

Indiana Department of Transportation

County Carroll Route SR 75 Des. No. 1601029

FHWA-Indiana Environmental Document CATEGORICAL EXCLUSION / ENVIRONMENTAL ASSESSMENT FORM GENERAL PROJECT INFORMATION

Road No./County:

State Road (SR) 75/ Carroll County

Designation Number:

1601029

Project Description/Termini:

Bridge deck replacement on State Road (SR) 75 over Wildcat Creek located approximately 4.5 miles south of SR 18, extending approximately 185 feet north and 170 feet south of the center of INDOT Bridge No. 075-08-03653 B.

After completing this form, I conclude that this project qualifies for the following type of Categorical Exclusion (FHWA must review/approve if Level 4 CE):

	Categorical Exclusion, Level 2 – The proposed action meets the criteria for Categorical Exclusion Manual Level 2 - table 1, CE Level Thresholds. Required Signatories: ESM (Environmental Scoping Manager)
	Categorical Exclusion, Level 3 – The proposed action meets the criteria for Categorical Exclusion Manual Level 3 - table 1, CE Level Thresholds. Required Signatories: ESM, ES (Environmental Services Division)
X	Categorical Exclusion, Level 4 – The proposed action meets the criteria for Categorical Exclusion Manual Level 4 - table 1, CE Level Thresholds. Required Signatories: ESM, ES, FHWA
	Environmental Assessment (EA) – EAs require a separate FONSI. Additional research and documentation is necessary to determine the effects on the environment. Required Signatories: ES, FHWA

Note: For documents prepared by or for Environmental Services Division, it is not necessary for the ESM of the district in which the project is located to release for public involvement or sign for approval.

Approval N/A

ESM Signature

Date

Brandon Miller

Digitally signed by
Brandon Miller
Date: 2021.10.19
13:49:03 -04'00'

ES Signature

10/19/2021

Date

KARSTIN MARIE
CARMANY-
GEORGE

Digitally signed by
KARSTIN MARIE
CARMANY-GEORGE
Date: 2021.10.20
13:03:50 -04'00'

FHWA Signature

10/20/21

Date

Release for Public Involvement

N/A

ESM Initials

Date

REB

ES Initials

4-16-2021

Date

Certification of Public Involvement

Lisa Shrader
Office of Public Involvement

08/17/2021

Date

Note: Do not approve until after Section 106 public involvement and all other environmental requirements have been satisfied.

INDOT ES/District Env.
Reviewer Signature:

Jessie Fais

Date: 10/19/2021

Name and Organization of CE/EA Preparer: Briana M. Hope (Lead)/ Leah C. Perry, American Structurepoint, Inc.

This is page 1 of 27 Project name: SR 75 over Wildcat Creek Bridge Improvement Date: April 15, 2021

Indiana Department of Transportation

County Carroll Route SR 75 Des. No. 1601029

Part I - PUBLIC INVOLVEMENT

Every Federal action requires some level of public involvement, providing for early and continuous opportunities throughout the project development process. **The level of public involvement should be commensurate with the proposed action.**

Does the project have a historic bridge processed under the Historic Bridges PA*?	Yes <input checked="" type="checkbox"/>	No <input type="checkbox"/>
If No, then: Opportunity for a Public Hearing Required?	<input type="checkbox"/>	<input type="checkbox"/>

*A public hearing is required for all historic bridges processed under the Historic Bridges Programmatic Agreement between INDOT, FHWA, SHPO, and the ACHP.

Discuss what public involvement activities (legal notices, letters to affected property owners and residents (i.e. notice of entry), meetings, special purpose meetings, newspaper articles, etc.) have occurred for this project.

Remarks: Notice of Entry letters were mailed to potentially affected property owners near the project area on June 16, 2020 notifying them about the project and that individuals responsible for land surveying and field activities may be seen in the area. A sample copy of the Notice of Entry letter is included in Appendix G, G-1.

To meet the public involvement requirements of Section 106, a legal notice of FHWA's finding of No Historic Properties Affected was published in the *Carroll County Comet* on March 4, 2020 offering the public an opportunity to submit comment pursuant to 36 CFR 800.2(d), 800.3(e), and 800.6(a)(4). The public comment period closed 30 days later on April 3, 2020. The text of the public notice and the affidavit of publication appear in Appendix D, D-92 to D-94. No comments or responses were received.

Pursuant to the Historic Bridge Programmatic Agreement (PA), a public hearing is required. A Legal Notice of Public Hearing was published in the June 16th and June 23rd, 2021 editions of *The Carroll County Comet* and was mailed to adjacent property owners, invited Section 106 Consulting Parties, and local or state officials who may be interested in the proposed project (G-2 to G-7). The environmental document was made available in-person at the Flora Library (109 N. Center Street, Flora, IN 46929) and online at the INDOT LaPorte District webpage (www.in.gov/indot/2705.htm) and the project webpage (www.structurepointpublic.com/sr75wildecat).

The public hearing was held on Thursday, July 1, 2021 at the Cutler General Store/Cutler Merchant Building located at 73 East 485 South, Cutler, IN 46920. A formal presentation began at 6:00 p.m. Attendees had an opportunity to view a project display, project plans (Appendix B, B-5 to B-14), and the environmental document and were provided with welcome letters/project information packets (Appendix G, G-11 to G-15). Fifteen people signed in to the hearing (Appendix G, G-8 to G-10). Six verbal public comments were received (Appendix G, G-23 to G-25). The hearing presentation and materials were also made available on the project webpage in an online open house through the entirety of the comment period (June 16, 2021- July 18, 2021). No one signed into the online open house. One written comment was received during the comment window (Appendix G, G-26 to G-30). The comments received primarily focused on how the bridge closure would affect fire department services, the distance of the proposed detour route, and the length of time that Wildcat Creek would be closed to canoers at the bridge. Concerning the effect to fire department services, information about the closure was sent to the Carroll County Sheriff's Office, Carroll County Emergency Management Agency, and the Democrat Township Volunteer Fire Department on June 16, 2021. However, it is the responsibility of the project sponsor to notify school corporations and emergency services at least two weeks prior to any construction that would block of limit access. Concerning the distance of the proposed detour route, it was noted that detours for state roads must utilize other state roads and any local detours would be up to local officials. Concerning closure of Wildcat Creek to canoers, it was noted that access under the bridge is not anticipated to be closed for the duration of construction, but may be temporarily closed for the protection of canoers. Signage will be placed and a pathway for canoers will be provided to circumvent the closure. Additionally, it was requested that signage be placed at the Adams Mill Public Access Site. This request has been added to the *Environmental Commitments* section of this document. A summation of the public comments received and the project team's response to each is included in Appendix G, G-31 to G-35.

Public Controversy on Environmental Grounds

Will the project involve substantial controversy concerning community and/or natural resource impacts?	Yes <input type="checkbox"/>	No <input checked="" type="checkbox"/>
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Remarks: At this time, there is no substantial public controversy concerning impacts to the community or to natural resources.

This is page 2 of 28 Project name: SR 75 over Wildcat Creek Bridge Improvement Date: October 19, 2021

Indiana Department of Transportation

County Carroll Route SR 75 Des. No. 1601029

Part II - General Project Identification, Description, and Design Information

Sponsor of the Project: INDOT, LaPorte District INDOT District: LaPorte
Local Name of the Facility: SR 75

Funding Source (mark all that apply): Federal ☒ State ☒ Local ☐ Other* ☐

*If other is selected, please identify the funding source: _____

PURPOSE AND NEED:

Describe the transportation problem that the project will address. The solution to the traffic problem should NOT be discussed in this section. (Refer to the CE Manual, Section IV.B.2. Purpose and Need)

The need for the proposed project is a result of the insufficient legal load rating, deteriorated condition of the structural steel, superstructure, and wearing surface, and the inadequate usable shoulder width on the bridge.

- The load rating is 30 tons. The legal load rating is 36 tons.

Secondary concerns include:

- Structural steel deficiencies have been observed:
 - Loss of section (approximately 2-inch x 2-inch voids) on several vertical and diagonal truss members.
 - Thinning of the steel plates connecting truss members.
 - Two bottom chord members are fracture critical and some localized rust has been observed near the connections for these members.
 - Deformation of north floor beam from impact during 2001 rehabilitation.
- The superstructure and wearing surface are rated 5 (fair) out of 9. The superstructure has been rated at fair for over a decade. As it continues to rust and deteriorate, a rating of poor is anticipated in the near future, if the "Do Nothing" alternative is selected. With the fracture critical details, it is not advisable to allow superstructure deterioration to advance without addressing.
- The existing shoulder width on the bridge is 2 feet, 3 inches and does not meet current safety standards, which require at least 3 feet.

The purpose of the proposed project is to improve the legal load rating of the bridge to 36 tons or more so that restrictions are not required according to FHWA Load Rating policy, to improve the superstructure rating from a 5 to a 7 (good), and to improve the wearing surface rating from 5 to an 8 (very good) or 9 (new).

PROJECT DESCRIPTION (PREFERRED ALTERNATIVE):

County: Carroll Municipality: Cutler

Limits of Proposed Work: Approximately 185 feet north and 170 feet south of the center of INDOT Bridge No. 075-08-03653 B

Total Work Length: 0.067 Mile(s) Total Work Area: 0.31 Acre(s)

Is an Interchange Modification Study / Interchange Justification Study (IMS/IJS) required?

If yes, when did the FHWA grant a conditional approval for this project?

Yes ¹	No
<input type="checkbox"/>	<input checked="" type="checkbox"/>
Date: _____	

¹If an IMS or IJS is required; a copy of the approved CE/EA document must be submitted to the FHWA with a request for final approval of the IMS/IJS.

This is page 3 of 28 Project name: SR 75 over Wildcat Creek Bridge Improvement Date: October 19, 2021

Indiana Department of Transportation

County Carroll Route SR 75 Des. No. 1601029

In the remarks box below, describe existing conditions, provide in detail the scope of work for the project, including the preferred alternative. Include a discussion of logical termini. Discuss any major issues for the project and how the project will improve safety or roadway deficiencies if these are issues.

Location:

The SR 75 over Wildcat Creek Bridge [Indiana Department of Transportation (INDOT) Bridge 075-08-03653 B, National Bridge Inventory (NBI) No. 024970, Historic Bridge (HB)-1897; Indiana Historic Sites and Structures Inventory (IHSSI) No.: 015-553-65014;] is located approximately 4.5 miles south of SR 18 in Cutler, Democrat Township, Carroll County, Indiana. The project area extends approximately 185 feet north and 170 feet south of the center of INDOT Bridge No. 075-08-03653 B. The project area is more specifically located in Section 33, Township 24 North, and Range 1 West on the Rossville 7.5 Minute United States Geological Survey (USGS) topographic quadrangle (Appendix B, B-2). The total project length is approximately 0.067 miles

Existing Conditions:

The existing Parker thru truss consists of two spans, Span A is 61-foot, 6-inch long deck girder supported by beams and Span B is a 175-foot long standard INDOT truss. The bridge was constructed in 1947 and reconstructed most recently in 2001. The bridge carries SR 75 over Wildcat Creek. The bridge has a structure length of 240 feet with an out-to-out coping width of 32 feet (Appendix B, B-12). The existing typical roadway section for the bridge consists of two 11.75-foot wide travel lanes (one northbound, one south bound) bordered by 2.25-foot wide usable paved shoulders and bridge railing. The clear roadway width is 28 feet (Appendix B, B-13).

The bridge was determined eligible for listing in the National Register of Historic Places (NRHP) in the Indiana Historic Bridge Inventory (IHBI) and was designated as "Non-Select." According to the IHBI, this bridge is eligible for the NHRP under Criterion C because it represents an early or distinctive phase in bridge construction, design, or engineering and it retains the historic integrity necessary to convey its engineering significance. It represents a significant phase or feature of the work of a master. It is distinguishable when compared with similar structures and retains historic integrity necessary to convey engineering or design significance. This bridge is also important as it is one of six or fewer examples within the Indiana Department of Transportation's LaPorte District. This bridge exhibits important contributions made by an accomplished Indiana engineer, designer, fabricator or builder and displays distinctive engineering and/or aesthetic characteristics.

SR 75 is classified as a rural major collector within the limits of the project and has a posted speed limit of 55 miles per hour (mph). SR 75 is a two lane highway, with one northbound lane and one southbound lane. The existing SR 75 typical approach section consists of two 11.75-foot wide travel lanes bordered by 2.25-foot wide usable paved shoulders. There are no sidewalks within the vicinity of the bridge. Drainage throughout the project area is conveyed towards Wildcat Creek via sheet flow.

The existing right-of-way throughout the project corridor is approximately 40 feet on each side of the centerline of SR 75. The project area is primarily maintained grassed right-of-way with some wooded area along the banks of Wildcat Creek. Beyond the existing right-of-way, the land use is predominantly agricultural with some residential properties. Ground level photographs of the existing conditions within the project area are included in Appendix B, B-5 to B-7.

Preferred Alternative:

The preferred alternative involves rehabilitation of the existing structure for continued vehicular through increasing the structural capacity to at or above the required load rating. The existing bridge deck will be removed from both spans and a new concrete deck will be constructed. The existing approach slabs will be removed and replaced. Additional rehabilitation of the deck will include installing a new expansion joint at pier 2, creating a new drainage system for the structure and replacing pavement markings in-kind matching previous roadway striping.

The top flanges of truss floor beams in span B and main beams in span A will be cleaned. Shear studs will be installed on top of beams to increase load carrying capacity. Damaged or deteriorated stringers or other structural steel components will be cleaned and patched or replaced "in-kind." The damaged floor beam will be heat straightened. Gusset plates connecting the cross bracing will be replaced. The existing bearing assemblies will remain in place and will be cleaned and painted. Spalled bearing pedestals will be patched. Spalled sections on abutments and pier 2 will be patched as well. Abutment 1 will be converted into a semi integral end bent. Existing railing will be removed and replaced along with railing transition at all four ends. The original railing (constructed in 1947) will stay in place. Existing guardrail will be reset as necessary. Approximately 88 rivets that have been determined inadequate will be replaced with high strength bolts to increase the load carrying capacity. After the repairs are done, the structural steel will be painted. The usable shoulders on the bridge are currently 2.25-feet on each edge. To meet minimum useable shoulder width requirements, the lane width will be reduced from 11.75-feet to 11-feet to increase the shoulder width to 3-feet. The total clear roadway width will remain unchanged. The structure length will be increased by 0.89 feet from 240 feet to approximately 240.89 feet due to converting abutment 1 to a semi-integral configuration. The bridge approach will be tied-in. No new permanent or temporary right-of-way is anticipated to be required. For project plans, please see Appendix B, B-8 to B-14.

Indiana Department of Transportation

County Carroll Route SR 75 Des. No. 1601029

Maintenance of Traffic (MOT):

It is anticipated that MOT will include a full closure of the bridge and a detour during construction utilizing SR 18, US 421, and SR 26. The MOT will remain in place for approximately eight months. For additional details, see the Maintenance of Traffic section of this document and Appendix B, B-10.

Logical Termini and Independent Utility:

The logical termini of the proposed project were selected to provide independent utility and fulfill the purpose and need of the project. The preferred alternative's termini represent the minimum limits needed to tie in the project with the existing roadway while meeting the purpose and need of the project. This alternative has independent utility as it does not create the need for additional work and does not rely on any other project to meet the purpose and need. Therefore, it is a single and complete project.

The preferred alternative described above meets the purpose and need for the project by rehabilitating the bridge to improve the structural capacity to improve the legal load rating of the bridge to 36 tons or more so that restrictions are not required according to FHWA Load Rating policy, to improve the superstructure rating from a 5 to a 7 (good), and to improve the wearing surface rating from 5 to an 8 (very good) or 9 (new).

OTHER ALTERNATIVES CONSIDERED:

Describe all discarded alternatives, including the Do-Nothing Alternative and an explanation of why each discarded alternative was not selected.

Do Nothing:

This alternative leaves the existing deteriorating bridge as it currently exists. While this alternative eliminates cost and any environmental impacts, it would not address the purpose and need, which is to improve the legal load rating of the bridge to 36 tons or more so that restrictions are not required according to FHWA Load Rating policy, to improve the superstructure rating from a 5 to a 7 (good), and to improve the wearing surface rating from 5 to an 8 (very good) or 9 (new). Therefore, this alternative was eliminated from further consideration.

Bridge Replacement:

This alternative would replace the existing bridge. This alternative addresses the purpose and need, which is to improve the legal load rating of the bridge to 36 tons or more so that restrictions are not required according to FHWA Load Rating policy, to improve the superstructure rating from a 5 to a 7 (good), and to improve the wearing surface rating from 5 to an 8 (very good) or 9 (new). However, this alternative would result in the demolition of a "Non-Select" historic bridge. Demolition of a "Non-Select" historic bridge is not considered prudent if rehabilitation of the bridge is reasonable and prudent. Rehabilitation of the bridge was determined reasonable and prudent. Therefore, this alternative was eliminated from further consideration.

The Do Nothing Alternative is not feasible, prudent or practicable because (Mark all that apply):

It would not correct existing capacity deficiencies;

It would not correct existing safety hazards;

It would not correct the existing roadway geometric deficiencies;

It would not correct existing deteriorated conditions and maintenance problems; or

It would result in serious impacts to the motoring public and general welfare of the economy.

Other (Describe)

X
X

Indiana Department of Transportation

County Carroll Route SR 75 Des. No. 1601029

ROADWAY CHARACTER: SR 75

Functional Classification: Rural Major Collector
 Current ADT: 2430 VPD (2022) Design Year ADT: 2680 VPD (2044)
 Design Hour Volume (DHV): 243 Truck Percentage (%) 5
 Designed Speed (mph): 55 Legal Speed (mph): 55

Existing

Proposed

Number of Lanes:	2		2	
Type of Lanes:	travel		travel	
Pavement Width:	28	ft.	28	ft.
Shoulder Width:	2.25	ft.	3	ft.
Median Width:	N/A	ft.	N/A	ft.
Sidewalk Width:	N/A	ft.	N/A	ft.

Setting: ☐ Urban ☐ Suburban ☒ Rural
 Topography: ☒ Level ☐ Rolling ☐ Hilly

If the proposed action has multiple roadways, this section should be filled out for each roadway.

DESIGN CRITERIA FOR BRIDGES:

Structure/NBI Number(s): INDOT Bridge No. 075-08-03653
B/NBI No. 024970/HB-1897/ IHSSI) Sufficiency Rating: 68.5, November 8, 2019 INDOT
No.: 015-553-65014 Bridge Inspection Report for SR 75
over Wildcat Creek
 (Rating, Source of Information)

Existing

Proposed

Bridge Type:	Steel, Parker thru truss		Steel, Parker thru truss	
Number of Spans:	2		2	
Weight Restrictions:	30	ton	N/A	ton
Height Restrictions:	N/A	ft.	N/A	ft.
Curb to Curb Width:	28	ft.	28	ft.
Outside to Outside Width:	32	ft.	32	ft.
Shoulder Width:	2.25	ft.	3	ft.
Length of Channel Work:			0	ft.

Describe bridges and structures; provide specific location information for small structures.

Indiana Department of Transportation

County Carroll Route SR 75 Des. No. 1601029

Remarks: The existing Parker thru truss consists of two spans, Span A is 61-foot, 6-inch long deck girder supported by beams and Span B is a 175-foot long standard INDOT truss. The bridge was constructed in 1947 and reconstructed most recently in 2001. The bridge carries SR 75 over Wildcat Creek. The bridge has a structure length of 240 feet with an out-to-out coping width of 32 feet (Appendix B, B-12). The existing typical roadway section for the bridge consists of two 11.75-foot wide travel lanes (one northbound, one south bound) bordered by 2.25-foot wide usable paved shoulders and bridge railing. The clear roadway width is 28 feet (Appendix B, B-13).

The bridge was determined eligible for listing in the National Register of Historic Places (NRHP) in the Indiana Historic Bridge Inventory (IHBI) and was designated as "Non-Select." According to the IHBI, this bridge is eligible for the NHRP under Criterion C because it represents an early or distinctive phase in bridge construction, design, or engineering and it retains the historic integrity necessary to convey its engineering significance. It represents a significant phase or feature of the work of a master. It is distinguishable when compared with similar structures and retains historic integrity necessary to convey engineering or design significance. This bridge is also important as it is one of six or fewer examples within the Indiana Department of Transportation's LaPorte District. This bridge exhibits important contributions made by an accomplished Indiana engineer, designer, fabricator or builder and displays distinctive engineering and/or aesthetic characteristics.

Will the structure be rehabilitated or replaced as part of the project? Yes ☒ No ☐ N/A ☐
 If the proposed action has multiple bridges or small structures, this section should be filled out for each structure.

MAINTENANCE OF TRAFFIC (MOT) DURING CONSTRUCTION:

	Yes	No
Is a temporary bridge proposed?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Is a temporary roadway proposed?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Will the project involve the use of a detour or require a ramp closure? (describe in remarks)	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Provisions will be made for access by local traffic and so posted.	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Provisions will be made for through-traffic dependent businesses.	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Provisions will be made to accommodate any local special events or festivals.	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Will the proposed MOT substantially change the environmental consequences of the action?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Is there substantial controversy associated with the proposed method for MOT?	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Remarks: The MOT for the project will require a full closure of the bridge and a detour during construction utilizing SR 18, US 421, and SR 26, a distance of approximately 19.5 miles from closure point to closure point. Access to all properties within and adjacent to the project limits will be maintained at all times during project construction. The MOT will remain in place for approximately eight months. See Appendix B, B-10 for additional information regarding MOT.

The Carroll County Commissioners and Carroll County Highway Department have expressed concern over timing of the closure of the bridge to traffic during construction (Appendix C, C-41). The bridge is located in a rural, agricultural area and the bridge is important for the movement of farm equipment during planting and harvest as there is no other bridge in the area capable of handling modern planting and tillage equipment or grain trucks. The Commissioners stated that they understood the bridge must be closed to traffic for some of the construction work and stated that it would be ideal to time the bridge closure outside of the planting and harvest season. The Commissioners requested to limit closure of the bridge to the months of June, July, and August (Appendix C, C-42). The Carroll County Commissioners' recommendation is included in the *Environmental Commitments* section of this document. For additional information, see the *Public Facilities & Services* section of this document.

The closure will pose a temporary inconvenience to traveling motorists (including school buses and emergency services); however, no significant delays are anticipated and all inconveniences will cease upon project completion. Delays would occur during construction but will cease with project completion.

Indiana Department of Transportation

County Carroll Route SR 75 Des. No. 1601029

ESTIMATED PROJECT COST AND SCHEDULE:

Engineering: \$ 630,000 (2018)*
150,000 (2020)

Right-of-Way: \$ 150,000** (2020)

Construction: \$ 150,000 (2020)
1,449,414 (2023)

Anticipated Start Date of Construction: August 2022

Date project incorporated into STIP July 2, 2019

*Information is from a previously approved STIP (2018-2021)

**An administrative modification will be required as the project no longer requires right-of-way and expected cost has decreased due to rehabilitating the bridge instead of replacing it. The anticipated project schedule has changed as well.

Is the project in an MPO Area? ☐ Yes ☒ No

If yes,

Name of MPO _____

Location of Project in TIP _____

Date of incorporation by reference into the STIP _____

RIGHT OF WAY:

Land Use Impacts	Amount (acres)	
	Permanent	Temporary
Residential	0.0	0.0
Commercial	0.0	0.0
Agricultural	0.0	0.0
Forest	0.0	0.0
Wetlands	0.0	0.0
Other:	0.0	0.0
Other:	0.0	0.0
TOTAL	0.0	0.0

Describe both Permanent and Temporary right-of-way and describe their current use. Typical and Maximum right-of-way widths (existing and proposed) should also be discussed. Any advance acquisition or reacquisition, either known or suspected, and there impacts on the environmental analysis should be discussed.

Remarks: The existing right-of-way throughout the project corridor is approximately 40 feet on each side of the centerline of SR 75. The project area is primarily maintained grassed right-of-way with wooded areas along the banks of Wildcat Creek. This project will occur within existing right-of-way. No new permanent or temporary right-of-way will be required for this project.

If the scope of work or permanent or temporary right-of-way amounts change, the INDOT Environmental Services Division (ESD) and the INDOT District Environmental Section will be contacted immediately

Indiana Department of Transportation

County Carroll Route SR 75 Des. No. 1601029

Part III – Identification and Evaluation of Impacts of the Proposed Action

SECTION A – ECOLOGICAL RESOURCES

Streams, Rivers, Watercourses & Jurisdictional Ditches

Federal Wild and Scenic Rivers

State Natural, Scenic or Recreational Rivers

Nationwide Rivers Inventory (NRI) listed

Outstanding Rivers List for Indiana

Navigable Waterways

Presence

X
X
X

Impacts

Yes	No
	X
	X
	X

Remarks:

Based on a desktop review, the 2016 aerial map of the project area (Appendix B, B-3), and the water resources map in the Red Flag Investigation (RFI) report (Appendix E, E-1 to E-9), there are four streams (eight stream segments) located within the 0.5 mile search radius. Based on a site visit on October 5, 2017 by American Structurepoint, Inc., there are two streams present within the project area.

INDOT Ecology and Waterway Permitting Office approved a *Wetland Delineation Report* on May 8, 2019 (Appendix F, F-37). Please refer to Appendix F, F-1 to F-36 for the *Wetland Delineation Report*. It was determined that two streams [Wildcat Creek and Unnamed Tributary (UNT) 1] are within the project area and would likely be considered jurisdictional waters of the U.S. The U.S. Army Corps of Engineers (USACE) makes all final determinations regarding jurisdiction.

The Federal Wild and Scenic Rivers listing, State Natural Scenic and Recreational Rivers listing, Outstanding Rivers List, navigable waterways list, and National Rivers Inventory list were researched by American Structurepoint, Inc. to determine the possible presence of protected waterways in the project area. This section of Wildcat Creek is a State Natural Scenic and Recreational River and is on the Outstanding Rivers List.

Wildcat Creek flows west through the project area under INDOT Bridge No. 075-08-03653 and has an upstream drainage area of 375 square miles (<https://water.usgs.gov/osw/streamstats/>). Wildcat Creek is a state designated scenic river, designated as outstanding, a state designated canoe/boating route, and is also designated as having high water quality. The ordinary high water mark (OHWM) of Wildcat Creek was approximately 110 feet wide by 2 feet deep. Wildcat Creek was classified as a good quality stream. Wildcat Creek is listed for *E. coli* and PCBs (and/or mercury) in fish tissue (Appendix E, E-1 to E-9). Workers who are working in or near water with *E. coli* should take care to wear appropriate Personal Protective Equipment (PPE), observe proper hygiene procedures, including regular hand washing, and limit personal exposure. Exposure to PCBs (and/or mercury) in fish tissue is considered low, assuming workers are not eating biota surrounding or associated with the water body. If there will be sediment and/or soils disturbed by construction, additional investigation may be necessary. This has been added as a firm commitment to this environmental document. It is anticipated that both permanent and temporary impacts to Wildcat Creek will be avoided by this project.

UNT 1 flows south through the project area into Wildcat Creek, approximately 40 feet east of SR 75. The OHWM of UNT 1 was approximately 6 feet wide by 4 inches deep. The stream was classified as a poor quality stream. It is anticipated that both permanent and temporary impacts to UNT 1 will be avoided by this project. UNT 1 has been marked "Do Not Disturb" on the plans (Appendix B, B-11). Therefore, no impacts are expected.

Early coordination letters were sent to the U.S. Fish and Wildlife Service (USFWS), the IDNR-Division of Fish and Wildlife (DFW), IDNR-Division of Outdoor Recreation, the USACE Louisville District, and the Carroll County Surveyor's Office on July 27, 2018 (Appendix C, C-1 to C-2). The USACE and Carroll County Surveyor's Office did not respond to the early coordination letter.

The USFWS responded on August 9, 2018 with recommendations about terrestrial habitat; however, none of the recommendations were about streams (Appendix C, C-18 to C-19).

The IDNR-DFW responded on August 24, 2018 with recommendations to avoid or minimize impacts to streams. The response included recommendations regarding stream crossing structure design, bank stabilization, riparian habitat,

Indiana Department of Transportation

County Carroll Route SR 75 Des. No. 1601029

revegetation, inchannel work, riprap, and timing restrictions on work in waterways (Appendix C, C-38 to C-40).

The IDNR-Division of Outdoor Recreation responded on August 8, 2018 with recommendations to avoid impacts to streams. The response stated the project will occur within a section of Wildcat Creek that is designated as a Natural, Scenic, and Recreation River System. If all preventative measures are taken to avoid impacts to the river and its recreational use there should be no negative impacts (Appendix C, C-17). In response to additional coordination, IDNR-Division of Outdoor Recreation stated the project will neither enhance nor detract from the scenic quality of the river because the project is rehabilitating an existing bridge. Furthermore, as long as there is signage signifying closures and there is a pathway for water trail users to circumvent potential closures, the Division of Outdoor Recreation would mark approval for this project (Appendix I, I-23 to I-25).

The IDEM automated response with standard recommendations about streams was received on July 30, 2018 (Appendix C, C-3 to C-10).

All applicable USFWS, IDNR-DFW, and IDNR-Division of Outdoor Recreation recommendations are included in the *Environmental Commitments* section of this CE document.

Other Surface Waters

Reservoirs

Lakes

Farm Ponds

Detention Basins

Storm Water Management Facilities

Other: _____

Presence

Impacts

Yes

No

Remarks:

Based on a desktop review, a site visit on October 5, 2017 by American Structurepoint, Inc., the 2016 aerial map of the project area (Appendix B, B-3), and the water resource map in the RFI report (Appendix E, E-7) there are three other surface waters within the 0.5 mile search radius. No other surface waters are present within the project area, therefore, no impacts are expected.

Early coordination letters were sent to the USFWS, the IDNR-DFW, and the USACE on July 27, 2018 (Appendix C, C-1 to C-2).

The USFWS responded on August 9, 2018 with recommendations about terrestrial habitat; however, none of the recommendations were about other surface waters (Appendix C, C-18 to C-19).

The IDNR-DFW responded on August 24, 2018 with recommendations to avoid and minimize impacts to fish, wildlife, and botanical resources; however, none of the recommendations were about other surface waters (Appendix C, C-38 to C-40).

The IDEM automated response with standard recommendations about other surface waters was received on July 30, 2018 (Appendix C, C-3 to C-10).

All applicable USFWS and IDNR-DFW recommendations are included in the *Environmental Commitments* section of this CE document.

Indiana Department of Transportation

County Carroll Route SR 75 Des. No. 1601029

Presence

Impacts

Yes

No

Wetlands

☒ X

☐

☒ X

Total wetland area: 0.126 acre(s) Total wetland area impacted: 0.0 acre(s)

(If a determination has not been made for non-isolated/isolated wetlands, fill in the total wetland area impacted above.)

Wetland No.	Classification	Total Size (Acres)	Impacted Acres	Comments
Wetland A	Palustrine, Emergent, Persistent, Seasonally Flooded/Saturated wetland (PEM1E)	0.002	N/A	Wetland A is located west of SR 75 and approximately 0.1 mile north of Wildcat Creek. Wetland A is located outside of the construction limits of the proposed project. Wetland A would be considered a poor wetland.
Wetland B	PEM1E	0.124	N/A	Wetland B is located along the south bank of Wildcat Creek, east and west of SR 75. Wetland B would be considered an average wetland.

Documentation

ES Approval Dates

Wetlands (Mark all that apply)

Wetland Determination

Wetland Delineation

USACE Isolated Waters Determination

Mitigation Plan

X

August 7, 2018

Improvements that will not result in any wetland impacts are not practicable because such avoidance would result in (Mark all that apply and explain):

Substantial adverse impacts to adjacent homes, business or other improved properties;

Substantially increased project costs;

Unique engineering, traffic, maintenance, or safety problems;

Substantial adverse social, economic, or environmental impacts, or

The project not meeting the identified needs.

Measures to avoid, minimize, and mitigate wetland impacts need to be discussed in the remarks box.

Remarks:

Based on a review of the National Wetlands Inventory (NWI) online mapper (<https://www.fws.gov/wetlands/data/Mapper.html>), the USGS topographic map (Appendix B, B-2), and the RFI report (Appendix E-1 to E-9) there are 13 wetlands located within the 0.5 mile search radius; of these, two are mapped within the project area. Based on a site visit on October 5, 2017 by American Structurepoint, Inc., there are two wetlands present within the project area.

INDOT Ecology and Waterway Permitting Office approved a *Wetland Delineation Report* on May 8, 2019 (Appendix F, F-37). Please refer to Appendix F, F-1 to F-36 for the *Wetland Delineation Report*. It was determined that two Wetlands (Wetland A and B) are within the project area and would likely be considered jurisdictional waters of the US. The USACE makes all final determinations regarding jurisdiction.

The wetlands will be avoided by construction. Wetland B has been marked "Do Not Disturb" on the plans (Appendix B, B-11). Wetland A is located far enough outside the construction limits that it does not appear on the plan sheets. Therefore, no impacts are expected.

Early coordination letters were sent to the USFWS, the IDNR-DFW, and the USACE Louisville District on July 27, 2018 (Appendix C, C-1 to C-2). The USACE did not respond to the early coordination letter.

The USFWS responded on August 9, 2018 with recommendations about terrestrial habitat; however, none of the

Indiana Department of Transportation

County Carroll Route SR 75 Des. No. 1601029

recommendations were about wetlands (Appendix C, C-18 to C-19).

The IDNR-DFW responded on August 24, 2018 with recommendations to avoid and minimize impacts to fish, wildlife, and botanical resources; however, none of the recommendations were about wetlands (Appendix C, C-38 to C-40).

The IDEM automated response with standard recommendations about wetlands was received on July 30, 2018 (Appendix C, C-3 to C-10).

All applicable USFWS and IDNR-DFW recommendations are included in the *Environmental Commitments* section of this CE document.

	<u>Presence</u>	<u>Impacts</u>	
		<u>Yes</u>	<u>No</u>
Terrestrial Habitat	X	X	
Unique or High Quality Habitat			

Use the remarks box to identify each type of habitat and the acres impacted (i.e. forested, grassland, farmland, lawn, etc).

Remarks:

Based on a desktop review, a site visit on October 5, 2017 by American Structurepoint, Inc., and the 2016 aerial map of the project area (Appendix B, B-3), there is maintained grassy right-of-way within the project area and woody vegetation along Wildcat Creek. Dominant floral species noted during the field investigation included common reed (*Phragmites australis*), tall fescue (*Schedonorus arundinaceus*), riverbank grape (*Vitis riparia*), giant foxtail (*Setaria faberi*), Canada goldenrod (*Solidago canadensis*), Virginia wild rye (*Elymus virginicus*), riverbank wild rye (*Elymus riparius*), clearweed (*Pilea pumila*), Reed canarygrass (*Phalaris arundinacea*), rice cutgrass (*Leersia oryzoides*), swamp smartweed (*Persicaria hydropiperoides*), calico aster (*Symphyotrichum lateriflorum*), green bristlegrass (*Setaria viridis*), Pennsylvania smartweed (*Persicaria pennsylvanica*), Kentucky bluegrass (*Poa pratensis*), Canadian black snakeroot (*Sanicula canadensis*), white snakeroot (*Ageratina altissima*), common sneezeweed (*Helenium autumnale*), stinging nettle (*Urtica dioica*), Poison ivy (*Toxicodendron radicans*), common woodsedge (*Carex blanda*), scouring rush (*Equisetum hyemale*), and bristly greenbrier (*Smilax hispida*). Tree species noted during the field investigation included black walnut (*Juglans nigra*), hackberry (*Celtis occidentalis*), silver maple (*Acer saccharinum*), honey locust (*Gleditsia triacanthos*), American elm (*Ulmus americana*), red mulberry (*Morus rubra*), paw-paw (*Asimina triloba*), and eastern redbud (*Cercis canadensis*). Ground level photos of the project area taken during the October 5, 2017 site visit can be found in Appendix F, F-19 to F-36.

Due to the need to provide access for construction, approximately 0.15 acre of terrestrial habitat (maintained grassy right-of-way) may be impacted. No tree clearing is anticipated. Implementation of INDOT Standard Specifications for re-vegetation of disturbed areas will promote re-establishment of similar ground cover in the areas temporarily impacted by construction equipment access. Therefore, the project is not expected to have an adverse impact on wildlife habitat or passage. No mitigation is anticipated.

Early coordination letters were sent to the USFWS and the IDNR-DFW on July 27, 2018 (Appendix C, C-1 to C-2).

The USFWS responded on August 9, 2018 with recommendations to avoid or minimize impacts to terrestrial habitat. The response included recommendations regarding potential new bridge placement, tree clearing, and mitigation (Appendix C, C-18 to C-19).

The IDNR-DFW responded on August 24, 2018 with recommendations to avoid or minimize impacts to terrestrial habitat. The response included recommendations regarding revegetation and tree and brush clearing (Appendix C, C-38 to C-40).

The IDEM automated response with standard recommendations about terrestrial habitat was received on July 30, 2018 (Appendix C, C-3 to C-10).

All applicable USFWS and IDNR-DFW recommendations are included in the *Environmental Commitments* section of this CE document.

If there are high incidences of animal movements observed in the project area, or if bridges and other areas appear to be the sole corridor for animal movement, consideration of utilizing wildlife crossings should be taken.

This is page 12 of 28 Project name: SR 75 over Wildcat Creek Bridge Improvement Date: October 19, 2021

Indiana Department of Transportation

County Carroll Route SR 75 Des. No. 1601029

Karst

Is the proposed project located within or adjacent to the potential Karst Area of Indiana?
Are karst features located within or adjacent to the footprint of the proposed project?

Yes	No
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>

If yes, will the project impact any of these karst features?

Use the remarks box to identify any karst features within the project area. (Karst investigation must comply with the Karst MOU, dated October 13, 1993)

Remarks:

Based on a desktop review, the project is located outside the designated karst region of Indiana as outlined in the October 13, 1993 Memorandum of Understanding (MOU). According to the topo map of the project area (Appendix B, B-2), and the RFI report (Appendix E-1 to E-9), there are no karst features identified within or adjacent to the project area. In the early coordination response, the Indiana Geological Survey (IGS) did not indicate that karst features exist in the project area (Appendix C, C-11 to C-13). The response indicated High liquefaction potential and 1% Annual Chance Flood Hazards as the geological hazards within the project area. The response also indicated mineral resources potentially exist within the project area. Bedrock resources are classified as having "high potential" and sand and gravel resources are classified as having "low potential." These features will not be affected because the project requires minimal excavation (approximately 3 feet). Response from IGS has been communicated with the designer on March 2, 2020. No impacts are expected.

Threatened or Endangered Species

Within the known range of any federal species
Any critical habitat identified within project area
Federal species found in project area (based upon informal consultation)
State species found in project area (based upon consultation with IDNR)

Presence

Impacts

Yes	No
<input checked="" type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>

Is Section 7 formal consultation required for this action?

Yes	No
<input type="checkbox"/>	<input checked="" type="checkbox"/>

Remarks:

Based on a desktop review and the RFI report (Appendix E-1 to E-9), completed by American Structurepoint, Inc. on December 18, 2018, the IDNR Carroll County Endangered, Threatened and Rare (ETR) Species List has been checked and is included in (Appendix E, E-8 to E-9). The highlighted species on the list reflect the federal and state identified ETR species located within the county. According to the IDNR-DFW early coordination response letter dated August 24, 2018 (Appendix C, C-38 to C-40), the Natural Heritage Program's Database has been checked and the Wavyrayed Lampmussell (*Lampsilis fasciola*), a state species of special concern, has been documented in Wildcat Creek within 0.5 mile of the project area. The response indicated the IDNR does not foresee any impacts to the Wavyrayed Lampmussell as a result of this project. Due to the age of the Natural Heritage Program Database check, a new check was conducted on March 2, 2021 (Appendix C, C-43 to C-45). No additional records were listed, Wavyrayed Lampmussell (*Lampsilis fasciola*) was still the only species documented within 0.5 mile of the project area. According to the USFWS early coordination response letter dated August 9, 2018, the project is within the range of the Federally endangered Indiana Bat (*Myotis sodalis*), clubshell mussel (*Pleurobema clava*), fanshell mussel (*Cyprogenia stegaria*), rayed bean mussel (*Villosa fabalis*), sheepnose mussel (*Plethobasus cyphus*), and snuffbox mussel (*Epioblasma triquetra*), and the threatened northern long-eared bat (*Myotis septentrionalis*), eastern massasauga rattlesnake (*Sistrurus catenatus*), and rabbitsfoot mussel (*Quadrula cylindrica cylindrica*). Impacts to the two bat species will be determined utilizing the bat programmatic informal consultation process (see below). The USFWS stated there is no habitat for the mussel species and eastern massasauga within Wildcat Creek, so they agreed that the proposed project is not likely to adversely affect these endangered and threatened mussel and snake species (Appendix C, C-18 to C-19).

Project information was submitted through the USFWS's Information for Planning and Consultation (IPaC) portal, and an official species list was generated (Appendix C, C-20 to C-25). The project is within range of the federally endangered Indiana bat (*Myotis sodalis*) and the federally threatened northern long-eared bat (NLEB) (*Myotis septentrionalis*). No additional species were found within or adjacent to the project area other than the Indiana bat and northern long-eared bat.

The project qualifies for the *Range-wide Programmatic Informal Consultation for the Indiana bat and northern long-*

Indiana Department of Transportation

County Carroll Route SR 75 Des. No. 1601029

earred bat (NLEB), dated May 2016 (revised February 2018), between FHWA, Federal Railroad Administration (FRA), Federal Transit Administration (FTA), and USFWS. An effect determination key was completed on October 14, 2020 and based on the responses provided, the project was found to may affect, not likely to adversely affect (NLAA) the Indiana bat and/or the NLEB. INDOT reviewed and verified the effect finding on October 14, 2020, and requested USFWS's review of the finding (Appendix C, C-26 to C-37). No response was received from USFWS within the 14-day review period; therefore, it was concluded they concur with the finding. Avoidance and Mitigation Measures (AMMs) are included as firm commitments in the *Environmental Commitments* section of this document.

Migratory Birds:

SR 75 over Wildcat Creek Bridge (INDOT Bridge 075-08-03653 B) has shown evidence of use (i.e. nests) by a bird species protected under the Migratory Bird Treaty Act (MBTA) during the October 5, 2017 inspection. Avoidance and minimization measures must be implemented prior to the start of and during the nesting season. Nests without eggs or young should be removed prior to construction during the non-nesting season (September 8 – April 30) and during the nesting season if no eggs or young are present. Nests with eggs or young cannot be removed or disturbed during the nesting season (May 1 – September 7). Nests with eggs or young should be screened or buffered from active construction. Details of the required procedures are outlined in the “Potential Migratory Bird on Structure Unique Special Provision.” This firm commitment is included in the *Environmental Commitments* of this document.

This precludes the need for further consultation on this project as required under Section 7 of the Endangered Species Act, as amended. If new information on endangered species at the site becomes available, or if project plans are changed, USFWS will be contacted for consultation.

SECTION B – OTHER RESOURCES

Drinking Water Resources

Wellhead Protection Area
Public Water System(s)
Residential Well(s)
Source Water Protection Area(s)
Sole Source Aquifer (SSA)

Presence

Impacts

Yes

No

If a SSA is present, answer the following:

Is the Project in the St. Joseph Aquifer System?
Is the FHWA/EPA SSA MOU Applicable?
Initial Groundwater Assessment Required?
Detailed Groundwater Assessment Required?

Yes

No

Remarks:

Sole Source Aquifer:

The project is located in Carroll County, which is not located within the area of the St. Joseph Sole Source Aquifer, the only legally designated sole source aquifer in the state of Indiana. Therefore, the FHWA/EPA Sole Source Aquifer Memorandum of Understanding (MOU) is not applicable to this project. Therefore a detailed groundwater assessment is not needed and no impacts are expected.

Wellhead Protection Area and Source Water :

The Indiana Department of Environmental Management's Wellhead Proximity Determinator website (<http://www.in.gov/idem/cleanwater/pages/wellhead/>) was accessed on March 17, 2020 by American Structurepoint, Inc. This project is not located within a Wellhead Protection Area or Source Water Area. No impacts are expected.

Water Wells:

The Indiana Department of Natural Resources Water Well Record Database website (<https://www.in.gov/dnr/water/3595.htm>) was accessed on March 17, 2020 by American Structurepoint, Inc. No wells are located near this project. Therefore, no impacts are expected.

Indiana Department of Transportation

County Carroll Route SR 75 Des. No. 1601029

Urban Area Boundary:

Based on a desktop review of the INDOT MS4 website (<https://entapps.indot.in.gov/MS4/>) by March 17, 2020 by American Structurepoint, Inc. and the RFI report; this project is not located in an Urban Area Boundary location. No impacts are expected.

Public Water System:

Based on a desktop review, a site visit on October 5, 2017 by American Structurepoint, Inc. and the 2016 aerial map of the project area (Appendix B, B-3) no public water systems were identified. Therefore, no impacts are expected.

Flood Plains

Longitudinal Encroachment
Transverse Encroachment
Project located within a regulated floodplain
Homes located in floodplain within 1000' up/downstream from project

Presence

X
X

Impacts

Yes	No
X	
X	

Discuss impacts according to classification system described in the "Procedural Manual for Preparing Environmental Studies".

Remarks:

Based on a desktop review of The IDNR Indiana Floodway Information Portal website (<http://dnrmmaps.dnr.in.gov/appsphp/fdms/>) on March 17, 2020 by American Structurepoint, Inc. and the RFI report (Appendix E, E-1 to E-9); this project is located in a regulatory floodplain as determined from approved IDNR floodplain maps (Appendix F, F-38). An early coordination letter was sent on October 5, 2020 to the local floodplain Administrator with Carroll County Area Plan Commission (Appendix C, C-1 to C-2). The floodplain administrator did not respond within the 30-day time frame. The IDNR-DFW responded to early coordination on August 24, 2018 and stated the project will require the formal approval for construction in a floodway under the Flood Control Act, IC 14-28-1 (Appendix C, C-38 to C-40). This project qualifies as a Category 3 per the current INDOT CE Manual, which states:

The modifications to drainage structures included in this project will result in an insubstantial change in their capacity to carry flood water. This change could cause a minimal increase in flood heights and flood limits. These minimal increases will not result in any substantial adverse impacts on the natural and beneficial floodplain values; they will not result in substantial change in flood risks or damage; and they do not have substantial potential for interruption or termination of emergency service or emergency routes; therefore, it has been determined that this encroachment is not substantial.

Farmland

Agricultural Lands
Prime Farmland (per NRCS)

Presence

X

Impacts

Yes	No
	X

Total Points (from Section VII of CPA-106/AD-1006* _____

**If 160 or greater, see CE Manual for guidance.*

See CE Manual for guidance to determine which NRCS form is appropriate for your project.

Remarks:

Based on a desktop review, a site visit on October 5, 2017 by American Structurepoint, Inc. and the 2016 aerial map of the project area (Appendix B, B-3), there is land that meets the definition of farmland under the Farmland Protection Policy Act (FPPA) adjacent to the project but not within the construction limits of the proposed project. This project will stay within existing right-of-way, therefore no impacts are expected. An early coordination letter was sent on July 27, 2018 to the Natural Resources Conservation Services (NRCS). NRCS responded on August 2, 2018 stating that the impacts would not be able to be determined at this time due to lack of site specific information (Appendix C, C-15). Additional information was provided to NRCS on October 5, 2020. NRCS responded on November 16, 2020 and stated the revised project will not cause a conversion of farmland (Appendix C, C-16). The requirements of the FPPA do not apply to this project; therefore, no impacts are expected.

Indiana Department of Transportation

County Carroll Route SR 75 Des. No. 1601029

SECTION C – CULTURAL RESOURCES

	Category	Type	INDOT Approval Dates	N/A
Minor Projects PA Clearance				X

Eligible and/or Listed Resource Present

Results of Research

Archaeology	
NRHP Buildings/Site(s)	
NRHP District(s)	
NRHP Bridge(s)	X

Project Effect

No Historic Properties Affected ☒ No Adverse Effect ☐ Adverse Effect ☐

Documentation Prepared

Documentation (mark all that apply)

	ES/FHWA Approval Date(s)	SHPO Approval Date(s)
Historic Properties Short Report	X	November 16, 2018
Historic Property Report		December 18, 2018
Archaeological Records Check/ Review		
Archaeological Phase Ia Survey Report	X	October 11, 2018
Archaeological Phase Ic Survey Report		December 18, 2018
Archaeological Phase II Investigation Report		
Archaeological Phase III Data Recovery		
APE, Eligibility and Effect Determination	X	February 28, 2020
800.11 Documentation	X	March 12, 2020

Memorandum of Agreement (MOA) ☐

MOA Signature Dates (List all signatories)

Describe all efforts to document cultural resources, including a detailed summary of the Section 106 process, using the categories outlined in the remarks box. The completion of the Section 106 process requires that a Legal Notice be published in local newspapers. Please indicate the publication date, name of paper(s) and the comment period deadline. Likewise include any further Section 106 work which must be completed at a later date, such as mitigation or deep trenching.

Remarks:

Area of Potential Effect (APE):

The APE for aboveground resources was drawn to encompass potential impacts—including visual, increased noise, and traffic-related impacts—that may result from the undertaking. The APE was generally drawn to include properties within a 0.25 mile (1,320 foot) buffer from the proposed project location. The APE was narrowed slightly to the southeast due to topography and intervening structures. The APE for archaeology is the project footprint. A map of the APE is included in Appendix D, D-13.

Coordination with Consulting Parties:

The SHPO is considered an automatic consulting party for all undertakings. On November 18, 2018, the parties identified in the table below were invited to participate as Section 106 consulting parties. On December 18, 2018, the Indiana SHPO responded to the Early Coordination Letter (ECL) (Appendix D, D-49 to D-50). In reviewing the consulting parties list provided with the ECL, the staff of the SHPO was “not aware of anyone who should be invited to become a consulting party for the purposes of the review of this project under Section 106, beyond those whom you have already invited.” If no response was received to the consulting party invitation after thirty (30) days, it was assumed the invited

Indiana Department of Transportation

County Carroll Route SR 75 Des. No. 1601029

parties did not wish to act as consulting parties for the undertaking. On December 10, 2018, at INDOT's request, the Carroll County Historic Bridge Coalition was sent an invitation to join in consultation. Additional consulting parties were added per their individual requests to INDOT or the consultant. Consulting parties overwhelmingly expressed support for rehabilitating the bridge for continued vehicular use. For reference to the Consulting Party Invitation, responses, and requests to join consultation see Appendix D, D-35 to D-77.

Agency/Organization	Response
Carroll County Commissioners	September 6, 2019-Will participate
Tippecanoe County Area Plan	No response
Indiana Landmarks, Western Regional Office	November 20, 2018-Will participate
Carroll County Historical Society and Museum	April 15, 2019-Will participate
Carroll County Historian	April 7, 2019-Will participate
Carroll County Highway Department Superintendent/Engineer	No response
Historic Spans Task Force	March 31, 2019 -Will participate
Eastern Shawnee Tribe of Oklahoma	No response
Forest County Potawatomi Community	December 20, 2018-Will participate
Miami Tribe of Oklahoma	December 13, 2018-Will participate
Peoria Tribe of Indians of Oklahoma	No response
Pokagon Band of Potawatomi Indians	No response
Carroll County Historic Bridge Coalition	No response
Heartland Heritage, Inc.	December 6, 2018-Joined via request
Delphi Preservation Society	April 6, 2019-Joined via request
NICHES Land Trust	April 9, 2019-Joined via request
Carroll County Wabash & Erie Canal Inc.	April 13, 2019-Joined via request
Adams Mill, Inc.	April 17, 2019-Joined via request
Carroll County Chamber of Commerce	April 18, 2019-Joined via request
Promoting Wildcat Valley	April 19, 2019-Joined via request
IDNR, Division of Outdoor Recreation	April 23, 2019-Joined via request
Wildcat Guardians	May 14, 2019-Joined via request
HistoricBridges.org	October 3, 2019-Joined via request

Archaeology:

Weintraut and Associates (W&A) initiated identification and evaluation of archaeological resources with a Phase Ia records check on February 23, 2018. Based upon INDOT procedures, a field reconnaissance for archaeology was conducted on March 8, 2018. The Phase Ia Archaeological Records Check and Field Reconnaissance: *State Road 75 over Wildcat Creek Bridge Project: Scope Undetermined in Democrat Township, Carroll County, Indiana, Des. No.: 1601029* was prepared by W&A personnel who meet the *Secretary of the Interior's Professional Qualification Standards* as per 36 CFR Part 61 and approved by INDOT CRO on October 11, 2018 Appendix D, D-26 to D-29. The Archaeology Short Report (ASR) identified no archaeological resources in the project area, and the project was recommended to proceed as planned.

SHPO responded to the ASR in a letter dated December 18, 2018. The staff stated that based upon the available documentation it had "not identified any currently known archaeological resources listed in or eligible for inclusion in the NRHP within the proposed project area..." and concurred with the opinion of the archaeologist that "no further archaeological investigations appear necessary at this proposed project area." The SHPO, however, cautioned that "if any prehistoric or historic archaeological artifacts or human remains are uncovered during construction, demolition, or earthmoving activities, state law (Indiana Code 14-21-1-27 and -29) requires that the discovery must be reported to the Department of Natural Resources within two (2) business days." SHPO also noted that the HPSR states that the bridge was most recently repaired in 2001 (Appendix D, D-49 to D-50).

Historic Properties:

The *Short Historic Property Report, State Road 75 over Wildcat Creek Bridge Project: Scope Undetermined in Democrat Township, Carroll County, Indiana, Des. No.: 1601029* was prepared by W&A personnel who meet the *Secretary of the Interior's Professional Qualification Standards* as per 36 CFR Part 61 and approved by INDOT CRO on November 16, 2018. The Historic Property Short Report (HPSR) identified one resource, the SR 75 Bridge over Wildcat Creek (Bridge No. 075-08-03653B, IHSSI No.: 015-553-65014; HB 1897; NBI No.: 24970) that had been previously determined eligible for listing in the NRHP in the Indiana Historic Bridge Inventory. Historians did not recommend any additional resources for listing in the NRHP (Appendix D, D-30 to D-31).

Indiana Department of Transportation

County Carroll Route SR 75 Des. No. 1601029

SHPO responded to the HPSR, in a letter dated December 18, 2018. SHPO agreed that the APE designated in the HPSR appeared appropriate. The staff of the SHPO noted that the SR 75 Bridge over Wildcat Creek has been identified as part of the Indiana Historic Bridge Inventory as a Non-Select Parker through truss bridge that is eligible for inclusion in the NRHP under Criterion C. Also, the staff concurred with the findings of the HPSR that the project bridge is "the only historic property listed in or eligible for listing in the NRHP identified within the APE." (Appendix D, D-49 to D-50).

A Historic Bridge Alternatives Analysis (HBAA) was transmitted to consulting parties on December 5, 2019. That document recommended rehabilitation for continued vehicular use as the preferred alternative ("B-1 Rehabilitation for Continued Vehicular Use Meeting Secretary of the Interior's Standards") (Appendix D, D-78 to D-89).

The Indiana SHPO responded to the HBAA on January 6, 2020, and agreed that B-1 Rehabilitation for Continued Vehicular Use Meeting Secretary of the Interior's Standards "is the feasible and prudent and should be pursued for the proposed project." SHPO stated it looked forward to receiving the 30 percent design plans for review and comment. SHPO also stated "photographic documentation of the bridge and its immediate surroundings should be produced before site preparation or construction work begins." SHPO also added that "we may have other specific recommendations for the photographic documentation" once more information is provided about the project in the design plans. SHPO also stated "We commend the Carroll County Board of Commissioners for its stated resolution urging INDOT to maintain and not replace the bridge." (Appendix D, D-85 to D-86).

No other efforts were undertaken to identify and evaluate historic properties, and no other comments were received.

Design plans will be submitted to consulting parties for review and comment at 30 percent, 60 percent, and final. In addition, photo documentation will be prepared prior to construction as requested by the SHPO in their letter dated January 6, 2020 (Appendix D, D-85 to D-86). This has been added as firm commitment to this environmental document.

Documentation Finding:

INDOT, acting on behalf of FHWA, has determined a finding of "No Historic Properties Affected" is appropriate for this undertaking because there are no resources listed in, or eligible for listing in, the NRHP within the APE other than the SR 75 over Wildcat Creek Bridge (IHSSI No.: 171-681-35023; HB 1739; NBI No.: 8600029). The SHPO concurred with the finding of "No Historic Properties Affected" on March 12, 2020 (Appendix D, D-90 to D-91). No other consulting parties provided comments on the "No Historic Properties Affected" finding or supporting documentation.

Per the terms of the Historic Bridge PA, the FHWA will satisfy its Section 106 responsibilities involving "Select" and "Non-Select" bridges through the Project Development Process (PDP) of the Historic Bridges PA (Stipulation III). The SR 75 over Wildcat Creek Bridge has been classified as a Non-Select Bridge by the INDOT Historic Bridge Inventory and, thus, the procedures outlined in Stipulation III.B. of the Historic Bridges PA will be followed to fulfill FHWA's Section 106 responsibilities for the bridge. Therefore, the finding for this project only applies to other resources located within the APE and not the SR 75 over Wildcat Creek Bridge.

It should be noted that per the permanent rule issued by the IDNR effective August 14, 2013 (312 IAC 20-4-11.5), INDOT is requesting that this project be subjected to "dual review"; that is, reviewed by the DHPA simultaneously under 54 U.S.C. 306108 (Section 106) and IC 14-21-1-18 (Indiana Preservation and Archaeology Law dealing with alterations of historic sites and structures requiring a Certificate of Approval). Pursuant to Section 11.5(f) of this rule, at the conclusion of the review process INDOT anticipates that the Division Director would issue a letter of clearance exempting this project from obtaining a Certificate of Approval under IC 14-21-1-18. This is included as a firm commitment in Section J of this document.

Public Involvement:

To meet the public involvement requirements of Section 106, a legal notice of FHWA's finding of No Historic Properties Affected was published in the *Carroll County Comet* on March 4, 2020 offering the public an opportunity to submit comment pursuant to 36 CFR 800.2(d), 800.3(e), and 800.6(a)(4). The public comment period closed 30 days later on April 3, 2020. The text of the public notice and the affidavit of publication appear in Appendix D, D-92 to D-94. No comments or responses were received.

Pursuant to the Historic Bridge PA, a public hearing is required. A Legal Notice of Public Hearing was published in the June 16th and June 23rd, 2021 editions of *The Carroll County Comet* and was mailed to adjacent property owners, invited Section 106 Consulting Parties, and local or state officials who may be interested in the proposed project (G-2 to G-7). The environmental document was made available in-person at the Flora Library (109 N. Center Street, Flora, IN 46929) and online at the INDOT LaPorte District webpage (www.in.gov/indot/2705.htm) and the project webpage

Indiana Department of Transportation

County Carroll Route SR 75 Des. No. 1601029

(www.structurepointpublic.com/sr75wildcat).

The public hearing was held on Thursday, July 1, 2021 at the Cutler General Store/Cutler Merchant Building located at 73 East 485 South, Cutler, IN 46920. A formal presentation began at 6:00 p.m. Attendees had an opportunity to view a project display, project plans (Appendix B, B-5 to B-14), and the environmental document and were provided with welcome letters/project information packets (Appendix G, G-11 to G-15). Fifteen people signed in to the hearing (Appendix G, G-8 to G-10). Six verbal public comments were received (Appendix G, G-23 to G-25). The hearing presentation and materials were also made available on the project webpage in an online open house through the entirety of the comment period (June 16, 2021- July 18, 2021). No one signed into the online open house. One written comment was received during the comment window (Appendix G, G-26 to G-30). The comments received primarily focused on how the bridge closure would affect fire department services, the distance of the proposed detour route, and the length of time that Wildcat Creek would be closed to canoers at the bridge. Further details regarding the public comments received and the project team's responses can be found in the *Public Involvement* section of this document. This completes the Section 106 process and the responsibilities of the FHWA under Section 106 have been fulfilled.

SECTION D – SECTION 4(f) RESOURCES/ SECTION 6(f) RESOURCES

Section 4(f) Involvement (mark all that apply)

Parks & Other Recreational Land

Publicly owned park
Publicly owned recreation area
Other (school, state/national forest, bikeway, etc.)

Presence

X

Use

Yes	No
	X

Evaluations Prepared

Programmatic Section 4(f)*
"De minimis" Impact*
Individual Section 4(f)

FHWA Approval date

--

Wildlife & Waterfowl Refuges

National Wildlife Refuge
National Natural Landmark
State Wildlife Area
State Nature Preserve

Presence

Use

Yes	No

Evaluations Prepared

Programmatic Section 4(f)*
"De minimis" Impact*
Individual Section 4(f)

FHWA Approval date

--

Historic Properties

Sites eligible and/or listed on the NRHP

Presence

X

Use

Yes	No
X	

Evaluations Prepared

Programmatic Section 4(f)*
"De minimis" Impact*
Individual Section 4(f)

X

FHWA Approval date

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This is page 19 of 28 Project name: SR 75 over Wildcat Creek Bridge Improvement Date: October 19, 2021

Indiana Department of Transportation

County Carroll Route SR 75 Des. No. 1601029

**FHWA approval of the environmental document also serves as approval of any Section 4f Programmatic and/or De minimis evaluation(s) discussed below.*

Discuss Programmatic Section 4(f) and "de minimis" Section 4(f) impacts in the remarks box below. Individual Section 4(f) documentation must be separate Draft and Final documents. For further discussions on Programmatic, "de minimis" and Individual Section 4(f) evaluations please refer to the "Procedural Manual for the Preparation of Environmental Studies". Discuss proposed alternatives that satisfy the requirements of Section 4(f).

Remarks:

Section 4(f) of the U.S. Department of Transportation Act of 1966 prohibits the use of certain public and historic lands for federally funded transportation facilities unless there is no feasible and prudent alternative. The law applies to significant publicly owned parks, recreation areas, wildlife / waterfowl refuges, and NRHP eligible or listed historic properties. Lands subject to this law are considered Section 4(f) resources. Based on a desktop review, a site visit on October 5, 2017 by American Structurepoint, Inc., and the 2016 aerial map of the project area (Appendix B, B-3) and the RFI report (Appendix E, E-1 to E-9), there are two 4(f) resources located within the 0.5 mile search radius. There are two 4(f) resources located within or adjacent to the project area.

The SR 75 over Wildcat Creek Bridge (INDOT Bridge 075-08-03653 B, NBI No. 024970, HB-1897; IHSSI No.: 015-553-65014) was determined eligible for listing in the NRHP in the Indiana Historic Bridge Inventory and was designated as "Non-Select." The proposed project would result in a use of the SR 75 over Wildcat Creek Bridge as it would rehabilitate the historic bridge for continued vehicular use. The Programmatic Section 4(f) Evaluation and Approval for FHWA Projects that Necessitate the Use of Historic Bridges applies, due to meeting the following criteria:

- The bridge is to be replaced or rehabilitated with Federal funds.
 - The SR 75 over Wildcat Creek Bridge will be rehabilitated with Federal funds.
- The project will require the use of a historic bridge structure which is on or is eligible for listing on the NRHP.
 - The project will require the use of the NRHP eligible SR 75 over Wildcat Creek Bridge.
- The bridge is not a National Historic Landmark.
 - The SR 75 over Wildcat Creek Bridge is not a National Historic Landmark.
- The FHWA Division Administrator determines that the facts of the project match those set forth in the sections of the Programmatic Section 4(f) Evaluation and Approval for FHWA Projects that Necessitate the Use of Historic Bridges labeled Alternatives, Findings, and Mitigation.
 - An alternatives analysis was completed. INDOT CRO and the SHPO concurred with the Section 4(f) Historic Bridge Alternatives Analysis.
 - Rehabilitation was found reasonable and prudent. The historic integrity of the bridge is to be preserved, to the greatest extent possible, consistent with unavoidable transportation needs, safety, and load requirements.
 - Measures to minimize harm have been incorporated into the project (Appendix I, I-14 to I-17) and photo documentation will be prepared prior to construction as requested by the SHPO in their letter dated January 6, 2020 (Appendix D, D-85 to D-86).
- Agreement among the FHWA, the SHPO, and the Advisory Council on Historic Preservation (ACHP) has been reached through procedures pursuant to Section 106.
 - The project adheres to the Historic Bridge PA Project Development Process. This process has been agreed upon by FHWA, SHPO, and the ACHP through procedures pursuant to Section 106.

FHWA final approval of the environmental document will serve as FHWA approval of the Programmatic Section 4(f) Evaluation and Approval for FHWA Projects that Necessitate the Use of Historic Bridges.

The portion of Wildcat Creek within the project area is a Natural, Scenic, and Recreational River System. Wildcat Creek is used by the public for recreation and as such it is eligible for protection under Section 4(f). As part of the project, no construction activities will occur in the stream but the stream may be temporarily closed to recreational use at the bridge during construction for the protection of the recreational users. INDOT ES concurred that this temporary closure is likely a temporary occupancy on June 16, 2020 (Appendix I, I-19 to I-21).

In a letter dated August 17, 2020 (Appendix I, I-28), the Carroll County Commissioners concurred that the temporary occupancy does not constitute a use under Section 4(f), as described in the FHWA's Section 4(f) Policy Paper (dated July 20, 2012), for the following reasons:

- Duration must be temporary, i.e., less than the time needed for construction of the project, and there should be no change in ownership of the land;
 - No right-of-way will be acquired as part of the project.
 - Wildcat Creek will be closed to recreational use at the bridge for less than the time needed to

Indiana Department of Transportation

County Carroll Route SR 75 Des. No. 1601029

construct the project (It is anticipated Wildcat Creek may be closed for 1 to 2 months).

- Scope of the work must be minor, i.e., both the nature and magnitude of the changes to the Section 4(f) property are minimal;
 - No work is occurring in Wildcat Creek. There will be no changes to Wildcat Creek.
- There are no anticipated permanent adverse physical impacts, nor will there be interference with the protected activities, features, or attributes of the property, on either a temporary or permanent basis;
 - There are no anticipated permanent adverse physical impacts to Wildcat Creek.
- The land being used must be fully restored, i.e., the property must be returned to a condition which is at least as good as that which existed prior to the project; and
 - Wildcat Creek will be fully restored to a condition at least as good as prior to the project.
- There must be documented agreement of the official(s) with jurisdiction over the Section 4(f) resource regarding the above conditions.
 - This letter serves as a documented agreement of the official(s) with jurisdiction over the Wildcat Creek canoe/boating trail (recreational use of the river).

These guidelines are included in the project commitments listed in Section J of this document. Because this meets the definition of temporary occupancy, there is no use of a Section 4(f) property, and thus no further Section 4(f) evaluation for this property is required. For reference to the coordination for the Section 4(f) "No Use" documentation, see Appendix I, I-19 to I-28.

Section 6(f) Involvement

Presence

Use

Yes

No

Section 6(f) Property

☐
☐
☐

Discuss proposed alternatives that satisfy the requirements of Section 6(f). Discuss any Section 6(f) involvement.

Remarks:

The U.S. Land and Water Conservation Fund Act of 1965 established the Land and Water Conservation Fund (LWCF), which was created to preserve, develop, and assure accessibility to outdoor recreation resources. Section 6(f) of this Act prohibits conversion of lands purchased with LWCF monies to a non-recreation use.

A review of 6(f) properties on the Land and Water Conservation Fund (LWCF) property list revealed a total of one property in Carroll County (Appendix I, I-18). This property is not located within or adjacent to the project area. Therefore, there will be no impacts to 6(f) resources as a result of this project.

SECTION E – Air Quality

Air Quality

Conformity Status of the Project

Is the project in an air quality non-attainment or maintenance area?

Yes

No

☐
☒

If YES, then:

Is the project in the most current MPO TIP?

☐
☐

Is the project exempt from conformity?

☐
☐

If the project is NOT exempt from conformity, then:

Is the project in the Transportation Plan (TP)?

☐
☐

Is a hot spot analysis required (CO/PM)?

☐
☐

Level of MSAT Analysis required?

Level 1a ☒ Level 1b ☐ Level 2 ☐ Level 3 ☐ Level 4 ☐ Level 5 ☐

Indiana Department of Transportation

County Carroll Route SR 75 Des. No. 1601029

Remarks:

STIP/TIP:

This project is included in the Fiscal Year (FY) 2020 to 2024 Statewide Transportation Improvement Program (STIP) (Appendix H, H-1).

Attainment Status:

This project is located in Carroll County, which is currently in attainment for all criteria pollutants according to the IDEM Nonattainment Status for Indiana Counties website (<https://www.in.gov/idem/airquality/2339.htm>). Therefore, the conformity procedures of 40 CFR Part 93 do not apply.

MSAT:

This project is of a type qualifying as a categorical exclusion (Group 1) under 23 CFR 771.117(c), or exempt under the Clean Air Act conformity rule under 40 CFR 93.126, and as such, a Mobile Source Air Toxics analysis is not required.

SECTION F - NOISE

Noise

Yes

No

Is a noise analysis required in accordance with FHWA regulations and INDOT's traffic noise policy?

☐☒

No

Yes/ Date

ES Review of Noise Analysis

Remarks:

This project is a Type III project. In accordance with 23 CFR 772 and the current *Indiana Department of Transportation Traffic Noise Analysis Procedure*, this action does not require a formal noise analysis.

SECTION G – COMMUNITY IMPACTS

Regional, Community & Neighborhood Factors

Yes

No

Will the proposed action comply with the local/regional development patterns for the area?

☒☐

Will the proposed action result in substantial impacts to community cohesion?

☐☒

Will the proposed action result in substantial impacts to local tax base or property values?

☐☒

Will construction activities impact community events (festivals, fairs, etc.)?

☐☒

Does the community have an approved transition plan?

☒☐

If No, are steps being made to advance the community's transition plan?

☐☐

Does the project comply with the transition plan? (explain in the remarks box)

☒☐

Remarks:

The proposed project is the rehabilitation of an existing bridge. The proposed project will not result in the relocation of businesses, residences or farms along the project corridor. The project will not affect community cohesion because it will not change access or travel patterns within the community.

Though this project may cause minor delays to the motoring public during construction, the work to rehabilitate SR 75 over Wildcat Creek will not result in permanent community or economic impacts to the surrounding area. It is anticipated that MOT would involve an official detour. Access to all properties within and adjacent to the project limits will be maintained at all times during project construction. The contractor will be responsible for following INDOT Design Manual and Standard Specifications and the Uniform Traffic Control Manual to implement the MOT.

In order for a municipality to be eligible to receive federal funds they must have in place, or at least under development, an Americans With Disability Act (ADA) Transition Plan. The Transition Plan inventories the municipality's infrastructure identifying those areas with features (i.e., sidewalks, crosswalks, curb ramps, building access, etc.) that are not in compliance with the ADA and establishes a plan to program funding for improvements intended to bring the facilities into compliance.

Indiana Department of Transportation

County Carroll Route SR 75 Des. No. 1601029

According to the Carroll County website, the Carroll County ADA Transition plan has been completed and was last updated on August 14, 2015 (https://www.carrollcountygovernment.org/uploads/9/1/2/1/9121287/ada_transition_plan.pdf.) The proposed project is a federal-aid project, meaning all improvements to the infrastructure must conform to the ADA. However, no sidewalks are present or proposed within the proposed project area. Therefore, the ADA transition plan is not needed for the proposed project.

The Carroll County Indiana (<https://www.carrollcountyindiana.com/visit/festivals>) website was checked to identify events or festivals occurring during the proposed construction period. To date, no events are listed in Cutler, Indiana. However, if an event occurs during the construction period, accommodations will be made to maintain access to local special events and/or festivals.

Indirect and Cumulative Impacts

Will the proposed action result in substantial indirect or cumulative impacts?

Yes

☐

No

☒

Remarks:

Indirect impacts are effects which are caused by the action and are later in time or farther removed in distance, but are still reasonably foreseeable. Indirect effects may include growth inducing effects and other effects related to induced changes in the pattern of land use, population density, or growth rate. Cumulative impacts affect the environment which result from the incremental impact of the action when added to other past, present, and reasonably foreseeable future actions regardless of what agency or person undertakes such actions.

Due to the scope of the project and the surrounding properties, this project is not likely to cause substantial indirect or cumulative impacts. The proposed project involves the rehabilitation of an existing bridge. The project is located within a landscape dominated by agricultural and residential uses. Land use within the right-of-way consists of maintained grass and forested area along Wildcat Creek. The project does not promote the opportunity for induced growth, but rather safer conditions from improvements to a deteriorating bridge.

Public Facilities & Services

Will the proposed action result in substantial impacts on health and educational facilities, public and private utilities, emergency services, religious institutions, airports, public transportation or pedestrian and bicycle facilities? *Discuss how the maintenance of traffic will affect public facilities and services.*

Yes

☐

No

☒

Remarks:

Based on a desktop review, a site visit on October 5, 2017 by American Structurepoint, Inc., the 2016 aerial map of the project area (Appendix B, B-3), and the RFI report (Appendix E, E-1 to E-9) there is one religious facility and one State Natural Scenic and Recreational River within the 0.5 mile search radius. Wildcat Creek, a State Natural Scenic and Recreational trail is within the project area. The project proposes no impacts to Wildcat Creek. The recreational use of Wildcat Creek may only be temporarily closed at the bridge with signage posted signifying closures and water trail users will be able to walk around the closure. For additional information about the Wildcat Creek trail, see the Section 4(f) section above. Access to all properties will be maintained during construction. Therefore, no impacts are expected.

Early coordination letters were sent to INDOT LaPorte District, INDOT Environmental Policy Office, IDNR-Division of Outdoor Recreation, Kankakee-Iroquois Regional Planning Commission, Carroll County Highway Department, Carroll County Commissioners, Carroll County Sheriff's Office, Carroll County Surveyor's Office, Carroll County Consolidated School Corporation, and Carroll County Emergency Management Agency on July 27, 2018. INDOT LaPorte District, INDOT Environmental Policy Office, Kankakee-Iroquois Regional Planning Commission, Carroll County Sheriff's Office, Carroll County Surveyor's Office, Carroll County Consolidated School Corporation, and Carroll County Emergency Management Agency did not respond to the early coordination letter.

The IDNR-Division of Outdoor Recreation responded on August 8, 2018 with recommendations to avoid impacts to streams. The response stated the project will occur within a section of Wildcat Creek that is designated as a Natural, Scenic, and Recreation River System. If all preventative measures are taken to avoid impacts to the river and its recreational use there should be no negative impacts (Appendix C, C-17). In response to additional coordination, IDNR-Division of Outdoor Recreation stated the project will neither enhance nor detract from the scenic quality of the river because the project is rehabilitating an existing bridge. Furthermore, as long as there is signage signifying closures and there is a pathway for water trail users to circumvent potential closures, the Division of Outdoor Recreation would mark approval for this project (Appendix I, I-23 to I-25).

Indiana Department of Transportation

County Carroll Route SR 75 Des. No. 1601029

The Carroll County Highway Department and Carroll County Commissioners did not respond to the early coordination letter when it was initially sent on July 27, 2018. As part of the Section 4(f) process, the Carroll County Commissioner's office was contacted and the early coordination letter was provided again. A meeting was held on August 27, 2020 with American Structurepoint, Inc., the Carroll County Commissioners, and Carroll County Highway Department. The Carroll County Commissioners and Carroll County Highway Department expressed concern over timing of the closure of the bridge to traffic during construction (Appendix C, C-41). The bridge is located in a rural, agricultural area and the bridge is important for the movement of farm equipment during planting and harvest as there is no other bridge in the area capable of handling modern planting and tillage equipment or grain trucks. The Commissioners stated that they understood the bridge must be closed to traffic for some of the construction work and stated that it would be ideal to time the bridge closure outside of the planting and harvest season. The Commissioners requested to limit closure of the bridge to the months of June, July, and August (Appendix C, C-42).

All applicable IDNR-Division of Outdoor Recreation and Carroll County's recommendations are included in the *Environmental Commitments* section of this CE document.

It is the responsibility of the project sponsor to notify school corporations and emergency services at least two weeks prior to any construction that would block or limit access.

Currently, two electric companies (Carroll White R.E.M.C. and Duke Energy), one cable company (Frontier), one Fiber Optic company (Zayo Bandwith), one telephone company (Level 3 Now CenturyLink), provide services to residents and businesses near the project area. Coordination with these utility companies to identify potential conflicts and relocation of the appropriate facilities, if needed, has been initiated. This coordination will continue through the duration of the engineering phase of the project.

Environmental Justice (EJ) (Presidential EO 12898)

During the development of the project were EJ issues identified?

Does the project require an EJ analysis?

If YES, then:

Are any EJ populations located within the project area?

Will the project result in adversely high or disproportionate impacts to EJ populations?

Yes	No
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>

Remarks:

Under FHWA Order 6640.23A, FHWA and the project sponsor, as a recipient of funding from FHWA, are responsible to ensure that their programs, policies, and activities do not have a disproportionately high and adverse effect on minority or low-income populations. Per the current INDOT Categorical Exclusion Manual, an Environmental Justice (EJ) Analysis is required for any project that has two or more relocations or 0.5 acre of additional permanent right-of-way. This project will have no relocations and will require no additional permanent right-of-way; therefore, an EJ analysis is not required.

Relocation of People, Businesses or Farms

Will the proposed action result in the relocation of people, businesses or farms?

Is a Business Information Survey (BIS) required?

Is a Conceptual Stage Relocation Study (CSRS) required?

Has utility relocation coordination been initiated for this project?

Yes	No
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input checked="" type="checkbox"/>	<input type="checkbox"/>

Number of relocations: Residences: _____ Businesses: _____ Farms: _____ Other: _____

If a BIS or CSRS is required, discuss the results in the remarks box.

