

FHWA-Indiana Environmental Document
CATEGORICAL EXCLUSION / ENVIRONMENTAL ASSESSMENT FORM
GENERAL PROJECT INFORMATION

Road No./County:

State Road (SR) 9 and SR 234 / Hancock County

Designation Number(s):

2000154

Project
Description/Termini:

Intersection improvement at SR 9 and SR 234 intersection from 0.05 mile South of SR 234 to 0.06 mile North of SR 234 and 0.09 mile West of SR 9 to 0.07 mile East of SR 9

X	Categorical Exclusion, Level 2 – Required Signatories: INDOT DE and/or INDOT ESD
	Categorical Exclusion, Level 3 – Required Signatories: INDOT ESD
	Categorical Exclusion, Level 4 – Required Signatories: INDOT ESD and FHWA
	Environmental Assessment (EA) – Required Signatories: INDOT ESD and FHWA
	Additional Investigation (AI) – The proposed action included a design change from the original approved environmental document. Required Signatories must include the appropriate environmental approval authority

Approval

INDOT DE Signature and Date

INDOT ESD Signature and Date

FHWA Signature and Date

Release for Public Involvement

REB 10/20/2025
INDOT DE Initials and Date

N/A
INDOT ESD Initials and Date

Certification of Public Involvement

INDOT Consultant Services Signature and Date

INDOT DE/ESD Reviewer Signature and Date:

Name and Organization of CE/EA Preparer:

Alex Tadda, Crawford, Murphy, and Tilly Inc.

Note: Refer to the most current INDOT CE Manual, guidance language, and other ESD resources for further guidance regarding any section of this form.

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Part I – Public Involvement

Every Federal action requires some level of public involvement, providing for early and continuous opportunities throughout the project development process. **The level of public involvement should be commensurate with the proposed action.**

	Yes	No
Does the project have a historic bridge processed under the Historic Bridges PA*?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
If No, then: Opportunity for a Public Hearing Required?	<input checked="" type="checkbox"/>	<input type="checkbox"/>

*A public hearing is required for all historic bridges processed under the Historic Bridges Programmatic Agreement between INDOT, FHWA, SHPO, and the ACHP.

Discuss what public involvement activities (legal notices, letters to affected property owners and residents (i.e. notice of entry), meetings, special purpose meetings, newspaper articles, etc.) have occurred for this project.

Notice of Survey letters were mailed to potentially affected property owners near the project area on May, 23, 2022, and April 26, 2023, notifying them about the project and that individuals responsible for land surveying and field activities may be seen in the area. A sample copy of the Notice of Survey letter is included in Appendix G, page 1.

To meet the public involvement requirements of Section 106, a legal notice of FHWA's finding of "No Adverse Effect" was published in the *Daily Reporter* on September 10, 2025 offering the public an opportunity to submit comment pursuant to 36 CFR 800.2(d), 800.3(e), and 800.6(a)(4). The public comment period closed 30 days later on October 11, 2025. The text of the public notice and the affidavit of publication appear in Appendix D, pages D-110 to D-112. One comment was received during the comment period. In an email dated September 25, 2025, the representative of the current owner of the Irving Materials, Inc. Office Building property indicated that the property owner was unaware of the "No Adverse Effect" documentation until a representative from the Irving Materials, Inc. property notified them on September 5, 2025. The property owner requested a 30-day extension of the comment period to allow for a reasonable response, which has been granted by INDOT (Appendix D, page D-113). No other public comments were received. This document will be revised once the 30-day comment period extension has closed on November 10, 2025.

The project will meet the minimum requirements described in the current *Indiana Department of Transportation (INDOT) Project Development Public Involvement Procedures Manual* which requires the project sponsor to offer the public an opportunity to submit comments and/or request a public hearing. Therefore, a legal notice will appear in a local publication contingent upon the release of this document for public involvement. This document will be revised after the public involvement requirements are fulfilled.

Public Controversy on Environmental Grounds

Discuss public controversy concerning community and/or natural resource impacts, including what is being done during the project to minimize impacts.

At this time, there is no substantial public controversy concerning impacts to the community or to natural resources.

Part II - General Project Identification, Description, and Design Information

Sponsor of the Project: Indiana Department of Transportation (INDOT) INDOT District: GreenfieldLocal Name of the Facility: SR 9 and SR 234Funding Source (mark all that apply): Federal ☒ State ☒ Local ☐ Other* ☐

*If other is selected, please identify the funding source: _____

This is page 2 of 26 Project name: SR 9 and SR 234 Intersection Improvement Date: October 14, 2025

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PURPOSE AND NEED:

The need should describe the specific transportation problem or deficiency that the project will address. The purpose should describe the goal or objective of the project. The solution to the traffic problem should NOT be discussed in this section.

INDOT has identified the need to address recurring queues that lead to elevated frequency of crashes throughout the limits of the project, specifically the pattern of rear end type crashes at the SR 9 and SR 234 intersection. The need for the project is supported by the crash history presented in the Traffic Analysis Memo (CMT, 2025) (Appendix I, pages I-2 to I-24) documenting high crash frequency.

The intersection of SR 9 and SR 234 is a four legged configuration, which presents 32 potential conflict points. Conflict points are locations where vehicle paths cross, merge, diverge, or queue behind one another, each representing an opportunity for a crash. The frequency of crashes at an intersection is closely tied to both the number of conflict points and the volume of conflicting traffic at each point. According to the Traffic Analysis Memo (CMT, 2025) for the project, within a 3-year period between 2022 and 2024, 24 crashes were reported at the SR 9 and SR 234 intersection in Hancock County. Of these crashes, 5 were rear-end crashes (21%) and 15 were right-angle crashes (63%).

Indices of Crash Frequency (ICF) and Crash Cost (ICC) are calculated to determine how many standard deviations away from the average an intersection's crash history and severity are compared to other similar intersections across Indiana. The ICF is used to consider the number of crashes, while the ICC is used to consider the severity of crashes. A positive ICF and ICC means an intersection is experiencing a higher frequency of crashes and a more expensive crash cost than similar intersections statewide. An ICF and ICC of greater than 1.5 indicates that an intersection should be evaluated for potential safety enhancements. An acceptable ICF and ICC for an intersection is 1.0 or less.

According to the Traffic Analysis Memo (CMT, 2025) for the project, from 2022 to 2024 the SR 9 and SR 234 intersection had an ICF of 1.22 and an ICC of -0.18. Therefore, the ICF results flag the project intersection as a safety concern. The memo states that although the ICF and ICC are an improvement from the 3.32 ICF and 1.92 ICC reported in the 2019 Engineering Assessment Report (INDOT, 2019) for the project, this should not be interpreted as a definitive indication of enhanced safety conditions at the intersection. The ICF is more than 1.0 standard deviations higher than similar intersections in the state. The high ICF is due to the high number of crashes at the intersection.

Intersection performance is measured by level of service (LOS), which is a measure of the quality of traffic flow. LOS values can range from LOS A, which is the least congested or free flow, to LOS F, which is the most congested or breakdown flow.

According to the Traffic Analysis Memo (CMT, 2025) (Appendix I, pages I-2 to I-24) for existing conditions, during PM peak hours, the southbound approach of the existing stop sign controlled intersection operates at a LOS E, and the eastbound and northbound approach operate at an unacceptable LOS F. If the current capacity issues are not addressed, in the design year 2045, the northbound and southbound approaches in the AM peak hours would also begin to operate at an unacceptable LOS E, and the southbound LOS would fall to LOS F along with the northbound and eastbound approaches.

The purpose of this project is to reduce the queues and achieve a LOS of B or better for this intersection and achieve an ICF of less than 1.0 or better for the intersection.

PROJECT DESCRIPTION (PREFERRED ALTERNATIVE):

County: HancockMunicipality: Town of Eden

Limits of Proposed Work: SR 9 from 0.05 mile South of SR 234 to 0.06 mile North of SR 234, and SR 234 from 0.09 mile West of SR 9 to 0.07 mile East of SR 9

Total Work Length: 0.30 Mile(s)Total Work Area: 2.62 Acre(s)Is an Interstate Access Document (IAD)¹ required?

If yes, when did the FHWA provide a Determination of Engineering and Operational Acceptability?

¹If an IAD is required; a copy of the approved CE/EA document must be submitted to the FHWA with a request for final approval of the IAD.

Yes¹

No

	X
Date:	

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Describe location of project including township, range, city, county, roads, etc. Existing conditions should include current conditions, current deficiencies, roadway description, surrounding features, etc. Preferred alternative should include the scope of work, anticipated impacts, and how the project will meet the Purpose and Need. Logical termini and independent utility also need discussed.

The INDOT Greenfield District and the Federal Highway Administration (FHWA) intend to proceed with an intersection improvement project located in Hancock County. The project is located at the intersection of SR 9 and SR 234 in Eden, Hancock County, Indiana (Appendix B, page B-2). The project is located within Sections 19 and 30, Township 17 North, and Range 7 East, on the United States Geological Survey (USGS) Ingalls, Indiana Quadrangle (Appendix B, page B-3). The project extends along SR 9 from approximately 0.06 mile north of SR 234 (Sta. 53+37.00) to approximately .05 mile south of SR 234 (Sta. 47+00.00). The project also extends along SR 234 from approximately 0.09 mile west of SR 9 (Sta. 5+50.00) to approximately 0.07 mile east of SR 9 (Sta. 13+75.00) (Appendix B, page B-12).

SR 9 is classified by INDOT as a *Rural Minor Arterial*. Within the project area, SR 9 consists of one 12-foot wide travel lane in each direction. North of the intersection, a 12-foot wide auxiliary lane is present along northbound SR 9 which also provides access to the drive leading to Eden Elementary School. The shoulders of SR 9 are between 2 and 4 feet within the project area. SR 9 travels north-south and the posted speed limit is 55 miles per hour (mph). There are several residential and commercial driveways along SR 9. The recorded 2025 traffic volume was 7,496 vehicles per day with 12.51% truck.

SR 234 is classified by INDOT as *Rural Major Collector*. Within the project area, SR 234 consists of one 12-foot wide travel lane in each direction and an existing 10-foot dedicated right turn lane on the west leg. The shoulders of SR 234 are between 2 and 4 feet within the project area. SR 234 travels east-west and the posted speed limit is 55 MPH. There are several residential and commercial driveways along SR 234. A total of six structures are located within the construction limits, in the form of 5 driveway culverts and 1 pipe inlet that did not have an outlet within the project area. The recorded 2025 traffic volume was 5,227 vehicles per day with 4.45% truck.

The existing SR 9 and SR 234 intersection is four legged and stop sign controlled with flashing beacons; the intersection presents 32 potential conflict points. The frequency of crashes at an intersection is closely tied to both the number of conflict points and the volume of conflicting traffic at each point. The intersection is experiencing elevated crash rates with a significant pattern of rear end crashes (21% of total crashes) associated with recurring queueing at the intersection, operating at a LOS of E at the southbound approach and operating at a LOS of F at the eastbound and northbound approaches at PM peak hours, respectively. Several interim measures including enhanced signage and an overhead flasher have not reduced the ICF at the intersection below 1.0.

Land use in the vicinity of the project is primarily rural with commercial and residential buildings scattered throughout the area. Green County Fire Department is located adjacent to the project area. Eden Elementary is located 528 feet northeast from the project area. Construction will happen in phases during the summer months when school is not in session to limit impacts to the surrounding community. Drainage in the project area is conveyed via roadside ditch (RSD) to the surrounding area. No water resources are reported within the project area.

The preferred alternative will involve converting the existing intersection from an all-way stop to a single lane roundabout. Construction of the roundabout consists of replacing the existing pavement with a full depth hot mixed asphalt (HMA) overlay through the intersection and asphalt milling and overlaying at the approaches. The posted speed limits on SR 9 and SR 234 will be reduced to 40 MPH. Residential and commercial driveways entrances will be impacted by construction and will be reconstructed with HMA overlays. Residential driveway culverts will need to be replaced and installed due to the construction of the roundabout. The five driveway culverts will be replaced, and the pipe inlet will be made into a culvert with an inlet and outlet within the project area. A total of 1.89-acres of permanent and 0.24-acre temporary right-of-way from adjacent property owners will be needed to allow for construction. The roundabout is expected to have a standard inscribed diameter and entry angles for the higher speeds along SR 234. The total length of the improvements is 0.30 mile. The roundabout will accommodate heavy vehicle traffic and bus traffic from the nearby fire station and school respectively. Every effort to avoid, minimize, and/or mitigate project impacts will be made. The proposed improvements are shown on the plan sheets included in Appendix B, pages B-12 to B-39.

The maintenance of traffic (MOT) for the project will involve phased construction and road closures along SR 234 west and east of the intersection with posted detours. One lane in each direction along SR 9 will remain open during Phase 1 and one bi-directional lane with the use of a temporary traffic signal will be used during phases 2-4 of the project. Refer to the Maintenance of Traffic (MOT) During Construction section of this document and plan sheets (Appendix B, pages B-22 to B-32) for details.

The preferred alternative will meet the purpose and need of the project by improving safety and traffic operations at the intersection. Specifically, it is expected to reduce the number of crashes to achieve an ICF of less than 1.0 or better and enhance the LOS from

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E/F to B or better. This improvement will help alleviate the standing queues currently caused by stop sign control, resulting in more efficient traffic movement.

In addition to meeting the purpose and need of the project, the preferred alternative, a single lane roundabout, offers a notable safety benefit through a significant reduction in conflict points compared to a conventional four leg intersection. According to Roundabouts: An Informational Guide (FHWA, 2000) (available at [Roundabouts: An Informational Guide](#)), a standard four leg intersection presents 32 potential conflict points. In contrast, a four leg single lane roundabout reduces that number to just 8, representing a 75% decrease in potential conflict points. This reduction contributes to improved safety outcomes, including fewer and less severe crashes, and supports smoother, more efficient traffic flow. According to the 2019 Engineering Assessment Report (INDOT, 2019), conversion of the SR 9 and SR 234 intersection to a single lane roundabout is expected to reduce the number of crashes by 49% (Appendix I, page I-29).

Construction is anticipated to begin in Spring 2026 and be completed by Spring 2027.

The project termini were established based on the limits of the necessary work to accommodate the intersection improvements. The project is not dependent on any other projects to meet the project purpose and need, and therefore exhibits independent utility.

OTHER ALTERNATIVES CONSIDERED:

Provide a header for each alternative. Describe all discarded alternatives, including the No Build Alternative. Explain why each discarded alternative was not selected. Make sure to state how each alternative meets or does not meet the Purpose and Need and why.

Signal Controlled Intersection

The Signal Controlled Intersection alternative would involve converting the existing intersection from an all-way stop controlled intersection to a signal-controlled intersection. This alternative would also include widening each approach to include left turn lanes. This alternative would maintain a LOS of A through the design year 2045. According to the 2019 Engineering Assessment Report (INDOT, 2019), it is estimated that the change in traffic control and added turn lanes would reduce crashes at the intersection by 44% and should significantly reduce the standing queue and the major crash pattern associated with the queue (Appendix I, page I-28). This alternative meets the purpose and need of this project by reducing the queue and therefore reducing the number of overall crashes at the intersection. However, this alternative would require a larger construction footprint, resulting in greater impacts to surrounding properties, such as increased tree removal and the need for approximately 2.81 acres of additional ROW acquisition, as estimated in the Addendum to the 2019 Engineering Assessment Report (CMT, 2022) (Appendix I, pages I-25 to I-26). Furthermore, unlike the preferred alternative (single lane roundabout), the signal controlled intersection would not provide the safety benefit of a 75% reduction in conflict points. As a result, it would be less effective in reducing the frequency and severity of crashes. Therefore, this alternative was eliminated from further consideration.

No Build Alternative

The No Build alternative has no cost and involves no action in the project area. The existing safety concerns at the intersection would continue to persist if no action is taken. The intersection would continue to be a four-way stop-controlled intersection with flashing beacons. Long term capacity issues with the existing traffic control would not be addressed and the significant rear-end crash rate would remain unchanged. This alternative does not address the purpose and need to reduce the queue and therefore reduce the number of overall crashes at the intersection. Since this alternative would not fulfill the purpose and need, it was eliminated from further consideration.

No other alternatives were considered.

The No Build Alternative is not feasible, prudent or practicable because (Mark all that apply)

It would not correct existing capacity deficiencies;

It would not correct existing safety hazards;

It would not correct the existing roadway geometric deficiencies;

It would not correct existing deteriorated conditions and maintenance problems; or

It would result in serious impacts to the motoring public and general welfare of the economy.

Other (Describe):

X
X

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ROADWAY CHARACTER:

If the proposed action includes multiple roadways, complete and duplicate for each roadway.

Name of Roadway SR 9
 Functional Classification: Minor Arterial
 Current ADT: 7,496 VPD (2025) Design Year ADT: 8,441 VPD (2045)
 Design Hour Volume (DHV): 950 Truck Percentage (%) 12.51%
 Designed Speed (mph): 40 Legal Speed (mph): 40

	Existing	Proposed
Number of Lanes:	2	2
Type of Lanes:	Through Lanes	Through Lanes
Pavement Width:	14 ft.	12-24 ft.
Shoulder Width:	2-4 ft.	2-4 ft.
Median Width:	N/A ft.	8-25 ft.
Sidewalk Width:	N/A ft.	N/A ft.

Setting: ☐ Urban ☐ Suburban ☒ Rural
 Topography: ☒ Level ☐ Rolling ☐ Hilly

Name of Roadway SR 234
 Functional Classification: Major Collector
 Current ADT: 5,227 VPD (2025) Design Year ADT: 6,409 VPD (2045)
 Design Hour Volume (DHV): 575 Truck Percentage (%) 4.45%
 Designed Speed (mph): 55 Legal Speed (mph): 40

	Existing	Proposed
Number of Lanes:	2	2
Type of Lanes:	Through Lanes	Through Lanes
Pavement Width:	14 ft.	12-24 ft.
Shoulder Width:	2-4 ft.	2-4 ft.
Median Width:	N/A ft.	8-25 ft.
Sidewalk Width:	N/A ft.	N/A ft.

Setting: ☐ Urban ☐ Suburban ☒ Rural
 Topography: ☒ Level ☐ Rolling ☐ Hilly

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BRIDGES AND/OR SMALL STRUCTURE(S):

If the proposed action includes multiple structures, complete and duplicate for each bridge and/or small structure. Include both existing and proposed bridge(s) and/or small structure(s) in this section.

Structure/NBI Number(s): N/A Sufficiency Rating: N/A
(Rating, Source of Information)

	Existing	Proposed
Bridge/Structure Type:		
Number of Spans:		
Weight Restrictions:	ton	ton
Height Restrictions:	ft.	ft.
Curb to Curb Width:	ft.	ft.
Outside to Outside Width:	ft.	ft.
Shoulder Width:	ft.	ft.

Describe impacts and work involving bridge(s), culvert(s), pipe(s), and small structure(s). Provide details for small structure(s): structure number, type, size (length and dia.), location and impacts to water. Use a table if the number of small structures becomes large. If the table exceeds a complete page, put it in the appendix and summarize the information below with a citation to the table.

A total of six structures are located within the construction limits, in the form of 5 driveway culverts and 1 pipe inlet that did not have an outlet within the project area. All 6 pipes are stormwater conveyance with a diameter of 36 inches or less and are not listed in Indiana Total Asset Management System (ITAMS). 4 driveway culverts were located along SR 234 and are 15 feet, 20 feet, 105 feet, and 112 feet long and 1 driveway culvert was located along SR 9 and is 50 feet long. All driveway culverts were 10-inch to 12-inch in diameter and four of the culverts were plastic pipes and one was corrugated metal. All five driveway culverts will be replaced during construction and the pipe inlet will become a culvert with an inlet and outlet within the project area. A table containing the location, existing structure dimensions and description, and work type are presented in Appendix C, page C-56. No bridges are located within the project area.

No impacts will occur to any other pipe, bridge, or small structure for the project.

MAINTENANCE OF TRAFFIC (MOT) DURING CONSTRUCTION:

	Yes	No
Is a temporary bridge proposed?		X
Is a temporary roadway proposed?		X
Will the project involve the use of a detour or require a ramp closure? (describe below)	X	
Provisions will be made for access by local traffic and so posted.	X	
Provisions will be made for through-traffic dependent businesses.	X	
Provisions will be made to accommodate any local special events or festivals.	X	
Will the proposed MOT substantially change the environmental consequences of the action?		X
Is there substantial controversy associated with the proposed method for MOT?		X
Will the project require a sidewalk, curb ramp, and/or bicycle lane closure? (describe below)		X
Provisions will be made for access by pedestrians and/or bicyclist and so posted (describe below).		

Discuss closures, detours, and/or facilities (if any) that will be provided for maintenance of traffic. Any known impacts from these temporary measures should be quantified to the extent possible, particularly with respect to properties such as Section 4(f) resources and wetlands. Discuss any pedestrian/bicycle closures. Any local concerns about access and traffic flow should be detailed as well.

The MOT for the project will require the closure and detour of SR 234. The detour will close SR 234 at the intersections of SR 234 and US 36 to the west of the project area and SR 234 and SR 109 to the east of the project area. Traffic will be rerouted to go north/south on SR 109 until reaching SR 38 where the traveler will go west/east along SR 38, and then go southwest/northeast along US 36 until reaching the SR 234 and U.S. Highway (US) 36 intersection. The added travel distance as a result of the detour is approximately 23 miles. The detour is expected to be in place for no more than 90 days. SR 9 will remain open for the duration of construction using lane width reductions and phased lane shifts. During the Phase 1 of construction, traffic along SR 9 will maintain a

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two-way, one lane configuration using the existing northbound pavement and a temporary traffic signal. During Phase 2 of construction, traffic will maintain a two-way, one lane configuration but be shifted to the newly constructed southbound pavement. During Phase 3 of construction, the roundabout will be open to traffic on SR 234 and SR 9 with an oversized vehicle restriction that will require a detour as the final construction activities commence. Oversized vehicles will be rerouted to go east/west on I-70 until reaching SR 109 where vehicles will travel northwest/southeast until reaching SR 38. From SR 38, oversized vehicles will travel west/east along SR 38, and then go southwest/northeast along US 36 until reaching the SR 9 and US 39 intersection. The added travel distance for oversized vehicles as a result of the detour is approximately 29 miles. The detour is expected to be in place for no more than 90 days. Traffic may need daytime restrictions controlled by a temporary flagger to finish the final construction activities. Local detours may be available. Residences and businesses along the detoured road will have maintained access to their property. Please refer to Appendix B for MOT details within the plans (Appendix B, pages B-22 to B-32).

The closures/lane restrictions will pose a temporary inconvenience to traveling motorists (including school buses and emergency services); however, no significant delays are anticipated, and all inconveniences and delays will cease upon project completion.

ESTIMATED PROJECT COST AND SCHEDULE:

Engineering: \$ 358,100 (2020) Right-of-Way: \$ 25,000 (2025) Construction: \$ 2,333,357.98 (2026)Anticipated Start Date of Construction: May 2026

RIGHT OF WAY:

Land Use Impacts	Amount (acres)	
	Permanent	Temporary
Residential	0.36	0.05
Commercial	1.09	0.14
Agricultural	0.44	0.05
Forest	0	0
Wetlands	0	0
Other:	0	0
Other:	0	0
TOTAL	1.89	0.24

Describe both Permanent and Temporary right-of-way and describe their current use. Typical and Maximum right-of-way widths (existing and proposed) should also be discussed. Any advance acquisition, reacquisition or easements, either known or suspected, and their impacts on the environmental analysis should be discussed.

The existing right-of-way (ROW) on SR 9 and SR 234 is 15 feet from the center of the existing alignment on each side. The proposed permanent ROW will typically be 35 feet wide, with a maximum of 65 feet. The proposed temporary right-of-way will typically be 15 feet with a maximum of 25 feet wide. The reported ROW widths are measured as the entire width of the roadway ROW.

The project requires approximately 1.89 acres of permanent ROW around the existing intersection. This includes approximately 0.36 acre of permanent ROW from five residential properties, approximately 1.09 acres of permanent ROW from four commercial properties, and approximately 0.44 acre of permanent ROW from four agricultural properties. The project also requires approximately 0.24 acre of temporary ROW for the project to tie in the existing driveway and road entrances. This includes approximately 0.05 acre from 4 residential properties, approximately 0.05 acre from 2 agricultural properties, and approximately 0.14 acre from 3 commercial properties along SR 9 and SR 234.

If the scope of work or permanent or temporary right-of-way amounts change, the INDOT Environmental Services Division (ESD) and the INDOT District Environmental Section will be contacted immediately.

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Part III – Identification and Evaluation of Impacts of the Proposed Action

SECTION A - EARLY COORDINATION:

List the date(s) coordination was sent and all resource agencies that were contacted as a part of the development of this Environmental Study. Also, include the date of their response or indicate that no response was received.

Early coordination letters were sent on March 14th and 15th, 2023, and December 11, 2024 (Appendix C, pages C-1 to C-2).

Agency	Date Sent	Date Response Received	Appendix
Indiana Geological & Water Survey (IGWS)	March 15, 2023	Automated Response received on March 15, 2023	Appendix C, pages C-6 to C-8
Indiana Department of Environmental Management (IDEM) – Institutional Controls	September 24, 2024	September 24, 2024	Appendix C, pages C-9 to C-10
INDOT – Strategic Communications Director	March 14, 2023	No Response Received	N/A
INDOT – Utilities and Roadway Director	March 14, 2023	No Response Received	N/A
United States Fish and Wildlife Agency (USFWS) – Bloomington Field Office	March 14, 2023	May 12, 2023	Appendix C, pages C-16 to C-18
Federal Highway Administration – Environmental Program Manager	March 14, 2023	No Response Received	N/A
Indiana Department of Natural Resources (IDNR) Division of Fish and Wildlife (DFW) – Environmental Unit Supervisor	March 14, 2023	April 13, 2024	Appendix C, pages C-11 to C-13
US Department of Housing and Urban Development – Regional Environmental Officer	March 14, 2023	No Response Received	N/A
IDNR Division of Oil and Gas – Orphan Well Manager	March 14, 2023	March 15, 2024	Appendix C, page C-53
US Army Corps of Engineers (USACE), Louisville District – Indianapolis Regulatory Office	March 14, 2023	No Response Received	N/A
Indianapolis Metropolitan Planning Organization (IMPO) – Executive Director	March 14, 2023	No Response Received	N/A
IMPO – Director of Transportation	March 14, 2023	No Response Received	N/A
National Park Service	March 14, 2023	March 28, 2023	Appendix C, pages C-54 to C-55
Hancock County Highway Superintendent	March 14, 2023	No Response Received	N/A
Hancock County Engineer	March 14, 2023	No Response Received	N/A
Hancock County Surveyor	March 14, 2023	No Response Received	N/A
Hancock County Commissioners Administrator	March 14, 2023	No Response Received	N/A
Hancock County Board Commissioner of Green township	March 14, 2023	No Response Received	N/A
Hancock County Councilor, At Large	March 14, 2023	No Response Received	N/A
Hancock County Councilor, District 1	March 14, 2023	No Response Received	N/A
Hancock County 911 Director	March 14, 2023	March 15, 2024	Appendix C, page C-51
City of Greenfield Fire Department	March 14, 2023	No Response Received	N/A
Greenfield Police Department – Division Chief of EMS	March 14, 2023	No Response Received	N/A
Greenfield Police Department - Chief	March 14, 2023	No Response Received	N/A

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Hancock County Sheriff's Department	March 14, 2023	No Response Received	N/A
Town of Fortville- Chief of Police	March 14, 2023	No Response Received	N/A
Greenfield-Central Community Schools - Superintendent	March 14, 2023	March 22, 2023	Appendix C, pages C-43 to C-44
Greenfield-Central Community Schools – Transportation Director	March 14, 2023	March 22, 2023	Appendix C, pages C-43 to C-44
Eden Elementary School - Principal	March 14, 2023	No Response Received	N/A
Hancock County Health Department – Health Officer	March 15, 2023	No Response Received	N/A
Green Township Fire Department	March 15, 2023	March 22, 2023	Appendix C, pages C-41 to C-42
State Soil Scientist, NRCS	December 11, 2024	December 30, 2024	Appendix C, pages C-14 to C-15

All applicable recommendations are included in the Environmental Commitments section of this Categorical Exclusion (CE) document.

SECTION B – ECOLOGICAL RESOURCES:

Streams, Rivers, Watercourses & Other Jurisdictional Features

Federal Wild and Scenic Rivers
State Natural, Scenic or Recreational Rivers
Nationwide Rivers Inventory (NRI) listed
Outstanding Rivers List for Indiana
Navigable Waterways

Presence

Impacts

Yes No

Total stream(s) in project area: _____ Linear feet Total impacted stream(s): _____ Linear feet

Describe all streams, rivers, watercourses and other jurisdictional features adjacent or within the project area. Include whether or not impacts (both permanent and temporary) will occur to the features identified. Include if the streams or rivers are listed on any federal or state lists for Indiana. Include if features are likely subject to federal or state jurisdiction. Discuss measures to avoid, minimize, and mitigate if impacts will occur.

Based on the desktop review, the aerial map (Appendix B, page B-2) of the project area, and the RFI report (Appendix E, pages E-1 to E-11) there are eight rivers, watercourses, or other jurisdictional features within the 0.5-mile search radius. There are no streams, rivers, watercourses, or other jurisdictional features within or adjacent to the project area, which was confirmed by the site visit on April 20, 2023 by CMT. Since the time of the waters report, the project termini have expanded. The expanded areas were all located adjacent to the existing project area and were surveyed at the same time as the existing area and no streams, rivers, watercourse, or other jurisdiction features were found. Therefore, no impacts are expected.

A Waters of the U.S. Determination was approved by INDOT Ecology, Waterway Permitting, and Stormwater Office (EWPSO) on August 10, 2023. Please refer to Appendix F, page F-1 for the Waters of the U.S Determination. It was determined that there are no Waters of the U.S. within the project area.

Open Water Feature(s)

Reservoirs
Lakes
Farm Ponds
Retention/Detention Basin
Storm Water Management Facilities
Other: _____

Presence

Impacts

Yes No

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Describe all open water feature(s) identified adjacent or within the project area. Include whether or not impacts (both permanent and temporary) will occur to the features identified. Include if features are likely subject to federal or state jurisdiction. Discuss measures to avoid, minimize, and mitigate if impacts will occur.

Based on the desktop review, the aerial map (Appendix B, page B-2) of the project area, and the RFI report (Appendix E, pages E-1 to E-11), there are three open water features within the 0.5-mile search radius. There are no open water feature(s) within or adjacent to the project area, which was confirmed by the site visit on April 20, 2023 by CMT. Since the time of the waters report, the project termini have expanded. The expanded areas were all located adjacent to the existing project area and were surveyed at the same time as the existing area and no open water features were found. Therefore, no impacts are expected.

A Waters of the U.S. Determination was approved by INDOT EWPSO on August 10, 2023. Please refer to Appendix F, page F-1 for the Waters of the U.S. Determination. It was determined that there are no Waters of the U.S. within the project area.

Wetlands

Presence

☐

Impacts

Yes

No

☐
☐

Total wetland area: _____ Acre(s)

Total wetland area impacted: _____ Acre(s)

(If a determination has not been made for non-isolated/isolated wetlands, fill in the total wetland area impacted above.)

Wetlands (Mark all that apply)

Documentation

ESD Approval Dates

Wetland Determination

X

August 10, 2023

Wetland Delineation

USACE Isolated Waters Determination

Improvements that will not result in any wetland impacts are not practicable because such avoidance would result in (Mark all that apply and explain):

Substantial adverse impacts to adjacent homes, business or other improved properties;

Substantially increased project costs;

Unique engineering, traffic, maintenance, or safety problems;

Substantial adverse social, economic, or environmental impacts, or

The project not meeting the identified needs.

Describe all wetlands identified adjacent or within the project area. Include whether or not impacts (both permanent and temporary) will occur to the features identified. Include if features are likely subject to federal or state jurisdiction. Discuss measures to avoid, minimize, and mitigate if impacts will occur.

Based on the desktop review, the aerial map of the project area (Appendix B, page B-2), and the RFI report (Appendix E, pages E-1 to E-11), there are six wetlands within the 0.5-mile search radius. There are no wetlands within or adjacent to the project area, which was confirmed by the site visit on April 20, 2023 by CMT. Since the time of the waters report, the project termini have expanded. The expanded areas were all located adjacent to the existing project area and were surveyed at the same time as the existing area and no wetlands were found. Therefore, no impacts are expected.

A Waters of the U.S. Determination was approved by INDOT EWPSO on August 10, 2023. Please refer to Appendix F, page F-1 for the Waters of the U.S. Determination. It was determined that there are no Waters of the U.S. within the project area.

Terrestrial Habitat

Presence

☒

Impacts

Yes

NO

☒
☐

Total terrestrial habitat in project area: 0.77 Acre

Total tree clearing: 0 Acre(s)

Describe types of terrestrial habitat (i.e. forested, grassland, farmland, lawn, etc) adjacent or within the project area. Include whether or not impacts will occur to habitat identified. Include total terrestrial habitat impacted and total tree clearing that will occur. Discuss measure to avoid, minimize, and mitigate if impacts will occur.

This is page 11 of 26 Project name: SR 9 and SR 234 Intersection Improvement Date: October 14, 2025

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Based on a desktop review, a site visit on April 20, 2023 by CMT, and the aerial map of the project area (Appendix B, page B-2), there is mowed grass/lawn, agricultural fields, and scattered trees within the project area. Mowed grass/lawn and agricultural fields are the only vegetative community types in the project area and the only ground covers expected to be directly impacted. Scattered trees throughout the project area will not be impacted. Approximately 0.73 acre of mowed grass/lawn containing turf grasses will be directly impacted for the intersection improvements. Approximately 0.04 acre of agricultural fields containing hay pastures will be directly impacted for the intersection improvements. A total of 0.77 acres of terrestrial habitat will be directly impacted for the intersection improvements. The dominant tree species within the project area is white pine. No tree removal or trimming will occur as a result of the project. Avoidance alternatives are not practicable because they would not allow the project to be constructed and the safety issues would not get addressed, and therefore would not fulfill the project's purpose and need.

USFWS responded on May 12, 2023, with recommendations to avoid or minimize impacts to fish, wildlife, and wildlife habitats, including streams (Appendix C, pages C-16 to C-18). IDNR-DFW responded on April 13, 2023, with recommendations to avoid, minimize, or compensate for impacts to fish, wildlife, and botanical resources (Appendix C, pages C-11 to C-13). All applicable recommendations are included in the Environmental Commitments section of this CE document.

Protected Species

Federally Listed Bats

Information for Planning and Consultation (IPaC) determination key completed
 Section 7 informal consultation completed (IPaC cannot be completed)
 Section 7 formal consultation Biological Assessment (BA) required

Yes	No
X	
	X
	X

Determination Received for Listed Bats from USFWS: NE ☐ NLAA ☒ LAA ☐

Other Species not included in IPaC

Additional federal species found in project area (based on IPaC species list)
 State species (not bird) found in project area (based upon consultation with IDNR)

Yes	No
X	
X	

Migratory Birds

Known usage or presence of birds (i.e. nests)
 State bird species based upon coordination with IDNR

Yes	No
	X
	X

Discuss IDNR coordination and species identified. Describe USFWS Section 7 consultation and determination received for Indiana bat and northern long-eared bat impacts. Discuss if other federally listed species were identified. If so, include consultation that has occurred and the determination that was received. Discuss if migratory birds have been observed and any impacts.

Based on a desktop review and the RFI report (Appendix E, pages E-1 to E-11), completed by CMT on September 25, 2024, the IDNR Hancock County Endangered, Threatened and Rare (ETR) Species List has been checked. According to the IDNR-DFW early coordination response letter dated April 13th, 2023 (Appendix C, pages C-11 to C-13), the Natural Heritage Program's Database has been checked. The following freshwater mussel species have been documented in Sugar Creek and within 0.5 miles of the project area:

- Clubshell (*Pleurobema clava*), State endangered
- Slippershell (*Alasmodonta viridis*), State special concern
- Purple Lilliput (*Toxolasma lividus*), State special concern
- Wavyrayed Lampmussel (*Lampsilis fasciola*), State special concern
- Kidneyshell (*Ptychobranhus fasciolaris*), State special concern
- Little Spectaclecase (*Villosa lienosa*), State special concern

IDNR-DFW stated that since no in-stream work is proposed, no impacts to the above-mentioned mussels should occur. In addition, IDNR-DFW responded with recommendations to avoid, minimize, or compensate for impacts to fish, wildlife, and botanical resources (Appendix C, pages C-11 to C-13). All applicable recommendations are included in the Environmental Commitments section of this CE document. An INDOT 0.5-mile bat review occurred on March 2, 2023, which did not indicate the presence of the Indiana bat or the northern long-eared bat in or within 0.5 mile of the project area.

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Project information was submitted through the USFWS's Information for Planning and Consultation (IPaC) portal, and an official species list was generated (Appendix C, pages C-19 to C-27). The project is within range of the federally endangered Indiana bat (*Myotis sodalis*). Other species were generated in the IPaC species list along with the Indiana bat. Refer to paragraph below.

The official species list generated from IPaC indicated three (3) other species present within the project area. The project is also within range of the endangered snuffbox mussel (*Epioblasma triquetra*), the candidate species monarch butterfly (*Danaus Plexippus*) and a non-essential experimental population of whooping crane (*Grus americana*). The monarch butterfly and whooping crane are not yet listed and there are no Section 7 requirements for these species. Suitable habitat for the snuffbox mussel is found in small to medium-sized creeks, inhabiting areas with a swift current, although it is also found in Lake Erie and some larger rivers. No streams, lakes, or rivers are located within or adjacent to the project. Based on coordination with the INDOT Greenfield District Office, it was determined the snuffbox mussel would not be impacted by the project due to the lack of surface water and suitable habitat, and a "No Effect" determination was reached on May 15, 2025. No further coordination is needed with USFWS.

The project qualifies for the *Programmatic Biological Opinion for Transportation Projects within the Range of the Indiana Bat, Northern Long-eared Bat, and Tricolored Bat*, dated December 2024, between FHWA, Federal Railroad Administration (FRA), Federal Transit Administration (FTA), and USFWS. All culverts within the project area are under 36 inches in diameter and did not require bat inspections at the time of the site visit. An effect determination key was completed on May 16, 2025 and based on the responses provided, the project was found to "May Affect, Not Likely to Adversely Affect" (NLAA) the Indiana bat (Appendix C, pages C-30 to C-39). INDOT reviewed and verified the effect finding on May 19, 2025, and requested USFWS's review of the findings. No response was received from USFWS within the 14-day review period; therefore, it was concluded they concur with the finding. Avoidance and Minimization Measures (AMMs) (*General AMM 1, Lighting AMM 1*) are included as firm commitments in the Environmental Commitments section of this document

This precludes the need for further consultation on this project as required under Section 7 of the Endangered Species Act, as amended. If new information on endangered species at the site becomes available, or if project plans are changed, USFWS will be contacted for consultation.

