

FHWA-Indiana Environmental Document

CATEGORICAL EXCLUSION / ENVIRONMENTAL ASSESSMENT FORM

GENERAL PROJECT INFORMATION

Road No./County:	State Road (SR) 63/Vermillion County
Designation Number(s):	2100968 & 2100969
Project Description/Termini:	Bridge Removal Project on State Road (SR) 63 over Former Railroad, 0.24 mile south of SR 234 in Cayuga, 370 feet south and 360 feet north of the centerline of the structures.

	Categorical Exclusion, Level 2 – Required Signatories: INDOT DE and/or INDOT ESD
<input checked="" type="checkbox"/>	Categorical Exclusion, Level 3 – Required Signatories: INDOT ESD
	Categorical Exclusion, Level 4 – Required Signatories: INDOT ESD and FHWA
	Environmental Assessment (EA) – Required Signatories: INDOT ESD and FHWA
	Additional Investigation (AI) – The proposed action included a design change from the original approved environmental document. Required Signatories must include the appropriate environmental approval authority

Approval

INDOT DE Signature and Date

INDOT ESD Signature and Date

N/A

FHWA Signature and Date

Release for Public Involvement*BNE*

1/9/2025

INDOT DE Initials and Date

ADWP

January 9, 2025

INDOT ESD Initials and Date

Certification of Public Involvement

INDOT Consultant Services Signature and Date

INDOT DE/ESD Reviewer Signature and Date:

R. Asadpour

January 9, 2025

Name and Organization of CE/EA Preparer:

Katie Finney; Egis BLN USA Inc. (Egis)

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County Vermillion

Route SR 63

Des. No. 2100968 and 2100969

Note: Refer to the most current INDOT CE Manual, guidance language, and other ESD resources for further guidance regarding any section of this form.

Part I – Public Involvement

Every Federal action requires some level of public involvement, providing for early and continuous opportunities throughout the project development process. **The level of public involvement should be commensurate with the proposed action.**

Does the project have a historic bridge processed under the Historic Bridges PA*?

Yes

No

If No, then:

Opportunity for a Public Hearing Required?

**A public hearing is required for all historic bridges processed under the Historic Bridges Programmatic Agreement between INDOT, FHWA, SHPO, and the ACHP.*

Discuss what public involvement activities (legal notices, letters to affected property owners and residents (i.e. notice of entry), meetings, special purpose meetings, newspaper articles, etc.) have occurred for this project.

Notice of Entry Letters were mailed to potentially affected property owners near the project area on January 30, 2023, notifying them about the project and that individuals responsible for land surveying and field activities may be seen in the area. A sample copy of the Notice of Entry letter is included in Appendix G, page 1.

Public Hearing to be Conducted

The project will meet the minimum requirements described in the current *Indiana Department of Transportation (INDOT) Project Development Public Involvement Procedures Manual* which requires the project sponsor to offer the public an opportunity to submit comments and/or request a public hearing. Therefore, a legal notice will appear in a local publication contingent upon the release of this document for public involvement. This document will be revised after the public involvement requirements are fulfilled.

On August 9, 2023, one of the Vermillion County Commissioners requested a public hearing be held for the project (Appendix C, page 13). The Vermillion County Commissioners and the INDOT project management team have been in coordination about the project. Holding a public hearing has been agreed to by INDOT at the request of the Vermillion County Commissioners.

Public Controversy on Environmental Grounds

Discuss public controversy concerning community and/or natural resource impacts, including what is being done during the project to minimize impacts.

No controversy

At this time, there is no substantial public controversy concerning impacts to the community or to natural resources.

Part II - General Project Identification, Description, and Design Information

Sponsor of the Project: Federal Highway Administration (FHWA) and INDOT INDOT District: Crawfordsville

Local Name of the Facility: SR 63 over abandoned railroad

Funding Source (mark all that apply): Federal State Local Other*

*If other is selected, please identify the funding source: _____

This is page 2 of 23 Project name: SR 63 over Former Railroad Date: January 8, 2025

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PURPOSE AND NEED:

The need should describe the specific transportation problem or deficiency that the project will address. The purpose should describe the goal or objective of the project. The solution to the traffic problem should NOT be discussed in this section.

Need: The project need is to address deteriorating conditions of the existing twin bridges carrying SR 63 northbound and SR 63 southbound over a former railroad. The northbound (NB) bridge is structure number 063-83-02002 ANBL, NBI # 022727, and the southbound (SB) bridge is structure number 063-83-02002 ASBLNBI # 022729.

Recent INDOT bridge inspection reports, dated August 7, 2024, and October 12, 2023, noted the bridges were in overall "fair" condition (Appendix I, pages 5, 42, and 43). According to the August 7, 2024, INDOT Bridge Inspection Report for the NB bridge, some bearings are loose or falling out, and both joins leak and have materials missing. Severe rust with advanced section loss is in all the end diaphragm at both end bents. The remainder of the bridge has numerous hairline to medium width cracks and spalling with efflorescence (Appendix I, page 5). According to the INDOT October 12, 2023, Bridge Inspection Report for the SB bridge, the structural joints are in critical condition with severe leaking and the approach slabs have mild cracking and wide spacing (Appendix I, page 42).

Purpose: The project purpose is to address the existing bridge deficiencies and provide a structurally sufficient roadway conveying traffic on SR 63 NB and SB.

PROJECT DESCRIPTION (PREFERRED ALTERNATIVE):

County: Vermillion

Municipality: The Town of Cayuga

Limits of Proposed Work: The project area is localized to the immediate area surrounding the twin bridges, extending approximately 370 feet south and 360 feet north of the centerline of the structures for a total of 730 feet.

Total Work Length: 0.19 Mile(s)

Total Work Area: 2.1 Acre(s)

<input type="checkbox"/>	<input checked="" type="checkbox"/>
Yes ¹	No
Date: N/A	

Is an Interstate Access Document (IAD)¹ required?

If yes, when did the FHWA provide a Determination of Engineering and Operational Acceptability?

¹If an IAD is required; a copy of the approved CE/EA document must be submitted to the FHWA with a request for final approval of the IAD.

Describe location of project including township, range, city, county, roads, etc. Existing conditions should include current conditions, current deficiencies, roadway description, surrounding features, etc. Preferred alternative should include the scope of work, anticipated impacts, and how the project will meet the Purpose and Need. Logical termini and independent utility also need discussed.

INDOT Crawfordsville District and the Federal Highway Administration (FHWA) intend to proceed with a bridge removal project in Vermillion County, Indiana.

Location

The project is on SR 63 over a former railroad corridor, approximately 0.24 mile south of SR 234 in Eugene Township, Vermillion County, Indiana. Specifically, the project is in Sections 4 and 5, Township 17 North, Range 9 West, as shown on the Newport 7.5-Minute US Geological Survey (USGS) topographic quadrangle (Appendix B, page 2). This project is within the incorporated limits of the Town of Cayuga.

Existing Conditions:

Structure Number 063-83-02002 ANBL (NB) and 063-83-02002 JBSB (SB) carrying SR 63 over a former railroad corridor, formerly the Norfolk and Southern Railroad, are twin three-span continuous steel beam bridges. The existing bridges will meet the 50-year National Register minimum age requirement at the time of construction; however, they are common examples of their type and, therefore, are not eligible for the National Register of Historic Places (NRHP). The NB and SB bridge lengths are 175.5 feet with a

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maximum span length of 70 feet. Originally built in 1976, they have had a latex-modified concrete (LMC) overlays installed in 1987. The wearing surfaces were epoxy-injected in 2019, and substantial beam and bearing repairs were completed in 2020. There have been several bridge crucial finds on the structures requiring emergency lane closures and repairs.

According to the August 7, 2024, Bridge Inspection Report for the NB bridge, the bridge has numerous hairline to wide width cracks, section loss, severe rust, and spall with efflorescence (Appendix I, page 5).

According to the October 12, 2023, Bridge Inspection Report for the SB bridge, the bridge joints are in critical condition with severe leaking, and the approach slabs have mild cracking and wide spacing (Appendix I, page 42).

The existing approach roadway on SR 63 has two 12-foot-wide travel lanes, one in each direction, with a four-foot paved inside shoulder and a 10-foot paved outside shoulder on both the NB and SB lanes. W-beam guardrails border the shoulder for the bridge approaches, and the median shoulders are bordered by double-faced W-beam guardrails. The existing SR 63 roadway facility is classified as a rural principal arterial (non-freeway). It is on the National Highway System and the National Truck Network, and the posted speed limit is 55 miles per hour (MPH). Forested areas and farm fields are located adjacent to the north and southwest quadrants of the project area, and the southeast quadrant contains a retention pond.

Preferred Alternative:

Removal of Existing Bridges with Reconstruction of SR 63 over Fill

The preferred alternative is to remove the existing bridges and reconstruct SR 63 over a fill section. The bridges conveying SR 63 over the former railroad corridor are no longer needed as the railroad is no longer in service, and the tracks have been removed. Removing the bridges will result in long-term cost savings through the elimination of maintenance costs.

The superstructure on the existing bridges will be removed entirely, and the end bents will be taken out to a depth of at least 2 feet below the proposed pavement subgrade. The bent caps will be removed from the interior bents. The existing concrete slope walls will be left in place and buried. The project will backfill the existing SR 63 crossing with roadway embankment material. SR 63 will be reconstructed with the same horizontal and vertical alignments, featuring two 12-foot travel lanes, a 4-foot median, and 10-foot outside paved shoulders in each direction. The existing median and side slopes meet current standards and will be maintained.

Pavement within the project limits will be replaced, and 50 feet of each approach will be milled and resurfaced to meet the existing pavement. Pavement markings will be replaced, and new traffic signs will be installed. Incidental construction will include pavement milling and resurfacing, guardrail removal, and linear grading.

The project requires approximately 1.2 acres of permanent right-of-way (ROW), 210 feet west of the centerline of SB SR 63 and 210 feet east of the centerline of NB SR 63 NB. The project does not require temporary ROW. The ROW will be acquired from one parcel, formerly the Norfolk and Southern Railroad corridor, which is located within INDOT apparent ROW. All ROW being acquired is presently within INDOT's apparent ROW, and the acquisition of ROW is to establish legally documented ownership. While 1.2 acre of ROW will be acquired from the former railroad corridor, it does not apply toward the public involvement requirements, CE level thresholds, or the need for Environmental Justice analysis. This alternative meets the purpose and need by removing the deteriorated existing bridges and providing a structurally sufficient roadway for motorists.

Detailed plan sheets for the work described above can be found in Appendix B, pages 11 to 23.

Trees along the roadway on the east side of the NB bridge and west of the SB bridge will be removed to create a clear construction area for a total of 0.6 acre. An underground waterline is located beneath Span 2 of the existing bridge and along the east side of the NB lanes. The waterline will be relocated, and coordination with local utilities will continue as the project advances.

No permanent lighting is required for this project, but temporary lighting will be utilized for possible night-time construction.

Maintenance of traffic (MOT) will require a phased approach and will be constructed under the lead Des number 2100188, in bundled contract R-43686 (Appendix B, pages 14 to 18). Please refer to this document's Maintenance of Traffic (MOT) section for more details.

Logical Termini/Independent Utility

The project area is localized to the immediate area surrounding the twin bridges, extending approximately 370 feet south and 360 feet north of the centerline of the structures for a total of 730 feet. Every effort will be made to avoid, minimize, and/or mitigate environmental impacts during this bridge removal project. This project demonstrates independent utility as it is a stand-alone project that is not dependent on other planned projects. However, this project is kinned with Des. No.'s 2100188, 2001776, 2100966, and 2100967, under Contract R-43686.

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Based on the above information, the preferred alternative will meet the purpose and need by removing the deteriorating existing bridges and delivering a structurally sufficient roadway for motorists.

OTHER ALTERNATIVES CONSIDERED:

Provide a header for each alternative. Describe all discarded alternatives, including the No Build Alternative. Explain why each discarded alternative was not selected. Make sure to state how each alternative meets or does not meet the Purpose and Need and why.

The “No Build” Alternative

The “No Build” alternative would leave the existing structure as is. While this alternative would avoid impacts to surrounding resources and would exclude construction costs, the bridge would continue to deteriorate and eventually fail. Because this alternative would not satisfy the purpose and need, it was dismissed from further consideration.

Bridge Removal and Replacement with an Underfill Three-Sided Small Structure

This alternative would remove and replace the existing bridges with a three-sided small structure (14-foot span, 13.5 foot rise) with no skew that would accommodate a future 10-foot pedestrian path (by others). This alternative was requested by the Town of Cayuga as a future pedestrian path was desired by the town officials. While this alternative would allow for a future pedestrian path if funded by Vermillion County or the Town of Cayuga, the structure would require future maintenance and additional funding. There are no current or future plans or funds allocated for a pedestrian path. The funding for the three-sided structure would be the responsibility of Vermillion County or the Town of Cayuga with local funds, which are not available. Due to the lack of funding for the three-sided structure and the lack of a plan for a trail, this was not a feasible alternative. This alternative would have the same impact on surrounding resources as the preferred alternative. This alternative would satisfy the purpose and need but was dismissed from further consideration due to lack of funding.

Bridge Rehabilitation

This alternative would repair the deficiencies of the existing bridges. While this alternative would meet the purpose and need, the former railroad SR 63 traversed has been removed, and therefore SR 63 does not need to cross any existing transportation corridors. This alternative would have the same impact on surrounding resources as the preferred alternative. Although this alternative would satisfy the purpose and need, it lacks necessity and would require future maintenance and additional funding. Therefore, it was dismissed from further consideration.

The No Build Alternative is not feasible, prudent, or practicable because (Mark all that apply):

- It would not correct existing capacity deficiencies;
- It would not correct existing safety hazards;
- It would not correct the existing roadway geometric deficiencies;
- It would not correct existing deteriorated conditions and maintenance problems; or
- It would result in serious impacts to the motoring public and general welfare of the economy.

Other (Describe):

X

ROADWAY CHARACTER:

If the proposed action includes multiple roadways, complete and duplicate for each roadway.

Name of Roadway	SR 63			
Functional Classification:	Principal Arterial (non-freeway)			
Current ADT:	7,007	VPD (2024)	Design Year ADT:	7307
634 VPH				VPD (2046)
Design Hour Volume (DHV):	(2046)	Truck Percentage (%)	26.92	
Designed Speed (mph):	60	Legal Speed (mph):	55	

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Existing		Proposed		
Number of Lanes:	2	2		
Type of Lanes:	Non-freeway		Non-freeway	
Pavement Width:	12	ft.	12	ft.
Shoulder Width:	4 (left), 10 (right)	ft.	4 (left), 10 (right)	ft.
Median Width:	60	ft.	60	ft.
Sidewalk Width:	N/A	ft.	N/A	ft.

Setting: Urban Suburban Rural
 Topography: Level Rolling Hilly

BRIDGES AND/OR SMALL STRUCTURE(S):

If the proposed action includes multiple structures, complete and duplicate for each bridge and/or small structure. Include both existing and proposed bridge(s) and/or small structure(s) in this section.

Structure/NBI Number(s): 063-83-02002 ANBL & ASBL/022727 NBL and 022729 SBL Sufficiency Rating: 88.6 out of 100 (NB) and 88.5 out of 100 (SB)-INDOT Bridge Inspection Report

 (Rating, Source of Information)

Existing		Proposed		
Bridge/Structure Type:	Continuous steel beam		N/A	
Number of Spans:	3		1	
Weight Restrictions:	N/A	ton	N/A	ton
Height Restrictions:	N/A		N/A	ft.
Curb to Curb Width:	39.9 (NB) 39.8 (SB)	ft.	N/A	ft.
Outside to Outside Width:	43.4 (NB) 43.3 (SB)	ft.	N/A	ft.
Shoulder Width:	4 (left) 10 (right)	ft.	4 (left) 10 (right)	ft.

Describe impacts and work involving bridge(s), culvert(s), pipe(s), and small structure(s). Provide details for small structure(s): structure number, type, size (length and dia.), location and impacts to water. Use a table if the number of small structures becomes large. If the table exceeds a complete page, put it in the appendix and summarize the information below with a citation to the table.

Presence

The existing twin bridges (Bridges 063-83-02002 ANBL & ASBL/022727 NBL and 022729 SBL) are three-span continuous steel beam bridges built in 1976. The bridges had an LMC overlay installed in 1987, the wearing surfaces were epoxy injected in 2019, and substantial beam and bearing repairs were completed in 2020. The existing bridges will meet the 50-year National Register minimum age requirement at the time of construction; however, they are common examples of their type and, therefore, are not eligible for the NRHP. The bridge lengths for both bridges are 175.5 feet with a maximum span length of 70 feet with no skew. Additionally, there have been several crucial finds on the bridges requiring emergency lane closures and repairs. The twin bridges cross a former railroad and will be removed. The project will completely backfill the existing crossing with roadway embankment material.

There are four additional inlets with 12 inch pipes beneath the bridge. The inlets will be removed, and the pipe will be plugged as part of the proposed project.

No other bridges or small structures are within the project area.

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MAINTENANCE OF TRAFFIC (MOT) DURING CONSTRUCTION:

	Yes	No
Is a temporary bridge proposed?		X
Is a temporary roadway proposed?	X	
Will the project involve the use of a detour or require a ramp closure? (describe below)		X
Provisions will be made for access by local traffic and so posted.		X
Provisions will be made for through-traffic dependent businesses.		X
Provisions will be made to accommodate any local special events or festivals.		X
Will the proposed MOT substantially change the environmental consequences of the action?		X
Is there substantial controversy associated with the proposed method for MOT?		X
Will the project require a sidewalk, curb ramp, and/or bicycle lane closure? (describe below)		X
Provisions will be made for access by pedestrians and/or bicyclist and so posted (describe below).		X

Discuss closures, detours, and/or facilities (if any) that will be provided for maintenance of traffic. Any known impacts from these temporary measures should be quantified to the extent possible, particularly with respect to properties such as Section 4(f) resources and wetlands. Discuss any pedestrian/bicycle closures. Any local concerns about access and traffic flow should be detailed as well.

Maintenance of traffic (MOT) will require a phased approach and will be included under the MOT plan for Lead Des number 2100188. The first phase will consist of reducing SR 63 to a single lane in each direction. The remaining NB lane will crossover to the SB lane, which will carry one 11-foot lane in each direction, separated by flexible tubular markers with lane separators. The existing NB bridge will be removed, and the NB roadway and embankment will be constructed. For the second phase, both lanes of traffic on the SB roadway will be crossed over to the NB roadway, which will carry one 11-foot lane in each direction, separated by flexible tubular markers with lane separators. The existing SB bridge will be removed, and the SB roadway and embankment will be constructed. Upon completion of this phase, the temporary crossovers will be removed, and the median will be restored to preconstruction conditions. Please refer to Appendix B, pages 14 to 18, for details on the MOT.

The closures/lane restrictions will pose a temporary inconvenience to traveling motorists (including school buses and emergency services); however, no significant delays are anticipated, and all inconveniences and delays will cease upon project completion.

ESTIMATED PROJECT COST AND SCHEDULE:

Engineering: \$ 420,000 (2023-2026) Right-of-Way: \$ 42,000 (2023-2026) Construction: \$ 2,315,000 (2026)

Anticipated Start Date of Construction: Spring 2026

*If necessary, the INDOT Project Manager will ensure an administrative modification to the STIP will occur prior to the Ready-for-Contract (RFC) date.

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RIGHT OF WAY:

Land Use Impacts	Amount (acres)	
	Permanent	Temporary
Residential	0.000	0.000
Commercial	0.000	0.000
Agricultural	0.000	0.000
Forest	0.600	0.000
Wetlands	0.000	0.000
Other: Former railroad corridor	0.600	0.000
Other:	0.000	0.000
TOTAL	1.200	0.000

Describe both Permanent and Temporary right-of-way and describe their current use. Typical and Maximum right-of-way widths (existing and proposed) should also be discussed. Any advance acquisition, reacquisition or easements, either known or suspected, and their impacts on the environmental analysis should be discussed.

The existing ROW varies from 202 to 210 feet wide on either side of the median centerline between the bridges. The ROW consists primarily of grass and forested areas.

Right-of-way (ROW) required

The project requires approximately 1.2 acres of permanent ROW, 210 feet wide on the west side of SR 63 SB and 210 feet wide on the east side of SR 63 NB. The project does not require temporary ROW. All of the ROW, which will be acquired from the former railroad corridor, is located within the existing INDOT ROW corridor of SR 63 (see Appendix B, page 19). INDOT District Environmental (DE) determined that the project is eligible for the Public Involvement (PI) Manual's exemption for "Acquiring ROW presently within INDOT apparent ROW to establish legal documented ownership." As the land to be acquired is within the apparent existing ROW, it would not need to be included in the ROW quantities toward the CE level threshold (see Appendix I, pages 73 to 76).

If the scope of work or permanent or temporary ROW amounts change, the INDOT Environmental Services Division (ESD) and the INDOT District Environmental Section will be contacted immediately.

Part III – Identification and Evaluation of Impacts of the Proposed Action

SECTION A - EARLY COORDINATION:

List the date(s) coordination was sent and all resource agencies that were contacted as a part of the development of this Environmental Study. Also, include the date of their response or indicate that no response was received.

Early coordination letters were sent on August 9, 2023 (Appendix C, pages 1 to 3).

Agency	Date Sent	Date Response Received	Appendix
Federal Highway Administration	August 9, 2023	No Response Received	N/A
US Fish and Wildlife Service	August 9, 2023	No Response Received	N/A
Indiana Geological and Water Survey (IGWS)	August 9, 2023	August 9, 2023	C-4 to C-5
Indiana Department of Natural Resources Division of Fish and Wildlife (IDNR-DFW)	August 9, 2023	September 8, 2023	C-6 to C-8
Vermillion County Surveyor	August 9, 2023	August 10, 2023	C-9 to C-10

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Town of Cayuga	August 9, 2023	August 9, 2023	C-11 to C-12
Vermillion County Commissioner	August 9, 2023	August 9, 2023	C-13 to C-16
Natural Resources Conservation Service (NRCS)	August 9, 2023	January 29, 2024	C-17 to C-18
Indiana Department of Environmental Management – Groundwater Section	August 9, 2023	January 19, 2023	C-19 to C-20
INDOT Utilities and Railroad	April 5, 2023	July 26, 2023 and August 21, 2023	C-21 to C-27
US Department of Housing and Urban Development	August 9, 2023	No Response Received	N/A
National Park Service	August 9, 2023	No Response Received	N/A
INDOT Environmental Section Manager, Crawfordsville District	August 9, 2023	No Response Received	N/A
INDOT Project Manager, Crawfordsville District	August 9, 2023	No Response Received	N/A
Vermillion County Council	August 9, 2023	No Response Received	N/A
Cayuga Fire Department	August 9, 2023	No Response Received	N/A
Vermillion County Sheriff's Office	August 9, 2023	No Response Received	N/A
Vermillion County Emergency Management	August 9, 2023	No Response Received	N/A
Vermillion County Zoning and Floodplain Administrator	August 9, 2023	No Response Received	N/A
Vermillion County Economic Development Council	August 9, 2023	No Response Received	N/A
Cayuga Water Department	August 9, 2023	No Response Received	N/A
City of Georgetown, Water Utilities	August 9, 2023	No Response Received	N/A
Wabash River Heritage Corridor Commission (WHRCC)	August 9, 2023	No Response Received	N/A
Vermillion Trails Alliance	August 9, 2023	No Response Received	N/A
IPaC (USFWS)	October 7, 2024	October 7, 2024 and October 23, 2024	C-28 to C-61

All applicable recommendations are included in the *Environmental Commitments* section of this CE document.

SECTION B – ECOLOGICAL RESOURCES:

Streams, Rivers, Watercourses & Other Jurisdictional Features
 Federal Wild and Scenic Rivers
 State Natural, Scenic or Recreational Rivers
 Nationwide Rivers Inventory (NRI) listed
 Outstanding Rivers List for Indiana
 Navigable Waterways

<u>Presence</u>	<u>Impacts</u>	
	<u>Yes</u>	<u>No</u>
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Total stream(s) in project area: N/A Linear feet Total impacted stream(s): N/A Linear feet

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Stream Name	Classification	Total Size in Project Area (linear feet)	Impacted linear feet	Comments (i.e. location, flow direction, likely Water of the US, appendix reference)
N/A	N/A	N/A	N/A	N/A

Describe all streams, rivers, watercourses, and other jurisdictional features adjacent to or within the project area. Include whether or not impacts (both permanent and temporary) will occur to the features identified. Include if the streams or rivers are listed on any federal or state lists for Indiana. Include if features are likely subject to federal or state jurisdiction. Discuss measures to avoid, minimize, and mitigate if impacts will occur.

No presence, no impacts

Based on the desktop review, the aerial map of the project area, and the Red Flag Investigation (RFI) report (Appendix E, pages 1 to 10), there are two (2) streams, rivers, watercourses, or other jurisdictional features within the 0.5-mile search radius. There are no streams, rivers, watercourses, or other jurisdictional features within or adjacent to the project area, which was confirmed by the site visit on May 25, 2023, by Egis. Therefore, no impacts are expected.

INDOT Ecology, Waterway Permitting, and Stormwater Office (EWPSO) responded on October 31, 2024, stating that they agreed that there are likely no jurisdictional features within the project limits. Since no impacts below the ordinary high-water mark, Q100 elevation, or wetlands are anticipated, no waters report is required for the project (Appendix F, page 2).

Open Water Feature(s)

Reservoirs
Lakes
Farm Ponds
Retention/Detention Basin
Storm Water Management Facilities
Other: _____

Presence

X

Impacts

Yes
X

Describe all open water feature(s) identified adjacent or within the project area. Include whether or not impacts (both permanent and temporary) will occur to the features identified. Include if features are likely subject to federal or state jurisdiction. Discuss measures to avoid, minimize, and mitigate if impacts will occur.

No presence, no impact

Based on the desktop review, the aerial map of the project area, and the RFI report (Appendix E, pages 1 to 10), there are four (4) open water features within the 0.5-mile search radius. There are no open water features within or adjacent to the project area, which was confirmed by the site visit on May 25, 2023, by Egis. Therefore, no impacts are expected.

INDOT EWPSO responded on October 31, 2024, stating that they agreed that there are likely no jurisdictional features within the project limits. Since no impacts below the ordinary high-water mark, Q100 elevation, or wetlands are anticipated, no waters report is required for the project (Appendix F, page 2).

Wetlands

Presence

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Impacts

Yes

No

Total wetland area: N/A Acre(s) Total wetland area impacted: N/A Acre(s)

(If a determination has not been made for non-isolated/isolated wetlands, fill in the total wetland area impacted above.)

Wetland No.	Classification	Total Size (Acres)	Impacted Acres	Comments (i.e. location, likely Water of the US, appendix reference)
N/A	N/A	N/A	N/A	N/A

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Wetlands (Mark all that apply)

Wetland Determination
Wetland Delineation
USACE Isolated Waters Determination

Documentation

ESD Approval Dates

Improvements that will not result in any wetland impacts are not practicable because such avoidance would result in (Mark all that apply and explain):

Substantial adverse impacts to adjacent homes, business or other improved properties;
Substantially increased project costs;
Unique engineering, traffic, maintenance, or safety problems;
Substantial adverse social, economic, or environmental impacts, or
The project not meeting the identified needs.

Describe all wetlands identified adjacent or within the project area. Include whether or not impacts (both permanent and temporary) will occur to the features identified. Include if features are likely subject to federal or state jurisdiction. Discuss measures to avoid, minimize, and mitigate if impacts will occur.

No presence, no impacts

Based on the desktop review, the aerial map of the project area, and the RFI report (Appendix E, pages 1 to 10), there are eleven (11) wetlands within the 0.5-mile search radius. There is one (1) wetland within the project area. That number was updated to zero (0) by the site visit on May 25, 2023, by EGIS. Therefore, no impacts are expected.

INDOT EWPSO responded on October 31, 2024, stating they agreed that there are likely not any jurisdictional features within the project limits and since no impacts below the ordinary high-water mark, Q100 elevation, or wetlands are anticipated, no waters report is required for the project (Appendix F, page 2).

Terrestrial Habitat

	<u>Presence</u>	<u>Impacts</u>	
	<u>Yes</u>	<u>NO</u>	
	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Total terrestrial habitat in project area: 1.4 Acre(s) Total tree clearing: 0.6 Acre(s)

Describe types of terrestrial habitat (i.e. forested, grassland, farmland, lawn, etc) adjacent or within the project area. Include whether or not impacts will occur to habitat identified. Include total terrestrial habitat impacted and total tree clearing that will occur. Discuss measure to avoid, minimize, and mitigate if impacts will occur.

Presence, with impacts

Based on a desktop review, a site visit on May 25, 2023, by Egis, and the aerial map of the project area (Appendix A, page 3), terrestrial habitats are present within and adjacent to the project area. Immediately beyond the roadway pavement, areas of maintained grass are present. Wooded terrain is present in the northwest quadrant and centrally in the project area. Dominant herbaceous species in the area consisted of Virginia creeper (*Parthenocissus quinquefolia*) and Reed managrass (*Glyceria maxima*). Dominant shrub species within the project area include Amur honeysuckle (*Lonicera maackii*). Dominant tree species within the project area included American elm (*Ulmus americana*) and White mulberry (*Morus alba*). Trees along the roadway on the east side of the NB bridge and west of the SB bridge will be removed to create a clear construction area for a total of 0.6 acre. The project will impact approximately 1.4 acres of terrestrial habitat. Any non-wooded construction impacts, such as grasses, will be re-seeded with an INDOT-approved seed mixture. Mitigation or additional planting beyond seeding or stabilizing disturbed areas are not anticipated; however, should they occur, they will be restored following the completion of construction activities.

Early Coordination

The IDNR-DFW responded on September 8, 2023, with recommendations to consider wildlife passage in design, the use of pollutant trapping technology, all work be restricted to the period between November 1 and March 1 to avoid the summer roosting period for most bats and performing a nest survey between May 7 to September 7 for nesting birds (Appendix C, pages 6 to 8).

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All applicable recommendations are included in the *Environmental Commitments* section of this CE document.

Protected Species

Federally Listed Bats

Information for Planning and Consultation (IPaC) determination key completed
 Section 7 informal consultation completed (IPaC cannot be completed)
 Section 7 formal consultation Biological Assessment (BA) required

Yes
<input checked="" type="checkbox"/>
<input checked="" type="checkbox"/>
<input type="checkbox"/>

No
<input type="checkbox"/>
<input checked="" type="checkbox"/>
<input type="checkbox"/>

Determination Received for Listed Bats from USFWS:

NE

NLAA

LAA

Other Species not included in IPaC

Additional federal species found in project area (based on IPaC species list)
 State species (not bird) found in project area (based upon consultation with IDNR)

Yes
<input type="checkbox"/>
<input type="checkbox"/>

No
<input checked="" type="checkbox"/>
<input checked="" type="checkbox"/>

Migratory Birds

Known usage or presence of birds (i.e. nests)
 State bird species based upon coordination with IDNR

Yes
<input checked="" type="checkbox"/>
<input type="checkbox"/>

No
<input type="checkbox"/>
<input checked="" type="checkbox"/>

Discuss IDNR coordination and species identified. Describe USFWS Section 7 consultation and determination received for Indiana bat and northern long-eared bat impacts. Discuss if other federally listed species were identified. If so, include consultation that has occurred and the determination that was received. Discuss if migratory birds have been observed and any impacts.

Based on a desktop review and the RFI report (Appendix E, pages 1 to 10), completed by Egis on December 7, 2023, the IDNR Vermillion County Endangered, Threatened and Rare (ETR) Species List has been checked. According to the IDNR-DFW early coordination response letter dated September 8, 2023 (Appendix C, pages 6 to 8), the Natural Heritage Program's Database has been checked, and no plant or animal species listed as state or federally threatened, endangered, or rare have been reported to occur in the project vicinity. An INDOT 0.5-mile bat review occurred June 21, 2023, and did not indicate the presence of endangered bat species in or within 0.5-mile of the project area. No critical habitats are present within the project area.

Bats, Limited Formal Programmatic Consultation (i.e. IPaC) – Likely to Adversely Affect

Project information was submitted through the USFWS's Information for Planning and Consultation (IPaC) portal, and an official species list was generated (Appendix C, pages 28 to 41). The project is within range of the federally endangered Indiana bat (*Myotis sodalis*) and the federally threatened Northern Long-eared bat (NLEB) (*Myotis septentrionalis*). At this time, there are no additional avoidance or minimization measures needed for the NLEB. Other species were generated in the IPaC species list along with the Indiana and NLEB. Refer to the paragraph below.

The project qualified and completed *Limited Formal Programmatic Consultation* for the Indiana bat and northern long-eared bat (NLEB) due to tree clearing 100-300 feet from the existing roadway. A bridge inspection occurred on May 25, 2023, and no bats or signs of bats were found using the bridges (Appendix C, pages 62 and 63). An effect determination key was completed on October 7, 2024, and based on the responses provided, the project was found to "May Affect – Likely to Adversely Affect" the Indiana bat and/or the NLEB (Appendix C, pages 42 to 56). Proposed impacts cannot be avoided as no alternatives found to meet the purpose and need of the project could be constructed within the footprint of the existing intersection. Impacts to the wooded areas have been minimized to the greatest extent possible.

INDOT reviewed and verified the effect finding on October 7, 2024, and requested USFWS's review of the finding on October 7, 2024. On October 23, 2024, USFWS issued a concurrence letter with the "Likely to Adversely Affect" finding (Appendix C, pages 57 to 61). USFWS responded on October 23, 2024, confirming that the proposed project's effects are consistent with those analyzed in the Programmatic Biological Opinion (PBO) (Appendix C, pages 57 to 61). The response further stated, "The Service has determined that projects consistent with the conservation measures and scope of the program analyzed in the PBO are not likely to jeopardize the continued existence of the Indiana bat or the NLEB." The project's impacts were determined to have a likely adverse effect on the Indiana bat and NLEB species due to impacts to habitat. Per the "Likely to Adversely Affect" consistency letter and USFWS response, compensatory mitigation is applicable. The determination key result included the commitments to implement Avoidance and Minimization Measures (AMMS). The AMMs include General AMM 1, Lighting AMM 1,

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and Tree Removal AMM 1 and 3 for this project. AMMs and/or commitments are included as firm commitments in the *Environmental Commitments* section of this document.

Additionally, a "Reinitiation Notice" is required if: more than 0.04 acre of suitable habitat for the Indiana bat and/or NLEB beyond 100 feet from the edge of pavement is to be cleared; more than five (5) Indiana bats and/or five (5) NLEBs are taken resulting from the bridge activity, new information about listed species is encountered; the project is modified in a manner that causes an effect to the listed species; or a new species or critical habitat is listed that the project may affect. These requirements, and the Avoidance and Minimization Measures (AMMs) from the Project Submittal Form, are included as firm commitments for this project.

