effects of a project of this nature could occur.

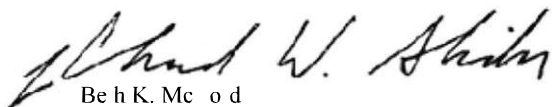
For the purposes of the Section 106 review of this federal undertaking, we agree with the conclusions in the HPSR that there are no previously recorded National Register of Historic Places ("NRHP") listed or eligible resources located within the APE. Furthermore, we also agree with the conclusions in the HPSR of the Contributing-rated status of the other historic-aged resources documented during survey and that there are no resources listed or eligible for the NRHP within the APE.

As INDOT's September 10, 2024, letter indicates, the report on investigations of archaeological resources is forthcoming. We look forward to reviewing and commenting on that report.

The Indiana SHPO staff's archaeological reviewer for this project is Wade T. Tharp, and the structures reviewer is Toni Lynn Giffin. However, if you have a question about the Section 106 process, please contact initially the INDOT Cultural Resources staff members who are assigned to this project.

In all future correspondence about the State Road 2 and County Road 500 West Intersection Improvement Project (Des. No. 2200043), which is proposed for areas located in Scipio Township, LaPorte County, Indiana, please refer to DHPA No. 31664.

Very truly yours,



Beth K. McLeod
Deputy State Historic Preservation Officer

BKM:TLG:WTT:wt

emc: Matt Coon, Ph.D., Indiana Department of Transportation
Susan Branigin, Indiana Department of Transportation
Candace Hudziak, Metric Environmental, LLC
Toni Lynn Giffin, Indiana DNR-DHPA
Wade T. Tharp, Indiana DNR-DHPA

From: [Laserfiche Notification](#)
To: [Coon, Matthew](#)
Subject: Section 106 Consultation - Des. No. 2200043; HPSR; SR 2 and CR 500 W Intersection Improvement Project
Date: Tuesday, November 12, 2024 4:13:02 PM

EXTERNAL EMAIL: This email was sent from outside your organization. Exercise caution when clicking links, opening attachments or taking further action, before validating its authenticity.

This email is in response to Des. No. 2200043; HPSR; SR 2 and CR 500 W Intersection Improvement Project.

The Shawnee Tribe's Tribal Historic Preservation Department concurs that no known historic properties will be negatively impacted by this project. However, there is still potential for the discovery of unknown resources.

We have no issues or concerns at this time. Please continue with the project as planned, but in the event archaeological materials are encountered during construction, use, or maintenance of this location, please re-notify us at that time as we would like to resume immediate consultation under such a circumstance.

If you have any questions, you may contact me via email at Section106@shawnee-tribe.com

Thank you for giving us the opportunity to comment on this project





REVIEW REQUEST SUBMITTAL

State Form 55031 (R2 / 3-23)

Indiana Department of Natural Resources

Division of Historic Preservation and Archaeology, Indiana State Historic Preservation Office (SHPO)



Please complete this form and attach it to the front of all submittals, along with any reports or supplemental materials you are providing to the Indiana DHPA for review. Please note that archaeological forms and reports shall be submitted in SHAARD, separate from structural information since archaeological site locations are confidential and not for public disclosure.

Date (month, day, year): March 12, 2025

- ☒ This is a new submittal.
☐ This is revised/additional information relating to DHPA number _____.
☐ This project is being undertaken pursuant to the terms and conditions of a programmatic or other interagency agreement.
Title of Agreement: _____
☐ This project will also be applying for Federal Rehabilitation Investment Tax Credit.
☐ This project includes work on a property that is under a preservation covenant held by DHPA.

THIS REVIEW REQUEST SUBMITTED BY:

Name: Samuel P Snell MS, RPA
Company/Organization: Metric Environmental, LLC
Address (number and street): 6958 Hillsdale Court
City: Indianapolis State: IN ZIP: 46250
Telephone number: 317-912-3499 E-mail address: sams@metricenv.com

PROJECT NAME & LOCATION [Please attach a map with location(s) marked]

Project Name/Reference: SR 2 and CR 500 Intersection Improvement Project/Des Number: 2200043
Project Address/Location: 1.99 mi South of I-90
City: _____ Township(s): Prairie
County/Countries: LaPorte
Section/Township/Range: Sections 5,6,7,8 Township 36N Range 3W
Latitude/Longitude: _____

STATE OR FEDERAL AGENCY INVOLVEMENT

Agency: _____ Program: _____
Type of funds, license, or permit to be obtained (if applicable): _____
Name of Agency Contact: _____
Address (number and street): _____
City: _____ State: _____ ZIP: _____
Telephone number: _____ E-mail address: _____

APPLICANT (if different than Federal Agency) *If available, please attach copy of authorization letter from federal agency.*

Applicant: Lawson-Fisher Associates P.C.

Name of Contact: Dan Delgado

Address (number and street): 525 West Washington Avenue,

City: South Bend State: IN ZIP: 46601

Telephone number: (574) 234-3167

E-mail address: ddelgado@lawson-fisher.com

ADDITIONAL CONTACT (IF APPLICABLE)

Name of Contact: _____

Organization/Agency: _____

Address (number and street): _____

City: _____ State: _____ ZIP: _____

Telephone number: _____ E-mail address: _____

Project Description – This should include a detailed scope of work, including any actions to be taken in relation to the project, such as all aspects of new construction, replacement/repair, demolition, ground disturbance, and all ancillary work (temporary roads, etc.), as applicable. Attach report or additional pages if necessary. If a detailed scope of work is not available yet, please explain and include all preliminary information.

The proposed project involves improvements at the intersection of State Road (SR) 2 and County Road (CR) 500 West (W) in Scipio Township, LaPorte County, Indiana. The project area will extend approximately 499.87 meters (m) (1,640 feet [ft]) west and 231.65 m (760 ft) east of CR 500 W along SR 2 as well as 91.44 m (300 ft) north and 132.56 m (435 ft) south of SR 2 along CR 500 W. The distance from the edge of pavement along the existing roads will vary from 7.62 to 27.43 m (25 to 90 ft). The existing intersection has exhibited a high frequency of car crashes, falling within the 99th percentile of crash occurrences with similar intersections. County Road 500 West's level of service is also expected to reach a "F", or breakdown flow, rating due to increasingly high volume traffic.

The proposed preferred alternative will modify the existing intersection by constructing a single-lane roundabout with a truck apron. The roundabout's location will be closely centered at the existing intersection to avoid property impacts and traffic volume. The approaches will be reconstructed along CR 500 W. Curb and gutter will be installed within the limits of the roundabout to drain excess runoff into a new detention basin to be constructed in the southwest quadrant of the roundabout. Ancillary work involves removing existing trees along the roadway, relocating overhead utilities, and lighting installation within the project. Preliminary right-of-way (ROW) amounts are 0.76 hectares (ha) (1.88 acres [ac]) of permanent ROW and 0.07 ha (0.18 ac) of temporary ROW.

The purpose of this project is to ensure greater safety of the SR 2 and CR 500 W intersection and provide a long-term solution to safer operations and reduced congestion. The need for this project derives from the high frequency of crashes occurring at the intersection and its predicted Level of Service F grading.

The proposed project encompasses an area of 3.9 ha (9.64 ac), which corresponds to the Phase Ia survey area.

Ground Disturbing Activity – This should include a detailed description of all proposed horizontal and vertical ground disturbance in relation to the project as well as any known previous and current land use, condition, and disturbances. Attach additional pages if necessary. Indicate if the project does not include any ground disturbing activities. Please note that agricultural tilling generally does not have a serious enough impact on archaeological sites to constitute a disturbance of the ground for this purpose.

ARCHAEOLOGICAL INVESTIGATION

If an archaeological investigation has been conducted for this project, the resulting report and site forms (if applicable) must be submitted in the State Historic Architectural and Archaeological Research Database (SHAARD) by the qualified archaeologist. DO NOT attach the archaeology report here.

For an archaeological report submitted in SHAARD, please indicate the SHAARD report number (i.e. AR-xx-xxxxxx).

SHAARD report number AR-46-00443

FINDINGS – Please note that a finding should only be submitted when the agency/delegatee believes it is appropriate or one has been requested by our office. Only those who represent the Federal Agency or an official delegatee of the federal agency are authorized to make findings of effect for an undertaking.

☐ **No Historic Properties Affected** – (i.e., none are present or there are historic properties present but the project will have no effect upon them). Attach necessary documentation, as described at 36 CFR 800.11.

☐ **No Adverse Effect** – The proposed undertaking will have no adverse effect on one or more historic properties located within the project APE under 36 CFR 800.5. Attach necessary documentation, as described at 36 CFR 800.11.

☐ **Adverse Effect** – The proposed undertaking will result in an adverse effect to one or more historic properties and the applicant, or other federally authorized representative, will consult with the SHPO and other consulting parties to resolve the adverse effect per 36 CFR 800.6. Attach necessary documentation, as described at 36 CFR 800.11, with a proposed plan to resolve adverse effect(s).

Please explain the basis for your determination.

Authorized Signature: Samuel P. Snell Date (month, day, year): 3/12/2025
Type or print name: Samuel P. Snell, MS, RPA
Organization/Agency: Metric Environmental, LLC

Please note that incomplete submissions may result in delays. To ensure an expeditious review, please be sure that the following has been provided:

- ☐ Completed Review Request Submittal Form
- ☐ Letter of authorization from Federal agency/agencies (if applicable)
- ☐ Consulting Parties – List of all consulting parties that have been invited to participate and copies of any responses received. Typical consulting parties would include the county historian, local historical society, the appropriate regional office of Indiana Landmarks, other local, state or national preservation organizations, tribes, local government and the general public.
- ☐ Map of project location with project area(s) and Area of Potential Effects (APE) clearly marked, streets labeled and a north arrow, aerial maps are preferable and areas of previous and proposed ground disturbance within the project area should be shown. Please indicate if any of the project area is located on state or federal property.
- ☐ Clear, current color photographs of project area and APE, including any buildings or structures fifty (50) years or older within the APE. (PDF format, no more than two (2) photographs per page). For large project areas/APEs, photo size may be reduced, or photos and photo log provided to DHPA approved file sharing or ftp site. Contact DHPA for current list of accepted sites.
- ☐ Architectural/Engineering Drawings (if applicable) – Must be labeled with north arrow, clearly indicate proposed changes to existing buildings and locations of any ground disturbance on site plans. Include both existing and proposed drawings/plans in PDF format. For large projects, plans may be provided by a DHPA approved file sharing or ftp site and/or hard copies no smaller than 11" x 17" with legible font.
- ☐ Identification of any known historic resources – All projects should consult the SHAARD database (access available on the DHPA home page) to locate known historic resources in the project area and APE. For any identified resources, the submission should include (in summary form) a list of the properties identified, including address, the site/reference number from SHAARD, the rating (IHSSI, Bridge Inventory) or status (National Register) of each property, and a current photograph. Please do not submit print outs of the individual SHAARD records.
- ☐ Archaeological report and site forms (if applicable) submitted in SHAARD for DHPA review. Do not attach to this form.
- ☐ Projects using State of Indiana funds to alter, demolish or remove a historic site or structure, include Application for a Certificate of Approval (SF 52889).

The thirty (30) day review period, as specified in 36 CFR part 800.3(c)(4), begins from the date that we receive the complete submission.

Return this Form and Attachments to:

**Indiana Department of Natural Resources
Division of Historic Preservation and Archaeology
402 W. Washington Street, Room W274
Indianapolis, Indiana 46204**

<http://www.in.gov/dnr/historic>

From: [Blum, Kaylee](#)
To: [Sam Snell](#)
Cc: [Coon, Matthew](#); [Branigin, Susan](#); [Kelly, Clint](#); [Susan Castle](#); [Elayna Stoner](#); [Dmitri Adams](#); [Grylewicz, Michael J](#); [Blum, Kaylee](#)
Subject: RE: Des 2200043 SR 2 and CR 500 Intersection Project Archaeology submission
Date: Wednesday, March 12, 2025 1:45:29 PM
Attachments: [image004.png](#)
[image007.png](#)

Good afternoon,

Thank you for submitting the revised archaeological report which has been reviewed by INDOT Cultural Resources personnel who meet the Secretary of the Interior's Professional Qualification Standards as per 36 CFR Part 61. It is our opinion that the report is acceptable, and we concur with the evaluations and recommendations made by the author (Peters et al. 2025).

I have very minor comments; please address those and update the date of the report prior to uploading to IN SCOPE.

Please upload the revised report to SHAARD and also upload to IN SCOPE for a 30-day review period, making sure that the project information is consistent with the other documents already in IN SCOPE. (Please let us know if any information in IN SCOPE needs to be corrected.) Once released in IN SCOPE, please email one PDF copy of the current Review Request Submittal Form to dhpareview@dnr.IN.gov and forward one hard copy of the form to SHPO, copying the assigned CRO reviewer on the email submission. When we receive that email, CRO will notify the Tribes. If there are any questions or concerns regarding this matter, please contact KayLee Blum by kblum@indot.in.gov or at 317-439-3337.

[SR2andCR500WIntersectionProject_Des2200043_Phase1a_INDOTcomments.pdf](#)

Best,

KayLee A. Blum, M.S.

Archaeologist

Indiana Department of Transportation, Cultural Resources Office
100 North Senate Ave., N758 — Environmental Services
Indianapolis, IN 46204

Office: (317) 439-3337

Email: kblum@indot.in.gov



From: Sam Snell <sams@metricenv.com>

Sent: Monday, October 7, 2024 2:45 PM

To: esdcro@indot.in.gov

Cc: Susan Castle <susanc@metricenv.com>; Elayna Stoner <elaynas@metricenv.com>; Dmitri Adams <dadams@lawson-fisher.com>

Subject: Des 2200043 SR 2 and CR 500 Intersection Project Archaeology submission

Hello,

Please find the linked below for your review.

LPA Project: No

Historic Bridge Project: No

Level of Section 106: Full

Reason why MPPA does not apply to a roundabout

Potential for Section 106 adverse effects: unknown

Potential for Section 4(f) use of historic property: unknown

Items to review: Phase Ia and shapefile

[SR2andCR500WIntersectionProject_Des2200043_PhaseIAGISFile_2024-10-04.zip](#)

[SR2andCR500WIntersectionProject_Des2200043_PhaseIA_2024-10-04.pdf](#)

Sam



Samuel P. Snell, MS, RPA

Senior Project Manager/Archaeological Principal Investigator

O 317.912.3499

M 317.450.9175

6958 Hillside Court
Indianapolis, IN 46250
www.metricenv.com

Certified DBE/MBE/SBE Company
Indiana | Kentucky | Ohio | West Virginia | Pennsylvania

Division of Historic Preservation & Archaeology · 402 W. Washington Street, W274 · Indianapolis, IN 46204-2739
Phone 317-232-1646 · Fax 317-232-0693 · dhpa@dnr.IN.gov ·

April 11, 2025

Samuel P. Snell, MS, RPA
Metric Environmental, LLC
6958 Hillsdale Court
Indianapolis, Indiana 46250

Federal Agency: Indiana Department of Transportation (“INDOT”),
on behalf of Federal Highway Administration, Indiana Division (“FHWA”)

Re: Indiana Archaeological Short Report (Peters et al., 03/12/2025) for the State Road 2 and
County Road 500 West Intersection Improvement Project (Des. No. 2200043; DHPA
No. 31664)

Dear Mr. Snell:

Pursuant to Section 106 of the National Historic Preservation Act of 1966, as amended (54 U.S.C. § 306108), 36 C.F.R. Part 800, and the “Programmatic Agreement (PA) Among the Federal Highway Administration, the Indiana Department of Transportation, the Advisory Council on Historic Preservation and the Indiana State Historic Preservation Officer Regarding the Implementation of the Federal Aid Highway Program In the State of Indiana,” the staff of the Indiana State Historic Preservation Officer (“Indiana SHPO”) has reviewed your March 12, 2025, submission, which enclosed the Indiana Archaeological Short Report (Peters et al., 03/12/2025), and which was received by our office March 12, 2025, for the above-referenced project, which has been proposed for areas located at Scipio Township, in LaPorte County, Indiana.

In terms of archaeological resources, based on the submitted information and the documentation available to the staff of the Indiana DNR-DHPA, we have not identified any currently known archaeological resources listed in or eligible for inclusion in the National Register of Historic Places (“NRHP”) within the proposed project area; and we concur with the opinion of the archaeologist, as expressed in the Indiana Archaeological Short Report (Peters et al., 03/12/2025), that no further archaeological investigations appear necessary at the proposed project area.

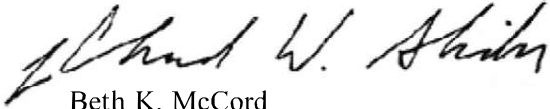
Thank you for uploading the Indiana Archaeological Short Report (Peters et al., 03/12/2025) to the Indiana DNR-DHPA SHAARD system database. It has been assigned the identifier *AR-46-00443*, and it has been reviewed and approved.

If any prehistoric or historic archaeological artifacts or human remains are uncovered during construction, demolition, or earthmoving activities, state law (Indiana Code 14-21-1-27 and Indiana Code 14-21-1-29) requires that the discovery be reported to the Indiana Department of Natural Resources, Division of Historic Preservation and Archaeology within two (2) business days. In that event, please call (317) 232-1646. Be advised that adherence to Indiana Code 14-21-1-27 and Indiana Code 14-21-1-29 does not obviate the need to adhere to applicable federal statutes and regulations, including but not limited to 36 C.F.R. Part 800.

The Indiana SHPO staff's archaeological reviewer for this project is Wade T. Tharp, and the structures reviewer is Toni Lynn Giffin. However, if you have a question about the Section 106 process, please contact initially the INDOT Cultural Resources staff members who are assigned to this project.

In all future correspondence about the State Road 2 and County Road 500 West Intersection Improvement Project (Des. No. 2200043), which is proposed for areas located in Scipio Township, LaPorte County, Indiana, please refer to DHPA No. 31664.

Very truly yours,

A handwritten signature in black ink, appearing to read "Beth K. McCord". The signature is fluid and cursive, with the first name "Beth" being more prominent.

Beth K. McCord
Deputy State Historic Preservation Officer

BKM:WTT:wt

emc: Matt Coon, Ph.D., Indiana Department of Transportation
Susan Branigin, Indiana Department of Transportation
Samuel P. Snell, Metric Environmental, LLC
Candace Hudziak, Metric Environmental, LLC
Toni Lynn Giffin, Indiana DNR-DHPA
Wade T. Tharp, Indiana DNR-DHPA

From: [Haley Receveur](#)
To: [McCord, Beth K](#)
Cc: [Branigin, Susan](#); [Blum, Kaylee](#); [Coon, Matthew](#); [Candace Hudziak](#); [Kyle Wertz](#); [Sam Snell](#); [Zoe Vorndran](#); [Michels, Stewart](#); [Grylewicz, Michael J](#); [Susan Castle](#); [Dmitri Adams](#); [Lisa Harris](#)
Subject: FHWA Project: Des. No. 2200043; No Historic Properties Affected Finding, State Road 2 and County Road 500 West Intersection Improvement Project, LaPorte County, Indiana
Date: Thursday, July 3, 2025 2:21:00 PM

Des. No.: 2200043

Project Description: State Road 2 and County Road 500 West Intersection Improvement Project

Location: Scipio Township, LaPorte County, Indiana

The Indiana Department of Transportation (INDOT), with funding from the Federal Highway Administration (FHWA), proposes to proceed with the State Road (SR) 2 and County Road (CR) 500 West (W) Intersection Improvement Project, Des. No. 2200043.

INDOT, on behalf of FHWA, has signed a determination of “No Historic Properties Affected” for this Section 106 undertaking. In accordance with 36 CFR 800.4(d), you and the other Section 106 consulting parties are being provided the documentation for this finding. You can view the determination of “No Historic Properties Affected” electronically by accessing INDOT’s Section 106 document posting website IN SCOPE at <http://erms12c.indot.in.gov/Section106Documents/> (the Des. No. is the most efficient search term, once in IN SCOPE). If a hard copy of the materials is needed, please respond to this email with your request within seven (7) days.

Consulting parties have thirty (30) calendar days from receipt of this information to review and provide comment.

Tribal Nation representatives please respond to INDOT’s Tribal Liaison, Taylor Payne, at tapayne@indot.in.gov (317-779-5775) with any responses pertaining to this project, including to provide INDOT/Indiana FHWA additional information about Tribal resources/concerns and questions/comments regarding cultural resources. The FHWA point of contact is Erica Tait at erica.tait@dot.gov (317-226-7481).

Thank you in advance for your input,



Haley Receveur

Staff Scientist – Architectural Historian

O 317.214.8258
M 812.786.1597

6958 Hillsdale Court
Indianapolis, IN 46250

From: [Kelly, Clint](#)
To: luke.heider@fcp-nsn.gov; [section106](#); [Larus Longie](#)
Cc: [Branigin, Susan](#); [Payne, Taylor](#); [Grylewicz, Michael J](#); [Haley Receveur](#); [Tait, Erica \(FHWA\)](#)
Subject: FHWA Project: Des. No. 2200043; No Historic Properties Affected Finding, State Road 2 and County Road 500 West Intersection Improvement Project, LaPorte County, Indiana
Date: Thursday, July 3, 2025 3:02:53 PM
Attachments: [image001.png](#)
[image002.png](#)

Des. No.: 2200043


Project Description: State Road 2 and County Road 500 West Intersection Improvement Project

Location: Scipio Township, LaPorte County, Indiana

The Indiana Department of Transportation (INDOT), with funding from the Federal Highway Administration (FHWA), proposes to proceed with the State Road (SR) 2 and County Road (CR) 500 West (W) Intersection Improvement Project, Des. No. 2200043.

INDOT, on behalf of FHWA, has signed a determination of “No Historic Properties Affected” for this Section 106 undertaking. In accordance with 36 CFR 800.4(d), you and the other Section 106 consulting parties are being provided the documentation for this finding. You can view the determination of “No Historic Properties Affected” electronically by accessing INDOT’s Section 106 document posting website IN SCOPE at <http://erms12c.indot.in.gov/Section106Documents/> (the Des. No. is the most efficient search term, once in IN SCOPE). If a hard copy of the materials is needed, please respond to this email with your request within seven (7) days.

Consulting parties have thirty (30) calendar days from receipt of this information to review and provide comment.

Tribal Nation representatives please respond to INDOT’s Tribal Liaison, Taylor Payne, at tapayne@indot.in.gov (317-779-5775) with any responses pertaining to this project, including to provide INDOT/Indiana FHWA additional information about Tribal resources/concerns and questions/comments regarding cultural resources. The FHWA point of contact is Erica Tait at erica.tait@dot.gov (317-226-7481). Tribal Nation representatives who do not utilize IN SCOPE can find a copy of the documentation  [HERE](#).

Thank you in advance for your input,

Clint Kelly

Section 106 Specialist/Historian

Cultural Resources Office

Environmental Services

100 N. Senate Ave., Rm. N758-ES

Indianapolis, IN 46204

Office: (317) 447-8707

Email: ckelly1@indot.in.gov

Core Office Hours: M-F 7:30-3:30

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INDIANA DEPARTMENT OF TRANSPORTATION

100 North Senate Avenue
Room N758-ES
Indianapolis, Indiana 46204

PHONE: (855) 463-6848

Mike Braun, Governor
Lindsay Quist, Commissioner

July 3, 2025

This letter was sent to the listed parties.

RE: State Road 2 and County Road 500 West Intersection Improvement Project
Des. No. 2200043
Scipio Township, LaPorte County, Indiana

Dear Consulting Party,

The Indiana Department of Transportation (INDOT), with funding from the Federal Highway Administration (FHWA), proposes to proceed with the State Road (SR) 2 and County Road (CR) 500 West (W) Intersection Improvement Project, Des. No. 2200043.

This letter is part of the Section 106 review process for this project. Section 106 of the National Historic Preservation Act requires federal agencies to take into account the effects of their undertakings on historic properties. We are requesting comments from you regarding the possible effects of this project. Please use the above Des. Number and project description in your reply and your comments will be incorporated into the formal environmental study.

A Section 106 early coordination letter for this project was distributed on December 8, 2023. In addition, a letter distributed on September 10, 2024 notified consulting parties that a historic property short report (HPSR) was available for review and comment. The Archaeology Report (Tribes only) (ASR) was distributed without a distribution letter on March 12, 2025 to consulting parties.

The proposed undertaking is at the intersection of SR 2 and CR 500 W in LaPorte County, Indiana, southwest of the City of La Porte between United States Highway (US) 421 and US 35. It is within Scipio Township, on the LaPorte West, IN, USGS Topographic Quadrangle, in Sections 5, 6, 7, 8, Township 36 North, Range 3 West. The project area can be viewed online at <https://arcg.is/jqueP> (the Des. No. is the most efficient search term once in the CRO - Public Web Map App).

State Road 2 and CR 500 W are both two-lane roadways. State Road 2 is classified as a Rural Principal Arterial Other – a rural route intended to accommodate high volumes of traffic for greater distances – with 12-foot lanes, narrow shoulders, and right turn lanes onto CR 500 W at the intersection. County Road 500 W is classified as a Rural Minor Collector – a roadway intended to connect local and rural roadways and to provide service to smaller communities – with 11-foot lanes and no shoulders in the project area. The intersection is controlled with stop signs on S CR 500 W.

The need for this project is evidenced by the high frequency of car crashes at this intersection. A total of 22 crashes occurred at the intersection during the study period 2017 to 2019, the majority being right and left hand turns. A RoadHAT crash analysis (traffic engineering safety modeling software) resulted in an Index of Crash Frequency of 2.96. The Index of Crash Frequency (ICF) compares the crash frequency data of this intersection with intersections of similar volumes, roadway classification, and control type throughout Indiana. Crash data for this intersection resulted in an ICF of 2.96, putting it in the 99th percentile compared with similar intersections, which indicates the existing stop signs on CR 500 W are not safely controlling traffic at this intersection. Additionally, southbound CR 500 W experiences a high volume of traffic and its Level of Service (LOS) is expected to reach F (breakdown flow) prior to the design year. The LOS is on a scale from A (free flow) to F (breakdown flow). The purpose of this project is to improve safety of the SR 2 and CR 500 W intersection and provide a long-term solution that ensures safe operation and reduced congestion at this intersection.

The proposed preferred alternative will modify the existing intersection by constructing a single-lane roundabout with a truck apron at the existing intersection of SR 2 and CR 500 W. The roundabout's location will be closely centered at the existing intersection to avoid property impacts and traffic volume. The approach work is estimated to begin approximately 1,640 feet west and terminate approximately 760 feet east of CR 500 W along SR 2, and begin approximately 300 feet north and 435 feet south of SR 2 along CR 500 W. Curb and gutter will be installed within the limits of the roundabout to drain excess runoff into a new detention basin to be constructed in the southwest quadrant of the roundabout. *Please note, since the distribution of the HPSR and Archaeology Report (Tribes only), the project limits in the southwest quadrant of the intersection have expanded to accommodate the new detention basin (refer to the figures attached to the RDL).* Ancillary work involves removing existing trees along the roadway, relocating overhead utilities, lighting installation, and reconstructing a driveway within the project area.

The distribution letter to the HPSR stated the project would acquire 1.88 acres of permanent right-of-way and 0.18 acre of temporary right-of-way; however, the right-of-way amounts have been refined to 2.020 acres of permanent right-of-way and 3.619 acres of reacquired permanent right-of-way (totaling 5.639 acres), and 0.119 acre of temporary right-of-way. No relocations of properties are anticipated for this project. During construction, traffic will be maintained using lane closures and a detour. A minimum of one lane in each direction will be maintained on SR 2, and a short closure of each leg of CR 500 W is anticipated for the reconstruction of each approach.

Lawson-Fisher Associates P.C. is under contract with INDOT to advance the environmental documentation for the referenced project. Metric Environmental, LLC, has been subcontracted to complete the Section 106 documentation for the project.

In accordance with 36 CFR 800.2(c), you were invited to become a consulting party as part of the Section 106 process, or you are hereby invited to become a consulting party as part of the Section 106 process. Entities that have previously accepted consulting party status--as well as additional entities that are currently being invited to become consulting parties--are identified in the attached list.

The Section 106 process involves efforts to identify historic properties potentially affected by the undertaking, to assess the undertaking's effects and to seek ways to avoid, minimize, or mitigate any adverse effects on historic properties. For more information regarding the protection of historic resources, please see the Advisory

Council on Historic Preservation's guide: *Protecting Historic Properties: A Citizen's Guide to Section 106 Review* available online at <https://www.achp.gov/sites/default/files/documents/2017-01/CitizenGuide.pdf>.

The Area of Potential Effects (APE) is the area in which the proposed project may cause alterations in the character or use of historic resources. The APE contains no resources listed in the National Register of Historic Places (NRHP).

A historian who meets the Secretary of the Interior's Professional Qualification Standards identified and evaluated above-ground resources within the APE for potential eligibility for the NRHP. As a result of the historic property identification and evaluation efforts, no above-ground resources are recommended as eligible for listing in the NRHP.

With regard to archaeological resources, an archaeologist who meets the Secretary of the Interior's Professional Qualification Standards identified no sites within the project area. As a result of these efforts, no sites were recommended eligible for listing in the NRHP and no further work is recommended.

In an email dated September 11, 2024, the Turtle Mountain Band of Chippewa Indians stated, "MBCI THPO office defer to the nearest THPO/SHPO office to the APE."

In a letter dated October 10, 2024, the Indiana State Historic Preservation Office (SHPO) stated the APE "proposed in the HPSR appears to be of adequate size to encompass the geographic area in which direct and indirect effects of a project of this nature could occur." Further, SHPO agreed "with the conclusions in the HPSR that there are no previously recorded [NRHP] listed or eligible resources located within the APE."

In an email dated November 12, 2024, the Shawnee Tribe concurred "that no known historic properties will be negatively impacted by this project," though they noted that "there is still potential for the discovery of unknown resources."

In a letter dated April 11, 2025, the SHPO acknowledged receipt of the ASR. The SHPO concurred with the ASR that no further archaeological work is necessary for this project. Copies of all consulting party correspondence can be found in Appendix E in the attached documentation.

The "No Historic Properties Affected" finding and 800.11 documentation is available for review in IN SCOPE at <http://erms12c.indot.in.gov/Section106Documents/> (the Des. No. is the most efficient search term, once in IN SCOPE). You are invited to review these documents and to respond with comments on any cultural resource impacts incurred as a result of this project. We also welcome your related opinions and other input to be considered in the preparation of the environmental document. If a hard copy of the materials is needed, please respond to this email with your request within seven (7) days.

Please review the information and comment within thirty (30) calendar days of receipt. If you indicate that you do not desire to be a consulting party or if you have not previously accepted consulting party status and you do not respond to this letter, you will not be included on the list of consulting parties for this project and will not receive further information about the project unless the design changes.

All future responses regarding the proposed project, including questions concerning specific project details, should be forwarded to:

Zoe Vorndran
Qualified Professional Historian
Metric Environmental, LLC
6958 Hillsdale Court
Indianapolis, Indiana, 46250
zoev@metricenv.com
317-504-4296

Tribal Nation representatives please respond to INDOT's Tribal Liaison, Taylor Payne tapayne@indot.in.gov; (317-779-5775) with any responses pertaining to this project including to provide INDOT/Indiana FHWA additional information about Tribal resources/concerns and questions/comments regarding cultural resources. The FHWA point of contact is Erica Tait at erica.tait@dot.gov (317-226-7481).

Sincerely,

A handwritten signature in black ink, appearing to read 'Matthew S. Coon', with a stylized flourish extending to the right.

Matthew S. Coon, Manager
Cultural Resources Office
Environmental Services

Enclosures:

Distribution List

Maps Depicting Updated Project Area

Distribution List

Previous Invitees (accepted CP status)

Beth McCord
Deputy State Historic Preservation Officer
Department of Natural Resources
Division of Historic Preservation and Archaeology
402 West Washington Street, Room W274
Indianapolis, IN 46204-2739
BMCCord@dnr.IN.gov

Tribal Consulting Party Contacts

Forest County Potawatomi Community

Shawnee Tribe

Turtle Mountain Band of Chippewa Indians

Haley Receveur

From: Payne, Taylor <TaPayne@indot.IN.gov>
Sent: Friday, July 11, 2025 2:40 PM
To: Haley Receveur
Cc: Kelly, Clint; Blum, Kaylee
Subject: FW: Section 106 Consultation - Des. No. 2200043

Hi Haley,

Please see attached response from The Shawnee Tribe.

Best,

Taylor Payne (she/her)

Tribal Liaison

Architectural Historian

Indiana Department of Transportation

100 North Senate Ave., N758 — ES

Indianapolis, IN 46204

Cell: : (317) 779-5775

Email: tapayne@indot.in.gov

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****For the latest updates from INDOT's Cultural Resources Office, subscribe to the Environmental Services listserv:** <https://www.in.gov/indot/3217.htm>

****Link to the CRO-Public Web Map App can be found [here](#).**

From: Laserfiche Notification <donotreply@laserfiche.com>

Sent: Wednesday, July 9, 2025 2:41 PM

To: Payne, Taylor <TaPayne@indot.IN.gov>

Subject: Section 106 Consultation - Des. No. 2200043

EXTERNAL EMAIL: This email was sent from outside your organization. Exercise caution when clicking links, opening attachments or taking further action, before validating its authenticity.