Geological and Mineral Resources

- Project located within the Indiana Karst Region
- Karst features identified within or adjacent to the project area
- Oil/gas or exploration/abandoned wells identified in the project area

Yes

No

X
X
X

Date Karst Evaluation reviewed by INDOT EWPSO (if applicable): N/A

Discuss if project is located in the Indiana Karst Region and if any karst features have been identified in the project area (from RFI). Discuss response received from IGWS coordination. Discuss if any mines, oil/gas, or exploration/abandoned wells were identified and if impacts will occur. Include discussion of karst study/report was completed and results. (Karst investigation must comply with the current Protection of Karst Features during Planning and Construction guidance and coordinated and reviewed by INDOT EWPSO)

Based on a desktop review and the Indiana Karst Region map, the project is located outside the designated Indiana Karst Region as outlined in the most current *Protection of Karst Features during Project Development and Construction*. According to the topo map of the project area (Appendix B, page B-3) and the RFI report (Appendix E, pages E-1 to E-11), there are no karst features identified within or adjacent to the project area. In the early coordination response on March 15, 2023, the IGWS did not indicate that karst features exist in the project area (Appendix C, pages C-6 to C-8). IGWS noted high liquefaction potential, the presence of a 100-year floodplain, a high potential to encounter bedrock, sand, and gravel resources, and petroleum exploration wells are documented in the area. The response from IGWS has been communicated with the designer on January 2, 2025. The IDNR Division of Reclamation responded to the early coordinated letter on March 15, 2023 stating there are no known oil and gas related wells within this project area. No impacts are expected.

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SECTION C – OTHER RESOURCES

Drinking Water Resources

Wellhead Protection Area(s)
Source Water Protection Area(s)
Water Well(s)
Urbanized Area Boundary
Public Water System(s)

Presence

X
X

Impacts

Yes	No
	X
	X

Is the project located in the St. Joseph Sole Source Aquifer (SSA):

If Yes, is the FHWA/EPA SSA MOU Applicable?

If Yes, is a Groundwater Assessment Required?

Yes	No
	X

Check the appropriate boxes and discuss each topic below. Provide details about impacts and summarize resource-specific coordination responses and any mitigation commitments. Reference responses in the Appendix.

The project is located in Hancock County, which is not located within the area of the St. Joseph Sole Source Aquifer, the only legally designated sole source aquifer in the state of Indiana. Therefore, the FHWA/EPA/INDOT Sole Source Aquifer Memorandum of Understanding (MOU) is not applicable to this project, a detailed groundwater assessment is not needed, and no impacts are expected.

The Indiana Department of Environmental Management's Wellhead Proximity Determinator website (<http://www.in.gov/idem/cleanwater/pages/wellhead/>) was accessed on December 9, 2024 by CMT. This project is not located within a Wellhead Protection Area or Source Water Area. No impacts are expected.

The Indiana Department of Natural Resources Water Well Record Database website (<https://www.in.gov/dnr/water/3595.htm>) was accessed on December 9, 2024 by CMT. Seven unconsolidated wells are located adjacent to the project area. Each quadrant of the SR 9 and 234 intersection contains at least one of the wells. IDEM Institutional Controls Group (IDEM IC) responded to a coordination request on September 27, 2024, confirming they did not have any concerns with the proposed work as it pertains to the limited groundwater use restriction provided that the proper handling, removal, and disposal of potentially contaminated soil and groundwater is conducted in accordance with applicable laws. IDEM IC also indicated a potable well is present on the southeast property of the SR 9 and SR 234 intersection (Appendix C, pages C-9 to C-10). The features will not be affected because they are all located outside of the construction limits. Therefore, no impacts are expected. Should it be determined during the right-of-way phase that these wells will be affected, a cost to cure will likely be included in the appraisal to restore the wells.

Based on a desktop review of the INDOT Municipal Separate Storm Sewer Systems (MS4) website (<https://entapps.indot.in.gov/MS4/>) by CMT on December 9, 2024, this project is located in an Urban Area Boundary (UAB) An early coordination letter was sent on March 14, 2023, to the Hancock County MS4 Coordinator. The MS4 coordinator did not respond.

Based on a desktop review, a site visit on April 20, 2024, by CMT, the aerial map (Appendix B, page B-2), and IDEM's public water supply system website ([IDEM: Water Quality in Indiana: Water Systems](#)) of the project area, no public water systems were identified. Therefore, no impacts are expected.

Floodplains

Project located within a regulated floodplain
Longitudinal encroachment
Transverse encroachment
Homes located in floodplain within 1000' up/downstream from project

Presence

Impacts

Yes	No

If applicable, indicate the Floodplain Level?

Level 1 ☐

Level 2 ☐

Level 3 ☐

Level 4 ☐

Level 5 ☐

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Use the IDNR Floodway Information Portal to help determine potential impacts. Include floodplain map in appendix. Discuss impacts according to the classification system. If encroachment on a flood plain will occur, coordinate with the Local Flood Plain Administrator during design to insure consistency with the local flood plain planning.

The Indiana Department of Natural Resources Indiana Floodway Information Portal website (<http://dnrmaps.dnr.in.gov/appsphp/fdms/>) was accessed on December 9, 2024 by CMT. This project is not located in a regulatory floodplain as determined from approved IDNR floodplain maps (Appendix F, page F-11). Therefore, it does not fall within the guidelines for the implementation of 23 CFR 650, 23 CFR 771, and 44 CFR. No impacts are expected.

FarmlandAgricultural Lands
Prime Farmland (per NRCS)**Presence**

X
X

Impacts

Yes	No
X	
X	

Total Points (from Section VII of CPA-106/AD-1006*) 137

*If 160 or greater, see CE Manual for guidance.

Discuss existing farmland resources in the project area, impacts that will occur to farmland, and mitigation and minimization measures considered.

Based on a desktop review, a site visit on April 20, 2023 by CMT and the aerial map of the project area (Appendix B, page B-2), the project will convert 0.12 acre of farmland as defined by the Farmland Protection Policy Act. An early coordination letter was sent on December 11, 2024, to Natural Resources Conservation Service (NRCS). Coordination with NRCS resulted in a score of 137 on the AD 1006 Form (Appendix C, pages C-14 and C-15). The total farmland to be converted stated on the initial form was based on construction limits that have since been updated to decrease impacts to farmland. NRCS's threshold score for significant impacts to farmland that result in the consideration of alternatives is 160. Since this project score is less than the threshold, no significant loss of prime, unique, statewide, or local important farmland will result from this project. No alternatives other than those previously discussed in this document will be investigated without reevaluating impacts to prime farmland.

SECTION D – CULTURAL RESOURCES

Minor Projects PA

Category(ies) and Type(s)

INDOT Approval Date(s)

N/A

☒**Full 106 Effect Finding**No Historic Properties Affected ☐No Adverse Effect ☒Adverse Effect ☐**Eligible and/or Listed Resources Present**NRHP Building/Site/District(s) ☒Archaeology ☐NRHP Bridge(s) ☐**Documentation Prepared** (mark all that apply)APE, Eligibility and Effect Determination
800.11 Documentation
Historic Properties Report or Short Report
Archaeological Records Check and Assessment
Archaeological Phase Ia Survey Report
Archaeological Phase Ic Survey Report
Other: Effects Report

X
X
X
X
X
X

ESD Approval Date(s)

September 5, 2025
September 5, 2025
October 22, 2024
April 14, 2025
April 14, 2025
July 3, 2025

SHPO Approval Date(s)

October 6, 2025
October 6, 2025
November 21, 2024
May 21, 2025
May 21, 2025
July 29, 2025

Memorandum of Agreement (MOA)

☐**MOA Signature Dates** (List all signatories)

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If the project falls under the MPPA, describe the category(ies) that the project falls under and any approval dates. If the project requires full Section 106, use the headings provided. The completion of the Section 106 process requires that a Legal Notice be published in local newspapers. Please indicate the publication date, name of the paper(s) and the comment period deadline. Include any further Section 106 work which must be completed at a later date, such as mitigation from a MOA or avoidance commitments.

Area of Potential Effect (APE): The Area of Potential Effects (APE) for above-ground resources includes properties adjacent to the undertaking and those with the potential for visual, auditory, or atmospheric impacts. For archaeological resources, the APE is defined as any existing, proposed, and temporary right-of-way that will be acquired and/or impacted by this undertaking and any surveyed areas surrounding it. The APE of the project includes all properties that extended 0.25 mile (1,320 feet) from the project limits in all directions. Historians then adjusted that boundary based on potential for visual, auditory, or atmospheric impacts. In a letter dated November 21, 2024, the Indiana State Historic Preservation Officer (SHPO) stated that the APE "appears to be of adequate size to encompass the geographic area in which direct and indirect effects of a project of this nature could occur" (Appendix D, pages D-53 to D-54). See Appendix D, pages D-20 and D-21 for maps of the APE.

Coordination with Consulting Parties: On April 3, 2023, an early coordination letter was distributed to the organizations, individuals, or Tribes listed below, inviting them to be consulting parties in the section 106 process (Appendix D, pages D-30 to D-32). Two additional organizations were invited to be consulting parties on October 22, 2024 (Appendix D, pages D-41 to D-44). The Indiana SHPO is an automatic consulting party. The FHWA is the federal agency undertaking the project with INDOT acting on behalf of the FHWA. Those identified in bold print are participating consulting parties.

Organization	Date Sent	Response Date	Appendix pages
Indiana SHPO (automatic consulting party)	April 3, 2023	May 3, 2023	Appendix D, page D-38
Eastern Shawnee Tribe of Oklahoma	April 3, 2023	May 16, 2023	Appendix D, page D-40
Miami Tribe of Oklahoma	April 3, 2023	April 17, 2023	Appendix D, page D-38
Peoria Tribe of Indians of Oklahoma	April 3, 2023	N/A	N/A
Pokagon Band of Potawatomi Indians	April 3, 2023	N/A	N/A
Shawnee Tribe	April 3, 2023	May 9, 2023	Appendix D, page D-39
Hancock County Commissioners	April 3, 2023	N/A	N/A
Hancock County Engineer	April 3, 2023	N/A	N/A
Hancock County Highway Clerk	April 3, 2023	N/A	N/A
Hancock County Highway Superintendent	April 3, 2023	N/A	N/A
Indianapolis Metropolitan Planning Organization (IMPO)	April 3, 2023	N/A	N/A
Indiana Landmarks – Central Office	April 3, 2023	April 4, 2023	Appendix D, page D-35
Hancock County Historian	April 3, 2023	April 3, 2023	Appendix D, page D-33
Hancock County Historical Society	April 3, 2023	N/A	N/A
Dauss Architects*	October 22, 2024	October 23, 2024	Appendix D, page D-49
Irving Materials, Inc.*	October 22, 2024	N/A	N/A

*Invited to join consultation upon availability of the Historic Property Report (HPR)

Archaeology: An Indiana Archaeological Short Report (ASR) was prepared by qualified professionals from Weintraut and Associates (W&A) on April 14, 2025 (Arnold, 2025) (Appendix D, pages D-98 to D-100). The report documented that no archaeological resources listed in, or eligible for listing in, the National Register of Historic Places (NRHP) are identified within the APE. No further archaeological work is proposed, and archaeological clearance was recommended. Consulting parties were notified of availability of the ASR on April 17, 2025, and April 18, 2025 (Appendix D, pages D-55 to D-58).

The Hancock County Historian responded to the findings of the ASR in an email dated April 17, 2025, and indicated that they have concerns with the Historic barn (Alford-Moore Farmstead, not eligible) at the site (Appendix D, page D-61). W&A responded to the Hancock County Historian in an email dated April 22, 2025, stating the comment was added to the project record (Appendix D, page D-61). The INDOT project manager responded to the Hancock County Historian in an email dated June 11, 2025 and explained that the barn or the fence around the barn will not be impacted and that the speed limit is being reduced to avoid the barn. The INDOT project manager then asked if the Hancock County Historian had any other issues with the project (Appendix D, pages D-71 and D-72). The Hancock County Historian responded on the same day and noted that they have no issues (Appendix D, pages D-71).

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Indiana Landmarks responded to the findings of the ASR in an email dated April 18, 2025, expressing concerns about the Irving Materials building and asked about an upcoming Effects Report (Appendix D, page D-65). W&A responded to Indiana Landmarks in an email dated April 22, 2025 and confirmed an effects report would be forthcoming (Appendix D, page D-64).

The Turtle Mountain Band Tribe of Chippewa Indians responded to the findings of the ASR in an email dated April 22, 2025, stating: "Defer to the nearest THPO office to the APE" (Appendix D, pages D-66 and D-67).

In a letter dated May 21, 2025, the Indiana SHPO concurred with the conclusions stated in the ASR (Appendix D, pages D-68 and D-69).

The Eastern Shawnee THPO responded to the findings of the ASR in a letter dated June 5, 2025, and stated the "project proposes NO Adverse Effect or endangerment to known sites of interest to the Eastern Shawnee Tribe." The THPO also stated that "should this project inadvertently discover an archeological site or object(s) we request that you immediately contact the Eastern Shawnee Tribe, as well as the appropriate state agencies (within 24 hours). We also ask that all ground disturbing activity stop until the Tribe and State agencies are consulted. Please note that any future changes to this project will require additional consultation" (Appendix D, page D-70).

Historic Properties: Qualified professional historians for W&A conducted a site visit of the project area on May 17, 2023. One resource was found to be eligible for listing in the NRHP.

The Irving Materials Inc., Office Building, located at 8032 SR 9, is a modern one-story building mainly composed of concrete, aluminum, and glass and was constructed in three phases beginning in 1962. The building was designed by architect James O. Johnson and won the First Honor Award from the Indiana Society of Architects. In addition, two sculpted bas relief walls were completed by artist Robert Youngman. This property is eligible under Criterion A for its association with the Greenfield-founded Irving Materials, Inc. and for its association with the period of postwar growth and expansion in the concrete building industry. It is also eligible under Criterion C as an example of a Modern/Late Modern building associated with architect James O. Johnson and for its association with thin shell concrete/hyperbolic paraboloid construction in the United States. The historic property boundary is the building parcel and includes the office building, sculpture, surrounding lawn and rear parking. The period of significance is 1962-1979 and accounts for the significant construction episodes (Appendix D, page D-5).

A Historic Property Report (HPR) was prepared by qualified professionals from W&A on October 22, 2024 (Natali, 2024) (Appendix D, pages D-96 to D-97). The HPR documented the Irving Materials, Inc. Office Building is eligible for listing in the NRHP under Criterion C. The HPR was sent to consulting parties on October 22, 2024, and two more consulting parties were invited to join consultation with this notification: Irving Materials, Inc., and Dauss Architects (Appendix D, pages D-41 to D-44).

In an email dated October 23, 2024, Dauss Architects accepted the invitation to be a consulting party and noted the report is accurate concerning Jim Johnson (Appendix D, page D-49).

On October 30, 2024, the Miami Tribe of Oklahoma THPO responded to the findings of the HPR, stating that they have no objection to the project, and that they are not currently aware of existing documentation directly linking a specific Miami cultural or historic site to this project site (Appendix D, page D-50).

Indiana Landmarks responded to the findings of the HPR in an email dated November 12, 2024, stating that they are interested in the Irving Materials, Inc. Office Building and its NR eligibility and hope the project does not endanger it (Appendix D, page D-51).

The Eastern Shawnee THPO responded to the findings of the HPR via a letter dated November 19, 2024, and noted, "we find our people occupied these areas historically and/or prehistorically," but that the current "project proposes NO Adverse Effect or endangerment to known sites of interest to the Eastern Shawnee Tribe." (Appendix D, page D-52).

In a letter dated November 21, 2024, the Indiana SHPO concurred with the conclusions of the HPR that the Irving Materials, Inc. Office Building is eligible for the NRHP under Criteria A and C, and that the Alford-Moore Farmstead is ineligible for the NRHP but concurred with the "Notable" rating in the HPR evaluation. The Indiana SHPO asked for clarification of impacts on the Irving Materials, Inc. Office Building in the following submission (Appendix D, pages D-53 to D-54).

Effects Report: An Effects Report was prepared by qualified professionals from W&A on June 24, 2025 (Natali, 2025) (Appendix D, pages D-101 to D-103). The report documented the preliminary Section 106 effect finding of "No Adverse Effect". The Effects Report was sent to consulting parties on July 3, 2025 (Appendix D, pages D-77 and D-80).

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In an email dated July 4, 2025, the Hancock County Historian responded to the Effects Report by asking if the Alford-Moore Farmstead and barn will be demolished for the project (Appendix D, page D-81). W&A responded to the Hancock County Historian in an email dated July 8, 2025 by indicating the Alford-Moore Farmstead and barn will not be demolished and that the surrounding fence will not be impacted by the project. It was explained that temporary right-of-way would be needed for reconstruction of the gravel drive immediately east of the barn and fence (Appendix D, page D-81). The Hancock County Historian responded on the same day, expressing gratitude for the response and offered no further comments (Appendix D, page D-81).

In an email dated July 7, 2025, Indiana Landmarks responded to the Effects Report by asking for clarification on the amount of right-of-way needed for the project (Appendix D, page D-83). W&A responded to Indiana Landmarks in an email dated July 8, 2025, clarifying the amount of right-of-way needed (Appendix D, pages D-82 and D-83). In a letter dated July 14, 2025, Indiana Landmarks concurred with the findings of the Effects Report (Appendix D, page D-84).

The Turtle Mountain Band Tribe of Chippewa Indians responded to the Effects Report in an email dated July 28, 2025, stating: "Defer to the nearest THPO office" near the APE (Appendix D, page D-87).

In a letter dated July 29, 2025, the Indiana SHPO concurred with the recommendations of the Effects Report (Appendix D, pages D-88 and D-89).

On August 14, 2025, the Miami Tribe of Oklahoma THPO responded to the Effects Report, stating: "The Miami Tribe offers no objection to the above-referenced project at this time, as we are not currently aware of existing documentation directly linking a specific Miami cultural or historic site to this project site. However, given the Miami Tribe's deep and enduring relationship to its historic lands and cultural property within present-day Indiana if any human remains or Native American cultural items falling under the Native American Graves Protection and Repatriation Act (NAGPRA) or archaeological evidence is discovered during any phase of this project, the Miami Tribe requests immediate consultation with the entity of jurisdiction for the location of discovery" (Appendix D, page D-90).

The Shawnee Tribal Historic Preservation Specialist replied to the Effects Report via email on August 20, 2025, and stated: "We have no issues or concerns at this time . . . but in the event archaeological materials are encountered during construction, use, or maintenance of this location, please renotify us at that time as we would like to resume immediate consultation under such a circumstance" (Appendix D, page D-91).

800.11 Documentation Finding: The 800.11 documented the Section 106 effect finding is "No Adverse Effect". The 800.11 documentation was signed by INDOT Cultural Resources Office (CRO), on behalf of FHWA, on September 5, 2025 (Appendix D, page D-6). The 800.11 documentation was sent to consulting parties, including the Indiana SHPO, on September 5, 2025 (Appendix D, pages D-104 to D-107). In a letter dated October 6, 2025, the Indiana SHPO concurred with the Section 106 finding of "No Adverse Effect" (Appendix D, pages D-108 and D-109).

Public Involvement: To meet the public involvement requirements of Section 106, a legal notice of FHWA's finding of "No Adverse Effect" was published in the *Daily Reporter* on September 10, 2025 offering the public an opportunity to submit comments pursuant to 36 CFR 800.2(d), 800.3(e), and 800.6(a)(4). The public comment period closed 30 days later on October 11, 2025. The text of the public notice and the affidavit of publication appear in Appendix D, pages D-110 to D-112. One comment was received during the comment period. In an email dated September 25, 2025, the representative of the current owner of the Irving Materials, Inc. Office Building property indicated that the property owner was unaware of the "No Adverse Effect" documentation until a representative from the Irving Materials, Inc. property notified them on September 5, 2025. The property owner requested a 30-day extension of the comment period to allow for a reasonable response, which has been granted by INDOT (Appendix D, page D-113). No other public comments were received. This CE document will be updated once the 30-day comment period extension has closed on November 10, 2025.

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SECTION E – SECTION 4(f) RESOURCES/ SECTION 6(f) RESOURCES

	<u>Presence</u>	<u>Use</u>	
		Yes	No
Parks and Other Recreational Land			
Publicly owned park	<input type="text"/>	<input type="text"/>	<input type="text"/>
Publicly owned recreation area	<input type="text"/>	<input type="text"/>	<input type="text"/>
Other (school, state/national forest, bikeway, etc.)	<input type="text"/>	<input type="text"/>	<input type="text"/>
Wildlife and Waterfowl Refuges			
National Wildlife Refuge	<input type="text"/>	<input type="text"/>	<input type="text"/>
National Natural Landmark	<input type="text"/>	<input type="text"/>	<input type="text"/>
State Wildlife Area	<input type="text"/>	<input type="text"/>	<input type="text"/>
State Nature Preserve	<input type="text"/>	<input type="text"/>	<input type="text"/>
Historic Properties			
Site eligible and/or listed on the NRHP	X	<input type="text"/>	X
<u>Evaluations</u>			
	<u>Prepared</u>		
Programmatic Section 4(f)	<input type="text"/>		
"De minimis" Impact	<input type="text"/>		
Individual Section 4(f)	<input type="text"/>		
Any exception included in 23 CFR 774.13	<input type="text"/>		

Discuss Programmatic Section 4(f) and "de minimis" Section 4(f) impacts in the discussion below. Individual Section 4(f) documentation must be included in the appendix and summarized below. Discuss proposed alternatives that satisfy the requirements of Section 4(f). FHWA has identified various exceptions to the requirement for Section 4(f) approval. Refer to 23 CFR § 774.13 - Exceptions.

Section 4(f) of the U.S. Department of Transportation Act of 1966 prohibits the use of certain public and historic lands for federally funded transportation facilities unless there is no feasible and prudent alternative. The law applies to significant publicly owned parks, recreation areas, wildlife / waterfowl refuges, and NRHP eligible or listed historic properties regardless of ownership. Lands subject to this law are considered Section 4(f) resources.

Based on a desktop review, the aerial map of the project area (Appendix B, page B-2), and the RFI report (Appendix E, pages E-1 to E-11), there is one potential Section 4(f) resource located within the 0.5-mile search radius. According to additional research, and the 800.11 documentation (Appendix D, pages D-5 to page D-18), there is one potential Section 4(f) resource within or adjacent to the project area.

The Irving Materials Company, Inc. Office Building is recommended eligible for the NRHP due to its association with the Greenfield-founded business on site, the post-war growth and expansion in the concrete building industry, Modern/Late Modern building, and thin shell concrete/hyperbolic paraboloid construction in the United States. Therefore, it is considered a Section 4(f) resource. The project will impact the Irving Materials Company, Inc. Office Building property through the acquisition of 0.38 acre of permanent right-of-way, construction of a compact roundabout, and addition of light fixtures within its property boundary. Right-of-way acquisition will amount to approximately 10.6 percent of the current historic property, along the south and east portions of the historic property boundary. Within that right-of-way acquisition, approximately 0.01 acre, or approximately 0.3 percent of the current historic property, will become part of the roundabout. In addition, approximately 0.02 acre of temporary right-of-way will be required for drive reconstruction where the two drives meet SR 9 and SR 234 (Appendix D, page D-12). These project activities will not disrupt the ability of the property to convey its significance under Criteria A or C of the NRHP. Historians recommend this project would have "No Adverse Effect" on the Irving Materials Company, Inc. Office Building and recommend an overall project finding of "No Adverse Effect." The project will use this resource by taking permanent right-of-way from its property boundary but will not use the resource in such a way that the protected activities, features, or attributes that qualify the resource for protection under Section 4(f) are substantially impaired. Therefore, no Section 4(f) use is expected. In a letter dated October 6, 2025, the Indiana SHPO concurred with the "No Adverse Effect" Section 106 effect finding (Appendix D, pages D-108 to D-109). Therefore, no section 4(f) evaluation is required.

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Section 6(f) Involvement

Presence

Use

Yes

No

Section 6(f) Property

☐
☐
☐

Discuss Section 6(f) resources present or not present. Discuss if any conversion would occur as a result of this project. If conversion will occur, discuss the conversion approval.

The U.S. Land and Water Conservation Fund Act of 1965 established the Land and Water Conservation Fund (LWCF), which was created to preserve, develop, and assure accessibility to outdoor recreation resources. Section 6(f) of this Act prohibits conversion of lands purchased with LWCF monies to a non-recreation use.

A review of Section 6(f) properties on the INDOT ESD website revealed a total of three properties in Hancock County (Appendix I, page I-1). None of these properties are located within or adjacent to the project area. Therefore, there will be no impacts to Section 6(f) resources.

SECTION F – Air Quality

STIP/TIP and Conformity Status of the Project

Is the project in the most current STIP/TIP?

Is the project located in an MPO Area?

Is the project in an air quality non-attainment or maintenance area?

If Yes, then:

Is the project in the most current MPO TIP?

Is the project exempt from conformity?

If No, then:

Is the project in the Transportation Plan (TP)?

Is a hot spot analysis required (CO/PM)?

Yes

No

X
X
X

X
X

Location in STIP:

Name of MPO (if applicable):

Location in TIP (if applicable):

Level of MSAT Analysis required?

Level 1a

☒

Level 1b

☐

Level 2

☐

Level 3

☐

Level 4

☐

Level 5

☐

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Indianapolis Metropolitan Planning Organization

N/A - Project not located within the Metropolitan Planning Area of the IMPO

Describe if the project is listed in the STIP and if it is in a TIP. Describe the attainment status of the county(ies) where the project is located. Indicate whether the project is exempt from a conformity determination. If the project is not exempt, include information about the TP and TIP. Describe if a hot spot analysis is required and the MSAT Level.

This project is included in the Fiscal Year (FY) 2026-2030 Statewide Transportation Improvement Program (STIP) (Appendix H, page H-1).

This project is located in Hancock County, which is currently a maintenance area for Ozone, under the 1997 Ozone 8-hour standard which was revoked in 2015 but is being evaluated for conformity due to the February 16, 2018, South Coast Air Quality Management District V. Environmental Protection Agency, Et. Al. Decision. The project's design concept and scope is accurately reflected in the STIP. Therefore, the conformity requirements of 40 CFR 93 have been met.

This project is of a type qualifying as a categorical exclusion (Group 1) under 23 CFR 771.117(c), or exempt under the Clean Air Act conformity rule under 40 CFR 93.126, and as such, a Mobile Source Air Toxics analysis is not required.

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SECTION G - NOISE

Noise**Yes****No**Is a noise analysis required in accordance with FHWA regulations and INDOT's traffic noise policy? ☐ ☒

Date Noise Analysis was approved/technically sufficient by INDOT ESD: _____

Describe if the project is a Type I or Type III project. If it is a Type I project, describe the studies completed to date and if noise impacts were identified. If noise impacts were identified, describe if abatement is feasible and reasonable and include a statement of likelihood.

This project is a Type III project. In accordance with 23 CFR 772 and the current *Indiana Department of Transportation Traffic Noise Analysis Procedure*, this action does not require a formal noise analysis.

SECTION H – COMMUNITY IMPACTS

Regional, Community & Neighborhood Factors

Will the proposed action comply with the local/regional development patterns for the area?

Yes	No
X	
	X
	X
	X
X	
X	

Will the proposed action result in substantial impacts to community cohesion?

Will the proposed action result in substantial impacts to local tax base or property values?

Will construction activities impact community events (festivals, fairs, etc.)?

Does the community have an approved transition plan?

If No, are steps being made to advance the community's transition plan?

Does the project comply with the transition plan? (explain in the discussion below)

Discuss how the project complies with the area's local/regional development patterns; whether the project will impact community cohesion; and impact community events. Discuss how the project conforms with the ADA Transition Plan.

Land use in the area of the project is primarily rural with commercial and residential buildings scattered. No changes in land use and no displacements or changes in access to businesses or residences are anticipated as a result of the project. The existing tax base, property value, and economic development potential are not expected to be affected by the project.

The 2023 Plan Hancock County Comprehensive Plan (available at [Future Hancock County Comprehensive Plan 2023](#)) indicates in order to keep up with development of the region, the county needs to invest in a robust transportation system and work with state, regional, and local partners to have a safe, efficient network of roads within the county to improve both freight and passenger mobility. The 2023 Hancock County Throughfare Plan (available at [hancockin.gov/DocumentCenter/View/688/Future-Hancock-Throughfare-Plan-Adopted-1323](#)) indicates the need to improve corridors in the county and specifically mentions the capacity issues SR 9 faces and will continue to face with the more intensive land uses planned for the corridor. Although the project is not specifically mentioned either plan, it conforms to both plans as the purpose of the project is to reduce or eliminate the queue at the existing intersection and therefore reduce the number of overall crashes by improving the safety and mobility of the intersection, which will allow for more efficient travel through the intersection.

The 2024 Americans with Disabilities Act (ADA) Transition Plan (available at [https://www.hancockin.gov/DocumentCenter/View/1283/2024-Hancock-County-ADA-Transition-Plan](#)) describes the town's commitment to meeting or exceeding the requirements of the ADA. The Transition Plan recommends upgrades to the county's buildings, curb ramps and sidewalks, transit facilities, and pedestrian bridges and walkways, and states that any upgrades meet Proposed Accessibility Guidelines for Pedestrian Facilities in the Public Right-of-Way (PROWAG), ADA Title II, and Section 504 requirements. Sidewalk replacements or upgrades will be prioritized based on proximity to schools, downtown, parks, and trails. ADA ramp upgrades will be prioritized based on schools, downtown, the parks, and businesses. Building upgrades will be prioritized based on building accessibility and room accessibility. Since the proposed project does not include plans for pedestrian facilities, the project complies with the Transition Plan.

The MOT for the project will involve the implementation of a detour on SR 234 while SR 9 remains open using lane width restrictions and lane shifts. Refer to the MOT section of this document and plan sheets (Appendix B, pages B-22 to B-32) for details. The detours will pose a temporary inconvenience to travelling motorists, including school buses and emergency services; however, no significant delays are anticipated, and all inconveniences will cease upon project completion.

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Construction activities will not impact community events. According to the Hancock Economic Development Council website, many annual events, including the Riley Festival, Hancock County 4-H Fair, and Hancock Flat 50 in October, June, and August, respectively, occur throughout the year in Downtown Greenfield, Indiana, approximately 5 miles south of the project. Other various community events occur in the surrounding area throughout the year. Any impacts to these events will likely be temporary with minor traffic delays during the construction period. No impacts to community events are expected after construction is completed.

Public Facilities and Services

Discuss what public facilities and services are present in the project area and impacts (such as MOT) that will occur to them. Include how the impacts have been minimized and what coordination has occurred. Some examples of public facilities and services include health facilities, educational facilities, public and private utilities, emergency services, religious institutions, airports, transportation or public pedestrian and bicycle facilities.

Based on a desktop review, the aerial map of the project area (Appendix B, page B-2), and the RFI report (Appendix E, pages E-1 to E-11), there is one religious facility, one cemetery, one school, one recreational facility, one fire station, and two pipelines located within the 0.5 mile of the project. There are four public facilities which includes one school and its associated recreational facility, a fire station and two pipelines, within or adjacent to the project area. That number was confirmed by the site visit on April 20, 2023, by CMT. The Eden Elementary school building is not within or adjacent to the project area, but one entrance to the school is located within the project area. A representative for the Green Township Trustee, the Green Township Board of Trustees, and Greenfield Central Community School Board of Trustees responded to the early coordination letter on March 22, 2023, supporting the project but wanting more information on how it would affect the school and fire station (Appendix C, pages C-41 to C-44). CMT responded on April 13, 2023, explaining the timeline for the project and assuring both school entrances would remain open to the school and that fire station can still operate at a normal capacity during construction (Appendix C, page C-45 to C-50). No impact to the school or its recreational facility is expected and continued coordination with the fire department will occur to minimize any impacts on training, access or daily operations. An early coordination letter was sent to the INDOT Utilities and Roadway Director on March 14, 2023, regarding the pipelines within the project area. No response was received. Access to all properties will be maintained during construction.

Environmental Justice (EJ) (Presidential EO 12898)

During the development of the project were EJ issues identified?

Does the project require an EJ analysis?

If YES, then:

Are any EJ populations located within the project area?

Will the project result in adversely high and disproportionate impacts to EJ populations?

Yes

No

Indicate if EJ issues were identified during project development. If an EJ analysis was not required, discuss why. If an EJ analysis was required, describe how the EJ population was identified. Include if the project has a disproportionately high or adverse effect on EJ populations and explain your reasoning. If yes, describe actions to avoid, minimize and mitigate these effects.

This analysis was performed for this project prior to the issuance of recent federal Executive Orders (EO) from January 2025, including EO 14154, EO 14148, and EO 14173. As such, this analysis is included for transparency but is no longer applicable to the impacts analysis for federal projects and this impact was not considered in the federal decision.

Under FHWA Order 6640.23A, FHWA and the project sponsor, as a recipient of funding from FHWA, are responsible to ensure that their programs, policies, and activities do not have a disproportionately high and adverse effect on minority or low-income populations. Per the current INDOT Categorical Exclusion Manual, an Environmental Justice (EJ) Analysis is required for any project that has two or more relocations or 0.5 acre of additional permanent right-of-way. The project will require 1.83 acres of permanent right-of-way and 0.21 acre of temporary right of way. Therefore, an EJ Analysis is required.

Potential EJ impacts are detected by locating minority and low-income populations relative to a reference population to determine if populations of EJ concern exists and whether there could be disproportionately high and adverse impacts to them. The reference population may be a county, city or town and is called the community of comparison (COC). In this project, the COC is Hancock County, Indiana. The community that overlaps the project area is called the affected community (AC). In this project, the AC is Green Township, Hancock County, Indiana. An AC has a population of concern for EJ if the population is more than 50% minority or low-income or if the low-income or minority population is 125% of the COC. Data from the 2022 census was obtained from the US Census Bureau Website on August 16, 2024 by CMT. The data collected for minority and low-income populations within the AC are summarized in the below table.

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	COC – Hancock County	AC - Green Township, Hancock County, Indiana
Percent Minority	12.3%	8.6%
125% of COC	15.4%	AC < 125% COC
EJ Population of Concern		No
Percent Low-Income	4.1%	15.8%
125% of COC	5.1%	AC > 125% COC
EJ Population of Concern		Yes

AC-1, Green Township, Hancock County, Indiana has a percent minority of 8.6% which is below 50% and is below the 125% COC threshold. Therefore, AC-1 is not a minority population of EJ concern.

AC-1, Green Township, Hancock County, Indiana has a percent low-income of (%) which is below 50% but is above the 125% COC threshold. Therefore, AC-1 is a low-income population of EJ concern.

Conclusion

This project will require 1.83 acres of permanent right-of-way and 0.21 acre of temporary right of way. The ROW needed for this project includes mowed grass, cement/gravel strips lining both sides of each road, and the entrance to several residential driveways and commercial parking lots. No commercial or residential relocations will be required. Access will be maintained to all other businesses, residences, and Eden Elementary School throughout construction. All restrictions will cease upon project completion.

The SR 9 and SR 234 intersection experiences long queues and a high frequency of crashes related to queues. The capacity analysis indicates the existing signalized intersection currently operates at a level of service (LOS) E at the northbound leg of the intersection and would operate at a LOS F by the year 2045. The project is expected to positively impact community cohesion by reducing congestion and improving safety at the intersection. The proposed roundabout is anticipated to maintain an LOS of A through 2045, reduce crashes by nearly 49%, and eliminate queues at the intersection. The use of a single lane compact roundabout will minimize impacts to adjacent property owners and land impacts.

Traffic traveling along SR 9 will not be interrupted, but will be reduced to a two-way, one-lane configuration using the existing northbound pavement. SR 234 will be closed during construction and traffic traveling along SR 234 will be rerouted. The detour will close SR 234 at the intersections of SR 234 and US 36 to the west of the project area and SR 234 and SR 109 to the east of the project area. Traffic will be rerouted to go north/south on SR 109 until reaching SR 38 where the traveler will go west/east along SR 38, and then go southwest/northeast along US 36 until reaching the SR 234 and US 36 intersection for a total of approximately 23 miles of extra travel due to the detour. Both starting points of the detour for SR 234 begin outside of the AC. Local roads would still be available within the AC for the EJ population to travel around the area. North Meridian Road is located approximately 1.83 miles west of the SR 9 and SR 234 intersection and North County Road (CR) 400 East is located approximately 2 miles east of the SR 9 and SR 234 intersection. Both of these roads lead to CR 900 North and would allow traffic within the AC to travel east and west similarly to how they would along SR 234. The total distance from the North Meridian Road and SR 234 intersection to the North CR 400 East and SR 234 intersection is approximately 6 miles. Because of the detour rerouting traffic coming from outside the AC and since the EJ populations will have other travel options within the AC that will not substantially increase travel time, traffic impacts to EJ populations are expected to be minor.

The proposed project will enable residents of the community, including the identified EJ population, to safely travel through the intersection to adjoining communities, schools, and businesses. The identified EJ population will not experience a disproportionately high and adverse impact from the project, and the project does not have an adverse effect on the population. The proposed project will not result in any secondary or cumulative negative impacts or negative environmental impacts including, air, water, vibration, and community character. No Environmental Justice issues were raised during coordination with stakeholders regarding the proposed project. EJ Analysis documentation is attached.

Relocation of People, Businesses or Farms

Will the proposed action result in the relocation of people, businesses or farms?
Is a BIS or CSRS required?

Yes No

	X
	X

Number of relocations: Residences: _____ Businesses: _____ Farms: _____ Other: _____

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Discuss any relocations that will occur due to the project. If a BIS or CSRS is required, discuss the results in the discussion below.

No relocations of people, businesses, or farms will take place as a result of this project.

SECTION I – HAZARDOUS MATERIALS & REGULATED SUBSTANCES

Hazardous Materials & Regulated Substances (Mark all that apply)

Red Flag Investigation (RFI)

Phase I Environmental Site Assessment (Phase I ESA)

Phase II Environmental Site Assessment (Phase II ESA)

Design/Specifications for Remediation required?

Documentation

X

Date RFI concurrence by INDOT SAM (if applicable): September 25, 2024

Include a summary of the potential hazardous material concerns found during review. Discuss in depth sites found within, directly adjacent to, or ones that could impact the project area. Refer to current INDOT SAM guidance. If additional documentation (special provisions, pay quantities, etc.) will be needed, include in discussion. Include applicable commitments.

Based on a review of GIS and available public records, the RFI was completed on September 23, 2024 and INDOT SAM provided their concurrence on September 25, 2024 (Appendix E, Pages E-1 to E-11). One leaking underground storage tank (LUST) site, one institutional control site, one NPDES Facility, and one NPDES Pipe Location are located within 0.5 mile of the project area. The LUST site and institutional control site could affect the project area.

The LUST site, Former Pavey Eden Grocery, 7987 N SR 9, AI ID #33596, is located within the project area at the southeast quadrant of the intersection. On December 16, 2003, three USTs were removed from this site: a 3,000 gallon gasoline tank, a 1,000 gallon diesel tank, and a 1,000 gallon gasoline tank. Following removal activities, an analysis of soil samples indicated the release of petroleum on the site. Analysis of soil samples during an initial site characterization indicated that Total Petroleum Hydrocarbon (TPH) concentrations at all boring locations exceeded the applicable IDEM screening levels. Additional sampling occurred through 2013 in order to delineate the extent of soil and groundwater contamination. It was determined that the contamination extends offsite to the north side of the property, including underneath SR 234. An Environmental Restrictive Covenant (ERC) was recorded for the site on December 17, 2015. Please see the Institutional Controls discussion below for more information regarding the ERC. On March 31, 2016 IDEM issued a No Further Action (NFA) Determination Pursuant to Remediation Closure Guide for the site. Monitoring well abandonment occurred on March 21, 2016. If excavation occurs in this area, it is possible that petroleum contamination will be encountered. Proper handling, removal, and disposal of soil and/or groundwater may be necessary.