INDOT shall satisfy the compensatory mitigation requirements of the formal consultation with USFWS through one of the conservation options outlined on page 41 of the May 20, 2016, Programmatic Biological Opinion for Transportation Projects in the Range of the Indiana bat and NLEB. The amount to be paid to the Range-wide In-lieu Fee Program, to be administered by The Conservation Fund, shall be \$881.16. This amount was determined by the Habitat Block Method. The area of suitable habitat to be cleared, multiplied by the mitigation ratio for inactive season tree clearing for Vermillion County, and the compensatory price per acre; (0.04) acre X (1.75) x \$12,588.

The official species list generated from IPaC indicated four other species present within the project area. The tricolored bat (*Perimyotis subflavus*, proposed endangered), whooping crane (*Grus americana*, experimental population, non-essential), Salamander Mussel (*Simpsoniaris ambigua*, proposed endangered), and Monarch butterfly (*Danaus plexippus*, candidate) are found within the project area. These species are candidate or experimental and do not require further coordination with USFWS. The IPaC determination does not cover the Tricolored Bat as a "proposed" species and is not yet afforded protection under Section 7.

A bridge assessment of Structures 063-83-02002 ANBL & ASBL/022727 NBL and 022729 SBL were completed by Egis on May 25, 2023, and no evidence of bats was found (Appendix C, pages 62 to 63). USFWS Bridge/Structure Assessments are only valid for two years. If construction begins after May 25, 2025, an inspection of the underfill tunnel by a qualified individual must be performed. Inspection of the structures should check for the presence of bats/bat indicators and/or the presence of birds. The results of the inspection must indicate no signs of bats or birds. If signs of bats or birds are documented during this inspection, the INDOT District Environmental Manager must be contacted immediately. This firm commitment is included in the *Environmental Commitments* section of this document.

Migratory Birds

Structures 063-83-02002 ANBL & ASBL/022727 NBL and 022729 SBL have shown evidence of use (i.e. nests) by a bird species protected under the Migratory Bird Treaty Act (MBTA) during the May 25, 2023, inspection. Avoidance and minimization measures must be implemented prior to the start of and during the nesting season. Nests without eggs or young should be removed prior to construction during the non-nesting season (September 8 through April 30) and during the nesting season if no eggs or young are present. Nests with eggs or young cannot be removed or disturbed during the nesting season (May 1 through September 7). Nests with eggs or young should be screened or buffered from active construction. Details of the required procedures are outlined in the "Potential Migratory Bird on Structure" Unique Special Provision (USP). This firm commitment is included in the *Environmental Commitments* section of this document.

This precludes the need for further consultation on this project as required under Section 7 of the Endangered Species Act, as amended. If new information on endangered species at the site becomes available, or if project plans are changed, USFWS will be contacted for consultation.

Geological and Mineral Resources

Project located within the Indiana Karst Region

Karst features identified within or adjacent to the project area

Oil/gas or exploration/abandoned wells identified in the project area

Yes

No

X
X
X

Date Karst Evaluation reviewed by INDOT EWPO (if applicable): N/A

Discuss if project is located in the Indiana Karst Region and if any karst features have been identified in the project area (from RFI). Discuss response received from IGWS coordination. Discuss if any mines, oil/gas, or exploration/abandoned wells were identified and if impacts will occur. Include discussion of karst study/report was completed and results. (Karst investigation must comply with the current Protection of Karst Features during Planning and Construction guidance and coordinated and reviewed by INDOT EWPO)

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Outside karst area

Based on a desktop review and the Indiana Karst Region map, the project is located outside the designated Indiana Karst Region as outlined in the most current Protection of Karst Features during Project Development and Construction. According to the topo map of the project area (Appendix B, page 2) and the RFI report (Appendix E, pages 1 to 10), there are no karst features identified within or adjacent to the project area.

In the early coordination response dated August 9, 2023, the IGWS did not indicate karst features exist in the project area (Appendix C, page 4). IGWS stated the project is within an area of high liquefaction potential, within a floodway, has a high potential for bedrock, sand, and gravel resources. They also indicated active and/or abandoned mineral resource extraction sites (i.e., petroleum exploration wells, underground coal mines, and surface coal mines) have not been documented in the area. The features will not be affected because no features were located during the site visit on May 25, 2023. Response from IGWS has been communicated to the designer on August 9, 2023. No impacts are expected.

SECTION C – OTHER RESOURCES

Drinking Water Resources

Wellhead Protection Area(s)
Source Water Protection Area(s)
Water Well(s)
Urbanized Area Boundary
Public Water System(s)

<u>Presence</u>	<u>Impacts</u>
<input checked="" type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>
<input checked="" type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>

Is the project located in the St. Joseph Sole Source Aquifer (SSA):

If Yes, is the FHWA/EPA SSA MOU Applicable?
If Yes, is a Groundwater Assessment Required?

<u>Yes</u>	<u>No</u>
<input type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>

Check the appropriate boxes and discuss each topic below. Provide details about impacts and summarize resource-specific coordination responses and any mitigation commitments. Reference responses in the Appendix.

Outside of Sole Source Aquifer (SSA)

The project is located in Vermillion County, which is not located within the area of the St. Joseph Sole Source Aquifer, the only legally designated sole source aquifer in the state of Indiana. Therefore, the FHWA/EPA/INDOT Sole Source Aquifer Memorandum of Understanding (MOU) is not applicable to this project, a detailed groundwater assessment is not needed, and no impacts are expected.

Located in a Wellhead Protection Area

The IDEM Wellhead Proximity Determinator website (<http://www.in.gov/idem/cleanwater/pages/wellhead/>) was accessed on July 18, 2023, by Egis. This project is located within a Wellhead Protection Area. In an early coordination letter dated January 19, 2024, IDEM stated the project is located within a Wellhead Protection Area (Appendix C, pages 19 and 20). Coordination with the Cayuga Water Department and the City of Georgetown occurred on August 9, 2023. No responses were received in the 30-day time frame. The Indiana Department of Natural Resources Water Well Record Database website (<https://www.in.gov/dnr/water/3595.htm>) was accessed on July 15, 2024, by Egis. There are two (2) unconsolidated wells and four (4) significant withdraw wells within a 0.5 mile radius of the project area. The nearest significant withdraw well (number 04620) is located 0.16 mi northwest of the project area. The features will not be affected because they are outside the construction area boundaries. Therefore, no impacts are expected. Should it be determined during the ROW phase that these wells will be affected, a cost to cure will likely be included in the appraisal to restore the wells. Avoidance alternatives are not practicable due to the location of the bridge and the scope of work that is necessary to remove the bridge.

No wells present, no impacts

The Indiana Department of Natural Resources Water Well Record Database website (<https://www.in.gov/dnr/water/3595.htm>) was accessed on July 18, 2023, by Egis. No wells are located near this project. Therefore, no impacts are expected.

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Not in an Urban Area Boundary Location

Based on a desktop review of IDEM's Municipal Separate Storm Sewer System (MS4) Boundaries Map for Indiana website (<https://www.in.gov/idem/cleanwater/ms4s-boundaries-map-for-indiana/>) by Egis on November 20, 2023, this project is not located within an Urban Area Boundary (UAB). No impacts are expected.

In a Public Water System Location

Based on a desktop review, a site visit on May 25, 2023, by Egis, and the aerial map of the project area (Appendix B, page 2), this project is located where there is a public water system. The public water system will be affected because an underground waterline is located beneath Span 2 of both the NB and SB bridges and along the east side of the NB lanes. Coordination with INDOT utilities and project management has been conducted, and coordination with the Cayuga Water Department is ongoing and will continue as the project advances. An early coordination letter was sent on August 9, 2023, to the Cayuga Water Department and the City of Georgetown Water Utilities (Appendix C, pages 1 to 3). No response was received in the 30-day time frame. Temporary disruption of service is possible if the water line requires relocation. Coordination with local utilities will be ongoing as the project advances.

Floodplains

Project located within a regulated floodplain
Longitudinal encroachment
Transverse encroachment
Homes located in floodplain within 1000' up/downstream from project

<u>Presence</u>	<u>Impacts</u>
Yes	No
<input type="checkbox"/>	<input type="checkbox"/>

If applicable, indicate the Floodplain Level?

Level 1 Level 2 Level 3 Level 4 Level 5

Use the IDNR Floodway Information Portal to help determine potential impacts. Include floodplain map in appendix. Discuss impacts according to the classification system. If encroachment on a flood plain will occur, coordinate with the Local Flood Plain Administrator during design to insure consistency with the local flood plain planning.

In a Floodplain

The Indiana Department of Natural Resources Indiana Floodway Information Portal website (<https://indnr.maps.arcgis.com/apps/webappviewer/index.html?id=05026dabc2e8461983e196d56a213c1e>) was accessed on September 23, 2023, by Egis. This project is not located in a regulatory floodplain as determined from IDNR floodplain map (Appendix F, page 1). The project is located in the FEMA-mapped Zone A floodplain, but not in the IDNR's floodway or floodplain fringe areas. The IDNR response letter from August 8, 2023, states that formal approval by IDNR's regulatory programs administered by the Division of Water is not required for this project. Therefore, it does not fall within the guidelines for the implementation of 23 CFR 650, 23 CFR 771, and 44 CFR. No impacts are expected.

Farmland

Agricultural Lands
Prime Farmland (per NRCS)

<u>Presence</u>	<u>Impacts</u>
Yes	No
<input checked="" type="checkbox"/>	<input type="checkbox"/>
<input checked="" type="checkbox"/>	<input type="checkbox"/>

Total Points (from Section VII of CPA-106/AD-1006*)

96

*If 160 or greater, see CE Manual for guidance.

Discuss existing farmland resources in the project area, impacts that will occur to farmland, and mitigation and minimization measures considered.

Presence, score under 160

Based on a desktop review, a site visit on May 25, 2023, by Egis, and the aerial map of the project area (Appendix B, page 3), there is farmland as defined by the Farmland Protection Policy Act (FPPA) adjacent to the project area. The project will not convert any farmland as the project limits are outside the farmland boundaries. An early coordination letter was sent on August 9, 2023, to the

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NRCS. Coordination with NRCS resulted in a score of 96 on the NRCS-AD 1006 Form (Appendix C, pages 17 to 18). NRCS's threshold score for significant impacts on farmland that result in the consideration of alternatives is 160. Since the project score is less than the threshold, no significant loss of prime, unique, statewide, or locally important farmland will result from this project. No alternatives other than those previously discussed in this document will be investigated without reevaluating impacts to prime farmland.

SECTION D – CULTURAL RESOURCES

Minor Projects PA	Category(ies) and Type(s)	INDOT Approval Date(s)	N/A
	B3, B12	6/14/2024 and 11/18/2024	

Full 106 Effect Finding

No Historic Properties Affected No Adverse Effect Adverse Effect

Eligible and/or Listed Resources Present

NRHP Building/Site/District(s) Archaeology NRHP Bridge(s)

Documentation Prepared (mark all that apply)

APE, Eligibility and Effect Determination
800.11 Documentation
Historic Properties Report or Short Report
Archaeological Records Check and Assessment
Archaeological Phase Ia Survey Report
Archaeological Phase Ic Survey Report
Other:

X	ESD Approval Date(s)	SHPO Approval Date(s)
	6/14/2024 and 11/18/2024	
		N/A

Memorandum of Agreement (MOA)

MOA Signature Dates (List all signatories)

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If the project falls under the MPPA, describe the category(ies) that the project falls under and any approval dates. If the project requires full Section 106, use the headings provided. The completion of the Section 106 process requires that a Legal Notice be published in local newspapers. Please indicate the publication date, name of the paper(s) and the comment period deadline. Include any further Section 106 work which must be completed at a later date, such as mitigation from a MOA or avoidance commitments.

Minor Projects PA Category B Projects

On June 14, 2024 and November 18, 2024, the INDOT Cultural Resources Office (CRO) determined that this project falls within the guidelines of Category B, Types B-3 and B-12 under the Minor Projects Programmatic Agreement (MPPA) (Appendix D, page 1 to 13).

Type B-3 covers construction of added travel, turning, or auxiliary lanes and shoulder widening under the following conditions. Type B-12 covers replacement, widening, or raising the elevation of the superstructure on existing bridges and bridge replacement projects (when both the superstructure and substructure are removed).

Work will occur in undisturbed soils, and an archaeological investigation conducted by the applicant and reviewed by INDOT CRO determined that no National Registered- listed or potentially National Register-eligible archaeological resources are present within the project area.

A Phase I Archaeological Reconnaissance Survey was conducted by Cultural Resources Analysts (CRA) on September 18, 2023 and October 4, 2024 (Appendix D, pages 14 to 18). No archaeological resources were present within the project area. It was recommended that the project be allowed to proceed as planned. No further consultation is required. This completes the Section 106

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process and the responsibilities of the FHWA under Section 106 have been fulfilled.

SECTION E – SECTION 4(f) RESOURCES/ SECTION 6(f) RESOURCES

Parks and Other Recreational Land

Publicly owned park
Publicly owned recreation area
Other (school, state/national forest, bikeway, etc.)

<u>Presence</u>	<u>Use</u>	
	<u>Yes</u>	<u>No</u>
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Wildlife and Waterfowl Refuges

National Wildlife Refuge
National Natural Landmark
State Wildlife Area
State Nature Preserve

<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Historic Properties

Site eligible and/or listed on the NRHP

<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
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Evaluations Prepared

Programmatic Section 4(f)
“De minimis” Impact
Individual Section 4(f)
Any exception included in 23 CFR 774.13

<input type="checkbox"/>
<input type="checkbox"/>
<input type="checkbox"/>
<input type="checkbox"/>

Discuss Programmatic Section 4(f) and “de minimis” Section 4(f) impacts in the discussion below. Individual Section 4(f) documentation must be included in the appendix and summarized below. Discuss proposed alternatives that satisfy the requirements of Section 4(f). FHWA has identified various exceptions to the requirement for Section 4(f) approval. Refer to 23 CFR § 774.13 - Exceptions.

No presence, no impact

Section 4(f) of the US Department of Transportation Act of 1966 prohibits the use of certain public and historic lands for federally funded transportation facilities unless there is no feasible and prudent alternative. The law applies to significant publicly owned parks, recreation areas, wildlife/waterfowl refuges, and NRHP-eligible or listed historic properties regardless of ownership. Lands subject to this law are considered Section 4(f) resources.

Based on a desktop review, the aerial map of the project area (Appendix B, page 3), and the RFI report (Appendix E, pages 1 to 10), there are no potential 4(f) resources located within the 0.5-mile search radius. According to additional research and the site visit on May 25, 2023, by Egis, there are no 4(f) resources within or adjacent to the project area. Therefore, no use is expected.

Section 6(f) Involvement

Section 6(f) Property

Presence

<u>Use</u>	<u>Yes</u>	<u>No</u>
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Discuss Section 6(f) resources present or not present. Discuss if any conversion would occur as a result of this project. If conversion will occur, discuss the conversion approval.

No presence, no impact

The US Land and Water Conservation Fund Act of 1965 established the Land and Water Conservation Fund (LWCF), which was created to preserve, develop, and assure accessibility to outdoor recreation resources. Section 6(f) of this Act prohibits conversion of lands purchased with LWCF monies to non-recreation use.

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A review of 6(f) properties on the INDOT ESD website revealed a total of four (4) properties in Vermillion County (Appendix I, pages 1 and 2). None of these properties are located within or adjacent to the project area. Therefore, there will be no impacts to 6(f) resources.

SECTION F – Air Quality

STIP/TIP and Conformity Status of the Project

Is the project in the most current STIP/TIP?

Yes

X

Is the project located in an MPO Area?

No

X
X

Is the project in an air quality non-attainment or maintenance area?

If Yes, then:

Is the project in the most current MPO TIP?

Is the project exempt from conformity?

If No, then:

Is the project in the Transportation Plan (TP)?

Is a hot spot analysis required (CO/PM)?

Location in STIP:

Statewide Transportation Improvement Program (STIP) Fiscal Year (FY) 2024-2028- Appendix H

Name of MPO (if applicable):

N/A

Location in TIP (if applicable):

Level of MSAT Analysis required?

Level 1a

Level 1b

Level 2

Level 3

Level 4

Level 5

Describe if the project is listed in the STIP and if it is in a TIP. Describe the attainment status of the county(ies) where the project is located. Indicate whether the project is exempt from a conformity determination. If the project is not exempt, include information about the TP and TIP. Describe if a hot spot analysis is required and the MSAT Level.

Project Bundled in Contract

The FY 2024-2028 Statewide Transportation Program (STIP) is listed based on the lead Des number in the contract. The lead Des number for this contract is Des. No. 2100188. The FY 2024-2028 STIP includes Des. Nos. 2100968/2100969 by reference with contract number R-43686 (Appendix H, page 1).

Attainment Status

This project is located in Vermillion County, which is currently in attainment for all criteria pollutants according to the Indiana Department of Environmental Management's Nonattainment Status of Counties website

(https://www.in.gov/idem/sips/files/nonattainment_areas_map.pdf). Therefore, the conformity procedures of 40 CFR Part 93 do not apply.

Mobile Source Air Toxics (MSAT) Level 1a Analysis

This project is of a type qualifying as a categorical exclusion (Group 1) under 23 CFR 771.117(c) or exempt under the Clean Air Act conformity rule under 40 CFR 93.126, and as such, a Mobile Source Air Toxics analysis is not required.

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SECTION G - NOISE

Noise

Yes No

Is a noise analysis required in accordance with FHWA regulations and INDOT's traffic noise policy?

Date Noise Analysis was approved/technically sufficient by INDOT ESD: N/A

Describe if the project is a Type I or Type III project. If it is a Type I project, describe the studies completed to date and if noise impacts were identified. If noise impacts were identified, describe if abatement is feasible and reasonable and include a statement of likelihood.

Type III Project

This project is a Type III project. In accordance with 23 CFR 772 and the current Indiana Department of Transportation Traffic Noise Analysis Procedure, this action does not require a formal noise analysis.

SECTION H – COMMUNITY IMPACTS

Regional, Community & Neighborhood Factors

Will the proposed action comply with the local/regional development patterns for the area?

Yes	No
X	
	X
	X
	X
X	
	X

Will the proposed action result in substantial impacts to community cohesion?

Will the proposed action result in substantial impacts to local tax base or property values?

Will construction activities impact community events (festivals, fairs, etc.)?

Does the community have an approved transition plan?

If No, are steps being made to advance the community's transition plan?

Does the project comply with the transition plan? (explain in the discussion below)

Discuss how the project complies with the area's local/regional development patterns; whether the project will impact community cohesion; and impact community events. Discuss how the project conforms with the ADA Transition Plan.

There may be temporary inconveniences associated with construction, such as increased travel times, possible construction noise and fugitive dust. There will be no substantial impacts on community cohesion or property value due to the project. Furthermore, no permanent or temporary economic effects are expected to result from the proposed project. Acquisition of the additional ROW will not appreciably affect the property tax base of Vermillion County. A review of <https://www.fairsandfestivals.net/>, an online resource for local fairs and festivals, there are no scheduled festivals or other public events that will be impacted as a result of the project.

As required by the Americans with Disabilities Act (ADA), Vermillion County has developed an <https://www.vermilliongov.us/wp-content/uploads/2021/06/Signed-VC-ADA-Ordinance-1.pdf>. As proposed, SR 63 is a rural road that does not include any ADA components. There are no existing sidewalks or other pedestrian facilities adjacent to the project area or within the project limits. There are no sidewalks or other pedestrian facilities included in the design; however, the project complies with local development patterns for the area.

Indirect impacts are effects that are caused by the action and are later in time or farther removed in distance by are still reasonably foreseeable. Indirect effects may include growth inducing effects and other effects related to induced changes in the patters of land use, population density, or growth rate. Cumulative impacts affect the environmental which result from the incremental impact of the action when added to other past, present, and reasonably foreseeable future actions regardless of what agency or person undertakes such actions.

The bridge removal project will not increase the capacity of the roadway or lead to any change in traffic patterns. The project will address structural deficiencies and provide hydraulically sound roadway that maintain traffic flow on SR 63 and allows for future development and planning of a pedestrian trail under SR 63.

It is the responsibility of the project sponsor to notify school corporations and emergency services at least two weeks prior to any construction that will block or limit access.

Indiana Department of Transportation

County Vermillion

Route SR 63

Des. No. 2100968 and 2100969

Public Facilities and Services

Discuss what public facilities and services are present in the project area and impacts (such as MOT) that will occur to them. Include how the impacts have been minimized and what coordination has occurred. Some examples of public facilities and services include health facilities, educational facilities, public and private utilities, emergency services, religious institutions, airports, transportation or public pedestrian and bicycle facilities.

Presence, no impact

Based on a desktop review, the aerial map of the project area (Appendix B, page 3), and the RFI report (Appendix E, pages 1 to 10), there are two (2) religious facilities, two (2) recreational facilities, one (1) railroad, and one (1) trail. There is one public facility, a former railroad, within the project area. That number was updated to zero (0) by the site visit on May 25, 2023, by Egis. The railroad was abandoned by the Norfolk and Southern Railroad in 1987, and the railroad ties have been removed. Due to the railroad no longer operating within the corridor and the railroad ties having been removed, no impacts are expected. Access to all properties will be maintained during construction.

Early Coordination

Coordination with INDOT Utilities and Railroads and the INDOT Rail Programs office occurred on July 26, 2023, and August 21, 2023, regarding the former railroad corridor. This section of the railroad is no longer in active use by the Norfolk and Southern Railroad since 1987 (Appendix C, pages 21 to 27). Access to all properties will be maintained during construction.

It is the responsibility of the project sponsor to notify school corporations and emergency services at least two weeks prior to any construction that will block or limit access.

Environmental Justice (EJ) (Presidential EO 12898)

During the development of the project were EJ issues identified?

<input type="checkbox"/>	Yes
<input checked="" type="checkbox"/>	No
<input type="checkbox"/>	
<input type="checkbox"/>	

Does the project require an EJ analysis?

If YES, then:

Are any EJ populations located within the project area?

<input type="checkbox"/>	Yes
<input type="checkbox"/>	No

Will the project result in adversely high and disproportionate impacts to EJ populations?

Indicate if EJ issues were identified during project development. If an EJ analysis was not required, discuss why. If an EJ analysis was required, describe how the EJ population was identified. Include if the project has a disproportionately high or adverse effect on EJ populations and explain your reasoning. If yes, describe actions to avoid, minimize and mitigate these effects.

No EJ analysis required

Under FHWA Order 6640.23A, FHWA and the project sponsor, as a recipient of funding from FHWA, are responsible to ensure that their programs, policies, and activities do not have a disproportionately high and adverse effect on minority or low-income populations. The project has 1.2 acre of ROW being acquired by the project. However, all ROW is located within the former railroad corridor and does not count toward the CE thresholds or EJ analysis ROW requirements; therefore, an EJ analysis is not required per the current INDOT Categorical Exclusion Manual.

Relocation of People, Businesses or Farms

Will the proposed action result in the relocation of people, businesses or farms?

<input type="checkbox"/>	Yes
<input checked="" type="checkbox"/>	No
<input type="checkbox"/>	
<input type="checkbox"/>	

Is a BIS or CSRS required?

Number of relocations: Residences: 0 Businesses: 0 Farms: 0 Other: 0

Discuss any relocations that will occur due to the project. If a BIS or CSRS is required, discuss the results in the discussion below.

No Relocations

No relocations of people, businesses, or farms will take place as a result of this project.

Indiana Department of Transportation

County Vermillion

Route SR 63

Des. No. 2100968 and 2100969

SECTION I – HAZARDOUS MATERIALS & REGULATED SUBSTANCES

Documentation

Hazardous Materials & Regulated Substances (Mark all that apply)

- Red Flag Investigation (RFI)
- Phase I Environmental Site Assessment (Phase I ESA)
- Phase II Environmental Site Assessment (Phase II ESA)
- Design/Specifications for Remediation required?

X

Date RFI concurrence by INDOT SAM (if applicable): December 13, 2023

Include a summary of the potential hazardous material concerns found during review. Discuss in depth sites found within, directly adjacent to, or ones that could impact the project area. Refer to current INDOT SAM guidance. If additional documentation (special provisions, pay quantities, etc.) will be needed, include in discussion. Include applicable commitments.

Presence, no impact

Based on a review of Geographic Information Systems (GIS) and available public records, the RFI was completed on December 7, 2023, by Egis and INDOT Site Assessment and Management (SAM) provided their concurrence on December 13, 2023 (Appendix E, pages 1 to 10). Two (2) UST sites, one (1) RCRA Generator/TSD site, and four (4) LUST sites are located within the 0.5-mile search radius. None of the hazmat sites identified will impact the project. Further investigation for hazardous material concerns is not required at this time.

Part IV – Permits and Commitments

PERMITS CHECKLIST

Permits (mark all that apply)

Likely Required

Army Corps of Engineers (404/Section10 Permit)

- Nationwide Permit (NWP)
- Regional General Permit (RGP)
- Individual Permit (IP)
- Other

IN Department of Environmental Management (401/Rule 5)

- Nationwide Permit (NWP)
- Regional General Permit (RGP)
- Individual Permit (IP)
- Isolated Wetlands
- Construction Stormwater General Permit
- Other

X

IN Department of Natural Resources

- Construction in a Floodway
- Navigable Waterway Permit
- Other

X

Mitigation Required

- US Coast Guard Section 9 Bridge Permit**
- Others (Please discuss in the discussion below)**

Indiana Department of Transportation

County Vermillion

Route SR 63

Des. No. 2100968 and 2100969

List the permits likely required for the project and summarize why the permits are needed, including permits designated as "Other."

A Construction Stormwater General Permit (CSGP) is required due to soil disturbance greater than one acre. No other permits are required at this time.

Applicable recommendations provided by resource agencies are included in the *Environmental Commitments* section of this document. If permits are found to be necessary, the conditions of the permit will be requirements of the project and will supersede these recommendations.

It is the responsibility of the project sponsor to identify and obtain all required permits.

ENVIRONMENTAL COMMITMENTS

List all commitments and include the name of agency/organization requesting/requiring the commitment(s). Listed commitments should be numbered.

Firm:

- 1) If the scope of work or permanent or temporary ROW amounts change, the INDOT Environmental Services Division (ESD) and the INDOT District Environmental Section will be contacted immediately (INDOT ESD and INDOT Crawfordsville District).
- 2) It is the responsibility of the project sponsor to notify school corporations and emergency services at least two weeks prior to any construction that will block or limit access (INDOT ESD).
- 3) USFWS Bridge/Structure Assessment shall take place no earlier than two (2) years prior to the start of construction. If construction will begin after May 25, 2025, an inspection of the structure by a qualified professional must be performed. Inspection of the structure should check for presence of bats/bat indicators and/or presence of birds. The results of the inspection must indicate no signs of bats or birds. If signs of bats or birds are documented during this inspection, the INDOT District Environmental Manager must be contacted immediately (INDOT ESD).
- 4) Structure #063-83-02002ANBL/ASBL and the project's surrounding habitat is conducive for use (i.e. nests) by a bird species protected under the Migratory Bird Treaty Act (MBTA). Prior to the start of nesting season (May 1), the structure must be inspected for birds or signs of birds. If birds or signs of birds are found during the inspection, avoidance and minimization measures must be implemented prior to the start of and during the nesting season. Nests without eggs or young should be removed prior to construction during the non-nesting season (September 8 – April 30) and during the nesting season if no eggs or young are present. Nests with eggs or young cannot be removed or disturbed during the nesting season (May 1 – September 7). Nests with eggs or young should be screened or buffered from active construction. Details of the required procedures are outlined in the "Potential Migratory Bird on Structure" USP/RSP (INDOT ESD).
- 5) Coordination with local waterway utilities will be ongoing as the project advances (Town of Cayuga Sanitary and Water Utility).
- 6) Tree Removal AMM 1: Modify all phases/aspects of the project (e.g., temporary work areas, alignments) to avoid tree removal (USFWS).
- 7) Lighting AMM 1: Direct temporary lighting away from suitable habitat during the active season (USFWS).
- 8) Tree Removal AMM 3: Ensure tree removal is limited to that specified in project plans and ensure that contractors understand clearing limits and how they are marked in the field (e.g., install bright colored flagging/fencing prior to any tree clearing to ensure contractors stay within clearing limits) (USFWS).
- 9) General AMM 1: Ensure all operators, employees, and contractors working in areas of known or presumed bat habitat or aware of all FHWA/FRA/FTA (Transportation Agencies) environmental commitments, including all applicable AMMs (USFWS).
- 10) Contractors must take care when handling dead or injured bats (regardless of species), and any other federally listed

Indiana Department of Transportation

County Vermillion Route SR 63 Des. No. 2100968 and 2100969

species that are found at the Project site in order to preserve biological material in the best possible condition and protect handler from exposure to diseases, such as rabies. Project personnel are responsible for ensuring that any evidence about determining the cause of death or injury is not unnecessarily disturbed. Reporting the discovery of dead or injured listed species is required in all cases to enable the Service to determine whether the level of incidental take exempted by this BO has been exceeded, and to ensure that the terms and conditions are appropriate and effective. Parties finding a dead, injured, or sick specimen of any endangered or threatened species must promptly notify the USFWS Indiana Field Office at (812) 334-4261. This commitment will be resolved with a Unique Special Provision (USP) into the contract (USFWS).

- 11) A "Reinitiation Notice" is required if: more than 0.04 acre of trees are to be cleared; the amount or extent of incidental take of Indiana bat and/or northern long-eared bat is exceeded; new information about listed species is encountered; new species is listed or critical habitat designated that the project may affect; more than five (5) Indiana bats and/or five (5) NLEBs are taken resulting from the bridge activity; the project is modified in a manner that causes an effect to the listed species; or, new information reveals that the project may affect listed species or critical habitat in a manner not considered in the BO or the project information. This commitment will be resolved with a USP into the contract. (USFWS)
- 12) The INDOT Project Manager will assure that \$881.16 of Preliminary Engineering funds will be allocated to the Rangewide In-Lieu Fee Program, administered by the Conservation Fund, to resolve formal consultation under the Rangewide Programmatic (0.04 acre X 1.75 x \$12,588 = \$881.16). Payment shall be in process for Ready for Contracts date. (USFWS)
- 13) If necessary, the INDOT Project Manager will ensure an administrative modification to the STIP will occur prior to the Ready-for-Contract (RFC) date (INDOT ESD).

For Further Consideration:

- 14) Improving wildlife passage at existing or proposed bridge locations is a priority of the IDNR-DFW to reduce wildlife mortality along roadways, specifically for white-tailed deer (*Odocoileus virginianus*). The project should not create conditions that are less favorable for wildlife passage under the structure compared to the current conditions. The IDNR- DFW encourages INDOT at a minimum collect data on wildlife use of the existing structures to determine if maintaining deer passage should be considered (IDNR-DFW).
- 15) Road runoff should be directed to riprap turnouts and sediment filtration prior to entering a stream to reduce impacts to aquatic species (IDNR-DFW).
- 16) Align the road along or through previously disturbed and degraded areas and disturb as narrow an area as possible to minimize negative impacts. Avoid tree removal to the greatest extent possible. Plant native hardwood trees to replace the vegetation destroyed during construction (IDNR-DFW).
- 17) Do not cut any trees suitable for Indiana bat or Northern Long-eared bat roosting from April 1 through September 30. Indiana and Northern Long-eared Bats typically roost in trees 3 inches or greater diameter-at-breast height, living or dead, with loose hanging tree bark, cracks, crevices, or cavities (IDNR-DFW).
- 18) Plant five trees, 1 inch to 2 inches in diameter-at-breast height, for each tree which is removed that is 10 inches or greater in diameter- at- breast height (IDNR-DFW).

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Appendix A:

INDOT Supporting Documentation

Categorical Exclusion Level Thresholds

	PCE	Level 1	Level 2	Level 3	Level 4 ¹
Section 106	Falls within guidelines of Minor Projects PA	“No Historic Properties Affected”	“No Adverse Effect”	-	“Adverse Effect” Or Historic Bridge involvement ²
Stream Impacts³	No construction in waterways or water bodies	< 300 linear feet of stream impacts	≥ 300 linear feet of stream impacts	-	USACE Individual 404 Permit ⁴
Wetland Impacts³	No adverse impacts to wetlands	< 0.1 acre	-	< 1.0 acre	≥ 1.0 acre
Right-of-way⁵	Property acquisition for preservation only or none	< 0.5 acre	≥ 0.5 acre	-	-
Relocations⁶	None	-	-	< 5	≥ 5
Threatened/Endangered Species (Species Specific Programmatic for Indiana bat & northern long eared bat)*	“No Effect”, “Not likely to Adversely Affect” (With select AMMs ⁷)	“Not likely to Adversely Affect” (With any AMMs or commitments)	-	“Likely to Adversely Affect”	Project does not fall under Species Specific Programmatic ⁸
Threatened/Endangered Species (Any other species)*	Falls within guidelines of USFWS 2013 Interim Policy or “No Effect”	“Not likely to Adversely Affect”	-	-	“Likely to Adversely Affect”
Environmental Justice	No disproportionately high and adverse impacts	-	-	-	Potential ⁹
Sole Source Aquifer	No Detailed Groundwater Assessment	-	-	-	Detailed Groundwater Assessment
Floodplain	No Substantial Impacts	-	-	-	Substantial Impacts
Section 4(f) Impacts	None	-	-	-	Any ¹⁰
Section 6(f) Impacts	None	-	-	-	Any
Permanent Traffic Alteration	None	-	-	-	Any
Noise Analysis Required	No	-	-	-	Yes
Air Quality Analysis Required	No	-	-	-	Yes ¹¹
Approval Level					
• District Env. (DE)	Concurrence by DE or ESD	DE or ESD	DE or ESD	DE and/or ESD	DE and/or ESD; and FHWA
• Env. Serv. Div. (ESD)					
• FHWA					

¹ Coordinate with INDOT Environmental Services Division. INDOT will then coordinate with the appropriate FHWA Environmental Specialist.

² Any involvement with a bridge processed under the Historic Bridge Programmatic Agreement.

³ Total permanent impacts to streams (linear feet) and wetlands (acres).

⁴ US Army Corps of Engineers Individual 404 Permit

⁵ Total permanent and temporary right-of-way. This does not include reacquisition of existing apparent right-of-way.

⁶ If any relocations are within an area with a known or suspected Environmental Justice (EJ) or disadvantaged population, or has greater than 5 relocations, a conversation with FHWA, through INDOT ESD, is needed to confirm NEPA classification and outreach plan for the project.

⁷ Avoidance and Mitigation Measures (AMMs) determined by the IPAC determination key to be required that are not tree AMMs, bridge AMMs, or structure AMMs.

⁸ Projects that do not fall under a Species Specific Programmatic and results in a “Likely to Adversely Affect”. Other findings can be processed as a lower-level CE.