Remarks:

No relocations of people, businesses, or farms will take place as a result of this project. Coordination with utility companies to identify potential conflicts and relocation of the appropriate facilities, if needed, has been initiated. This coordination will continue through the duration of the engineering phase of the project.

Indiana Department of Transportation

County Carroll Route SR 75 Des. No. 1601029

SECTION H – HAZARDOUS MATERIALS & REGULATED SUBSTANCES

Documentation

Hazardous Materials & Regulated Substances (Mark all that apply)

Red Flag Investigation

Phase I Environmental Site Assessment (Phase I ESA)

Phase II Environmental Site Assessment (Phase II ESA)

Design/Specifications for Remediation required?

X

No Yes/ Date

ES Review of Investigations	No	Yes/ Date X/ December 18, 2018
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Include a summary of findings for each investigation.

Remarks: Based on a review of GIS and available public records, a RFI was concurred on December 18, 2018 by INDOT Site Assessment and Management (SAM) (Appendix E-1 to E-9). The RFI layers were rechecked on March 6, 2020 and no updates to the RFI were warranted. No sites with hazardous material concerns (hazmat sites) or sites involved with regulated substances were identified in or within 0.5 mile of the project area. Further investigation for hazardous material concerns or regulated substances is not required at this time.

Wildcat Creek is listed for *E. coli* and PCBs (and/or mercury) in fish tissue (Appendix E, E-1 to E-9). Workers who are working in or near water with *E. coli* should take care to wear appropriate Personal Protective Equipment (PPE), observe proper hygiene procedures, including regular hand washing, and limit personal exposure. Exposure to PCBs (and/or mercury) in fish tissue is considered low, assuming workers are not eating biota surrounding or associated with the water body. If there will be sediment and/or soils disturbed by construction, additional investigation may be necessary. This has been added as a firm commitment to this environmental document.

SECTION I – PERMITS CHECKLIST

Permits (mark all that apply)

Likely Required

Army Corps of Engineers (404/Section10 Permit)

Individual Permit (IP)

Nationwide Permit (NWP)

Regional General Permit (RGP)

Pre-Construction Notification (PCN)

Other

Wetland Mitigation required

Stream Mitigation required

IDEM

Section 401 WQC

Isolated Wetlands determination

Rule 5

Other

Wetland Mitigation required

Stream Mitigation required

IDNR

Construction in a Floodway

Navigable Waterway Permit

Lake Preservation Permit

Other

Mitigation Required

US Coast Guard Section 9 Bridge Permit

Others (Please discuss in the remarks box below)

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This is page 25 of 28 Project name: SR 75 over Wildcat Creek Bridge Improvement Date: October 19, 2021

Indiana Department of Transportation

County Carroll Route SR 75 Des. No. 1601029

Remarks: As the project will not include work below the Q100 elevation, formal application for a Construction in a Floodway (CIF) Permit from the IDNR will not be required pursuant to the Flood Control Act (IC-14-28-1).

Applicable recommendations provided by USACE and IDNR are included in the *Environmental Commitments* section of this document. If permits are found to be necessary, the conditions of the permit will be requirements of the project and will supersede these recommendations.

It is the responsibility of the project sponsor, or its authorized agent, to identify and obtain all required permits.

SECTION J- ENVIRONMENTAL COMMITMENTS

The following information should be provided below: List all commitments, name of agency/organization requesting the commitment(s), and indicating which are firm and which are for further consideration. The commitments should be numbered.

Remarks:

Firm:

- 1) If the scope of work or permanent or temporary right-of-way amounts change, the INDOT Environmental Services Division (ESD) and the INDOT District Environmental Section will be contacted immediately. (INDOT ESD and INDOT District)
- 2) It is the responsibility of the project sponsor to notify school corporations and emergency services at least two weeks prior to any construction that would block or limit access. (INDOT ESD)
- 3) Any work in a wetland area within right-of-way or in borrow/waste areas is prohibited unless specifically allowed in the U.S. Army Corps of Engineers permit. (INDOT ESD)
- 4) SR 75 over Wildcat Creek Bridge (INDOT Bridge 075-08-03653 B) has shown evidence of use (i.e. nests) by a bird species protected under the Migratory Bird Treaty Act (MBTA) during the October 5, 2017 inspection. Avoidance and minimization measures must be implemented prior to the start of and during the nesting season. Nests without eggs or young should be removed prior to construction during the non-nesting season (September 8 – April 30) and during the nesting season if no eggs or young are present. Nests with eggs or young cannot be removed or disturbed during the nesting season (May 1 – September 7). Nests with eggs or young should be screened or buffered from active construction. Details of the required procedures are outlined in the “Potential Migratory Bird on Structure Unique Special Provision”. (INDOT ESD)
- 5) USFWS Bridge/Structure Assessment shall take place no earlier than two (2) years prior to the start of construction. If construction will begin after November 08, 2021, an inspection of the structure by a qualified individual, must be performed. Inspection of the structure should check for presence of bats/bat indicators and/or presence of birds. The results of the inspection must indicate no signs of bats or birds. If signs of bats or birds are documented during this inspection, the INDOT District Environmental Manager must be contacted immediately. (INDOT ESD)
- 6) Wildcat Creek is listed for E. coli and PCBs (and/or mercury) in fish tissue. Workers who are working in or near water with E. coli should take care to wear appropriate Personal Protective Equipment (PPE), observe proper hygiene procedures, including regular hand washing, and limit personal exposure. Exposure to PCBs (and/or mercury) in fish tissue is considered low, assuming workers are not eating biota surrounding or associated with the water body. If there will be sediment and/or soils disturbed by construction, additional investigation may be necessary. [INDOT Site Assessment and Management (SAM)]
- 7) This project will occur within a section of Wildcat Creek that is designated as a Natural, Scenic, and Recreational River System (I.C. 14-29-6). If all preventative measures are taken to avoid impacts to the river and its recreational use there should be no negative impacts. If this is not the case please contact us for further coordination. (IDNR-Division of Outdoor Recreation)
- 8) This project will occur within a section of Wildcat Creek that is designated as a Natural, Scenic, and Recreational River under the Indiana Natural, Scenic, and Recreational River System (I.C. 14-29-6). Signage must be used to signify any closures, including the Adams Mill Public Access Site, and there must be a pathway provided for water trail users to circumvent potential closures. (IDNR-Division of Outdoor Recreation).
- 9) Design plans will be submitted to SHPO and consulting parties for review and comment at 30 percent, 60 percent, and final. (IDNR-SHPO)
- 10) Photo documentation will be prepared prior to construction as requested by the SHPO in their letter dated January 6, 2020. (IDNR-SHPO)
- 11) Prior to the ready for contracts (RFC) date (which is prior to construction), INDOT will obtain a Director's Letter of Clearance for the project, indicating compliance with Indiana Code 14-21-1-18. (IDNR-SHPO)

This is page 26 of 28 Project name: SR 75 over Wildcat Creek Bridge Improvement Date: October 19, 2021

Indiana Department of Transportation

County Carroll Route SR 75 Des. No. 1601029

- 12) General AMM 1: Ensure all operators, employees, and contractors working in areas of known or presumed bat habitat are aware of all FHWA/FRA/FTA (Transportation Agencies) environmental commitments, including all applicable AMMs. (USFWS)
- 13) Lighting AMM 1: Direct temporary lighting away from suitable habitat during the active season. (USFWS)
- 14) Regarding Wildcat Creek, duration of use must be temporary (i.e. less than the time needed for construction of the project, and there should be no change in ownership of the land. No right-of-way will be acquired as part of the project and Wildcat Creek will be closed to recreational use at the bridge for less than the time needed to construct the project. (Carroll County Commissioners)
- 15) Regarding Wildcat Creek, the land being used must be fully restored, i.e., the property must be returned to a condition which is at least as good as that which existed prior to the project. Wildcat Creek will be fully restored to a condition at least as good as prior to the project. (Carroll County Commissioners)
- 16) Limit work requiring closure of the bridge to the months of June, July, and August. (Carroll County)

For Further Consideration:

- 17) The new, replacement, or rehabbed structure, and any bank stabilization under the structure, should not create conditions that are less favorable for wildlife passage under the structure compared to current conditions. The Division of Fish and Wildlife would like to emphasize the importance of wildlife passage issues and transportation infrastructure projects. The following is a good place to start in terms of resources to consider in the design of stream crossing structures. <http://www.fs.fed.us/wildlifecrossings/library/>. The following are recommended resources for designing and constructing stream crossings for maintenance of instream habitat and aquatic organism passage: https://www.fs.fed.us/biology/nsaec/fishxing/aop_pdfs.html: <https://www.fhwa.dot.gov/engineering/hydraulics/pubs/11008/hif11008.pdf>. (IDNR-DFW)
- 18) Riprap or other hard bank stabilization materials should be used only at the toe of the sideslopes up to the ordinary high water mark (OHWM) with the exception of areas directly under bridges for instance. The banks above the OHWM should be restored, stabilized, and revegetated using geotextiles and a mixture of grasses, sedges, wildflowers, shrubs, and trees native to Central Indiana and specifically for stream bank/floodway stabilization purposes as soon as possible upon completion. For streambed stabilization or scour protection, riprap or other stabilization materials should not be placed in the active stream channel above the existing streambed elevation. This is to prevent obstructions to the movement of aquatic organisms upstream and downstream. (IDNR-DFW)
- 19) Do not construct any temporary runarounds, access bridges, causeways, cofferdams, diversions, or pumparounds. (IDNR-DFW)
- 20) Operate equipment used to replace the bridge from the existing roadway. (IDNR-DFW)
- 21) Use minimum average 6 inch graded riprap stone extended below the normal water level to provide habitat for aquatic organisms in the voids. (IDNR-DFW)

Indiana Department of Transportation

County Carroll Route SR 75 Des. No. 1601029

SECTION K- EARLY COORDINATION

Please list the date coordination was sent and all agencies that were contacted as a part of the development of this Environmental Study. Also, include the date of their response or indicate that no response was received. INDOT and FHWA are automatically considered early coordination participants and should only be listed if a response is received.

Remarks:

Early coordination was initiated on July 27, 2018 with applicable federal, state, and local agencies. An early coordination letter was sent to the floodplain administrator with Carroll County Area Plan Commission on October 5, 2020. Coordination was re-initiated and the early coordination letter was provided to the Carroll County Commissioners and Carroll County Highway Department during 4(f) coordination on August 12, 2020. Review comments from those agencies that returned a reply have been incorporated into this study, as appropriate (Appendix C). The agencies contacted and the date on which they replied is identified in the table below.

Agency	Date of Response	Appendix Location
USFWS, Northern Indiana Suboffice	August 9, 2018	Appendix C, C-18 to C-19
US Natural Resources Conservation Service	August 2, 2018 and November 16, 2020	Appendix C, C-15 to C-16
US Army Corps of Engineers, Louisville District	N/A	N/A
US Department of Housing and Urban Development	N/A	N/A
National Park Service	N/A	N/A
Federal Highway Administration	N/A	N/A
Indiana Geological Survey	July 30, 2018	Appendix C, C-11 to C-13
INDOT, Public Involvement	July 31, 2018	Appendix C, C-14
INDOT, LaPorte District	N/A	N/A
INDOT, Environmental Policy Office	N/A	N/A
IDNR, Division of Fish and Wildlife	August 24, 2018	Appendix C, C-38 to C-40
IDNR, Division of Outdoor Recreation	August 8, 2018	Appendix C, C-17
Indiana Department of Environmental Management	July 30, 2018	Appendix C, C-3 to C-10
Kankakee-Iroquois Regional Planning Commission	N/A	N/A
Carroll County Highway Department	August 27, 2020	Appendix C, C-41
Carroll County Commissioners	August 27, 2020 and September 8, 2020	Appendix C, C-41 to C-42
Carroll County Sheriff's Office	N/A	N/A
Carroll County Surveyor's Office	N/A	N/A
Carroll Consolidated School Corporation	N/A	N/A
Carroll County Emergency Management Agency	N/A	N/A
Carroll County Area Plan Commission Floodplain Administrator	N/A	N/A

Table of Contents for Appendix Items

	Page
Appendix A: INDOT Supporting Documents	A
• Threshold Chart	A-1
Appendix B: Graphics	B
• Project Location – State Location Map	B-1
• Project Location – USGS Topographic Map (Rossville Quadrangle)	B-2
• Project Location – 2016 Aerial Photography	B-3
• Photo Location Map	B-4
• Project Photographs	B-5 to B-7
• Project Plans	B-8 to B-14
Appendix C: Early Coordination	C
• Early Coordination Request Letter – July 27, 2018	C-1 to C-2
• Indiana Department of Environmental Management – July 30, 2018	C-3 to C-10
• Indiana Geological Survey – July 30, 2018	C-11 to C-13
• INDOT, Office of Public Involvement – July 31, 2018	C-14
• U.S. National Resources Conservation Service – August 2, 2018	C-15
• U.S. National Resources Conservation Service – November 16, 2020	C-16
• Indiana Department of Natural Resources, Division of Outdoor Recreation – August 08, 2018	C-17
• United States Fish and Wildlife Service – August 09, 2018	C-18 to C-19
• USFWS Official Species List – April 05, 2021	C-20 to C-25
• Indiana Bat and Northern Long-Eared Bat Range-Wide Programmatic Informal Consultation, Verification Letter – October 14, 2020	C-26 to C-37
• Indiana Department of Natural Resources Division of Fish and Wildlife– August 24, 2018	C-38 to C-40
• Carroll County Highway Department and Commissioners – August 27, 2020	C-41
• Carroll County Commissioners – September 8, 2020	C-42
• Indiana Department of Natural Resources- Natural Heritage Data Center	C-43 to C-45
Appendix D: Section 106 of NHPA	D
• Section 106 Findings and Supporting 800.11(d) Documentation – February 28, 2020	D-1 to D-89
• SHPO Concurrence for Effect Finding – March 12, 2020	D-90 to D-91
• Public Notice of “No Historic Properties Affected” Finding and Publisher’s Affidavit– March 04, 2020	D-92 to D-94
Appendix E: Red Flag and Hazardous Materials	E
• Red Flag Investigation – December 18, 2018	E-1 to E-9
Appendix F: Water Resources and Ecological Information	F
• Excerpts from Wetland Delineation and Waters Report – July 26, 2018	F-1 to F-36
• Waters Report Approval Email – August 07, 2018	F-37
• Indiana Floodplain Information Portal Report – March 17, 2020	F-38

Appendix G: Public Involvement	G
• Notice of Survey Letter – June 16, 2020	G-1
• Legal Notice of Public Hearing	G-2 to G-3
• Affidavits of Publication of Legal Notice	G-4 to G-5
• Legal Notice Mailing List	G-6 to G-7
• Hearing Sign-In Sheets	G-8 to G-10
• Welcome Letter/Project Information Packet	G-11 to G-15
• Hearing Presentation Slideshow	G-16 to G-22
• Public Comments Received	G-23 to G-30
• Response to Public Comments	G-31 to G-35
Appendix H: Air Quality	H
• Page from the 2020-2024 <i>STIP</i>	H-1
Appendix I: Additional Studies	I
• Bridge Inspection Report –075-08-03653 B- November 08, 2019	I-1 to I-8
• Historic Bridge Alternative Analysis- November 11, 2019	I-9 to I-17
• Carroll County LWCF List- July 2020	I-18
• 4(f) Coordination	I-19 to I-28

Categorical Exclusion Level Thresholds

	PCE	Level 1	Level 2	Level 3	Level 4 ¹
Section 106	Falls within guidelines of Minor Projects PA	“No Historic Properties Affected”	“No Adverse Effect”	-	“Adverse Effect” Or Historic Bridge involvement ²
Stream Impacts	No construction in waterways or water bodies	< 300 linear feet of stream impacts	≥ 300 linear feet of stream impacts	-	Individual 404 Permit
Wetland Impacts	No adverse impacts to wetlands	< 0.1 acre	-	< 1 acre	≥ 1 acre
Right-of-way³	Property acquisition for preservation only or none	< 0.5 acre	≥ 0.5 acre	-	-
Relocations	None	-	-	< 5	≥ 5
Threatened/Endangered Species (Species Specific Programmatic for Indiana bat & northern long eared bat)	“No Effect”, “Not likely to Adversely Affect” (Without AMMs ⁴ or with AMMs required for all projects ⁵)	“Not likely to Adversely Affect” (With any other AMMs)	-	“Likely to Adversely Affect”	Project does not fall under Species Specific Programmatic
Threatened/Endangered Species (Any other species)	Falls within guidelines of USFWS 2013 Interim Policy	“No Effect”, “Not likely to Adversely Affect”	-	-	“Likely to Adversely Affect”
Environmental Justice	No disproportionately high and adverse impacts	-	-	-	Potential ⁶
Sole Source Aquifer	Detailed Assessment Not Required	-	-	-	Detailed Assessment
Floodplain	No Substantial Impacts	-	-	-	Substantial Impacts
Coastal Zone Consistency	Consistent	-	-	-	Not Consistent
National Wild and Scenic River	Not Present	-	-	-	Present
New Alignment	None	-	-	-	Any
Section 4(f) Impacts	None	-	-	-	Any
Section 6(f) Impacts	None	-	-	-	Any
Added Through Lane	None	-	-	-	Any
Permanent Traffic Alteration	None	-	-	-	Any
Coast Guard Permit	None	-	-	-	Any
Noise Analysis Required	No	-	-	-	Yes
Air Quality Analysis Required	No	-	-	-	Yes ⁷
Approval Level <ul style="list-style-type: none"> District Env. Supervisor Env. Services Division FHWA 	Concurrence by INDOT District Environmental or Environmental Services	Yes	Yes	Yes Yes	Yes Yes Yes

¹Coordinate with INDOT Environmental Services. INDOT will then coordinate with the appropriate FHWA Environmental Specialist.

²Any involvement with a bridge processed under the Historic Bridge Programmatic Agreement.

³Permanent and/or temporary right-of-way.

⁴AMMs = Avoidance and Mitigation Measures.

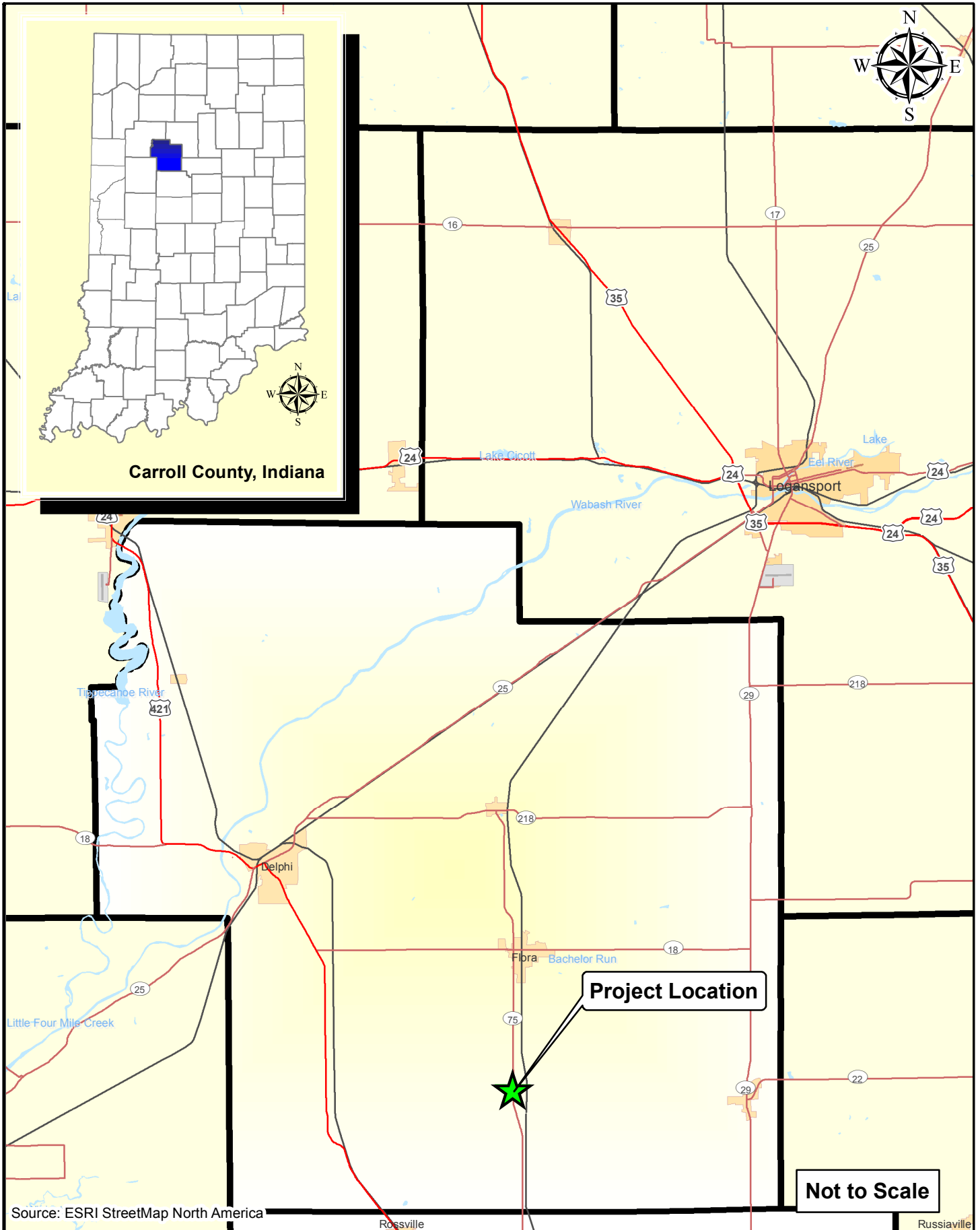
⁵AMMs determined by the IPAC decision key to be needed that are listed in the USFWS *User's Guide for the Range-wide Programmatic Consultation for Indiana bat and Northern long-eared bat* as “required for all projects”.

⁶Potential for causing a disproportionately high and adverse impact.

⁷Hot Spot Analysis and/or MSAT Quantitative Emission Analysis.

*Substantial public or agency controversy may require a higher-level NEPA document.

Path: P:\2017\00220\0.D. Drawings\Environmental\ArcView\Maps\2017_00220.EV\2017-09-27 map_SR75WildcatCreek_Location.mxd Date: 9/27/2017 User: mdelreal



AMERICAN
STRUCTUREPOINT
INC.

State Location Map

Indiana Department of Transportation
100 N. Senate Ave., IGCN 755
Indianapolis, Indiana 46204

SR 75 over Wildcat Creek

Bridge Improvement

Des. No. 1601029

Location: Cutler

Township: Democrat

County: Carroll

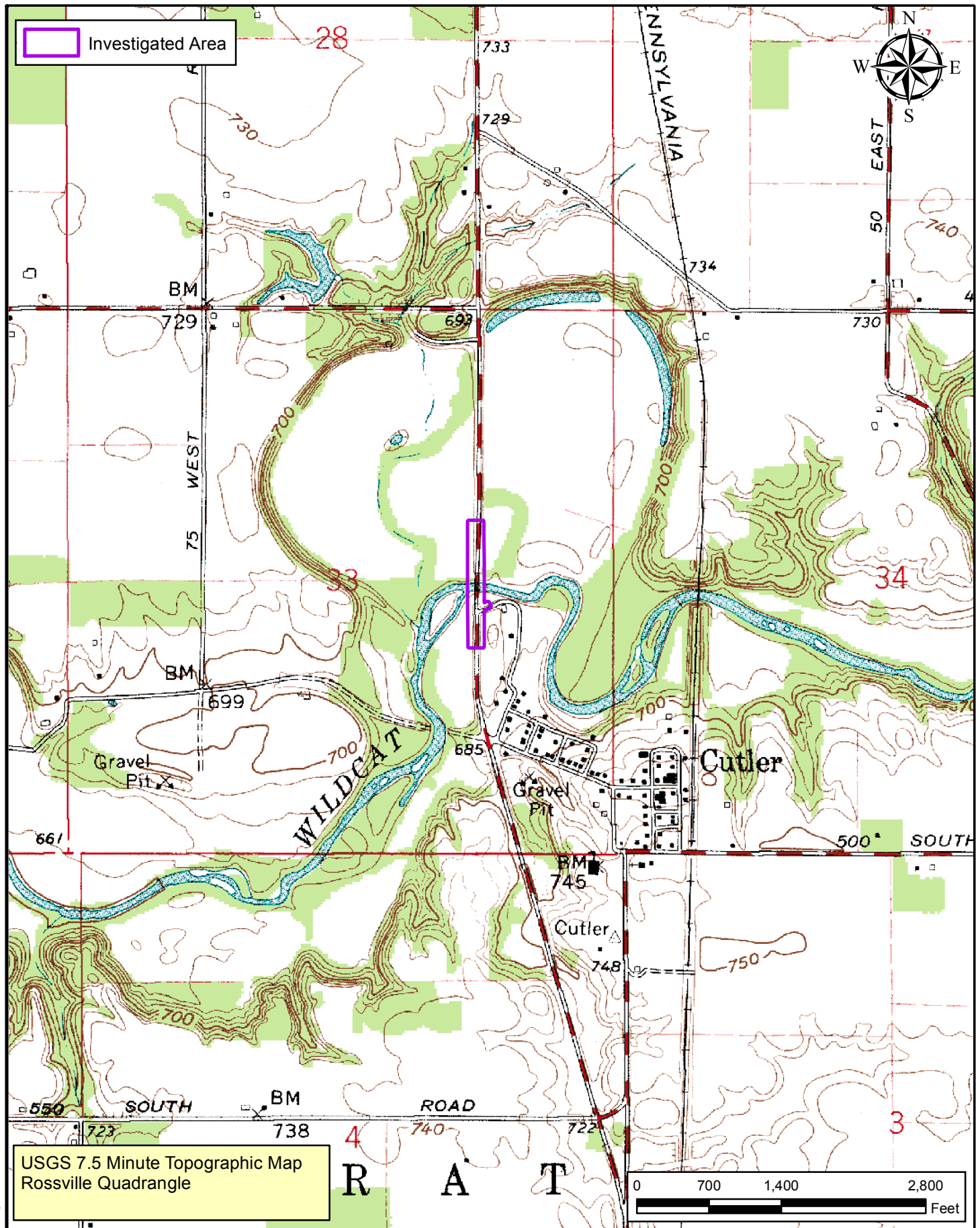
State: Indiana

Date: 09/27/2017

Appendix B

Page B-1

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AMERICAN
STRUCTUREPOINT
INC.

USGS Topographic Map

Indiana Department of Transportation
100 N. Senate Ave., IGCN 755
Indianapolis, Indiana 46204

SR 75 over Wildcat Creek
Bridge Improvement
Des. No. 1601029
Location: Cutler
Township: Democrat
County: Carroll
State: Indiana

Date: 09/27/2017

Appendix B
Page B-2

 Project Area



0 100 200 400
Feet

Source: 2016 IndianaMap Aerial Photography



AMERICAN
STRUCTUREPOINT
INC.

2016 Aerial Photography Map

Indiana Department of Transportation
100 N. Senate Ave., IGCN 755
Indianapolis, Indiana 46204

State Road 75 over Wildcat Creek Bridge Improvement

Des. No. 1601029

Location: Cutler

Township: Democrat

County: Carroll

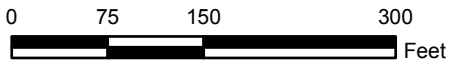
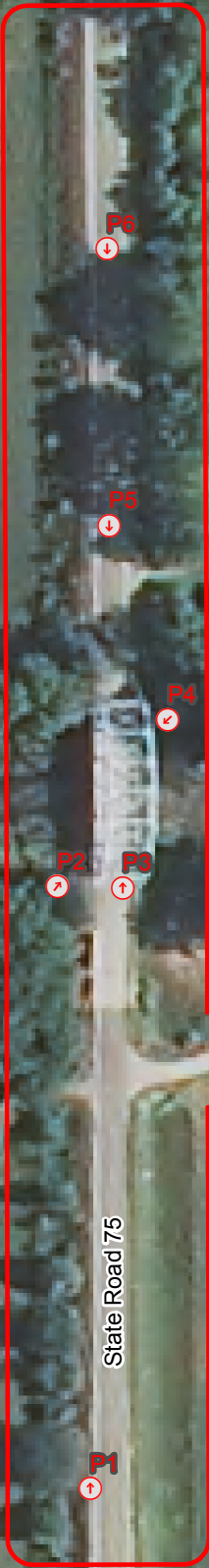
State: Indiana

Date: 03/08/2021

Appendix B
Page B-3

Project Area

Photo Location



Source: 2011 National Agricultural Imagery
Program Aerial Photography



AMERICAN
STRUCTUREPOINT
INC.

Photo Location Map

Indiana Department of Transportation
100 N. Senate Ave., IGCN 755
Indianapolis, Indiana 46204

State Road 75 over Wildcat Creek
Bridge Improvement
Des. No. 1601029
Location: Cutler
Township: Democrat
County: Carroll
State: Indiana

Date: 10/06/2017

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**SR 75 over Wildcat Creek Bridge Work
Des. No. 1600129
Democrat Township, Carroll County, Indiana
October 5, 2017**



Photo 1. Looking north along SR 75 toward the bridge over Wildcat Creek.



Photo 2. Looking northeast toward the SR 75 bridge over Wildcat Creek.

**SR 75 over Wildcat Creek Bridge Work
Des. No. 1600129
Democrat Township, Carroll County, Indiana
October 5, 2017**



Photo 3. Looking north on the SR 75 bridge over Wildcat Creek.



Photo 4. Looking southwest toward the SR 75 bridge and Wildcat Creek.

**SR 75 over Wildcat Creek Bridge Work
Des. No. 1600129
Democrat Township, Carroll County, Indiana
October 5, 2017**



Photo 5. Looking south toward the SR 75 bridge over Wildcat Creek.

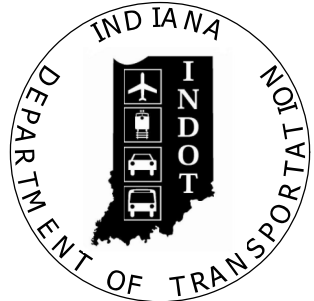


Photo 6. Looking south along SR 75 toward the bridge over Wildcat Creek.

PROJECT	DESIGNATION
1601029	1601029
CONTRACT	BRIDGE FILE
B-39876	075-08-03653C

STRUCTURE	TYPE	SPAN AND SKEW	OVER	STATION
075-08-03653C	STEEL BEAM AND STEEL TRUSS	2 SPANS: 61'-6", 175'-0" SQUARE	WILDCAT CREEK	187+72.11 "A"

INDIANA DEPARTMENT
OF TRANSPORTATION



BRIDGE REHABILITATION PLANS

FOR SPANS OVER 20 FEET

ROUTE: S.R. 75 AT: RP 50+545

PROJECT NO. 1601029 P.E.

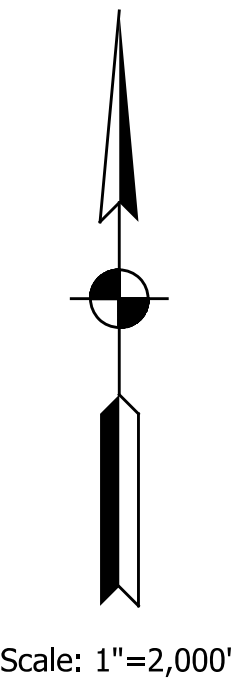
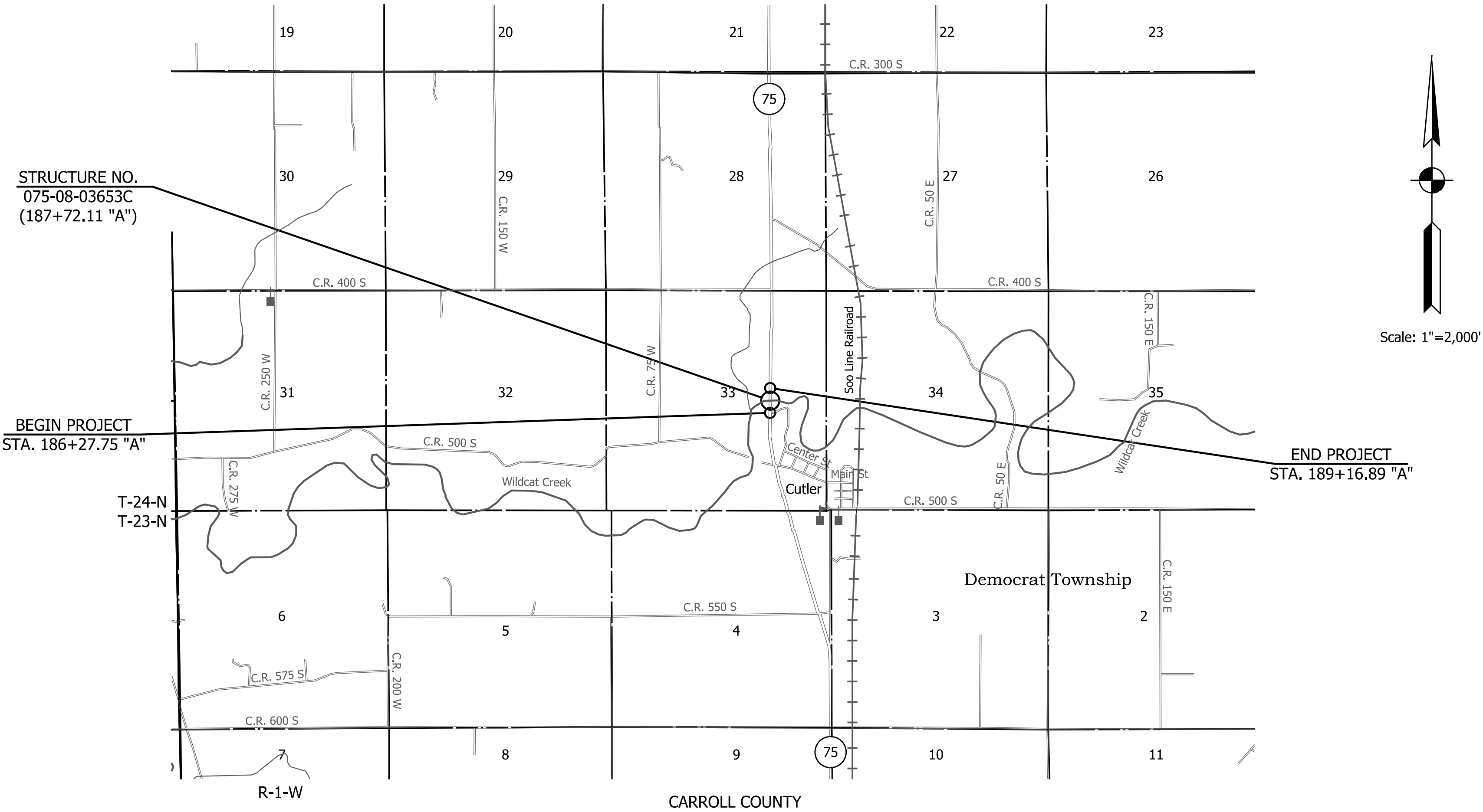
NO ADDITIONAL RIGHT-OF-WAY
REQUIRED FOR THIS PROJECT

R/W

1601029 CONST.

PROJECT DESCRIPTION

This Bridge Deck Replacement is on State Road 75 over Wildcat Creek located approximately 4.5 miles south of State Road 18 in Section 33, T-24-N, R-1-W, Democrat Township, Carroll County, Indiana.



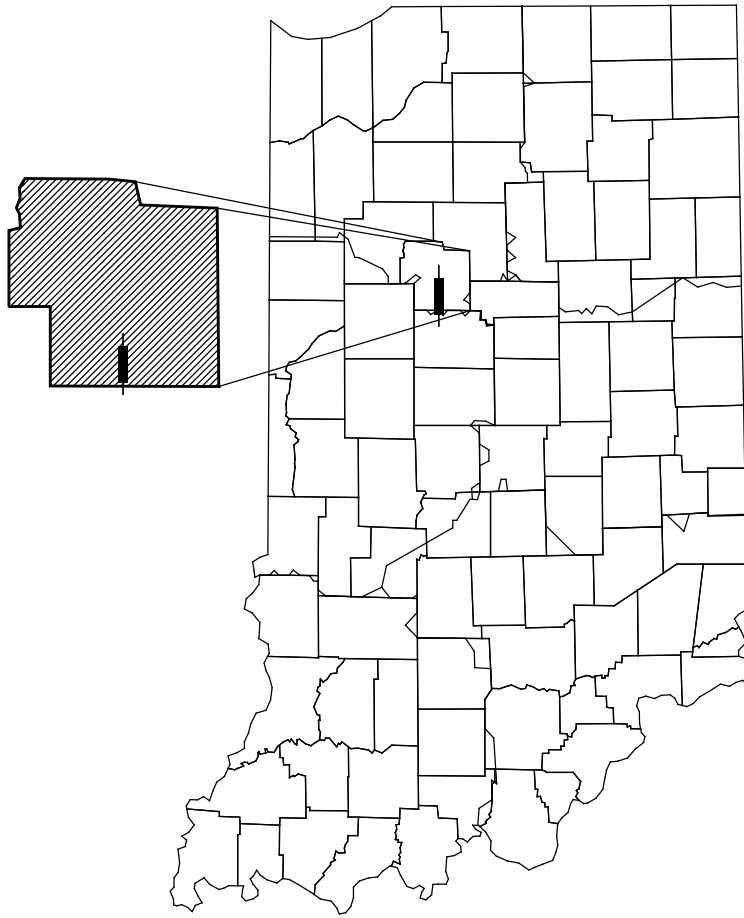
TRAFFIC DATA

S.R. 75

A.A.D.T.	2022	2430	V.P.D.
A.A.D.T.	2044	2680	V.P.D.
D.H.V	2044	243	V.P.H.
DIRECTIONAL DISTRIBUTION		61	%
TRUCKS		5	% A.A.D.T.
			% D.H.V.

DESIGN DATA

DESIGN SPEED	55	M.P.H.
PROJECT DESIGN CRITERIA	3R (NON-FREEWAY)	
FUNCTIONAL CLASSIFICATION	RURAL MAJOR COLLECTOR	
RURAL/URBAN	RURAL	
TERRAIN	LEVEL	
ACCESS CONTROL	NONE	



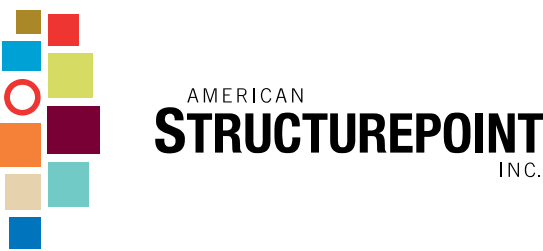
PROJECT LOCATION SHOWN BY

LATITUDE: 40°28'54" N LONGITUDE: 86°31'48" W

BRIDGE LENGTH:	0.046	MI.
ROADWAY LENGTH:	0.021	MI.
TOTAL LENGTH:	0.067	MI.
MAX. GRADE:	0.00	%

HUC: 051201070408

INDIANA DEPARTMENT OF TRANSPORTATION
STANDARD SPECIFICATIONS DATED 2020
TO BE USED WITH THESE PLANS.



9025 RIVER ROAD, SUITE 200
INDIANAPOLIS, IN 46240
TEL 317.547.5580 FAX 317.543.0270
www.structurepoint.com

PLANS PREPARED BY:	American Structurepoint, Inc.	(317) 547-5580 PHONE NUMBER
CERTIFIED BY:		DATE
APPROVED FOR LETTING:	INDIANA DEPARTMENT OF TRANSPORTATION	DATE

BRIDGE FILE	
075-08-03653C	
DESIGNATION	
1601029	
SHEETS	
SURVEY BOOK	1 of 19
ELECTRONIC	PROJECT
CONTRACT	1601029
B-39876	

UTILITIES		
<div>ELECTRIC Carroll White R.E.M.C. (Formerly White Co. R.E.M.C.) P.O. Box 599 Monticello, IN 47960 Jim Garbison (574) 583-0253 jgarbison@cwremc.com Terry LaOrange (574) 583-0256 tlaorange@cwremc.com</div> <div>ELECTRIC Duke Energy 390 N. Main St. Martinsville, IN 46151 (765) 349-4012 Tim Umbaugh tim.umbaugh@duke-energy.com</div>	<div>CABLE Frontier utilitycordreq@ftr.com</div> <div>FIBER OPTIC ZAYO Bandwidth 625 E. 11th St. Indianapolis, IN 46202 (317) 524-5711 John Senese jsenese@zayo.com</div>	<div>TELEPHONE Level 3 Now CenturyLink (877) 366-8344 ext. 2 Level 3 Communications Network Relocation Nationalrelo@centurylink.com</div>

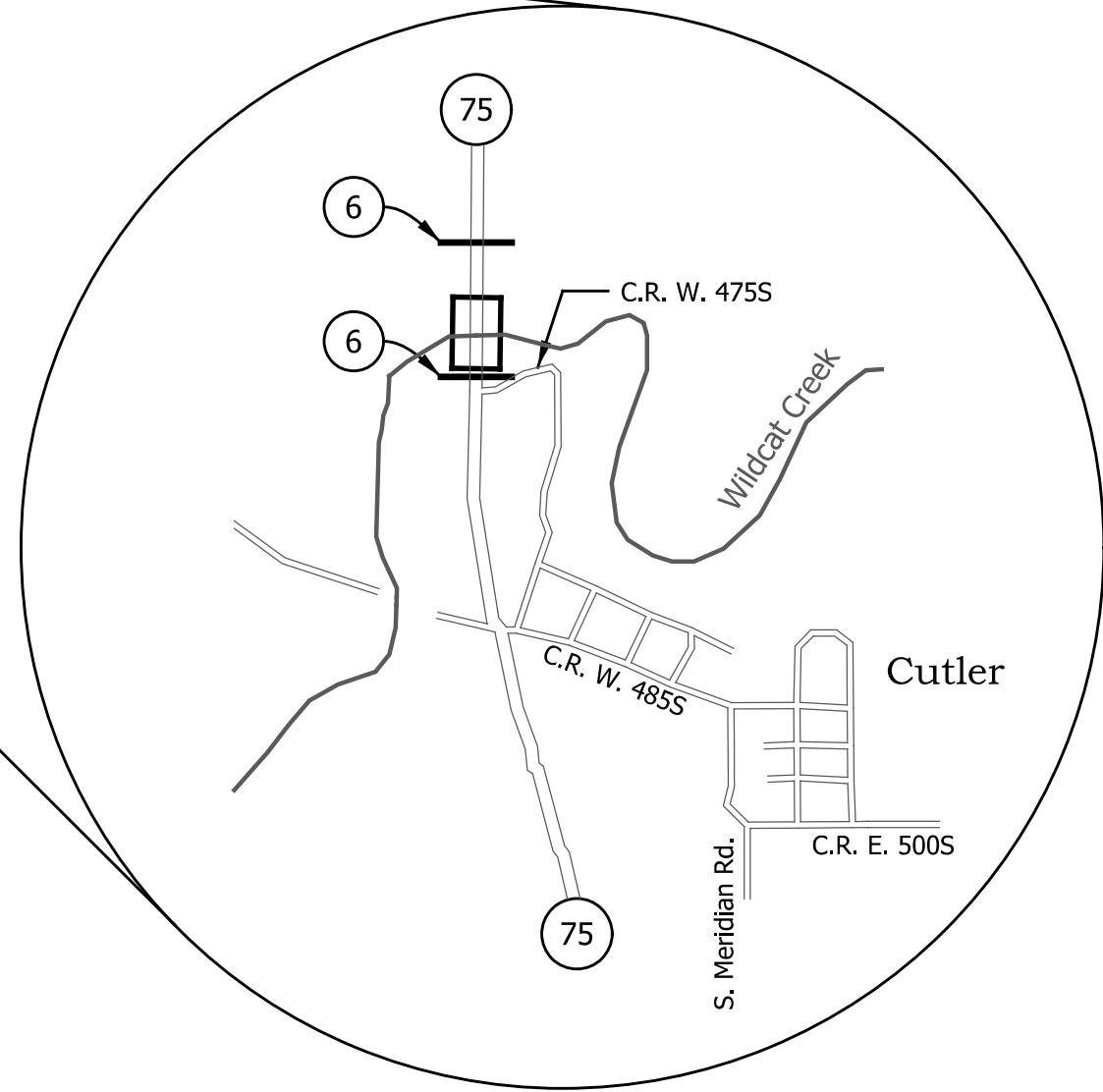
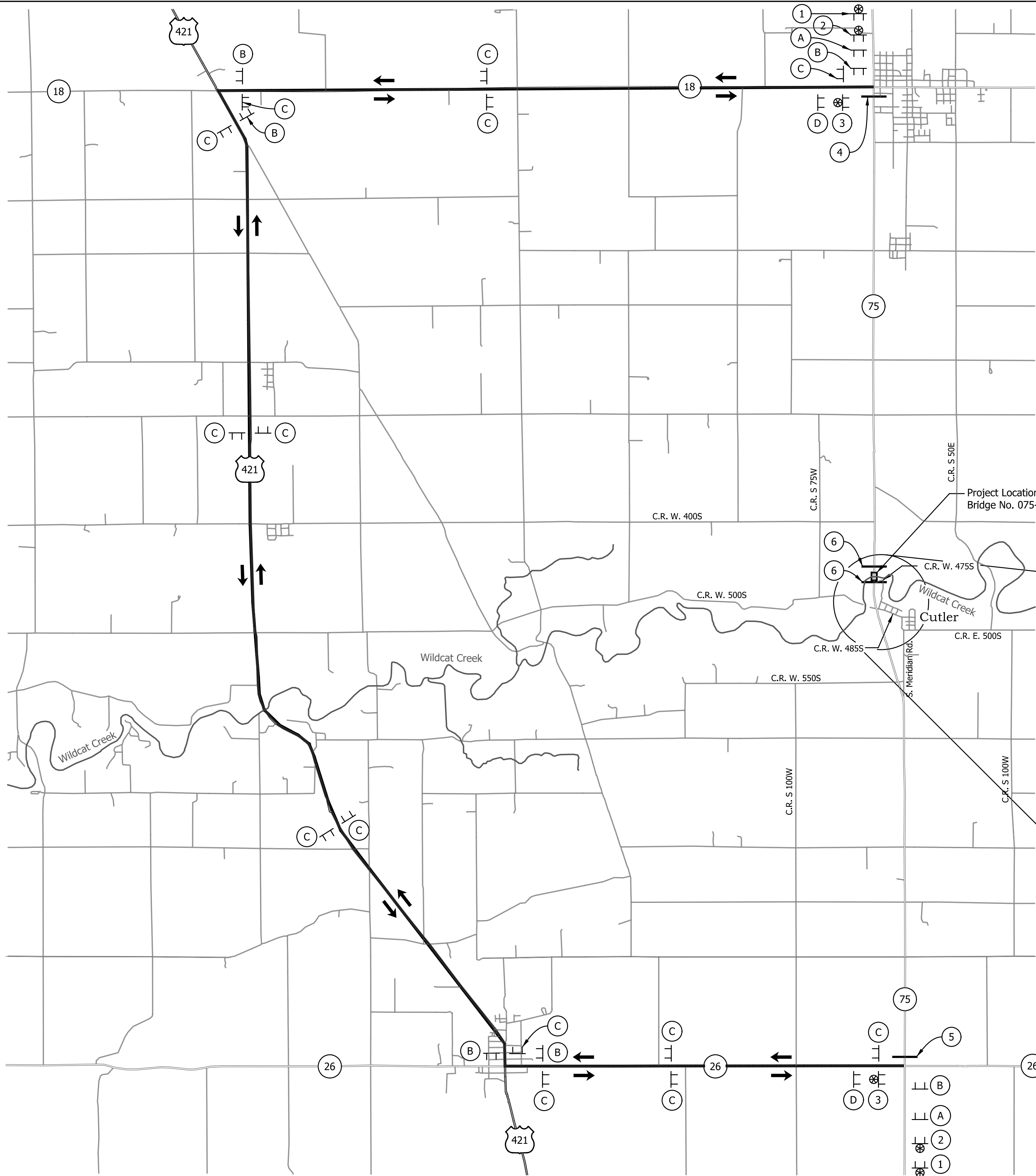
GENERAL NOTES	
	All earth shoulders, median areas, and cut and fill slopes shall be plain or mulch seeded except where sodding is specified
	The final cross sections of the grading contract will be the original cross sections of the paving contract. However, partial or complete cross sections shall be taken if necessary to determine the actual excavation quantities.
	The paper relocation will be cross sectioned by the Engineer before construction.
	Existing asphalt pavement located outside the construction limits, between Sta. _____ and Sta. _____, shall be removed as directed.
	The quantity of peat excavation shown on the plans has been estimated on the basis of theoretical cross sections by using treatment of existing fills, treatment by removal, or treatment by displacement, where each treatment applies.
	All limited access right-of-way (L.A. R/W) is to be fenced with chain link type fence (CLTF) or farm field type fence (FFTF) where specified in the plans.
	Contractor shall verify existing flowline elevations to set the appropriate sump depth.

** REPRESENTS GENERAL NOTES REQUIRED

INDEX	
SHEET NO.	DRAWING INDEX
1	TITLE SHEET
2	INDEX AND GENERAL NOTES
3	DETOUR PLAN
4	LAYOUT
5-7	GENERAL PLAN
8	REMOVAL DETAILS
9-12	STRUCTURAL STEEL DETAILS
13-16	SUPERSTRUCTURE DETAILS
17-18	R.C. BRIDGE APPROACH DETAILS
19	BRIDGE SUMMARY OF QUANTITIES

REVISIONS		
SHEET NO.	DATE	REVISED

		RECOMMENDED FOR APPROVAL _____ DESIGN ENGINEER _____ DATE _____		INDIANA DEPARTMENT OF TRANSPORTATION		HORIZONTAL SCALE		BRIDGE FILE	
						NONE		075-08-03653C	
						VERTICAL SCALE		DESIGNATION	
		DESIGNED: _____ DSH _____ DRAWN: _____ DSH _____ CHECKED: _____ DAD _____ CHECKED: _____ DAD _____		INDEX AND GENERAL NOTES		NONE		1601029	
						SURVEY BOOK		SHEETS	
						ELECTRONIC		2	of
CONTRACT		PROJECT							
B-39876		1601029							



END
CONSTRUCTION

XG20-2

ROAD
CLOSED

R11-2

ROAD CLOSED
4.5 MILE AHEAD
LOCAL TRAFFIC ONLY

DETOUR

R11-3
XM4-10(R)

DETOUR
AHEAD

XW20-2

ROAD CLOSED
4.5 MILE AHEAD
LOCAL TRAFFIC ONLY

R11-3
XM4-10(L)



R3-2 (L)

ROAD
CLOSED
AHEAD

XW20-3

QUANTITY SUMMARY

Detour Route Marker Assembly	24	Each
Construction Sign, Type A	10	Each
Construction Sign, Type B	2	Each
Type III-A Barricade	82	Lft.
Type III-B Barricade	48	Lft.
Road Closure Sign Assembly	5	Each

LEGEND

- ① XW20-3 ROAD CLOSED AHEAD
- ② XW20-2 DETOUR AHEAD
- ③ XG20-2 END CONSTRUCTION
- ④ Type III-B Barricade W/(R 11-3)
Road Closed "4.5" Miles Ahead Local Traffic Only &
XM4-10 (R) Detour Arrow (Right)
- ⑤ Type III-B Barricade W/(R 11-3)
Road Closed "4.5" Miles Ahead Local Traffic Only &
XM4-10 (L) Detour Arrow (Left)
- ⑥ 3-12' Type III-A Barricades &
R11-2 ROAD CLOSED
- ⊕ Construction Sign With Type A Warning Light
- TT Detour Route Marker Assembly
- Detour Route

NOTES:

For Detour Route Marker Assemblies (A), (B), (C) & (D),
see Standard Drawing E801-TCDT-03.

For Traffic Control Device Details, see Standard
Drawing E801-TCDV-04 thru 08 & E801-TCDT-03.

DATE	REVISION

RECOMMENDED
FOR APPROVAL

DESIGN ENGINEER

DATE

DESIGNED: DSH

DRAWN: DSH

CHECKED: DAD

CHECKED: DAD

INDIANA
DEPARTMENT OF TRANSPORTATION

DETOUR PLAN

HORIZONTAL SCALE

NONE

VERTICAL SCALE

NONE

SURVEY BOOK

ELECTRONIC

CONTRACT

B-39876

BRIDGE FILE

075-08-03653C

DESIGNATION

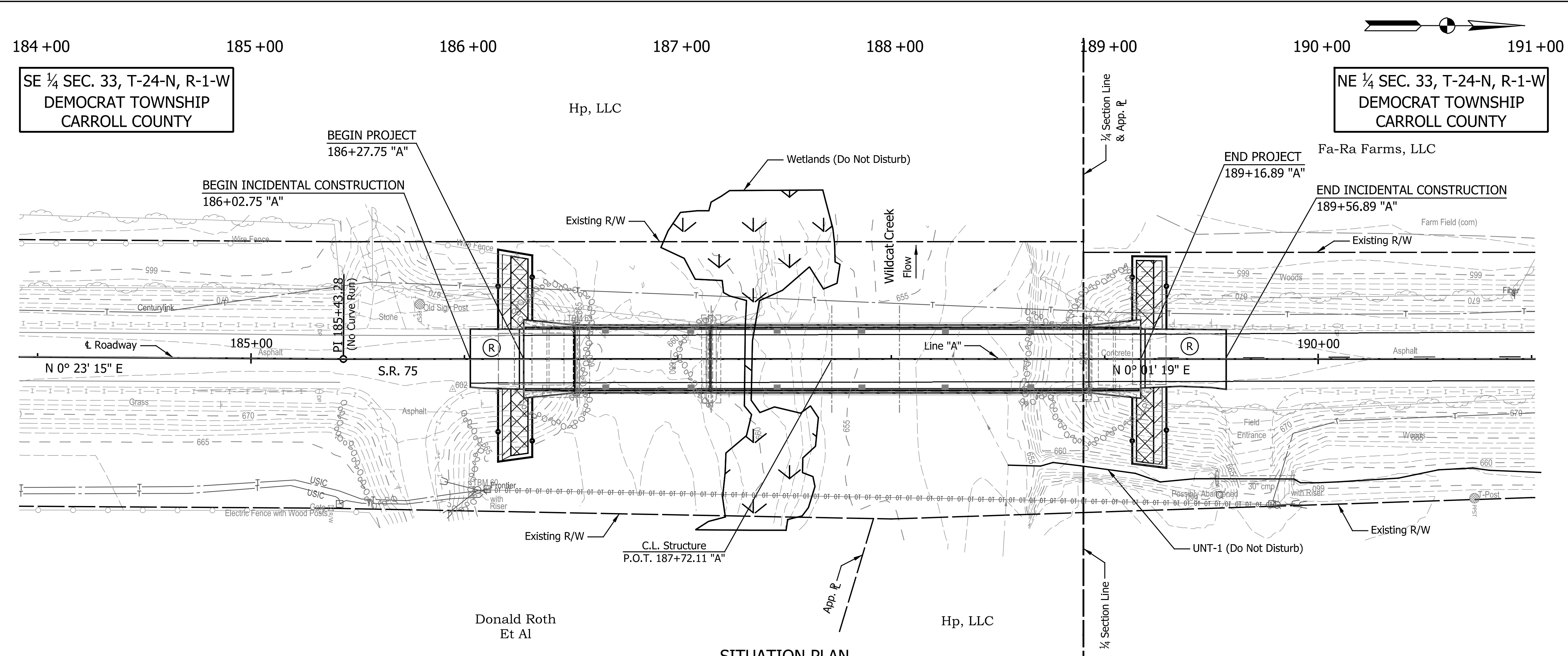
1601029

SHEETS

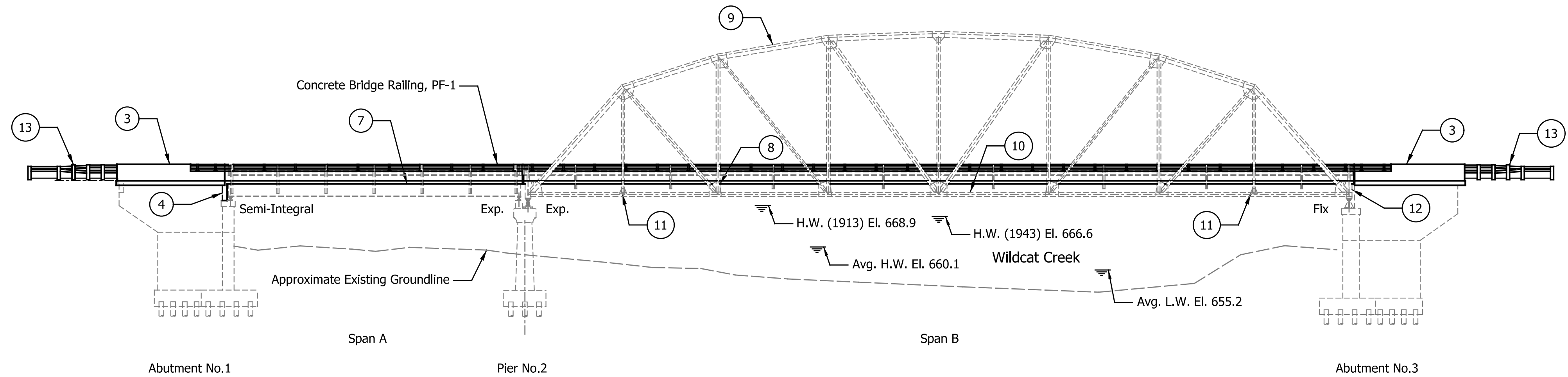
3 of 19

PROJECT

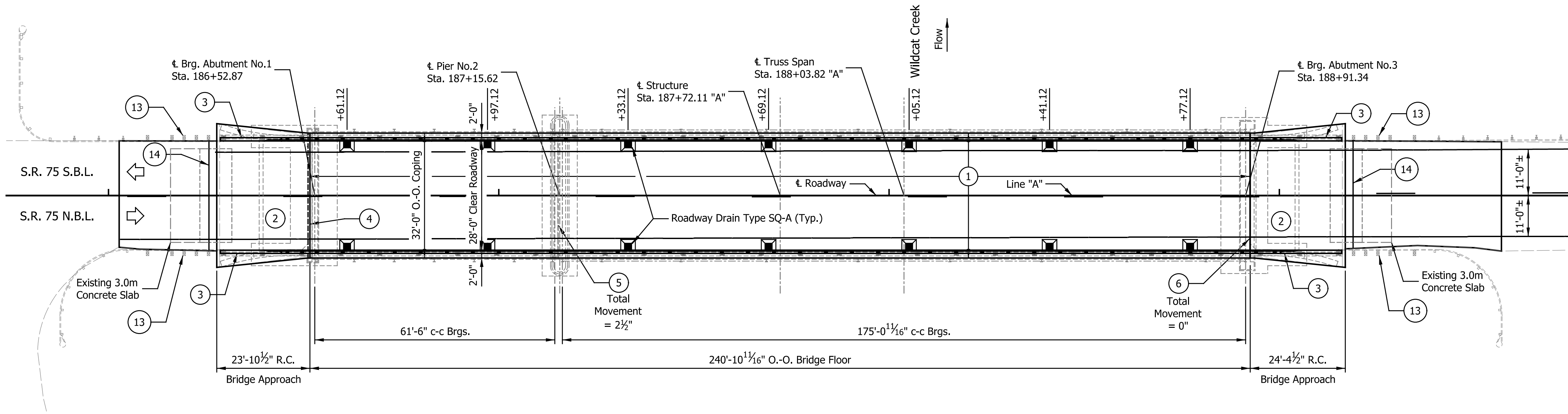
1601029



Structure Built To A 0.00% Grade



ELEVATION



PLAN

GENERAL NOTES

Plans for the existing structure are on file with INDOT as Bridge File 75-J-3653, 75-08-3653A and 75-08-3653B, and are available upon request.

Portions of the present structure shall be removed.

Reinforcing steel covering to be 2 1/2" in the top and 1" minimum in the bottom of floor slabs, and 2" in all other parts, unless noted.

Surface seal exposed faces of concrete bridge railing, concrete bridge railing transitions, top, face and underside of copings and bridge approach slabs.

DESIGN DATA

- LIVE LOAD: Originally designed for H20 loading in accordance with 1941 AASHTO Specifications. Proposed concrete deck designed for HS20 loading with impact and distribution of loads and live load in accordance with 2002 AASHTO Standard Specifications and Interim Specifications for Highway Bridges using Load Factor Design Method.
- DEAD LOAD: Actual Weight plus 35 Lbs./Sft. for future wearing surface and 15 Lbs./Sft. for permanent metal deck forms.
- FLOOR SLAB: Designed for 16,000 Lbs. wheel load impact with a structural depth of 7 1/2".
- UNIT STRESSES: Reinforcing Steel, Fy = 60,000 psi
Concrete Class B, Fc = 3,000 psi
Concrete Class A, Fc = 3,500 psi
Concrete Class C, Fc = 4,000 psi
- ALLOWABLE STRESSES: In accordance with 2002 AASHTO Specifications and Interim Specifications using Load Factor Design Method.

LEGEND

- 1 Remove Existing Concrete Bridge Deck, Microsilica Overlay and Bridge Railing, CF-1. Construct Concrete Bridge Deck and Concrete Bridge Railing, PF-1.
- 2 Remove Existing R.C. Bridge Approach Slab and Bridge Railing, CF-1. Construct R.C. Bridge Approach Slab.
- 3 Construct Concrete Bridge Railing, PF-1 and Concrete Bridge Railing Transition, TPF-1.
- 4 Remove Existing Mudwall Down To Bridge Seat. Existing Steel Bearings To Remain In Place. Construct Semi-Integral End Bent Diaphragm.
- 5 Install Expansion Joint Class SS. (Est. Qty. = 32 Lft.)
- 6 Install Pre-Compressed Foam Joint. (Est. Qty. = 32 Lft.)
- 7 Install Shear Studs On Top Flange Of Existing Beams. (Est. Qty. = 1302 Each)
- 8 Install Shear Studs On Top Flange Of Existing Floor Beams. (Est. Qty. = 1242 Each)
- 9 Paint Structural Steel.
- 10 Repair Structural Steel With Patching, Or Replace With In-Kind Structural Steel Members.
- 11 Replace Inadequate Rivets With High Strength Bolts.
- 12 Heat Straighten North End Floorbeam and Replace Connecting Gusset Plates For Cross Bracing.
- 13 Reset Existing Guardrail Transition Type TGT.
- 14 Retrofit Terminal Joint With Polymer Modified Asphalt.

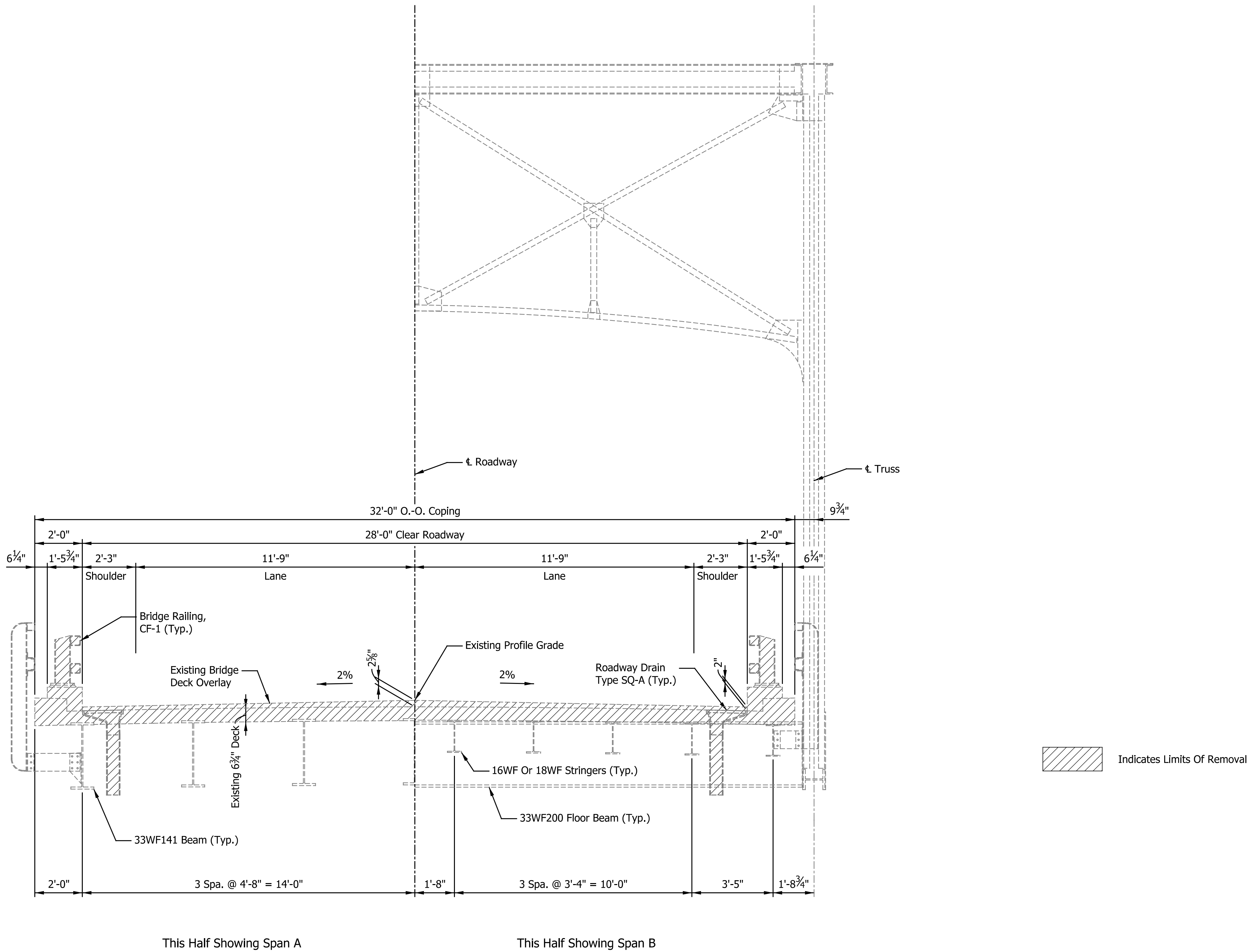
STEEL BEAM AND STEEL TRUSS BRIDGE
DECK REPLACEMENT

2 SPANS: 61'-6", 175'-0 1/16" SKREW: SQUARE CLEAR ROADWAY: 28'-0"
S.R. 75 ROAD OVER WILDCAT CREEK
CARROLL COUNTY

DATE	REVISION					INDIANA DEPARTMENT OF TRANSPORTATION		HORIZONTAL SCALE	BRIDGE FILE
								1/16" = 1'-0", U.N.	075-08-03653C
								VERTICAL SCALE	DESIGNATION
								1/16" = 1'-0", U.N.	1601029
								SURVEY BOOK	SHEETS
								ELECTRONIC	5 of 19
								CONTRACT	PROJECT
								B-39876	1601029

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: NFG	DRAWN: DSH	
CHECKED: DAD	CHECKED: DAD	

GENERAL PLAN



EXISTING TYPICAL SECTION

STEEL BEAM AND STEEL TRUSS BRIDGE
DECK REPLACEMENT
2 SPANS: 61'-6", 175'-0"^{1 1/16"} SKEW: SQUARE CLEAR ROADWAY: 28'-0"
S.R. 75 ROAD OVER WILDCAT CREEK
CARROLL COUNTY

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AMERICAN
STRUCTUREPOINT
INC.

July 27, 2018

Example Early Coordination Letter

Regional Environmental Coordinator
Midwest Regional Office
National Park Service
601 Riverfront Drive
Omaha, Nebraska 68102

Re: Des. No. 1601029, SR 75 over Wildcat Creek Bridge Improvement, Bridge No. 075-08-03653 B,
Carroll County, Indiana

Dear Sir or Madam:

The Indiana Department of Transportation (INDOT) LaPorte District and Federal Highway Administration (FHWA) intend to proceed with a project involving the aforementioned bridge in Carroll County. This letter is part of the early coordination phase of the environmental review process. We are requesting comments from your area of expertise regarding any possible environmental effects associated with this project. **Please use the above designation number and description in your reply.** We will incorporate your comments into a study of the project's environmental impacts.

The project is located at the State Road (SR) 75 over Wildcat Creek Bridge in Carroll County. The current bridge over Wildcat Creek [INDOT Bridge No. 075-08-03653 B; Historic Bridge (HB)-1897; Indiana Historic Sites and Structures Inventory (IHSSI) No.: 015-553-65014; National Bridge Inventory (NBI) No.: 24970] is a two-span bridge consisting of a thru truss with cast-in-place deck span and a steel girder span. The bridge was constructed in 1947 and reconstructed most recently in 2001. The bridge was determined eligible for listing in the National Register of Historic Places (NRHP) in the Indiana Historic Bridge Inventory and was designated as "Non-Select," meaning it is not a good candidate for preservation in place. The existing right-of-way throughout the project corridor is approximately 40 feet on each side of the centerline of the bridge. Land use in the proposed project corridor consists of maintained grass right-of-way, agricultural land, scattered residences and wooded areas along Wildcat Creek.

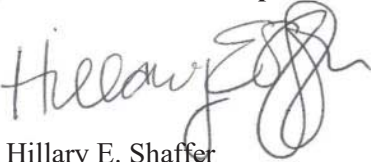
Existing clear roadway width is 28 feet with two lanes. Proposed clear roadway width for reconstruction of the bridge would be 30 feet with two 12-foot lanes and two 3-foot shoulders. Proposed clear roadway width for replacement of the bridge would be a maximum of 46 feet with two 12-foot lanes and two 11-foot shoulders. Deficiencies of the existing bridge include spalling and cracking on the approach slabs, deck, wearing surface, and substructure. Additionally, holes are present in the wearing surface and superstructure. Superstructure also exhibits thinning of splice and gusset plates along bottom chord and rivet corrosion along lower chords. The load rating and existing shoulder width of the bridge are insufficient and do not meet current safety standards.

An alternatives analysis will be completed to determine the scope of work for the proposed project. Since the bridge is listed as a Non-Select Bridge, rehabilitation of the existing bridge will occur if determined feasible and prudent. Additional options, including a one-way pair and replacement, will be considered in case rehabilitation is determined unfeasible or imprudent. Right-of-way requirements are not yet known for this project. No relocations are anticipated as part of this project. For each of the alternatives, the preferred method of traffic maintenance for this project would include closure of SR 75 over Wildcat Creek. Detour routes would be set up for through traffic, and access would be maintained for local traffic at all times. A full MOT plan will be developed once the alternative analysis has been completed and design further develops.

A wetland delineation and waters investigation will be performed to identify ecological resources that may be present. Coordination for the Indiana Bat and Northern Long-eared Bat will be completed using the USFWS's Information for Planning and Consulting (IPaC) system, and the results of the IPaC determination will be reviewed by the USFWS. In addition, the areas of additional right-of-way will be investigated for archaeological and historic resources for Section 106 compliance. The results of this investigation will be coordinated with INDOT Cultural Resources and the State Historic Preservation Officer for review and concurrence.

Should we not receive your response **within thirty (30) calendar days** from the date of this letter, it will be assumed that your agency feels that there will be no adverse effects incurred as a result of the proposed project. However, should you find that an extension to the response time is necessary, a reasonable amount may be granted upon request. If you have any questions regarding this matter, please feel free to contact Hillary E. Shaffer, American Structurepoint, Inc. by phone at (317) 547-5580 or e-mail at hshaffer@structurepoint.com, or Jackie Hockaday INDOT LaPorte District Project Manager at 219-325-7529 or jhockaday@indot.in.gov. Thank you in advance for your input.

Very truly yours,
American Structurepoint, Inc.



Hillary E. Shaffer
Environmental Specialist

Enclosures

State Location Map
USGS Topographic Map – Rossville Quadrangle
2011 Aerial Photography
Photo Location Map and Ground-Level Photos

Note: State Location Map, USGS Topographic Map, 2011 Aerial Photography, Photograph Location Map and Ground Level Photos are included in Appendix B.

Distribution List

USFWS, Northern Indiana Suboffice
US Natural Resources Conservation Service
US Army Corps of Engineers, Louisville District
US Department of Housing and Urban Development
National Park Service
Federal Highway Administration
Indiana Geological Survey
INDOT, Public Involvement
INDOT, LaPorte District
INDOT, Environmental Policy Office

IDNR, Division of Fish and Wildlife
IDNR, Division of Outdoor Recreation
Indiana Department of Environmental Management
Kankakee-Iroquois Regional Planning Commission
Carroll County Highway Department
Carroll County Commissioners
Carroll County Sheriff's Office
Carroll County Surveyor's Office
Carroll Consolidated School Corporation
Carroll County Emergency Management Agency

This letter was also distributed to the floodplain administrator with Carroll County Area Plan Commission on 10/5/2020.



Indiana Department of Environmental Management

We Protect Hoosiers and Our Environment.

100 North Senate Avenue - Indianapolis, IN 46204
(800) 451-6027 - (317) 232-8603 - www.idem.IN.gov

INDOT LaPorte District
Jackie Hockaday
315 E. Boyd Blvd.
LaPorte , IN 46350
Date

American Structurepoint, Inc.
Hillary Shaffer
7260 Shadeland Station
Indianapolis , IN 46256

To Engineers and Consultants Proposing Roadway Construction Projects:

RE: The proposed project corridor is located at the State Road (SR) 75 Bridge over Wildcat Creek in Carroll County, Indiana (INDOT Bridge No. 075-08-03653 B). The project corridor extends approximately 0.08 mile north and south of the bridge approaches and extends 0.03 mile east of SR 75 along S 20 W. The existing SR 75 over Wildcat Creek Bridge [INDOT Bridge No. 075-08-03653 B; Historic Bridge (HB)-1897; Indiana Historic Sites and Structures Inventory (IHSSI) No.: 015-553-65014; National Bridge Inventory (NBI) No.: 24970] is a two-span bridge consisting of a thru truss with cast-in-place deck span and a steel girder span. The bridge was constructed in 1947 and reconstructed most recently in 2001. The bridge was determined eligible for listing in the National Register of Historic Places (NRHP) in the Indiana Historic Bridge Inventory and was designated as "Non-Select," meaning it is not a good candidate for preservation in place. Because of this, an alternatives analysis will be completed to determine the scope of work for the proposed project. Alternatives considered include rehabilitation of the existing structure, construction of a one-way pair, or a structure replacement.

This letter from the Indiana Department of Environmental Management (IDEM) serves as a standardized response to enquiries inviting IDEM comments on roadway construction, reconstruction, or other improvement projects within existing roadway corridors when the proposed scope of the project is beneath the threshold requiring a formal National Environmental Policy Act-mandated Environmental Assessment or Environmental Impact Statement. As the letter attempts to address all roadway-related environmental topics of potential concern, it is possible that not every topic addressed in the letter will be applicable to your particular roadway project.

For additional information on specific roadway-related topics of interest, please visit the appropriate Web pages cited below, many of which provide contact information for persons within the various program areas who can answer questions not fully addressed in this letter. Also please be mindful that some environmental requirements may be subject to change and so each person intending to include a copy of this letter in their project documentation packet is advised to download the most recently revised version of the letter; found at: <http://www.in.gov/idem/5283.htm> (<http://www.in.gov/idem/5283.htm>).

To ensure that all environmentally-related issues are adequately addressed, IDEM recommends that you read this letter in its entirety, and consider each of the following issues as you move forward with the planning of your proposed roadway construction, reconstruction, or improvement project:

WATER AND BIOTIC QUALITY

1. Section 404 of the Clean Water Act requires that you obtain a permit from the U.S. Army Corps of Engineers (USACE) before discharging dredged or fill materials into any wetlands or other waters, such as rivers, lakes, streams, and ditches. Other activities regulated include the relocation, channelization, widening, or other such alteration of a stream, and the mechanical clearing (use of heavy construction equipment) of wetlands. Thus, as a project owner or sponsor, it is your responsibility to ensure that no wetlands are disturbed without the proper permit. Although you may initially refer to the U.S. Fish and Wildlife Service National Wetland Inventory maps as a means of identifying potential areas of concern, please be mindful that those maps do not depict jurisdictional wetlands regulated by the USACE or the Department of Environmental Management. A valid jurisdictional wetlands determination can only be made by the USACE, using the 1987 Wetland Delineation Manual.

USACE recommends that you have a consultant check to determine whether your project will abut, or lie within, a wetland area. To view a list of consultants that have requested to be included on a list posted by the USACE on their Web site, see USACE Permits and Public Notices (<http://www.lrl.usace.army.mil/orf/default.asp>) (<http://www.lrl.usace.army.mil/orf/default.asp>) and then click on "Information" from the menu on the right-hand side of that page. Their "Consultant List" is the fourth entry down on the "Information" page. Please note that the USACE posts all consultants that request to appear on the list, and that inclusion of any particular consultant on the list does not represent an endorsement of that consultant by the USACE, or by IDEM.

Much of northern Indiana (Newton, Lake, Porter, LaPorte, St. Joseph, Elkhart, LaGrange, Steuben, and Dekalb counties; large portions of Jasper, Starke, Marshall, Noble, Allen, and Adams counties; and lesser portions of Benton, White, Pulaski, Kosciusko, and Wells counties) is served by the USACE District Office in Detroit (313-226-6812). The central and southern portions of the state (large portions of Benton, White, Pulaski, Kosciusko, and Wells counties; smaller portions of Jasper, Starke, Marshall, Noble, Allen, and Adams counties; and all other Indiana counties located in north-central, central, and southern Indiana) are served by the USACE Louisville District Office (502-315-6733).