The institutional control site, Former Pavey Eden Grocery, 7987 N SR 9, AI ID #33596, is located within the project area at the southeast quadrant of the intersection and will be acquired as part of the project. IDEM issued a No Further Action Approval Determination Pursuant to Remediation Closure Guide on March 31, 2016. Soil and groundwater contamination remain on the site. An ERC was placed on the property on December 17, 2015. The ERC specifically prohibits the use or extraction of groundwater. If excavation occurs in this area, it is possible petroleum contamination be encountered. Proper handling, removal, and disposal of soil and/or groundwater may be necessary. Coordination with IDEM's Institutional Controls Group was sent on September 24, 2024. They responded on September 27, 2024 and stated the group does not have any concerns with the proposed work as it pertains to the limited groundwater use restriction in the ERC for 7989 N SR 9, provided the proper handling, removal, and disposal of potentially contaminated soil and groundwater is conducted in accordance with applicable laws, when and where appropriate.

Further investigation for hazardous material concerns is not required at this time.

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Part IV – Permits and Commitments

PERMITS CHECKLIST

Permits (mark all that apply)

Likely Required**Army Corps of Engineers (404/Section10 Permit)**

Nationwide Permit (NWP)

Regional General Permit (RGP)

Individual Permit (IP)

Other

IN Department of Environmental Management (401/Rule 5)

Nationwide Permit (NWP)

Regional General Permit (RGP)

Individual Permit (IP)

Isolated Wetlands

Rule 5 (Construction Stormwater General Permit (CSGP))

Other

IN Department of Natural Resources

Construction in a Floodway

Navigable Waterway Permit

Other

Mitigation Required**US Coast Guard Section 9 Bridge Permit****Others (Please discuss in the discussion below)**

X

List the permits likely required for the project and summarize why the permits are needed, including permits designated as "Other."

An IDEM Construction Stormwater General Permit (CSGP) will be required since more than one acre of land will be disturbed.

Applicable recommendations provided by resource agencies are included in the Environmental Commitments section of this document. If permits are found to be necessary, the conditions of the permit will be requirements of the project and will supersede these recommendations.

It is the responsibility of the project sponsor to identify and obtain all required permits.

ENVIRONMENTAL COMMITMENTS

List all commitments and include the name of agency/organization requesting/requiring the commitment(s). Listed commitments should be numbered.

Firm:

- 1) If the scope of work or permanent or temporary right-of-way amounts change, the INDOT Environmental Services Division (ESD) and the INDOT District Environmental Section will be contacted immediately. (INDOT ESD/SAM and INDOT District)
- 2) It is the responsibility of the project sponsor to notify school corporations and emergency services at least two weeks prior to any construction that would block or limit access. (INDOT ESD)
- 3) Continued coordination with the Green Township fire department and Greenfield School Corporation will occur to minimize any impacts on training, access or daily operations. (INDOT ESD)
- 4) If excavation occurs at the Former Pavey Eden Grocery, 7987 N SR 9, AI ID #33596, it is possible that petroleum contamination will be encountered. Proper handling, removal, and disposal of soil and/or groundwater may be necessary. (INDOT SAM)

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- 5) If excavation occurs at 7987 N SR 9, AI #33596, proper handling, removal, and disposal of potentially contaminated soil and groundwater is conducted in accordance with applicable laws, when and where appropriate. (IDEM Institutional Controls)
- 6) If the potable well is encountered at 7987 N SR 9, AI #33596, then it will be abandoned in accordance with applicable laws, when and where appropriate. (IDEM Institutional Controls)
- 7) General AMM 1: Ensure all operators, employees, and contractors working in areas of known or presumed bat habitat are aware of all FHWA/FRA/FTA (Transportation Agencies) environmental commitments, including all applicable AMMs. (USFWS)
- 8) Lighting AMM 1: Direct temporary lighting away from suitable habitat during the active season. (USFWS)

For Further Consideration:

- 9) Evaluate wildlife crossings under bridge/culverts projects in appropriate situations. Suitable crossings include flat areas below bridge abutments with suitable ground cover, high water shelves in culverts, amphibian tunnels and diversion fencing. (USFWS)

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Appendix I: Additional Studies/Reports

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2025 Traffic Analysis Memo	I-2
2022 Engineering Assessment Amendment.....	I-25
2019 Engineering Assessment	I-27
Environmental Justice Analysis.....	I-31

SR 9 and SR 234 Intersection Improvement

APPENDIX A: INDOT SUPPORTING DOCUMENTATION



Categorical Exclusion Level Thresholds

	PCE	Level 1	Level 2	Level 3	Level 4 ¹
Section 106	Falls within guidelines of Minor Projects PA	"No Historic Properties Affected"	"No Adverse Effect"	-	"Adverse Effect" Or Historic Bridge involvement ²
Stream Impacts³	No construction in waterways or water bodies	< 300 linear feet of stream impacts	≥ 300 linear feet of stream impacts	-	USACE Individual 404 Permit ⁴
Wetland Impacts³	No adverse impacts to wetlands	< 0.1 acre	-	< 1.0 acre	≥ 1.0 acre
Right-of-way⁵	Property acquisition for preservation only or none	< 0.5 acre	≥ 0.5 acre	-	-
Relocations⁶	None	-	-	< 5	≥ 5
Threatened/Endangered Species (Species Specific Programmatic for Indiana bat & northern long eared bat)*	"No Effect", "Not likely to Adversely Affect" (With select AMMs ⁷)	"Not likely to Adversely Affect" (With any AMMs or commitments)	-	"Likely to Adversely Affect"	Project does not fall under Species Specific Programmatic ⁸
Threatened/Endangered Species (Any other species)*	Falls within guidelines of USFWS 2013 Interim Policy or "No Effect"	"Not likely to Adversely Affect"	-	-	"Likely to Adversely Affect"
Sole Source Aquifer	No Detailed Groundwater Assessment	-	-	-	Detailed Groundwater Assessment
Floodplain	No Substantial Impacts	-	-	-	Substantial Impacts
Section 4(f) Impacts	None	-	-	-	Any ¹⁰
Section 6(f) Impacts	None	-	-	-	Any
Permanent Traffic Alteration	None	-	-	-	Any
Noise Analysis Required	No	-	-	-	Yes
Air Quality Analysis Required	No	-	-	-	Yes ¹¹
Approval Level					
<ul style="list-style-type: none"> District Env. (DE) Env. Serv. Div. (ESD) FHWA 	Concurrence by DE or ESD	DE or ESD	DE or ESD	DE and/or ESD	DE and/or ESD; and FHWA

¹ Coordinate with INDOT Environmental Services Division. INDOT will then coordinate with the appropriate FHWA Environmental Specialist.

² Any involvement with a bridge processed under the Historic Bridge Programmatic Agreement.

³ Total permanent impacts to streams (linear feet) and wetlands (acres).

⁴ US Army Corps of Engineers Individual 404 Permit

⁵ Total permanent and temporary right-of-way. This does not include reacquisition of existing apparent right-of-way.

⁶ If any relocations are within an area with a known or suspected Environmental Justice (EJ) or disadvantaged population, or has greater than 5 relocations, a conversation with FHWA, through INDOT ESD, is needed to confirm NEPA classification and outreach plan for the project.

⁷ Avoidance and Mitigation Measures (AMMs) determined by the IPAC determination key to be required that are not tree AMMs, bridge AMMs, or structure AMMs.

⁸ Projects that do not fall under a Species Specific Programmatic and results in a "Likely to Adversely Affect". Other findings can be processed as a lower-level CE.

¹⁰ Section 4(f) use resulting in an Individual, Programmatic, or *de minimis* evaluation. The only exception is a *de minimis* evaluation for historic properties (Effective January 2, 2020). If a historic property *de minimis* and no other use, mark the *None* column.

¹¹ Hot Spot Analysis and/or MSAT Quantitative Emission Analysis.

* Includes the threatened/endangered species critical habitat

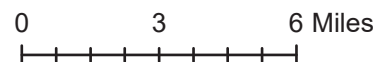
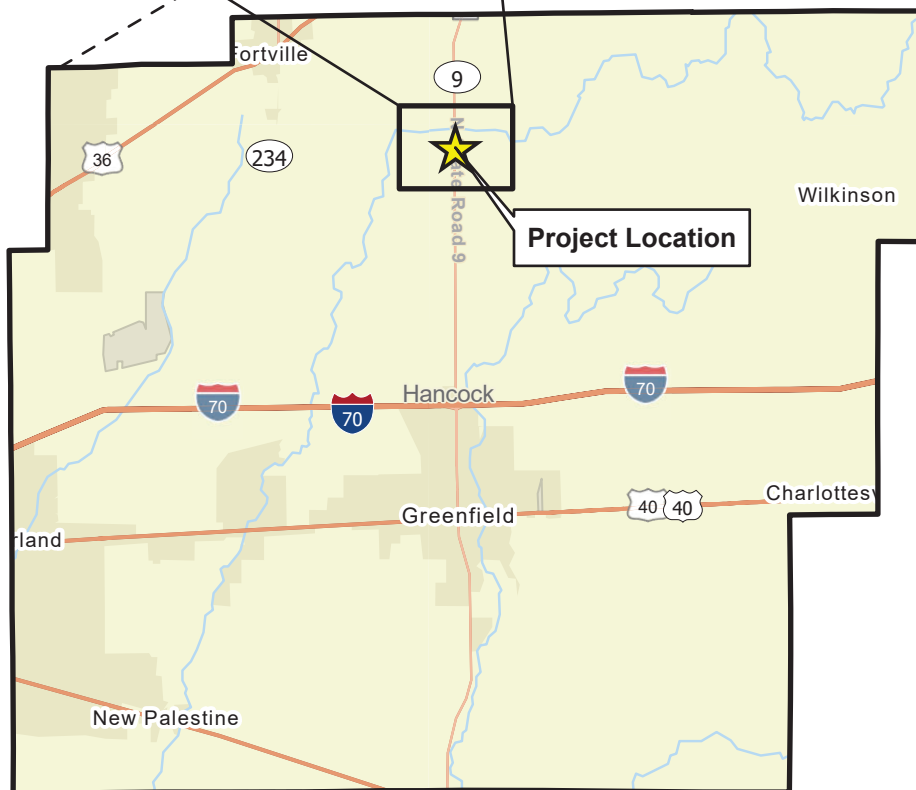
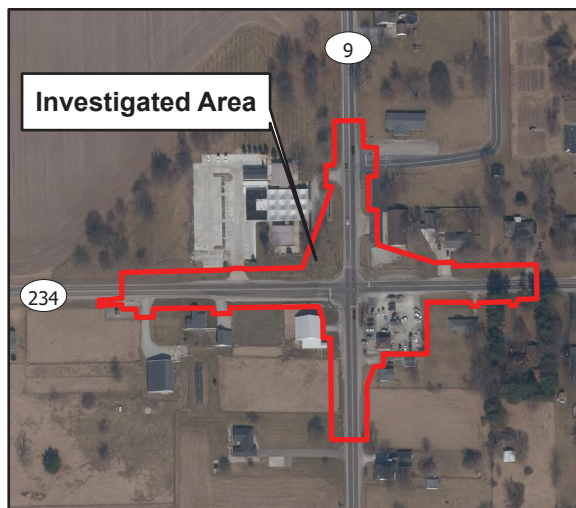
Note: Substantial public or agency controversy may require a higher-level NEPA document.

The Environmental justice threshold is no longer applicable and has been redacted.

SR 9 and SR 234 Intersection Improvement

APPENDIX B: GRAPHICS

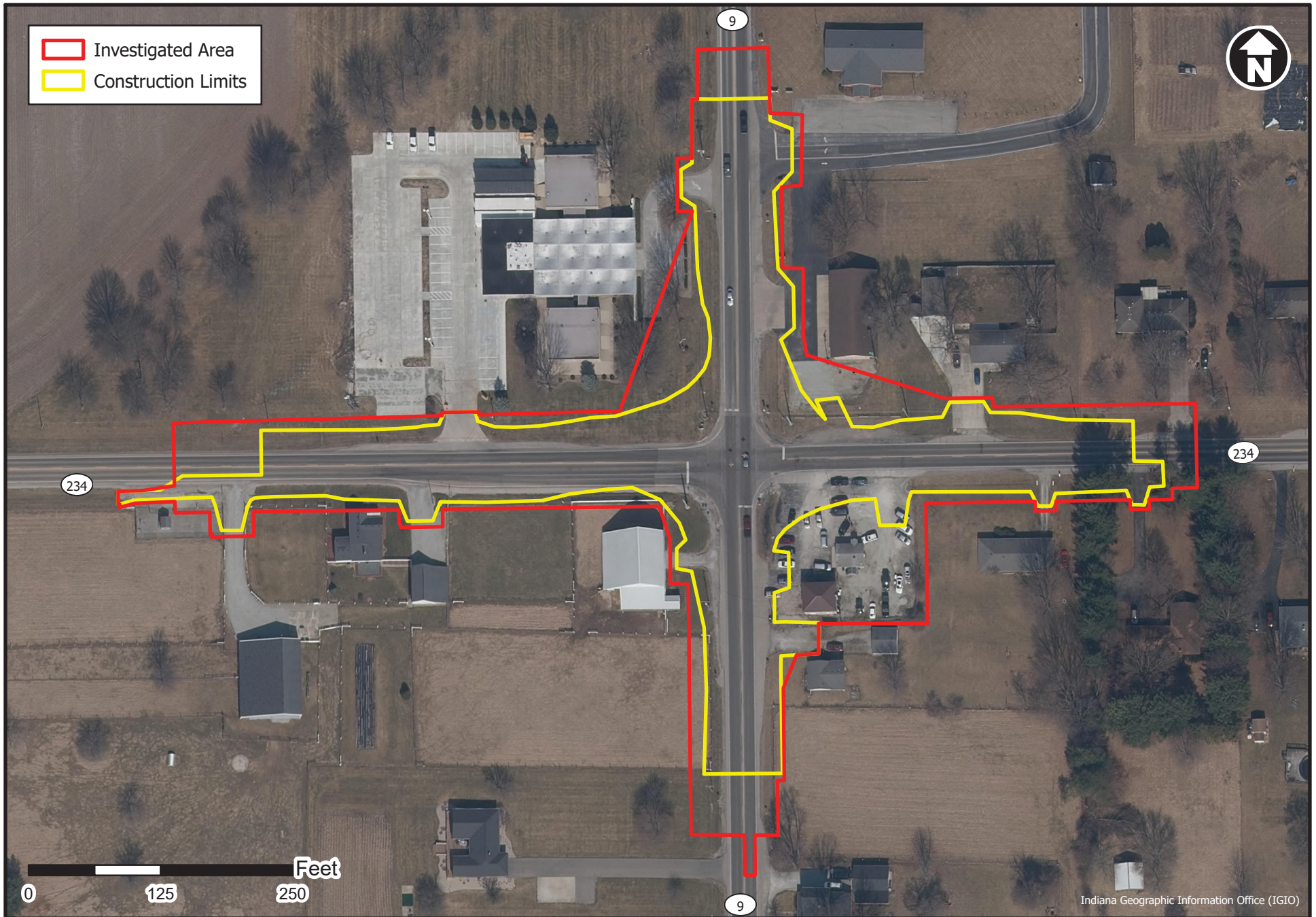




Service Layer Credits: Sources: Esri, TomTom, Garmin, FAO, NOAA, USGS, © OpenStreetMap contributors, and the GIS User Community, Indiana Geographic Information Office (IGIO)

SR 9 and SR 234 Intersection Improvements (Des No. 2000154) Location Map - Hancock County, IN



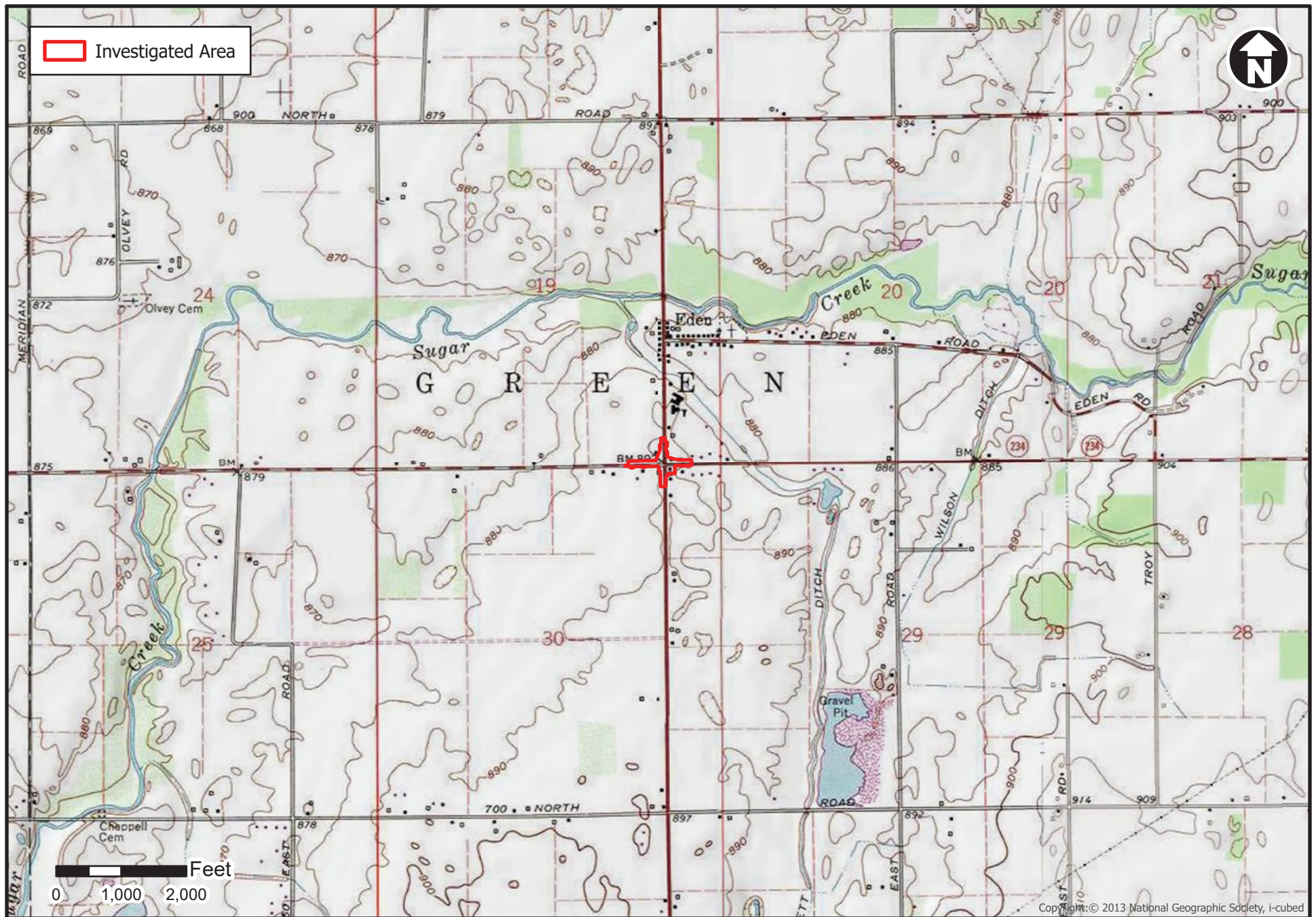


SR 9 and SR 234 Intersection Improvements (Des No. 2000154)

Aerial Map

Author: Alex Tadda Date: 10/10/2025

B-2

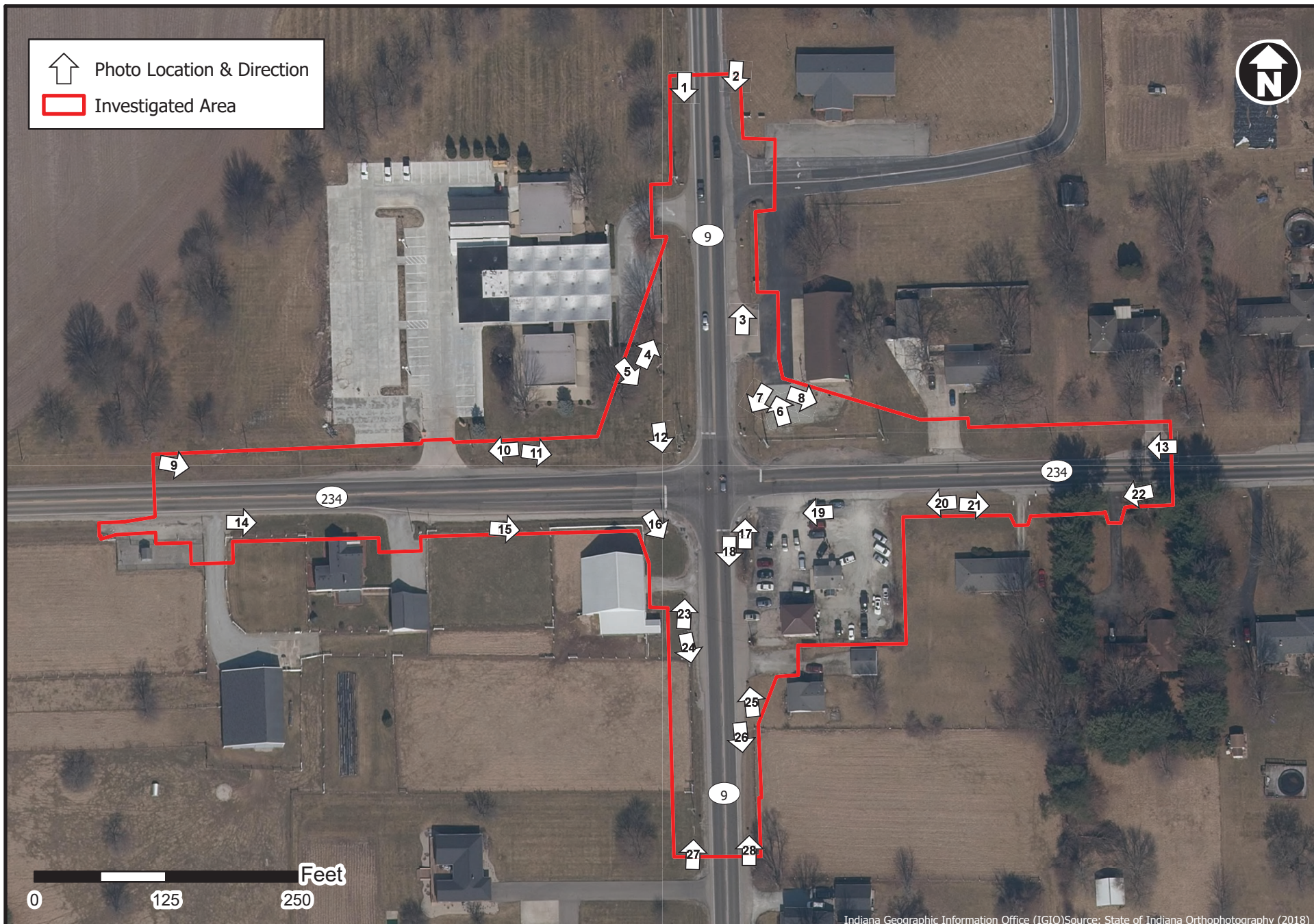


SR 9 and SR 234 Intersection Improvements (Des No. 2000154)

USGS Topographic Map - Ingalls, IN Quadrangle

Author: Alex Tadda Date: 5/8/2025

B-3



SR 9 and SR 234 Intersection Improvement (Des No 2000154) - Hancock County, IN

Overall Photo Key



Crawford, Murphy & Tilly



1. View of mowed grass along the west side of SR 9 at the northern terminus of the project area, looking south. 4/20/2023



3. View of mowed grass along the east side of SR 9, looking north. 4/20/2023



2. View of mowed grass along the east side of SR 9 at the northern terminus of the project area, looking south. 4/20/2023



4. View of mowed grass and a mature tree along the west side of SR 9 north of SR 234 looking northeast. 4/20/2023



5. View of mowed grass at the northwest corner of the SR 9 and SR 234 intersection, looking southeast. 4/20/2023



7. View towards the SR 9 and SR 234 intersection, looking southwest. 4/20/2023



6. View of Green Township Volunteer Fire Department's driveway and parking lot on the east side of SR 9 north of SR 234, looking northwest. 4/20/2023



8. View of Green Township Volunteer Fire Department gravel parking lot north of SR 234, looking southeast. 4/20/2023



9. View of mowed grass along the north side of SR 234 at the western terminus of the project area, looking east.
4/20/2023



11. View of mowed grass along the northside of SR 234 west of SR 9, looking east. 4/20/2023



10. View of mowed grass and commercial driveway along the north side of SR 234 west of SR 9, looking west.
4/20/2023



12. View of mowed grass at the northwest corner of the SR 9 and SR 234 intersection, looking south across SR 234.
4/20/2023



13. View of residential driveway and mowed grass along the north side of SR 234, looking west. 4/20/2023



15. View along the south side of SR 234 west of SR 9, looking east. 4/20/2023



14. View of mowed grass along the south side of SR 234 west of SR 9, looking east. 4/20/2023



16. View of mowed grass at the southwest corner of the SR 9 and SR 234 intersection, looking southeast. 4/20/2023



17. View at the southeast corner of the SR 9 and SR 234 intersection, looking north. 4/20/2023



19. View of paved business parking lot on the south side of SR 234 east of the intersection with SR 9, looking west. 4/20/2023



18. View of mowed grass and commercial property along the east side of SR 9 south of SR 234, looking south. 4/20/2023



20. View of mowed grass on the south side of SR 234, looking west. 4/20/2023



21. View of mowed grass and a residential driveway along the south side of SR 234, looking east. 4/20/2023



23. View of mowed grass and agricultural driveway on the west side of SR 9 south of the intersection with SR 234, looking north. 4/20/2023



22. View of residential driveway, mowed grass, and mature trees along the south side of SR 234 near the eastern terminus of the project area, looking west. 4/20/2023



24. View of mowed grass, gravel slope, and agricultural fencing on the west side of SR 9 south of SR 234, looking south. 4/20/2023



25. View of mowed grass and commercial driveway on the east side of SR 9, looking north. 4/20/2023



27. View of mowed grass along the west side of SR 9 at the southern terminus of the project area, looking north. 4/20/2023



26. View of mowed grass and agricultural fencing on the east side of SR 9 south of SR 234, looking south. 4/20/2023



28. View of mowed grass and agricultural fencing along the east side of SR 9 at the southern terminus of the project area, looking north. 4/20/2023

DESIGNATION
2000154
CONTRACT
R-44131

INDIANA DEPARTMENT
OF TRANSPORTATION



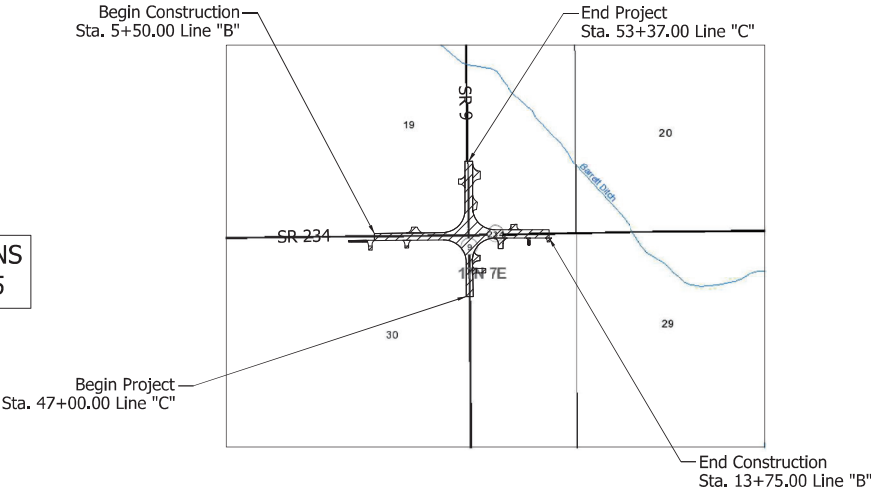
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ROUTE: SR 9 FROM: RP 54+29 TO: RP 54+41

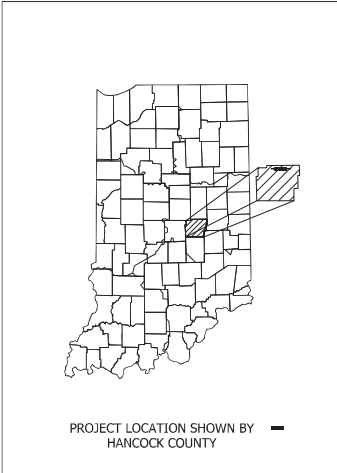
DESIGNATION NO. 2000154

TRAFFIC DATA	SR 9	SR 234
A.A.D.T. (2025)	9,990 V.P.D.	4,390 V.P.D.
A.A.D.T. (2045)	11,200 V.P.D.	5,320 V.P.D.
D.V.L.V. (2045)	950 V.P.H.	575 V.P.H.
DIRECTIONAL DISTRIBUTION	49% / 51%	54% / 46%
TRUCKS	12.51% A.A.D.T.	4.45% A.A.D.T.
	9.89% D.V.L.V.	3.83% D.V.L.V.
DESIGN DATA		
DESIGN SPEED	40 M.P.H.	40 M.P.H.
PROJECT DESIGN CRITERIA	RECONSTRUCTION, NON-FREEWAY	RECONSTRUCTION, NON-FREEWAY
FUNCTIONAL CLASSIFICATION	MINOR ARTERIAL	STATE COLLECTOR
RURAL/URBAN	RURAL	RURAL
TERRAIN	LEVEL	LEVEL
ACCESS CONTROL	NONE	NONE

Intersection Improvement with New Roundabout, at SR 9 and SR 234
Located on SR 9 from 0.06 mile South of SR 234 to 0.06 mile North of SR 234
Sections 19 & 30, T-17-N, R-7-E, Green Township, Hancock County, Indiana.



PUBLIC HEARING PLANS
SEPTEMBER 12, 2025



LATITUDE: 39° 54' 04" N LONGITUDE: 85° 46' 15" W

GROSS LENGTH: 0.12 MI.
NET LENGTH: 0.12 MI.
MAX. GRADE: 5.0 %

INDIANA DEPARTMENT OF TRANSPORTATION
STANDARD SPECIFICATIONS DATED 2026
TO BE USED WITH THESE PLANS



Copyright CMT, Inc.

8750 PURDUE ROAD
INDIANAPOLIS, IN 46268-6128
PHONE: (317) 298-4500

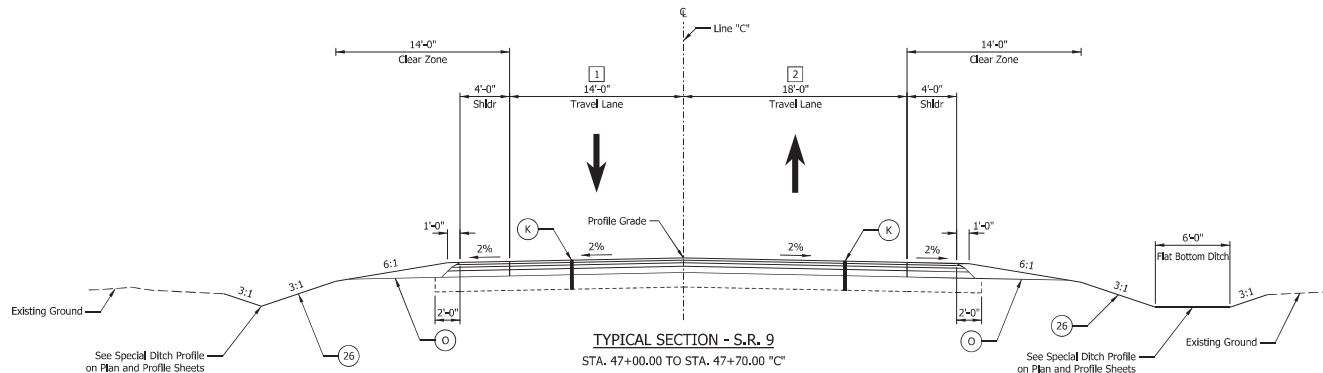
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CERTIFIED BY: _____ DATE _____
RECOMMENDED FOR LETTING: _____ DATE _____

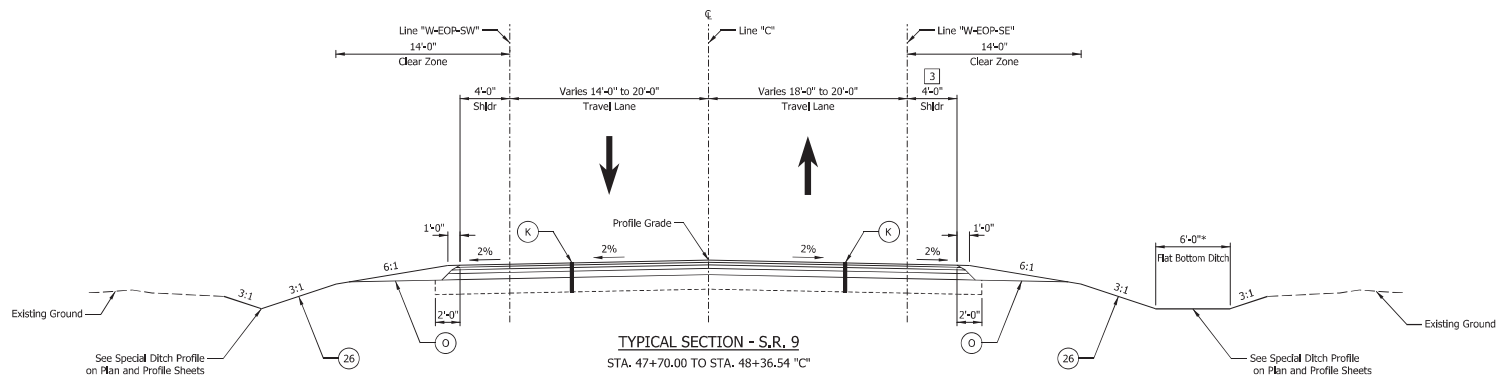
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DESIGNATION
2000154
SHEETS
1 of 129
CONTRACT
R-44131

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- 1 12'-0" from Sta. 47+00.00 to Sta. 47+12.73 "C"
Varies from 12'-0" at Sta. 47+12.73 to 14'-0" at Sta. 47+72.73 "C"
- 2 12'-0" from Sta. 47+00.00 to Sta. 47+21.26 "C"
Varies from 12'-0" at Sta. 47+21.26 to 18'-0" at Sta. 48+11.26 "C"



- 3 Shoulder stops and Curb begins at Sta. 48+08.00
- * Flat Bottom Ditch from Sta. 47+70.00 to Sta. 48+50.00
Special V Ditch from Sta. 48+50.00 to Sta. 48+39.88

Legend:

- 13 Curb, Concrete, Modified
- 15 Curb and Gutter, Concrete
- 16 Curb and Gutter, Concrete, Type B, Modified
- 17 Curb and Gutter, Concrete, Modified
- 22 Center Curb, Concrete, Type D, Modified
- 26 Sodding
- A PCCP, 10 in., Colored, with D-1 Contraction Joints spaced at 15 ft, with 1.25 in. diameter Dowels, on 6 in. Compacted Aggregate, No. 53, on Subgrade Treatment, Type IC, on Blaxial Geogrid Type IB
- K 220 lbs/sys QC/QA+HMA, 3, 58H, Surface, 9.5 mm, on 275 lbs/sys QC/QA+HMA, 3, 58H, Intermediate, 19.0 mm, on 385 lbs/sys QC/QA+HMA, 3, 58S, Base, 25.0 mm, on 660 lbs/sys QC/QA+HMA, 3, 58S, Base, 25.0 mm, on Subgrade Treatment, Type IC, on Blaxial Geogrid Type IB
- M 220 lbs/sys QC/QA+HMA, 3, 58H, Surface, 9.5 mm, on Milling, Asphalt, 2 in.
- O Compacted Aggregate, No. 53
- R PCCP, 4 in., on 16 in. Compacted Aggregate, No. 53, on Subgrade Treatment, Type IC
- R2 660 lbs/sys QC/QA+HMA, 3, 58S, Base, 25.0 mm, on Subgrade Treatment, Type IC, on Blaxial Geogrid Type IB

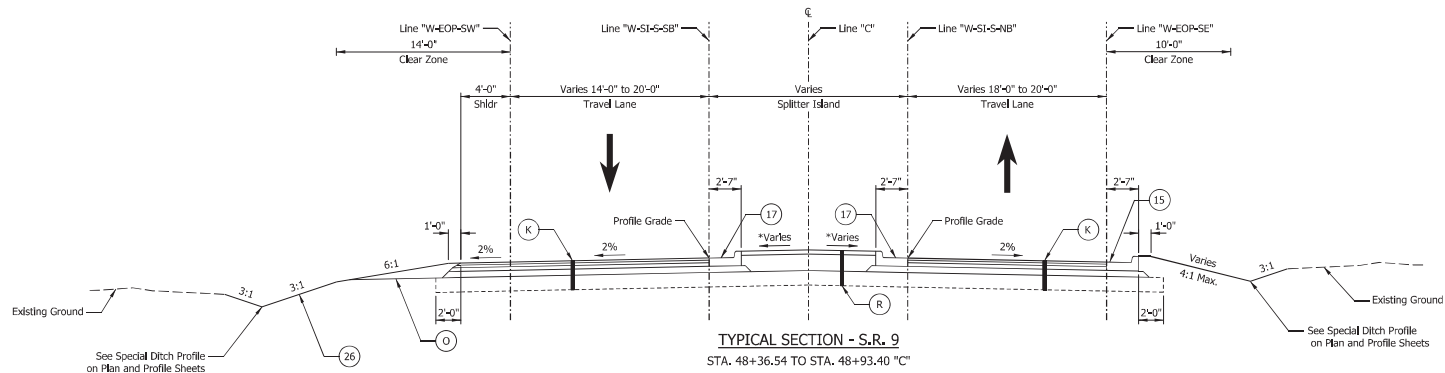
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CHECKED: ELM	8/11/2025	CHECKED: RJK

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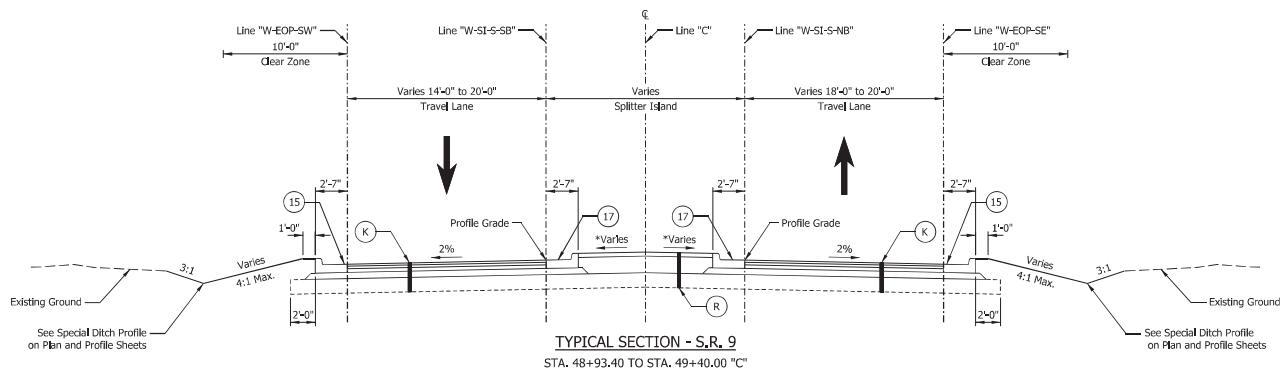
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3	of 129
CONTRACT	
R-44131	