⁹ Potential for causing a disproportionately high and adverse impact.

¹⁰ Section 4(f) use resulting in an Individual, Programmatic, or *de minimis* evaluation. The only exception is a *de minimis* evaluation for historic properties (Effective January 2, 2020). If a historic property *de minimis* and no other use, mark the *None* column.

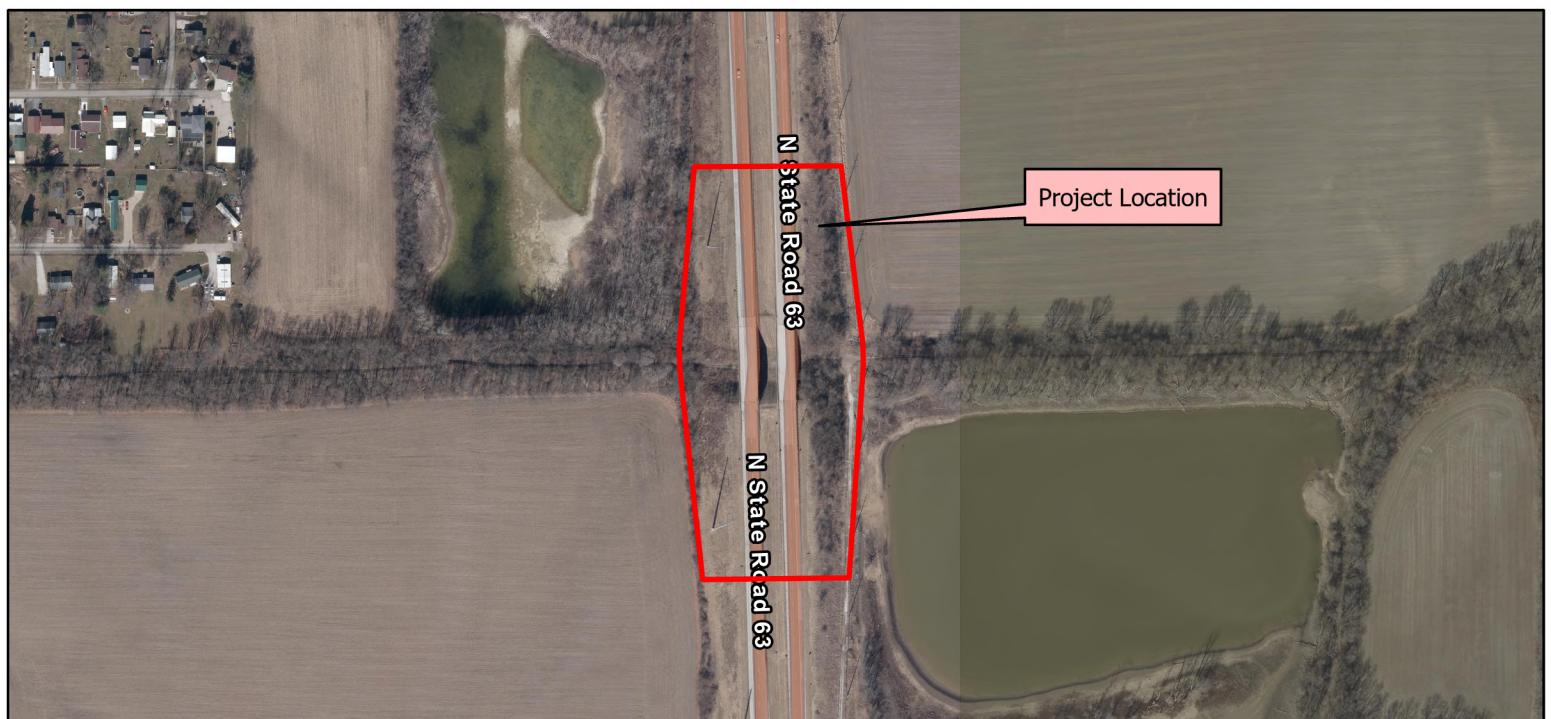
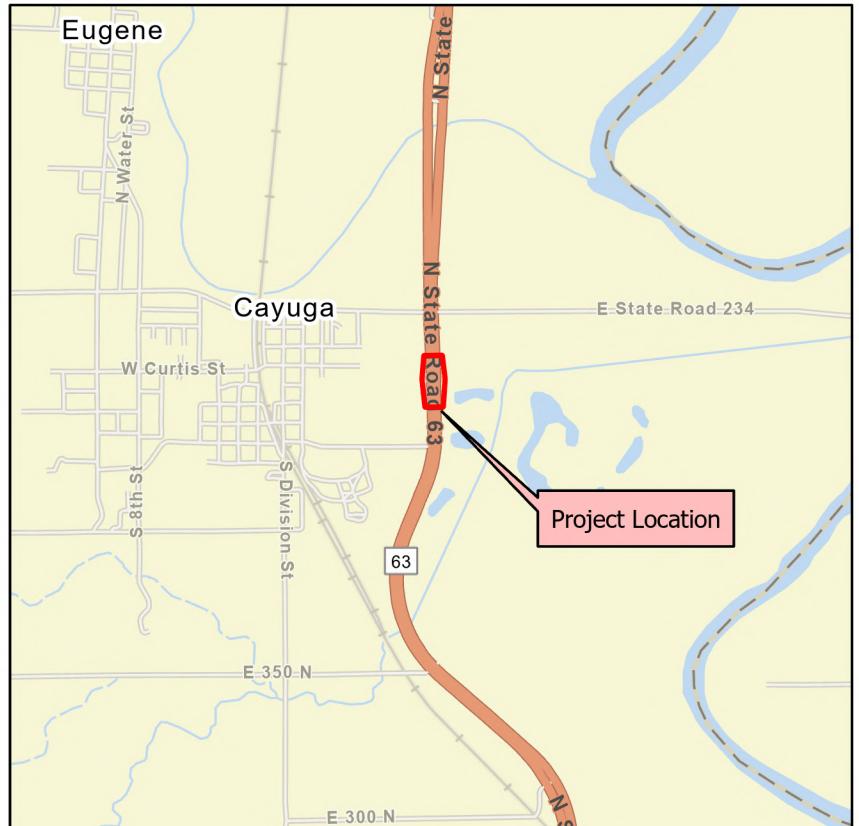
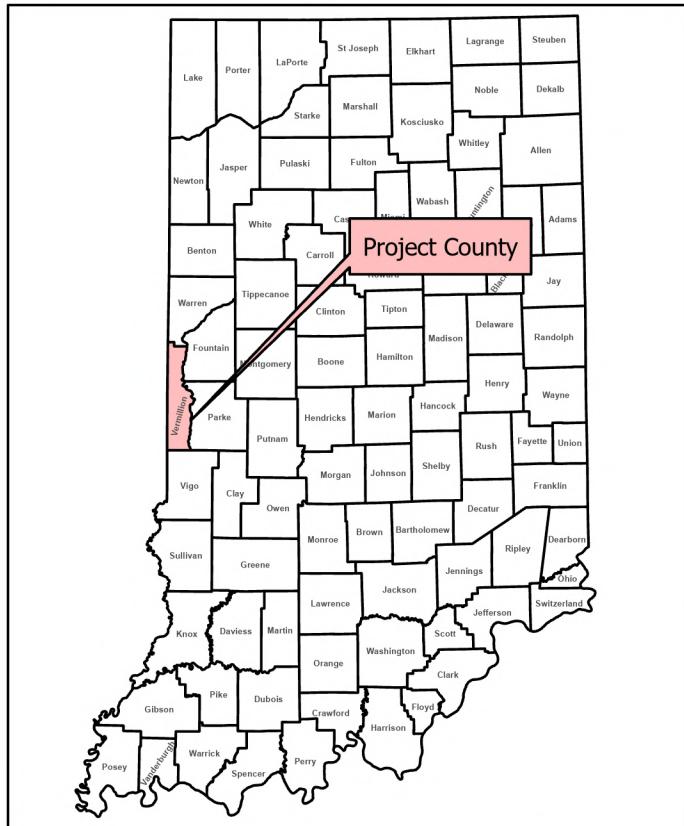
¹¹ Hot Spot Analysis and/or MSAT Quantitative Emission Analysis.

* Includes the threatened/endangered species critical habitat

Note: Substantial public or agency controversy may require a higher-level NEPA document.

Appendix B:

Graphics



N
 Source: gis.in.gov/arcgis/rest/services, Google.com
 0 500 1,000 Feet

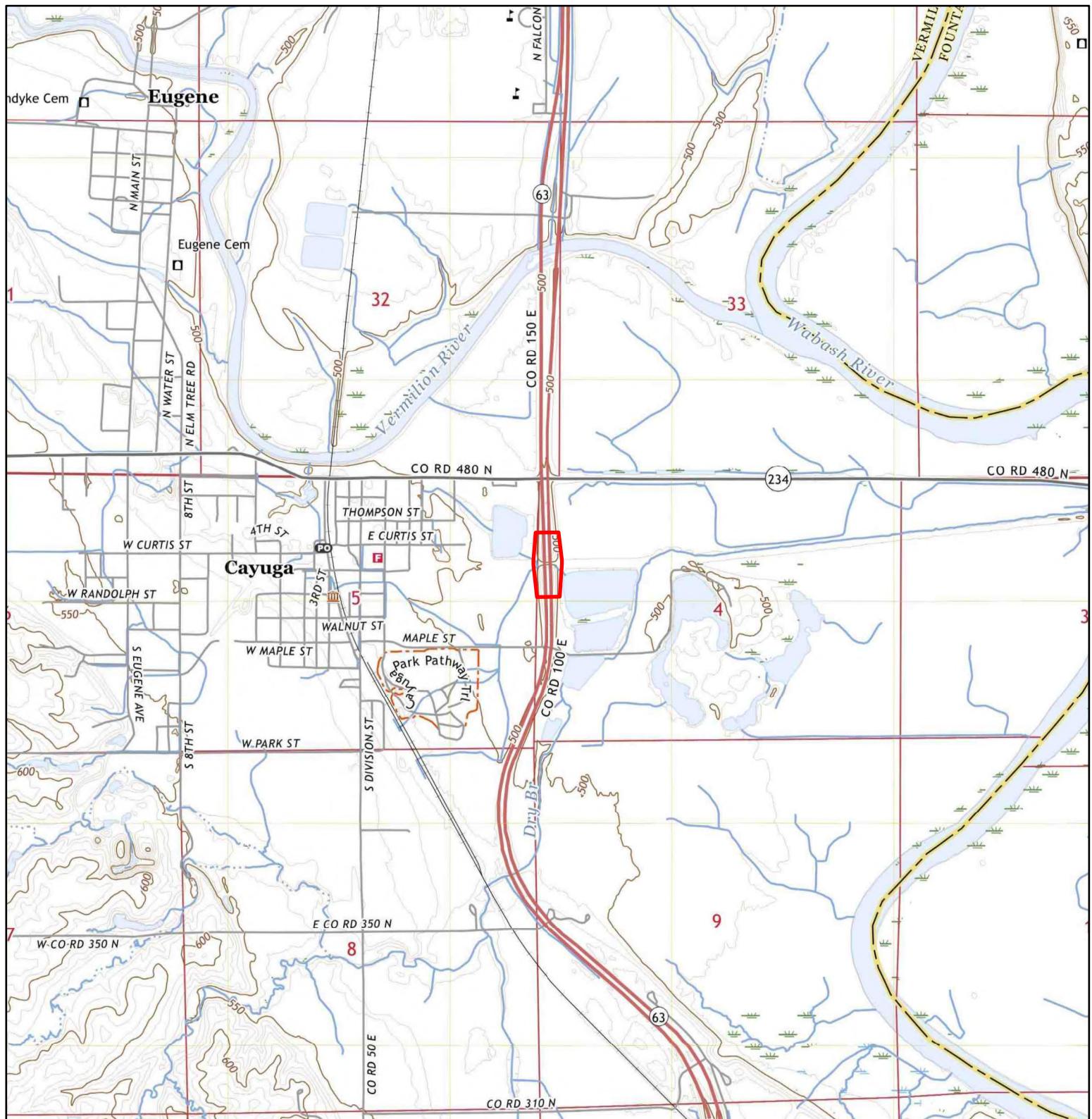
 egis

Legend

- Project Area
- County Selection

B-1

Figure 1: Project Location Map
 Bridge Removal Project
 SR 63 over Former Railroad
 0.24 Mile South of SR 234
 Vermillion County, Indiana
 Des. 2100968 / 2100969
 Author: Katie Finney



Source: gis.in.gov/arcgis/rest/services

0 0.5 1 Miles



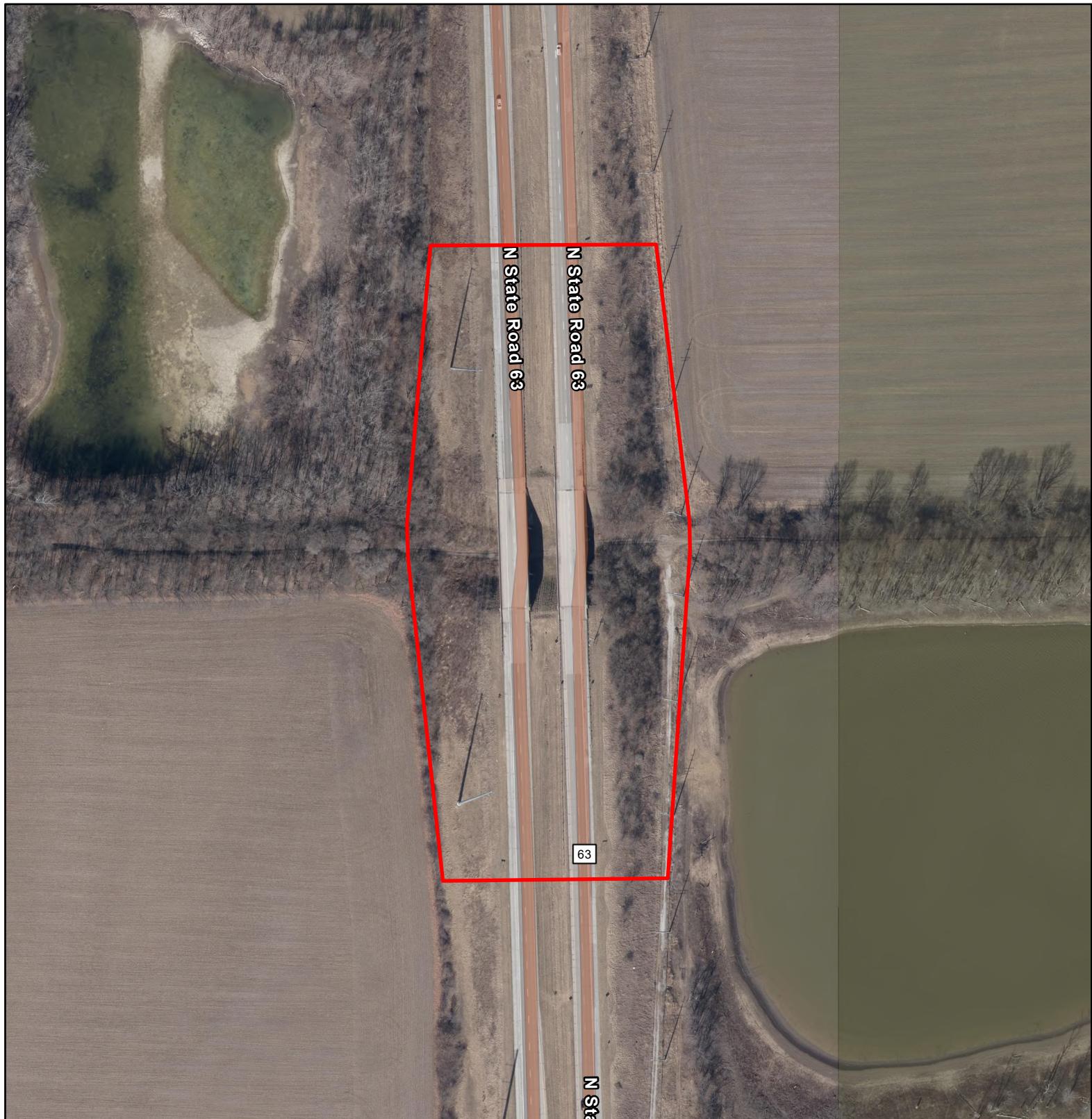
Legend

Project Area

B-2

Figure 2: USGS Topo Map
 Newport 7.5 Minute Quadrangle
 Bridge Removal Project
 SR 63 over Former Railroad
 0.24 Mile South of SR 234
 Vermillion County, Indiana
 Des. 2100968 / 2100969
 Author: Katie Finney

Date Exported: July 29, 2024



Source: gis.in.gov/arcgis/rest/services

0 100 200 Feet

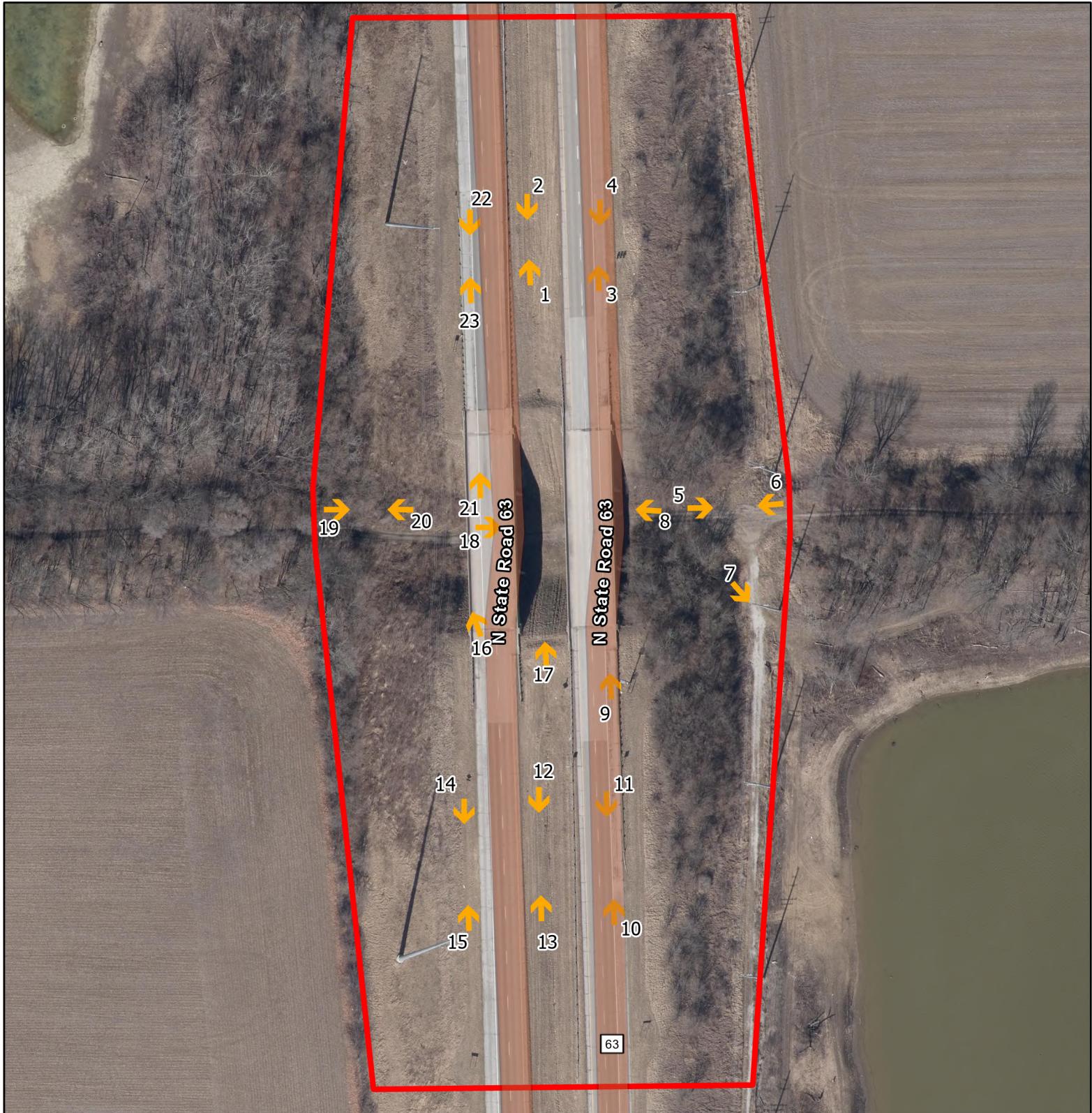


Legend

Project Area

B-3

Figure 3: Aerial Map
Bridge Removal Project
SR 63 over Former Railroad
0.24 Mile South of SR 234
Vermillion County, Indiana
Des. 2100968 / 2100969
Author: Katie Finney



Source: gis.in.gov/arcgis/rest/services

0 90 180 Feet

 egis

Legend

-  Project Area
-  Photo Points

B-4

Figure 4: Photo Location and Orientation Map
Bridge Removal Project
SR 63 Over Former Railroad,
0.24 Mile South of SR 234
Vermillion County, Indiana
Des. 210968 and 2100969
Author: Katie Finney



Photo 1: Facing north from the median, north of the bridges.



Photo 2: Facing south from the median, north of the bridges.

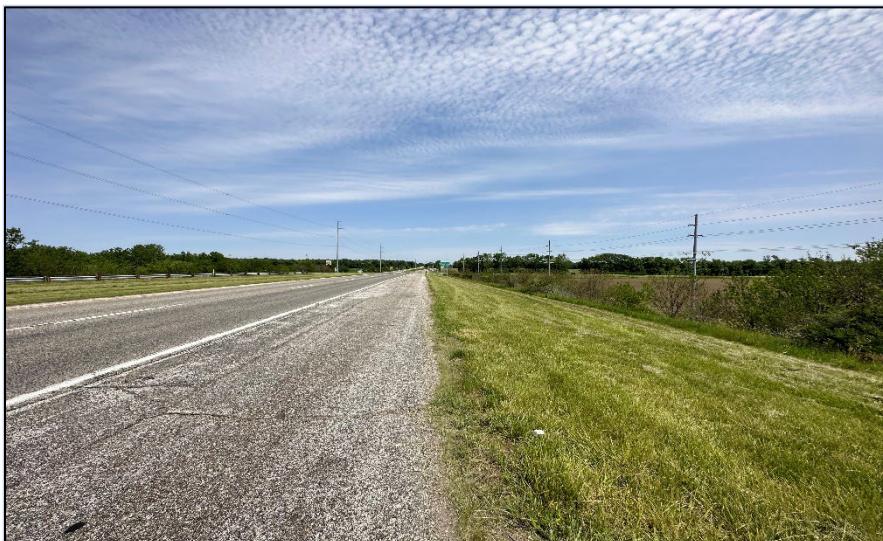


Photo 3: Facing north from SR 63 northbound, north of the bridges.



Photo 4: Facing south from SR 63 northbound, north of the bridges.



Photo 5: Facing east, toward the path that runs through the project area, east of SR 63.



Photo 6: Facing west, toward the path that runs through the project area, east of SR 63.



Photo 7: Facing southeast, east of SR 63, toward a pond and gravel road southeast of the bridges. The pond is outside of the project area.



Photo 8: Facing west toward the bridges, east of SR 63.

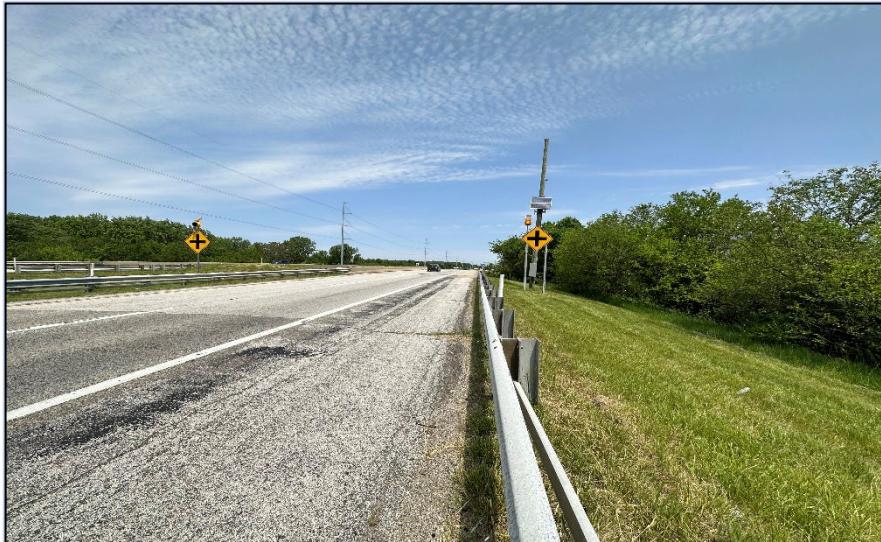


Photo 9: Facing north toward the northbound bridge from SR 63 northbound.



Photo 10: Facing north toward from SR 63 northbound, south of the bridges.



Photo 11: Facing south from SR 63 northbound, south of the bridges.



Photo 12: Facing south from the median, south of the bridges.



Photo 13: Facing north from the median, south of the bridges



Photo 14: Facing south from SR 63 southbound.



Photo 15: Facing north from SR 63 southbound, south of the bridges.

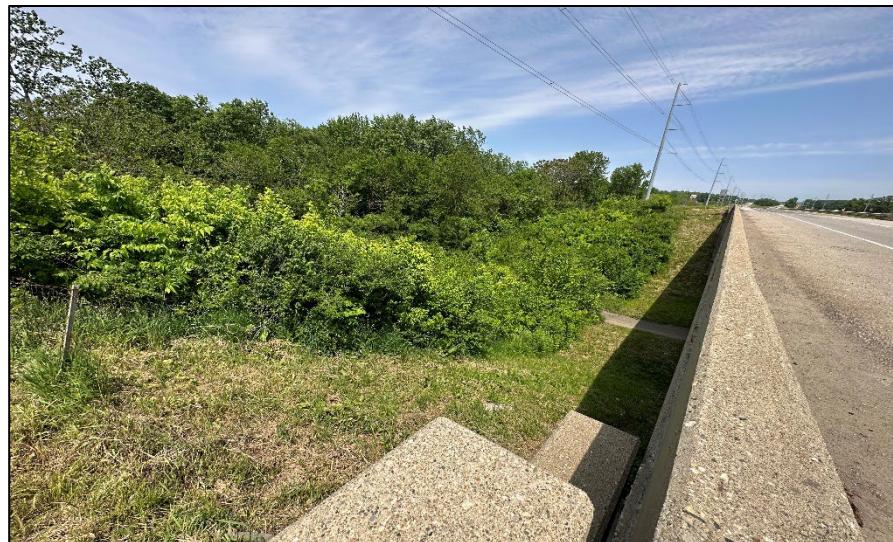


Photo 16: Facing northwest from the southbound bridge toward the right-of-way.



Photo 17: Facing north looking down between the bridges from the median.



Photo 18: Facing east, looking down between the bridges, from the southbound bridge.



Photo 19: Facing east toward the bridges, west of SR 63.

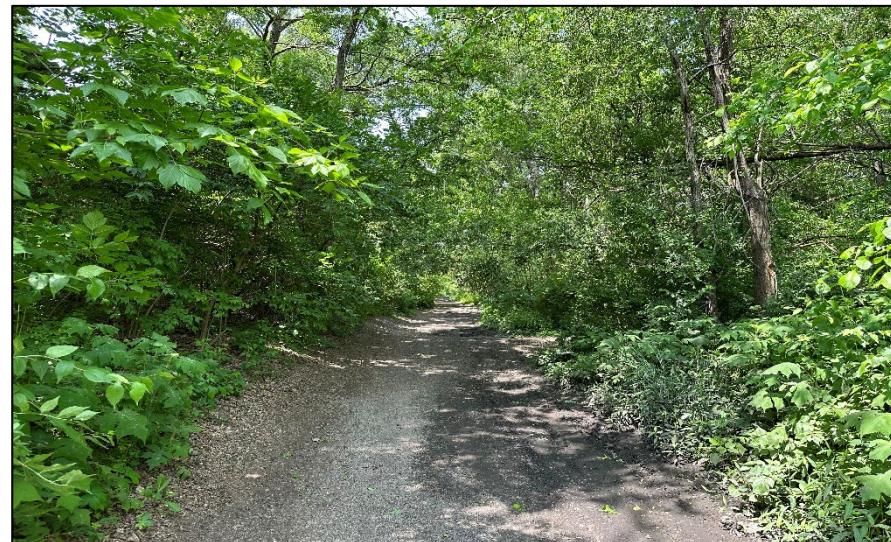


Photo 20: Facing west, toward the path that runs through the project area, west of SR 63.



Photo 21: Facing north looking at the swallow's nests on the underside of the southbound bridge.



Photo 22: Facing south toward the southbound bridge from SR 63 southbound.



Photo 23: Facing north from SR 63 southbound, north of the bridges.

PROJECT	DESIGNATION
2100968	2100968 & 2100969
CONTACT	BRIDGE FILE
R-43085	063-83-02002 ANBL & ASBL

STRUCTURE INFORMATION				
STRUCTURE	TYPE	SPAN AND SKEW	OVER	STATION
063-83-02002 (TO BE REMOVED)	CONTINUOUS COMPOSITE STEEL BEAM BRIDGE	3 SPANS 51'-0", 71'-0", 51'-0" SKEW: SQUARE	FORMER NORFOLK & SOUTHERN RAILROAD CORRIDOR	4 STRUCTURE STA 1117+71.50 'L'

KIN PROJECT INFORMATION	
DESIGNATION	PROJECT DESCRIPTION
2100968 (LEAD)	STRUCTURAL HPA OVERLAY ON SR 63
2100977	SMALL CULVERTS AND DRAINS ON SR 63
2100996	BRIDGE DECK REPLACEMENT FOR STRUCTURE 063-83-03560, DNEBL
2100962	BRIDGE DECK REPLACEMENT FOR STRUCTURE 063-83-03561, XCSB
2100968	BRIDGE REMOVAL OF STRUCTURE 063-83-02002 ANBL
2100969	BRIDGE REMOVAL OF STRUCTURE 063-83-02002 ASBL

INDIANA DEPARTMENT OF TRANSPORTATION



ROAD PLANS

BRIDGE REMOVAL

ROUTE: SR 63 AT: RP 66+47

PROJECT NO. 2100968 & 2100969 P.E.
2100968 & 2100969 R/W
2100968 & 2100969 CONST.

Bridge Removal on SR 63 over Former Norfolk & Southern Railroad Corridor
Located 0.24 Miles South of SR 234 in
Sections 4 & 5, T-17-N, R-9-W, Eugene Township, Vermillion County, Indiana

PROJECT LOCATION
Begin Project-Sta.1116+40.00 'L'
End Project-Sta.1118+90.00 'L'



LOCATION MAP
SCALE: 1" = 2000'

TRAFFIC DATA

A.A.D.T. (2006)	7034 V.P.D.
A.A.D.T. (2046)	7074 V.P.D.
D.H.V. (2046)	5815 V.P.H.
DIRECTIONAL DISTRIBUTION	50.76 % N.
TRUCKS	26.92 % A.A.D.T. 15.21 % D.H.V.

DESIGN DATA

DESIGN SPEED	60 MPH
PROJECT DESIGN CRITERIA	RECONSTRUCTION (NON-FREEWAY)
FUNCTIONAL CLASSIFICATION	PRINCIPAL ARTERIAL
RURAL/URBAN	RURAL
TERRAIN	LEVEL
ACCESS CONTROL	PARTIAL



PROJECT LOCATION SHOWN BY
VERMILLION COUNTY

LATITUDE: 39°56'55.70" N LONGITUDE: 87°27'04.50" W

BRIDGE LENGTH: 0.00 MI.
ROADWAY LENGTH: 0.047 MI.
TOTAL LENGTH: 0.047 MI.
MAX. GRADE: 0.75 %

HUC 12: 051201081602
HUC 14: 05120108140010

INDIANA DEPARTMENT OF TRANSPORTATION
STANDARD SPECIFICATIONS DATED 2024
TO BE USED WITH THESE PLANS.