This email is in response to Des. No. 2200043.

The Shawnee Tribe's Tribal Historic Preservation Department concurs that no known historic properties will be negatively impacted by this project. However, there is still potential for the discovery of unknown resources.

We have no issues or concerns at this time. Please continue with the project as planned, but in the event archaeological materials are encountered during construction, use, or maintenance of this location, please re-notify us at that time as we would like to resume immediate consultation under such a circumstance.

If you have any questions, you may contact me via email at Section106@shawnee-tribe.com

Thank you for giving us the opportunity to comment on this project



Erin Paden

TRIBAL HISTORIC PRESERVATION
SPECIALIST

Office: (918) 542-2441, x140

Email: epaden@shawnee-tribe.com

29 S Hwy 69A

Miami, OK 74354

shawnee-tribe.com

Haley Receveur

From: Kelly, Clint <CKelly1@indot.IN.gov>
Sent: Thursday, August 7, 2025 11:44 AM
To: Payne, Taylor; Haley Receveur
Cc: Branigin, Susan
Subject: FW: FHWA Project: Des. No. 2200043; No Historic Properties Affected Finding, State Road 2 and County Road 500 West Intersection Improvement Project, LaPorte County, Indiana

Please find a response from the FCPC below.

Clint Kelly

Section 106 Specialist/Historian

Cultural Resources Office

Environmental Services

100 N. Senate Ave., Rm. N758-ES

Indianapolis, IN 46204

Office: (317) 447-8707

Email: ckelly1@indot.in.gov

Core Office Hours: M-F 7:30-3:30

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From: Luke Heider <Luke.Heider@fcp-nsn.gov>
Sent: Wednesday, August 6, 2025 1:04 PM
To: Kelly, Clint <CKelly1@indot.IN.gov>
Subject: RE: FHWA Project: Des. No. 2200043; No Historic Properties Affected Finding, State Road 2 and County Road 500 West Intersection Improvement Project, LaPorte County, Indiana

EXTERNAL EMAIL: This email was sent from outside your organization. Exercise caution when clicking links, opening attachments or taking further action, before validating its authenticity.

Pursuant to consultation under Section 106 of the National Historic Preservation Act (1966 as amended) the Forest County Potawatomi Community (FCPC), a Federally Recognized Native American Tribe, reserves the right to comment on Federal undertakings, as defined under the act.

The Tribal Historic Preservation Office (THPO) staff has reviewed the information you provided for the project. Upon review of site data and supplemental cultural history within our Office, the FCPC THPO is pleased to offer a finding of No Historic Properties affected of significance to the FCPC, however, **we do wish to remain as a consulting party for this project.**

As a standard caveat sent with each proposed project reviewed by the FCPC THPO, the following applies. In the event an Inadvertent Discovery (ID) occurs at any phase of a project or undertaking as defined, and human remains or archaeological materials are exposed as a result of project activities, work should cease immediately, and the Tribe(s) must be included with the SHPO in any consultation regarding treatment and disposition of the find.

Thank you for protecting cultural and historic properties and if you have any questions or concerns, please contact me at the email or number listed below.

Best,

Luke Heider | Tribal Historic Preservation Officer | Land & Natural Resources

Forest County Potawatomi | 5320 Wensaut Lane | PO Box 340, Crandon, WI 54520

P: 715-478-7354 | C: 715-889-0202 | Main: 715-478-7222

www.fcpotawatomi.com | luke.heider@fcp-nsn.gov

Please note the office hours are Monday – Thursday: 7:00 am – 5:00 pm. Our office is closed on Fridays

From: Kelly, Clint <CKelly1@indot.IN.gov>

Sent: Thursday, July 3, 2025 2:03 PM

To: Luke Heider <Luke.Heider@fcp-nsn.gov>; section106 <section106@shawnee-tribe.com>; Larus Longie <larus.longie@tmbci.org>

Cc: Branigin, Susan <SBranigin@indot.IN.gov>; Payne, Taylor <TaPayne@indot.IN.gov>; Grylewicz, Michael J <MGrylewicz@indot.IN.gov>; haley@metricenv.com; Tait, Erica (FHWA) <erica.tait@dot.gov>

Subject: FHWA Project: Des. No. 2200043; No Historic Properties Affected Finding, State Road 2 and County Road 500 West Intersection Improvement Project, LaPorte County, Indiana

Des. No.: 2200043


Project Description: State Road 2 and County Road 500 West Intersection Improvement Project

Location: Scipio Township, LaPorte County, Indiana

The Indiana Department of Transportation (INDOT), with funding from the Federal Highway Administration (FHWA), proposes to proceed with the State Road (SR) 2 and County Road (CR) 500 West (W) Intersection Improvement Project, Des. No. 2200043.

INDOT, on behalf of FHWA, has signed a determination of “No Historic Properties Affected” for this Section 106 undertaking. In accordance with 36 CFR 800.4(d), you and the other Section 106 consulting parties are being provided the documentation for this finding. You can view the determination of “No Historic Properties Affected” electronically by accessing INDOT’s Section 106 document posting website IN SCOPE at <http://erms12c.indot.in.gov/Section106Documents/> (the Des. No. is the most efficient search term, once in IN SCOPE). If a hard copy of the materials is needed, please respond to this email with your request within seven (7) days.

Consulting parties have thirty (30) calendar days from receipt of this information to review and provide comment.

Tribal Nation representatives please respond to INDOT’s Tribal Liaison, Taylor Payne, at tapayne@indot.in.gov (317-779-5775) with any responses pertaining to this project, including to provide INDOT/Indiana FHWA additional information about Tribal resources/concerns and questions/comments regarding cultural resources. The FHWA point of contact is Erica Tait at erica.tait@dot.gov (317-226-7481). Tribal Nation representatives who do not utilize IN SCOPE can find a copy of the documentation  [HERE](#).

Thank you in advance for your input,

Clint Kelly

Section 106 Specialist/Historian

***Cultural Resources Office
Environmental Services***

100 N. Senate Ave., Rm. N758-ES
Indianapolis, IN 46204

Office: (317) 447-8707

Email: ckelly1@indot.in.gov

Core Office Hours: M-F 7:30-3:30

[Find us on social media!](#)



Appendix F. Project Plans

| PROJECT | DESIGNATION |
|----------|-------------|
| 2200043 | 2200043 |
| CONTRACT | |
| R-44648 | |

| CULVERT ASSETS | | |
|----------------|-----------------------|-------------|
| DES. NO. | CULVERT ASSET ID | WORK TYPE |
| 2200043 | Sta. 926+50.00 "A" | New Culvert |
| 2200043 | Sta. 48+75.00 "S-1-A" | New Culvert |
| 2200043 | Sta. 929+60.00 "A" | New Culvert |

INDIANA DEPARTMENT OF TRANSPORTATION



RIGHT-OF-WAY PLANS

ROUTE: S.R. 2 AT: 55+04

PROJECT NO. 2200043 R/W

L.A. CODE: 7638
DES. NO.: 2200043

| TRAFFIC DATA | S.R. 2 | CR 500 W |
|---------------------------|------------------------------|------------------------------|
| A.A.D.T. (2027) | 9,008 V.P.D. | 2,249 V.P.D. |
| A.A.D.T. (2047) | 9,666 V.P.D. | 2,249 V.P.D. |
| D.H.W (2047) | 1,001 V.P.D. | 224 V.P.D. |
| DIRECTIONAL DISTRIBUTION | 48.90 % | 51.49 % |
| TRUCKS | 5.08 % A.A.D.T. | 1.73 % A.A.D.T. |
| | 1.97 % D.H.W. | 1.34 % D.H.W. |
| DESIGN DATA | | |
| DESIGN SPEED | 55 MPH | 30 AND 40 MPH |
| PROJECT DESIGN CRITERIA | RECONSTRUCTION (NON-FREEWAY) | RECONSTRUCTION (NON-FREEWAY) |
| FUNCTIONAL CLASSIFICATION | PRINCIPAL ARTERIAL | LOCAL AGENCY COLLECTOR |
| RURAL/URBAN | RURAL | RURAL |
| TERRAIN | LEVEL | LEVEL |
| ACCESS CONTROL | NONE | NONE |

PROJECT LOCATION SHOWN BY

LA PORTE COUNTY

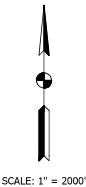
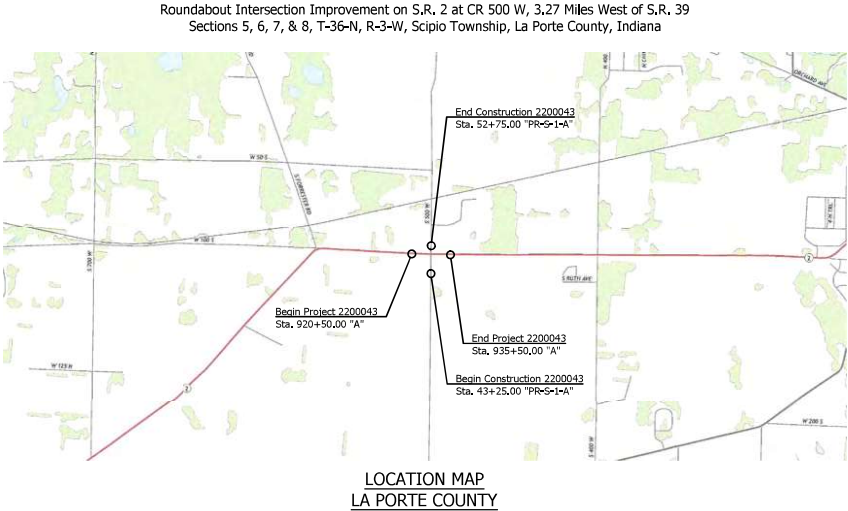
LATITUDE: 41°35'29" N LONGITUDE: 86°47'53" W

GROSS LENGTH: 0.284 MI.
NET LENGTH: 0.284 MI.
MAX. GRADE: 1.68 %

HYDROLOGIC UNIT CODE (S): 071200010406

FINAL R/W PLANS
JANUARY 9, 2025

INDIANA DEPARTMENT OF TRANSPORTATION
STANDARD SPECIFICATIONS DATED 2024
TO BE USED WITH THESE PLANS



LAWSON-FISHER ASSOCIATES P.C.
525 W. WASHINGTON AVENUE
SOUTH BEND, INDIANA 46601
PH. (574) 234-3167

NOT FOR
CONSTRUCTION

| | | |
|-----------------------|--------------------------------------|--------------|
| PLANS PREPARED BY: | LAWSON-FISHER ASSOC. P.C. | 574-234-3167 |
| | | PHONE NUMBER |
| CERTIFIED BY: | | DATE |
| APPROVED FOR LETTING: | INDIANA DEPARTMENT OF TRANSPORTATION | DATE |

| | |
|-------------|-------------|
| | DESIGNATION |
| | 2200043 |
| | SHEETS |
| SURVEY BOOK | 1 of 31 |
| CONTRACT | PROJECT |
| R-44648 | 2200043 |

U:\2023\202318 INDOT SR2 Roundabout\Cad\Plan Set\Civil\231800RW_CS.dwg 1/9/2025 3:18 PM U:\2023\202318 INDOT SR2 Roundabout\Cad\Plan Set\Civil\231800RW_CS.dwg

| UTILITIES | | |
|------------------|---|---|
| UTILITY | OWNER | ADDRESS |
| CABLE - | Comcast Contact: Larry Smith Phone: (224)229-5849 Email: Larry_Smith3@comcast.com | 688 Industrial Dr. Elmhurst, IL 60126 |
| COMMUNICATIONS - | ACME Communications Contact: Bradley Hutchinson Phone: (219)809-0305 Email: bradley@mse.adsnet.com | 618 Franklin St. Michigan City, IN 46360 |
| ELECTRIC - | NIPSCO Electric (LaPorte) Contact: Zac Topoll (DVG Engineering) Phone: (219)921-1441 Email: ztopoll@dvgtteam.com | 1155 Troutwine Rd. Crown Point, IN 46307 |
| FIBER OPTIC - | ACME Communications Contact: Bradley Hutchinson Phone: (219)809-0305 Email: bradley@mse.adsnet.com | 618 Franklin St. Michigan City, IN 46360 |
| | US Signal Contact: Jim Engels Phone: (630)454-8070 Email: jengels@tkns.net | 201 Iona Ave. S.W. Grand Rapids, MI 46701 |
| GAS - | NIPSCO Gas (LaPorte) Contact: Lisa Taylor Phone: (219)379-7168 Email: letaylor@nisource.com | 1502 W S.R. 2 La Porte, IN 46350 |
| TELEPHONE - | Frontier Contact: Richard Ferris (Troyer Group) Phone: (260)387-0799 Email: rferris@troyergroup.com | 1502 Magnavox Way, Ste. 100 Fort Wayne, IN 46804 |

| GENERAL NOTES |
|---|
| All earth shoulders, median area, cut and fill slopes shall be plain or mulched seeded except where sodding is specified. |
| The final cross sections of the grading contract will be the original cross sections of the paving contract. However, partial or complete cross sections shall be taken if necessary to determine the actual excavation quantities. |
| It is the Contractor's responsibility to contact any and all utility companies within the limits of this project 3 weeks prior to any construction. |
| This set of plans shall not be construed to be a property retracement survey. Where apparent property lines, corners, subdivision or section corner information is shown, it is based on physical evidence or testimony. |
| |

| INDEX | |
|-----------|--|
| SHEET NO. | DRAWING INDEX |
| 1 | Title Sheet |
| 2 | Index and General Notes |
| 3 - 5 | Location Control Route Survey Plat |
| 6 - 7 | Plat No. 1 |
| 8 - 13 | Typical Sections |
| 14 - 24 | Plan and Profiles |
| 25 - 28 | Roundabout Profiles |
| 29 - 30 | Detention Basin Plan and Sections |
| 31 | Pavement Quantities and Approach Table |

LEGEND

| | | | |
|----------|-----------------------|-------------------------------------|--------------------------------|
| R/W | Right-of-Way | App. Ex. R/W | Apparent Existing Right-of-Way |
| Ex. R/W | Existing R/W | <div><div>B</div><div>E</div></div> | Begin L.A. R/W |
| E.P. | Edge of Pavement | | End L.A. R/W |
| C.L.T.F. | Chain-Link Type Fence | N.E.P.L. | No Evidence of Property Line |
| F.F.T.F. | Farm-Field Type Fence | Comm. Rec. Line | Commissioner's Record Line |
| R | Property Line | | |



Per Indiana State Law IC-8-1-26-16: It is against the law to excavate without notifying the underground location service two (2) working days before commencing work.

INDIANA 811
1-800-382-5544 OR CALL 811
24 HOURS A DAY 7 DAYS A WEEK

Note: Utility locations are shown based upon information (maps and paint marks) supplied by others, and there is not guarantee of the accuracy or completeness of said locations.

| REVISIONS | | |
|-----------|------|---------|
| SHEET NO. | DATE | REVISED |
| | | |
| | | |
| | | |
| | | |
| | | |
| | | |
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| | | |

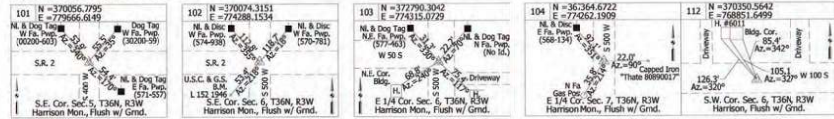
NOT FOR CONSTRUCTION

| | | | |
|--------------------------|--------------|-----------------|------|
| RECOMMENDED FOR APPROVAL | | DESIGN ENGINEER | DATE |
| DESIGNED: JDD | DRAWN: GDH | | |
| CHECKED: DGA | CHECKED: DGA | | |

| | |
|---|--|
| INDIANA DEPARTMENT OF TRANSPORTATION | |
| INDEX AND GENERAL NOTES | |

| | |
|---------------------|------------------------|
| SCALE | BRIDGE FILE |
| | DESIGNATION 2200043 |
| SURVEY BOOK | SHEETS |
| | 2 of 31 |
| CONTRACT R-4461B | PROJECT 2200043 |

SECTION CORNER REFERENCE TIES



ALIGNMENT POINT TABLE - Line "A"

| Pt. No. | Station | Northing | Easting |
|---------|-----------------------|-------------|-------------|
| 115 | P.I. Sta. 903+23.19 | 370182.5992 | 772141.4697 |
| 101399 | P.I. Sta. 912+36.88 | 370152.1872 | 772737.1253 |
| 501 | P.I. Sta. 927+45.13 | 370076.5562 | 774243.5165 |
| 510 | P.O.T. Sta. 950+20.70 | 370062.0927 | 776519.0783 |

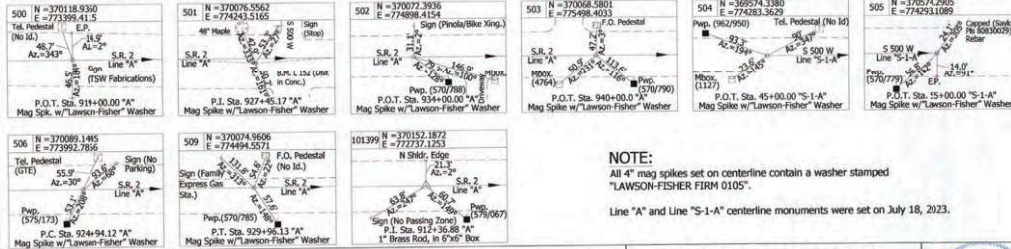
ALIGNMENT POINT TABLE - Line "S-1-A"

| Pt. No. | Station | Northing | Easting |
|---------|----------------------|-------------|-------------|
| 102 | P.I. Sta. 50+00.00 | 370074.3151 | 774288.1534 |
| 504 | P.O.T. Sta. 45+00.00 | 369574.3380 | 774283.3629 |
| 505 | P.O.T. Sta. 55+00.00 | 370574.2905 | 774293.1089 |

CONTROL POINT REFERENCE TIES



ALIGNMENT POINT REFERENCE TIES



NOTE:

All 4" mag spikes set on centerline contain a washer stamped "LAWSON-FISHER FIRM 0105".

Line "A" and Line "S-1-A" centerline monuments were set on July 18, 2023.

FIELD SURVEYOR STATEMENT

THIS SURVEY, TO THE BEST OF MY KNOWLEDGE AND BELIEF, IS EXECUTED ACCORDING TO THE PROVISIONS OF TITLE 36, I.A.C. 1-12-20 THROUGH 1-12-26 REGARDING ROUTE SURVEYS, EXCEPT THAT ANY DATA SHOWN REGARDING THE LOCATION OR DESCRIPTION OF THE EXISTING PARCELS IS NOT PART OF THIS SURVEY.



I AFFIRM, UNDER THE PENALTIES FOR PERJURY, THAT I HAVE TAKEN REASONABLE CARE TO REDACT EACH SOCIAL SECURITY NUMBER IN THIS DOCUMENT, UNLESS REQUIRED BY LAW.

Michelle M.G. Slack 10/26/2023
PROFESSIONAL SURVEYOR DATE

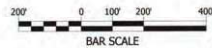
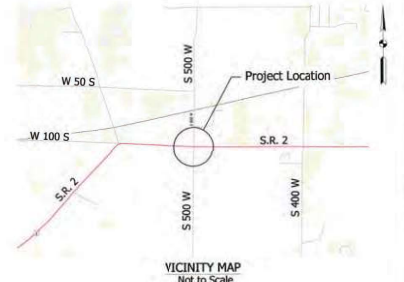
PROPERTY OWNER INFORMATION

- MARTIN R. BARR TRUST NO. 4 (35% INTEREST), MARSHA H. BARR TRUST NO. 54 (35% INTEREST), AND EVAN S. BARR (30% INTEREST)
Inst. #2017R-04116
- HARRY B. MILLER
Inst. #93-03562
- CORRECT ROOFING, INC.
Inst. #2015R-10159
- RYN G. SCHMIDT AND ARLENE L. SCHMIDT, ROBERT W. SCHMIDT, LIFE ESTATE INTEREST
Inst. #2004R-05791
- GARWOOD ORCHARDS, INC.
Inst. #2014R-13876
- SCONI INVESTMENTS, LLC
Inst. #2021R-18272
- HARRY W. FRITZ
Inst. #72-7356
- HARRY W. FRITZ AND APRIL L. FORNSHELL
Inst. #2013R-07837
Inst. #2017R-04476
- FAMILY EXPRESS CORPORATION
Inst. #2012R-15122
- LAKE COUNTY TRUST COMPANY-TRUST #5148
Inst. #99-24360
- JOHN K. BECKER AND MELLISSA D. BECKER
Inst. #2019R-00678
- DANIEL E. HAMMOND AND DORCAS E. HAMMOND
Inst. #2010R-10864
- DOMINIC E. SEMENTO JR. AND PAMELA A. SEMENTO
Inst. #2016R-00250
- JOHN E. SHEBEL AND ELYSIA D. SHEBEL
Inst. #2013R-08211
- DIANA L. HAYES
- AGNL CROWN, LLC
Inst. #2017R-5860

INDIANA DEPARTMENT OF TRANSPORTATION
LOCATION CONTROL ROUTE SURVEY PLAT

S.R. 2 at C.R. S 500 WEST
S.R. 2 PLAN VIEW

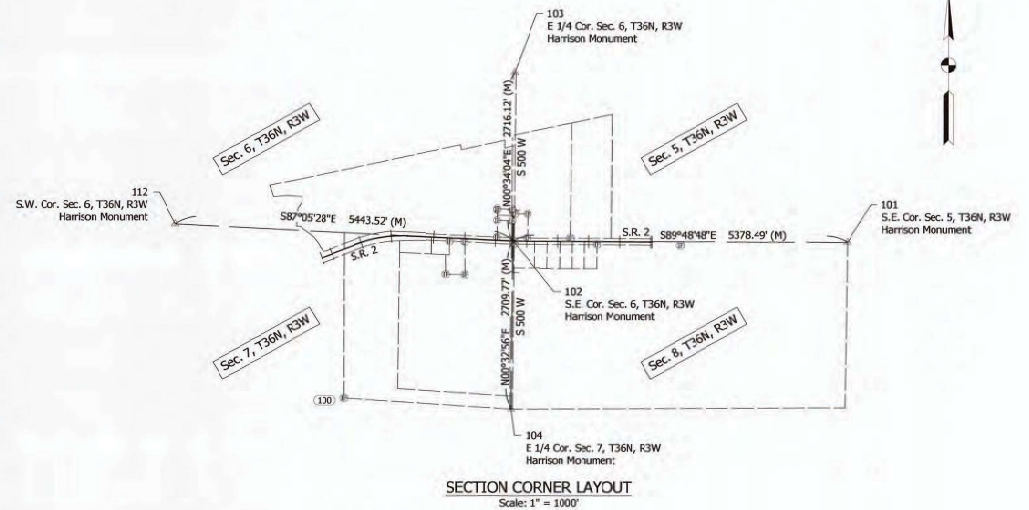
| | |
|-----------------------|-----------------------|
| SCALE 1" = 200' | BRIDGE FILE |
| COUNTY LAPORTE | DESIGNATION 220043 |
| SURVEY BOOK R-4468 | SHEETS 3 of 31 |
| CONTRACT 220043 | PROJECT 220043 |



M:\S - 10726702318 INDOT 582 Route204\Draw\Plan Set\Survey\231800_LCSP-02.dwg (LCSP SECTION CORNER LAYOUT)

| FOUND MONUMENT POINT LISTING - LINE "A" | | | | |
|---|-------------|-------------|-----------------------|--|
| Pl. No. | Northing | Easting | Station, Offset | Description |
| 100 | 367547.3056 | 771560.4486 | N/A | SC 1" Pipe // Flush // Good Cond. - Center Sec 7 |
| 113 | 370160.1390 | 772582.4131 | 907+57.72, 0.05' Lt. | Rdwy. Mon. - 1" Brass Rod in 6x6" Box // 3" Below // Good Cond. |
| 114 | 370146.3889 | 772650.2088 | 913+50.11, 0.12' Rt. | Rdwy. Mon. - 1" Brass Rod in 6x6" Box // 3" Below // Good Cond. |
| 115 | 370182.5992 | 772141.4697 | 903+23.19, 48.23' Lt. | Rdwy. Mon. - 1" Brass Rod in 8" Conc. Mon // Flush // Good Cond. |
| 200 | 370074.0181 | 773256.5474 | 917+59.57, 52.03' Rt. | 1" Bar // 1" Below // Good Cond. |
| 201 | 370120.5596 | 775205.8990 | 937+07.17, 50.12' Lt. | 1" Pipe // Flush // Good Cond. |
| 208 | 370127.8899 | 774513.9608 | 930+15.20, 53.05' Lt. | Capped Rebar - Kel 50125 // 1" Below // Good Cond. |
| 209 | 370064.2062 | 773501.0859 | 920+04.29, 49.56' Rt. | Capped Rebar - Kel 50125 // 1" Below // Good Cond. |
| 211 | 370137.8258 | 774022.3130 | 925+21.29, 50.07' Lt. | Capped Rebar - Saylor 80880029 // Flush // Good Cond. |
| 216 | 370018.6109 | 775633.8927 | 941+35.80, 48.91' Rt. | Capped Rebar - Thate 80890017 // Flush // Good Cond. |
| 303 | 370210.0929 | 772888.0362 | 913+84.70, 65.40' Lt. | R/W Mkr. 16" Above // Poor Cond. (Leaning) |
| 304 | 370236.3090 | 772318.7802 | 905+00.12, 79.96' Lt. | R/W Mkr. 18" Above // Good Cond. |
| 305 | 370082.6813 | 772460.4593 | 906+34.88, 79.88' Rt. | R/W Mkr. 18" Above // Good Cond. |
| 310 | 370217.3290 | 772740.2154 | 912+36.64, 65.21' Lt. | R/W Mkr. 4" Above // Poor Cond. (Laying Flat) |
| 312 | 370225.0826 | 772585.3973 | 907+57.41, 65.06' Lt. | R/W Mkr. 6" Above // Poor Cond. (Leaning) |
| 313 | 370084.1625 | 772882.3442 | 913+85.33, 60.56' Rt. | R/W Mkr. Flush // Good Cond. |
| 314 | 370092.3569 | 772733.5919 | 912+35.80, 59.91' Rt. | R/W Mkr. Flush // Good Cond. |
| 100394 | 370024.9604 | 774631.6809 | 951+33.57, 49.13' Rt. | Post 6" Wooden |
| 101399 | 370152.1872 | 772737.1253 | 912+36.88, 0.07' Rt. | Rdwy. Mon. - 1" Brass Rod in 6x6" Box // 3" Below // Good Cond. |

| FOUND MONUMENT POINT LISTING - LINE "S-1-A" | | | | |
|---|-------------|-------------|-----------------------|--|
| Pl. No. | Northing | Easting | Station, Offset | Description |
| 202 | 370409.4912 | 774024.7484 | 53+32.55, 266.71' Lt. | 1/2" Pipe // Flush // Good Cond. |
| 204 | 369539.8362 | 773505.5854 | N/A | 5/8" Rebar - // 6" Below // Good Cond. |
| 205 | 370564.2847 | 774273.3068 | 54+89.80, 19.70' Lt. | 5/8" Rebar - // Flush // Good Cond. |
| 206 | 370511.6385 | 774518.2648 | 54+39.58, 225.77' Rt. | 5/8" Rebar // 2" Below // Good Cond. |
| 207 | 370512.5029 | 774317.1805 | 54+38.45, 24.68' Rt. | Capped Rebar - Illegible // 1" Below // Good Cond. |
| 210 | 369545.2517 | 773197.1628 | N/A | Capped Rebar - Kel 50125 // 1" Below // Good Cond. |
| 212 | 370400.7782 | 774271.6161 | 53+26.28, 19.77' Lt. | Capped Rebar - Saylor 80880029 // Flush // Good Cond. |
| 213 | 370573.7387 | 774026.4020 | 54+96.80, 266.69' Lt. | Capped Rebar - Saylor 80880029 // 1" Below // Good Cond. |
| 214 | 370595.7578 | 774026.7021 | 55+18.83, 266.61' Lt. | Capped Rebar - Saylor 80880029 // Flush // Good Cond. |
| 215 | 370588.2234 | 774273.4692 | 55+13.74, 19.78' Lt. | Capped Rebar - Saylor 80880029 // Flush // Good Cond. |
| 217 | 370827.9611 | 774307.7648 | 49+53.84, 20.05' Rt. | Capped Rebar - Thate 80890017 // 6" Above // Good Cond. |
| 100002 | 369646.4888 | 774305.2523 | 45+72.36, 21.20' Rt. | Post 6" Wooden |



| LEGEND | | INDOT | Indiana Department of Transportation | P.O.C. | Point on Curve |
|----------|-----------------------|-----------|--------------------------------------|----------|-----------------------|
| ● | Centerline Monument | Inst. | Instrument | P.O.S.T. | Point on Semi-Tangent |
| ○ | Post | LFA | Lawson-Fisher Associates | P.O.T. | Point on Tangent |
| ⊕ | Property Corner Found | Lt. | Left | Pl. No. | Point Number |
| App. | Apparent | M | Measured | Pwp. | Power Pole |
| Az. | Azimuth | Mbox. | Mailbox | R/W | Right-of-Way |
| Bldg. | Building | Mkr. | Marker | R | Range |
| B.M. | Benchmark | Nl. | Nail | Rt. | Right |
| Cor. | Corner | N | North | Sec. | Section |
| C.R. | County Road | N.E. | Northeast | S.E. | Southeast |
| E | East | No E.P.L. | No Evidence of Property Line | S | South |
| E.P. | Edge of Pavement | N.W. | Northwest | S.R. | State Route |
| Ex. | Existing | R | Property Line | Sta. | Station |
| fa. | Face | F.C. | Point of Curvature | S.W. | Southwest |
| F.F.T.F. | Farm Field Type Fence | P.I. | Point of Intersection | T | Township |
| F.O. | Fiber Optic | F.T. | Point of Tangency | Tel. | Telephone |
| Gnd. | Ground | | | W | West |

| | | | | | | | |
|-------------------------------|--|---|--|--|--|------------------------------------|--|
| SURVEY STARTED MAY 2023 | This survey was prepared by: Michelle M.G. Slack | FIELD SURVEYOR STATEMENT THIS SURVEY, TO THE BEST OF MY KNOWLEDGE AND BELIEF, IS EXECUTED ACCORDING TO THE PROVISIONS OF TITLE 36, I.A.C. 1-12-20 THROUGH 1-12-24 REGARDING ROUTE SURVEYS, EXCEPT THAT ANY DATA SHOWN REGARDING THE LOCATION OR DESCRIPTION OF THE EXISTING PARCELS IS NOT PART OF THIS SURVEY. | | I AFFIRM, UNDER THE PENALTIES FOR PERJURY, THAT I HAVE TAKEN REASONABLE CARE TO REDACT EACH SOCIAL SECURITY NUMBER IN THIS DOCUMENT, UNLESS REQUIRED BY LAW. | INDIANA DEPARTMENT OF TRANSPORTATION LOCATION CONTROL ROUTE SURVEY PLAT S.R. 2 at C.R. S 500 WEST SECTION CORNER LAYOUT & POINT TABLES | SCALE 1" = 100' | BRIDGE FILE |
| SURVEY COMPLETED JULY 2023 | | | | | | COUNTY LAPORTE | DESIGNATION 220043 |
| ROUTE PLAT SHEET'S 2 of 3 | | | | | | SURVEY BOOK CONTRACT E-44648 | SHEETS 4 of 31 PROJECT 220043 |

SURVEYOR'S REPORT for S.R. 2 and C.R. S. 500 W Intersection
Survey started: May 2023 Survey completed: July 2023

This project is located at the intersection of S.R. 2 with C.R. S. 500 W, 5.8 miles east of junction with U.S. 42; and 4.6 miles west of junction with U.S. 35. The survey limits fall within Sections 5, 6, 7, and 8, all in Township 36 North, Range 3 West, Scioto Township, La Porte County, Indiana.

The purpose of this survey is to collect data pertinent for the design of a roundabout and to provide a basis for describing any right-of-way needed for this project. This survey was completed for Indiana Department of Transportation. It is not a property retracement survey. Where title lines or survey/site corners are shown, their location is based on physical evidence, testimony, or minimal deed analysis. Section corners shown are based on found monumentation and records provided by the La Porte County Recorder's Office and the La Porte County Surveyor's Office, unless otherwise noted.

Documents used in the creation of this route survey plat:

- State of Indiana State Highway Commission Plan and Profile of Proposed State Highway, Federal Aid Project No. 17 - Section "D", dated 1920.
- Location Control Route Survey, recorded in Instrument 2008R-16007, La Porte County Records.
- Indiana Department of Transportation Right-of-Way Plans, SR 2 Realignment at Forrester Road and CR 1005, Project No. STP-206-3 (004) R/W, R/W Code 4418, Des. No. 0014450 dated 2009.
- Indiana Department of Transportation Road Plans, SR 2 Realignment at Forrester Road and CR 1005, Project No. STP-206-3 (004) CONST., dated 03/05/2010.
- Survey by Keil & Associates, Inc., dated 03/28/1995 and recorded in Instrument 95-084-4.
- Survey by Keil & Associates, Inc., dated 03/22/1999 and recorded in Instrument 99-21288.
- Un-recorded survey by R. Edward Carnes, dated 12/10/1995, provided by La Porte County Surveyor's Office.
- Survey by McHahon Engineers/Architects, dated 10/11/2012 and recorded in Instrument 2013R-02346.
- Survey by Saylor Land Surveying, Inc., dated 03/13/2017 and recorded in Instrument 2017R-04042.
- Un-recorded survey by Earl L. Alexander, dated 06/18/1949, provided by La Porte County Surveyor's Office.
- Survey by John A. Doyle & Associates, Inc., dated 10/17/2014 and recorded in Instrument 2014R-39517.
- Survey by Keil & Associates, Inc., dated 04/20/2011 and recorded in Instrument 2011R-07558.
- Un-recorded survey by Thale Land Surveying Services, dated 12/23/2003 provided by La Porte County Surveyor's Office.