Legend:

- (13) Curb, Concrete, Modified
- (15) Curb and Gutter, Concrete
- (16) Curb and Gutter, Concrete, Type B, Modified
- (17) Curb and Gutter, Concrete, Modified
- (22) Center Curb, Concrete, Type D, Modified
- (26) Sodding
- (A) PCCP, 10 in., Colored, with D-1 Contraction Joints spaced at 15 ft, with 1.25 in. diameter Dowels, on 6 in. Compacted Aggregate, No. 53, on Subgrade Treatment, Type IC, on Blaxial Geogrid Type IB
- (K) 220 lbs/sys QC/QA+HMA, 3, 58H, Surface, 9.5 mm, on 275 lbs/sys QC/QA+HMA, 3, 58H, Intermediate, 19.0 mm, on 385 lbs/sys QC/QA+HMA, 3, 58S, Base, 25.0 mm, on 660 lbs/sys QC/QA+HMA, 3, 58S, Base, 25.0 mm, on Subgrade Treatment, Type IC, on Blaxial Geogrid Type IB
- (M) 220 lbs/sys QC/QA+HMA, 3, 58H, Surface, 9.5 mm, on Milling, Asphalt, 2 in.
- (O) Compacted Aggregate, No. 53
- (R) PCCP, 4 in., on 16 in. Compacted Aggregate, No. 53, on Subgrade Treatment, Type IC
- (R2) 660 lbs/sys QC/QA+HMA, 3, 58S, Base, 25.0 mm, on Subgrade Treatment, Type IC, on Blaxial Geogrid Type IB

* See Spot Elevation Detail sheets for more info



* See Spot Elevation Detail sheets for more info

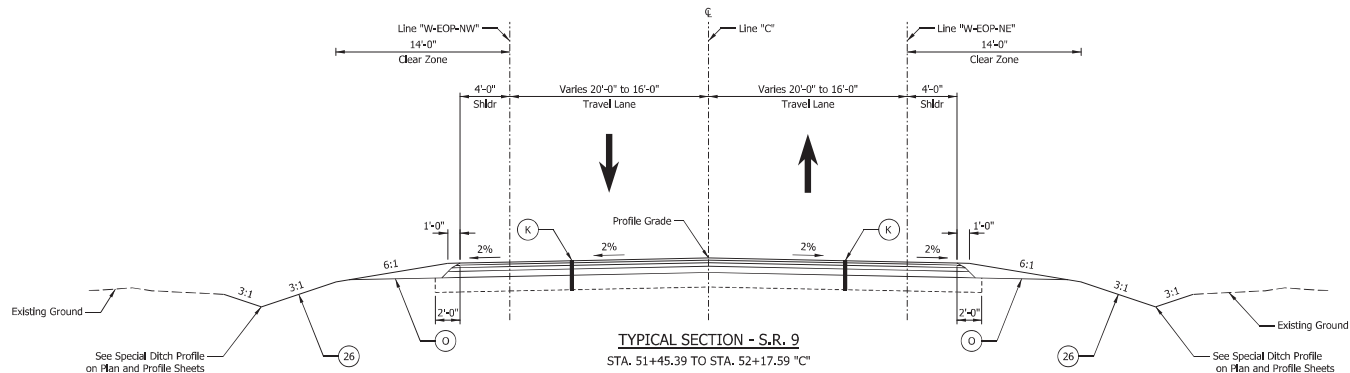
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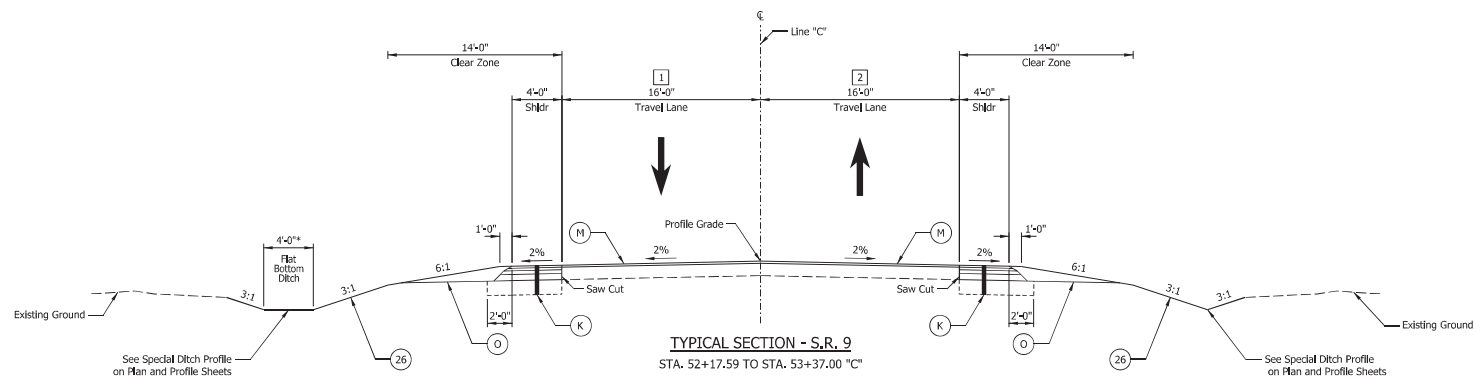
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- (13) Curb, Concrete, Modified
- (15) Curb and Gutter, Concrete
- (16) Curb and Gutter, Concrete, Type B, Modified
- (17) Curb and Gutter, Concrete, Modified
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- (26) Sodding
- (A) PCCP, 10 in., Colored, with D-1 Contraction Joints spaced at 15 ft, with 1.25 in. diameter Dowels, on 6 in. Compacted Aggregate, No. 53, on Subgrade Treatment, Type IC, on Blaxial Geogrid Type IB
- (K) 220 lbs/sys QC/QA+HMA, 3, 58H, Surface, 9.5 mm, on 275 lbs/sys QC/QA+HMA, 3, 58H, Intermediate, 19.0 mm, on 385 lbs/sys QC/QA+HMA, 3, 58S, Base, 25.0 mm, on 660 lbs/sys QC/QA+HMA, 3, 58S, Base, 25.0 mm, on Subgrade Treatment, Type IC, on Blaxial Geogrid Type IB
- (M) 220 lbs/sys QC/QA+HMA, 3, 58H, Surface, 9.5 mm, on Milling, Asphalt, 2 in.
- (O) Compacted Aggregate, No. 53
- (R) PCCP, 4 in., on 16 in. Compacted Aggregate, No. 53, on Subgrade Treatment, Type IC
- (R2) 660 lbs/sys QC/QA+HMA, 3, 58S, Base, 25.0 mm, on Subgrade Treatment, Type IC, on Blaxial Geogrid Type IB



- [1] Varies from 16'-0" at Sta. 52+17.00 to 12'-0" at Sta. 53+37.00 "C"
- [2] Varies from 16'-0" at Sta. 52+22.59 to 12'-0" at Sta. 53+37.00 "C"
- * Special V-Ditch from Sta. 52+17.59 to Sta. 52+87.00 "C"
- * Flat Bottom Ditch from Sta. 52+87.00 to Sta. 53+37.00 "C"

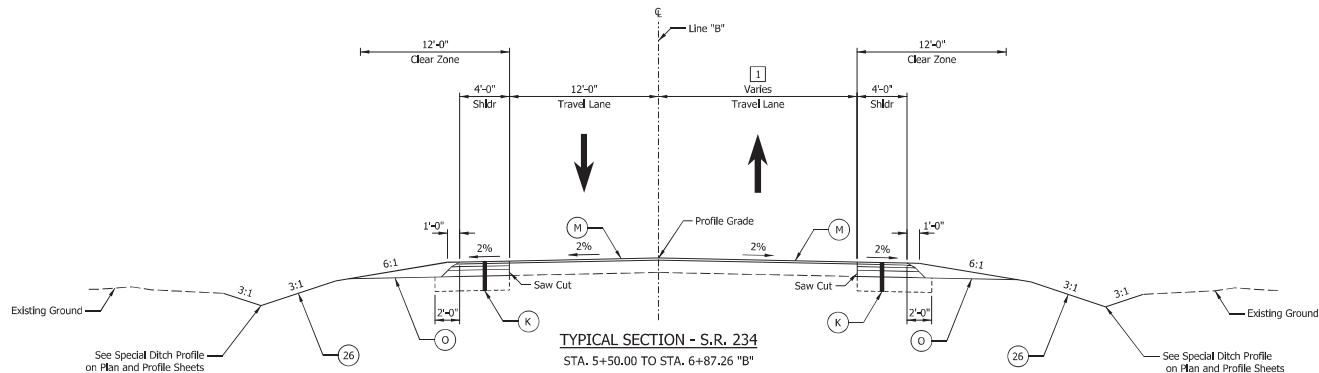
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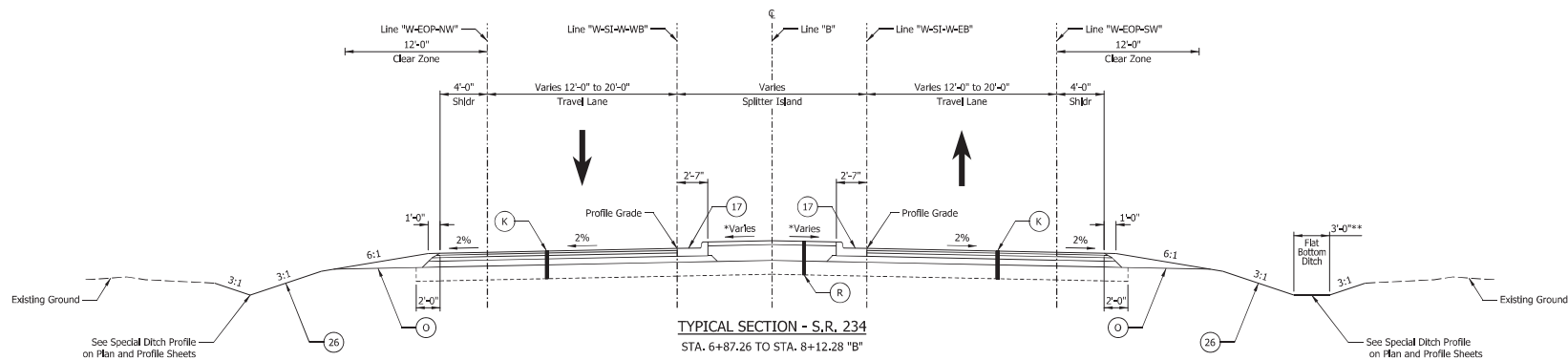
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1" = 5'	2000154
SHEETS	
6	of 129
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Varies from 12'-0" at Sta. 6+06.30 to 19'-6" at Sta. 6+87.26 "B"



* See Spot Elevation Detail sheets for more info
** Special V-Ditch from Sta. 6+87.26 to Sta. 7+15.00 "B"
Flat Bottom Ditch from Sta. 7+15.00 to Sta. 8+12.28 "B"

Legend:

- (13) Curb, Concrete, Modified
- (15) Curb and Gutter, Concrete
- (16) Curb and Gutter, Concrete, Type B, Modified
- (17) Curb and Gutter, Concrete, Modified
- (22) Center Curb, Concrete, Type D, Modified
- (26) Sodding
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- (K) 220 lbs/sys QC/QA+HMA, 3, 58H, Surface, 9.5 mm, on 275 lbs/sys QC/QA+HMA, 3, 58H, Intermediate, 19.0 mm, on 385 lbs/sys QC/QA+HMA, 3, 58S, Base, 25.0 mm, on 660 lbs/sys QC/QA+HMA, 3, 58S, Base, 25.0 mm, on Subgrade Treatment, Type IC, on Blaxial Geogrid Type IB
- (M) 220 lbs/sys QC/QA+HMA, 3, 58H, Surface, 9.5 mm, on Milling, Asphalt, 2 in.
- (O) Compacted Aggregate, No. 53
- (R) PCCP, 4 in., on 16 in. Compacted Aggregate, No. 53, on Subgrade Treatment, Type IC
- (R2) 660 lbs/sys QC/QA+HMA, 3, 58S, Base, 25.0 mm, on Subgrade Treatment, Type IC, on Blaxial Geogrid Type IB

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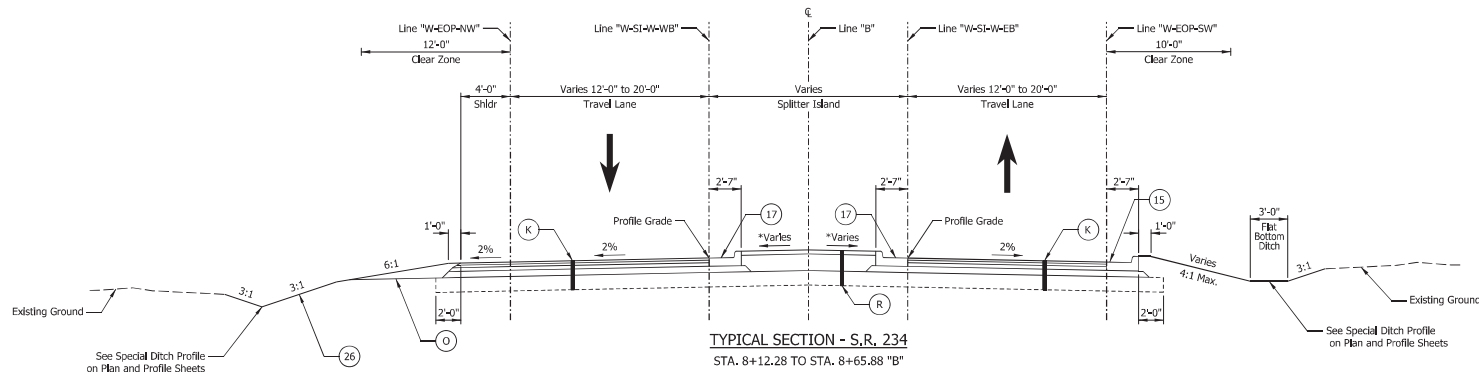
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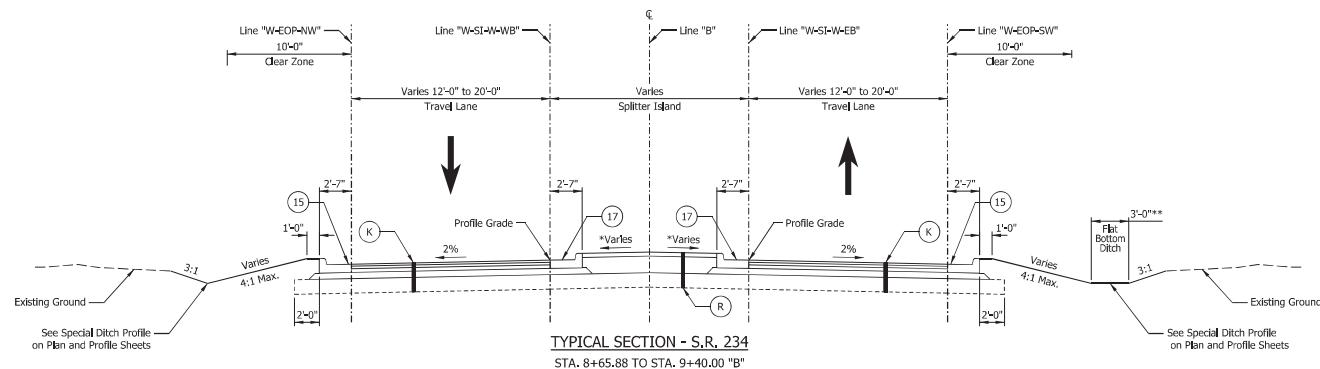
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SHEETS	
7	of 129
CONTRACT	
R-44131	



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 - (O) Compacted Aggregate, No. 53
 - (R) PCCP, 4 in., on 16 in. Compacted Aggregate, No. 53, on Subgrade Treatment, Type IC
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* See Spot Elevation Detail sheets for more info



* See Spot Elevation Detail sheets for more info

** Flat Bottom Ditch from Sta. 8+65.88 to Sta. 8+79.65 "B"
Special V-Ditch from Sta. 8+79.65 to Sta. 8+90.18 "B"

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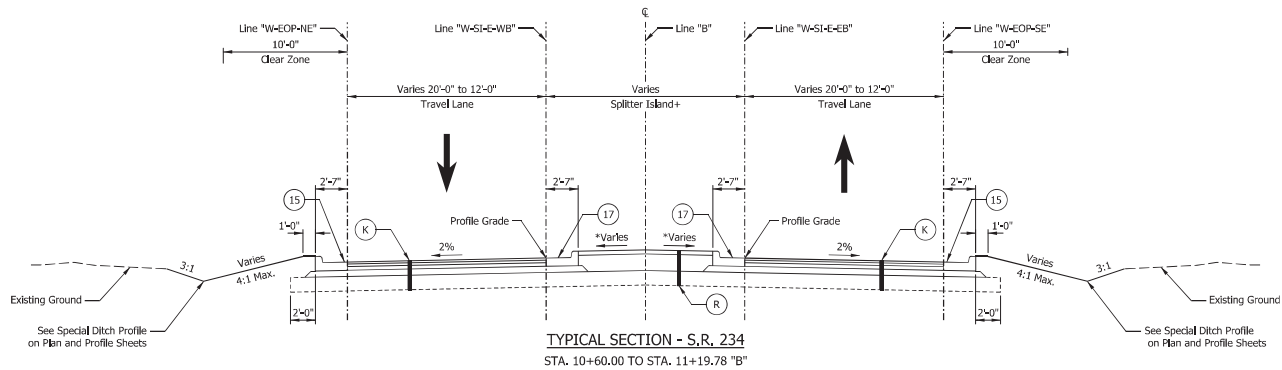
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INDIANA
DEPARTMENT OF TRANSPORTATION

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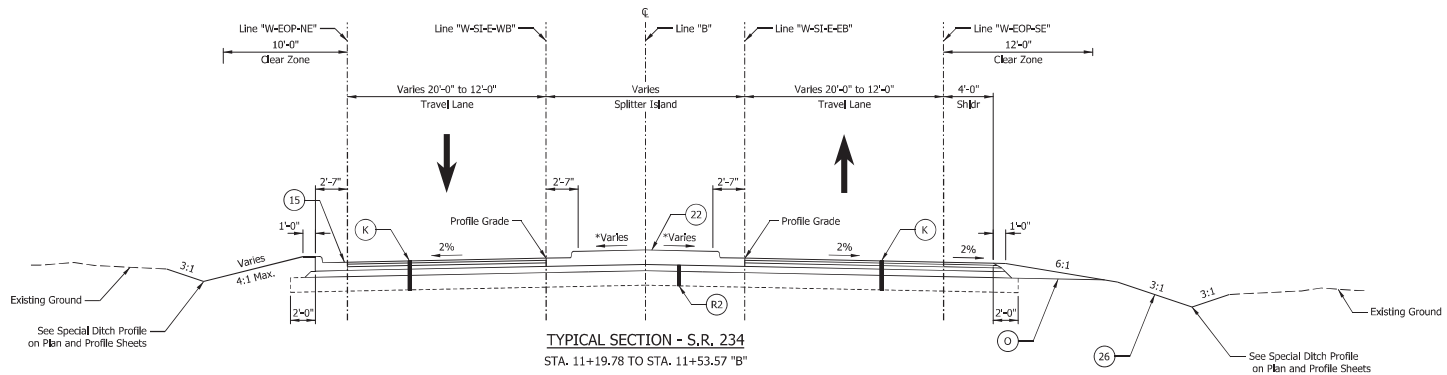


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- (15) Curb and Gutter, Concrete
- (16) Curb and Gutter, Concrete, Type B, Modified
- (17) Curb and Gutter, Concrete, Modified
- (22) Center Curb, Concrete, Type D, Modified
- (26) Sodding
- (A) PCCP, 10 in., Colored, with D-1 Contraction Joints spaced at 15 ft, with 1.25 in. diameter Dowels, on 6 in. Compacted Aggregate, No. 53, on Subgrade Treatment, Type IC, on Blaxial Geogrid Type IB
- (K) 220 lbs/sys QC/QA+HMA, 3, 58H, Surface, 9.5 mm, on 275 lbs/sys QC/QA+HMA, 3, 58H, Intermediate, 19.0 mm, on 385 lbs/sys QC/QA+HMA, 3, 58S, Base, 25.0 mm, on 660 lbs/sys QC/QA+HMA, 3, 58S, Base, 25.0 mm, on Subgrade Treatment, Type IC, on Blaxial Geogrid Type IB
- (M) 220 lbs/sys QC/QA+HMA, 3, 58H, Surface, 9.5 mm, on Milling, Asphalt, 2 in.
- (O) Compacted Aggregate, No. 53
- (R) PCCP, 4 in., on 16 in. Compacted Aggregate, No. 53, on Subgrade Treatment, Type IC
- (R2) 660 lbs/sys QC/QA+HMA, 3, 58S, Base, 25.0 mm, on Subgrade Treatment, Type IC, on Blaxial Geogrid Type IB

* See Spot Elevation Detail sheets for more Info

+ Curb and Gutter, Concrete, Modified ends at Sta. 10+93.31 "B"
Center Curb, Concrete, Type D, Modified from Sta. 10+93.31 to Sta. 11+19.78 "B"



* See Spot Elevation Detail sheets for more Info

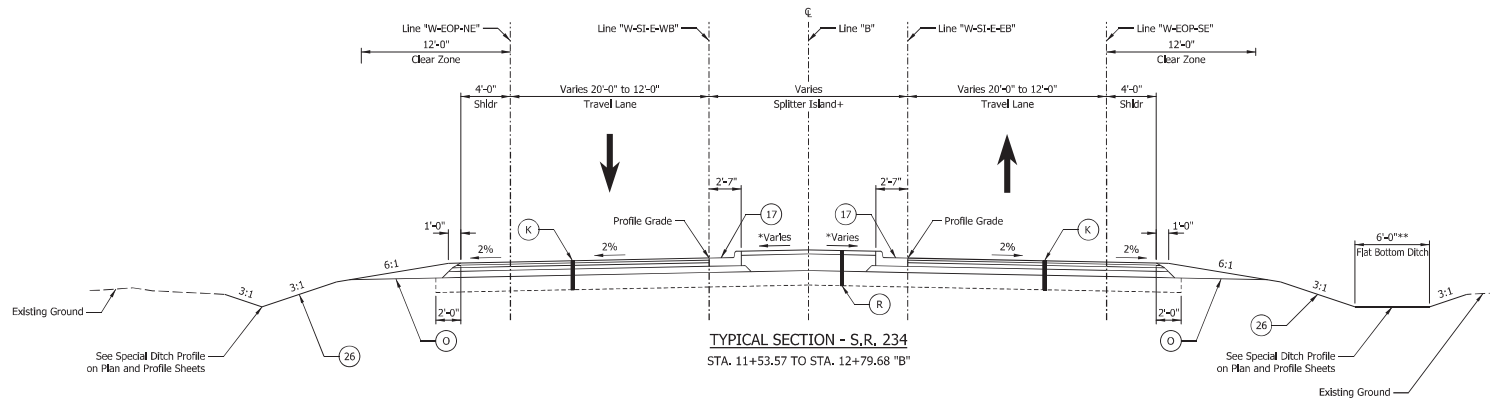
NOT FOR
CONSTRUCTION

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: RK	8/11/2025	DRAWN: BTS
CHECKED: ELM	8/11/2025	CHECKED: RK

INDIANA
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTION
SR 234 - LINE "B"

HORIZONTAL SCALE	BRIDGE FILE
1" = 3'	N/A
VERTICAL SCALE	DESIGNATION
1" = 5'	2000154
SHEETS	
9	of 129
CONTRACT	
R-44131	

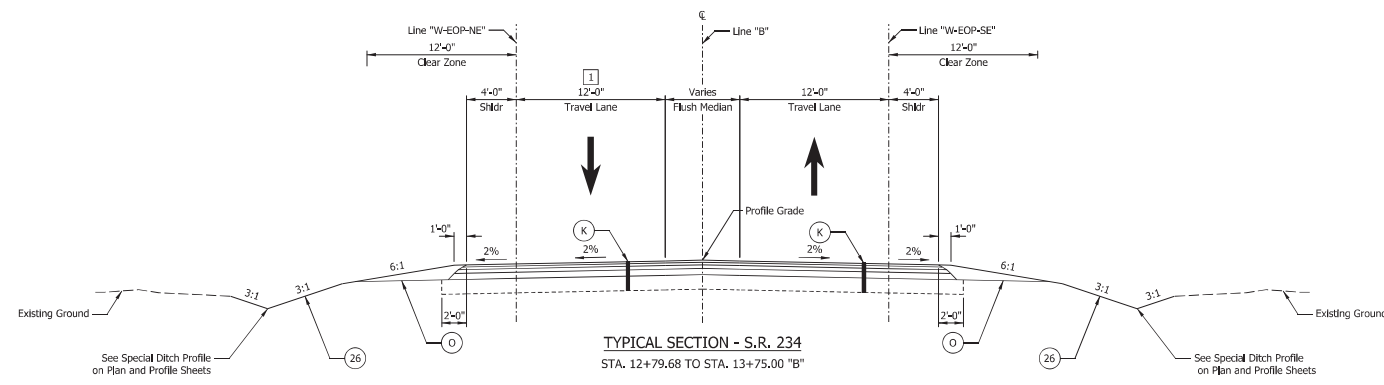


- Legend:**
- (13) Curb, Concrete, Modified
 - (15) Curb and Gutter, Concrete
 - (16) Curb and Gutter, Concrete, Type B, Modified
 - (17) Curb and Gutter, Concrete, Modified
 - (22) Center Curb, Concrete, Type D, Modified
 - (26) Sodding
 - (A) PCCP, 10 in., Colored, with D-1 Contraction Joints spaced at 15 ft, with 1.25 in. diameter Dowels, on 6 in. Compacted Aggregate, No. 53, on Subgrade Treatment, Type IC, on Blaxial Geogrid Type IB
 - (K) 220 lbs/sy QC/QA+HMA, 3, 58H, Surface, 9.5 mm, on 275 lbs/sy QC/QA+HMA, 3, 58H, Intermediate, 19.0 mm, on 385 lbs/sy QC/QA+HMA, 3, 58S, Base, 25.0 mm, on 660 lbs/sy QC/QA+HMA, 3, 58S, Base, 25.0 mm, on Subgrade Treatment, Type IC, on Blaxial Geogrid Type IB
 - (M) 220 lbs/sy QC/QA+HMA, 3, 58H, Surface, 9.5 mm, on Milling, Asphalt, 2 in.
 - (O) Compacted Aggregate, No. 53
 - (R) PCCP, 4 in., on 16 in. Compacted Aggregate, No. 53, on Subgrade Treatment, Type IC
 - (R2) 660 lbs/sy QC/QA+HMA, 3, 58S, Base, 25.0 mm, on Subgrade Treatment, Type IC, on Blaxial Geogrid Type IB

* See Spot Elevation Detail sheets for more info

** Special V-Ditch from Sta. 11+53.57 to Sta. 11+64.00 "B"
 Flat Bottom Ditch Width 6'-0" from Sta. 11+64.00 to Sta. 12+79.00 "B"
 Flat Bottom Ditch Width 4'-0" from Sta. 13+04.00 to Sta. 13+69.00 "B"

+ Center Curb, Concrete, Type D, Modified from Sta. 11+53.57 to Sta. 11+74.96 "B"
 Curb and Gutter, Concrete, Modified begins at Sta. 11+74.96 "B"



[1] Varies from 13'-6" at Sta. 13+15.00 to 12'-0" at Sta. 13+48.26 "B"

* See Spot Elevation Detail sheets for more info

File: 8/10/2025 3:26 PM

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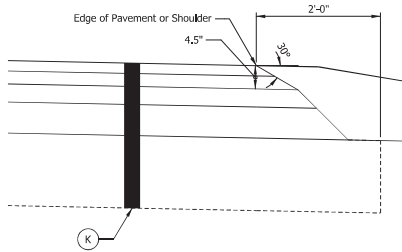
NOT FOR
CONSTRUCTION

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: RK	8/11/2025	DRAWN: BTS 8/11/2025
CHECKED: ELM	8/11/2025	CHECKED: RK 8/11/2025

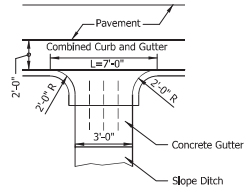
INDIANA
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTION
SR 234 - LINE "B"

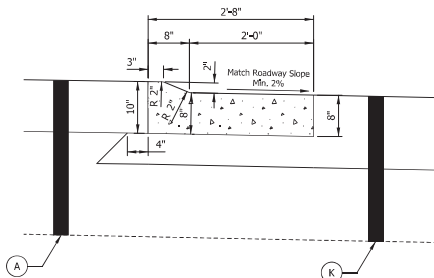
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1" = 3'	N/A
VERTICAL SCALE	DESIGNATION
1" = 5'	2000154
SHEETS	
10 of 129	
CONTRACT	
R-44131	



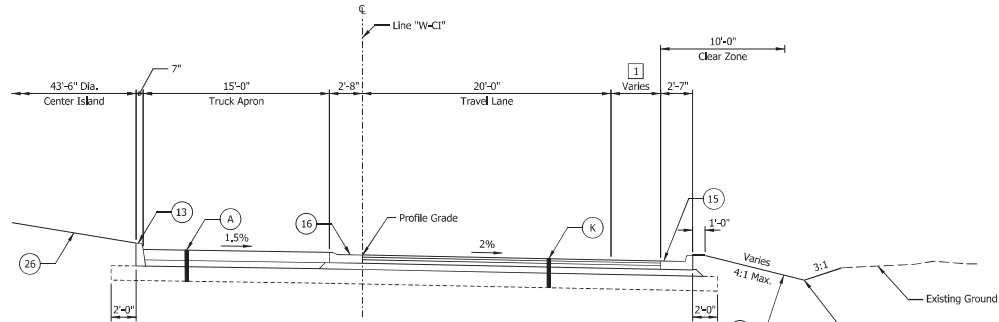
SAFETY EDGE
Not to Scale



COMBINED CURB AND GUTTER TURNOUT
Not to Scale



16 CURB AND GUTTER, CONCRETE, TYPE B, MODIFIED
Not to Scale



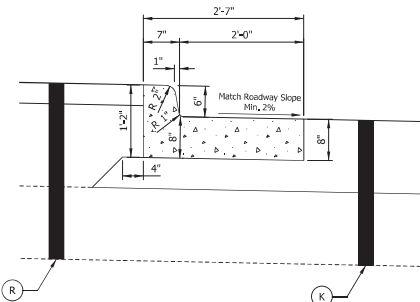
TYPICAL SECTION - ROUNDABOUT CIRCULATORY ROADWAY

Scale: 1"=5'
CENTER POINT: P.O.T. Sta. 49+41.01, 14.92' RT "C"
Sta. 49+40.00 to Sta. 50+60.00 "C"

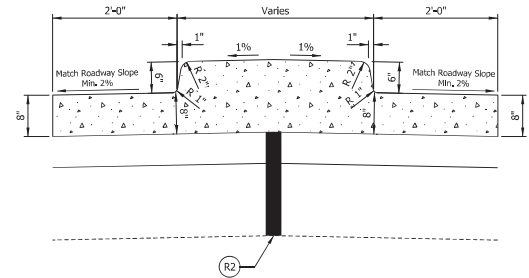
1 Varies from 5'-8" to 7'-6"

Legend:

- 13 Curb, Concrete, Modified
- 15 Curb and Gutter, Concrete
- 16 Curb and Gutter, Concrete, Type B, Modified
- 17 Curb and Gutter, Concrete, Modified
- 22 Center Curb, Concrete, Type D, Modified
- 26 Sodding
- A PCCP, 10 in., Colored, with D-1 Contraction Joints spaced at 15 ft, with 1.25 in. diameter Dowels, on 6 in. Compacted Aggregate, No. 53, on Subgrade Treatment, Type IC, on Blaxial Geogrid Type IB
- K 220 lbs/sys QC/QA+HMA, 3, 58H, Surface, 9.5 mm, on 275 lbs/sys QC/QA+HMA, 3, 58H, Intermediate, 19.0 mm, on 385 lbs/sys QC/QA+HMA, 3, 58S, Base, 25.0 mm, on 660 lbs/sys QC/QA+HMA, 3, 58S, Base, 25.0 mm, on Subgrade Treatment, Type IC, on Blaxial Geogrid Type IB
- M 220 lbs/sys QC/QA+HMA, 3, 58H, Surface, 9.5 mm, on Milling, Asphalt, 2 in.
- O Compacted Aggregate, No. 53
- R PCCP, 4 in., on 16 in. Compacted Aggregate, No. 53, on Subgrade Treatment, Type IC
- R2 660 lbs/sys QC/QA+HMA, 3, 58S, Base, 25.0 mm, on Subgrade Treatment, Type IC, on Blaxial Geogrid Type IB



17 CURB AND GUTTER, CONCRETE, MODIFIED
Not to Scale



22 CENTER CURB, CONCRETE, TYPE D, MODIFIED
Not to Scale

File: 8/10/2025 3:26 PM

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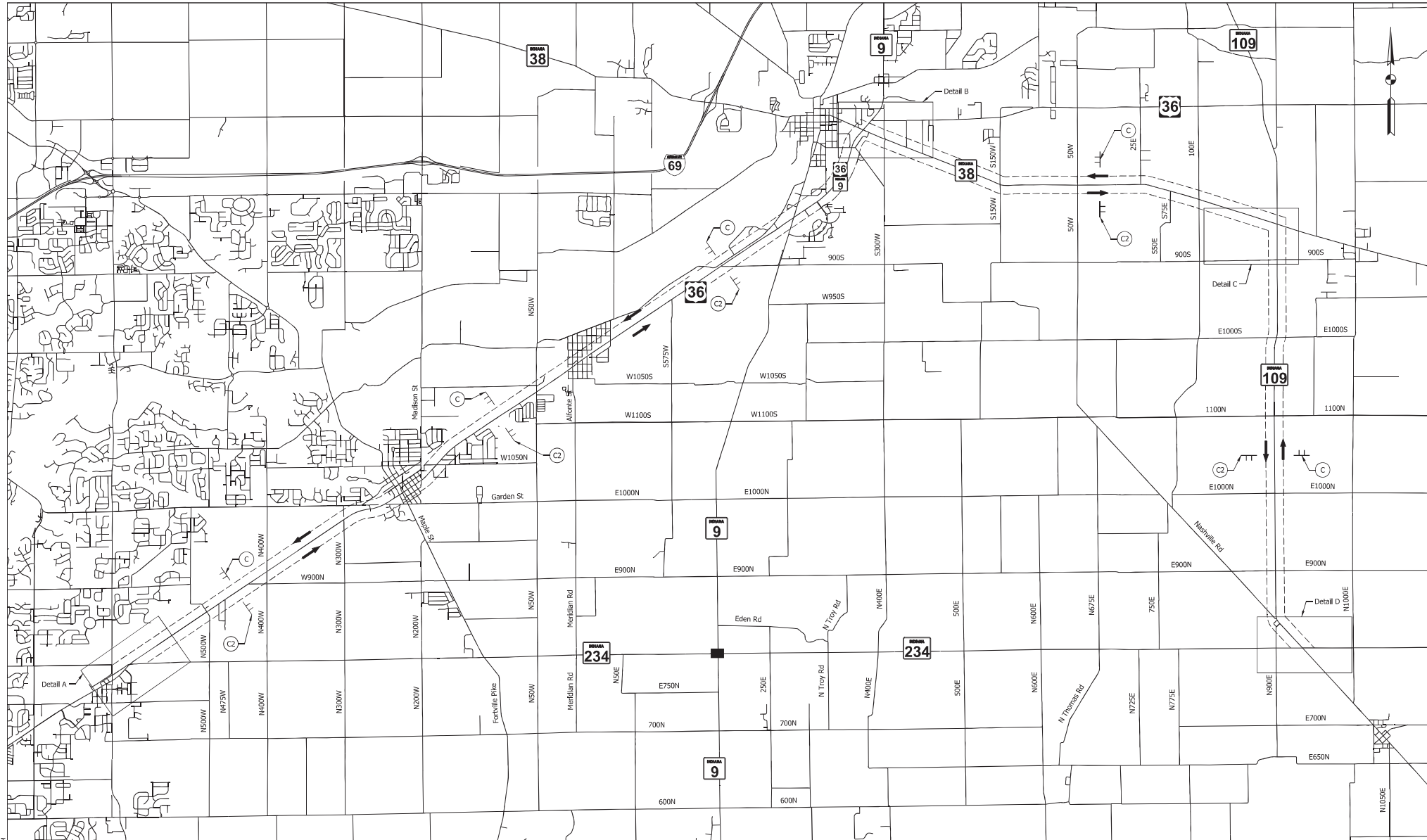
NOT FOR CONSTRUCTION

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: RK	8/11/2025	DRAWN: BTS
CHECKED: ELM	8/11/2025	CHECKED: RK

INDIANA
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTION
ROUNDABOUT LINE "W-CI"

HORIZONTAL SCALE	BRIDGE FILE
As Noted	N/A
VERTICAL SCALE	DESIGNATION
As Noted	2000154
	SHEETS
	11 of 129
	CONTRACT
	R-44131



File: 8/10/2025 3:37 PM

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Model: Sheet01

NOT FOR
CONSTRUCTION

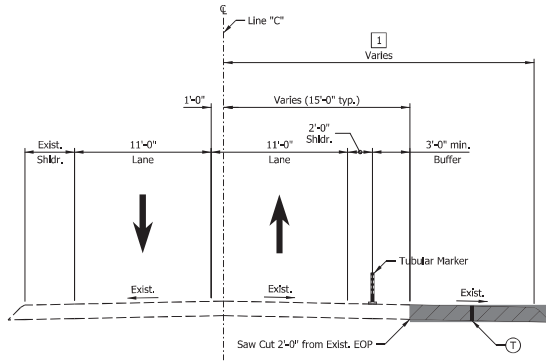
RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: RK	8/11/2025	DRAWN: BTS
CHECKED: ELM	8/11/2025	CHECKED: RK

INDIANA DEPARTMENT OF TRANSPORTATION
MAINTENANCE OF TRAFFIC DETOUR PLAN SR 234 - LINE "B"

HORIZONTAL SCALE	BRIDGE FILE
1" = 300'	N/A
VERTICAL SCALE	DESIGNATION
N/A	2000154
	SHEETS
	19 of 129
	CONTRACT
	R-44131

FILE: 8/10/2025 3:37 PM

MAINTENANCE OF TRAFFIC QUANTITIES								
ITEM DESCRIPTION	UNIT	PRE CONSTRUCTION PHASE	PHASE I	PHASE II A	PHASE II B	PHASE III A	PHASE III B	DETOUR
TEMPORARY PAVEMENT MESSAGE MARKING, LANE INDICATION ARROW	EACH	12	11	9	9	12	12	0
TEMPORARY PAVEMENT	SYS	468	216	0	0	0	0	0
ROAD CLOSURE SIGN ASSEMBLY	EACH	0	0	0	0	0	0	6
TEMPORARY PAVEMENT MARKING, REMOVABLE, 6 IN.	LFT	2,864	3,266	2,150	2,150	230	298	0
TEMPORARY TRANSVERSE PAVEMENT MARKING, REMOVABLE, 24 IN.	LFT	0	24	24	24	0	0	0
TEMPORARY TRAFFIC SIGNAL INSTALLATION, MAINTAIN	LS	0	1	1	1	0	0	0
DETOUR ROUTE MARKER ASSEMBLY	EACH	0	0	0	0	0	0	55
CONSTRUCTION SIGN, TYPE A	EACH	4	6	6	6	4	4	3
CONSTRUCTION SIGN, TYPE B	EACH	3	6	6	6	4	4	5
MAINTAINING TRAFFIC	LS	1	1	1	1	1	1	1
BARRICADE, TYPE III-B	LFT	0	208	310	460	0	0	148
TEMPORARY TRAFFIC BARRIER, TYPE 2	LFT	0	637	640	640	0	0	640
CONTROLLER AND CABINET	EACH	0	1	1	1	0	0	1



TYPICAL MOT SECTION - S.R. 9

STA. 45+53.24 TO STA. 55+98.03 "C"

- 1 24'-0" from Sta. 46+34.26 to Sta. 48+04.47 "C"
24'-0" from Sta. 48+99.30 to Sta. 49+70.65 "C"
25'-0" from Sta. 50+36.24 to Sta. 51+22.31 "C"
Varies from 23'-0" at Sta. 53+67.74 to 17'-0" at Sta. 55+47.67 "C"

Sequence of Operations

Pre Construction Phase:

CZ Design Speed = 40 mph

1. Reduce both the northbound and southbound lanes from 12'-0" to 11'-0" and shift the centerline 1'-0" to the west.
2. Widen the existing shoulder with temporary pavement.
3. Construct a temporary drive from SR 234 to the Fire Station on the north east corner of the intersection.