PLANS PREPARED BY:



PLANS PREPARED BY:
Egis BLN USA, Inc.
8720 Craig Street | Indianapolis, IN 46250

(317) 849-5832
www.egis-group.com
PHONE NUMBER

CERTIFIED BY: _____ DATE: _____

APPROVED FOR LETTING: _____ DATE: _____

INDIANA DEPARTMENT OF TRANSPORTATION

BRIDGE FILE

063-83-02002 ANBL & ASBL

DESIGNATION

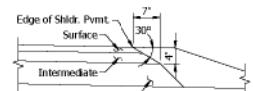
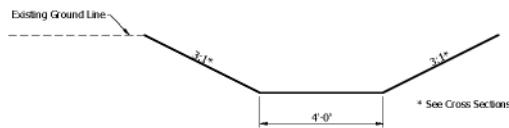
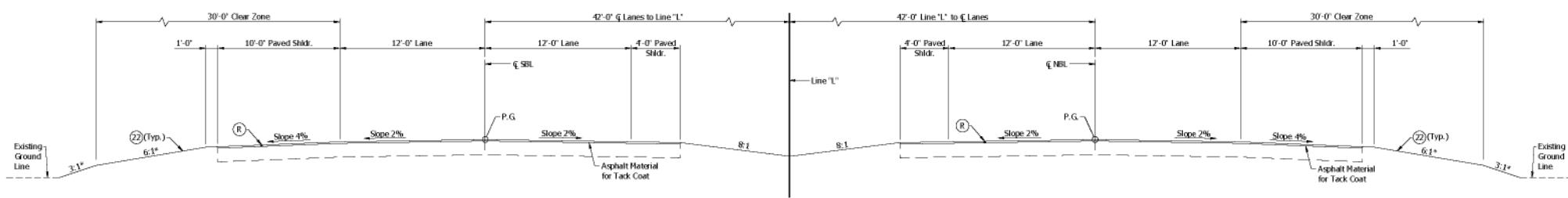
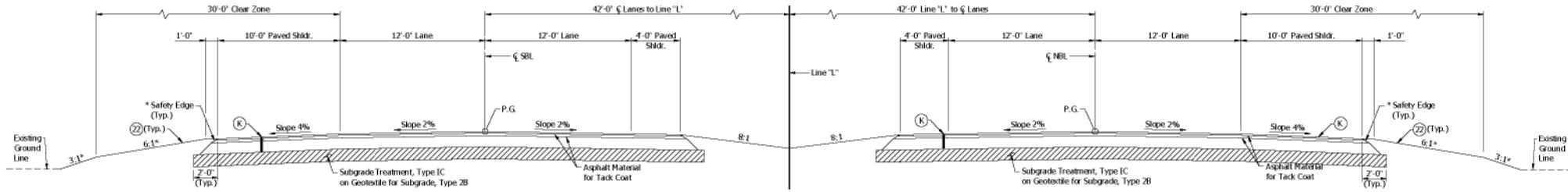
2100968 & 2100969

DRAWING NO. SHEETS

1 of 23

CONTRACT PROJECT

R-43085 2100968



MATCHING EXISTING PAVEMENT
(Req'd. @ End of Project)
No Scale

HORIZONTAL SCALE		BRIDGE FILE	
AS NOTED		003-05-0202, ANBL & ASBL	
VERTICAL SCALE		DESIGNATION	
AS NOTED		210098 & 2110599	
DRAWING NO.		SHEETS	
3		of 23	
CONTRACT		PROJECT	
R-1506		2100188	

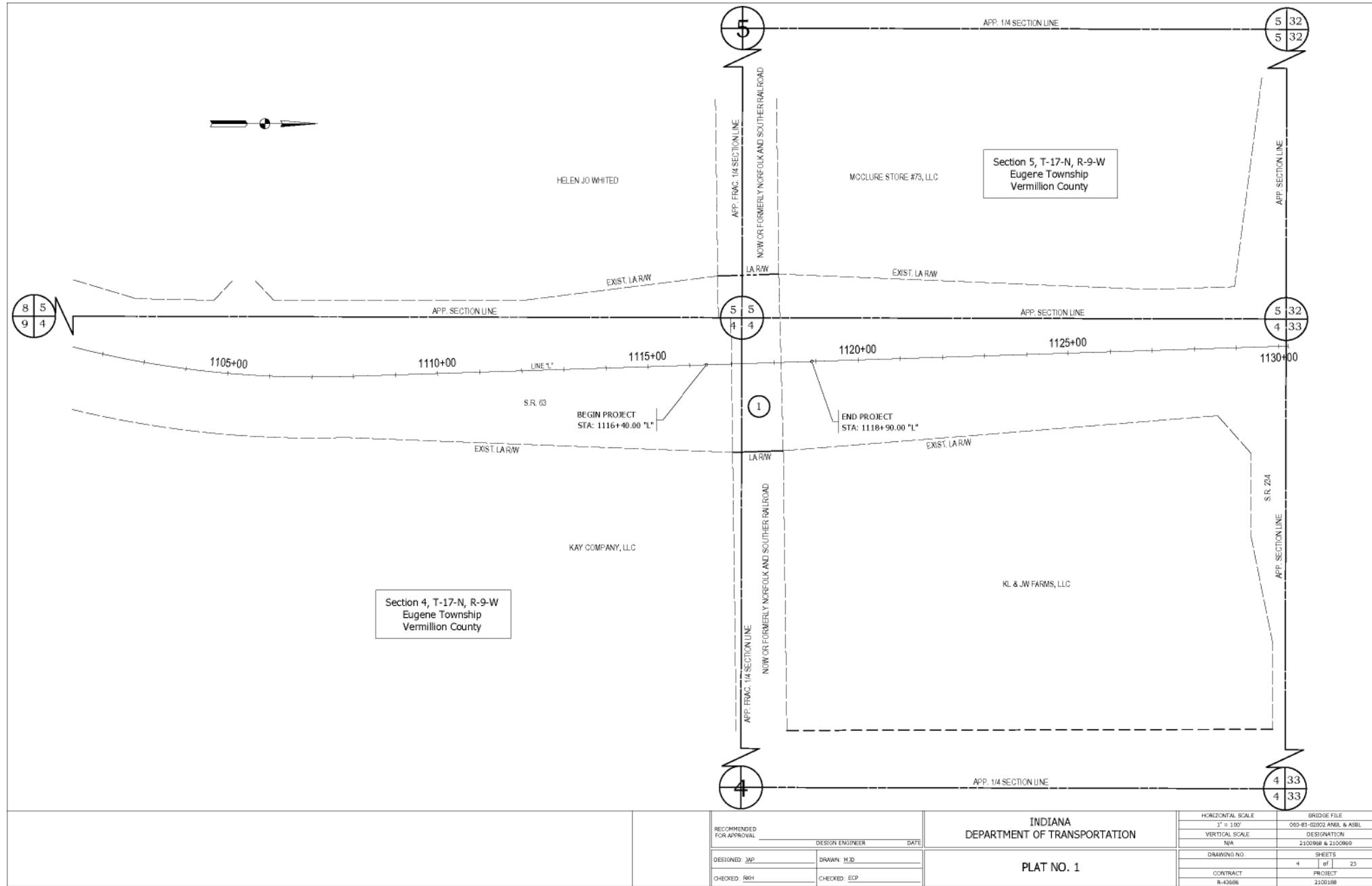
RECOMMENDED FOR APPROVAL
DESIGN ENGINEER DATE

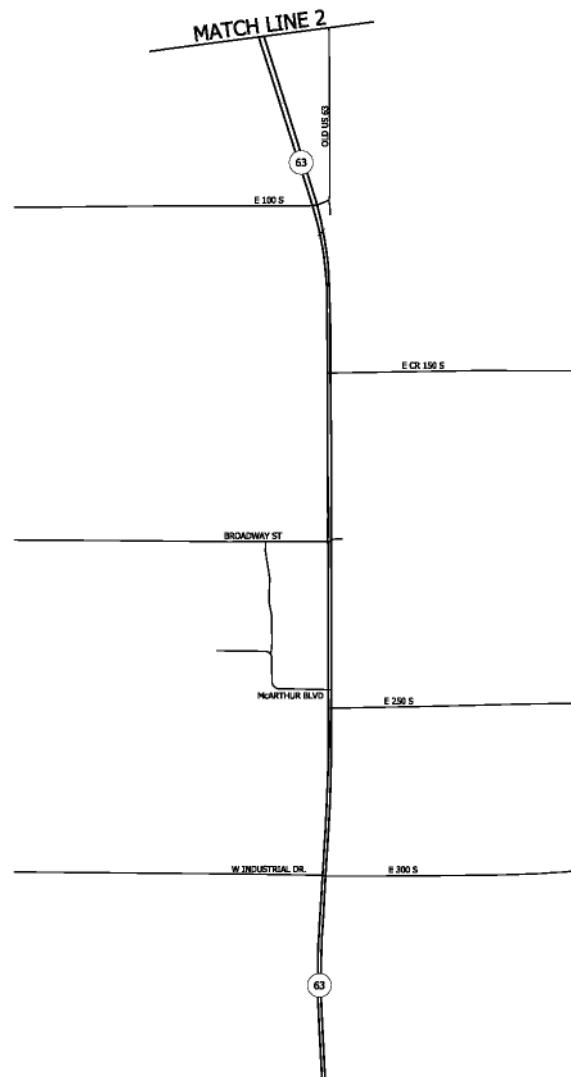
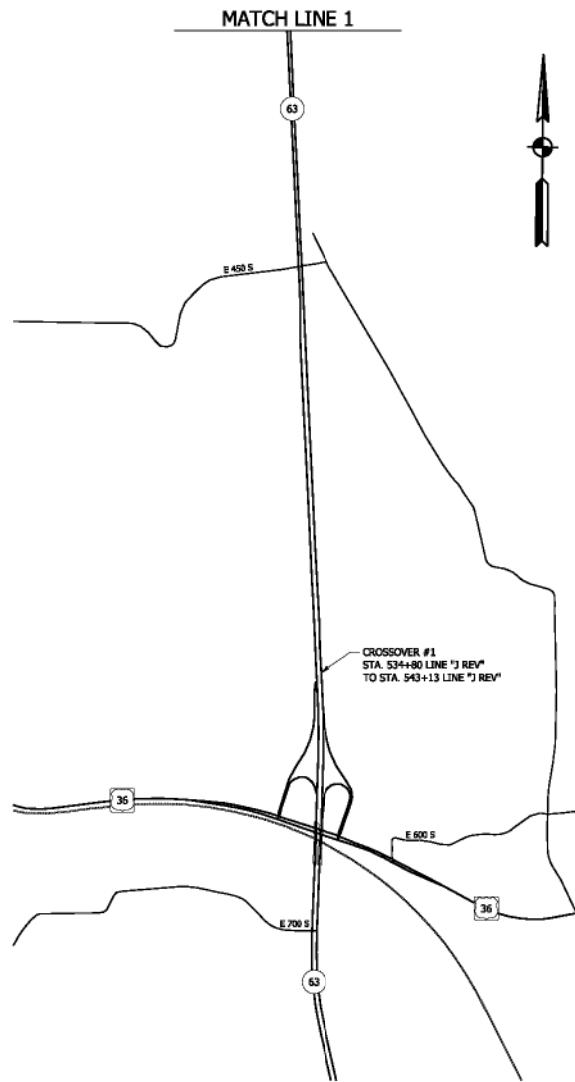
DESIGNED: JAP DRAWN: NW

CHECKED: RAB CHECKED: JAP

INDIANA
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS





FOR INFORMATION ONLY

LEGEND				INDIANA DEPARTMENT OF TRANSPORTATION	HORIZONTAL SCALE	BRIDGE FILE			
	RECOMMENDED FOR APPROVAL								
	DESIGN ENGINEER								
DESIGNED BY	DRAWN BY			MAINTENANCE OF TRAFFIC OVERVIEW MAP					
CHECKED LOW	CHECKED MAE			SURVEY BOOK	5 of 1 of 23	DESIGNATION			
CULVERT REPLACEMENTS - DES 2001773				CONTRACT	2100188	PROJECT			
TRENCHLESS CULVERT REPLACEMENTS - DES 2001773				CONTACT	2100188	PROJECT			
CULVERT REPLACEMENTS - DES 2100188				VERTICAL SCALE	N/A	DESIGNATION			
				DATE	N/A	PROJECT			

CONSTRUCTION ZONE DESIGN SPEED: 55 MPH

HORIZONTAL SCALE	BRIDGE FILE		
N/A			
VERTICAL SCALE	DESIGNATION		
N/A	2100188		
SURVEY BOOK	SHEETS		
	5	of	23
CONTRACT	PROJECT		
JB-4196	7105188		

- SEE CROSSOVER - SOUTHBOUND CONSTRUCTION AND CROSSOVER - NORTHBOUND CONSTRUCTION FOR CROSSOVER DETAILS
- SEE CR 450 S - FLAGGER DETAILS FOR MAINTENANCE OF TRAFFIC AT THE SR 63 INTERSECTION WITH CR 450 S
- SEE S-LINE AND DRIVE ACCESS DETAILS FOR MAINTENANCE OF TRAFFIC DETAILS ON ALL OTHER S-LINES

CONSTRUCTION SEQUENCING

PHASE I:

- CONSTRUCT CROSSOVERS #1 AND #2 USING ADVANCED SIGNING FOR CROSSOVER INSTALLATION DETAIL
- RESURFACE RAMPS UTILIZING RAMP CLOSURES
- STRUCTURE INSTALLATION AND PAVING BETWEEN BEGINNING OF PROJECT AND CROSSOVER #1 WILL BE COMPLETED UNDER FLAGGING OPERATION

PHASE IIAC

- SHIFT TRAFFIC TO SB SR 63 UTILIZING CROSSOVER #1 AND #2
- COMPLETE PAVING OPERATIONS AND BRIDGE
- DECK REPLACEMENT ON NB SR 63

PHASE IIIB:
- SH

- COMPLETE STRUCTURE INSTALLATION AND BRIDGE DECK REPLACEMENT AND BRIDGE REMOVAL ON NB SR 63

Phase IIIA:

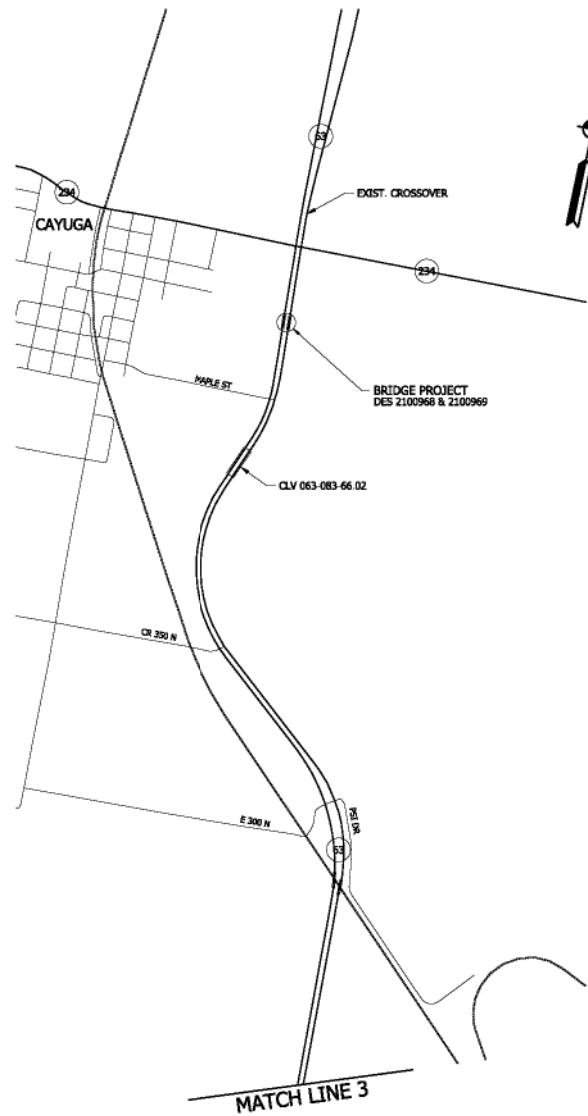
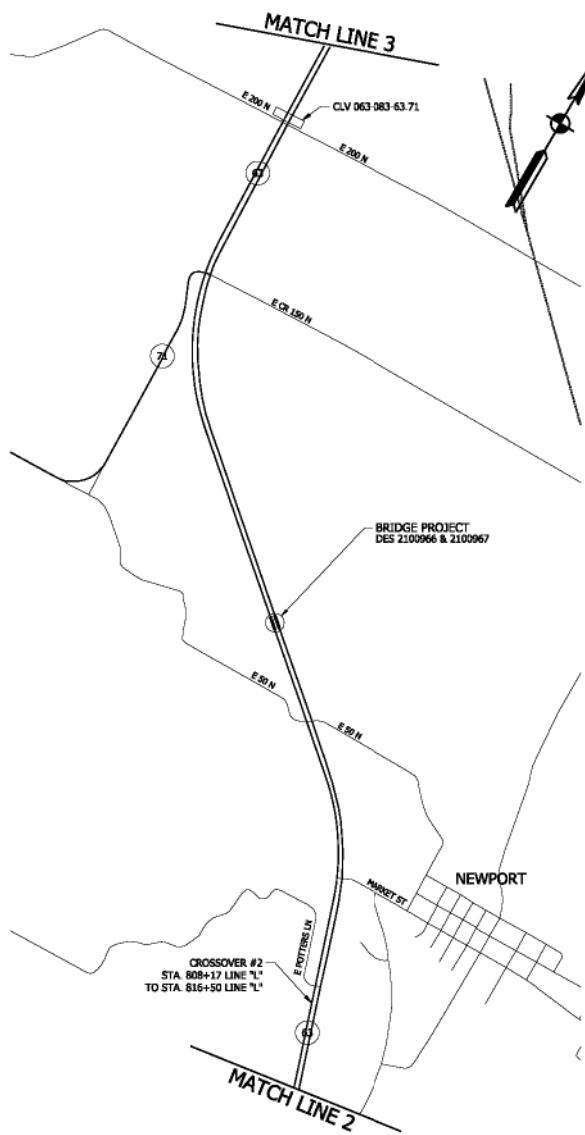
- SHIFT TRAFFIC TO NB SR 63 UTILIZING CROSSOVERS #1 AND #2
- COMPLETE PAVING OPERATIONS AND BRIDGE

DEO

PHASE IIIB:
- SHIFT TRAFFIC TO NB SR 63 UTILIZING CROSSOVERS #2 AND #3
- COMPLETE STRUCTURE INSTALLATION AND

NOTE

- SEE CROSSOVER - SOUTHBOUND CONSTRUCTION AND CROSSOVER - NORTHBOUND CONSTRUCTION FOR CROSSOVER DETAILS
- SEE CR 450 S - FLAGGER DETAILS FOR MAINTENANCE OF TRAFFIC AT THE SR 63 INTERSECTION WITH CR 450 S
- SEE S-LINE AND DRIVE ACCESS DETAILS FOR MAINTENANCE OF TRAFFIC DETAILS ON ALL OTHER S-LINES



FOR INFORMATION ONLY

CONSTRUCTION ZONE DESIGN SPEED: 55 MPH

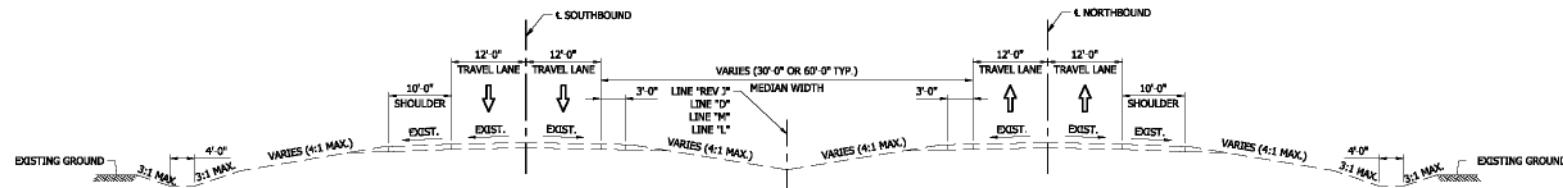
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- TRENCHLESS CULVERT REPLACEMENTS - DES 2001773
- CULVERT REPLACEMENTS - DES 2100188

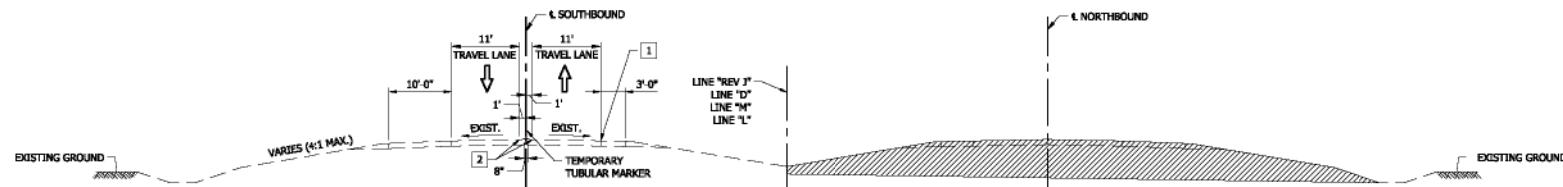
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DESIGNED: DWD	DRAWN: JBT	
CHECKED: LDW	CHECKED: MAE	

INDIANA
DEPARTMENT OF TRANSPORTATION

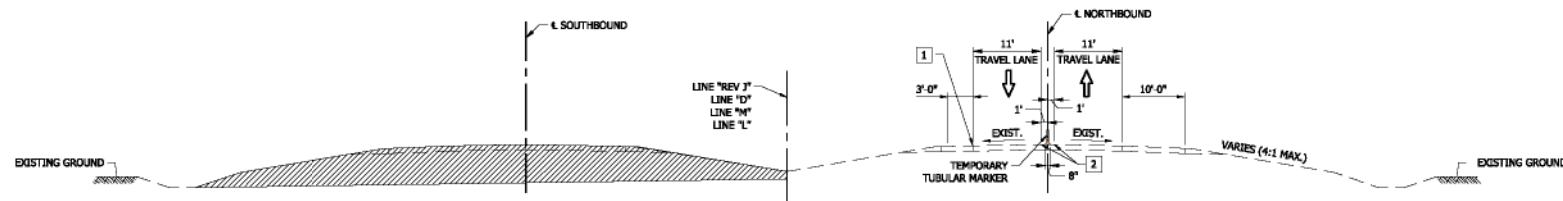
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VERTICAL SCALE	DESIGNATION		
N/A	2100188		
SURVEY BOOK		SHEETS	
	6	of	23
CONTRACT		PROJECT	
JB-4396		71505.98	



TYPICAL SECTION EXISTING



TYPICAL SECTION - NORTHBOUND CLOSURE (PHASE III)



TYPICAL SECTION - SOUTHBOUND CLOSURE (PHASE III)

CONSTRUCTION ZONE DESIGN SPEED: 55 MPH

FOR INFORMATION ONLY

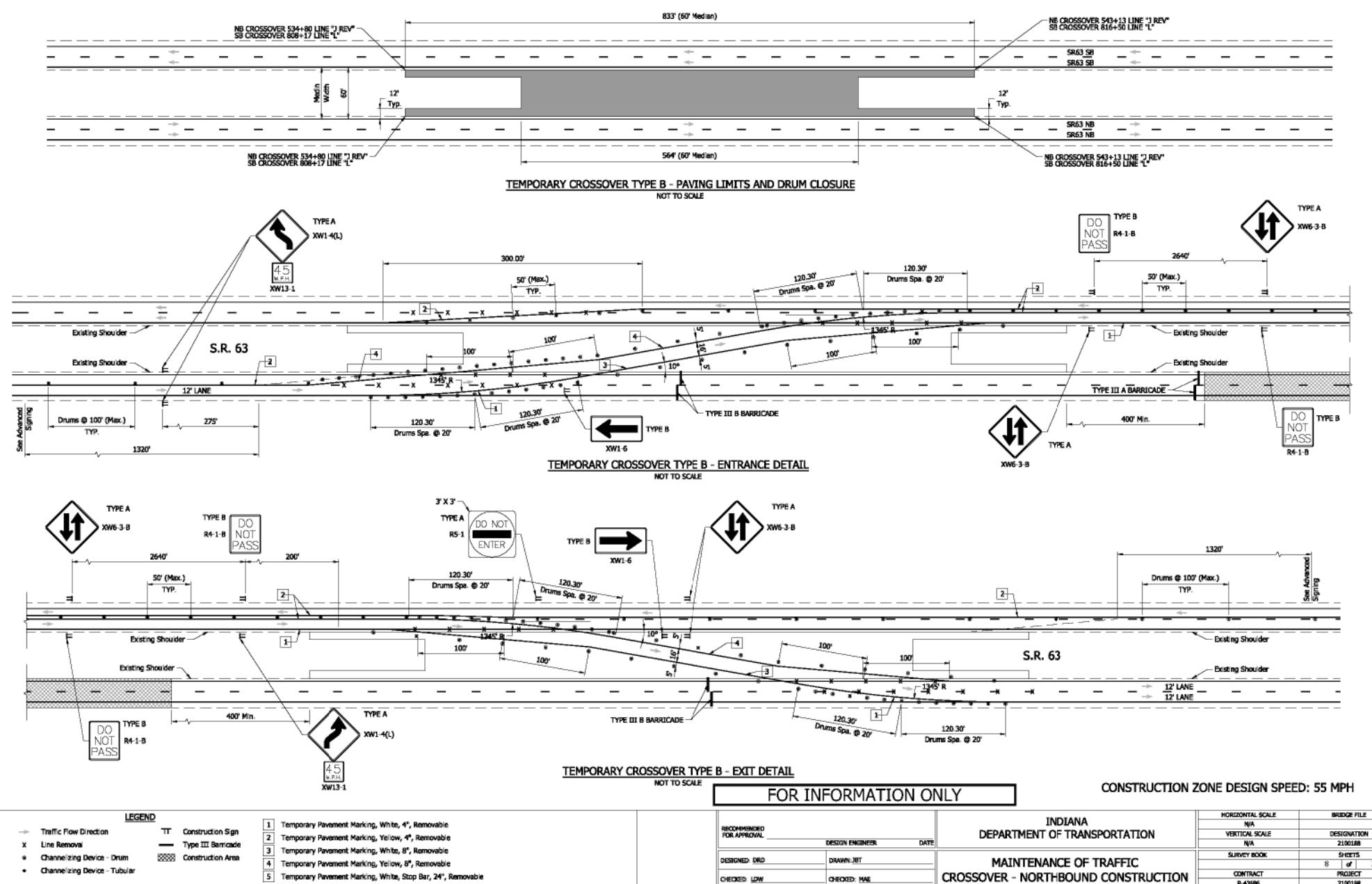
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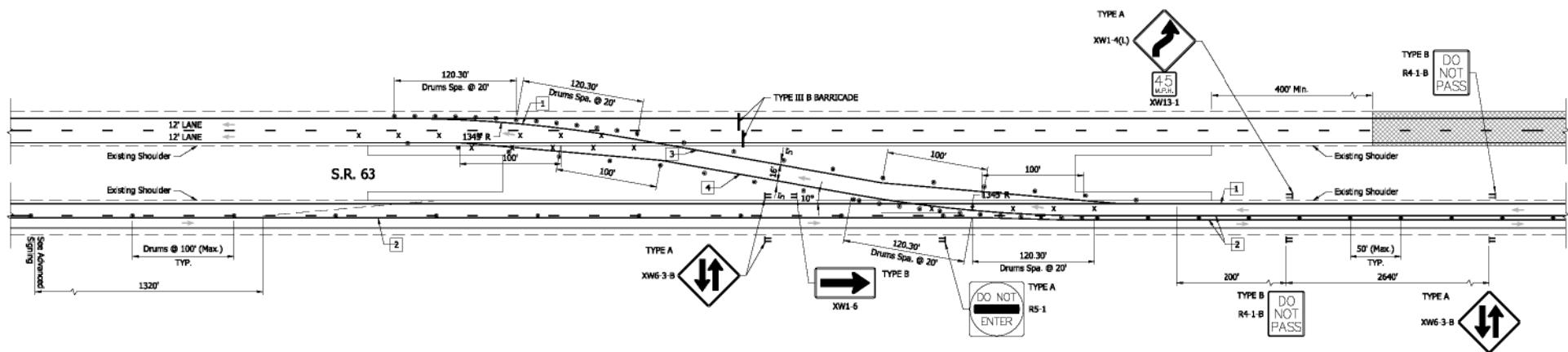
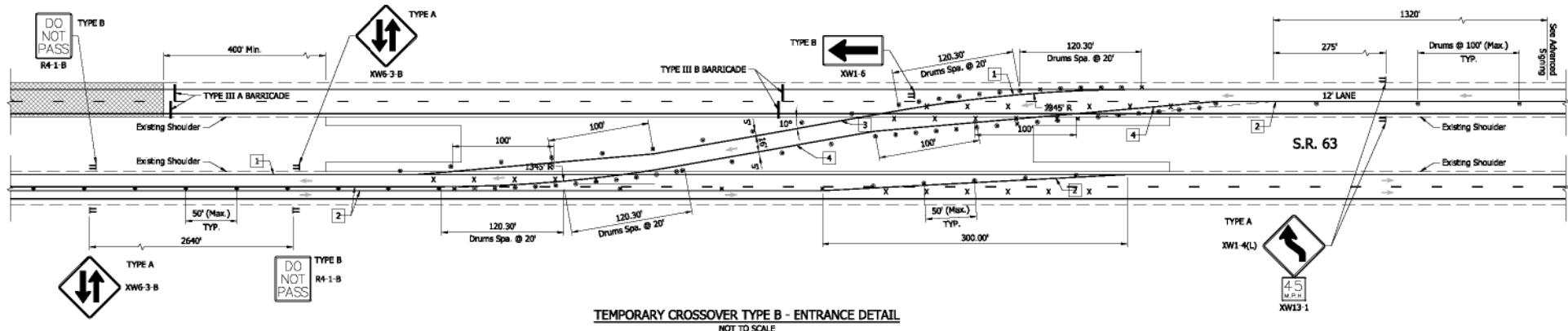
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RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: DRG	DRAWN: JBT	
CHECKED: LDW	CHECKED: MAE	

INDIANA
DEPARTMENT OF TRANSPORTATION

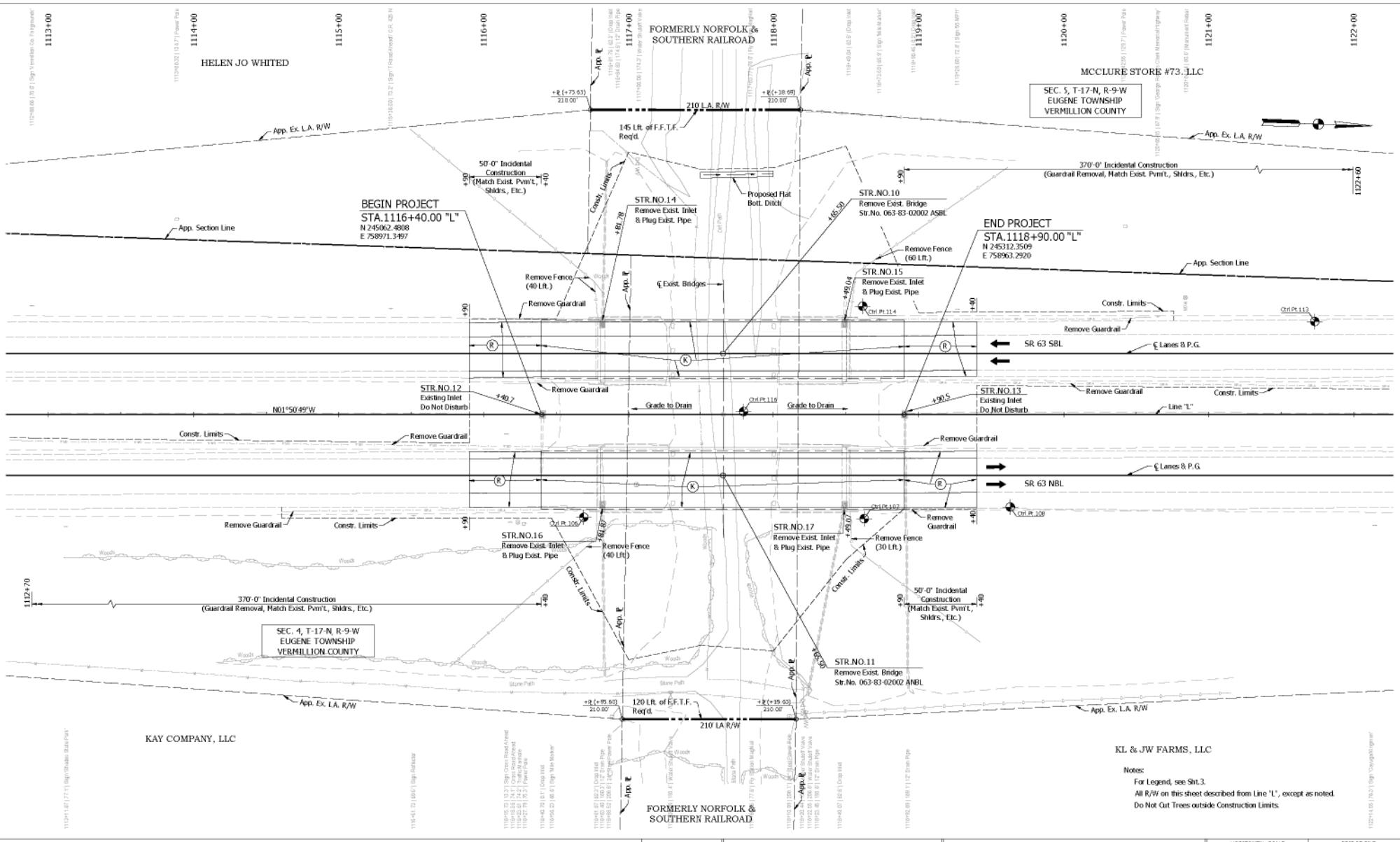
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VERTICAL SCALE	DESIGNATION		
1' = 10'	2100188		
SURVEY BOOK		Sheets	
CONTRACT	7	of	23
DATE			PROJECT
04/19/95			04/06/95





FOR INFORMATION ONLY

CONSTRUCTION ZONE DESIGN SPEED: 55 MPH



KL & JW FARMS, LLC

Not

For legend, see Sh. 3.

All R/W on this sheet described from Line "L", except as noted.
Do Not Cut Trees outside Construction Limits.