HORIZONTAL CONTROL

Unless noted otherwise, all bearings, distances, areas, and coordinates shown hereon are based upon the Indiana Geospatial Coordinate System's (InGCS) "LaPorte" zone per NAD 83 (2011) epoch 2010.00 and are reported in U.S. Survey feet and decimal parts thereof. The "LaPorte," "Pulaski," and "Starke" zones have identical parameters. These zones were developed to minimize the differences between ground-measured horizontal distances and the corresponding grid coordinate (map) distances within the counties bearing these zones names.

InGCS "LaPorte" Zone Parameters

Geometric Datum: NAD 83(2011) epoch 2010.00
Projection Type: Transverse Mercator
Central Meridian: 86°45'00" west longitude
Central Meridian scale factor: 1.000027
Latitude of Grid Origin: 40°54'00" north latitude
False Northing: 36,000,000 m (118,110.00 U.S.Ft)
False Easting: 240,000,000 m (787,400.00 U.S.Ft)

The horizontal datum was established by Global Navigation Satellite Systems (GNSS) Real Time Kinematic (RTK) observations utilizing the Trimble Virtual Reference Station Now Network (TVN) as the correction source. Vertical site calibration of +0.249 feet was applied to match National Geodetic Survey (NGS) Bench Mark L 152.

Coordinates are shown to four decimal places, not to indicate the accuracy of the survey, but to provide repeatability of calculation. English units (U.S. Survey Feet) are used for this survey. Bearings are derived from InGCS Coordinate System. Control point numbers are actual field survey point numbers used for control and topography. The relative positional accuracy of this survey meets the requirements for an urban survey (0.07", plus 0.05 parts per million) and does not exceed five-tenths (0.5) feet.

ALIGNMENTS

Line "A"

Line "A" has been roughly established based on the above referenced 1920 plans. There is ambiguity between the 1920-dated plan set and what is built on the ground today. Not only is there a discrepancy between documents of record, but there is also a visible discrepancy between what was planned in 1920 and the improvements that exist today.

- On the 1920 plans, right-of-way is graphically depicted and labeled 30' each side of the centerline. 1920 right-of-way grants were not provided by INDOT and a quick search of county records did not reveal any either. There are Right-of-Way Grants dated 1939 that acquired right-of-way 50' each side of the centerline. This indicates that there should be an additional plan set beyond 1920, or the 1920 plans were updated further and used to acquire additional r/w in 1939. Any updates to plans could possibly call for a revision of the centerline alignment.
- The curve located and described at PI 926+00.00 in the 1920 plans does not fit the current improvements that are located at the intersection of CR 500. The radius shown at this curve is half the length of what it should be based on the degree of curve of 1° 20'.
- The curve located and described at PI 890+87.75 in the 1920 plans calls for an 18° degree of curve. There is a 1932 right-of-way grant that notes "Additional right of way needed to change an eighteen (18) degree curve to a ten (10) degree curve". This right-of-way was acquired at 40' from centerline in 1932. From that same property owner, there is a 1939 grant that acquires 50' from centerline. The 1920 plans show and label it a 30' from centerline.

Based on the above discrepancies, it can be presumed that the centerline alignment has been revised from the 1920 plans, particularly at the intersection of CR 500. In lieu of revised plan/curve data information, the tangent lines and curve data will be placed based on a best fit of the improvements.

The back tangent extends into the 2010 project area where centerline monuments were found and located from that project. The 2010 plan set shows Line "PR-A" being into Line "A" from 1920. The bearing of the best fit of this tangent is within 10" of a line run between found monument #101399 (at PI of 2010 plans) and section corner #1122. The line between the found monuments was helix as the back tangent. The fore tangent is a best fit of the split of pavement.

These two tangents were intersected to establish the PI of the curve at an S 50°. 1920 plan delta = 2°40'. Measured delta = 2°10'34". The measured delta and an even degree of curve of 0°30'00" was held to establish the curve. It appears from improvements that the curve is flatter than what was originally designed in 1920.

A 2010 plan station of 912+36.88 was held at found monument #101399 (PI); all other stations were derived by linear measurement.

Theoretically, Line "A" is an original alignment and has no uncertainty. However, uncertainty has been established with respect to the 1920 plans, but should be used for information only. Based on the discrepancy information provided above, these comparisons do nothing to aid the reader in the accuracy of this survey. The uncertainty of Line "A" with respect to the 1920 plans is as follows:

- 11" in a north-south direction, based on fore tangent not fitting centerline when holding plan curve delta.
- 50' by stationing, in an east-west direction, based on the location of section line compared to where it fell in 1920.

For this survey, we found several points from the 2010 project for the curve to the west of our project. The points have been shown and labeled in plan view.

Line "S-1-A"

Line "S-1-A" was established along the east lines of Sections 6 and 7 using section corner monuments found in the field. A station of 50+00 was held at the Southeast Corner of Section 6 (#102), which is the corner common to Sections 5, 6, 7, and 8. All other stations were derived by linear measurements. This is an original alignment and therefore has no uncertainty.

SECTION CORNERS

#101: A Harrison Monument was found flush at the Southeast Corner of Section 5, Township 36 North, Range 3 West. Monument was found based on corner record information provided by the La Porte County Surveyor's Office. Monument was found as described. Three of the four references were found to be in good condition and distances measure as called for. The uncertainty of this point with respect to the county corner record is estimated to be negligible.

#102: A Harrison Monument was found in good condition flush with the surface at the Southeast Corner of Section 6, Township 36 North, Range 3 West. Monument was found based on corner record information provided by the La Porte County Surveyor's Office. Monument was found as described. Four of the six references were found to be in good condition and distances measure as called for. The uncertainty of this point with respect to the county corner record is estimated to be negligible.

#103: A Harrison Monument was found in good condition flush with the surface at the East Quarter Corner of Section 6, Township 36 North, Range 3 West. Monument was found based on corner record information provided by the La Porte County Surveyor's Office. Monument was found as described. All four references were found to be in good condition and distances measure as called for. The uncertainty of this point with respect to the county corner record is estimated to be negligible.

#104: A Harrison Monument was found in good condition flush with the surface at the East Quarter Corner of Section 7, Township 36 North, Range 3 West. Monument was found based on corner record information provided by the La Porte County Surveyor's Office. Monument was found as described. One of the four references was found to be in good condition and distances measure as called for. Two additional ties were set. The uncertainty of this point with respect to the county corner record is estimated to be negligible.

#112: A Harrison Monument was found flush at the Southwest Corner of Section 6, Township 36 North, Range 3 West. Monument was found based on corner record information provided by the La Porte County Surveyor's Office. Monument was found as described. Three of the four references were found to be in good condition and distances measure as called for. The uncertainty of this point with respect to the county corner record is estimated to be negligible.

EXISTING RIGHT-OF-WAY

The apparent existing right-of-way for S.R. 2 was established based on right-of-way grants provided by INDOT. As noted above, there appears to be a plan set missing beyond the 1920's plans. The grants are dated from the early 1930's and recorded in 1976. They consistently convey right-of-way 50' on each side of the centerline, which does not match what is shown in the 1920 plans. These right-of-way lines coincide with property lines found, fence lines and private improvements in the field.

LEGAL DRAIN

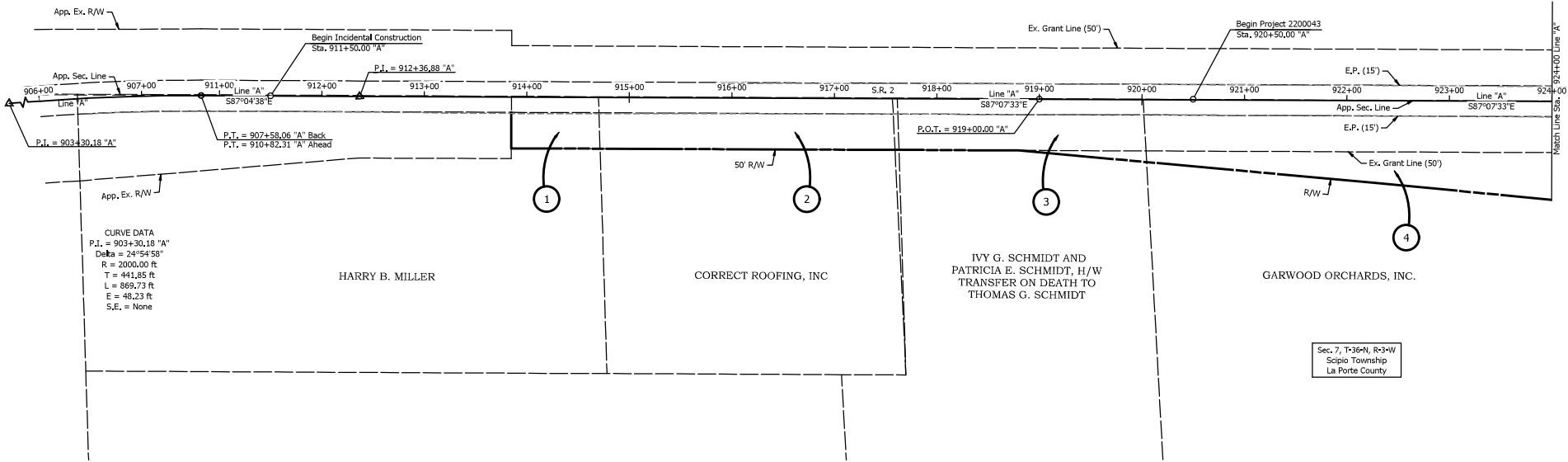
There are no legal drains in the survey area per LaPorte County records.

| | | | | | | | | | |
|-------------------------------|--|--|---|--|---|--|--|------------------------|--|
| SURVEY STARTED MAY 2023 | | This survey was prepared by: Michele M.G. Slack  | FIELD SURVEYOR STATEMENT THIS SURVEY, TO THE BEST OF MY KNOWLEDGE AND BELIEF, IS EXECUTED ACCORDING TO THE PROVISIONS OF TITLE 866 I.A.C. 1-10-20 THROUGH 1-12-26 REGARDING ROUTE SURVEYS, EXCEPT THAT ANY DATA SHOWN REGARDING THE LOCATION OR DESCRIPTION OF THE EXISTING PARCELS IS NOT PART OF THIS SURVEY. | |  Michele M.G. Slack PROFESSIONAL SURVEYOR 10/26/2023 DATE | INDIANA DEPARTMENT OF TRANSPORTATION LOCATION CONTROL ROUTE SURVEY PLAT | | SCALE BRIDGE FILE | |
| SURVEY COMPLETED JULY 2023 | | | | | | COUNTRY LAPORTE | | DESIGNATION 2200043 | |
| ROUTE PLAT SHEETS 3 of 3 | | | | | | SURVEY BOOK | | SHEETS 5 of 31 | |
| | | | | | | CONTRACT R-4468 | | PROJECT 2200043 | |
| | | | | | | S.R. 2 at C.R. S 500 WEST SURVEYOR'S REPORT | | | |



MARTIN R. BARR TRUST NO. 4 (35% INTEREST),
MARSHA H. BARR TRUST NO. 54 (35% INTEREST),
AND EVAN S. BARR (30% INTEREST)

Sec. 6, T-36-N, R-3-W
Scipio Township
La Porte County



CURVE DATA
P.I. = 903+30.18 "A"
Delta = 24°54'58"
R = 2000.00 ft
T = 441.85 ft
L = 869.73 ft
E = 48.23 ft
S.E. = None

HARRY B. MILLER

CORRECT ROOFING, INC

IVY G. SCHMIDT AND
PATRICIA E. SCHMIDT, H/W
TRANSFER ON DEATH TO
THOMAS G. SCHMIDT

GARWOOD ORCHARDS, INC.

Sec. 7, T-36-N, R-3-W
Scipio Township
La Porte County

NOT FOR
CONSTRUCTION

| | | | |
|-----------------------------|--------------|-----------------|------|
| RECOMMENDED FOR APPROVAL | | DESIGN ENGINEER | DATE |
| DESIGNED: DDE | DRAWN: GDH | | |
| CHECKED: DGA | CHECKED: DGA | | |

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| INDIANA DEPARTMENT OF TRANSPORTATION | |
| PLAT NO. 1 | |

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|---------------------|------------------------|
| SCALE 1" = 50' | BRIDGE FILE |
| | DESIGNATION 2200043 |
| SURVEY BOOK | SHEETS |
| 6 | of 31 |
| CONTRACT R-4454B | PROJECT 2200043 |

MARTIN R. BARR TRUST
NO. 4 (35% INTEREST),
MARSHA H. BARR TRUST
NO. 54 (35% INTEREST),
AND EVAN S. BARR
(30% INTEREST)

Sec. 6, T-36-N, R-3-W
Scipio Township
La Porte County

MIKE GILLESPIE,
KERRY DEARTH, & DAN MELVIN,
AS TRUSTEES FOR TEAMSTER
LOCAL UNION NO. 135 30' R/W

FAMILY EXPRESS
CORPORATION

LAKE COUNTY TRUST
COMPANY-TRUST #5148

Sec. 5, T-36-N, R-3-W
Scipio Township
La Porte County

AGNL CROWN,
L.L.C.

GARWOOD ORCHARDS, INC.

Sec. 7, T-36-N, R-3-W
Scipio Township
La Porte County

JOHN K. BECKER AND
MELLISSA D. BECKER

DANIEL E. HAMMOND AND
DORCAS E. HAMMOND, H/W
TRANSFER ON DEATH TO
ANTOINETTE HAMMOND,
SHARON WROBLEWSKI,
HEATHER HAMMOND AND
DANIEL E. HAMMOND

DOMINIC E. SEMENTO JR. AND
PAMELA A. SEMENTO, H/W

Sec. 8, T-36-N, R-3-W
Scipio Township
La Porte County

JOHN E. SHEBEL AND
ELYSIA D. SHEBEL, H/W

JOHN E. SHEBEL AND
ELYSIA D. SHEBEL, H/W

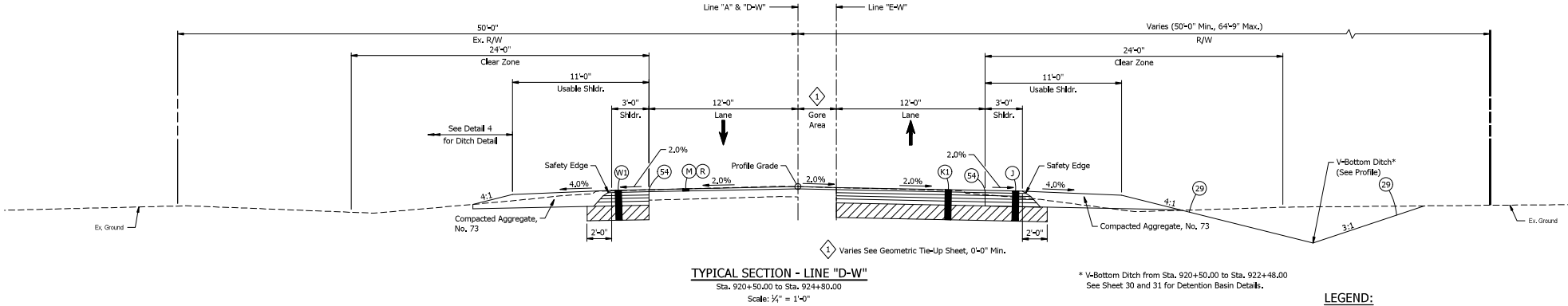
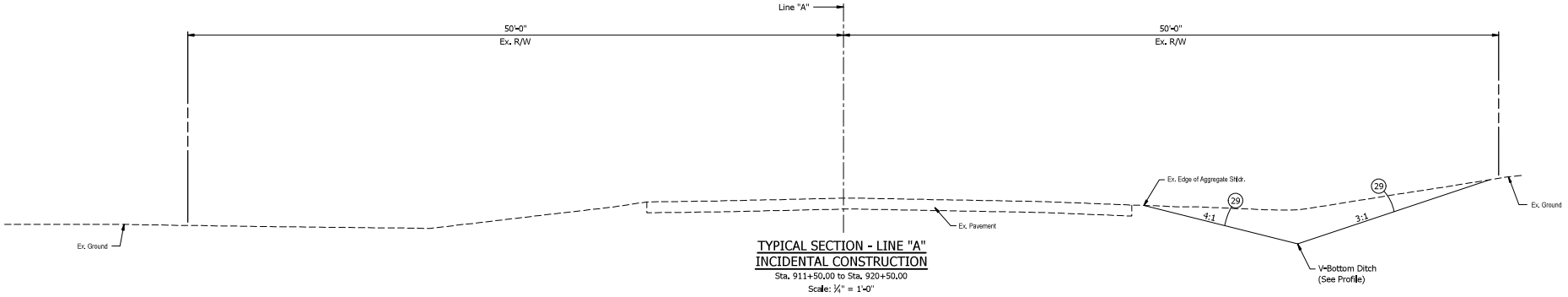
BRIAN L. ROSEMAN AND
WENDY S. ROSEMAN, H/W
TRANSFER ON DEATH TO
TONJA WORKMAN, KIMBER L. CORNELL AND
MICHAEL A. CORNELL

NOT FOR
CONSTRUCTION

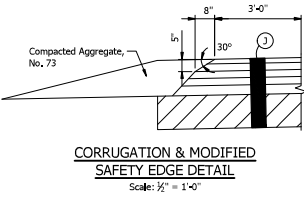
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| DESIGNED: DDE | DRAWN: GDH | | |
| CHECKED: DGA | CHECKED: DGA | | |

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| INDIANA DEPARTMENT OF TRANSPORTATION | |
| PLAT NO. 1 | |

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| SCALE 1" = 50' | BRIDGE FILE |
| | DESIGNATION 2200043 |
| SURVEY BOOK | SHEETS 7 of 31 |
| CONTRACT R-44648 | PROJECT 2200043 |



- LEGEND:**
- (J) 220 #/sys QC/QA+HMA, 2, 58S, Surface, 9.5mm on
330 #/sys QC/QA+HMA, 2, 58S, Intermediate, 19.0mm on
330 #/sys QC/QA+HMA, 2, 58S, Base, 19.0mm on
4 in. Compacted Aggregate No. 53 on
Subgrade Treatment, Type IBC
 - (K) 220 #/sys QC/QA+HMA, 2, 58S, Surface, 9.5mm on
330 #/sys QC/QA+HMA, 2, 58S, Intermediate, 19.0mm on
330 #/sys QC/QA+HMA, 2, 58S, Base, 19.0mm on
4 in. Compacted Aggregate No. 53 on
Subgrade Treatment, Type IBC
 - (M) Milling, Profile (0" - 2")
 - (R) 220 #/sys QC/QA+HMA, 2, 58S, Surface, 9.5 mm
 - (W) 220 #/sys QC/QA+HMA, 2, 58S, Surface, 9.5mm on
330 #/sys HMA Intermediate, Type B on
330 #/sys HMA Base, Type B on
4 in. Compacted Aggregate No. 53 on
Subgrade Treatment, Type IBC
 - (29) Seed Mixture, R
Mulching
Fertilizer
 - (54) Milled HMA Corrugations, Sinusoidal (Edgeline)

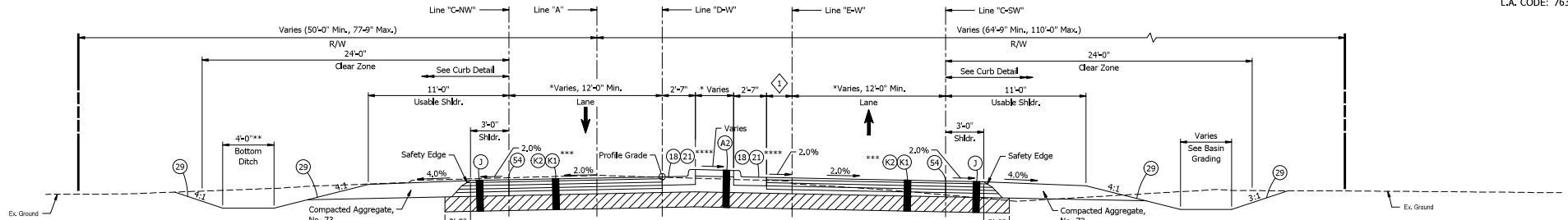


NOT FOR
CONSTRUCTION

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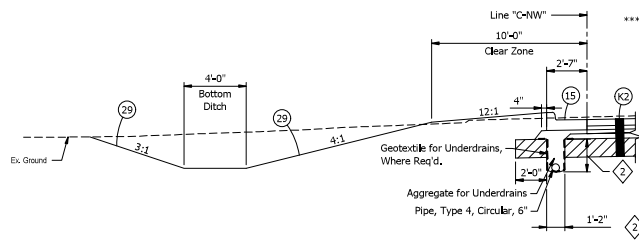
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| INDIANA DEPARTMENT OF TRANSPORTATION | |
| TYPICAL SECTIONS LINE "A" AND "D-W" | |

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| SCALE | BRIDGE FILE |
| AS NOTED | DESIGNATION |
| | 2200043 |
| SURVEY BOOK | SHEETS |
| | 8 of 31 |
| CONTRACT | PROJECT |
| R-4464B | 2200043 |



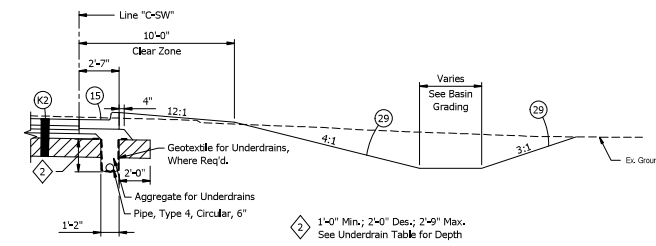
* See Geometric Tie-Up Sheet for Curb Alignments
 ** Ditch Begins at Sta. 924+28.00
 *** (K1) From Sta. 924+80.00 to Sta. 926+56.00
 (K2) From Sta. 926+56.00 to Sta. 927+40.65
 **** (21) From Sta. 924+80.00 to Sta. 925+80.00
 (18) From Sta. 925+80.00 to Sta. 927+40.65

TYPICAL SECTION - LINE "D-W"
 Sta. 924+80.00 to Sta. 927+40.65
 Scale: 1/2" = 1'-0"



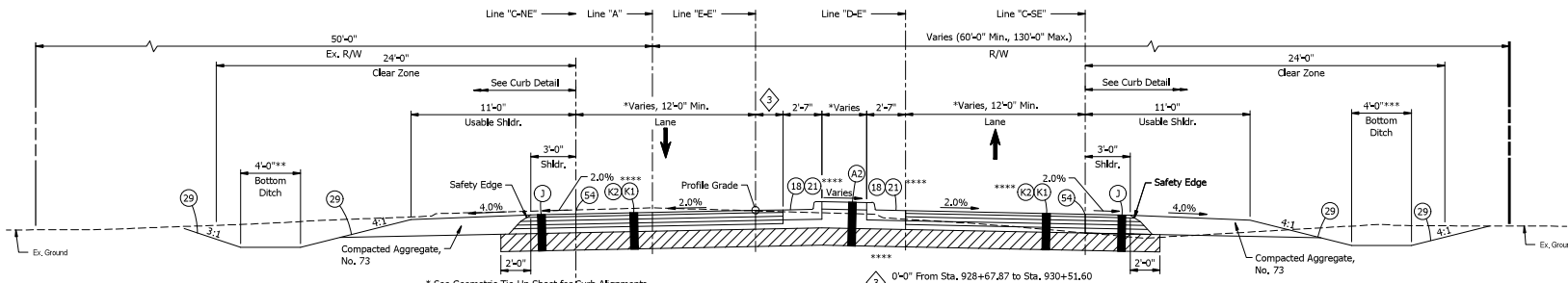
CURB DETAIL

Sta. 926+55.00 to Sta. 928+46.00 Line "C-NW"
 Sta. 927+44.00 to Sta. 929+45.00 Line "C-NW"
 Scale: 1/2" = 1'-0"



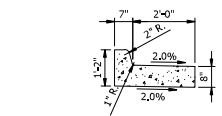
CURB DETAIL

Sta. 926+56.00 to Sta. 928+71.00 Line "C-SW"
 Sta. 927+33.00 to Sta. 929+47.00 Line "C-SW"
 Scale: 1/2" = 1'-0"

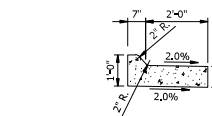


* See Geometric Tie-Up Sheet for Curb Alignments
 ** Ditch Ends at Sta. 931+00.00
 *** Ditch Ends at Sta. 931+61.00
 **** (K2) From Sta. 928+67.87 to Sta. 929+45.00
 (K1) From Sta. 929+45.00 to Sta. 931+20.00
 ***** (18) From Sta. 928+67.87 to Sta. 930+20.00
 (21) From Sta. 930+20.00 to Sta. 931+20.00

TYPICAL SECTION - LINE "E-E"
 Sta. 928+67.87 to Sta. 931+20.00
 Scale: 1/2" = 1'-0"



CURB AND GUTTER, COMBINED, MODIFIED, REVERSE
 Scale: 1/2" = 1'-0"



CURB AND GUTTER, TYPE B COMBINED, MODIFIED, REVERSE
 Scale: 1/2" = 1'-0"

LEGEND:

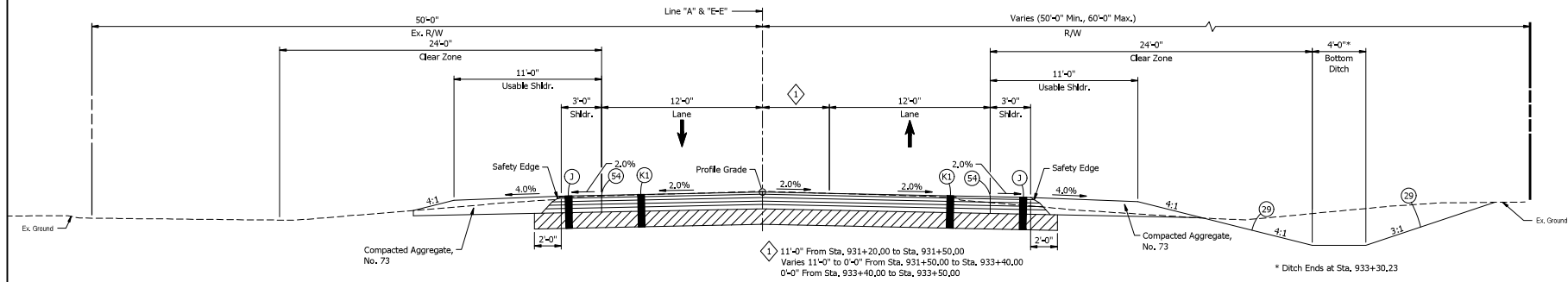
- (A2) Color PCCP, 6" on 13" - 15" Compacted Aggregate, No. 53 on (To Match Subgrade Treatment Elevation) Geotextile for Pavement, Type 2A on Subgrade Treatment, Type IBC
- (1) 220 #/sys QC/QA+HMA, 2, SBS, Surface, 9.5mm on 330 #/sys QC/QA+HMA, 2, SBS, Intermediate, 19.0mm on 330 #/sys QC/QA+HMA, 2, SBS, Base, 19.0mm on 330 #/sys QC/QA+HMA, 2, SBS, Base, 19.0mm on 4 in. Compacted Aggregate No. 53 on Subgrade Treatment, Type IBC
- (K1) 220 #/sys QC/QA+HMA, 2, SBS, Surface, 9.5mm on 330 #/sys QC/QA+HMA, 2, SBS, Intermediate, 19.0mm on 330 #/sys QC/QA+HMA, 2, SBS, Base, 19.0mm on 330 #/sys QC/QA+HMA, 2, SBS, Base, 19.0mm on 4 in. Compacted Aggregate No. 53 on Subgrade Treatment, Type IBC
- (K2) 220 #/sys QC/QA+HMA, 2, SBS, Surface, 9.5mm on 330 #/sys QC/QA+HMA, 2, SBS, Intermediate, 19.0mm on 330 #/sys QC/QA+HMA, 2, SBS, Base, 19.0mm on 330 #/sys QC/QA+HMA, 2, SBS, Base, 19.0mm on 4 in. Compacted Aggregate No. 53 on Geotextile for Pavement, Type 2A on Subgrade Treatment, Type IBC
- (15) Curb and Gutter, Combined
- (18) Curb and Gutter, Combined Modified, Reverse
- (21) Curb and Gutter, Type B, Combined Modified, Reverse
- (29) Seed Mixture, R Mulching Fertilizer
- (54) Milled HMA Corrugations, Sinusoidal (Edgeline)

NOT FOR CONSTRUCTION

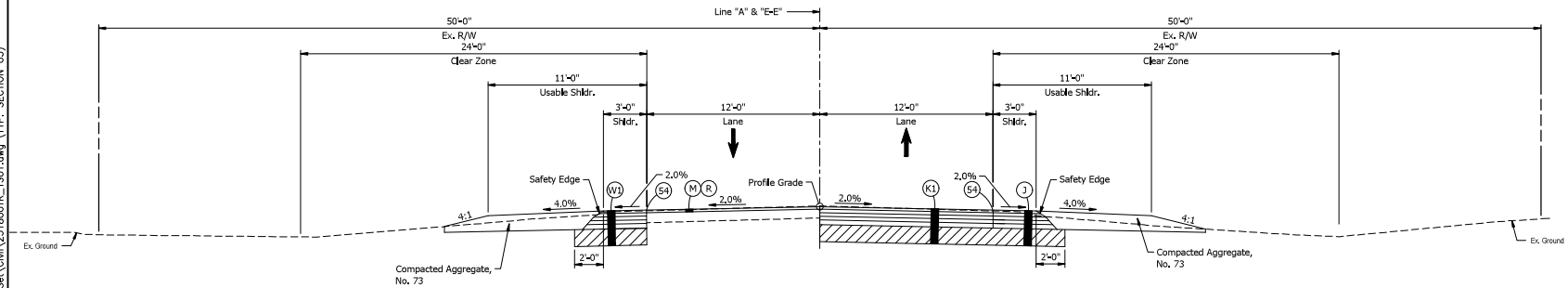
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| RECOMMENDED FOR APPROVAL | DESIGN ENGINEER | DATE |
| DESIGNED: DDE | DRAWN: GDH | |
| CHECKED: DGA | CHECKED: DGA | |

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| INDIANA DEPARTMENT OF TRANSPORTATION |
| TYPICAL SECTIONS LINE "D-W" & "E-E" |

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| SCALE | BRIDGE FILE |
| AS NOTED | DESIGNATION 2200043 |
| SURVEY BOOK | SHEETS 9 of 31 |
| CONTRACT R-4454B | PROJECT 2200043 |



TYPICAL SECTION - LINE "E-E"
Sta. 931+20.00 to Sta. 933+50.00
Scale: 1/4" = 1'-0"



TYPICAL SECTION - LINE "E-E"
Sta. 933+50.00 to Sta. 935+50.00
Scale: 1/4" = 1'-0"

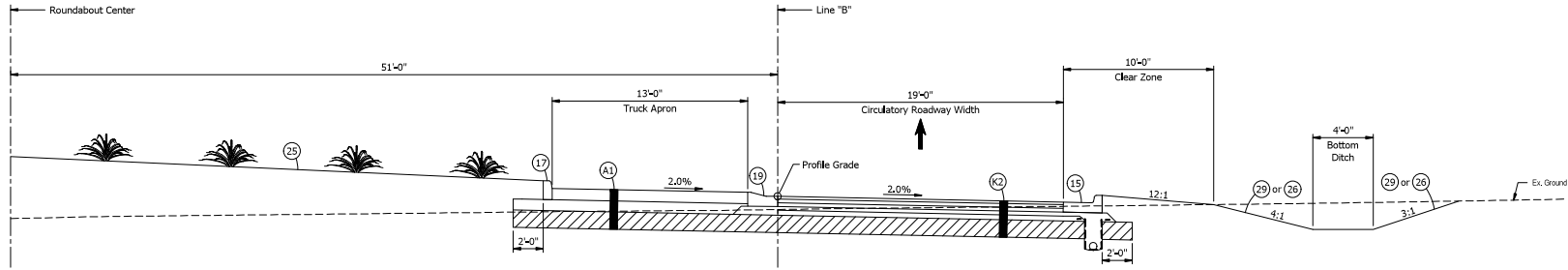
- LEGEND:**
- (J) 220 #/sys QC/QA+HMA, 2, 58S, Surface, 9.5mm on 330 #/sys QC/QA+HMA, 2, 58S, Intermediate, 19.0mm on 330 #/sys QC/QA+HMA, 2, 58S, Base, 19.0mm on 4 in. Compacted Aggregate No. 53 on Subgrade Treatment, Type IBC
 - (K1) 220 #/sys QC/QA+HMA, 2, 58S, Surface, 9.5mm on 330 #/sys QC/QA+HMA, 2, 58S, Intermediate, 19.0mm on 330 #/sys QC/QA+HMA, 2, 58S, Base, 19.0mm on 4 in. Compacted Aggregate No. 53 on Subgrade Treatment, Type IBC
 - (M) Milling, Profile (0" x 2")
 - (R) 220 #/sys QC/QA+HMA, 2, 58S, Surface, 9.5 mm
 - (W2) 220 #/sys QC/QA+HMA, 2, 58S, Surface, 9.5mm on 330 #/sys HMA Base, Type B on 330 #/sys HMA Base, Type B on 4 in. Compacted Aggregate No. 53 on Subgrade Treatment, Type IBC
 - (29) Seed Mixture, R Mulching Fertilizer
 - (S4) Milled HMA Corrugations, Sinusoidal (Edgeline)