Additional information on contacting these U.S. Army Corps of Engineers (USACE) District Offices, government agencies with jurisdiction over wetlands, and other water quality issues, can be found at <http://www.in.gov/idem/4396.htm> (<http://www.in.gov/idem/4396.htm>). IDEM recommends that impacts to wetlands and other water resources be avoided to the fullest extent.

2. In the event a Section 404 wetlands permit is required from the USACE, you also must obtain a Section 401 Water Quality Certification from the IDEM Office of Water Quality Wetlands Program. To learn more about the Wetlands Program, visit: <http://www.in.gov/idem/4384.htm> (<http://www.in.gov/idem/4384.htm>).
3. If the USACE determines that a wetland or other water body is isolated and not subject to Clean Water Act regulation, it is still regulated by the state of Indiana . A State Isolated Wetland permit from IDEM's Office of Water Quality (OWQ) is required for any activity that results in the discharge of dredged or fill materials into isolated wetlands. To learn more about isolated wetlands, contact the OWQ Wetlands Program at 317-233-8488.
4. If your project will involve over a 0.5 acre of wetland impact, stream relocation, or other large-scale alterations to water bodies such as the creation of a dam or a water diversion, you should seek additional input from the OWQ Wetlands Program staff. Consult the Web at: <http://www.in.gov/idem/4384.htm> (<http://www.in.gov/idem/4384.htm>) for the appropriate staff contact to further discuss your project.

5. Work within the one-hundred year floodway of a given water body is regulated by the Department of Natural Resources, Division of Water. The Division issues permits for activities regulated under the follow statutes:

- IC 14-26-2 Lakes Preservation Act 312 IAC 11
- IC 14-26-5 Lowering of Ten Acre Lakes Act No related code
- IC 14-28-1 Flood Control Act 310 IAC 6-1
- IC 14-29-1 Navigable Waterways Act 312 IAC 6
- IC 14-29-3 Sand and Gravel Permits Act 312 IAC 6
- IC 14-29-4 Construction of Channels Act No related code

For information on these Indiana (statutory) Code and Indiana Administrative Code citations, see the DNR Web site at: <http://www.in.gov/dnr/water/9451.htm> (<http://www.in.gov/dnr/water/9451.htm>) . Contact the DNR Division of Water at 317-232-4160 for further information.

The physical disturbance of the stream and riparian vegetation, especially large trees overhanging any affected water bodies should be limited to only that which is absolutely necessary to complete the project. The shade provided by the large overhanging trees helps maintain proper stream temperatures and dissolved oxygen for aquatic life.

6. For projects involving construction activity (which includes clearing, grading, excavation and other land disturbing activities) that result in the disturbance of one (1), or more, acres of total land area, contact the Office of Water Quality – Watershed Planning Branch (317/233-1864) regarding the need for of a Rule 5 Storm Water Runoff Permit. Visit the following Web page

- <http://www.in.gov/idem/4902.htm> (<http://www.in.gov/idem/4902.htm>)

To obtain, and operate under, a Rule 5 permit you will first need to develop a Construction Plan (<http://www.in.gov/idem/4917.htm#constreq> (<http://www.in.gov/idem/4917.htm#constreq>)), and as described in 327 IAC 15-5-6.5 (<http://www.in.gov/legislative/iac/T03270/A00150> [PDF] (<http://www.in.gov/legislative/iac/T03270/A00150.PDF>), pages 16 through 19). Before you may apply for a Rule 5 Permit, or begin construction, you must submit your Construction Plan to your county Soil and Water Conservation District (SWCD) (<http://www.in.gov/isda/soil/contacts/map.html> (<http://www.in.gov/isda/soil/contacts/map.html>)).

Upon receipt of the construction plan, personnel of the SWCD or the Indiana Department of Environmental Management will review the plan to determine if it meets the requirements of 327 IAC 15-5. Plans that are deemed deficient will require re-submittal. If the plan is sufficient you will be notified and instructed to submit the verification to IDEM as part of the Rule 5 Notice of Intent (NOI) submittal. Once construction begins, staff of the SWCD or Indiana Department of Environmental Management will perform inspections of activities at the site for compliance with the regulation.

Please be mindful that approximately 149 Municipal Separate Storm Sewer System (MS4) areas are now being established by various local governmental entities throughout the state as part of the implementation of Phase II federal storm water requirements. All of these MS4 areas will eventually take responsibility for Construction Plan review, inspection, and enforcement. As these MS4 areas obtain program approval from IDEM, they will be added to a list of MS4 areas posted on the IDEM Website at: <http://www.in.gov/idem/4900.htm> (<http://www.in.gov/idem/4900.htm>).

If your project is located in an IDEM-approved MS4 area, please contact the local MS4 program about meeting their storm water requirements. Once the MS4 approves the plan, the NOI can be submitted to IDEM.

Regardless of the size of your project, or which agency you work with to meet storm water requirements, IDEM recommends that appropriate structures and techniques be utilized both during the construction phase, and after completion of the project, to minimize the impacts associated with storm water runoff. The use of appropriate planning and site development and appropriate storm water quality measures are recommended to prevent soil from leaving the construction site during active land disturbance and for post construction water quality concerns. Information and assistance regarding storm water related to construction activities are available from the Soil and Water Conservation District (SWCD) offices in each county or from IDEM.

7. For projects involving impacts to fish and botanical resources, contact the Department of Natural Resources - Division of Fish and Wildlife (317/232-4080) for addition project input.
8. For projects involving water main construction, water main extensions, and new public water supplies, contact the Office of Water Quality - Drinking Water Branch (317-308-3299) regarding the need for permits.
9. For projects involving effluent discharges to waters of the State of Indiana , contact the Office of Water Quality - Permits Branch (317-233-0468) regarding the need for a National Pollutant Discharge Elimination System (NPDES) permit.
10. For projects involving the construction of wastewater facilities and sewer lines, contact the Office of Water Quality - Permits Branch (317-232-8675) regarding the need for permits.

AIR QUALITY

The above-noted project should be designed to minimize any impact on ambient air quality in, or near, the project area. The project must comply with all federal and state air pollution regulations. Consideration should be given to the following:

1. Regarding open burning, and disposing of organic debris generated by land clearing activities; some types of open burning are allowed (<http://www.in.gov/idem/4148.htm> (<http://www.in.gov/idem/4148.htm>)) under specific conditions. You also can seek an open burning variance from IDEM.

However, IDEM generally recommends that you take vegetative wastes to a registered yard waste composting facility or that the waste be chipped or shredded with composting on site (you must register with IDEM if more than 2,000 pounds is to be composted; contact 317/232-0066). The finished compost can then be used as a mulch or soil amendment. You also may bury any vegetative wastes (such as leaves, twigs, branches, limbs, tree trunks and stumps) onsite, although burying large quantities of such material can lead to subsidence problems, later on.

Reasonable precautions must be taken to minimize fugitive dust emissions from construction and demolition activities. For example, wetting the area with water, constructing wind barriers, or treating dusty areas with chemical stabilizers (such as calcium chloride or several other commercial products). Dirt tracked onto paved roads from unpaved areas should be minimized.

Additionally, if construction or demolition is conducted in a wooded area where blackbirds have roosted or abandoned buildings or building sections in which pigeons or bats have roosted for 3-5 years precautionary measures should be taken to avoid an outbreak of histoplasmosis. This disease is caused by the fungus *Histoplasma capsulatum*, which stems from bird or bat droppings that have accumulated in one area for 3-5 years. The spores from this fungus become airborne when the area is

disturbed and can cause infections over an entire community downwind of the site. The area should be wetted down prior to cleanup or demolition of the project site. For more detailed information on histoplasmosis prevention and control, please contact the Acute Disease Control Division of the Indiana State Department of Health at (317) 233-7272.

2. The U.S. EPA and the Surgeon General recommend that people not have long-term exposure to radon at levels above 4 pCi/L. (For a county-by-county map of predicted radon levels in Indiana, visit: <http://www.in.gov/idem/4145.htm> (<http://www.in.gov/idem/4145.htm>).)

The U.S. EPA further recommends that all homes (and apartments within three stories of ground level) be tested for radon. If in-home radon levels are determined to be 4 pCi/L, or higher, EPA recommends a follow-up test. If the second test confirms that radon levels are 4 pCi/L, or higher, EPA recommends the installation of radon-reduction measures. (For a list of qualified radon testers and radon mitigation (or reduction) specialists visit:

http://www.in.gov/isdh/regsvcs/radhealth/pdfs/radon_testers_mitigators_list.pdf

(http://www.in.gov/isdh/regsvcs/radhealth/pdfs/radon_testers_mitigators_list.pdf.) It also is

recommended that radon reduction measures be built into all new homes, particularly in areas like Indiana that have moderate to high predicted radon levels.

To learn more about radon, radon risks, and ways to reduce exposure visit:

<http://www.in.gov/isdh/regsvcs/radhealth/radon.htm>

(<http://www.in.gov/isdh/regsvcs/radhealth/radon.htm>), <http://www.in.gov/idem/4145.htm>

(<http://www.in.gov/idem/4145.htm>), or <http://www.epa.gov/radon/index.html>

(<http://www.epa.gov/radon/index.html>).

3. With respect to asbestos removal: all facilities slated for renovation or demolition (except residential buildings that have (4) four or fewer dwelling units and which will not be used for commercial purposes) must be inspected by an Indiana-licensed asbestos inspector prior to the commencement of any renovation or demolition activities. If regulated asbestos-containing material (RACM) that may become airborne is found, any subsequent demolition, renovation, or asbestos removal activities must be performed in accordance with the proper notification and emission control requirements.

If no asbestos is found where a renovation activity will occur, or if the renovation involves removal of less than 260 linear feet of RACM off of pipes, less than 160 square feet of RACM off of other facility components, or less than 35 cubic feet of RACM off of all facility components, the owner or operator of the project does not need to notify IDEM before beginning the renovation activity.

For questions on asbestos demolition and renovation activities, you can also call IDEM's Lead/Asbestos section at 1-888-574-8150.

However, in all cases where a demolition activity will occur (even if no asbestos is found), the owner or operator must still notify IDEM 10 working days prior to the demolition, using the form found at

<http://www.in.gov/icpr/webfile/formsdiv/44593.pdf>

(<http://www.in.gov/icpr/webfile/formsdiv/44593.pdf>).

Anyone submitting a renovation/demolition notification form will be billed a notification fee based upon the amount of friable asbestos containing material to be removed or demolished. Projects that involve the removal of more than 2,600 linear feet of friable asbestos containing materials on pipes, or 1,600 square feet or 400 cubic feet of friable asbestos containing material on other facility components, will be billed a fee of \$150 per project; projects below these amounts will be billed a fee of \$50 per project. All notification remitters will be billed on a quarterly basis.

For more information about IDEM policy regarding asbestos removal and disposal, visit:
<http://www.in.gov/idem/4983.htm> (<http://www.in.gov/idem/4983.htm>).

4. With respect to lead-based paint removal: IDEM encourages all efforts to minimize human exposure to lead-based paint chips and dust. IDEM is particularly concerned that young children exposed to lead can suffer from learning disabilities. Although lead-based paint abatement efforts are not mandatory, any abatement that is conducted within housing built before January 1, 1978, or a child-occupied facility is required to comply with all lead-based paint work practice standards, licensing and notification requirements. For more information about lead-based paint removal visit:
<http://www.in.gov/isdh/19131.htm> (<http://www.in.gov/isdh/19131.htm>).
5. Ensure that asphalt paving plants are permitted and operate properly. The use of cutback asphalt, or asphalt emulsion containing more than seven percent (7%) oil distillate, is prohibited during the months April through October. See 326 IAC 8-5-2, Asphalt Paving Rule (<http://www.ai.org/legislative/iac/T03260/A00080.PDF> (<http://www.ai.org/legislative/iac/T03260/A00080.PDF>)).
6. If your project involves the construction of a new source of air emissions or the modification of an existing source of air emissions or air pollution control equipment, it will need to be reviewed by the IDEM Office of Air Quality (OAQ). A registration or permit may be required under 326 IAC 2 (View at: www.ai.org/legislative/iac/t03260/a00020.pdf (<http://www.ai.org/legislative/iac/t03260/a00020.pdf>)). New sources that use or emit hazardous air pollutants may be subject to Section 112 of the Clean Air Act and corresponding state air regulations governing hazardous air pollutants.
7. For more information on air permits visit: <http://www.in.gov/idem/4223.htm> (<http://www.in.gov/idem/4223.htm>), or to initiate the IDEM air permitting process, please contact the Office of Air Quality Permit Reviewer of the Day at (317) 233-0178 or OAMPROD atdem.state.in.us.

LAND QUALITY

In order to maintain compliance with all applicable laws regarding contamination and/or proper waste disposal, IDEM recommends that:

1. If the site is found to contain any areas used to dispose of solid or hazardous waste, you need to contact the Office of Land Quality (OLQ) at 317-308-3103.
2. All solid wastes generated by the project, or removed from the project site, need to be taken to a properly permitted solid waste processing or disposal facility. For more information, visit <http://www.in.gov/idem/4998.htm> (<http://www.in.gov/idem/4998.htm>).
3. If any contaminated soils are discovered during this project, they may be subject to disposal as hazardous waste. Please contact the OLQ at 317-308-3103 to obtain information on proper disposal procedures.
4. If PCBs are found at this site, please contact the Industrial Waste Section of OLQ at 317-308-3103 for information regarding management of any PCB wastes from this site.
5. If there are any asbestos disposal issues related to this site, please contact the Industrial Waste Section of OLQ at 317-308-3103 for information regarding the management of asbestos wastes (Asbestos removal is addressed above, under Air Quality).

6. If the project involves the installation or removal of an underground storage tank, or involves contamination from an underground storage tank, you must contact the IDEM Underground Storage Tank program at 317/308-3039. See: <http://www.in.gov/idem/4999.htm> (<http://www.in.gov/idem/4999.htm>).

FINAL REMARKS

Should you need to obtain any environmental permits in association with this proposed project, please be mindful that IC 13-15-8 requires that you notify all adjoining property owners and/or occupants within ten days your submittal of each permit application. However, if you are seeking multiple permits, you can still meet the notification requirement with a single notice if all required permit applications are submitted with the same ten day period.

Should the scope of the proposed project be expanded to the extent that a National Environmental Policy Act Environmental Assessment (EA) or Environmental Impact Statement (EIS) is required, IDEM will actively participate in any early interagency coordination review of the project.

Meanwhile, please note that this letter does not constitute a permit, license, endorsement or any other form of approval on the part of the Indiana Department of Environmental Management regarding any project for which a copy of this letter is used. Also note that it is the responsibility of the project engineer or consultant using this letter to ensure that the most current draft of this document, which is located at <http://www.in.gov/idem/5284.htm> (<http://www.in.gov/idem/5284.htm>), is used.

Signature(s) of the Applicant

I acknowledge that the following proposed roadway project will be financed in part, or in whole, by public monies.

Project Description

The proposed project corridor is located at the State Road (SR) 75 Bridge over Wildcat Creek in Carroll County, Indiana (INDOT Bridge No. 075-08-03653 B). The project corridor extends approximately 0.08 mile north and south of the bridge approaches and extends 0.03 mile east of SR 75 along S 20 W. The existing SR 75 over Wildcat Creek Bridge [INDOT Bridge No. 075-08-03653 B; Historic Bridge (HB)-1897; Indiana Historic Sites and Structures Inventory (IHSSI) No.: 015-553-65014; National Bridge Inventory (NBI) No.: 24970] is a two-span bridge consisting of a thru truss with cast-in-place deck span and a steel girder span. The bridge was constructed in 1947 and reconstructed most recently in 2001. The bridge was determined eligible for listing in the National Register of Historic Places (NRHP) in the Indiana Historic Bridge Inventory and was designated as "Non-Select," meaning it is not a good candidate for preservation in place. Because of this, an alternatives analysis will be completed to determine the scope of work for the proposed project. Alternatives considered include rehabilitation of the existing structure, construction of a one-way pair, or a structure replacement.

With my signature, I do hereby affirm that I have read the letter from the Indiana Department of Environment that appears directly above. In addition, I understand that in order to complete that project in which I am interested, with a minimum of impact to the environment, I must consider all the issues addressed in the aforementioned letter, and further, that I must obtain any required permits.

Date: 11/5/2020

Signature of the INDOT

Project Engineer or Other Responsible Agent

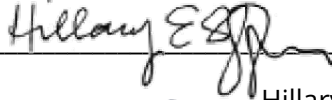


Jackie Hockaday

Date: July 30, 2018

Signature of the

For Hire Consultant



Hillary Shaffer



Organization and Project Information

Project ID:

Des. ID: 1601029

Project Title: SR 75 over Wildcat Creek Bridge Improvement

Name of Organization: American Structurepoint, Inc.

Requested by: Hillary Shaffer

Environmental Assessment Report

1. Geological Hazards:

- High liquefaction potential
- 1% Annual Chance Flood Hazard

2. Mineral Resources:

- Bedrock Resource: High Potential
- Sand and Gravel Resource: Low Potential

3. Active or abandoned mineral resources extraction sites:

- None documented in the area

*All map layers from Indiana Map (maps.indiana.edu)

DISCLAIMER:

This document was compiled by Indiana University, Indiana Geological Survey, using data believed to be accurate; however, a degree of error is inherent in all data. This product is distributed "AS-IS" without warranties of any kind, either expressed or implied, including but not limited to warranties of suitability to a particular purpose or use. No attempt has been made in either the design or production of these data and document to define the limits or jurisdiction of any federal, state, or local government. The data used to assemble this document are intended for use only at the published scale of the source data or smaller (see the metadata links below) and are for reference purposes only. They are not to be construed as a legal document or survey instrument. A detailed on-the-ground survey and historical analysis of a single site may differ from these data and this document.

This information was furnished by Indiana Geological Survey

Address: 611 N. Walnut Grove Avenue, Bloomington, IN 47405-2208

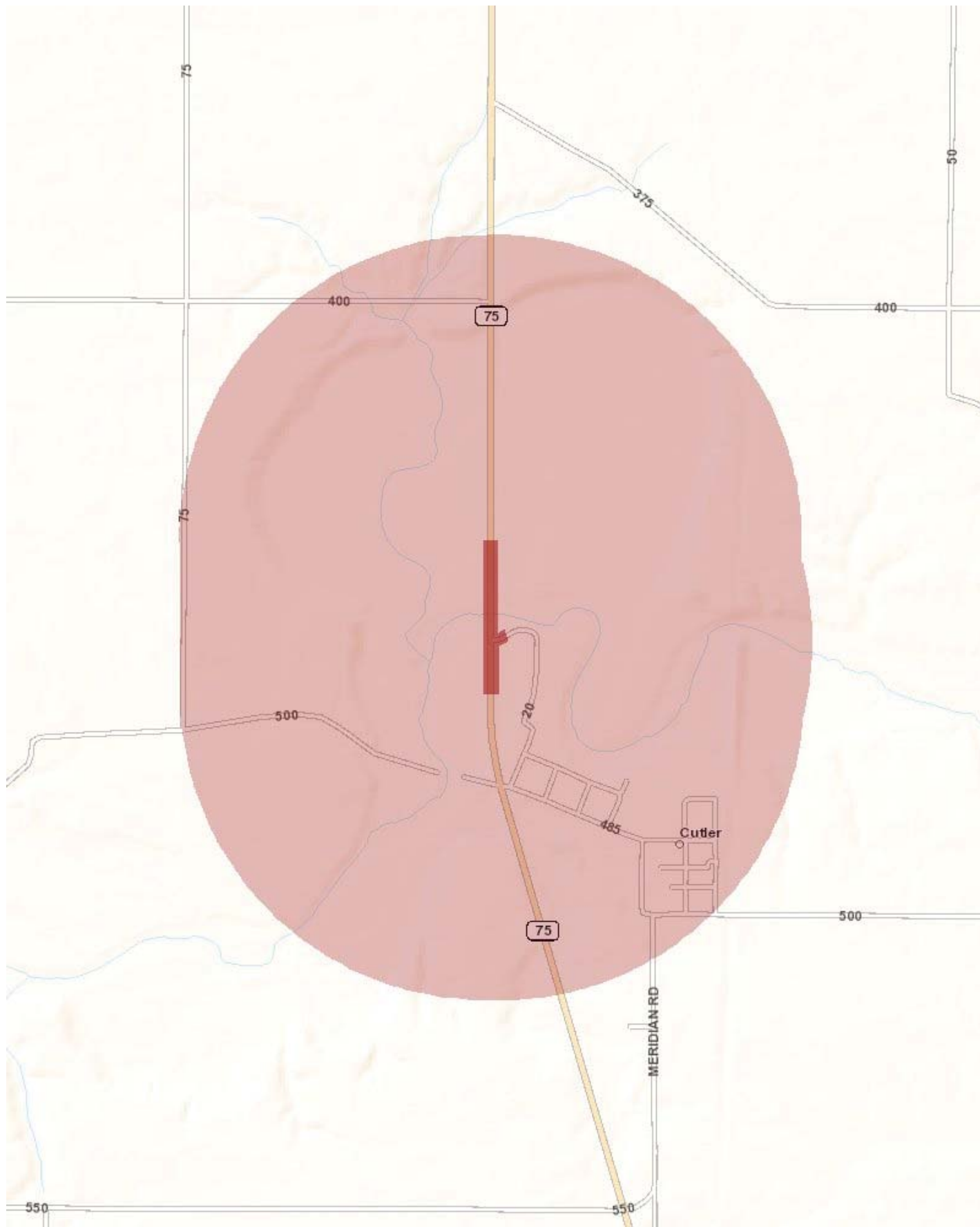
Email: IGSEnvir@indiana.edu

Phone: 812 855-7428

Date: July 30, 2018

Appendix C

Page C-11



Metadata:

- https://maps.indiana.edu/metadata/Geology/Seismic_Earthquake_Liquefaction_Potential.html
- https://maps.indiana.edu/metadata/Geology/Industrial_Minerals_Sand_Gravel_Resources.html
- https://maps.indiana.edu/metadata/Hydrology/Floodplains_FIRM.html
- https://maps.indiana.edu/metadata/Geology/Bedrock_Geology.html

Perry, Leah

From: Wright, Mary <MWRIGHT@indot.IN.gov>
Sent: Tuesday, July 31, 2018 8:32 AM
To: Shaffer, Hillary
Subject: RE: Early Coordination, 75 over Wildcat Creek - Des. No. 1601029

Early Coordination and Creating a Public Involvement Plan (PIP)

We have received your early coordination notification packet for the above referenced project(s). Our office prefers to be notified at the early coordination stage in order to encourage early and ongoing public involvement aside from the specific legal requirements as outlined in our Public Involvement Manual <http://www.in.gov/indot/2366.htm>. Seeking the public's understanding of transportation improvement projects early in the project development stage can allow the opportunity for the public to express their concerns, comments, and to seek buy-in. Early coordination is the perfect opportunity to examine the proposed project and its impacts to the community along with the many ways and or tools to inform the public of the improvements and seek engagement. A good public involvement plan, or PIP, should consider the type, scope, impacts, and the level of public awareness that should, or could, be implemented. In other words, although there are cases where no public involvement is legally required, sometimes it is simply the right thing to do in order to keep the public informed.

The public involvement office is always available to provide support and resources to bolster any public involvement activities you may wish to implement or discuss. Please feel free to contact our office anytime should you have any questions or concerns. Thank you for notifying our office about your proposed project. We trust you will not only analyze the appropriate public involvement required, but also consider the opportunity to do go above and beyond those requirements in creating a good PIP.

Rickie Clark, Manager
100 North Senate Avenue, Room N642
Indianapolis, IN 46204
Phone: 317-232-6601
Email: rclark@indot.in.gov

Mary Wright, Hearing Examiner
Phone: 317-234-0796
Email: mwright@indot.in.gov

From: Shaffer, Hillary [mailto:Hshaffer@structurepoint.com]
Sent: Friday, July 27, 2018 4:23 PM
To: Clark, Rickie <RCLARK@indot.IN.gov>
Cc: Wright, Mary <MWRIGHT@indot.IN.gov>
Subject: Early Coordination, 75 over Wildcat Creek - Des. No. 1601029

Dear Mr. Clark,

Please find attached the Early Coordination letter prepared for the 75 over Wildcat Creek Bridge Improvement project in Carroll County, Indiana. Please review the attached information and supply our office with any comments your office may have regarding the proposed project.

Sincerely,

August 2, 2018

Hillary Shaffer
Environmental Specialist
7260 Shadeland Station
Indianapolis, Indiana 46256

Dear Ms. Shaffer:

The proposed project to improve bridge number 075-08-03653 B in Carroll County, Indiana (Des. No. 1601029), as referred to in your letter received July 27, 2018, will not be able to be determined at this time due to a lack of site specific information. Please resubmit when specific/potential impacts are determined

If you need additional information, please contact Rick Neilson at 317-295-5875.

Sincerely,

JERRY RAYNOR Digitally signed by JERRY RAYNOR
Date: 2018.08.13 18:19:13 -04'00'

JERRY RAYNOR
State Conservationist



November 16, 2020

Leah Perry
American StructurePoint
9025 River Road, Suite 200
Indianapolis, Indiana 46240

Dear Ms. Perry:

The revised project to make improvements to bridge number 075-08-03653 B in Carroll County, Indiana (Des. No. 1601029), as referred to in your letter received October 5, will not cause a conversion of prime farmland

If you need additional information, please contact John Allen at 317-295-5859.

Sincerely,

RICHARD
NEILSON
Digitally signed by
RICHARD NEILSON
Date: 2020.11.17
07:42:23 -05'00'

RICK NEILSON
State Soil Scientist



Perry, Leah

From: Herber, Lisa <LHerber1@dnr.IN.gov>
Sent: Wednesday, August 8, 2018 1:18 PM
To: Shaffer, Hillary
Cc: Bronson, Bob
Subject: FW: Early Coordination, 75 over Wildcat Creek - Des. No. 1601029
Attachments: Herber.2017.00220.EV.2017-11-27.EC.Letter.mdd.hes - Emailing.hes-7.pdf

Ms. Shaffer—this project will not impact a Land and Water Conservation Fund site but it will occur within a section of Wildcat Creek that is designated as a Natural, Scenic, and Recreational River System (I.C. 14-29-6). If all preventative measures are taken to avoid impacts to the river and its recreational use there should be no negative impacts. If this is not the case please contact us for further coordination.

Sincerely,

Lisa Herber

Grants Coordinator
Indiana Department of Natural Resources
Division of Outdoor Recreation
402 W. Washington Street, W271
Indianapolis, Indiana 46204
Phone: 317-232-4074
Email: LHerber1@dnr.IN.gov

“Be more conscious and intentional with your communication.

Ask others if they understand what you intended.

Spend more time in clarifying messages you send – and receive.”

From: Shaffer, Hillary [mailto:Hshaffer@structurepoint.com]
Sent: Friday, July 27, 2018 4:22 PM
To: Herber, Lisa <LHerber1@dnr.IN.gov>
Cc: Boits, Leah <lboits@structurepoint.com>
Subject: Early Coordination, 75 over Wildcat Creek - Des. No. 1601029

**** This is an EXTERNAL email. Exercise caution. DO NOT open attachments or click links from unknown senders or unexpected email. ****

Dear Ms. Herber,

Please find attached the Early Coordination letter prepared for the 75 over Wildcat Creek Bridge Improvement project in Carroll County, Indiana. Please review the attached information and supply our office with any comments your office may have regarding the proposed project.



United States Department of the Interior

Fish and Wildlife Service



Indiana Field Office (ES)
620 South Walker Street
Bloomington, IN 47403-2121
Phone: (812) 334-4261 Fax: (812) 334-4273

August 9, 2018

Ms. Hillary E. Shaffer
American Structurepoint, Inc.
7260 Shadeland Station
Indianapolis, Indiana 46256

Project No.: Des. 1601029
Project: SR 75 over Wildcat Creek
Location: Cutler, Carroll County

Dear Ms. Shaffer:

This responds to your letter dated July 27, 2018, requesting our comments on the aforementioned project.

These comments have been prepared under the authority of the Fish and Wildlife Coordination Act (16 U.S.C. 661 et. seq.) and are consistent with the intent of the National Environmental Policy Act of 1969, the Endangered Species Act of 1973, and the U. S. Fish and Wildlife Service's Mitigation Policy.

The proposed project consists of upgrades at the existing bridge site, but the exact project has not yet been determined. The existing bridge may be rehabilitated, a new one-way bridge may be constructed adjacent to the existing bridge to provide a one-way pair, or the existing bridge may be replaced with a wider 2-lane bridge. Since the scope of the project has not yet been determine, the possible need for additional permanent right-of-way has also not yet been determined.

Given these unknowns, we recommend that if an additional bridge is determined to be necessary, it be constructed on the upstream side (east) of the existing bridge in order to lessen impact to the woodland habitats present within the overall project area. Because of a rural residence in the southeastern quadrant, there are fewer trees along both the highway and Wildcat Creek in this quadrant. A new bridge would require considerable tree clearing in the other 3 quadrants. Rehabilitation of the existing bridge may require little or no tree clearing.

If an alternative is selected that requires tree clearing, we request a commitment to mitigation in the form of planting native hardwoods to replace the woodland to be removed. We support the woodland mitigation guidelines of the Indiana Department of Natural Resources contained in their Information Bulletin #17 (<http://www.in.gov/legislative/iac/20140806-IR-312140295NRA.xml.pdf>) which states that the mitigation ratio for non-wetland floodway forest losses of more than 1 acre is to be 2:1 (2 acres replanted for every acre destroyed), planted as close to the impact site as possible; loss of less than an acre in a rural area is to be mitigated at a 1:1 ratio. If any of the woodland that would be removed is forested wetland, the mitigation ratio is 4:1.

ENDANGERED SPECIES

The proposed project is within the range of the Federally endangered Indiana bat (*Myotis sodalis*), clubshell mussel (*Pleurobema clava*), fanshell mussel (*Cyprogenia stegaria*), rayed bean mussel (*Villosa fabalis*), sheepsnose mussel (*Plethobasus cyphus*), and snuffbox mussel (*Epioblasma triquetra*), and the threatened northern long-eared bat (*Myotis septentrionalis*), eastern massasauga rattlesnake (*Sistrurus catenatus*), and rabbitsfoot mussel (*Quadrula cylindrica cylindrica*). Impacts to the 2 bat species will be determined utilizing the bat programmatic informal consultation process. There is no habitat for the mussel species and eastern massasauga within Wildcat Creek, so we agree that the proposed project is not likely to adversely affect these endangered and threatened mussel and snake species.

This precludes the need for further consultation on this project on the mussel species and eastern massasauga as required under Section 7 of the Endangered Species Act of 1973, as amended. However, should new information arise pertaining to project plans or a revised species list be published, it will be necessary for the Federal agency to reinitiate consultation.

We appreciate the opportunity to comment on this proposed project. Please keep us informed about project planning as it progresses. For further discussion, please contact Elizabeth McCloskey at (219) 983-9753 or elizabeth.mccloskey@fws.gov.

Sincerely yours,

/s/ *Elizabeth S. McCloskey*

for Scott E. Pruitt
Supervisor

Sent via email August 9, 2018; no hard copy to follow.

cc: Christie Stanifer, Environmental Coordinator, Division of Fish and Wildlife, Indianapolis, IN



United States Department of the Interior

FISH AND WILDLIFE SERVICE

Indiana Ecological Services Field Office

620 South Walker Street

Bloomington, IN 47403-2121

Phone: (812) 334-4261 Fax: (812) 334-4273

<http://www.fws.gov/midwest/Endangered/section7/s7process/step1.html>



In Reply Refer To:

April 05, 2021

Consultation Code: 03E12000-2021-SLI-0033

Event Code: 03E12000-2021-E-05039

Project Name: Des. No. 1601029, SR 75 over Wildcat Creek Bridge Project

Subject: Updated list of threatened and endangered species that may occur in your proposed project location or may be affected by your proposed project

To Whom It May Concern:

The attached species list identifies any federally threatened, endangered, proposed and candidate species that may occur within the boundary of your proposed project or may be affected by your proposed project. The list also includes designated critical habitat if present within your proposed project area or affected by your project. This list is provided to you as the initial step of the consultation process required under section 7(c) of the Endangered Species Act, also referred to as Section 7 Consultation.

Section 7 of the Endangered Species Act of 1973 requires that actions authorized, funded, or carried out by Federal agencies not jeopardize federally threatened or endangered species or adversely modify designated critical habitat. To fulfill this mandate, Federal agencies (or their designated non-federal representative) must consult with the Service if they determine their project “may affect” listed species or critical habitat.

Under 50 CFR 402.12(e) (the regulations that implement Section 7 of the Endangered Species Act) the accuracy of this species list should be verified after 90 days. This verification can be completed formally or informally. You may verify the list by visiting the ECOS-IPaC website <http://ecos.fws.gov/ipac/> at regular intervals during project planning and implementation and completing the same process you used to receive the attached list. As an alternative, you may contact this Ecological Services Field Office for updates.

Please use the species list provided and visit the U.S. Fish and Wildlife Service’s Region 3 Section 7 Technical Assistance website at - <http://www.fws.gov/midwest/endangered/section7/s7process/index.html>. This website contains step-by-step instructions which will help you determine if your project will have an adverse effect on listed species and will help lead you through the Section 7 process.

For all **wind energy projects** and **projects that include installing towers that use guy wires or are over 200 feet in height**, please contact this field office directly for assistance, even if no federally listed plants, animals or critical habitat are present within your proposed project or may be affected by your proposed project.

Although no longer protected under the Endangered Species Act, be aware that bald eagles are protected under the Bald and Golden Eagle Protection Act (16 U.S.C. 668 *et seq.*) and Migratory Bird Treaty Act (16 U.S.C. 703 *et seq.*), as are golden eagles. Projects affecting these species may require measures to avoid harming eagles or may require a permit. If your project is near an eagle nest or winter roost area, see our Eagle Permits website at <http://www.fws.gov/midwest/midwestbird/EaglePermits/index.html> to help you determine if you can avoid impacting eagles or if a permit may be necessary.

We appreciate your concern for threatened and endangered species. Please include the Consultation Tracking Number in the header of this letter with any request for consultation or correspondence about your project that you submit to our office.

Attachment(s):

- Official Species List

Official Species List

This list is provided pursuant to Section 7 of the Endangered Species Act, and fulfills the requirement for Federal agencies to "request of the Secretary of the Interior information whether any species which is listed or proposed to be listed may be present in the area of a proposed action".

This species list is provided by:

Indiana Ecological Services Field Office

620 South Walker Street
Bloomington, IN 47403-2121
(812) 334-4261

Project Summary

Consultation Code: 03E12000-2021-SLI-0033

Event Code: 03E12000-2021-E-05039

Project Name: Des. No. 1601029, SR 75 over Wildcat Creek Bridge Project

Project Type: TRANSPORTATION

Project Description: The SR 75 over Wildcat Creek Bridge (INDOT Bridge 075-08-03653 B) is located approximately 4.5 miles south of SR 18 in Democrat Township, in Cutler, Carroll County, Indiana. The proposed project area extends approximately 175 feet north and 115 feet south of the bridge centerline along Wildcat Creek. Incidental construction will extend an additional 40 feet north and 25 feet south along SR 75. The project area is more specifically located in Section 33, Township 24 North, and Range 1 West on the Rossville 7.5 Minute United States Geological Survey (USGS) topographic quadrangle.

The project proposes to rehabilitate of the existing structure for continued vehicular use by increasing the structural capacity to at or above the legal load rating. The existing bridge deck would be removed from both of the spans and a new concrete deck would be constructed. The existing approach slabs would be removed and replaced. Additional rehabilitation of the deck would include installing a new expansion joint at pier 2, creating a new drainage system for the structure and replacing pavement markings in-kind with previous roadway striping.

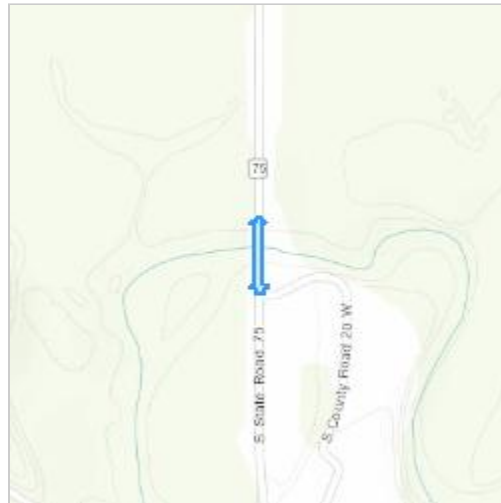
The top flanges of truss floor beams in span B and main beams in span will be cleaned. Shear studs will be installed on top of these beams to increase the load carrying capacity of the beams. Any damaged or deteriorated stringers or other structural steel components will be cleaned and patched or replaced “in-kind.” The damaged floor beam will be heat straightened. Gusset plates connecting the cross bracing will be replaced. The existing bearing assemblies will remain in place and will be cleaned and painted. Spalled bearing pedestals will be patched. Spalled sections on abutments and pier 2 will be patched as well. Abutment 1 will be converted into a semi integral end bent. Existing type CF-1 railing will be removed and replaced with type PF-1 railing and type TPF railing transition at all four ends. The original railing (constructed in 1947) will stay in place. Existing guardrail will be reset as necessary. Approximately 88 rivets that have been determined inadequate will be replaced with high strength bolts to increase the load carrying capacity. After the repairs are done, the structural steel will be painted. The usable shoulder on the bridge is currently 2.25-feet on each edge. To meet minimum useable shoulder width requirements, the lane width will be reduced from 11.75-feet to 11-feet to increase the shoulder width to 3-feet. The total clear roadway width will remain unchanged. The incidental approach will be

tied-in. No permanent or temporary right-of-way is anticipated to be required.

A review of the USFWS database on October 7, 2020 by INDOT LaPorte district did not indicate the presence of endangered and threatened bat species within a half mile of the project area. Some suitable bat summer habitat is within and adjacent to the project area. No trees will be cleared. The 11/08/2019 Bridge Inspection Report for the SR 75 over Wildcat Creek Bridge stated there was no evidence of bats. Construction is anticipated to occur between August 2022 and November 2023. Temporary lighting may be used during construction, but all lighting will be directed away from potential bat roosts. No permanent lighting will be added or changed.

Project Location:

Approximate location of the project can be viewed in Google Maps: <https://www.google.com/maps/@40.48174199365111,-86.53006810386336,14z>



Counties: Carroll County, Indiana

Endangered Species Act Species

There is a total of 2 threatened, endangered, or candidate species on this species list.

Species on this list should be considered in an effects analysis for your project and could include species that exist in another geographic area. For example, certain fish may appear on the species list because a project could affect downstream species. Note that 1 of these species should be considered only under certain conditions.

IPaC does not display listed species or critical habitats under the sole jurisdiction of NOAA Fisheries¹, as USFWS does not have the authority to speak on behalf of NOAA and the Department of Commerce.

See the "Critical habitats" section below for those critical habitats that lie wholly or partially within your project area under this office's jurisdiction. Please contact the designated FWS office if you have questions.

-
1. [NOAA Fisheries](#), also known as the National Marine Fisheries Service (NMFS), is an office of the National Oceanic and Atmospheric Administration within the Department of Commerce.

Mammals

NAME	STATUS
Indiana Bat <i>Myotis sodalis</i> There is final critical habitat for this species. The location of the critical habitat is not available. Species profile: https://ecos.fws.gov/ecp/species/5949	Endangered
Northern Long-eared Bat <i>Myotis septentrionalis</i> No critical habitat has been designated for this species. This species only needs to be considered under the following conditions: <ul style="list-style-type: none">▪ Incidental take of the NLEB is not prohibited here. Federal agencies may consult using the 4(d) rule streamlined process. Transportation projects may consult using the programmatic process. See www.fws.gov/midwest/endangered/mammals/nleb/index.html Species profile: https://ecos.fws.gov/ecp/species/9045	Threatened

Critical habitats

THERE ARE NO CRITICAL HABITATS WITHIN YOUR PROJECT AREA UNDER THIS OFFICE'S JURISDICTION.



United States Department of the Interior

FISH AND WILDLIFE SERVICE

Indiana Ecological Services Field Office

620 South Walker Street

Bloomington, IN 47403-2121

Phone: (812) 334-4261 Fax: (812) 334-4273

<http://www.fws.gov/midwest/Endangered/section7/s7process/step1.html>



In Reply Refer To:

October 14, 2020

Consultation Code: 03E12000-2021-I-0033

Event Code: 03E12000-2021-E-00230

Project Name: Des. No. 1601029, SR 75 over Wildcat Creek Bridge Project

Subject: Concurrence verification letter for the 'Des. No. 1601029, SR 75 over Wildcat Creek Bridge Project' project under the revised February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion for Transportation Projects within the Range of the Indiana Bat and Northern Long-eared Bat.

To whom it may concern:

The U.S. Fish and Wildlife Service (Service) has received your request to verify that the **Des. No. 1601029, SR 75 over Wildcat Creek Bridge Project** (Proposed Action) may rely on the concurrence provided in the February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion for Transportation Projects within the Range of the Indiana Bat and Northern Long-eared Bat (PBO) to satisfy requirements under Section 7(a)(2) of the Endangered Species Act of 1973 (ESA) (87 Stat. 884, as amended; 16 U.S.C 1531 *et seq.*).

Based on the information you provided (Project Description shown below), you have determined that the Proposed Action is within the scope and adheres to the criteria of the PBO, including the adoption of applicable avoidance and minimization measures, and may affect, but is not likely to adversely affect (NLAA) the endangered Indiana bat (*Myotis sodalis*) and/or the threatened Northern long-eared bat (*Myotis septentrionalis*).

The Service has 14 calendar days to notify the lead Federal action agency or designated non-federal representative if we determine that the Proposed Action does not meet the criteria for a NLAA determination under the PBO. If we do not notify the lead Federal action agency or designated non-federal representative within that timeframe, you may proceed with the Proposed Action under the terms of the NLAA concurrence provided in the PBO. This verification period allows Service Field Offices to apply local knowledge to implementation of the PBO, as we may identify a small subset of actions having impacts that were unanticipated. In such instances, Service Field Offices may request additional information that is necessary to verify inclusion of the proposed action under the PBO.

For Proposed Actions that include bridge/structure removal, replacement, and/or maintenance activities: If your initial bridge/structure assessments failed to detect Indiana bats, but you later detect bats during construction, please submit the Post Assessment Discovery of Bats at Bridge/Structure Form (User Guide Appendix E) to this Service Office. In these instances, potential incidental take of Indiana bats may be exempted provided that the take is reported to the Service.

If the Proposed Action is modified, or new information reveals that it may affect the Indiana bat and/or Northern long-eared bat in a manner or to an extent not considered in the PBO, further review to conclude the requirements of ESA Section 7(a)(2) may be required. If the Proposed Action may affect any other federally-listed or proposed species, and/or any designated critical habitat, additional consultation between the lead Federal action agency and this Service Office is required. If the proposed action has the potential to take bald or golden eagles, additional coordination with the Service under the Bald and Golden Eagle Protection Act may also be required. In either of these circumstances, please contact this Service Office.

Project Description

The following project name and description was collected in IPaC as part of the endangered species review process.

Name

Des. No. 1601029, SR 75 over Wildcat Creek Bridge Project

Description

The SR 75 over Wildcat Creek Bridge (INDOT Bridge 075-08-03653 B) is located approximately 4.5 miles south of SR 18 in Democrat Township, in Cutler, Carroll County, Indiana. The proposed project area extends approximately 175 feet north and 115 feet south of the bridge centerline along Wildcat Creek. Incidental construction will extend an additional 40 feet north and 25 feet south along SR 75. The project area is more specifically located in Section 33, Township 24 North, and Range 1 West on the Rossville 7.5 Minute United States Geological Survey (USGS) topographic quadrangle.

The project proposes to rehabilitate of the existing structure for continued vehicular use by increasing the structural capacity to at or above the legal load rating. The existing bridge deck would be removed from both of the spans and a new concrete deck would be constructed. The existing approach slabs would be removed and replaced. Additional rehabilitation of the deck would include installing a new expansion joint at pier 2, creating a new drainage system for the structure and replacing pavement markings in-kind with previous roadway striping.

The top flanges of truss floor beams in span B and main beams in span will be cleaned. Shear studs will be installed on top of these beams to increase the load carrying capacity of the beams. Any damaged or deteriorated stringers or other structural steel components will be cleaned and patched or replaced “in-kind.” The damaged floor beam will be heat straightened. Gusset plates connecting the cross bracing will be replaced. The existing bearing assemblies will remain in place and will be cleaned and painted. Spalled bearing pedestals will be patched. Spalled sections on abutments and pier 2 will be patched as well. Abutment 1 will be converted into a semi integral end bent. Existing type CF-1 railing will be removed and replaced with type PF-1 railing and type TPF railing transition at all four ends. The original railing (constructed in 1947) will stay in place. Existing guardrail will be reset as necessary. Approximately 88 rivets that have been determined inadequate will be replaced with high strength bolts to increase the load carrying capacity. After the repairs are done, the structural steel will be painted. The usable shoulder on the bridge is currently 2.25-feet on each edge. To meet minimum useable shoulder width requirements, the lane width will be reduced from 11.75-feet to 11-feet to increase the shoulder width to 3-feet. The total clear roadway width will remain unchanged. The incidental approach will be tied-in. No permanent or temporary right-of-way is anticipated to be required.

A review of the USFWS database on October 7, 2020 by INDOT LaPorte district did not indicate the presence of endangered and threatened bat species within a half mile of the project area. Some suitable bat summer habitat is within and adjacent to the project area. No trees will be cleared. The 11/08/2019 Bridge Inspection Report for the SR 75 over Wildcat Creek Bridge stated there was no evidence of bats. Construction is anticipated to occur between August 2022 and November 2023. Temporary lighting may be used during construction, but all lighting will be directed away from potential bat roosts. No permanent lighting will be added or changed.

Determination Key Result

Based on your answers provided, this project(s) may affect, but is not likely to adversely affect the endangered Indiana bat and/or the threatened Northern long-eared bat, therefore, consultation with the U.S. Fish and Wildlife Service pursuant to Section 7(a)(2) of the Endangered Species Act of 1973 (ESA) (87 Stat. 884, as amended 16 U.S.C. 1531 *et seq.*) is required. However, also based on your answers provided, this project may rely on the concurrence provided in the revised February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion for Transportation Projects within the Range of the Indiana Bat and Northern Long-eared Bat.

Qualification Interview

1. Is the project within the range of the Indiana bat^[1]?

[1] See [Indiana bat species profile](#)

Automatically answered

Yes

2. Is the project within the range of the Northern long-eared bat^[1]?

[1] See [Northern long-eared bat species profile](#)

Automatically answered

Yes

3. Which Federal Agency is the lead for the action?

A) Federal Highway Administration (FHWA)

4. Are *all* project activities limited to non-construction^[1] activities only? (examples of non-construction activities include: bridge/abandoned structure assessments, surveys, planning and technical studies, property inspections, and property sales)

[1] Construction refers to activities involving ground disturbance, percussive noise, and/or lighting.

No

5. Does the project include *any* activities that are **greater than** 300 feet from existing road/rail surfaces^[1]?

[1] Road surface is defined as the actively used [e.g. motorized vehicles] driving surface and shoulders [may be pavement, gravel, etc.] and rail surface is defined as the edge of the actively used rail ballast.

No

6. Does the project include *any* activities **within** 0.5 miles of a known Indiana bat and/or NLEB hibernaculum^[1]?

[1] For the purpose of this consultation, a hibernaculum is a site, most often a cave or mine, where bats hibernate during the winter (see suitable habitat), but could also include bridges and structures if bats are found to be hibernating there during the winter.

No

7. Is the project located **within** a karst area?

No

8. Is there *any* suitable^[1] summer habitat for Indiana Bat or NLEB **within** the project action area^[2]? (includes any trees suitable for maternity, roosting, foraging, or travelling habitat)

[1] See the Service's [summer survey guidance](#) for our current definitions of suitable habitat.

[2] The action area is defined as all areas to be affected directly or indirectly by the Federal action and not merely the immediate area involved in the action (50 CFR Section 402.02). Further clarification is provided by the [national consultation FAQs](#).

Yes

9. Will the project remove *any* suitable summer habitat^[1] and/or remove/trim any existing trees **within** suitable summer habitat?

[1] See the Service's [summer survey guidance](#) for our current definitions of suitable habitat.

No

10. Does the project include activities **within documented Indiana bat habitat**^{[1][2]}?

[1] Documented roosting or foraging habitat – for the purposes of this consultation, we are considering documented habitat as that where Indiana bats and/or NLEB have actually been captured and tracked using (1) radio telemetry to roosts; (2) radio telemetry biangulation/triangulation to estimate foraging areas; or (3) foraging areas with repeated use documented using acoustics. Documented roosting habitat is also considered as suitable summer habitat within 0.25 miles of documented roosts.)

[2] For the purposes of this key, we are considering documented corridors as that where Indiana bats and/or NLEB have actually been captured and tracked to using (1) radio telemetry; or (2) treed corridors located directly between documented roosting and foraging habitat.

No

11. Does the project include activities **within documented NLEB habitat**^{[1][2]}?

[1] Documented roosting or foraging habitat – for the purposes of this consultation, we are considering documented habitat as that where Indiana bats and/or NLEB have actually been captured and tracked using (1) radio telemetry to roosts; (2) radio telemetry triangulation/triangulation to estimate foraging areas; or (3) foraging areas with repeated use documented using acoustics. Documented roosting habitat is also considered as suitable summer habitat within 0.25 miles of documented roosts.)

[2] For the purposes of this key, we are considering documented corridors as that where Indiana bats and/or NLEB have actually been captured and tracked to using (1) radio telemetry; or (2) treed corridors located directly between documented roosting and foraging habitat.

No

12. Does the project include wetland or stream protection activities associated with compensatory wetland mitigation?

No

13. Does the project include slash pile burning?

No

14. Does the project include *any* bridge removal, replacement, and/or maintenance activities (e.g., any bridge repair, retrofit, maintenance, and/or rehabilitation work)?

Yes

15. Is there *any* suitable habitat^[1] for Indiana bat or NLEB **within** 1,000 feet of the bridge? (includes any trees suitable for maternity, roosting, foraging, or travelling habitat)

[1] See the Service's current [summer survey guidance](#) for our current definitions of suitable habitat.

Yes

16. Has a bridge assessment^[1] been conducted **within** the last 24 months^[2] to determine if the bridge is being used by bats?

[1] See [User Guide Appendix D](#) for bridge/structure assessment guidance

[2] Assessments must be completed no more than 2 years prior to conducting any work below the deck surface on all bridges that meet the physical characteristics described in the Programmatic Consultation, regardless of whether assessments have been conducted in the past. Due to the transitory nature of bat use, a negative result in one year does not guarantee that bats will not use that bridge/structure in subsequent years.

Yes

SUBMITTED DOCUMENTS

- 2019 bridge inspection report.pdf <https://ecos.fws.gov/ipac/project/QLQIF76MGJCE7FNIAZYLABSEME/projectDocuments/23777828>

17. Did the bridge assessment detect *any* signs of Indiana bats and/or NLEBs roosting in/under the bridge (bats, guano, etc.)^[1]?

[1] If bridge assessment detects signs of *any* species of bats, coordination with the local FWS office is needed to identify potential threatened or endangered bat species. Additional studies may be undertaken to try to identify which bat species may be utilizing the bridge prior to allowing *any* work to proceed.

Note: There is a small chance bridge assessments for bat occupancy do not detect bats. Should a small number of bats be observed roosting on a bridge just prior to or during construction, such that take is likely to occur or does occur in the form of harassment, injury or death, the PBO requires the action agency to report the take. Report all unanticipated take within 2 working days of the incident to the USFWS. Construction activities may continue without delay provided the take is reported to the USFWS and is limited to 5 bats per project.

No

18. Will the bridge removal, replacement, and/or maintenance activities include installing new or replacing existing **permanent** lighting?

No

19. Does the project include the removal, replacement, and/or maintenance of *any* structure other than a bridge? (e.g., rest areas, offices, sheds, outbuildings, barns, parking garages, etc.)

No

20. Will the project involve the use of **temporary** lighting *during* the active season?

Yes

21. Is there *any* suitable habitat **within** 1,000 feet of the location(s) where **temporary** lighting will be used?

Yes

22. Will the project install new or replace existing **permanent** lighting?

No

23. Does the project include percussives or other activities (**not including tree removal/trimming or bridge/structure work**) that will increase noise levels above existing traffic/background levels?

Yes

24. Will the activities that use percussives (**not including tree removal/trimming or bridge/structure work**) and/or increase noise levels above existing traffic/background levels be conducted *during* the active season^[1]?

[1] Coordinate with the local Service Field Office for appropriate dates.

Yes

25. Will *any* activities that use percussives (**not including tree removal/trimming or bridge/structure work**) and/or increase noise levels above existing traffic/background levels be conducted *during* the inactive season^[1]?

[1] Coordinate with the local Service Field Office for appropriate dates.

Yes

26. Are *all* project activities that are **not associated with** habitat removal, tree removal/trimming, bridge and/or structure activities, temporary or permanent lighting, or use of percussives, limited to actions that DO NOT cause any additional stressors to the bat species?

Examples: lining roadways, unlighted signage , rail road crossing signals, signal lighting, and minor road repair such as asphalt fill of potholes, etc.

Yes

27. Will the project raise the road profile **above the tree canopy**?

No

28. Are the project activities that use percussives (not including tree removal/trimming or bridge/structure work) consistent with a Not Likely to Adversely Affect determination in this key?

Automatically answered

Yes, because the activities are within 300 feet of the existing road/rail surface, greater than 0.5 miles from a hibernacula, and conducted during the active season within undocumented habitat.

29. Are the project activities that use percussives (not including tree removal/trimming or bridge/structure work) and/or increase noise levels above existing traffic/background levels consistent with a No Effect determination in this key?

Automatically answered

Yes, because the activities are within 300 feet of the existing road/rail surface, greater than 0.5 miles from a hibernacula, and conducted during the inactive season

30. Is the bridge removal, replacement, or maintenance activities portion of this project consistent with a No Effect determination in this key?

Automatically answered

Yes, because the bridge has been assessed using the criteria documented in the BA and no signs of bats were detected

31. **General AMM 1**

Will the project ensure *all* operators, employees, and contractors working in areas of known or presumed bat habitat are aware of *all* FHWA/FRA/FTA (Transportation Agencies) environmental commitments, including all applicable Avoidance and Minimization Measures?

Yes

32. **Lighting AMM 1**

Will *all* **temporary** lighting be directed away from suitable habitat during the active season?

Yes

Project Questionnaire

1. Have you made a No Effect determination for *all* other species indicated on the FWS IPaC generated species list?

N/A

2. Have you made a May Affect determination for *any* other species on the FWS IPaC generated species list?

N/A

3. Please describe the proposed bridge work:

The project proposes to rehabilitate of the existing structure for continued vehicular use by increasing the structural capacity to at or above the legal load rating. The existing bridge deck would be removed from both of the spans and a new concrete deck would be constructed. The existing approach slabs would be removed and replaced. Additional rehabilitation of the deck would include installing a new expansion joint at pier 2, creating a new drainage system for the structure and replacing pavement markings in-kind with previous roadway striping.

4. Please state the timing of all proposed bridge work:

Construction is anticipated to occur between August 2022 and November 2023.

5. Please enter the date of the bridge assessment:

11/08/2019

Avoidance And Minimization Measures (AMMs)

This determination key result includes the commitment to implement the following Avoidance and Minimization Measures (AMMs):

GENERAL AMM 1

Ensure all operators, employees, and contractors working in areas of known or presumed bat habitat are aware of all FHWA/FRA/FTA (Transportation Agencies) environmental commitments, including all applicable AMMs.

LIGHTING AMM 1

Direct temporary lighting away from suitable habitat during the active season.

Determination Key Description: FHWA, FRA, FTA Programmatic Consultation For Transportation Projects Affecting NLEB Or Indiana Bat

This key was last updated in IPaC on December 02, 2019. Keys are subject to periodic revision.

This decision key is intended for projects/activities funded or authorized by the Federal Highway Administration (FHWA), Federal Railroad Administration (FRA), and/or Federal Transit Administration (FTA), which may require consultation with the U.S. Fish and Wildlife Service (Service) under Section 7 of the Endangered Species Act (ESA) for the endangered **Indiana bat** (*Myotis sodalis*) and the threatened **Northern long-eared bat** (NLEB) (*Myotis septentrionalis*).

This decision key should only be used to verify project applicability with the Service's [February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion for Transportation Projects](#). The programmatic biological opinion covers limited transportation activities that may affect either bat species, and addresses situations that are both likely and not likely to adversely affect either bat species. This decision key will assist in identifying the effect of a specific project/activity and applicability of the programmatic consultation. The programmatic biological opinion is not intended to cover all types of transportation actions. Activities outside the scope of the programmatic biological opinion, or that may affect ESA-listed species other than the Indiana bat or NLEB, or any designated critical habitat, may require additional ESA Section 7 consultation.

State of Indiana
DEPARTMENT OF NATURAL RESOURCES
Division of Fish and Wildlife
Early Coordination/Environmental Assessment

DNR #: ER-20737

Request Received: July 27, 2018

Requestor: American Structurepoint, Inc
Hillary Shaffer
7260 Shadeland Station
Indianapolis, IN 46256

Project: SR 75 bridge (INDOT #075-08-03653, NBI #24970) improvement over Wildcat Creek;
Des #1601029

County/Site info: Carroll

The Indiana Department of Natural Resources has reviewed the above referenced project per your request. Our agency offers the following comments for your information and in accordance with the National Environmental Policy Act of 1969.

If our agency has regulatory jurisdiction over the project, the recommendations contained in this letter may become requirements of any permit issued. If we do not have permitting authority, all recommendations are voluntary.

Regulatory Assessment: This proposal will require the formal approval for construction in a floodway under the Flood Control Act, IC 14-28-1. Please submit a copy of this letter with the permit application.

Natural Heritage Database: The Natural Heritage Program's data have been checked.
The Wavyrayed Lampmussel (*Lampsilis fasciola*), a state species of special concern, has been documented in Wildcat Creek within 1/2 mile of the project area.

Fish & Wildlife Comments: We do not foresee any impacts to the Wavyrayed Lampmussel as a result of this project.

Avoid and minimize impacts to fish, wildlife, and botanical resources to the greatest extent possible, and compensate for impacts. The following are recommendations that address potential impacts identified in the proposed project area:

1) Stream Crossing Design:

For purposes of maintaining fish and wildlife passage through a crossing structure, the Environmental Unit recommends bridges rather than culverts and bottomless culverts rather than box or pipe culverts. Wide culverts are better than narrow culverts, and culverts with shorter through lengths are better than culverts with longer through lengths. If box or pipe culverts are used, the bottoms should be buried a minimum of 6" (or 20% of the culvert height/pipe diameter, whichever is greater up to a maximum of 2') below the stream bed elevation to allow a natural streambed to form within or under the crossing structure. Crossings should: span the entire channel width (a minimum of 1.2 times the bankfull width); maintain the natural stream substrate within the structure; have a minimum openness ratio (height x width / length) of 0.25; and have stream depth and water velocities during low-flow conditions that are approximate to those in the natural stream channel.

The new, replacement, or rehabbed structure, and any bank stabilization under the structure, should not create conditions that are less favorable for wildlife passage under the structure compared to the current conditions. The Division of Fish and Wildlife would like to emphasize the importance of wildlife passage issues and transportation infrastructure projects. The following is a good place to start in terms of resources to consider in the design of stream crossing structures:

State of Indiana
DEPARTMENT OF NATURAL RESOURCES
Division of Fish and Wildlife
Early Coordination/Environmental Assessment

<http://www.fs.fed.us/wildlifecrossings/library/>.

The following are recommended resources for designing and constructing stream crossings for maintenance of instream habitat and aquatic organism passage:

https://www.fs.fed.us/biology/nsaec/fishxing/aop_pdfs.html;

<https://www.fhwa.dot.gov/engineering/hydraulics/pubs/11008/hif11008.pdf>.

2) Bank Stabilization:

Some form of bank and/or streambed stabilization is almost always needed with the construction, repair, replacement, or modification of a stream channel or crossing structure. For streambank stabilization and erosion control, regrading to a stable slope (2:1 or shallower) and establishing native vegetation along the banks are typically the most effective techniques. A variety of methods to accomplish this include: planting plugs, whips, container stock, seeding, and live stakes. In addition to vegetation establishment, some additional level of bioengineered bank stabilization may be needed under certain circumstances (inability to regrade to a stable slope, flow velocities that exceed the limits of vegetation alone, etc.). Combining vegetation with any of the following bank stabilization methods can provide additional bank protection while not compromising benefits to fish, wildlife, and botanical resources: geotextiles (erosion control blankets and/or turf reinforcement mats that are heavy-duty, biodegradable, and net free or that use loose-woven / Leno-woven netting to minimize the entrapment and snaring of small-bodied wildlife such as snakes and turtles), vegetated geogrids or soil lifts, fiber rolls, glacial stone, or riprap. Information about bioengineering techniques can be found at <http://www.in.gov/legislative/iac/20120404-IR-312120154NRA.xml.pdf>. Additionally, the following is a link to a USDA/NRCS document that outlines many different bioengineering techniques for streambank stabilization: <http://directives.sc.egov.usda.gov/17553.wba>.

Riprap or other hard bank stabilization materials should be used only at the toe of the sideslopes up to the ordinary high water mark (OHWM) with the exception of areas directly under bridges for instance. The banks above the OHWM should be restored, stabilized, and revegetated using geotextiles and a mixture of grasses, sedges, wildflowers, shrubs, and trees native to Central Indiana and specifically for stream bank/floodway stabilization purposes as soon as possible upon completion. For streambed stabilization or scour protection, riprap or other stabilization materials should not be placed in the active stream channel above the existing streambed elevation. This is to prevent obstructions to the movement of aquatic organisms upstream and downstream.

3) Riparian Habitat

We recommend a mitigation plan be developed (and submitted with the permit application) for any unavoidable habitat impacts that will occur. The DNR's Floodway Habitat Mitigation guidelines (and plant lists) can be found online at: <http://www.in.gov/legislative/iac/20140806-IR-312140295NRA.xml.pdf>.

In general, impacts to non-wetland forest of one (1) acre or more should be mitigated at a minimum 2:1 ratio. If less than one acre of non-wetland forest is removed in a rural setting, replacement should be at a 1:1 ratio based on area. Impacts to non-wetland forest under one (1) acre in an urban setting should be mitigated by planting five trees, at least 2 inches in diameter-at-breast height (dbh), for each tree which is removed that is 10" dbh or greater or by using the 1:1 replacement ratio based on area depending on the type of habitat impacted (individual canopy tree removal in an urban streetscape or park-like environment versus removal of habitat supporting a tree canopy, woody understory, and herbaceous layer). Impacts under 0.10 acres still involve the replacement of large diameter trees at 5:1 but typically do not require any additional mitigation or additional plantings beyond seeding and stabilizing disturbed areas,

State of Indiana
DEPARTMENT OF NATURAL RESOURCES
Division of Fish and Wildlife
Early Coordination/Environmental Assessment

though there are exceptions for high quality habitat sites.


The additional measures listed below should be implemented to avoid, minimize, or compensate for impacts to fish, wildlife, and botanical resources:

1. Revegetate all bare and disturbed areas that will not be mowed and maintained with a mixture of grasses, sedges, and wildflowers native to Central Indiana and specifically for stream bank/floodway stabilization purposes as soon as possible upon completion; turf-type grasses (including low-endophyte, friendly endophyte, and endophyte free tall fescue but excluding all other varieties of tall fescue) may be used in regularly mowed areas only.
2. Minimize and contain within the project limits inchannel disturbance and the clearing of trees and brush.
3. Do not work in the waterway from April 1 through June 30 without the prior written approval of the Division of Fish and Wildlife.
4. Do not cut any trees suitable for Indiana bat or Northern Long-eared bat roosting (greater than 3 inches dbh, living or dead, with loose hanging bark, or with cracks, crevices, or cavities) from April 1 through September 30.
5. Do not construct any temporary runarounds, access bridges, causeways, cofferdams, diversions, or pumparounds.
6. Operate equipment used to replace the bridge from the existing roadway.
7. Use minimum average 6 inch graded riprap stone extended below the normal water level to provide habitat for aquatic organisms in the voids.
8. Do not use broken concrete as riprap.
9. Underlay the riprap with a bedding layer of well graded aggregate or a geotextile to prevent piping of soil underneath the riprap.
10. Minimize the movement of resuspended bottom sediment from the immediate project area.
11. Do not deposit or allow demolition/construction materials or debris to fall or otherwise enter the waterway.
12. Appropriately designed measures for controlling erosion and sediment must be implemented to prevent sediment from entering the stream or leaving the construction site; maintain these measures until construction is complete and all disturbed areas are stabilized.
13. Seed and protect disturbed stream banks and slopes not protected by other methods that are 3:1 or steeper with erosion control blankets that are heavy-duty, biodegradable, and net free or that use loose-woven / Leno-woven netting to minimize the entrapment and snaring of small-bodied wildlife such as snakes and turtles (follow manufacturer's recommendations for selection and installation); seed and apply mulch on all other disturbed areas.

Contact Staff:

Christie L. Stanifer, Environ. Coordinator, Fish & Wildlife

Our agency appreciates this opportunity to be of service. Please contact the above staff member at (317) 232-4080 if we can be of further assistance.


Christie L. Stanifer
Environ. Coordinator
Division of Fish and Wildlife

Date: August 24, 2018

Perry, Leah

From: Perry, Leah
Sent: Thursday, August 27, 2020 4:12 PM
To: 'Ted Johnson'
Cc: Hope, Briana; Day, Dave; 'highwaydept@centurylink.net'; Paul Coutts
Subject: SR 75 over Wildcat Creek- Construction Timing/Detour Discussion

Mr. Johnson,

I just wanted to provide you with a short recap of our meeting today regarding the construction timing and detour of the SR 75 over Wildcat Creek bridge project (Des. 1601029).

In attendance was Ted Johnson (Carroll County Attorney), Paul Coutts (Carroll County Highway Department), Dave Day, Leah Perry, and Bri Hope (American Structurepoint).

Carroll County has two concerns about the project.

- The crossing is important for farm to market transportation during planting and harvest. It would be ideal to time the closure of the bridge outside of planting and harvest (winter or summer).
- There is a lack of other suitable crossings in the area. The only nearby bridge is Adams Mill, a covered bridge, that is not accessible to modern farm equipment and can't handle the increased traffic.

Mr. Coutts proposed a potential local detour route (using Xenia bridge, 400 S, 600 S, 350 W, and Prince William as one-way routes to avoid road damage).

Mr. Day stated that INDOT projects are required to use state roads for the official detour. Mr. Coutts had some concerns that locals will not use the INDOT detour.

We discussed that the current project schedule is a July 2022 letting with the construction window being August 2022 to November 2023. Mr. Day discussed that due to the amount of work that would merit a bridge closure, it seems possible to only close the bridge for 90 days and limit the closure to June-August. If this were the case, the contractor could do other work on the bridge during the construction window, but could only close the bridge during June, July, and August. American Structurepoint is willing to discuss this possibility with INDOT.

We are requesting that Carroll County provide us with a letter stating their concerns about the project and that they request the timing of the bridge closure to be limited to June-August. As discussed in the meeting, the letter from the county will be provided to American Structurepoint after the September 8th council meeting. We need this as soon as possible.

Thank you,

Leah Perry
Environmental Specialist
9025 River Road, Suite 200
Indianapolis, IN 46240
317.547.5580 [OFFICE](tel:317.547.5580)
structurepoint.com [WEB](http://structurepoint.com)



CARROLL COUNTY COMMISSIONERS

101 W Main St Suite 209
Delphi IN 46923
765-564-3172

William R Brown, President
Loren Hylton
Steve Pearson
Ted Johnson, County Attorney
Beth L Myers, County Auditor

September 8, 2020

Leah Perry
American Structurepoint, Inc.
9025 River Road, Suite 200
Indianapolis, Indiana 46240

Re: Des. 1601029
Bridge carrying SR 75 over Wildcat Creek
Carroll County, Indiana

Dear Mrs. Perry:


We are aware that the current schedule for this project is a July 2022 letting with the construction window being August 2022 to November 2023. We are writing to express our concerns about the timing of the closure of the bridge to traffic during construction.

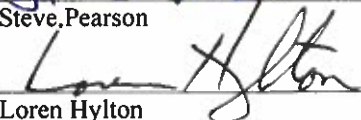
Located in a rural area dominated by grain producing farms, this bridge is important for the movement of farm equipment in planting season and for farm to market transportation of grain during the harvest. The nearest bridge is the Adams Mill Bridge, a covered bridge. There are no other bridges in the area capable of handling modern planting and tillage equipment or grain trucks.

We understand that the bridge must be closed to traffic for some but not all of the construction work. It would be ideal to time the closure of the bridge outside of planting and harvest, leaving summer and winter. We are requesting that the construction documents be drawn to limit work requiring closure of the bridge to the months of June, July and August.

BOARD OF COMMISSIONERS OF CARROLL COUNTY


William R. Brown, Chairman


Steve Pearson


Loren Hylton



Division of Nature Preserves
402 W. Washington St., Rm W267
Indianapolis, IN 46204-2739

March 2, 2021

Leah Perry
American Structurepoint, Inc.
9025 River Road, Suite 200
Indianapolis, IN 46240

Dear Leah Perry:

I am responding to your request for information on the threatened or endangered (T&E) species, high quality natural communities, and natural areas for the SR 75 over Wildcat Creek Bridge Improvements Project located in Carroll County, Indiana. The Indiana Natural Heritage Data Center has been checked and included you will find a datasheet with information on the T&E species documented within 0.5 mile of the project area.

If you need a review of the impacts to the animal species mentioned or a general environmental review, you can submit the project information to Christie Stanifer, DNR Environmental Coordinator, at environmentalreview@dnr.in.gov (preferred), or send to the street address below. For more help or guidance contact Christie Stanifer at cstanifer@dnr.in.gov.

Department of Natural Resources
Environmental Review
Division of Fish and Wildlife
402 W. Washington Street, Room W273
Indianapolis, IN 46204

The information I am providing does not preclude the requirement for further consultation with the U.S. Fish and Wildlife Service as required under Section 7 of the Endangered Species Act of 1973. If you have concerns about potential Endangered Species Act issues you should contact the Service at their Bloomington, Indiana office.

U.S. Fish and Wildlife Service
620 South Walker St.
Bloomington, Indiana 47403-2121
812-334-4261

Please note that the Indiana Natural Heritage Data Center relies on the observations of many individuals for our data. In most cases, the information is not the result of comprehensive field surveys conducted at

particular sites. Therefore, our statement that there are no documented significant natural features at a site should not be interpreted to mean that the site does not support special plants or animals.

Due to the dynamic nature and sensitivity of the data, this information should not be used for any project other than that for which it was originally intended. It may be necessary for you to request updated material from us in order to base your planning decisions on the most current information.

Thank you for contacting the Indiana Natural Heritage Data Center. You may reach me at (317)233-2558 if you have any questions or need additional information.

Sincerely,

A handwritten signature in cursive script that reads "Taylor Davis". The signature is written in black ink and is positioned above the printed name.

Taylor Davis
Indiana Natural Heritage Data Center

Enclosure: datasheet

March 2, 2021

INDIANA HERITAGE DATA WITHIN 0.5 MILE OF: SR 75 over Wildcat Creek Bridge Improvements, Carroll County

Sci. Name	Com. Name	State	Fed.	Date	Site	Comments
Mollusk						
<i>Lampsilis fasciola</i>	wavyrayed lampmussel	SSC		2013	NORTH FORK WILDCAT CREEK	2013: WEATHERED DEAD (FISHER)

Fed: E = Federal endangered; T = Federal threatened; C = Federal candidate species

State: SE = State endangered; ST= State threatened; SR = State rare; SSC = State species of special concern; SG = State significant; WL = watch list; no rank - not ranked but tracked to monitor status



INDIANA DEPARTMENT OF TRANSPORTATION

100 North Senate Avenue
Room N642
Indianapolis, Indiana 46204

PHONE: (317) 234-5168

Eric Holcomb, Governor
Joe McGuinness, Commissioner

February 28, 2020

This letter was sent to the listed parties.

RE: Dual Review Project: SR 75 over Wildcat Creek Bridge Project (Bridge No. 075-0-03653 B)
Des. No. 1601029 and DHPA No. 23247

Dear Consulting Party,

The Indiana Department of Transportation (INDOT), with funding from the Federal Highway Administration (FHWA), proposes to proceed with the SR 75 over Wildcat Creek Bridge project (Des. No. 1601029).

This letter is part of the Section 106 review process for this project. Section 106 of the National Historic Preservation Act requires federal agencies to take into account the effects of their undertakings on historic and archaeological properties. We are requesting comments from you regarding the possible effects of this project. Please use the above Des. Number and project description in your reply and your comments will be incorporated into the formal environmental study.

A Section 106 early coordination letter was distributed on November 19, 2018. In addition, the November 19, 2018 letter notified consulting parties that a historic property report and an archaeology report was available for review and comment. The December 5, 2019 letter notified consulting parties that the Historic Bridge Alternatives Analysis (HBAA) was available for review and comment.

The proposed undertaking is on State Road (SR) 75 over Wildcat Creek Bridge (INDOT Bridge No. 075-08-03653B) from approximately 0.08 mile north and south of the bridge approaches in Carroll County. It is within Democrat Township, Rossville 7.5 Minute USGS Topographic Quadrangle in Section 33, Township 24 North, Range 1 West.

The current bridge over Wildcat Creek [INDOT Bridge No. 075-08-03653 B; Historic Bridge (HB)-1897; Indiana Historic Sites and Structures Inventory (IHSSI) No.: 015-553-65014; National Bridge Inventory (NBI) No.: 24970] is a two-span bridge consisting of a thru truss and a steel girder approach span. The bridge was constructed in 1947 and reconstructed most recently in 2001. The bridge was determined eligible for listing in the National Register of Historic Places (NRHP) in the Indiana Historic Bridge Inventory and was designated as "Non-Select."

The need for the proposed project is a result of the insufficient legal load rating, deteriorated condition of the structural steel, superstructure, and wearing surface and the inadequate usable shoulder width on the bridge. The purpose of the proposed project is to improve the inventory load rating of the bridge to 36 tons or more, improve the superstructure rating, and improve the wearing surface rating.

www.in.gov/dot/

An Equal Opportunity Employer



American Structurepoint, Inc. is under contract with INDOT to advance the environmental documentation for the referenced project. Weintraut and Associates, Inc. has been subcontracted to complete the Section 106 documentation for the project.

In accordance with 36 CFR 800.2 (c), you were invited to become a consulting party as part of the Section 106 process, or you are hereby invited to become a consulting party as part of the Section 106 process. Entities that have previously accepted consulting party status--as well as additional entities that are currently being invited to become consulting parties--are identified in the attached list.

The Section 106 process involves efforts to identify historic properties potentially affected by the undertaking, to assess the undertaking's effects and to seek ways to avoid, minimize, or mitigate any adverse effects on historic properties. For more information regarding the protection of historic resources, please see the Advisory Council on Historic Preservation's guide: *Protecting Historic Properties: A Citizen's Guide to Section 106 Review* available online at <https://www.achp.gov/sites/default/files/documents/2017-01/CitizenGuide.pdf>.

Per the terms of the "Programmatic Agreement Regarding Management and Preservation of Indiana's Historic Bridges" (Historic Bridges PA), the Federal Highway Administration—Indiana Division (FHWA) will satisfy its Section 106 responsibilities involving "Select" and "Non-Select" bridges through the Project Development Process (PDP) of the Historic Bridges PA (Stipulation III). Because Bridge No. 075-08-03653 B is a "Non-Select" bridge, the procedures outlined in Stipulation III.B. of the Historic Bridges PA will be followed to fulfill FHWA's Section 106 responsibilities for the bridge. (A copy of the Historic Bridges PA can be downloaded here: <http://www.in.gov/indot/2530.htm>).

Please note that, per the permanent rule issued by the Indiana Department of Natural Resources effective August 14, 2013 (312 IAC 20-4-11.5), INDOT is requesting that this project be subjected to "dual review"; that is, reviewed by the Division of Historic Preservation and Archaeology simultaneously under 54 U.S.C. 306108 (Section 106) and IC 14-21-1-18 (Indiana Preservation and Archaeology Law dealing with alterations of historic sites and structures requiring a Certificate of Approval). Pursuant to Section 11.5(f) of this rule, at the conclusion of the review process we anticipate that the Division Director would issue a letter of clearance exempting this project from obtaining a Certificate of Approval under IC 14-21-1-18.

The Area of Potential Effects (APE) is the area in which the proposed project may cause alterations in the character or use of historic resources. The APE contains no resources listed in the National Register of Historic Places (NRHP).

A historian who meets the Secretary of the Interior's Professional Qualification Standards identified and evaluated above-ground resources within the APE for potential eligibility for the NRHP. As a result of the historic property identification and evaluation efforts, the SR 75 Bridge over Wildcat Creek (IHSSI No.: 015-553-65014) is recommended as eligible for listing in the NHRP as it retains the characteristics for which it was previously determined eligible for listing in the NRHP.

With regard to archaeological resources, an archaeologist who meets the Secretary of the Interior's Professional Qualification Standards identified no sites within the project area. As a result of these efforts, no sites were recommended as eligible for listing in the NRHP and no further work or further work is recommended.