Phase 1:

CZ Design Speed = 40 mph

1. Close SR 234 and detour traffic.
2. Maintain SR 9 traffic in a two-way, one-lane configuration, with portable temporary traffic signals, using the existing northbound pavement and temporary pavement. Install Driveway Assistance Devices for each driveway within the two-way, one-lane configuration.
3. Construct the west half of the intersection, omitting portions of the center island and south splitter island.
4. Construct temporary pavement in place of the omitted portions of the center island and south splitter island.

Phase 2A:

CZ Design Speed = 40 mph

1. SR 234 remains closed and the detour remains in place.
2. Maintain SR 9 traffic in a two-way, one-lane configuration, with portable temporary traffic signals, using the newly constructed southbound pavement and temporary pavement in the center island and south splitter island. Install Driveway Assistance Devices for each driveway within the two-way, one-lane configuration.
3. Utilize the temporary drive access to the Fire Station constructed in the Pre Construction Phase for emergency vehicles.
4. Construct the east half of the north leg, south leg, and circulatory roadway of the intersection.
5. Remove the temporary pavement along northbound SR 9 shoulder.

Phase 2B:

CZ Design Speed = 40 mph

1. SR 234 remains closed and the detour remains in place.
2. Continue to maintain SR 9 traffic in a two-way, one-lane configuration, with portable temporary traffic signals, using the newly constructed southbound pavement and temporary pavement in the center island and south splitter island. Install Driveway Assistance Devices for each driveway within the two-way, one-lane configuration.
3. Construct the east leg of the intersection.

Phase 3A:

CZ Design Speed = 30 mph

1. The roundabout intersection is opened to traffic on SR 234 and SR 9.
2. Oversized vehicles on SR 9 must utilize detour.
3. Shift the traffic to the outside of the circulating travel lane with tubular markers on the west side of the roundabout.
4. Remove the temporary pavement and construct the remainder of the center island.

Phase 3B:

CZ Design Speed = 30 mph

1. The roundabout intersection is opened to traffic on SR 234 and SR 9.
2. Oversized vehicles on SR 9 must utilize detour.
3. Shift the southbound traffic to the outside of the circulating travel lane and the west side of the south leg with tubular markers.
4. Remove the temporary pavement and construct the remainder of the south splitter island.

LEGEND:

- Construction Area
- Constructed in Previous Phase
- Temporary Pavement
- Temp. Concrete Barrier, Type 2
- 50" Spaced Drums
- 25' Spaced Tubular Markers
- Construction Sign
- Portable Temp. Traffic Signal
- Traffic Flow Arrow

- (80) - Temp. Pavement Marking, Removable, 6", Solid, Yellow
- (81) - Temp. Pavement Marking, Removable, 6", Broken, Yellow
- (82) - Temp. Transverse Marking, Removable, 24", Stop Line, White
- (83) - Temp. Pavement Marking, Removable, 6", Solid, White
- (T) - HMA for Temp. Pavement, Type C, consisting of:
 - 165 lbs/sys HMA Surface, Type C, on
 - 275 lbs/sys HMA Intermediate, Type C, on
 - 660 lbs/sys HMA Base, Type C, on
 - Subgrade Treatment, Type II

NOT FOR CONSTRUCTION

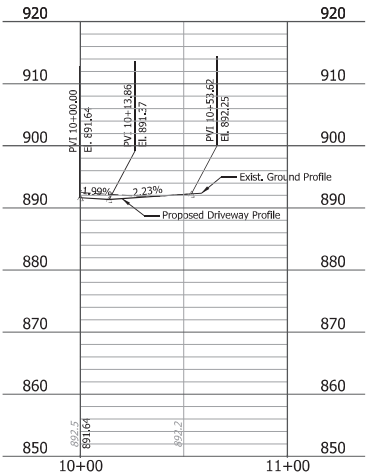
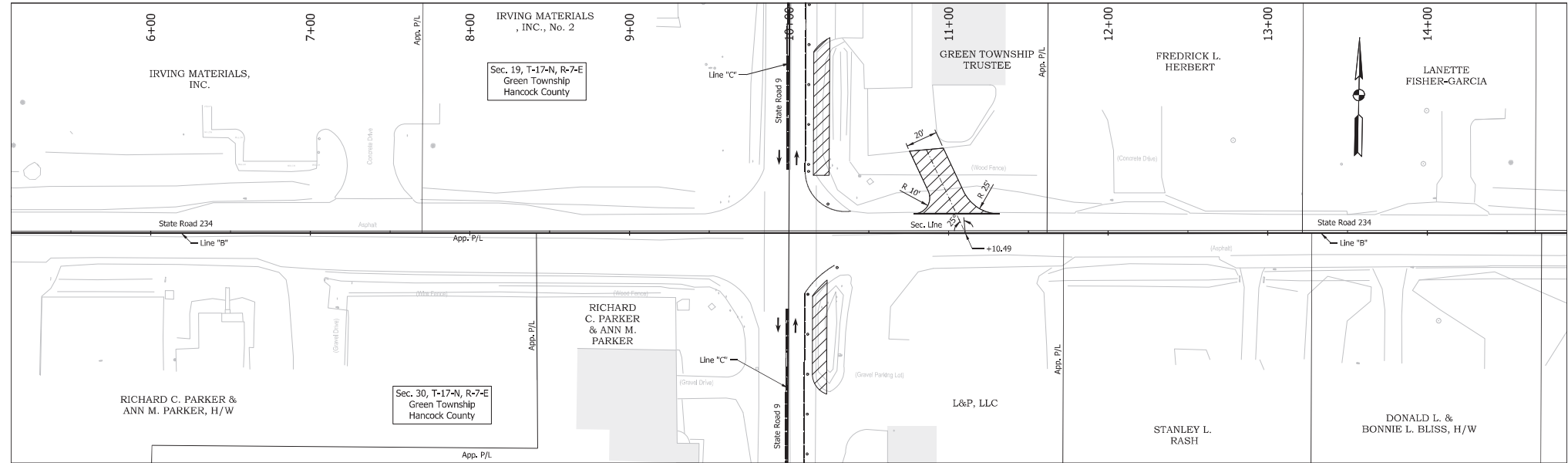
RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: RK	8/11/2025	DRAWN: BTS
CHECKED: ELM	8/11/2025	CHECKED: RK

INDIANA
DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC
PRE-CONSTRUCTION PHASE
TYPICAL SECTION

HORIZONTAL SCALE	BRIDGE FILE
1" = 3'	N/A
VERTICAL SCALE	DESIGNATION
1" = 5'	2000154
SHEETS	
24	of 129
CONTRACT	
R-44131	

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Model: Sheet001



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Model: Sheet01

LEGEND:			
	- Construction Area		- 50' Spaced Drums
	- Constructed in Previous Phase		- 25' Spaced Tubular Markers
	- Temporary Pavement		- Construction Sign
	- Temp. Concrete Barrier, Type 2		- Portable Temp. Traffic Signal
	- Traffic Flow Arrow		- Temp. Pavement Marking, Removable, 6', Solid, Yellow
			- Temp. Pavement Marking, Removable, 6', Broken, Yellow
			- Temp. Transverse Marking, Removable, 24', Stop Line, White
			- Temp. Pavement Marking, Removable, 6', Solid, White
			- HMA for Temp. Pavement, Type C, consisting of: 165 lbs/sys HMA Surface, Type C, on 275 lbs/sys HMA Intermediate, Type C, on 660 lbs/sys HMA Base, Type C, on Subgrade Treatment, Type II

NOT FOR CONSTRUCTION

RECOMMENDED FOR APPROVAL		
DESIGNED: RK	8/11/2025	DESIGN ENGINEER: DATE
CHECKED: ELM	8/11/2025	CHECKED: RK

INDIANA DEPARTMENT OF TRANSPORTATION	
MAINTENANCE OF TRAFFIC PRE-CONSTRUCTION PHASE TEMPORARY DRIVE	

HORIZONTAL SCALE		BRIDGE FILE	
1" = 30'		N/A	
VERTICAL SCALE		DESIGNATION	
1" = 10'		2000154	
SHEETS		CONTRACT	
25 of 129		R-44131	

FILE: 8/10/2025 3:37 PM

- LEGEND:
- Construction Area
 - Constructed in Previous Phase
 - Temporary Pavement
 - Temp. Concrete Barrier, Type 2
 - 50' Spaced Drums
 - 25' Spaced Tubular Markers
 - Construction Sign
 - Portable Temp. Traffic Signal
 - Traffic Flow Arrow

- (80) - Temp. Pavement Marking, Removable, 6", Solid, Yellow
- (81) - Temp. Pavement Marking, Removable, 6", Broken, Yellow
- (82) - Temp. Transverse Marking, Removable, 24", Stop Line, White

- (83) - Temp. Pavement Marking, Removable, 6", Solid, White
- (T) - HMA for Temp. Pavement, Type C, consisting of:
165 lbs/sys HMA Surface, Type C, on
275 lbs/sys HMA Intermediate, Type C, on
660 lbs/sys HMA Base, Type C, on
Subgrade Treatment, Type II

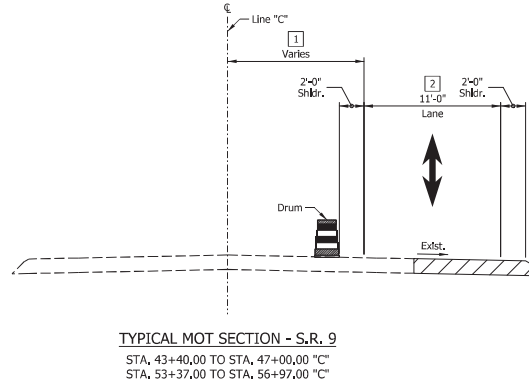
NOT FOR
CONSTRUCTION

RECOMMENDED FOR APPROVAL		DESIGN ENGINEER	DATE
DESIGNED: RK	8/11/2025	DRAWN: BTS	8/11/2025
CHECKED: ELM	8/11/2025	CHECKED: RK	8/11/2025

INDIANA
DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC
PHASE 1
TYPICAL SECTION

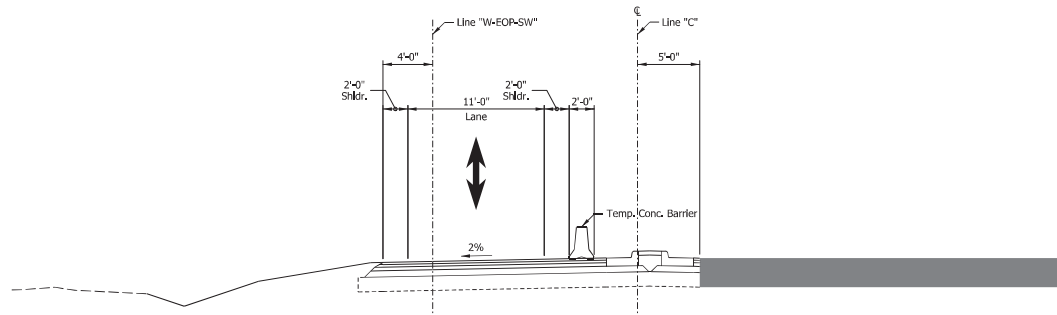
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1" = 3'		N/A	
VERTICAL SCALE		DESIGNATION	
1" = 5'		2000154	
		SHEETS	
		29	of 129
		CONTRACT	
		R-44131	



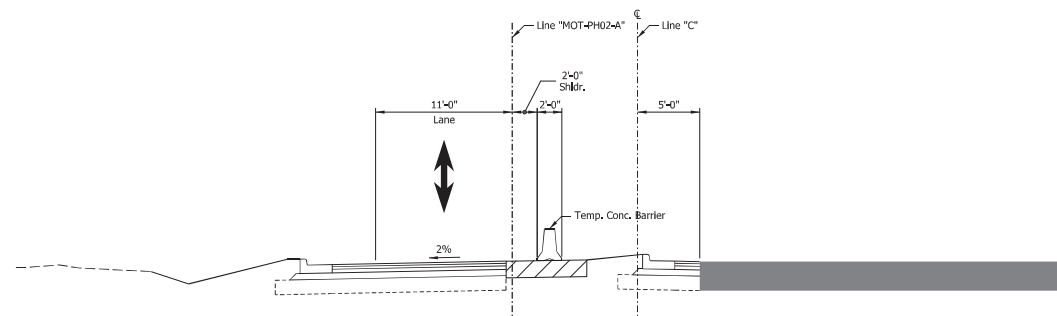
- 1 0'-0" from Sta. 43+40.00 to Sta. 43+70.00 "C"
Varies from 0'-0" at Sta. 43+70.00 to 11'-0" at Sta. 47+00.00 "C"
Varies from 11'-0" at Sta. 53+37.00 to 0'-0" at Sta. 56+67.00 "C"
0'-0" from Sta. 56+67.00 to Sta. 56+97.00 "C"
- 2 Varies from 12'-0" at Sta. 43+40.00 to 11'-0" at Sta. 43+70.00 "C"
Varies from 11'-0" at Sta. 56+67.00 to 12'-0" at Sta. 56+97.00 "C"



- 3 Varies from 7'-0" at Sta. 50+00.00 to 8'-0" at Sta. 50+50.00 "C"
8'-0" from Sta. 50+50.00 to Sta. 51+85.00 "C"
Varies from 8'-0" at Sta. 51+85.00 to 7'-0" at Sta. 52+40.00 "C"



TYPICAL MOT SECTION - S.R. 9
STA, 48+36.54 TO STA, 48+61.61 "C"

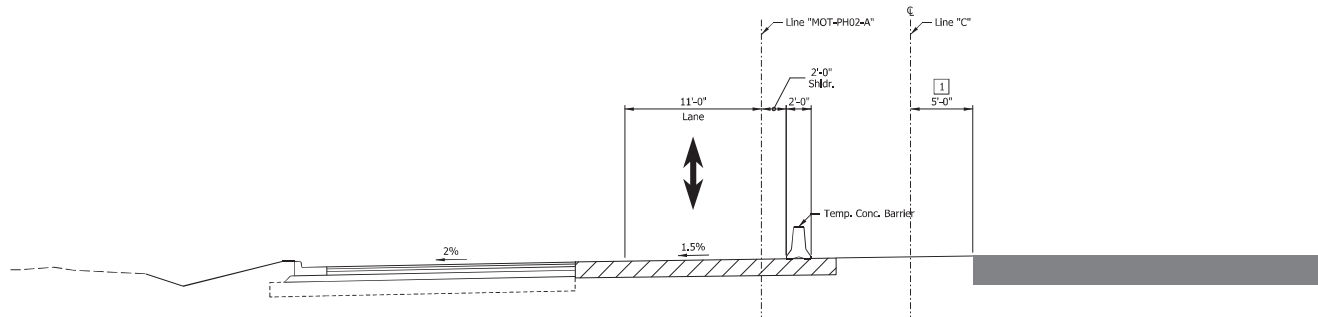


TYPICAL MOT SECTION - S.R. 9
STA, 48+61.61 TO STA, 49+40.00 "C"

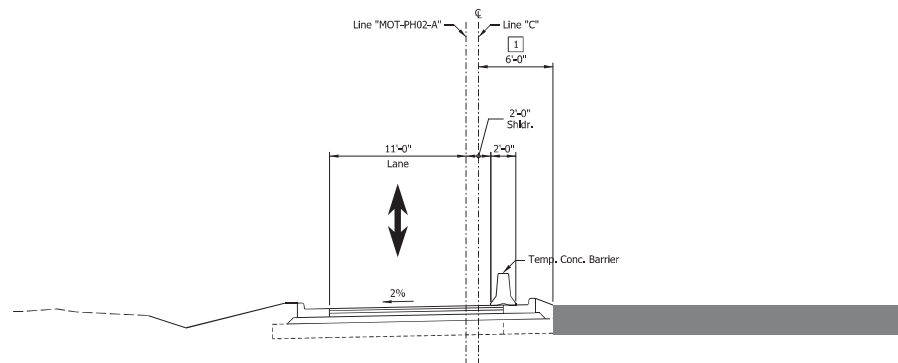
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Model: Sheet02

LEGEND:			
	- Construction Area		- 50' Spaced Drums
	- Constructed in Previous Phase		- 25' Spaced Tubular Markers
	- Temporary Pavement		- Construction Sign
	- Temp. Concrete Barrier, Type 2		- Portable Temp. Traffic Signal
	- Traffic Flow Arrow		- Temp. Pavement Marking, Removable, 6", Solid, Yellow
			- Temp. Pavement Marking, Removable, 6", Broken, Yellow
			- Temp. Transverse Marking, Removable, 24", Stop Line, White
			- Temp. Pavement Marking, Removable, 6", Solid, White
			- HMA for Temp. Pavement, Type C, consisting of: 165 lbs/sys HMA Surface, Type C, on 275 lbs/sys HMA Intermediate, Type C, on 660 lbs/sys HMA Base, Type C, on Subgrade Treatment, Type II

NOT FOR CONSTRUCTION	RECOMMENDED FOR APPROVAL _____				INDIANA DEPARTMENT OF TRANSPORTATION				HORIZONTAL SCALE		BRIDGE FILE	
	DESIGN ENGINEER _____ DATE _____								1" = 5'		N/A	
					MAINTENANCE OF TRAFFIC PHASE 2 TYPICAL SECTION				VERTICAL SCALE		DESIGNATION	
									1" = 5'		2000154	
DESIGNED: RK		8/11/2025		DRAWN: BTS		8/11/2025				SHEETS		
CHECKED: ELM		8/11/2025		CHECKED: RK		8/11/2025				38 of 129		
										CONTRACT		
										R-44131		



TYPICAL MOT SECTION - S.R. 9
STA, 49+40.00 TO STA, 50+60.00 "C"



TYPICAL MOT SECTION - S.R. 9
STA, 50+60.00 TO STA, 52+06.80 "C"

1 Varies from 5'-0" at Sta. 50+00.00 to 6'-0" at Sta. 50+50.00 "C"
6'-0" from Sta. 50+50.00 to Sta. 51+85.00 "C"
Varies from 6'-0" at Sta. 51+85.00 to 5'-0" at Sta. 52+40.00 "C"

LEGEND:

- - Construction Area
- - Constructed in Previous Phase
- - Temporary Pavement
- - Temp. Concrete Barrier, Type 2
- - 50' Spaced Drums
- - 25' Spaced Tubular Markers
- - Construction Sign
- - Portable Temp. Traffic Signal
- - Traffic Flow Arrow

- (80) - Temp. Pavement Marking, Removable, 6", Solid, Yellow
- (81) - Temp. Pavement Marking, Removable, 6", Broken, Yellow
- (82) - Temp. Transverse Marking, Removable, 24", Stop Line, White

- (83) - Temp. Pavement Marking, Removable, 6", Solid, White
- (T) - HMA for Temp. Pavement, Type C, consisting of: 165 lbs/sys HMA Surface, Type C, on 275 lbs/sys HMA Intermediate, Type C, on 660 lbs/sys HMA Base, Type C, on Subgrade Treatment, Type II

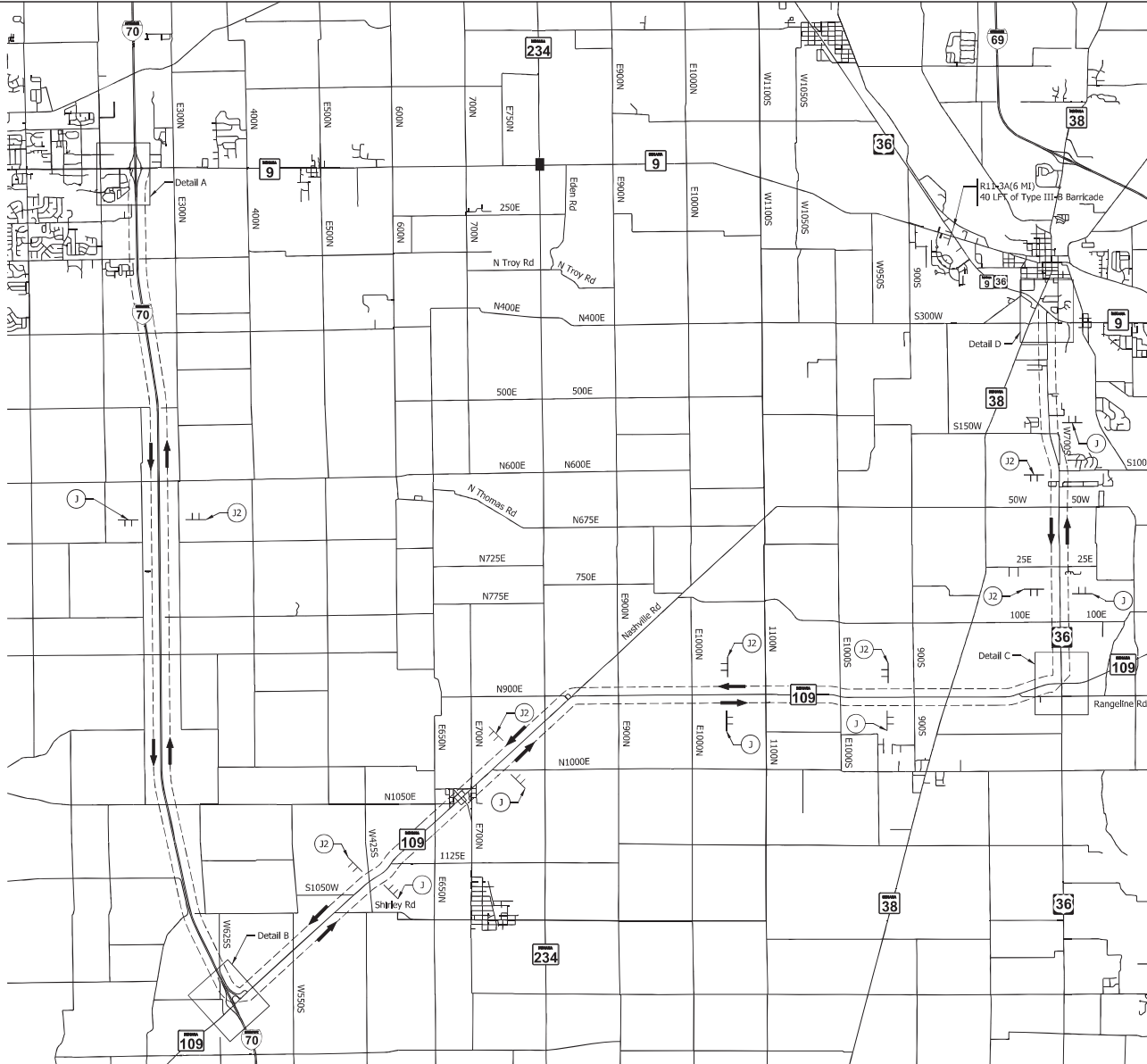
NOT FOR CONSTRUCTION

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: RK	8/11/2025	DRAWN: BTS
CHECKED: ELM	8/11/2025	CHECKED: RK
	8/11/2025	

INDIANA DEPARTMENT OF TRANSPORTATION
MAINTENANCE OF TRAFFIC PHASE 2 TYPICAL SECTION

HORIZONTAL SCALE	BRIDGE FILE
1" = 3'	N/A
VERTICAL SCALE	DESIGNATION
1" = 5'	2000154
SHEETS	
39	of 129
CONTRACT	
R-44131	

FILE: 8/10/2025 3:38 PM
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Model: Sheet03



NOT FOR
CONSTRUCTION

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: RK	8/11/2025	DRAWN: BTS 8/11/2025
CHECKED: ELM	8/11/2025	CHECKED: RK 8/11/2025

INDIANA
DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC
TRUCK DETOUR PLAN
SR 9 - LINE "C"

HORIZONTAL SCALE	BRIDGE FILE
1" = 400'	N/A
VERTICAL SCALE	DESIGNATION
N/A	2000154
	SHEETS
	50 of 129
	CONTRACT
	R-44131

File: 8/10/2025 3:38 PM

File: L:\IDOT\20070904\07090407\Drawings\990407_ContainerSR_9_MOT_Detour_C.dgn
Model: Sheet01

FILE: 8/10/2025 3:38 PM

- LEGEND:
- Construction Area
 - Constructed in Previous Phase
 - Temporary Pavement
 - Temp. Concrete Barrier, Type 2
 - 50' Spaced Drums
 - 25' Spaced Tubular Markers
 - Construction Sign
 - Portable Temp. Traffic Signal
 - Traffic Flow Arrow

- (80) - Temp. Pavement Marking, Removable, 6", Solid, Yellow
- (81) - Temp. Pavement Marking, Removable, 6", Broken, Yellow
- (82) - Temp. Transverse Marking, Removable, 24", Stop Line, White

- (83) - Temp. Pavement Marking, Removable, 6", Solid, White
- (T) - HMA for Temp. Pavement, Type C, consisting of:
 - 165 lbs/sys HMA Surface, Type C, on
 - 275 lbs/sys HMA Intermediate, Type C, on
 - 660 lbs/sys HMA Base, Type C, on
 - Subgrade Treatment, Type II

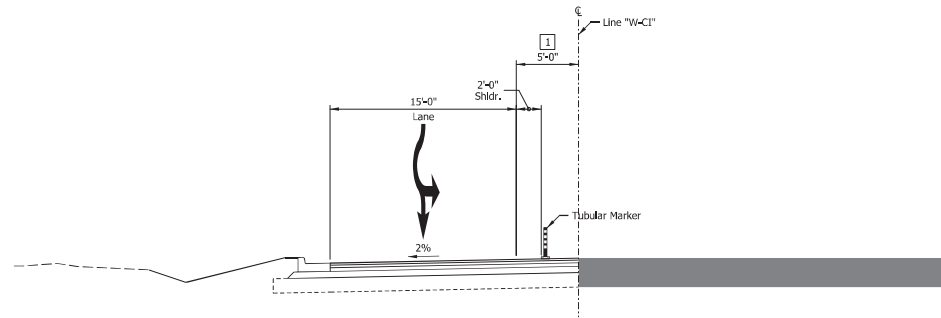
NOT FOR
CONSTRUCTION

RECOMMENDED FOR APPROVAL		DESIGN ENGINEER	DATE
DESIGNED: RK	8/11/2025	DRAWN: BTS	8/11/2025
CHECKED: ELM	8/11/2025	CHECKED: RK	8/11/2025

INDIANA
DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC
PHASE 3A
TYPICAL SECTION

HORIZONTAL SCALE		BRIDGE FILE	
1" = 3'		N/A	
VERTICAL SCALE		DESIGNATION	
1" = 5'		2000154	
		SHEETS	
		55	of 129
		CONTRACT	
		R-44131	



TYPICAL MOT SECTION - CENTER ISLAND
STA. 30+16.00 TO STA. 32+26.00 "W-C1"

1 Varies from 0'-0" at Sta. 30+16.00 to 5'-0" at Sta. 30+66.00 "W-C1"
5'-0" from Sta. 30+66.00 to Sta. 31+76.00 "W-C1"
Varies from 5'-0" at Sta. 31+76.00 to 0'-0" at Sta. 32+26.00 "W-C1"

FILE: 8/10/2025 3:39 PM

- LEGEND:
- Construction Area
 - Constructed in Previous Phase
 - Temporary Pavement
 - Temp. Concrete Barrier, Type 2
 - 50' Spaced Drums
 - 25' Spaced Tubular Markers
 - Construction Sign
 - Portable Temp. Traffic Signal
 - Traffic Flow Arrow

- (80) - Temp. Pavement Marking, Removable, 6", Solid, Yellow
- (81) - Temp. Pavement Marking, Removable, 6", Broken, Yellow
- (82) - Temp. Transverse Marking, Removable, 24", Stop Line, White

- (83) - Temp. Pavement Marking, Removable, 6", Solid, White
- (T) - HMA for Temp. Pavement, Type C, consisting of:
 - 165 lbs/sys HMA Surface, Type C, on
 - 275 lbs/sys HMA Intermediate, Type C, on
 - 660 lbs/sys HMA Base, Type C, on
 - Subgrade Treatment, Type II

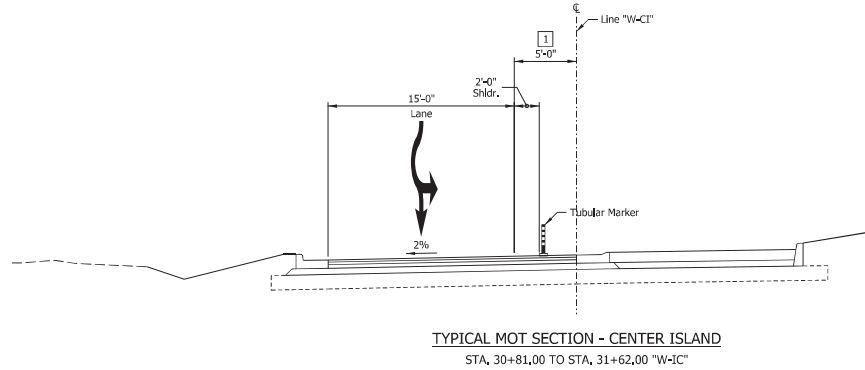
NOT FOR CONSTRUCTION

RECOMMENDED FOR APPROVAL		DESIGN ENGINEER	DATE
DESIGNED: RK	8/11/2025	DRAWN: BTS	8/11/2025
CHECKED: ELM	8/11/2025	CHECKED: RK	8/11/2025

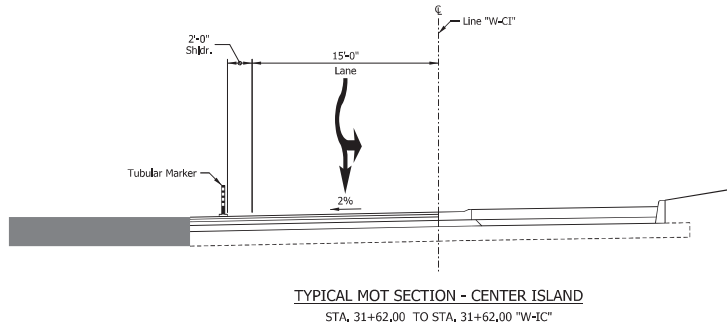
INDIANA
DEPARTMENT OF TRANSPORTATION

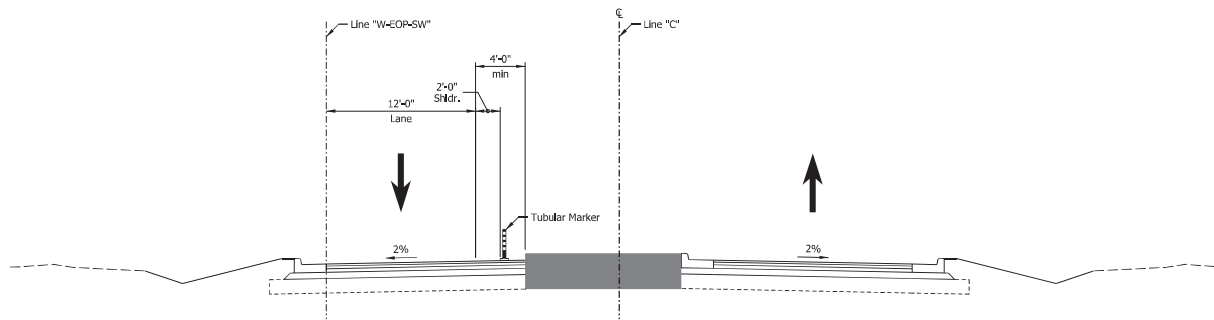
MAINTENANCE OF TRAFFIC
PHASE 3B
TYPICAL SECTION

HORIZONTAL SCALE		BRIDGE FILE	
1" = 3'		N/A	
VERTICAL SCALE		DESIGNATION	
1" = 5'		2000154	
		SHEETS	
		60	of 129
		CONTRACT	
		R-44131	



- 1 Varies from 0'-0" at Sta. 30+81.00 to 5'-0" at Sta. 31+11.00 "W-IC"
5'-0" from Sta. 31+11.00 to Sta. 31+32.00 "W-IC"
Varies from 5'-0" at Sta. 31+32.00 to 0'-0" at Sta. 31+62.00 "W-IC"





TYPICAL MOT SECTION - S.R. 9
STA. 48+00.00 TO STA. 49+40.00 "C"

FILE: 8/10/2025 3:39 PM

LEGEND:

- Construction Area
- Constructed in Previous Phase
- Temporary Pavement
- Temp. Concrete Barrier, Type 2

- - 50' Spaced Drums
- - 25' Spaced Tubular Markers
- 4 - Construction Sign
- - Portable Temp. Traffic Signal
- ➔ - Traffic Flow Arrow

- (80) - Temp. Pavement Marking, Removable, 6", Solid, Yellow
- (81) - Temp. Pavement Marking, Removable, 6", Broken, Yellow
- (82) - Temp. Transverse Marking, Removable, 24", Stop Line, White

- (83) - Temp. Pavement Marking, Removable, 6", Solid, White
- (T) - HMA for Temp. Pavement, Type C, consisting of:
165 lbs/sys HMA Surface, Type C, on
275 lbs/sys HMA Intermediate, Type C, on
660 lbs/sys HMA Base, Type C, on
Subgrade Treatment, Type II

NOT FOR CONSTRUCTION

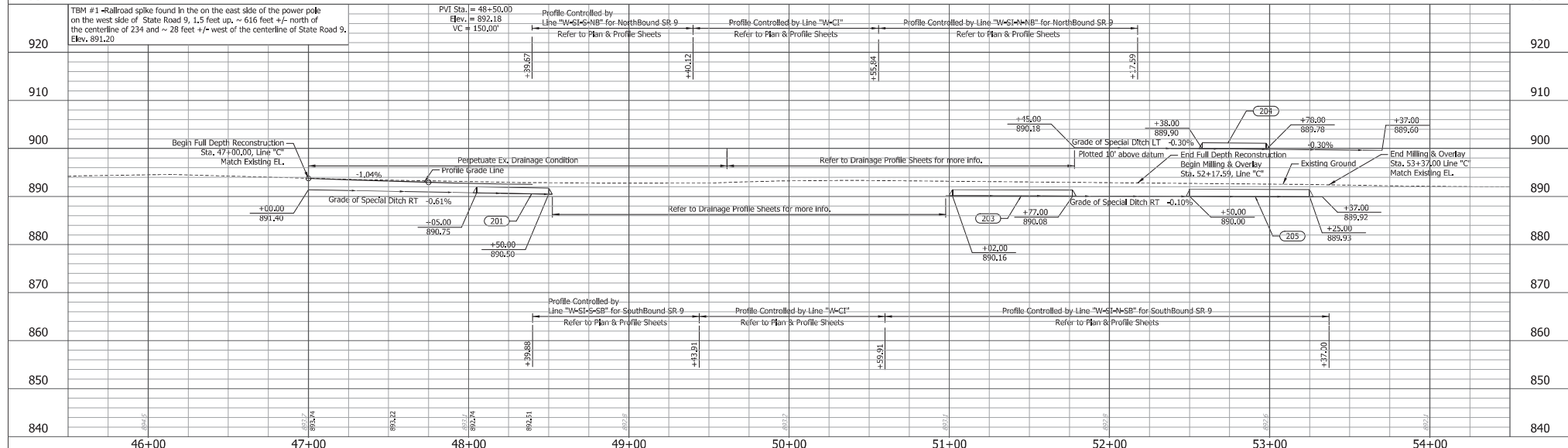
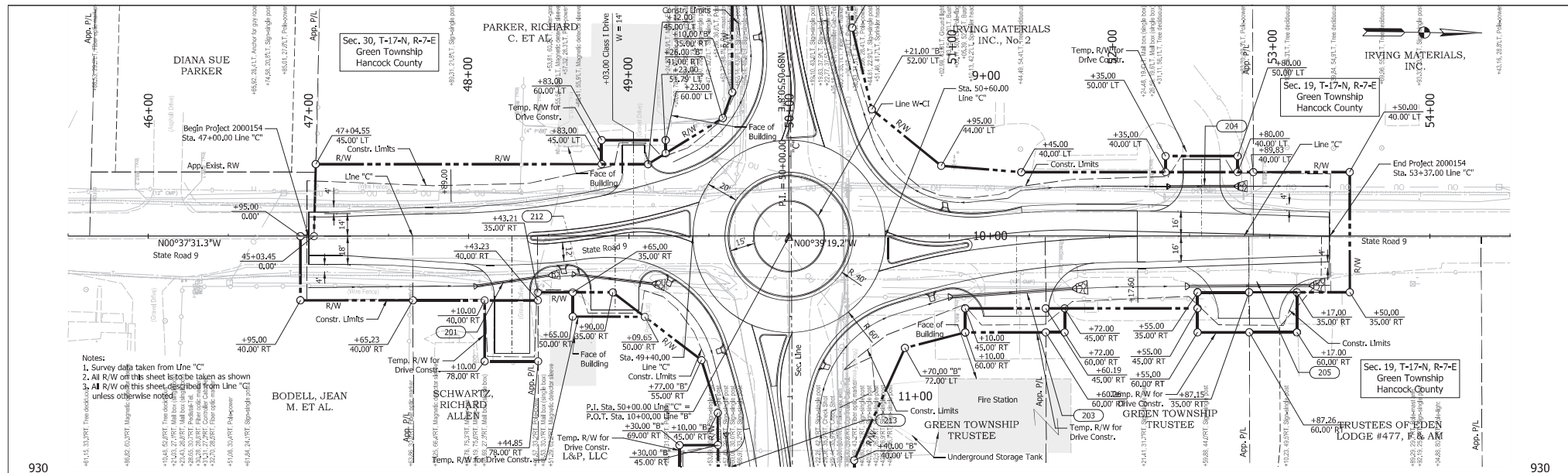
RECOMMENDED FOR APPROVAL		DESIGN ENGINEER	DATE
DESIGNED: RK	8/11/2025	DRAWN: BTS	8/11/2025
CHECKED: ELM	8/11/2025	CHECKED: RK	8/11/2025