NOT FOR CONSTRUCTION

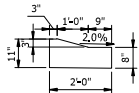
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| RECOMMENDED FOR APPROVAL _____ | | DESIGN ENGINEER _____ | | DATE _____ | |
| DESIGNED: DDE | | DRAWN: GDH | | | |
| CHECKED: DGA | | CHECKED: DGA | | | |

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| INDIANA DEPARTMENT OF TRANSPORTATION | |
| TYPICAL SECTIONS LINE "E-E" | |

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| SCALE | BRIDGE FILE |
| AS NOTED | |
| DESIGNATION | 2200043 |
| SURVEY BOOK | SHEETS |
| 10 | of 31 |
| CONTRACT | PROJECT |
| R-4454B | 2200043 |



TYPICAL SECTION - LINE "B"
Scale: 1/4" = 1'-0"



19 CURB, INTEGRAL, B,
CONCRETE, MODIFIED
Scale: 1/2" = 1'-0"

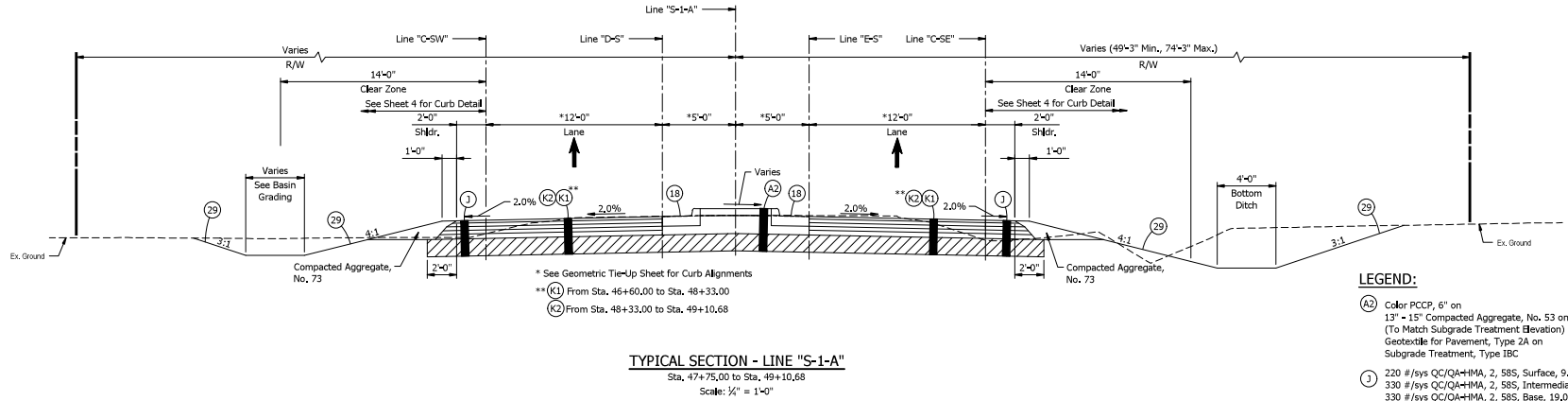
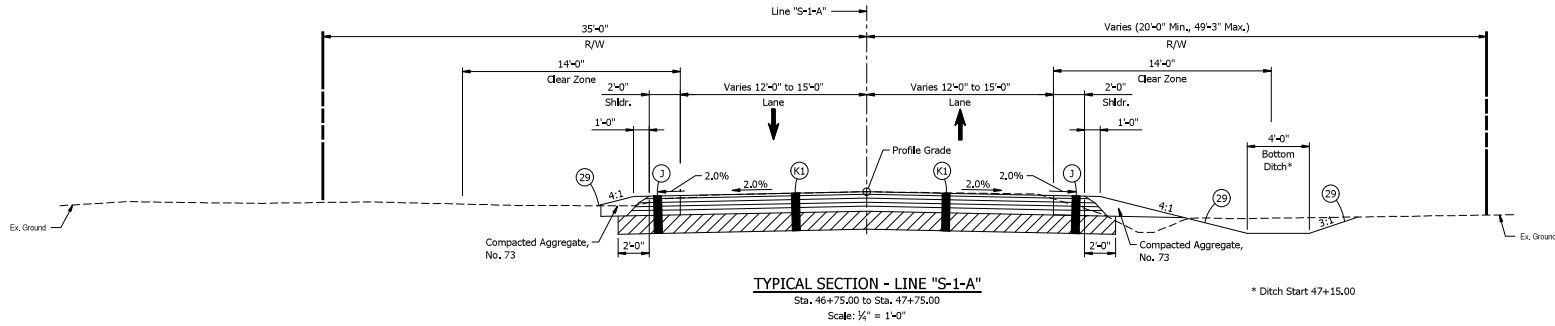
- LEGEND:**
- A1 Color PCCP 9" on 9" Compacted Aggregate, No. 53 on Geotextile for Pavement, Type 2A on Subgrade Treatment, Type IBC
 - K2 220 #/sys QC/QA+HMA, 2, 58S, Surface, 9.5mm on 330 #/sys QC/QA+HMA, 2, 58S, Intermediate, 19.0mm on 330 #/sys QC/QA+HMA, 2, 58S, Base, 19.0mm on 300 #/sys QC/QA+HMA, 4, 58E, Intermediate, OG, 19.0mm on 4 in. Compacted Aggregate No. 53 on Geotextile for Pavement, Type 2A on Subgrade Treatment, Type IBC
 - 15 Curb and Gutter, Combined
 - 17 Integral Curb
 - 19 Curb and Gutter, Type B, Modified, 3" Reveal, Reverse
 - 25 Native Seed Mixture
 - 26 Sodding
 - 29 Seed Mixture, R Matching Fertilizer

NOT FOR
CONSTRUCTION

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| RECOMMENDED FOR APPROVAL | | DESIGN ENGINEER | | DATE | |
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| CHECKED: DGA | | CHECKED: DGA | | | |

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| INDIANA DEPARTMENT OF TRANSPORTATION | |
| TYPICAL SECTION LINE "B" | |

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| SCALE | | BRIDGE FILE | |
| AS NOTED | | DESIGNATION | |
| | | 2200043 | |
| SURVEY BOOK | | SHEETS | |
| | | 11 of 31 | |
| CONTRACT | | PROJECT | |
| R-445-B | | 2200043 | |



- LEGEND:**
- (A2) Color PCCP, 6" on 13" x 15" Compacted Aggregate, No. 53 on (To Match Subgrade Treatment Elevation) Geotextile for Pavement, Type 2A on Subgrade Treatment, Type IBC
 - (J) 220 #/sys QC/QA+HMA, 2, 58S, Surface, 9.5mm on 330 #/sys QC/QA+HMA, 2, 58S, Intermediate, 19.0mm on 330 #/sys QC/QA+HMA, 2, 58S, Base, 19.0mm on 330 #/sys QC/QA+HMA, 2, 58S, Base, 19.0mm on 4 in. Compacted Aggregate No. 53 on Subgrade Treatment, Type IBC
 - (K1) 220 #/sys QC/QA+HMA, 2, 58S, Surface, 9.5mm on 330 #/sys QC/QA+HMA, 2, 58S, Intermediate, 19.0mm on 330 #/sys QC/QA+HMA, 2, 58S, Base, 19.0mm on 330 #/sys QC/QA+HMA, 2, 58S, Base, 19.0mm on 4 in. Compacted Aggregate No. 53 on Subgrade Treatment, Type IBC
 - (K2) 220 #/sys QC/QA+HMA, 2, 58S, Surface, 9.5mm on 330 #/sys QC/QA+HMA, 2, 58S, Intermediate, 19.0mm on 330 #/sys QC/QA+HMA, 2, 58S, Base, 19.0mm on 300 #/sys QC/QA+HMA, 4, 58E, Intermediate, OG, 19.0mm on 4 in. Compacted Aggregate No. 53 on Geotextile for Pavement, Type 2A on Subgrade Treatment, Type IBC
 - (18) Curb and Gutter, Combined Modified, Reverse
 - (29) Seed Mixture, R Mulching Fertilizer

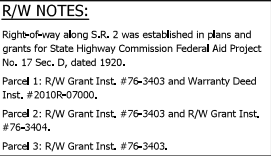
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| CHECKED: DGA | CHECKED: DGA | |

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| INDIANA DEPARTMENT OF TRANSPORTATION |
| TYPICAL SECTIONS LINE "S-1-A" |

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| SCALE | BRIDGE FILE |
| 1/4" = 1'-0" | |
| DESIGNATION | |
| 2200043 | |
| SURVEY BOOK | SHEETS |
| | 12 of 31 |
| CONTRACT | PROJECT |
| R-4454B | 2200043 |





LEGEND:

(D1) HMA for Approaches, Type B
 165 #/yds HMA Surface, Type B, on
 275 #/yds HMA Intermediate, Type B, on
 6" Compacted Aggregate, No. 53 on
 Subgrade Treatment, Type II

(11) Saw Cut

(26) Sodding

(29) Seed Mixture, R
 Mulching
 Fertilizer

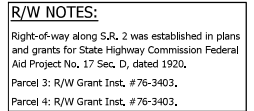
| HORIZONTAL SCALE | | BRIDGE FILE | |
|------------------|-------------|-------------|----|
| 1" = 20' | | | |
| VERTICAL SCALE | DESIGNATION | | |
| 1" = 5' | 2200043 | | |
| SURVEY BOOK | | SHEETS | |
| | 15 | of | 31 |
| CONTRACT | | PROJECT | |
| 8-4468 | | 2200043 | |

NOT FOR
CONSTRUCTION

| | | | |
|-----------------------------|--|-----------------|------|
| RECOMMENDED FOR APPROVAL | | DESIGN ENGINEER | DATE |
| DESIGNED: JDD | | DRAWN: GDH | |
| CHECKED: DGA | | CHECKED: DGA | |

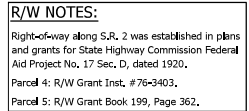
INDIANA
DEPARTMENT OF TRANSPORTATION

PLAN AND PROFILE - LINE "A"
STA. 914+00 TO STA. 918+00



| | | | |
|------------------|-------------|----|----|
| HORIZONTAL SCALE | BRIDGE FILE | | |
| 1" = 20' | | | |
| VERTICAL SCALE | DESIGNATION | | |
| 1" = 5' | 2200043 | | |
| SURVEY BOOK | SHEETS | | |
| | 16 | of | 31 |
| CONTRACT | PROJECT | | |
| R-44648 | 2200043 | | |

PLAN AND PROFILE - LINE "A"
STA. 918+00 TO STA. 922+00



Color PCCP, 6" on
13" - 15" Compacted Aggregate, No. 53 on
(To Match Subgrade Treatment Elevation)
Geotextile for Pavement, Type 2A on
Subgrade Treatment, Type IBC

J 220 # /sfs QC/QA/HMA, 2, 58S, Surface, 9.5mm on
330 # /sfs QC/QA/HMA, 2, 58S, Intermediate, 19.0mm on
330 # /sfs QC/QA/HMA, 2, 58S, Base, 19.0mm on
330 # /sfs QC/QA/HMA, 2, 58S, Base, 19.0mm on
4 in. Compacted Aggregate No. 53 on
Subgrade Treatment, Type IBC

K1 220 # /sfs QC/QA/HMA, 2, 58S, Surface, 9.5mm on
330 # /sfs QC/QA/HMA, 2, 58S, Intermediate, 19.0mm on
330 # /sfs QC/QA/HMA, 2, 58S, Base, 19.0mm on
330 # /sfs QC/QA/HMA, 2, 58S, Base, 19.0mm on
4 in. Compacted Aggregate No. 53 on
Subgrade Treatment, Type IBC

M Milling, Profile (0" - 2")

R 220 # /sfs QC/QA/HMA, 2, 58S, Surface, 9.5 mm

W1 220 # /sfs QC/QA/HMA, 2, 58S, Surface, 9.5mm on
330 # /sfs HMA Intermediate, Type B on
330 # /sfs HMA Base, Type B on
330 # /sfs HMA Base, Type B on
4 in. Compacted Aggregate No. 53 on
Subgrade Treatment, Type IBC

18 Curb and Gutter, Combined Modified, Reverse

21 Curb and Gutter, Type B, Combined Modified, Reverse

26 Sodding

29 Seed Mixture, R
Mulching
Fertilizer

92 Remove Existing Sign & Supports

92 Tree Removal

| | | | |
|------------------|--|-------------|----|
| HORIZONTAL SCALE | | BRIDGE FILE | |
| 1" = 20' | | | |
| VERTICAL SCALE | | DESIGNATION | |
| 1" = 5' | | 2200043 | |
| SURVEY BOOK | | SHEETS | |
| | | 17 | 31 |
| CONTRACT | | PROJECT | |
| R-44648 | | 2200043 | |

NOT FOR
CONSTRUCTION

| | | |
|-----------------------------|-----------------|------|
| RECOMMENDED FOR APPROVAL | DESIGN ENGINEER | DATE |
| DESIGNED: DDE | DRAWN: GDH | |
| CHECKED: DGA | CHECKED: DGA | |

PLAN AND PROFILE - LINE "A"
STA. 922+00 TO STA. 926+00

506 N=370089.1445
E=773992.7866

Tel. Pedestal
(GTE)

55.9'
Az=30°

93.8'
Az=108°

Sign (No
Parking)

S.B. 2
Line "A"

P.W.
(575/173)

53.1'
Az=210°

P.C. Sta. 924+94.12 "A"
Mag Spike w/ "Lawson-Fisher" Washer

R/W NOTES:

Right-of-way along S.R. 2 was established in plans and grants for State Highway Commission Federal Aid Project No. 17 Sec. D, dated 1920.

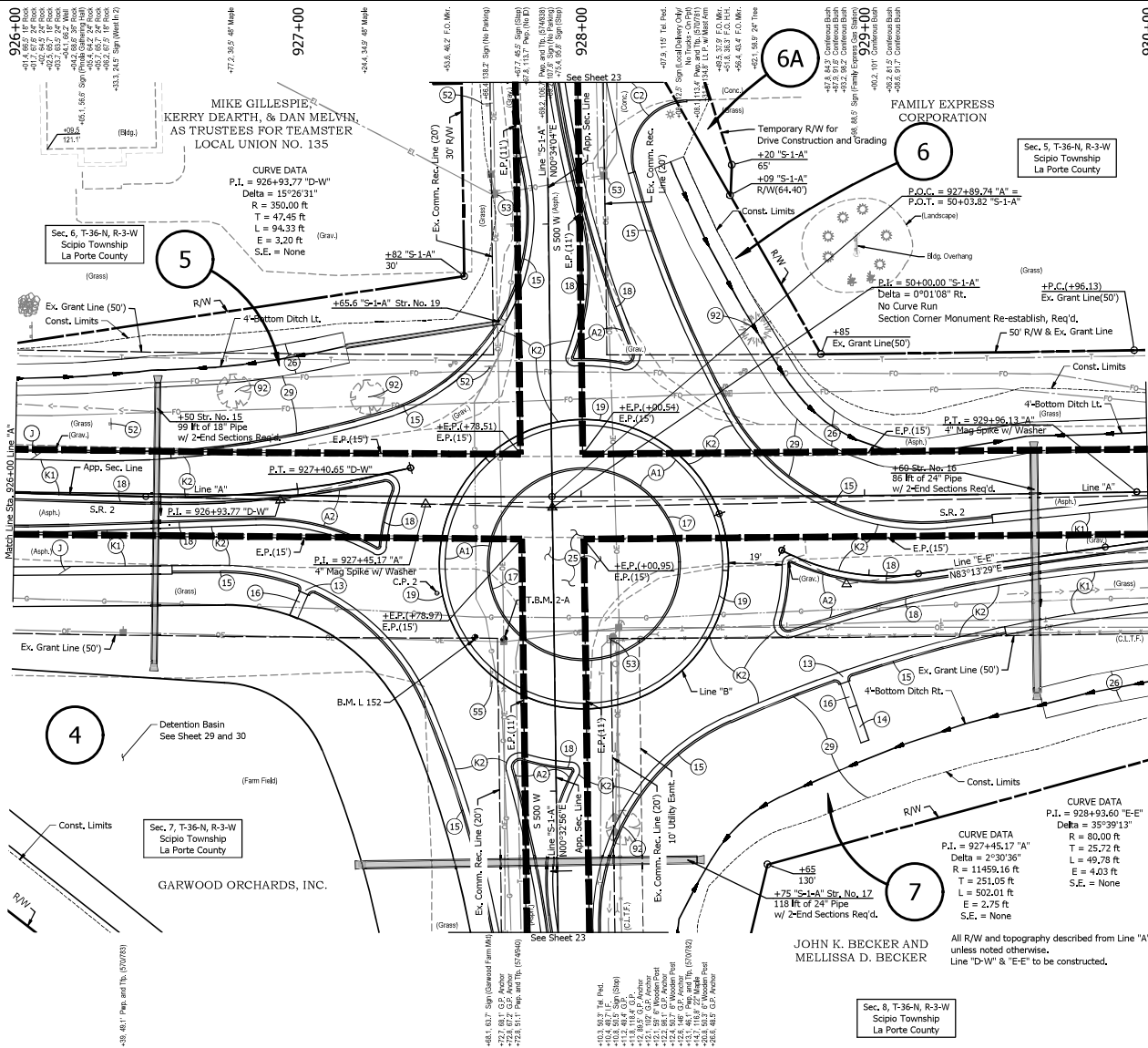
Right-of-way along C.R. 500 W was established by Commissioner's Record Book B, Pages 226, 227, and 534, dated 1841.

Parcel 4: R/W Grant Inst. # 76-3403 and Commissioner's Record Book B, Pages 226, 227, and 534.

Parcel 5: R/W Grant Book 199, Page 362 and Commissioner's Record Book B, Pages 226, 227, and 534.

Parcel 6: R/W Grant Inst. # 76-3405 and Commissioner's Record Book B, Pages 226, 227, and 534.

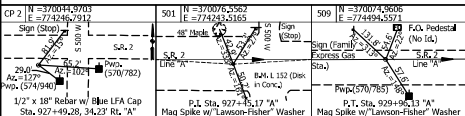
Parcel 7: R/W Grant Inst. # 76-3406 and Commissioner's Record Book B, Pages 226, 227, and 534.



LEGEND:

- (A1) Color PCP 9" on 9" Compacted Aggregate, No. 53 on Geotextile for Pavement, Type 2A on Subgrade Treatment, Type IBC
- (A2) Color PCP 6" on 13" - 15" Compacted Aggregate, No. 53 on (To Match Subgrade Treatment Elevation) Geotextile for Pavement, Type 2A on Subgrade Treatment, Type IBC
- (C2) PCP for Approaches, 9" on Dense Graded Subbase, 6" on Geogrid, Type IB on Subgrade Treatment, Type II
- (J) 220 #/sqs QC/QA+HMA, 2, 58S, Surface, 9.5mm on 330 #/sqs QC/QA+HMA, 2, 58S, Intermediate, 19.0mm on 330 #/sqs QC/QA+HMA, 2, 58S, Base, 19.0mm on 330 #/sqs QC/QA+HMA, 2, 58S, Base, 19.0mm on 4 in. Compacted Aggregate No. 53 on Subgrade Treatment, Type IBC
- (K1) 220 #/sqs QC/QA+HMA, 2, 58S, Surface, 9.5mm on 330 #/sqs QC/QA+HMA, 2, 58S, Intermediate, 19.0mm on 330 #/sqs QC/QA+HMA, 2, 58S, Base, 19.0mm on 330 #/sqs QC/QA+HMA, 2, 58S, Base, 19.0mm on 4 in. Compacted Aggregate No. 53 on Subgrade Treatment, Type IBC
- (K2) 220 #/sqs QC/QA+HMA, 2, 58S, Surface, 9.5mm on 330 #/sqs QC/QA+HMA, 2, 58S, Intermediate, 19.0mm on 330 #/sqs QC/QA+HMA, 2, 58S, Base, 19.0mm on 300 #/sqs QC/QA+HMA, 4, 58E, Intermediate, OG, 19.0mm on 4 in. Compacted Aggregate No. 53 on Subgrade Treatment, Type IBC
- (13) Curb and Gutter, Turnout Combined
- (14) Paved Side Ditch, Type A
- (15) Curb and Gutter, Combined
- (16) Concrete Curb, A
- (17) Integral Curb
- (18) Curb and Gutter, Combined Modified, Reverse
- (19) Curb and Gutter, Type B, Modified, 3" Reveal, Reverse
- (25) Native Seed Mixture
- (26) Sodding
- (29) Seed Mixture, R Mulching Fertilizer
- (52) Remove Existing Sign & Supports
- (53) Remove Existing Sign From Existing Pole
- (55) Sign Relocate
- (92) Tree Removal

JAN - 12/17/2024 1:51 PM - U:\2023\202318 INDOT SR2 Roundabout\Cor Plan Set\Civil\231800TR_P103.dwg (PLAN - 926+00 TO 930+00 - LINE A)

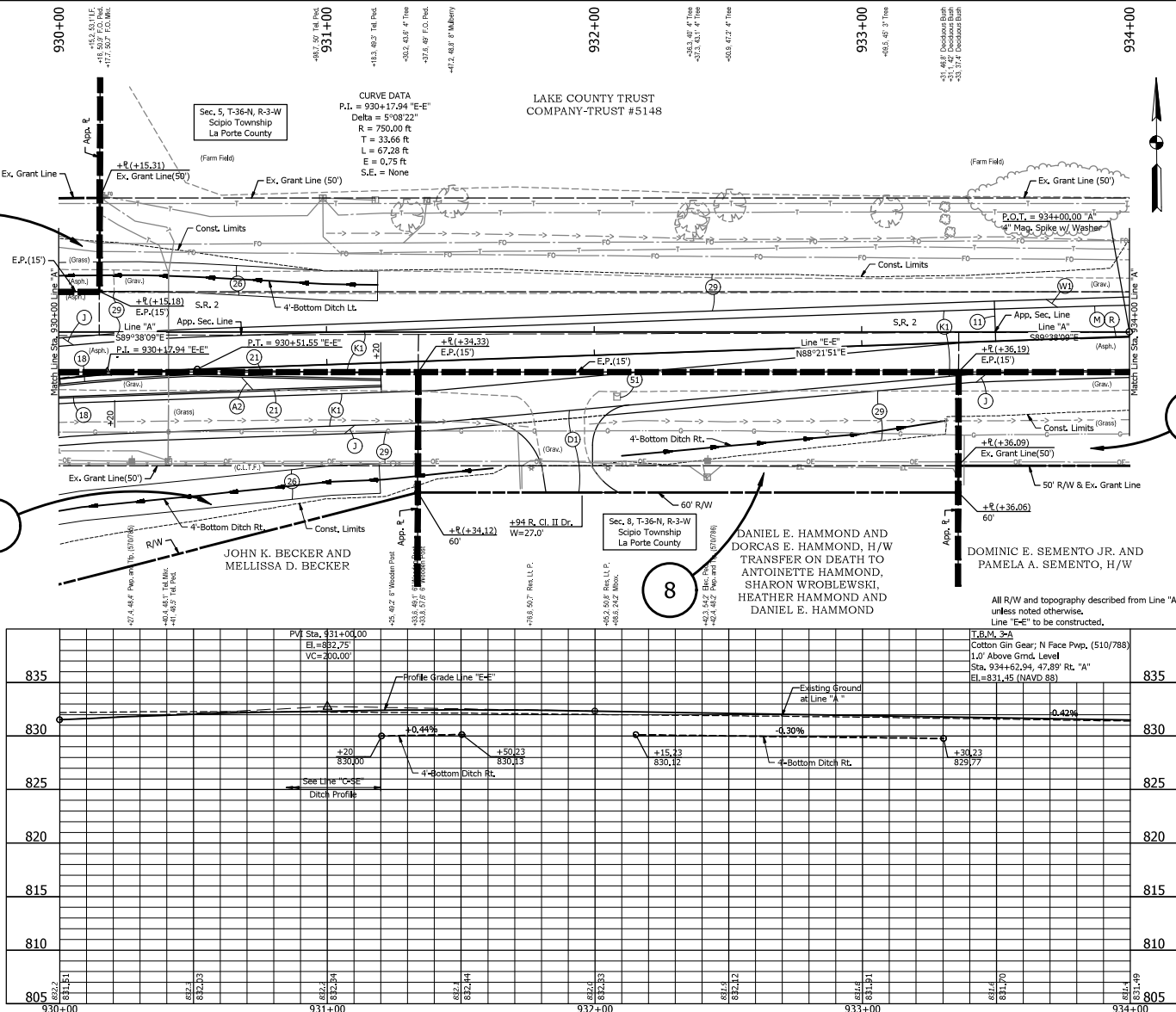


NOT FOR CONSTRUCTION

| RECOMMENDED FOR APPROVAL | | DESIGN ENGINEER | | DATE | |
|--------------------------|--|-----------------|--|------|--|
| DESIGNED: DDE | | DRAWN: GDH | | | |
| CHECKED: DGA | | CHECKED: DGA | | | |

| INDIANA DEPARTMENT OF TRANSPORTATION | | SCALE | | BRIDGE FILE | |
|--------------------------------------|--|-------------|--|-------------|--|
| PLAN - LINE "A" | | 1" = 20' | | DESIGNATION | |
| | | | | 2200043 | |
| STA. 926+00 TO STA. 930+00 | | SURVEY BOOK | | SHEETS | |
| | | | | 18 of 31 | |
| | | CONTRACT | | PROJECT | |
| | | R-4464B | | 2200043 | |

R/W NOTES:
 Right-of-way along S.R. 2 was established in plans and grants for State Highway Commission Federal Aid Project No. 17 Sec. D, dated 1920.
 Parcel 6: R/W Grant Inst. #76-3405.
 Parcel 7: R/W Grant Inst. #76-3406.
 Parcel 8: R/W Grant Inst. #76-3406.
 Parcel 9: R/W Grant Inst. #76-3406.

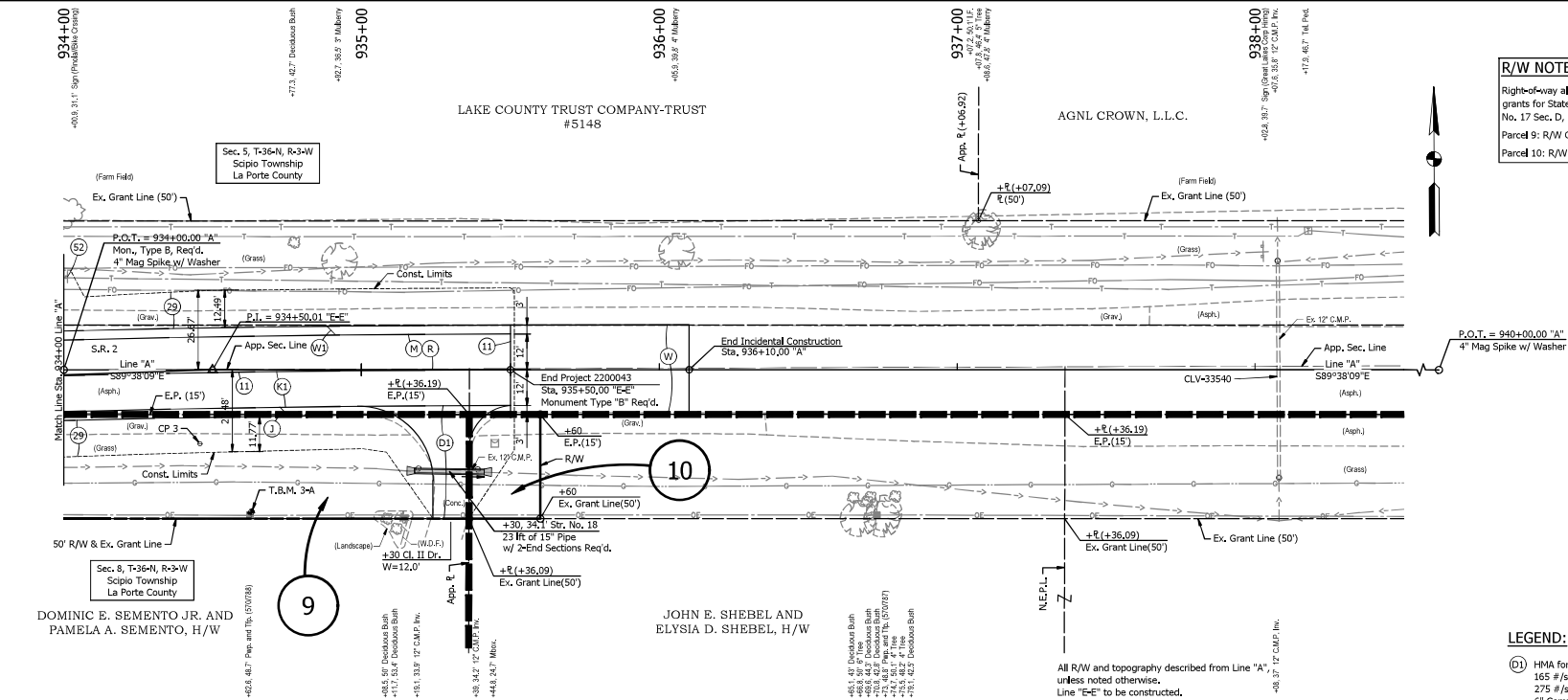


NOT FOR CONSTRUCTION

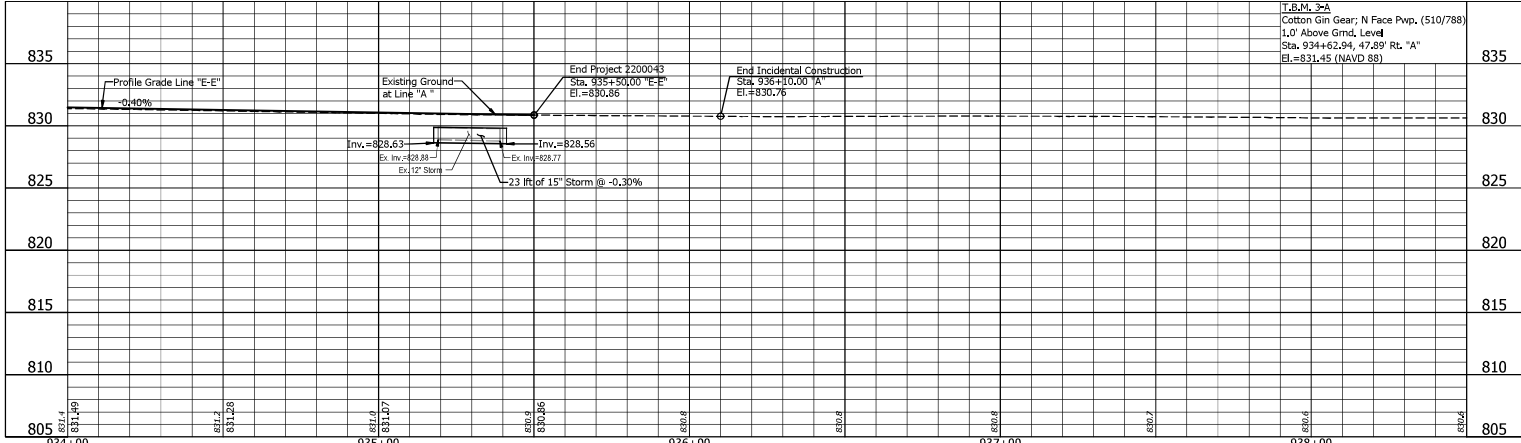
RECOMMENDED FOR APPROVAL
 DESIGN ENGINEER DATE
 DESIGNED: DDE DRAWN: GDH
 CHECKED: DGA CHECKED: DGA

INDIANA
 DEPARTMENT OF TRANSPORTATION
 PLAN AND PROFILE - LINE "A"
 STA. 930+00 TO STA. 934+00

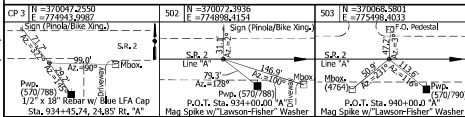
| | |
|------------------------------|------------------------|
| HORIZONTAL SCALE 1" = 20' | BRIDGE FILE |
| VERTICAL SCALE 1" = 5' | DESIGNATION 2200043 |
| SURVEY BOOK | SHEETS 20 of 31 |
| CONTRACT R-44618 | PROJECT 2200043 |



R/W NOTES:
 Right-of-way along S.R. 2 was established in plans and grants for State Highway Commission Federal Aid Project No. 17 Sec. D, dated 1920.
 Parcel 9: R/W Grant Inst. #76-3406.
 Parcel 10: R/W Grant Inst. #76-3406.