INDIANA
DEPARTMENT OF TRANSPORTATION

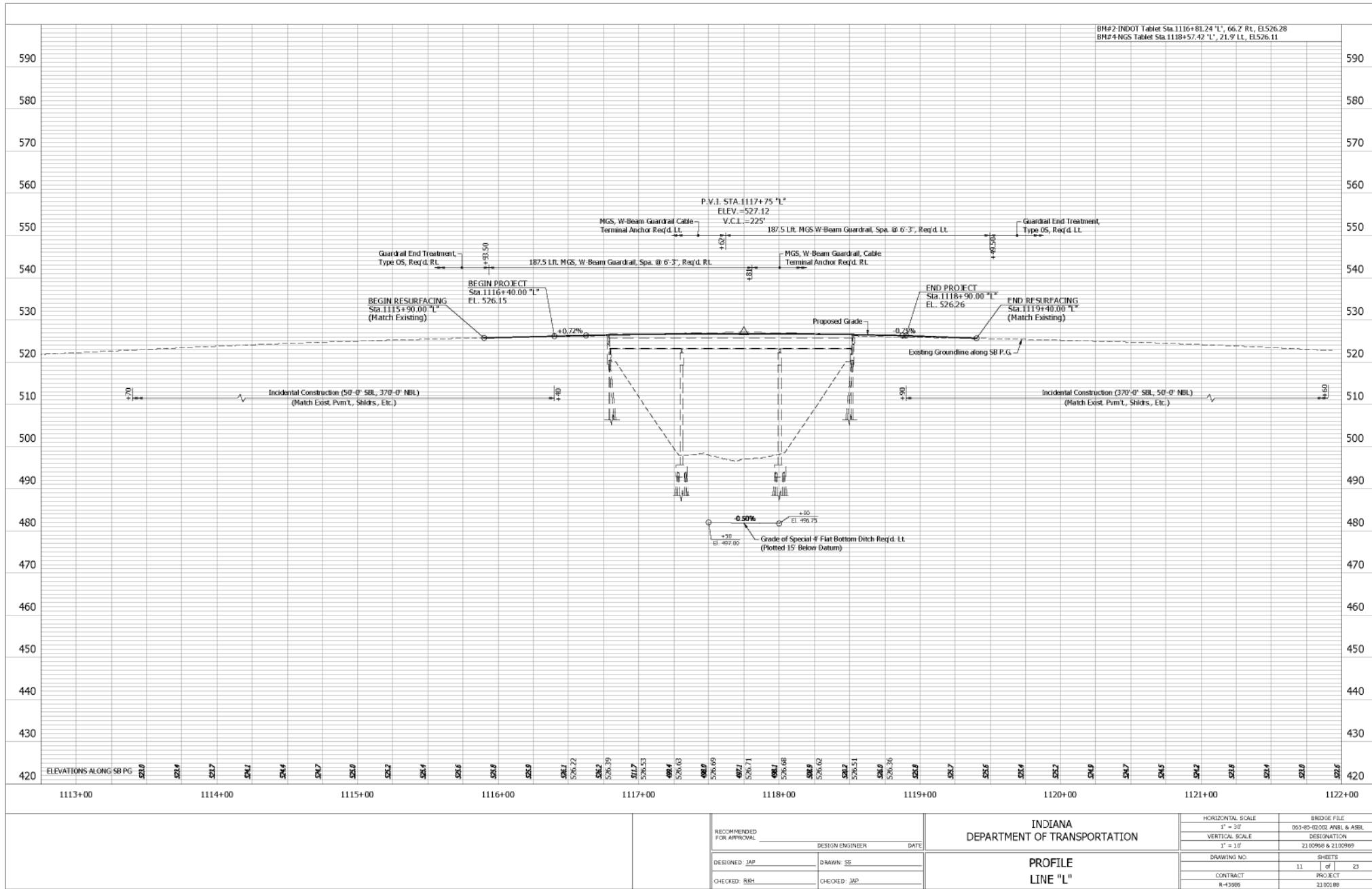
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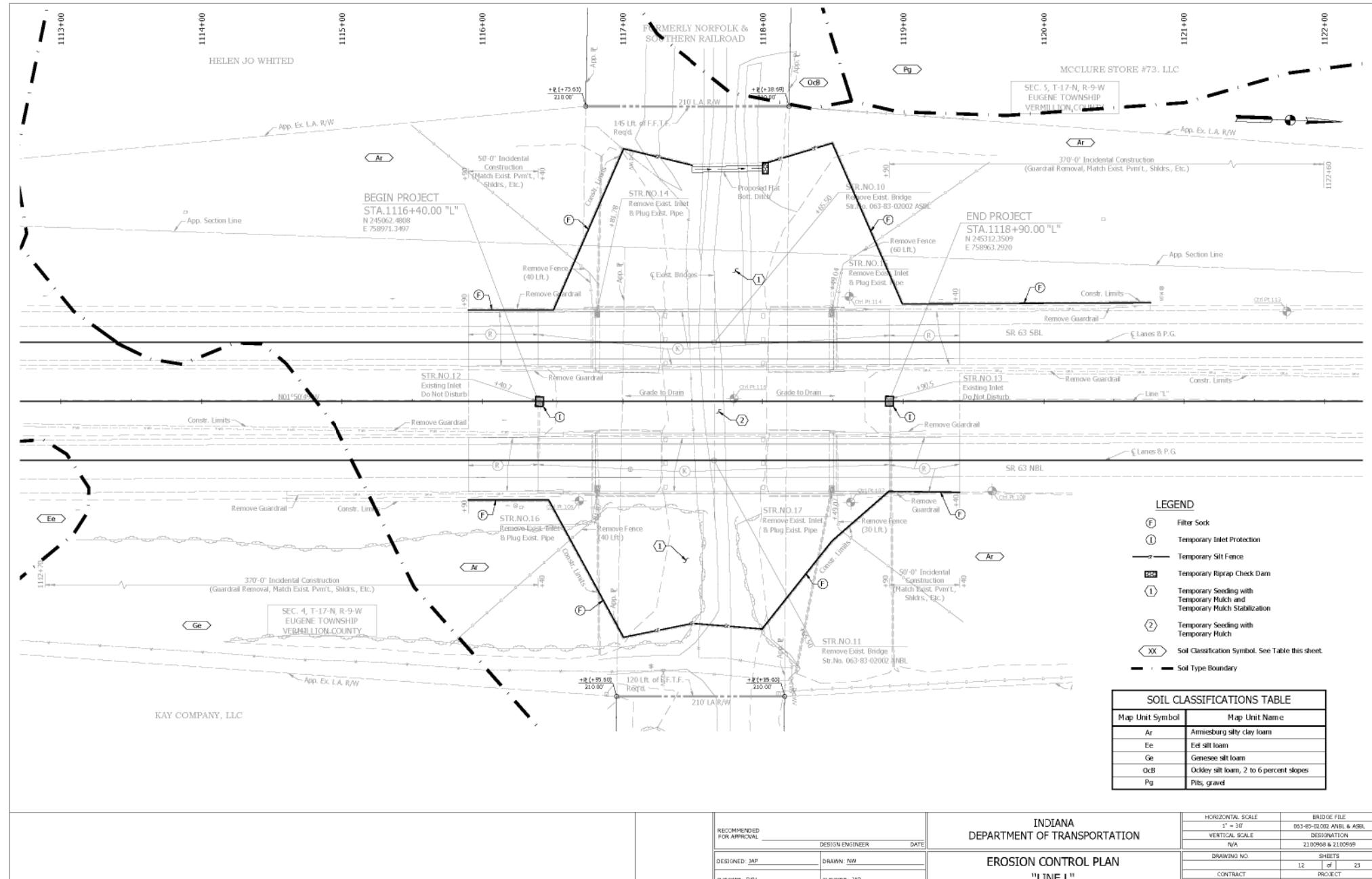
LTNE "I"

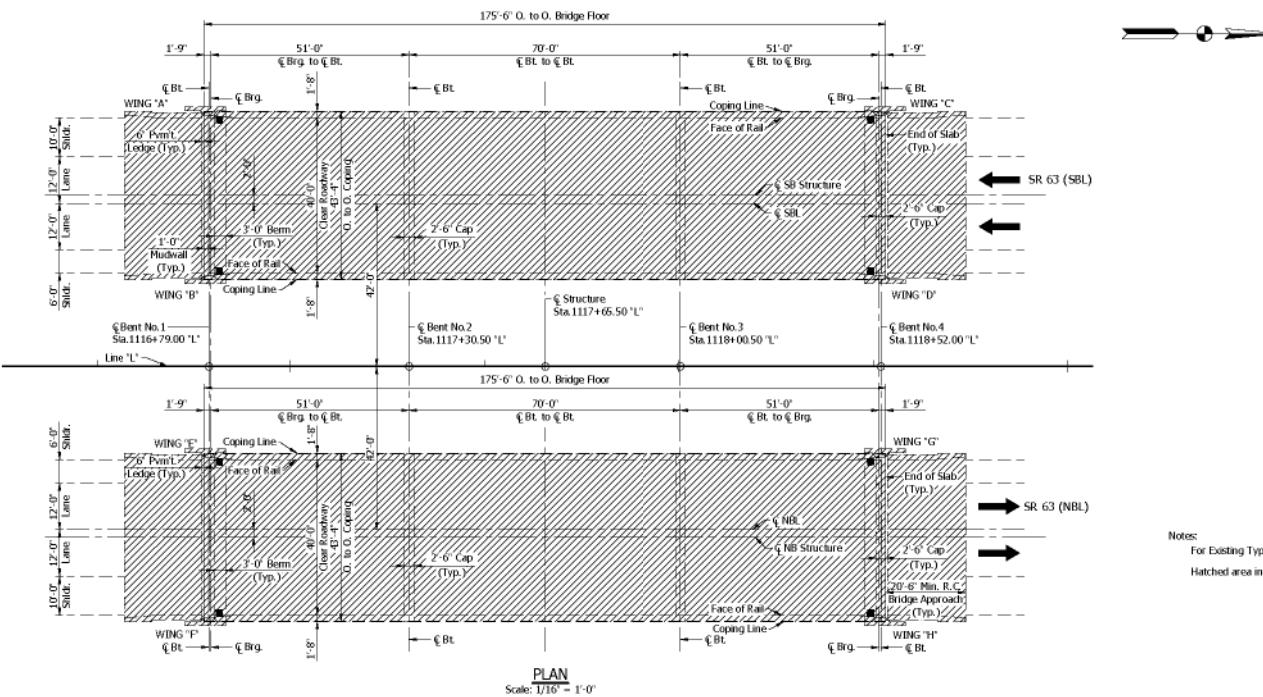
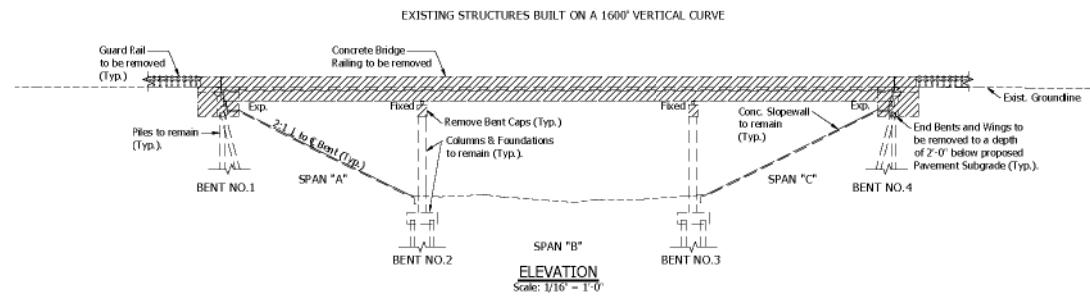
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Bridge N Wing	N Int Cor. Rkt Rkt N Wing	N Int Cor. Rkt Rkt N Wing	N Int Cor. Rkt Rkt N Wing
SR 63 N.E. N Int Cor. Rkt Rkt N Wing			
CONTROL PT #106 N Wing	CONTROL PT #107 N Wing	CONTROL PT #108 N Wing	CONTROL PT #108 N Wing

     	<p>RECOMMENDED FOR APPROVAL</p> <p>DESIGN ENGINEER</p> <p>DESIGNED: JAP DRAWN: SS</p> <p>CHEEDED: RIO CHECKED: JAP</p>

DATE	INDIANA DEPARTMENT OF TRANSPORTATION	HORIZONTAL SCALE 1" = 30'	BRODE FILE 003-00-10001 ANBL & ASBL	
		VERTICAL SCALE 1" = 30'	DESIGNATION 2109068 & 2109099	
PLAN LINE "I"	DRAWING NO.	SHEETS		
	CONTRACT	10	of	23
	CONTRACT	PROJECT		

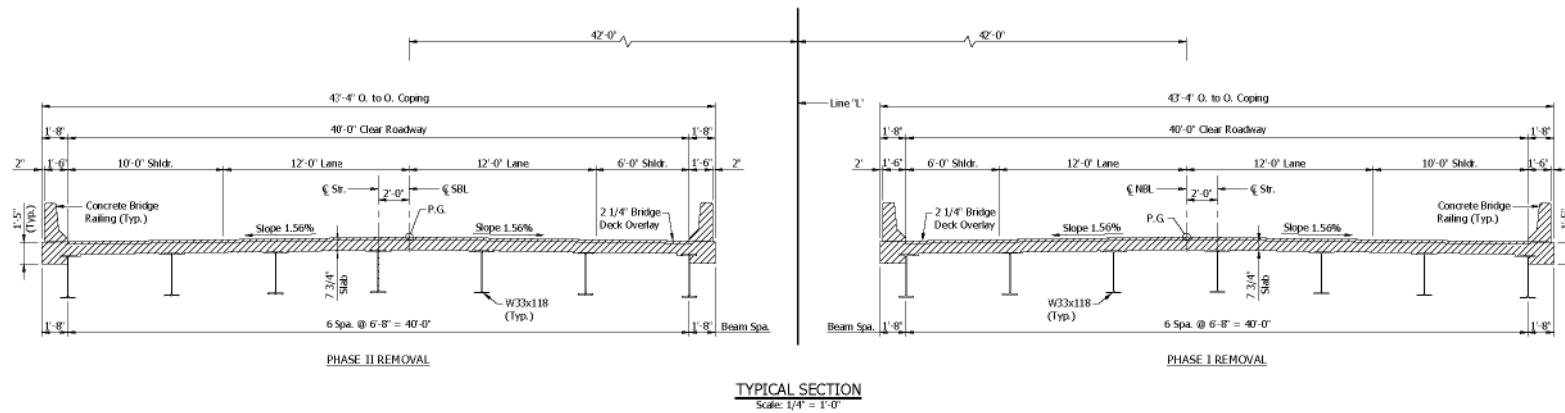






TWIN CONTINUOUS COMPOSITE STEEL BEAM BRIDGE
3 SPANS: 51'-0", 70'-0", 51'-0"
40'-0" CLEAR ROADWAY SKEW: SQUARE
SR 63 OVER ABANDONED RAILROAD
VERMILLION COUNTY

RECOMMENDED FOR APPROVAL _____			INDIANA DEPARTMENT OF TRANSPORTATION		HORIZONTAL SCALE 1/16" = 1'-0"		BRIDGE FILE 005-00-10200 ANBL & ABL		
DESIGN ENGINEER _____ DATE _____					VERTICAL SCALE 1/16" = 1'-0"		DESIGNATION 2310508 & 2310509		
DESIGNED: JAP	DRAWN: NW			GENERAL PLAN EXISTING		DRAWING NO. C1 of C2		SHEETS 13 of 23	
CHECKED: RRB	CHECKED: JAP					CONTRACT B-41198		PROJECT 2310508	



Notes:
For Existing General Plan, see Dwg.C1.
Hatched area indicates portions to be removed.

TWIN CONTINUOUS COMPOSITE STEEL BEAM BRIDGE
3 SPANS: 51'-0", 70'-0", 51'-0"
40'-0" CLEAR ROADWAY SKEW: SQUARE
SR 63 OVER ABANDONED RAILROAD
VERMILLION COUNTY

RECOMMENDED FOR APPROVAL DESIGN ENGINEER _____ DATE _____			INDIANA DEPARTMENT OF TRANSPORTATION GENERAL PLAN TYPICAL SECTION			VERMILLION COUNTY HORIZONTAL SCALE $1/4" = 1-0"$ VERTICAL SCALE $1/4" = 100'$ DRAWING NO. $CS-2$ CONTRACT $B-1899$ PROJECT 2102088		

Appendix C:

Early Coordination



INDIANA DEPARTMENT OF TRANSPORTATION

100 North Senate Avenue
Room N758-ES
Indianapolis, Indiana 46204

PHONE: (317) 694-8284
(855) INDOT4U

Eric Holcomb, Governor
Michael Smith, Commissioner

August 9, 2023

Sample Early Coordination

Early Coordination Agency

Re: Des. Nos. 2100968 & 2100969, Bridge Removal Project on State Road (SR) 63 over Abandoned Railroad, 0.24 mile south of SR 234 in Cayuga, Vermillion County, Indiana

Dear Early Coordination Agency:

The Indiana Department of Transportation (INDOT), with funding from the Federal Highway Administration (FHWA), intends to proceed with a project involving Bridge No. 063-83-02002 ANBL/ASBL (NBI: 022727/022729) in Vermillion County, Indiana. This letter is part of the early coordination phase of the environmental review process. We are requesting comments from your area of expertise regarding any possible environmental effects associated with this project. **Please use the referenced designation numbers and description in your reply.** We will incorporate your comments into a study of the project's environmental impacts.

This project is on SR 63 over an abandoned railroad, 0.24 mile south of SR 234 in Cayuga, Vermillion County, Indiana. The existing structures (063-83-02002 ANBL/ASBL) consist of twin three-span continuous steel beam bridges, 175.5 feet long, originally built in 1976. The bridges had latex-modified concrete (LCM) overlays installed in 1987, the wearing surface was epoxy injected in 2019, and substantial beam and bearing repairs were completed in 2020. The railroad tracks have been removed, and a silt/gravel path exists in the abandoned railroad bed. The need of this project is to address the existing structures worsening deteriorating conditions. In addition, these bridges are nearing the end of their design life. Recent bridge inspection reports noted the structures were in overall "fair" condition (five out of nine). In addition, the October 20, 2021, bridge inspection report for the southbound bridge documented a critical find, resulting in emergency lane closures until repairs could be made. The purpose of this project is to provide structurally sufficient structures to convey traffic on SR 63 over the abandoned railroad.

Two alternatives are being considered. The first is to remove the existing bridges and install a 14-foot span, 13.5-foot rise, three-sided small structure. The proposed structure would be approximately 232 feet long with 2-foot, 6-inch minimum headwalls with precast concrete wingwalls, or mechanically stabilized earth (MSE) retaining walls. These could be used as headwalls and wingwalls to shorten the proposed structure length. SR 63 would be reconstructed on the same horizontal and vertical alignments and would provide two 12-foot travel lanes, one in each direction, bordered by a 4-foot median and 10-foot outside paved shoulders in each direction. This alternative meets the purpose and need, would accommodate a future pedestrian path, and would reduce long-term maintenance and inspection costs.

The second alternative would remove the existing bridges and backfill the railroad crossing with roadway embankment material. SR 63 would be reconstructed on the same horizontal and vertical alignment and would provide two 12-foot travel lanes, one in each direction, bordered by a 4-foot median and 10-foot outside paved shoulders. The existing median and side slopes meet current standards and

would be maintained. This alternative meets the purpose and need of the project; however, it would limit the use of the abandoned railroad corridor in the future.

For both alternatives, the project limits would be approximately 250 feet long, measured 125 feet each way from the centerline of the existing bridges. Pavement within the project limits would be replaced, including the underdrains. Incidental construction limits would extend approximately 300 feet beyond the project limits for each approach. Work in the incidental construction limits would primarily include 50 feet of milling and resurfacing, guardrail removal, and linear grading. Median drainage structures would be reconstructed as required to maintain the existing drainage pattern.

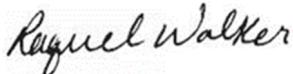
The maintenance of traffic (MOT) is anticipated to utilize phased construction. Temporary crossovers will be constructed south of Maple Street and north of SR 234. Traffic in each direction will be reduced to a single lane, and the southbound SR 63 traffic will be shifted onto the northbound SR 63 roadway. The northbound roadway will carry one 11.5-foot lane in each direction, separated by flexible tubular markers and lane separators. The existing shoulder widths, 4 foot left and 10 foot right, will be maintained. All construction for southbound SR 63 will be completed and the process reversed for northbound SR 63 construction. MOT is anticipated to be in place for approximately 12 months. Approximately 0.5 acre of permanent right-of-way acquisition may be needed. In addition, tree clearing may be necessary. Construction is anticipated to begin in Spring of 2026.

Land use in the vicinity is primarily wooded and agricultural. No publicly owned parks, other recreational facilities or wildlife refuges that would be afforded protection under Section 4(f) have been identified in the immediate project area. No potential Section 6(f) sites were identified within the project area.

BLN will prepare a Waters of the U.S. Report (WOUSR) including wetland determinations as appropriate. The Waters of the U.S. Report will be reviewed by the INDOT Ecology and Waterway Permitting Office. The project is anticipated to qualify for the Rangewide Programmatic Agreement for the Indiana bat and Northern Long-eared bat by completing the Information for Planning and Consultation (IPaC). In addition, BLN will have Qualified Professionals (QPs) investigate the project area for archaeological and historic resources for compliance with Section 106 of the National Historic Preservation Act (NHPA). The results of this investigation will be forwarded to the Indiana State Historic Preservation Officer (IN SHPO) for review and concurrence.

Please provide your response within thirty (30) calendar days from the date of this letter. However, should you find that an extension to the response time is necessary, a reasonable amount may be granted upon request. If you have questions regarding this matter, please get in touch with Jessica Miller, INDOT Project Manager, at jemiller1@indot.in.gov or 765) 361-5224 or Raquel Walker at rwalker@b-l-n.com or 317-558-7546. Thank you for your cooperation.

Sincerely,



Raquel Walker
Senior Environmental Analyst
Beam, Longest, and Neff, LLC

Attachments:

Mailing List
Maps
Ground-Level Photographs

Attachments have been
removed to avoid duplication

EARLY COORDINATION MAILING LIST

Federal Highway Administration Federal Office Building, Room 254 575 North Pennsylvania Street Indianapolis, IN 46204 Electronic Coordination: k.carmanyeorge@dot.gov	Zane Kurtz, Environmental Section Manager INDOT, Crawfordsville District 41 West 300 North Crawfordsville, IN 47933 Electronic Coordination: rkurtz@indot.in.gov
Indiana Geological and Water Survey 611 North Walnut Grove Bloomington, IN 47405 Electronic Coordination: https://igws.indiana.edu/eAssessment/	Jessica Miller, Project Manager INDOT, Crawfordsville District 41 West 300 North Crawfordsville, IN 47933 Electronic Coordination: jemiller1@indot.in.gov
Environmental Coordinator Indiana Department of Natural Resources Division of Fish and Wildlife 402 West Washington Street, Room W273 Indianapolis, IN 46204 Electronic Coordination: environmentalreview@dnr.in.gov	Chief, Groundwater Section Indiana Department of Environmental Management 100 North Senate Avenue Indianapolis, IN 46204 IDEM's Wellhead Proximity Determinator Website: www.in.gov/idem/cleanwater/pages/wellhead/
Field Supervisor US Fish and Wildlife Service Bloomington Indiana Field Office 620 South Walker Street Bloomington, Indiana 47403-2121 Electronic Coordination: robin_mcwilliams@fws.gov	Vermillion County Council 255 South Main Street Newport, IN 47966 Electronic Coordination: amy.tolbert@vermillioncounty.in.gov , brenda.furry@vermillioncounty.in.gov
Regional Environmental Coordinator National Park Service, Midwest Regional Office 601 Riverfront Drive Omaha, NE 68102 Electronic Coordination: mwro_compliance@nps.gov	State Conservationist Natural Resources Conservation Service 6013 Lakeside Boulevard Indianapolis, IN 46278 Electronic Coordination: john.allen@usda.gov
Field Environmental Officer Chicago Regional Office U.S. Department of Housing and Urban Development Metcalf Federal Building 77 West Jackson Blvd, Room 2401 Chicago, IL 60604 Electronic Coordination: erik.r.sandstedt@hud.gov	Vermillion County Commissioner's Po Box 190 Newport, IN 47966 Electronic Coordination: tim.yocum@vermillioncounty.in.gov ; ronalddunavan@gmail.com ; britton.luther@vermillioncounty.in.gov
Cayuga Fire Department 103 S Logan Street Cayuga, IN 47928 Electronic Coordination: cayugafire@sbcglobal.net	Vermillion County Sheriff's Office 1888 South State Road 63 Hillsdale, IN. 47854 Electronic Coordination: sheriff@vcsheriff.com
Ronald A. Mack, Vermilion County Surveyor Vermillion County Courthouse, Room 206 Main Street, P.O. Box 280225 Newport, IN 47966 Electronic Coordination: ronald.mack@vermillioncounty.in.gov	Penney Carpenter, Executive Director Vermillion County Zoning 255 South Main Street Newport, Indiana 47966 Electronic Coordination: penney.carpenter@vermillioncounty.in.gov
Mark O'Heir, Director Vermillion County Emergency Management 259 Vine Street Clinton, IN 47842 Electronic Coordination: mark.oheir@vermillioncounty.in.gov	Vermillion County Economic Development Council 703 West Park Street Cayuga, IN 47928 Electronic Coordination: douglas@vermillioncountyedc.com
Cayuga Water Department 301 South First Street, P.O. Box 33, Cayuga, IN 47928 Electronic Coordination: townofcayuga@sbcglobal.net	City of Georgetown, Water Utilities 208 South Walnut Street Georgetown, IL 61846 Electronic Coordination: gtownwaterandsewer@gmail.com
Wabash River Heritage Corridor Commission (WHRCC) Vermillion County Electronic Coordination: zimmerman.les@gmail.com , larrysouthard@att.net	Vermillion Trails Alliance 703 West Park Street Cayuga, Indiana 47928 Electronic Coordination: vermilliontrails@gmail.com

Organization and Project Information

Project ID: 220083
Des. ID: 2100968 & 2100969
Project Title: SR 63 over Abandoned Railroad
Name of Organization: Beam, Longest and Neff (BLN)
Requested by: Raquel Walker

Environmental Assessment Report

1. Geological Hazards:

- High liquefaction potential
- Floodway

2. Mineral Resources:

- Bedrock Resource: High Potential
- Sand and Gravel Resource: High Potential

3. Active or abandoned mineral resources extraction sites:

- None documented in the area

*All map layers from Indiana Map (maps.indiana.edu)

DISCLAIMER:

This document was compiled by Indiana University, Indiana Geological Survey, using data believed to be accurate; however, a degree of error is inherent in all data. This product is distributed "AS-IS" without warranties of any kind, either expressed or implied, including but not limited to warranties of suitability to a particular purpose or use. No attempt has been made in either the design or production of these data and document to define the limits or jurisdiction of any federal, state, or local government. The data used to assemble this document are intended for use only at the published scale of the source data or smaller (see the metadata links below) and are for reference purposes only. They are not to be construed as a legal document or survey instrument. A detailed on-the-ground survey and historical analysis of a single site may differ from these data and this document.

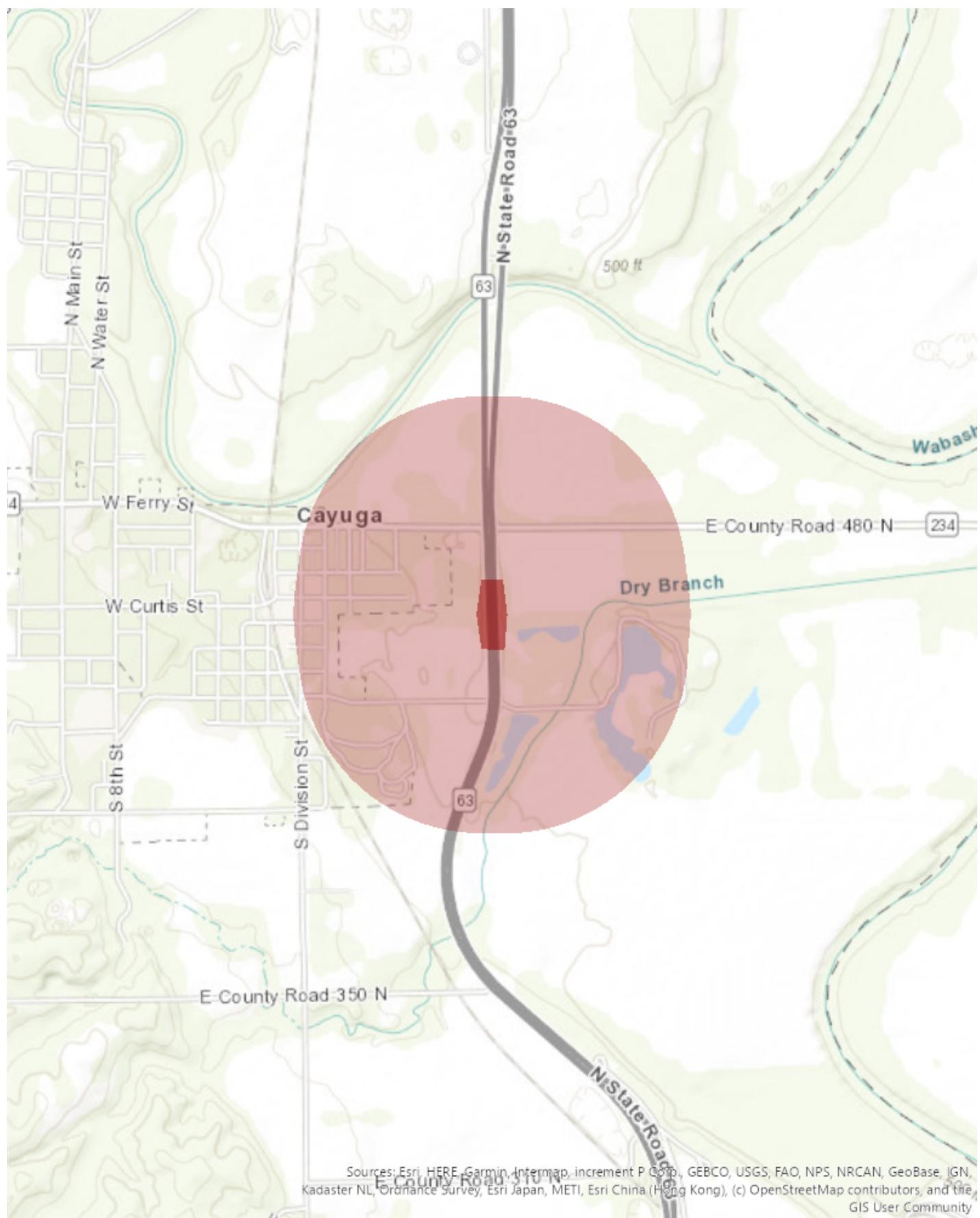
This information was furnished by Indiana Geological Survey

Address: 1001 E. 10th St., Bloomington, IN 47405

Email: IGSEnvir@indiana.edu

Phone: 812 855-7428

Date: August 09, 2023



THIS IS NOT A PERMIT

**State of Indiana
DEPARTMENT OF NATURAL RESOURCES
Division of Fish and Wildlife
Early Coordination/Environmental Assessment**

DNR#: ER-25856

Request Received: August 9, 2023

Requestor:

Raquel Walker
Beam, Longest & Neff, LLC
8320 Craig Street
Indianapolis, IN 46250

Project:

SR 63 bridge (#063-83-02002 ANBL/ASBL / NBI 022727/022729) removal and alternative replacement (small structure installation or backfilling with roadway embankment material) over UNT Dry Branch & an abandoned railroad, 0.24 miles south of SR 234, Cayuga; Des #2100968 & 2100969

County/Site Info: Vermillion County

The Indiana Department of Natural Resources has reviewed the above referenced project per your request. Our agency offers the following comments for your information and in accordance with the National Environmental Policy Act of 1969.

If our agency has regulatory jurisdiction over the project, the recommendations contained in this letter may become requirements of any permit issued. If we do not have permitting authority, all recommendations are voluntary.

Regulatory Assessment:

Formal approval by the Department of Natural Resources under the regulatory programs administered by the Division of Water is not required for this project.

Natural Heritage Database:

The Natural Heritage Program's data have been checked. To date, no plant or animal species listed as state or federally threatened, endangered, or rare have been reported to occur in the project vicinity.

Fish and Wildlife Comments:

Avoid and minimize impacts to fish, wildlife, and botanical resources to the greatest extent possible, and compensate for impacts. The following are recommendations that address potential impacts identified in the proposed project area:

A) Wildlife Passage

The existing bridges, while no longer required for carrying SR 63 over the abandoned railroad, are likely providing wildlife passage under the roadway. Specifically, the structures may be providing passage for white-tailed deer (*Odocoileus virginianus*). The Division of Fish and Wildlife (DFW) is concerned that removing the structures may potentially force deer up onto the roadway. Wildlife/vehicle collisions are very costly in terms of injury, loss of life and damage to property. The DFW has made addressing wildlife passage issues a priority and encourages INDOT at a minimum to collect data on wildlife use of the existing structures to determine if maintaining deer passage should be considered. If deer passage is evident, we recommend exploring opportunities to rehabilitate the existing bridges or design replacement structures that are smaller than the existing bridges but still accommodate white-tailed deer passage. Minimum structure dimensions for white-tailed deer passage are 20 feet of width clearance (overall span of the structure) and 8 feet of height

clearance. If deer passage is not evident but small animal passage is, a relatively small box or pipe culvert could be used to maintain wildlife passage under the roadway. The DFW is actively looking into ways to support INDOT on projects of this nature and would like to encourage continued dialogue and open communication on these issues. In the interim, there are potential funding sources for wildlife/vehicle collision reduction projects that may help with data collection, planning, design, rehabilitation and/or replacement costs for the existing structures on the Federal Highway Administration website:

https://www.fhwa.dot.gov/bipartisan-infrastructure-law/wildlife_crossings_pilot_program_fact_sheet.cfm

The following are links to general information on roadways and wildlife crossing issues:

<https://www.fs.usda.gov/wildlifecrossings/library/index.php>

https://www.fhwa.dot.gov/clas/ctip/wildlife_crossing_structures/

B) Pavement Rehabilitation

Pavement rehabilitation projects typically do not have a significant impact on fish, wildlife, and botanical resources if best management practices (BMPs) are in place to limit the migration of polycyclic aromatic hydrocarbons (PAHs) into local waterways. PAHs are a byproduct of asphalt and coal tar-based sealants and negatively impact aquatic systems. The use of sealants that are free of petroleum and coal tar-based products is encouraged whenever possible. Contaminated road runoff can significantly impact the aquatic environment through increased turbidity and release of sediment into the stream which can be harmful to fish and other aquatic organisms, their eggs, and their food supply. Where possible, road runoff should be directed to riprap turnouts and sediment filtration prior to entering a stream to reduce impacts to aquatic species. We recommend the use of pollutant trapping technology such as storm drain inserts to reduce the runoff of roadside pollutants where appropriate.

C) LED Lighting

The need for new lighting was not mentioned in the submitted information but could potentially be needed in certain areas. A replacement structure was discussed as an alternative to accommodate a future pedestrian trail. Lighting the structure for pedestrian safety would not be compatible with wildlife passage. A separate structure for wildlife passage or leaving the replacement structure unlit would be recommended. Most transportation corridor designers and municipalities are trending toward LED lighting. Certain types of LED lighting can have negative impacts on both human and wildlife health and safety. The DFW strongly encourages visiting the International Dark-Sky Association's website to learn more about the potential negative impacts of improperly selected LED lighting systems, if required: <https://www.darksky.org/our-work/lighting/lighting-for-citizens/led-guide/>

D) Nesting Birds/Roosting Bats

Replacement of the bridges could affect nesting birds or roosting bats. Cliff and Barn Swallows, among other species, often nest on the underside of road bridges and many bat species roost in expansion joints and other concrete crevices on road bridges. Survey the bridges for any bird nests prior to construction. Nest surveys should occur between May 7 and September 7, which denotes the main nesting season for most bird species. If nests are found with eggs, chicks, or parents actively attending to the nest (building the nest and visiting often), then repairs should be put on hold until the nests complete their nesting cycle (to fledging) or fail (by natural causes).

The DFW recommends bridge replacement activities be restricted to the period between November 1 and March 1 to avoid the summer roosting period for most bats in the central part of the State. However, some endangered bats could use a bridge to roost between November and March. No matter when work is proposed, the bridge must be inspected for the presence of bats. If there is no evidence of active bat use, work can proceed. If there is evidence of active bat use, work must not occur until either the bats leave the structure for the season, or a separate permit is issued to remove the bats. Please contact Linnea Petercheff (lpetercheff@dnr.in.gov) regarding permits to handle bats. If bats are present, a more formal survey to determine what species are present may be required.

The DFW recommends consulting with the State Mammologist or the US Fish and Wildlife Service before scheduling a bridge maintenance, repair, or replacement project where evidence of bat use of the structure has been observed.

Information about bat use of transportation structures as well as avoidance and exclusion measures can be found at <https://www.batcon.org/pdfs/bridges/BatsBridges2.pdf> and <https://www.whitenosesyndrome.org/mmedia-education/acceptable-management-practices-for-bat-species-inhabiting-transportation-infrastructure>.

E) Tree Removal Outside a Floodway

The DFW recommends avoiding removing trees along roadways and trails to the greatest extent possible and replacing trees that must be removed. Trees along roadways and trails are important to fish and wildlife resources in urban and rural areas. The following links give a good overview of the benefits of a tree replacement program and how to select the right species to avoid the negative impacts of non-native invasive species such as Bradford pear: <https://www.in.gov/dnr/forestry/forestry-publications-and-presentations/> (scroll down to the Community & Urban Forestry section).