INDOT, on behalf of FHWA has signed a determination of “No Historic Properties Affected” for this Section 106 undertaking. The finding for this project only applies to other resources located within the APE and not to the SR 75 over Wildcat Creek Bridge, since FHWA will follow Stipulation III.B. of the Historic Bridges PA to fulfill its Section 106 responsibilities. In accordance with 36 CFR 800.4(d), you and the other consulting parties that responded to the early coordination letter are being provided the documentation for this finding. You can view the determination of “No Historic Properties Affected” electronically by accessing INDOT’s Section 106 document posting website IN SCOPE at <http://erms.indot.in.gov/Section106Documents/> (the Des. No. is the most efficient search term, once in IN SCOPE). You are invited to review these documents and to respond with comments on any historic resource impacts incurred as a result of this project so that an environmental report can be completed. We also welcome your related opinions and other input to be considered in the preparation of the environmental document. If you prefer a hard-copy of this material, please respond to this email with your request within seven (7) days.

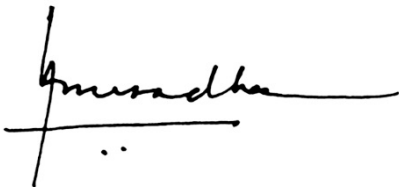
Please review the information and comment within thirty (30) calendar days of receipt. If you indicate that you do not desire to be a consulting party or if you have not previously accepted consulting party status and you do not respond to this letter, you will not be included on the list of consulting parties for this project and will not receive further information about the project unless the design changes.

For questions concerning specific project details, you may contact Leah Perry of American Structurepoint, Inc. at (317) 547-5580 or lperry@structurepoint.com. All future responses regarding the proposed project should be forwarded to American Structurepoint, Inc. at the following address:

Leah Perry
Environmental Specialist
American Structurepoint, Inc.
9025 River Road, Suite 200
Indianapolis, IN 46240
lperry@structurepoint.com

Tribal contacts may contact Shaun Miller at smiller@indot.in.gov or 317-233-6795 or Michelle Allen at FHWA at michelle.allen@dot.gov or 317-226-7344.

Sincerely,

A handwritten signature in black ink, appearing to read "Anuradha", written over a horizontal line.

Anuradha V. Kumar, Manager
Cultural Resources Office
Environmental Services

Distribution List:

- Chad Slider, Indiana Department of Natural Resources/Division of Historic Preservation & Archaeology (IDNR/DHPA), Indiana State Historic Preservation Officer (SHPO), cslider@dnr.in.gov
- William Brown, Carroll County Commissioners, commissioner3@carrollcountyin.gov, abjakes@yahoo.com
- John Brown, Carroll County Commissioners, commissioner1@carrollcountyin.gov
- Steve Pearson, Carroll County Commissioners, commissioner2@carrollcountyin.gov
- Emily Royer, Indiana Landmarks, Western Regional Office, eroyer@indianalandmarks.org
- Randy Myers, Carroll County Historical Museum, carrollcountyhistoricalsociety@ffni.com
- Mark Smith, Carroll County Historian, markalansmith12171948@gmail.com
- Paul Brandenburg, Historic Spans Task Force, indianabridges@sbcglobal.net
- Bonnie Maxwell, Heartland Heritage, Inc., bmaxwell62@gmail.com
- Mr. Joe Mayfield, Delphi Preservation Society, joe.Mayfield5@gmail.com
- Gus Nyberg, NICHES Land Trust, gnyberg@nicheslandtrust.org
- Dan McCain, Carroll County Wabash & Erie Canal Inc., dan.mccain@gmail.com
- Al Auffert, Adams Mill Inc., amauffart@gmail.com
- Julia Leahy, Carroll County Chamber of Commerce, julia@carrollcountychamber.com
- Mary Blue, Promoting Wildcat Valley, maryloublueeyes@gmail.com
- Dale Brier, Division of Outdoor Recreation, Indiana Department of Natural Resources, dbrier@dnr.in.gov
- Rick Parsons, Wildcat Guardians, rpars17542@aol.com
- Nathan Holth, HistoricBridges.org, nathan@hstoricbridges.org
- Forest County Potawatomi Community
- Miami Tribe of Oklahoma

**FEDERAL HIGHWAY ADMINISTRATION'S
SECTION 4(F) COMPLIANCE REQUIREMENTS (for historic properties) AND
SECTION 106 FINDINGS AND DETERMINATIONS
AREA OF POTENTIAL EFFECTS
ELIGIBILITY DETERMINATIONS
EFFECT FINDING
STATE ROAD 75 OVER WILDCAT CREEK BRIDGE PROJECT
DEMOCRAT TOWNSHIP, CARROLL COUNTY, INDIANA
DES. NO.: 1601029**

AREA OF POTENTIAL EFFECTS

(Pursuant to 36 CFR Section 800.4(a)(1))

Pursuant to 36 CFR 800.16(d), the Area of Potential Effects (APE) was drawn as an approximately 0.25-mile (1,320 foot) buffer from the proposed project location. The APE for archaeology is the project footprint. (See Appendix A: Maps.)

ELIGIBILITY DETERMINATIONS

(Pursuant to 36 CFR 800.4(c)(2))

There is one resource eligible for listing in the National Register of Historic Places (NRHP):

State Road (SR) 75 over Wildcat Creek Bridge (HB-1897; IHSSI No.: 015-553-65014; NBI No.: 24970; Bridge No. 075-08-03653B) is a two-span bridge consisting of a thru truss and a steel girder approach span. The bridge was constructed in 1947 and reconstructed most recently in 2001. The bridge was determined eligible for listing in the NRHP in the *Indiana Historic Bridge Inventory* under Criterion C as it "represents an early or distinctive phase in bridge construction, design, or engineering" and because it "represents important contributions made by an accomplished Indiana engineer, designer, fabricator or builder and displays distinctive engineering and/or aesthetic characteristics." The period of significance is 1947, the date of construction. The structure is designated as a "Non-Select Bridge" in the *Inventory*.

EFFECT FINDING

Per the terms of the "Programmatic Agreement Regarding Management and Preservation of Indiana's Historic Bridges" (Historic Bridges PA), the Federal Highway Administration-Indiana Division (FHWA) will satisfy its Section 106 responsibilities involving "Select" and "Non-Select" bridges through the Project Development Process (PDP) of the Historic Bridges PA (Stipulation III). The **SR 75 over Wildcat Creek Bridge (HB-1897; IHSSI No.: 015-553-65014; NBI No.: 24970; Bridge No. 075-08-03653B)** has been classified as a "Non-Select" bridge by the Indiana Department of Transportation (INDOT) *Historic Bridge Inventory*, and thus, the procedures outlined in Stipulation III.B of the Historic Bridges PA will be followed to fulfill FHWA's Section 106 responsibilities for the bridge.

Therefore, the finding for this project only applies to other resources located within the APE and not to the SR 75 over Wildcat Creek Bridge (HB-1897; IHSSI No.: 015-553-65014; NBI No.: 24970; Bridge No. 075-08-03653B). This document will satisfy the Section 106 responsibilities for other resources located within the APE. Regarding other resources located in the project area, there are no historic properties located within the APE. INDOT, acting on FHWA's behalf, has determined a "No Historic Properties Affected" finding is appropriate for this undertaking.

INDOT respectfully requests the Indiana State Historic Preservation Officer provide written concurrence with the Section 106 determination of effect.

SECTION 4(F) COMPLIANCE REQUIREMENTS (for historic properties)

SR 75 over Wildcat Creek Bridge - This resource is used for transportation purposes and no Section 4(f) conversion will take place with this project; therefore no Section 4(f) evaluation must be completed for the SR 75 over Wildcat Creek Bridge.

Anuradha Kumar V.

Anuradha Kumar, for FHWA
Manager, INDOT Cultural Resources

02/28/2020

Approved Date

**FEDERAL HIGHWAY ADMINISTRATION'S
DOCUMENTATION OF SECTION 106 FINDING OF
NO HISTORIC PROPERTIES AFFECTED
SUBMITTED TO THE STATE HISTORIC PRESERVATION OFFICER
PURSUANT TO 36 CFR Section 800.4(d)(1)
STATE ROAD 75 OVER WILDCAT CREEK BRIDGE PROJECT
DEMOCRAT TOWNSHIP, CARROLL COUNTY, INDIANA
DES. NO.: 1601029**

1. DESCRIPTION OF THE UNDERTAKING

The Indiana Department of Transportation (INDOT), with funding from the Federal Highway Administration (FHWA), proposes to proceed with the State Road (SR) 75 over Wildcat Creek Bridge (Bridge No. 075-08-03653B, Indiana Historic Sites and Structures Inventory [IHSSI] No.: 015-553-65014; Historic Bridge [HB] 1897; National Bridge Inventory [NBI] No.: 24970) in Democrat Township, Carroll County, Indiana. More specifically, the project is located near Cutler, Indiana, in Section 33, Township 24 North, Range 1 West. (See Appendix A: Maps.)

The existing structure is a two-span bridge consisting of a thru truss and a steel girder approach span. The bridge was constructed in 1947 and reconstructed most recently in 2001. The bridge was determined eligible for listing in the National Register of Historic Places (NRHP) in the *Indiana Historic Bridge Inventory*. The *Inventory* also designated the structure as a “Non-Select” Bridge.

The need for the proposed project is a result of the insufficient legal load rating, deteriorated condition of the structural steel, superstructure, and wearing surface and the inadequate usable shoulder width on the bridge. The purpose of the proposed project is to improve the inventory load rating of the bridge to 36 tons or more, improve the superstructure rating, and improve the wearing surface rating.

Rehabilitation of the bridge for continued vehicular use meeting the Secretary of Interior's Standards is the preferred alternative. This alternative would remove and replace the existing bridge deck and concrete railing; install shear studs on top flange of existing beams; heat-straighten the north floor beam and replace the connecting gusset plate; repair the structural steel with patching or replacement with in-kind members; replace inadequate rivets with high strength bolts; paint the structural steel; semi-integral bent conversion at Bent 1; incidental approach tie-in.

The Area of Potential Effects (APE) is “the geographic area or areas within which an undertaking may directly or indirectly cause alterations in the character or use of historic properties, if any such properties exist. The area of potential effects is influenced by the scale and nature of an undertaking and may be different for different kinds of effects caused by the undertaking.” [36 CFR § 800.16(d)]

The APE for aboveground resources was drawn to encompass potential impacts—including visual, increased noise, and traffic-related impacts—that may result from the undertaking. The APE was generally drawn to include properties within a 0.25 mile (1,320 foot) buffer. The APE was narrowed slightly to the southeast due to topography and intervening structures. The APE for archaeology is the project footprint. (See Appendix A: Maps.)

2. EFFORTS TO IDENTIFY HISTORIC PROPERTIES

Pursuant to 36 CFR § 800.4(b), historians for Weintraut & Associates (W&A) reviewed the NRHP, Indiana Register of Historic Sites and Structures Inventory (IHSSI), the State Historical Architectural and Archaeological Research Database (SHAARD), the Indiana Historic Buildings, Bridges, and Cemeteries Map (IHBBM), the *Indiana Historic Bridge Inventory*, and the *Carroll County Interim Report* (2011), for previously identified properties. In conducting research, historians examined primary and secondary resources. Documentary research for the project included a review of county histories, aerial photographs, GIS records, and online resources.

A field reconnaissance of the aboveground APE was conducted on November 8, 2017. W&A historians recorded survey notes and took photographs of properties more than fifty years of age within the APE

and photographed representative landscapes within the APE. Historians evaluated individual properties for architectural and contextual integrity and historical significance. (See Appendix B: Photographs.)

Historians prepared a Historic Property Short Report (HPSR) (Fivecoat October 2018). The HPSR identified one resource, the SR 75 Bridge over Wildcat Creek (Bridge No. 075-08-03653B, IHSSI No.: 015-553-65014; HB 1897; NBI No.: 24970) that had been previously determined eligible for listing in the NRHP in the *Indiana Historic Bridge Inventory*. Historians did not recommend any additional resources for listing in the NRHP. (See Appendix C: Reports.)

Pursuant to 36 CFR § 800.4(b), W&A initiated identification and evaluation of archaeological resources with a Phase Ia records check on February 23, 2018. Based upon INDOT procedures, a field reconnaissance for archaeology was conducted on March 8, 2018. An Archaeology Short Report (ASR) was prepared (Arnold, September 2018). No archaeological resources were identified in the project area, and the project was recommended to proceed as planned. (See Appendix C: Reports.)

A Section 106 Early Coordination Letter (ECL), sent via email and dated November 18, 2018, described the project, provided instructions for accessing the HPSR and ASR (Tribes Only) via INSCOPE, and invited the following consulting parties to join consultation: Indiana State Historic Preservation Officer (SHPO); Carroll County Commissioners; Tippecanoe County Area Plan; Indiana Landmarks—Western Regional Office; Carroll County Historical Museum; Carroll County Historian; Carroll County Highway Department Superintendent/Engineer; Dr. James Cooper; Historic SPANS Task Force; Eastern Shawnee Tribe of Oklahoma; Forest County Potawatomi Community; Miami Tribe of Oklahoma; Peoria Tribe of Indians of Oklahoma; and Pokagon Band of Potawatomi Indians. The SHPO, a designated consulting party, was sent a paper copy of the HPSR and ASR. (See Appendix D: Consulting Parties and Appendix E: Correspondence.)

On November 20, 2018, Indiana Landmarks—Western Regional Office accepted the invitation to join in consultation and noted it expected that Statewide Historic Bridge Inventory and Select/Non-Select designations would be reevaluated by INDOT as the Historic Bridge Programmatic Agreement (HBPA) neared its 10-year review cycle. Landmarks stated “A number of Select bridges have been lost over the past decade to demolition or other non-preservation activity. Considering these losses, the 10-year review period, and the ‘Outstanding’ rating for the [IHSSI], Landmarks recommends that the bridge over Wildcat Creek. . .be preserved in-place for continued vehicular use on SR 75. Landmarks also intends to push for a categorical change of this bridge’s status from Non-Select to Select.” (See Appendix E: Correspondence.)

INDOT responded to Landmarks on November 21, 2018, and noted an alternatives analysis was being prepared pursuant to the HBPA and would be distributed for review and comment. Regarding the HBPA, INDOT, FHWA, and SHPO met several times in 2016 regarding the reevaluation of historic bridges per Stipulation II.C.2 and decided that a full review was not necessary at that time. INDOT created an updated list of the status of Select/Non-Select bridges which was made available on INDOT’s website in December 2016. INDOT also provided instructions for accessing the list. (See Appendix E: Correspondence.)

Heartland Heritage, Inc. joined consultation via an email request on December 6, 2018. That email outlined the organization’s interest in preserving structures in the Carroll County region. (See Appendix E: Correspondence.)

On December 10, 2018, at INDOT’s request, the Carroll County Historic Bridge Coalition was sent an invitation to join in consultation (email and via United States Postal Service). No response was received. (See Appendix E: Correspondence.)

On December 13, 2018, the Miami Tribe of Oklahoma accepted the invitation to join in consultation and offered “no objection to the...project at this time.” The Tribe noted that the site is within the aboriginal homelands of the Miami Tribe and requested to be contacted immediately by the entity of jurisdiction “if any human remains or Native American cultural items falling under the Native American Graves

Protection and Repatriation Act (NAGPRA) or archaeological evidence is discovered during any phase of this project.” (See Appendix E: Correspondence.)

SHPO responded to the HPR, ASR, and ECL in a letter dated December 18, 2019. SHPO concurred with the list of consulting parties and agreed that the APE designated in the HPSR appeared appropriate. The staff of the SHPO noted that the SR 75 Bridge over Wildcat Creek has been identified as part of the *Indiana Historic Bridge Inventory* as a Non-Select Parker through truss bridge that is eligible for inclusion in the NRHP under Criterion C. Also, the staff concurred with the findings of the HPSR that the project bridge is “the only historic property listed in or eligible for listing in the NRHP identified within the APE.” (See Appendix E: Correspondence.)

Additionally, the staff stated that based upon the available documentation it had “not identified any currently known archaeological resources listed in or eligible for inclusion in the NRHP within the proposed project area...” and concurred with the opinion of the archaeologist that “no further archaeological investigations appear necessary at this proposed project area.” The SHPO, however, cautioned that “if any prehistoric or historic archaeological artifacts or human remains are uncovered during construction, demolition, or earthmoving activities, state law (Indiana Code 14-21-1-27 and -29) requires that the discovery must be reported to the Department of Natural Resources within two (2) business days.” (See Appendix E: Correspondence.)

On December 20, 2018, the Forest County Potawatomi Community responded to the ECL and ASR and offered a finding of “No Historic Properties Affected” for this project. The Forest County Potawatomi asked that if “any archaeological artifacts or human remains are uncovered during construction, demolition, or earthmoving activities,” state law (Indiana Code 12-21-1-27 and 29) be followed, work in the immediate vicinity stop, and the Forest County Potawatomi be contacted for “consultation regarding treatment and handling protocols prior to removal.” (See Appendix E: Correspondence.)

Indiana Historic SPANS Task Force joined consultation on March 31, 2019. (See Appendix E: Correspondence.)

On April 6, 2019, the Delphi Preservation Society joined consultation via an email request. (See Appendix E: Correspondence.)

On April 7, 2019, the Carroll County Historian sent an email agreeing to join consultation for the project. (See Appendix E: Correspondence.)

On April 9, 2019, NICHES Land Trust joined consultation via an email request. (See Appendix E: Correspondence.)

On April 13, 2019, the Carroll County Wabash & Erie Canal, Inc. joined consultation via an email request. The group noted it has experience in acquiring and restoring historic bridges and placing them in their Canal Park. The group sent a follow up email on April 14, 2019, that provided information about the group’s work restoring other bridges. (See Appendix E: Correspondence.)

On April 15, 2019, the Carroll County Historical Society agreed to join consultation. (See Appendix E: Correspondence.)

On April 17, 2019, Adams Mill, Inc., a not-for-profit organization that operated a historic grist mill upstream from the bridge on Wildcat Creek, joined consultation via an email request. (See Appendix E: Correspondence.)

On April 18, 2019, the Carroll County Chamber of Commerce joined consultation via an email request. (See Appendix E: Correspondence.)

On April 19, 2019, the Promoting Wildcat Valley group joined consultation via an email request. The email also included information about the group’s accomplishments throughout the Wildcat River Valley and

offered their purpose statement from its bylaws, which included “supporting the history and historic structures of Carroll County’s Wildcat Valley.” (See Appendix E: Correspondence.)

The Indiana Department of Natural Resources (IDNR), Division of Outdoor Recreation joined consultation via email request on April 23, 2019. (See Appendix E: Correspondence.)

The Wildcat Guardians joined consultation via email request on May 14, 2019. (See Appendix E: Correspondence.)

Carroll County Commissioner William Brown spoke with the INDOT historic bridge specialist on September 6, 2019. Commissioner Brown is in favor of keeping the bridge for continued vehicular use. (See Appendix E: Correspondence.)

On October 3, 2019, HistoricBridges.org was added to list of the consulting parties following updates to INDOT’s consultation contact list for projects involving historic bridges. (See Appendix E: Correspondence.)

On October 21, 2019, the Carroll County Commissioners signed a resolution urging INDOT to “maintain and not replace” the State Road 75 over Wildcat Creek Bridge. (See Appendix E: Correspondence.)

A Historic Bridge Alternatives Analysis (HBAA) was transmitted to consulting parties on December 5, 2019. That document recommended rehabilitation for continued vehicular use as the preferred alternative (“B-1 Rehabilitation for Continued Vehicular Use Meeting Secretary of the Interior’s Standards”). (Appendix F: Historic Bridge Alternatives Analysis).

The Indiana SHPO responded to the HBAA on January 6, 2020, and agreed that B-1 Rehabilitation for Continued Vehicular Use Meeting Secretary of the Interior’s Standards “is the feasible and prudent and should be pursued for the proposed project.” SHPO stated it looked forward to receiving the 30 percent design plans for review and comment. SHPO also stated “photographic documentation of the bridge and its immediate surroundings should be produced before site preparation or construction work begins.” SHPO also added that “we may have other specific recommendations for the photographic documentation” once more information is provided about the project in the design plans. (See Appendix E: Correspondence.)

No other efforts were undertaken to identify and evaluate historic properties, and no other comments were received.

Design plans will be submitted to consulting parties for review and comment at 30 percent, 60 percent, and final. In addition, photo documentation will be prepared prior to construction as requested by the SHPO in their letter (January 6, 2020).

A public notice of “No Historic Properties Affected” will be posted in a local newspaper and the public will be afforded thirty (30) days to respond. If appropriate, this document will be revised after the expiration of the public comment period

3. BASIS FOR FINDING

A recommendation of “No Historic Properties Affected” is appropriate for this undertaking because there are no resources listed in, or eligible for listing in, the NRHP within the APE other than the SR 75 over Wildcat Creek Bridge (IHSSI No.:171-681-35023; HB 1739; NBI No.: 8600029).

Per Stipulation III of the Historic Bridges PA, the project sponsor will hold a public hearing for the project prior to completion of National Environmental Policy Act (NEPA) studies and all consulting parties will be notified of the public hearing.

Per the terms of the Historic Bridges PA, FHWA will satisfy its Section 106 responsibilities involving “Select” and “non-Select” bridges through the PDP of the Historic Bridge PA (Stipulation III) and, therefore, the finding for this project only applies to other resources located within the APE and not to the State Road 75 over Wildcat Creek Bridge (HB-1897; IHSSI No.: 015-553-65014; NBI No.: 24970; Bridge No. 075-08-03653B).

Appendices

Appendix A: Maps

Appendix B: Photographs

Appendix C: Report Summaries

Appendix D: Consulting Parties

Appendix E: Correspondence

Appendix F: Historic Bridge Alternatives Analysis

Note: These
Appendices are a
part of the 800.11

APPENDIX A. Maps

Note: This is Appendix A
of the 800.11 document.

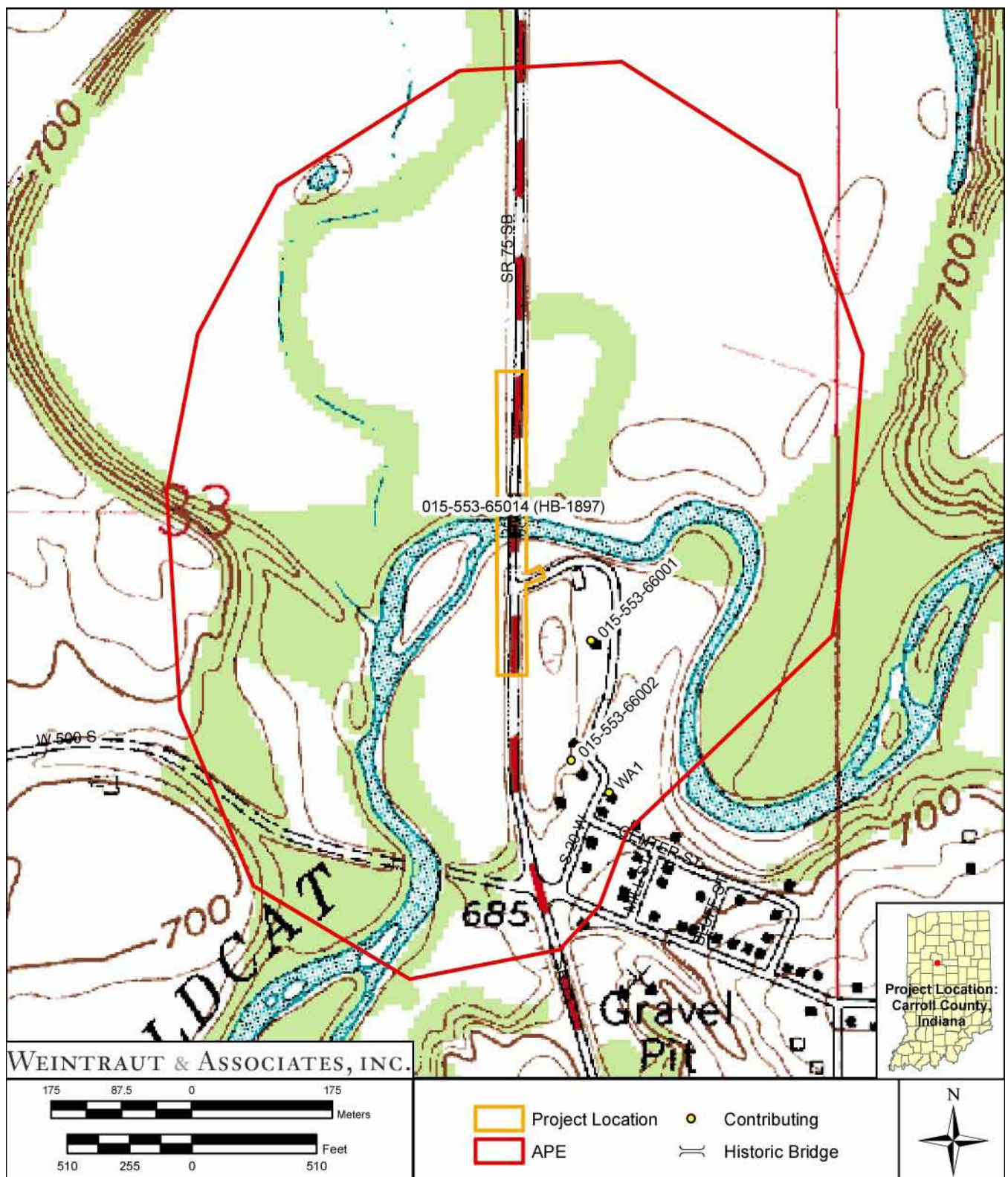


FIGURE 1. PROJECT LOCATION, APE, AND CONTRIBUTING PROPERTIES AS SHOWN ON PORTION OF THE USGS ROSSVILLE, INDIANA QUADRANGLE MAP.

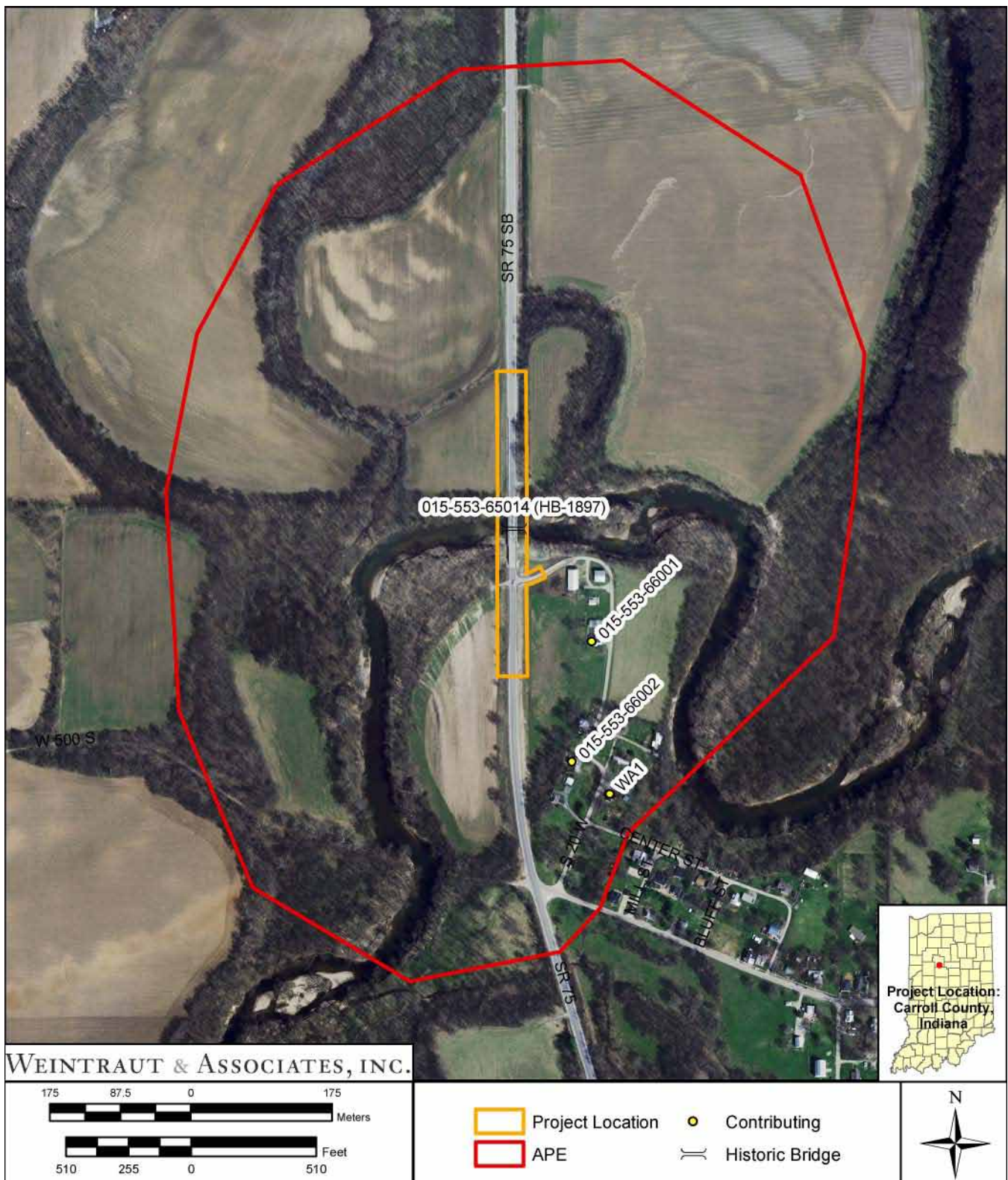
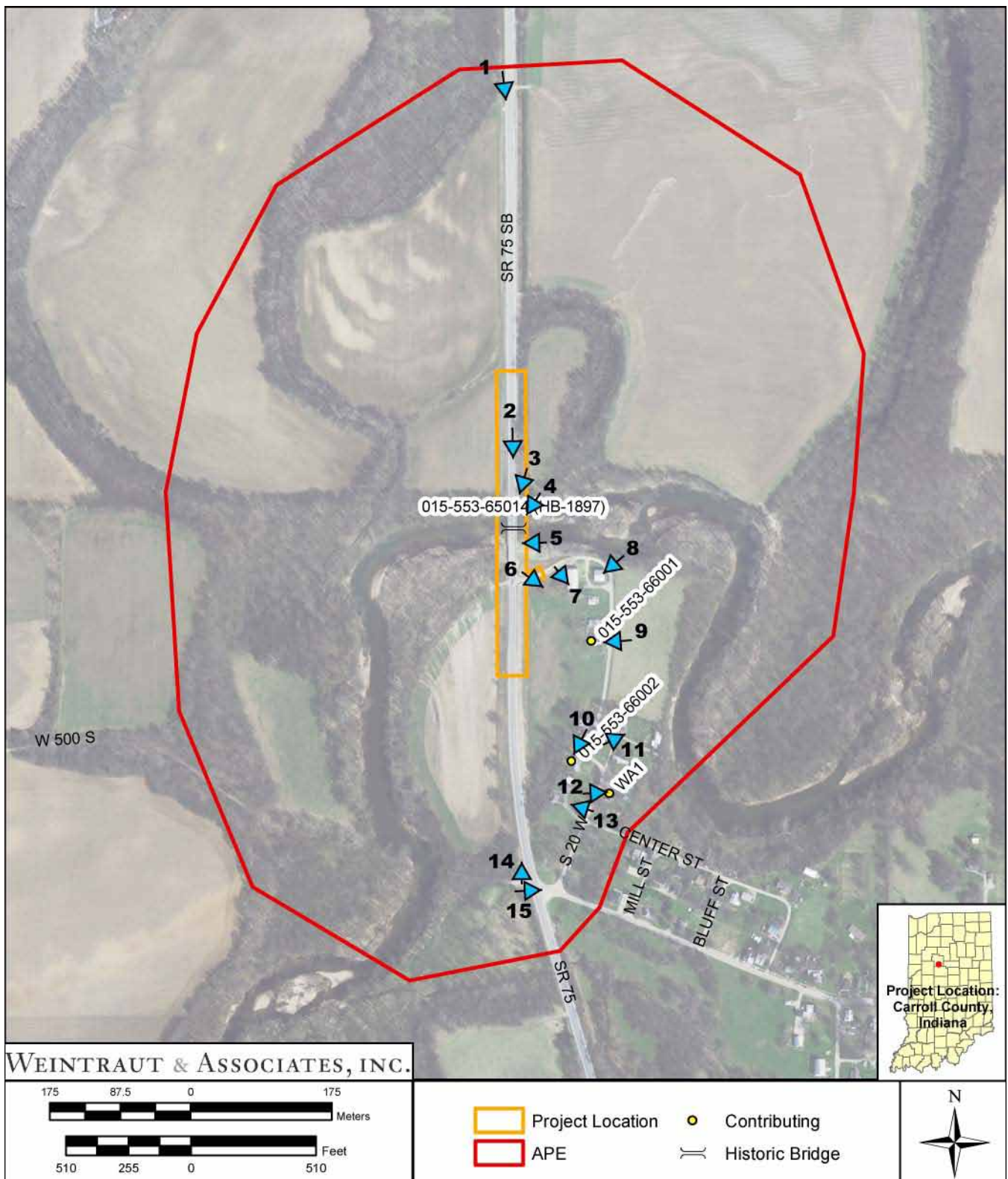


FIGURE 2. PROJECT LOCATION, APE, AND CONTRIBUTING PROPERTIES AS SHOWN ON AN AERIAL PHOTOGRAPH (2005).



THE PROJECT LOCATION, APE, PROJECT BRIDGE, AND CONTRIBUTING PROPERTIES SHOWN ON AN AERIAL PHOTOGRAPH (2005) WITH PHOTO LOCATIONS.

APPENDIX B. Photographs

Note: This is Appendix B
of the 800.11 document.



I. View from the north end of the APE, looking south.



2. The north elevation of the Parker through truss bridge (NBI No.: 24970).



3. The project bridge has a concrete deck and has been recently painted (2007).



4. Looking south along the eastern elevation of the bridge.



5. The east elevation of the pier connecting the bridge with its approach span.



6. The farm at 4758 S CR 20 W (IHSSI No.: 015-553-66001) sits adjacent to the project bridge.



7. A modern metal barn on the farm at 4758 S CR 20 W (IHSSI No.: 015-553-66001).



8. A wooden drive through corn crib on the farm at 4758 S CR 20 W (IHSSI No.: 015-553-66001).



9. The east elevation of the Contributing-rated farm house at 4758 S CR 20 W (IHSSI No.: 015-553-66001).



10. A Non-contributing metal sided garage located at 4769 S CR 20 W.



11. An altered Contributing-rated house at 4802 S CR 20 W.



12. A Contributing-rated house (WAI) located at 4787 S CR 20 W.



13. The Non-contributing house at 4802 S CR 20 W, looking west.



14. Looking north to undertaking from SR 75 and CR 485 S.



15. Looking east from intersection of SR 75 and CR 485 S.

APPENDIX C. Report Summaries

Note: This is Appendix C
of the 800.11 document.



**Phase Ia Archaeological Records Check and Field Reconnaissance:
State Road 75 over Wildcat Creek Bridge Project: Scope Undeter-
mined in Democrat Township, Carroll County, Indiana
Des. No.: 1601029**

Prepared for
**American Structurepoint &
Federal Highway Administration/Indiana Department of Transportation**

Prepared by
WEINTRAUT & ASSOCIATES, INC.

Principal Investigator: Craig R. Arnold

Author: Colin D. Graham

P.O. Box 5034 | Zionsville, Indiana | (317)733-9770 | (linda@weintrautinc.com)

September 2018



INDIANA ARCHAEOLOGICAL SHORT REPORT

State Form 54566 (1-11)

INDIANA DEPARTMENT OF NATURAL RESOURCES
DIVISION OF HISTORIC PRESERVATION
AND ARCHAEOLOGY

402 West Washington Street, Room W274
Indianapolis, Indiana 46204-2739
Telephone Number: (317) 232-1646
Fax Number: (317) 232-0693
E-mail: dhpa@dnr.IN.gov

Where applicable, the use of this form is recommended but not required by the Division of Historic Preservation and Archaeology.

Author: Colin D. Graham, B.A.

Date (month, day, year):

September 25, 2018

Project Title:

Phase Ia Archaeological Records Check and Field Reconnaissance: State Road 75 over Wildcat Creek Bridge Project: Scope Undetermined in Democrat Township, Carroll County, Indiana (Des. No.:1601029).

PROJECT OVERVIEW

Project Description:

The Indiana Department of Transportation (INDOT) with funding from the Federal Highway Administration (FHWA) intends to proceed with a bridge improvement project along State Road (SR) 75 over Wildcat Creek in Democrat Township, Carroll County, Indiana. More specifically, the project is located northwest of Cutler, Indiana, in Section 33, Township 24 North, Range 1 West, on the United States Geological Survey (USGS) 7.5-minute series Rossville, Indiana, topographic quadrangle map (Figure 1).

The project is located at the State Road (SR) 75 over Wildcat Creek bridge in Carroll County. The current bridge over Wildcat Creek [INDOT Bridge No. 075-08-03653 B; Historic Bridge (HB)-1897; Indiana Historic Sites and Structures Inventory (IHSSI) No.: 015-553-65014; National Bridge Inventory (NBI) No.: 24970] is a two-span bridge consisting of a through truss with cast-in-place deck span and a steel girder span. The bridge was constructed in 1947 and reconstructed most recently in 2001. The bridge was determined eligible for listing in the National Register of Historic Places (NRHP) in the Indiana Historic Bridge Inventory and was designated as "Non-Select," meaning it is not a good candidate for preservation in place. The existing right-of-way throughout the project corridor is approximately 40 feet (ft) on each side of the centerline of the bridge. Section 106 of the National Historic Preservation Act requires Federal agencies to take into account the effects of their undertakings on historic properties. The federal involvement is funding from the FHWA.

Existing clear roadway width is 28 ft with two lanes. Proposed clear roadway width for reconstruction would be 30 ft with two, 12-ft lanes and two, 3-ft shoulders. Proposed clear roadway width for bridge replacement would be a maximum of 46 ft with two 12-ft lanes and two 11-ft shoulders. Deficiencies of the existing bridge include spalling and cracking on the approach slabs, deck, wearing surface, and substructure. Additionally, holes are present in the wearing surface and superstructure. The superstructure also exhibits thinning of splice and gusset plates along bottom chord and rivet corrosion along lower chords. The load rating and existing shoulder width of

the bridge are insufficient and do not meet current safety standards.

An alternatives analysis will be completed to determine the scope of work for the proposed project. Since the bridge is listed as a Non-Select Bridge, rehabilitation of the existing bridge will occur if determined feasible and prudent. Additional options, including a one-way pair and replacement, will be considered in case rehabilitation is determined unfeasible or imprudent. Right-of-way requirements are not yet known for this project. No relocations are anticipated as part of this project. For each of the alternatives, the preferred method of traffic maintenance for this project would include closure of SR 75 over Wildcat Creek. Detour routes would be set up for through traffic, and access would be maintained for local traffic at all times. A full Maintenance of Traffic (MOT) plan will be developed once the alternative analysis has been completed and design further develops.

INDOT Designation Number/ Contract Number:

1601029

Project Number:

DHPA Number:

Approved DHPA Plan Number:

Prepared For:

American Structurepoint

Contact Person:

Briana Hope

Address:

7260 Shadeland Station

City:

Indianapolis

State:

IN

ZIP Code:

46256

Telephone Number:

317.547.5580

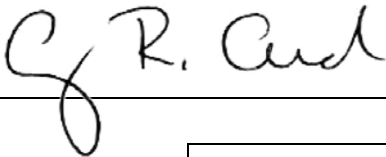
E-mail Address:

bhope@structurepoint.com

Principal Investigator:

Craig R. Arnold, M.A.

Signature:



Company/Institution:

Weintraut & Associates, Inc. (W&A)

Address:

PO BOX 5034

Comments:

A typical soil profile from the shovel test probes consisted of a very dark grayish-brown (10YR3/2) sandy loam that extended to a depth of 18 cm (7 in). Below this, a subsoil of dark yellowish-brown (10YR3/4) sand and gravel was encountered. No precontact or historic cultural materials or deposits were identified during the pedestrian survey or from the shovel probes.

RECOMMENDATION

- ☐ The archaeological records check has determined that the project area has the potential to contain archaeological resources and a Phase Ia archaeological reconnaissance is recommended.
- ☐ The archaeological records check has determined that the project area does not have the potential to contain archaeological resources and no further work is recommended before the project is allowed to proceed.
- ☒ The Phase Ia archaeological reconnaissance has located no archaeological sites within the project area and it is recommended that the project be allowed to proceed as planned.
- ☐ The Phase Ia archaeological reconnaissance has determined that the project area includes landforms which have the potential to contain buried archaeological deposits. It is recommended that Phase Ic archaeological subsurface reconnaissance be conducted before the project is allowed to proceed.
- ☐ The Phase Ia archaeological reconnaissance has determined that the project area is within 100 feet of a cemetery and a Cemetery Development Plan is required per IC-14-21-1-26.5.

Cemetery Name:

Other Recommendations/Commitments:

Pursuant to IC-14-21-1, if any archaeological artifacts or human remains are uncovered during construction, demolition, or earthmoving activities, state law (Indiana Code 14-21-1-27 and 29) requires that the discovery must be reported to the Department of Natural Resources within two (2) business days. In that event, please call (317) 232-1646.

Attachments

- ☒ Figure showing project location within Indiana.
- ☒ USGS topographic map showing the project area (1:24,000 scale).
- ☒ Aerial photograph showing the project area, land use and survey methods.
- ☒ Photographs of the project area.
- ☐ Project plans (if available)

Other Attachments:



Short Historic Property Report

**State Road 75 over Wildcat Creek Bridge Project: Scope Undetermined
in Democrat Township, Carroll County, Indiana**

Des. No.: 1601029

Prepared for: American Structurepoint
Indiana Department of Transportation/Federal Highway Administration

Prepared by
WEINTRAUT & ASSOCIATES, INC.
Principal Investigator: Dr. Linda Weintraut
Author: Douglas Fivecoat, M.A.
P.O. Box 5034 | Zionsville, Indiana | (317) 733-9770 | (Linda@weintrautinc.com)

October 2018

Executive Summary

The Indiana Department of Transportation (INDOT) with funding from the Federal Highway Administration (FHWA) intends to proceed with a bridge project along State Road (SR) 75 over Wildcat Creek in Democrat Township, Carroll County, Indiana. More specifically, the project is located near Cutler, Indiana, in Section 33, Township 24 North, Range 1 West. Section 106 of the National Historic Preservation Act requires Federal agencies to take into account the effects of their undertakings on historic properties. The federal involvement is funding from the FHWA.

The undertaking, the bridge over Wildcat Creek [INDOT Bridge No. 075-08-03653 B; Historic Bridge (HB)-1897; Indiana Historic Sites and Structures Inventory (IHSSI) No.: 015-553-65014; National Bridge Inventory (NBI) No.: 24970] is a two-span bridge consisting of a Parker through truss span with a cast-in-place deck and a steel girder approach span. The bridge was determined eligible for listing in the National Register of Historic Places (NRHP) in the Indiana Historic Bridge Inventory and was designated as “Non-Select,” meaning it is not a good candidate for preservation in place. An Alternatives Analysis is being prepared to review alternatives and to recommend a preferred alternative.

The Area of Potential Effects (APE) is “the geographic area or areas within which an

undertaking may directly or indirectly cause alterations in the character or use of historic properties, if any such properties exist” [36 CFR § 800.16(d)]. The APE was drawn approximately 0.25 mile (1,320 feet) from the proposed project location and was narrowed where topography or existing structures limited potential impacts (Figures 1 and 2).

Project historians who meet or exceed the Secretary of Interior’s Professional Standards identified and evaluated historic properties within the APE for this project in accordance with Section 106 of the National Historic Preservation Act (1966), as amended and the regulations implementing Section 106 (36 C.F.R. Part 800).

One property within the APE has been previously determined eligible for listing in the NRHP: **Bridge No. 075-08-03653B** (HB-1897; IHSSI No.: 015-553-65014; NBI No.: 24970). No other resources are recommended eligible for listing in the NRHP.

Per guidance from the INDOT-*Cultural Resources Manual* (August 2018), this report was produced in a short format and without a historic context because there were no NRHP-eligible or listed resources other than the project bridge and because there were no properties rated Notable or Outstanding under IHSSI criteria within the APE.

APPENDIX D. Consulting Parties

Note: This is Appendix D
of the 800.11 document.

Consulting Party List			
SR 75 over Wildcat Creek Des. No.: I60I029 Cutler, Indiana			
Name	Title	Agency/Company	Accepted/ Participating
		Eastern Shawnee Tribe of Oklahoma	
Allison Daniels	Tribal Historic Preservation Officer	Forest County Potawatomi Community	✓
Diane Hunter	Tribal Historic Preservation Officer	Miami Tribe of Oklahoma	✓
		Peoria Tribe of Indians of Oklahoma	
		Pokagon Band of Potawatomi Indians	
Ms. Mary Kennedy	Historic Bridge Specialist	Indiana Department of Transportation	✓
Mr. Chad Slider	Assistant Director	Indiana Department of Natural Resources	✓
Mr. William Brown	Chairman	Carroll County Commissioners	✓
Mr. John Brown	Vice Chairman	Carroll County Commissioners	✓
Mr. Steve Pearson		Carroll County Commissioners	
Ms. Sallie Fahey	Executive Director	Tippecanoe County Area Plan	
Emily Royer	Regional Director	Indiana Landmarks Western Regional Office	✓
Mr. Randy Myers	President	Carroll County Historical Society and Museum	✓
Mark Smith	Carroll County Historian		✓
Paul Coutts	Highway Superintendent	Carroll County Highway Department	
Dr. James Cooper	Professor Emeritus of History	DePauw University	
Mr. Paul Brandenburg	Chair	Historic Spans Task Force	✓
Ms. Bonnie Maxwell	President	Heartland Heritage, Inc.	✓
Ms. Mary Mears		Carroll County Historic Bridge Coalition	
Mr. Joe Mayfield		Delphi Preservation Society	✓
Mr. Gus Nyberg		NICHES Land Trust	✓
Mr. Dan McCain		Carroll County Wabash & Erie Canal	✓
Mr. Al Auffart	President	Adams Mill, Inc.	✓
Ms. Julia Leahy	Executive Director	Carroll County Chamber of Commerce	✓
Ms. Mary Blue	President	Promoting Wildcat Valley	✓
Mr. Dale Brier	Division Director	Indiana Department of Natural Resources Division of Outdoor Recreation	✓
Mr. Rick Parsons	President	Wildcat Guardians	✓
Mr. Nathan Holth		HistoricBridges.org	✓

APPENDIX E. Correspondence

Note: This is Appendix E
of the 800.11 document.



INDIANA DEPARTMENT OF TRANSPORTATION

100 North Senate Avenue
Room N642
Indianapolis, Indiana 46204

PHONE: (317) 234-5168

Eric Holcomb, Governor
Joe McGuinness, Commissioner

November 19, 2018

This letter was sent to the listed parties.

RE: Dual Review Project: SR 75 over Wildcat Creek Bridge Project (Bridge No. 075-0-03653 B)
Des. No. 1601029
Carroll County, Indiana

Dear Consulting Party,

The Indiana Department of Transportation (INDOT), with funding from the Federal Highway Administration, proposes to proceed with the SR 75 over Wildcat Creek Bridge project (Des. No. 1601029). American Structurepoint, Inc. is under contract with INDOT to advance the environmental documentation for the referenced project.

This letter is part of the early coordination phase of the environmental review process requesting comments associated with this project. We are requesting comments from your area of expertise regarding any possible environmental effects associated with this project. Please use the above Des. Number and project description in your reply and your comments will be incorporated into the formal environmental study.

The proposed undertaking is on State Road (SR) 75 over Wildcat Creek Bridge (INDOT Bridge No. 075-08-03653B) from approximately 0.08 mile north and south of the bridge approaches in Carroll County. It is within Democrat Township, Rossville 7.5 Minute USGS Topographic Quadrangle in Section 33, Township 24 North, Range 1 West.

The current bridge over Wildcat Creek [INDOT Bridge No. 075-08-03653 B; Historic Bridge (HB)-1897; Indiana Historic Sites and Structures Inventory (IHSSI) No.: 015-553-65014; National Bridge Inventory (NBI) No.: 24970] is a two-span bridge consisting of a thru truss and a steel girder approach span. The bridge was constructed in 1947 and reconstructed most recently in 2001. The bridge was determined eligible for listing in the National Register of Historic Places (NRHP) in the Indiana Historic Bridge Inventory and was designated as "Non-Select," meaning it is not a good candidate for preservation in place.

The need for the proposed project is a result of the insufficient inventory load rating, deteriorated condition of the structural steel, superstructure, and wearing surface and the inadequate usable shoulder width on the bridge. The purpose of the proposed project is to improve the inventory load rating of the bridge to 36 tons or more and to provide safe driving conditions for vehicles traveling on SR 75 over Wildcat Creek.

Section 106 of the National Historic Preservation Act requires federal agencies to take into account the effects of their undertakings on historic and archaeological properties. In accordance with 36 CFR 800.2 (c), you are hereby requested to be a consulting party to participate in the Section 106 process. Entities that have been invited to participate in the Section 106 consultation process for this project are identified in the attached list. Per 36 CFR 800.3(f), we hereby request that the Indiana State Historic Preservation Officer (SHPO) notify this

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An Equal Opportunity Employer



office if the SHPO staff is aware of any other parties that may be entitled to be consulting parties or should be contacted as potential consulting parties for the project.

The Section 106 process involves efforts to identify historic properties potentially affected by the undertaking, assess its effects and seek ways to avoid, minimize or mitigate any adverse effects on historic properties. For more information regarding the protection of historic resources, please see the Advisory Council on Historic Preservation's guide: *Protecting Historic Properties: A Citizen's Guide to Section 106 Review* available online at <http://www.achp.gov/citizensguide.pdf>.

Per the terms of the "Programmatic Agreement Regarding Management and Preservation of Indiana's Historic Bridges" (Historic Bridges PA), the Federal Highway Administration—Indiana Division (FHWA) will satisfy its Section 106 responsibilities involving "Select" and "Non-Select" bridges through the Project Development Process (PDP) of the Historic Bridges PA (Stipulation III). Because Bridge No. 075-08-03653 B is a "Non-Select" bridge, the procedures outlined in Stipulation III.B. of the Historic Bridges PA will be followed to fulfill FHWA's Section 106 responsibilities for the bridge. (A copy of the Historic Bridges PA can be downloaded here: <http://www.in.gov/indot/2530.htm>). Per Stipulation III.B. an alternatives analysis will be completed to determine the scope of work for the proposed project. Alternatives to be considered will include rehabilitation of the existing structure, construction of a one-way pair, or a structure replacement. Right-of-way acquisition will depend on the alternative selected, but is not anticipated to exceed 0.5 acre.

Please note that per the permanent rule issued by the Indiana Department of Natural Resources effective August 14, 2013 (312 IAC 20-4-11.5), INDOT is requesting that this project be subjected to "dual review"; that is, reviewed by the Division of Historic Preservation and Archaeology simultaneously under 54 U.S.C. 306108 (Section 106) and IC 14-21-1-18 (Indiana Preservation and Archaeology Law dealing with alterations of historic sites and structures requiring a Certificate of Approval). Pursuant to Section 11.5(f) of this rule, at the conclusion of the review process we anticipate that the Division Director would issue a letter of clearance exempting this project from obtaining a Certificate of Approval under IC 14-21-1-18. Enclosed with this letter is a detailed list of the consulting parties with contact information, including email addresses, for processing the dual review submission.

The Area of Potential Effects (APE) is the area in which the proposed project may cause alterations in the character or use of historic resources. The APE contains one resource, SR 75 over Wildcat Creek Bridge, that has been previously determined eligible for listing in the NRHP. A historian who meets the Secretary of the Interior's Professional Qualification Standards identified and evaluated above-ground resources within the APE for potential eligibility for the NRHP. As a result of the historic property identification and evaluation efforts, the SR 75 Bridge over Wildcat Creek (IHSSI No.: 015-553-65014) is recommended that it retains the characteristics for which it was determined eligible for listing in the NRHP. No other properties in the APE were identified as being NRHP eligible.

With regards to archaeological resources, an archaeologist who meets the Secretary of the Interior's Professional Qualification Standards identified no sites within the project area. As a result of these efforts, no sites were recommended as eligible to the NRHP and no further work is recommended.

The Historic Property Report and Archaeology Report (Tribes only) are available for review in IN SCOPE at <http://erms.indot.in.gov/Section106Documents/> (the Des. No. is the most efficient search term, once in IN SCOPE). You are invited to review these documents and respond with comments on any historic resource impacts incurred as a result of this project so that an environmental report can be completed. We also welcome your related opinions and other input to be considered in the preparation of the environmental document. If you prefer a hard copy of this material, please respond to this email with your request within seven (7) days.

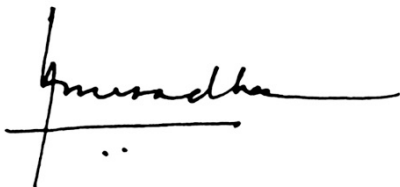
Please review the information and comment within thirty (30) calendar days of receipt. If you indicate that you do not desire to be a consulting party, or if you do not respond, you will not be included on the list of consulting parties for this project. If we do not receive your response in the time allotted, the project will proceed consistent with the proposed design and you will not receive further information about the project unless the design changes.

For questions concerning specific project details, you may contact Hillary Shaffer of American Structurepoint at 317-547-5580 or hshaffer@structurepoint.com. All future responses regarding the proposed project should be forwarded to American Structurepoint at the following address:

Hillary E. Shaffer
Environmental Specialist
American Structurepoint, Inc.
7260 Shadeland Station
Indianapolis, Indiana 46256

Tribal contacts may contact Shaun Miller at smiller@indot.in.gov or 317-233-6795 or Michelle Allen at FHWA at michelle.allen@dot.gov or 317-226-7344.

Sincerely,



Anuradha V. Kumar, Manager
Cultural Resources Office
Environmental Services

Enclosures:

USGS Topographic Map – Rossville Quadrangle

Distribution List:

Chad Slider, Indiana Department of Natural Resources/Division of Historic Preservation & Archaeology (IDNR/DHPA), Indiana State Historic Preservation Officer (SHPO)
William Brown, Carroll County Commissioners
John Brown, Carroll County Commissioners
Steve Pearson, Carroll County Commissioners
Sallie Fahey, Tippecanoe County Area Plan
Indiana Landmarks, Western Regional Office
Randy Myers, Carroll County Historical Museum
Mark Smith, Carroll County Historian
Paul Coutts, Carroll County Highway Department Superintendent/Engineer
James Cooper, DePauw University
Paul Brandenburg, Historic Spans Task Force
Eastern Shawnee Tribe of Oklahoma

Forest County Potawatomi Community
Miami Tribe of Oklahoma
Peoria Tribe of Indians of Oklahoma
Pokagon Band of Potawatomi Indians

FHWA Project: Des. No. 1601029; SR 75 Bridge Project, Carroll County, Indiana

1 message

Linda Weintraut <linda@weintrautinc.com>

Mon, Nov 19, 2018 at 2:44 PM

To: "Slider, Chad" <CSlider@dnr.in.gov>, commissioner@carrollcountyin.gov, commissioner2@carrollcountyin.gov, commissioner3@carrollcountyin.gov, sfahey@tippecanoe.in.gov, west@indianalandmarks.org, carrollcountyhistoricalsociety@ffni.com, markalansmith12171948@gmail.com, highwaydept2@centurylink.net, "James L. Cooper" <jlcooper@ccrtc.com>, Paul Brandenburg <indianabridges@sbcglobal.net>, bmccord@dnr.in.gov
Cc: "Kumar, Anuradha" <akumar@indot.in.gov>, "Kennedy, Mary" <mkennedy@indot.in.gov>, "Branigin, Susan" <sbranigin@indot.in.gov>, "Miller, Shaun (INDOT)" <smiller@indot.in.gov>, "Boits, Leah" <lboits@structurepoint.com>, "Hockaday, Jacqueline R" <JHockaday@indot.in.gov>, "Werner, Lynette K" <LWerner@indot.in.gov>, Craig Arnold <carold@weintrautinc.com>, Colin Graham <cgraham@weintrautinc.com>, bethany w <bethany@weintrautinc.com>, Doug Fivecoat <dfivecoat@weintrautinc.com>

Des. No.: 1601029

Project Description: SR 75 over Wildcat Creek Bridge Project

Location: Carroll County, Indiana

The Indiana Department of Transportation (INDOT), with funding from the Federal Highway Administration, proposes to proceed with the SR 75 over Wildcat Creek Bridge project (Des. No. 1601029). American Structurepoint, Inc. is under contract with INDOT to advance the environmental documentation for the referenced project.

Section 106 of the National Historic Preservation Act requires Federal agencies to take into account the effects of their undertakings on historic properties. The following agencies/individuals are being invited to become consulting parties:

Indiana State Historic Preservation Officer
Carroll County Commissioners
Tippecanoe County Area Plan
Indiana Landmarks, Western Regional Office
Carroll County Historical Museum
Carroll County Historian
Carroll County Highway Department Superintendent/Engineer
James Cooper, DePauw University
Historic Spans Task Force
Eastern Shawnee Tribe of Oklahoma
Forest County Potawatomi Community
Peoria Tribe of Indians of Oklahoma
Pokagon Band of Potawatomi Indians

This letter is part of the early coordination phase of the environmental review process requesting comments associated with this project. We are requesting comments from your area of expertise regarding any possible environmental effects associated with this project. Please use the above DES Number and project description in your reply and your comments will be incorporated into the formal environmental study.

Please review the letter, Short Historic Property Report, and Archaeology Short Report (Tribes only) located in IN-SCOPE at <http://erms.indot.in.gov/Section106Documents/> (the Des No. is the most efficient search term, once in IN-SCOPE), and respond with your comments on any historic resource impacts incurred as a result of this project so that an environmental report can be completed. We also welcome your related opinions and other input to be considered in the preparation of the environmental document. If a hard copy of the materials is needed, please respond to this email with your request within seven (7) days.

Consulting parties have thirty (30) days from receipt of this information to review and provide comment. If we do not receive a response from an invited consulting party in the time allotted, the project will proceed consistent with the proposed design. **Therefore, if we do not receive a response within thirty (30) days, your agency or organization will not receive any further information on the project unless the scope of work changes.**

Tribal contacts may contact Shaun Miller at smiller@indot.in.gov or 317- 233-6795 or Michelle Allen at FHWA at michelle.allen@dot.gov or 317-226-7344.

Thank you in advance for your input,

--

Linda Weintraut, Ph.D.
Weintraut & Associates, Inc.
PO Box 5034
4649 Northwestern Drive
Zionsville, Indiana 46077
317.733.9770 ext. 310

www.weintrautinc.com

Fwd: FHWA Project: Des. No. 1601029; SR 75 Bridge Project, Carroll County, Indiana

1 message

Linda Weintraut <linda@weintrautinc.com>
To: Doug Fivecoat <dfivecoat@weintrautinc.com>

Wed, Nov 21, 2018 at 9:55 AM

----- Forwarded message -----

From: **Miller, Shaun (INDOT)** <smiller@indot.in.gov>
Date: Mon, Nov 19, 2018 at 3:14 PM
Subject: FHWA Project: Des. No. 1601029; SR 75 Bridge Project, Carroll County, Indiana
To: thpo@estoo.net <thpo@estoo.net>, Diane Hunter <dhunter@miamination.com>, lpappenfort@peoriatribe.com <lpappenfort@peoriatribe.com>, mathew.bussler@pokagonband-nsn.gov <mathew.bussler@pokagonband-nsn.gov>, Allison Daniels <Allison.Daniels@fcpotawatomi-nsn.gov>
Cc: Linda Weintraut <linda@weintrautinc.com>, Bethany Hughes <bhughes@weintrautinc.com>, Kennedy, Mary <MKENNEDY@indot.in.gov>, Michelle Allen <michelle.allen@dot.gov>

Des. No.: 1601029**Project Description: SR 75 over Wildcat Creek Bridge Project****Location: Carroll County, Indiana**

Dear Consulting Parties,

The Indiana Department of Transportation (INDOT), with funding from the Federal Highway Administration, proposes to proceed with the SR 75 over Wildcat Creek Bridge project (Des. No. 1601029). American Structurepoint, Inc. is under contract with INDOT to advance the environmental documentation for the referenced project.

Section 106 of the National Historic Preservation Act requires Federal agencies to take into account the effects of their undertakings on historic properties. The following agencies/individuals are being invited to become consulting parties:

Indiana State Historic Preservation Officer
Carroll County Commissioners
Tippecanoe County Area Plan
Indiana Landmarks, Western Regional Office
Carroll County Historical Museum
Carroll County Historian
Carroll County Highway Department Superintendent/Engineer
James Cooper, DePauw University
Historic Spans Task Force
Eastern Shawnee Tribe of Oklahoma
Forest County Potawatomi Community
Miami Tribe of Oklahoma
Peoria Tribe of Indians of Oklahoma
Pokagon Band of Potawatomi Indians

This letter is part of the early coordination phase of the environmental review process requesting comments associated with this project. We are requesting comments from your area of expertise regarding any possible environmental effects associated with this project. Please use the above DES Number and project description in your reply and your comments will be incorporated into the formal environmental study.

Please review the letter, Short Historic Property Report, and Archaeology Short Report (Tribes only) located in IN-SCOPE at <http://erms.indot.in.gov/Section106Documents/> (the Des No. is the most efficient search term, once in IN-SCOPE), and respond with your comments on any historic resource impacts incurred as a result of this project so that an environmental report can be completed. We also welcome your related opinions and other input to be considered in the preparation of the environmental document. If a hard copy of the materials is needed, please respond to this email with your request within seven (7) days.

Consulting parties have thirty (30) days from receipt of this information to review and provide comment. If we do not receive a response from an invited consulting party in the time allotted, the project will proceed consistent with the proposed design. **Therefore, if we do not receive a response within thirty (30) days, your agency or organization will not receive any further information on the project unless the scope of work changes.**

Tribal contacts may contact Shaun Miller at smiller@indot.in.gov or 317- 233-6795 or Michelle Allen at FHWA at michelle.allen@dot.gov or 317-226-7344.

Thank you in advance for your input,

Shaun Miller

Archaeological Team Lead

INDOT, Cultural Resources Office

smiller@indot.in.gov

(317) 233-6795

--

Linda Weintraut, Ph.D.
Weintraut & Associates, Inc.
PO Box 5034

Fwd: FHWA Project: Des. No. 1601029; SR 75 Bridge Project, Carroll County, Indiana

1 message

Bethany Natali <bethany@weintrautinc.com>
To: Doug Fivecoats <dfivecoat@weintrautinc.com>

Tue, Nov 20, 2018 at 4:33 PM

----- Forwarded message -----

From: **Linda Weintraut** <linda@weintrautinc.com>
Date: Tue, Nov 20, 2018 at 4:30 PM
Subject: Re: FHWA Project: Des. No. 1601029; SR 75 Bridge Project, Carroll County, Indiana
To: <eroyer@indianalandmarks.org>
Cc: bethany w <bethany@weintrautinc.com>, Boits, Leah <lboits@structurepoint.com>

Thank you for your response. We will add Indiana Landmarks as a consulting party and forward your concerns regarding the bridge's non-select status.

On Tue, Nov 20, 2018 at 4:17 PM Emily Royer <eroyer@indianalandmarks.org> wrote:

Dr. Weintraut,

Indiana Landmarks is interested in acting as a consulting party for the project with Des. No. 1601029, SR 75 over Wildcat Creek Bridge.

The Historic Bridge Programmatic Agreement entered into by the FHWA, INDOT, Indiana SHPO, and ACHP is approaching a 10-year review as stipulated in the agreement. As part of that review, Indiana Landmarks will expect the historic bridge survey and the Select/Non-select list of bridges to be reevaluated. A number of Select bridges have been lost over the past decade to demolition or other non-preservation activity. Considering these losses, the 10-year review period, and the "Outstanding" rating in the Carroll County Historic Sites and Structures inventory, Indiana Landmarks recommends that the bridge over Wildcat Creek (bridge # 075-08-03653B and IHSSI # 015-553-65014) be preserved in-place for continued vehicular use on SR 75. Indiana Landmarks also intends to push for a categorical change of this bridge's status from Non-Select to Select.

Best,

.....
Emily Royer

Community Preservation Specialist

.....
Indiana Landmarks

Western Regional Office

669 Ohio Street

Terre Haute, IN 47807

Ph. 812-232-4534

Fax: 812-234-0156

www.indianalandmarks.org

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From: Linda Weintraut <linda@weintrautinc.com>

Sent: Monday, November 19, 2018 2:45 PM

To: Slider, Chad <CSlider@dnr.in.gov>; commissioner@carrollcountyin.gov; commissioner2@carrollcountyin.gov; commissioner3@carrollcountyin.gov; sfahey@tippecanoe.in.gov; West <West@indianalandmarks.org>; carrollcountyhistoricalsociety@ffni.com; markalansmith12171948@gmail.com; highwaydept2@centurylink.net; Jim Cooper <jlcooper@crtc.com>; Paul Brandenburg <indianabridges@sbcglobal.net>; bmccord@dnr.in.gov

Cc: Kumar, Anuradha <akumar@indot.in.gov>; Mary Kennedy <mkennedy@indot.in.gov>; Branigin, Susan <sbranigin@indot.in.gov>; Miller, Shaun (INDOT)

<smiller@indot.in.gov>; Boits, Leah <lboits@structurepoint.com>; Hockaday, Jacqueline R <JHockaday@indot.in.gov>; Werner, Lynette K <LWerner@indot.in.gov>; Craig Arnold <camold@weintrautinc.com>; Colin Graham <cgraham@weintrautinc.com>; bethany w <bethany@weintrautinc.com>; Doug Fivecoat <dfivecoat@weintrautinc.com>

Subject: FHWA Project: Des. No. 1601029; SR 75 Bridge Project, Carroll County, Indiana

Des. No.: 1601029

Project Description: SR 75 over Wildcat Creek Bridge Project

Location: Carroll County, Indiana

The Indiana Department of Transportation (INDOT), with funding from the Federal Highway Administration, proposes to proceed with the SR 75 over Wildcat Creek Bridge project (Des. No. 1601029). American Structurepoint, Inc. is under contract with INDOT to advance the environmental documentation for the referenced project.

Section 106 of the National Historic Preservation Act requires Federal agencies to take into account the effects of their undertakings on historic properties. The following agencies/individuals are being invited to become consulting parties:

Indiana State Historic Preservation Officer
Carroll County Commissioners
Tippecanoe County Area Plan
Indiana Landmarks, Western Regional Office
Carroll County Historical Museum
Carroll County Historian
Carroll County Highway Department Superintendent/Engineer
James Cooper, DePauw University
Historic Spans Task Force
Eastern Shawnee Tribe of Oklahoma
Forest County Potawatomi Community
Peoria Tribe of Indians of Oklahoma
Pokagon Band of Potawatomi Indians

This letter is part of the early coordination phase of the environmental review process requesting comments associated with this project. We are requesting comments from your area of expertise regarding any possible environmental effects associated with this project. Please use the above DES Number and project description in your reply and your comments will be incorporated into the formal environmental study.

Please review the letter, Short Historic Property Report, and Archaeology Short Report (Tribes only) located in IN-SCOPE at <http://erms.indot.in.gov/Section106Documents/> (the Des No. is the most efficient search term, once in IN-SCOPE), and respond with your comments on any historic resource impacts incurred as a result of this project so that an environmental report can be completed. We also welcome your related opinions and other input to be considered in the preparation of the environmental document. If a hard copy of the materials is needed, please respond to this email with your request within seven (7) days.

Consulting parties have thirty (30) days from receipt of this information to review and provide comment. If we do not receive a response from an invited consulting party in the time allotted, the project will proceed consistent with the proposed design. **Therefore, if we do not receive a response within thirty (30) days, your agency or organization will not receive any further information on the project unless the scope of work changes.**

Tribal contacts may contact Shaun Miller at smiller@indot.in.gov or 317- 233-6795 or Michelle Allen at FHWA at michelle.allen@dot.gov or 317-226-7344.

Thank you in advance for your input,

--

Linda Weintraut, Ph.D.

Weintraut & Associates, Inc.

PO Box 5034

4649 Northwestern Drive

Zionsville, Indiana 46077

317.733.9770 ext. 310

www.weintrautinc.com

Fwd: FHWA Project: Des. No. 1601029; SR 75 Bridge Project, Carroll County, Indiana

1 message

----- Forwarded message -----

From: **Kennedy, Mary** <MKENNEDY@indot.in.gov>

Date: Wed, Nov 21, 2018 at 10:36 AM

Subject: RE: FHWA Project: Des. No. 1601029; SR 75 Bridge Project, Carroll County, Indiana

To: eroyer@indianalandmarks.org <eroyer@indianalandmarks.org>

Cc: Branigin, Susan <SBRanigin@indot.in.gov>, Kumar, Anuradha <akumar@indot.in.gov>, bethany w <bethany@weintrautinc.com>, Boits, Leah <lboits@structurepoint.com>, Miller, Shaun (INDOT) <smiller@indot.in.gov>, Linda Weintraut <linda@weintrautinc.com>, Hockaday, Jacqueline R <JHockaday@indot.in.gov>, Allen, Michelle (FHWA) <michelle.allen@dot.gov>, Joyce.Newland@dot.gov <Joyce.Newland@dot.gov>

Emily,

Thank you for the response and the willingness to be a consulting party on this project. As indicated in the early coordination letter, per Stipulation III.B. of the Historic Bridge Programmatic Agreement, an alternatives analysis will be completed to determine the scope of work for the proposed project. Consulting parties will have a 30-day review of that document. Consulting parties will be notified via email when the document is available for review.

With regard to the 10-year review of the Historic Bridge Programmatic Agreement, the PA was executed in 2006, and the 10-year review issue was addressed in 2016. The FHWA issued a letter on December 28, 2016 explaining that FHWA, SHPO and INDOT met several times in 2016 regarding Stipulation II.C.2. of the Historic Bridge PA. The agencies decided that a full review of bridge determinations was not necessary at that time, and the issue will be revisited at the next 10-year review (2026). As a result of the meetings in 2016, INDOT was tasked with creating an updated list of the current status of Select/Non-Select bridges. The letter and the list have been available for download from INDOT's website since December 28, 2016 (<https://www.in.gov/indot/2531.htm>). An announcement regarding this letter was made via INDOT's Environmental Services listserv on December 28, 2016, and the Historic Bridge Task group was directly emailed about this letter on the same date.

We look forward to your continued participation in the SR 75 Bridge Project. Please don't hesitate to let us know if you have any questions or need more information.

Mary E. Kennedy

Historic Bridge Specialist

Cultural Resources Office

Environmental Services

100 N. Senate Ave., Room N642-ES

Indianapolis, IN 46204

Office: (317) 232-5215

Email: mkennedy@indot.in.gov

[Facebook](#)[Twitter](#)[YouTube](#)[Core4cid:image005.jpg@01D3B139.7A60C1D0](#)

BarTransActSizeF

*Updated Historic Property Report (HPR) guidelines can be found [here](#)

*Design Memorandum 18-02 regarding the new procedures for Historic Bridge Alternatives Analysis Documents can be found [here](http://www.in.gov/dot/div/contracts/standards/memos/2018/18-02%20ta%20Historic%20Bridge.pdf): <http://www.in.gov/dot/div/contracts/standards/memos/2018/18-02%20ta%20Historic%20Bridge.pdf>

*For the latest updates from INDOT's Cultural Resources Office, subscribe to the Environmental Services listserv: <https://www.in.gov/indot/3217.htm>

----- Forwarded message -----

From: **Emily Royer** <eroyer@indianalandmarks.org>

Date: Tue, Nov 20, 2018 at 4:17 PM

Subject: RE: FHWA Project: Des. No. 1601029; SR 75 Bridge Project, Carroll County, Indiana

To: Linda Weintraut <linda@weintrautinc.com>

Dr. Weintraut,

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Best,

.....
Emily Royer

Community Preservation Specialist
.....

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Western Regional Office

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Ph. 812-232-4534

Fax: 812-234-0156

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From: Linda Weintraut <linda@weintrautinc.com>

Sent: Monday, November 19, 2018 2:45 PM

To: Slider, Chad <CSlider@dnr.in.gov>; commissioner@carrollcountyin.gov; commissioner2@carrollcountyin.gov; commissioner3@carrollcountyin.gov; sfahey@tippecanoe.in.gov; West <West@indianalandmarks.org>; carrollcountyhistoricalsociety@ffni.com; markalansmith12171948@gmail.com; highwaydept2@centurylink.net; Jim Cooper <jcooper@ccrtc.com>; Paul Brandenburg <indianabridges@sbcglobal.net>; bmccord@dnr.in.gov

Cc: Kumar, Anuradha <akumar@indot.in.gov>; Mary Kennedy <mkennedy@indot.in.gov>; Branigin, Susan <sbranigin@indot.in.gov>; Miller, Shaun (INDOT) <smiller@indot.in.gov>; Boits, Leah <lboits@structurepoint.com>; Hockaday, Jacqueline R <JHockaday@indot.in.gov>; Werner, Lynette K <LWerner@indot.in.gov>; Craig Arnold <carold@weintrautinc.com>; Colin Graham <cgraham@weintrautinc.com>; bethany w <bethany@weintrautinc.com>; Doug Fivecoat <dfivecoat@weintrautinc.com>

Subject: FHWA Project: Des. No. 1601029; SR 75 Bridge Project, Carroll County, Indiana

Des. No.: 1601029

Project Description: SR 75 over Wildcat Creek Bridge Project

Location: Carroll County, Indiana

The Indiana Department of Transportation (INDOT), with funding from the Federal Highway Administration, proposes to proceed with the SR 75 over Wildcat Creek Bridge project (Des. No. 1601029). American Structurepoint, Inc. is under contract with INDOT to advance the environmental documentation for the referenced project.

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Tippecanoe County Area Plan
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Carroll County Historical Museum
Carroll County Historian
Carroll County Highway Department Superintendent/Engineer
James Cooper, DePauw University
Historic Spans Task Force
Eastern Shawnee Tribe of Oklahoma
Forest County Potawatomi Community
Peoria Tribe of Indians of Oklahoma
Pokagon Band of Potawatomi Indians

Re: SR 75 Bridge project

Linda Weintraut <linda@weintrautinc.com>

Fri, Dec 7, 2018 at 4:53 PM

To: Bonnie Maxwell <bmaxwell62@gmail.com>

Cc: natali <bethany@weintrautinc.com>, lboits@structurepoint.com

We would be pleased to have Heartland Heritage, Inc. serve as a consulting party for this project. We will add you to the list and you will receive notifications of available information.

We look forward to your participation.

Linda Weintraut

On Dec 6, 2018, at 6:38 PM, Bonnie Maxwell <bmaxwell62@gmail.com> wrote:

Linda—

I've been asked by the board of Heartland Heritage, Inc., of which I am president, to ask if we could be included in the consulting parties for FHWA Project: Des. No. 1601029; SR 75 Bridge Project, Carroll County, Indiana. We are a nonprofit organization in Carroll County which has undertaken a number of significant projects since we incorporated in January 2013. We've received numerous grants and own several pieces of property. Below is our statement of purpose from our bylaws:

- Preserving and promoting buildings, structures, sites, events, and archival resources which are of cultural or historic significance in the region that includes Carroll County, IN, and its environs;
- Preserving and promoting unique natural resources and sites;
- Partnering with other similar organizations in the area for particular projects;
- Partnering with owners of the above types of properties to provide encouragement and technical assistance to preserve, maintain, and promote the properties;
- Providing stewardship to important sites and structures as needed;
- Assisting other organizations and entities whose objectives are similar to or compatible with the goals of Heartland Heritage.

If you need other information to consider our request, please let me know. Thanks for your assistance.

Bonnie Maxwell

President, Heartland Heritage, Inc.

Fwd: FHWA Project: Des. No. 1601029; SR 75 Bridge Project, Carroll County, Indiana

1 message

Linda Weintraut <linda@weintrautinc.com>

Mon, Dec 10, 2018 at 1:39 PM

To: jmmears@remconline.net

Cc: bethany w <bethany@weintrautinc.com>, "Kennedy, Mary" <mkennedy@indot.in.gov>, "Boits, Leah" <lboits@structurepoint.com>

----- Forwarded message -----

From: Linda Weintraut <linda@weintrautinc.com>

Date: Mon, Nov 19, 2018 at 2:44 PM

Subject: FHWA Project: Des. No. 1601029; SR 75 Bridge Project, Carroll County, Indiana

To: Slider, Chad <CSlider@dnr.in.gov>, <commissioner@carrollcountyin.gov>, <commissioner2@carrollcountyin.gov>, <commissioner3@carrollcountyin.gov>, <sfahey@tippecanoe.in.gov>, <west@indianalandmarks.org>, <carrollcountyhistoricalsociety@ffni.com>, <markalansmith12171948@gmail.com>, <highwaydept2@centurylink.net>, James L. Cooper <jlcooper@ccrtc.com>, Paul Brandenburg <indianabridges@sbcglobal.net>, <bmccord@dnr.in.gov>

Cc: Kumar, Anuradha <akumar@indot.in.gov>, Kennedy, Mary <mkennedy@indot.in.gov>, Branigin, Susan <sbranigin@indot.in.gov>, Miller, Shaun (INDOT) <smiller@indot.in.gov>, Boits, Leah <lboits@structurepoint.com>, Hockaday, Jacqueline R <JHockaday@indot.in.gov>, Werner, Lynette K <LWerner@indot.in.gov>, Craig Arnold <carold@weintrautinc.com>, Colin Graham <cgraham@weintrautinc.com>, bethany w <bethany@weintrautinc.com>, Doug Fivecoat <dfivecoat@weintrautinc.com>

Des. No.: 1601029**Project Description: SR 75 over Wildcat Creek Bridge Project****Location: Carroll County, Indiana**

The Indiana Department of Transportation (INDOT), with funding from the Federal Highway Administration, proposes to proceed with the SR 75 over Wildcat Creek Bridge project (Des. No. 1601029). American Structurepoint, Inc. is under contract with INDOT to advance the environmental documentation for the referenced project.