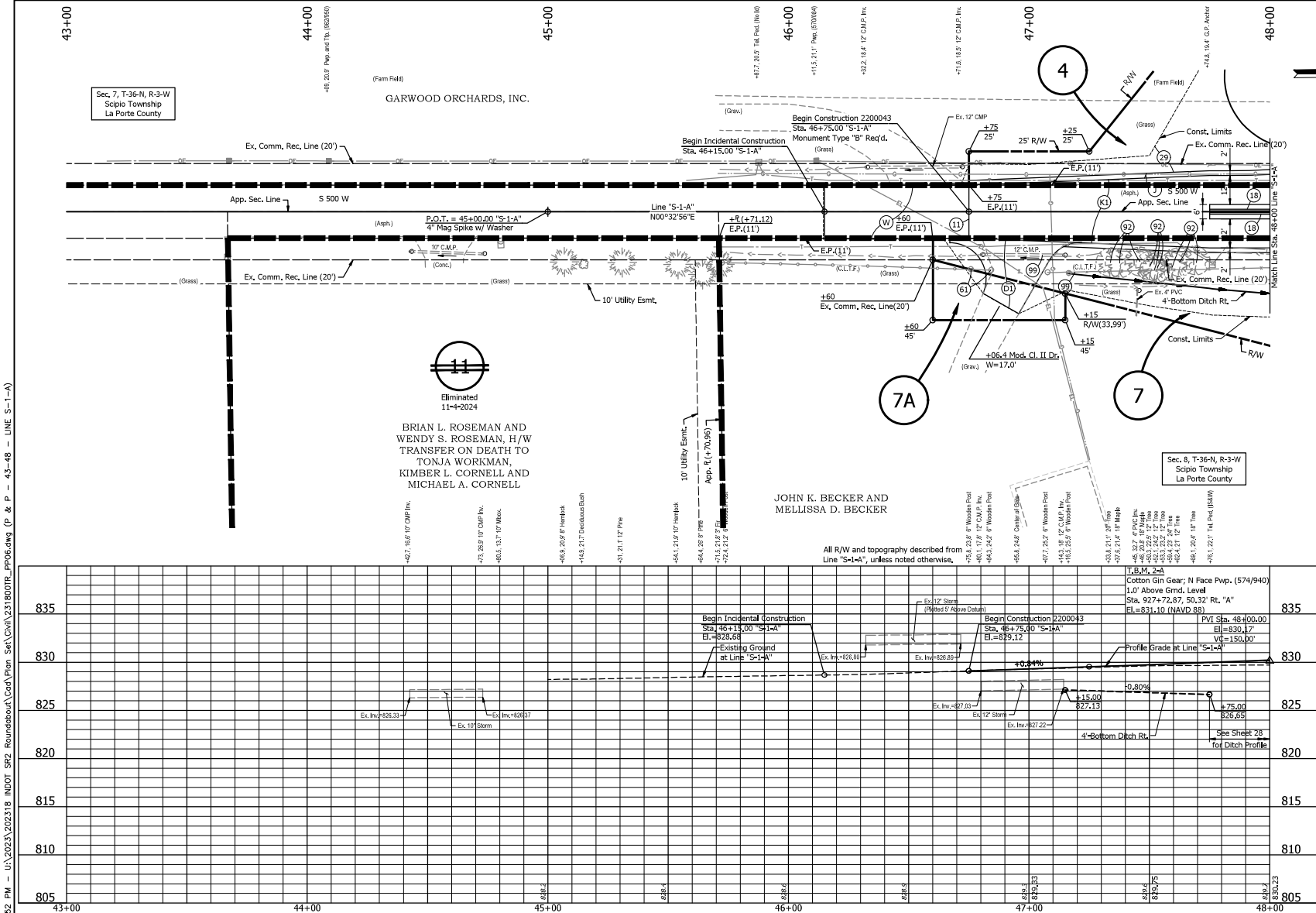


- LEGEND:**
- (D1) HMA for Approaches, Type B
 165 #/sys HMA Surface, Type B, on
 275 #/sys HMA Intermediate, Type B, on
 6" Compacted Aggregate, No. 53 on
 Subgrade Treatment, Type II
 - (J) 220 #/sys QC/QA-HMA, 2, S85, Surface, 9.5mm on
 330 #/sys QC/QA-HMA, 2, S85, Intermediate, 19.0mm on
 330 #/sys QC/QA-HMA, 2, S85, Base, 19.0mm on
 330 #/sys QC/QA-HMA, 2, S85, Base, 19.0mm on
 4 in. Compacted Aggregate No. 53 on
 Subgrade Treatment, Type IBC
 - (K) 220 #/sys QC/QA-HMA, 2, S85, Surface, 9.5mm on
 330 #/sys QC/QA-HMA, 2, S85, Intermediate, 19.0mm on
 330 #/sys QC/QA-HMA, 2, S85, Base, 19.0mm on
 330 #/sys QC/QA-HMA, 2, S85, Base, 19.0mm on
 4 in. Compacted Aggregate No. 53 on
 Subgrade Treatment, Type IBC
 - (M) Milling, Profile (0" - 2")
 - (R) 220 #/sys QC/QA-HMA, 2, S85, Surface, 9.5 mm
 - (W) 220 #/sys QC/QA-HMA, 2, S85, Surface, 9.5mm on
 Transition Milling
 - (W1) 220 #/sys QC/QA-HMA, 2, S85, Surface, 9.5mm on
 330 #/sys HMA Intermediate, Type B on
 330 #/sys HMA Base, Type B on
 330 #/sys HMA Base, Type B on
 4 in. Compacted Aggregate No. 53 on
 Subgrade Treatment, Type IBC
 - (11) Saw Cut
 - (29) Seed Mixture, R
 Mulching
 Fertilizer
 - (52) Remove Existing Sign & Supports



NOT FOR CONSTRUCTION

| | | | | | |
|---|--|------------------------|--|------|--|
| RECOMMENDED FOR APPROVAL | | DESIGN ENGINEER | | DATE | |
| DESIGNED: DDE | | DRAWN: GDH | | | |
| CHECKED: DGA | | CHECKED: DGA | | | |
| INDIANA DEPARTMENT OF TRANSPORTATION | | | | | |
| PLAN AND PROFILE - LINE "A" STA. 934+00 TO STA. 938+50 | | | | | |
| HORIZONTAL SCALE 1" = 20' | | BRIDGE FILE | | | |
| VERTICAL SCALE 1" = 5' | | DESIGNATION 2200043 | | | |
| SURVEY BOOK | | SHEETS | | | |
| CONTRACT R-446-48 | | PROJECT 2200043 | | | |



R/W NOTES:
Right-of-way along C.R. 500 W was established by Commissioner's Record Book B, Pages 226, 227, and 534, dated 1841.
Parcel 4: Commissioner's Record Book B, Pages 226, 227, and 534.
Parcel 7: Commissioner's Record Book B, Pages 226, 227, and 534.

- LEGEND:**
- (D1) HMA for Approaches, Type B
 - 165 #/sys HMA Surface, Type B, on
 - 275 #/sys HMA Intermediate, Type B, on
 - 6" Compacted Aggregate, No. 53 on
 - Subgrade Treatment, Type II
 - (J) 220 #/sys QC/QA-HMA, 2, 585, Surface, 9.5mm on
 - 330 #/sys QC/QA-HMA, 2, 585, Intermediate, 19.0mm on
 - 330 #/sys QC/QA-HMA, 2, 585, Base, 19.0mm on
 - 330 #/sys QC/QA-HMA, 2, 585, Base, 19.0mm on
 - 4 in. Compacted Aggregate No. 53 on
 - Subgrade Treatment, Type IBC
 - (K1) 220 #/sys QC/QA-HMA, 2, 585, Surface, 9.5mm on
 - 330 #/sys QC/QA-HMA, 2, 585, Intermediate, 19.0mm on
 - 330 #/sys QC/QA-HMA, 2, 585, Base, 19.0mm on
 - 330 #/sys QC/QA-HMA, 2, 585, Base, 19.0mm on
 - 4 in. Compacted Aggregate No. 53 on
 - Subgrade Treatment, Type IBC
 - (W) 220 #/sys QC/QA-HMA, 2, 585, Surface, 9.5mm on
 - Transition Milling
 - (11) Saw Cut
 - (18) Curb and Gutter, Combined Modified, Reverse
 - (29) Seed Mixture, R
 - Mulching
 - Fertilizer
 - (61) Fence, Remove and Reset
 - (92) Tree Removal
 - (99) Remove

504 IN - 389574.1380
E = 774263.3629
P.O.B. Sta. 45+00.00 "S-1-A"
Main Spike w/ "Lawson-Fisher" Washer

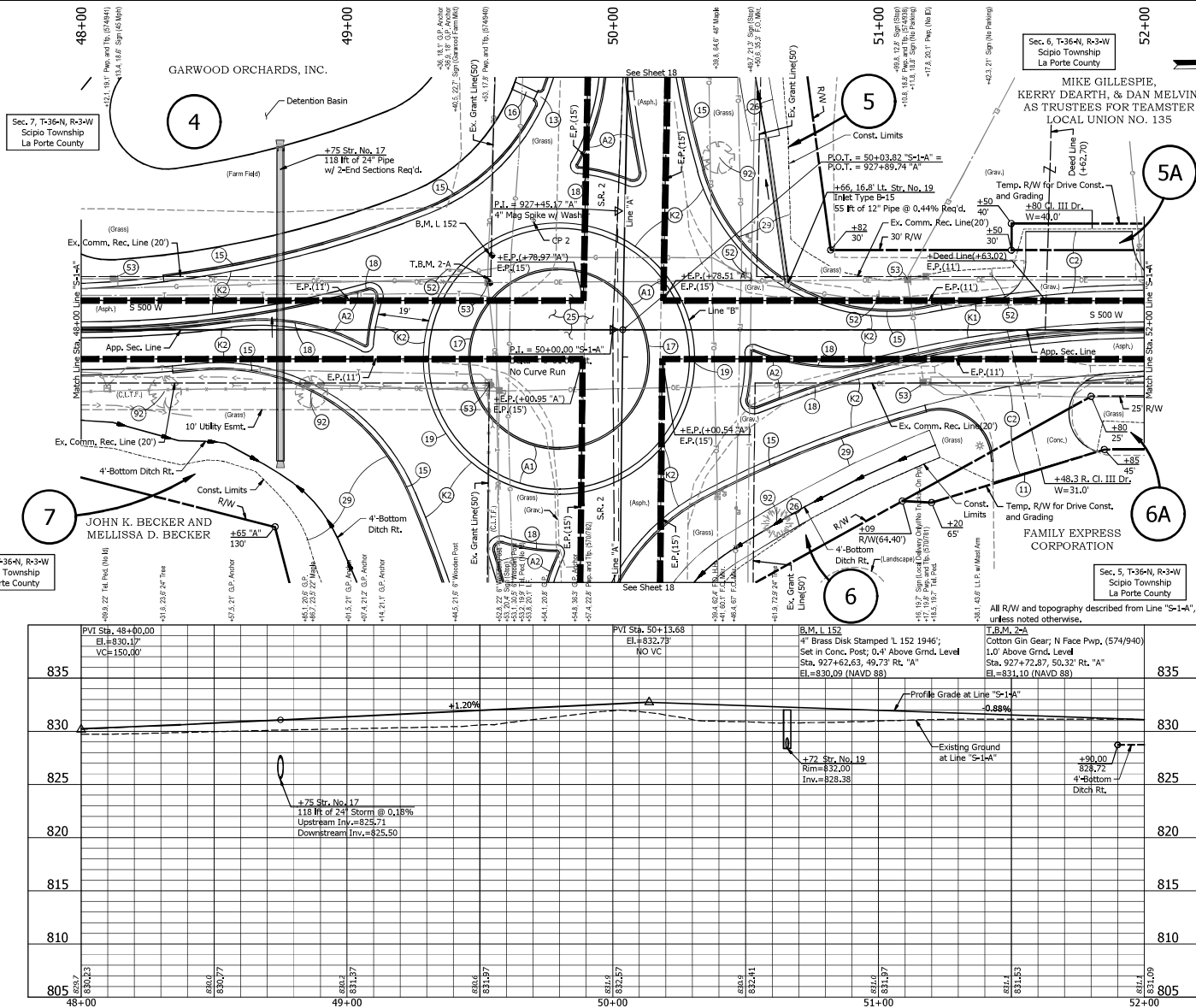
NOT FOR CONSTRUCTION

| | | |
|--------------------------|-----------------|------|
| RECOMMENDED FOR APPROVAL | DESIGN ENGINEER | DATE |
| DESIGNED: DDE | DRAWN: GDH | |
| CHECKED: DGA | CHECKED: DGA | |

INDIANA
DEPARTMENT OF TRANSPORTATION

PLAN AND PROFILE - LINE "S-1-A"
STA. 43+00 TO STA. 48+00

| | |
|------------------|-------------|
| HORIZONTAL SCALE | BRIDGE FILE |
| 1" = 20' | |
| VERTICAL SCALE | DESIGNATION |
| 1" = 5' | 2200043 |
| SURVEY BOOK | SHEETS |
| | 22 of 31 |
| CONTRACT | PROJECT |
| R-4464B | 2200043 |



R/W NOTES:

Right-of-way along S.R. 2 was established in plans and grants for State Highway Commission Federal Aid Project No. 17 Sec. D, dated 1920.

Right-of-way along C.R. 500 W was established by Commissioner's Record Book B, Pages 226, 227, and 534, dated 1841.

Parcel 4: R/W Grant Inst. #76-3403 and Commissioner's Record Book B, Pages 226, 227, and 534.

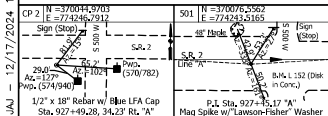
Parcel 5: R/W Grant Book 199, Page 362 and Commissioner's Record Book B, Pages 226, 227, and 534.

Parcel 6: R/W Grant Inst. #76-3405 and Commissioner's Record Book B, Pages 226, 227, and 534.

Parcel 7: R/W Grant Inst. #76-3406 and Commissioner's Record Book B, Pages 226, 227, and 534.

- LEGEND:**
- (A1) Color PCCP 9" on 9" Compacted Aggregate, No. 53 on Geotextile for Pavement, Type 2A on Subgrade Treatment, Type IBC
 - (A2) Color PCCP 6" on 13" - 15" Compacted Aggregate, No. 53 on (To Match Subgrade Treatment Elevation) Geotextile for Pavement, Type 2A on Subgrade Treatment, Type IBC
 - (C2) PCCP for Approaches, 9" on Dense Graded Subbase, 6" on Geosid, Type IB on Subgrade Treatment, Type II
 - (K1) 220 #/sq CC/QA-HMA, 2, 585, Surface, 9.5mm on 330 #/sq CC/QA-HMA, 2, 585, Intermediate, 19.0mm on 330 #/sq CC/QA-HMA, 2, 585, Base, 19.0mm on 4 in. Compacted Aggregate No. 53 on Subgrade Treatment, Type IBC
 - (K2) 220 #/sq CC/QA-HMA, 2, 585, Surface, 9.5mm on 330 #/sq CC/QA-HMA, 2, 585, Intermediate, 19.0mm on 330 #/sq CC/QA-HMA, 2, 585, Base, 19.0mm on 4 in. Compacted Aggregate No. 53 on Geotextile for Pavement, Type 2A on Subgrade Treatment, Type IBC
 - (11) Saw Cut
 - (13) Curb and Gutter, Turnout Combined
 - (15) Curb and Gutter, Combined
 - (16) Concrete Gutter, A
 - (17) Integral Curb
 - (18) Curb and Gutter, Combined Modified, Reverse
 - (19) Curb and Gutter, Type B, Modified, 3" Reveal, Reverse
 - (25) Native Seed Mixture
 - (26) Sodding
 - (29) Seed Mixture, R Mulching Fertilizer
 - (52) Remove Existing Sign & Supports

JAN - 12/17/2024 1:52 PM - U:\2023\202318 INDOT SR2 Roundabout\Con\Plan Set\Civil\231800TR_P007.dwg (P & P - 48-52 - LINE S-1-A)



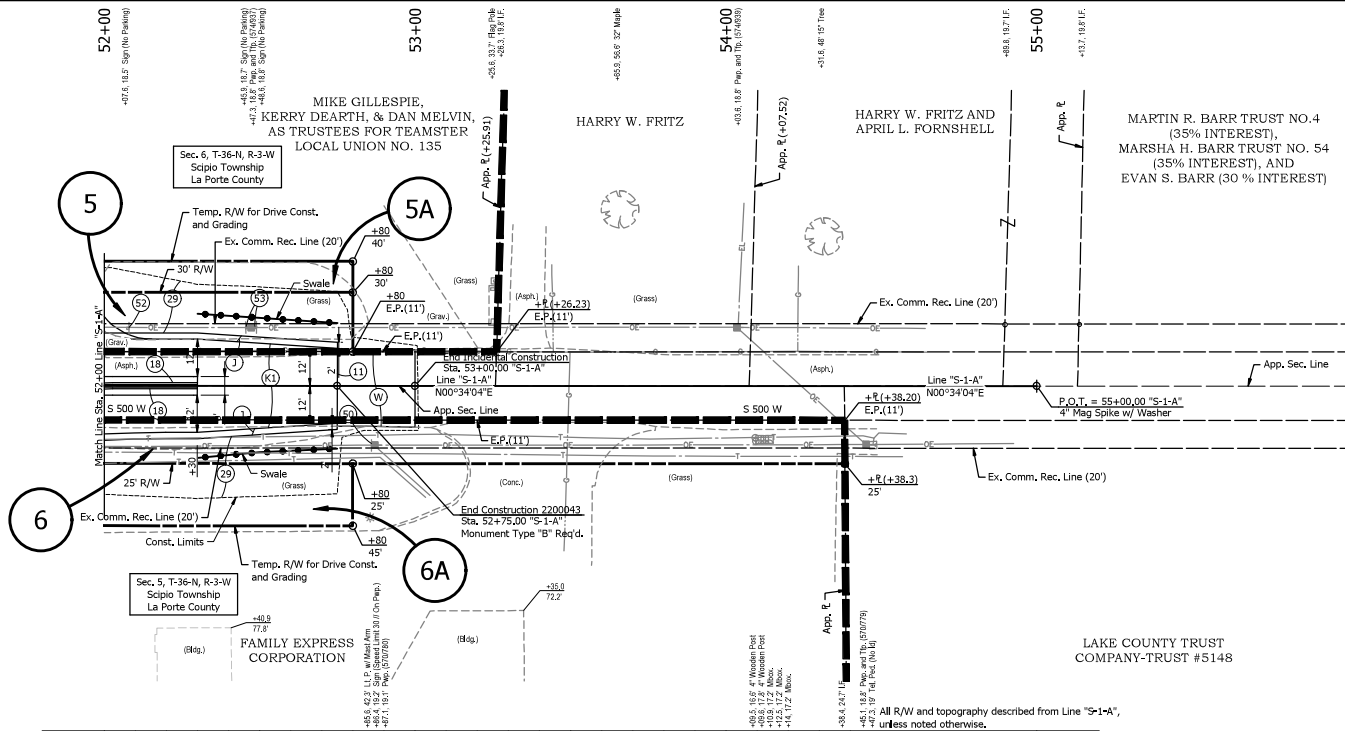
NOT FOR CONSTRUCTION

| | | |
|---------------|--------------|-------|
| DESIGNED: DDE | DRAWN: GDH | DATE: |
| CHECKED: DGA | CHECKED: DGA | |

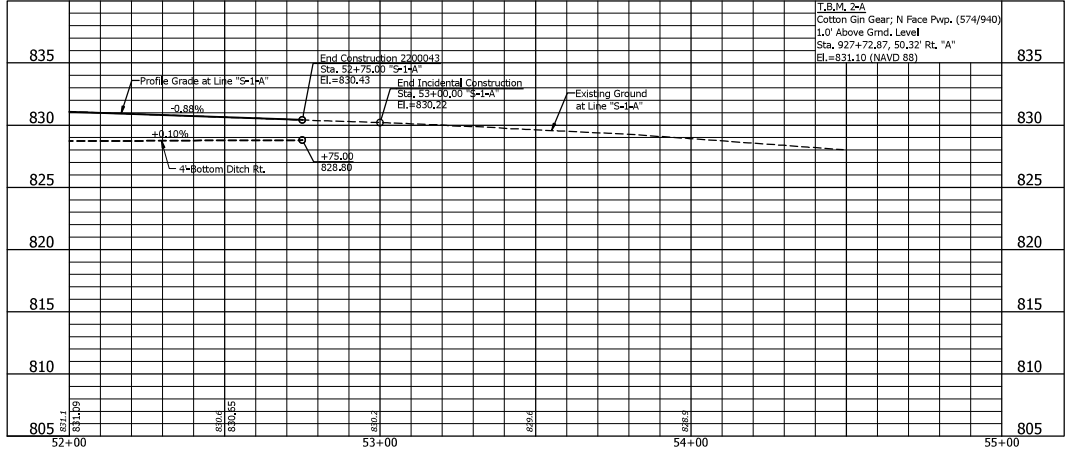
INDIANA
DEPARTMENT OF TRANSPORTATION

PLAN AND PROFILE - LINE "S-1-A"
STA. 48+00 TO STA. 52+00

| | |
|------------------------------|------------------------|
| HORIZONTAL SCALE 1" = 20' | BRIDGE FILE |
| VERTICAL SCALE 1" = 5' | DESIGNATION 2200043 |
| SURVEY BOOK | SHEETS 23 of 31 |
| CONTRACT R-44648 | PROJECT 2200043 |

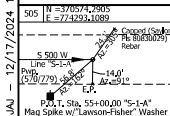


R/W NOTES:
Right-of-way along C.R. 500 W was established by Commissioner's Record Book B, Pages 226, 227, and 534, dated 1841.
Parcel 5: Commissioner's Record Book B, Pages 226, 227, and 534.
Parcel 6: Commissioner's Record Book B, Pages 226, 227, and 534.



- LEGEND:**
- (J) 220 #/sys QC/QA+HMA, 2, S8S, Surface, 9.5mm on 330 #/sys QC/QA+HMA, 2, S8S, Intermediate, 19.0mm on 330 #/sys QC/QA+HMA, 2, S8S, Base, 19.0mm on 330 #/sys QC/QA+HMA, 2, S8S, Base, 19.0mm on 4 in. Compacted Aggregate No. 53 on Subgrade Treatment, Type IBC
 - (K) 220 #/sys QC/QA+HMA, 2, S8S, Surface, 9.5mm on 330 #/sys QC/QA+HMA, 2, S8S, Intermediate, 19.0mm on 330 #/sys QC/QA+HMA, 2, S8S, Base, 19.0mm on 330 #/sys QC/QA+HMA, 2, S8S, Base, 19.0mm on 4 in. Compacted Aggregate No. 53 on Subgrade Treatment, Type IBC
 - (W) 220 #/sys QC/QA+HMA, 2, S8S, Surface, 9.5mm on Transition Milling
 - (11) Saw Cut
 - (18) Curb and Gutter, Combined Modified, Reverse
 - (29) Seed Mixture, R Mulching Fertilizer
 - (50) No Change Required to Existing Sign and Supports
 - (52) Remove Existing Sign & Supports
 - (53) Remove Existing Sign From Existing Pole

U:\2023\202318 INDOT SR2 Roundabout\Con\Plan Set\G:\231800TR_PP08.dwg (P & P - 52-55 - LINE S-1-A)



NOT FOR CONSTRUCTION

| | | |
|--------------------------|-----------------|------|
| RECOMMENDED FOR APPROVAL | DESIGN ENGINEER | DATE |
| DESIGNED: DDE | DRAWN: GDH | |
| CHECKED: DGA | CHECKED: DGA | |

| |
|--------------------------------------|
| INDIANA DEPARTMENT OF TRANSPORTATION |
| PLAN AND PROFILE - LINE "S-1-A" |
| STA. 52+00 TO STA. 55+00 |

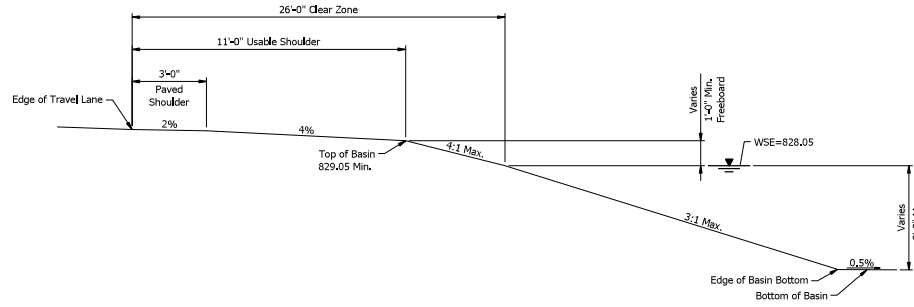
| | |
|------------------|-------------|
| HORIZONTAL SCALE | BRIDGE FILE |
| 1" = 20' | |
| VERTICAL SCALE | DESIGNATION |
| 1" = 5' | 2200043 |
| SURVEY BOOK | SHEETS |
| | 24 of 31 |
| CONTRACT | PROJECT |
| R-4454B | 2200043 |



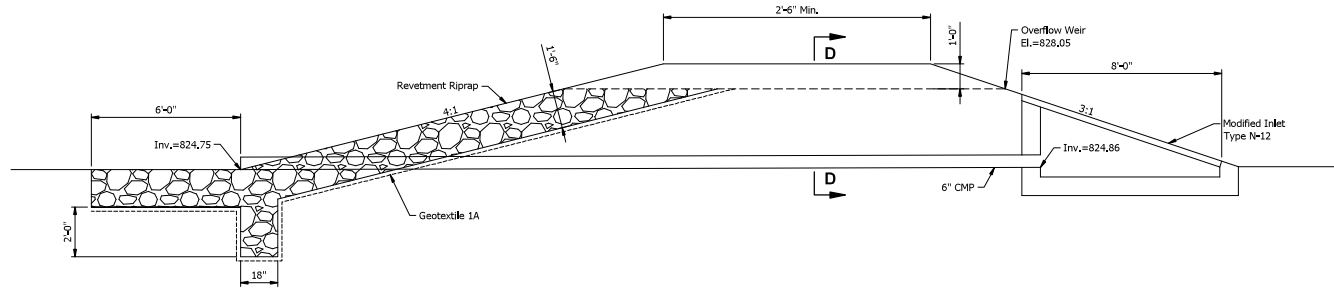
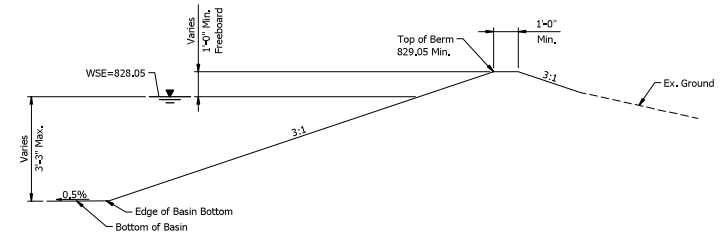
NOT FOR
CONSTRUCTION

Mulching Fertilizer

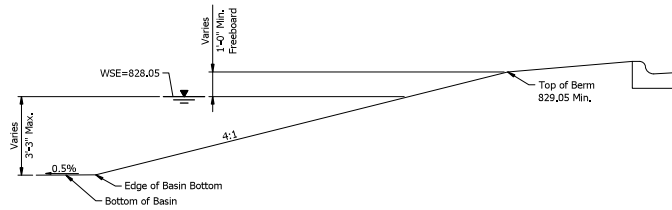
PJV - 12/17/2024 1:53 PM - U:\2023\202318 INDOT SR2 Roundabout\Cad\Plan Set\Civil\231800TR_GP01.dwg (DETENTION BASIN)



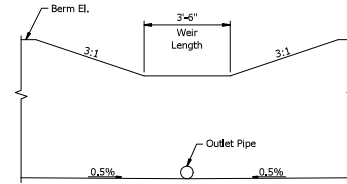
SECTION A-A
Not to Scale



SECTION B-B
Not to Scale



SECTION C-C
Not to Scale



SECTION D-D
Not to Scale

NOT FOR
CONSTRUCTION

| | | |
|-----------------------------|-----------------|------|
| RECOMMENDED FOR APPROVAL | DESIGN ENGINEER | DATE |
| DESIGNED: DJT | DRAWN: PIV | |
| CHECKED: DGA | CHECKED: DGA | |

INDIANA
DEPARTMENT OF TRANSPORTATION

DETENTION BASIN SECTIONS

| | |
|--------------|-------------|
| SCALE | BRIDGE FILE |
| NOT TO SCALE | |
| DESIGNATION | |
| 2200043 | |
| SURVEY BOOK | SHEETS |
| 30 | of 31 |
| CONTRACT | PROJECT |
| R-4464B | 2200043 |

Appendix G. Affidavit of Publication and Public Notice

AFFP

Public Notice Des. No. 2200043

Affidavit of Publication

STATE OF IN }
COUNTY OF LA PORTE } SS

Meagan Hall, being duly sworn, says:

That she is Accounting Clerk of the La Porte County Herald Dispatch, a daily newspaper of general circulation, printed and published in Michigan City, La Porte County, IN; that the publication, a copy of which is attached hereto, was published in the said newspaper on the following

July 09, 2025

Publication Fees: \$ 52.61

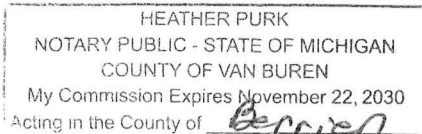
That said newspaper was regularly issued and circulated on those dates.

SIGNED:



Subscribed to and sworn to me this 9th day of July 2025.

Heather Purk, Notary Public 11/22/2030



70105571 71268161

Karen M Blake
Metric Environmental
6958 Hillside Court
Indianapolis, IN 46250

Public Notice
Des. No. 2200043

The Indiana Department of Transportation (INDOT) is planning to undertake an intersection improvement project, funded in part by the Federal Highway Administration. The project is located at the intersection of State Road (SR) 2 and County Road (CR) 500 West in LaPorte County, Indiana, southwest of the City of La Porte between United States Highway (US) 421 and US 35.

Under the preferred alternative, the proposed project will involve constructing a single-lane roundabout closely centered to the existing intersection with a truck apron. Along SR 2, the approach work is estimated to begin approximately 1,640 feet west of CR 500 W and terminate approximately 760 feet east of CR 500 W. Along CR 500 W, the approach work is estimated to begin approximately 300 feet north of SR 2 and terminate approximately 435 feet south of SR 2. Curb and gutter will be installed within the limits of the roundabout to drain excess runoff into a new detention basin to be constructed in the southwest quadrant of the roundabout. Ancillary work involves removing existing trees along the roadway, relocating overhead utilities, and installation of lighting within the project area. The anticipated amount of right-of-way acquisition is 2.020 acres of permanent right-of-way and 3.619 acres of reacquired permanent right-of-way (totaling 5.639 acres), and 0.119 acre of temporary right-of-way. No relocations of properties are anticipated for this project.

The proposed action does not impact properties listed in or eligible for the National Register of Historic Places. The Indiana Department of Transportation (INDOT), on behalf of the FHWA, has issued a "No Historic Properties Affected" finding for the project due to the fact that no historic properties are present within the Area of Potential Effects (APE). In accordance with the National Historic Preservation Act, the views of the public are being sought regarding the effect of the proposed project on the historic elements as per 36 CFR 800.2(d), 800.3(e) and 800.6(a)(4). Pursuant to 36 CFR 800.4(d)(1), the documentation specified in 36 CFR 800.11(d) is available for inspection in Metric Environmental, LLC. Additionally, this documentation can be viewed electronically by accessing INDOT's Section 106 document posting website IN SCOPE at <http://erms12c.indot.in.gov/Section106Documents>. This documentation serves as the basis for the "No Historic Properties Affected" finding. The views of the public on this effect finding are being sought. Please reply with any comments to Candace Hudziak of Metric Environmental, LLC (317-443-4123, candaceh@metricenv.com) no later than August 8, 2025.

In accordance with the "Americans with Disabilities Act", if you have a disability for which INDOT needs to provide accessibility to the document(s) such as interpreters or readers, please contact Michael Grylewicz, at 219-325-7539 or mgrylewicz@indot.in.gov.

hspaxlp

APPENDIX E

Red Flag and Hazardous Materials



INDIANA DEPARTMENT OF TRANSPORTATION

100 North Senate Avenue
Room N758-ES
Indianapolis, Indiana 46204

PHONE: (855) 463-6848
(855) INDOT4U

Eric Holcomb, Governor
Michael Smith, Commissioner

Date: March 7, 2024

To: Site Assessment & Management (SAM)
Environmental Policy Office - Environmental Services Division (ESD)
Indiana Department of Transportation (INDOT)
100 N Senate Avenue, Room N758-ES
Indianapolis, IN 46204

From: Colin Keith
Metric Environmental, LLC
6958 Hillsdale Court
Indianapolis, IN 46250
colink@metricenv.com

Re: RED FLAG INVESTIGATION
DES #2200043, State Project
Intersection Improvement Project
State Route (SR) 2 at County Road (CR) 500 West, 3.27 Miles West of SR 39
LaPorte County, Indiana

PROJECT DESCRIPTION

Brief Description of Project: The project is located at the junction of SR 2 and CR 500 West. SR 2 traffic at the intersection is free-flowing with stop signs posted for CR 500 West traffic. The intersection has a high frequency of car crashes, with the majority being right- and left-hand turns. Additionally, southbound CR 500 West experiences a high volume of traffic and long peak-hour delays. The goal of the project is to improve safety and reduce congestion at the intersection. The preferred alternative is to convert the intersection to a single-lane roundabout with a truck apron. The roundabout will be centered at the existing intersection to minimize property impacts, although additional right-of-way will be required. Curb and gutter will be installed within the limits of the roundabout to drain excess runoff into a new detention basin that will be constructed in the southwest quadrant of the junction. Ancillary work will include removing existing trees along the roadway, relocating overhead utilities, and installing lighting.