The additional measures listed below should be implemented to avoid, minimize, or compensate for impacts to fish, wildlife, and botanical resources:

1. Revegetate all bare and disturbed areas that are not currently mowed and maintained with a mixture of grasses, sedges, and wildflowers native to Central Indiana as soon as possible upon completion; turf-type grasses (including low-endophyte, friendly endophyte, and endophyte free tall fescue but excluding all other varieties of tall fescue) may be used in currently mowed areas only. A native herbaceous seed mixture must include at least 5 species of grasses and sedges and 5 species of wildflowers.
2. Do not cut any trees suitable for Indiana Bat or Northern Long-eared Bat roosting (3 inches or greater diameter-at-breast height, living or dead, with loose hanging bark, or with cracks, crevices, or cavities) from April 1 through September 30.
3. Appropriately designed measures for controlling erosion and sediment must be implemented to prevent sediment from entering the waterbody or leaving the construction site; maintain these measures until construction is complete and all disturbed areas are stabilized.
4. Seed and protect all disturbed streambanks and slopes not protected by other methods that are 3:1 or steeper with erosion control blankets that are heavy-duty, biodegradable, and net free or that use loose-woven / Leno-woven netting to minimize the entrapment and snaring of small-bodied wildlife such as snakes and turtles (follow manufacturer's recommendations for selection and installation); seed and apply mulch on all other disturbed areas.
5. Plant five trees, 1 inch to 2 inches in diameter-at-breast height, for each tree which is removed that is 10 inches or greater in diameter-at-breast height.

Contact Staff:

Our agency appreciates this opportunity to be of service. Please contact me at RVanVoorhis@dnr.IN.gov or (317) 232-8163 if we can be of further assistance.

Rachel Van Voorhis

Rachel Van Voorhis
Environmental Coordinator
Division of Fish and Wildlife

Date: September 8, 2023

From: [Ronald Mack](#)
To: [Raquel Walker](#)
Cc: [Carmany-George, Karstin \(FHWA\)](#); [Kurtz, Randy](#); [Miller, Jessica](#); [DNR Environmental Review](#); [mwro_compliance@nps.gov](#); [erik.r.sandstedt@hud.gov](#); [cayugafire@sbcglobal.net](#); [mark.oheir@vermillioncounty.in.gov](#); [townofcayuga@sbcglobal.net](#); [zimmerman.les@gmail.com](#); [larrysouthard@att.net](#); [amy.tolbert@vermillioncounty.in.gov](#); [brenda.furry@vermillioncounty.in.gov](#); [tim.yocum@vermillioncounty.in.gov](#); [ronalddunavan@gmail.com](#); [britton.luther@vermillioncounty.in.gov](#); [sheriff@vcsheriff.com](#); [penney.carpenter@vermillioncounty.in.gov](#); [douglas@vermillioncountycitydc.com](#); [gtownwaterandsewer@gmail.com](#); [vermilliontrails@gmail.com](#); [Brian Shaw](#); [Jeff Parke](#)
External Email
Subject: Re: Early Coordination Letter - Des 2100968 & 2100969 - State Road (SR) 63 over Abandoned Railroad, Vermillion County, Indiana
Date: Thursday, August 10, 2023 11:20:06 AM
Attachments: [Capture - NE 8, ND 7,8,9.JPG](#)

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Madam:

I don't see that the Vermillion County Surveyor's office has any conflict with the proposed work. With that said, if during the design it appears that the project length might be expanded, we have reason to believe that errors in properly monumenting the correct locations of Section Corners, NE8, ND 7, 8, 9 have been made and so would welcome the opportunity to work with contractors to fix it. Please see attached map.

Thank you,

Ronald A. Mack

Vermillion County Surveyor

Vermillion County Courthouse - Rm. 206

P.O. Box 280

225 Main Street

Newport, IN 47966

Ph: 765-492-5366

Email: ronald.mack@vermillioncounty.in.gov

The price Agriculture pays for indifference to politics is to be controlled by Ignorance!

An environment where disagreement is not allowed is focused on control - not growth!

Thomas Payne -

"It is the duty of every man, as far as his ability extends, to detect and expose delusion and error."

"A body of men, holding themselves accountable to nobody, ought not to be trusted by anybody."

"He who dares not offend cannot be Honest."

This communication is deemed CONFIDENTIAL by the sender and the information herein is intended only for review by those it is addressed to. If you are not the intended receiver, you are ordered to notify the sender immediately and to destroy this document, all electronic addressing, and any or all related information under statute IC 5-14-3-10 Classified confidential information; unauthorized disclosure or failure to protect; offense; discipline Sec. 10. (a) A public employee, a public official, or an employee or officer of a contractor or subcontractor of a public agency, except as provided by IC 4-15-10, who knowingly or intentionally discloses information classified as confidential by state statute, including information declared confidential under: (1) section 4(a) of this chapter; or (2) section 4(b) of this chapter if the public agency having control of the information declares it to be confidential; commits a Class A infraction.

On Wed, Aug 9, 2023 at 11:18 AM Raquel Walker <rwalker@b-l-n.com> wrote:

Hello,

The Indiana Department of Transportation (INDOT), with funding from the Federal Highway Administration (FHWA), intends to proceed with a bridge removal project on SR 63 in Vermillion County. Please see the attached early coordination letter with information regarding this project.

We are requesting comments from your area of expertise regarding any possible environmental effects associated with this project. If you would like to respond, we request that you do so within 30 calendar days of the date of this letter. Please use the referenced designation number and description in your reply.

Feel free to reach out if you have any questions or need additional information.

Thank you,

RAQUEL WALKER

Senior Analyst
Environmental Services
o: [317-558-7546](tel:317-558-7546)
B-L-N.COM

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Egis GROUP
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From: [Town of Cayuga](#)
To: [Raquel Walker](#)
Subject: Fwd: Early Coordination Letter - Des 2100968 & 2100969 - State Road (SR) 63 over Abandoned Railroad, Vermillion County, Indiana
Date: Wednesday, August 9, 2023 12:51:03 PM

External Email

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Good Afternoon,

I am the new clerk for the town of Cayuga. Please utilize this email for communications to the town going forward :)

Thank you so much!

Thank you,
Briana Noggle

Begin forwarded message:

From: Ronald Dunavan <ronalddunavan@gmail.com>
Date: August 9, 2023 at 12:42:45 PM EDT
To: Briana Noggle <bree2609@rocketmail.com>, Sarah Hathaway <slewis8807@yahoo.com>
Subject: Fwd: Early Coordination Letter - Des 2100968 & 2100969 - State Road (SR) 63 over Abandoned Railroad, Vermillion County, Indiana

Bree here is email if you didn't receive it.

Thank you,
Rj Dunavan

Begin forwarded message:

From: Raquel Walker <rwalker@b-l-n.com>
Date: August 9, 2023 at 11:17:43 AM EDT
To: "Carmany-George, Karstin (FHWA)" <k.carmanygeorge@dot.gov>, "Kurtz, Randy" <RKurtz@indot.in.gov>, "Miller, Jessica" <JeMiller1@indot.in.gov>, DNR Environmental Review <environmentalreview@dnr.in.gov>, mwro_compliance@nps.gov, Erik.R.Sandstedt@hud.gov, cayugafire@sbcsglobal.net, ronald.mack@vermillioncounty.in.gov, mark.oheir@vermillioncounty.in.gov, townofcayuga@sbcglobal.net, zimmerman.les@gmail.com, larrysouthard@att.net, amy.tolbert@vermillioncounty.in.gov,

Brenda.furry@vermillioncounty.in.gov,
tim.yocum@vermillioncounty.in.gov, ronalddunavan@gmail.com,
britton.luther@vermillioncounty.in.gov, Sheriff@vcsheriff.com,
penney.carpenter@vermillioncounty.in.gov,
douglas@vermillioncountyedc.com,
gtownwaterandsewer@gmail.com, vermilliontrails@gmail.com
Cc: Brian Shaw <bshaw@b-l-n.com>, Jeff Parke <jparke@b-l-n.com>
Subject: Early Coordination Letter - Des 2100968 & 2100969 -
State Road (SR) 63 over Abandoned Railroad, Vermillion
County, Indiana

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Feel free to reach out if you have any questions or need additional information.

Thank you,

RAQUEL WALKER

Senior Analyst
Environmental Services
o: [317-558-7546](tel:317-558-7546)
B-L-N.COM

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<Early Coordination Letter - Des 2100968 & 2100969 .pdf>

From: [RJ Dunavan](#)
To: [Raquel Walker](#); jemiller1@indot.in.gov
Cc: [Debbie Calder](#)
Subject: Re: Early Coordination Letter - Des 2100968 & 2100969 - State Road (SR) 63 over Abandoned Railroad, Vermillion County, Indiana
Date: Wednesday, August 9, 2023 12:37:24 PM
Attachments: [Early Coordination Letter - Des 2100968 & 2100969 .pdf](#)

External Email

CAUTION: This email originated from outside your organization. Exercise caution when opening attachments or clicking links, especially from unknown senders.

Good Afternoon!

First I'm thankful for the early coordination letter, however, this was sent to my personal email address, is there anyway all future correspondence can be sent to this email address I'm replying from. My Vermillion County email address, I'm not sure if maybe the county provided my personal or if the state used it.

I do have several comments/questions and wish to talk to someone recommending the two options for state road 63, if I can have the persons contact information, I'd love to ask some questions over the phone. Last meeting I had has a Commissioner with INDOT it was offered the County had to pay for this concrete box under the road, until this letter was sent, that was my understanding. I'm very appreciative of the change of heart and direction, I'm just curious on what changed. I greatly appreciate it.

Some rather quick comments, will any traffic be diverted to the Town of Cayuga? I've heard the state is restricting a certain amount of weight like over 84,000 pounds just seek alternative route and they are being sent they Cayuga? Cayuga has weight limits and there is no way their roads can handle trucks over that weight if indeed the state is going to restrict weight on 63. Traffic should be re routed elsewhere, 74/41 or 63/36.

Furthermore, as a County Commissioner, if it is not scheduled, I'd like to officially request a public hearing be done, so the public can offer the same input, and or comments as us elected officials. I'm unsure if one has been scheduled or is planned to be done. It is very important to our community and myself as a leader, that public hearings be conducted before any major project is done. I look forward to hearing from someone on this matter.

Thank you, respectfully
Vermillion County Commissioner,
Rj Dunavan

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RAQUEL WALKER

Senior Analyst
Environmental Services
o: [317-558-7546](tel:317-558-7546)
B-L-N.COM

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Katie Finney

Subject: FW: Early Coordination Letter - Des 2100968 & 2100969 - State Road (SR) 63 over Abandoned Railroad, Vermillion County, Indiana

From: Raquel Walker

Sent: Wednesday, August 9, 2023 2:23 PM

To: RJ Dunavan <rj.dunavan@vermillioncounty.in.gov>

Cc: jemiller1@indot.in.gov; Debbie Calder <dcalder@indot.in.gov>; Jeff Parke <jparke@b-l-n.com>; Brian Shaw <bshaw@b-l-n.com>

Subject: RE: Early Coordination Letter - Des 2100968 & 2100969 - State Road (SR) 63 over Abandoned Railroad, Vermillion County, Indiana

Hi RJ,

Thank you for your response. We're reaching out to the INDOT District to get answers and responses to your questions and concerns and will be in touch with you soon.

Thank you,

RAQUEL WALKER

Senior Analyst

Environmental Services

o: 317-558-7546

B-L-N.COM

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From: RJ Dunavan <rj.dunavan@vermillioncounty.in.gov>

Sent: Wednesday, August 9, 2023 12:34 PM

To: Raquel Walker <rwalker@b-l-n.com>; jemiller1@indot.in.gov

Cc: Debbie Calder <dcalder@indot.in.gov>

Subject: Re: Early Coordination Letter - Des 2100968 & 2100969 - State Road (SR) 63 over Abandoned Railroad, Vermillion County, Indiana

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Thank you,

RAQUEL WALKER
Senior Analyst
Environmental Services
o: 317-558-7546
B-L-N.COM

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United States
Department of
Agriculture

Farm
Production
and
Conservation

Natural
Resources
Conservation
Service

Indiana State Office
6013 Lakeside Boulevard
Indianapolis, Indiana 46278
317-295-5800

January 29, 2024

Katie Finney
Beam, Longest and Neff, L.L.C.
8320 Craig Street
Indianapolis, Indiana 46250

Dear Ms. Finney:

The proposed Bridge Removal on SR 63 over Abandoned Railroad in Vermillion County, Indiana (Des. No. 2100968 & 2100969), as referred to in your letter received on August 9, 2023, will cause a conversion of prime farmland.

The attached packet of information is for your use competing Parts VI and VII of the AD-1006. After completion, the federal funding agency needs to forward one copy to NRCS for our records.

If you need additional information, please contact John Allen at 317-295-5859 or john.allen@usda.gov.

Sincerely,

JOHN ALLEN

JOHN ALLEN
State Soil Scientist

 Digitally signed by JOHN ALLEN
Date: 2024.01.30 06:50:04 -05'00'

Enclosers

FARMLAND CONVERSION IMPACT RATING

PART I (To be completed by Federal Agency)						Date Of Land Evaluation Request			
Name of Project		DES2100968 969 SR^# over Abandoned				Federal Agency Involved FHWA			
Proposed Land Use		Bridge Replacement Project				County and State Vermillion County, IN			
PART II (To be completed by NRCS)		Date Request Received By NRCS 1/31/2024				Person Completing Form: JRA			
Does the site contain Prime, Unique, Statewide or Local Important Farmland? (If no, the FPPA does not apply - do not complete additional parts of this form)				YES <input checked="" type="checkbox"/>	NO <input type="checkbox"/>	Acres Irrigated		Average Farm Size 435 ac	
Major Crop(s) Corn		Farmable Land In Govt. Jurisdiction Acres: 145525 % 87				Amount of Farmland As Defined in FPPA Acres: 13035 % 78			
Name of Land Evaluation System Used LESA		Name of State or Local Site Assessment System				Date Land Evaluation Returned by NRCS 1/29/2024			
PART III (To be completed by Federal Agency)						Alternative Site Rating			
						Site A	Site B	Site C	Site D
A. Total Acres To Be Converted Directly						0			
B. Total Acres To Be Converted Indirectly						0			
C. Total Acres In Site						0			
PART IV (To be completed by NRCS) Land Evaluation Information									
A. Total Acres Prime And Unique Farmland						0.25			
B. Total Acres Statewide Important or Local Important Farmland						0.00			
C. Percentage Of Farmland in County Or Local Govt. Unit To Be Converted						<0.001			
D. Percentage Of Farmland in Govt. Jurisdiction With Same Or Higher Relative Value						71			
PART V (To be completed by NRCS) Land Evaluation Criterion Relative Value of Farmland To Be Converted (Scale of 0 to 100 Points)						68			
PART VI (To be completed by Federal Agency) Site Assessment Criteria (Criteria are explained in 7 CFR 658.5 b. For Corridor project use form NRCS-CPA-106)				Maximum Points	Site A	Site B	Site C	Site D	
1. Area In Non-urban Use				(15)	13				
2. Perimeter In Non-urban Use				(10)	10				
3. Percent Of Site Being Farmed				(20)	0				
4. Protection Provided By State and Local Government				(20)	0				
5. Distance From Urban Built-up Area				(15)	5				
6. Distance To Urban Support Services				(15)	0				
7. Size Of Present Farm Unit Compared To Average				(10)	0				
8. Creation Of Non-farmable Farmland				(10)	0				
9. Availability Of Farm Support Services				(5)	0				
10. On-Farm Investments				(20)	0				
11. Effects Of Conversion On Farm Support Services				(10)	0				
12. Compatibility With Existing Agricultural Use				(10)	0				
TOTAL SITE ASSESSMENT POINTS				160	28	0	0	0	
PART VII (To be completed by Federal Agency)									
Relative Value Of Farmland (From Part V)				100	68	0	0	0	
Total Site Assessment (From Part VI above or local site assessment)				160	28	0	0	0	
TOTAL POINTS (Total of above 2 lines)				260	96	0	0	0	
						Was A Local Site Assessment Used?			
Site Selected:		Date Of Selection				YES <input checked="" type="checkbox"/>	NO <input type="checkbox"/>		
Reason For Selection:									
Name of Federal agency representative completing this form: Katie Finney, Egis BLN USA, Inc.						Date: 1/31/2024			
(See Instructions on reverse side)									Form AD-1006 (03-02)



INDIANA DEPARTMENT OF ENVIRONMENTAL MANAGEMENT

We Protect Hoosiers and Our Environment.

100 N. Senate Avenue • Indianapolis, IN 46204

(800) 451-6027 • (317) 232-8603 • www.idem.IN.gov

Eric J. Holcomb
Governor

Brian C. Rockensuess
Commissioner

January 19, 2024

Beam, Longest and Neff, LLC
Attention: Katie Finney
8320 Craig Street
Indianapolis, Indiana 46250

Dear Katie Finney:

Re: Wellhead Protection Area
Proximity Determination
Des No 2100968/2100969
Bridge Removal Project
SR 63 over Abandoned Railroad
Vermillion County, Indiana

Upon review of the above referenced project site, it has been determined that the proposed project area **is located within** a Wellhead Protection Area. If the contact information is needed for the WHPA, please contact the reference located at the bottom of the letter for the appropriate information. The information is accurate to the best of our knowledge; however, there are in some cases a few factors that could impact the accuracy of this determination. Some Wellhead Protection Area Delineations have not been submitted, and many have not been approved by this office. In these cases, we use a 3,000-foot fixed radius buffer to make the proximity determination. To find the status of a Public Water Supply System's (PWSS's) Wellhead Protection Area Delineation please visit our tracking database at <http://www.in.gov/idem/cleanwater/2456.htm> and scroll to the bottom of the page.

The project area **is not located within** a Source Water Assessment Area for a PWSS's surface water intake. The Source Water Assessment Area relates to the surface water drainage area that water could potentially flow and influence water quality for a PWSS's source of drinking water.

In the future, **please consider using this self-service tool** if it suits your needs. The Drinking Water Branch has a self-service tool which allows one to determine wellhead proximity without submitting the application form. Go to <https://www.in.gov/idem/cleanwater/pages/wellhead/> and use the instructions at the bottom of the page.



Please Reduce, Reuse, Recycle

Katie Finney
Page 2

If you have any additional questions, please feel free to contact me at the address above or at 317-233-9158 and aturnbow@idem.in.gov.

Sincerely,

A handwritten signature in black ink that reads "Alisha Turnbow". The signature is fluid and cursive, with "Alisha" on the top line and "Turnbow" on the bottom line.

Alisha Turnbow,
Environmental Manager
Ground Water Section
Drinking Water Branch
Office of Water Quality

Jeff Parke

From: Anderson, Samantha <SaAnderson2@indot.IN.gov>
Sent: Monday, August 21, 2023 3:18 PM
To: Jeff Parke
Subject: FW: Des. No. 2100968 & 2100969, SR 63 over Abandoned RR Bridge Removal Project - Engineering Assessment Data

CAUTION: This email originated from outside your organization. Exercise caution when opening attachments or clicking links, especially from unknown senders.

Hi Jeff,

See below

Sam Anderson

Director of Utilities and Railroad

Indiana Department of Transportation
100 North Senate Ave., N758 —U&RR
Indianapolis, IN 46204

Office: 317- 232-2860

Cell: 317-446-4168

Email: saanderson2@indot.in.gov



From: Rueschhoff, Thomas <trueschhoff@indot.IN.gov>
Sent: Monday, August 21, 2023 3:15 PM
To: Anderson, Samantha <SaAnderson2@indot.IN.gov>
Subject: RE: Des. No. 2100968 & 2100969, SR 63 over Abandoned RR Bridge Removal Project - Engineering Assessment Data

Hi Sam,

Sorry for the delay in getting back with you. I was able to finally find the Abandonment that included this section of the track owed by Norfolk and Western RR. The bridge on SR 63 is located near the railroad milepost 266.5.

The 1987 Abandonment [**AB-10 (42)**] is from Linden, IN (MP 231.2) to Coffeen, IL (MP 271.0) at the IN/IL State Line.



INDOT Rail Crossing Locator

Search

Results

Search by County _Query result

Displayed features: 61/61

Crossing: 342197X

Crossing ID	342197X
Status	Open
Crossing Position	RR Under
County	VERMILLION
City	HILLSDALE
State	IN
Street or Road Name	US 36
Railroad	CSX

[Click here for Photos.](#)[Click here for the FRA Report.](#)

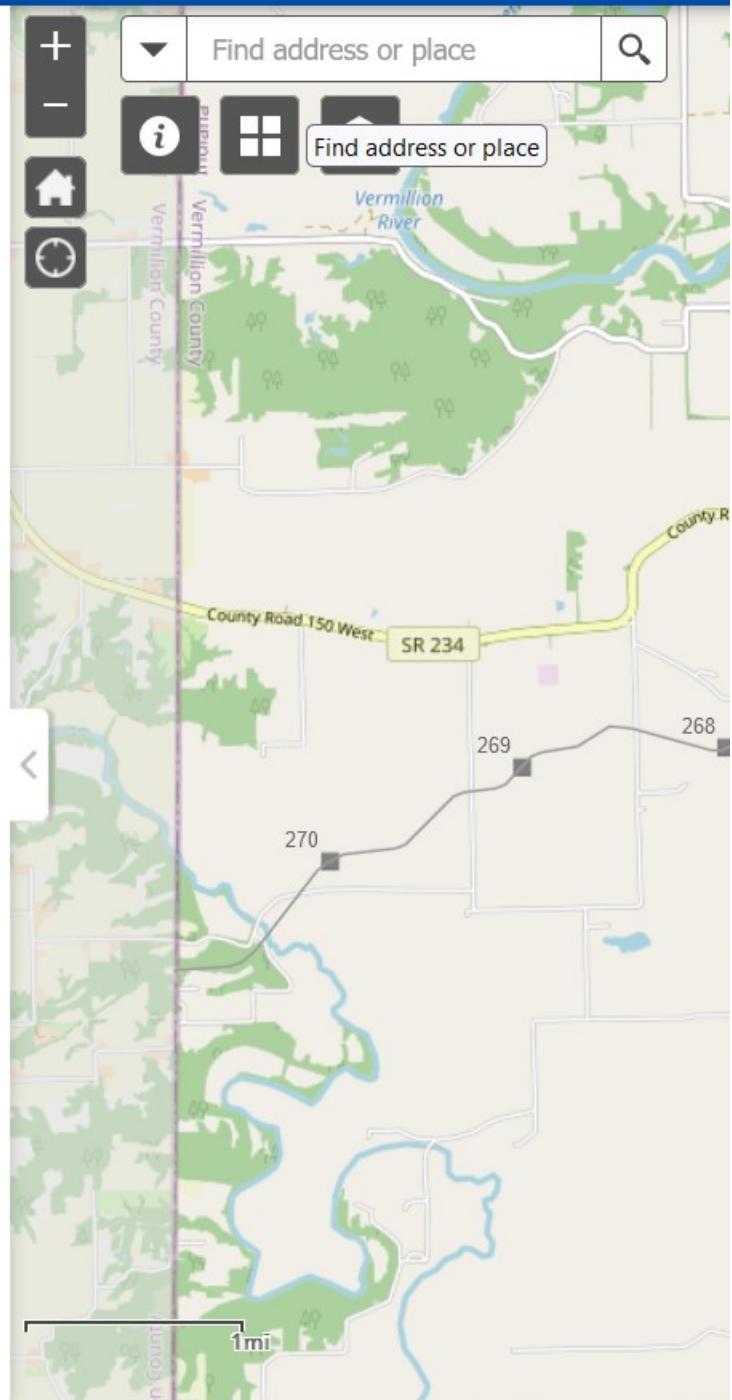
Crossing: 342184W

Crossing ID	342184W
Status	Open
Crossing Position	RR Under
County	VERMILLION
City	CAYUGA
State	IN
Street or Road Name	SR 63
Railroad	CSX

[Click here for Photos.](#)[Click here for the FRA Report.](#)

Crossing: 342175X

Crossing ID	342175X
Status	Open
Crossing Position	RR Over
County	VERMILLION
City	CAYUGA
State	IN



Type here to search

*Tom R. Rueschhoff, P.E., PMP*

Sniior Rail Project Manager, Rail Programs Office
Indiana Department of Transportation
100 North Senate Avenue
IGCN - Room N758-MM
Indianapolis, IN. 46204
Cell Phone: (317) 691-3126
trueschhoff@indot.in.gov

From: Anderson, Samantha <SaAnderson2@indot.IN.gov>
Sent: Monday, August 21, 2023 8:43 AM
To: Rueschhoff, Thomas <trueschhoff@indot.IN.gov>
Subject: FW: Des. No. 2100968 & 2100969, SR 63 over Abandoned RR Bridge Removal Project - Engineering Assessment Data

Hi Tom,
Were you able to find anything on this?

Sam Anderson
Director of Utilities and Railroad
Indiana Department of Transportation
100 North Senate Ave., N758 —U&RR
Indianapolis, IN 46204
Office: 317- 232-2860
Cell: 317-446-4168
Email: saanderson2@indot.in.gov

From: Jeff Parke <jparke@b-l-n.com>
Sent: Wednesday, July 26, 2023 2:10 PM
To: Anderson, Samantha <SaAnderson2@indot.IN.gov>
Subject: RE: Des. No. 2100968 & 2100969, SR 63 over Abandoned RR Bridge Removal Project - Engineering Assessment Data

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Sam,

I've attached the original design plans and a google earth pin. We have not developed plans yet since we are working on the engineering assessment.

Thanks,

Jeff

From: Anderson, Samantha <SaAnderson2@indot.IN.gov>
Sent: Wednesday, July 26, 2023 1:04 PM
To: Jeff Parke <jparke@b-l-n.com>; Gannaway, Douglas T. <DTGannaway@indot.IN.gov>; Miller, Jessica

<JeMiller1@indot.IN.gov>; Rueschhoff, Thomas <trueschhoff@indot.IN.gov>

Cc: Michael L. McCool Jr. <MMcCool@b-l-n.com>

Subject: RE: Des. No. 2100968 & 2100969, SR 63 over Abandoned RR Bridge Removal Project - Engineering Assessment Data

CAUTION: This email originated from outside your organization. Exercise caution when opening attachments or clicking links, especially from unknown senders.

I agree with the title search and do you have a map or plans to provide so Tom can research the abandonment

Sam Anderson

Director of Utilities and Railroad

Indiana Department of Transportation

100 North Senate Ave., N758 —U&RR

Indianapolis, IN 46204

Office: 317- 232-2860

Cell: 317-446-4168

Email: saanderson2@indot.in.gov

From: Jeff Parke <jparke@b-l-n.com>

Sent: Wednesday, July 26, 2023 12:41 PM

To: Anderson, Samantha <SaAnderson2@indot.IN.gov>; Gannaway, Douglas T. <DTGannaway@indot.IN.gov>; Miller, Jessica <JeMiller1@indot.IN.gov>; Rueschhoff, Thomas <trueschhoff@indot.IN.gov>

Cc: Mike McCool <MMcCool@b-l-n.com>

Subject: RE: Des. No. 2100968 & 2100969, SR 63 over Abandoned RR Bridge Removal Project - Engineering Assessment Data

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Sam,

The railroad is Norfolk and Southern. We have been coordinating with Mark Felicetti. Mr. Felicetti just let us know that he will investigate the corridor more and tell us what he learns by Friday this week. Hopefully, he uncovers more information in their files to share.

The property owners that we have been using at this early stage were established from the property deeds obtained from the Vermillion County Recorder's Office.

We do not have R/W engineering in our contract, so we have not done a title search. We recommended doing so in the email to Jessica below.

Let me know if there is anything else that you need, and I'll do my best to get it.

Thanks,

Jeffery A. Parke, P.E.

Senior Bridge Engineer

o: 317.806.3009

c: 765.891.0693

B-L-N.com

BEAM, LONGEST and NEFF

A Tradition of Excellence Since 1945

From: Anderson, Samantha <SaAnderson2@indot.IN.gov>

Sent: Wednesday, July 26, 2023 10:32 AM

To: Jeff Parke <jparke@b-l-n.com>; Gannaway, Douglas T. <DTGannaway@indot.IN.gov>; Miller, Jessica

<JeMiller1@indot.IN.gov>; Rueschhoff, Thomas <trueschhoff@indot.IN.gov>

Cc: Michael L. McCool Jr. <MMcCool@b-l-n.com>

Subject: RE: Des. No. 2100968 & 2100969, SR 63 over Abandoned RR Bridge Removal Project - Engineering Assessment Data

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Jeff I have a couple questions. How far back was a T&E ran?

Tom, have you been able to look this up to see if it was formally abandoned?

What RR is this and who have you been speaking to?

Sam Anderson

Director of Utilities and Railroad

Indiana Department of Transportation

100 North Senate Ave., N758 —U&RR

Indianapolis, IN 46204

Office: 317- 232-2860

Cell: 317-446-4168

Email: saanderson2@indot.in.gov

From: Jeff Parke <jparke@b-l-n.com>

Sent: Wednesday, July 26, 2023 10:14 AM

To: Gannaway, Douglas T. <DTGannaway@indot.IN.gov>; Miller, Jessica <JeMiller1@indot.IN.gov>; Rueschhoff, Thomas <trueschhoff@indot.IN.gov>

Cc: Anderson, Samantha <SaAnderson2@indot.IN.gov>; Mike McCool <mmccool@b-l-n.com>

Subject: RE: Des. No. 2100968 & 2100969, SR 63 over Abandoned RR Bridge Removal Project - Engineering Assessment Data

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Jessica,

We were able to get a val map from the railroad, but it does not show that they have abandoned the corridor. We have reached out once again to get additional information but I'm not sure how long that will take to get a response. The railroads are generally not quick to respond. We have not found any records that has shown that the abandonment has been legally recorded. Our R/W engineering manager recommended that a title search be performed, but we do not have that work in our fees.

We've been asked to include the ownership of the railroad corridor in the engineering assessment, but I do not believe that we will be able to resolve this issue for that document. We currently have that this portion of the corridor requires acquisition and that this will elevate the CE to a level 2 (from CE1) since it is a large area. This would be the most conservative approach, but the CE2 is not part of our scope of work.

Would you like me to schedule a call with you to go over this in more detail?

Thanks,

Jeffery A. Parke, P.E.
Senior Bridge Engineer
o: 317.806.3009
c: 765.891.0693
B-L-N.com

BEAM, LONGEST and NEFF
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From: Gannaway, Douglas T. <DTGannaway@indot.IN.gov>
Sent: Thursday, April 6, 2023 8:41 AM
To: Miller, Jessica <JeMiller1@indot.IN.gov>; Rueschhoff, Thomas <trueschhoff@indot.IN.gov>
Cc: Jeff Parke <jparke@b-l-n.com>; Anderson, Samantha <SaAnderson2@indot.IN.gov>
Subject: RE: Des. No. 2100968 & 2100969, SR 63 over Abandoned RR Bridge Removal Project - Engineering Assessment Data

CAUTION: This email originated from outside your organization. Exercise caution when opening attachments or clicking links, especially from unknown senders.

Jessica,

I've added Tom to this email as the Rail Office has an active role in Abandonments and can correct any misstatements on my part.

Typically when a RR abandons a portion of their rail system, they petition Tom's group and eventually it works its way to the Surface Transportation Board (STB) for approval. If the STB approves, the RR's property is given to the adjacent property owners. In a nut shell, if you had a line running east to west abandoned and the RR had a 100' wide corridor in that area, they divide the RoW right down the middle and 50' of that RoW goes to Mr. North Property owner and the other 50' goes to Miss South property owner.

This is where it gets fuzzy for me.....

I assume there is a mechanism for this to be formally recorded so "its legal"but I have no idea who is responsible for that and should issues arise like this where a project, state or local, involves this property is the fact that it was a formal and approved abandonment, does it matter if the ball got dropped on recording it.

I've also CCed Sam, my director, as she has tons of RE experience and her and I have been working on some abandoned RR property recently and she may have insight I've omitted.

Hope this helps, but if not, don't lose my number. We are currently working on an abandonment and reviewing info Tom provided about the corridor.....I may be able to help from a different direction. I will be thinking on it.

Have a great day all!

Douglas T. Gannaway

Railroad Administrator

100 North Senate Avenue, Room N758-U&R
Indianapolis, IN 46204

Office: (317) 232-5050

Cell: (765) 721-7197

Email: dtgannaway@indot.in.gov



From: Miller, Jessica <JeMiller1@indot.IN.gov>
Sent: Wednesday, April 05, 2023 3:21 PM
To: Gannaway, Douglas T. <DTGannaway@indot.IN.gov>
Cc: Jeff Parke <jparke@b-l-n.com>
Subject: FW: Des. No. 2100968 & 2100969, SR 63 over Abandoned RR Bridge Removal Project - Engineering Assessment Data

Afternoon Doug,

Can you take a look my designers request below and give us any info you have regarding transfer of RR Right of Way at 2100968 & 2100969, SR 63 over Abandoned RR Bridge Removal Project?

Jessica K. Miller

Project Manager

41 West 300 North
Crawfordsville, IN 47933

Office: (765) 361-5224

Cell: (765) 366-5930

Email: jemiller1@indot.in.gov





United States Department of the Interior



FISH AND WILDLIFE SERVICE
Indiana Ecological Services Field Office
620 South Walker Street
Bloomington, IN 47403-2121
Phone: (812) 334-4261 Fax: (812) 334-4273

In Reply Refer To:

10/15/2024 13:10:41 UTC

Project Code: 2024-0041810

Project Name: 2100968 and 2100969 - SR 63 over Abandoned Railroad

Subject: List of threatened and endangered species that may occur in your proposed project location or may be affected by your proposed project

To Whom It May Concern:

The enclosed species list identifies threatened, endangered, proposed and candidate species, as well as proposed and final designated critical habitat, that may occur within the boundary of your proposed project and/or may be affected by your proposed project. The species list fulfills the requirements of the U.S. Fish and Wildlife Service (Service) under section 7(c) of the Endangered Species Act (Act) of 1973, as amended (16 U.S.C. 1531 *et seq.*).

New information based on updated surveys, changes in the abundance and distribution of species, changed habitat conditions, or other factors could change this list. Please feel free to contact us if you need more current information or assistance regarding the potential impacts to federally proposed, listed, and candidate species and federally designated and proposed critical habitat. Please note that under 50 CFR 402.12(e) of the regulations implementing section 7 of the Act, the accuracy of this species list should be verified after 90 days. This verification can be completed formally or informally as desired. The Service recommends that verification be completed by visiting the IPaC website at regular intervals during project planning and implementation for updates to species lists and information. An updated list may be requested through the IPaC system by completing the same process used to receive the enclosed list.