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James Cooper, DePauw University
Historic Spans Task Force
Eastern Shawnee Tribe of Oklahoma
Forest County Potawatomi Community
Peoria Tribe of Indians of Oklahoma
Pokagon Band of Potawatomi Indians

This letter is part of the early coordination phase of the environmental review process requesting comments associated with this project. We are requesting comments from your area of expertise regarding any possible environmental effects associated with this project. Please use the above DES Number and project description in your reply and your comments will be incorporated into the formal environmental study.

Please review the letter, Short Historic Property Report, and Archaeology Short Report (Tribes only) located in IN-SCOPE at <http://erms.indot.in.gov/Section106Documents/> (the Des No. is the most efficient search term, once in IN-SCOPE), and respond with your comments on any historic resource impacts incurred as a result of this project so that an environmental report can be completed. We also welcome your related opinions and other input to be considered in the preparation of the environmental document. If a hard copy of the materials is needed, please respond to this email with your request within seven (7) days.

Consulting parties have thirty (30 days) from receipt of this information to review and provide comment. If we do not receive a response from an invited consulting party in the time allotted, the project will proceed consistent with the proposed design. **Therefore, if we do not receive a response within thirty (30) days, your agency or organization will not receive any further information on the project unless the scope of work changes.**

Tribal contacts may contact Shaun Miller at smiller@indot.in.gov or 317- 233-6795 or Michelle Allen at FHWA at michelle.allen@dot.gov or 317-226-7344.

Thank you in advance for your input,

--

Linda Weintraut, Ph.D.
Weintraut & Associates, Inc.
PO Box 5034
[4649 Northwestern Drive](#)
[Zionsville, Indiana 46077](#)
317.733.9770 ext. 310

www.weintrautinc.com

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[Zionsville, Indiana 46077](#)
317.733.9770 ext. 310

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Miami Tribe of Oklahoma

3410 P St. NW, Miami, OK 74354 • P.O. Box 1326, Miami, OK 74355

Ph: (918) 541-1300 • Fax: (918) 542-7260

www.miamination.com



December 13, 2018

Shaun Miller
Archaeological Team Lead
Cultural Resources Office
Indiana DOT
575 North Pennsylvania Street
Indianapolis, IN 46204

Re: Des. No. 1601029 SR 75 Bridge Project, Carroll County, Indiana– Comments of the Miami Tribe of Oklahoma

Dear Mr. Miller:

Aya, kikwehsitoole – I show you respect. My name is Diane Hunter, and I am the Tribal Historic Preservation Officer for the Federally Recognized Miami Tribe of Oklahoma. In this capacity, I am the Miami Tribe's point of contact for all Section 106 issues.

The Miami Tribe offers no objection to the above-mentioned project at this time, as we are not currently aware of existing documentation directly linking a specific Miami cultural or historic site to the project site. However, as this site is within the aboriginal homelands of the Miami Tribe, if any human remains or Native American cultural items falling under the Native American Graves Protection and Repatriation Act (NAGPRA) or archaeological evidence is discovered during any phase of this project, the Miami Tribe requests immediate consultation with the entity of jurisdiction for the location of discovery. In such a case, please contact me at 918-541-8966 or by email at dhunter@miamination.com to initiate consultation.

The Miami Tribe accepts the invitation to serve as a consulting party to the proposed project. In my capacity as Tribal Historic Preservation Officer I am the point of contact for consultation.

Respectfully,

Diane Hunter
Tribal Historic Preservation Officer



Division of Historic Preservation & Archaeology · 402 W. Washington Street, W274 · Indianapolis, IN 46204-2739
Phone 317-232-1646 · Fax 317-232-0693 · dhpa@dnr.IN.gov · www.IN.gov/dnr/historic



December 18, 2018

Hillary E. Shaffer
Environmental Specialist
American Structurepoint, Inc.
7260 Shadeland Station
Indianapolis, Indiana 46256

Federal Agency: Indiana Department of Transportation ("INDOT"),
on behalf of Federal Highway Administration, Indiana Division ("FHWA")

Re: DUAL REVIEW: Early coordination letter, proposal for dual review, historic property short report (Fivecoat, 10/2018), and Indiana archaeological short report (Graham, 09/2018), for the SR 75 over Wildcat Creek Bridge Project, Democrat Township, Carroll County, Indiana (Des. No. 1601029, DHPA No. 23247)

Dear Ms. Shaffer:

The Indiana Department of Natural Resources, Division of Historic Preservation and Archaeology ("INDNR-DHPA"), which also serves as the staff of the Indiana State Historic Preservation Officer ("Indiana SHPO"), is in receipt of Weintraut & Associates, Inc.'s review request submittal form, with enclosures, dated November 19, 2018, transmitting INDOT's proposal for a dual review, pursuant to 312 Indiana Administrative Code ("IAC") 20-4-11.5, of the aforementioned project, all of which we received on November 21, 2018.

The Indiana SHPO will review the information submitted under Section 106 of the National Historic Preservation Act of 1966, as amended (54 U.S.C. § 306108), implementing regulations at 36 C.F.R. Part 800, the Indiana Historic Bridges Programmatic Agreement, as well as Indiana Code 14-21-1-18 and 312 IAC 20-4. By copy of this letter, Indiana SHPO is providing notification of the commencement of the dual review to potentially interested persons and members of the Indiana Historic Preservation Review Board ("Review Board"). Notice of the commencement will also be posted on the division's website (www.in.gov/dnr/historic/7440.htm).

We are not aware of anyone who should be invited to become a consulting party for the purposes of the review of this project under Section 106, beyond those whom you already have invited. For the purposes of Indiana Code 14-21-1-18 and 312 IAC 20-4, we have added the members of the Review Board and additional, potentially interested parties to the list of parties we intend to copy with our comment letters.

The area of potential effects ("APE") proposed in the historic property short report ("HPSR"; Fivecoat, 10/2018) appears to be appropriate for a project of this nature in which direct or indirect effects may occur.

As the October 2018 HPSR indicates, the SR 75 Bridge over Wildcat Creek (Bridge No. 075-08-03653B; National Bridge Inventory ["NBI"] No. 24970) has been identified as a Non-Select Bridge in the *Indiana Historic Bridge Inventory*. We agree that the steel Parker through truss span is eligible for inclusion in the National Register of Historic Places ("NRHP") under Criterion C as a bridge that "represents an early or distinctive phase in bridge construction, design, or engineering" and "represents a significant phase or feature of the work of a master."

Furthermore, we also agree with the conclusions in the HPSR that the SR 75 Bridge is the only historic property listed in or eligible for listing in the NRHP identified within the APE. We also note that the HPSR states that the bridge was most recently repaired in 2001.

In regard to archaeological resources, based upon the submitted information and the documentation available to the staff of the Indiana SHPO, we have not identified any currently known archaeological resources listed in or eligible for inclusion in the NRHP within the proposed project area; and we concur with the opinion of the archaeologist, as expressed in the Indiana archaeological short report (Graham, 09/2018), that no further archaeological investigations appear necessary at the proposed project area.

If any prehistoric or historic archaeological artifacts or human remains are uncovered during construction, demolition, or earthmoving activities, state law (Indiana Code 14-21-1-27 and 29) requires that the discovery must be reported to the Department of Natural Resources within two (2) business days. In that event, please call (317) 232-1646. Be advised that adherence to Indiana Code 14-21-1-27 and -29 does not obviate the need to adhere to applicable federal statutes and regulations, including but not limited to 36 C.F.R. 800.

We look forward to receiving a hard copy of the historic bridge alternatives analysis for our review and comment.

If you have questions regarding our dual review of the aforementioned project, please contact the Indiana SHPO. Questions about archaeological issues should be directed to Wade T. Tharp at (317) 232-1650 or wtharp1@dnr.IN.gov. Questions about historic buildings or structures pertaining to this review should be directed to Danielle Kauffmann at (317) 232-0582 or dkauffmann@dnr.IN.gov.

For the benefit of those recipients of a copy of this letter who are not Section 106 consulting parties, please be aware that the documents discussed here can be found online in IN SCOPE at <http://erms.indot.in.gov/Section106Documents/>. From there, search by this project's designation number: 1601029.

Anyone receiving an e-mailed copy of this letter who does *not* wish to receive future copies of our correspondence about this bridge project is asked to reply to wtharp1@dnr.in.gov and dkauffmann@dnr.in.gov and so advise us.

In all future correspondence regarding the dual review of this project involving the SR 75 over Wildcat Creek Bridge Project in Democrat Township, Carroll County, Indiana (Des. No. 1601029), please refer to DHPA No. 23247.

Very truly yours,



Beth K. McCord
Deputy State Historic Preservation Officer

BKM:DMK:WTT:wt

emc: Joyce Newland, FHWA
Michelle Allen, FHWA
Anuradha Kumar, INDOT
Shaun Miller, INDOT
Susan Branigin, INDOT
Mary Kennedy, INDOT
Shirley Clark, INDOT
Tommy Kleckner, Indiana Landmarks, Western Field Office
Emily Royer, Indiana Landmarks, Western Field Office
James L. Cooper, Ph.D., Professor Emeritus of History, DePauw University
Paul Brandenburg, Indiana Historic Spans Task Force
Mary Mears, Carroll County Historic Bridge Coalition
Bonnie Maxwell, Heartland Heritage, Inc.
Dan McCain, Wabash & Erie Canal Association
William R. Brown, Carroll County Commissioners
Paul Coutts, Carroll County Highway Superintendent/Engineer
Linda Weintraut, Weintraut & Associates, Inc.
Leah Boits, American Structurepoint, Inc.
Hillary Shaffer, American Structurepoint, Inc.
J. Scott Keller, Review Board
Daniel Kloc, AIA, Review Board
Jason Larrison, AIA, Review Board
Joshua Palmer, AIA, Review Board
April Sievert, Ph.D., Review Board
Christopher Smith, Deputy Director, INDNR
Beth K. McCord, INDNR-DHPA
Wade T. Tharp, INDNR-DHPA
Danielle Kauffmann, INDNR-DHPA

Fwd: FW: FHWA Project: Des. No. 1601029; SR 75 Bridge Project, Carroll County, Indiana

1 message

From: Allison Daniels [mailto:Allison.Daniels@fcpotawatomi-nsn.gov]

Sent: Thursday, December 20, 2018 3:03 PM

To: Miller, Shaun (INDOT) <smiller@indot.IN.gov>

Cc: Michelle Allen <michelle.allen@dot.gov>

Subject: RE: FHWA Project: Des. No. 1601029; SR 75 Bridge Project, Carroll County, Indiana

**** This is an EXTERNAL email. Exercise caution. DO NOT open attachments or click links from unknown senders or unexpected email. ****

Re: FHWA Project: Des. No. 1601029; SR 75 Bridge Project, Carroll County, Indiana.

Dear Mr. Miller,

Pursuant to consultation under Section 106 of the National Historic Preservation Act (1966 as amended) the Forest County Potawatomi as a Federally Recognized Native American Tribe reserves the right to comment on Federal undertakings, as defined under the act. Thank you for your participation in the process.

This response is regarding the projects mentioned above. Based on archaeology report provided, the Tribal Historic Preservation Office on behalf of Forest County Potawatomi Community would like to offer the finding of NO HISTORIC PROPERTIES AFFECTED. In the event any archaeological artifacts or human remains are uncovered during construction, demolition, or earthmoving activities, state law (Indiana Code 14-21-1-27 and 29) is followed and in addition that work in the immediate vicinity of the find halt and the Tribal Historic Preservation Office is contacted for consultation regarding treatment and handling protocols prior to removal.

Your interest in protecting cultural and historic properties is appreciated. If you have any questions or concerns, please contact me at the phone number or email address listed below.

Respectfully,

Allison Daniels

Assistant Tribal Historic Preservation Officer

Forest County Potawatomi Community

Land & Natural Resource Division, PO Box 340 Crandon, WI 54520

Office: (715) 478-4704 | **Fax:** (715) 478-7225 | Allison.Daniels@fcpotawatomi-nsn.gov

Please note the office hours are Monday through Thursday, 7:00 am – 5:00 pm. Our office is closed on Fridays.

Re: FHWA Project: Des. No. 1601029; SR 75 Bridge Project, Carroll County, Indiana

1 message

Linda Weintraut <linda@weintrautinc.com>

Sun, Mar 31, 2019 at 8:27 PM

To: indianabridges@sbcglobal.net

Cc: "Kumar, Anuradha" <akumar@indot.in.gov>, "Kennedy, Mary" <mkennedy@indot.in.gov>, "Slider, Chad" <CSlider@dnr.in.gov>, bhope@structurepoint.com

Thank you, Paul. We will add you to the list.

Best, Linda

On Mar 31, 2019, at 7:29 PM, <indianabridges@sbcglobal.net> <indianabridges@sbcglobal.net> wrote:

Dear Ms. Weintraut;

Please forgive the delay in response to your original message. Indiana Historic SPANs Taskforce respectfully requests Consulting Party Status to Des. No.: 1601029 - SR 75 over Wildcat Creek Bridge Project.

My thanks for reaching out with the critical initial information and again my apology for the delay in responding.

Please let me know if you have any questions.

Cheers,

Paul

From: Linda Weintraut <linda@weintrautinc.com>

Sent: Monday, November 19, 2018 2:45 PM

Subject: FHWA Project: Des. No. 1601029; SR 75 Bridge Project, Carroll County, Indiana

Des. No.: 1601029

Project Description: SR 75 over Wildcat Creek Bridge Project

Location: Carroll County, Indiana

The Indiana Department of Transportation (INDOT), with funding from the Federal Highway Administration, proposes to proceed with the SR 75 over Wildcat Creek Bridge project (Des. No. 1601029). American Structurepoint, Inc. is under contract with INDOT to advance the environmental documentation for the referenced project.

Section 106 of the National Historic Preservation Act requires Federal agencies to take into account the effects of their undertakings on historic properties. The following agencies/individuals are being invited to become consulting parties:

Indiana State Historic Preservation Officer
Carroll County Commissioners
Tippecanoe County Area Plan
Indiana Landmarks, Western Regional Office
Carroll County Historical Museum
Carroll County Historian
Carroll County Highway Department Superintendent/Engineer
James Cooper, DePauw University
Historic Spans Task Force
Eastern Shawnee Tribe of Oklahoma
Forest County Potawatomi Community
Peoria Tribe of Indians of Oklahoma
Pokagon Band of Potawatomi Indians

This letter is part of the early coordination phase of the environmental review process requesting comments associated with this project. We are requesting comments from your area of expertise regarding any possible environmental effects associated with this project. Please use the above DES Number and project description in your reply and your comments will be incorporated into the formal environmental study.

Please review the letter, Short Historic Property Report, and Archaeology Short Report (Tribes only) located in IN-SCOPE at <http://erms.indot.in.gov/Section106Documents/> (the Des No. is the most efficient search term, once in IN-SCOPE), and respond with your comments on any historic resource impacts

Re: FHWA Project: Des. No.1601029 SR 75 Bridge Project, Carroll County Indiana

Linda Weintraut <linda@weintrautinc.com>

Sat, Apr 6, 2019 at 10:20 AM

To: Joe Mayfield <joe.mayfield5@gmail.com>

Cc: akumar@indot.in.gov, mkennedy@indot.in.gov, cslider@dnr.in.gov, natali <bethany@weintrautinc.com>

We will add you to the list.

Linda Weintraut

> On Apr 6, 2019, at 7:35 AM, Joe Mayfield <joe.mayfield5@gmail.com> wrote:

>

> Linda,

>

> The Delphi Preservation Society is a 25 year old non-profit organization dedicated to helping preserve historic structures in Delphi and Carroll county. We would, if still possible, like to be considered as a consulting party for this project.

>

> Thank you,

>

> Joe Mayfield, Delphi Preservation Society

Fwd: FHWA Project: DES 161029; SR 75 Bridge Project, Carroll County, Indiana

1 message

Linda Weintraut <linda@weintrautinc.com>
To: bethany w <bethany@weintrautinc.com>

Mon, Apr 8, 2019 at 9:34 AM

----- Forwarded message -----

From: **Mark Smith** <markalansmith12171948@gmail.com>

Date: Sun, Apr 7, 2019 at 3:18 PM

Subject: Re: FHWA Project: DES 161029; SR 75 Bridge Project, Carroll County, Indiana

To: Linda Weintraut <linda@weintrautinc.com>

Cc: <akumar@indot.in.gov>, <mkennedy@indot.in.gov>, <CSlider@dnr.in.gov>, <bhope@structurepoint.com>

Linda and all-thanks much. --Mark.

On Sun, Apr 7, 2019 at 3:00 PM Linda Weintraut <linda@weintrautinc.com> wrote:

Thanks for your interest. We will add you to the list.

Linda Weintraut

> On Apr 7, 2019, at 1:35 PM, Mark Smith <markalansmith12171948@gmail.com> wrote:

>

> Dear Sirs: Due to my interest in local history and bridge preservation, I am requesting that I be considered as a consulting party in the matter of the State Road 75 Bridge Project.

> Historically Yours,

> Mark A. Smith, Carroll County Historian.

--

Linda Weintraut, Ph.D.
Weintraut & Associates, Inc.
PO Box 5034
4649 Northwestern Drive
Zionsville, Indiana 46077
317.733.9770 ext. 310

www.weintrautinc.com

Re: FHWA Project: Des. No. 1601029; SR 75 Bridge Project; Carroll County, Indiana.

Linda Weintraut <linda@weintrautinc.com>

Tue, Apr 9, 2019 at 4:36 PM

To: gnyberg@nicheslandtrust.org

Cc: "Kumar, Anuradha" <akumar@indot.in.gov>, mkennedy@indot.gov, "Slider, Chad" <cslider@dnr.in.gov>, "Hope, Briana" <bhope@structurepoint.com>, bethany w <bethany@weintrautinc.com>

Thank you for your email. We shall add your name to the list of consulting parties.

On Tue, Apr 9, 2019 at 4:28 PM <gnyberg@nicheslandtrust.org> wrote:

TO Whom it may concern,

I wish to express NICHES Land Trust's appreciation and concern for the fate of the Cutler Bridge. NICHES Land Trust protects lands up and down the creek and encourage people to paddle through the maintenance of access points. The interesting bridges of the creek are one of the many and various interesting cultural features along with the natural feel. In our opinion, it would be a shame to lose a bridge with such character.

Please keep me updated as to the progress and fate of the bridge.

Thanks.

Gus Nyberg

Executive Director

NICHES Land Trust

765-423-1605 Office

765-427-4345 Cell

--

Linda Weintraut, Ph.D.
Weintraut & Associates, Inc.
PO Box 5034
4649 Northwestern Drive
Zionsville, Indiana 46077
317.733.9770 ext. 310

www.weintrautinc.com

Fwd: FHWA Project: Des. No. 1601029; SR 75 Bridge Project; Carroll County

1 message

Linda Weintraut <linda@weintrautinc.com>
To: natali <bethany@weintrautinc.com>

Sat, Apr 13, 2019 at 3:54 PM

Linda Weintraut

Begin forwarded message:

From: <dan.mccain@gmail.com>
Date: April 13, 2019 at 10:35:03 AM EDT
To: <linda@weintrautinc.com>
Cc: <akumar@indot.in.gov>, <mkennedy@indot.in.gov>, <CSlider@dnr.in.gov>
Subject: FHWA Project: Des. No. 1601029; SR 75 Bridge Project; Carroll County

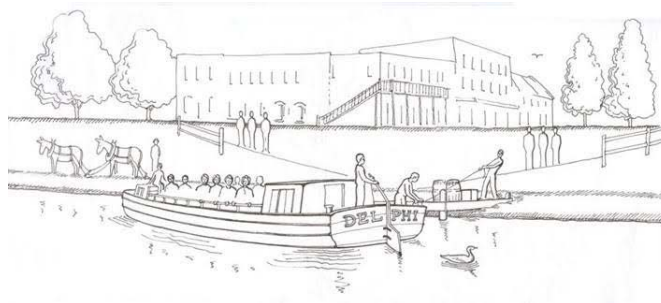
Dear Ms. Weintraut:

I represent the Carroll County Wabash & Erie Canal, Inc. as President of this 501-c-3 organization. We would like to be recognized at a Consulting Party to the project named above in the Subject line. That bridge is a significant span worthy of protecting its integrity. Sorry to be later in the process but we wish now to be included as a consulting party.

The Wabash & Erie Canal runs through Carroll County and Delphi and we have Canal Park which features the historic waterway (much as the historic and scenic Wildcat Creek). And we, the Canal Association directors/members, have acquired and restored three historic trail bridges spanning the canal much like the SR 75 iron bridge over the Wildcat Creek near Cutler.

Our long range goals include preservation and development of sections of the Wabash & Erie Canal and making the public aware of its historical connections. We entertain thousands of visitors and school children annually. Our facilities are owned and controlled by this organization and we own/control three miles of the old waterway and towpath including the public trails.

We see the preservation of metal bridges and the public enlightenment of historically significant structures as important to those who support our 50 year old organization. Please include us in the listing of consulting parties. Thank you, DAN MCCAIN, President



Fwd: Bridge restoration projects for Delphi Historic Trails

1 message

Mon, Apr 15, 2019 at 8:30 AM

Begin forwarded message:

From: <dan.mccain@gmail.com>
Date: April 14, 2019 at 9:55:27 AM EDT
To: <linda@weintrautinc.com>, <mkennedy@indot.in.gov>, <akumar@indot.in.gov>, <CSlider@dnr.in.gov>
Subject: Bridge restoration projects for Delphi Historic Trails

Linda, Mary Anuradha and Curt:

Recently I send you a request for the Wabash & Erie Canal organization to be a consulting party on the highway 75 bridge at Cutler. We are restoration enthusiasts and have had first-hand experience with saving historic iron bridges so we are especially interested in INDOT doing that with the Cutler bridge as well.

Last month I responded to a request from a person in an eastern state to highlight what can be done in restoring old iron bridges. [See the pictures and commentary of works we have accomplished below](#). Some of this might add creditability to our request to become a consulting party as we do care about old iron (and wrought iron) bridges. DAN

From: dan.mccain@gmail.com <dan.mccain@gmail.com>
Sent: Saturday, March 30, 2019 11:31 PM
To: 'William Gerber' <wegerber@icloud.com>
Cc: 'mike riley' <mriley20@twcny.rr.com>; INDCANAL@aol.com
Subject: Bridge restoration projects for Delphi Historic Trails

Bill:

Very interesting that you talked with Emiline Young yesterday from Rhode Island that both cares about old bridges and cares about how they can be restored/preserved. We found ourselves in this business in Carroll County, Indiana of finding a bridge when we had a need within the Canal Association's emerging trail system that began in the late 1980s.

First of all was an alliance with a retired liberal college history professor from 75 miles away. He really was more of an structural engineer in his heart. We bonded and he started taking me along on some speaking engagements to historical organizations, etc.

I liked his take on the beauty and artistry of many of the old spans. He had written two books highlighting his inventory of hundreds of Indiana bridges—mostly from 1870s through 1930s. I was intrigued with his findings.

He led me to one of those he had inventoried within 15 miles of Canal Park and it was abandoned and land and bridge turned back to the farmer/landowner. Once I saw it I knew we needed it for our trail crossing. It was an 1873 wrought iron "bowstring" arch.



The professor knew our volunteer construction crew at the Canal had talent to do some of the basic restoration steps but likely lacked the knowledge of metal restoration – especially wrought iron restoration. He introduced me to a vocational training specialist from Central Michigan and that got us all enthused.



The specialist was semi-retired and would come to Delphi with his wife and spend a couple days every couple months. We learned about the properties of wrought iron and how to straighten and repair. And we learned something special - How to HOT REVET metal parts together.



The restoration of our first span was in 1998-99 is called the RED BRIDGE and our Canal Boat goes under it on cruises.



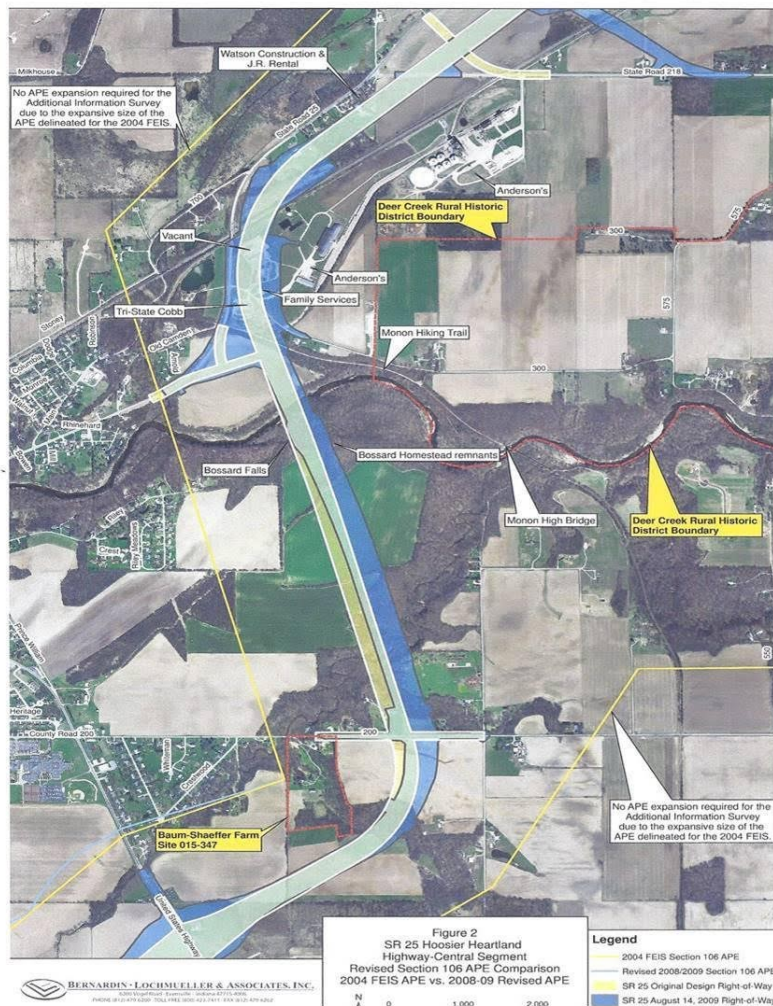
Then we got more bold and searched for another very special bridge and the professor knew of a Stearns Truss that span too was abandoned 60 miles north of Delphi. It became the BLUE BRIDGE for a trail over the canal waterway in 2006.



And then we had needs for a crossing within our Canal Park and it had to be wider than we had dealt with in the first two crossings. This 132 foot iron span also needed straightening and riveting but would be taken apart for us by INDOT's contractor which was replacing this bridge with a concrete beam new span. This span is known as the GRAY BRIDGE in Canal Park.

That project was bigger than anything we had done before but we learned how to handle larger pieces for repairs, painting and completing. However we had more money in this because of its size. We were fortunate to get two grants and donations to raise the \$175,000 needed to cover costs.

All three of these bridges were completed primarily with our volunteer workers and hiring crane operators to do the things we couldn't handle. Comparing our costs to what is often quoted for costs of restoration where the work is all done by professional contractors seems that we get our bridges finished for 20-30 percent of those costs.



The last historic bridge the Canal took on was to replace a section of our Monon Trail where INDOT severed the trail in building a new four lane super highway. We had held INDOT's feet to the fire with insistence that there must be a proper crossing to keep our Monon rail trail intact.

We won this argument to create a bridge span but the caveat was that we "had to locate and acquire a bridge to suite our needs" and we did again by going through our history professor—the one that found our first three iron spans. He knew of this long special span that had been over the White River southwest of Indianapolis.

The latest was the longest at 300 feet (to cross the four lanes) and the owner of this span was, Conner Prairie, a historical organization near Indianapolis that had removed the bridge from Freedom, Indiana and held it until they realized it wasn't possible to put it across the White River—so it was donated to us by Conner Prairie and INDOT put it in!



INDOT contracted to do the work of restoration, assembly and installation but that was a blessing for us because we volunteers didn't have any business placing 300 foot bridge across a four lane highway. It became known as the FREEDOM BRIDGE and is a glorious gateway unlike the multitude of modern concrete beam bridges on Interstates everywhere.

Throughout our restoration efforts we have enjoyed other specialists who also became Canal volunteers like the history professor and the metal restoration specialist and even the retired professional mason from 75 miles away that taught us how to lay stone to face our bridge piers.

I hope this helps describe what transpired with the Canal Association's acquisition, restoration and installation of these trail bridges. DAN

From: William Gerber <wegerber@icloud.com>
Sent: Saturday, March 30, 2019 8:56 PM
Subject: Bridge restoration

Hi Dan,

At a Society for Industrial Archaeology symposium, today, Emaline Young (working on her Masters in Historical Preservation at Roger Williams University in RI) presented her research into how to preserve a bridge vaguely like the one you and your MWF gang took down, refurbished and relocated. Apparently no one in RI DoT has been able to figure out how to restore it.

I suggested that she talk with you and/or gang to hear about what you did and how you did it, etc. I hope you don't mind, I gave her your contact information, suggested she call you (and blame it on me). So, my plea, please help her out in whatever way you can.

Hope all is well with you and yours.

Bill G.

From Gerb's iPad.

--
Bethany Natali
Historian
Weintraut & Associates, Inc.
PO Box 5034
Zionsville, Indiana 46077
T: (317) 733-9770 ext. 311
F: (317) 733-9773

Fwd: Linda, I am the president of the Carroll County Historical Society and would like to be a consulting party for the St. Rd. 75 bridge project. Randy Myers

1 message

Mon, Apr 15, 2019 at 2:13 PM

From: Rebecca Myers <myersrl1950@gmail.com>

Date: April 15, 2019 at 1:30:34 PM EDT

To: linda@weintrautinc.com

Subject: Linda, I am the president of the Carroll County Historical Society and would like to be a consulting party for the St. Rd. 75 bridge project. Randy Myers

Re: FHWA Project: Des. No. 1601029; SR 75 Bridge Project; Carroll County, Indiana

1 message

Linda Weintraut <linda@weintrautinc.com>

To: Al Auffart <amauffart@gmail.com>

Cc: mkennedy@indot.in.gov, CSlider@dnr.in.gov, akumar@indot.in.gov, bhope@structurepoint.com

Wed, Apr 17, 2019 at 7:37 PM

Thank you for your email. We shall add you to the list.

Linda Weintraut

On Apr 17, 2019, at 7:20 PM, Al Auffart <amauffart@gmail.com> wrote:

Dear Ms. Weintraut,

My name is Al Auffart and I am President of Adams Mill Inc. We are a not-for-profit operation and operate a historic grist mill just upstream on the Wildcat Creek from the subject bridge. If possible, we would like to be included as a Consulting Party for the subject project as it is very near our facilities and relates directly to our interest in local history and utilization of Wildcat Creek. My contact information is amauffart@gmail.com and my address is [5283 S 225 E](#), Cutler, IN 46920. Home phone is (765) 268-2530. Thank you.

Re: FHWA Project: Des. No. 1601029; SR 75 Bridge Project; Carroll County, Indiana

1 message

Linda Weintraut <linda@weintrautinc.com>

Thu, Apr 18, 2019 at 1:09 PM

To: Julia Leahy <Julia@carrollcountychamber.com>

Cc: "akumar@indot.in.gov" <akumar@indot.in.gov>, "mkennedy@indot.in.gov" <mkennedy@indot.in.gov>, "cslider@dnr.in.gov" <cslider@dnr.in.gov>, "Hope, Briana" <bhope@structurepoint.com>, bethany w <bethany@weintrautinc.com>

Thank you for your email. We shall add you to the list.

On Thu, Apr 18, 2019 at 12:52 PM Julia Leahy <Julia@carrollcountychamber.com> wrote:

Ms. Wintraut:

My apologies for being so late, but our organization serves as the tourism authority for Carroll County and we would like to be kept in the loop as a Consulting Party for the bridge project in Cutler. Historic bridges are a very popular tourist attraction in our county and would like to know more about the fate of this structure.

Thank you so much,
Julia Leahy
Executive Director
Carroll County Chamber of Commerce

FHWA Project: Des. No. 1601029; SR 75 Bridge Project; Carroll County, Indiana.

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Linda Weintraut, Ph.D.
Weintraut & Associates, Inc.
PO Box 5034
[4649 Northwestern Drive](#)
[Zionsville, Indiana 46077](#)
317.733.9770 ext. 310

www.weintrautinc.com

Re: FHWA Project: Des. No. 1601029; SR 75 Cutler Bridge Project; Carroll County, Indiana.

Linda Weintraut <linda@weintrautinc.com>

Fri, Apr 19, 2019 at 11:20 AM

To: Mary <maryloublueeyes@gmail.com>

Cc: bethany w <bethany@weintrautinc.com>, "Hope, Briana" <bhope@structurepoint.com>, "Kennedy, Mary" <mkennedy@indot.in.gov>, "Kumar, Anuradha" <akumar@indot.in.gov>

Thank you, Mary. We will add you to the list of consulting parties

On Fri, Apr 19, 2019 at 11:14 AM Mary <maryloublueeyes@gmail.com> wrote:

Our organization, Promoting Wildcat Valley is a nonprofit group formed in 2008 in southern Carroll County devoted to preservation and conservation of all aspects of our history and environment. At the very first meeting, we discussed 14 potential projects, 12 of which seemed quite feasible. An early one involved working with students and faculty from Purdue University Horticulture and Landscape Architecture Department laying out potential trails and a wetland restoration in the Wildcat Creek Oxbow adjacent to the 1845 Adams Mill. Since then we have accomplished many projects, installed several historical markers, instituted a neighborhood watch, published books, restored a church bell to its cemetery, held narrated tours of the Wildcat area, supported many sorts of activities at Adams Mill, and took ownership of an early stagecoach hotel on the Michigan Road. We've done many fundraisers of all sorts and received several substantial grants.

The following is the purpose statement from our bylaws:

- Supporting the history and historic structures of Carroll County's Wildcat Valley;
- Maintaining an association of persons interested in Carroll County's Wildcat Valley;
- Focusing public attention on Carroll County's Wildcat Valley;
- Stimulating the use of the Carroll County Wildcat Valley resources and tourism;
- Supporting and cooperating with local services and facilities for the community;
- Preserving community history for future generations.

We are very interested in the fate of the Cutler bridge mentioned above and hope that you will include us among your list of Consulting Partners. The bridge is an important part of what we hold dear.

Sincerely,

Mary Blue, President
Promoting Wildcat Valley

--

Linda Weintraut, Ph.D.
Weintraut & Associates, Inc.
PO Box 5034
4649 Northwestern Drive
Zionsville, Indiana 46077
317.733.9770 ext. 310

www.weintrautinc.com

Re: FHWA Project: Des. No. 1601029; SR 75 Bridge Project; Carroll County, Indiana.

1 message

Linda Weintraut <linda@weintrautinc.com>

Tue, Apr 23, 2019 at 11:43 AM

To: "Brier, Dale" <dbrier@dnr.in.gov>

Cc: "Kumar, Anuradha" <akumar@indot.in.gov>, "Kennedy, Mary" <MKENNEDY@indot.in.gov>, "Slider, Chad (DNR)" <CSlider@dnr.in.gov>, "Hope, Briana" <bhope@structurepoint.com>, bethany w <bethany@weintrautinc.com>

Dale,
Thank you for your email. We will add you to the list.

Linda

On Tue, Apr 23, 2019 at 11:30 AM Brier, Dale <dbrier@dnr.in.gov> wrote:

Linda,

Please add me to the interested parties for this bridge project.

Thank you,

Dale Brier

Division Director

Division of Outdoor Recreation
Department of Natural Resources
402 W. Washington St. W271
Indianapolis IN, 46204
317-232-4072
www.IN.gov/dnr/outdoor



--

Linda Weintraut, Ph.D.
Weintraut & Associates, Inc.
PO Box 5034
4649 Northwestern Drive
Zionsville, Indiana 46077
317.733.9770 ext. 310

www.weintrautinc.com

Fwd: FW: Wildcat Guardians response-FHWA Project: Des. No. 1601029 S.R. 75 Bridge Project, Carroll County Indiana
1 message

----- Forwarded message -----

From: **Kennedy, Mary** <MKENNEDY@indot.in.gov>

Date: Wed, May 15, 2019 at 10:11 AM

Subject: FW: Wildcat Guardians response-FHWA Project: Des. No. 1601029 S.R. 75 Bridge Project, Carroll County Indiana

To: Linda Weintraut <linda@weintrautinc.com>

FYI - I just realized although addressed to you, your email is not in the "to" line. Chad's email is listed twice.

From: Rick Parsons [mailto:rpars17542@aol.com]

Sent: Tuesday, May 14, 2019 1:44 PM

To: Slider, Chad (DNR) <CSlider@dnr.IN.gov>; Kumar, Anuradha <akumar@indot.IN.gov>; Kennedy, Mary <MKENNEDY@indot.IN.gov>; Slider, Chad (DNR) <CSlider@dnr.IN.gov>

Subject: Wildcat Guardians response-FHWA Project: Des. No. 1601029 S.R. 75 Bridge Project, Carroll County Indiana

**** This is an EXTERNAL email. Exercise caution. DO NOT open attachments or click links from unknown senders or unexpected email. ****

Dear Linda,

My name is Rick Parsons and I am the president of the Wildcat Guardians. We are very involved in the protection of the Wildcat Creek and it's watershed. We are involved in education, cleanups, water monitoring, and much more. I am requesting that our group be a consulting partner on the Highway 75 bridge. Thanks for your consideration in this matter.

Sincerely, Rick

--
Linda Weintraut, Ph.D.
Weintraut & Associates, Inc.
PO Box 5034
[4649 Northwestern Drive](mailto:linda@weintrautinc.com)
[Zionsville, Indiana 46077](mailto:linda@weintrautinc.com)
317.733.9770 ext. 310

www.weintrautinc.com

Fwd: FHWA Project: Des. No. 1601029; SR 75 Bridge Project, Carroll County, Indiana

1 message

Sat, Sep 7, 2019 at 10:57 AM

----- Forwarded message -----

From: **Kennedy, Mary** <MKENNEDY@indot.in.gov>

Date: Fri, Sep 6, 2019 at 2:40 PM

Subject: RE: FHWA Project: Des. No. 1601029; SR 75 Bridge Project, Carroll County, Indiana

To: abjakes@yahoo.com <abjakes@yahoo.com>

Cc: Linda Weintraut <linda@weintrautinc.com>, Hockaday, Jacqueline R <JHockaday@indot.in.gov>, Branigin, Susan <SBranigin@indot.in.gov>

Commissioner Brown,

It was good to talk with you today about this project. For the benefit of those copied and our record-keeping, I will summarize your comments with the following points:

- You are in favor of keeping this bridge for continued vehicular use.
- You live near the bridge and you have not heard from any businesses or farmers who think the bridge does not meet their needs.
- From the feedback you've received, the citizenry of Carroll County is in favor of keeping this bridge for continued vehicular use.
- Potential detour routes for this bridge are long and/or not appropriate for larger vehicles (one includes a covered bridge).
- The best way to reach you via email is abjakes@yahoo.com.

Please feel free to let us know if you have any other information for our consideration or if I have incorrectly summarized anything above. As I mentioned in our conversation, the next project document to go out for the review process will likely be the historic bridge alternatives analysis (HBAA) within the next month or so. You will be added to the list for project notifications moving forward.

Regards,

Mary E. Kennedy

Historic Bridge Specialist

100 N. Senate Ave., Room N642-ES

Indianapolis, IN 46204

Office: (317) 232-5215

Email: mkennedy@indot.in.gov



****Updated guidance for historic bridge projects can be found in the links below:**

[Overview-Indiana Historic Bridges Program](#)

[Historic Bridge Project Development Process](#)

Procedures for Public Hearings under the Historic Bridges PA

*For the latest updates from INDOT's Cultural Resources Office, subscribe to the Environmental Services listserv: <https://www.in.gov/indot/3217.htm>

From: Kennedy, Mary
Sent: Wednesday, August 14, 2019 11:15 AM
To: commissioner3@carrollcountyin.gov
Cc: Linda Weintraut <linda@weintrautinc.com>; Hockaday, Jacqueline R <JHockaday@indot.IN.gov>; Branigin, Susan <SBranigin@indot.IN.gov>
Subject: RE: FHWA Project: Des. No. 1601029; SR 75 Bridge Project, Carroll County, Indiana

Commissioner Brown,

I received your voicemail that you wanted to talk about this bridge. I have called back a couple of times with no answer and a message that says no voicemail has been set up for that phone number. So, I thought I'd try to reach out via email instead. Since the email below in November distributing an early coordination letter & historic properties report to potential consulting parties for this project, no other information has been sent out. If you would like to be a consulting party on this project so that you will be notified via email when any new documents are distributed for review, we can do that. Please let us know if this email address is the best way to reach you for such notices, and you will be added to the list for all notifications moving forward.

Also, feel free to call me if you want to discuss further. I will be in the office until 4:15 today. I will be in 8:15-4:15 tomorrow, but I do have meetings at 9:00 AM & 1:30 PM.

Regards,

Mary E. Kennedy
Historic Bridge Specialist

100 N. Senate Ave., Room N642-ES

Indianapolis, IN 46204

Office: (317) 232-5215

Email: mkennedy@indot.in.gov



** Historic Property Report (HPR) guidelines can be found [here](#)

*Design Memorandum 18-02 regarding the procedures for Historic Bridge Alternatives Analysis Documents can be found here: <http://www.in.gov/dot/div/contracts/standards/memos/2018/18-02%20ta%20Historic%20Bridge.pdf>

*For the latest updates from INDOT's Cultural Resources Office, subscribe to the Environmental Services listserv: <https://www.in.gov/indot/3217.htm>

FHWA Project: Des. No. 1601029; SR 75 Bridge Project, Carroll County, Indiana- additional CP

1 message

Kennedy, Mary <MKENNEDY@indot.in.gov>

Thu, Oct 3, 2019 at 2:17 P

To: Linda Weintraut <linda@weintrautinc.com>

Cc: Bethany Natali <bethany@weintrautinc.com>, Doug Fivecoat <dfivecoat@weintrautinc.com>, "Hockaday, Jacqueline R" <JHockaday@indot.in.gov>, "Kauffmann, Danielle M" <DKauffmann@dnr.in.gov>, "Tharp, Wade" <WTharp1@dnr.in.gov>, "Hshaffer@structurepoint.com" <Hshaffer@structurepoint.com>, Nathan Holth <nathan@historicbridges.org>

Hi Linda,

You probably saw the attached email from our listserv last week regarding additional potential consulting parties for historic bridge projects. Nathan Holth, of HistoricBridges.org, has requested to be a consulting party on this project. Please be sure to include him on all consulting party correspondence moving forward. I have copied him on this email & his contact information is below.

Nathan Holth
Author/Photographer/Webmaster
HistoricBridges.org
2767 Eastway Drive
Okemos, MI, 48864
(269) 290-2593
nathan@historicbridges.org

Thank you,

Mary E. Kennedy

Historic Bridge Specialist

100 N. Senate Ave., Room N642-ES

Indianapolis, IN 46204

Office: (317) 232-5215

Email: mkennedy@indot.in.gov



****Updated guidance for historic bridge projects can be found in the links below:**

RESOLUTION 2019-05

**A RESOLUTION
OF THE BOARD OF COMMISSIONERS OF CARROLL COUNTY
URGING THE INDIANA DEPARTMENT OF TRANSPORTATION TO MAINTAIN
THE ARCHED STEEL BRIDGE CONVEYING STATE ROAD 75 OVER WILDCAT
CREEK AND REQUESTING NOTICE OF ANY AND ALL PROCEEDINGS,
HEARINGS AND PROPOSED ACTIONS REGARDING SAID BRIDGE**

WHEREAS, it has been called to the attention of the Board of Commissioners of Carroll County ("Board") that the steel arch bridge conveying State Road 75 over Wildcat Creek ("the Bridge") has been proposed, recommended or scheduled to be replaced by a new bridge; and

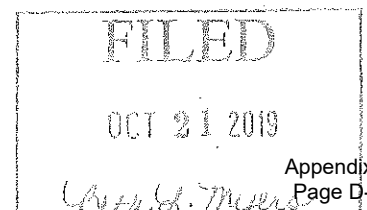
WHEREAS, the Board's attention has been called to the fact that the closing of the Bridge for demolition and replacement would necessitate the closing of the nearest Carroll County bridge crossing Wildcat Creek on County Road 50E due to its rated gross weight limit of 9000 pounds; and

WHEREAS, the nearest Carroll County bridge of sufficient weight bearing capacity crossing Wildcat Creek in the vicinity of the Bridge is the bridge on County Road 350E, a distance of 3 1/2 miles from State Road 75; and

WHEREAS, the predominant land use within a radius of 20 miles of the Bridge is grain and livestock farming; and

WHEREAS, the owners and tenants of farms north and south of Wildcat Creek have assured the Board that the Bridge safely and adequately accommodates modern farm machinery and implements; and

WHEREAS, the same owners and tenants have brought to the Board's attention their concerns about the great inconvenience and costs which would attend the lengthy disruption of preparation, planting, tilling, spraying and harvesting operations as well as the transportation of grain and livestock so long a distance away from State Road 75, which the demolition and replacement of the Bridge would cause; and



WHEREAS, the Board shares the concerns of the agricultural community who the Board joins in urging that the Bridge be maintained; and

WHEREAS, the citizens, private non-profit organizations and local governments of Carroll County have a long-standing record of commitment to the preservation of historic structures, particularly the eight Carroll County bridges which are listed on the State inventory of historic bridge; and

WHEREAS, the Board speaks as one of those mentioned organizations who encourage the State of Indiana to maintain historic structures when they are functional and economically important to their communities, as this Bridge is; and

WHEREAS, the Board introduced the concept of this Resolution at its regular meeting on September 16, 2019, placed this Resolution on the agenda for its October 21, 2019, meeting, entertained public comment thereon in advance of and during its meeting held October 21, 2019, and received no unfavorable comments;

NOW, THEREFORE,

BE IT RESOLVED, that the Board of Commissioners of Carroll County urges the Indiana Department of Transportation to maintain and not replace the bridge conveying State Road 75 over Wildcat Creek; and it is

FURTHER RESOLVED, that the Indiana Department of Transportation is requested to notify the Board of Commissioners in writing in advance of any and all proceedings, hearings or proposed actions regarding said bridge; and it is

FURTHER RESOLVED, that the Auditor is directed to transmit a signed copy of this Resolution via certified mail to the Indiana Department of Transportation with a covering letter providing the name, email address and telephone number of Commissioners Chairman Brown as the Board's contact person for this matter.

providing the name, email address and telephone number of Commissioners Chairman Brown as the Board's contact person for this matter.

The Auditor is directed to post copies of this resolution on bulletin boards outside the Meeting Room and near the elevator on the first floor of the Carroll County Courthouse until November 14, 2019.

ADOPTED AND ORDERED this 21 day of October 2019.

BOARD OF COMMISSIONERS OF CARROLL COUNTY

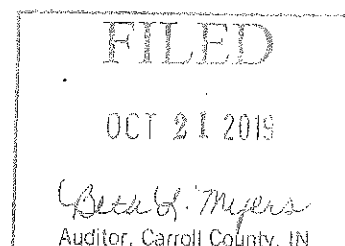
William R. Brown
William R. Brown, Chairman

Steve Pearson
Steve Pearson

Loren Hylton
Loren Hylton

ATTEST:

Beth L. Myers
Beth L. Myers, Auditor



This brochure featuring the historic bridges of Carroll County was developed to provide a self-guided automobile and cycling tour for both visitors and residents. A brief overview of the design and history of the bridges, as well as some photographs, provides for an educational opportunity while enjoying the beauty of the rural landscape.



Located in north-central Indiana, Carroll County is primarily rural and is dotted with several small towns, including the county seat of Delphi. There are 111 county owned bridges crossing major rivers such as the Wabash and Tippecanoe, as well as smaller creeks and tributaries. Of this large number of bridges, 17 historic spans remain usable for vehicles. Five more have been restored for pedestrian use. Four are located along Delphi Historic Trails, as well as the Tioga Bridge over Lake Freeman. Historic Trail maps are available at several Delphi locations. These 22 bridges have been identified as being historically significant. They vary in style and materials, from older stone arch and wooden covered bridges to those made from metal and concrete.

This bridge tour is divided into three routes plus one other individual span that you could add to your tour. Although it is not necessary to follow these routes, they are helpful for those who wish to see as many bridges as possible. Each route is recommended as a single outing, and if the map is followed, the user will remain on black-topped roads.

We hope you enjoy Carroll County's historic bridges and rural landscapes.

Brochure concept developed by Carroll County Wabash & Erie Canal Association, with guidance from James Cooper, PhD, Paul Brandenburg, Chair, Indiana Historic SPANS Taskforce, and promoted by Indiana Landmarks and Indiana Humanities Council. The purpose of this brochure is to promote awareness for advocating the preservation of Carroll County's significant historic bridges.

For more information about the county's historic bridges and related events, please contact:



Carroll County Wabash & Erie Canal Association
1030 West Washington Street
Delphi, Indiana 46923
www.wabashanderiecanal.org



INDIANA LANDMARKS



Funding for this brochure was provided by Indiana Landmarks, Indiana Humanities and the National Endowment for the Humanities.

Carroll County
INDIANA

Carroll County



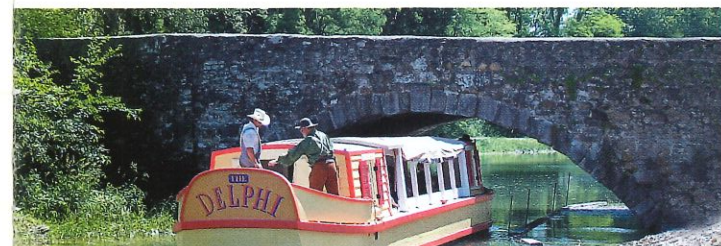
Tioga Trail Bridge over Lake Freeman

HISTORIC



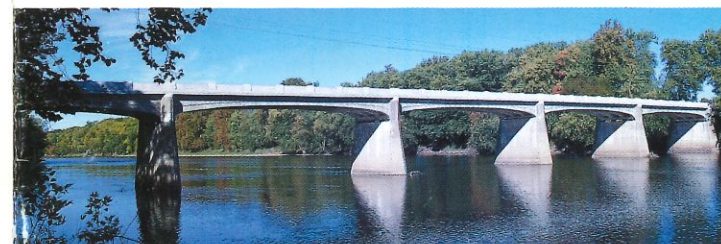
Rendering of Freedom Bridge on the Monon Trail

BRIDGE



Stone Arch Bridge at Canal Park

TOUR



Lockport Bridge over the Wabash River

A Driving & Cycling Tour

Metal Bridge Styles



WARREN



BOWSTRING ARCH - TRUSS



PRATT



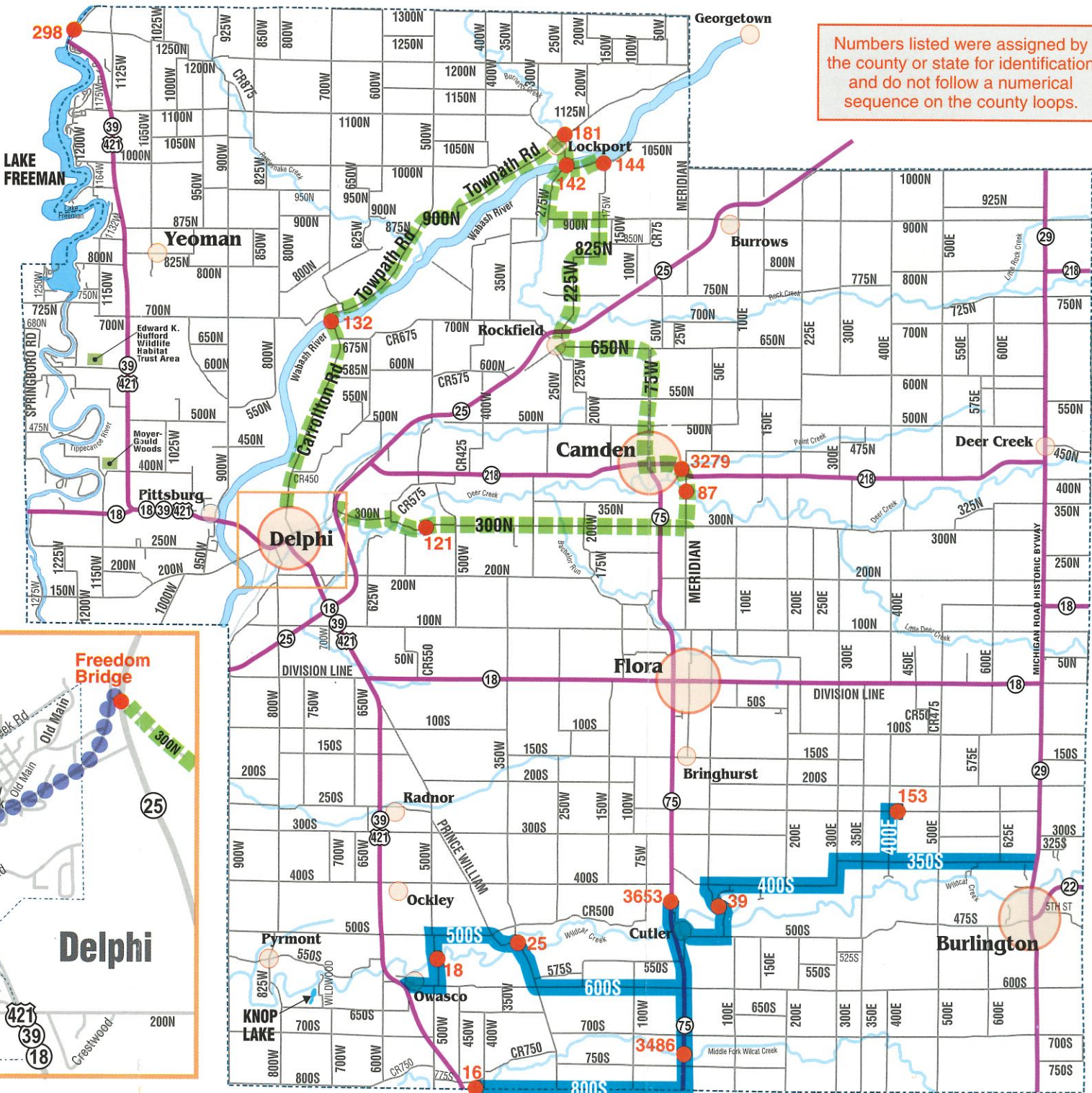
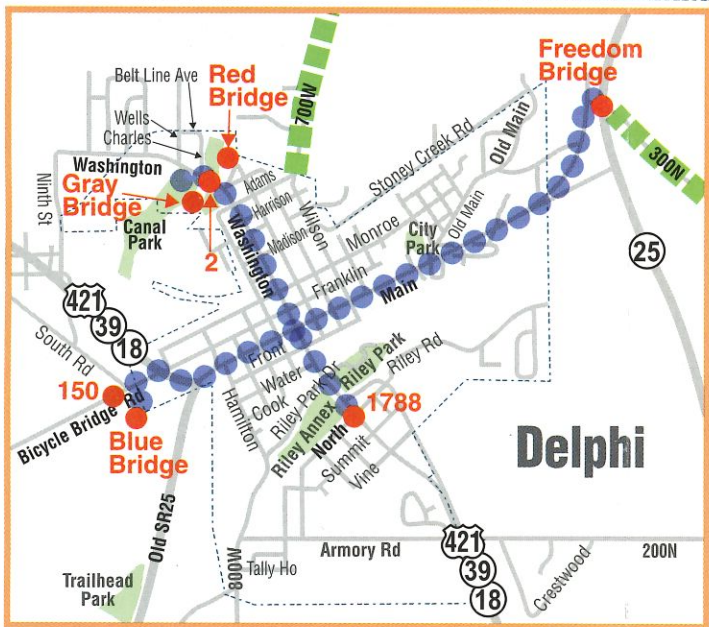
PARKER



PENNSYLVANIA



WHIPPLE



CARROLL COUNTY HISTORIC BRIDGE TOUR

BRIDGE TYPE: STONE COVERED METAL CONCRETE

SOUTHERN ROUTE

Wildcat Creek Corridor

BRIDGE	DATE/TYPE	LOCATION
18	1872 Howe Covered Truss	500W • 550S Wildcat Creek
25	1931 Parker Through Truss	350W • 525S Wildcat Creek
3653	1947 Parker Through Truss	SR75 • 230S Wildcat Creek
39	1873 Howe Covered Truss	50E • 440S Wildcat Creek
153	circa 1910 Lattice Pony Truss	250S • 410E Bachelor Run
3486	1942 Concrete Arch	SR75 • 750S Middle Fork/Wildcat Creek
16	1901 Pratt Through Truss	400W • 800S Middle Fork/Wildcat Creek

The first location number indicates the road where each bridge is located. The second number indicates the closest coordinate.

NORTHERN ROUTE

Wabash River/Deer Creek Corridor

BRIDGE	DATE/TYPE	LOCATION
132	1927 Concrete Arch	Carrollton Rd • 690N Wabash River
181	1840 Stone Arch 21'6"	Tow Path Rd • N of Lockport Burnetts Creek
142	1938 Continuous Concrete T-beam	SE of Lockport Wabash River
144	circa 1903 Stone Arch 20'6"	1025N • 175W Little Rock Creek
3279	circa 1940 Concrete Arch	SR218 • 425W Paint Creek
87	1932 Warren Pony Truss	Meridian Rd • 360N Deer Creek
121	1898 Pratt Thru Truss	300N • 510W Deer Creek
298	1890 Whipple Truss	1250N • 1150W Lake Freeman

No. 298 Tioga Bridge is a pedestrian bridge not located on any of the routes shown on the map, but would be best accessed by traveling north of Delphi on US 421.

DELPHI ROUTE

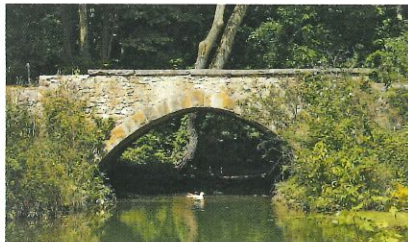
Wabash & Erie Canal Corridor

BRIDGE	DATE/TYPE	LOCATION
85	1874 (relocated) Bow String Arch Pony Truss	Red Bridge Settlement Canal Park Wabash & Erie Canal
2	1901 Stone Arch 33'3"	N Washington St. Wabash & Erie Canal
Gray Bridge	1913 (relocated) Pratt Through Truss	Canal Park Wabash & Erie Canal
150	1901 Stone Arch 24'0"	1901 W Franklin St Wabash & Erie Canal
Blue Bridge	1905 (relocated) Stearns Variant Through Truss	Delphi Historic Trails Wabash & Erie Canal
1788	1935 Concrete Ridged Frame	US421 • North Street Washington Street
Freedom Bridge	1898 (relocated) Pennsylvania Through Truss	Delphi Historic Trails SR25

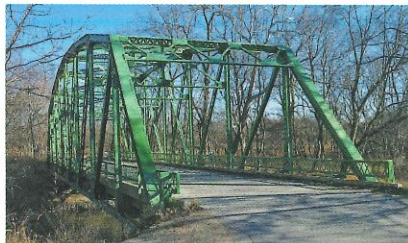
No. 85 Red Bridge, Gray Bridge, and No. 2 Washington Street Stone Arch are all easily accessible by walking the Underhill Towpath Trail from Canal Park.

- 2 This stone arch was built by John C. O'Connor across the Wabash & Erie Canal.
- 16 A Pratt through truss structure, this bridge was built by James Peirce of Delphi and the Attica Bridge Co.
Note: Access is limited since the bridge has been incorporated into a local golf course.
- 25 Xenia Bridge is a Parker truss designed by the Indiana State Highway Commission and relocated over Wildcat Creek in 1968.
- 87 This large Warren pony truss was designed by the Indiana State Highway Commission and moved to Deer Creek in 1968.
- 142 Designed in 1938 by Fort Wayne's city engineer, Chauncey McAnlis, the Lockport Bridge is Indiana's longest continuous T-beam bridge. The streamlining of its outer beams and their graceful arches give the bridge a distinctive Art Deco appearance. It has been restored.
- 144 This stone arch design was built across Little Rock Creek by John C. O'Connor in 1903.
- 150 This stone arch design was built by prominent local mason John C. O'Connor.
- 153 This small, skewed metal bridge is a rare survivor of a lattice pony-truss design.

- 298 Currently a pedestrian bridge, this superstructure spanning Lake Freeman in northern Carroll County, is the longest of a handful of Whipple trusses remaining in the state.
Note: 298 is a White County bridge number.
- 3279 In 1940, the State Highway Commission advertised the letting of a slightly-skewed, 65-foot, three-centered concrete arch on concrete pile bents to be erected over Paint Creek.
- 3486 In 1942, the State Highway Commission placed this plain concrete arch over Middle Fork of Wildcat Creek. With the nation engaged in the second World War, this was one of the very few bridges built that year and was one of two concrete arches built without reinforcing because all steel was reserved for military use.
- 3653 One of the latest examples of a fairly long Parker through truss span.
- Freedom Bridge The Lafayette Bridge Company fabricated this pin-connected Pennsylvania through truss span. This is the only remaining span of its type built by this once prolific Indiana firm. The structure is unusual for the extension of the twelve-panel pattern to sixteen with extra side sections. The bridge was dismantled in 2001, stored and subsequently donated to the City of Delphi and the Canal Association for use as a pedestrian bridge on the Monon trail over the Hoosier Heartland Highway.



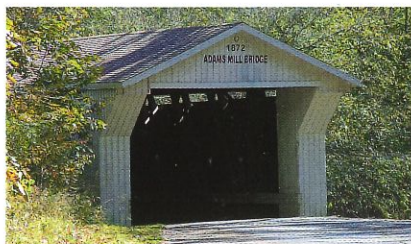
1901 Stone Arch over Wabash & Erie Canal (150)



1931 Xenia Bridge on Prince William Road over Wildcat Creek (25)



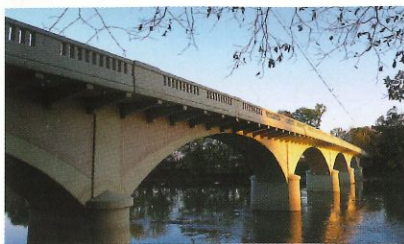
18 The Lancaster Covered Bridge is one of two remaining in the county built by the Wheelock Bridge Company in 1872 and featuring a Howe-truss design. This bridge is one of two in Indiana with a unique cast-iron abutment patented by Alpheus Wheelock in 1870. It has been restored.



39 Adams Mill Covered Bridge was built in 1873 and lays near the scenic mill and site it was named for. It features a Howe-truss design and spans Wildcat Creek near Cutler. It has also been restored.



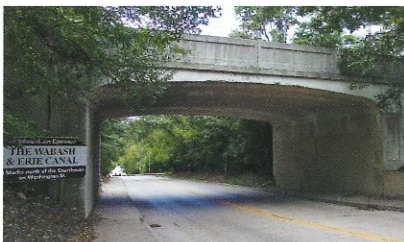
121 The wooded setting of Deer Creek is the location for Wilson's Bridge, named after an adjoining property owner who petitioned for its construction in 1898. The Lafayette Bridge Company fabricated the Pratt through truss structure for \$2,499. The bridge retains its original truss members and decorative latticed guardrails. Preservation efforts were successful in Wilson's Bridge restoration and reopening.



132 Prominent bridge designer Daniel Luten developed the plans for the six-span Carrollton Bridge adjacent to the site of the village of Carrollton. This place had prospered as a canal town, but since has disappeared. Built in 1926-27, the 615-foot long filled-spandrel arches carry a deck extending beyond the spandrels on wide brackets. Recently the focus of local preservation efforts, it has been restored.

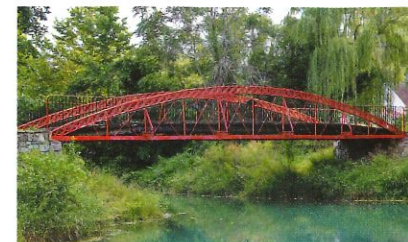


181 Burnett's Creek Arch, built around 1840, is Indiana's oldest bridge in continuous public use. This stone arch once carried the Wabash & Erie Canal over Burnett's Creek. When the Canal ceased operations, the county turned the towpath into a road.



1788 Here is the only example of a rigid frame span designed by the Indiana State Highway Commission. Also known as the North Street Viaduct, it replaced a 1908 structure.

Pedestrian Bridges



The **Red Bridge** is an 1874 Bowstring Pony Arch built to Joseph Davenport's patented design. It originally sat over Paint Creek east of Camden, Indiana, until volunteers brought it to Delphi and completely restored the graceful span. It serves as a trail bridge over the Wabash & Erie Canal.



The **Blue Bridge** was built in 1905 by the Winamac Bridge Company to a variant of W. E. Stearn's patented design. It is the last known Stearn's design existing in the United States. The bridge, including its four unique caisson columns, was brought from Pulaski County by volunteers in 2005 and restored to carry a pedestrian trail over the Wabash & Erie Canal.



The **Gray Bridge** is a 1913 Pratt through truss originally located in Putnam County and disassembled there. It was brought to Canal Park for restoration and placement over the Wabash & Erie Canal to link a pedestrian trail.



INDIANA DEPARTMENT OF TRANSPORTATION

100 North Senate Avenue
Room N642
Indianapolis, Indiana 46204

PHONE: (317) 234-5168

Eric Holcomb, Governor
Joe McGuinness, Commissioner

December 5, 2019

This letter was sent to the listed parties.

RE: Dual Review Project: SR 75 over Wildcat Creek Bridge Project (Bridge No. 075-0-03653 B)
Des. No. 1601029 and DHPA No. 23247

Dear Consulting Party,

The Indiana Department of Transportation (INDOT), with funding from the Federal Highway Administration (FHWA), proposes to proceed with the SR 75 over Wildcat Creek Bridge project (Des. No. 1601029).

This letter is part of the Section 106 review process for this project. Section 106 of the National Historic Preservation Act requires federal agencies to take into account the effects of their undertakings on historic and archaeological properties. We are requesting comments from you regarding the possible effects of this project. Please use the above Des. Number and project description in your reply and your comments will be incorporated into the formal environmental study.

A Section 106 early coordination letter was distributed on November 19, 2018. In addition, the November 19, 2018 letter notified consulting parties that a historic property report and an archaeology report was available for review and comment.

The proposed undertaking is on State Road (SR) 75 over Wildcat Creek Bridge (INDOT Bridge No. 075-08-03653B) from approximately 0.08 mile north and south of the bridge approaches in Carroll County. It is within Democrat Township, Rossville 7.5 Minute USGS Topographic Quadrangle in Section 33, Township 24 North, Range 1 West.

The current bridge over Wildcat Creek [INDOT Bridge No. 075-08-03653 B; Historic Bridge (HB)-1897; Indiana Historic Sites and Structures Inventory (IHSSI) No.: 015-553-65014; National Bridge Inventory (NBI) No.: 24970] is a two-span bridge consisting of a thru truss and a steel girder approach span. The bridge was constructed in 1947 and reconstructed most recently in 2001. The bridge was determined eligible for listing in the National Register of Historic Places (NRHP) in the Indiana Historic Bridge Inventory and was designated as "Non-Select."

The need for the proposed project is a result of the insufficient legal load rating, deteriorated condition of the structural steel, superstructure, and wearing surface and the inadequate usable shoulder width on the bridge. The purpose of the proposed project is to improve the inventory load rating of the bridge to 36 tons or more, improve the superstructure rating, and improve the wearing surface rating.

American Structurepoint, Inc. is under contract with INDOT to advance the environmental documentation for the referenced project. Weintraut and Associates, Inc. has been subcontracted to complete the Section 106 documentation for the project.

In accordance with 36 CFR 800.2 (c), you were invited to become a consulting party as part of the Section 106 process, or you are hereby invited to become a consulting party as part of the Section 106 process. Entities that have previously accepted consulting party status--as well as additional entities that are currently being invited to become consulting parties--are identified in the attached list. Additional consulting parties have been added per request of the individual parties. These parties are included in the attached list.

The Section 106 process involves efforts to identify historic properties potentially affected by the undertaking, to assess the undertaking's effects and to seek ways to avoid, minimize, or mitigate any adverse effects on historic properties. For more information regarding the protection of historic resources, please see the Advisory Council on Historic Preservation's guide: *Protecting Historic Properties: A Citizen's Guide to Section 106 Review* available online at <https://www.achp.gov/sites/default/files/documents/2017-01/CitizenGuide.pdf>.

Per the terms of the "Programmatic Agreement Regarding Management and Preservation of Indiana's Historic Bridges" (Historic Bridges PA), the Federal Highway Administration—Indiana Division (FHWA) will satisfy its Section 106 responsibilities involving "Select" and "Non-Select" bridges through the Project Development Process (PDP) of the Historic Bridges PA (Stipulation III). Because Bridge No. 075-08-03653 B is a "Non-Select" bridge, the procedures outlined in Stipulation III.B. of the Historic Bridges PA will be followed to fulfill FHWA's Section 106 responsibilities for the bridge. (A copy of the Historic Bridges PA can be downloaded here: <http://www.in.gov/indot/2530.htm>).

Please note that, per the permanent rule issued by the Indiana Department of Natural Resources effective August 14, 2013 (312 IAC 20-4-11.5), INDOT is requesting that this project be subjected to "dual review"; that is, reviewed by the Division of Historic Preservation and Archaeology simultaneously under 54 U.S.C. 306108 (Section 106) and IC 14-21-1-18 (Indiana Preservation and Archaeology Law dealing with alterations of historic sites and structures requiring a Certificate of Approval). Pursuant to Section 11.5(f) of this rule, at the conclusion of the review process we anticipate that the Division Director would issue a letter of clearance exempting this project from obtaining a Certificate of Approval under IC 14-21-1-18.

The Area of Potential Effects (APE) is the area in which the proposed project may cause alterations in the character or use of historic resources. The APE contains no resources listed in the National Register of Historic Places (NRHP).

A historian who meets the Secretary of the Interior's Professional Qualification Standards identified and evaluated above-ground resources within the APE for potential eligibility for the NRHP. As a result of the historic property identification and evaluation efforts, the SR 75 Bridge over Wildcat Creek (IHSSI No.: 015-553-65014) is recommended as eligible for listing in the NRHP as it retains the characteristics for which it was previously determined eligible for listing in the NRHP.

With regard to archaeological resources, an archaeologist who meets the Secretary of the Interior's Professional Qualification Standards identified no sites within the project area. As a result of these efforts, no sites were recommended as eligible for listing in the NRHP and no further work or further work is recommended.

The Historic Bridge Alternatives Analysis (HBAA) is available for review in IN SCOPE at <http://erms.indot.in.gov/Section106Documents/> (the Des. No. is the most efficient search term, once in IN SCOPE). You are invited to review the HBAA and to respond with comments on any historic resource impacts incurred as a result of this project so that an environmental report can be completed. We also welcome your

related opinions and other input to be considered in the preparation of the environmental document. If you prefer a hard-copy of this material, please respond to this email with your request within seven (7) days.

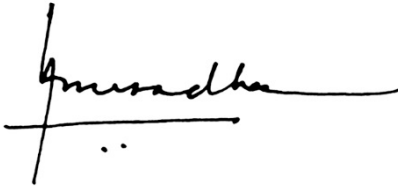
Please review the information and comment within thirty (30) calendar days of receipt. If you indicate that you do not desire to be a consulting party or if you have not previously accepted consulting party status and you do not respond to this letter, you will not be included on the list of consulting parties for this project and will not receive further information about the project unless the design changes.

For questions concerning specific project details, you may contact Leah Perry of American Structurepoint, Inc. at (317) 547-5580 or lperry@structurepoint.com. All future responses regarding the proposed project should be forwarded to American Structurepoint, Inc. at the following address:

Leah Perry
Environmental Specialist
American Structurepoint, Inc.
9025 River Road, Suite 200
Indianapolis, IN 46240
lperry@structurepoint.com

Tribal contacts may contact Shaun Miller at smiller@indot.in.gov or 317-233-6795 or Michelle Allen at FHWA at michelle.allen@dot.gov or 317-226-7344.

Sincerely,

A handwritten signature in black ink, appearing to read "Anuradha", with a horizontal line and two dots below it.

Anuradha V. Kumar, Manager
Cultural Resources Office
Environmental Services

Distribution List:

- Chad Slider, Indiana Department of Natural Resources/Division of Historic Preservation & Archaeology (IDNR/DHPA), Indiana State Historic Preservation Officer (SHPO), cslider@dnr.in.gov
- William Brown, Carroll County Commissioners, commissioner3@carrollcountyin.gov, abjakes@yahoo.com
- John Brown, Carroll County Commissioners, commissioner1@carrollcountyin.gov
- Steve Pearson, Carroll County Commissioners, commissioner2@carrollcountyin.gov
- Emily Royer, Indiana Landmarks, Western Regional Office, eroyer@indianalandmarks.org
- Randy Myers, Carroll County Historical Museum, carrollcountyhistoricalsociety@ffni.com
- Mark Smith, Carroll County Historian, markalansmith12171948@gmail.com
- Paul Brandenburg, Historic Spans Task Force, indianabridges@sbcglobal.net
- Bonnie Maxwell, Heartland Heritage, Inc., bmaxwell62@gmail.com
- Mr. Joe Mayfield, Delphi Preservation Society, joe.Mayfield5@gmail.com
- Gus Nyberg, NICHES Land Trust, gnyberg@nicheslandtrust.org
- Dan McCain, Carroll County Wabash & Erie Canal Inc., dan.mccain@gmail.com
- Al Auffert, Adams Mill Inc., amauffart@gmail.com
- Julia Leahy, Carroll County Chamber of Commerce, julia@carrollcountychamber.com
- Mary Blue, Promoting Wildcat Valley, maryloublueeyes@gmail.com
- Dale Brier, Division of Outdoor Recreation, Indiana Department of Natural Resources, dbrier@dnr.in.gov
- Rick Parsons, Wildcat Guardians, rpars17542@aol.com
- Nathan Holth, HistoricBridges.org, nathan@historicbridges.org
- Forest County Potawatomi Community
- Miami Tribe of Oklahoma

FHWA Project: Des. No. 1601029; SR 75 over Wildcat Creek Bridge Project, Carroll County, Indiana

1 message

Perry, Leah <lperry@structurepoint.com>

Thu, Dec 5, 2019 at 11:56 AM

To: "commissioner3@carrollcountyin.gov" <commissioner3@carrollcountyin.gov>, "abjakes@yahoo.com" <abjakes@yahoo.com>, "commissioner1@carrollcountyin.gov" <commissioner1@carrollcountyin.gov>, "commissioner2@carrollcountyin.gov" <commissioner2@carrollcountyin.gov>, "eroyer@indianalandmarks.org" <eroyer@indianalandmarks.org>, "carrollcountyhistoricalsociety@ffni.com" <carrollcountyhistoricalsociety@ffni.com>, "markalansmith12171948@gmail.com" <markalansmith12171948@gmail.com>, "indianabridges@sbcglobal.net" <indianabridges@sbcglobal.net>, "bmaxwell62@gmail.com" <bmaxwell62@gmail.com>, "joe.Mayfield5@gmail.com" <joe.Mayfield5@gmail.com>, "gnyberg@nicheslandtrust.org" <gnyberg@nicheslandtrust.org>, "dan.mccain@gmail.com" <dan.mccain@gmail.com>, "amauffart@gmail.com" <amauffart@gmail.com>, "julia@carrollcountyin.gov" <julia@carrollcountyin.gov>, "maryloubblueeyes@gmail.com" <maryloubblueeyes@gmail.com>, "dbrier@dnr.in.gov" <dbrier@dnr.in.gov>, "rpars17542@aol.com" <rpars17542@aol.com>, "nathan@historicbridges.org" <nathan@historicbridges.org>
Cc: "cslider@dnr.in.gov" <cslider@dnr.in.gov>, "Hope, Briana" <bhope@structurepoint.com>, "Day, Dave" <Dday@structurepoint.com>, Bethany Natali <bethany@weintrautinc.com>, Linda Weintraut <linda@weintrautinc.com>, "jhockaday@indot.in.gov" <jhockaday@indot.in.gov>, "Kumar, Anuradha" <akumar@indot.in.gov>, "smiller@indot.in.gov" <smiller@indot.in.gov>, "Branigin, Susan" <SBranigin@indot.in.gov>, "Kennedy, Mary" <MKENNEDY@indot.in.gov>

Des. No.: 1601029

Project Description: SR 75 over Wildcat Creek Bridge Project (Bridge No. 075-0-03653 B)

Location: SR 75 over Wildcat Creek Bridge (INDOT Bridge No. 075-08-03653B) from approximately 0.08 mile north and south of the bridge approaches in Carroll County

The Indiana Department of Transportation, with funding from the Federal Highway Administration, proposes to proceed with the SR 75 over Wildcat Creek Bridge project. The Section 106 Early Coordination Letter for this project was originally distributed on November 19, 2018. In addition, the November 19, 2018 letter notified consulting parties that a historic property report and an archaeology report was available for review and comment.

As part of Section 106 of the National Historic Preservation Act, the Historic Bridge Alternatives Analysis (HBAA) has been prepared and is ready for review and comment by consulting parties.