Bridge Work Included in Project: Yes ☐ No ☒ Structure #(s) _____

Is the bridge Historical? Yes ☐ No ☐; Select ☐ Non-Select ☐

(Note: If the project involves a historical bridge, please include the bridge information in the Recommendations Section of the report).

Culvert Work Included in Project: Yes ☐ No ☒ Structure #(s) _____

Proposed right of way: Temporary ☒ #Acres 0.003, Permanent ☒ #Acres 2.36, Not Applicable (N/A) ☐

Type and proposed depth of excavation: Excavation up to five (5) feet below grade will be required for drainage improvements and creating the detention basin.

Maintenance of traffic (MOT): Construction can be done while maintaining a minimum of one lane in each direction for SR 2 traffic. A short closure of each leg of CR 500 West is anticipated in order to reconstruct each approach, with a detour route to be coordinated with county authorities.

Work in waterway: Yes ☐ No ☒ Below ordinary high water mark: Yes ☐ No ☐

State Project: ☒ LPA: ☐

Any other factors influencing recommendations: N/A

INFRASTRUCTURE TABLE AND SUMMARY

| Infrastructure Indicate the number of items of concern found within the 0.5 mile search radius. If there are no items, please indicate N/A: | | | |
|---|------------|-------------------------|------------|
| Religious Facilities | N/A | Recreational Facilities | N/A |
| Airports ¹ | 1 | Pipelines | 2 |
| Cemeteries | N/A | Railroads | 2 |
| Hospitals | N/A | Trails | 1 |
| Schools | N/A | Managed Lands | N/A |

¹In order to complete the required airport review, a review of public-use airports within 3.8 miles (20,000 feet) is required.

Explanation:

Airports: Although not located within the 0.5 mile search radius, one (1) public-use airport, LaPorte Municipal Airport, is located within 3.8 miles (20,000 feet) of the project area. LaPorte Municipal Airport is approximately 3.05 miles southeast of the project area; therefore, early coordination with INDOT Aviation will occur.

Pipelines: Two (2) pipeline segments are located within the 0.5 mile search radius. The nearest pipeline segment, a natural gas line owned by Northern Indiana Public Service Co., is approximately 0.12 mile north of the project area. No impact is expected.

Railroads: Two (2) railroad segments are located within the 0.5 mile search radius. The nearest railroad segment, an active track owned by Norfolk Southern Railroad, is approximately 0.24 mile north of the project area. No impact is expected.

Trails: One (1) trail segment is located within the 0.5 mile search radius. The trail segment, identified as *Porter County Line to LaPorte*, is a potential extension of the SR 2/Westville Trail Corridor that crosses through the project area. Coordination with the managing entity, the Northwestern Indiana Regional Planning Commission, will occur.

WATER RESOURCES TABLE AND SUMMARY

| Water Resources Indicate the number of items of concern found within the 0.5 mile search radius. If there are no items, please indicate N/A: | | | |
|--|------------|-------------------------|------------|
| NWI – Points | N/A | Canal Routes – Historic | N/A |
| Karst Springs | N/A | NWI – Wetlands | 3 |
| Canal Structures – Historic | N/A | Lakes | 1 |
| NPS NRI Listed | N/A | Floodplain – DFIRM | N/A |
| NWI – Lines | N/A | Cave Entrance Density | N/A |
| IDEM 303d Listed Streams and Lakes (Impaired) | N/A | Sinkhole Areas | N/A |
| Rivers and Streams | N/A | Sinking-Stream Basins | N/A |

If unmapped water features are identified that might impact the project area, direct coordination with INDOT ESD Ecology and Waterway Permitting will occur.

Explanation:

NWI – Wetlands: Three (3) NWI wetland polygons are located within the 0.5 mile search radius. The nearest wetland polygon is approximately 0.07 mile north/northeast of the east end of the project area. No impact is expected.

Lakes: One (1) lake polygon is located within the 0.5 mile search radius. The lake polygon is approximately 0.08 mile north/northeast of the east end of the project area. No impact is expected.

MINING AND MINERAL EXPLORATION TABLE AND SUMMARY

| Mining/Mineral Exploration Indicate the number of items of concern found within the 0.5 mile search radius. If there are no items, please indicate N/A: | | | |
|---|------------|---------------------|------------|
| Petroleum Wells | N/A | Mineral Resources | N/A |
| Mines – Surface | N/A | Mines – Underground | N/A |

Explanation:

No mining or mineral exploration features were identified within the 0.5 mile search radius.

HAZARDOUS MATERIAL CONCERNS TABLE AND SUMMARY

| Hazardous Material Concerns Indicate the number of items of concern found within the 0.5 mile search radius. If there are no items, please indicate N/A: | | | |
|--|------------|-----------------------------------|------------|
| Superfund | N/A | Manufactured Gas Plant Sites | N/A |
| RCRA Generator/ TSD | N/A | Open Dump Waste Sites | N/A |
| RCRA Corrective Action Sites | N/A | Restricted Waste Sites | N/A |
| State Cleanup Sites | N/A | Waste Transfer Stations | N/A |
| Septage Waste Sites | N/A | Tire Waste Sites | N/A |
| Underground Storage Tank (UST) Sites | 1 | Confined Feeding Operations (CFO) | N/A |
| Voluntary Remediation Program | N/A | Brownfields | N/A |
| Construction Demolition Waste | N/A | Institutional Controls | N/A |
| Solid Waste Landfill | N/A | NPDES Facilities | 1 |
| Infectious/Medical Waste Sites | N/A | NPDES Pipe Locations | N/A |
| Leaking Underground Storage (LUST) Sites | N/A | Notice of Contamination Sites | N/A |

Unless otherwise noted, site specific details presented in this section were obtained from documents reviewed on the Indiana Department of Environmental Management (IDEM) Virtual File Cabinet (VFC).

Explanation:

Underground Storage Tank (UST) Sites: One (1) UST site is located within the 0.5 mile search radius. Pinola Porte (aka Family Express #59), 995 CR 500 W, AI ID #35631, is in the northeast quadrant of the intersection. IDEM conducted an Underground Storage Tank Inspection on February 2, 2021, and the facility was found to be in compliance with equipment, operating, and maintenance requirements set forth in Indiana's UST Rule 329 IAC 9. No impact is expected.

NPDES Facilities: One (1) NPDES facility is located within the 0.5 mile search radius. Scipio Township Fire Station, Permit #INRA09546, is mapped adjacent to the north of the project area; however, the fire station is physically located approximately 0.31 mile north of the project area. The permit status is effective, with an expiration date of June 6, 2027. No impact is expected.

ECOLOGICAL INFORMATION SUMMARY

The LaPorte County listing of the Indiana Natural Heritage Data Center information on endangered, threatened, or rare (ETR) species and high-quality natural communities is provided at https://www.in.gov/dnr/nature-preserves/files/np_laporte.pdf. A preliminary review of the Indiana Natural Heritage Database by INDOT ESD did not indicate the presence of ETR species within the 0.5 mile search radius. Due to the nature of project activities, this project will fall under the guidelines set forth under USFWS Interim Policy for the Review of Highway Transportation Projects in Indiana dated May 29, 2013. No further coordination is necessary.

A review of the USFWS database did not indicate the presence of endangered bat species in or within 0.5 mile of the project area. The range-wide programmatic consultation for the Indiana Bat and Northern Long-eared Bat will be completed according to the most recent "Using the USFWS's IPaC System for Listed Bat Consultation for INDOT Projects".

RECOMMENDATIONS SECTION

Include recommendations from each section. If there are no recommendations, please indicate N/A:

INFRASTRUCTURE:

Airports: One (1) public-use airport, LaPorte Municipal Airport, is located approximately 3.05 miles southeast of the project area; therefore, early coordination with INDOT Aviation will occur.

Trails: One (1) trail segment, identified as *Porter County Line to LaPorte* (a potential extension of the SR 2/Westville Trail Corridor), crosses through the project area. Coordination with the Northwestern Indiana Regional Planning Commission will occur.

WATER RESOURCES: N/A

MINING/MINERAL EXPLORATION: N/A

HAZARDOUS MATERIAL CONCERNS: N/A

ECOLOGICAL INFORMATION: The range-wide programmatic consultation for the Indiana Bat and Northern Long-eared Bat will be completed according to the most recent "Using the USFWS's IPaC System for Listed Bat Consultation for INDOT Projects".

Shelby
O'Neal

Digitally signed by Shelby
O'Neal
Date: 2024.03.07 12:16:52
-05'00'

INDOT ESD concurrence: _____ (Signature)

Prepared by:

Colin Keith

Project Scientist

Metric Environmental, LLC

Graphics:

A map for each report section with a 0.5 mile search radius buffer around all project area(s) showing all items identified as possible items of concern is attached. If there is not a section map included, please change the YES to N/A:

SITE LOCATION: YES

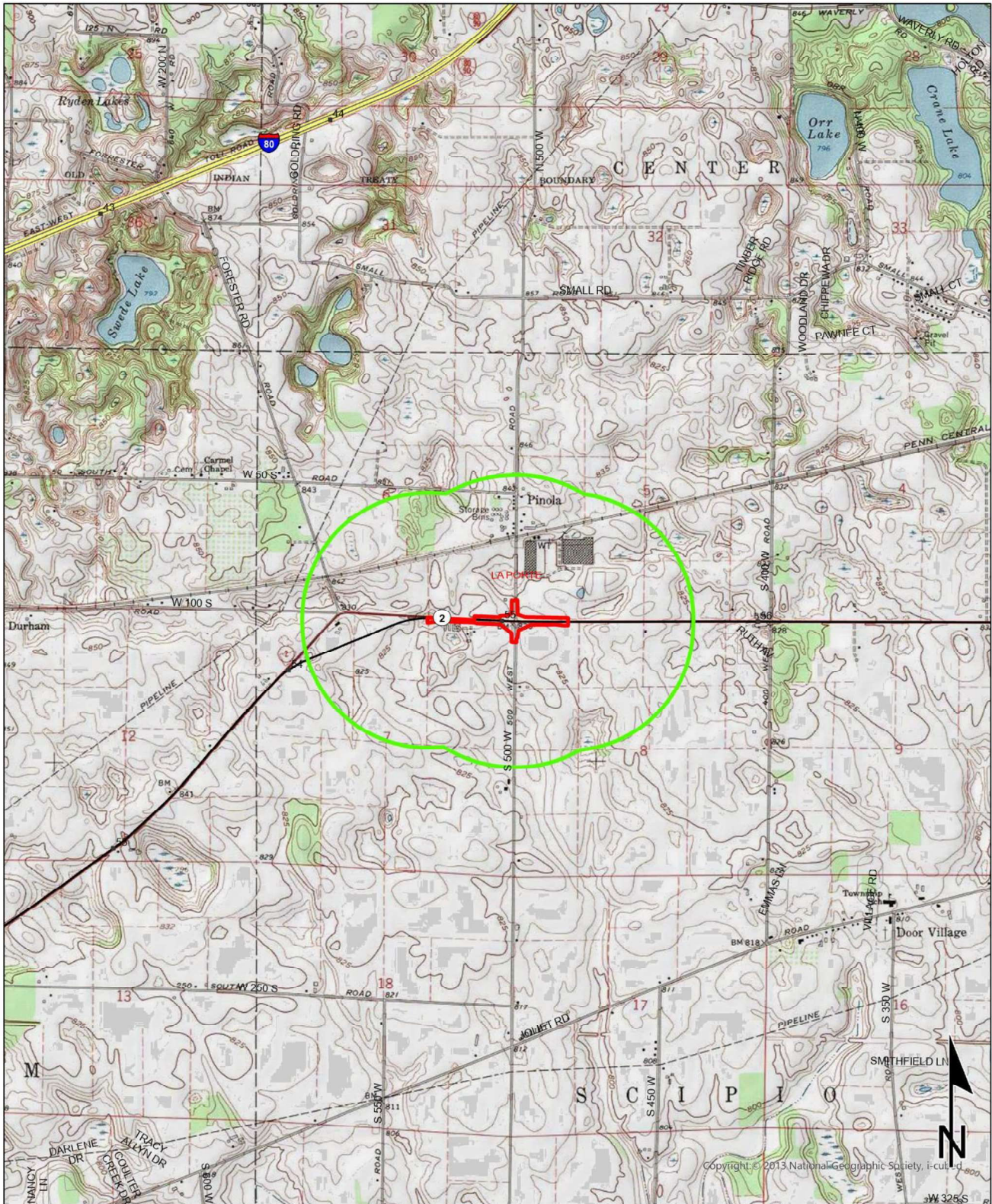
INFRASTRUCTURE: YES

WATER RESOURCES: YES

MINING/MINERAL EXPLORATION: N/A

HAZARDOUS MATERIAL CONCERNS: YES

Red Flag Investigation - Site Location
SR 2 at CR 500 West, 3.27 Miles West of SR 39
Des. No. 2200043, Intersection Improvement Project
LaPorte County, Indiana



Sources: 0.45 0.23 0 0.45 Miles
 Non Orthophotography
 Data - Obtained from the State of Indiana Geographical Information Office Library
 Orthophotography - Obtained from Indiana Map Framework Data (www.indianamap.org)
 Map Projection: UTM Zone 16 N Map Datum: NAD83
 This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.

LAPORTE WEST QUADRANGLE
INDIANA
7.5 MINUTE SERIES
(TOPOGRAPHIC)

Red Flag Investigation - Infrastructure
 SR 2 at CR 500 West, 3.27 Miles West of SR 39
 Des. No. 2200043, Intersection Improvement Project
 LaPorte County, Indiana



Sources:
Non Orthophotography Data - Obtained from the State of Indiana Geographical Information Office Library
Orthophotography - Obtained from Indiana Map Framework Data (www.indianamap.org)
Map Projection: UTM Zone 16 N **Map Datum:** NAD83

This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.

| | | | | | |
|--|--------------------|--|---------------------|--|------------------|
| | Religious Facility | | Recreation Facility | | Project Area |
| | Airport | | Pipeline | | Half Mile Radius |
| | Cemeteries | | Railroad | | Toll |
| | Hospital | | Trails | | Interstate |
| | School | | Managed Lands | | State Route |
| | | | County Boundary | | US Route |
| | | | | | Local Road |

Red Flag Investigation - Water Resources
 SR 2 at CR 500 West, 3.27 Miles West of SR 39
 Des. No. 2200043, Intersection Improvement Project
 LaPorte County, Indiana



Sources:

Non Orthophotography

Data - Obtained from the State of Indiana Geographical Information Office Library

Orthophotography - Obtained from Indiana Map Framework Data

(www.indianamap.org)

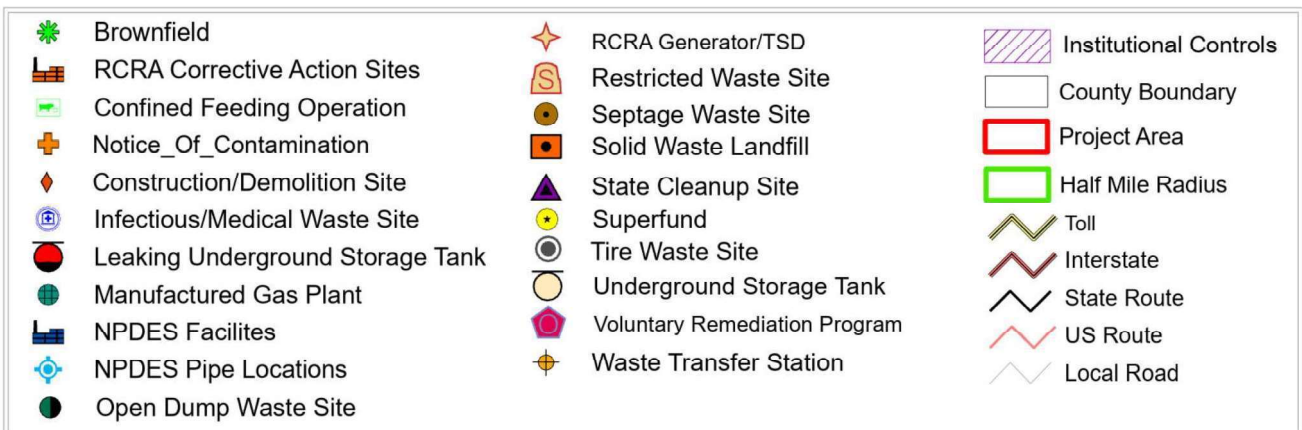
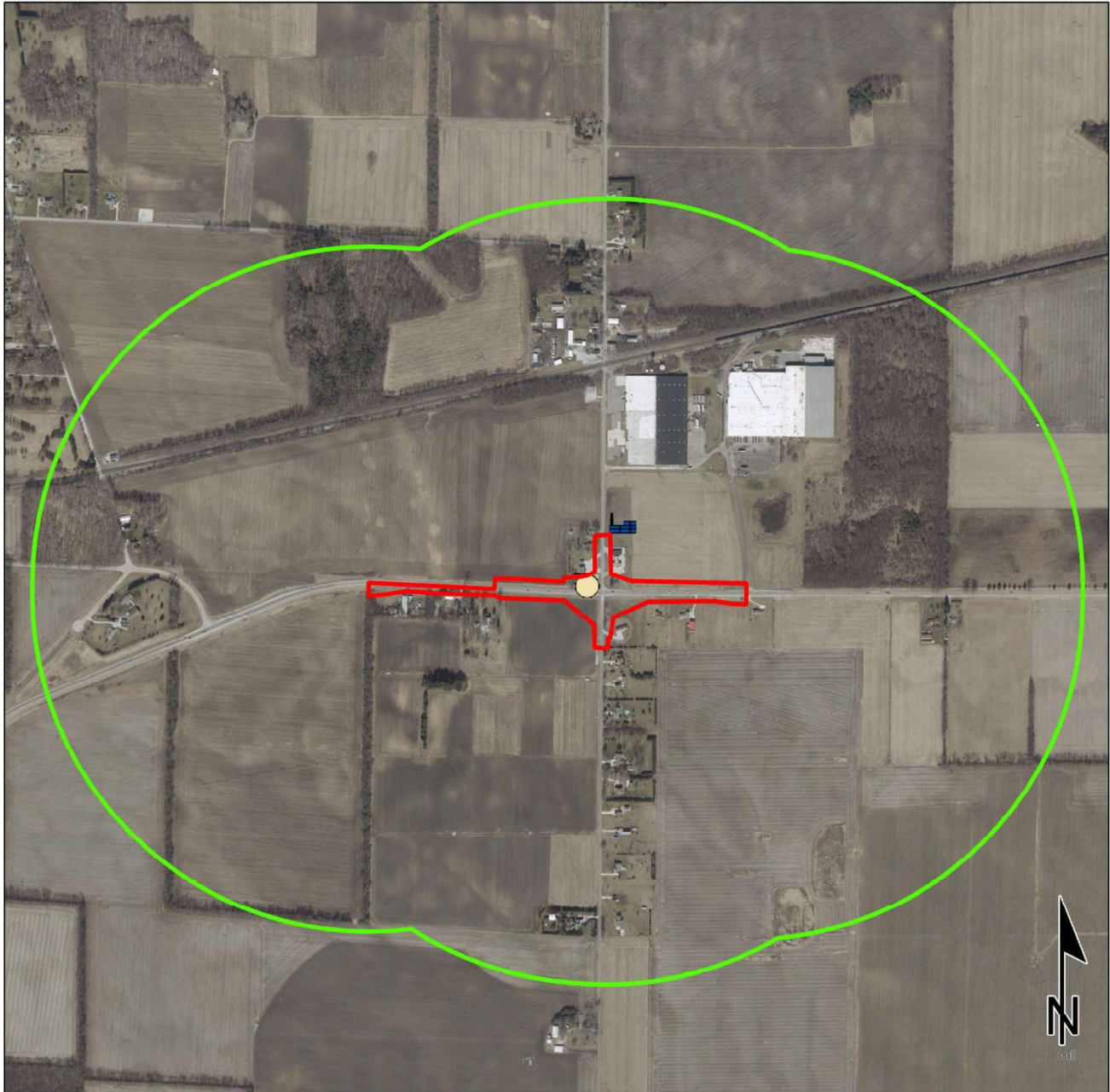
Map Projection: UTM Zone 16 N Map Datum: NAD83

This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.

0.2 0.1 0 0.2 Miles



Red Flag Investigation - Hazardous Material Concerns
 SR 2 at CR 500 West, 3.27 Miles West of SR 39
 Des. No. 2200043, Intersection Improvement Project
 LaPorte County, Indiana



0.2 0.1 0 0.2
 Miles

This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.

Sources:
Non Orthophotography
Data - Obtained from the State of Indiana Geographical Information Office Library
Orthophotography - Obtained from Indiana Map Framework Data (www.indianamap.org)
Map Projection: UTM Zone 16 N **Map Datum:** NAD83

APPENDIX F

Water Resources



Approved on 3.6.2024

Wetland Determination Forms have Been Omitted

WATERS OF THE U.S. DETERMINATION REPORT

State Road 2 and County Road 500 West

SR 2 Intersection Improvement

LaPorte County, Indiana

Des. No. 2200043

Prepared By: Seth Snyder, Metric Environmental, LLC

March 5, 2024

Date of Waters Field Investigation: August 2, 2023

Location:

12-Digit HUC Watershed: 071200010406, (Headwaters Mill Creek) (**Exhibit 1**)

Sections 5, 6, 7, and 8; Township 36 North; Range 3 West

LaPorte West, IN 7.5 minute USGS Topographic Quadrangles (**Exhibit 2**)

Scipio Township, LaPorte County, Indiana

Latitude: 41.591497 Longitude: -86.797925

Project Description:

The proposed project involves the improvements at the intersection of State Road (SR) 2 and County Road (CR) 500 West (W), in Scipio Township, LaPorte County. The preferred alternative is to reconfigure the existing four-leg intersection into a roundabout. The Investigated Area (IA) will extend approximately 200 ft. from the center of the intersection within the southwest quadrant and 150 feet (ft.) from the center of the intersection within all other quadrants. Additionally, the investigated area will extend for approximately 350 ft. north and south along CR 500W, and 700 ft. east and west along SR 2, from the intersection.

National Wetlands Inventory (NWI) Information:

No mapped NWI polygons are located within the IA. The nearest mapped NWI polygon is located approximately 460 feet northeast of the IA. The NWI map is provided as **Exhibit 3**.

Soils: According to the Natural Resources Conservation Service (NRCS) Soil Survey Geographic (SSURGO) Database for LaPorte County, Indiana, the IA contained five mapped soil units, listed in **Table 1** below. The NRCS soil survey map is provided as **Exhibit 3**.

Des. No. 2200043
State Road 2 and County Road 500 West
SR 2 Intersection Improvement
LaPorte County, Indiana
Metric Project No. 22-0216-1



Table 1: NRCS Soil Summary Table

| Soil Unit Symbol | Soil Unit Name | Hydric Soil Category | SSURGO Hydric Rating |
|------------------|--|----------------------|----------------------|
| ChB | Chelsea fine sand, 2 to 6 percent slopes | Non-Hydric | 0% Hydric |
| CoA | Coupee silt loam, 0 to 2 percent slopes | Non-Hydric | 0% Hydric |
| CoB | Coupee silt loam, 2 to 6 percent slopes | Non-Hydric | 0% Hydric |
| EsA | Elston silt loam, 0 to 2 percent slopes | Non-Hydric | 0% Hydric |
| TcB | Tracy sandy loam, 2 to 6 percent slopes | Non-Hydric | 0% Hydric |

Indiana Department of Natural Resources (IDNR) Floodway and FEMA Flood Insurance Rate Map (FIRM):

According to the *IDNR Floodway Information Portal* on August 14, 2023, no mapped IDNR floodway is located within the IA. The nearest mapped IDNR floodway was located approximately 2.2 miles (mi.) southeast of the IA. No mapped floodplains are located within the investigated area. The nearest mapped floodplain was located approximately 2.4 miles (mi.) northeast of the IA and was associated with Stone Lake. The IDNR floodway map and FIRM map for this area is provided as **Exhibit 4**.

USGS National Hydrography Dataset (NHD) Information:

Three mapped NHD flowlines are located within the IA. The NHD map is provided in **Exhibit 4**.

Attached Documents:

Maps of the Investigated Area (**Exhibits 1-6**)
Photograph Location Map (**Exhibit 6**)
Site Photographs
Wetland Determination Data Forms
Preliminary Jurisdictional Determination Form
Wetland Summary Table

Field Reconnaissance:

The wetland determination field visit was conducted on August 2, 2023 by Zachary Root and Seth Snyder of Metric Environmental, LLC. The IA consists of the area that has the potential to be impacted, based on the provided design scenario. This area was evaluated for the presence of wetlands and Waters of the United States. This investigation was conducted in accordance with

Des. No. 2200043
State Road 2 and County Road 500 West
SR 2 Intersection Improvement
LaPorte County, Indiana
Metric Project No. 22-0216-1



the 1987 U.S. Army Corps of Engineers (USACE) Wetland Delineation Manual and the January 2012 Northcentral and Northeast Regional Supplement (Version 2.0) Manual.

A Location Map showing the investigated area location is provided as **Exhibit 1**. The proposed project is located in the western half of LaPorte County, Indiana, on SR 2 approximately 0.8 mi. west of S 400 W. The IA extended along SR 2 for approximately 0.56 mi. and approximately 70 ft. from the SR 2 centerline. The IA extended along S 500 W for approximately 0.17 mi. An aerial map of sampling points and water features is provided as **Exhibit 5**. A photograph location map is provided as **Exhibit 6** and site photographs are attached.

Streams:

No streams were identified within the IA during the field reconnaissance.

Wetlands:

The site was investigated for evidence of hydrophytic vegetation, hydric soil, and wetland hydrology to determine if the project impacts wetlands and other Waters of U.S. The sampling point locations were chosen in possible wetland areas within the IA. The majority of the site consisted of residential lawn and row crop. These areas were investigated and determined to be upland due to upward sloping topography, the presence of dominant upland vegetation, and quality drainage. Dominant upland species observed within these upland areas included red mulberry (*Morus rubra*, FACU) in the tree stratum, and red fescue (*Festuca rubra*, FACU) and common milkweed (*Asclepias syriaca*, FACU) in the herb stratum. Two sampling points were taken and are identified as A1 and A2. One wetland was observed within the IA. The sampling points were recorded on the USACE Wetland Determination Data Forms, described on the **Table 6 Wetland Summary Table**, and shown on **Exhibits 5 and 6**.

Wetland A (0.012 ac.) – PEM1A

Wetland A was classified as a Palustrine, Emergent, Persistent, Temporarily Flooded (PEM1A) wetland. This wetland is located within a concave depression on the western edge of the IA. Approximately 0.012 ac. of Wetland A was contained within the IA and the wetland likely continued west beyond the IA. The boundaries of Wetland A were delineated by lack of wetland vegetation and increased elevation. Due to its location within the depression, Wetland A likely receives flood waters on a consistent basis during rain events. The wetland was not associated with an NWI polygon and was formed within the TcB mapped soil unit, which is listed as 0 percent hydric (nonhydric). The wetland is located adjacent to SR 2 and likely receives run-off from the adjacent paved road. The wetland exhibited poor plant species diversity and contained a dominant invasive species of reed canary grass (*Phalaris arundinacea*, FACW). These factors contribute to the conclusion that Wetland A can support a limited amount of wildlife or aquatic habitat and therefore should be considered to be of poor quality. Based on topography and

Des. No. 2200043
State Road 2 and County Road 500 West
SR 2 Intersection Improvement
LaPorte County, Indiana
Metric Project No. 22-0216-1



overland flow, it can be deduced that Wetland A does not contribute flow to a TNW. Because Wetland A does not contribute flow to a TNW, it should be considered a Waters of the State and will likely be regulated by the state under the Isolated Wetlands Law. However, INDOT requests that the USACE take jurisdiction over it.

Roadside Ditches:

Four roadside ditches (RSD) were identified within the IA as shown in **Table 3**. These features consisted of vegetated drainage swales consisting of red fescue (*Festuca rubra*, FACU). No OHWM and/or bed and bank was observed in these features, so they are likely non-jurisdictional.

Table 3: Roadside Ditch Summary Table

| Name | Photo # | Location | Description | LFT |
|--------------|---------|---------------|-----------------------|-----|
| RSD 1 | 20-22 | West | Corrugated Metal Pipe | 97 |
| RSD 2 | 32-33 | South-central | Corrugated Metal Pipe | 149 |
| RSD 3 | 34-35 | South-central | Corrugated Metal Pipe | 143 |
| RSD 4 | 44-45 | East | Corrugated Metal Pipe | 376 |

Wildlife Observations/Crossings

No tracks were observed within any of the culverts at the time of the reconnaissance.

Culverts and Drains:

Five culverts, CV 1-5, were identified within the IA as shown in **Table 4**. The culverts were made of CMP (corrugated metal pipe). These culverts served to aid in roadside drainage and stormwater conveyance. These culverts did not carry jurisdictional waters due to a lack of an OHWM, bed and bank, and lack of a significant nexus to any jurisdictional Waters of the U.S. Locations of these culverts are shown on **Exhibits 5, Exhibit 6**, and attached photosheet.

Table 4: Culverts and Drains Summary Table

| Culvert and Drain Number | Type | Purpose |
|---------------------------------|-------------|--|
| CV 1, CV 2, CV 3 | CMP | Roadside, stormwater drainage |
| CV 4 (CLV-336460) | CMP | Equalizes roadside drainage under SR 2 |

Conclusion:

One PEM1A wetland, totaling 0.012 ac. was identified within the IA during the field reconnaissance. This waterway should be considered a Waters of the State and will likely be regulated by the state under the Isolated Wetlands Law. However, INDOT requests that the USACE take jurisdiction over it. Every effort should be taken to avoid and minimize impacts to the waterway and wetlands. If impacts are necessary, then mitigation may be required. The INDOT Environmental Services Division should be contacted immediately if impacts will occur. The final determination of jurisdictional waters is ultimately made by the U.S. Army Corps of Engineers. This report is our best judgment based on the guidelines set forth by the Corps.

Acknowledgements:

This waters determination has been prepared based on the best available information, interpreted in light of the investigator's training, experience and professional judgement in conformance with the 1987 *Corps of Engineers Wetlands Delineation Manual*, the appropriate regional supplement, the USACE *Jurisdictional Determination Form Instructional Guidebook*, and other appropriate agency guidelines. See **Table 5** for a list of the associated Metric investigators.