The purpose of the Act is to provide a means whereby threatened and endangered species and the ecosystems upon which they depend may be conserved. Under sections 7(a)(1) and 7(a)(2) of the Act and its implementing regulations (50 CFR 402 *et seq.*), Federal agencies are required to utilize their authorities to carry out programs for the conservation of threatened and endangered species and to determine whether projects may affect threatened and endangered species and/or designated critical habitat.

Please use the species list provided and visit the U.S. Fish and Wildlife Service's Region 3 Section 7 Technical Assistance website at - <http://www.fws.gov/midwest/endangered/section7/s7process/index.html>. This website contains step-by-step instructions which will help you

determine if your project will have an adverse effect on listed species and will help lead you through the Section 7 process. For all **wind energy projects** and **projects that include installing towers that use guy wires or are over 200 feet in height**, please contact this field office directly for assistance, even if no federally listed plants, animals or critical habitat are present within your proposed project or may be affected by your proposed project.

A Biological Assessment is required for construction projects (or other undertakings having similar physical impacts) that are major Federal actions significantly affecting the quality of the human environment as defined in the National Environmental Policy Act (42 U.S.C. 4332(2)(c)). For projects other than major construction activities, the Service suggests that a biological evaluation similar to a Biological Assessment be prepared to determine whether the project may affect listed or proposed species and/or designated or proposed critical habitat. Recommended contents of a Biological Assessment are described at 50 CFR 402.12.

If a Federal agency determines, based on the Biological Assessment or biological evaluation, that listed species and/or designated critical habitat may be affected by the proposed project, the agency is required to consult with the Service pursuant to 50 CFR 402. In addition, the Service recommends that candidate species, proposed species and proposed critical habitat be addressed within the consultation. More information on the regulations and procedures for section 7 consultation, including the role of permit or license applicants, can be found in the "Endangered Species Consultation Handbook" at:

<https://www.fws.gov/sites/default/files/documents/endangered-species-consultation-handbook.pdf>

Migratory Birds: In addition to responsibilities to protect threatened and endangered species under the Endangered Species Act (ESA), there are additional responsibilities under the Migratory Bird Treaty Act (MBTA) and the Bald and Golden Eagle Protection Act (BGEPA) to protect native birds from project-related impacts. Any activity, intentional or unintentional, resulting in take of migratory birds, including eagles, is prohibited unless otherwise permitted by the U.S. Fish and Wildlife Service (50 C.F.R. Sec. 10.12 and 16 U.S.C. Sec. 668(a)). For more information regarding these Acts, see <https://www.fws.gov/program/migratory-bird-permit/what-we-do>.

The MBTA has no provision for allowing take of migratory birds that may be unintentionally killed or injured by otherwise lawful activities. It is the responsibility of the project proponent to comply with these Acts by identifying potential impacts to migratory birds and eagles within applicable NEPA documents (when there is a federal nexus) or a Bird/Eagle Conservation Plan (when there is no federal nexus). Proponents should implement conservation measures to avoid or minimize the production of project-related stressors or minimize the exposure of birds and their resources to the project-related stressors. For more information on avian stressors and recommended conservation measures, see <https://www.fws.gov/library/collections/threats-birds>.

In addition to MBTA and BGEPA, Executive Order 13186: *Responsibilities of Federal Agencies to Protect Migratory Birds*, obligates all Federal agencies that engage in or authorize activities that might affect migratory birds, to minimize those effects and encourage conservation measures that will improve bird populations. Executive Order 13186 provides for the protection of both migratory birds and migratory bird habitat. For information regarding the implementation of

Executive Order 13186, please visit <https://www.fws.gov/partner/council-conservation-migratory-birds>.

We appreciate your concern for threatened and endangered species. The Service encourages Federal agencies to include conservation of threatened and endangered species into their project planning to further the purposes of the Act. **Please include the Consultation Code in the header of this letter with any request for consultation or correspondence about your project that you submit to our office.**

Attachment(s):

- Official Species List
- Bald & Golden Eagles
- Migratory Birds
- Wetlands

OFFICIAL SPECIES LIST

This list is provided pursuant to Section 7 of the Endangered Species Act, and fulfills the requirement for Federal agencies to "request of the Secretary of the Interior information whether any species which is listed or proposed to be listed may be present in the area of a proposed action".

This species list is provided by:

Indiana Ecological Services Field Office

620 South Walker Street
Bloomington, IN 47403-2121
(812) 334-4261

PROJECT SUMMARY

Project Code: 2024-0041810
Project Name: 2100968 and 2100969 - SR 63 over Abandoned Railroad
Project Type: Bridge - Removal
Project Description: The proposed project consists of the removal of the existing bridges (#063-83-02002 northbound and southbound) that carry State Route (SR) 63 over an abandoned railroad. The project is located approximately 0.24 mile south of SR 234 in Vermillion County. The estimated project limits, without incidental construction is 250 feet, 125 feet north and 125 feet south of the centerline of structures. Incidental would be an additional 370 feet south and 360 feet north of the centerline of the structure. The existing bridge superstructures will be completely removed, and the end bents will be removed to a minimum depth of 2 feet below the proposed pavement subgrade. The bent caps will be removed from the interior bents, leaving the columns and foundations in place. The existing concrete slope walls will not be removed from the spill slopes at each end bent. SR 63 will be reconstructed on the same horizontal and vertical alignments and will provide two 12-foot travel lanes bordered by a 4-foot median and 10-foot outside paved shoulders in each direction. Guardrail will be installed along the outside shoulders. Pavement within the project limits will be replaced and 50-feet of each approach will be milled and resurfaced. Incidental construction will include pavement milling and resurfacing, guardrail removal, and linear grading. The project will backfill the existing crossing with roadway embankment material. An additional 1.3 acres of permanent right-of-way will be acquired for this project. Approximately 0.6 acre of tree removal will be required; 0.56 acre from within 100 feet of the roadway pavement and 0.04 from 100 to 300 feet of the pavement. Suitable summer habitat is located adjacent to the project area on the east and west sides of SR 63. Suitable summer habitat will be removed as a result of this project.
The review of the USFWS database on September 8, 2023, did not indicate the presence of endangered bat species within 0.5 mile of the project area. The most recent bridge inspections were conducted on May 25, 2023 (southbound) and August 7, 2024 (northbound), and no evidence of bats was found, but evidence of swallows nests was observed.
Temporary lighting may be used for this project. The project's scheduled letting date is October 8, 2025, with construction anticipated to be in winter 2025.

Project Location:

The approximate location of the project can be viewed in Google Maps: <https://www.google.com/maps/@39.94874045,-87.45136810499028,14z>



Counties: Vermillion County, Indiana

ENDANGERED SPECIES ACT SPECIES

There is a total of 6 threatened, endangered, or candidate species on this species list.

Species on this list should be considered in an effects analysis for your project and could include species that exist in another geographic area. For example, certain fish may appear on the species list because a project could affect downstream species.

IPaC does not display listed species or critical habitats under the sole jurisdiction of NOAA Fisheries¹, as USFWS does not have the authority to speak on behalf of NOAA and the Department of Commerce.

See the "Critical habitats" section below for those critical habitats that lie wholly or partially within your project area under this office's jurisdiction. Please contact the designated FWS office if you have questions.

1. [NOAA Fisheries](#), also known as the National Marine Fisheries Service (NMFS), is an office of the National Oceanic and Atmospheric Administration within the Department of Commerce.

MAMMALS

NAME	STATUS
Gray Bat <i>Myotis grisescens</i> No critical habitat has been designated for this species. Species profile: https://ecos.fws.gov/ecp/species/6329	Endangered
Indiana Bat <i>Myotis sodalis</i> There is final critical habitat for this species. Your location does not overlap the critical habitat. Species profile: https://ecos.fws.gov/ecp/species/5949	Endangered
Northern Long-eared Bat <i>Myotis septentrionalis</i> No critical habitat has been designated for this species. Species profile: https://ecos.fws.gov/ecp/species/9045	Endangered

BIRDS

NAME	STATUS
Whooping Crane <i>Grus americana</i> Population: U.S.A. (AL, AR, CO, FL, GA, ID, IL, IN, IA, KY, LA, MI, MN, MS, MO, NC, NM, OH, SC, TN, UT, VA, WI, WV, western half of WY) No critical habitat has been designated for this species. Species profile: https://ecos.fws.gov/ecp/species/758	Experimental Population, Non-Essential

CLAMS

NAME	STATUS
Salamander Mussel <i>Simpsonaias ambigua</i> There is proposed critical habitat for this species. Your location does not overlap the critical habitat. Species profile: https://ecos.fws.gov/ecp/species/6208	Proposed Endangered

INSECTS

NAME	STATUS
Monarch Butterfly <i>Danaus plexippus</i> No critical habitat has been designated for this species. Species profile: https://ecos.fws.gov/ecp/species/9743	Candidate

CRITICAL HABITATS

THERE ARE NO CRITICAL HABITATS WITHIN YOUR PROJECT AREA UNDER THIS OFFICE'S JURISDICTION.

YOU ARE STILL REQUIRED TO DETERMINE IF YOUR PROJECT(S) MAY HAVE EFFECTS ON ALL ABOVE LISTED SPECIES.

BALD & GOLDEN EAGLES

Bald and golden eagles are protected under the Bald and Golden Eagle Protection Act¹ and the Migratory Bird Treaty Act².

Any person or organization who plans or conducts activities that may result in impacts to bald or golden eagles, or their habitats³, should follow appropriate regulations and consider implementing appropriate conservation measures, as described in the links below. Specifically, please review the ["Supplemental Information on Migratory Birds and Eagles"](#).

1. The [Bald and Golden Eagle Protection Act](#) of 1940.
2. The [Migratory Birds Treaty Act](#) of 1918.
3. 50 C.F.R. Sec. 10.12 and 16 U.S.C. Sec. 668(a)

There are likely bald eagles present in your project area. For additional information on bald eagles, refer to [Bald Eagle Nesting and Sensitivity to Human Activity](#)

For guidance on when to schedule activities or implement avoidance and minimization measures to reduce impacts to migratory birds on your list, see the PROBABILITY OF PRESENCE SUMMARY below to see when these birds are most likely to be present and breeding in your project area.

NAME	BREEDING SEASON
Bald Eagle <i>Haliaeetus leucocephalus</i> This is not a Bird of Conservation Concern (BCC) in this area, but warrants attention because of the Eagle Act or for potential susceptibilities in offshore areas from certain types of development or activities. https://ecos.fws.gov/ecp/species/1626	Breeds Sep 1 to Jul 31

PROBABILITY OF PRESENCE SUMMARY

The graphs below provide our best understanding of when birds of concern are most likely to be present in your project area. This information can be used to tailor and schedule your project activities to avoid or minimize impacts to birds. Please make sure you read ["Supplemental Information on Migratory Birds and Eagles"](#), specifically the FAQ section titled "Proper Interpretation and Use of Your Migratory Bird Report" before using or attempting to interpret this report.

Probability of Presence (■)

Green bars; the bird's relative probability of presence in the 10km grid cell(s) your project overlaps during that week of the year.

Breeding Season (■)

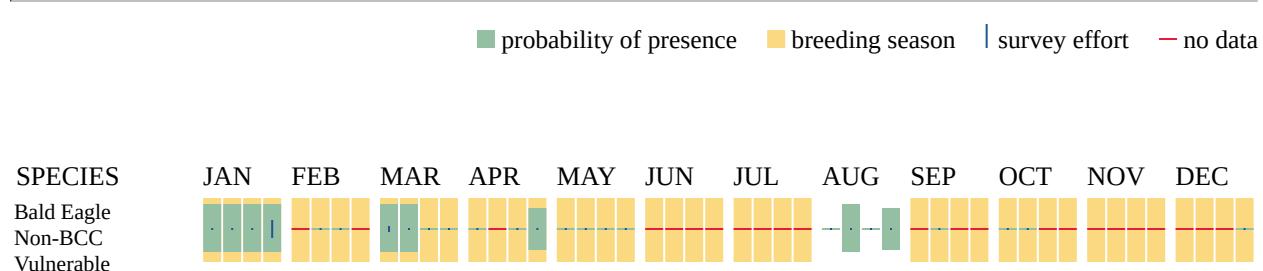
Yellow bars; liberal estimate of the timeframe inside which the bird breeds across its entire range.

Survey Effort (|)

Vertical black lines; the number of surveys performed for that species in the 10km grid cell(s) your project area overlaps.

No Data (-)

A week is marked as having no data if there were no survey events for that week.



Additional information can be found using the following links:

- Eagle Management <https://www.fws.gov/program/eagle-management>
- Measures for avoiding and minimizing impacts to birds <https://www.fws.gov/library/collections/avoiding-and-minimizing-incidental-take-migratory-birds>
- Nationwide conservation measures for birds <https://www.fws.gov/sites/default/files/documents/nationwide-standard-conservation-measures.pdf>
- Supplemental Information for Migratory Birds and Eagles in IPaC <https://www.fws.gov/media/supplemental-information-migratory-birds-and-bald-and-golden-eagles-may-occur-project-action>

MIGRATORY BIRDS

Certain birds are protected under the Migratory Bird Treaty Act¹ and the Bald and Golden Eagle Protection Act².

Any person or organization who plans or conducts activities that may result in impacts to migratory birds, eagles, and their habitats³ should follow appropriate regulations and consider implementing appropriate conservation measures, as described in the links below. Specifically, please review the ["Supplemental Information on Migratory Birds and Eagles"](#).

1. The [Migratory Birds Treaty Act](#) of 1918.
2. The [Bald and Golden Eagle Protection Act](#) of 1940.
3. 50 C.F.R. Sec. 10.12 and 16 U.S.C. Sec. 668(a)

For guidance on when to schedule activities or implement avoidance and minimization measures to reduce impacts to migratory birds on your list, see the PROBABILITY OF PRESENCE SUMMARY below to see when these birds are most likely to be present and breeding in your project area.

NAME	BREEDING SEASON
Bald Eagle <i>Haliaeetus leucocephalus</i> This is not a Bird of Conservation Concern (BCC) in this area, but warrants attention because of the Eagle Act or for potential susceptibilities in offshore areas from certain types of development or activities. https://ecos.fws.gov/ecp/species/1626	Breeds Sep 1 to Jul 31
Bobolink <i>Dolichonyx oryzivorus</i> This is a Bird of Conservation Concern (BCC) throughout its range in the continental USA and Alaska. https://ecos.fws.gov/ecp/species/9454	Breeds May 20 to Jul 31
Chimney Swift <i>Chaetura pelagica</i> This is a Bird of Conservation Concern (BCC) throughout its range in the continental USA and Alaska. https://ecos.fws.gov/ecp/species/9406	Breeds Mar 15 to Aug 25
Field Sparrow <i>Spizella pusilla</i> This is a Bird of Conservation Concern (BCC) only in particular Bird Conservation Regions (BCRs) in the continental USA https://ecos.fws.gov/ecp/species/9446	Breeds Mar 1 to Aug 15
Grasshopper Sparrow <i>Ammodramus savannarum perpallidus</i> This is a Bird of Conservation Concern (BCC) only in particular Bird Conservation Regions (BCRs) in the continental USA https://ecos.fws.gov/ecp/species/8329	Breeds Jun 1 to Aug 20
Kentucky Warbler <i>Geothlypis formosa</i> This is a Bird of Conservation Concern (BCC) throughout its range in the continental USA and Alaska. https://ecos.fws.gov/ecp/species/9443	Breeds Apr 20 to Aug 20
Prairie Warbler <i>Setophaga discolor</i> This is a Bird of Conservation Concern (BCC) throughout its range in the continental USA and Alaska. https://ecos.fws.gov/ecp/species/9513	Breeds May 1 to Jul 31
Red-headed Woodpecker <i>Melanerpes erythrocephalus</i> This is a Bird of Conservation Concern (BCC) throughout its range in the continental USA and Alaska. https://ecos.fws.gov/ecp/species/9398	Breeds May 10 to Sep 10
Semipalmated Sandpiper <i>Calidris pusilla</i> This is a Bird of Conservation Concern (BCC) only in particular Bird Conservation Regions (BCRs) in the continental USA https://ecos.fws.gov/ecp/species/9603	Breeds elsewhere

NAME	BREEDING SEASON
Wood Thrush <i>Hylocichla mustelina</i> This is a Bird of Conservation Concern (BCC) throughout its range in the continental USA and Alaska. https://ecos.fws.gov/ecp/species/9431	Breeds May 10 to Aug 31

PROBABILITY OF PRESENCE SUMMARY

The graphs below provide our best understanding of when birds of concern are most likely to be present in your project area. This information can be used to tailor and schedule your project activities to avoid or minimize impacts to birds. Please make sure you read "[Supplemental Information on Migratory Birds and Eagles](#)", specifically the FAQ section titled "Proper Interpretation and Use of Your Migratory Bird Report" before using or attempting to interpret this report.

Probability of Presence (■)

Green bars; the bird's relative probability of presence in the 10km grid cell(s) your project overlaps during that week of the year.

Breeding Season (■)

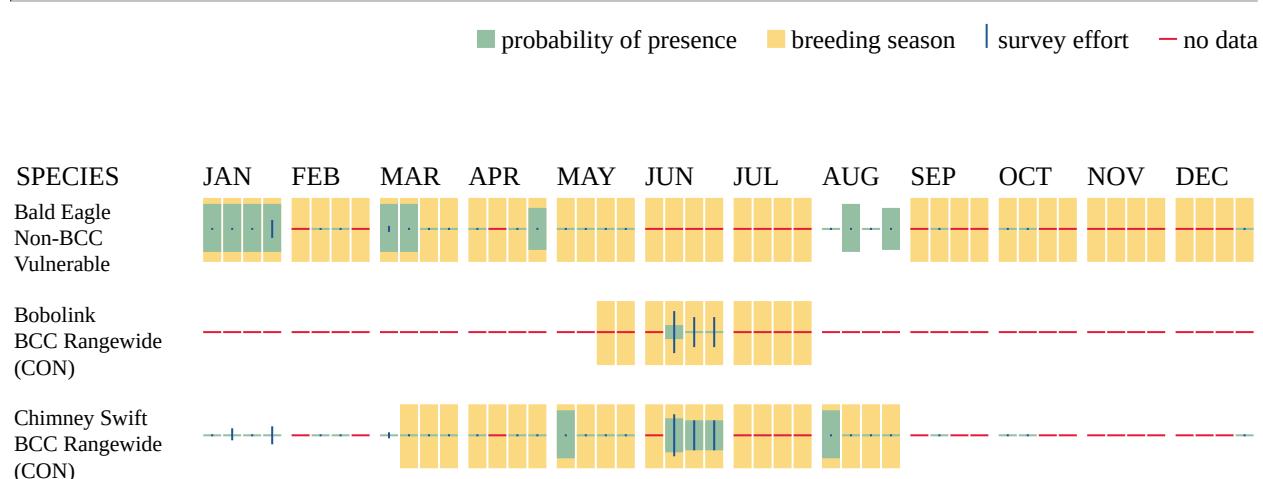
Yellow bars; liberal estimate of the timeframe inside which the bird breeds across its entire range.

Survey Effort (|)

Vertical black lines; the number of surveys performed for that species in the 10km grid cell(s) your project area overlaps.

No Data (-)

A week is marked as having no data if there were no survey events for that week.





Additional information can be found using the following links:

- Eagle Management <https://www.fws.gov/program/eagle-management>
- Measures for avoiding and minimizing impacts to birds <https://www.fws.gov/library/collections/avoiding-and-minimizing-incidental-take-migratory-birds>
- Nationwide conservation measures for birds <https://www.fws.gov/sites/default/files/documents/nationwide-standard-conservation-measures.pdf>
- Supplemental Information for Migratory Birds and Eagles in IPaC <https://www.fws.gov/media/supplemental-information-migratory-birds-and-bald-and-golden-eagles-may-occur-project-action>

WETLANDS

Impacts to [NWI wetlands](#) and other aquatic habitats may be subject to regulation under Section 404 of the Clean Water Act, or other State/Federal statutes.

For more information please contact the Regulatory Program of the local [U.S. Army Corps of Engineers District](#).

Please note that the NWI data being shown may be out of date. We are currently working to update our NWI data set. We recommend you verify these results with a site visit to determine the actual extent of wetlands on site.

LAKE

- L1UBHx

IPAC USER CONTACT INFORMATION

Agency: Indiana Department of Transportation

Name: Kathryn Finney

Address: 8320 Craig St.

City: Indianapolis

State: IN

Zip: 46250

Email: kfinney@b-l-n.com

Phone: 3178064340

LEAD AGENCY CONTACT INFORMATION

Lead Agency: Federal Highway Administration



United States Department of the Interior



FISH AND WILDLIFE SERVICE
Indiana Ecological Services Field Office
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In Reply Refer To:

10/07/2024 14:32:50 UTC

Project code: 2024-0041810

Project Name: 2100968 and 2100969 - SR 63 over Abandoned Railroad

Subject: Consistency letter for the '2100968 and 2100969 - SR 63 over Abandoned Railroad' project under the amended February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion (dated March 23, 2023) for Transportation Projects within the Range of the Indiana Bat and Northern Long-eared Bat (NLEB).

To whom it may concern:

The U.S. Fish and Wildlife Service (Service) has received your request dated October 07, 2024 to verify that the **2100968 and 2100969 - SR 63 over Abandoned Railroad** (Proposed Action) may rely on the amended February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion Opinion (dated March 23, 2023) for Transportation Projects within the Range of the Indiana Bat and Northern Long-eared Bat (PBO) to satisfy requirements under section 7(a)(2) of the Endangered Species Act of 1973 (ESA) (87 Stat.884, as amended; 16 U.S.C. 1531 *et seq.*).

Based on the information you provided (Project Description shown below), you have determined that the Proposed Action is within the scope and adheres to the criteria of the PBO, including the adoption of applicable avoidance and minimization measures. **At least one of the qualification interview questions indicated an activity or portion of your project is consistent with a likely to adversely affect therefore, the overall determination for your project is, may affect, and is likely to adversely affect the endangered Indiana bat (*Myotis sodalis*) and/or the endangered northern long-eared bat (*Myotis septentrionalis*).** Consultation with the Service pursuant to section 7(a)(2) of the ESA (87 Stat. 884, as amended; 16 U.S.C. 1531 *et seq.*) is required.

This "may affect - likely to adversely affect" determination becomes effective when the lead Federal action agency or designated non-federal representative requests the Service rely on the PBO to satisfy the agency's consultation requirements for this project. Please provide this consistency letter to the lead Federal action agency or its designated non-federal representative for review, and as the agency deems appropriate, transmit to this Service Office for verification that the project is consistent with the PBO.

This Service Office will respond by letter to the requesting Federal action agency or designated non-federal representative within 30 calendar days after receiving request for verification to:

- verify that the Proposed Action is consistent with the scope of actions covered under the PBO;
- verify that all applicable avoidance, minimization, and compensation measures are included in the action proposal;
- identify any action-specific monitoring and reporting requirements, consistent with the monitoring and reporting requirements of the PBO, and
- identify anticipated incidental take.

ESA Section 7 compliance for this Proposed Action is not complete until the Federal action agency or its designated non-federal representative receives a verification letter from the Service.

If the Proposed Action is modified, or new information reveals that it may affect the Indiana bat and/or Northern long-eared bat in a manner or to an extent not considered in the PBO, further review to conclude the requirements of ESA Section 7(a)(2) may be required.

For Proposed Actions that include bridge/culvert or structure removal, replacement, and/or maintenance activities: If your initial bridge/culvert or structure assessments failed to detect Indiana bats, but you later detect bats prior to, or during construction, please submit the Post Assessment Discovery of Bats at Bridge/Culvert or Structure Form (User Guide Appendix E) to this Service Office. In these instances, potential incidental take of Indiana bats may be exempted provided that the take is reported to the Service.

If the Proposed Action may affect any other federally-listed or proposed species and/or designated critical habitat, additional consultation between the lead Federal action agency and this Service Office is required. If the proposed action has the potential to take bald or golden eagles, additional coordination with the Service under the Bald and Golden Eagle Protection Act may also be required. In either of these circumstances, please advise the lead Federal action agency accordingly.

The following species may occur in your project area and **are not** covered by this determination:

- Gray Bat *Myotis grisescens* Endangered
- Monarch Butterfly *Danaus plexippus* Candidate
- Salamander Mussel *Simpsonaias ambigua* Proposed Endangered
- Whooping Crane *Grus americana* Experimental Population, Non-Essential

PROJECT DESCRIPTION

The following project name and description was collected in IPaC as part of the endangered species review process.

NAME

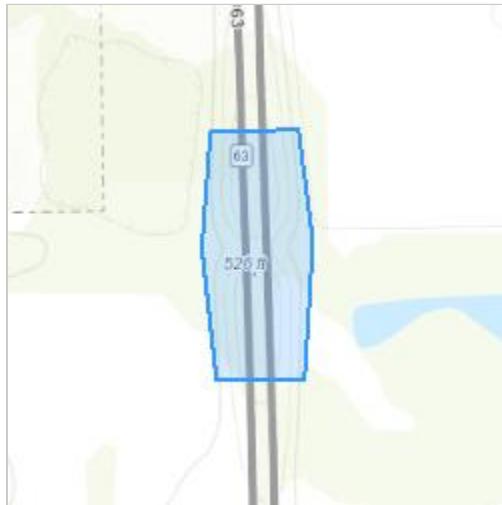
2100968 and 2100969 - SR 63 over Abandoned Railroad

DESCRIPTION

The proposed project consists of the removal of the existing bridges (#063-83-02002 northbound and southbound) that carry State Route (SR) 63 over an abandoned railroad. The project is located approximately 0.24 mile south of SR 234 in Vermillion County. The estimated project limits, without incidental construction is 250 feet, 125 feet north and 125 feet south of the centerline of structures. Incidental would be an additional 370 feet south and 360 feet north of the centerline of the structure. The existing bridge superstructures will be completely removed, and the end bents will be removed to a minimum depth of 2 feet below the proposed pavement subgrade. The bent caps will be removed from the interior bents, leaving the columns and foundations in place. The existing concrete slope walls will not be removed from the spill slopes at each end bent. SR 63 will be reconstructed on the same horizontal and vertical alignments and will provide two 12-foot travel lanes bordered by a 4-foot median and 10-foot outside paved shoulders in each direction. Guardrail will be installed along the outside shoulders. Pavement within the project limits will be replaced and 50-feet of each approach will be milled and resurfaced. Incidental construction will include pavement milling and resurfacing, guardrail removal, and linear grading. The project will backfill the existing crossing with roadway embankment material. An additional 1.3 acres of permanent right-of-way will be acquired for this project. Approximately 0.6 acre of tree removal will be required; 0.56 acre from within 100 feet of the roadway pavement and 0.04 from 100 to 300 feet of the pavement. Suitable summer habitat is located adjacent to the project area on the east and west sides of SR 63. Suitable summer habitat will be removed as a result of this project.

The review of the USFWS database on September 8, 2023, did not indicate the presence of endangered bat species within 0.5 mile of the project area. The most recent bridge inspections were conducted on May 25, 2023 (southbound) and August 7, 2024 (northbound), and no evidence of bats was found, but evidence of swallows nests was observed. Temporary lighting may be used for this project. The project's scheduled letting date is October 8, 2025, with construction anticipated to be in winter 2025.

The approximate location of the project can be viewed in Google Maps: <https://www.google.com/maps/@39.94809535,-87.45134702806831,14z>



DETERMINATION KEY RESULT

Based on your answers provided, this project is likely to adversely affect the endangered Indiana bat and/or the endangered northern long-eared bat. Therefore, consultation with the U.S. Fish and Wildlife Service pursuant to Section 7(a)(2) of the Endangered Species Act of 1973 (ESA) (87 Stat. 884, as amended 16 U.S.C. 1531 *et seq.*) is required. However, also based on your answers provided, this project may rely on the conclusion and Incidental Take Statement provided in the amended February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion (dated March 23, 2023) for Transportation Projects within the Range of the Indiana Bat and Northern Long-eared Bat.

QUALIFICATION INTERVIEW

1. Is the project within the range of the Indiana bat^[1]?

[1] See [Indiana bat species profile](#)

Automatically answered

Yes

2. Is the project within the range of the northern long-eared bat^[1]?

[1] See [northern long-eared bat species profile](#)

Automatically answered

Yes

3. Which Federal Agency is the lead for the action?

A) Federal Highway Administration (FHWA)

4. Are *all* project activities limited to non-construction^[1] activities only? (examples of non-construction activities include: bridge/abandoned structure assessments, surveys, planning and technical studies, property inspections, and property sales)

[1] Construction refers to activities involving ground disturbance, percussive noise, and/or lighting.

No

5. Does the project include *any* activities that are **greater than** 300 feet from existing road/rail surfaces^[1]?

[1] Road surface is defined as the actively used [e.g. motorized vehicles] driving surface and shoulders [may be pavement, gravel, etc.] and rail surface is defined as the edge of the actively used rail ballast.

No

6. Does the project include *any* activities **within** 0.5 miles of a known Indiana bat and/or NLEB hibernaculum^[1]?

[1] For the purpose of this consultation, a hibernaculum is a site, most often a cave or mine, where bats hibernate during the winter (see suitable habitat), but could also include bridges and structures if bats are found to be hibernating there during the winter.

No

7. Is the project located **within** a karst area?

No

8. Is there *any* suitable^[1] summer habitat for Indiana Bat or NLEB **within** the project action area^[2]? (includes any trees suitable for maternity, roosting, foraging, or travelling habitat)

[1] See the Service's [summer survey guidance](#) for our current definitions of suitable habitat.

[2] The action area is defined as all areas to be affected directly or indirectly by the Federal action and not merely the immediate area involved in the action (50 CFR Section 402.02). Further clarification is provided by the [User's Guide for the Range-wide Programmatic Consultation for Indiana Bat and Northern Long-eared Bat](#).

Yes

9. Will the project remove *any* suitable summer habitat^[1] and/or remove/trim any existing trees **within** suitable summer habitat?

[1] See the Service's [summer survey guidance](#) for our current definitions of suitable habitat.

Yes

10. Will the project clear more than 20 acres of suitable habitat per 5-mile section of road/rail?

No

11. Have presence/probable absence (P/A) summer surveys^{[1][2]} been conducted^{[3][4]} **within** the suitable habitat located within your project action area?

[1] See the Service's [summer survey guidance](#) for our current definitions of suitable habitat.

[2] Presence/probable absence summer surveys conducted within the fall swarming/spring emergence home range of a documented Indiana bat hibernaculum (contact local Service Field Office for appropriate distance from hibernacula) that result in a negative finding requires additional consultation with the local Service Field Office to determine if clearing of forested habitat is appropriate and/or if seasonal clearing restrictions are needed to avoid and minimize potential adverse effects on fall swarming and spring emerging Indiana bats.

[3] For projects within the range of either the Indiana bat or NLEB in which suitable habitat is present, and no bat surveys have been conducted, the transportation agency will assume presence of the appropriate species. This assumption of presence should be based upon the presence of suitable habitat and the capability of bats to occupy it because of their mobility.

[4] Negative presence/probable absence survey results obtained using the [summer survey guidance](#) are valid for a minimum of two years from the completion of the survey unless new information (e.g., other nearby surveys) suggest otherwise.

No

12. Does the project include activities **within documented Indiana bat habitat**^{[1][2]}?

[1] Documented roosting or foraging habitat – for the purposes of this consultation, we are considering documented habitat as that where Indiana bats and/or NLEB have actually been captured and tracked using (1) radio telemetry to roosts; (2) radio telemetry biangulation/triangulation to estimate foraging areas; or (3) foraging areas with repeated use documented using acoustics. Documented roosting habitat is also considered as suitable summer habitat within 0.25 miles of documented roosts.)

[2] For the purposes of this key, we are considering documented corridors as that where Indiana bats and/or NLEB have actually been captured and tracked to using (1) radio telemetry; or (2) treed corridors located directly between documented roosting and foraging habitat.

No

13. Will the removal or trimming of habitat or trees occur **within** suitable but **undocumented Indiana bat** roosting/foraging habitat or travel corridors?

Yes

14. What time of year will the removal or trimming of habitat or trees **within** suitable but **undocumented Indiana bat** roosting/foraging habitat or travel corridors occur^[1]?

[1] Coordinate with the local Service Field Office for appropriate dates.

B) During the inactive season

15. Does the project include activities **within documented NLEB habitat**^{[1][2]}?

[1] Documented roosting or foraging habitat – for the purposes of this consultation, we are considering documented habitat as that where Indiana bats and/or NLEB have actually been captured and tracked using (1) radio telemetry to roosts; (2) radio telemetry biangulation/triangulation to estimate foraging areas; or (3) foraging areas with repeated use documented using acoustics. Documented roosting habitat is also considered as suitable summer habitat within 0.25 miles of documented roosts.)

[2] For the purposes of this key, we are considering documented corridors as that where Indiana bats and/or NLEB have actually been captured and tracked to using (1) radio telemetry; or (2) treed corridors located directly between documented roosting and foraging habitat.

No

16. Will the removal or trimming of habitat or trees occur **within** suitable but **undocumented NLEB** roosting/foraging habitat or travel corridors?

Yes

17. What time of year will the removal or trimming of habitat or trees **within** suitable but **undocumented NLEB** roosting/foraging habitat or travel corridors occur?

B) During the inactive season

18. Will *any* tree trimming or removal occur **within** 100 feet of existing road/rail surfaces?

Yes

19. Will *any* tree trimming or removal occur **between** 100-300 feet of existing road/rail surfaces?

Yes

20. Are *all* trees that are being removed clearly demarcated?

Yes

21. Will the removal of habitat or the removal/trimming of trees include installing new or replacing existing **permanent** lighting?

No

22. Does the project include wetland or stream protection activities associated with compensatory wetland mitigation?

No

23. Does the project include slash pile burning?

No

24. Does the project include *any* bridge removal, replacement, and/or maintenance activities (e.g., any bridge repair, retrofit, maintenance, and/or rehabilitation work)?

Yes

25. Is there *any* suitable habitat^[1] for Indiana bat or NLEB **within** 1,000 feet of the bridge? (includes any trees suitable for maternity, roosting, foraging, or travelling habitat)

[1] See the Service's current [summer survey guidance](#) for our current definitions of suitable habitat.

Yes

26. Has a bridge assessment^[1] been conducted **within** the last 24 months^[2] to determine if the bridge is being used by bats?

[1] See [User Guide Appendix D](#) for bridge/structure assessment guidance

[2] Assessments must be completed no more than 2 years prior to conducting any work below the deck surface on all bridges that meet the physical characteristics described in the Programmatic Consultation, regardless of whether assessments have been conducted in the past. Due to the transitory nature of bat use, a negative result in one year does not guarantee that bats will not use that bridge/structure in subsequent years.