Please review this documentation located in IN SCOPE at <http://erms.indot.in.gov/Section106Documents/> (the Des. No. is the most efficient search term, once in IN SCOPE), and respond with any comments that you may have. If a hard copy of the materials is needed, please respond to this email with your request within seven (7) days.

Consulting parties have thirty (30) calendar days from receipt of this information to review and provide comment. Tribal contacts may contact Shaun Miller at smiller@indot.in.gov or 317-233-6795 or Michelle Allen at FHWA at michelle.allen@dot.gov or 317-226-7344.

Thank you in advance for your input,

Leah Perry

Environmental Specialist

9025 River Road, Suite 200

Indianapolis, IN 46240

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FW: FHWA Project: Des. No. 1601029; SR 75 over Wildcat Creek Bridge Project, Carroll County, Indiana

1 message

Perry, Leah <lperry@structurepoint.com>

Thu, Dec 5, 2019 at 2:04 PM

To: "carrollcountycchs@gmail.com" <carrollcountycchs@gmail.com>

Cc: Linda Weintraut <linda@weintrautinc.com>, "Hope, Briana" <bhope@structurepoint.com>, "cslider@dnr.in.gov" <cslider@dnr.in.gov>, Bethany Natali <bethany@weintrautinc.com>

Mr. Myers,

I tried to send you this email earlier today, but the email address we had on file was outdated. I spoke with Mark and he provided this email address and said he forwarded you the email. I am copying anyone else who may need the updated email address.

Thank you,

Leah Perry

Environmental Specialist

9025 River Road, Suite 200

Indianapolis, IN 46240

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FW: FHWA Project: Des. No. 1601029; SR 75 over Wildcat Creek Bridge Project, Carroll Co, Ind--HBAA

1 message

From: Kennedy, Mary [mailto:MKENNEDY@indot.IN.gov]
Sent: Thursday, December 05, 2019 12:05 PM
To: 'Diane Hunter' <dhunter@miamination.com>; Michael LaRonge <Michael.LaRonge@fcpotawatomi-nsn.gov>
Cc: Miller, Shaun (INDOT) <smiller@indot.IN.gov>; Allen, Michelle (FHWA) <michelle.allen@dot.gov>; Linda Weintraut <linda@weintrautinc.com>; Perry, Leah <lperry@structurepoint.com>
Subject: FHWA Project: Des. No. 1601029; SR 75 over Wildcat Creek Bridge Project, Carroll Co, Ind--HBAA

Des. No.: 1601029

Project Description: SR 75 over Wildcat Creek Bridge Project (Bridge No. 075-0-03653 B)

Location: SR 75 over Wildcat Creek Bridge (INDOT Bridge No. 075-08-03653B) from approximately 0.08 mile north and south of the bridge approaches in Carroll County

The Indiana Department of Transportation, with funding from the Federal Highway Administration, proposes to proceed with the SR 75 over Wildcat Creek Bridge project. The Section 106 Early Coordination Letter for this project was originally distributed on November 19, 2018. In addition, the November 19, 2018 letter notified consulting parties that a historic property report and an archaeology report were available for review and comment.

As part of Section 106 of the National Historic Preservation Act, the Historic Bridge Alternatives Analysis (HBAA) has been prepared and is ready for review and comment by consulting parties.

Please review this documentation located in IN SCOPE at <http://erms.indot.in.gov/Section106Documents/> (the Des. No. is the most efficient search term, once in IN SCOPE), and respond with any comments that you may have. If a hard copy of the materials is needed, please respond to this email with your request within seven (7) days.

Consulting parties have thirty (30) calendar days from receipt of this information to review and provide comment. Tribal contacts may contact Shaun Miller at smiller@indot.in.gov or 317-233-6795 or Michelle Allen at FHWA at michelle.allen@dot.gov or 317-226-7344.

Thank you in advance for your input,

Mary E. Kennedy
Historic Bridge Specialist

100 N. Senate Ave., Room N642-ES

Indianapolis, IN 46204

Office: (317) 232-5215

Email: mkennedy@indot.in.gov



****Updated guidance for historic bridge projects can be found in the links below:**

[Overview-Indiana Historic Bridges Program](#)

[Historic Bridge Project Development Process](#)

[Procedures for Public Hearings under the Historic Bridges PA](#)

***For the latest updates from INDOT's Cultural Resources Office, subscribe to the Environmental Services listserv: <https://www.in.gov/indot/3217.htm>**

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Division of Historic Preservation & Archaeology · 402 W. Washington Street, W274 · Indianapolis, IN 46204-2739
Phone 317-232-1646 · Fax 317-232-0693 · dhpa@dnr.IN.gov · www.IN.gov/dnr/historic



January 6, 2020

Leah Perry
Environmental Specialist
American Structurepoint, Inc.
9025 River Road, Suite 200
Indianapolis, Indiana 46240

Federal Agency: Indiana Department of Transportation ("INDOT"),
on behalf of Federal Highway Administration, Indiana Division ("FHWA")

Re: DUAL REVIEW: Historic bridge alternative analysis report (Day/Gomes, 11/11/2019) for the SR 75
over Wildcat Creek Bridge Project, Democrat Township, Carroll County, Indiana (Des. No. 1601029,
DHPA No. 23247)

Dear Ms. Perry:

Pursuant to Section 106 of the National Historic Preservation Act of 1966, as amended (54 U.S.C. § 306108); implementing regulations at 36 C.F.R. Part 800; the "Programmatic Agreement Among the Federal Highway Administration, the Indiana Department of Transportation, the Indiana Historic Preservation Officer, and the Advisory Council on Historic Preservation Regarding Management and Preservation of Indiana's Historic Bridges" ("Indiana Historic Bridges PA"); and the "Programmatic Agreement (PA) Among the Federal Highway Administration, the Indiana Department of Transportation, the Advisory Council on Historic Preservation and the Indiana State Historic Preservation Officer Regarding that Implementation of the Federal Aid Highway Program In the State of Indiana" ("Indiana Minor Projects PA"); and also pursuant to Indiana Code 14-21-1-18 and 312 Indiana Administrative Code ("IAC") 20-4, the staff of the Indiana State Historic Preservation Officer ("Indiana SHPO" or "INDNR-DHPA") has reviewed your December 5, 2019 submittal, which enclosed the aforementioned historic bridge alternative analysis report ("HBAA"; Day/Gomes, 11/11/2019), all of which we received on December 9, 2019.

As the HBAA indicates, the SR 75 Bridge over Wildcat Creek (Bridge No. 075-08-03653B; National Bridge Inventory No. 24970) was identified as being historic – or eligible for inclusion in the National Register of Historic Places ("NRHP") under Criterion C – but also as Non-Select in the *Indiana Historic Bridge Inventory*. The bridge is a two-span, steel, Parker thru truss constructed ca. 1947.

We agree with the HBAA that Alternative B-1: Rehabilitation for Continued Vehicular Use Meeting Secretary of Interior's Standards is feasible and prudent and should be pursued for the proposed project. We commend the Carroll County Board of Commissioners for its stated resolution urging INDOT "to maintain and not replace the bridge."

We look forward to receiving the 30% of design plans for our review and comment. We also think that photographic documentation of the bridge and its immediate surroundings should be produced before site preparation or construction work begins. Once we learn more about the rehabilitation design from the plans, we may have other specific recommendations for the photographic documentation.

As previously indicated, in regard to archaeological resources, based upon the submitted information and the documentation available to the staff of the Indiana SHPO, we have not identified any currently known archaeological resources listed in or eligible for inclusion in the NRHP within the proposed project area; and we concur with the opinion of the archaeologist, as expressed in the Indiana archaeological short report (Graham, 09/2018), that no further archaeological investigations appear necessary at the proposed project area.

If any prehistoric or historic archaeological artifacts or human remains are uncovered during construction, demolition, or earthmoving activities, state law (Indiana Code 14-21-1-27 and 29) requires that the discovery must be reported to the Department of Natural Resources within two (2) business days. In that event, please call (317) 232-1646. Be advised that adherence to Indiana Code 14-21-1-27 and -29 does not obviate the need to adhere to applicable federal statutes and regulations, including but not limited to 36 C.F.R. 800.

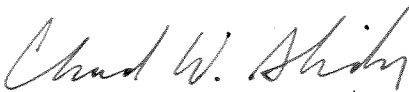
If you have questions regarding our dual review of the aforementioned project, please contact the Indiana SHPO. Questions about archaeological issues should be directed to Wade T. Tharp at (317) 232-1650 or wtharp1@dnr.IN.gov. Questions about historic buildings or structures pertaining to this review should be directed to Danielle Kauffmann at (317) 232-0582 or dkauffmann@dnr.IN.gov.

For the benefit of those recipients of a copy of this letter who are not Section 106 consulting parties, please be aware that the documents discussed here can be found online in IN SCOPE at <http://erms.indot.in.gov/Section106Documents/>. From there, search by this project's designation number: 1601029.

Anyone receiving an e-mailed copy of this letter who does *not* wish to receive future copies of our correspondence about this bridge project is asked to reply to wtharp1@dnr.in.gov and dkauffmann@dnr.in.gov and so advise us.

In all future correspondence regarding the dual review of this project involving the SR 75 over Wildcat Creek Bridge Project in Democrat Township, Carroll County, Indiana (Des. No. 1601029), please refer to DHPA No. 23247.

Very truly yours,



Beth K. McCord
Deputy State Historic Preservation Officer

BKM:DMK:dmk

emc: Joyce Newland, FHWA
Michelle Allen, FHWA
Anuradha Kumar, INDOT
Shaun Miller, INDOT
Susan Branigin, INDOT
Mary Kennedy, INDOT
Shirley Clark, INDOT
William R. Brown, Carroll County Commissioners
Steve Pearson, Carroll County Commissioners
Loren Hylton, Carroll County Commissioners
Randy Myers, Carroll County Historical Museum
Mark Smith, Carroll County Historian
Emily Royer, Indiana Landmarks, Western Field Office
James L. Cooper, Ph.D., Professor Emeritus of History, DePauw University
Paul Brandenburg, Indiana Historic Spans Task Force
Mary Mears, Carroll County Historic Bridge Coalition
Bonnie Maxwell, Heartland Heritage, Inc.
Joe Mayfield, Delphi Preservation Society
Gus Nyberg, NICHES Land Trust
Dan McCain, Wabash & Erie Canal Association
Al Auffert, Adams Mill, Inc.
Julia Leahy, Carroll County Chamber of Commerce
Mary Blue, Promoting Wildcat Valley
Dale Brier, IDNR- Division of Outdoor Recreation
Rick Parson, Wildcat Guardians
Nathan Holth, HistoricBridges.org
Michael LaRonge, Forest County Potawatomi Community
Diane Hunter, Miami Tribe of Oklahoma
Paul Coutts, Carroll County Highway Superintendent/Engineer
Leah Perry, American Structurepoint, Inc.
J. Scott Keller, Review Board
Daniel Kloc, AIA, Review Board
Anne Shaw Kingery, Review Board
Jason Larrison, AIA, Review Board
Joshua Palmer, AIA, Review Board
Chandler Lighty, Review Board
April Sievert, Ph.D., Review Board
Christopher Smith, Deputy Director, INDNR
Beth K. McCord, INDNR-DHPA
Wade T. Tharp, INDNR-DHPA
Danielle Kauffmann, INDNR-DHPA

APPENDIX F. Historic Bridge Alternatives Analysis

Note: This is Appendix F
of the 800.11 document.

Alternative Analysis Report

SR 75 over Wildcat Creek

Section 33, T-24-N, R-01-W, Democrat Township, Carroll County, Indiana

Project Number: 1601029
Bridge File No: 075-08-03653 B
NBI Number: 024970
DES No: 1601029
Location: 40°28' 54" N; 86° 31'48" W
4.51 mi South of SR 18
Reference Post: 50 + 52



David A. Day, PE

Nevil Gomes, EI

DATE: 11/11/2019
American Structurepoint, Inc.
9025 River Road
Indianapolis, IN 46240

DATE: 11/11/2019
American Structurepoint, Inc.
9025 River Road
Indianapolis, IN 46240

This bridge was evaluated by personnel from the Indiana Department of Transportation (INDOT) Bridge Design Unit, the District Office and the designer. The attached Draft Historic Bridge Alternatives Analysis has been reviewed by the INDOT Bridge Design Unit and Cultural Resources Office for thoroughness of the rehabilitation option and compliance with INDOT design policies. Concurrence by INDOT with the proposed Scope of Work does not constitute Final Approval of the Historic Bridge Alternatives Analysis. This draft HBAA may now be distributed to the historic consulting parties for review.



AMERICAN
STRUCTUREPOINT
INC.

6.2 Mitigation

INDOT will consult with the Indiana SHPO to decide if photo documentation of the bridge is needed prior to construction.

7.0 Preliminary Preferred Alternative

The rehabilitation alternative is feasible and prudent, and therefore is the preferred alternative. The comparison of the costs of all the alternatives that were considered is listed below. See Appendix D for cost details.

Alternative	Meets Project Purpose & Need?	Construction Cost	Right of Way Cost	Total Cost	Other Factors	Feasible and Prudent
A-No Build	No	\$0	N/A	\$0	Bridge does not meet the required load capacity. Deterioration of superstructure would progress to eventual closure of the bridge.	The alternative is not prudent because it does not meet the project purpose and need.
B-1-Rehabilitation for Continued Vehicular Use (Two-Way Option)	Yes	\$1,416,400	No New ROW Required	\$1,416,400	This alternative meets all the project requirements.	This alternative is feasible because it would meet the INDOT design criteria. It is prudent because the rehabilitation cost is about 52% of replacement costs.



Division of Historic Preservation & Archaeology · 402 W. Washington Street, W274 · Indianapolis, IN 46204-2739
Phone 317-232-1646 · Fax 317-232-0693 · dhpa@dnr.IN.gov · www.IN.gov/dnr/historic



March 12, 2020

Leah Perry
Environmental Specialist
American Structurepoint, Inc.
9025 River Road, Suite 200
Indianapolis, Indiana 46240

Federal Agency: Federal Highway Administration ("FHWA")

State Agency: Indiana Department of Transportation ("INDOT")

Re: DUAL REVIEW: Indiana Department of Transportation's finding of "no historic properties affected" on behalf of the Federal Highway Administration for the SR 75 over Wildcat Creek Bridge Project, Democrat Township, Carroll County, Indiana (Des. No. 1601029, DHPA No. 23247)

Dear Ms. Perry:

Pursuant to Section 106 of the National Historic Preservation Act of 1966, as amended (54 U.S.C. § 306108); implementing regulations at 36 C.F.R. Part 800; the "Programmatic Agreement Among the Federal Highway Administration, the Indiana Department of Transportation, the Indiana Historic Preservation Officer, and the Advisory Council on Historic Preservation Regarding Management and Preservation of Indiana's Historic Bridges" ("Indiana Historic Bridges PA"); and the "Programmatic Agreement (PA) Among the Federal Highway Administration, the Indiana Department of Transportation, the Advisory Council on Historic Preservation and the Indiana State Historic Preservation Officer Regarding that Implementation of the Federal Aid Highway Program In the State of Indiana"; and also pursuant to Indiana Code 14-21-1-18 and 312 Indiana Administrative Code ("IAC") 20-4, the staff of the Indiana State Historic Preservation Officer ("Indiana SHPO" or "INDNR-DHPA") has reviewed your February 28, 2020 submittal, with enclosures, all of which we received on March 2, 2020.

For the benefit of members of the Indiana Historic Preservation Review Board ("Review Board") and other recipients of a copy of this letter who are not Section 106 consulting parties, please be aware that a copy of the finding and documentation can be found online at IN SCOPE (<http://erms.indot.in.gov/Section106Documents>). From there, search by this project's designation number: 1601029.

As we said in our December 18, 2019 letter, we agree with the historic property short report (Fivecoat, 10/2018) that the subject bridge is the only historic property within the area of potential effects that is listed or eligible for listing in the National Register of Historic Places ("NRHP"). The SR 75 Bridge over Wildcat Creek (Bridge No. 075-08-03653B; National Bridge Inventory No. 24970) is a two-span, steel, Parker thru truss constructed ca. 1947 that is eligible for listing in the NRHP under Criterion C. We also note that the historic bridge alternatives analysis (Day/Gomes, 11/11/2019) determined that Alternative B-1: Rehabilitation for Continued Vehicular Use Meeting Secretary of Interior's Standards is feasible and prudent and the preferred alternative for this project.

Because the Indiana Historic Bridges PA takes into account the effects of projects on all historic, Select and Non-Select bridges in Indiana, a Section 106 finding in a bridge project applies only to historic properties, if any, within the APE, *other than the bridge*. Accordingly, we will comment on the federal Section 106 finding here, but we cannot issue a final comment or a director's letter of clearance regarding the project's impact on this historic, Non-Select Bridge for the purposes of the state preservation law review until we have had the opportunity to review what would be essentially the final plans for the bridge. Once we receive the final stages of design plans, we will then comment on the project's impact on the historic bridge for state preservation law purposes.

Also, as previously indicated, in regard to archaeological resources, based upon the submitted information and the documentation available to the staff of the Indiana SHPO, we have not identified any currently known archaeological resources listed in or eligible for inclusion in the NRHP within the proposed project area; and we concur with the opinion of the archaeologist, as expressed in the Indiana archaeological short report (Graham, 09/2018), that no further archaeological investigations appear necessary at the proposed project area.

If any prehistoric or historic archaeological artifacts or human remains are uncovered during construction, demolition, or earthmoving activities, state law (Indiana Code 14-21-1-27 and 29) requires that the discovery must be reported to the Department of Natural Resources

within two (2) business days. In that event, please call (317) 232-1646. Be advised that adherence to Indiana Code 14-21-1-27 and -29 does not obviate the need to adhere to applicable federal statutes and regulations, including but not limited to 36 C.F.R. 800.

Accordingly, we concur with INDOT's February 28, 2020 Section 106 finding, on behalf of FHWA, of "No Historic Properties Affected" for this federal undertaking.

If you have questions regarding our dual review of the aforementioned project, please contact the Indiana SHPO. Questions about archaeological issues should be directed to Wade T. Tharp at (317) 232-1650 or wtharp1@dnr.IN.gov. Questions about historic buildings or structures pertaining to this review should be directed to Danielle Kauffmann at (317) 232-0582 or dkauffmann@dnr.IN.gov.

Anyone receiving an e-mailed copy of this letter who does *not* wish to receive future copies of our correspondence about this bridge project is asked to reply to wtharp1@dnr.in.gov and dkauffmann@dnr.in.gov and so advise us. In all future correspondence regarding the dual review of this project involving the SR 75 over Wildcat Creek Bridge Project in Democrat Township, Carroll County, Indiana (Des. No. 1601029), please refer to DHPA No. 23247.

Very truly yours,



Beth K. McCord
Deputy State Historic Preservation Officer

BKM:DMK:dmk

emc: Kari Carmany-George, FHWA
Michelle Allen, FHWA
Anuradha Kumar, INDOT
Shaun Miller, INDOT
Susan Branigin, INDOT
Mary Kennedy, INDOT
Shirley Clark, INDOT
Leah Perry, American Structurepoint, Inc.
William R. Brown, Carroll County Commissioners
Steve Pearson, Carroll County Commissioners
Loren Hylton, Carroll County Commissioners
Randy Myers, Carroll County Historical Museum
Mark Smith, Carroll County Historian
Emily Royer, Indiana Landmarks, Western Field Office
James L. Cooper, Ph.D., Professor Emeritus of History, DePauw University
Paul Brandenburg, Indiana Historic Spans Task Force
Mary Mears, Carroll County Historic Bridge Coalition
Bonnie Maxwell, Heartland Heritage, Inc.
Joe Mayfield, Delphi Preservation Society
Gus Nyberg, NICHS Land Trust
Dan McCain, Wabash & Erie Canal Association
Al Auffert, Adams Mill, Inc.
Julia Leahy, Carroll County Chamber of Commerce
Mary Blue, Promoting Wildcat Valley
Dale Brier, IDNR- Division of Outdoor Recreation
Rick Parsons, Wildcat Guardians
Nathan Holth, HistoricBridges.org
Allison Daniels, Forest County Potawatomi Community
Diane Hunter, Miami Tribe of Oklahoma
Paul Coutts, Carroll County Highway Superintendent/Engineer
J. Scott Keller, Review Board
Daniel Kloc, AIA, Review Board
Anne Shaw Kingery, Review Board
Jason Larrison, AIA, Review Board
Joshua Palmer, AIA, Review Board
Chandler Lighty, Review Board
April Sievert, Ph.D., Review Board
Christopher Smith, Deputy Director, INDNR
Beth K. McCord, INDNR-DHPA
Wade T. Tharp, INDNR-DHPA
Danielle Kauffmann, INDNR-DHPA

Claim No. _____ Warrant No. _____

IN FAVOR OF

CARROLL COUNTY COMET

P.O. BOX 26, FLORA IN 46929

\$46.01

Appropriation No. _____

CARROLL COUNTY, SR 75 BRIDGE PROJECT

ALLOWED _____

IN THE SUM OF \$ _____

Attest

I have examined the within claim and hereby certify as follows:

That it is in proper form.

That it is duly authenticated as required by law.

That it is based upon statutory authority.

That it is apparently correct

incorrect

I certify that the within claim is true and correct; that the services there in itemized and for which charge is made were ordered by me and were necessary to the public business

PUBLIC NOTICE
Des. No. 1601029

The Indiana Department of Transportation (INDOT) is planning to undertake a bridge project, funded in part by the Federal Highway Administration. The project is located near Cutler, in Carroll County, Indiana.

Under the preferred alternative, the proposed project would involve rehabilitation of the State Road (SR) 75 over Wildcat Creek Bridge (INDOT Bridge No. 075-08-03653B) for continued vehicular use meeting the Secretary of Interior's standards. This alternative would remove and replace the existing bridge deck and concrete railing; install shear studs on top flange of existing beams; heat-straighten the north floor beam and replace the connecting gusset plate; repair the structural steel with patching or replacement with in-kind members; replace inadequate rivets with high strength bolts; paint the structural steel; include semi-integral bent conversion at Bent 1; and incidental approach tie-in. No new Right-of-Way is anticipated.

Properties listed in or eligible for the National Register of Historic Places located within the Area of Potential Effects (APE) include the SR 75 over Wildcat Creek Bridge. INDOT, on behalf of the FHWA, has issued a "No Historic Properties Affected" finding for the project due to the fact that no historic properties are present within the Area of Potential Effects (APE) other than the SR 75 over Wildcat Creek Bridge, which has been classified as a "Non-Select" Bridge by the INDOT Historic Bridge Inventory, and thus, the procedures outlined in Stipulation III.B of the "Programmatic Agreement Regarding Management and Preservation of Indiana's Historic Bridges" (Historic Bridges PA) will be followed to fulfill FHWA's Section 106 responsibilities for the bridge. Per Stipulation III.B, INDOT will hold a public hearing for the project prior to completion of National Environmental Policy Act (NEPA) studies. In accordance with the National Historic Preservation Act, the views of the public are being sought regarding the effect of the proposed project on the historic elements as per 36 CFR 800.2(d), 800.3(e) and 800.6(a)(4). Pursuant to 36 CFR 800.4(d)(1), the documentation specified in 36 CFR 800.11(d) is available

for inspection in American Structurepoint, Inc. located at 9025 River Road, Suite 200, Indianapolis, IN 46240. Additionally, this documentation can be viewed electronically by accessing INDOT's Section 106 document posting website IN SCOPE at <http://erms.indot.in.gov/Section106Documents>. This documentation serves as the basis for the "No Historic Properties Affected" finding. The views of the public on this effect finding are being sought. Please reply with any comments to Mrs. Leah Perry, American Structurepoint, Inc., 9025 River Road, Suite 200, Indianapolis, IN 46240, Ph: (317) 547-5580, Fax: (317) 543-0270, Lperry@structurepoint.com no later than (**date to respond-30 days after notice published**). April 3, 2020.

In accordance with the "Americans with Disabilities Act", if you have a disability for which INDOT needs to provide accessibility to the document(s) such as interpreters or readers, please contact Rickie Clark at 317-232-6601 or rclark@indot.in.gov.

hspaxlp 1011

INDIANA DEPT. OF TRANSPORTATION
(Governmental Unit)
INDIANAPOLIS, INDIANA

To:

CARROLL COUNTY COMET
P.O. BOX 26
FLORA, IN 46929

PUBLISHER'S CLAIM

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advertisement is set) -- number of equivalent lines.....
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Charge for extra proofs of publication (\$1.00 for each proof
in excess of two).....
TOTAL AMOUNT OF CLAIM.....\$46.01

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Number of insertions 1

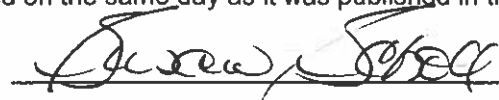
Pursuant to the provisions and penalties of IC 5-11-10-1, I hereby certify that the foregoing account is just and correct, that the amount claimed is legally due, after allowing all just credits, and that no part of the same has been paid.

I also certify that the printed matter attached hereto is a true copy, of the same column width and type size, which was duly published in said paper 1 time. The date of publication being as follows:

MARCH 4, 2020

Additionally, the statement below is true and correct:

The Comet has a website and this public notice was posted on the same day as it was published in the newspaper.



Date: MARCH 4, 2020

Title:

EDITOR



INDIANA DEPARTMENT OF TRANSPORTATION

Driving Indiana's Economic Growth

100 North Senate Avenue
Room N642
Indianapolis, Indiana 46204-2216 (317) 232-5348 FAX: (317) 233-4929

Eric Holcomb, Governor
Joe McGuinness, Commissioner

Date: December 18, 2018

To: Site Assessment & Management
Environmental Services
Indiana Department of Transportation
100 N Senate Avenue, Room N642
Indianapolis, IN 46204

The Red Flag Investigation GIS layers
were checked on 3/6/2020 and no
updates to the Red Flag are warranted.

From: Leah S. Boits
7260 Shadeland Station
Indianapolis, IN 46256
Lboits@structurepoint.com

Re: RED FLAG INVESTIGATION
DES 1601029, State Project
Bridge Improvement
State Road 75 over Wildcat Creek, Bridge No. 075-08-03653 B
Near Cutler, Carroll County, Indiana

PROJECT DESCRIPTION

The proposed project corridor is located at the State Road (SR) 75 Bridge over Wildcat Creek in Carroll County, Indiana (INDOT Bridge No. 075-08-03653 B). The project corridor extends approximately 0.08 mile north and south of the bridge approaches and extends 0.03 mile east of SR 75 along S 20 W. The project corridor is more specifically located on the Rossville 7.5 Minute USGS topographic quadrangle in Section 33, Township 24 North, and Range 1 West.

The existing SR 75 over Wildcat Creek Bridge [INDOT Bridge No. 075-08-03653 B; Historic Bridge (HB)-1897; Indiana Historic Sites and Structures Inventory (IHSSI) No.: 015-553-65014; National Bridge Inventory (NBI) No.: 24970] is a two-span bridge consisting of a thru truss with cast-in-place deck span and a steel girder span. The bridge was constructed in 1947 and reconstructed most recently in 2001. The bridge was determined eligible for listing in the National Register of Historic Places (NRHP) in the Indiana Historic Bridge Inventory and was designated as "Non-Select," meaning it is not a good candidate for preservation in place. Because of this, an alternatives analysis will be completed to determine the scope of work for the proposed project. Alternatives considered include rehabilitation of the existing structure, construction of a one-way pair, or a structure replacement. The project area included in this investigated will cover all alternatives.

Bridge and/or Culvert Project: Yes ☒ No ☐ Structure # 075-08-03653 B

If this is a bridge project, is the bridge Historical? Yes ☒ No ☐ Select ☐ Non-Select ☒

(Note: If the project involves a historical bridge, please include the bridge information in the Recommendations Section of the report).

Proposed right of way: Temporary ☒ # Acres >0.5 Permanent ☒ # Acres >0.5

Type of excavation: The anticipated maximum depth of excavation is estimated to be 10 feet if replacement is the

selected alternative. Excavation would occur directly adjacent to the waterway, and the use of cofferdams may be necessary. If rehabilitation is the selected alternative, maximum depth of excavation would be 3 feet for replacement of riprap along the banks of Wildcat Creek.

Maintenance of traffic: The preferred method of traffic maintenance for this project would include closure of SR 75 over Wildcat Creek. Detour routes would be set up for through traffic, and access would be maintained for local traffic at all times. A full MOT plan will be developed once the alternative analysis has been completed and design further develops.

Work in waterway: Yes ☒ No ☐ Above ordinary high water mark: Yes ☐ No ☒

State Project: ☒ LPA: ☐

Any other factors influencing recommendations: N/A

INFRASTRUCTURE TABLE AND SUMMARY

Infrastructure Indicate the number of items of concern found within the 0.5 mile search radius. If there are no items, please indicate N/A:			
Religious Facilities	1	Recreational Facilities	N/A
Airports ¹	1	Pipelines	N/A
Cemeteries	N/A	Railroads	1
Hospitals	N/A	Trails	N/A
Schools	N/A	Managed Lands	N/A

¹In order to complete the required airport review, a review of public airports within 3.8 miles (20,000 feet) is required.

Explanation:

Religious Facilities: According to a Google Map review, one (1) religious facility is located within the 0.5 mile search radius, approximately 0.48 mile southeast of the project area. No impact is expected.

Airports: One (1) Airport is located within the 0.5 mile search radius. The airport, Avery Airport, is mapped approximately 0.45 mile northwest of the project area. However, upon further research, no evidence has been found to indicate that an airport is still present at this location. Therefore, no impact is expected.

Railroads: One (1) railroad is located within the 0.5 mile search radius. The Conrail Railroad, which is inactive, is located approximately 0.4 mile east of the project area. No impact is expected.

WATER RESOURCES TABLE AND SUMMARY

Water Resources Indicate the number of items of concern found within the 0.5 mile search radius. If there are no items, please indicate N/A:			
NWI - Points	N/A	Canal Routes - Historic	N/A
Karst Springs	N/A	NWI - Wetlands	13
Canal Structures – Historic	N/A	Lakes	3
NPS NRI Listed	N/A	Floodplain - DFIRM	1
NWI-Lines	N/A	Cave Entrance Density	N/A
IDEM 303d Listed Streams and Lakes (Impaired)	6	Sinkhole Areas	N/A
Rivers and Streams	8	Sinking-Stream Basins	N/A

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Explanation:

IDEM 303d List River and Streams: Six (6) impaired stream segments are located within the 0.5 mile search radius. One (1) impaired stream segment, Wildcat Creek, is located within the project area. Wildcat Creek is listed for *E. coli* and PCBs (and/or mercury) in fish tissue. Workers who are working in or near water with *E. coli* should take care to wear appropriate Personal Protective Equipment (PPE), observe proper hygiene procedures, including regular hand washing, and limit personal exposure. Exposure to PCBs (and/or mercury) in fish tissue is considered low, assuming workers are not eating biota surrounding or associated with the water body. If there will be sediment and/or soils disturbed by construction, additional investigation may be necessary. Coordination with INDOT ES will occur.

Rivers and Streams: Eight (8) stream segments are located within the 0.5 mile search radius. One (1) stream segment, Wildcat Creek, is located within the central portion of the project area. A Waters of the US Report will be prepared and coordination with INDOT ES Ecology and Waterway Permitting will occur.

NWI-Wetlands: Thirteen (13) NWI-Wetlands are located within the 0.5 mile search radius. Two (2) wetlands are located within the central portion of the project area. A Waters of the US Report will be prepared and coordination with INDOT ES Ecology and Waterway Permitting will occur.

Lakes: Three (3) lakes are located within the 0.5 mile search radius. The closest lake is located approximately 0.20 mile northwest of the project area. No impact is expected.

Floodplain – DFIRM: One (1) floodplain polygon associated with Wildcat Creek is located within the 0.5 mile search radius. The entire project area is located within the Wildcat Creek floodplain. Coordination with INDOT ES Ecology and Waterway Permitting will occur.

URBANIZED AREA BOUNDARY SUMMARY

Urbanized Area Boundary (UAB): The project area is not mapped within an Urbanized Area Boundary.

MINING AND MINERAL EXPLORATION TABLE AND SUMMARY

Mining/Mineral Exploration			
Indicate the number of items of concern found within the 0.5 mile search radius. If there are no items, please indicate N/A:			
Petroleum Wells	N/A	Petroleum Fields	N/A
Mines – Surface	N/A	Mines – Underground	N/A

Explanation: No mining/mineral exploration resources were identified within the 0.5 mile search radius.

HAZARDOUS MATERIAL CONCERNS TABLE AND SUMMARY

Hazardous Material Concerns Indicate the number of items of concern found within the 0.5 mile search radius. If there are no items, please indicate N/A:			
Superfund	N/A	Manufactured Gas Plant Sites	N/A
RCRA Generator/ TSD	N/A	Open Dump Waste Sites	N/A
RCRA Corrective Action Sites	N/A	Restricted Waste Sites	N/A
State Cleanup Sites	N/A	Waste Transfer Stations	N/A
Septage Waste Sites	N/A	Tire Waste Sites	N/A
Underground Storage Tank (UST) Sites	N/A	Confined Feeding Operations (CFO)	N/A
Voluntary Remediation Program	N/A	Brownfields	N/A
Construction Demolition Waste	N/A	Institutional Controls	N/A
Solid Waste Landfill	N/A	NPDES Facilities	N/A
Infectious/Medical Waste Sites	N/A	NPDES Pipe Locations	N/A
Leaking Underground Storage (LUST) Sites	N/A	Notice of Contamination Sites	N/A

Explanation: No hazardous material concerns were identified within the 0.5 mile search radius.

ECOLOGICAL INFORMATION SUMMARY

The Carroll County listing of the Indiana Natural Heritage Data Center information on endangered, threatened, or rare (ETR) species and high quality natural communities is attached with ETR species highlighted. A preliminary review of the Indiana Natural Heritage Database by INDOT Environmental Services did not indicate the presence of endangered species. Coordination with USFWS and IDNR will occur.

A review of the USFWS database did not indicate the presence of endangered bat species in or within 0.5 mile of the project area. The project area is located in a rural area surrounded by farm fields and wooded areas. The November 16, 2017, inspection report for Bridge No. 075-08-03653 B states that no evidence of bats was seen or heard under the bridge. The range-wide programmatic consultation for the Indiana Bat and Northern Long-eared Bat will be completed according to "Using the USFWS's IPaC System for Listed Bat Consultation for INDOT Projects."

An inquiry using the USFWS Information for Planning and Consultation (IPaC) website did not indicate the presence of the federally endangered species, the Rusty Patched Bumblebee, in or within 0.5 mile of the project area. No impact is expected.

RECOMMENDATIONS SECTION

INFRASTRUCTURE: N/A

WATER RESOURCES: The presence of following water resources will require the preparation of a Waters of the US Report and coordination with INDOT ES Ecology and Waterway Permitting:

- One (1) stream segment, Wildcat Creek, flows through the project area.
- Two (2) wetlands are located within the central portion of the project area.
- The entire project area is located within a floodplain polygon (Coordination Only).

One (1) IDEM 303d Listed stream segment, Wildcat Creek, is located within the central portion of the project area. Wildcat Creek is listed for *E. coli* and PCBs (and/or mercury) in fish tissue. Workers who are working in or near water with *E. coli* should take care to wear appropriate PPE, observe proper hygiene procedures, including regular hand washing, and limit personal exposure. Exposure to PCBs (and/or mercury) in fish tissue is considered low, assuming workers are not eating biota surrounding or associated with the water body. If there will be sediment and/or soils disturbed by construction, additional investigation may be necessary.

MINING/MINERAL EXPLORATION: N/A

URBANIZED AREA BOUNDARY: N/A

HAZMAT CONCERNS: N/A

ECOLOGICAL INFORMATION: Coordination with USFWS and IDNR will occur. The range-wide programmatic consultation for the Indiana Bat and Northern Long-eared Bat will be completed according to "Using the USFWS's IPaC System for Listed Bat Consultation for INDOT Projects."

HISTORIC BRIDGE:

The current bridge over Wildcat Creek [INDOT Bridge No. 075-08-03653 B; Historic Bridge (HB)-1897; Indiana Historic Sites and Structures Inventory (IHSSI) No.: 015-553-65014; National Bridge Inventory (NBI) No.: 24970] is a two-span bridge consisting of a thru truss with cast-in-place deck span and a steel girder span. The bridge was constructed in 1947 and reconstructed most recently in 2001. The bridge was determined eligible for listing in the National Register of Historic Places (NRHP) in the Indiana Historic Bridge Inventory and was designated as "Non-Select," meaning it is not a good candidate for preservation in place. Coordination with INDOT CRO and SHPO will occur.

INDOT Environmental Services concurrence: Nicole Fohey-Breting (Signature)

Prepared by:

Leah S. Boits

Project Manager

American Structurepoint, Inc.

December 18, 2018

Graphics:

PROJECT LOCATION MAP: YES

INFRASTRUCTURE: YES

WATER RESOURCES: YES

MINING/MINERAL EXPLORATION: N/A

URBANIZED AREA BOUNDARY: N/A

HAZMAT CONCERNS: N/A

HIGHLIGHTED CARROLL COUNTY ETR LISTING: YES

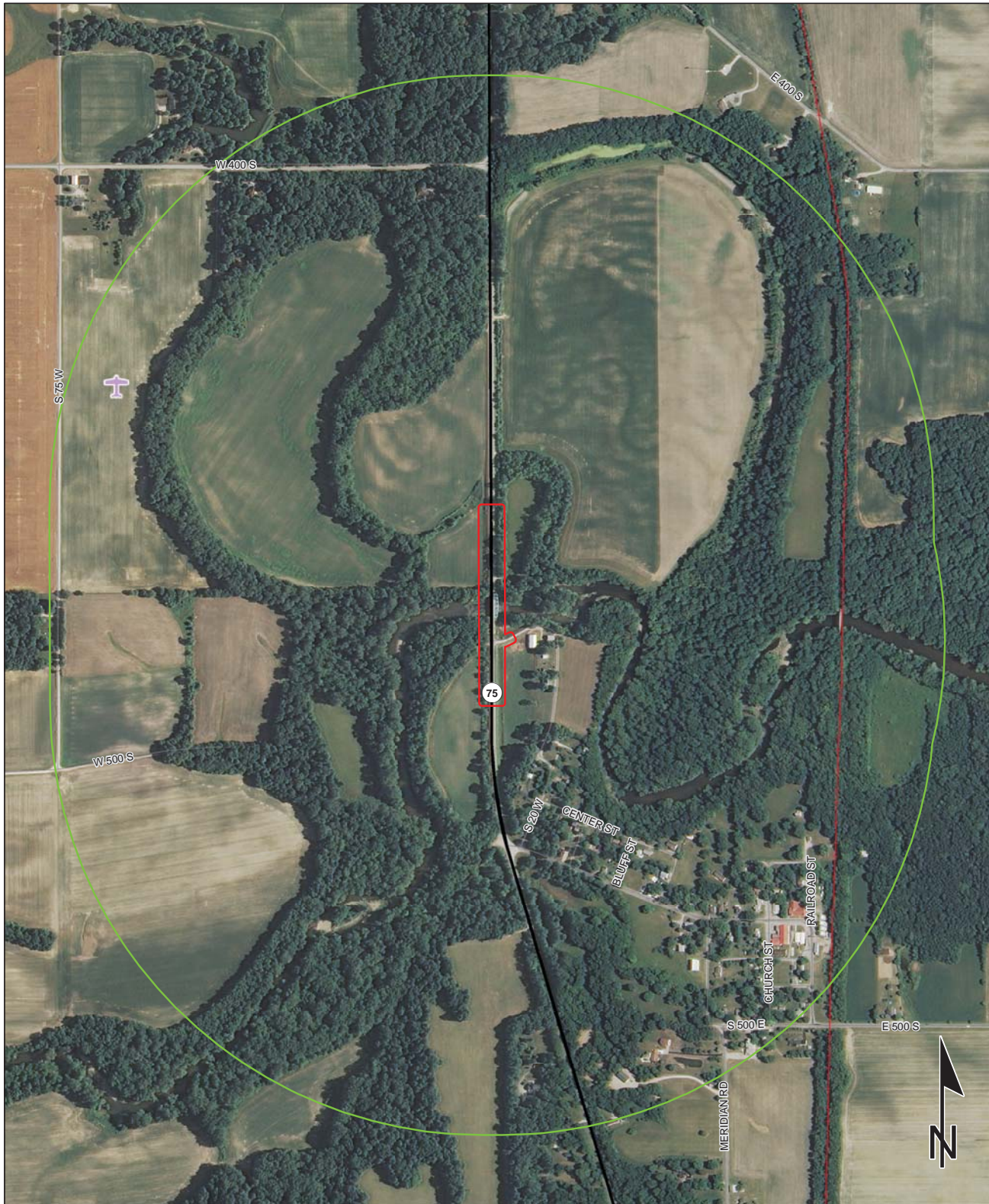
Note: Duplicate Mapping has been removed to reduce file size. The Project Location Map (USGS Topographic Map) can be found in Appendix B, B-2)

Red Flag Investigation - Infrastructure

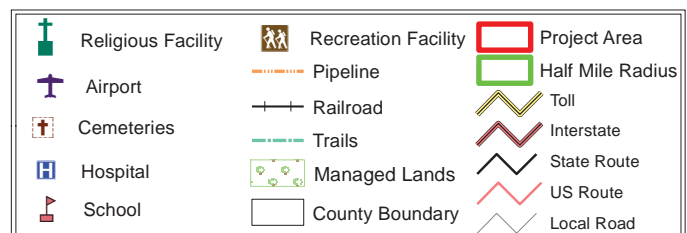
SR 75 over Wildcat Creek

Des. No. 1601029 , Bridge Work

Carroll County, Indiana



Sources: 0.1 0.05 0 0.1 Miles
Non Orthophotography
 Data - Obtained from the State of Indiana Geographical Information Office Library
Orthophotography - Obtained from Indiana Map Framework Data (www.indianamap.org)
 Map Projection: UTM Zone 16 N Map Datum: NAD83
 This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.

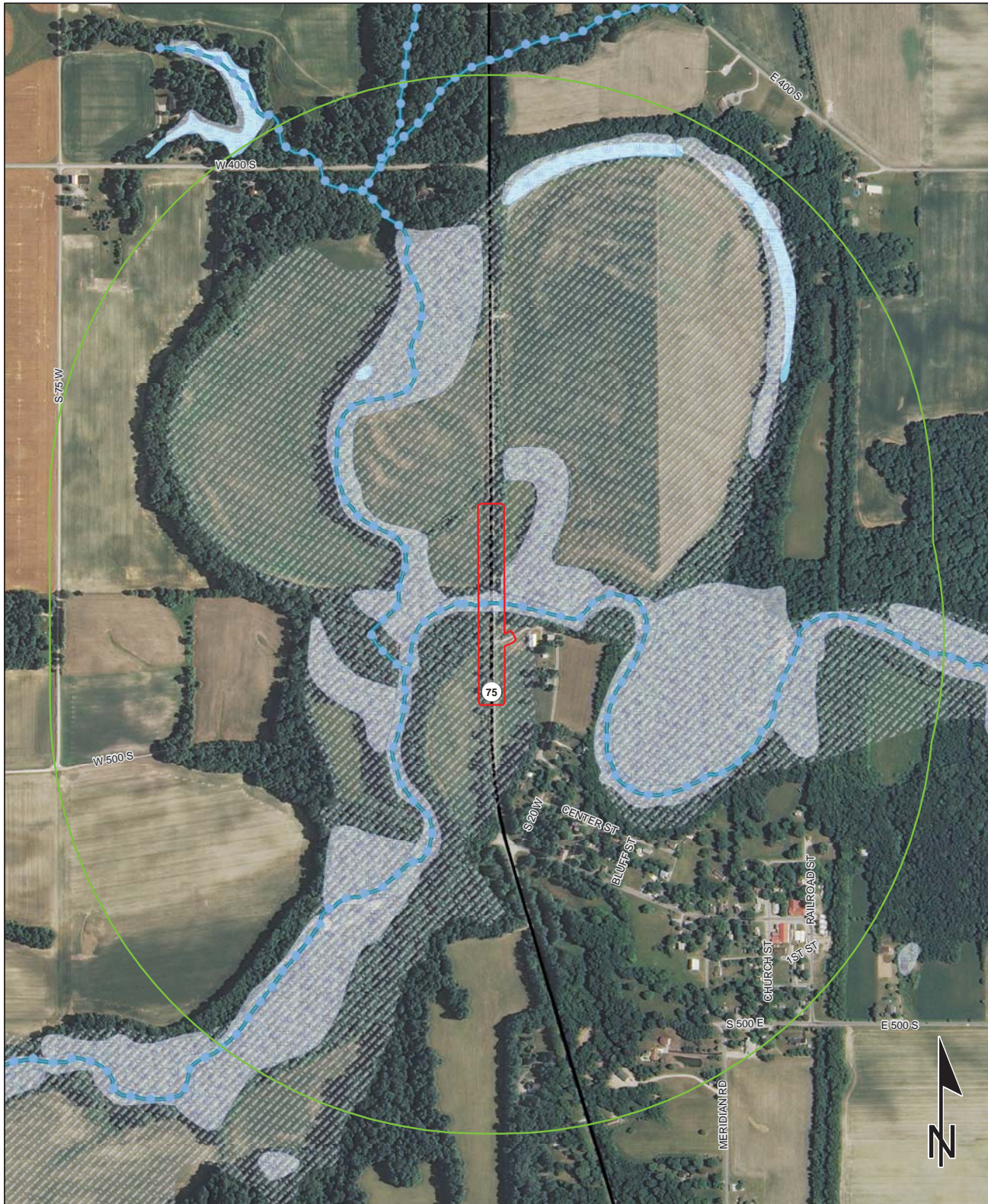


Red Flag Investigation - Water Resources

SR 75 over Wildcat Creek

Des. No. 1601029 , Bridge Work

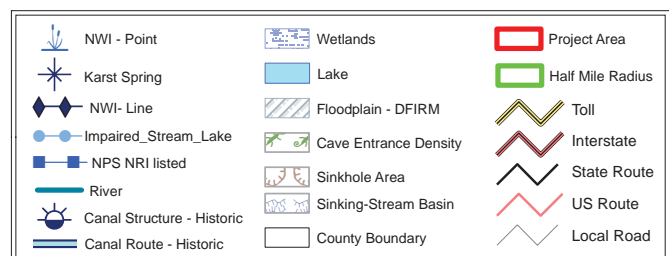
Carroll County, Indiana



Sources:
Non Orthophotography
Data - Obtained from the State of Indiana Geographical Information Office Library
Orthophotography - Obtained from Indiana Map Framework Data (www.indianamap.org)
Map Projection: UTM Zone 16 N **Map Datum:** NAD83

This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.

0.1 0.05 0 0.1 Miles



Indiana County Endangered, Threatened and Rare Species List

County: Carroll

Species Name	Common Name	FED	STATE	GRANK	SRANK
Mollusk: Bivalvia (Mussels)					
Cyprogenia stegaria	Eastern Fanshell Pearlymussel	LE	SE	G1Q	S1
Epioblasma torulosa rangiana	Northern Riffleshell	LE	SE	G2T2	S1
Epioblasma torulosa torulosa	Tubercled Blossom	LE	SE	G2TX	SX
Epioblasma triquetra	Snuffbox	LE	SE	G3	S1
Fusconaia subrotunda	Longsolid	C	SE	G3	SX
Lampsilis fasciola	Wavyrayed Lampmussel		SSC	G5	S3
Lampsilis ovata	Pocketbook			G5	S2
Ligumia recta	Black Sandshell			G4G5	S2
Obovaria subrotunda	Round Hickorynut	C	SE	G4	S1
Plethobasus cyphus	Sheepnose	LE	SE	G3	S1
Pleurobema clava	Clubshell	LE	SE	G1G2	S1
Pleurobema cordatum	Ohio Pigtoe		SSC	G4	S2
Pleurobema plenum	Rough Pigtoe	LE	SE	G1	S1
Pleurobema pyramidatum	Pyramid Pigtoe		SE	G2G3	SX
Potamilus capax	Fat Pocketbook	LE	SE	G2	S1
Ptychobranchus fasciolaris	Kidneyshell		SSC	G4G5	S2
Quadrula cylindrica cylindrica	Rabbitsfoot	LT	SE	G3G4T3	S1
Toxolasma lividus	Purple Lilliput	C	SSC	G3Q	S2
Villosa fabalis	Rayed Bean	LE	SE	G2	S1
Villosa lienosa	Little Spectaclecase		SSC	G5	S3
Insect: Coleoptera (Beetles)					
Dryobius sexnotatus	Six-banded Longhorn Beetle		ST	GNR	S2
Insect: Odonata (Dragonflies & Damselflies)					
Enallagma divagans	Turquoise Bluet		SR	G5	S3
Ophiogomphus rupinsulensis	Rusty Snaketail		SR	G5	S2S3
Fish					
Etheostoma maculatum	Spotted Darter		SSC	G2G3	S2S3
Etheostoma tippecanoe	Tippecanoe Darter	C	SSC	G3G4	S3
Percina copelandi	Channel Darter		SE	G4	S2
Percina evides	Gilt Darter		SE	G4	S1
Reptile					
Clemmys guttata	Spotted Turtle	C	SE	G5	S2
Clonophis kirtlandii	Kirtland's Snake	C	SE	G2	S2
Sistrurus catenatus catenatus	Eastern Massasauga	LT	SE	G3	S2
Bird					
Haliaeetus leucocephalus	Bald Eagle		SSC	G5	S2
Mammal					
Mustela nivalis	Least Weasel		SSC	G5	S2?

Indiana Natural Heritage Data Center
Division of Nature Preserves
Indiana Department of Natural Resources
This data is not the result of comprehensive county surveys.

Fed: LE = Endangered; LT = Threatened; C = candidate; PDL = proposed for delisting
State: SE = state endangered; ST = state threatened; SR = state rare; SSC = state species of special concern; SX = state extirpated; SG = state significant; WL = watch list
GRANK: Global Heritage Rank: G1 = critically imperiled globally; G2 = imperiled globally; G3 = rare or uncommon globally; G4 = widespread and abundant globally but with long term concerns; G5 = widespread and abundant globally; G? = unranked; GX = extinct; Q = uncertain rank; T = taxonomic subunit rank
SRANK: State Heritage Rank: S1 = critically imperiled in state; S2 = imperiled in state; S3 = rare or uncommon in state; G4 = widespread and abundant in state but with long term concern; SG = state significant; SH = historical in state; SX = state extirpated; B = breeding status; S? = unranked; SNR = unranked; SNA = nonbreeding status unranked

Indiana County Endangered, Threatened and Rare Species List

County: Carroll

Species Name	Common Name	FED	STATE	GRANK	SRANK
Myotis sodalis	Indiana Bat or Social Myotis	LE	SE	G2	S1
Taxidea taxus	American Badger		SSC	G5	S2
Vascular Plant					
Berberis canadensis	American Barberry		SE	G3	S1
Carex eburnea	Ebony Sedge		SR	G5	S2
Cirsium hillii	Hill's Thistle		SE	G3	S1
Cypripedium calceolus var. parviflorum	Small Yellow Lady's-slipper		SR	G5	S2
Deschampsia cespitosa	Tufted Hairgrass		SR	G5	S2
Erysimum capitatum	Prairie-rocket Wallflower		ST	G5	S2
Eurybia furcata	Forked Aster		SR	G3	S2
Napaea dioica	Glade Mallow		SR	G4	S2
Oryzopsis racemosa	Black-fruit Mountain-ricegrass		SR	G5	S2
Panax trifolius	Dwarf Ginseng		WL	G5	S2
Rudbeckia fulgida var. fulgida	Orange Coneflower		WL	G5T4?	S2
Satureja glabella var. angustifolia	Calamint		SE	G5	S1
Scutellaria parvula var. australis	Southern Skullcap		WL	G4T4?	S2
Scutellaria parvula var. parvula	Small Skullcap		SE	G4T4	S1
Spiranthes lucida	Shining Ladies'-tresses		SR	G4	S2
Viburnum molle	Softleaf Arrow-wood		SR	G5	S2
High Quality Natural Community					
Wetland - fen	Fen		SG	G3	S3
Wetland - seep circumneutral	Circumneutral Seep		SG	GU	S1
Other Significant Feature					
Geomorphic - Nonglacial Erosional Feature - Water Fall and Cascade	Water Fall and Cascade			GNR	SNR

Indiana Natural Heritage Data Center
Division of Nature Preserves
Indiana Department of Natural Resources
This data is not the result of comprehensive county surveys.

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SRANK: State Heritage Rank: S1 = critically imperiled in state; S2 = imperiled in state; S3 = rare or uncommon in state; G4 = widespread and abundant in state but with long term concern; SG = state significant; SH = historical in state; SX = state extirpated; B = breeding status; S? = unranked; SNR = unranked; SNA = nonbreeding status unranked

Duplicate mapping, routine wetland data sheets, QHEI forms, and PJD form have been removed to reduce file size. Please see Appendix B for State Location and USGS Topographic Map.

WETLAND DELINEATION AND WATERS REPORT

STATE ROAD 75 OVER WILDCAT CREEK BRIDGE IMPROVEMENT

DES. NO. 1601029

ASSET ID#: 075-08-03653 B

DEMOCRAT TOWNSHIP, CARROLL COUNTY, INDIANA



Prepared for:

INDIANA DEPARTMENT OF TRANSPORTATION
LAPORTE DISTRICT OFFICE
315 E. BOYD BOULEVARD
LAPORTE, INDIANA 46350

Prepared by:

AMERICAN STRUCTUREPOINT, INC.
7260 SHADELAND STATION
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07/26/2018

TABLE OF CONTENTS

1.0	Introduction	1
2.0	Site Characterization – Records Review	2
2.1	USGS Topographic Mapping	2
2.2	National Wetlands Inventory Mapping (NWI) Maps	2
2.3	County Soil Survey	2
2.4	Aerial Photography and Light Detection and Ranging (LiDAR) Data	2
2.5	Floodways and Floodplains	3
2.6	Legal Drain	3
2.7	12-Digit Hydrologic Unit Code	3
3.0	Field Reconnaissance	3
3.1	Wetlands	3
3.2	Drainage Features, Streams, and Other Potential “Waters of the US”	4
3.3	Non-Jurisdictional Features and Non-Wetland Data Points	6
4.0	Conclusions.....	6
5.0	Acknowledgement.....	6
6.0	References.....	7

Appendix A - Aquatic Resource Summary Tables

Appendix B - Routine Wetland Determination Data Forms

Appendix C - Quality Assessment Forms

Appendix D - Mapping

Appendix E - Photographs

Appendix F - Preliminary Jurisdictional Determination

Appendix B, C , and F as well as duplicate mapping within Appendix D have been removed to reduce file size. Please see Appendix B for State Location and USGS Topographic Map.

1.0 Introduction

American Structurepoint, Inc. was contracted by the Indiana Department of Transportation (INDOT), LaPorte District to perform a wetland delineation and waters investigation for the proposed work on the State Road (SR) 75 bridge over Wildcat Creek (Des. No. 1601029; INDOT Structure No. 075-08-03653 B; NBI No. 24970) in Carroll County, Indiana. The investigated area begins approximately 500 feet south of the bridge over Wildcat Creek and extends north approximately 1,250 feet along SR 75. The investigated area is approximately 160 feet wide. The investigated area is more specifically located on the Rossville USGS 7.5 Minute Quadrangle Map in the Northeast $\frac{1}{4}$ and Southeast $\frac{1}{4}$ of Section 33, Township 24 North, and Range 1 West. The location and approximate boundaries of the study area can be seen in the attached maps and aerial photographs (Appendix D).

American Structurepoint staff visited the site on October 5, 2017, to conduct a wetland delineation. The proposed project is located in Land Resource Region (LRR) M, as recognized by the US Department of Agriculture. As such, this wetland delineation was conducted in accordance with the *Corps of Engineers Wetland Delineation Manual* (Environmental Laboratory, 1987) and the *Regional Supplement to the Corps of Engineers Wetland Delineation Manual: Midwest Region* (U.S. Army Corps of Engineers, 2010).

Two wetlands (Wetlands A and B) totaling 0.126 acre and two streams (Wildcat Creek and UNT 1) totaling 730 linear feet were delineated within the investigated area. Wetlands A and B, and UNT 1 drain to Wildcat Creek, which drains to the Wabash River, a Traditional Navigable Waterway (TNW). Therefore, it is anticipated that these four delineated features would be considered “waters of the U.S.”

2.0 Site Characterization – Records Review

2.1 USGS Topographic Mapping

The investigated area is shown on the Rossville USGS 7.5 Minute Quadrangle Map in the northeast ¼ and southeast ¼ of Section 33, Township 24 North, and Range 1 West. The topographic map depicts the investigated area as mostly cleared land with SR 75 transecting the investigated area from north to south. Some forested areas are depicted north of Wildcat Creek, east and west of SR 75. Wildcat Creek is depicted as a solid blue line (perennial) flowing west through the investigated area. The topography is depicted as generally flat, with steeper banks along Wildcat Creek and SR 75. Wildcat Creek was field verified during the October 5, 2017 field investigation.

2.2 National Wetlands Inventory Mapping (NWI) Maps

The NWI Mapping was reviewed for the investigated area. Two NWI mapped wetlands are located within the investigated area. One mapped wetland is a Riverine, Lower Perennial, Unconsolidated Bottom, Permanently Flooded (R2UBH) wetland located along Wildcat Creek through the center of the investigated area. The other mapped wetland within the investigated area is a Palustrine, Forested, Broad-Leaved Deciduous, Temporarily Flooded (PFO1A) wetland located north of Wildcat Creek and east of SR 75 within the investigated area. In addition, another PFO1A wetland is mapped approximately 0.04 mile west of the investigated area. Wildcat Creek was delineated during the October 5, 2017 field investigation. However, a wetland was not delineated within the area of the mapped PFO1A NWI wetland during the October field visit.

2.3 County Soil Survey

The Natural Resources Conservation Service (NRCS) Web Soil Survey was accessed on September 27, 2017 to determine soil classification and drainage features within the investigated area. Soil types mapped within the investigated area include:

Soil Name	Soil Symbol	Hydric or Non-hydric	SSURGO Hydric Rating by Mapped Unit
<i>Cohoctah variant very fine sandy loam, frequently flooded</i>	Cr	Hydric	100%
Moundhaven-Landes variant complex, frequently flooded	Mv	Non-Hydric	1-32%

Source: Natural Resources Conservation Service, United States Department of Agriculture. <http://sdmdataaccess.nrcs.usda.gov>

The 1958 Carroll County Soil Survey depicts Wildcat Creek as a perennial stream flowing west through the investigated area. A perennial UNT to Wildcat Creek is depicted just north of the investigated area. Wildcat Creek was field verified during the October field investigation, and the UNT mapped north of the investigated area was verified and confirmed to be outside of the investigated area.

2.4 Aerial Photography and Light Detection and Ranging (LiDAR) Data

Aerial photography from 2005 and 2012 (IndianaMap) was reviewed for the investigated area. The 2005 and 2012 aerial photography show Wildcat Creek flowing through the investigated area. Both aeriels show the investigated area as containing maintained right-of-way along SR 75, woody vegetation along Wildcat Creek and northeast of the bridge, and agricultural land. During the October 5, 2017 field investigation, the

agricultural fields in the northeast and southwest quadrants of the bridge crossing appeared to be planted with trees and out of agricultural production.

The 2018 LiDAR data indicates areas of low elevation along Wildcat Creek as well as east and west of SR 75. UNT 1 was delineated within a low elevation area along the eastern edge of SR 75. Wetland A was delineated within the low elevation area along the western edge of SR 75. Wetland B was delineated within the low elevation area along Wildcat Creek.

2.5 Floodways and Floodplains

Federal Emergency Management Agency (FEMA) Flood Insurance Rate Mapping (FIRM) was reviewed for the investigated area. The western half of the investigated area is located within the 100-year floodplain of Wildcat Creek.

2.6 Legal Drain

The Carroll County GIS website (<https://beacon.schneidercorp.com/>) was accessed on October 4, 2017 by American Structurepoint staff. No legal drains were depicted within or near the investigated area.

2.7 12-Digit Hydrologic Unit Code

The investigated area is located within the Town of Owasco – Wildcat Creek and Hurricane Creek-Hurricane Creek HUCs 051201070407 and 051201070408.

3.0 Field Reconnaissance

The SR 75 over Wildcat Creek Bridge Improvement project area was examined for the presence of wetlands and “waters of the US” on the site. Data points were strategically placed to identify appropriate boundaries of delineated wetlands and to determine the presence or absence of jurisdictional wetlands and “waters of the US.” Two wetlands (Wetlands A and B) totaling 0.26 acre and two streams (Wildcat Creek and UNT 1) totaling 730 linear feet were delineated within the investigated area. Data sheets and a map indicating the location of data points documenting the field investigation are included in the appendix.

3.1 Wetlands

3.1.1 Wetland A

Wetland A is an emergent wetland located in the northwest corner of the investigated area. The wetland is associated with a low lying area near a stream located north of the investigated area. Wetland A is located within the floodplain of Wildcat Creek and appears to extend north and west beyond the limits of the investigated area. Wetland A appears to drain to Wildcat Creek, which drains to the Wabash River, a TNW. Therefore, it is anticipated that Wetland A would be considered a “Waters of the U.S.”

The dominant vegetation consisted of *Phragmites australis* (FACW). The hydrologic indicators included Geomorphic Position (D2) and FAC-Neutral Test (D5). The hydric soil indicator included Depleted Matrix (F3). The wetland was delineated for approximately 0.002 acre within the investigated area and would be considered a Palustrine, Emergent, Persistent, Seasonally Flooded/Saturated wetland (PEM1E) under the Cowardin Classification System. While Wetland A is located within the floodplain of Wildcat Creek, it has low species diversity, dominated by invasive species (*Phragmites australis*), and is surrounded by agricultural

fields. Therefore, it would be considered a poor quality wetland. For reference to field data collected for this wetland see Data Point (DP) 1 included in Appendix B. DP 2 included in Appendix B is representative of the upland area surrounding Wetland A.

3.1.2 Wetland B

Wetland B is an emergent wetland located along the south bank of Wildcat Creek, east and west of SR 75. The wetland is associated with a low lying area within the floodplain of Wildcat Creek and appears to extend east and west beyond the limits of the investigated area. Wetland B appears to drain to Wildcat Creek, which drains to the Wabash River, a TNW. Therefore, it is anticipated that Wetland B would be considered a “Waters of the U.S.”

The dominant vegetation consisted of *Acer negundo* (FAC), *Ulmus Americana* (FACW), *Pilea pumila* (FACW), *Phalaris arundinacea* (FACW), *Leersia oryzoides* (OBL), and *Persicaria hydropiperoides* (OBL). Trees identified as dominant vegetation for this wetland are due to the presence of canopy cover; trees are not located within the wetland boundaries and therefore Wetland B does not represent a forested wetland. The hydrologic indicators included Drainage Patterns (B10), Geomorphic Position (D4), and FAC-Neutral Test (D5). The hydric soil indicators included Sandy Redox (S5) and Depleted Matrix (F3). The wetland was delineated for approximately 0.124 acre within the investigated area and would be considered a PEM1E wetland under the Cowardin Classification System. Wetland B is located within the floodplain of Wildcat Creek, along the bank of the stream; however, Wetland B is dominated by invasive species (*Phalaris arundinacea*). Therefore, it would be considered an average quality wetland. For reference to field data collected for this wetland see DP 5 and DP 8 included in Appendix B. DP 6 and DP 7 included in Appendix B are representative of the upland area surrounding Wetland B.

3.2 Drainage Features, Streams, and Other Potential “Waters of the US”

3.2.1 Wildcat Creek

Wildcat Creek is a perennial stream that flows west through the investigated area. Wildcat Creek crosses under SR 75 approximately 170 feet north of South CR 20 West. The stream is conveyed under SR 75 via a single span, steel truss bridge (Structure No. 075-08-03653 B). The stream is depicted on the USGS topographic map and 1958 Carroll County Soil Survey as a perennial stream (solid blue line). The Carroll County Beacon site was checked on October 4, 2017 by American Structurepoint staff, and Wildcat Creek is not depicted as a legal drain. The north fork of Wildcat Creek in Carroll County is included in the Indiana Department of Natural Resources (IDNR) natural, scenic, and recreational river system, and is designated as a scenic river. The north fork of Wildcat Creek is also listed on the Indiana Outstanding State Resource Waters by the USACE. The stream was delineated for approximately 160 linear feet within the investigated area. Wildcat Creek flows west out of the investigated area to the Wabash River, a TNW. Therefore, it is anticipated that Wildcat Creek would be considered a “waters of the U.S.”

The ordinary high water mark (OHWM) was approximately 110 feet wide by 2 feet deep. The channel width at the top of bank was approximately 125 feet. Bank height was approximately 3 feet on the left and right banks. The flow regime appears to be perennial. Riffle (10%) run (40%) complexes were observed in this stretch of Wildcat Creek. The substrate was primarily cobble and sand, with some gravel and silt. The upstream drainage area at this reach of Wildcat Creek is approximately 375 square miles (<https://streamstats.usgs.gov/ss/>). Herbaceous and woody vegetation covered both banks of the stream.

Wildcat Creek would be considered a Riverine, Lower Perennial, Unconsolidated Bottom, Permanently Flooded (R2UBH) stream under the Cowardin Classification System.

A QHEI was conducted for Wildcat Creek beginning approximately 25 feet west of SR 75 (QHEI 1). The overall QHEI score for the 200-foot sampled reach was 71. This is a Good narrative rating in the manual. The stream scored highest for Riffle/Run Quality (7/8). The stream scored lowest in Instream Cover (10/20).

3.2.2 UNT 1

UNT 1 is an intermittent stream that flows south through the investigated area into Wildcat Creek. UNT 1 is located approximately 40 feet east of SR 75. The stream is not depicted on the USGS topographic map or 1958 Carroll County Soil Survey. The Carroll County Beacon site was checked on October 4, 2017 by American Structurepoint staff. UNT 1 is not depicted as a legal drain. The stream was delineated for approximately 570 linear feet within the investigated area. UNT 1 flows south to Wildcat Creek, which drains to the Wabash River, a TNW. Therefore, it is anticipated that UNT 1 would be considered a “waters of the U.S.”

The ordinary high water mark (OHWM) was approximately 6 feet wide by 4 inches deep. The channel width at the top of bank was approximately 10 feet. Bank height was approximately 5 feet on the left and right banks. The flow regime appears to be intermittent. The stream had a moist channel with isolated pools at the time of the field investigation; therefore, no riffle/run complexes were present. The substrate was primarily silt and sand, with some gravel, cobble, and detritus. The upstream drainage area at this reach of UNT 1 is approximately 0.2 square mile. Woody vegetation and emergent wetland covered both banks of the stream. UNT 1 would be considered a Riverine, Intermittent, Unconsolidated Bottom, Seasonally Flooded/Saturated (R4UBE) stream under the Cowardin Classification System.

A QHEI was conducted for UNT 1 beginning approximately 300 feet north of Wildcat Creek (QHEI 2). The overall QHEI score for the 200-foot sampled reach was 36. This is a Poor narrative rating in the manual. The stream scored highest for Bank Erosion and Riparian Zone (7/10). The stream scored lowest in Pool/Glide (0/12) and Riffle/Run Quality (0/8).

The banks of UNT 1 were dominated by hydrophytic vegetation; therefore, DP 11 was collected to characterize the banks of the stream. The dominant vegetation consisted of *Ulmus Americana* (FACW), *Juglans nigra* (FACU), *Leersia oryzoides* (OBL), and *Equisetum hyemale* (FACW). Trees identified as dominant vegetation for this data point are due to the presence of canopy cover. Trees were located at the top of bank of UNT 1. Hydrologic indicators included Saturation (A3), Sediment Deposits (B2), Drainage Patterns (B10), Geomorphic Position (D2), and FAC-Neutral Test (D5). The hydric soil indicator included Depleted Matrix (F3). Although the banks of UNT 1 met all three criteria to be considered a wetland, because the hydrophytic vegetation was confined to the banks of UNT 1, this is considered a feature of UNT 1 and not a separate wetland resource. DPs 9 and 10 included in Appendix B are representative of the upland area surrounding UNT 1.

3.3 Non-Jurisdictional Features and Non-Wetland Data Points

3.3.1 Non-Jurisdictional Features

No surface drainage system (constructed roadside ditch) are present along SR 75 within the investigated area.

3.3.2 Non-Wetland Data Points

DPs 4 and 9 were collected to characterize the upland conditions in the remaining two quadrants of the SR 75 bridge crossing over Wildcat Creek. DP 4 was collected along the north bank of Wildcat, west of SR 75. DP 9 was collected along the north bank of Wildcat, east of SR 75. In addition, DP 3 was collected to characterize the upland conditions along the western side of SR 75. DP 3 and DP 4 met hydric soil criteria but did not meet hydrophytic vegetation or wetland hydrology criteria. DP 9 met hydrophytic vegetation criteria, but did not meet hydric soil or wetland hydrology criteria.

4.0 Conclusions

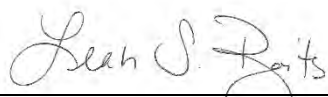
Two emergent wetlands (Wetlands A and B) and two streams (Wildcat Creek and UNT 1) were identified within the investigated area. The total delineated wetland acreage within the investigated area is approximately 0.126 acre. Stream length within the investigated area totals approximately 730 linear feet (0.483 acre). Wetlands A and B, and UNT 1 appear to have a jurisdictional connection to Wildcat Creek, which drains to the Wabash River, a TNW. Therefore, these features are anticipated to be considered “waters of the U.S.”

All jurisdictional “waters of the U.S.” are under the regulatory authority of the USACE under Section 404 of the Clean Water Act. Every effort should be taken to avoid and minimize impacts to the waterway and wetlands. If impacts are necessary, then mitigation may be required. The INDOT Environmental Services Division and the LaPorte District Environmental Unity should be contacted immediately if impacts will occur. The final determination of jurisdictional waters is ultimately made by the USACE. This report is our best judgment based on the guidelines set forth by the USACE.

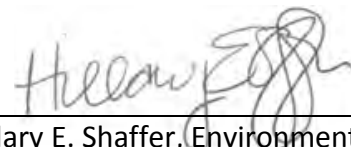
5.0 Acknowledgement

This waters determination has been prepared based on the best available information, interpreted in the light of the investigator’s training, experience and professional judgement in conformance with the 1987 *Corps of Engineers Wetlands Delineation Manual*, the appropriate regional supplement, the USACE *Jurisdictional Determination Form Instructional Guidebook*, and other appropriate agency guidelines.

AUTHORS:



Leah S. Boits, Project Manager
American Structurepoint, Inc.



Hillary E. Shaffer, Environmental Specialist
American Structurepoint, Inc.

6.0 References

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- US Geological Survey. *Rossville, Indiana* [map]. 1967. 1:24,000. 7.5 Minute Series. Reston, Va: United States Department of the Interior, USGS.
- U. S. Army Corps of Engineers. 2010. *Regional Supplement to the Corps of Engineers Wetland Delineation Manual: Midwest Region (Version 2.0)*, ed. J. S. Wakeley, R. W. Lichvar, and C. V. Noble. ERDC/EL TR-10-16. Vicksburg, MS: U.S. Army Engineer Research and Development Center.

Table 1 – Data Points Summary

Data Points Summary						
Photos	Data Point					
		Water Resources	Hydrophytic Vegetation	Wetland Hydrology	Hydric Soils	Within a Wetland
1-2	1	Wetland A	Yes	Yes	Yes	Yes
3-4	2	Wetland A	No	No	Yes	No
6-7	3	N/A	No	No	Yes	No
8-9	4	N/A	No	No	Yes	No
10-11	5	Wetland B	Yes	Yes	Yes	Yes
12-13	6	Wetland B	Yes	No	No	No
17-18	7	Wetland B	No	No	No	No
19-20	8	Wetland B	Yes	Yes	Yes	Yes
26-27	9	UNT 1	Yes	No	No	No
29-30	10	UNT 1	No	No	No	No
31-32	11	UNT 1	Yes	Yes	Yes	No*

**Although this data point met all three wetland criteria, the hydrophytic vegetation was confined to the banks of UNT 1. Therefore, this area is considered to be part of UNT 1 and not a separate wetland feature.*

Table 2 – Aquatic Resources Summary

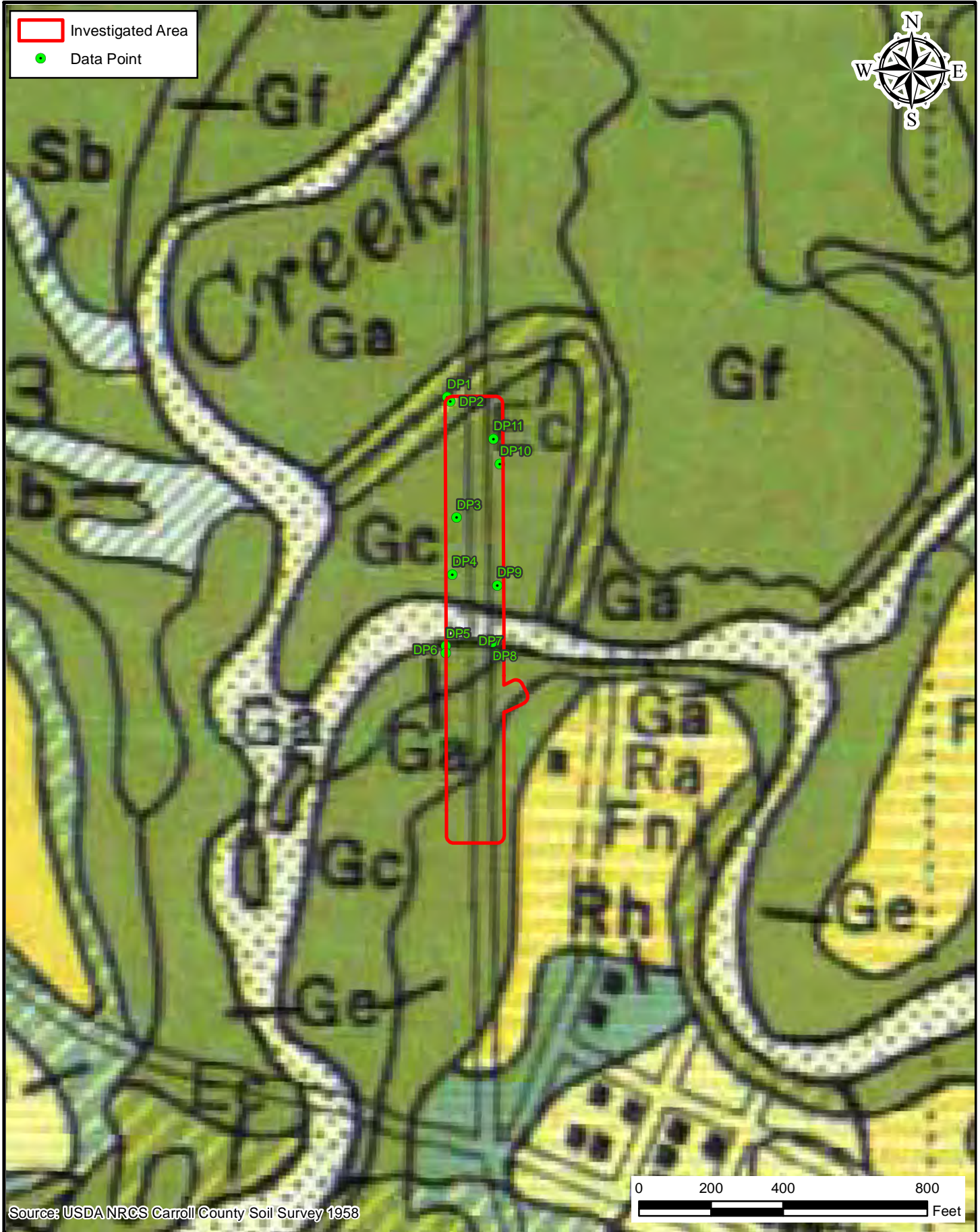
Aquatic Resources Summary: Wetlands						
Delineated Resource	Photos	Lat/ Long	Type	Quality	Jurisdiction	Total Acreage within Investigated Area
Wetland A	1-2	40.483532/ -86.530325	PEM1C	Poor	"waters of the U.S."	0.002
Wetland B	10-11, 18-20	40.481660/ -86.529954	PEM1C	Average	"waters of the U.S."	0.124
Total						0.126

Aquatic Resources Summary: Streams											
Delineated Resource	Photos	Lat/ Long	USGS Blue Line	OHWB Width	OHWB Depth	Narrative Quality*	Riffle/Run Presence	Substrate	Jurisdiction	Total Linear Feet within Investigated Area	Total Acres within Investigated Area
Wildcat Creek	18, 21, 23-25, 34-36	40.481800/ -86.530088	Yes	110 feet	2 feet	Good	10% riffle/ 40% run	Sand, Cobble, Silt, Gravel	"waters of the U.S."	160	0.404
UNT 1	26-28, 31-33	40.482357/ -86.529865	No	6 feet	4 inches	Poor	100% run	Silt, Sand, Cobble, Gravel, Detritus	"waters of the U.S."	570	0.079
Total										730	0.483

*Narrative Quality was determined using the Qualitative Habitat Evaluation Index (QHEI) Narrative Rating

Investigated Area

Data Point



Source: USDA NRCS Carroll County Soil Survey 1958



AMERICAN
STRUCTUREPOINT
INC.