INDIANA
DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC
PHASE 3B
TYPICAL SECTION

HORIZONTAL SCALE		BRIDGE FILE	
1" = 3'		N/A	
VERTICAL SCALE		DESIGNATION	
1" = 5'		2000154	
		SHEETS	
		61	of 129
		CONTRACT	
		R-44131	

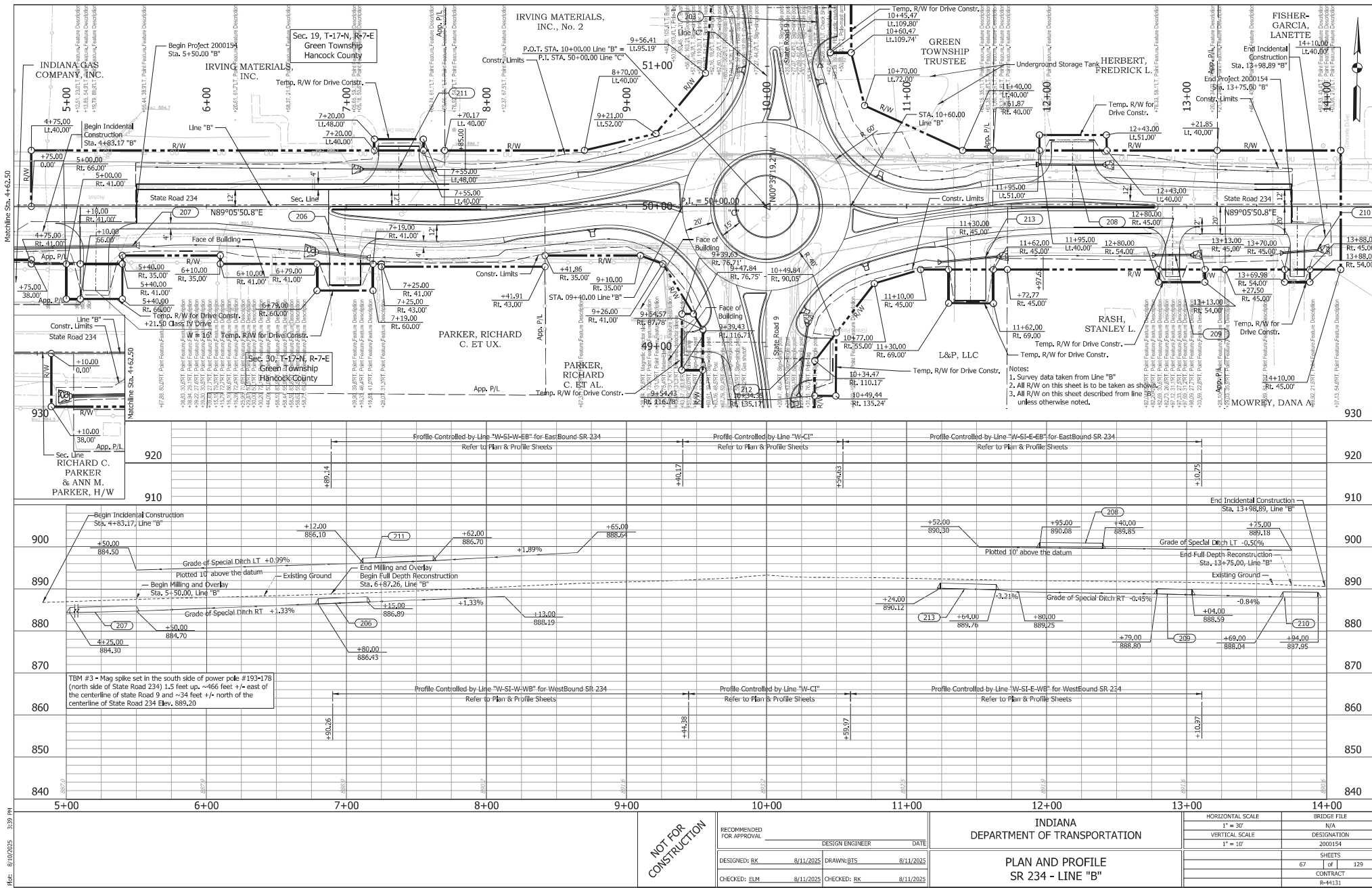
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Model: Sheet02



NOT FOR CONSTRUCTION	RECOMMENDED FOR APPROVAL _____		DESIGN ENGINEER _____ DATE _____		INDIANA DEPARTMENT OF TRANSPORTATION		HORIZONTAL SCALE 1" = 30'		BRIDGE FILE	
	DESIGNED: RK 8/11/2025		DRAWN: BTS 8/11/2025				VERTICAL SCALE 1" = 10'		DESIGNATION 2000154	
	CHECKED: ELM 8/11/2025		CHECKED: RK 8/11/2025		PLAN AND PROFILE SR 9 - LINE "C"		SHEETS 66 of 129		CONTRACT R-44131	

FILE: 8/10/2025 3:39 PM

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Model: C - Plan 1 [Sheet]



File: L:\IND001\20070994\07090407\Drawings\990407_Container\SR234\Profile_P_30.dgn
Model: B - Plan 1 [Sheet]

NOT FOR
CONSTRUCTION

RECOMMENDED FOR APPROVAL		
DESIGNED: RK	8/11/2025	DATE
CHECKED: ELM	8/11/2025	CHECKED: RK

INDIANA
DEPARTMENT OF TRANSPORTATION

PLAN AND PROFILE
SR 234 - LINE "B"

HORIZONTAL SCALE		BRIDGE FILE	
1" = 30'		N/A	
VERTICAL SCALE		DESIGNATION	
1" = 10'		2000154	
		SHEETS	
		67	of 129
		CONTRACT	
		R-44131	



INDIANA DEPARTMENT
OF TRANSPORTATION

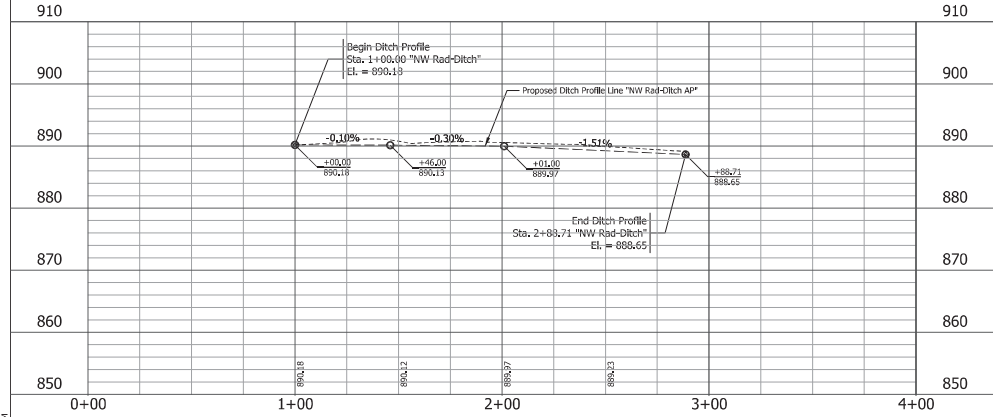
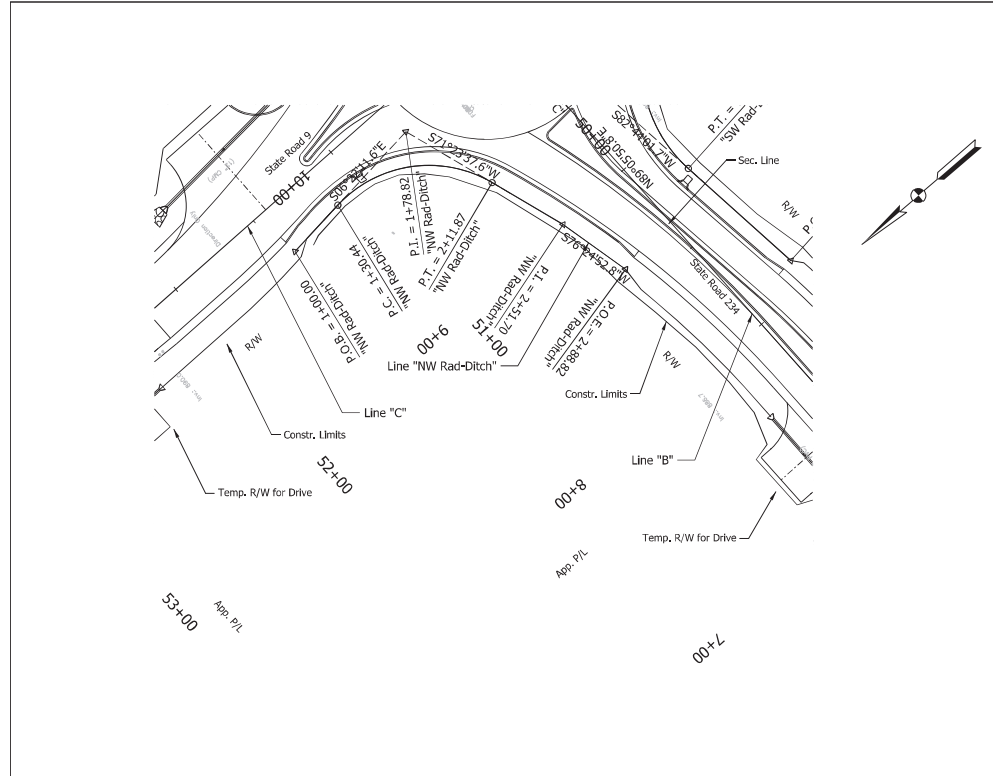
MOT PHASE 01
TEMPORARY DRIVE TO FIRE STATION

SCALE		BRIDGE FILE	
HOR : 1" = 30'		N/A	
		DESIGNATION	
		2000154	
SURVEY BOOK		SHEETS	
ELECTRONIC	20	of	94
CONTRACT	PROJECT		
Rd 1079	2000154		

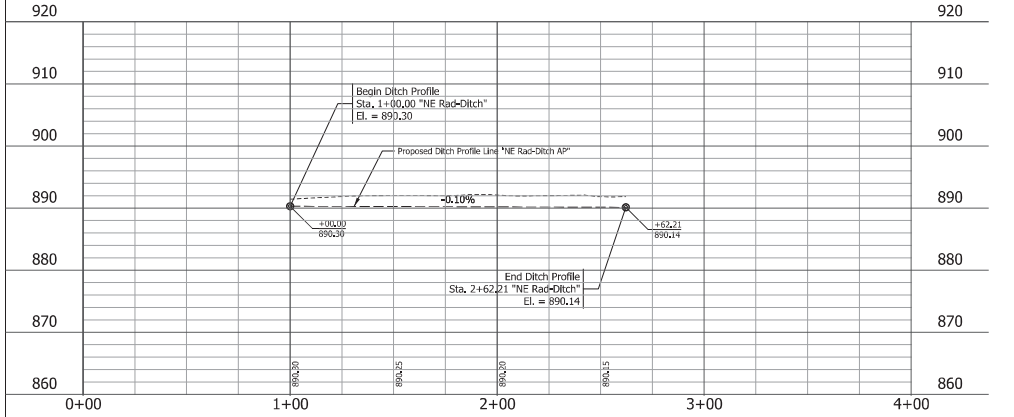
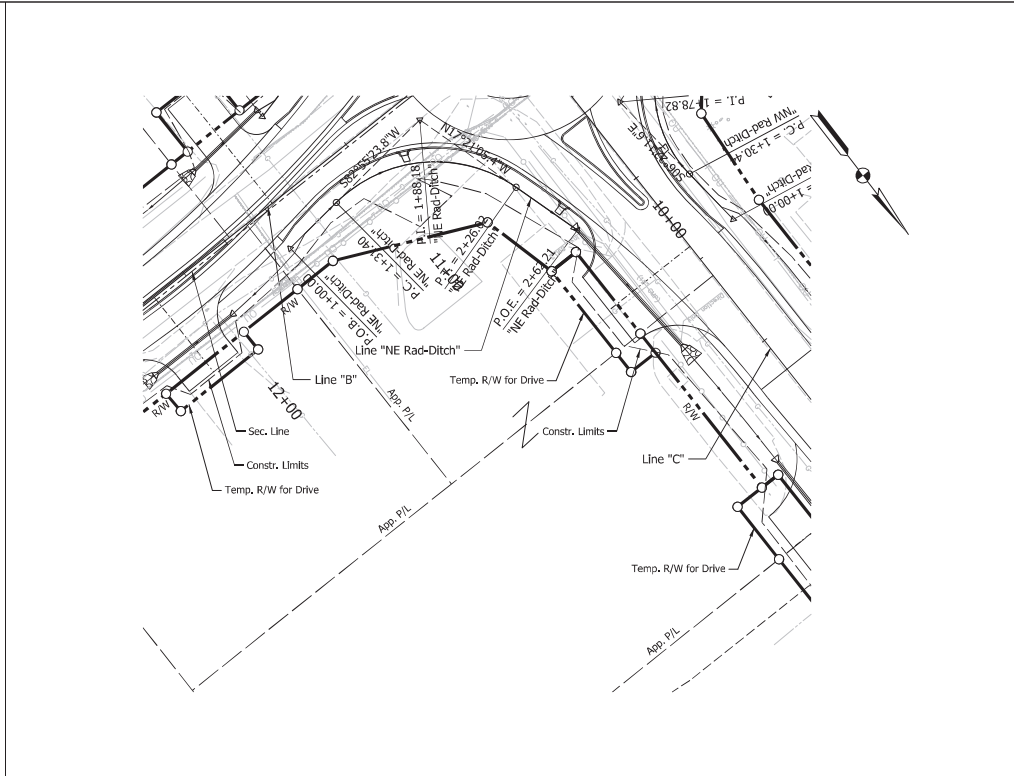
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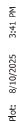
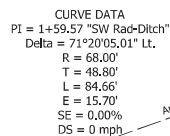
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Model: NW Rad-Ditch - Plan 1 (Sheet)



NOT FOR
CONSTRUCTION



RECOMMENDED FOR APPROVAL		DESIGN ENGINEER		DATE		INDIANA DEPARTMENT OF TRANSPORTATION		HORIZONTAL SCALE		BRIDGE FILE	
DESIGNED: RK		8/11/2025		DRAWN: BTS		8/11/2025		VERTICAL SCALE		DESIGNATION	
CHECKED: ELM		8/11/2025		CHECKED: RK		8/11/2025		1" = 10'		2000154	
										SHEETS	
										85 of 129	
										CONTRACT	
										R-44131	

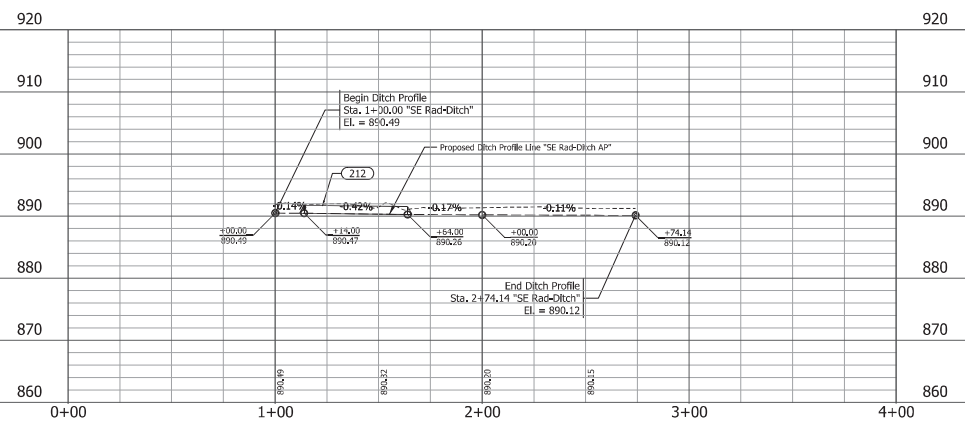


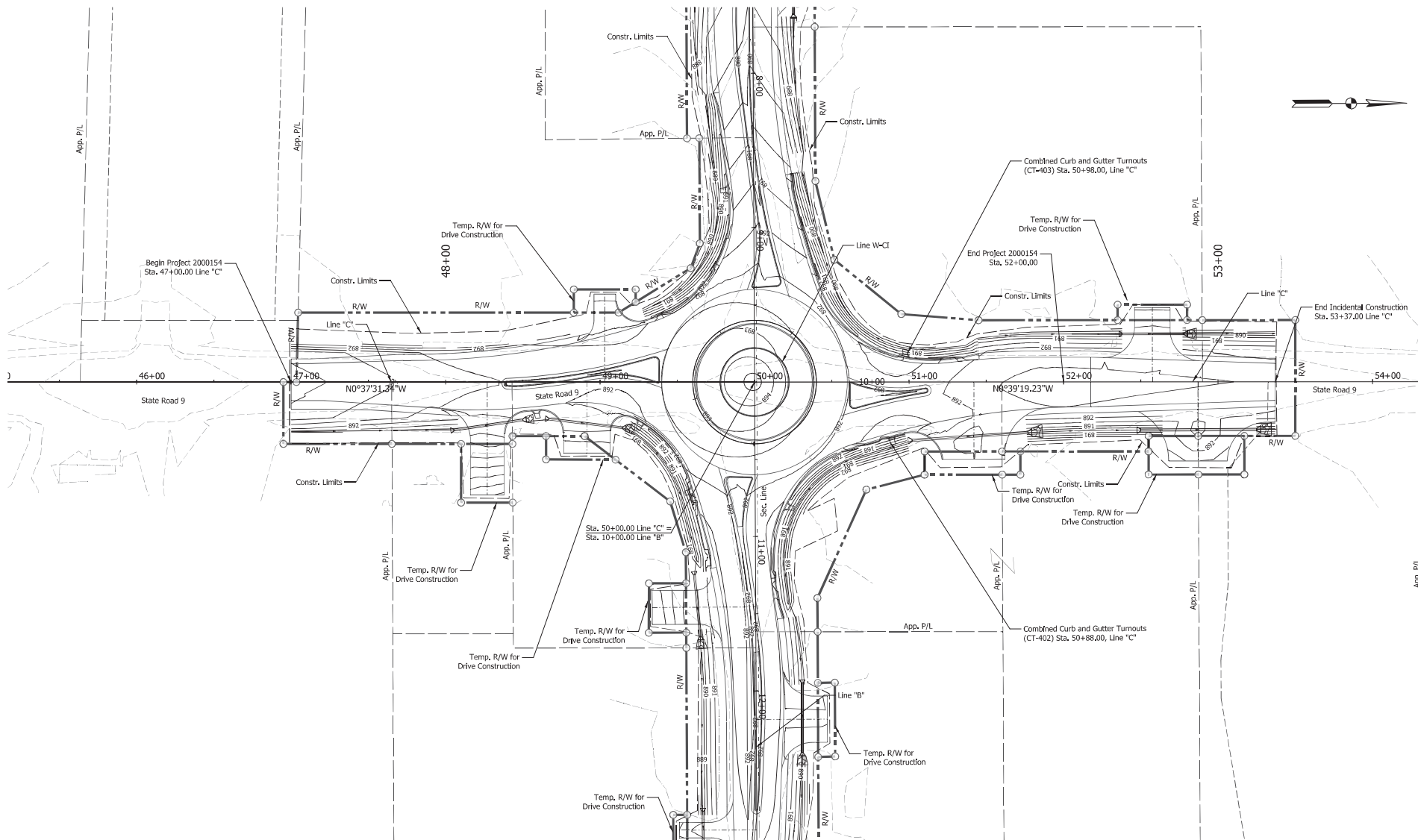
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Model: SW Rad-Ditch - Plan 1 (Sheet)

NOT FOR
CONSTRUCTION

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER		DATE
DESIGNED: <u>ACG</u>	<u>8/11/2025</u>	DRAWN: <u>ACG</u>	<u>8/11/2025</u>
CHECKED: <u>ELM</u>	<u>8/11/2025</u>	CHECKED: <u>RK</u>	<u>8/11/2025</u>

HORIZONTAL SCALE	BRIDGE FILE		
1" = 30'	N/A		
VERTICAL SCALE	DESIGNATION		
1" = 10'	2000154		
	SHEETS		
	86	of	129
	CONTRACT		
	R-44131		





File: 8/10/2025 3:41 PM

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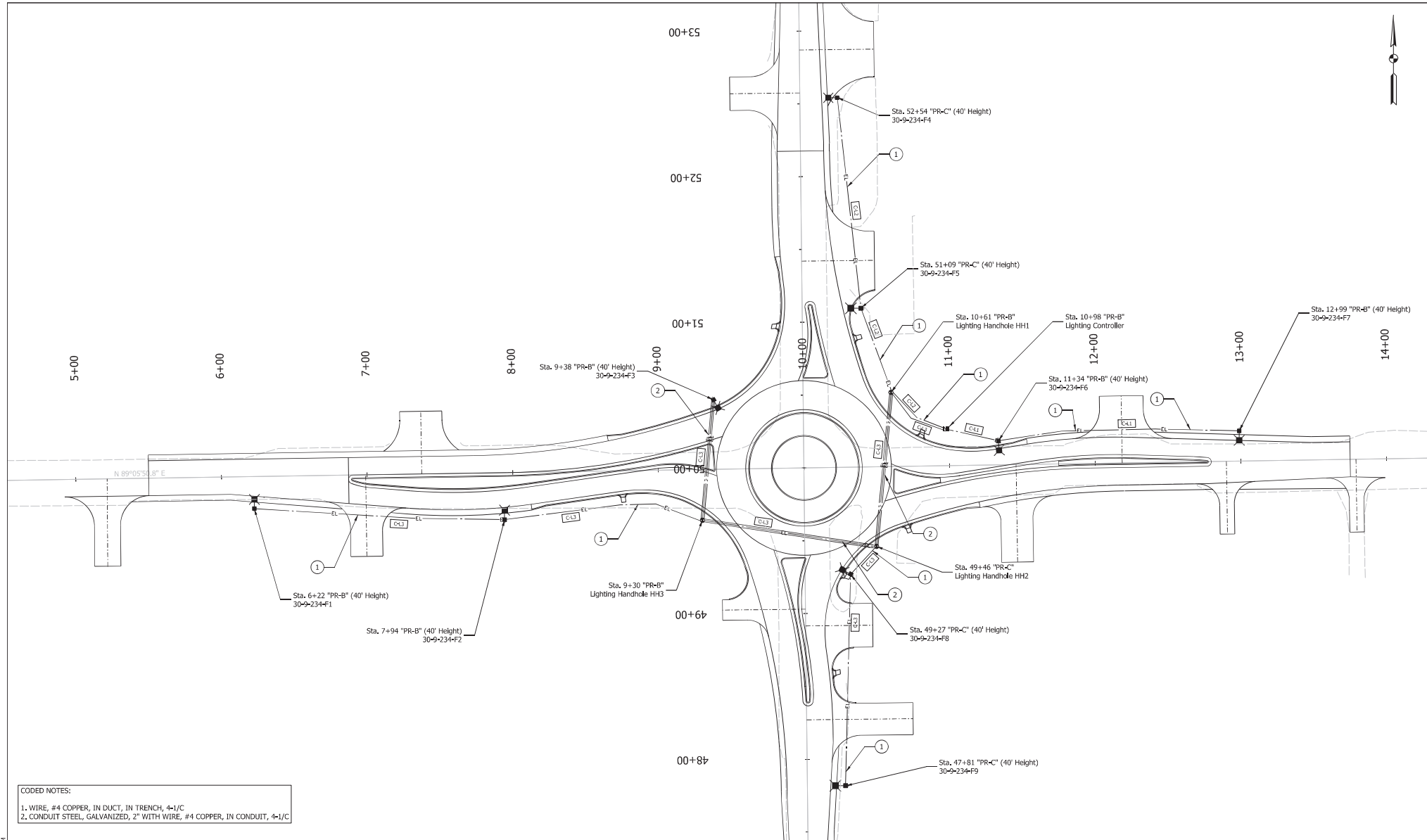
NOT FOR
CONSTRUCTION

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: ACG	8/11/2025	DRAWN: ACG
CHECKED: ELM	8/11/2025	CHECKED: BK

INDIANA
DEPARTMENT OF TRANSPORTATION

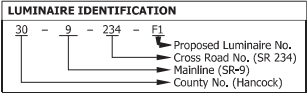
GRADING DETAILS
SR 9 - LINE "C"

HORIZONTAL SCALE	BRIDGE FILE
1" = 30'	N/A
VERTICAL SCALE	DESIGNATION
N/A	2000154
	SHEETS
	87 of 129
	CONTRACT
	R-44131



CODED NOTES:
1. WIRE, #4 COPPER, IN DUCT, IN TRENCH, 4-1/C
2. CONDUIT STEEL, GALVANIZED, 2" WITH WIRE, #4 COPPER, IN CONDUIT, 4-1/C

- Light Standard & Foundation W/ 194 Watt LED, Height 40' Mounted on a Breakaway Transformer Base
- Controller, Cabinet
- Lighting Handhole
- Service Point
- Lighting Circuit



NOT FOR
CONSTRUCTION

RECOMMENDED FOR APPROVAL		DESIGN ENGINEER	DATE
DESIGNED: RK	8/11/2025	DRAWN: BTS	8/11/2025
CHECKED: ELM	8/11/2025	CHECKED: RK	8/11/2025

INDIANA
DEPARTMENT OF TRANSPORTATION

**LIGHTING PLANS
SR 9 AT SR 234**

HORIZONTAL SCALE	BRIDGE FILE
1" = 30'	N/A
VERTICAL SCALE	DESIGNATION
N/A	2000154
SHEETS	
109	of 129
CONTRACT	
R-44131	

FILE: 8/10/2025 3:43 PM
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Model: Sht_Lighting_30_01 [Sheet]

SR 9 and SR 234 Intersection Improvement

APPENDIX C: EARLY COORDINATION





INDIANA DEPARTMENT OF TRANSPORTATION

100 North Senate Avenue
Room N758-ES
Indianapolis, Indiana 46204

PHONE: (855) 463-6848
(855) INDOT4U

Eric Holcomb, Governor
Michael Smith, Commissioner

December 11, 2024

Coordination with IDEM - Institutional Controls was completed via email and is attached within the appendices below

{ See Attached List }

Re: SR 9 and SR 234 Intersection
Improvements

Hancock County, Indiana
INDOT Des No.: 2000154
CMT Project No.: 20070904.07

Dear Interested Party:

The Indiana Department of Transportation (INDOT), with federal funding, intends to proceed with a project involving intersection improvements in Hancock County, Indiana. This letter is part of the early coordination phase of the environmental review process. We are requesting comments from your area of expertise regarding any possible environmental effects associated with this project. **Please use the above designation numbers and description in your reply.** We will incorporate your comments into a study of the project's environmental impacts.

Project Description

This project is located at the intersection of State Road (SR) 9 and SR 234 in the town of Eden, Hancock County, Indiana. The project will extend approximately 0.07 mile north and 0.06 mile south of the intersection along SR 9. It will also extend approximately 0.08 mile east and 0.10 mile west of the intersection along SR 234. The project is located in Sections 19 and 30, Township 17 North, Range 7 East on the United States Geological Survey (USGS) Ingalls, Indiana Quadrangle.

SR 9 is classified as a Minor Arterial and travels north/south through the project area. SR 234 is classified as a Major Collector and travels east/west through the project area. SR 9 is also included in the National Truck Network. SR 9 and SR 234 have posted speed limits between 40 and 55 mph throughout the project area. A 30 mph school zone is located in the northern leg of the project area. SR 9 and SR 234 consist of one lane traffic in each direction with no curb or sidewalk. The intersection is currently four-way stop controlled with flashing beacons. This intersection experiences long queues as well as a high frequency of crashes. Many of the crashes within the project area are rear end crashes related to queues.

The current proposed project will convert the existing stop-controlled intersection to a compact, single lane roundabout. A mill and overlay will occur along the approaches to the roundabout. A compact roundabout intersection is anticipated to reduce crashes at the intersection by at least 49% and minimize the need for right-of-way acquisition.

Permanent right-of-way will be needed for the project totaling approximately 3.4 acres. Temporary right-of-way will be needed for the project totaling 0.1 acres for driveway reconstruction. The proposed method of traffic maintenance is anticipated to require full closure of SR 234 with a detour. Construction along

SR 9 will utilize phased construction to avoid a full closure. One lane of traffic will be maintained using a temporary signal. Removal of individual trees may be required for the project and should total less than 0.5 acre throughout the project area. The project is anticipated to begin construction in the spring of 2025.

The area in the vicinity of the project is generally rural. Medium intensity commercial and residential development is present in the immediate surrounding area. Crawford, Murphy and Tilly, Inc. (CMT) will perform a waters and wetlands determination to identify any water resources that may be present within the study area. No streams or wetlands have been previously identified within the project area. 401/404 Permits may be required for this project. A DNR Construction in a Floodway Permit is not anticipated to be required for this project. This project is anticipated to qualify for the USFWS Rangewide Programmatic Agreement for the Indiana bat and Northern long-eared bat by completing the USFWS's Information for Planning and Consultation (IPaC).

Coordination will occur with INDOT Cultural Resources Office (CRO) to evaluate the project area for archaeological and historic resources and for Section 106 compliance. The results of this investigation will be forwarded to the State Historic Preservation Officer (SHPO) for review and concurrence as appropriate. No previously recorded historic properties are located within or adjacent to the project area. INDOT will ensure compliance with Section 106 of the National Historic Preservation Act of 1966.

Should we not receive a response **within thirty (30) calendar days** from the date of this letter, it will be assumed that your agency feels that there will be no adverse effects incurred as a result of the proposed project. However, should you find that an extension to the response time is necessary; a reasonable amount may be granted upon request.

If you have any questions or concerns regarding this matter, please feel free to contact Donald Mcghghy at DMcghghy@indot.IN.gov or contact me at 317.492.9162 or via email at nbatta@cmtengr.com. Thank you in advance for your input.

Sincerely,

Crawford, Murphy & Tilly, Inc.



Nick Batta
Project Manager

Attachments-
Maps (Location, Aerial, USGS Topographic)
Photographs
Early Coordination Letter Recipients



INDIANA DEPARTMENT OF TRANSPORTATION

100 North Senate Avenue
Room N758-ES
Indianapolis, Indiana 46204

PHONE: (855) 463-6848
(855) INDOT4U

Eric Holcomb, Governor
Michael Smith, Commissioner

The following agencies received Early Coordination Letters sent March 14 and 15, 2023, and December 11, 2024:

Robin McWilliams
Field Supervisor
U.S. Fish and Wildlife Service
Bloomington Indiana Field Office
620 South Walker Street
Bloomington, Indiana 47403-2121
robin_mcwilliams@fws.gov

Kari Carmany-George
Federal Highway Administration
Federal Office Building, Room 254
575 North Pennsylvania Street
Indianapolis, Indiana 46204
k.carmanygeorge@dot.gov

Indiana Geological and Water Survey
611 North Walnut Grove
Bloomington, Indiana 47405
Early Coordination submittal at
<https://igws.indiana.edu/eAssessment/>

Environmental Coordinator
Indiana Department of Natural Resources
Division of Fish and Wildlife
402 West Washington Street, Rm W273
Indianapolis, Indiana 46204
environmentalreview@dnr.in.gov

Utilize the IDEM's Wellhead Proximity
Determinator website
<https://www.in.gov/idem/cleanwater/pages/wellhead/>

Jay Turner
Randy Braun
Section Chief, Wetlands and Stormwater
Programs

Indiana Department of Environmental
Management
100 N. Senate Avenue
Indianapolis, IN 46204
JTurner2@idem.in.gov
rbraun@idem.in.gov

Field Environmental Officer
Chicago Regional Office
US Department of Housing & Urban
Development
Metcalf Fed. Bldg.
77 W. Jackson Blvd. Rm 2401
Chicago, IL 60604
erik.r.sandstedt@hud.gov

Regional Environmental Coordinator
Midwest Regional Office
National Park Service
601 Riverfront Drive
Omaha, Nebraska 68102
mwro_compliance@nps.gov

Donald Mcghghy
Indiana Department of Transportation
Project Manager - Greenfield District
32 South Broadway
Greenfield, IN 46140
dmcghghy@indot.in.gov

Brian Royer
Orphan Well Manager
Indiana Department of Natural Resources
Division of Oil & Gas
402 W. Washington Street, Room W293
Indianapolis, Indiana 46204
BRoyer@dnr.in.gov

Natalie Garrett
Strategic Communications Director
Indiana Department of Transportation
100 N. Senate Avenue IGCN Room N758
IGCN Room N758
Indianapolis, IN 46204
nagarrett@indot.IN.gov

Ms. Deborah Snyder
US Army Corps of Engineers,
Louisville District, Indianapolis Regulatory
Office,
Indianapolis, IN 46216
RegulatoryApplicationsLRL@usace.army.mil

Anna Gremling, Executive Director
Indianapolis Metropolitan Planning
Organization
200 E. Washington Street
Indianapolis, IN 46204
anna.gremling@indympo.org

Sean Northup, Director of Transportation
Planning
Indianapolis Metropolitan Planning
Organization
200 E. Washington Street
Indianapolis, IN 46204
sean.northup@indympo.org

Randy Moore
Hancock County Highway Superintendent
921 W Osage Street
Greenfield, IN 46140
rmoore@hancockcoingov.org

Gary Pool, P.E.
Hancock County Engineer
921 W Osage Street
Greenfield, IN 46140
gpool@hancockcoingov.org

Chad Coughenour, Surveyor
Hancock County Surveyor's Office

111 S American Legion Place
Greenfield, IN 46140
ccoughenour@hancockcoingov.org

Trish George
Administrator for the Commissioners of
Hancock County
111 S American Legion Place Suite 219
Greenfield, IN 46140
tgeorge@hancockcoingov.org

John Jessup
Hancock County Board Commissioner of Green
Township
111 S American Legion Place Suite 219
Greenfield, IN 46140
john@hancockcounty.in

Robin Lowder, At Large
Hancock County Councilor
111 American Legion Place
Greenfield, IN 46140
rlowder@hancockcoingov.org

Jeannine Gray, District 1
Hancock County Councilor
111 American Legion Place
Greenfield, IN 46140
Jeannine4HancockCounty@gmail.com

Dr. Gary Sharp, Health Officer
Hancock County Health Department
111 American Legion Place, Suite 150
Hancock County Annex
Greenfield, IN 46140

John Jokantas
Hancock County, 911 Director
640 S. Franklin Street
Greenfield, IN 46140
911Director@hancockcoingov.org

Jason Horning, Fire Chief
City of Greenfield Fire Department

17 W South Street
Greenfield, IN 46140
jashorning@greenfieldin.org

R.J. Beaver
Division Chief of EMS
17 W South Street
Greenfield, IN 46140
rjbeaver@greenfieldin.org

Green Township Fire Department
8031 North State Road 9
Greenfield, IN 46140

Brad Burkhardt, Sheriff
Hancock County Sheriff's Department
398 Malcolm Grass Way
Greenfield, IN 46140
pweiler@hancockcoingov.org

Brian Hartman, Chief of Police
Greenfield Police Department
116 S State Street
Greenfield, IN
bhartman@greenfieldin.org

Patrick Bratton, Chief of Police
Town of Fortville

John Allen
State Conservationist
Natural Resources Conservation Service
6013 Lakeside Boulevard
Indianapolis, Indiana 46278
john.allen@usda.gov

714 E Broadway
Fortville, IN 46040
pbratton@fortvilleindiana.org

Dr. Harold Olin, Superintendent of Schools
Greenfield-Central Administration Center
110 West North Street
Greenfield, IN 46140
holin@gcsc.k12.in.us

Todd Prazeau, Transportation Director
Greenfield-Central Transportation
102 North Main Street
Maxwell, IN 46154
tprazeau@gcsc.k12.in.us

Melia Hammons, Principal
Eden Elementary School
8185 N State Road 9
Greenfield, IN 47140
mhammons@gcsc.k12.in.us

Sam Anderson
Utilities and Railroad Director
100 N. Senate Avenue IGCN 758 – UTI/RR
Indianapolis, IN 46204
saanderson2@indot.in.gov



INDIANA GEOLOGICAL & WATER SURVEY

INDIANA UNIVERSITY

Organization and Project Information

Project ID: 20070904.07
Des. ID: 2000154
Project Title: SR 9 and SR 234 Intersection Improvements
Name of Organization: CMT
Requested by: Megan Ottenweller

Environmental Assessment Report

1. Geological Hazards:
 - High liquefaction potential
 - 1% Annual Chance Flood Hazard
2. Mineral Resources:
 - Bedrock Resource: High Potential
 - Sand and Gravel Resource: High Potential
3. Active or abandoned mineral resources extraction sites:
 - Petroleum Exploration Wells

*All map layers from Indiana Map (maps.indiana.edu)

DISCLAIMER:

This document was compiled by Indiana University, Indiana Geological Survey, using data believed to be accurate; however, a degree of error is inherent in all data. This product is distributed "AS-IS" without warranties of any kind, either expressed or implied, including but not limited to warranties of suitability to a particular purpose or use. No attempt has been made in either the design or production of these data and document to define the limits or jurisdiction of any federal, state, or local government. The data used to assemble this document are intended for use only at the published scale of the source data or smaller (see the metadata links below) and are for reference purposes only. They are not to be construed as a legal document or survey instrument. A detailed on-the-ground survey and historical analysis of a single site may differ from these data and this document.