Table 5: Acknowledgement Summary Table

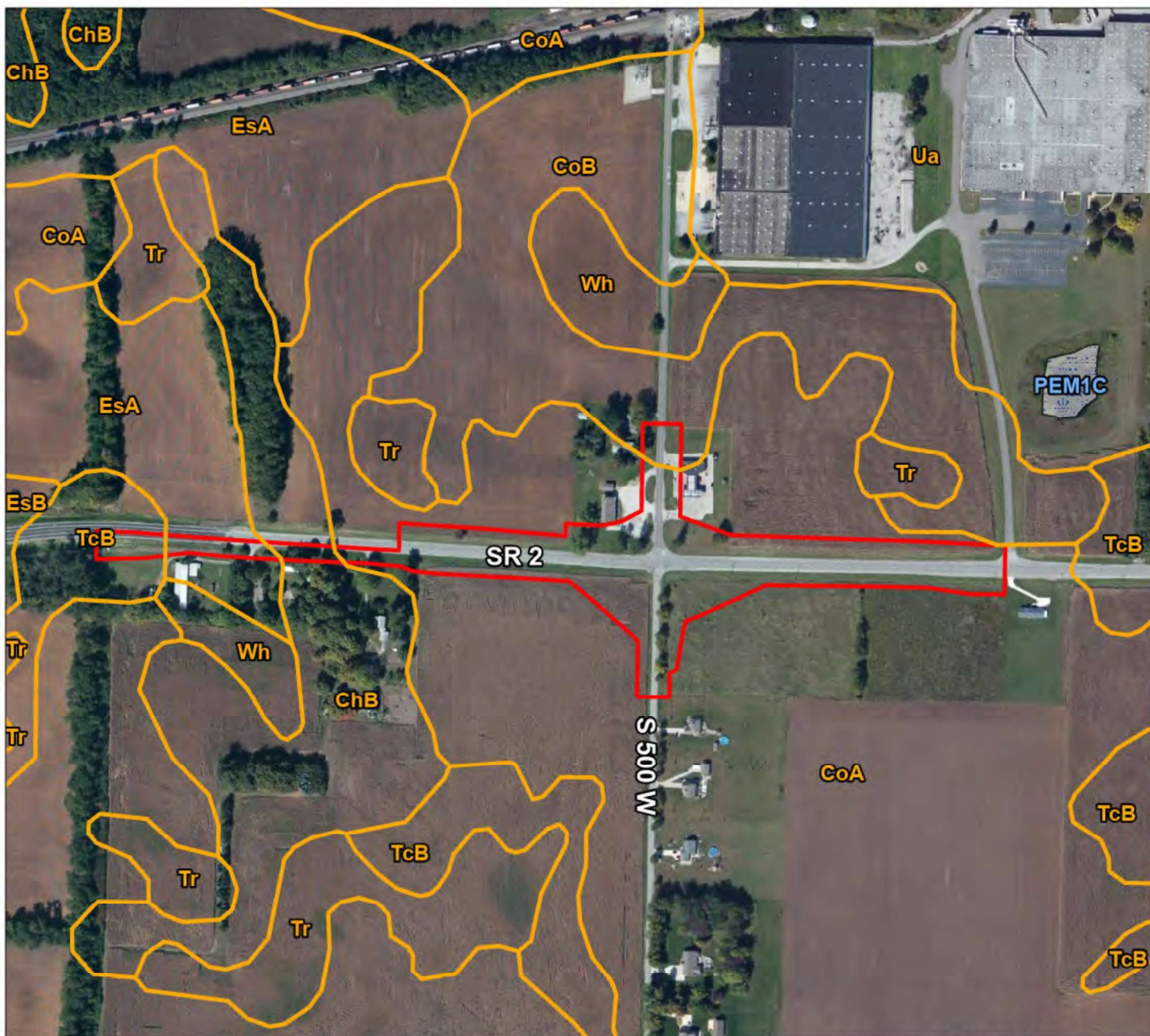
| Metric Environmental Staff | Position | Contributing Effort | Signature/Date |
|----------------------------|--------------------------|---|----------------------------------|
| Alex Gray | Senior Project Manager 1 | QAQC | <i>Alex M. Gray</i> 3/5/24 |
| Juliana Clayton | Project Scientist 1 | QAQC | <i>Juliana Clayton</i> 3/5/24 |
| Zachary Root | Project Scientist 1 | Field Data Collection | <i>Zachary Root</i> 3/5/24 |
| Seth Snyder | Natural Resources Intern | Field Data Collection, Report Preparation | <i>Seth Snyder</i> 3/5/24 |

Duplicate exhibits have been intentionally removed from this document. Please refer to Exhibit B in the CE document.

Des. No. 2200043
State Road 2 and County Road 500 West
SR 2 Intersection Improvement
LaPorte County, Indiana
Metric Project No. 22-0216-1



| Wetland ID | Type | Acreage | Quality | Photo IDs | Associated Structure ID | Likely WOTUS? | Data Point ID | Lat/Long | Dominant Vegetation | Hydric Soil Indicator(s) | Hydrology Indicator(s) | Within Wetland? | Notes |
|------------|-------|---------|---------|-----------|-------------------------|---------------|---------------|-----------------------|--|--------------------------|------------------------|-----------------|--|
| Wetland A | PEM1A | 0.012 | Poor | 11-13 | None | Yes | A1 | 41.591584, -86.804598 | <i>Phalaris arundinacea</i> , FACW <i>Cirsium arvense</i> , FACU <i>Persicaria lapathifolia</i> , FACW | F6 | D2, D5 | Yes | Wetland A extends west beyond the west terminus of the IA. |
| n/a | n/a | n/a | n/a | 14-16 | None | n/a | A2 | 41.591623, -86.804430 | <i>Ambrosia trifida</i> , FAC <i>Festuca rubra</i> , FACU | F7 | None | No | n/a |



| Symbol | Map Unit Name | Hydric Rating |
|--------|--|-----------------|
| ChB | Chelsea fine sand, 2 to 6 percent slopes | Non-Hydric (0%) |
| CoA | Coupee silt loam, 0 to 2 percent slopes | Non-Hydric (0%) |
| CoB | Coupee silt loam, 2 to 6 percent slopes | Non-Hydric (0%) |
| EsA | Elston silt loam, 0 to 2 percent slopes | Non-Hydric (0%) |
| TcB | Tracy sandy loam, 2 to 6 percent slopes | Non-Hydric (0%) |

□ Investigated Area (IA)
 □ NRCS
 NWI Wetlands

Exhibit 3 - NWI, and NRCS Soil Survey Map
 SR 2 - Intersection Improvement
 Roundabout Installation
 LaPorte County, Indiana
 Des. No. 2200043
 Metric Project No. 22-0216-1
 Map Date: 7/31/3\2023
 Map Author: Seth Snyder

All locations approximate
 Source: ESRI World Imagery Clarity (2022)



0 250 500 1,000
 Feet



Exh. 3

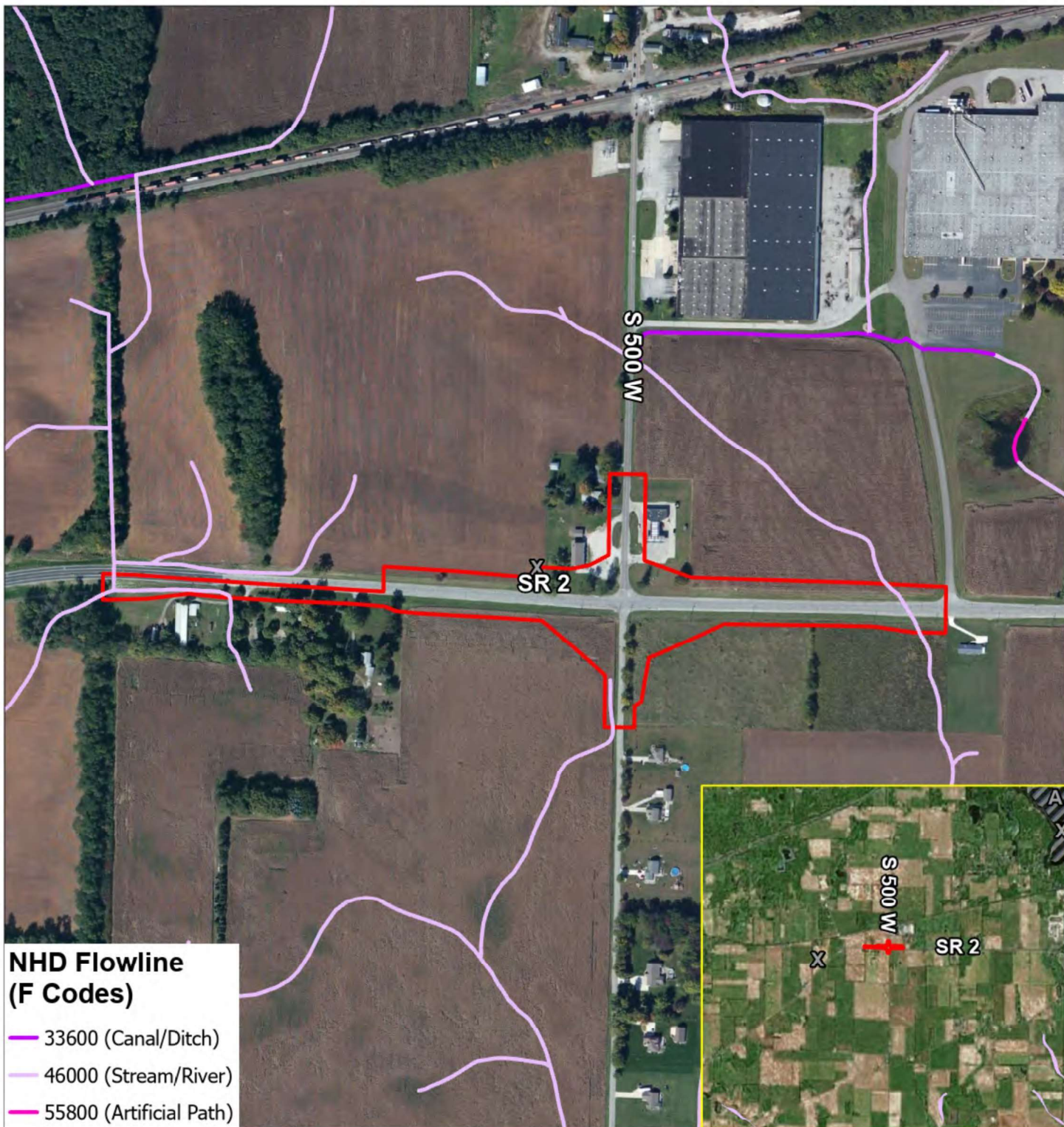


Exhibit 4A - FIRM, IDNR Floodway, NHD Flowline Map
 SR 2 - Intersection Improvement
 Roundabout Installation
 LaPorte County, Indiana
 Des. No. 2200043
 Metric Project No. 22-0216-1
 Map Date: 7/31/2023
 Map Author: Seth Snyder

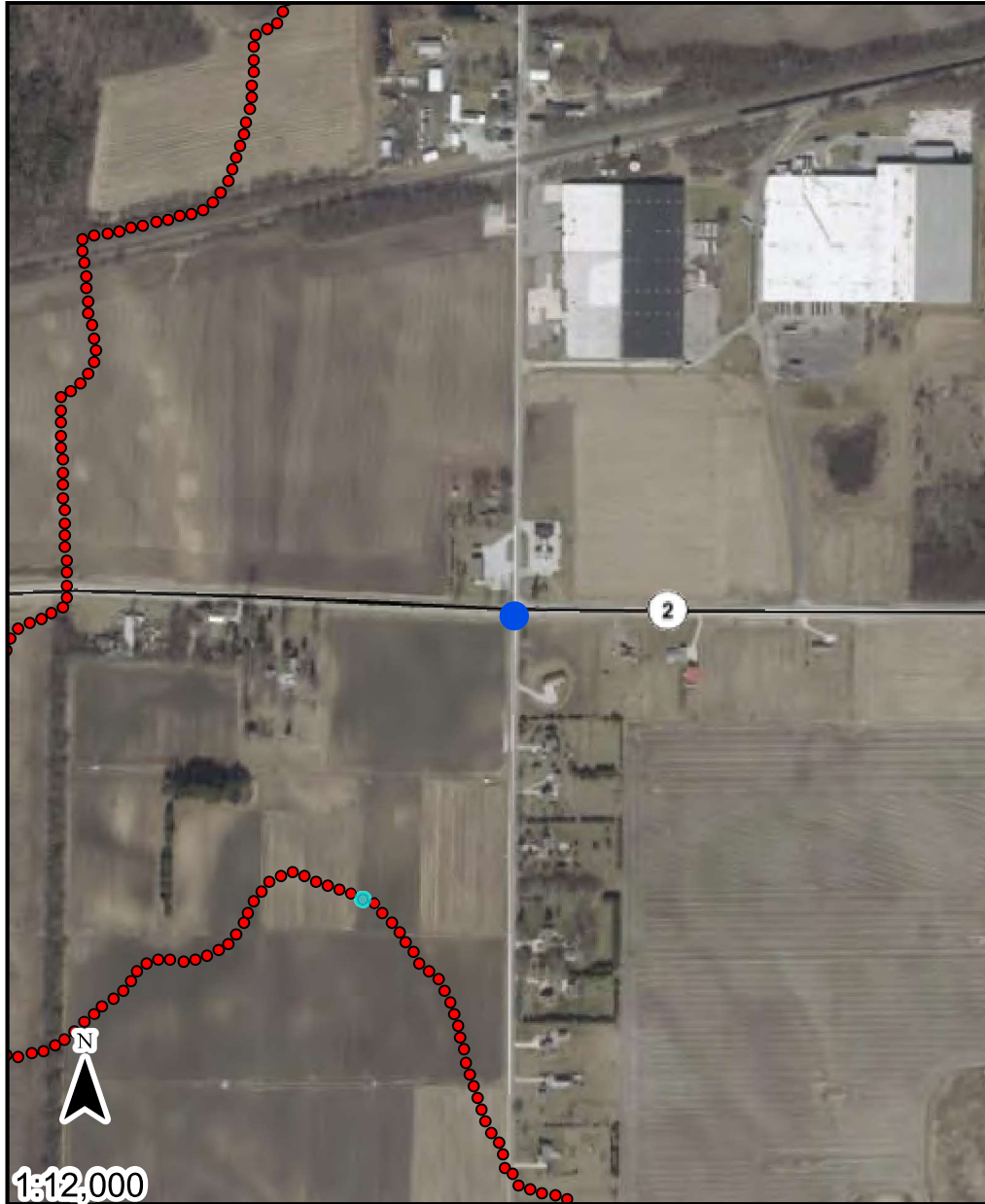
All locations approximate
 Source: ESRI World Imagery Clarity (2022)



0 250 500 1,000
 Feet



Exh. 4A



● Point of Interest

● Base Flood Elevation Point

VERSION

● 1.5

FLD_ZONE, SOURCE_DNR,
ZONE_SUBTY

Not Mapped

Long: -86.79812438859864

Lat: 41.591573369837306

The information provided below is based on the point of interest shown in the map above.

County: **Laporte**

Stream Name:

Unnamed Tributary

Approximate Ground Elevation: **831.1 feet (NAVD88)**

Base Flood Elevation: **Not Available**

Drainage Area: **Not Available**

Best Available Flood Hazard Zone: **Not Mapped**

National Flood Hazard Zone: **Not Mapped**

Is a Flood Control Act permit from the DNR needed for this location? **See following pages**

Is a local floodplain permit needed for this location? **Contact your local Floodplain Administrator-**

Floodplain Administrator: **Michael Polan, Building Commissioner**

Community Jurisdiction: **Laporte County, County proper**

Phone: **(219) 326-6808**

Email: **mpolan@laporteco.in.gov**

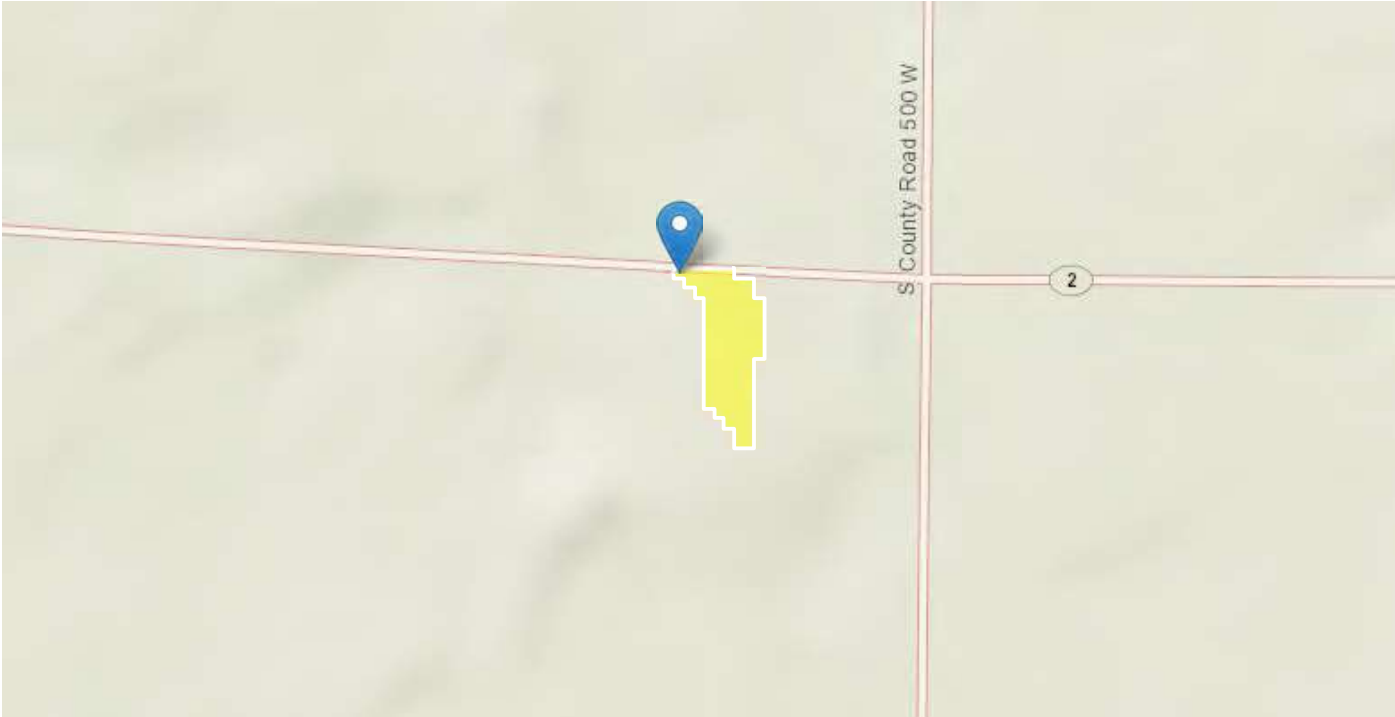
US Army Corps of Engineers District: **Detroit**

Exhibit 4B: FARA Report
Des. No. 2200043

Date Generated: 12/8/2023

StreamStats Report

Region ID: IN
Workspace ID: IN20231208151433060000
Clicked Point (Latitude, Longitude): 41.59150, -86.80088
Time: 2023-12-08 10:15:21 -0500



+ Collapse All

Basin Characteristics

| Parameter Code | Parameter Description | Value | Unit |
|----------------|---|-------|--------------|
| CSL10_85 | Change in elevation divided by length between points 10 and 85 percent of distance along main channel to basin divide - main channel method not known | 45 | feet per mi |
| DRNAREA | Area that drains to a point on a stream | 0.004 | square miles |

General Disclaimers

Exhibit 4C: StreamStats Report
Des. No. 2200043

The delineation point is in an exclusion area. SITE LOCATED IN NON-CONTRIBUTING AREA: DELINEATION MAY NOT BE VALID.

Appendix 2 - PRELIMINARY JURISDICTIONAL DETERMINATION (PJD) FORM

BACKGROUND INFORMATION

A. REPORT COMPLETION DATE FOR PJD: March 5, 2024

B. NAME AND ADDRESS OF PERSON REQUESTING PJD:

Seth Snyder
Metric Environmental, LLC
6971 Hillsdale Court
Indianapolis, IN 46250
317-793-6217
seths@metricenv.com

C. DISTRICT OFFICE, FILE NAME, AND NUMBER:

D. PROJECT LOCATION(S) AND BACKGROUND INFORMATION:

The proposed project (Des. 2200043) involves the improvements at the intersection of State Road (SR) 2 and County Road (CR) 500 West (W), in Scipio Township, LaPorte County. The preferred alternative is to reconfigure the existing four-leg intersection into a roundabout. The Investigated Area (IA) will extend approximately 200 ft. from the center of the intersection within the southwest quadrant and 150 feet (ft.) from the center of the intersection within all other quadrants. Additionally, the investigated area will extend for approximately 350 ft. north and south along CR 500W, and 700 ft. east and west along SR 2, from the intersection.

(USE THE TABLE BELOW TO DOCUMENT MULTIPLE AQUATIC RESOURCES AND/OR AQUATIC RESOURCES AT DIFFERENT SITES)

State: IN County/parish/borough: LaPorte County City: Pinola

Center coordinates of site (lat/long in degree decimal format):

Lat.: 41.591504°

Long.: -86.798184°

Universal Transverse Mercator: 16 T 516820.39 E 4604442.67 N

Name of nearest waterbody: Mill Creek

E. REVIEW PERFORMED FOR SITE EVALUATION (CHECK ALL THAT APPLY):

☐ Office (Desk) Determination. Date:

☐ Field Determination. Date(s):

TABLE OF AQUATIC RESOURCES IN REVIEW AREA WHICH "MAY BE" SUBJECT TO REGULATORY JURISDICTION.

| Site number | Latitude (decimal degrees) | Longitude (decimal degrees) | Estimated amount of aquatic resource in review area (acreage and linear feet, if applicable) | Type of aquatic resource (i.e., wetland vs. non-wetland waters) | Geographic authority to which the aquatic resource "may be" subject (i.e., Section 404 or Section 10/404) |
|--------------------|-----------------------------------|------------------------------------|---|--|--|
| Wetland A | 41.591603 | -86.804578 | 0.012 acres | Wetland | Section 404 |

- 1) The Corps of Engineers believes that there may be jurisdictional aquatic resources in the review area, and the requestor of this PJD is hereby advised of his or her option to request and obtain an approved JD (AJD) for that review area based on an informed decision after having discussed the various types of JDs and their characteristics and circumstances when they may be appropriate.
- 2) In any circumstance where a permit applicant obtains an individual permit, or a Nationwide General Permit (NWP) or other general permit verification requiring "pre-construction notification" (PCN), or requests verification for a non-reporting NWP or other general permit, and the permit applicant has not requested an AJD for the activity, the permit applicant is hereby made aware that: (1) the permit applicant has elected to seek a permit authorization based on a PJD, which does not make an official determination of jurisdictional aquatic resources; (2) the applicant has the option to request an AJD before accepting the terms and conditions of the permit authorization, and that basing a permit authorization on an AJD could possibly result in less compensatory mitigation being required or different special conditions; (3) the applicant has the right to request an individual permit rather than accepting the terms and conditions of the NWP or other general permit authorization; (4) the applicant can accept a permit authorization and thereby agree to comply with all the terms and conditions of that permit, including whatever mitigation requirements the Corps has determined to be necessary; (5) undertaking any activity in reliance upon the subject permit authorization without requesting an AJD constitutes the applicant's acceptance of the use of the PJD; (6) accepting a permit authorization (e.g., signing a proffered individual permit) or undertaking any activity in reliance on any form of Corps permit authorization based on a PJD constitutes agreement that all aquatic resources in the review area affected in any way by that activity will be treated as jurisdictional, and waives any challenge to such jurisdiction in any administrative or judicial compliance or enforcement action, or in any administrative appeal or in any Federal court; and (7) whether the applicant elects to use either an AJD or a PJD, the JD will be processed as soon as practicable. Further, an AJD, a proffered individual permit (and all terms and conditions contained therein), or individual permit denial can be administratively appealed pursuant to 33 C.F.R. Part 331. If, during an administrative appeal, it becomes appropriate to make an official determination whether geographic jurisdiction exists over aquatic resources in the review area, or to provide an official delineation of jurisdictional aquatic resources in the review area, the Corps will provide an AJD to accomplish that result, as soon as is practicable. This PJD finds that there "*may be*" waters of the U.S. and/or that there "*may be*" navigable waters of the U.S. on the subject review area, and identifies all aquatic features in the review area that could be affected by the proposed activity, based on the following information:

SUPPORTING DATA. Data reviewed for PJD (check all that apply)

Checked items should be included in subject file. Appropriately reference sources below where indicated for all checked items:

☒ Maps, plans, plots or plat submitted by or on behalf of the PJD requestor:

☒ Map: _____ Dated 7/31/2023 & 8/17/2023

_____ Data sheets prepared/submitted by or on behalf of the PJD requestor.

☐ Office concurs with data sheets/delineation report.

☐ Office does not concur with data sheets/delineation report. Rationale: _____

☐ Data sheets prepared by the Corps: _____

☐ Corps navigable waters' study: _____

☐ U.S. Geological Survey Hydrologic Atlas: _____

☐ USGS NHD data.

☒ USGS 8 and 12 digit HUC maps.

☒ U.S. Geological Survey map(s). Cite scale & quad name: LaPorte, IN 7.5 min, 1966

☒ Natural Resources Conservation Service Soil Survey. Citation: SSURGO LaPorte County

☒ National wetlands inventory map(s). Cite name: http://www.fws.gov/wetlands/

☐ State/local wetland inventory map(s): _____

☒ FEMA/FIRM maps: ; Effective _____

☐ 100-year Floodplain Elevation is: _____ (National Geodetic Vertical Datum of 1929)

☒ Photographs: ☒ Aerial (Name & Date): Indiana Aerial Photograph, 2013

or ☒ Other (Name & Date): Site Photographs, 8/02/2023

☐ Previous determination(s). File no. and date of response letter: _____

☐ Other information (please specify): _____

IMPORTANT NOTE: The information recorded on this form has not necessarily been verified by the Corps and should not be relied upon for later jurisdictional determinations.

Signature and date of
Regulatory staff member
completing PJD

Seth Snyder 3/5/2024

Signature and date of
person requesting PJD
(REQUIRED, unless obtaining
the signature is impracticable)¹

¹ Districts may establish timeframes for requestor to return signed PJD forms. If the requestor does not respond within the established time frame, the district may presume concurrence and no additional follow up is necessary prior to finalizing an action.

APPENDIX G

Public Involvement

202318.10

Sample Notice of Entry Letter

July 27, 2023



RE: Intersection Improvement at State Road 2
and County Road 500 West
Des. No. 2200043
La Porte County, Indiana
Notice of Survey
County Parcel ID: 461008100053000060

CHRISTOPHER J. JETER, PE
DAN G. DELGADO, PE
JARED M. HUSS, PE
AARON W. BLANK, PS, PE
KEVIN J. SIEDLECKI, PE
CHRISTOPHER M. VANHULLE, PE
MICHELLE M.G. SLACK, PS
DMITRI G. ADAMS, PE
AMANDA R. BUDREAU, PE
JOSEPH D. DUNBAR, PE
PAULIN HAKIZIMANA, PE, PTOE
MARK H. FOSTER, PE
EASA KHAN, PhD, PE, PMP, PTOE
BLAKE R. WARNER, PE
REBECCA L. DUNBAR, PS, EI
DAVID J. TEGGELAAR, PE
TREVOR M. CREAGER, PE
JOHN J. LABOUNTY, PE

Dear Property Owner:

Lawson-Fisher Associates P.C. has been retained by the LaPorte District, Indiana Department of Transportation, to perform a survey for an intersection improvement project in LaPorte County, Indiana.

Our information indicates you own or occupy property near the subject project. Our employees have completed a survey of the project area. It may have been necessary for them to come onto your property to complete this work. This is allowed by law by Indiana Code IC 8-23-7-26. Ideally, this notice would have preceded our field survey. That was an oversight on our part, and we do apologize for any inconvenience or misunderstanding that may have resulted. If you have sold this property or it is occupied by someone else, please let us know the name and address of the new owner or current occupant so we can contact them about the survey.

At this stage, we generally do not know what effect, if any, the project can eventually have on your property. If we determine your property is involved, we will contact you with additional information.

The survey work included mapping the location of features such as trees, fences, drainage features, pavement, drive entrances, utilities, and obtaining ground elevations. We also located evidence of property and right-of-way limits, which may have involved digging for property irons along the roadway frontage. The survey is needed for the proper planning and design of this intersection improvement project. Please be assured of our sincere desire to have caused you as little inconvenience as possible during this survey. If you have any concerns, please contact me at the phone number or address shown below.

Very truly yours,
LAWSON-FISHER ASSOCIATES P.C.

Aaron W. Blank, PS, PE
Vice President, Survey Director

AWB/maf
c: Dan G. Delgado, PE

APPENDIX H

Air Quality

**Federal Transit
Administration**
Region V
200 West Adams St., Suite 320
Chicago, IL 60606-5253



**U.S. Department
of Transportation**

Federal Highway Administration
Indiana Division
575 N. Pennsylvania St., Rm 254
Indianapolis, IN 46204-1576

August 28, 2025

Ms. Lyndsay Quist
Commissioner
Indiana Department of Transportation
100 N Senate Ave. N955
Indianapolis, IN 46204

SUBJECT: Indiana FY2026-2030 STIP Approval and Associated Federal Planning Finding

Dear Ms. Quist:

The Federal Highway Administration (FHWA) and the Federal Transit Administration (FTA) have completed our review of the FY2026-2030 Indiana Statewide Transportation Improvement Program (INSTIP), which was submitted by the Indiana Department of Transportation (INDOT) request letter dated June 6, 2025.

Based on our review of the information provided, certifications of the Statewide and Metropolitan transportation planning processes for and within the state of Indiana, and our participation in those transportation planning processes (including planning certification reviews conducted in Transportation Management Areas), FHWA and FTA are jointly approving the FY2026-2030 STIP, including the Metropolitan Planning Organization (MPO) Transportation Improvement Programs (TIPs) incorporated into the STIP by reference, subject to the corrective action identified in the attached Federal Planning Finding (FPF) report. FHWA and FTA consider the projects in the 5th year for informational purposes only, and our approval does not exceed four years per 23 CFR 450.220(c).

FHWA and FTA are required under 23 CFR 450.220(b) to document and issue an FPF in conjunction with the approval of the FY2026-2030 STIP. At a minimum, the FPF verifies that the development of the STIP is consistent with the provisions of both the Statewide and Metropolitan transportation planning requirements. FHWA and FTA find that the Indiana FY2026-2030 STIP substantially meets the transportation planning requirements and are approving the STIP subject to the corrective action outlined in the FPF. This approval is effective August 22, 2025, and is given with the understanding that an eligibility determination of individual projects for funding must be met, and INDOT must ensure the satisfaction of all administrative and statutory requirements, as well as address the corrective actions outlined in the attached report.

If you have questions or need additional information concerning our approval and the FPF, please contact Ms. Erica Tait of the FHWA Indiana Division at (317) 226-7481, or by email at erica.tait@dot.gov, or Mr. Anthony Greep of the FTA Region 5 Office at (312) 353-2866, or by email at anthony.greep@dot.gov.

Sincerely,

**KELLEY
BROOKINS**

Kelley Brookins
Regional Administrator
FTA Region V

Digitally signed by
KELLEY BROOKINS
Date: 2025.08.27
08:09:52 -05'00'

Sincerely,



Christopher J. Hall
Interim Division Administrator
FHWA Indiana Division

Digitally signed by
CHRISTOPHER J HALL
Date: 2025.08.28
10:04:48 -05'00'



INDIANA DEPARTMENT OF TRANSPORTATION

100 North Senate Avenue
Room N758-Executive Office
Indianapolis, Indiana 46204

PHONE: (855) 463-6848

Mike Braun, Governor
Lyndsay Quist, Commissioner

June 6, 2025

Mr. Christopher J Hall, Interim Division Administrator
FHWA Indiana Division
575 North Pennsylvania St., Room 254
Indianapolis, IN 46204

Ms. Kelley Brookins, Regional Administrator
FTA Region 5
200 West Adams St.
Suite 320
Chicago, IL 60606-5253

Dear Mr. Hall /Ms. Brookins:

The Indiana Department of Transportation is pleased to submit its Draft FY 2026-2029 Statewide Transportation Improvement Program (STIP) for review and comment by your offices.

Included in the final submitted document is a listing of the state's expansion/preservation and local small urban and rural and rural transit projects. The following Metropolitan Planning Organization TIP's will be included in the FY 2024-2028 STIP by reference.

| | |
|--|--------------|
| Area Plan Commission of Tippecanoe County (APCTC) | FY 2026-2030 |
| • APCTC TIP FY2026-2030 | |
| Bloomington-Monroe County Metropolitan Planning Organization (BMCMPPO) | FY 2026-2030 |
| • BMCMPPO TIP FY2026-2030 | |
| Columbus Area Metropolitan Planning Organization (CAMPO) | FY 2026-2030 |
| • CAMPO TIP FY2026-2030 | |
| Delaware-Muncie Metropolitan Plan Commission (DMMPC) | FY 2026-2030 |
| • DMMPC TIP FY2026-2030 | |
| Evansville Metropolitan Planning Organization (EMPO) | FY 2026-2030 |
| • EMPO TIP FY2026-2030 | |
| Indianapolis Metropolitan Planning Organization (IMPO) | FY 2026-2029 |
| • IMPO TIP FY2026-2029 | |
| • IMPO Project Listing FY2026-2029 | |

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| | |
|--|--------------|
| Kokomo-Howard County Governmental Coordinating Council (KHCGCC) | FY 2026-2030 |
| • KHCGCC TIP FY2026-2030 | |
| Kentuckiana Regional Planning and Development Agency (KIPDA) | FY 2025-2028 |
| • KIPDA TIP FY2025-2028 | |
| Michiana Area Council of Governments (MACOG) | FY 2026-2030 |
| • MACOG TIP FY2026-2030 | |
| Madison County Council of Governments (MCCOG) | FY 2026-2030 |
| • MCCOG TIP FY2026-2030 | |
| Northeastern Indiana Regional Coordinating Council (NIRCC) | FY 2026-2030 |
| • NIRCC TIP FY2026-2030 | |
| Northwestern Indiana Regional Planning Commission (NIRPC) | FY 2026-2030 |
| • NIRPC TIP FY2026-2030 | |
| Ohio-Kentucky-Indiana Regional Council of Governments (OKI) | FY 2026-2029 |
| • OKI TIP FY2026-2029 | |
| Terre Haute Area Metropolitan Planning Organization (THAMPO) TIP | FY 2026-2030 |
| • THAMPO TIP FY2026-2029 | |

We greatly appreciate FHWA/FTA support in the development of the STIP 2026-2029 and look forward to working together to achieve our mutual goals. Should you have any questions pertaining to this amendment, please contact April Leckie, STIP Administration at 317-232-5466 or at aleckie@indot.in.gov.