1958 Carroll County
Soil Survey Map

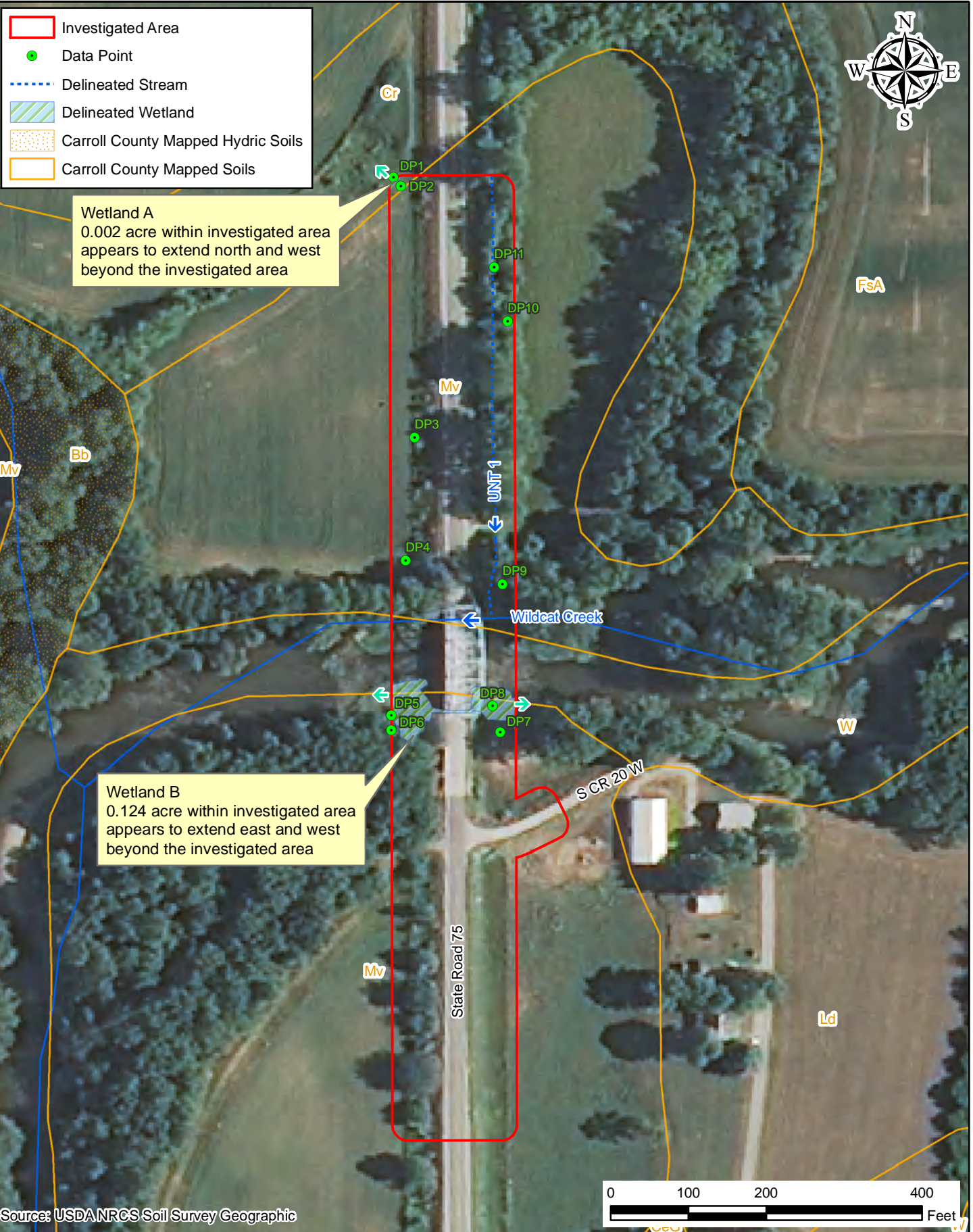
Indiana Department of Transportation
100 N. Senate Ave., IGCN 755
Indianapolis, Indiana 46204

State Road 75 over Wildcat Creek
Bridge Improvement
Des. No. 1601029

Location: Cutler
Township: Democrat
County: Carroll
State: Indiana
Date: 10/06/2017
Appendix F
Page F-12

Path: P:\2017\002\201D Drawings\Environmental\ArcView\Maps\2017_00220.EV\2017-09-27 map.SR75WildcatCreek SoilSurvey.mxd Date: 10/6/2017 User:mdelreal

Path: \\indySANIP\Projects\2017\00220\Drawings\Environmental\ArcView\Map\2017-09-27 map.SR75WildcatCreek.SSURGO.mxd Date: 7/26/2018 User:iboits



Source: USDA NRCS Soil Survey Geographic



Carroll County SSURGO

Indiana Department of Transportation
100 N. Senate Ave., IGCN 755
Indianapolis, Indiana 46204

State Road 75 over Wildcat Creek Bridge Replacement

Des. No. 1601029

Location: Cutler

Township: Democrat

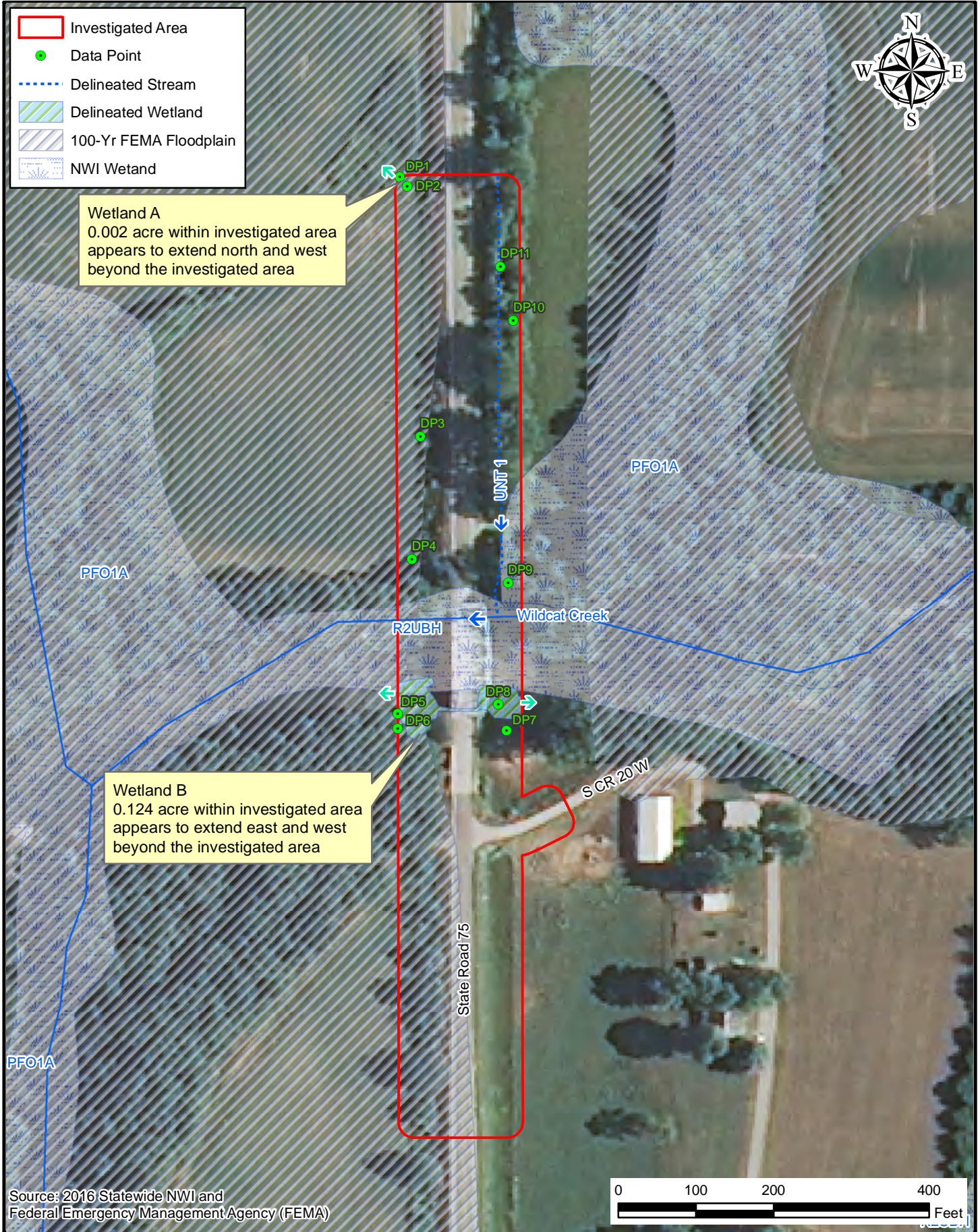
County: Carroll

State: Indiana

Date: 10/06/2017

Appendix F
Page F-13

Path: \\indySANP\Projects\201700220\Drawings\Environmental\ArcView\Map\2017-09-27 map.SR75WildcatCreekNWI_FEMA.mxd Date: 7/26/2018 User: liboits



Source: 2016 Statewide NWI and Federal Emergency Management Agency (FEMA)



National Wetland Inventory and 100-Year FEMA Map

Indiana Department of Transportation
100 N. Senate Ave., IGCN 755
Indianapolis, Indiana 46204

State Road 75 over Wildcat Creek Bridge Replacement

Des. No. 1601029

Location: Cutler

Township: Democrat

County: Carroll

State: Indiana

Date: 10/06/2017

Appendix F

Page F-14

Path: \\VndySAN\P\Projects\201700220\Drawings\Environmental\ArcView\Maps\2017 00220.E\2018-06-22 map.SR75WildcatCreek.LiDar.hes.mxd Date:6/22/2018 User:Hshaffer

Project Area

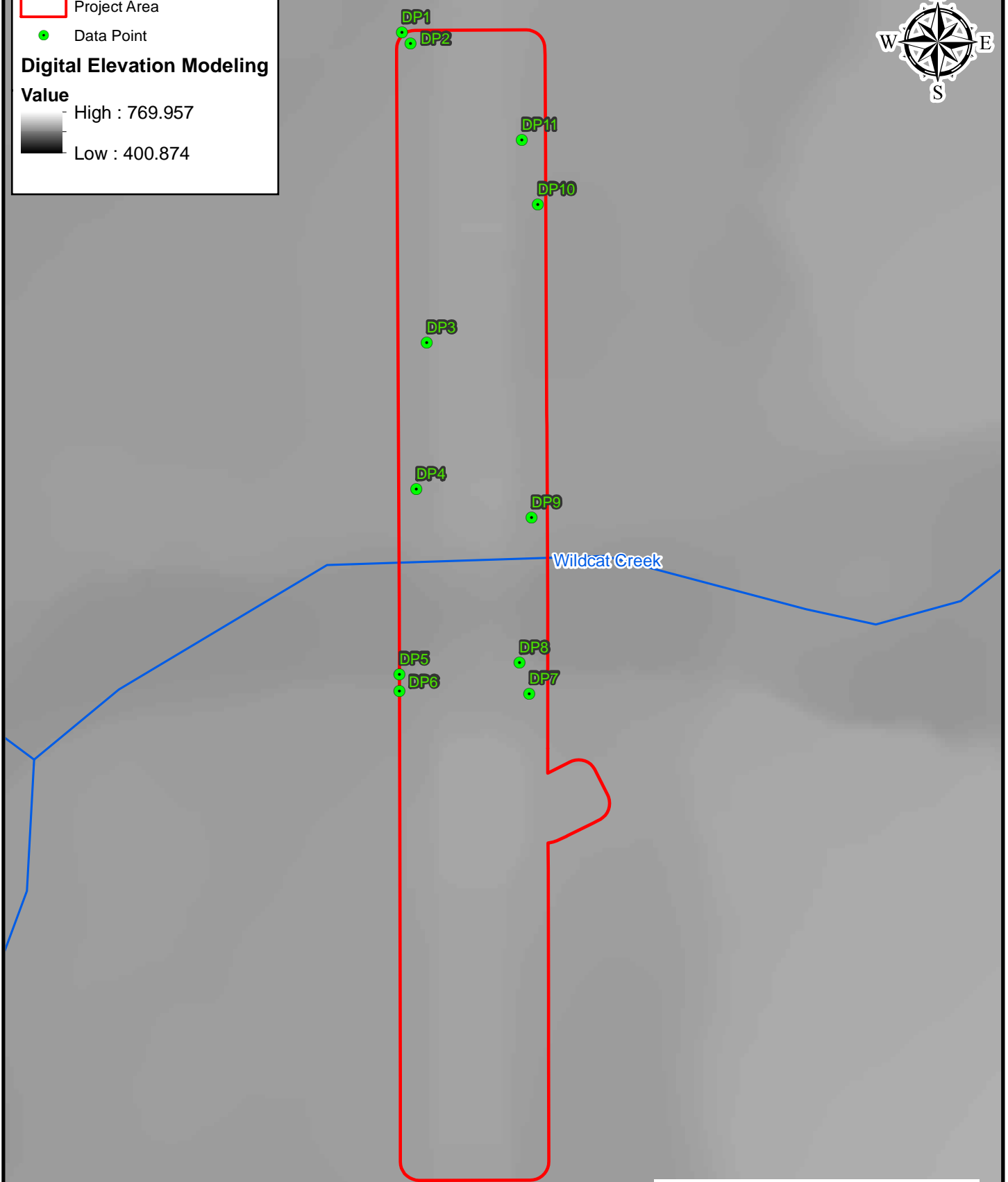
Data Point

Digital Elevation Modeling

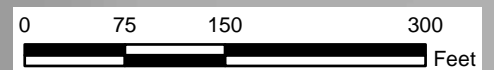
Value

High : 769.957

Low : 400.874



Source: 2005 IndianaMap
Digital Elevation Modeling



LiDar Mapping

Indiana Department of Transportation
100 N. Senate Ave., IGCN 755
Indianapolis, Indiana 46204

State Road 75 over Wildcat Creek Bridge Replacement

Des. No. 1601029

Location: Cutler

Township: Democrat

County: Carroll

State: Indiana

Date: 06/22/2018

Appendix F
Page F-15

 Investigated Area



Wildcat Creek

SCR 20 W

State Road 75

Source: 2005 IndianaMap Aerial Photography

0 100 200 400
Feet



2005 Aerial Photography Map

Indiana Department of Transportation
100 N. Senate Ave., IGCN 755
Indianapolis, Indiana 46204

State Road 75 over Wildcat Creek Bridge Improvement

Des. No. 1601029

Location: Cutler

Township: Democrat

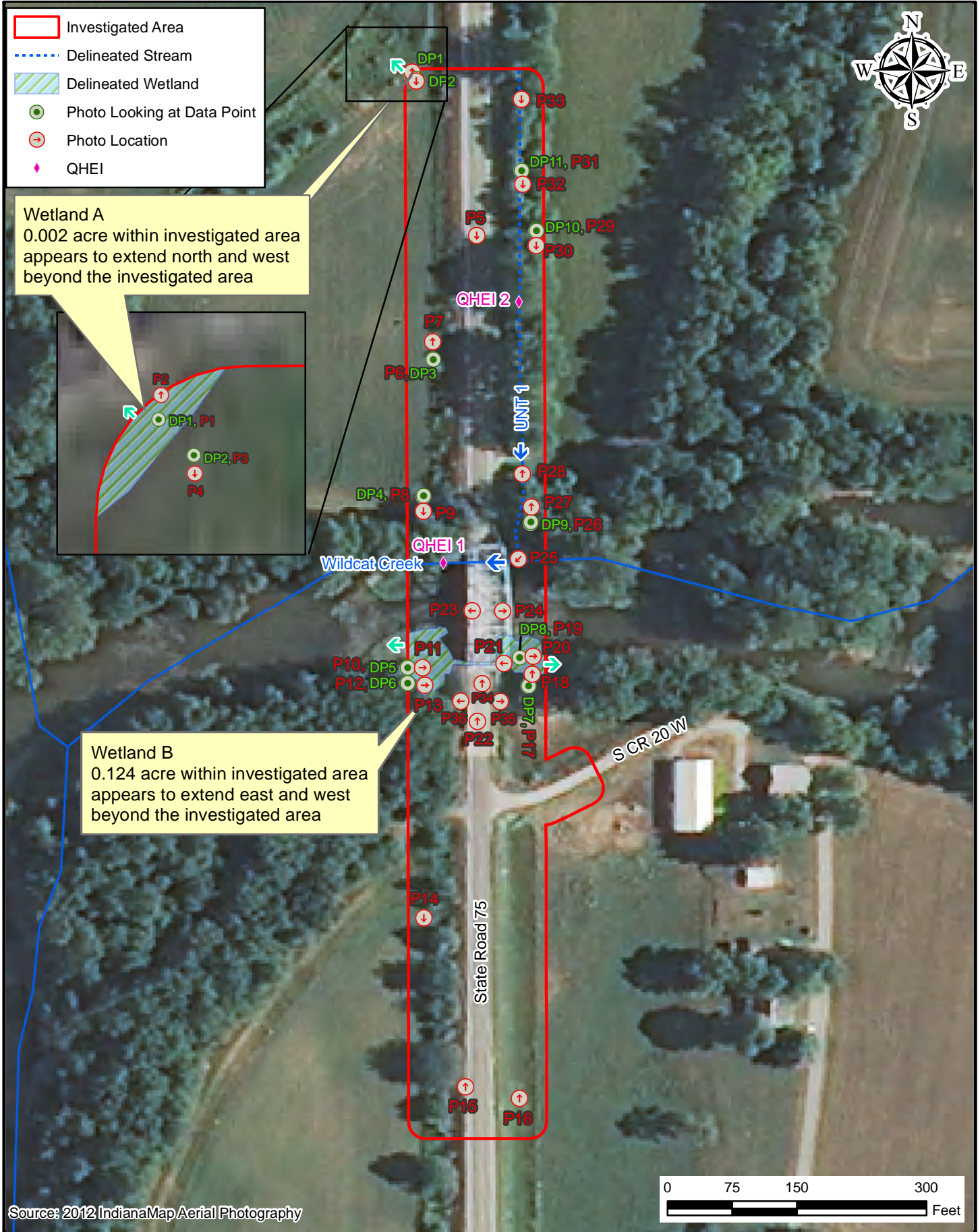
County: Carroll

State: Indiana

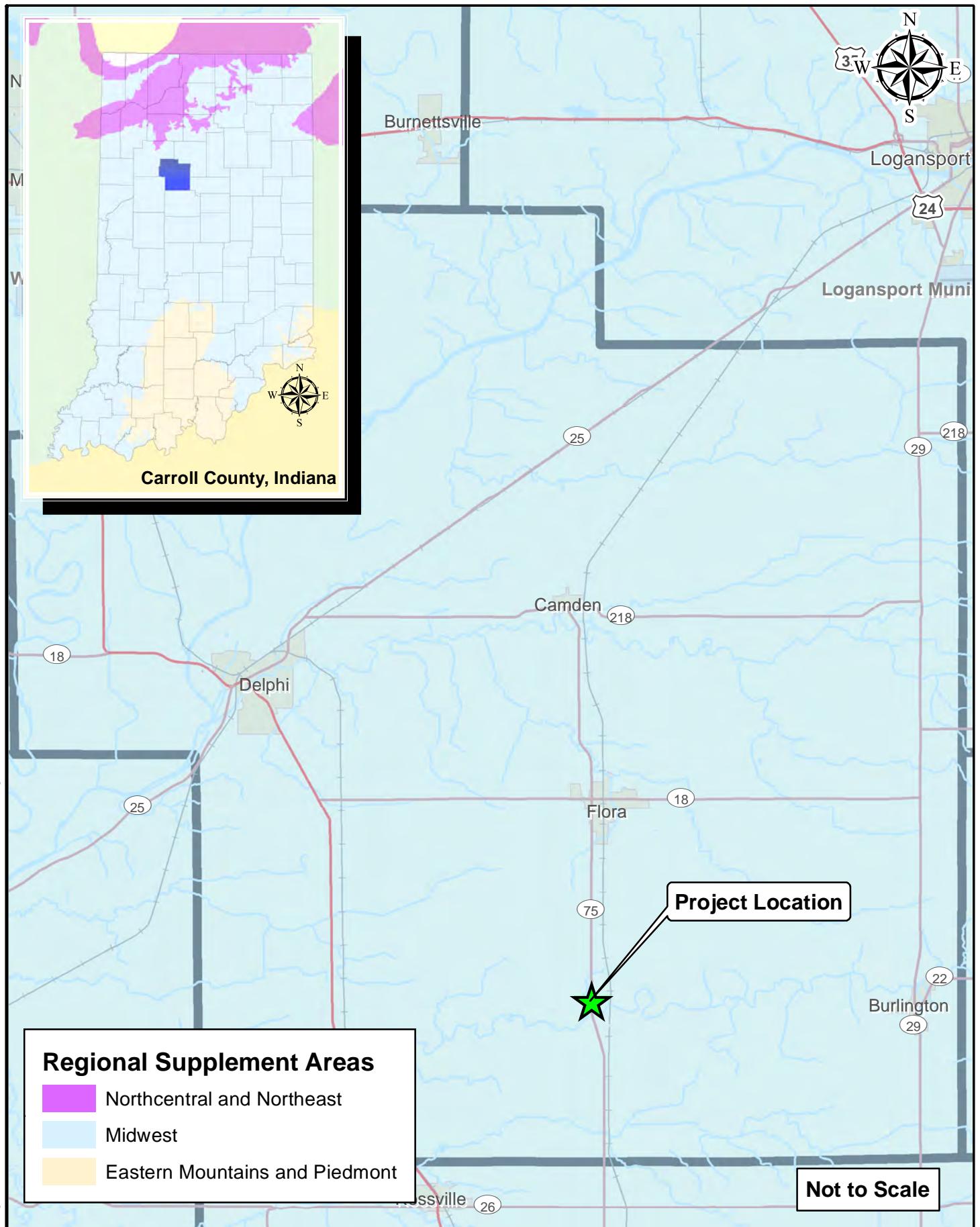
Date: 10/06/2017

Appendix F
Page F-16

Path: \\indySAN\P\Projects\2017\00220\1D. Drawings\Environmental\ArcView\Map\2017-09-27 map.SR75WildcatCreek Photos.mxd Date: 7/26/2018 User: libbols



 <p>AMERICAN STRUCTUREPOINT INC.</p>	<p>Field Investigation and Photo Location Map</p> <p>Indiana Department of Transportation 100 N. Senate Ave., IGCN 755 Indianapolis, Indiana 46204</p>	<p>State Road 75 over Wildcat Creek Bridge Improvement Des. No. 1601029 Location: Cutler Township: Democrat County: Carroll State: Indiana</p> <p>Date: 10/06/2017</p> <p>Appendix F Page F-17</p>
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**State Road 75 over Wildcat Creek Bridge Improvement
Democrat Township, Carroll County, Indiana
Des. No. 1601029
October 5, 2017**



Photo 1. Looking at DP 1 (Wetland A) west of SR 75 and north of Wildcat Creek.



Photo 2. Looking north from DP 1 toward Wetland A, west of SR 75 and north of Wildcat Creek.

**State Road 75 over Wildcat Creek Bridge Improvement
Democrat Township, Carroll County, Indiana
Des. No. 1601029
October 5, 2017**



Photo 3. Looking at DP 2 and upland vegetation surrounding Wetland A.



Photo 4. Looking south from DP 2 toward upland vegetation surrounding Wetland A.

**State Road 75 over Wildcat Creek Bridge Improvement
Democrat Township, Carroll County, Indiana
Des. No. 1601029
October 5, 2017**



Photo 5. Looking south along SR 75 toward the bridge over Wildcat Creek.



Photo 6. Looking at DP 3

**State Road 75 over Wildcat Creek Bridge Improvement
Democrat Township, Carroll County, Indiana
Des. No. 1601029
October 5, 2017**



Photo 7. Looking north from DP 3



Photo 8. Looking at DP 4 and the upland area in the northwest quadrant of the SR 75 bridge over Wildcat Creek.

**State Road 75 over Wildcat Creek Bridge Improvement
Democrat Township, Carroll County, Indiana
Des. No. 1601029
October 5, 2017**



Photo 9. Looking south from DP 4 toward the upland area in the northwest quadrant of the SR 75 bridge over Wildcat Creek.



Photo 10. Looking at DP 5 (Wetland B) west of SR 75 and south of Wildcat Creek.

**State Road 75 over Wildcat Creek Bridge Improvement
Democrat Township, Carroll County, Indiana
Des. No. 1601029
October 5, 2017**



Photo 11. Looking east from DP 5 toward Wetland B, west of SR 75 and south of Wildcat Creek.



Photo 12. Looking at DP 6 and upland vegetation surrounding Wetland B.

**State Road 75 over Wildcat Creek Bridge Improvement
Democrat Township, Carroll County, Indiana
Des. No. 1601029
October 5, 2017**



Photo 13. Looking east from DP 6 toward upland vegetation surrounding Wetland B.



Photo 14. Looking south at the upland area west of SR 75, south of Wildcat Creek.

**State Road 75 over Wildcat Creek Bridge Improvement
Democrat Township, Carroll County, Indiana
Des. No. 1601029
October 5, 2017**



Photo 15. Looking north along SR 75.



Photo 16. Looking north along the upland area east of SR 75, south of Wildcat Creek.

**State Road 75 over Wildcat Creek Bridge Improvement
Democrat Township, Carroll County, Indiana
Des. No. 1601029
October 5, 2017**



Photo 17. Looking at DP 7 and upland vegetation surrounding Wetland B.



Photo 18. Looking north from DP 7 toward Wetland B.

**State Road 75 over Wildcat Creek Bridge Improvement
Democrat Township, Carroll County, Indiana
Des. No. 1601029
October 5, 2017**



Photo 19. Looking at DP 8 (Wetland B) east of SR 75 and south of Wildcat Creek.



Photo 20. Looking east from DP 8 toward Wetland B, east of SR 75 and south of Wildcat Creek.

**State Road 75 over Wildcat Creek Bridge Improvement
Democrat Township, Carroll County, Indiana
Des. No. 1601029
October 5, 2017**



Photo 21. Looking west from DP 8 toward the SR 75 bridge over Wildcat Creek.



Photo 22. Looking north along the SR 75 bridge over Wildcat Creek.

**State Road 75 over Wildcat Creek Bridge Improvement
Democrat Township, Carroll County, Indiana
Des. No. 1601029
October 5, 2017**



Photo 23. Looking west along Wildcat Creek from the SR 75 bridge.



Photo 24. Looking east along Wildcat Creek from the SR 75 bridge.

**State Road 75 over Wildcat Creek Bridge Improvement
Democrat Township, Carroll County, Indiana
Des. No. 1601029
October 5, 2017**



Photo 25. Looking southwest toward the SR 75 bridge and Wildcat Creek.



Photo 26. Looking at DP 9 and the upland vegetation surrounding UNT 1 and the northeast quadrant of SR 75 over Wildcat Creek.

**State Road 75 over Wildcat Creek Bridge Improvement
Democrat Township, Carroll County, Indiana
Des. No. 1601029
October 5, 2017**



Photo 27. Looking north from DP 9 and the upland vegetation surrounding UNT 1 and the northeast quadrant of SR 75 over Wildcat Creek.



Photo 28. Looking north along UNT 1 toward a culvert under an access drive.

**State Road 75 over Wildcat Creek Bridge Improvement
Democrat Township, Carroll County, Indiana
Des. No. 1601029
October 5, 2017**



Photo 29. Looking at DP 10 and the upland vegetation surrounding UNT 1.



Photo 30. Looking south from DP 10 and the upland vegetation surrounding UNT 1.

**State Road 75 over Wildcat Creek Bridge Improvement
Democrat Township, Carroll County, Indiana
Des. No. 1601029
October 5, 2017**



Photo 31. Looking at DP 11 (UNT 1) east of SR 75 and north of Wildcat Creek.



Photo 32. Looking south from DP 11 toward hydrophytic vegetation along the banks of UNT 1, east of SR 75 and north of Wildcat Creek.

**State Road 75 over Wildcat Creek Bridge Improvement
Democrat Township, Carroll County, Indiana
Des. No. 1601029
October 5, 2017**



Photo 33. Looking south along UNT 1.



Photo 34. Looking north toward Wildcat Creek from beneath the bridge.

**State Road 75 over Wildcat Creek Bridge Improvement
Democrat Township, Carroll County, Indiana
Des. No. 1601029
October 5, 2017**



Photo 35. Looking east along Wildcat Creek from beneath the bridge
(Wetland B is on right side of photo)



Photo 36. Looking west along Wildcat Creek from beneath the bridge
(Wetland B is on the left side of the photo).

Perry, Leah

From: Landry, James <JLandry@indot.IN.gov>
Sent: Tuesday, August 7, 2018 1:42 PM
To: Boits, Leah; Hockaday, Jacqueline R
Cc: Shaffer, Hillary; Todd, Kristi (INDOT)
Subject: APPROVED Waters Report for DES 1601029 - SR 75 over Wildcat Creek
Attachments: ENV DraftWatersDoc 1601029 for Environmental Services.pdf

Thank you for submitting the waters report for **the SR 65 over Wildcat Creek Bridge Improvement**, Designation **DES 1601029**. The approved report is attached and can also be found on Projectwise through this link: [1601029 Final Waters Report](#) . *It is the responsibility of the Project Manager to forward a copy of this report to the Project Designer.*

The information in this report should be used by the Project Designer to determine if waters of the U.S. will be impacted by the project. Avoidance and minimization of impacts must occur *before* mitigation will be considered. If mitigation is required, the Project Manager or Project Designer must coordinate with the Ecology and Waterway Permitting Office to discuss how adequate compensatory mitigation will be provided.

The Project Manager should notify the Ecology and Waterway Permitting Office if there is any change to the project footprint presented in this report. Such changes may require additional fieldwork and submittal of an updated waters report covering areas not previously investigated. *This report is only valid for a period of five years from the date of earliest fieldwork.* If the report expires prior to waterway permit application submittal, additional fieldwork and a revised waters report will be required.

It will not be sent to the United States Army Corps of Engineers (USACE) or the Indiana Department of Environmental Management (IDEM) until the waterways permit applications are submitted to these agencies.

Thank you,

James Landry

Environmental Manager

INDOT - Ecology and Waterways Permitting Office

100 N. Senate Ave, Room N642

Indianapolis, IN 46204

Office: (317) 234-5241

Email: JLandry@indot.IN.gov





Indiana Floodplain Information Portal Report

Point of Interest

Effective Flood Zone:

A

Preliminary Flood Zone:

N/A

Best Available Flood Zone:

A

Approximate Flood Elevation:

667.6ft NAVD88

Source:

Zone A Model Delineation

Nearest Stream:

WILDCAT CREEK

Map Legend



Point of Interest



Nearest Point on Stream

Effective Flood Zone



0.2% Annual Chance Flood Hazard



1% Annual Chance Flood Hazard - Zone A (Approximate Study)



1% Annual Chance Flood Hazard - Zone AE (Detailed Study)



1% Annual Chance Flood Hazard - Floodway



1% Annual Chance Flood Hazard - Zone AH



1% Annual Chance Flood Hazard - Zone AO



Zone X - Protected by Levee

Site Map with Effective Flood Zone



Approximate scale 1:2,400

Disclaimer

This data is a digital representation of the former paper Flood Insurance Rate Maps (FIRMs) for counties that have completed the Map Modernization Initiative. The data on counties derived from the official FEMA digital products (DFIRM) represent official FEMA designations of the Special Flood Hazard Areas. This data can be used for official National Flood Insurance Program (NFIP) purposes in accordance with the FEMA Mitigation Directorate Policy document titled "Use of Digital Flood Hazard Data" dated November 29, 2007. For the non-modernized counties, the Effective is enhanced by the addition of the floodplain data from digitized paper copies of the FIRMs and the information should be considered advisory only. For these non-modernized counties, the paper maps are the official FEMA documents for regulatory and insurance purposes. Once the NFHL is official, the Effective is updated with the newly published information. For the status of counties published by FEMA please see <http://www.floodmaps.fema.gov/NFHL/status.shtml>.

Generated on Tuesday March 17th 2020 at 08:24:32am



June 16, 2020

Example Notice of Survey Letter

Re: Notice of Survey and Environmental Work
SR 75 over Wildcat Creek (Des #1601029)
Cutler, Indiana

Dear Property Owner:

American Structurepoint, Inc., has been retained by the Indiana Department of Transportation to perform survey and environmental work for a road improvement project along State Road 75 in Democrat Township, Carroll County, Indiana. The project limits begin approximately 350 feet north and south of the SR 75 bridge over Wildcat Creek.

Our information indicates you either own or occupy property near this proposed improvement project. Our employees will begin conducting a topographic survey and environmental survey of the project area in the near future and may continue for several weeks. It may be necessary for us to enter onto your property (exterior only) to complete this work. The work may include, but is not limited to shovel probes for archeological studies and wetland identification; topographic survey; photographing; and geotechnical surveys. The information we obtain from the above-mentioned work is necessary for the development of this transportation project. Our employees have been instructed to identify themselves to you, if you are available, before they enter onto your property. If you no longer own this property, or it is currently occupied by someone other than yourself, please let us know the name and/or address of the new owner or occupant so we may contact them about the survey.

Please be advised that you have the right to be compensated for damage that occurs to your property as a result of the entry upon, over, or under your property or work performed during the entry.

Please be assured of our sincere desire to cause you as little inconvenience as possible during this survey. If you have any questions or concerns, please contact me at (317) 547-5580.

Very truly yours,
American Structurepoint, Inc.

David A. Day, PE
Senior Project Manager, Bridge Group



INDIANA DEPARTMENT OF TRANSPORTATION

LEGAL NOTICE OF PUBLIC HEARING - DES. #1601029 Proposed Bridge Improvements in Cutler, Carroll County, Indiana

The Indiana Department of Transportation (INDOT) will host a public hearing on Thursday, July 1, 2021 at the Cutler General Store/Cutler Merchant Building located at 73 East 485 South, Cutler, IN 46920, the hearing will begin at 6:00 p.m. The purpose of the public hearing is to offer all interested persons an opportunity to comment on current preliminary design plans to improve a historic bridge on SR 75 over Wildcat Creek in Cutler, Carroll County, IN. The purpose of the project is to improve the legal load rating of the bridge to 36 tons or more so that restrictions are not required according to FHWA Load Rating policy, to improve the superstructure rating from a 5 to a 7 (good), and to improve the wearing surface rating from 5 to an 8 (very good) or 9 (new).

As proposed, the project involves rehabilitation of the existing structure for continued vehicular use by increasing the structural capacity to at or above the required load rating. The existing bridge deck will be removed from both spans and a new concrete deck will be constructed. The existing approach slabs will be removed and replaced. Additional rehabilitation of the deck will include installing a new expansion joint at pier 2, creating a new drainage system for the structure and replacing pavement markings in-kind matching previous roadway striping.

Shear studs will be installed on top of beams to increase load carrying capacity. Damaged or deteriorated stringers or other structural steel components will be cleaned and patched or replaced "in-kind." Existing railing will be replaced along with railing transitions at all four ends. The original railing (constructed in 1947) will stay in place. Approximately 88 rivets that have been determined inadequate will be replaced with high strength bolts to increase the load carrying capacity. After the repairs are done, the structural steel will be painted. To meet minimum useable shoulder width requirements, the lane width will be reduced from 11.75-feet to 11-feet to increase the shoulder width to 3-feet. The structure length will be increased by 0.89 feet from 240 feet to approximately 240.89 feet.

The Maintenance of Traffic (MOT) plan for the project will require a full closure of the bridge and a detour during construction utilizing SR 18, US 421, and SR 26, a distance of approximately 19.5 miles from closure point to closure point. The closure of the bridge will be limited to June-August. MOT details will be presented during the public hearing. Access to all properties will be maintained during construction. INDOT will coordinate with emergency services, local school corporation officials and project stakeholders to ensure potential disruptions and impacts are minimized as much as possible. The project is not anticipated to require any permanent or temporary right-of-way.

The hearing will include appropriate social distancing measures. Attendees are advised that face coverings and adherence to CDC guidelines for social distancing are recommended. Hand-sanitization stations as well as hand-washing facilities will be available.

Federal and state funds are proposed to be used for construction of this project. INDOT and the Federal Highway Administration have agreed that this project poses minimal impact to natural environment.

A Categorical Exclusion (CE) environmental document has been prepared for the project. This document and project information can be mailed upon request. The environmental documentation and preliminary design information is available to view prior at the following locations:

1. Flora Public Library (109 N. Center Street, Flora, IN 46929)
2. American Structurepoint, Inc. webpage: www.structurepointpublic.com/sr75wildcat
3. INDOT LaPorte District webpage: <https://www.in.gov/indot/2705.htm>

A project webpage and virtual open house will be created prior to the public hearing to ensure project information, including the hearing presentation, is available on-line via the American Structurepoint, Inc. webpage (<http://www.structurepointpublic.com/sr75wildcat>) and will be linked on the INDOT LaPorte District page (<https://www.in.gov/indot/2705.htm>).

Public statements for the record will be taken as part of the public hearing procedure. All verbal statements recorded during the public hearing and all written comments submitted prior to, during and for a period of two (2) weeks following the hearing date, will be evaluated, considered and addressed in subsequent environmental documentation. Written comments may be submitted prior to the public hearing and within the comment period to: Leah Perry, at American Structurepoint, Inc., 9025 River Road, Suite 200, Indianapolis, Indiana 46240. E-Mail: lperry@structurepoint.com. Comments can also be submitted on the project webpage: <http://www.structurepointpublic.com/sr75wildcat>. **INDOT respectfully requests comments be submitted by July 16, 2021.**

With advance notice, INDOT will provide accommodations for persons with disabilities with regards to participation and access to project information as part of the hearings process including arranging auxiliary aids, interpretation services for the hearing impaired, services for the sight impaired and other services as needed. In addition, INDOT will provide accommodations for persons of Limited English Proficiency (LEP) requiring auxiliary aids including language interpretation services and document conversion. Should accommodation be required please contact Leah Perry, at American Structurepoint, Inc., 9025 River Road, Suite 200, Indianapolis, Indiana 46240. E-Mail: lperry@structurepoint.com.

This notice is published in compliance with Code of Federal Regulations, Title 23, Section 771 (CFR 771.111(h)(1) states: "Each State must have procedures approved by the FHWA to carry out a public involvement/public hearing program." 23 CFR 450.212(a)(7) states: "Public involvement procedures shall provide for periodic review of the effectiveness of the public involvement process to ensure that the process provides full and open access to all and revision of the process as necessary." approved by the Federal Highway Administration, U.S. Department of Transportation on August 16, 2012.

INDIANA DEPT. OF TRANSPORTATION
(Governmental Unit)
DELPHI, INDIANA

To:

CARROLL COUNTY COMET
P.O. BOX 26
FLORA, IN 46929

PUBLISHER'S CLAIM

LINE COUNT

Display Master (Must not exceed two actual lines, neither of which shall
total more than four solid lines of type in which the body of the
advertisement is set) -- number of equivalent lines.....
Head -- number of lines.....
Body -- number of lines.....
Tail -- number of lines.....
Total number of lines in notice.....

COMPUTATION OF CHARGES

191 lines, 1 column wide equals 191 equivalent lines
at .7522 cents per line..... \$143.67
Additional charge for notices containing rule or tabular work
(50 percent of above amount).....
Charge for extra proofs of publication (\$1.00 for each proof
in excess of two).....
TOTAL AMOUNT OF CLAIM..... \$143.67

DATA FOR COMPUTING COST

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Number of insertions 2

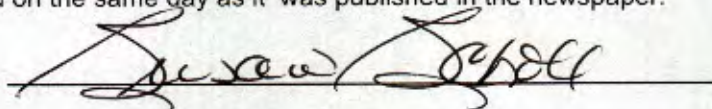
Pursuant to the provisions and penalties of IC 5-11-10-1, I hereby certify that the foregoing account is just and correct, that the amount claimed is legally due, after allowing all just credits, and that no part of the same has been paid.

I also certify that the printed matter attached heretto is a true copy, of the same column width and type size, which was duly published in said paper 2 times. The dates of publication being as follows:

JUNE 16 & 23, 2021

Additionally, the statement checked below is true and correct::

The Comet has a website and this public notice was posted on the same day as it was published in the newspaper.



Date: JUNE 16 & 23, 2021

Title:

EDITOR

Claim No. _____ Warrant No. _____

IN FAVOR OF

CARROLL COUNTY COMET

P.O. BOX 26, FLORA IN 46929

\$143.67

ON ACCOUNT OF APPROPRIATION FOR

PUBLIC HEARING ON SR 75 BRIDGE PROJECT

Appropriation No. _____

ALLOWED _____

IN THE SUM OF \$ _____

Attest

I have examined the within claim and hereby certify as follows:

That it is in proper form.

That it is duly authenticated as required by law.

That it is based upon statutory authority.

That it is apparently

correct

incorrect

I certify that the within claim is true and correct; that the services there in itemized and for which charge is made were ordered by me and were necessary to the public business

LEGAL NOTICE
OF PUBLIC HEARING
- DES. #1601029

Proposed Bridge Improvements
in Cutler,
Carroll County, Indiana

The Indiana Department of Transportation (INDOT) will host a public hearing on Thursday, July 1, 2021 at the Cutler General Store/Cutler Merchant Building located at 73 East 485 South, Cutler, IN 46929. The hearing will begin at 6:00 p.m. The purpose of the public hearing is to offer all interested persons an opportunity to comment on current preliminary design plans to improve a historic bridge on SR 75 over Wildcat Creek in Cutler, Carroll County, IN. The purpose of the project is to improve the legal load rating of the bridge to 36 tons or more so that restrictions are not required according to FHWA Load Rating policy, to improve the superstructure rating from a 5 to a 7 (good), and to improve the wearing surface rating from 5 to an 8 (very good) or 9 (new).

As proposed, the project involves rehabilitation of the existing structure for continued vehicular use by increasing the structural capacity to at or above the required load rating. The existing bridge deck will be removed from both spans and a new concrete deck will be constructed. The existing approach slabs will be removed and replaced. Additional rehabilitation of the deck will include installing a new expansion joint at pier 2, creating a new drainage system for the structure and replacing pavement markings in-kind matching previous roadway striping.

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The Maintenance of Traffic (MOT) plan for the project will require a full closure of the bridge and a detour during construction utilizing SR 18, US 421, and SR 26, a distance of approximately 19.5 miles from clo-

sure point to closure point. The closure of the bridge will be limited to June-August. MOT details will be presented during the public hearing. Access to all properties will be maintained during construction. INDOT will coordinate with emergency services, local school corporation officials and project stakeholders to ensure potential disruptions and impacts are minimized as much as possible. The project is not anticipated to require any permanent or temporary right-of-way.

The hearing will include appropriate social distancing measures. Attendees are advised that face coverings and adherence to CDC guidelines for social distancing are recommended. Hand-sanitization stations as well as hand-washing facilities will be available.

Federal and state funds are proposed to be used for construction of this project. INDOT and the Federal Highway Administration have agreed that this project poses minimal impact to natural environment. A Categorical Exclusion (CE) environmental document has been prepared for the project. This document and project information can be mailed upon request. The environmental documentation and preliminary design information is available to view prior at the following locations:

1. Flora Public Library (109 N. Center Street, Flora, IN 46929)
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<http://www.structurepointpublic.com/sr75wildcat>. INDOT respectfully requests comments be submitted by July 16, 2021.

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hpaxlp 24

Public Involvement Mailing List

Project Name:	SR 75 over Wildcat Creek Bridge Improvement
Route/Street:	SR 75
DES No:	1601029
Location:	Cutler, Carroll County, Indiana
ASI Project No:	2017.00220

Name	Title	Attn:	Agency/Company	Address 1	Address 2	City	State	Zip	Delivery Method	Email Addresses
Mr. Edwin Buswell	Executive Director		Kankakee - Iroquois Regional Planning Commission							
Mr. Paul Coutts			Carroll County Highway Department							
Mr. Ethan H. Brown	District 3		Carroll County Council							
Mr. Kamron Yates			Area Plan Commission							
Mr. Michael Fincher			Carroll County Emergency Management							
			Democrat Township Volunteer Fire Department							
			Carroll County Drainage Boards							
Mr. Tobe H. Leazenby	Sheriff		Carroll County Sheriff							
Mr. Donald Shockley	County Surveyor		Carroll County Surveyor							
Mr. Keith Thackery	Superintendent		Carroll Consolidated School Corporation							
Mr. Ted Johnson	Attorney		Board of Commissioners of Carroll County							
Mr. Stewart Michels	Environmental Section Manager		INDOT LaPorte District Office							
Fa-Ra Farms LLC										
HP LLC										
Roth, Donald L & Sandra L										
Ms. Mary Kennedy	Historic Bridge Specialist		Indiana Department of Transportation							
Mr. Chad Slider	Assistant Director		Indiana Department of Natural Resources							
Mr. William Brown	Chairman		Carroll County Commissioners							
Mr. Loren Hylton			Carroll County Commissioners							

Name	Title	Attn:	Agency/Company	Address 1	Address 2	City	State	Zip	Delivery Method	Email Addresses
Mr. Steve Pearson	Vice Chairman		Carroll County Commissioners							
Emily Royer	Regional Director		Indiana Landmarks Western Regional Office							
Mr. Randy Myers	President		Carroll County Historical Society and Museum							
Mark Smith	Carroll County Historian									
Mr. Paul Brandenburg	Chair		Historic Spans Task Force							
Ms. Bonnie Maxwell	President		Heartland Heritage, Inc.							
Mr. Joe Mayfield			Delphi Preservation Society							
Mr. Gus Nyberg			NICHES Land Trust							
Mr. Dan McCain			Carroll County Wabash & Erie Canal							
Mr. Al Auffart	President		Adams Mill, Inc.							
Ms. Julia Leahy	Executive Director		Carroll County Chamber of Commerce							
Ms. Mary Blue	President		Promoting Wildcat Valley							
Mr. Dale Brier	Division Director		Indiana Departemnt of Natural Resources Division of Outdoor Recreation							
Mr. Rick Parsons	President		Wildcat Guardians							
Mr. Nathan Holth			HistoricBridges.org							
Ms. Sallie Fahey	Executive Director		Tippecanoe County Area Plan							
Dr. James Cooper	Professor Emeritus of History		DePauw University							
Ms. Mary Mears			Carroll County Historic Bridge Coalition							



SIGN-IN / MAILING LIST

PUBLIC HEARING

SR 75 over Wildcat Creek Bridge Improvement (Des. No. 1601029)
Thursday, July 1, 2021/6:00 p.m.
Cutler General Store/Cutler Merchant Building

Before including your address, phone number, e-mail address, or other personal identifying information on the meeting Sign-In Sheet or on your comment submittal, be advised that your comment - including your personal identifying information - may be publicly available at any time. While you can ask us to withhold personal identifying information from public review, we cannot guarantee that we will be able to do so.

Name	Mailing Address	Email
James Rich		
Charles Book Miller		
Tom Overholser		
Kent Overholser		
Dennis Kern		
Bonnie Maxwell		
Garry Hill		



AMERICAN
STRUCTUREPOINT
INC.

SIGN-IN / MAILING LIST

PUBLIC HEARING

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Name	Mailing Address	Email
William Brown		
Ross Brown		
PAUL BRANDENBURG		
KEVIN GARY OKAMURA		
Cody Miller		
Jeremy Reef		
Bill La Beau		



SPEAKER SIGN-IN SHEET

PUBLIC HEARING

SR 75 over Wildcat Creek Bridge Improvement (Des. No. 1601029)

Thursday, July 1, 2021

Cutler General Store/Cutler Merchant Building at 73 East 485 South, Cutler, IN 46920

NAME

ADDRESS

William Brown (William Brown)	



INDIANA DEPARTMENT OF TRANSPORTATION

Date: June 16, 2021

Dear Local Resident, Interested Citizen, and Elected / Local Public Official:

The Indiana Department of Transportation (INDOT), in partnership with the Federal Highway Administration (FHWA), is developing a federal-aid project at the SR 75 over Wildcat Creek Bridge, located approximately 4.5 miles south of SR 18, extending approximately 185 feet north and 170 feet south of the center of the bridge in Cutler, Carroll County, Indiana (Des. No. 1601029). The purpose of the project is to improve the legal load rating of the bridge to 36 tons or more so that restrictions are not required according to FHWA Load Rating policy, to improve the superstructure rating from a 5 to a 7 (good), and to improve the wearing surface rating from 5 to an 8 (very good) or 9 (new).

American Structurepoint, Inc. is under contract with the INDOT to advance the environmental documentation for the referenced project.

There are several ways your comments may be presented concerning this project. You may submit comments in the following manner:

1. **Complete** a comment form and return it to American Structurepoint, Inc. Comment forms are available for download at the INDOT LaPorte District website (<https://www.in.gov/indot/2705.htm>) and the project website (www.structurepointpublic.com/sr75wildcat). Comment forms will also be available at the public hearing.
2. **Complete** the digital comment box on the project website or within the virtual open house at www.structurepointpublic.com/sr75wildcat.
3. **Participate** as a speaker during the verbal comment session during the public hearing.
4. **E-mail** comments to Leah Perry of American Structurepoint, Inc. at lperry@structurepoint.com.
5. **Mail** comments to Leah Perry at American Structurepoint, Inc., 9025 River Road, Suite 200, Indianapolis, Indiana 46240.
6. **Submit** comments or have comments postmarked by **July 16, 2021**. Comments will be reviewed and considered as part of the INDOT decision making process.
7. **Questions?** Contact Leah Perry of American Structurepoint, Inc. at (317) 547-5580 or lperry@structurepoint.com or INDOT Customer Service 1-855-463-6848 (1-855-INDOT4U) INDOT@indot.in.gov.

All substantive comments received will be evaluated and responded to in writing within subsequent project documentation. The documentation will address concerns presented during the public comment process and describe project decisions reached following careful consideration of the views and concerns of the public.

The draft environmental document is available for public review and inspection at the following locations, dependent upon operating hours:

- Flora Public Library (109 N. Center Street, Flora, IN 46929)
- American Structurepoint, Inc. webpage: www.structurepointpublic.com/sr75wildcat
- INDOT LaPorte District webpage: <https://www.in.gov/indot/2705.htm>

The SR 75 over Wildcat Creek Bridge Improvement Project Team thanks you for your participation in this project.

Project Description

INDOT, in partnership with the Federal Highway Administration (FHWA), is developing a federal-aid project to improve the SR 75 over Wildcat Creek Bridge (INDOT Bridge 075-08-03653 B, National Bridge Inventory (NBI) No. 024970, Historic Bridge (HB)-1897; Indiana Historic Sites and Structures Inventory (IHSSI) No.: 015-553-65014) located approximately 4.5 miles south of SR 18, extending approximately 185 feet north and 170 feet south of the center of the bridge in Cutler, Democrat Township, Carroll County, Indiana.

The need for the proposed project is a result of the insufficient legal load rating, deteriorated condition of the structural steel, superstructure, and wearing surface, and the inadequate usable shoulder width on the bridge.

- The load rating is 30 tons. The legal load rating is 36 tons.

Secondary concerns include:

- Structural steel deficiencies have been observed:
 - Loss of section (approximately 2-inch x 2-inch voids) on several vertical and diagonal truss members.
 - Thinning of the steel plates connecting truss members.
 - Two bottom chord members are fracture critical and some localized rust has been observed near the connections for these members.
 - Deformation of north floor beam from impact during 2001 rehabilitation.
- The superstructure and wearing surface are rated 5 (fair) out of 9. The superstructure has been rated at fair for over a decade. As it continues to rust and deteriorate, a rating of poor is anticipated in the near future, if the “Do Nothing” alternative is selected. With the fracture critical details, it is not advisable to allow superstructure deterioration to advance without addressing.
- The existing shoulder width on the bridge is 2 feet, 3 inches and does not meet current safety standards, which require at least 3 feet.

The purpose of the proposed project is to improve the legal load rating of the bridge to 36 tons or more so that restrictions are not required according to FHWA Load Rating policy, to improve the superstructure rating from a 5 to a 7 (good), and to improve the wearing surface rating from 5 to an 8 (very good) or 9 (new).

As proposed, the project involves rehabilitation of the existing structure for continued vehicular through increasing the structural capacity to at or above the required load rating. The existing bridge deck will be removed from both spans and a new concrete deck will be constructed. The existing approach slabs will be removed and replaced. Additional rehabilitation of the deck will include installing a new expansion joint at pier 2, creating a new drainage system for the structure and replacing pavement markings in-kind matching previous roadway striping.

The top flanges of truss floor beams in span B and main beams in span A will be cleaned. Shear studs will be installed on top of beams to increase load carrying capacity. Damaged or deteriorated stringers or other structural steel components will be cleaned and patched or replaced “in-kind.” The damaged floor beam will be heat straightened. Gusset plates connecting the cross bracing will be replaced. The existing bearing assemblies will remain in place and will be cleaned and painted. Spalled bearing pedestals will be patched. Spalled sections on abutments and pier 2 will be patched as well. Abutment 1 will be converted into a semi integral end bent. Existing railing will be removed and replaced along with railing transition at all four ends. The original railing (constructed in 1947) will stay in place. Existing guardrail will be reset as necessary. Approximately 88 rivets that have been determined inadequate will be replaced with high strength bolts to increase the load carrying capacity. After the repairs are done, the structural steel will be painted. The usable shoulders on the bridge are currently 2.25-feet on each edge. To meet minimum useable shoulder width requirements, the lane width will be reduced from 11.75-feet to 11-feet to increase the shoulder width to 3-feet. The total clear roadway width will remain unchanged. The structure length will be increased by 0.89 feet from 240 feet to approximately 240.89 feet due to converting abutment 1 to a semi-integral configuration. The bridge approach will be tied-in.

Description of Bridge

The existing Parker thru truss consists of two spans, Span A is 61-foot, 6-inch long deck girder supported by beams and Span B is a 175-foot long standard INDOT truss. The bridge was constructed in 1947 and reconstructed most recently in 2001. The bridge carries SR 75 over Wildcat Creek. The bridge has a structure length of 240 feet with an out-to-out coping width of 32 feet. The existing typical roadway section for the bridge consists of two 11.75-foot wide travel lanes (one northbound, one south bound) bordered by 2.25-foot wide usable paved shoulders and bridge railing. The clear roadway width is 28 feet.

The bridge was determined eligible for listing in the National Register of Historic Places (NRHP) in the Indiana Historic Bridge Inventory (IHBI) and was designated as “Non-Select.” According to the IHBI, this bridge is eligible for the NHRP under Criterion C because it represents an early or distinctive phase in bridge construction, design, or engineering and it retains the historic integrity necessary to convey its engineering significance. It represents a significant phase or feature of the work of a master. It is distinguishable when compared with similar structures and retains historic integrity necessary to convey engineering or design significance. This bridge is also important as it is one of six or fewer examples within the Indiana Department of Transportation’s LaPorte District. This bridge exhibits important contributions made by an accomplished Indiana engineer, designer, fabricator or builder and displays distinctive engineering and/or aesthetic characteristics.

Description of Right-of-Way

The existing right-of-way throughout the project corridor is approximately 40 feet on each side of the centerline of SR 75. The project area is primarily maintained grassed right-of-way with wooded areas along the banks of Wildcat Creek. This project will occur within existing right-of-way. No new permanent or temporary right-of-way will be required for this project.

Maintenance of Traffic (MOT)

The MOT for the project will require a full closure of the bridge and a detour during construction utilizing SR 18, US 421, and SR 26, a distance of approximately 19.5 miles from closure point to closure point. The bridge closure will be limited to the months of June, July, and August to avoid planting and harvest season, as the bridge is located in a rural, agricultural area and the bridge is important for the movement of farm equipment during planting and harvest as there is no other bridge in the area capable of handling modern planting and tillage equipment or grain trucks. Access to all properties within and adjacent to the project limits will be maintained at all times during project construction.

Project Schedule

Milestone	Expected Dates
Categorical Exclusion Released for Public Involvement	April 16, 2021
Public Comment Opportunity	June 16 – July 16, 2021
Proposed Construction	Anticipated Summer 2023

Estimated Project Cost Summary

The estimated cost for this project is \$1,565,000 which includes design and construction. Both federal and state funding will be used. The project is included in the 2020-2024 Statewide Transportation Improvement Program.

Environmental Documentation

The INDOT and FHWA have reviewed the Categorical Exclusion (CE) Document prepared by American Structurepoint, Inc. for this project and released the document for public involvement on April 16, 2021. The CE evaluates the impact of the SR 75 over Wildcat Creek Bridge Improvement project on the natural and human environment. No areas of potentially significant impacts have been identified.

Water Resources

The proposed project area was examined for the presence of wetlands and “waters of the U.S.” Two streams and two wetlands were identified within the project area as potentially regulated resources. It is anticipated that the preferred alternative will avoid impacting any streams or wetlands.

Cultural Resources

INDOT, acting on behalf of FHWA, has determined a finding of “No Historic Properties Affected” is appropriate for this undertaking because there are no resources listed in, or eligible for listing in, the NRHP within the APE other than the SR 75 over Wildcat Creek Bridge. Per the terms of the Historic Bridges Programmatic Agreement (PA), the FHWA will satisfy its Section 106 responsibilities involving “Select” and “Non-Select” bridges through the Project Development Process (PDP) of the Historic Bridges PA (Stipulation III). The SR 75 over Wildcat Creek Bridge has been classified as a Non-Select Bridge by the INDOT Historic Bridge Inventory and, thus, the procedures outlined in Stipulation III.B. of the Historic Bridges PA will be followed to fulfill FHWA’s Section 106 responsibilities for the bridge. Therefore, the finding for this project only applies to other resources located within the APE and not the SR 75 over Wildcat Creek Bridge. Pursuant to the Historic Bridge PA, a public hearing is required. After the public comment period for the hearing has passed, Section 106 will be complete.

Section 4(f) Resources

The SR 75 over Wildcat Creek Bridge (INDOT Bridge 075-08-03653 B, NBI No. 024970, HB-1897; IHSSI No.: 015-553-65014) was determined eligible for listing in the NRHP in the Indiana Historic Bridge Inventory and was designated as “Non-Select.” The proposed project would result in a use of the SR 75 over Wildcat Creek Bridge as it would rehabilitate the historic bridge for continued vehicular use. The Historic Bridge PA applies to this project. The project adheres to the Historic Bridge PA Project Development Process. INDOT CRO and the SHPO concurred with the Section 4(f) Historic Bridge Alternatives Analysis.

The portion of Wildcat Creek within the project area is a Natural, Scenic, and Recreational River System. Wildcat Creek is used by the public for recreation and as such it is eligible for protection under Section 4(f). As part of the project, no construction activities will occur in the stream but the stream may be temporarily closed to recreational use at the bridge during construction for the protection of the recreational users. INDOT ES concurred that this temporary closure is likely a temporary occupancy on June 16, 2020. In a letter dated August 17, 2020, the Carroll County Commissioners concurred that the temporary occupancy does not constitute a use under Section 4(f), as described in the FHWA’s Section 4(f) Policy Paper (dated July 20, 2012).

Endangered Species and Terrestrial Habitat

Due to the need to provide access for construction, approximately 0.15 acre of terrestrial habitat (maintained grassy right-of-way) may be impacted. No tree clearing is anticipated. The project is within the range of the Federally endangered Indiana Bat (*Myotis sodalis*), clubshell mussel (*Pleurobema clava*), fanshell mussel (*Cyprogenia stegaria*), rayed bean mussel (*Villosa fabalis*), sheepnose mussel (*Plethobasus cyphus*), and snuffbox mussel (*Epioblasma triquetra*), and the threatened northern long-eared bat (*Myotis septentrionalis*), eastern massasauga rattlesnake (*Sistrurus catenatus*), and rabbitsfoot mussel (*Quadrula cylindrica cylindrica*). The USFWS stated there is no habitat for the mussel species and eastern massasauga within Wildcat Creek, so they agreed that the proposed project is not likely to adversely affect these endangered and threatened mussel and snake species. Impacts to the two bat species were determined utilizing the bat programmatic informal consultation process. The project was found to “*may effect but not likely to adversely affect*” the Indiana bat and/or the northern long-eared bat.



WELCOME TO THE PUBLIC HEARING

SR 75 OVER WILDCAT CREEK
BRIDGE IMPROVEMENT
B-39876
DES. 1601029

Cutler, Carroll County, Indiana

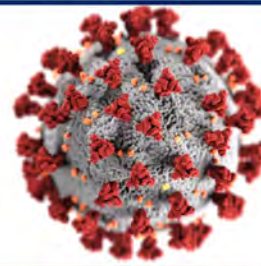
INDIANA DEPARTMENT OF TRANSPORTATION
JULY 1, 2021
6 PM

CUTLER GENERAL STORE/CUTLER MERCHANT BUILDING



PRESENTATION ALSO POSTED ONLINE AT: WWW.STRUCTUREPOINTPUBLIC.COM/SR75WILDCAT & [HTTPS://WWW.IN.GOV/INDOT/2705.HTM](https://WWW.IN.GOV/INDOT/2705.HTM)

COVID-19



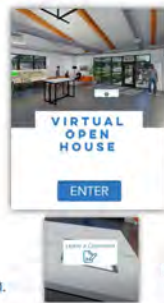
SUBMIT PUBLIC COMMENTS

In-person public hearing:

- Verbal comment session after presentation using microphone
- Public Comment Form available in information packet

Via website, mail, or email:

- Project Website:
www.structurepointpublic.com/sr75wildcat
- Information Packet, Comment Box
- Virtual Open House "Leave a Comment" Button
- INDOT LaPorte district website:
<https://www.in.gov/indot/2705.htm>
- Mail: Leah Perry, American Structurepoint
9025 River Road, Suite 200
Indianapolis, IN 46240
- Email: lperry@structurepoint.com



INDOT respectfully requests comments be submitted by July 16, 2021.

PRESENTATION AGENDA



- Public Hearing Intent
- Project Information
 - Project Location
 - Purpose and Need Overview
 - Proposed Project Improvements
 - Maintenance of Traffic
 - Right-of-Way Requirements
 - Alternatives Considered
 - Environmental Process
 - Anticipated Project Schedule/Costs
- Comment Opportunities

PUBLIC HEARING INTENT

- Requirement of the environmental review process due to Historic Bridge Programmatic Agreement (PA)
- Continuation of the opportunity to engage the public in the decision making process
- Solicit comments on the environmental document and preliminary design plans
- All comments submitted will become part of the public record, and they will be entered into a transcript, reviewed, evaluated, and given full consideration during the decision-making process



PUBLIC HEARING PROCESS

Legal Notice:

- Public noticed advertised in *Carroll County Comet* newspaper
- Legal notice was mailed to local businesses, adjacent residences, known property owners, government officials, and consulting parties
- Announcement of hearing posted to INDOT LaPorte District website (<https://www.in.gov/indot/2705.htm>) and project website (www.structurepointpublic.com/sr75wildcat)
- INDOT sent out a media release



PROJECT RESOURCE LOCATIONS

- Environmental document (CE-4) available online at:
 - Project website: www.structurepointpublic.com/s75wildcat
 - INDOT LaPorte district website: <https://www.in.gov/indot/2705.htm>
- Physical Copy of the environmental document (with plans):
 - At hearing
 - At Flora Public Library, 109 N Center St, Flora, IN 46929
 - Can be mailed upon request
- Transportation Services Call Center
 - Provides citizens and business customers with a single point of contact to request transportation services, obtain information, or provide feedback through multiple channels of communication.

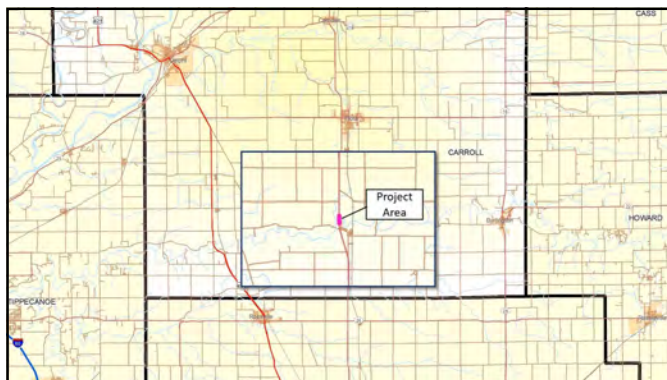
855-463-6848 • INDOT4U.com • INDOT@indot.in.gov



PROJECT LOCATION



Cutler, Carroll County, IN



PURPOSE & NEED

Need for Project:

- Current load rating is 30 tons; does not meet the required load rating of 36 tons
- Bridge in deteriorated condition:
 - Wearing surface has cracking
 - Twisted floor beam
 - Superstructure has several holes, rust, and thinning of steel plates
- Shoulders are too narrow



Next Level

ALTERNATIVES CONSIDERED

Do Nothing Alternative:

- No improvements would be made
- Purpose and need of project would not be met

Bridge Replacement:

- Existing bridge would be replaced instead of rehabilitated
- Purpose and need would be met
- This would result in the demolition of a "Non-Select" historic bridge
- Demolition of a "Non-Select" historic bridge is not considered prudent if rehabilitation of the bridge is reasonable and prudent
- Rehabilitation of the bridge was determined reasonable and prudent

Next Level

PREFERRED ALTERNATIVE (PROPOSED IMPROVEMENTS)**Rehabilitation of existing bridge:**

- Replace bridge deck
- Replace approach slabs



NextLevel
Engineering

PREFERRED ALTERNATIVE (PROPOSED IMPROVEMENTS)

- New expansion joint at pier 2
- New drainage system



NextLevel
Engineering

PREFERRED ALTERNATIVE (PROPOSED IMPROVEMENTS)

- Replace pavement markings in-kind
- Clean Top flanges of truss floor beams in span B and main beams in span A



NextLevel
Engineering

PREFERRED ALTERNATIVE (PROPOSED IMPROVEMENTS)

- Install shear studs on top of beams to increase load carrying capacity
- Clean, patch, or replace "in-kind" damaged or deteriorated stringers or other structural steel components
- Heat straighten damaged floor beam



NextLevel
Engineering

PREFERRED ALTERNATIVE (PROPOSED IMPROVEMENTS)

- Replace gusset plates connecting the cross bracing
- Clean and paint existing bearing assemblies



NextLevel
Engineering

PREFERRED ALTERNATIVE (PROPOSED IMPROVEMENTS)

- Patch spalled bearing pedestals
- Patch spalled sections on abutments and pier 2
- Convert Abutment 1 into a semi-integral end bent



NextLevel
Engineering

PREFERRED ALTERNATIVE (PROPOSED IMPROVEMENTS)

- Replace existing railing along with railing transition at all four ends
- Original 1947 railing will stay in place
- Reset or replace existing guardrail as necessary



NextLevel
Engineering

PREFERRED ALTERNATIVE (PROPOSED IMPROVEMENTS)

- Replace some inadequate rivets with high strength bolts to increase load carrying capacity
- After the repairs are done, paint structural steel



NextLevel
Engineering

PREFERRED ALTERNATIVE (PROPOSED IMPROVEMENTS)

- To meet minimum useable shoulder width requirements, reduce lane width from 11.75-feet to 11-feet to increase the shoulder width to 3-feet from 2.25 feet
- Total clear roadway width will remain unchanged
- Tie in bridge-approach



NextLevel
Engineering

MAINTENANCE OF TRAFFIC

- Close bridge during construction
- Bridge closure will be limited to months of June, July, and August
- Detour using SR 18, US 421, and SR 26
- Access to properties along the project maintained at all times



RIGHT-OF-WAY

- This project will take place in existing right-of-way
- No right-of-way acquisition
- No relocations
- The existing right-of-way is approximately 40 feet on each side of the centerline of SR 75



ANTICIPATED PROJECT SCHEDULE & COST



- The estimated cost for this project is approximately \$1,565,000 which includes design and construction
- The project includes both Federal and State Funding

NextLevel
Engineering

ENVIRONMENTAL PROCESS

Requirement of the National Environmental Policy Act (NEPA):

- Requires evaluation of environmental impacts of the project on the natural and social environment
 - Waterways, wetlands, endangered species, etc.
 - Historic resources
 - Social and economic factors



ENVIRONMENTAL PROCESS

Categorical Exclusion (CE), Level 4:

- Prepared in accordance with state and federal guidelines
- Evaluates impacts of proposed project
- Evaluates a number of possible alternatives including a "Do Nothing" alternative as a baseline for comparison as discussed earlier

The goal is to avoid, minimize, and then mitigate impacts.



ENVIRONMENTAL STATUS

NEPA Status:

- Coordinated with local, state, and federal agencies
- Evaluated impacts

STATE AGENCIES
EMERGENCY SERVICES IDEM INDOT
LOCAL AGENCIES ELECTED OFFICIALS
SCHOOL DISTRICTS NPS USACE HUD
 USFWS **FEDERAL AGENCIES** IGS
 FHWA DNR NRCS



ENVIRONMENTAL PROCESS

Historic Properties:

- Archaeological reconnaissance conducted
 - No archaeological resources identified within project area
- Historic property report completed
 - No historic properties other than the historic bridge
 - "No Historic Properties Affected" for resources other than the bridge



ENVIRONMENTAL PROCESS

- Parker Thru Truss Bridge constructed in 1947
- Eligible for listing in the National Register of Historic Places (NRHP)
- Possesses engineering and historical significance
- Designated as "Non-Select"
 - Not as suitable for preservation
 - Not considered excellent example



ENVIRONMENTAL PROCESS

- Project must follow Historic Bridge Programmatic Agreement (HBPA)
- Historic Bridge Alternative Analysis (HBAA) required
- HBAA recommended rehabilitation of bridge as proposed



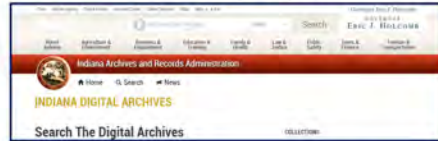
ENVIRONMENTAL PROCESS

- Consulting Parties overwhelmingly supported rehabilitation
- Public hearing required
- Environmental document approval
 - After hearing comment period ends

Consulting Party List
Carroll County Commissioners
Indiana Landmarks, Western Regional Office
Carroll County Historical Society and Museum
Carroll County Historian
Historic Spans Task Force
Forest County Potawatomi Community
Miami Tribe of Oklahoma
Heartland Heritage, Inc.
Delphi Preservation Society
NICHES Land Trust
Carroll County Wabash & Erie Canal Inc.
Adams Mill, Inc.
Carroll County Chamber of Commerce
Promoting Wildcat Valley
IDNR, Division of Outdoor Recreation
Wildcat Guardians
HistoricBridges.org

ENVIRONMENTAL PROCESS

- Design plans will be submitted to consulting parties for review and comment at 30 percent, 60 percent, and final
- SHPO requested photo documentation of the bridge
 - Photographs of bridge to be submitted to SHPO and the Indiana State Digital Archives (<https://secure.in.gov/apps/iara/search/>)



ENVIRONMENTAL PROCESS

Section 4(f) Resources:

- SR 75 over Wildcat Creek Bridge is a 4(f) resource
 - Project will result in a "use" of the bridge as it would rehabilitate the bridge
 - Project will follow Historic Bridge Programmatic Agreement



ENVIRONMENTAL PROCESS

- Wildcat Creek is a 4(f) resource
 - Wildcat Creek is a Natural, Scenic, and Recreational River System and used by the public for recreation
 - No construction in stream; stream may be temporarily closed to recreational use for safety
 - Carroll County Commissioners concurred the temporary occupancy is not a "use" under Section 4(f)



ENVIRONMENTAL PROCESS

Water Resources:

- Two wetlands and two streams (Wildcat Creek & UNT 1) were identified near project
- No wetland or stream impacts anticipated



COMMUNITY INVOLVEMENT

- Notice of Survey
- Virtual Open House
- Public Hearing

YOU
QUESTIONS **MEETING**
INVOLVEMENT
PUBLIC INFORMATION **FEEDBACK**
CONCERNS **COMMUNITY**
COMMENTS **DISCUSSION**



SUBMITTING A COMMENT

The team is interested in hearing your feedback on the project:

- Comment forms available at
 - Project website:
<https://www.structurepointpublic.com/sr75wildcat>
 - INDOT LaPorte district website:
<https://www.in.gov/indot/2705.htm>
- Mail or e-mail your comments to Leah Perry
 - American Structurepoint, 9025 River Road, Suite 200
Indianapolis, IN 46240
 - Email: lperry@structurepoint.com
- If attending the hearing in-person, verbal comments accepted after this presentation and a comment form is in information packet for written comments



Please submit comments by July 16, 2021



NEXT STEPS

Public and project stakeholder input:

- Submit comments either online, via mail, email, fax, or in-person at the hearing (See information packet for more details)
- Please submit all comments by July 16, 2021
- All comments are part of public record

INDOT review of public comments:

- All comments are given full consideration during decision-making process
- Address comments, finalize/approve environmental document, complete project design



THANK YOU!

In-person hearing attendees: Verbal Comments to follow shortly

Please be sure to view visuals and project layout in the virtual open house at:
<https://www.structurepointpublic.com/sr75wildcat>

or in-person at the public hearing information stations for hearing attendees after the Verbal Comment Session

For questions, please contact Leah Perry at:
lperry@structurepoint.com | 317.547.5580
<https://www.structurepointpublic.com/sr75wildcat>



855.463.6848 | INDOT4U.com | INDOT@indot.in.gov



SR 75 over Wildcat Creek Public Hearing – Verbal Comments
2017.00220

Adam Parkhouse <i>INDOT</i>	We are going to move into public comment now. Again, a few ways to make a public comment ... in a few moments I'll open up the public comment period. We did have one person sign up, but if you didn't sign up, that's okay. You can still come up and make a comment. If you want to submit a written comment, there is a form on the back of the packet that hopefully you have. If you do not, we have some extras over here. And also you can submit a public comment by emailing Leah at the address mentioned: lperry@structurepoint.com . The deadline is two weeks from today. You don't have to do just one or the other. If you want to speak tonight and send something later, that is fine. Or if you want to submit something written later, that is fine as well. So I will go ahead and open up the public comment period. We had one speaker that signed up: William Brown.
William Brown <i>Carroll County Commissioner</i>	I just want to, as one of the commissioners thank you for taking into consideration our request that (a) this bridge be preserved and (b) the length of time and the time that it be done be as concise as possible and that you take into consideration the planting and harvest seasons, which it seems like you have done. Those were our principal concerns about it and I think you have addressed that. So thank you.
Adam Parkhouse <i>INDOT</i>	Sure. We definitely always want to be mindful of those kinds of things when we are setting up a project. We know our projects impact in a lot of ways. Especially if you drive around anywhere in northwest Indiana today you probably encountered one of our projects. We just want to take steps to minimize those impacts. So thank you for that. Anyone else would like to make a public comment?
Kent Overholser	Is there any plans for raising the bridge higher? From the road up to the top of the I-beam?
Dave Day <i>American Structurepoint</i>	No. There is not. Without creating for lack of a better terms, an incredible mess (<i>Inaudible</i>)
Adam Parkhouse <i>INDOT</i>	One more thing about public comment period, feel free to ask questions, but we will also be available afterwards for one-on-one questions if people would like. So feel free to take advantage of that as well. Anyone else have a question or public comment?
Tom Overholser	With it being shut down three months, some of us live just on the other side of the Wildcat. I guess I'm a little bit concerned about fire protection.
Adam Parkhouse <i>INDOT</i>	Sure. A valid concern. I think there is a number of things at play here. In the video we said there is a request at the hands of the commissioner to try to squeeze this in between planting and harvest season. And we can do that in that time line, under full closure, but certainly ... What we try to do in those situations is get the word out to everyone to everybody as soon as possible. We are about two years out to construction so hopefully plans can be made to minimize those impacts. We can certainly understand that there can be some challenges with any full closure.

Dennis Kern	I know the presentation indicated the stream may be shut down for certain periods for quote “safety”. I am assuming you won’t have it shut down for the full three-month period. Could you estimate what would the length of those periods would be?
Dave Day <i>American Structurepoint</i>	It will be shut down during the time when they are tearing down the concrete bridge deck, and then more or less immediately after that they could put another (<i>Inaudible</i>) in place and at that point it wouldn’t be any danger to canoers (<i>Inaudible</i>). The other times may be when they are painting it... (<i>Inaudible</i>) scaffolding... (<i>Inaudible</i>) and maybe when they are pouring the deck. Beyond that, in my mind there won’t be a need to close down the stream.
Adam Parkhouse <i>INDOT</i>	So not for the entire duration but for portions of it if we need to, to protect public safety.
Renita Overholser	I would like to know what the detours will be to go north.
Adam Parkhouse <i>INDOT</i>	Do we have details for that?
Dave Day <i>American Structurepoint</i>	The formal detours are 421 and 26.
Adam Parkhouse <i>INDOT</i>	A note about detours – detours for our INDOT projects have to follow state roads, like official marked detours. But we know locals will always find a different way around because sometimes the State detours, which primarily in place for truck traffic because trucks can only go on certain roads, but we also understand the locals will find a different way around.
William Brown	I’d like everyone’s opinion on this, but it seems to me that we will have to close ... the nearest bridge to the highway bridge is a covered bridge, and I know we’ll have heavier trucks trying to get across that covered bridge so we have talked about it and our plan is to close it during that time – close the covered bridge. Fire trucks can’t cross it anyway, so that isn’t going to affect the fire coverage, but we’re going to have to alert our dispatch, our central dispatch, so ones like Tom here, who are just across the creek, at that point are probably going to be closer to the Flora fire department than they are to the Cutler fire department. So they’ll be dispatched first from Flora to try to take care of that. But I’d like your opinions on closing the covered bridge because I don’t see another way.
Adam Parkhouse <i>INDOT</i>	Yeah, that would be a local decision. I think for the reasons you just said, that makes sense, but that would have to be a local decision.
William Brown	I didn’t know if anyone here had opinions.
Male	I have one suggestion. Why couldn’t we construct a steel beam that nothing higher than a pickup truck can get through?
Male	That didn’t work before, when the well guy got his truck stuck in there. (<i>Inaudible</i>) The only thing that will completely stop them is putting a barricade completely across the front of the bridge that you could only get a bicycle through. And that was the only thing that

	stopped them to stop them and they still tried to burn it down. So what are you going to do?
William Brown	That was an idea we brought up before on other bridges. And usually our county attorney and county engineer say no you cannot do that for safety reasons. People will try to get under it. It sounds like a great idea to me. That's probably what I would do, but the lawyers don't think so.
Adam Parkhouse <i>INDOT</i>	Anybody else like to make a public comment? Okay, so again we'll conclude the public comment period, but we aren't going anywhere. We are going to hang out here.