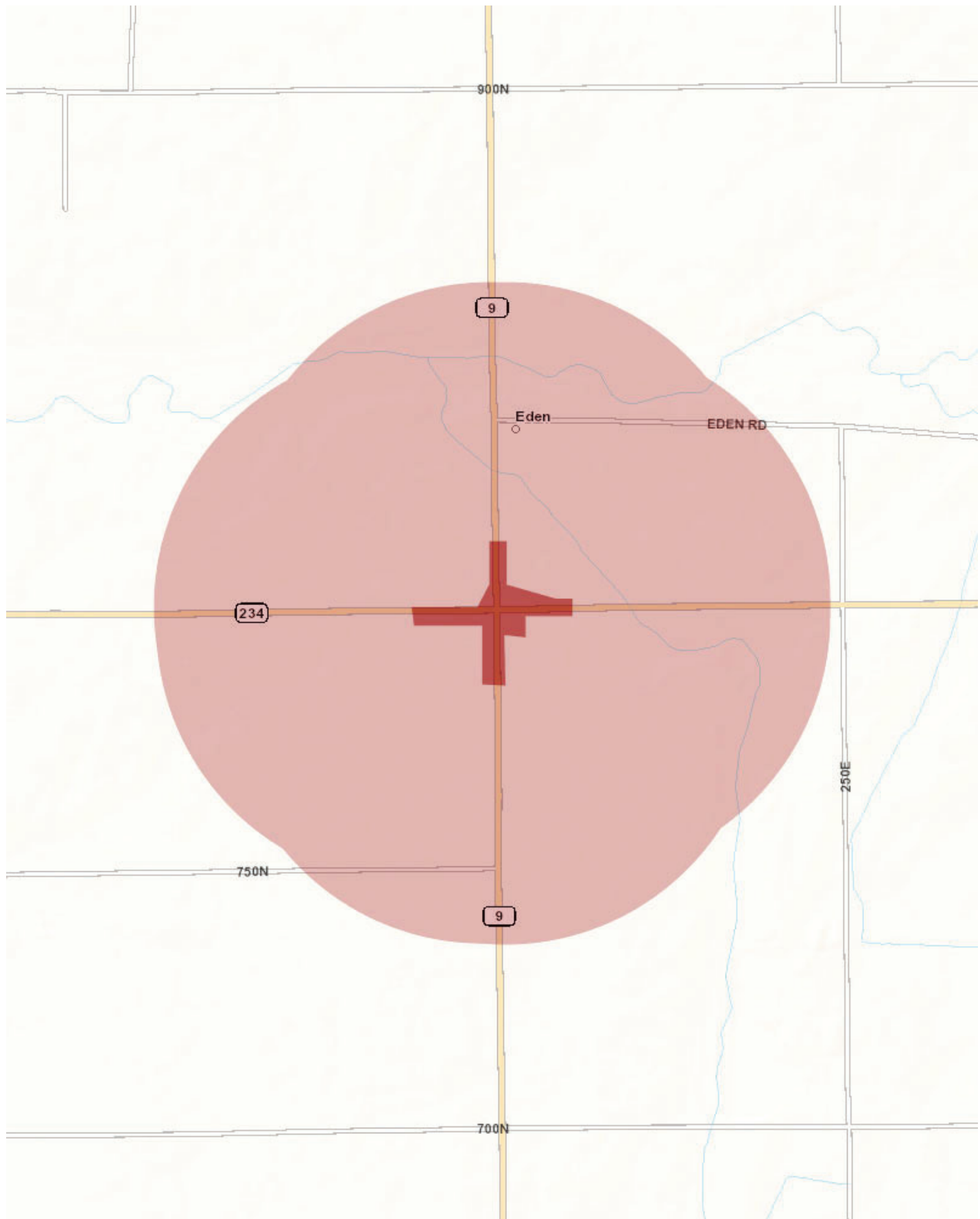
This information was furnished by Indiana Geological Survey

Address: 1001 E. 10th St., Bloomington, IN 47405

Email: IGSEnvir@indiana.edu

Phone: 812 855-7428

Date: March 15, 2023



Metadata:

- https://maps.indiana.edu/metadata/Geology/Petroleum_Wells.html
- https://maps.indiana.edu/metadata/Geology/Seismic_Earthquake_Liquefaction_Potential.html
- https://maps.indiana.edu/metadata/Geology/Industrial_Minerals_Sand_Gravel_Resources.html
- https://maps.indiana.edu/metadata/Hydrology/Floodplains_FIRM.html
- https://maps.indiana.edu/metadata/Geology/Bedrock_Geology.html



Outlook

FW: Institutional Controls Coordination- SR 9 and 234 Intersection Improvement project (Des No. 2000154)

From Haulter, Crystal <CHaulter@idem.IN.gov>

Date Fri 9/27/2024 2:06 PM

To Austin Clarridge <aclarridge@cmtengr.com>

Cc Nick Batta <nbatta@cmtengr.com>

 1 attachments (2 MB)

Haz Mat Concerns RFI.pdf;

External Message: This email was sent from someone outside of CMT. Please use caution with links and attachments from unknown senders or receiving unexpected emails.

Good afternoon Austin,

Thank you for your message regarding Des No. 2000154, SR 9 and 234 Intersection Improvement project.

IDEM's Institutional Controls Group does not have any concerns with the proposed work as it pertains to the limited groundwater use restriction in the Environmental Restrictive Covenant (ERC) for 7987 North SR 9, provided that proper handling, removal, and disposal of potentially contaminated soil and groundwater is conducted in accordance with applicable laws, when and where appropriate. The ERC also indicates a potable well is present on the property, which may need to be abandoned if it is still present. The location of the well is depicted in Exhibit B of the ERC (VFC #[80270649](#)).

No further coordination is anticipated. Please contact me directly if you have any questions regarding this matter.

Thank you,
Crystal Haulter

Crystal Haulter

(She/Her/Hers)

Technical Environmental Specialist

Institutional Controls Group, Remediation Services Branch

(317) 234-1957 • chaulter@idem.IN.gov

| | | | | www.idem.IN.gov

From: Austin Clarridge <aclarridge@cmtengr.com>

Sent: Tuesday, September 24, 2024 4:25 PM

To: IDEM Institutional Controls <InstitutionalControls@idem.IN.gov>

Cc: Nick Batta <nbatta@cmtengr.com>

Subject: Institutional Controls Coordination- SR 9 and 234 Intersection Improvement project (Des No. 2000154)

Good afternoon

The SR 9 and 234 Intersection Improvement project (Des No. 2000154) will construct a compact single lane roundabout at the intersection of SR 9 and SR 234 in Hancock County. During the completion of the RFI, a former LUST site and IC site was identified within the project limits. The current Crossroads Motors/former Pavey Eden Grocery (AI ID 33596) property is located at 7987 N SR 9, Fortville, IN 46040. This property will be acquired for construction of the project. We are reaching out to your office based on the RFI recommendation. Copied below is the discussion from the RFI, developed from a review of applicable VFC documents. Attached is an aerial map of the project area and the mapped boundary of the subject parcel.

Thank you and we look forward to your input on this project

Leaking Underground Storage (LUST) Sites: One (1) LUST site is located within the 0.5 search radius. The site, Former Pavey Eden Grocery, 7987 N SR 9, AI ID #33596, is located within the project area at the southeast quadrant of the intersection and will be acquired as part of the project. On December 16, 2003, three USTs were removed from this site: a 3,000 gallon gasoline tank, a 1,000 gallon diesel tank, and a 1,000 gallon gasoline tank. Following removal activities, an analysis of soil samples indicated the release of petroleum on the site. Analysis of soil samples during an initial site characterization indicated that Total Petroleum Hydrocarbon (TPH) concentrations at all boring locations exceeded the applicable IDEM screening levels. Additional sampling occurred through 2013 in order to delineate the extent of soil and groundwater contamination. It was determined that the contamination extends offsite to the north side of the property, including underneath SR 234. An Environmental Restrictive Covenant (ERC) was recorded for the site on December 17, 2015. Please see the Institutional Controls section below for more information regarding the ERC. On March 31, 2016 IDEM issued an No Further Action (NFA) Determination Pursuant to Remediation Closure Guide for the site. Monitoring well abandonment occurred on March 21, 2016. If excavation occurs in this area, it is possible that petroleum contamination will be encountered. Proper handling, removal, and disposal of soil and/or groundwater may be necessary. Coordination will be conducted with the IDEM Institutional Controls section (institutionalcontrols@idem.IN.gov) before RFC. Refer to Appendix G of the SAM Manual for the recommended procedure to manage and report contamination.

Institutional Controls: One (1) institutional control site is located within the 0.5 search radius. The site, Former Pavey Eden Grocery, 7987 N SR 9, AI ID #33596, is located within the project area at the southeast quadrant of the intersection and will be acquired as part of the project. IDEM issued a No Further Action Approval Determination Pursuant to Remediation Closure Guide on March 31, 2016. Soil and groundwater contamination remain on the site. An ERC was placed on the property on December 17, 2015. The ERC specifically prohibits the use or extraction of groundwater. If excavation occurs in this area, it is possible petroleum contamination will be encountered. Proper handling, removal, and disposal of soil and/or groundwater may be necessary. Coordination will be conducted with the IDEM Institutional Controls section (institutionalcontrols@idem.IN.gov) before RFC. Refer to Appendix G of the SAM Manual for the recommended procedure to manage and report contamination.

AUSTIN CLARRIDGE | Project Environmental Scientist



**State of Indiana
DEPARTMENT OF NATURAL RESOURCES
Division of Fish and Wildlife
Early Coordination/Environmental Assessment**

DNR#: ER-25466

Request Received: March 14, 2023

Requestor:

Morgan Stumpf
Crawford, Murphy & Tilly, Inc
8790 Purdue Road
Indianapolis, IN 46268

Project:

SR 9 & SR 234 roundabout construction, town of Eden; CMT #20070904.07, Des #2000154

County/Site Info: Hancock

The Indiana Department of Natural Resources has reviewed the above referenced project per your request. Our agency offers the following comments for your information and in accordance with the National Environmental Policy Act of 1969.

If our agency has regulatory jurisdiction over the project, the recommendations contained in this letter may become requirements of any permit issued. If we do not have permitting authority, all recommendations are voluntary.

Regulatory Assessment:

Formal approval by the Department of Natural Resources under the regulatory programs administered by the Division of Water is not required for this project.

Natural Heritage Database:

The Natural Heritage Program's data have been checked. The following freshwater mussel species have been documented in Sugar Creek and within 0.5 miles of the project area.

- Clubshell (*Pleurobema clava*), State endangered
- Slippershell (*Alasmodonta viridis*), State special concern
- Purple Lilliput (*Toxolasma lividus*), State special concern
- Wavyrayed Lampmussel (*Lampsilis fasciola*), State special concern
- Kidneyshell (*Ptychobranhus fasciolaris*), State special concern
- Little Spectaclecase (*Villosa lienosa*), State special concern

Fish and Wildlife Comments:

Since no in-stream work is proposed, no impacts to the above-mentioned mussels should occur.

Avoid and minimize impacts to fish, wildlife, and botanical resources to the greatest extent possible, and compensate for impacts. The following are recommendations that address potential impacts identified in the proposed project area:

A) Pavement Rehabilitation:

Pavement rehabilitation projects typically do not have a significant impact on fish, wildlife, and botanical resources as long as best management practices (BMPs) are in place to limit the migration of Polycyclic aromatic hydrocarbons (PAHs) into local waterways. PAHs are a byproduct of asphalt and coal tar based

sealants and have a negative impact on aquatic and semi-aquatic species. The use of sealants that are free of petroleum and coal tar based products is encouraged whenever possible. Contaminated road runoff can significantly impact the aquatic environment through increased turbidity and release of sediments into the stream that can be harmful to fish and other aquatic organisms, their eggs, and their food supply. Where possible, road runoff should be directed to riprap turnouts and sediment filtration prior to entering a stream to reduce impacts to aquatic species. We recommend the use of pollutant trapping technology such as storm drain inserts, etc. to reduce the runoff of roadside pollutants.

B) Drainage and Stormwater Management:

The Division of Fish and Wildlife recommends considering a more sustainable approach to stormwater management. The traditional model of stormwater management aims to drain runoff as quickly as possible with the help of channels and pipes, which increases peak flows and costs of stormwater management. This type of solution only transfers drainage problems from one section of a basin to another. A more sustainable approach should aim to rebuild the natural water cycle by using storage techniques (retention basins, constructed wetlands, raingardens, etc.) and recharging groundwater using infiltration techniques (infiltration basins or trenches, pervious pavement, etc.). The following links give a good overview of traditional and sustainable stormwater management systems and their pros and cons for consideration during the design of the proposed project: <https://www.epa.gov/greeningepa/epa-facility-stormwater-management>; <https://www.epa.gov/greeningepa/stormwater-management-practices-epa-facilities>

C) Street Trees:

The Division of Fish and Wildlife recommends avoiding removing urban trees to the greatest extent possible and replacing trees that must be removed. Street trees are important to fish and wildlife resources in urban areas. Indiana's street trees also provide millions of dollars of tangible benefits to Indiana communities by their presence in the urban environment. Their shade and beauty contribute to the quality of life. They provide significant increases in real estate values, create attractive settings for commercial businesses, and improve community neighborhood appeal. Trees decrease energy consumption by providing shade and acting as windbreaks. They reduce water treatment costs and impede soil erosion by slowing the runoff of stormwater. Trees also cool the air temperature, cleanse pollutants from the air, and produce oxygen while absorbing carbon dioxide. Trees are an integral component of the urban environment. Proactively managing and maintaining a street tree population will ultimately maximize the benefits afforded by their aesthetic and ecological functions. The following links give a good overview of the benefits of a street tree program and how to select the right species to avoid the negative impacts of non-native invasive species such as the common and popular Bradford pear: <https://www.in.gov/dnr/forestry/forestry-publications-and-presentations/> (scroll down to the Community & Urban Forestry section).

The additional measures listed below should be implemented to avoid, minimize, or compensate for impacts to fish, wildlife, and botanical resources:

1. Revegetate all bare and disturbed areas that are not currently mowed and maintained with a mixture of grasses, sedges, and wildflowers native to Central Indiana as soon as possible upon completion; turf-type grasses (including low-endophyte, friendly endophyte, and endophyte free tall fescue but excluding all other varieties of tall fescue) may be used in currently mowed areas only. A native herbaceous seed mixture must include at least 5 species of grasses and sedges and 5 species of wildflowers.
2. Appropriately designed measures for controlling erosion and sediment must be implemented to prevent sediment from entering the waterbody or leaving the construction site; maintain these measures until construction is complete and all disturbed areas are stabilized.
3. Seed and protect all disturbed streambanks and slopes not protected by other methods that are 3:1 or steeper with erosion control blankets that are heavy-duty, biodegradable, and net free or that use loose-woven / Leno-woven netting to minimize the entrapment and snaring of small-bodied wildlife such as snakes and turtles (follow manufacturer's recommendations for selection and installation); seed and apply mulch on all other disturbed areas.

Contact Staff:

Our agency appreciates this opportunity to be of service. Please contact me at mbuffington@dnr.in.gov or (317) 233-4666 if we can be of further assistance.

Matt Buffington
Matt Buffington
Environmental Unit Supervisor
Division of Fish and Wildlife

Date: April 13, 2023

December 26, 2024

Alex Tadda
Crawford, Murphy & Tilly
8790 Purdue Road
Indianapolis, Indiana 46268
atadda@cmtengr.com

Dear Alex Tadda:

The proposed SR 9 and SR 234 Intersection Improvements (Des No. 2000154) in Hancock County, Indiana as referred to in your letter received December 11, 2024, will cause a conversion of prime farmland.


The attached packet of information is for your competing Parts VI and VII of the AD-1006. After completion, the federal agency needs to forward one copy to NRCS for our records.

If you need additional information, please contact John Allen at 317-295-5859 or john.allen@usda.gov

Sincerely,

JOHN ALLEN

JOHN ALLEN
State Soil Scientist

 Digitally signed by JOHN ALLEN
Date: 2024.12.30 10:49:23 -05'00'

Enclosures

FARMLAND CONVERSION IMPACT RATING

PART I (To be completed by Federal Agency)		Date Of Land Evaluation Request 12/11/2024				
Name of Project Des No. 2000154 SR 9 and SR 234 Inter		Federal Agency Involved Indiana Department of Transportati				
Proposed Land Use Round-a-bout		County and State Hancock County, Indiana				
PART II (To be completed by NRCS)		Date Request Received By NRCS		Person Completing Form: JRA		
Does the site contain Prime, Unique, Statewide or Local Important Farmland? (If no, the FPPA does not apply - do not complete additional parts of this form)		YES <input checked="" type="checkbox"/>	NO <input type="checkbox"/>	Acres Irrigated	Average Farm Size 365	
Major Crop(s) Corn	Farmable Land In Govt. Jurisdiction Acres: 191125% 97	Amount of Farmland As Defined in FPPA Acres: 18415% 94				
Name of Land Evaluation System Used LESA	Name of State or Local Site Assessment System	Date Land Evaluation Returned by NRCS				
PART III (To be completed by Federal Agency)		Alternative Site Rating				
		Site A	Site B	Site C	Site D	
A. Total Acres To Be Converted Directly		0.12				
B. Total Acres To Be Converted Indirectly		0				
C. Total Acres In Site		0.12				
PART IV (To be completed by NRCS) Land Evaluation Information						
A. Total Acres Prime And Unique Farmland		0.12				
B. Total Acres Statewide Important or Local Important Farmland		0.00				
C. Percentage Of Farmland in County Or Local Govt. Unit To Be Converted		<0.001				
D. Percentage Of Farmland in Govt. Jurisdiction With Same Or Higher Relative Value		40				
PART V (To be completed by NRCS) Land Evaluation Criterion Relative Value of Farmland To Be Converted (Scale of 0 to 100 Points)		83				
PART VI (To be completed by Federal Agency) Site Assessment Criteria (Criteria are explained in 7 CFR 658.5 b. For Corridor project use form NRCS-CPA-106)		Maximum Points	Site A	Site B	Site C	Site D
1. Area In Non-urban Use	(15)	14				
2. Perimeter In Non-urban Use	(10)	4				
3. Percent Of Site Being Farmed	(20)	1				
4. Protection Provided By State and Local Government	(20)	0				
5. Distance From Urban Built-up Area	(15)	15				
6. Distance To Urban Support Services	(15)	0				
7. Size Of Present Farm Unit Compared To Average	(10)	0				
8. Creation Of Non-farmable Farmland	(10)	0				
9. Availability Of Farm Support Services	(5)	3				
10. On-Farm Investments	(20)	10				
11. Effects Of Conversion On Farm Support Services	(10)	0				
12. Compatibility With Existing Agricultural Use	(10)	7				
TOTAL SITE ASSESSMENT POINTS		160	54	0	0	0
PART VII (To be completed by Federal Agency)						
Relative Value Of Farmland (From Part V)		100	83	0	0	0
Total Site Assessment (From Part VI above or local site assessment)		160	54	0	0	0
TOTAL POINTS (Total of above 2 lines)		260	137	0	0	0
Site Selected: Site A	Date Of Selection 05/12/2025	Was A Local Site Assessment Used? YES <input type="checkbox"/> NO <input checked="" type="checkbox"/>				
Reason For Selection:						
Name of Federal agency representative completing this form:					Date:	

(See Instructions on reverse side)

Form AD-1006 (03-02)

Megan Ottenweller

From: McWilliams, Robin <robin_mcwilliams@fws.gov>
Sent: Friday, May 12, 2023 11:15 AM
To: Megan Ottenweller
Subject: Re: [EXTERNAL] Early Coordination Letter: SR 9 and SR 234 Intersection Improvements #2000154

External Message: This email was sent from someone outside of CMT. Please use caution with links and attachments from unknown senders or receiving unexpected emails.

Dear Megan,

These comments have been prepared under the authority of the Fish and Wildlife Coordination Act (16 U.S.C. 661 et. seq.) and are consistent with the intent of the National Environmental Policy Act of 1969, the Endangered Species Act of 1973, and the U. S. Fish and Wildlife Service's Mitigation Policy.

The project is within the range of the Indiana bat (*Myotis sodalis*) and northern long-eared bat (*Myotis septentrionalis*; NLEB) and should follow the Indiana bat/NLEB Federal Highway Administration, Federal Rail Administration, and Federal Transit Administration's programmatic consultation process, if applicable (*i.e.* a federal transportation nexus is established). The Service has 14 days after a “not likely to adversely affect” determination letter is generated to review the project and provide additional comments or request additional information (there is no review for projects with a “no effect” determination); if you do not receive a response from us within 14 days, we have no additional comments. **Please note**, if tree-clearing extends beyond 100 feet of the edge of pavement, compensatory mitigation may be required, and, if beyond 300 feet, a stand alone informal consultation may be needed.

Other Species of Concern

Tricolored Bat

On September 14, 2022, the Service published a proposal in the Federal Register to list the tricolored bat (*Perimyotis subflavus*; TCB) as endangered under the Endangered Species Act (ESA). The Service has up to 12 months from the date the proposal was published to make a final determination, either to list the tricolored bat under the Act or to withdraw the proposal. The Service determined the bat faces extinction primarily due to the range-wide impacts of WNS. Because TCB populations have been greatly reduced due to WNS, surviving bat populations are now more vulnerable to other stressors such as human disturbance and habitat loss. Species proposed for listing are not afforded protection under the Act; however, as soon as a listing becomes effective (typically 30 days after publication of the final rule in the Federal Register), the prohibitions against jeopardizing its continued existence and “take” will apply. Therefore, if this project or other future or existing projects have the potential to adversely affect the TCB after the potential new listing goes into effect, we recommend that the effects of the project on TCBs and their habitat be analyzed to determine whether authorization under ESA section 7 or 10 is necessary. Projects or programs with an existing section 7 biological opinion may require reinitiation of consultation, and projects with an existing section 10 incidental take permit may require an amendment to provide uninterrupted authorization for covered activities. Contact your local U.S. Fish and Wildlife Service Ecological Services Office for assistance.

The TCB is a small insectivorous bat that typically overwinters in caves, abandoned mines and tunnels, and road-associated culverts (southern portion of the range) and spends the rest of the year in forested habitats, typically roosting among live and dead leaf clusters in tree branches. For more information on TCB and the

proposed rule, please see: <https://www.fws.gov/species/tricolored-bat-perimyotis-subflavus> and for more information on WNS, please see: <https://www.whitenosesyndrome.org/>

Wetland and stream impacts may require permits from the U.S. Army Corps of Engineers, the Indiana Department of Environmental Management's Water Quality Certification program, and the Indiana Department of Natural Resources. Wetland impacts should be avoided, and any unavoidable impacts should be compensated for in accordance with agency mitigation guidelines.

Based on a review of the information you provided, the U.S. Fish and Wildlife Service has no other comments on the projects as currently proposed. However, should new information arise pertaining to project plans or a revised species list be published, it will be necessary for the Federal agency to reinstate consultation. Standard recommendations are provided below.

We appreciate the opportunity to comment at this early stage of project planning. If you have any questions about our recommendations, please contact me at robin_mcwilliams@fws.gov or you may call 812-334-4261 x. 207.

Sincerely,
Robin McWilliams Munson

Standard Recommendations:

1. Do not clear trees or understory vegetation outside the construction zone boundaries. **(This restriction is not related to the "tree clearing" restriction for potential Indiana Bat habitat.)**
2. Restrict below low-water work in streams to placement of culverts, piers, pilings and/or footings, shaping of the spill slopes around the bridge abutments, and placement of riprap.
Culverts should span the active stream channel, should be either embedded or a 3-sided or open-arch culvert, and be installed where practicable on an essentially flat slope. When an open-bottom culvert or arch is used in a stream, which has a good natural bottom substrate, such as gravel, cobbles and boulders, the existing substrate should be left undisturbed beneath the culvert to provide natural habitat for the aquatic community.
3. Restrict channel work and vegetation clearing to the minimum necessary for installation of the stream crossing structure.
4. Minimize the extent of hard armor (riprap) in bank stabilization by using bioengineering techniques whenever possible. If riprap is utilized for bank stabilization, extend it below low-water elevation to provide aquatic habitat.
5. Implement temporary erosion and sediment control methods within areas of disturbed soil. All disturbed soil areas upon project completion will be vegetated following INDOT's standard specifications.
6. Avoid all work within the inundated part of the stream channel (in perennial streams and larger intermittent streams) during the fish spawning season (April 1 through June 30), except for work within sealed structures such as caissons or cofferdams that were installed prior to the spawning season. No equipment shall be operated below Ordinary High-Water Mark during this time unless the machinery is within the caissons or on the cofferdams.
7. Evaluate wildlife crossings under bridge/culverts projects in appropriate situations. Suitable crossings include flat areas below bridge abutments with suitable ground cover, high water shelves in culverts, amphibian tunnels and diversion fencing.

Robin McWilliams Munson
Fish and Wildlife Biologist
U.S. Fish and Wildlife Service
620 South Walker Street
Bloomington, IN 47403
812-334-4261

Mon-Tues 8-3:30p
Wed-Thurs 8:30-3p Telework

From: Megan Ottenweller <mottenweller@cmtengr.com>
Sent: Tuesday, March 14, 2023 3:40 PM
To: McWilliams, Robin <robin_mcwilliams@fws.gov>
Subject: [EXTERNAL] Early Coordination Letter: SR 9 and SR 234 Intersection Improvements #2000154

This email has been received from outside of DOI - Use caution before clicking on links, opening attachments, or responding.

Dear Interested Party,

The Indiana Department of Transportation intends to proceed with a project involving intersection improvements at S.R. 9/S.R. 234 in Harrison Township, Hancock County, Indiana (INDOT Des No.: 2000154). Please see the attached letter, which is part of the early coordination phase of the environmental review process. We are requesting comments from your area of expertise regarding any possible environmental effects associated with this project. We will incorporate your comments into a study of the project's environmental impacts.

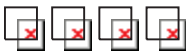
If you have any questions or concerns regarding this matter, please feel free to contact me at 317-808-9470 or via e-mail. Thank you for your assistance, and we look forward to your response.

Thank you,
Megan

MEGAN OTTENWELLER | Environmental Specialist I



Crawford, Murphy & Tilly | Engineers & Consultants
8790 Purdue Road | Indianapolis, IN 46268
w 317.808.9470 | mottenweller@cmtengr.com



Centered in Value



United States Department of the Interior

FISH AND WILDLIFE SERVICE
Indiana Ecological Services Field Office
620 South Walker Street
Bloomington, IN 47403-2121
Phone: (812) 334-4261 Fax: (812) 334-4273



In Reply Refer To:

05/16/2025 17:07:14 UTC

Project Code: 2025-0014431

Project Name: SR 9 and SR 234 Intersection Improvements (Des No. 2000154)

Subject: List of threatened and endangered species that may occur in your proposed project location or may be affected by your proposed project

To Whom It May Concern:

The enclosed species list identifies threatened, endangered, proposed and candidate species, as well as proposed and final designated critical habitat, that may occur within the boundary of your proposed project and/or may be affected by your proposed project. The species list fulfills the requirements of the U.S. Fish and Wildlife Service (Service) under section 7(c) of the Endangered Species Act (Act) of 1973, as amended (16 U.S.C. 1531 *et seq.*).

New information based on updated surveys, changes in the abundance and distribution of species, changed habitat conditions, or other factors could change this list. Please feel free to contact us if you need more current information or assistance regarding the potential impacts to federally proposed, listed, and candidate species and federally designated and proposed critical habitat. Please note that under 50 CFR 402.12(e) of the regulations implementing section 7 of the Act, the accuracy of this species list should be verified after 90 days. This verification can be completed formally or informally as desired. The Service recommends that verification be completed by visiting the IPaC website at regular intervals during project planning and implementation for updates to species lists and information. An updated list may be requested through the IPaC system by completing the same process used to receive the enclosed list.

The purpose of the Act is to provide a means whereby threatened and endangered species and the ecosystems upon which they depend may be conserved. Under sections 7(a)(1) and 7(a)(2) of the Act and its implementing regulations (50 CFR 402 *et seq.*), Federal agencies are required to utilize their authorities to carry out programs for the conservation of threatened and endangered species and to determine whether projects may affect threatened and endangered species and/or designated critical habitat.

Please use the species list provided and visit the U.S. Fish and Wildlife Service's Region 3 Section 7 Technical Assistance website at - <http://www.fws.gov/midwest/endangered/section7/s7process/index.html>. This website contains step-by-step instructions which will help you

determine if your project will have an adverse effect on listed species and will help lead you through the Section 7 process. For all **wind energy projects and projects that include installing towers that use guy wires or are over 200 feet in height**, please contact this field office directly for assistance, even if no federally listed plants, animals or critical habitat are present within your proposed project or may be affected by your proposed project.

A Biological Assessment is required for construction projects (or other undertakings having similar physical impacts) that are major Federal actions significantly affecting the quality of the human environment as defined in the National Environmental Policy Act (42 U.S.C. 4332(2) (c)). For projects other than major construction activities, the Service suggests that a biological evaluation similar to a Biological Assessment be prepared to determine whether the project may affect listed or proposed species and/or designated or proposed critical habitat. Recommended contents of a Biological Assessment are described at 50 CFR 402.12.

If a Federal agency determines, based on the Biological Assessment or biological evaluation, that listed species and/or designated critical habitat may be affected by the proposed project, the agency is required to consult with the Service pursuant to 50 CFR 402. In addition, the Service recommends that candidate species, proposed species and proposed critical habitat be addressed within the consultation. More information on the regulations and procedures for section 7 consultation, including the role of permit or license applicants, can be found in the "Endangered Species Consultation Handbook" at:

<https://www.fws.gov/sites/default/files/documents/endangered-species-consultation-handbook.pdf>

Migratory Birds: In addition to responsibilities to protect threatened and endangered species under the Endangered Species Act (ESA), there are additional responsibilities under the Migratory Bird Treaty Act (MBTA) and the Bald and Golden Eagle Protection Act (BGEPA) to protect native birds from project-related impacts. Any activity, intentional or unintentional, resulting in take of migratory birds, including eagles, is prohibited unless otherwise permitted by the U.S. Fish and Wildlife Service (50 C.F.R. Sec. 10.12 and 16 U.S.C. Sec. 668(a)). For more information regarding these Acts, see <https://www.fws.gov/program/migratory-bird-permit/what-we-do>.

The MBTA has no provision for allowing take of migratory birds that may be unintentionally killed or injured by otherwise lawful activities. It is the responsibility of the project proponent to comply with these Acts by identifying potential impacts to migratory birds and eagles within applicable NEPA documents (when there is a federal nexus) or a Bird/Eagle Conservation Plan (when there is no federal nexus). Proponents should implement conservation measures to avoid or minimize the production of project-related stressors or minimize the exposure of birds and their resources to the project-related stressors. For more information on avian stressors and recommended conservation measures, see <https://www.fws.gov/library/collections/threats-birds>.

In addition to MBTA and BGEPA, Executive Order 13186: *Responsibilities of Federal Agencies to Protect Migratory Birds*, obligates all Federal agencies that engage in or authorize activities that might affect migratory birds, to minimize those effects and encourage conservation measures that will improve bird populations. Executive Order 13186 provides for the protection of both migratory birds and migratory bird habitat. For information regarding the implementation of

Executive Order 13186, please visit <https://www.fws.gov/partner/council-conservation-migratory-birds>.

We appreciate your concern for threatened and endangered species. The Service encourages Federal agencies to include conservation of threatened and endangered species into their project planning to further the purposes of the Act. **Please include the Consultation Code in the header of this letter with any request for consultation or correspondence about your project that you submit to our office.**

Attachment(s):

- Official Species List
- Bald & Golden Eagles
- Migratory Birds
- Wetlands

OFFICIAL SPECIES LIST

This list is provided pursuant to Section 7 of the Endangered Species Act, and fulfills the requirement for Federal agencies to "request of the Secretary of the Interior information whether any species which is listed or proposed to be listed may be present in the area of a proposed action".

This species list is provided by:

Indiana Ecological Services Field Office

620 South Walker Street
Bloomington, IN 47403-2121
(812) 334-4261

PROJECT SUMMARY

Project Code: 2025-0014431
Project Name: SR 9 and SR 234 Intersection Improvements (Des No. 2000154)
Project Type: Road/Hwy - Maintenance/Modification
Project Description: This project (Des No. 2000154) is located at the existing SR 9 and SR 234 intersection, 4.10 miles southeast of US in Hancock County, Indiana. The project is located in Section 28, Township 1 south, and Range 5 west, on the USGS Jasper, IN Quadrangle.

The current proposed project will convert the existing stop-controlled intersection to a compact, single lane roundabout. A mill and overlay will occur along the approaches to the roundabout. Permanent right-of-way will be needed for the project totaling approximately 1.98 acres. Temporary right-of-way will be needed for the project totaling 0.21 acres for driveway reconstruction.

Construction is anticipated to begin in May 2026 and be completed by March 2027. No tree removal will be required for the construction of the project. Construction timing and tree clearing requirements have been verified by the project designer.

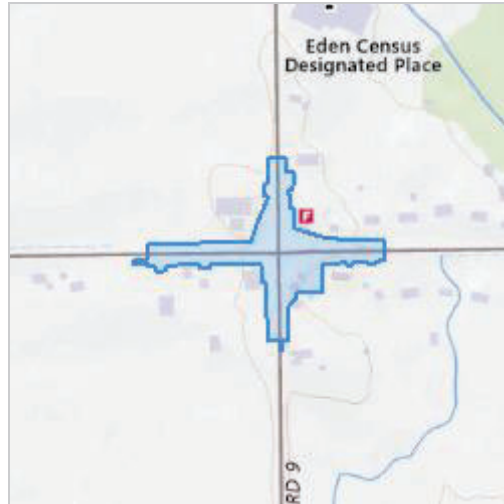
The project is located in a rural area consisting primarily of residential and agricultural properties. The project is bordered by residential and commercial properties. No parks, forested areas, or waterways are within the study area. Barrett Ditch is located approximately 950 feet east of the project area and Sugar Creek is located approximately 2,300 feet north.

A review of the USFWS database on March 2, 2023, did not indicate the presence of the Indiana bat or the northern long-eared bat within 0.5 mile of the study area. A total of eight culverts were inspected for bats. Suitable summer habitat is located within and near the study area in the form of individual trees, manmade structures, and the Barrett Ditch and Sugar Creek riparian corridors. No suitable summer habitat will be impacted for the construction of the project. No tree removal will be required for the project. The project activities will not include the use of percussives. Permanent lighting will be installed as part of the project. Although temporary lighting is not expected to be required for the construction of the project, it is possible some night work will be performed.

No mitigation will be required.

Project Location:

The approximate location of the project can be viewed in Google Maps: <https://www.google.com/maps/@39.9010472,-85.7708708956379,14z>



Counties: Hancock County, Indiana

ENDANGERED SPECIES ACT SPECIES

There is a total of 4 threatened, endangered, or candidate species on this species list.

Species on this list should be considered in an effects analysis for your project and could include species that exist in another geographic area. For example, certain fish may appear on the species list because a project could affect downstream species.

IPaC does not display listed species or critical habitats under the sole jurisdiction of NOAA Fisheries¹, as USFWS does not have the authority to speak on behalf of NOAA and the Department of Commerce.

See the "Critical habitats" section below for those critical habitats that lie wholly or partially within your project area under this office's jurisdiction. Please contact the designated FWS office if you have questions.

-
1. [NOAA Fisheries](#), also known as the National Marine Fisheries Service (NMFS), is an office of the National Oceanic and Atmospheric Administration within the Department of Commerce.

MAMMALS

NAME	STATUS
Indiana Bat <i>Myotis sodalis</i> There is final critical habitat for this species. Your location does not overlap the critical habitat. Species profile: https://ecos.fws.gov/ecp/species/5949	Endangered

BIRDS

NAME	STATUS
Whooping Crane <i>Grus americana</i> Population: U.S.A. (AL, AR, CO, FL, GA, ID, IL, IN, IA, KY, LA, MI, MN, MS, MO, NC, NM, OH, SC, TN, UT, VA, WI, WV, western half of WY) No critical habitat has been designated for this species. Species profile: https://ecos.fws.gov/ecp/species/758	Experimental Population, Non- Essential

CLAMS

NAME	STATUS
Snuffbox Mussel <i>Epioblasma triquetra</i> There is proposed critical habitat for this species. Your location does not overlap the critical habitat. Species profile: https://ecos.fws.gov/ecp/species/4135	Endangered

INSECTS

NAME	STATUS
Monarch Butterfly <i>Danaus plexippus</i> There is proposed critical habitat for this species. Your location does not overlap the critical habitat. Species profile: https://ecos.fws.gov/ecp/species/9743	Proposed Threatened

CRITICAL HABITATS

THERE ARE NO CRITICAL HABITATS WITHIN YOUR PROJECT AREA UNDER THIS OFFICE'S JURISDICTION.

YOU ARE STILL REQUIRED TO DETERMINE IF YOUR PROJECT(S) MAY HAVE EFFECTS ON ALL ABOVE LISTED SPECIES.

BALD & GOLDEN EAGLES

Bald and Golden Eagles are protected under the Bald and Golden Eagle Protection Act ² and the Migratory Bird Treaty Act (MBTA) ¹. Any person or organization who plans or conducts activities that may result in impacts to Bald or Golden Eagles, or their habitats, should follow appropriate regulations and consider implementing appropriate avoidance and minimization measures, as described in the various links on this page.

-
1. The [Bald and Golden Eagle Protection Act](#) of 1940.
 2. The [Migratory Birds Treaty Act](#) of 1918.
 3. 50 C.F.R. Sec. 10.12 and 16 U.S.C. Sec. 668(a)

BALD & GOLDEN EAGLES INFORMATION WAS NOT AVAILABLE WHEN THIS SPECIES LIST WAS GENERATED. PLEASE CONTACT THE FIELD OFFICE FOR FURTHER INFORMATION.

MIGRATORY BIRDS

The Migratory Bird Treaty Act (MBTA) ¹ prohibits the take (including killing, capturing, selling, trading, and transport) of protected migratory bird species without prior authorization by the Department of Interior U.S. Fish and Wildlife Service (Service). The incidental take of migratory birds is the injury or death of birds that results from, but is not the purpose, of an activity. The Service interprets the MBTA to prohibit incidental take.

-
1. The [Migratory Birds Treaty Act](#) of 1918.
 2. The [Bald and Golden Eagle Protection Act](#) of 1940.
 3. 50 C.F.R. Sec. 10.12 and 16 U.S.C. Sec. 668(a)

MIGRATORY BIRD INFORMATION WAS NOT AVAILABLE WHEN THIS SPECIES LIST WAS GENERATED. PLEASE CONTACT THE FIELD OFFICE FOR FURTHER INFORMATION.

WETLANDS

Impacts to [NWI wetlands](#) and other aquatic habitats may be subject to regulation under Section 404 of the Clean Water Act, or other State/Federal statutes.

For more information please contact the Regulatory Program of the local [U.S. Army Corps of Engineers District](#).

Please note that the NWI data being shown may be out of date. We are currently working to update our NWI data set. We recommend you verify these results with a site visit to determine the actual extent of wetlands on site.

THERE ARE NO WETLANDS WITHIN YOUR PROJECT AREA.

IPAC USER CONTACT INFORMATION

Agency: INDOT

Name: Alexander Tadda

Address: 8790 Purdue Rd

City: Indianapolis

State: IN

Zip: 46268

Email: atadda@cmtengr.com

Phone: 6144681200

LEAD AGENCY CONTACT INFORMATION

Lead Agency: Federal Highway Administration

Alex Tadda

From: Bales, Ronald <rbales@indot.IN.gov>
Sent: Thursday, May 15, 2025 1:31 PM
To: Alex Tadda
Cc: Austin Clarridge
Subject: RE: the SR 9 and SR 234 Intersection Improvement Project (Des No. 2000154)

Follow Up Flag: Follow up
Flag Status: Flagged

***External Message:** This email was sent from someone outside of CMT. Please use caution with links and attachments from unknown senders or receiving unexpected emails.*

Thank you Alex. Yes, best to just update determination key to be consistent with new PBO. The determination key was deleted so you should have access to it.

I concur with the No Effect and that can be documented in the NEPA document and that coordination with USFWS was not required for the mussel species due to a No Effect determination.