Sincerely,



Lyndsay Quist, Commissioner
Indiana Department of Transportation

cc: (w/enclosure): Angelica Salgado, FTA
Cecilia Crenshaw, FTA
Anthony Greep, FTA
Bill Wheeler, FTA
Kelley Brookins, FTA
Matt Kane, FTA
Susan Weber, FTA
Erica Tait, FHWA
Paige Story, FHWA
Lyndsay Quist, INDOT
Blake Martain, INDOT

Kathy Eaton-McKalip, INDOT
Louis Feagans, INDOT
April Leckie, INDOT
Roy Nunnally, INDOT
Larry Buckel, INDOT
Jay Mitchell, INDOT
Jason Casteel, INDOT
Ryan Pennington, INDOT
Michael McNeil, INDOT

Northwestern MPO

Transportation Improvement Program Fiscal Year Adoption 2024-2028

415 Projects Listed

| 2301099 | | (Ver 4) 26-00,26-00 | | | FEDERAL | |
|--------------|---|---------------------|-------------|-----------------|---------|--------------------------------|
| Title: | Intersection Improvement Project | | | County: Porter | | District: LaPorte |
| Work Type: | Intersect. Improv. W/ Added Turn Lanes | | | | | |
| Limits: | | | | | | Total 2026 - 2029: \$1,341,717 |
| Description: | Intersect. Improv. W/ Added NB Left Turn Lane (700 ft in length), SR149 at CR600N, 2.0 miles N of SR130 | | | | | |
| Phase | Fund Source | FY 2026 | FY 2027 | FY 2028 | FY 2029 | FY 2030 |
| CN | NHPP Non Interstate | \$0 | \$0 | \$1,341,717 | \$0 | \$0 |
| 2003099 | | (Ver 5) 26-00,26-00 | | | FEDERAL | |
| Title: | District Intersection Improvement Project | | | County: Porter | | District: LaPorte |
| Work Type: | Intersect. Improv. W/ New Signals | | | | | |
| Limits: | | | | | | Total 2026 - 2029: \$1,056,995 |
| Description: | Intersect. Improv. W/ New Signals on US 20, 1.5 mi east of SR51 (Dombey Rd) | | | | | |
| Phase | Fund Source | FY 2026 | FY 2027 | FY 2028 | FY 2029 | FY 2030 |
| CN | NHPP Non Interstate | \$1,056,995 | \$0 | \$0 | \$0 | \$0 |
| 2000052 | | (Ver 5) 26-00,26-00 | | | FEDERAL | |
| Title: | Intersection Improvement Project in Lake County | | | County: Lake | | District: LaPorte |
| Work Type: | Intersection Improvement | | | | | |
| Limits: | | | | | | Total 2026 - 2029: \$1,848,000 |
| Description: | Intersection Improvement, I-65 at 61st Avenue (SB Ramp Terminal) | | | | | |
| Phase | Fund Source | FY 2026 | FY 2027 | FY 2028 | FY 2029 | FY 2030 |
| CN | NHPP Interstate | \$0 | \$1,680,000 | \$0 | \$0 | \$0 |
| CN | State Match | \$0 | \$168,000 | \$0 | \$0 | \$0 |
| 2200043 | | (Ver 4) 26-00,26-00 | | | FEDERAL | |
| Title: | Intersection Improvement, Roundabout | | | County: Laporte | | District: LaPorte |
| Work Type: | Intersection Improvement | | | | | |
| Limits: | | | | | | Total 2026 - 2029: \$2,150,000 |
| Description: | Intersection Improvement, Roundabout at SR 2 & CR 500W, 3.27mi West of SR 39. | | | | | |
| Phase | Fund Source | FY 2026 | FY 2027 | FY 2028 | FY 2029 | FY 2030 |
| CE | NHPP Non Interstate | \$0 | \$150,000 | \$0 | \$0 | \$0 |
| CN | NHPP Non Interstate | \$0 | \$2,000,000 | \$0 | \$0 | \$0 |

APPENDIX I

Additional Studies

ENGINEER'S REPORT
Intersection Improvement
SR 2 at CR 500 W
LaPorte County

Excerpt

Des. No. 2200043

November 05, 2021



Prepared By:



Prepared For:

Indiana Department of Transportation
LaPorte District

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| TRAFFIC DATA: | 2 |
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| ALTERNATIVES AND RECOMMENDATION: | 4 |
| MAINTENANCE OF TRAFFIC DURING CONSTRUCTION: | 8 |
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APPENDIX

Section A Graphics

| | |
|--|-----|
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Section B Data and Analysis

| | |
|-------------------------------|------------|
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Section C Cost Estimates

| | |
|----------------------|------------|
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Section D Utility Information

| | |
|---------------------------|------------|
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|---------------------------|------------|

ENGINEER'S REPORT

Intersection Improvement
SR 2 at CR 500 W
3.27 miles West of SR 39
LaPorte County

Des. No. 2200043

By Greg R. Wendling, P.E.
USI Consultants, Inc.
November 05, 2021

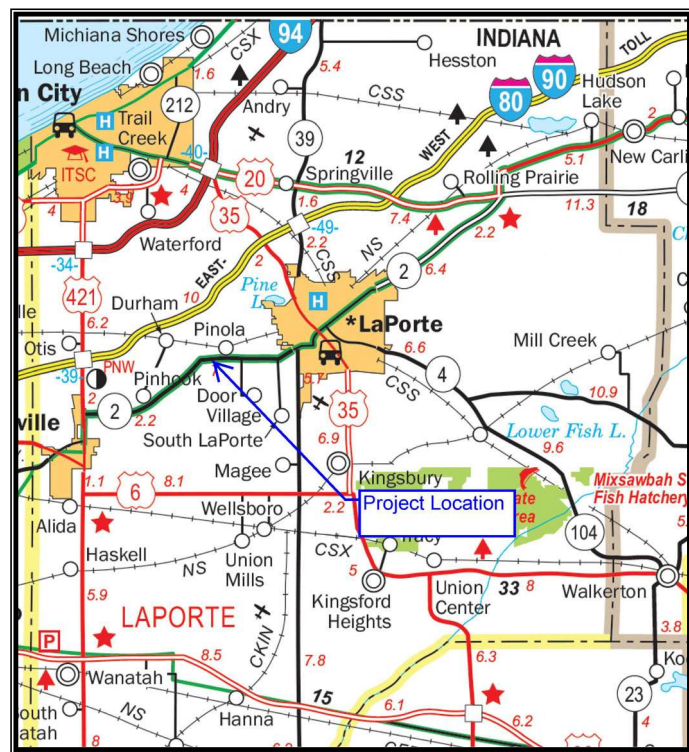
1. PURPOSE OF REPORT:

The purpose of this report is to document the engineering assessment phase of project development, including all coordination that has been completed in preparation for this intersection improvement project. This document outlines the proposal and is intended to serve as a guide for subsequent survey, design, environmental, right of way, and other project activities leading to construction. The preferred alternative identified in this document is considered predecisional, pending the outcome of environmental studies.

2. PROJECT LOCATION:

This SR 2 Intersection Improvement project is located at its intersection with CR 500 W. Reference post along SR 2 is 55+04. GPS Coordinates are 41.59149 North and -86.79792 West.

The project is located 2 miles west of the city of LaPorte, in LaPorte County, in the LaPorte District. The adjacent map shows the project location.



3. NEED AND PURPOSE:

This intersection has a very high frequency of Crashes (Index of Crash Frequency, ICF is 2.92). Right angle crashes are the most prevalent crash type, and often result in injury.

Motorist delay on CR 500 W is significant, with the southbound Level of Service (LOS) expected to reach F prior to the design year.

The purpose of this project would be to improve intersection safety and provide a long-term solution to ensure safe and efficient operation of the SR 2 and CR 500 W intersection.

4. EXISTING FACILITY:

See the ground level photographs, pages B-1 to B-2 of the Appendix, and the aerial plan sheets on page A-1 of the Appendix for existing conditions in the project area.

Basic Design Elements:

| | SR 2 | CR 500 W |
|---------------------|--|----------------------------------|
| Functional Class | Urban Principal Arterial- Other (Suburban) | Urban Major Collector (Suburban) |
| Posted Speed | 55 mph | 35 mph |
| Member Road Systems | 3R Network | |
| | Off National Highway System | |
| | On National Truck Network | |
| | SubRegional Corridor (Statewide Mobility Corridor Map) | |
| Lane Width | 12' | 11' |
| Shoulder Width | 0' paved (6' usable) | 0' paved (0' usable) |

Pavement History/Condition:

SR 2 Pavement Data has not been provided. The designer shall obtain this information early in the design phase and include it with the pavement design request.

Existing Right-of-way (R/W):

An INDOT Records request indicated one previous project within the project limits. The R/W for project 17 Section D 1920 was acquired by R/W grant in 1939 and was not recorded until 1976. The plans show a 30' R/W each side of the centerline, however the R/W grants indicate a 50' R/W each side of the centerline. For purposes of this report, the apparent R/W was shown as the 50' described in the grants. Due to the untimeliness of the recording of the project grants reacquisition of this r/w may be necessary.

Drainage:

Drainage within the project area is generally with sheet flow to existing side ditches. The drainage along SR 2 in this area is generally from the east to the west, eventually outletting off of the south right-of-way.

5. FIELD CHECK:

USI personnel visited the project site on January 05, 2021.

Data collected included field measurements, photographs, and identification of utilities.

6. TRAFFIC DATA:

Turning movement counts were performed by the LaPorte District on 10/20/2020. AADT along SR 2 was obtained from the INDOT TCDS website. This information is included in the appendix on page B-3. 2026 Traffic volumes were inflated 105%, and 2046 Design Year traffic was inflated 115%. Official traffic projections from INDOT should be requested early in the design phase of the project.

Capacity analysis is being performed for the existing condition and for some of the alternates. This information will be included in the section titled “Alternates and Recommendations”.

7. CRASH DATA:

Crash data was reviewed as part of this assessment and a RoadHAT analysis was prepared. A total of 22 recorded crashes took place within the project limits during the three-year crash study period (2017 through 2019). The following tables summarize the number and types of crashes, as well as the RoadHAT results.

Crash History

| | | | |
|--------------------------|------|--|----|
| ICC | 0.69 | Number of Crashes | 22 |
| ICF | 2.92 | Number of Fatal and Incapacitating Crashes | 0 |
| First Year of Crash Data | 2017 | Number of Non-Incapacitating Crashes | 6 |
| Last Year of Crash Data | 2019 | Number of Property Damage Only Crashes | 16 |

NOTE: * The RoadHAT ICF and ICC data has excluded the four animal crashes from the dataset (per RoadHAT guidelines).

Crash Patterns: Manner of Collision

| Manner of Collision | Number | Percent |
|--|--------|---------|
| Backing Crash | 0 (0) | 0.00% |
| Collision With Animal (Including Deer) * | 4 (1) | 18.18% |
| Collision With Object in Road | 0 (0) | 0.00% |
| Head On (Between Motor Vehicles) | 1 (0) | 4.55% |
| Left Turn, Right Turn or Angle | 8 (3) | 36.36% |
| Opposite Direction Sideswipe | 3 (0) | 13.64% |
| Ran Off Road | 1 (0) | 4.55% |
| Rear End | 3 (2) | 13.64% |
| Same Direction Sideswipe | 1 (0) | 4.55% |
| Other | 1 (0) | 4.55% |
| Total | 22 (6) | 100.00% |

X (Y): X indicates the number of crash type
Y indicates those resulting in injury

The RoadHAT analysis resulted in an Index of Crash Frequency (ICF) of 2.96, indicating that the crash frequency at this intersection is in the 99th percentile compared to similar intersections. The Index of Crash Cost (ICC) is 0.69, indicating that the crash costs at this intersection are in the 64th percentile compared to similar intersections.

Proposed improvement alternates shall address ways to reduce right angle crashes on SR 2.

Crash Pattern Analysis

Pavement Condition Percentages

| Type | Number | Percent | Standard Value* Comparison: |
|-----------------------------|--------|---------|-----------------------------|
| On Snowy or Icy Pavement | 2 | 9.09% | 11.18% |
| On Wet Pavement | 3 | 13.64% | 15.49% |
| On Dry Pavement | 17 | 77.27% | 73.17% |
| On Other Condition Pavement | 0 | 0.00% | 0.16% |

*Standard values are based on 2014-2018 data for all state-owned facilities. Standard values are included for comparison purposes only.

Lighting Condition Percentages

| Type | Number | Percent | Standard Value* Comparison: |
|-----------------------------|--------|---------|-----------------------------|
| Dark (Lighted or Unlighted) | 5 | 22.73% | 32.76% |
| Dawn/Dusk | 2 | 9.09% | 5.49% |
| Daylight | 15 | 68.18% | 61.66% |
| Other | 0 | 0.00% | 0.10% |

*Standard values are based on 2014-2018 data for all state-owned facilities. Standard values are included for comparison purposes only.

Weather Condition Percentages

| Type | Number | Percent | Standard Value* Comparison: |
|------------------------|--------|---------|-----------------------------|
| Clear | 14 | 63.64% | 62.39% |
| Cloudy | 2 | 9.09% | 18.33% |
| Fog (Or Smoke or Smog) | 2 | 9.09% | 0.68% |
| Rain | 2 | 9.09% | 9.79% |
| Snow or Sleet | 0 | 0.00% | 6.41% |
| Blowing Material | 2 | 9.09% | 2.13% |
| Severe Cross Winds | 0 | 0.00% | 0.26% |

*Standard values are based on 2014-2018 data for all state-owned facilities. Standard values are included for comparison purposes only.

8. ALTERNATIVES AND RECOMMENDATION:

This report will look at possible intersection improvement alternates. All build alternates discussed in the Indiana Intersection Design Guide will be evaluated.

Alternate 1: Do Nothing

The Do Nothing Alternate does not address the high amount of crashes, nor the growing delay that occurs for the unsignalized approaches.

A Level of Service (LOS) analysis for the unsignalized intersection was performed for the intersection using Synchro (10.3). The following table summarizes LOS for the current traffic and design year.

| Intersection | | | 2020 | | | | | | 2046 | | | | | |
|------------------------------|---------|----------------|--------------|-----------|------------|--------------|-----------|------------|--------------|-----------|------------|--------------|-----------|------------|
| | | | AM Peak Hour | | | PM Peak Hour | | | AM Peak Hour | | | PM Peak Hour | | |
| | | | LOS | Delay (s) | Queue (ft) | LOS | Delay (s) | Queue (ft) | LOS | Delay (s) | Queue (ft) | LOS | Delay (s) | Queue (ft) |
| SR 2 at CR 500 W Alternate 1 | Overall | | B | | | C | | | C | | | C | | |
| | EB | LT TH RT | A | 8 | 2 | A | 8.4 | 2 | A | 0.5 | 2 | A | 8.6 | 2 |
| | WB | LT TH RT | A | 8.2 | 6 | A | 8.0 | 2 | A | 2.0 | 6 | A | 8.2 | 2 |
| | NB | LT TH RT | C | 17.2 | 10 | C | 19.9 | 10 | C | 20.7 | 14 | C | 24.9 | 14 |
| | SB | LT TH RT | C | 20.2 | 28 | D | 33.0 | 66 | D | 27.9 | 46 | F | 65.5 | 122 |

As user delay on the south approach already fails (LOS F) and is expected to get worse as traffic growth occurs and this alternate does not address the purpose of improving intersection safety, this alternate is being dismissed as a viable long-term solution.

Alternate 2: Signalized Intersection

Alternate 2 would consist of installing a traffic signal at the SR 2 and CR 500 W intersection. An eastbound and westbound left turn auxiliary lane would be constructed. Widening of approximately 6 feet each side of the existing pavement will be required along SR 2. Installing a signal without the left turn lane construction on SR 2 was not considered viable due to rear-end crash concerns.

A Synchro (10.3) traffic signal analysis provided the following Level of Service (LOS) results for the signalized intersection. The summary tables provides LOS, Delay and Queuing length for each movement.

| Intersection | | | 2026 | | | | | | 2046 | | | | | |
|------------------------------|---------|----|--------------|-----------|------------|--------------|-----------|------------|--------------|-----------|------------|--------------|-----------|------------|
| | | | AM Peak Hour | | | PM Peak Hour | | | AM Peak Hour | | | PM Peak Hour | | |
| | | | LOS | Delay (s) | Queue (ft) | LOS | Delay (s) | Queue (ft) | LOS | Delay (s) | Queue (ft) | LOS | Delay (s) | Queue (ft) |
| SR 2 at CR 500 W Alternate 2 | Overall | | A | 9.7 | | A | 8.0 | | A | 9.9 | | A | 9.5 | |
| | EB | LT | A | 6.2 | 9 | A | 6.2 | 15 | A | 6.1 | 10 | A | 6.5 | 16 |
| | | TH | B | 11.1 | 81 | A | 8.5 | 92 | B | 11.3 | 91 | B | 10.1 | 103 |
| | | RT | A | 0 | 0 | A | 0.2 | 1 | A | 0.1 | 1 | A | 0.4 | 1 |
| | WB | LT | A | 8.9 | 38 | A | 5.8 | 10 | B | 9.4 | 33 | A | 5.9 | 11 |
| | | TH | B | 10.8 | 75 | A | 8.7 | 100 | B | 11.0 | 84 | B | 10.6 | 113 |
| | | RT | A | 0 | 0 | A | 2.2 | 13 | A | 0.1 | 1 | A | 2.3 | 14 |
| | NB | LT | A | | | A | | | A | | | A | | |
| | | TH | A | 6.6 | 18 | A | 8.3 | 19 | A | 7.0 | 21 | A | 8.6 | 21 |
| | | RT | | | | | | | | | | | | |
| | SB | LT | | | | | | | | | | | | |
| | | TH | A | 6.3 | 33 | A | 8.8 | 56 | A | 6.7 | 37 | B | 11.2 | 62 |
| | | RT | | | | | | | | | | | | |

As shown in the above table, the intersection functions at LOS A or B for all approach movements.

A Traffic Signal warrant analysis was performed utilizing 2026 projected traffic data. Only warrant 3, Peak-Hour warrant, meets the warrant conditions. This signal warrant shall be applied only in unusual cases, such as office complexes, manufacturing plants, industrial complexes, or high-occupancy vehicle facilities that attract or discharge large numbers of vehicles over a short time. A copy of the warrant analysis is included in the appendix, pages B-5 to B-6.

This alternate meets the primary purpose and need of reducing user delay and improving safety by controlling the movements via a traffic signal. Alternate 2 will be moved forward for further discussion and analysis.

Alternate 3: Median U-turn (J-turn)

As SR 2 is a two-lane roadway without a median, this project would require extensive reconstruction of the SR 2 corridor to construct. It is being dismissed as a viable option.

Alternate 4: Roundabout Intersection

Alternate 4 (see plan display A-3 of the appendix) would consist of a constructing a single lane roundabout at the SR 2 and CR 500 W intersection. The proposed roundabout is closely centered at the existing intersection. Approach work is approximately 420' east and west along SR 2 and 380' north and south along CR 500 W.

The anticipated LOS for this alternate is shown in the following table.

| Intersection | | | 2026 | | | | | | 2046 | | | | | |
|------------------------------|---------|----------------|--------------|-----------|------------|--------------|-----------|------------|--------------|-----------|------------|--------------|-----------|------------|
| | | | AM Peak Hour | | | PM Peak Hour | | | AM Peak Hour | | | PM Peak Hour | | |
| | | | LOS | Delay (s) | Queue (ft) | LOS | Delay (s) | Queue (ft) | LOS | Delay (s) | Queue (ft) | LOS | Delay (s) | Queue (ft) |
| SR 2 at CR 500 W Alternate 3 | Overall | | A | 5.2 | | A | 5.9 | | A | 5.2 | | A | 5.8 | |
| | EB | LT TH RT | A | 5.0 | 51 | A | 5.7 | 61 | A | 4.9 | 52 | A | 5.6 | 61 |
| | WB | LT TH RT | A | 4.7 | 63 | A | 5.0 | 70 | A | 4.7 | 63 | A | 4.9 | 70 |
| | NB | LT TH RT | A | 6.5 | 7 | A | 7.3 | 6 | A | 6.4 | 7 | A | 7.3 | 6 |
| | SB | LT TH RT | A | 7.6 | 16 | A | 8.8 | 25 | A | 6.4 | 7 | A | 8.7 | 26 |

This alternate meets the primary purpose and need of reducing the number of right angle crashes and improving the LOS at the intersection. This alternate will be moved forward for further discussion and analysis.

Alternate 5: Displaced Left-turn intersection

Displaced Left-turn intersections should be considered in situations with heavy left turn and heavy through movements. This intersection improvement would require 3 signals in order to safely remove left turns from the main intersection at SR 2 and CR 500 W. This type of intersection is primarily used in urban and suburban areas where the left turn volume is greater than 250 vehicles. As the volumes at the SR 2 and CR 500 W intersection do not approach the warrants associated with this alternate, and the footprint of this alternate is much greater in terms of costs and environmental impact, this alternate is being dismissed from further consideration.

Alternate 6: Jughandle Intersection

The Jughandle intersection removes the left turns from the major road (SR 2) by constructing ramps to the minor road (CR 500 W). Jughandle intersections are typically

considered when an Urban signalized intersection is nearing saturation. By removing the left-turns from the intersection nearing full saturation, the signal phasing can be simplified, thus improving the LOS of the main intersection.

This alternate is being dismissed as a viable alternate for the SR 2 and CR 500 W intersection for the following reasons:

- Intersection volumes do not resemble that of a typical Jughandle application
- Large footprint required to construct ramps
- High costs in terms of construction cost and right-of-way impact

Alternate 7: Offset T Intersection

The offset T intersection is typically utilized when the major road and the minor road have a skew. Construction of the Offset T at this location would involve reconstruction of the north leg of CR 500 W, bringing it into SR 2 approximately 700' west of its current location. This would involve approximately 2000' of new alignment roadway for the north leg of CR 500 W. Due to this substantial S-line work, this alternate is being dismissed from further consideration.

Alternate 8: Continuous Green T Intersection (CGT)

A continuous green "T" intersection is only applicable to an existing "T" intersection. As Alternate 7 (Offset "T" Intersection) was dismissed, Alternate 8 is not applicable.

Alternate 9: Quadrant Roadway Intersection

A quadrant intersection is utilized at locations where high volumes of left turn movements exist along the major road. The improvements associated with this alternate would construct a new roadway in one designated quadrant in order to remove all of the left turns from the main intersection. This alternate is similar in application and impact to Alternate 6 (Jughandle Intersection), and is being dismissed for similar reasons.

Alternate 10: Grade Separation

A grade separation would take CR 500 W over SR 2. Vertical clearance over SR 2 would be 16'-6" minimum. Additionally, this alternate would only serve the CR 500 W through movements. CR 500 W traffic that currently turns east or west onto SR 2 would have to utilize the existing road network.

This alternate is being dismissed due to the high costs associated with constructing a new bridge (estimated length 80') over SR 2, and the limited benefit to motorists as turning movements would be eliminated.

Preliminary Recommendation: Alternates 2 & 4 are being moved forward as viable alternates. Additional analysis and discussion will take place in this report prior to the final recommendation. The following table is a direct relative comparison matrix of Alternates 2 & 4.

| Comparison Matrix | | |
|----------------------------|-------------------------|-----------------------------|
| | Alternate 2 (Signal) | Alternate 4 (Roundabout) |
| Safety | 2 | 1 |
| Operational Performance | 1 | 1 |

| | | |
|-----------------------|---|---|
| Cost | 1 | 2 |
| Environmental Impacts | 1 | 1 |
| Total | 5 | 5 |

Safety: Alternate 4 is viewed as the best alternate to reduce right angle crashes. Roundabouts by their physical nature have a high reduction in physical injury accidents due to the slowing of speeds and entry angles to potential conflict points. Alternate 2, with construction of left turn auxiliary lanes along SR 2 will have mitigated the expected increase in rear end crashes associated with installation of a traffic signal.

Operational Performance: The overall intersection LOS for both Alternate 2 and 4 is “A” through the design year for both the AM Peak and the PM Peak traffic volumes. These two alternates are rated equally in terms of Operational Performance.

Costs: Costs for both alternates have been developed and are summarized in the following table.

| Alternate Cost Summary Table | |
|------------------------------|-------------------|
| Alternate | Construction Cost |
| Alternate 2 | \$920,000 |
| Alternate 4 | \$1,220,000 |

Detailed Construction cost estimates are included on pages C-1 to C-4 of the appendix.

Environmental Impacts: Alternate 2 and 4 are similar in the amount of new construction area and the amount of R/W needed to build each project. The two alternates score equally in terms of environmental impact.

Final Recommendation:

Alternate 2 and 4 rank equally in the Comparison Matrix. Alternate 4 is the recommended alternate as it provides the greatest improvement to motorist safety, meets the purpose and need of improving safety and capacity at this intersection.

9. MAINTENANCE OF TRAFFIC DURING CONSTRUCTION:

Construction of Alternate 4 can be done while maintaining a minimum of one lane in each direction for SR 2 traffic. A short closure of each leg of CR 500 W is anticipated in order to reconstruct each approach.

10. COST ESTIMATE (2021):

Estimated Costs for Alternate 4 are summarized in the following table:

| | <i>Alternate 4</i> |
|--------------------------------|---------------------------|
| <i>Phase</i> | |
| Right of Way Purchase: | \$60,000 |
| Right of Way Services: | \$50,000 |
| Preliminary Engineering 1: | \$300,000 |
| Railroad PE 1: | \$0 |
| Utilities PE (UT1): | \$10,000 |
| Utilities CN (UT2): | \$0 |
| Construction Total: | \$1,220,000 |
| Construction (CN): | \$1,220,000 |
| Railroad Construction: | \$0 |
| Maintenance of Traffic: | Included in CN |
| Environmental Mitigation: | \$0 |
| ADA | \$0 |
| Construction Engineering (CE): | \$150,000 |
| Other Considerations: | \$0 |
| TOTAL: | \$1,790,000 |

11. ENVIRONMENTAL CONSIDERATIONS:

Preliminary desktop review utilizing ArcGIS and INDOT SAM layers indicate minor environmental concerns within the project study area.

A planned Trail “State Road 2/Westville Trail Corridor” is mapped running parallel with SR 2 along the north side of the roadway. The potential project is sponsored by the Northwestern Indiana Regional Planning Commission (NIRPC). According to the NIRPC website the corridor was identified in the “2010 Ped and Pedal” report as a “medium priority”. Based on the site visit conducted in January 2021, no indication of a proposed trail was identified near the project area.

One (1) Underground Storage Tank (UST) is mapped near the project area. The feature appears to be registered to the gas station “Family Express” located in the northeast quadrant of the project area. Although the UST is mapped in the northwest quadrant of the project area, the gas station tanks appear to be located on the southernmost portion of the property in the northeast quadrant. Further investigation will be required during the project development process to determine if a Phase 1 Environmental Site Assessment (ESA) would be required for R/W acquisition from the property with noted history with the IDEM Office of Land Quality.

No surface water features (streams or wetlands) were identified within the project area. Further investigation will be required to confirm if any jurisdictional features are present. Based on the preliminary desktop review no permits are anticipated for the proposed alternative.

Based on the findings of the preliminary desktop review of the existing and proposed conditions, it is anticipated that a Level 1 Categorical Exclusion (CE) will be the required NEPA

documentation for the recommended alternative. If proposed R/W increases above 0.5 acre of new R/W, the CE document will escalate to a Level 2 and will require opportunity for public hearing. The estimated completion time for this NEPA document is 9 months from receipt of preliminary plans if there are no requests for public hearing. No waterway permits would be anticipated. If the project area results in greater than 1 acre then a Rule 5 permit would be required.

12. SURVEY REQUIREMENTS:

Survey for Alternate 4 should extend 6000' west and 600' east of CR 500 W and extend 100' north and south of the centerline. Survey coverage on CR 500 W should extend 400' north and south of SR 2 and extend 75' east and west of the centerline.

13. RIGHT-OF-WAY IMPACT:

Alternate 4 will require permanent right-of-way from 4 parcels. Estimated right-of-way is shown on the plan display (A-3 of the appendix). The total estimated amount of right-of-way needed for Alternate 4 is approximately 0.13 acre of new r/w. Reacquisition of apparent r/w of nearly 2.0 acres may be necessary.

14. UTILITY IMPACT:

A utility locate request identified the following utilities within the project area.

| Utility Name | Type of Facility | Contact Person | Initial Notice Response |
|--------------|--------------------|--|--|
| Frontier | Telecommunications | Diana Sacks Diana.l.sacks@ftr.com 219-531-2118 | Frontier has buried and aerial facilities within the project area. See map of facilities, Section D of appendix. |
| NIPSCO | Electric | Dean Garrett utilitycoordination@nisource.com 219-547-6260 | Overhead transmission and distribution exist throughout the project. See map of facilities, Section D of appendix. |
| NIPSCO | Gas | Dean Garrett utilitycoordination@nisource.com 219-547-6260 | 2" and 4" facilities exist throughout the project. See map of facilities, Section D of appendix. |

Overhead electric facilities along SR 2 will likely be impacted with the development of Alternate 2. Determining if buried gas, water and sewer will be impacted is dependent upon the exact horizontal and vertical location of their facilities. During the design, exact horizontal and vertical location of the buried facilities in critical areas will need to be determined utilizing Subsurface Utility Engineering (SUE).

15. RELATED PROJECTS:

USI reviewed the INDOT website <https://entapps.indot.in.gov/dotmaps/nlri/>. This site shows programmed projects throughout the state through Fiscal Year 2025.


Des. No 1702989: Intersection Improvement, Roundabout, US 6 at US 421 is on the proposed detour route for the subject project. Des. No. 1702989 is scheduled for

letting in Q1 of 2023 and will be completed by the time this subject project is completing design development.

As this project develops, the designer, along with the INDOT Project Manager shall continue to monitor other projects along the corridor and detour.

16. CONCURRENCE:

This document was prepared by:


Greg R. Wendling, P.E.
USI Consultants, Project Manager

DATE: November 05, 2021

Reviewed by:

**Alan
Holderread**
Alan Holderread, PE
District Traffic Engineer, LaPorte District

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Reviewed by:

Paul South
Paul South, PE
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System Asset Manager, LaPorte District

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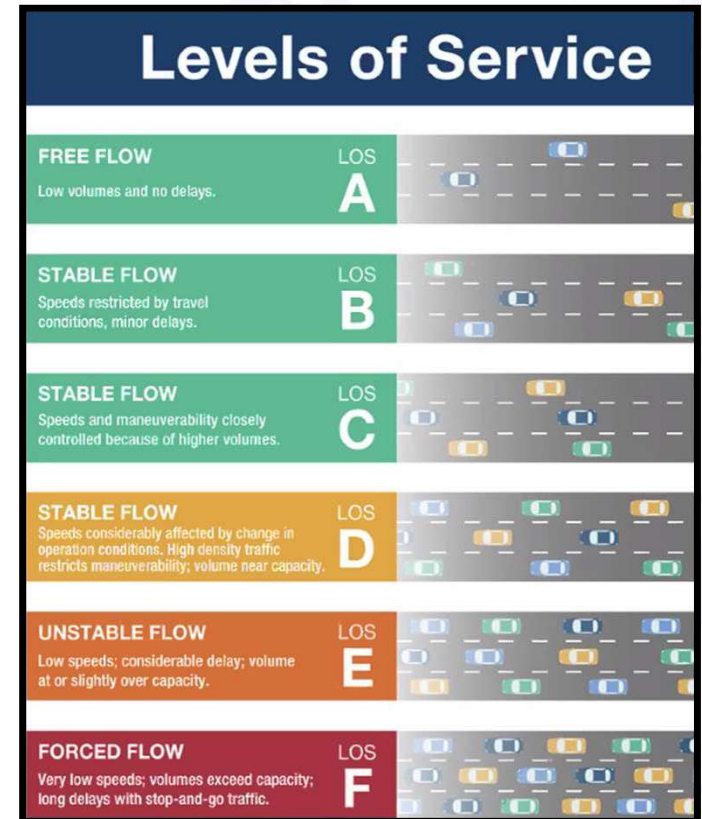
Main Alternatives Considered

No Build Alternative

- Baseline for comparison of build alternative
- Does not meet purpose and need of project
- Does not improve the mobility or safety of the intersection

Traffic Signal with Added Left Turn Lanes

- Left turn lanes required on S.R. 2 for mobility and safety
- Signals are known to increase crashes due to rear-ends and red light running
- Does not reduce intersection conflict points or crashes
- Provides Level of Service B
- Does not fully meet the purpose and need of the project



Land and Water Conservation Fund (LWCF) County Property List for Indiana (Last Updated March 2022)

| ProjectNumber | SubProjectCode | County | Property |
|---------------|----------------|---------|---------------------------------------|
| 1800200 | 1800200 | LaPorte | Fox Memorial Park |
| 1800228 | 1800228 | LaPorte | Fox Memorial Park |
| 1800265 | 1800265 | LaPorte | Kesling Park |
| 1800332 | 1800332 | LaPorte | Rumley Park |
| 1800351 | 1800351 | LaPorte | Westville Park (Prairie Meadow Park) |
| 1800373 | 1800373 | LaPorte | Kesling Park |
| 1800402 | 1800402 | LaPorte | Nelson Park |
| 1800405 | 1800405V | LaPorte | Galena Marsh Nature Preserve |
| 1800453 | 1800453 | LaPorte | Luhr Park |
| 1800547 | 1800547 | LaPorte | Hansen & Gifford Parks/Old Spur Trail |
| 1800608 | 1800608 | LaPorte | Luhr County Park |

*Park names may have changed. If acquisition of publically owned land or impacts to publically owned land is anticipated, coordination with IDNR, Division of Outdoor Recreation, should occur.