INDIANA DEPARTMENT OF TRANSPORTATION

Thank you for your participation in this project. Please submit comments by using the space provided below.

PROJECT: State Road (SR) 75 over Wildcat Creek, Cutler, Carroll County (Des. No. 1601029)

PUBLIC COMMENT PERIOD: June 16 – July 16, 2021

SUBMISSION: E-mail or mail comments to Leah Perry of American Structurepoint, Inc. at lperry@structurepoint.com, 9025 River Road, Suite 200, Indianapolis, Indiana 46240

Please submit comments by July 16, 2021 for inclusion into the public record:

~~You~~ When the Creek is Closed Under the Bridge,
You should post a "Creek Closed" Sign at
the Adams Mill Public Access Site.
Most Paddlers that travel under SR-75 Bridge
start at Adams Mill.

There Are 2 IDNR Adopt-A-River Signs
At the SR-75 Bridge.
Also, there are 2 Indiana Scenic River Signs
At the SR-75 Bridge.
These Signs Should Be Saved & Replaced
in Visible Locations when the project
is complete.

I will offer a ~~see~~ Paddling trip to my River Friends
to celebrate the completion of your/our Project.

~~~~~ Garry N. Hill ~~~~~



See You On The River,  
"Muskrat"

NAME (PLEASE PRINT): GARRY HILL

SIGNATURE:



# SR-75 Bridge Painting Project August 26, 2007





They Gave Us An EASY Path  
Under Their "TENT"



# Garry Hill



## Wildcat Guardians

Box 6421, Kokomo, IN 46904 [www.wildcatguardians.org](http://www.wildcatguardians.org)

**"Dedicated To Preserving The Health and Beauty  
of Wildcat Creek and its Tributaries"**

**~ ~ ~ Garry Hill ~ ~ ~**



# Muskrat's Wildcat Creek Expeditions

An Internet-based Group of Canoers & Kayakers  
Who Participate in Trips to Wildcat Creek & Other Streams  
No Schedule, No Meetings, No Dues, Just Good Times



**Wildcat Guardians is an organization that works  
Year-Round to Protect Our Wildcat Creek.**

**On-Going Programs include.....**

**“Adopt-A-River”      “Discover Wildcat Creek”**


**“Water Quality Monitoring”**

**Plus Cleanups, Recreational Events, Speakers,  
And joining with other groups to promote  
Wildcat Creek as a valuable Natural Resource.**


**Our mission is to establish Wildcat Creek as  
one of the most cared for Streams in Indiana**





SR 75 over Wildcat Creek Bridge Improvement Project  
(Des. No. 1601029)  
Public Hearing Comments Summary, July 1, 2021

| Comment No. | Name / Organization / Comment Date                                                                                                   | Comment                                                                                                                                                                                                                                                                                                                                                                                                                     | Response                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                               |
|-------------|--------------------------------------------------------------------------------------------------------------------------------------|-----------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|--------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|
| 1           | William Brown<br>Carroll County Commissioner<br>P.O Box 591<br>Cutler, IN 46920<br><br>July 1, 2021(verbal comment)                  | I just want to, as one of the commissioners thank you for taking into consideration our request that (a) this bridge be preserved and (b) the length of time and the time that it be done be as concise as possible and that you take into consideration the planting and harvest seasons, which it seems like you have done. Those were our principal concerns about it and I think you have addressed that. So thank you. | 1. <u>Bridge Preservation and Construction Time Frame.</u><br>Adam Parkhouse answers: "Sure. We definitely always want to be mindful of those kinds of things when we are setting up a project. We know our projects impact in a lot of ways. Especially if you drive around anywhere in northwest Indiana today you probably encountered one of our projects. We just want to take steps to minimize those impacts. So thank you for that."<br><br>Thank you for your comment and previous input on this project. The project will preserve the bridge and limit work requiring closure of the bridge to the months of June, July, and August to avoid planting and harvest seasons.                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                  |
| 2           | Kent Overholser<br><br>July 1, 2021(verbal comment) | Is there any plans of raising the bridge higher? From the road to the top of the I-beam?                                                                                                                                                                                                                                                                                                                                    | 1. <u>Raising bridge-</u><br>Dave Day Answers: "No. There is not. Without creating for lack of a better terms, an incredible mess it would not be possible."<br><br>The project does not propose to raise the bridge. As proposed, the project involves rehabilitation of the existing structure for continued vehicular use by increasing the structural capacity to at or above the required load rating. The existing bridge deck will be removed from both spans and a new concrete deck will be constructed. The existing approach slabs will be removed and replaced. Additional rehabilitation of the deck will include installing a new expansion joint at pier 2, creating a new drainage system for the structure and replacing pavement markings in-kind matching previous roadway striping.<br><br>Shear studs will be installed on top of beams to increase load carrying capacity. Damaged or deteriorated stringers or other structural steel components will be cleaned and patched or replaced "in-kind." Existing railing will be replaced along with railing transitions at all four ends. The original railing (constructed in 1947) will stay in place. Approximately 88 rivets that have been determined inadequate will be replaced with high strength bolts to increase the load carrying capacity. After the repairs are done, the structural steel will be painted. To meet minimum useable shoulder width requirements, the lane width will be reduced from 11.75-feet to 11-feet to increase the shoulder width to 3-feet. The structure length will be increased by 0.89 feet from 240 feet to approximately 240.89 feet. |

SR 75 over Wildcat Creek Bridge Improvement Project  
(Des. No. 1601029)  
Public Hearing Comments Summary, July 1, 2021

| Comment No. | Name / Organization / Comment Date                                                                                                  | Comment                                                                                                                                                | Response                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                               |
|-------------|-------------------------------------------------------------------------------------------------------------------------------------|--------------------------------------------------------------------------------------------------------------------------------------------------------|--------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|
| 3           | Tom Overholser<br><br>July 1, 2021(verbal comment) | With it being shut down three months, some of us live just on the other side of the Wildcat. I guess I'm a little bit concerned about fire protection. | <p>1. <u>Fire protection-</u><br/>Adam Parkhouse answers: "Sure. A valid concern. I think there is a number of things at play here. In the video we said there is a request at the hands of the commissioner to try to squeeze this in between planting and harvest season. And we can do that in that time line, under full closure, but certainly ... What we try to do in those situations is get the word out to everyone to everybody as soon as possible. We are about two years out to construction so hopefully plans can be made to minimize those impacts. We can certainly understand that there can be some challenges with any full closure."</p> <p>Information about the closure (included within the Public hearing invitations) was sent to the Carroll County Sheriff's Office, Carroll County Emergency Management Agency, and Democrat Township Volunteer Fire Department on June, 16, 2021. These agencies did not provide comment about this project. Additionally, it is the responsibility of the project sponsor to notify school corporations and emergency services at least two weeks prior to any construction that would block or limit access.</p> <p>The closure will pose a temporary inconvenience to traveling motorists (including emergency services); however, no significant delays are anticipated and all inconveniences will cease upon project completion. Delays would occur during construction but will cease with project completion.</p> <p>See Comment 6, William Brown- Commissioner Brown indicates that central dispatch will be alerted and the fire department may be dispatched from the Flora fire department instead of Cutler if closer.</p> |

SR 75 over Wildcat Creek Bridge Improvement Project  
(Des. No. 1601029)  
Public Hearing Comments Summary, July 1, 2021

| Comment No. | Name / Organization / Comment Date                                                                                                     | Comment                                                                                                                                                                                                                                              | Response                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                        |
|-------------|----------------------------------------------------------------------------------------------------------------------------------------|------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|-----------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|
| 4           | Dennis Kern<br><br>July 1, 2021(verbal comment)       | I know the presentation indicated the stream may be shut down for certain periods for quote “safety.” I am assuming you won’t have it shut down for the full three-month period. Could you estimate what would the length of those periods would be? | <p>1. <u>Stream closure-</u><br/>Dave Day Answers: “It will be shut down during the time when they are tearing down the concrete bridge deck, and then more or less immediately after that they could put those metal deck pins in place and at that point it wouldn’t be any danger to canoers. The other times may be when they are painting it where they will have some scaffolding hanging off the bridge and maybe when they are pouring the deck just to be safe. Beyond that, in my mind there won’t be a need to close down the stream.”<br/>Adam Parkhouse clarifies “not for the entire duration but for portions of it if we need to, to protect public safety.”</p> <p>As part of the project, no construction activities will occur in the stream but the stream may be temporarily closed to recreational use at the bridge during construction for the protection of the recreational users. Wildcat Creek will be closed to recreational use at the bridge for less than the time needed to construct the project (It is anticipated Wildcat Creek may be closed for 1 to 2 months). There will be signage to signify closures and a pathway for water trail users to circumvent closures.</p> |
| 5           | Renita Overholser<br><br>July 1, 2021(verbal comment) | I would like to know what the detours will be to go north.                                                                                                                                                                                           | <p>1. <u>Detours-</u><br/>Dave Day Answers: “The formal detours are 421 and 26.</p> <p>Adam Parkhouse clarifies “A note about detours – detours for our INDOT projects have to follow state roads, like official marked detours. But we know locals will always find a different way around because sometimes the State detours, which primarily in place for truck traffic because trucks can only go on certain roads, but we also understand the locals will find a different way around.”</p> <p>The Maintenance of Traffic (MOT) for the project will require a full closure of the bridge and a state detour during construction utilizing SR 18, US 421, and SR 26, a distance of approximately 19.5 miles from closure point to closure point. Access to all properties within and adjacent to the project limits will be maintained at all times during project construction. The MOT will remain in place for approximately eight months. Any local detours would be up to local officials.</p>                                                                                                                                                                                                       |

SR 75 over Wildcat Creek Bridge Improvement Project  
(Des. No. 1601029)  
Public Hearing Comments Summary, July 1, 2021

| Comment No. | Name / Organization / Comment Date                                                                                  | Comment                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                        | Response                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                            |
|-------------|---------------------------------------------------------------------------------------------------------------------|------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|-----------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|
| 6           | William Brown<br>Carroll County Commissioner<br>P.O Box 591<br>Cutler, IN 46920<br><br>July 1, 2021(verbal comment) | I'd like everyone's opinion on this, but it seems to me that we will have to close ... the nearest bridge to the highway bridge is a covered bridge, and I know we'll have heavier trucks trying to get across that covered bridge so we have talked about it and our plan is to close it during that time – close the covered bridge. Fire trucks can't cross it anyway, so that isn't going to affect the fire coverage, but we're going to have to alert our dispatch, our central dispatch, so ones like Tom here, who are just across the creek, at that point are probably going to be closer to the Flora fire department than they are to the Cutler fire department. So they'll be dispatched first from Flora to try to take care of that. But I'd like your opinions on closing the covered bridge because I don't see another way. | <ol style="list-style-type: none"> <li>1. <u>Closing Adams Mill Covered Bridge during construction</u>- Adam Parkhouse Answers: "Yeah, that would be up to the local decision. I think for the reasons you just said, that makes sense, but that would have to be a local decision."</li> <li>2. <u>Local opinions on closing Adams Mill Covered Bridge during construction</u>- William Brown: "I didn't know if anyone here had opinions?" <ol style="list-style-type: none"> <li>a. Speaker did not give name: "I have one suggestion. Why couldn't we construct a steel beam that nothing higher than a pickup truck can get through?"</li> <li>b. Speaker did not give name: "That didn't work before, when the well guy got his truck stuck in there. The only thing that will completely stop them is putting a barricade completely across the front of the bridge that you could only get a bicycle through. And that was the only thing that stopped them to stop them and they still tried to burn it down. So what are you going to do?"</li> <li>c. William Brown: "That was an idea we brought up before on other bridges. And usually our county attorney and county engineer say no you cannot do that for safety reasons. People will try to get under it. It sounds like a great idea to me. That's probably what I would do, but the lawyers don't think so."</li> <li>d. Whether or not Adams Mill Covered Bridge is closed during construction of this project is a local decision. Please direct any comments about this to the Carroll County Commissioners and/or Carroll County Highway Department.</li> </ol> </li> </ol> |



SR 75 over Wildcat Creek Bridge Improvement Project  
(Des. No. 1601029)  
Public Hearing Comments Summary, July 1, 2021

| Comment No. | Name / Organization / Comment Date                                                                                                                                     | Comment                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                              | Response                                                                                                                                                                                                                                                                                                                                                                |
|-------------|------------------------------------------------------------------------------------------------------------------------------------------------------------------------|--------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|-------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|
|             | <p>Garry Hill<br/>Wildcat Guardians<br/>Muskrat's Wildcat Creek Expeditions<br/>1021 Eastcrest Drive<br/>Greentown, IN 46396</p> <p>July 12, 2021(written comment)</p> | <p>When the creek is closed under the bridge, you should post a "Creek Closed" sign at the Adams Mill Public Access Site. Most paddlers that travel under SR-75 Bridge start at Adams Mill.</p> <p>There are 2 IDNR Adopt-A-River signs at the SR-75 Bridge. Also, there are 2 Indiana Scenic River signs at the SR-75 bridge. These signs should be saved and replaced in visible locations when the project is complete.</p> <p>I will offer a paddling trip to my river friends to celebration the completion of your/our project.</p> <p>See you on the River,<br/>"Muskrat"</p> | <ol style="list-style-type: none"> <li>1. <u>Stream Closure Signage</u>. Signage notifying users of the stream closure will be placed at Adams Mill Public Access Site.</li> <li>2. <u>Adopt-A-River/ Indiana Scenic River Signage</u>. These signs will be saved and reset in a visible location if disturbed by the project after the project is complete.</li> </ol> |



Indiana Department of Transportation (INDOT)  
State Preservation and Local Initiated Projects FY 2020 - 2024

| SPONSOR                                                                      | CONTRACT # / LEAD DES | STIP NAME | ROUTE   | WORK TYPE                              | LOCATION                                                                     | DISTRICT | MILES | FEDERAL CATEGORY | Total Cost of Project* | PROGRAM              | PHASE | FEDERAL         | MATCH          | 2020           | 2021             | 2022            | 2023           | 2024         |
|------------------------------------------------------------------------------|-----------------------|-----------|---------|----------------------------------------|------------------------------------------------------------------------------|----------|-------|------------------|------------------------|----------------------|-------|-----------------|----------------|----------------|------------------|-----------------|----------------|--------------|
| Indiana Department of Transportation                                         | 2000753               | A 22      | SR 25   | Bridge Thin Deck Overlay               | SR 25 NB over ROBINSON BRANCH, 00.55 N SR 218                                | LaPorte  | 0     | NHPP             | \$768,084.00           | Bridge Construction  | CN    | \$514,467.20    | \$128,616.80   |                |                  |                 | \$15,000.00    | \$628,084.00 |
|                                                                              |                       |           |         |                                        |                                                                              |          |       |                  |                        | Bridge Consulting    | PE    | \$100,000.00    | \$25,000.00    |                | \$75,000.00      |                 |                | \$50,000.00  |
| Performance Measure Impacted: Bridge Condition                               |                       |           |         |                                        |                                                                              |          |       |                  |                        |                      |       |                 |                |                |                  |                 |                |              |
| Comments:No MPO. 2000753, Amend FY21 PE                                      |                       |           |         |                                        |                                                                              |          |       |                  |                        |                      |       |                 |                |                |                  |                 |                |              |
| Indiana Department of Transportation                                         | 2001044               | A 17      | SR 29   | Small Structure Maint and Repair       | SR 29 over , 00.21 N of S JCT SR 18                                          | LaPorte  | 0     | STBG             | \$112,000.00           | Bridge Construction  | CN    | \$76,000.00     | \$19,000.00    |                |                  | \$95,000.00     |                |              |
|                                                                              |                       |           |         |                                        |                                                                              |          |       |                  |                        | Bridge Consulting    | PE    | \$13,600.00     | \$3,400.00     |                | \$17,000.00      |                 |                |              |
| Performance Measure Impacted: Safety                                         |                       |           |         |                                        |                                                                              |          |       |                  |                        |                      |       |                 |                |                |                  |                 |                |              |
| Comments:No MPO. Amend FY21 PE \$17,000 and FY22 CN \$95,000.                |                       |           |         |                                        |                                                                              |          |       |                  |                        |                      |       |                 |                |                |                  |                 |                |              |
| Indiana Department of Transportation                                         | 2002086               | A 36      | US 421  | Bridge Deck Overlay                    | US 421 over DEER CREEK, 00.17 S OLD SR 25                                    | LaPorte  | 0     | NHPP             | \$770,119.00           | Bridge Construction  | CN    | \$8,000.00      | \$2,000.00     |                |                  |                 | \$5,000.00     | \$5,000.00   |
|                                                                              |                       |           |         |                                        |                                                                              |          |       |                  |                        | Bridge Consulting    | PE    | \$160,000.00    | \$40,000.00    |                |                  | \$200,000.00    |                |              |
| Performance Measure Impacted: Bridge Condition                               |                       |           |         |                                        |                                                                              |          |       |                  |                        |                      |       |                 |                |                |                  |                 |                |              |
| Comments:No MPO. Amend FY22 PE and FY23 CN. FY24 CN is illustrative of FY24. |                       |           |         |                                        |                                                                              |          |       |                  |                        |                      |       |                 |                |                |                  |                 |                |              |
| Indiana Department of Transportation                                         | 38757 / 1600507       | Init.     | US 421  | HMA Overlay Minor Structural           | From SR 18 W Jct to 1.79 mi E of SR 18 W Jct (Bridge over Wabash River)      | LaPorte  | 1.893 | STBG             |                        | Bridge Construction  | CN    | \$808,338.40    | \$202,084.60   | \$1,010,423.00 |                  |                 |                |              |
|                                                                              |                       |           |         |                                        |                                                                              |          |       |                  |                        | Road Construction    | CN    | \$1,380,963.20  | \$345,240.80   | \$1,726,204.00 |                  |                 |                |              |
| Performance Measure Impacted: Pavement Condition                             |                       |           |         |                                        |                                                                              |          |       |                  |                        |                      |       |                 |                |                |                  |                 |                |              |
| Carroll County                                                               | 39013 / 1500549       | Init.     | VA VARI | Bridge Inspections                     | Countywide Bridge Inspection and Inventory Program for Cycle Years 2017-2020 | LaPorte  | 0     | STBG             |                        | Local Funds          | PE    | \$0.00          | \$703.80       | \$703.80       |                  |                 |                |              |
|                                                                              |                       |           |         |                                        |                                                                              |          |       |                  |                        | Local Bridge Program | PE    | \$2,815.20      | \$0.00         | \$2,815.20     |                  |                 |                |              |
| Performance Measure Impacted: Bridge Condition                               |                       |           |         |                                        |                                                                              |          |       |                  |                        |                      |       |                 |                |                |                  |                 |                |              |
| Indiana Department of Transportation                                         | 39876 / 1601029       | Init.     | SR 75   | Bridge Replacement, Other Construction | SR 75 at Wildcat Creek, 4.51 S SR 18                                         | LaPorte  | 0     | STBG             |                        | Bridge Construction  | CN    | \$4,174,643.20  | \$1,043,660.80 | \$150,000.00   | \$5,068,304.00   |                 |                |              |
|                                                                              |                       |           |         |                                        |                                                                              |          |       |                  |                        | Bridge ROW           | RW    | \$120,000.00    | \$30,000.00    | \$150,000.00   |                  |                 |                |              |
|                                                                              |                       |           |         |                                        |                                                                              |          |       |                  |                        |                      |       |                 |                |                |                  |                 |                |              |
| Indiana Department of Transportation                                         | 39876 / 1601029       | M 33      | SR 75   | Bridge Replacement                     | SR 75 at Wildcat Creek, 4.51 S SR 18                                         | LaPorte  | 0     | STBG             | \$2,217,251.00         | Bridge Construction  | CN    | -\$2,895,112.00 | -\$723,778.00  |                | (\$5,068,304.00) |                 | \$1,449,414.00 |              |
| Performance Measure Impacted: Bridge Condition                               |                       |           |         |                                        |                                                                              |          |       |                  |                        |                      |       |                 |                |                |                  |                 |                |              |
| Comments:Modify CN from FY21 to FY23 \$1,449,414. No MPO.                    |                       |           |         |                                        |                                                                              |          |       |                  |                        |                      |       |                 |                |                |                  |                 |                |              |
| Indiana Department of Transportation                                         | 40607 / 1700101       | Init.     | US 421  | Bridge Replacement, Steel              | Over Wildcat Creek, 4.06mi N of SR 26                                        | LaPorte  | 0     | STBG             |                        | Bridge Construction  | CN    | \$8,456,869.60  | \$2,114,217.40 |                | \$490,000.00     | \$10,081,087.00 |                |              |





# Bridge Inspection Report

075-08-03653 B

SR 75

over

WILDCAT CREEK



Inspection Date: 11/08/2019

Inspected By: Andrew Raynor

Inspection Type(s): Routine

Inspector: Andrew Raynor  
Inspection Date: 11/08/2019

Asset Name: 075-08-03653 B  
Facility Carried: SR 75

### Bridge Inspection Report

This inspection was conducted by Andrew Raynor and Cristy Burlage on 11/8/2019. This is a steel thru truss bridge that is on SR 75. There is minor cracking throughout the wearing surface and approach span. The steel truss beams have several vertical post and diagonal (channel sections) members exhibiting localized rusting-thru holes. The gusset plates have thinning splicing along the bottom of the cord. There is rivet corrosion along the lower tension chords near the bearing locations. . The north most floor beam has been twisted due to an errant vehicle during the substructure reconstruction All piers have moderate vertical cracking with efflorescence and some pier cap cracking.

Rehab C (Bridge replacement programmed 7/8/2020) DES# 1601029 Contract B-39876 (New bridge 75-08-10244)

Inspector: Andrew Raynor  
Inspection Date: 11/08/2019

Asset Name: 075-08-03653 B  
Facility Carried: SR 75

### Bridge Inspection Report

#### IDENTIFICATION

|                              |                       |                                |           |
|------------------------------|-----------------------|--------------------------------|-----------|
| (1) STATE CODE:              | 185 - Indiana         | (12) BASE HIGHWAY NETWORK:     | 0         |
| (8) STRUCTURE:               | 024970                | (13A) INVENTORY ROUTE:         |           |
| (5 A-B-C-D-E) INV. ROUTE:    | 1 - 3 - 1 - 00075 - 0 | (13B) SUBROUTE NUMBER:         |           |
| (2) HIGHWAY AGENCY DISTRICT: | 04 - La Porte         | (16) LATITUDE:                 | 40.48176  |
| (3) COUNTY CODE:             | 008 - CARROLL         | (17) LONGITUDE:                | -86.53007 |
| (4) PLACE CODE:              | 00000 - N/A           | (98) BORDER                    |           |
| (6) FEATURES INTERSECTED:    | WILDCAT CREEK         | A) STATE NAME:                 |           |
| (7) FACILITY CARRIED:        | SR 75                 | B) PERCENT                     | %         |
| (9) LOCATION:                | 04.51 S SR 18         | (99) BORDER BRIDGE STRUCT. NO: |           |
| (11) MILEPOINT:              | 0003.520              |                                |           |

#### STRUCTURE TYPE AND MATERIAL

|                                      |                                    |                                        |                                        |
|--------------------------------------|------------------------------------|----------------------------------------|----------------------------------------|
| (43) STRUCTURE TYPE, MAIN:           |                                    | (45) NUMBER OF SPANS IN MAIN 001 UNIT: |                                        |
| A) KIND OF MATERIAL/DESIGN:          | 3 - Steel                          | (46) NUMBER OF APPROACH SPANS:         | 0001                                   |
| B) TYPE OF DESIGN/CONSTR:            | 10 - Truss - Thru                  | (107) DECK STRUCTURE TYPE:             | 1 - Concrete Cast-in-Place             |
| (44) STRUCTURE TYPE, APPROACH SPANS: |                                    | (108) WEARING SURFACE/PROT SYS:        |                                        |
| A) KIND OF MATERIAL/DESIGN:          | 3 - Steel                          | A) WEARING SURFACE:                    | 3 - Latex Concrete or similar additive |
| B) TYPE OF DESIGN/CONSTR:            | 02 - Stringer/Multi-beam or Girder | B) DECK MEMBRANE:                      | 0 - None                               |
|                                      |                                    | C) DECK PROTECTION:                    | 0 - None                               |

#### AGE OF SERVICE

|                           |                        |                                     |        |
|---------------------------|------------------------|-------------------------------------|--------|
| (27) YEAR BUILT:          | 1947                   | (28) LANES:                         |        |
| (106) YEAR RECONSTRUCTED: | 2001                   | A) ON BRIDGE:                       | 02     |
| (42) TYPE OF SERVICE:     |                        | B) UNDER BRIDGE:                    | 00     |
| A) ON BRIDGE:             | 5 - Highway-pedestrian | (29) AVERAGE DAILY TRAFFIC:         | 002679 |
| B) UNDER BRIDGE:          | 5 - Waterway           | (30) YEAR OF AVERAGE DAILY TRAFFIC: | 2004   |
|                           |                        | (109) AVERAGE DAILY TRUCK TRAFFIC:  | 10 %   |
|                           |                        | (19) BYPASS DETOUR LENGTH:          | 004 MI |

Inspector: Andrew Raynor  
Inspection Date: 11/08/2019

Asset Name: 075-08-03653 B  
Facility Carried: SR 75

### Bridge Inspection Report

#### GEOMETRIC DATA

|                                        |               |     |                                         |              |
|----------------------------------------|---------------|-----|-----------------------------------------|--------------|
| (48) LENGTH OF MAX SPAN:               | 0175.0        | FT  | (35) STRUCTURE FLARED:                  | 0 - No flare |
| (49) STRUCTURE LENGTH:                 | 00240.0       | FT  | (10) INV RTE, MIN VERT<br>CLEARANCE:    | 14.83 FT     |
| (50) CURB/SIDEWALK WIDTHS:             |               |     | (47) TOT HORIZ CLEARANCE:               | 028.0 FT     |
| A) LEFT                                | 00.0          | FT  | (53) VERT CLEAR OVER BR RDWY:           | 14.36 FT     |
| B) RIGHT:                              | 00.0          | FT  | (54) MIN VERTICAL<br>UNDERCLEARANCE:    |              |
| (51) BRDG RDWY WIDTH CURB-<br>TO-CURB: | 028.0         | FT  | A) REFERENCE FEATURE:                   | N            |
| (52) DECK WIDTH, OUT-TO-OUT:           | 032.0         | FT  | B) MIN VERT UNDERCLEAR:                 | 0 FT         |
| (32) APPROACH ROADWAY                  | 027.0         | FT  | (55) LATERAL UNDERCLEARANCE<br>RIGHT:   |              |
| (33) BRIDGE MEDIAN:                    | 0 - No median |     | A) REFERENCE FEATURE:                   | N            |
| (34) SKEW:                             | 00            | DEG | B) MIN LATERAL UNDERCLEAR:              | 000.0 FT     |
|                                        |               |     | (56) MIN LATERAL UNDERCLEAR<br>ON LEFT: | 00.0 FT      |

#### INSPECTIONS

|                                                    |            |                                           |            |
|----------------------------------------------------|------------|-------------------------------------------|------------|
| (90) INSPECTION DATE:                              | 11/08/2019 | (91) DESIGNATED INSPECTION                | 24 MONTHS  |
| (92) CRITICAL FEATURE<br>INSPECTION:               |            | FREQUENCY:                                |            |
| A) FRACTURE CRITICAL<br>REQUIRED/FREQUENCY:        | Y 24       | (93) CRITICAL FEATURE<br>INSPECTION DATE: |            |
| B) UNDERWATER INSPECTION<br>REQUIRED/FREQUENCY:    | N          | A) FRACTURE CRITICAL DATE:                | 06/06/2019 |
| C) OTHER SPECIAL INSPECTION<br>REQUIRED/FREQUENCY: | N          | B) UNDERWATER INSP DATE:                  |            |
|                                                    |            | C) OTHER SPECIAL INSP DATE:               |            |

#### CONDITION

|                          |                                             |                                     |                                                        |
|--------------------------|---------------------------------------------|-------------------------------------|--------------------------------------------------------|
| (58) DECK:               | 7 - Good Condition<br>(some minor problems) | (60) SUBSTRUCTURE:                  | 6 - Satisfactory<br>Condition (minor<br>deterioration) |
| (58.01) WEARING SURFACE: | 5 - Fair Condition                          | (61) CHANNEL/CHANNEL<br>PROTECTION: | 7 - Bank protection<br>needs minor repairs             |
| (59) SUPERSTRUCTURE:     | 5 - Fair Condition<br>(minor section loss)  | (62) CULVERTS:                      | N - Not Applicable                                     |

#### CONDITION COMMENTS

(58) DECK: 7 - Good Condition (some minor problems)

Comments:

Minor random cracking and minor spalls next to stringer and floor beam flanges, on underside of deck.

(58.01) WEARING SURFACE: 5 - Fair Condition

Comments:

Lots of parallel longitudinal cracking in the wearing surface of both spans, (hairline to 0.06" wide). Some cracking propagates through the nosing material of the XJS joints.



Inspector: Andrew Raynor  
Inspection Date: 11/08/2019

Asset Name: 075-08-03653 B  
Facility Carried: SR 75

### Bridge Inspection Report

(59) SUPERSTRUCTURE: 5 - Fair Condition (minor section loss)

Comments:

The north most floor beam has been twisted due to an errant vehicle during the substructure reconstruction

Several vertical post and diagonal (channel sections) members exhibiting localized rusting-thru holes.

Thinning of splice and gusset plates along bottom chord; rivet corrosion along lower (tension) chords, especially at bearing support locations.

(60) SUBSTRUCTURE: 6 - Satisfactory Condition (minor deterioration)

Comments:

Piers 2,3,4 all have long vertical cracking with efflorescence present. There is cracking in the pier caps in pier 2. Old patching material can be seen on the piers.

(61) CHANNEL/CHANNEL PROTECTION 7 - Bank protection needs minor repairs

Comments:

Very large rip rap at on both sides of the waterway. There are islands on both sides of the channel. Under Approach span on the southside there is a pathway that leads to a wood line to the west and residence to the east.

(62) CULVERTS: N - Not Applicable

Comments:

### LOAD RATING AND POSTING

|                                    |                                   |                                                    |    |
|------------------------------------|-----------------------------------|----------------------------------------------------|----|
| (31) DESIGN LOAD:                  | 5 - HS 20                         | (66) INVENTORY RATING:                             | 29 |
| (70) BRIDGE POSTING                | 5 - Equal to or above legal loads | (65) INVENTORY RATING METHOD: 1 - Load Factor (LF) |    |
|                                    |                                   | (66B) INVENTORY RATING (H):                        | 17 |
| (41) STRUCTURE OPEN/POSTED/CLOSED: | A - Open                          | (66C) TONS POSTED :                                |    |
| (64) OPERATING RATING:             | 51                                | (66D) DATE POSTED/CLOSED:                          |    |
| (63) OPERATING RATING METHOD:      | 1 - Load Factor (LF)              |                                                    |    |

### APPRAISAL

|                                                    |                                         |                               |   |
|----------------------------------------------------|-----------------------------------------|-------------------------------|---|
| SUFFICIENCY RATING:                                | 68.5                                    | (36) TRAFFIC SAFETY FEATURE:  |   |
| STATUS:                                            | 0                                       | 36A) BRIDGE RAILINGS:         | 1 |
| (67) STRUCTURAL EVALUATION:                        | 5                                       | 36B) TRANSITIONS:             | 1 |
| (68) DECK GEOMETRY:                                | 4                                       | 36C) APPROACH GUARDRAIL:      | 1 |
| (69) UNDERCLEARANCES, VERTICAL & HORIZONTAL:       | N                                       | 36D) APPROACH GUARDRAIL ENDS: | 1 |
| (71) WATERWAY ADEQUACY:                            | 8 - Bridge Above Approaches             |                               |   |
| Comments:                                          |                                         |                               |   |
| (72) APPROACH ROADWAY ALIGNMENT:                   | 8 - Equal to present desirable criteria |                               |   |
| Comments:                                          |                                         |                               |   |
| Due to road alignment no need for speed reduction. |                                         |                               |   |

Inspector: Andrew Raynor  
Inspection Date: 11/08/2019

Asset Name: 075-08-03653 B  
Facility Carried: SR 75

### Bridge Inspection Report

(113) SCOUR CRITICAL BRIDGES: 8 - Stable for scour conditions  
Comments:

This item is coded as an '8' = LOW Risk for Vulnerability for scour. This rating is based on all of the foundations being set on piles. Also, the 2001 Rehab called for Designed Class-2 type rip rap to be placed on the banks of the channel under the bridge, to protect Pier #2 and the North Abutment.

The 2001 Flow Line as shown on the Plans = elv. 652.19', with the low elevation at the toe of the new rip rap keyway, at the north bank.

### CLASSIFICATION

|                               |                                    |                                          |                                   |
|-------------------------------|------------------------------------|------------------------------------------|-----------------------------------|
| (20) TOLL:                    | 3 - On Free Road                   | (21) MAINT. RESPONSIBILITY:              | 01 - State Highway Agency         |
| (22) OWNER:                   | 01 - State Highway Agency          | (26) FUNCTIONAL CLASS OF INVENTORY RTE:  | 07 - Rural - Major Collector      |
| (37) HISTORICAL SIGNIFICANCE: | 2 - Eligible for National Register | (100) STRAHNET HIGHWAY:                  | Not a STRAHNET route              |
| (101) PARALLEL STRUCTURE:     | N - No parallel structure          | (102) DIRECTION OF TRAFFIC:              | 2-way traffic                     |
| (103) TEMPORARY STRUCTURE:    |                                    | (104) HIGHWAY SYSTEM OF INVENTORY ROUTE: | 0 - Structure/Route is NOT on NHS |
| (105) FEDERAL LANDS HIGHWAYS: | 0-Not Applicable                   | (110) DESIGNATED NATIONAL NETWORK:       | Inventory route not on network    |
| (112) NBIS BRIDGE LENGTH:     | Yes                                |                                          |                                   |

### NAVIGATION DATA

|                                    |                                                                    |                                                              |           |
|------------------------------------|--------------------------------------------------------------------|--------------------------------------------------------------|-----------|
| (38) NAVIGATION CONTROL:           | 0 - No navigation control on waterway (bridge permit not required) | (39) NAVIGATION VERTICAL CLEAR:                              | 000.0 FT  |
| (111) PIER OR ABUTMENT PROTECTION: |                                                                    | (116) MINIMUM NAVIGATION VERT. CLEARANCE, VERT. LIFT BRIDGE: | FT        |
|                                    |                                                                    | (40) NAV HORIZONTAL CLEARANCE:                               | 0000.0 FT |

### PROPOSED IMPROVEMENTS

|                               |                                  |           |
|-------------------------------|----------------------------------|-----------|
| (75A) TYPE OF WORK:           | (95) ROADWAY IMPROVEMENT COST:   | \$ 000000 |
| (75B) WORK DONE BY:           | (96) TOTAL PROJECT COST:         | \$ 000000 |
| (76) LENGTH OF IMPROVEMENT:   | (97) YR OF IMPROVEMENT COST EST: |           |
| (94) BRIDGE IMPROVEMENT COST: | (114) FUTURE AVG DAILY TRAFFIC:  | 004446    |
|                               | (115) YR OF FUTURE ADT:          | 2030      |

**Miscellaneous Asset Data**  
**Asset Management**

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024970

**Load Rating 2:**

Has the dead load or the structural condition of the primary load carrying members changed since the last inspection?

No - Load Rating Update Not Required

**Extended Frequency:**

Submittal Date:

Inspector:

INDOT Reviewer:

This bridge has been accepted into the Extended Frequency Program.

Approval Date:

**Joints:**      *\* Indicate location, type, and rating of lowest rated joint.*

Transverse South/W

O

6 - Satisfactory Condition,  
minor wear, no leaks

Comments:

"Exp. Jt. Type - Coded as an "O" = Expansion Joint Sealing System Type Joint = (XJS), over both Abutments and over Pier #2. Minor adhesion damage in approach span joint. The mid joint has a diagonal cracking and a small spall.

**Terminal Joints:**      *\*Rating of lowest rated terminal joint.*

N

Comments:

**Concrete Slopewall:**      *\*Rating of lowest rated slopewall.*

N

Comments:

**Bearings:**      *\* Indicate type, and rating of lowest rated bearing.*

1 - Steel

6

Comments:

Minor areas of pack rust in the piers.

**Approach Slabs:**      *\* Indicate if present & condition rating.*

1 - Approach Slabs

7 - Good condition, minor cracking, wide spacing

Comments:

---

**Paint:** \* *Indicate if paint present , year painted & condition rating.*

1 - Steel Beams

6 - Satisfactory Cond

2007

Comments:

Paint looks good from a distance, but rust is re-forming on many lower chords connections and gusset plates, with section loss in certain areas. Upper truss areas look good except at a few locations. Diaphragms have rusting throughout as well.

---

**Scour Analysis:** 5

**Scour Critical:**

**Scour POA?**

No

NBI 113 Scour Comment:

This item is coded as an '8' = LOW Risk for Vulnerability for scour. This rating is based on all of the foundations being set on piles. Also, the 2001 Rehab called for Designed Class-2 type rip rap to be placed on the banks of the channel under the bridge, to protect Pier #2 and the North Abutment.

The 2001 Flow Line as shown on the Plans = elv. 652.19', with the low elevation at the toe of the new rip rap keyway, at the north bank.

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**Endangered Species:** \* *If yes, add one photo to the dropdown field*

Bats: seen or heard under structure? \*

N - No evidence of bats

Birds/swallows/nests seen? Empty nests present? \*

N - No Birds and/or Nests Visi

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**BRIDGE Culvert Geometry:**

Barrel Length:

Height:

Width:



# Alternative Analysis Report

## SR 75 over Wildcat Creek

*Section 33, T-24-N, R-01-W, Democrat Township, Carroll County, Indiana*

Project Number: 1601029  
Bridge File No: 075-08-03653 B  
NBI Number: 024970  
DES No: 1601029  
Location: 40°28' 54" N; 86° 31'48" W  
4.51 mi South of SR 18  
Reference Post: 50 + 52



David A. Day, PE

Nevil Gomes, EI

DATE: 11/11/2019  
American Structurepoint, Inc.  
9025 River Road  
Indianapolis, IN 46240

DATE: 11/11/2019  
American Structurepoint, Inc.  
9025 River Road  
Indianapolis, IN 46240

This bridge was evaluated by personnel from the Indiana Department of Transportation (INDOT) Bridge Design Unit, the District Office and the designer. The attached Draft Historic Bridge Alternatives Analysis has been reviewed by the INDOT Bridge Design Unit and Cultural Resources Office for thoroughness of the rehabilitation option and compliance with INDOT design policies. Concurrence by INDOT with the proposed Scope of Work does not constitute Final Approval of the Historic Bridge Alternatives Analysis. This draft HBAA may now be distributed to the historic consulting parties for review.



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INC.

## 1.0 Field Inspection Data

Inspection Date: 10/05/2017 10:00 A.M. EDT

Conditions: 65°, Cloudy

Attendance: Jackie Hockaday, INDOT, LaPorte District Project Manager  
Don Leonard, INDOT, LaPorte District Area Engineer  
Mark Pittman, INDOT, LaPorte District Bridge Asset Engineer  
Ashley Sharkey, INDOT, LaPorte District Environmental  
Jessica Spiess, INDOT, LaPorte District Environmental  
Edgar Zambrano, INDOT  
John Myers, INDOT

David Day, PE, American Structurepoint, Inc., Senior Project Manager  
Karl Krukenberg, PE, American Structurepoint, Inc., Project Manager  
Nevil Gomes, EI, American Structurepoint, Inc., Bridge Engineer  
Leah Boits, PE, American Structurepoint, Inc., Environmental Project Manager  
Monica Del Real, American Structurepoint, Inc., Environmental Engineer

## 2.0 Existing Structure Data

### 2.1 Identification/History

Original Construction: 1947

Rehabilitation: 1974 and 2001

Most Recent Field Inspection Date: June 13, 2017

Average Daily Traffic (ADT)/Year of ADT: 2448 / Aug 2017

Percentage of Commercial Vehicles: 10%

Posted Speed Limit: 55 mph

Low Volume Road: No

Functional Classification: Rural Major Collector

Detour Length: 10.4 Miles

Current Inventory Rating (HS-20): 30 Tons

Sufficiency Rating: 69.6

National Register of Historic Places: Eligible

Historic Bridge Prioritization Status: Non-Select

Historic Character-Defining Features: This is a Steel, Parker thru truss. According to Indiana Historic Bridge Inventory, *"This Bridge represents an early or distinctive phase in bridge construction, design, or engineering and it retains the historic integrity necessary to convey its engineering significance. It represents a significant phase or feature of the work of a master. It is distinguishable when compared with similar structures and retains historic integrity necessary to convey engineering or design significance."*

*"This bridge is important as one of six or fewer examples within a district of Indiana department of transportation. It also exhibits the important contributions made by an accomplished Indiana*

*engineer, designer, fabricator or builder and displays distinctive engineering and/or aesthetic characteristics."*

The original structure was designed for H20 (20 Tons) loading. The structure consists of two spans. Span A is 61'-6" long deck girder supported by 7 WF 33 × 141 beams. Span B is a 175'-0" long standard INDOT truss (Standard Drawing No. 1551, March 24, 1941). Bridge has been rehabilitated in 1974 and in 2001.

## **2.2 Structure and Dimensions**

Surface Type: 6.5" Thick Reinforced Concrete Slab with 2.5" Microsilica Overlay (Per 2001 plans)

Out-to-out Copings: 32'-0"

Out-to-out Bridge Floor: 240'-0"

Clear Roadway Width: 28'-0"

Number of Lanes on the Structure: 2 Lanes

Skew: No Skew

Superstructure Type: Span A: Deck Girder; Span B: Thru-Truss.

Spans: 2 Spans, 61'-6", 175'-0" C-C Bents

Type of Substructure, Foundation: Abutment 1: Stub wall abutment on footing with 68 piles; Pier 2: wall pier on pile footing with 42 piles; Abutment 3: Stub wall abutment on footing with 72 piles. All piles are 12" untreated timber piles driven to 20 ton bearing.

## **2.3 Appurtenances**

Bridge Railing: Present, Type CF-1, Height 36" From Roadway surface, 26" From top of Curb.

Curbs: Present on both sides, Height 10 inch, 18 inch wide.

Sidewalks: None Present.

Utilities: An overhead electric line is present on the East of the structure.

Railroad: None present.

## **2.4 Approaches**

Roadway Width: 28'-0", (2) 11'-9" travel lanes, (2) 2'-3" paved/usable shoulders

Surface Type: Reinforced Concrete. Drains present on both sides.

Guardrail: W-Beam Guardrail

NE Quadrant\*: 335'-0" W-Beam Guardrail

SE Quadrant\*: 380'-0" W-Beam Guardrail

SW Quadrant\*: 320'-0" W-Beam Guardrail

NW Quadrant\*: 320'-0" W-Beam Guardrail

\*Lengths are approximate.

Guardrail Transition: Guardrail Transition Type TGT which transitions to CF-1 railing.

### **3.0 Existing Conditions**

#### **3.1 Deck [7-Good]; Wearing Surface [5-Fair]**

Overlay: 2.5" Microsilica overlay.

Deck Condition: The deck surface has transverse and longitudinal cracks. Light debris present on the shoulders on both sides. Exposed reinforcement in some locations on copings. The existing roadway does not meet the current geometric design criteria. The roadway does not have adequate shoulder width that is required for a state route [IDM Figure 55-3B].

Underside Condition: The underside of the deck is in good condition with some light cracking and efflorescence. Barn Swallow nests found under the deck on diaphragms.

Joints: 45 mm expansion joint sealing system present. The joints are in good condition. Some sand debris has accumulated in the joints.

Drainage: Drains present on both sides along the railings and appear to be in good condition. There is some surface rust present. Drain pipe under one of the inlets is missing and the water drains directly on a cross bracing member.

Bridge Railing: Type CF-1 railing is located on the bridge. Railing appears to be in an overall good condition except major section loss at the locations where the railing crosses the inclined end posts. Existing CF-1 railings do not satisfy INDOT's current performance criteria.

Curbs or Sidewalk: 10" x 13" curbs present on both sides of the structure.

#### **3.2 Superstructure [5-Fair]**

Repair/Maintenance: Superstructure has been rehabilitated twice (1974 and 2001) since its construction in 1947. Bridge rehabilitation in 1974 consisted of deck and approach slab replacement, bituminous overly, structural steel diaphragm repair, and, mud wall reconstruction. The structural steel was also painted as a part of this contract.

Bridge rehabilitation in 2001 consisted of Microsilica overlay and full/partial depth patching, and, approach slab replacement. Expansion joint was reconstructed and lower chord (splice plates) were replaced. New bridge railing (Type CF-1) was installed on the bridge. Bridge was last painted in September 2007.

Specific Deficiencies: Loss of section (Approx. 2" x 2") on some vertical and diagonal members on East Truss. Thinning of gusset plates on the bottom chord. Gusset plate analysis conducted in 2012 reveals that gusset plate fastener capacities at some connections govern over the member capacities. North cross bracing is deformed. North floorbeam is deformed due to a truck collision during 2001 rehabilitation. Rust on structural members emerged within three years of the most recent painting per the historical inspection reports.

Fracture Critical Members: Bottom chord members and members L1U1 and L7U7 are identified as fracture critical members. There is no significant damage noted to these members. There is some localized surface rust present near the connections of these members. No fracture critical members present on Span A.

Damage: North floor beam is heavily deformed. The bottom flange plate is bent up 2" and the web is out of plane. The beam was hit by an errant truck during 2001 approach slab rehabilitation. No accident damage noted on other truss members.

Bearings, Pedestals: Bearings are in satisfactory condition and are functioning as desired.

Other: Typical surface rust and paint chipping present near the riveted connections.

### **3.3 Substructure and Foundation [6-Satisfactory]**

Repair/Maintenance: Abutments 1 and 3 and pier 2 were repaired during 1974 rehabilitation. Portions of pier 2 and abutment 3 were repaired in 2001. Riprap was placed around pier 2 and abutment 3.

Specific Deficiencies: Minor cracking and efflorescence present on the abutments and the pier.

Drainage: There is no substructure drainage present.

Scour: Class 2 Riprap present around pier 2 and abutment 3. Riprap is in excellent condition and it is functioning as desired. The bridge is not considered scour critical.

Channel: There are large tree logs present in the channel near the bridge on the upstream side. Mussels present in the stream. This section of the Wildcat Creek is a State Scenic River and it is a popular canoe route.

### **3.4 Approaches**

Approach Pavement: Approach pavement was replaced in 2001. The approach roadway has a number of longitudinal cracks that have been previously sealed. North approach has severe rutting along the east construction joint. The existing approach pavement does not meet the current geometric design criteria. The roadway does not have adequate shoulder width that is required for a state route [IDM Figure 55-3B].

Guardrail: See Section "2.5: Approaches" for guardrail details. Guardrail appears to be in fair condition.

Drive or Public Road: CR W 485 S (Main Street, Custer, IN) is present approximately 1500'-0" south of the structure. CR W 400 S is present approximately 2500'-0" north of the structure. CR W 475 S present approximately 60'-0" south of the structure on the east shoulder. There are two Class V Drives (field entrance) located approximately 60'-0" away from the structure in NE and SW quadrants respectively.

Traffic Control Devices: There are pavement markings and snowplowable raised pavement markers (SPRM's) present on the bridge. Bridge End Markers are located at the end of the guardrail and near the bridge ends in each quadrant. There is a reference post marker present in the SE quadrant. A State Scenic River Signs are present in SE and NW quadrants. A stop sign is present on CR 475 S at the intersection.

Roadway Drainage and Pipes: There are no drainage structures present on the approaches.

Utilities: See Section "2.4: Appurtenances" for utilities identified during the field inspection.

### **3.5 Slopewalls**

There are no slopewalls at this structure.



#### 4.0 Purpose and Need

The need for the proposed project is a result of the insufficient legal load rating, deteriorated condition of the structural steel, superstructure, and wearing surface, and the inadequate usable shoulder width on the bridge.

- The load rating is 30 tons. The legal load rating is 36 tons.

Secondary concerns include:

- Structural steel deficiencies have been observed:
  - Loss of section (approximately 2-inch x 2-inch voids) on several vertical and diagonal truss members
  - Thinning of the steel plates connecting truss members
  - Two bottom chord members are fracture critical and some localized rust has been observed near the connections for these members
  - Deformation of north floor beam from impact during 2001 rehabilitation
- The superstructure and wearing surface are rated 5 (fair) out of 9. The superstructure has been rated at fair for over a decade. As it continues to rust and deteriorate, a rating of poor is anticipated in the near future, if the "Do Nothing" alternative is selected. With the fracture critical details, it is not advisable to allow superstructure deterioration to advance without addressing.
- The existing shoulder width on the bridge is 2 feet, 3 inches and does not meet current safety standards, which require at least 3 feet.

The purpose of the proposed project is to improve the legal load rating of the bridge to 36 tons or more so that restrictions are not required according to FHWA Load Rating policy, to improve the superstructure rating from a 5 to a 7 (good), and to improve the wearing surface rating from 5 to an 8 (very good) or 9 (new).

#### 5.0 Alternatives

The following alternatives were considered in the development of this project.

##### 5.1 Alternative A: Do Nothing/No Build (\$0 CN)

This alternative means no federal funds would be expended and no work would be done, even rehabilitation, to the bridge. Without rehabilitation, the bridge would continue to deteriorate until some point when the bridge would have to be closed to traffic due to unsafe conditions. This alternative is not feasible as it does not maintain a functional bridge. This alternative is not prudent since continued deterioration of the steel may lead to load restrictions within the next 20 years. The bypass detour length for this structure is 10.4 miles. The bridge currently load rates at 30 tons, which is below the required 36 - doing nothing does not address the deficiency and risks decline in the load rating. Therefore, other alternatives shall be considered.

## **5.2 Alternative B-1: Rehabilitation for Continued Vehicular Use Meeting Secretary of Interior's Standards (\$1,416,400 CN)**

This alternative involves rehabilitation of the existing structure for continued vehicular use by increasing the structural capacity to at or above the legal load rating. The existing bridge deck would be removed from both of the spans and a new concrete deck would be constructed. The existing approach slabs will be replaced as well. Additional rehabilitation of the deck would include installing a new expansion joint at pier 2, creating a new drainage system for the structure and replacing pavement markings in-kind with previous roadway striping. After the repairs are done, the structural steel will be painted to prevent corrosion and prolong the life of the bridge.

Superstructure rehabilitation would include cleaning top flanges of truss floor beams in span B and main beams in span A. Shear studs will be installed on top of these beams for effective shear transfer between the beams and the slab. The size of shear studs and the spacing has not been established. Composite action will increase the load carrying capacity of the floor beams and deck girder beams. Increased member capacity via the composite action of shear studs will raise the legal load rating of the structure. A nominal amount of structural steel will be included for repair/replacement of some stringers. Proposed structural steel would include replacement of any damaged or deteriorated components with an "in-kind" member. Previous experience with the repairs of such bridges indicate that some stringers may have corrosion on top flanges that might not be evident until the slab is removed. Some vertical and diagonal members have lost some section (approximately 2" x 2") in some locations. These holes will be cleaned and patched.

The damaged floor beam (north, near abutment 3) is locally deformed. Such deformation is classified as a category L damage according to Federal Highway Administration's (FHWA) *"Guide for Heat-Straightening of Damaged Steel Bridge Members"*. According to these guidelines, the beam can be heat-straightened without causing a significant change the material properties of the structural steel. Gusset plates connecting the cross bracing will be replaced. The existing bearing assemblies will remain in place and will be cleaned and painted. Spalled bearing pedestals will be patched. Spalled sections on abutments and pier 2 will be patched as well. Abutment 1 will be converted into a semi integral end bent. Existing CF-1 railing will be removed and replaced with PF-1 railing and TPF railing transition at all four ends. The original railing (constructed in 1947) will stay in place. Existing guardrail will be reset as necessary.

Gusset plate analysis shows that the fastener strength governs the load rating of the truss. In order to increase the structural capacity to at or above the legal load rating, it is necessary to replace the rivets at panel points U1 and U7. About 88 of these inadequate rivet fasteners at panel points U1 and U7 will be replaced with high strength bolts.

Existing drains on the structure are spaced at 40'-0" intervals. Preliminary drainage calculation reveals that the drain spacing is inadequate. SQ type deck drains at 18'-0" intervals will be provided on the new deck.

The usable shoulder on the bridge is currently 2'-3" on each edge. Per 3R Geometric Design Criteria for Rural Collector State Route, the minimum usable shoulder width required is 3 feet to meet Level-1 Criteria. To avoid a Design Exception for shoulder width, the lane width will be reduced from 11'-

9" to 11'-0" allowing the shoulder width to be increased from 2'-3" to 3'-0". The total clear roadway width will remain unchanged. The rehabilitation for continued vehicular use alternative is feasible as it meets the project purpose and need. The following table summarizes the design criteria:

| Design Element             | Design Manual Table/Section | Minimum Design Criteria | Existing Criteria | Proposed Condition | Design Exception Required |
|----------------------------|-----------------------------|-------------------------|-------------------|--------------------|---------------------------|
| Clear Roadway Width        | Figure 55-3 B               | 28 ft                   | 28 ft             | 28 ft              | No                        |
| Travel Lane                | Figure 55-3 B               | 11 ft                   | 11 ft 9 in        | 11 ft              | No                        |
| Usable Shoulder            | Figure 55-3 B               | 3 ft                    | 2 ft 3 in         | 3 ft               | No                        |
| Paved Shoulder             | Figure 55-3 B               | 2 ft                    | 2 ft 3 in         | 3 ft               | No                        |
| Structural Capacity        | Figure 55-3 B               | HS-15                   | HS-20             | HS-20              | No                        |
| Bridge Railing Performance | Figure 55-3 B               | TL-4                    | TL-2              | TL-4               | No                        |

The proposed scope of the repair work for B-1 includes:

- Remove and replace the existing bridge deck and concrete railing
- Install shear studs on top flange of existing beams
- Heat-straighten the north floor beam and replace the connecting gusset plate
- Repair the structural steel with patching or replacement with in-kind members
- Replace inadequate rivets with high strength bolts
- Paint the structural steel
- Semi-integral bent conversion at Bent 1
- Incidental approach tie-in

This alternative is feasible because it would meet the INDOT design criteria. The estimated cost to rehabilitate the existing bridge is \$1,416,000. Preliminary costs for a replacement bridge along the existing alignment are \$2,732,000, making rehabilitation costs about 52% of replacement costs making this alternate also prudent. The steel truss will most likely require painting and bridge deck overlay rehabilitation work to maintain serviceability over the next 25 years which is typical of most structures.

## 6.0 Minimization and Mitigation

### 6.1 Minimization (Rehabilitation Option)

- Members that will be replaced with this rehabilitation will be replaced "in-kind".
- Rivets replaced could utilize round-headed bolts if standard hex is not allowed, but at an additional cost.

## 6.2 Mitigation

INDOT will consult with the Indiana SHPO to decide if photo documentation of the bridge is needed prior to construction.

## 7.0 Preliminary Preferred Alternative

The rehabilitation alternative is feasible and prudent, and therefore is the preferred alternative. The comparison of the costs of all the alternatives that were considered is listed below. See Appendix D for cost details.

| Alternative                                                     | Meets Project Purpose & Need? | Construction Cost | Right of Way Cost   | Total Cost  | Other Factors                                                                                                                      | Feasible and Prudent                                                                                                                                           |
|-----------------------------------------------------------------|-------------------------------|-------------------|---------------------|-------------|------------------------------------------------------------------------------------------------------------------------------------|----------------------------------------------------------------------------------------------------------------------------------------------------------------|
| A-No Build                                                      | No                            | \$0               | N/A                 | \$0         | Bridge does not meet the required load capacity. Deterioration of superstructure would progress to eventual closure of the bridge. | The alternative is not prudent because it does not meet the project purpose and need.                                                                          |
| B-1-Rehabilitation for Continued Vehicular Use (Two-Way Option) | Yes                           | \$1,416,400       | No New ROW Required | \$1,416,400 | This alternative meets all the project requirements.                                                                               | This alternative is feasible because it would meet the INDOT design criteria. It is prudent because the rehabilitation cost is about 52% of replacement costs. |

**Land and Water Conservation Fund (LWCF) County Property List for Indiana (Last Updated July 2020)**

| ProjectNumber | SubProjectCode | County  | Property             |
|---------------|----------------|---------|----------------------|
| 1800514       | 1800514        | Carroll | Flora Community Park |

\*Park names may have changed. If acquisition of publically owned land or impacts to publically owned land is anticipated, coordination with IDNR, Division of Outdoor Recreation, should occur.



## Perry, Leah

---

**From:** Miller, Brandon <BraMiller1@indot.IN.gov>  
**Sent:** Tuesday, June 16, 2020 1:47 PM  
**To:** Perry, Leah  
**Cc:** Day, Dave; Hope, Briana  
**Subject:** RE: SR 75 over Wildcat Creek, 4-f coordination, Des. 1601029

**Follow Up Flag:** Follow up  
**Flag Status:** Flagged

Likely temporary occupancy. You might have difficulties with IDNR Outdoor Rec claiming to be the OWJ (based on past experience). They might refer you to local groups that manage the river recreation.

**Brandon Miller**  
**INDOT Environmental Services Division**  
**Office:** (317) 234-5108

---

**From:** Perry, Leah <lperry@structurepoint.com>  
**Sent:** Tuesday, June 16, 2020 12:01 PM  
**To:** Miller, Brandon <BraMiller1@indot.IN.gov>  
**Cc:** Day, Dave <Dday@structurepoint.com>; Hope, Briana <bhope@structurepoint.com>  
**Subject:** RE: SR 75 over Wildcat Creek, 4-f coordination, Des. 1601029

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Hi Brandon,

I just wanted to verify regarding the emails below. Is this a temporary occupancy? Or do you need more information from me to make that determination.

Thank you,

---

**Leah Perry**  
**Environmental Specialist**  
9025 River Road, Suite 200  
Indianapolis, IN 46240  
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---

**From:** Perry, Leah  
**Sent:** Monday, May 18, 2020 2:30 PM  
**To:** 'Miller, Brandon' <[BraMiller1@indot.IN.gov](mailto:BraMiller1@indot.IN.gov)>

**Cc:** Day, Dave <[Dday@structurepoint.com](mailto:Dday@structurepoint.com)>; Hope, Briana <[bhope@structurepoint.com](mailto:bhope@structurepoint.com)>

**Subject:** RE: SR 75 over Wildcat Creek, 4-f coordination, Des. 1601029

Good afternoon Brandon,

To our knowledge, IDNR Outdoor Recreation would be the official with jurisdiction. We are not confident there would be enough space to keep the river open during construction, therefore, we are assuming the project would result in a temporary closure. A closure would likely be in place for 1 to 2 months.

Thank you,

---

**Leah Perry**

**Environmental Specialist**

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---

**From:** Miller, Brandon <[BrMiller1@indot.IN.gov](mailto:BrMiller1@indot.IN.gov)>

**Sent:** Monday, May 11, 2020 11:59 AM

**To:** Perry, Leah <[lperry@structurepoint.com](mailto:lperry@structurepoint.com)>

**Cc:** Day, Dave <[Dday@structurepoint.com](mailto:Dday@structurepoint.com)>; Hope, Briana <[bhope@structurepoint.com](mailto:bhope@structurepoint.com)>

**Subject:** RE: SR 75 over Wildcat Creek, 4-f coordination, Des. 1601029

Leah,

This sounds as if it would potentially be a temporary occupancy (if the river would be restricted for any time frame for construction concerns). Is there an estimated time for closure or is there enough space to keep one side open at all times? Is IDNR Outdoor Rec the OWJ or is there someone else?

**Brandon Miller**

**INDOT Environmental Services Division**

**Office:** (317) 234-5108

---

**From:** Perry, Leah <[lperry@structurepoint.com](mailto:lperry@structurepoint.com)>

**Sent:** Monday, May 11, 2020 7:50 AM

**To:** Miller, Brandon <[BrMiller1@indot.IN.gov](mailto:BrMiller1@indot.IN.gov)>

**Cc:** Day, Dave <[Dday@structurepoint.com](mailto:Dday@structurepoint.com)>; Hope, Briana <[bhope@structurepoint.com](mailto:bhope@structurepoint.com)>

**Subject:** SR 75 over Wildcat Creek, 4-f coordination, Des. 1601029

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Hello Brandon,

We are working on a project that involves a historic bridge over Wildcat Creek (SR 75 over Wildcat Creek in Carroll County, Des. 1601029). The portion of Wildcat Creek in the project area is a state Natural Scenic and Recreational River and is on the Outstanding Rivers list. It is also an official canoe/boating route and designated high water quality. Construction activities will not be occurring in the stream, only above to rehab the bridge. However, with the proposed deck removal and replacement, there could be debris that fall to the creek below (by accident-the contractor is supposed to prevent debris from falling). If needed, a special provision can be added for the Contractor to place plywood or something similar along the lower flanges of the floor beams and stringers to make sure no debris falls below to the creek. Signs can also be posted along Wildcat Creek notifying canoers that there is construction ahead and to be aware. The proposed work should not require a closure of the waterway to users/canoers. We will be cleaning and painting the truss. As part of this, the contractor is required to control the fugitive dust from the cleaning as well as the paint particles to protect the stream. It is possible the stream could be closed by the contractor at the bridge for the protection of the users during the cleaning and painting due to the dust/paint control methods. If closed, the contractor would put up signs notifying the canoers and the canoers would walk around the construction site.

In addition, as part of the early coordination process, we coordinated with DNR Division of Outdoor Recreation and they stated: "If all preventative measures are taken to avoid impacts to the river and its recreational use there should be no negative impacts." Because Wildcat Creek is open to the public and identified as an official canoe/boating route we believe it would be considered a 4(f) resource. However, we do not believe this project would result in a "use" of the resource temporary or, permanent or that it would result in a temporary occupancy. Please let us know if you concur with our findings.

Thank you,

---

**Leah Perry**

**Environmental Specialist**

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## Perry, Leah

---

**From:** Shepherdson, Markita <mshepherdson@dnr.IN.gov>  
**Sent:** Friday, July 31, 2020 7:46 AM  
**To:** Perry, Leah  
**Subject:** RE: Wildcat Creek canoe/boating trail/recreational use, SR 75 over Wildcat Creek - Des. No. 1601029, temporary occupancy

Rats! Let me try someone else.

---

**From:** Perry, Leah [mailto:lperry@structurepoint.com]  
**Sent:** Friday, July 31, 2020 7:45 AM  
**To:** Shepherdson, Markita <mshepherdson@dnr.IN.gov>  
**Cc:** Hope, Briana <bhope@structurepoint.com>  
**Subject:** RE: Wildcat Creek canoe/boating trail/recreational use, SR 75 over Wildcat Creek - Des. No. 1601029, temporary occupancy

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---

Markita,

No, I have not heard anything from anyone on this.

Thanks for your help.

---

**Leah Perry**  
**Environmental Specialist**  
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---

**From:** Shepherdson, Markita <[mshepherdson@dnr.IN.gov](mailto:mshepherdson@dnr.IN.gov)>  
**Sent:** Friday, July 31, 2020 7:44 AM  
**To:** Perry, Leah <[lperry@structurepoint.com](mailto:lperry@structurepoint.com)>  
**Cc:** Hope, Briana <[bhope@structurepoint.com](mailto:bhope@structurepoint.com)>  
**Subject:** RE: Wildcat Creek canoe/boating trail/recreational use, SR 75 over Wildcat Creek - Des. No. 1601029, temporary occupancy

Leah,

I sent this on to the Division of Law Enforcement, to see if they handled it. Did anyone get back with you?

Markita

---

**From:** Perry, Leah [<mailto:lperry@structurepoint.com>]

**Sent:** Monday, July 13, 2020 1:03 PM

**To:** Shepherdson, Markita <[mshepherdson@dnr.IN.gov](mailto:mshepherdson@dnr.IN.gov)>

**Cc:** Hope, Briana <[bhope@structurepoint.com](mailto:bhope@structurepoint.com)>

**Subject:** FW: Wildcat Creek canoe/boating trail/recreational use, SR 75 over Wildcat Creek - Des. No. 1601029, temporary occupancy

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Markita,

Thank you for looking into this for me. As discussed, we are trying to determine who the “official with jurisdiction” or OWJ would be for Wildcat Creek as it is a public canoe trail protected by Section 4-f. We believe this would be a temporary occupancy under Section 4-f. As a temporary occupancy, we need to coordinate with the OWJ.

Please see the email chain below for additional information and a letter that serves as the coordination needed under Section 4(f),

Sincerely,

---

**Leah Perry**

**Environmental Specialist**

9025 River Road, Suite 200

Indianapolis, IN 46240

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---

**From:** Hurst, Allen A <[AHurst@dnr.IN.gov](mailto:AHurst@dnr.IN.gov)>

**Sent:** Thursday, July 2, 2020 3:19 PM

**To:** Perry, Leah <[lperry@structurepoint.com](mailto:lperry@structurepoint.com)>

**Cc:** Herber, Lisa <[LHerber1@dnr.IN.gov](mailto:LHerber1@dnr.IN.gov)>; Brier, Dale <[dbrier@dnr.IN.gov](mailto:dbrier@dnr.IN.gov)>

**Subject:** RE: Wildcat Creek canoe/boating trail/recreational use, SR 75 over Wildcat Creek - Des. No. 1601029, temporary occupancy

**Des. No:** 1601029

**Description:** SR 75 over Wildcat Creek Bridge No. 075-08-03653 B, Carroll County, Indiana

Leah,

This project will occur within a section of Wildcat Creek that is designated as a Natural, Scenic, and Recreational River under the Indiana Natural, Scenic, and Recreational River System (I.C. 14-29-6). The project will neither enhance nor detract from the scenic quality of the river because the project is rehabilitating an existing bridge. Furthermore, as long



as there is signage signifying closures and there is a pathway for water trail users to circumvent potential closures, the Division of Outdoor Recreation would mark approval for this project.

With concerns to OWJ, our division does not hold it nor can sign off as such. It likely resides with the Division of Water who handles all the permitting. The extent of our involvement is reviewing projects and commenting as it relates to natural, scenic, and recreational rivers.

**ALLEN HURST**

TRAILS COORDINATOR

DIVISION OF OUTDOOR RECREATION

DEPARTMENT OF NATURAL RESOURCES | [DNR.IN.GOV](http://DNR.IN.GOV)

402 W. WASHINGTON ST. ROOM W271, INDIANAPOLIS, IN 46204

P: (317)232-4070 | [AHURST@DNR.IN.GOV](mailto:AHURST@DNR.IN.GOV)

\* Please let us know about the quality of our service by taking this brief [customer survey](#).

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**From:** Herber, Lisa

**Sent:** Friday, June 26, 2020 11:16 AM

**To:** Hurst, Allen A <[AHurst@dnr.IN.gov](mailto:AHurst@dnr.IN.gov)>

**Subject:** FW: Wildcat Creek canoe/boating trail/recreational use, SR 75 over Wildcat Creek - Des. No. 1601029, temporary occupancy

For you Allen...

*Sincerely,*

*Lisa Herber*

Grants Coordinator

Indiana Department of Natural Resources

Division of Outdoor Recreation – Grants and Planning Section

402 W. Washington Street, W271

Indianapolis, Indiana 46204

Phone: 317-232-4074

Email: [LHerber1@dnr.IN.gov](mailto:LHerber1@dnr.IN.gov)

\* Please let us know about the quality of our service by taking this brief [customer survey](#).

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**From:** Perry, Leah [<mailto:lperry@structurepoint.com>]

**Sent:** Friday, June 26, 2020 10:06 AM

**To:** Herber, Lisa <[LHerber1@dnr.IN.gov](mailto:LHerber1@dnr.IN.gov)>

**Cc:** Hope, Briana <[bhope@structurepoint.com](mailto:bhope@structurepoint.com)>; Day, Dave <[Dday@structurepoint.com](mailto:Dday@structurepoint.com)>

**Subject:** Wildcat Creek canoe/boating trail/recreational use, SR 75 over Wildcat Creek - Des. No. 1601029, temporary occupancy

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Lisa,

American Structurepoint is working on design plans to rehabilitate SR 75 over Wildcat Creek in Carroll County, Des. 1601029. As part of the project, a temporary closure of Wildcat Creek to recreational use may be required during

construction at the bridge. We believe this qualifies as a section 4-f temporary occupancy. The original early coordination letter that was sent to your office in 2018 is attached, including the state location map, topographic map, aerial photography and ground-level photos of the project area. We are required coordinate with the official with jurisdiction (OWJ) for the Wildcat Creek Canoe/boating trail (recreational use of the river) and believe IDNR Outdoor Rec is the OWJ. I have attached a letter that serves as the coordination needed under Section 4(f), as described in the Federal Highway Administration's Section 4(f) Policy Paper (dated July 20, 2012) (23 CFR 774.13(d)). The letter has been provided as a word document so that it may be placed on letterhead if desired before it is signed, dated and sent back to me. We believe IDNR Outdoor Rec is the OWJ, but please let us know if this assumption is incorrect.

Please see below for additional information regarding the proposed project:

The project proposes to rehabilitate the existing bridge for continued vehicular use by increasing the structural capacity to at or above the legal load rating. The existing bridge deck would be removed from both of the spans and a new concrete deck would be constructed. The existing approach slabs would be removed and replaced. Additional rehabilitation of the deck would include installing a new expansion joint at pier 2, creating a new drainage system for the structure and replacing pavement markings in-kind with previous roadway striping.

The top flanges of truss floor beams in span B and main beams in span will be cleaned. Shear studs will be installed on top of these beams to increase the load carrying capacity of the beams. Any damaged or deteriorated stringers or other structural steel components will be cleaned and patched or replaced "in-kind." The damaged floor beam will be heat straightened. Gusset plates connecting the cross bracing will be replaced. The existing bearing assemblies will remain in place and will be cleaned and painted. Spalled bearing pedestals will be patched. Spalled sections on abutments and pier 2 will be patched as well. Abutment 1 will be converted into a semi integral end bent. Existing type CF-1 railing will be removed and replaced with type PF-1 railing and type TPF railing transition at all four ends. The original railing (constructed in 1947) will stay in place. Existing guardrail will be reset as necessary. Approximately 88 rivets that have been determined inadequate will be replaced with high strength bolts to increase the load carrying capacity. After the repairs are done, the structural steel will be painted. The usable shoulder on the bridge is currently 2.25-feet on each edge. To meet minimum useable shoulder width requirements, the lane width will be reduced from 11.75-feet to 11-feet to increase the shoulder width to 3-feet. The total clear roadway width will remain unchanged. The incidental approach will be tied-in. No permanent or temporary right-of-way is anticipated to be required.

The portion of Wildcat Creek in the project area is a state Natural Scenic and Recreational River and is on the Outstanding Rivers list. It is also an official canoe/boating route and designated high water quality. Construction activities will not be occurring in the stream, only above to rehab the bridge. However, with the proposed deck removal and replacement, there could be debris that fall to the creek below (by accident-the contractor is supposed to prevent debris from falling). A special provision will be added for the Contractor to place plywood or something similar along the lower flanges of the floor beams and stringers to make sure no debris falls below to the creek. Signs will also be posted along Wildcat Creek notifying canoers that there is construction ahead and to be aware. The truss will be cleaned and painted. As part of this, the contractor is required to control the fugitive dust from the cleaning as well as the paint particles to protect the stream. It is possible the stream could be closed by the contractor at the bridge for the protection of the users during the cleaning and painting due to the dust/paint control methods. If closed, the contractor would put up signs notifying the canoers and the canoers would walk around the construction site. If a closure is necessary, it would likely be in place for 1 to 2 months.

Please let me know if you have any questions.

Thank you,

## Perry, Leah

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**From:** Ted Johnson <trjohnson011@yahoo.com>  
**Sent:** Wednesday, August 12, 2020 1:38 PM  
**To:** Perry, Leah  
**Subject:** RE: Coordination regarding SR 75 over Wildcat Creek Project in CarrollCounty, Des. 1601029  
**Attachments:** 2020-08-17 ltr re recreational use - Wildcat Bridge Project.docx

Ms. Perry,

Thank you for the information and reference. I have taken the liberty to make non-substantive edits to the form of the letter you provided. The edited letter is attached. Please review the edited letter and let me know if it is still satisfactory. If so, I will ask the auditor to process the letter on the Board of Commissioners letterhead and distribute it to the board members with the agenda in advance of their meeting this coming Monday morning.

When and to whom should the Board address a letter with their suggestions and concerns about timing the closure to avoid the logistical problem for moving grain and heavy farm equipment?

Thank you for your attention and assistance.

Ted Johnson, Attorney  
Board of Commissioners of Carroll County  
P.O. Box 70  
Springville, IN 47462  
(765)650-0258

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**From:** [Perry, Leah](#)  
**Sent:** Wednesday, August 12, 2020 12:54 PM  
**To:** [trjohnson011@yahoo.com](mailto:trjohnson011@yahoo.com)  
**Cc:** [Hope, Briana](#); [Day, Dave](#)  
**Subject:** Coordination regarding SR 75 over Wildcat Creek Project in CarrollCounty, Des. 1601029

Mr. Johnson,

Thank you for your phone call today. Please let me know if you have any specific questions about the project.

Specifically regarding the maintenance of traffic, we are anticipating use of a detour. Construction is anticipated to occur between August 2022 and November 2023.

Please find attached an early coordination letter that was mailed out for this project. Please note, since this time the alternatives analysis has been completed and it has been determined rehabilitation of the existing bridge is feasible and prudent. The alternatives analysis can be found in INSCOPE (link below) and has detail regarding the rehabilitation in section 5.2.

This link to IN SCOPE at <http://erms.indot.in.gov/Section106Documents/> (the Des. No. 1601029 is the most efficient search term, once in IN SCOPE) will take you to the SEC 106 documents for this project (Sec 106 Early coordination letter, Historic property report, finding, and alternative analysis).

The commissioners are consulting parties on the project.

Please let me know if any questions or concerns come up with this at next Monday's commissioners meeting.

Thanks,

---

**Leah Perry**

**Environmental Specialist**

9025 River Road, Suite 200

Indianapolis, IN 46240

317.547.5580 [OFFICE](#)

[structurepoint.com](http://structurepoint.com) [WEB](#)



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August 17, 2020

Leah Perry  
American Structurepoint, Inc.  
9025 River Road, Suite 200  
Indianapolis, Indiana 46240

Re: Section 4(f) Coordination, Des. No. 1601029  
Bridge carrying SR 75 over Wildcat Creek  
Carroll County, Indiana

Dear Mrs. Perry:

We understand that the project to rehabilitate the historic steel bridge carrying SR 75 over Wildcat Creek near Cutler (unincorporated) will affect the recreational use of the creek in Carroll County, Indiana. The stream is a state Natural Scenic and Recreational River and is a designated canoe/boating trail for the general public. As part of the project, no construction activities will occur in the stream, but it is possible that the stream could be closed temporarily to recreational use at the bridge during construction for 1-2 months for protection of the users.

As the executive of Carroll County and official with jurisdiction (OWJ) over the project area of Wildcat Creek, we agree that this temporary occupancy constitutes a no use under Section 4(f), as described in the Federal Highway Administration's *Section 4(f) Policy Paper (dated July 20, 2012)*, as stated below. Under Section 4(f), temporary occupancy will not constitute a use (23 CFR 774.13(d)), if:

- Duration must be temporary, i.e., less than the time needed for construction of the project, and there should be no change in ownership of the land;
  - No right-of-way will be acquired as part of the project.
  - Wildcat Creek will be closed to recreational use at the bridge for less than the time needed to construct the project (It is anticipated Wildcat Creek may be closed for 1 to 2 months).
- Scope of the work must be minor, i.e., both the nature and magnitude of the changes to the Section 4(f) property are minimal;
  - No work is occurring in Wildcat Creek. There will be no changes to Wildcat Creek.
- There are no anticipated permanent adverse physical impacts, nor will there be interference with the protected activities, features, or attributes of the property, on either a temporary or permanent basis;
  - There are no anticipated permanent adverse physical impacts to Wildcat Creek.
- The land being used must be fully restored, i.e., the property must be returned to a condition which is at least as good as that which existed prior to the project; and
  - Wildcat Creek will be fully restored to a condition at least as good as prior to the project.
- There must be documented agreement of the official(s) with jurisdiction over the Section 4(f) resource regarding the above conditions.
  - This letter serves as a documented agreement of the official(s) with jurisdiction over the Wildcat Creek canoe/boating trail (recreational use of the river).

Therefore, under the above conditions, we agree the project will not adversely affect the recreational activities, features, and attributes that qualify Wildcat Creek for protection under Section 4(f) of the US Department of Transportation Act.

BOARD OF COMMISSIONERS OF CARROLL COUNTY

  
William R. Brown, Chairman

  
Steve Pearson

  
Loren Hylton