Ron Bales

Environmental Section Supervisor

Indiana Department of Transportation

Greenfield District

Cell: (317) 515-7908

Email: rbales@indot.in.gov

[Find us on social media!](#)



From: Alex Tadda <atadda@cmtengr.com>
Sent: Thursday, May 15, 2025 1:18 PM
To: Bales, Ronald <rbales@indot.IN.gov>
Cc: Austin Clarridge <aclarridge@cmtengr.com>
Subject: RE: the SR 9 and SR 234 Intersection Improvement Project (Des No. 2000154)

EXTERNAL EMAIL: This email was sent from outside your organization. Exercise caution when clicking links, opening attachments or taking further action, before validating its authenticity.

Hello Ron,

With the recent changes to the Determination Key in IPaC along with the reversal of the 2013 USFWS interim policy, I am hoping to coordinate with you on what next steps should be taking regarding threatened and endangered species for the SR 9 and SR 234 Intersection Improvement Project (Des No. 2000154).

We received the concurrence letter for the project in IPaC on November 18, 2024. Since the DKey has been updated, does the updated version need to be completed and replace the old one in the CE, or does the current concurrence letter still stand? If we should update the existing DKey to receive a new consistency and concurrence letter, could you delete the old DKey in IPaC so I am able to start the new one?

Due to the reversal of the 2013 USFWS interim policy with INDOT for threatened and endangered species, could you confirm a "No Effect" call for the Snuffbox mussel (*Epioblasma triquetra*) to satisfy Section 7 of the Endangered Species Act for the SR 9 and SR 234 Intersection Improvement project? Suitable habitat for the snuffbox mussel is usually found in small- to medium-sized creeks, inhabiting areas with a swift current, although it is also found in Lake Erie and some larger rivers. No streams, lakes, or rivers are located within or adjacent to the project. Due to the lack of surface water and suitable habitat, we are requesting confirmation on a no effect on the snuffbox mussel.

Please let me know if you have any questions.

Thank you,
Alex Tadda

ALEX TADDA | Crawford, Murphy & Tilly | w 614.468.1211
Environmental Scientist

From: Bales, Ronald <rbales@indot.IN.gov>
Sent: Monday, November 18, 2024 9:30 AM
To: Alex Tadda <atadda@cmtengr.com>
Cc: Austin Clarridge <aclarridge@cmtengr.com>
Subject: RE: the SR 9 and SR 234 Intersection Improvement Project (Des No. 2000154)

External Message: This email was sent from someone outside of CMT. Please use caution with links and attachments from unknown senders or receiving unexpected emails.

INDOT has reviewed the IPaC determination and concurs. The determination was submitted to USFWS on November 18, 2024 for their 14-day review period.

Ron Bales
Environmental Section Supervisor
Indiana Department of Transportation
Greenfield District
Cell: (317) 515-7908
Email: rbales@indot.in.gov
[Find us on social media!](#)





United States Department of the Interior

FISH AND WILDLIFE SERVICE
Indiana Ecological Services Field Office
620 South Walker Street
Bloomington, IN 47403-2121
Phone: (812) 334-4261 Fax: (812) 334-4273



In Reply Refer To:

05/16/2025 16:56:02 UTC

Project code: 2025-0014431

Project Name: SR 9 and SR 234 Intersection Improvements (Des No. 2000154)

Subject: Not Likely to Adversely Affect Technical assistance letter for the 'SR 9 and SR 234 Intersection Improvements (Des No. 2000154)' project under the December 13, 2024, FHWA, FRA, FTA Programmatic Biological Opinion for Transportation Projects within the Range of the Indiana Bat, Northern Long-eared Bat, and Tricolored Bat.

To whom it may concern:

This letter records the determination of effects to federally listed (or proposed) bat species anticipated to result from the SR 9 and SR 234 Intersection Improvements (Des No. 2000154) (the Project). This determination is based upon information you entered into the assisted determination key (Dkey) associated with the above referenced Programmatic Biological Opinion/Conference Opinion (PBO/PCO) in the U.S. Fish and Wildlife Service's (Service) Information for Planning and Consultation (IPaC) system on the date listed above. **This letter does not satisfy compliance requirement under section 7(a)(2) of the Endangered Species Act of 1973 (ESA) (16 USC 1536), as amended.** Those requirements will be met upon taking the actions described below.

Ensuring Accurate Determinations When Using IPaC:

The Service developed the IPaC system and this Dkey in accordance with the ESA and based on the PBO/PCO. All information submitted by the project proponent into IPaC must accurately represent the full scope and details of the Project.

Failure to accurately represent or implement the Project as detailed in the Dkey invalidates this letter. Answers to certain questions in the Dkey commit the project proponent to implementation of conservation measures that must be followed for the ESA determinations to remain valid. Carefully review this letter, your ESA requirements are NOT yet complete.

Determinations:

Based on the information you provided (Project Description shown below), you have determined that the Project is within the scope and adheres to the criteria of the PBO/PCO, including the adoption of applicable avoidance and minimization measures. Based on your IPaC submission and the PBO/PCO, the Project is consistent with the following effect determinations:

Species	Listing Status	Determination
Indiana Bat (<i>Myotis sodalis</i>)	Endangered	NLAA

The tricolored bat is proposed for listing as endangered under the ESA, but not yet listed. For actions that may affect a proposed species, agencies cannot consult, but they can confer under the authority of section 7(a)(4) of the ESA. Such conferences can follow the procedures for a consultation and be adopted as such if and when the proposed species is listed. Should the tricolored bat be listed, agencies must review projects that are not yet complete, or projects with ongoing effects within the tricolored bat range that previously received a no effect or not likely to adversely affect (NLAA) determination from the key to confirm that the determination is still accurate.

This "may affect - not likely to adversely affect" determination becomes effective when the lead Federal action agency or designated non-federal representative requests the Service rely on the PBO/PCO to satisfy the agency's consultation requirements for this project. **To fulfill the next steps in the consultation process, add the lead Federal action agency or designated non-federal representative as a Project Member in the IPaC system and provide this letter to the lead Federal action agency or its designated non-federal representative with a request for review, and as the agency deems appropriate, submit for concurrence verification through the IPaC system. ESA section 7 compliance for this Proposed Action is NOT yet complete until the Federal action agency or its designated non-federal representative receives a not likely to adversely affect verification letter from the Service IPaC system.**

If the Project is modified, or new information reveals that it may affect the Indiana bat, northern long-eared bat, or tricolored bat in a manner or to an extent not considered in the PBO/PCO, further review to conclude the requirements of ESA section 7(a)(2) may be required.

For Proposed Actions that include bridge/culvert or structure removal, replacement, and/or maintenance activities:

If your initial bridge, culvert, or structure assessment failed to detect Indiana bat, northern long-eared bat, or tricolored bat use or occupancy, yet bats are later detected prior to, or during construction, promptly notify the local Service Field Office within 2 working days of the discovery. In addition, please document whether incidental take occurred, and if so, the type (i.e. kill or harm) and amount (i.e. number of individuals) and submit documentation to the local Service Field Office within 5 working days from the completion of the bridge, culvert, or structure construction (use Appendix E - Post Assessment Discovery of Bats at Bridge/Culvert or Structure Form in the [User's Guide](#)). In these instances, potential incidental take of Indiana bats, northern long-eared bats, or tricolored bats may be exempted provided that the take is reported to

the Service. In these instances, potential incidental take of Indiana bats, northern long-eared bats, or tricolored bats may be exempted provided that the take is reported to the Service.

If the Project may affect any other federally listed or proposed species and/or designated critical habitat, additional consultation between the lead Federal action agency and this Service Field Office is required for those species/designated critical habitat. If the Project has the potential to take bald or golden eagles, additional coordination with the Service under the Bald and Golden Eagle Protection Act may also be required. In either of these circumstances, please advise the lead Federal action agency to contact this Service Field Office

The following species may occur in your project area and **are not** covered by this determination:

- Monarch Butterfly *Danaus plexippus* Proposed Threatened
- Snuffbox Mussel *Epioblasma triquetra* Endangered
- Whooping Crane *Grus americana* Experimental Population, Non-Essential

PROJECT DESCRIPTION

The following project name and description was collected in IPaC as part of the endangered species review process.

NAME

SR 9 and SR 234 Intersection Improvements (Des No. 2000154)

DESCRIPTION

This project (Des No. 2000154) is located at the existing SR 9 and SR 234 intersection, 4.10 miles southeast of US in Hancock County, Indiana. The project is located in Section 28, Township 1 south, and Range 5 west, on the USGS Jasper, IN Quadrangle.

The current proposed project will convert the existing stop-controlled intersection to a compact, single lane roundabout. A mill and overlay will occur along the approaches to the roundabout. Permanent right-of-way will be needed for the project totaling approximately 1.98 acres. Temporary right-of-way will be needed for the project totaling 0.21 acres for driveway reconstruction.

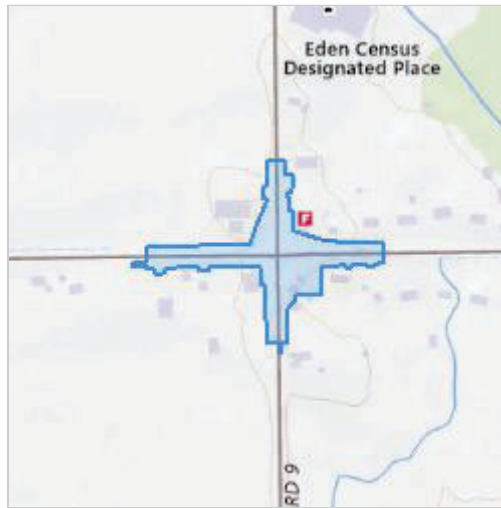
Construction is anticipated to begin in May 2026 and be completed by March 2027. No tree removal will be required for the construction of the project. Construction timing and tree clearing requirements have been verified by the project designer.

The project is located in a rural area consisting primarily of residential and agricultural properties. The project is bordered by residential and commercial properties. No parks, forested areas, or waterways are within the study area. Barrett Ditch is located approximately 950 feet east of the project area and Sugar Creek is located approximately 2,300 feet north.

A review of the USFWS database on March 2, 2023, did not indicate the presence of the Indiana bat or the northern long-eared bat within 0.5 mile of the study area. A total of eight culverts were inspected for bats. Suitable summer habitat is located within and near the study area in the form of individual trees, manmade structures, and the Barrett Ditch and Sugar Creek riparian corridors. No suitable summer habitat will be impacted for the construction of the project. No tree removal will be required for the project. The project activities will not include the use of percussives. Permanent lighting will be installed as part of the project. Although temporary lighting is not expected to be required for the construction of the project, it is possible some night work will be performed.

No mitigation will be required.

The approximate location of the project can be viewed in Google Maps: <https://www.google.com/maps/@39.9010472,-85.7708708956379,14z>



DETERMINATION KEY RESULT

Based on your answers provided, this project(s) may affect, but is not likely to adversely affect the Indiana bat, northern long-eared bat or tricolored bat, therefore, consultation with the U.S. Fish and Wildlife Service pursuant to Section 7(a)(2) of the Endangered Species Act of 1973 (ESA) (87 Stat. 884, as amended 16 U.S.C. 1531 et seq.) is required. However, also based on your answers provided, this project may rely on the concurrence provided in the Programmatic Biological Opinion/Conference Opinion for Transportation Projects in the Range of the Indiana bat, northern long-eared bat, and tricolored bat, dated December 13, 2024.

QUALIFICATION INTERVIEW

1. Which Federal Agency is the lead federal agency the action?

A) Federal Highway Administration (FHWA)

2. Does the Action Area intersect the species list area of the Indiana bat?

Automatically answered

Yes

3. Is the project within 0.5 miles radius of an entrance/opening to any known Indiana bat hibernaculum?

No

4. Does your project's activities include raising the road profile above the tree canopy in documented habitat for the Indiana bat, NLEB, or TCB?

Note: For the definition of documented habitat, refer to Appendix A: <https://www.fws.gov/media/users-guide-range-wide-programmatic-consultation-indiana-bat-and-northern-long-eared-bat>

No

5. Is your project located within a karst area?

No

6. Will the project include bridge, culvert, or structure removal, replacement, and/or alteration activities?

Note: For definitions of bridge, culvert, and structure, refer to Appendix A: <https://www.fws.gov/media/users-guide-range-wide-programmatic-consultation-indiana-bat-and-northern-long-eared-bat>.

Yes

7. Do your project's activities involve tree removal/trimming, temporary lighting, new/additional permanent lighting, ground disturbance, percussives that involves noise/vibration above existing background levels, vibrations, or slash pile burning?

Yes

8. Is there suitable summer habitat for the Indiana bat, NLEB, or TCB within the project action area?

Note: See the Service's summer survey guidance for current definitions of suitable habitat [<https://www.fws.gov/midwest/endangered/mammals/inba/inbasummersurveyguidance.html>].

Yes

9. Have P/A surveys for the Indiana bat, NLEB, or TCB been conducted within the suitable summer habitat located within your project action area? This refers to mist-netting or acoustic surveys, not bridge assessments.

Note: See the Service's survey guidance <https://www.fws.gov/midwest/endangered/mammals/inba/inbasummersurveyguidance.html>

No

10. Will the project involve the removal or trimming of trees within suitable habitat for the Indiana bat, NLEB, or TCB?

No

11. Does your project include activities involving the temporary or permanent exclusion of Indiana bats, NLEBs, or TCBs from a bridge/culvert or structure?

Note: exclusion is conducted to deny bats' entry or reentry into a bridge/culvert or structure. To be effective and to avoid harming bats, it should be done according to established standards.

No

12. Does your project involve the use of temporary lighting within Indiana bat, NLEB, or TCB suitable habitat?

Note: For the definition of lighting, refer to Appendix A: <https://www.fws.gov/media/users-guide-range-wide-programmatic-consultation-indiana-bat-and-northern-long-eared-bat>

Yes

13. Will the use of temporary lighting be conducted during the Indiana bat, NLEB, or TCB active season?

Yes

14. Will temporary lighting be directed away from Indiana bat, NLEB, or TCB suitable habitat)?

Yes

15. Will the project substantially increase baseline light conditions via the use of permanent lighting (replacement or new/additional) in suitable habitat.

No

16. Will your project include percussive activities?

Note: Refer to Stressor #2 Noise/Vibration on page 109 of the PBO/PCO.

No

17. Will the project include **bridge** removal, replacement, and/or alteration activities?
No
18. Does the project include **culvert** removal, replacement, and/or alteration activities?
Yes
19. Is there any suitable habitat for the Indiana bat or NLEB within 1,000 feet of the **culvert** (includes any trees suitable for maternity, roosting, foraging, or travelling habitat)?
Yes
20. Does the culvert equal or exceed 23 feet (7.0 meters) in length?
Yes
21. Are the interior dimensions of the culvert less than 4 ft in diameter/height?
Yes
22. Does the project include **structure** removal, replacement, and/or alteration activities?
No
23. Does the Action Area intersect the species list area of the Indiana bat?
Automatically answered
Yes
24. Does the Action Area intersect the species list area of the tricolored Bat (TCB)?
Automatically answered
No

AVOIDANCE AND MINIMIZATION MEASURES (AMMS)

This determination key result includes the commitment to implement the following Avoidance and Minimization Measures (AMMs):

GAMM1

Ensure all operators, employees, and contractors working in areas of Indiana bat, NLEB, or TCB suitable habitat are aware of all Transportation Agency environmental commitments, including all applicable AMMs.

LAMM1

Direct temporary lighting away from suitable habitat during the active season

DETERMINATION KEY DESCRIPTION: FHWA, FRA, FTA PROGRAMMATIC CONSULTATION FOR TRANSPORTATION PROJECTS AFFECTING IBAT, NLEB, OR TCB

This key was last updated in IPaC on April 29, 2025. Keys are subject to periodic revision.

This decision key is intended for projects/activities funded or authorized by the Federal Highway Administration (FHWA), Federal Railroad Administration (FRA), and/or Federal Transit Administration (FTA), which may require consultation with the U.S. Fish and Wildlife Service (Service) under Section 7 of the Endangered Species Act (ESA) and may affect the federally listed endangered Indiana bat (*Myotis sodalis*), northern long-eared bat (*Myotis septentrionalis*), and/or federally proposed endangered tricolored bat (*Perimyotis subflavus*).

This decision key should only be used to verify project applicability with the Service's Programmatic Biological Opinion/Conference Opinion for Transportation Projects in the Range of the Indiana bat, northern long-eared bat, and tricolored bat, dated December 13, 2024. The programmatic consultation limited transportation activities that may affect the covered bat species and addresses situations that are both likely and not likely to adversely affect the covered bat species. This decision key will assist in identifying the effect of a specific project/activity and the applicability of the programmatic consultation. The programmatic consultation is not intended to cover all types of transportation actions. Activities outside the scope of the programmatic consultation, or that may affect ESA-listed species other than the Indiana bat, northern long-eared bat, or tricolored bat, or their designated critical habitat, may require additional ESA Section 7 consultation.

IPAC USER CONTACT INFORMATION

Agency: INDOT

Name: Alexander Tadda

Address: 8790 Purdue Rd

City: Indianapolis

State: IN

Zip: 46268

Email: atadda@cmtengr.com

Phone: 6144681200

LEAD AGENCY CONTACT INFORMATION

Lead Agency: Federal Highway Administration

Dan L. Strahl, Attorneys at Law
8 West Main Street/PO Box 581
Greenfield, Indiana 46140

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MORGAN STUMPF
Crawford, Murphy & Tilly Inc
8790 PURDUE RD STE 165
INDIANAPOLIS IN 46268-0080

46268-008040



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March 22, 2023

Morgan Stumpf
Project Manager
Crawford, Murphy & Tilly, Inc.
8790 Purdue Road
Indianapolis, IN 46268

Donald Mcghghy, PM
Indiana Department of Transportation
100 North Senate Avenue
Room N758-ES
Indianapolis, IN 46204

RE: SR 9 and SR 234 Intersection Improvements
Hancock County, Indiana
INDOT Des No.: 2000154
CMT Project No.: 20070904.07

Dear Ms. Stumpf and Mr. Mcghghy::

I am writing to you today as the representative of the Green Township Trustee and the Green Township Board of Trustees. Your letter of March 14, 2023 was transmitted to the Green Township Fire Department who brought this matter to our attention. The information provided to date show that the above referenced project will have an immediate impact upon the operation of the Green Township Fire Department which is managed under the authority of the Township Trustee and Board.

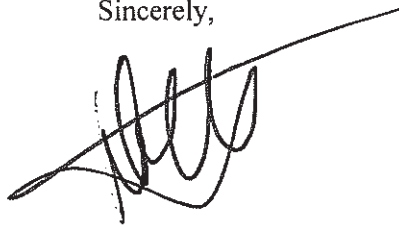
Since your project is in the planning stage, additional information is needed to allow the Trustee and Board to accurately evaluate their response to your request. Several questions posed require your response and are outlined below.

- a. Plans for the roundabout location to determine the extent of damage and inconvenience to the daily operation of the fire department.
- b. Impact upon the fire department to perform routine inspection and maintenance on the vehicles used by the fire department
- c. Impact upon the training site used by the firefighters which will be in the right of way designated.
- d. Impact upon the building housing the fire station.

- e. Impact upon the fire department's ability to service the residents of Green Township with fire and medical services during construction and closure of Indiana 234.
- f. Since the drawing attached to your letter show that the car lot on the southeast corner of the intersection will be included in the right of way for the roundabout, will the roundabout be constructed off of center to allow for use of the car lot while not impacting the western landowners?

Please provide answers to these question to my office. If you require further information, please contact me at your convenience.

Sincerely,

A handwritten signature in black ink, appearing to read 'Dan Strahl', with a long horizontal line extending from the end of the signature.

Dan Strahl
Attorney for Green Township Trustee and
Green Township Board of Trustees

cc: Stephanie Jones, Green Township Trustee
Mike Maroska, Board Member
Bob Yeager, Board Member
William Jones, Board Member

DAN L. STRAHL
Attorneys at Law

*Eight West Main Street
Post Office Box 581
Greenfield, Indiana 46140-0581
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Dan L. Strahl
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Steven M. Elsbury
Email: selsbury@strahlapple.com

March 22, 2023

Morgan Stumpf
Project Manager
Crawford, Murphy & Tilly, Inc.
8790 Purdue Road
Indianapolis, IN 46268

Donald Mcghghy, PM
Indiana Department of Transportation
100 North Senate Avenue
Room N758-ES
Indianapolis, IN 46204

RE: SR 9 and SR 234 Intersection Improvements
Hancock County, Indiana
INDOT Des No.: 2000154
CMT Project No.: 20070904.07

Dear Ms. Stumpf and Mr. Mcghghy:

I am writing to you today as the representative of the Greenfield Central Community School Board of Trustees who were just made aware of the plans to construct a roundabout at State Roads 9 and 234. While the traffic situation at that intersection needs to be addressed information, provided to date show that the above referenced project will have an immediate impact upon the operation of the Eden Elementary School and the transportation patterns for Greenfield Central Schools.

Since your project is in the planning stage, additional information is needed to allow the School Board and Superintendent of Greenfield Central Schools to accurately understand the impact that may occur due to this project. Several questions posed require your response and are outlined below.

- a. Will the roundabout damage any of the entrance or exits from Eden Elementary?
- b. What will be the impact to Greenfield Central's transportation system due to State Road 234 being closed?
- c. What is the length of time State Road 234 will be closed?
- d. How will parents and other people be allowed to enter the school grounds at Eden?
- e. To what extent will the traffic pattern on State Road 9 be affected by the construction?
- f. What is the time line for the start and end of construction?

In addition, please provide updated drawings of the planned roundabout that show its actual location. Please provide answers to these question to my office. If you require further information, please contact me at your convenience.

Sincerely,

A handwritten signature in black ink, appearing to read 'Dan Strahl', with a stylized, cursive script.

Dan Strahl
Attorney for Greenfield Central Community
School Board of Trustees

cc: Greenfield Central Community School Board of Trustees
Dr. Harold Olin, Superintendent Greenfield Central Community Schools
Nate Day, Business Manager, Greenfield Central Community Schools



TO: Dan Strahl, Attorney for Green Township Trustee & Green Township Board of Trustees

FROM: Morgan Stumpf, CMT

DATE: April 13, 2023

SUBJECT: Des. No. 200154 – SR 9 and SR 234 Intersection

Dear Mr. Strahl,

We received your letter as the representative of the Green Township Trustee and the Green Township Board of trustees regarding the proposed roundabout at the intersection of State Route 9 and State Route 234. Thank you for your review of the project and your interest. Feedback from the community is such an integral part of our project development process and we appreciate your involvement in the project. Please find the responses to your questions below.

a. Plans for the roundabout location to determine the extent of damage and inconvenience to the daily operation of the fire department.

Response:

The construction of the roundabout is expected to cause some inconvenience to the daily operations of the fire department during the construction phase. However, the project team is working closely with the fire department to minimize the impact as much as possible. This includes regular communication with the fire department to keep them informed about the construction schedule, and to identify any potential issues that may arise.

In addition, the project team is taking steps to accommodate the specific needs of the fire department. For example, we are exploring options for alternative routes and access points to ensure that emergency vehicles can still respond quickly and efficiently to calls during the construction phase. We are also working to ensure that any necessary detours or road closures are clearly marked and communicated to the public.

Overall, while there may be some temporary inconvenience, we are committed to working collaboratively with the fire department to ensure that their daily operations are minimally impacted during the construction of the roundabout. Please find the attached preliminary plans for the project. Additional plan sets can be provided at the appropriate milestones for further review.

b. Impact upon the fire department to perform routine inspection and maintenance on the vehicles used by the fire department.

Response:

The construction of the roundabout is not expected to have a significant impact on the ability of the fire department to perform routine inspections and maintenance on their vehicles. Once the roundabout is constructed, the existing access to the fire station will remain largely unchanged, and most of the existing site will not be impacted.

There may be some minor impacts around the southeast corner of the fire station that are still being defined. However, the project team is working closely with the fire department to ensure that any potential impacts are minimized as much as possible. This includes considering alternative access routes and identifying any necessary modifications to the site design to accommodate the needs of the fire department.

Overall, while there may be some minor impacts, the project team is committed to working collaboratively with the fire department to ensure that their ability to perform routine inspections and maintenance on their vehicles is not significantly impacted by the construction of the roundabout.

c. Impact upon the training site used by the firefighters which will be in the right of way designated.

Response:

The impact of the roundabout on the training site used by the firefighters will depend on a number of factors, such as the location of the site and the specific design of the roundabout. Without more information, it's difficult to provide a definitive answer to this question.

However, the project team is committed to minimizing any potential impacts to the training site and working closely with the fire department as we design the project. This includes considering alternative design options and staging areas that could help to minimize any impacts to the training site.

In addition, the project team will work with the fire department to identify any specific concerns or requirements related to the training site, and develop strategies to address these concerns.

d. Impact upon the building housing the fire station

Response:

The construction of the roundabout is not expected to have any direct impacts on the building housing the fire station. However, there may be some impacts related to the underground water tank located on the south east corner of the site, such as right-of-way acquisition.

The project team is committed to working closely with the fire department to identify any potential impacts to the underground water tank, and develop strategies to

minimize these impacts as much as possible. However, it is important to note that the tank is not expected to need to be removed as part of the construction process.

Overall, while there may be some potential impacts to the underground water tank, the project team is committed to working collaboratively with the fire department to ensure that the building housing the fire station is not impacted by the construction of the roundabout.

e. Impact upon the fire department's ability to service the residents of Green Township with fire and medical services during construction and closure of SR 234.

Response:

Ensuring that the fire department is able to continue to provide fire and medical services to the residents of Green Township during the construction and closure of SR 234 is a top priority for the project team. Coordination with the fire station during the construction of the roundabout is an important part of the project design, and discussions are ongoing to identify solutions to minimize impacts on the fire department's ability to operate.

A recent conference call was held on March 30th with Brad Stephenson of the Green Township fire department to discuss these issues. During the call, possible solutions were discussed, including the installation of a temporary gravel driveway connecting the south side of the fire station parking lot to SR 234, which would allow access off the east leg of the intersection. The current preliminary design also allows for access off SR 9.

The project team is committed to working closely with the fire department to identify any additional needs or concerns related to the construction and closure of SR 234, and to develop strategies to minimize any impacts on their ability to provide vital services to the community.

Overall, while there may be some impacts on the fire department's ability to service the residents of Green Township during construction and closure of SR 234, the project team is committed to working collaboratively with the fire department to ensure that these impacts are minimized as much as possible.

f. Since the drawing attached to your letter show that the car lot on the southeast corner of the intersection will be included in the right of way for the roundabout, will the roundabout be constructed off of center to allow for use of the car lot while not impacting the western landowners?

Response:

The location of the new roundabout has been carefully planned to balance a number of factors, including the location of the car lot on the southeast corner of the intersection. It is important to note that the drawing attached to our letter is only a

preliminary design, and adjustments may be made as we move forward with the project.

When designing the roundabout, the project team has taken into account a variety of factors, including the location of properties that are eligible for listing on the National Register and the presence of contaminated soils in the southeast quadrant. Additionally, the buildings, frontages, and access drives of properties in the northern two quadrants are important to their operations and must be taken into consideration when planning the location of the roundabout.

While the project team understands that the location of the car lot on the southeast corner of the intersection is an important factor to consider, it is not the only factor. The roundabout must be designed to balance the needs of all stakeholders and minimize impacts as much as possible.

As the project moves forward, the project team will continue to work closely with all stakeholders, including the owners of the car lot and western landowners, to identify any additional concerns or needs related to the design of the roundabout. We are committed to ensuring that all stakeholders are heard and that their concerns are taken into account as we move forward with this important project.

Please do not hesitate to reach out with additional questions regarding the project. We look forward to coordinating with the Green Township Trustee and the Green Township Board of trustees.

Sincerely,
CRAWFORD MURPHY AND TILLY, INC.



Morgan Stumpf, PE
Project Engineer

Cc: Don McGhghy, INDOT
Luis Laracuenta, INDOT

Enclosed:
Preliminary Design Plans



TO: Dan Strahl, Attorney for Greenfield Central Community School Board of Trustees

FROM: Morgan Stumpf, CMT

DATE: April 13, 2023

SUBJECT: Des. No. 200154 – SR 9 and SR 234 Intersection

Dear Mr. Strahl,

We received your letter as the representative of the Greenfield Central Community School Board of Trustees regarding the proposed roundabout at the intersection of State Route 9 and State Route 234. Thank you for your review of the project and your interest. Feedback from the community is such an integral part of our project development process and we appreciate your involvement in the project. Please find the responses to your questions below.

a. Will the roundabout damage any of the entrance or exits from Eden Elementary?

Response:

Thank you for your question about the impact of the roundabout construction on the entrance and exits of Eden Elementary. I can assure you that the roundabout will not permanently alter either of the drives to Eden Elementary off SR 9. Both drives will continue to have full access to SR 9.

As part of the project, the south drive will be rebuilt in its current location within the construction limits of the project. The reconstruction will match the existing width and elevations of the drive. However, the north drive is outside of the project limits, so no work will occur to it.

b. What is the length of time State Road 234 will be closed?

Response:

While the closure duration is still preliminary, the current plan is to close and detour SR 234 in order to construct the new roundabout in a shorter time frame. The approximate length of this closure is 90 workdays, but we will make every effort to complete the project as quickly as possible.

We understand that this closure will impact the community and school transportation, and we are coordinating closely with school officials to develop a construction sequence that minimizes disruptions to the extent possible. We anticipate that the majority of the construction will occur outside of the school year, but we will continue to work with the school district to ensure safe and efficient transportation for students and staff.

c. How will parents and other people be allowed to enter the school grounds at Eden?

Response:

The current design will allow for both driveways at the school to remain operating as is. However, the current plan is expected to minimize the impact to school transportation during the construction period. Special signage and advanced notifications will be included to help direct traffic and allow for parents and other visitors to enter the school grounds at Eden through the existing driveways.

d. To what extent will the traffic pattern on State Road 9 be affected by construction?

Response:

Due to its traffic volumes and importance to north-south connectivity, SR 9 is expected to remain open to traffic during construction. However, to provide space for construction activities, SR 9 will be reduced to one travel lane with a temporary traffic signal controlling two-way traffic. This may cause some delays during peak hours, but the school driveways will remain open and accessible."

e. What is the time line for the start and end of construction?

Response:

The current plan is to start construction in May and to have the roundabout open to traffic by the end of July, taking advantage of the summer months. The ability to target these specific months is contingent upon the timing of utility relocations, acquisition of new right-of-way, and awarding a successful low-bid Contractor. The design team will continue to coordinate with school officials as this project enters final design.

Please do not hesitate to reach out with additional questions regarding the project. We look forward to coordinating with the Greenfield Central Community School Board of Trustees.

Sincerely,
CRAWFORD MURPHY AND TILLY, INC.



Morgan Stumpf, PE
Project Engineer

Cc: Don McGhghy, INDOT
Luis Laracuenta, INDOT

Megan Ottenweller

From: Megan Ottenweller
Sent: Thursday, March 16, 2023 10:35 AM
To: 911 Director
Cc: Misty Moore
Subject: RE: [EXTERNAL]Early Coordination Letter: SR 9 and SR 234 Intersection Improvements # 2000154

Hi John and Misty,

Thank you, John, for including Misty Moore in this correspondence.

I have updated the Township name for any future endeavors. Thank you for bringing this to my attention.

Misty, please let me know if you have any concerns so that I can document your knowledge of the area. We want to make sure that any of your concerns are taken into consideration as we move forward with this intersection improvement project.

Thank you,

MEGAN OTTENWELLER | Crawford, Murphy & Tilly | w 317.808.9470
Environmental Specialist I

From: 911 Director <911director@hancockcoingov.org>
Sent: Wednesday, March 15, 2023 6:16 PM
To: Megan Ottenweller <mottenweller@cmtengr.com>
Cc: Misty Moore <mmoore@hancockcoingov.org>
Subject: FW: [EXTERNAL]Early Coordination Letter: SR 9 and SR 234 Intersection Improvements #2000154

***External Message:** This email was sent from someone outside of CMT. Please use caution with links and attachments from unknown senders or receiving unexpected emails.*

Hi Megan,

Not that I'm not an interested party, but I have attached our Homeland Security/Emergency Management Director (Misty Moore) to this email, she would be the SME for this request.

Also, it would be Green Township in Hancock County, not Harrison.

John



John Jokantas | Director

a: Hancock County 911 | 640 S Franklin St. | 46140, IN
o: 317-477-0180 | **c:** 317-526-3448

From: Megan Ottenweller <mottenweller@cmtengr.com>

Sent: Tuesday, March 14, 2023 4:02 PM

To: 911 Director <911director@hancockcoingov.org>

Subject: [EXTERNAL]Early Coordination Letter: SR 9 and SR 234 Intersection Improvements #2000154

You don't often get email from mottenweller@cmtengr.com. [Learn why this is important](#)

[EXTERNAL EMAIL] DO NOT CLICK links or OPEN attachments unless you recognize the sender and know the content is safe.

Dear Interested Party,

The Indiana Department of Transportation intends to proceed with a project involving intersection improvements at S.R. 9/S.R. 234 in Harrison Township, Hancock County, Indiana (INDOT Des No.: 2000154). Please see the attached letter, which is part of the early coordination phase of the environmental review process. We are requesting comments from your area of expertise regarding any possible environmental effects associated with this project. We will incorporate your comments into a study of the project's environmental impacts.

If you have any questions or concerns regarding this matter, please feel free to contact me at 317-808-9470 or via e-mail. Thank you for your assistance, and we look forward to your response.

Thank you,
Megan

MEGAN OTTENWELLER | Environmental Specialist I



Crawford, Murphy & Tilly | Engineers & Consultants

8790 Purdue Road | Indianapolis, IN 46268

w 317.808.9470 | mottenweller@cmtengr.com



Centered in Value

Megan Ottenweller

From: Royer, Brian <BRoyer@dnr.IN.gov>
Sent: Wednesday, March 15, 2023 12:49 PM
To: Megan Ottenweller
Subject: RE: Early Coordination Letter: SR 9 and SR 234 Intersection Improvements #2000154

External Message: This email was sent from someone outside of CMT. Please use caution with links and attachments from unknown senders or receiving unexpected emails.

There are no known oil and gas related wells within this project area.

Thanks,

Brian Royer
Orphan Well Manager
Indiana Department of Natural Resources
Division of Reclamation
317-417-6556
broyer@dnr.IN.gov
www.dnr.IN.gov

** Please let us know about the quality of our service by taking this brief [customer survey](#).*

From: Megan Ottenweller <mottenweller@cmtengr.com>
Sent: Tuesday, March 14, 2023 3:50 PM
To: Royer, Brian <BRoyer@dnr.IN.gov>
Subject: Early Coordination Letter: SR 9 and SR 234 Intersection Improvements #2000154

**** This is an EXTERNAL email. Exercise caution. DO NOT open attachments or click links from unknown senders or unexpected email. ****

Dear Interested Party,
The Indiana Department of Transportation intends to proceed with a project involving intersection improvements at S.R. 9/S.R. 234 in Harrison Township, Hancock County, Indiana (INDOT Des No.: 2000154). Please see the attached letter, which is part of the early coordination phase of the environmental review process. We are requesting comments from your area of expertise regarding any possible environmental effects associated with this project. We will incorporate your comments into a study of the project's environmental impacts.

If you have any questions or concerns regarding this matter, please feel free to contact me at 317-808-9470 or via e-mail. Thank you for your assistance, and we look forward to your response.

Thank you,
Megan

MEGAN OTTENWELLER | Environmental Specialist I



From: [MWRO Compliance, NPS](#)
To: [Megan Ottenweller](#)
Subject: Re: [EXTERNAL] Early Coordination Letter: SR 9 and SR 234 Intersection Improvements #2000154
Date: Tuesday, March 28, 2023 10:07:34 AM

External Message: This email was sent from someone outside of CMT. Please use caution with links and attachments from unknown senders or receiving unexpected emails.

Good day,

The National Park Service Midwest Regional Office (MWRO) has reviewed the correspondence concerning Early Coordination and has determined that this project is not in the proximity of any National Park Service unit or related area.

To determine if a project is near or could potentially impact access to a unit of the National Park System, please see the following map (zoom in to project location): [Find a Park \(U.S. National Park Service\) \(nps.gov\)](#).

A list of units and designated related areas of the National Park Service can be found here: [National Park System Units and Related Areas \(nps.gov\)](#)

The NPS also administers the National Natural Landmarks Program in coordination with other federal agencies and private entities. A map of these locations can be found here: [National Natural Landmarks Directory \(nps.gov\)](#)

Thank you for your assistance in helping to expedite the review and coordination process.

Regards,

MWRO Compliance Team
Midwest Region (Interior Regions 3/4/5)
mwro_compliance@nps.gov

From: Megan Ottenweller <mottenweller@cmtengr.com>
Sent: Tuesday, March 14, 2023 2:49 PM
To: MWRO Compliance, NPS <MWRO_Compliance@nps.gov>
Subject: [EXTERNAL] Early Coordination Letter: SR 9 and SR 234 Intersection Improvements #2000154

This email has been received from outside of DOI - Use caution before clicking on links, opening attachments, or responding.

Dear Interested Party,

The Indiana Department of Transportation intends to proceed with a project involving intersection improvements at S.R. 9/S.R. 234 in Harrison Township, Hancock County, Indiana (INDOT Des No.: 2000154). Please see the attached letter, which is part of the early coordination phase of the environmental review process. We are requesting comments from your area of expertise regarding any possible environmental effects associated with this project. We will incorporate your comments into a study of the project's environmental impacts.

If you have any questions or concerns regarding this matter, please feel free to contact me at 317-808-9470 or via e-mail. Thank you for your assistance, and we look forward to your response.

Thank you,
Megan

MEGAN OTTENWELLER | Environmental Specialist I



Crawford, Murphy & Tilly | Engineers & Consultants

8790 Purdue Road | Indianapolis, IN 46268

w 317.808.9470 | mottenweller@cmtengr.com



Centered in Value

NO.	STRUCTURE NO.	LATITUDE	LONGITUDE	WATERBODY	INSPECTION DATE	EVIDENCES OF BATS	EXISTING STRUCTURE FT	LENGTH FT	WORK TYPE	DES. NO.
1	N/A	39.90183	-85.770808	N/A	4/20/2023	No	12" RCP	50	To be Replaced	2000154
2	N/A	39.90137	-85.770589	N/A	4/21/2023	No	11" CMP	Unknown	To be Replaced	2000154
3	N/A	39.90096	-85.771732	N/A	4/22/2023	No	12" RCP	Unknown	To be Replaced	2000154
4	N/A	39.90115	-85.772395	N/A	4/23/2023	No	12 " CCP	Unknown	To be Replaced	2000154
5	N/A	39.90096	-85.77235	N/A	4/24/2023	No	10" CCP	28	To be Replaced	2000154
6	N/A	39.90096	-85.770979	N/A	4/25/2023	No	4" CCP	Unknown	To be Replaced	2000154
7	N/A	39.90112	-85.771546	N/A	4/26/2023	No	9" CCP	55	To be Replaced	2000154
8	N/A	39.90099	-85.769704	N/A	4/27/2023	No	10" RCP	14	To be Replaced	2000154

*CMP = Corrugated metal pipe

*CPP = Corrugated plastic pipe

*RCP= Reinforced Concrete Pipe