Yes

SUBMITTED DOCUMENTS

- *Structure Bat Assessment Form - NB Signed.pdf* <https://ipac.ecosphere.fws.gov/project/THD4LS3X7RDZDCMYATPVWHSJAU/projectDocuments/137723078>
- *Structure Bat Assessment Form SB Signed.pdf* <https://ipac.ecosphere.fws.gov/project/THD4LS3X7RDZDCMYATPVWHSJAU/projectDocuments/137723079>
- *NB ITAMS 2024.08.07.pdf* <https://ipac.ecosphere.fws.gov/project/THD4LS3X7RDZDCMYATPVWHSJAU/projectDocuments/150247763>

27. Did the bridge assessment detect *any* signs of Indiana bats and/or NLEBs roosting in/under the bridge (bats, guano, etc.)^[1]?

[1] If bridge assessment detects signs of *any* species of bats, coordination with the local FWS office is needed to identify potential threatened or endangered bat species. Additional studies may be undertaken to try to identify which bat species may be utilizing the bridge prior to allowing *any* work to proceed.

Note: There is a small chance bridge assessments for bat occupancy do not detect bats. Should a small number of bats be observed roosting on a bridge just prior to or during construction, such that take is likely to occur or does occur in the form of harassment, injury or death, the PBO requires the action agency to report the take. Report all unanticipated take within 2 working days of the incident to the USFWS. Construction activities may continue without delay provided the take is reported to the USFWS and is limited to 5 bats per project.

No

28. Will the bridge removal, replacement, and/or maintenance activities include installing new or replacing existing **permanent** lighting?

No

29. Does the project include the removal, replacement, and/or maintenance of *any* structure other than a bridge? (e.g., rest areas, offices, sheds, outbuildings, barns, parking garages, etc.)

No

30. Will the project involve the use of **temporary** lighting *during* the active season?

Yes

31. Is there *any* suitable habitat **within** 1,000 feet of the location(s) where **temporary** lighting will be used?

Yes

32. Will the project install new or replace existing **permanent** lighting?

No

33. Does the project include percussives or other activities (**not including tree removal/trimming or bridge/structure work**) that will increase noise levels above existing traffic/background levels?

No

34. Are *all* project activities that are **not associated with** habitat removal, tree removal/trimming, bridge and/or structure activities, temporary or permanent lighting, or use of percussives, limited to actions that DO NOT cause any additional stressors to the bat species?

Examples: lining roadways, unlighted signage, rail road crossing signals, signal lighting, and minor road repair such as asphalt fill of potholes, etc.

Yes

35. Will the project raise the road profile **above the tree canopy**?

No

36. Are the project activities that are not associated with habitat removal, tree removal/trimming, bridge and/or structure activities, temporary or permanent lighting, or use of percussives consistent with a No Effect determination in this key?

Automatically answered

Yes, other project activities are limited to actions that DO NOT cause any additional stressors to the bat species as described in the BA/BO

37. Is the habitat removal portion of this project consistent with a Not Likely to Adversely Affect determination in this key?

Automatically answered

Yes, because the tree removal/trimming that occurs outside of the Indiana bat's active season occurs greater than 0.5 miles from the nearest hibernaculum, is less than 100 feet from the existing road/rail surface, includes clear demarcation of the trees that are to be removed, and does not alter documented roosts and/or surrounding summer habitat within 0.25 miles of a documented roost.

38. Is the habitat removal portion of this project consistent with a Likely to Adversely Affect determination in this key?

Automatically answered

Yes, because the tree removal that occurs outside the Indiana bat's active season is 100-300 feet from the existing road/rail surface, and is not in documented roosting/foraging habitat or travel corridors.

39. Is the habitat removal portion of this project consistent with a Not Likely to Adversely Affect determination in this key?

Automatically answered

Yes, because the tree removal/trimming that occurs outside of the NLEB's active season occurs greater than 0.5 miles from the nearest hibernaculum, is less than 100 feet from the existing road/rail surface, includes clear demarcation of the trees that are to be removed, and does not alter documented roosts and/or surrounding summer habitat within 0.25 miles of a documented roost.

40. Is the habitat removal portion of this project consistent with a Likely to Adversely Affect determination in this key?

Automatically answered

Yes, because the tree removal that occurs outside the NLEB's active season is 100-300 feet from the existing road/rail surface, and is not in documented roosting/foraging habitat or travel corridors.

41. Is the bridge removal, replacement, or maintenance activities portion of this project consistent with a No Effect determination in this key?

Automatically answered

Yes, because the bridge has been assessed using the criteria documented in the BA and no signs of bats were detected

42. General AMM 1

Will the project ensure *all* operators, employees, and contractors working in areas of known or presumed bat habitat are aware of *all* FHWA/FRA/FTA (Transportation Agencies) environmental commitments, including all applicable Avoidance and Minimization Measures?

Yes

43. Tree Removal AMM 1

Can *all* phases/aspects of the project (e.g., temporary work areas, alignments) be modified, to the extent practicable, to avoid tree removal^[1] in excess of what is required to implement the project safely?

Note: Tree Removal AMM 1 is a minimization measure, the full implementation of which may not always be practicable. Projects may still be NLAA as long as Tree Removal AMMs 2, 3, and 4 are implemented and LAA as long as Tree Removal AMMs 3, 5, 6, and 7 are implemented.

[1] The word “trees” as used in the AMMs refers to trees that are suitable habitat for each species within their range. See the USFWS’ current summer survey guidance for our latest definitions of suitable habitat.

Yes

44. Tree Removal AMM 3

Can tree removal be limited to that specified in project plans and ensure that contractors understand clearing limits and how they are marked in the field (e.g., install bright colored flagging/fencing prior to any tree clearing to ensure contractors stay within clearing limits)?

Yes

45. Lighting AMM 1

Will *all temporary* lighting be directed away from suitable habitat during the active season?

Yes

46. For Indiana bat, if applicable, compensatory mitigation measures are required to offset adverse effects on the species (see Section 2.10 of the BA). Please select the mechanism in which compensatory mitigation will be implemented:

5. Unknown

PROJECT QUESTIONNAIRE

1. Please describe the proposed bridge work:

The proposed project consists of the removal of the existing bridges (#063-83-02002 northbound and southbound) that carry State Route (SR) 63 over an abandoned railroad. The project is located approximately 0.24 mile south of SR 234 in Vermillion County. The estimated project limits, without incidental construction is 250 feet, 125 feet north and 125 feet south of the centerline of structures. Incidental would be an additional 370 feet south and 360 feet north of the centerline of the structure. The existing bridge superstructures will be completely removed, and the end bents will be removed to a minimum depth of 2 feet below the proposed pavement subgrade. The bent caps will be removed from the

interior bents, leaving the columns and foundations in place. The existing concrete slope walls will not be removed from the spill slopes at each end bent. SR 63 will be reconstructed on the same horizontal and vertical alignments and will provide two 12-foot travel lanes bordered by a 4-foot median and 10-foot outside paved shoulders in each direction. Guardrail will be installed along the outside shoulders. Pavement within the project limits will be replaced and 50-feet of each approach will be milled and resurfaced. Incidental construction will include pavement milling and resurfacing, guardrail removal, and linear grading. The project will backfill the existing crossing with roadway embankment material. An additional 1.3 acres of permanent right-of-way will be acquired for this project. Approximately 0.6 acre of tree removal will be required; 0.56 acre from within 100 feet of the roadway pavement and 0.04 from 100 to 300 feet of the pavement. Suitable summer habitat is located adjacent to the project area on the east and west sides of SR 63. Suitable summer habitat will be removed as a result of this project.
The review of the USFWS database on September 8, 2023, did not indicate the presence of endangered bat species within 0.5 mile of the project area. The most recent bridge inspections were conducted on May 25, 2023 (southbound) and August 7, 2024 (northbound), and no evidence of bats was found, but evidence of swallows nests was observed. Temporary lighting may be used for this project. The project's scheduled letting date is October 8, 2025, with construction anticipated to be in winter 2025.

2. How many acres^[1] of trees are proposed for removal between 100-300 feet of the existing road/rail surface?

[1] If described as number of trees, multiply by 0.09 to convert to acreage and enter that number.

0.04

3. Please state the timing of all proposed bridge work:

Winter 2025

4. **Please verify:**

All tree removal will occur greater than 0.5 mile from any hibernaculum.

Yes, I verify that all tree removal will occur greater than 0.5 miles from any hibernaculum.

5. Is the project location 0-100 feet from the edge of existing road/rail surface?

Yes

6. Is the project location 100-300 feet from the edge of existing road/rail surface?

Yes

7. Please enter the date of the bridge assessment:

August 7, 2024 and May 25, 2023

8. Have you made a No Effect determination for *all* other species indicated on the FWS IPaC generated species list?

Yes

9. **Please verify:**

No documented Indiana bat roosts or surrounding summer habitat within 0.25 mile of documented roosts will be impacted between May 1 and July 31.

Yes, I verify that no documented Indiana bat roosts or surrounding summer habitat within 0.25 mile of documented roosts will be impacted during this period.

10. Have you made a May Affect determination for any other species on the FWS IPaC generated species list?

No

11. **Please verify:**

No documented NLEB roosts or surrounding summer habitat within 150 feet of documented roosts will be impacted between June 1 and July 31.

Yes, I verify that no documented NLEB roosts or surrounding summer habitat within 150 feet of documented roosts will be impacted during this period.

12. How many acres^[1] of trees are proposed for removal between 0-100 feet of the existing road/rail surface?

[1] If described as number of trees, multiply by 0.09 to convert to acreage and enter that number.

0.56

13. You have indicated that the following Avoidance and Minimization Measures (AMMs) will be implemented as part of the proposed project:

- *Tree Removal AMM 1*
- *Lighting AMM 1*
- *Tree Removal AMM 3*
- *General AMM 1*

AVOIDANCE AND MINIMIZATION MEASURES (AMMS)

This determination key result includes the commitment to implement the following Avoidance and Minimization Measures (AMMs):

TREE REMOVAL AMM 1

Modify all phases/aspects of the project (e.g., temporary work areas, alignments) to avoid tree removal.

LIGHTING AMM 1

Direct temporary lighting away from suitable habitat during the active season.

TREE REMOVAL AMM 3

Ensure tree removal is limited to that specified in project plans and ensure that contractors understand clearing limits and how they are marked in the field (e.g., install bright colored flagging/fencing prior to any tree clearing to ensure contractors stay within clearing limits).

GENERAL AMM 1

Ensure all operators, employees, and contractors working in areas of known or presumed bat habitat are aware of all FHWA/FRA/FTA (Transportation Agencies) environmental commitments, including all applicable AMMs.

DETERMINATION KEY DESCRIPTION: FHWA, FRA, FTA PROGRAMMATIC CONSULTATION FOR TRANSPORTATION PROJECTS AFFECTING NLEB OR INDIANA BAT

This key was last updated in IPaC on October 30, 2023. Keys are subject to periodic revision.

This decision key is intended for projects/activities funded or authorized by the Federal Highway Administration (FHWA), Federal Railroad Administration (FRA), and/or Federal Transit Administration (FTA), which may require consultation with the U.S. Fish and Wildlife Service (Service) under Section 7 of the Endangered Species Act (ESA) for the endangered **Indiana bat** (*Myotis sodalis*) and the endangered **northern long-eared bat** (NLEB) (*Myotis septentrionalis*).

This decision key should only be used to verify project applicability with the Service's [amended February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion \(dated March 23, 2023\) for Transportation Projects](#). The programmatic biological opinion covers limited transportation activities that may affect either bat species, and addresses situations that are both likely and not likely to adversely affect either bat species. This decision key will assist in identifying the effect of a specific project/activity and applicability of the programmatic consultation. The programmatic biological opinion is not intended to cover all types of transportation actions. Activities outside the scope of the programmatic biological opinion, or that may affect ESA-listed species other than the Indiana bat or NLEB, or any designated critical habitat, may require additional ESA Section 7 consultation.

IPAC USER CONTACT INFORMATION

Agency: Indiana Department of Transportation

Name: Robabeh Asadpour

Address: 41 West 300 North

City: Crawfordsville

State: IN

Zip: 47933

Email: rasadpour@indot.in.gov

Phone: 7653615621

LEAD AGENCY CONTACT INFORMATION

Lead Agency: Federal Highway Administration



United States Department of the Interior

Fish and Wildlife Service



Indiana Field Office (ES)
620 South Walker Street
Bloomington, IN 47403-2121
Phone: (812) 334-4261 Fax: (812) 334-4273

October 23, 2024

Ms. Karstin Carmany-George
Federal Highway Administration
575 N. Pennsylvania Street, Room 254
Indianapolis, Indiana 46204
(Sent via email)

USFWS Project Code: 2024-0041810

RE: Des. 2100968 and 2100969 - SR 63 over Abandoned Railroad, Vermillion County, IN.

Dear Ms. Carmany-George:

The U.S. Fish and Wildlife Service (Service) is responding to your request dated October 7, 2024, to verify that the proposed SR 63 over Abandoned Railroad Project (the Project) may rely on the amended February 5, 2018, Programmatic Biological Opinion (BO) (dated March 23, 2023) for federally funded or approved transportation projects that may affect the federally listed endangered Indiana bat (*Myotis sodalis*) and/or federally listed endangered northern long-eared bat (NLEB) (*Myotis septentrionalis*). We received your request and the associated Likely to Adversely Affect (LAA) Consistency Letter on October 7, 2024.

This letter provides the Service's response as to whether the Federal Highway Administration (FHWA) may rely on the BO to comply with Section 7(a)(2) of the Endangered Species Act of 1973 (ESA) (87 Stat. 884, as amended; 16 U.S.C. 1531 *et seq.*) for the Project's effects to the Indiana bat and NLEB.

The FHWA has determined that the Project is *likely to adversely affect* the Indiana bat and/or the NLEB.

Conclusion

The Service has reviewed the effects of the proposed Project, which includes the FHWA's commitment to implement any applicable mitigation measures as indicated on the LAA Consistency Letter. We confirm that the proposed Project's effects are consistent with those analyzed in the BO. The Service has determined that projects consistent with the conservation measures and scope of the program analyzed in the BO are not likely to jeopardize the continued existence of the Indiana bat or the NLEB. In coordination with your agency and the other sponsoring Federal Transportation Agencies, the Service will reevaluate this conclusion annually considering any new pertinent information under the adaptive management provisions of the BO.

Incidental Take

Indiana Bat and Northern Long-eared Bat

Tree Removal

The Service anticipates that tree removal associated with the proposed Project will cause incidental take (IT) of Indiana bats and NLEBs. As described in the IT Statement (ITS) of the BO, quantifying the specific number of individuals affected is not practicable. Therefore, the Services uses a surrogate (acreage of tree removal) to prove a means of expressing and monitoring take of the Indiana bat and the NLEB.

The proposed Project will remove **0.6 acres** of trees from habitat that is suitable for the Indiana bat and NLEB. All tree removal will occur in winter and comply with all other conservation measures in the BO. Based on the BO, 0.56 acres of the removal are not anticipated to result in any adverse effects, and **0.04 acres** are anticipated to result in adverse effects.

The FHWA used the mitigation ratio of 1.75 from Table 3 of the BO¹ to calculate the compensatory mitigation required to offset adverse impacts to the Indiana bat for a total of 0.07 acres² of trees that is suitable for the Indiana bat. **Mitigation is not required for the NLEB.**

To comply with the mitigation requirements of the BO, the FHWA will contribute **\$881.16** to The Conservation Fund (TCF), the Program Sponsor, within 1 year of this letter or prior to the start of construction, whichever is earliest. These calculations are based on the mitigation identified above² and the 2024 Land Use Values in Table 2 of Exhibit E in TCF's In Lieu Fee Instrument³. If payment is made later than 1 year from the date of this letter, the mitigation cost may change because of updated land use values in Table 2 of Exhibit E. The FHWA or designated non-federal representative must notify TCF at least five days prior to payment so that TCF can verify that the appropriate land value has been used. At the time of payment, the FHWA or designated non-federal representative shall notify the Service of compliance with the compensatory mitigation requirements as described above.

The purchase of species conservation credits and/or in-lieu fee contributions shall occur prior to construction of a transportation project covered under this programmatic BO. Exceptions to this program stipulation include emergency projects that do not require a letting prior to construction. In these cases, purchase of credits and/or in-lieu fee contributions shall occur within three months of completion of the project. This timeframe allows for measuring the acres of habitat affected by the emergency project and for financial processing.

1 <https://www.fws.gov/media/compensatory-mitigation-ratios-indiana-bat-table-3-biological-opinion>

2 XX acres * XX ratio

3 <https://www.fws.gov/sites/default/files/documents/IBAT-NLEB-ILF-Exhibit-E-Fee-Schedule-2023-01-04.pdf>

Bridge, Culvert, and/or Structure Activities

Range-wide, the Service estimates IT of a small number of Indiana bats and/or NLEBs is reasonably certain to occur at up to 10 bridges/culverts or structures in a 12-month period when signs of bat use or occupancy are observed. For projects in which five or fewer bats are indicated, take will be covered under the ITS in the BO. Some of this take may occur when an initial bridge/culvert or structure bat assessment fails to detect Indiana bat or NLEB use or occupancy, yet bats are later detected prior to, or during construction. In this situation, potential IT of Indiana bats and/or NLEBs may be exempted provided that the take is reported to the Service. Please contact this Service office and submit the Post Assessment Discovery of Bats at Bridge/Culvert or Structure Form (User Guide Appendix E) within 2 working days of any such incidents.

Tricolored Bat

On September 13, 2022, the Service published a proposal in the Federal Register to list the tricolored bat (*Perimyotis subflavus*) as endangered under the ESA. The Service has up to 12-months from the date the proposal was published to make a final determination, either to list the tricolored bat under the ESA or to withdraw the proposal. The Service determined the bat faces extinction primarily due to the range-wide impacts of white-nose syndrome (WNS), a deadly fungal disease affecting cave-dwelling bats across North America. Because tricolored bat populations have been greatly reduced due to WNS, surviving bat populations are now more vulnerable to other stressors such as human disturbance and habitat loss. Species proposed for listing are not afforded protection under the ESA; however, as soon as a listing becomes effective (typically 30 days after publication of the final rule in the Federal Register), the prohibitions against jeopardizing its continued existence and “take” will apply. Therefore, if this project or other future or existing projects have the potential to adversely affect tricolored bats after the potential new listing goes into effect, we recommend that the effects of the project on tricolored bat and their habitat be analyzed to determine whether authorization under ESA section 7 is necessary. Projects or programs with an existing section 7 biological opinion may require reinitiation of consultation.

The tricolored bat is a small insectivorous bat that typically overwinters in caves, abandoned mines and tunnels, and road-associated culverts (southern portion of the range) and spends the rest of the year in forested habitats, typically roosting among live and dead leaf clusters. For more information on tricolored bats and the proposed rule, please see:

<https://www.fws.gov/species/tricolored-bat-perimyotis-subflavus>

and for more information on WNS, please see: <https://www.whitenosesyndrome.org/>

Reasonable and Prudent Measures

The Service will add the acreage of Project-related tree removal to the annual total acreage attributed to the BO as a surrogate measure of Indiana bat and NLEB IT and exempted from the prohibitions of section 9 of the ESA. Such exemption is effective as long as your agency implements the reasonable and prudent measure (RPM) and accompanying terms and conditions of the BO’s ITS.

The sole RPM of the BO's ITS requires the Federal Transportation Agencies to ensure that State/Local transportation agencies, who choose to include eligible projects under the programmatic action, incorporate all applicable conservation measures in the project proposals submitted to the Service for ESA section 7 compliance using the BO. The implementing terms and conditions for this RPM require the Federal Transportation Agencies to offer training to appropriate personnel about using the BO, and promptly report sick, injured, or dead bats (regardless of species) or any other federally listed species located at the project site.

Reporting Dead or Injured Bats

The FHWA, its State/Local cooperators, and any contractors must take care when handling dead or injured Indiana bats and NLEBs, or any other federally listed species that are found at the project site to preserve biological material in the best possible condition and to protect the handler from exposure to diseases, such as rabies. Project personnel are responsible for ensuring that any evidence about determining the cause of death or injury is not unnecessarily disturbed. Reporting the discovery of dead or injured listed species is required in all cases to enable the Service to determine whether the level of IT exempted by this BO has been exceeded, and to ensure that the terms and conditions are appropriate and effective. Parties finding a dead, injured, or sick specimen of any endangered or threatened species must promptly notify this Service Office.

Reinitiation Notice

This letter concludes consultation for the Project, which qualifies for inclusion in the BO issued to the Federal Transportation Agencies. To maintain this inclusion, a reinitiation of this Project-level consultation is required where the FHWA's discretionary involvement or control over the Project has been retained (or is authorized by law) and if:

1. the amount or extent of IT of Indiana bats or NLEBs is exceeded;
2. new information reveals that the Project may affect listed species or critical habitat in a manner or to an extent not considered in the BO;
3. the Project is subsequently modified in a manner that causes an effect to listed species or designated critical habitat not considered in the BO; or
4. a new species is listed or critical habitat designated that may be affected by the Project.

Per condition #1 above, the anticipated IT is exceeded when:

- the Project removes more than 0.04 acre(s) of habitat suitable for the Indiana bat and/or NLEB beyond 100 feet from the edge of pavement; and/or
- the Project takes more than 5 Indiana bats and/or 5 NLEBs resulting from bridge, culvert, or structure activity⁴.

⁴ Annual reports will be completed each year as described in the *Monitoring and Reporting* section of the BO to track the number of projects range-wide where IT of Indiana bat and/or NLEB is reasonably certain to occur from bridge, culvert, or structures activities per annual reporting year.

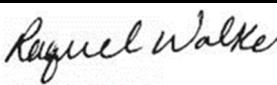
In instances where the amount or extent of IT is exceeded, the FHWA is required to immediately request a reinitiation of this Project-level consultation.

We appreciate your continued efforts to ensure that this Project is fully consistent with all applicable provisions of the BO. If you have any questions regarding our response or if you need additional information, please contact Robin McWilliams Munson at Robin_McWilliams@fws.gov.

Sincerely,
ROBIN
MCWILLIAMS-
MUNSON
For Susan E. Cooper
Field Office Supervisor

 Digitally signed by ROBIN
MCWILLIAMS-MUNSON
Date: 2024.10.23 16:13:50
-04'00'

Date & Time of Assessment	March 25, 2023 13:00	DOT Project Number	2100968/2100969	Route/Facility Carried	SR 63	County	Vermillion
Federal Structure ID	063-83-02002 ANBL	Structure Coordinates (latitude and longitude)	39.9488, -87.45124	Structure Height (approximate)		Structure Length	174 feet
Structure Type (check one)			Structure Material (check all that apply)				
<i>Bridge Construction Style</i>			<i>Deck Material</i>	<i>Beam Material</i>	<i>End/Back Wall Material</i>		
<input checked="" type="radio"/> Cast-in-place		<input type="radio"/> Pre-stressed Girder		Metal	None	Concrete	
<input type="radio"/> Flat Slab/Box		<input type="radio"/> Steel I-beam		<input checked="" type="checkbox"/> Concrete	Concrete	Timber	
<input type="radio"/> Truss		<input type="radio"/> Covered		Timber	<input checked="" type="checkbox"/> Steel	Stone/Masonry	
<input type="radio"/> Parallel Box Beam		<input type="radio"/> Other:		Open grid	Timber	Other:	
<input type="radio"/> Other:		<input type="radio"/> Other:		Other:	Other:	Creosote Evidence	
<i>Culvert Type</i>			<i>Culvert Material</i>		<input type="radio"/> Yes	<input checked="" type="checkbox"/> No	<input type="radio"/> Unknown
<input type="radio"/> Box		<input type="radio"/> Other Structure	Metal	Concrete	<i>Notes:</i>		
<input type="radio"/> Pipe/Round			Plastic	Stone/Masonry			
<input type="radio"/> Other:			Other:				
Crossings Traversed (check all that apply)			Surrounding Habitat (check all that apply)				
Bare ground	<input checked="" type="checkbox"/> Open vegetation	<input checked="" type="checkbox"/> Agricultural	Grassland				
Rip-rap	<input type="checkbox"/> Closed vegetation	<input type="checkbox"/> Commercial	Ranching				
Flowing water	<input type="checkbox"/> Railroad	<input type="checkbox"/> Residential-urban	Riparian/wetland				
Standing water	<input type="checkbox"/> Road/trail - Type:	<input type="checkbox"/> Residential-rural	Mixed use				
Seasonal water	<input checked="" type="checkbox"/> Other: Abandoned Railroad	<input checked="" type="checkbox"/> Woodland/forested	Other:				
Areas Assessed (check all that apply)							
Check all areas that apply. If an area is not present in the structure, check the "not present" box.							
Document all bat indicators observed during the assessment. Include the species present, if known, and provide photo documentation as indicated.							
Area (check if assessed)	Assessment Notes		Evidence of Bats (include photos if present)				
<input type="checkbox"/> All crevices and cracks:	<input checked="" type="checkbox"/> Not present		<input type="checkbox"/> Visual - live #	<input type="checkbox"/> dead #	<input type="checkbox"/> Audible	<input type="checkbox"/> Species	
<input checked="" type="checkbox"/> Bridges/culverts: rough surfaces or imperfections in concrete			<input type="checkbox"/> Guano	<input type="checkbox"/> Odor	<input type="checkbox"/> Photos		
<input type="checkbox"/> Other structures: soffits, rafters, attic areas			<input type="checkbox"/> Staining				
<input checked="" type="checkbox"/> Concrete surfaces (open roosting on concrete)	<input checked="" type="checkbox"/> Not present		<input type="checkbox"/> Visual - live #	<input type="checkbox"/> dead #	<input type="checkbox"/> Audible	<input type="checkbox"/> Species	
<input checked="" type="checkbox"/> Spaces between concrete end walls and the bridge deck	<input checked="" type="checkbox"/> Not present		<input type="checkbox"/> Guano	<input type="checkbox"/> Odor	<input type="checkbox"/> Photos		
<input checked="" type="checkbox"/> Crack between concrete railings on top of the bridge deck	<input checked="" type="checkbox"/> Not present		<input type="checkbox"/> Staining		<input type="checkbox"/> Audible	<input type="checkbox"/> Species	
<input checked="" type="checkbox"/> Vertical surfaces on concrete I-beams	<input checked="" type="checkbox"/> Not present		<input type="checkbox"/> Visual - live #	<input type="checkbox"/> dead #	<input type="checkbox"/> Odor		
<input checked="" type="checkbox"/> Spaces between walls, ceiling joists	<input checked="" type="checkbox"/> Not present		<input type="checkbox"/> Guano	<input type="checkbox"/> Photos	<input type="checkbox"/> Photos		
<input checked="" type="checkbox"/> Weep holes, scupper drains, and inlets/pipes	<input checked="" type="checkbox"/> Not present		<input type="checkbox"/> Staining		<input type="checkbox"/> Audible	<input type="checkbox"/> Species	
<input checked="" type="checkbox"/> All guiderails	<input checked="" type="checkbox"/> Not present		<input type="checkbox"/> Visual - live #	<input type="checkbox"/> dead #	<input type="checkbox"/> Odor		
<input checked="" type="checkbox"/> All expansion joints	<input checked="" type="checkbox"/> Not present		<input type="checkbox"/> Guano	<input type="checkbox"/> Photos	<input type="checkbox"/> Photos		
			<input type="checkbox"/> Staining		<input type="checkbox"/> Audible	<input type="checkbox"/> Species	
Name: Raquel Walker			Signature:				

Date & Time of Assessment	March 25, 2023 13:00	DOT Project Number	2100968/2100969	Route/Facility Carried	SR 63	County	Vermillion	
Federal Structure ID	063-83-02002 ANBL	Structure Coordinates (latitude and longitude)	39.9488, -87.45124	Structure Height (approximate)		Structure Length	174 feet	
Structure Type (check one)			Structure Material (check all that apply)					
Bridge Construction Style			Deck Material	Beam Material	End/Back Wall Material			
<input type="radio"/> Cast-in-place		<input type="radio"/> Pre-stressed Girder		Metal	None	Concrete		
<input type="radio"/> Flat Slab/Box		<input type="radio"/> Steel I-beam		Concrete	Concrete	Timber		
<input type="radio"/> Truss		<input type="radio"/> Covered		Timber	Steel	Stone/Masonry		
<input type="radio"/> Parallel Box Beam		<input type="radio"/> Other:		Open grid	Timber	Other:		
<input type="radio"/> Other:		<input type="radio"/> Other:		Other:	Other:	Creosote Evidence		
Culvert Type			Culvert Material		<input type="radio"/> Yes	<input type="radio"/> Unknown	<input type="radio"/> No	
<input type="radio"/> Box		<input type="radio"/> Other Structure	Metal		Notes:			
<input type="radio"/> Pipe/Round			Concrete					
<input type="radio"/> Other:			Plastic					
Crossings Traversed (check all that apply)			Surrounding Habitat (check all that apply)					
<input type="radio"/> Bare ground	<input checked="" type="checkbox"/> Open vegetation	<input checked="" type="checkbox"/> Agricultural	Grassland					
<input type="radio"/> Rip-rap	<input type="checkbox"/> Closed vegetation	<input type="checkbox"/> Commercial	Ranching					
<input type="radio"/> Flowing water	<input type="checkbox"/> Railroad	<input type="checkbox"/> Residential-urban	Riparian/wetland					
<input type="radio"/> Standing water	<input type="checkbox"/> Road/trail - Type:	<input type="checkbox"/> Residential-rural	Residential-rural		Mixed use			
<input type="radio"/> Seasonal water	<input checked="" type="checkbox"/> Other: Abandoned Railroad	<input checked="" type="checkbox"/> Woodland/forested	Woodland/forested		Other:			
Areas Assessed (check all that apply)								
Check all areas that apply. If an area is not present in the structure, check the "not present" box.								
Document all bat indicators observed during the assessment. Include the species present, if known, and provide photo documentation as indicated.								
Area (check if assessed)		Assessment Notes		Evidence of Bats (include photos if present)				
<input type="checkbox"/> All crevices and cracks:		<input checked="" type="checkbox"/> Not present		<input type="checkbox"/> Visual - live #	<input type="checkbox"/> dead #	<input type="checkbox"/> Audible	<input type="checkbox"/> Species	
<input checked="" type="checkbox"/> Bridges/culverts: rough surfaces or imperfections in concrete				<input type="checkbox"/> Guano	<input type="checkbox"/> Odor	<input type="checkbox"/> Photos		
<input checked="" type="checkbox"/> Other structures: soffits, rafters, attic areas				<input type="checkbox"/> Staining				
<input checked="" type="checkbox"/> Concrete surfaces (open roosting on concrete)		<input checked="" type="checkbox"/> Not present		<input type="checkbox"/> Visual - live #	<input type="checkbox"/> dead #	<input type="checkbox"/> Audible	<input type="checkbox"/> Species	
				<input type="checkbox"/> Guano	<input type="checkbox"/> Odor	<input type="checkbox"/> Photos		
				<input type="checkbox"/> Staining				
<input checked="" type="checkbox"/> Spaces between concrete end walls and the bridge deck		<input checked="" type="checkbox"/> Not present		<input type="checkbox"/> Visual - live #	<input type="checkbox"/> dead #	<input type="checkbox"/> Audible	<input type="checkbox"/> Species	
				<input type="checkbox"/> Guano	<input type="checkbox"/> Odor	<input type="checkbox"/> Photos		
				<input type="checkbox"/> Staining				
<input checked="" type="checkbox"/> Crack between concrete railings on top of the bridge deck		<input checked="" type="checkbox"/> Not present		<input type="checkbox"/> Visual - live #	<input type="checkbox"/> dead #	<input type="checkbox"/> Audible	<input type="checkbox"/> Species	
				<input type="checkbox"/> Guano	<input type="checkbox"/> Odor	<input type="checkbox"/> Photos		
				<input type="checkbox"/> Staining				
<input checked="" type="checkbox"/> Vertical surfaces on concrete I-beams		<input checked="" type="checkbox"/> Not present		<input type="checkbox"/> Visual - live #	<input type="checkbox"/> dead #	<input type="checkbox"/> Audible	<input type="checkbox"/> Species	
				<input type="checkbox"/> Guano	<input type="checkbox"/> Odor	<input type="checkbox"/> Photos		
				<input type="checkbox"/> Staining				
<input checked="" type="checkbox"/> Spaces between walls, ceiling joists		<input checked="" type="checkbox"/> Not present		<input type="checkbox"/> Visual - live #	<input type="checkbox"/> dead #	<input type="checkbox"/> Audible	<input type="checkbox"/> Species	
				<input type="checkbox"/> Guano	<input type="checkbox"/> Odor	<input type="checkbox"/> Photos		
				<input type="checkbox"/> Staining				
<input checked="" type="checkbox"/> Weep holes, scupper drains, and inlets/pipes		<input checked="" type="checkbox"/> Not present		<input type="checkbox"/> Visual - live #	<input type="checkbox"/> dead #	<input type="checkbox"/> Audible	<input type="checkbox"/> Species	
				<input type="checkbox"/> Guano	<input type="checkbox"/> Odor	<input type="checkbox"/> Photos		
				<input type="checkbox"/> Staining				
<input checked="" type="checkbox"/> All guiderails		<input checked="" type="checkbox"/> Not present		<input type="checkbox"/> Visual - live #	<input type="checkbox"/> dead #	<input type="checkbox"/> Audible	<input type="checkbox"/> Species	
				<input type="checkbox"/> Guano	<input type="checkbox"/> Odor	<input type="checkbox"/> Photos		
				<input type="checkbox"/> Staining				
<input checked="" type="checkbox"/> All expansion joints		<input checked="" type="checkbox"/> Not present		<input type="checkbox"/> Visual - live #	<input type="checkbox"/> dead #	<input type="checkbox"/> Audible	<input type="checkbox"/> Species	
				<input type="checkbox"/> Guano	<input type="checkbox"/> Odor	<input type="checkbox"/> Photos		
				<input type="checkbox"/> Staining				
Name: Raquel Walker				Signature: 				

Appendix D:

Section 106 of the NHPA

Minor Projects PA Project Submittal and Assessment Form

SECTION 1

Submittal of this form is only required for projects where Category B applies. Projects qualifying under Category A do not require submittal of this form. SECTION 2 (for Conditions of Category B.1 for curb/sidewalk) or SECTION 3 (for Conditions of Category B.9 for drainage structures) may be required as determined by INDOT-Cultural Resources Office (INDOT-CRO) review. INDOT-CRO will notify the applicant if the Minor Projects PA does not apply.

Part I: Project Information-Completed by Applicant (Consultant/PM/Project Sponsor/INDOT District Staff)*

**A qualified professional historian (QP) is not required to complete Part I. INDOT-Cultural Resources Office (INDOT-CRO) staff will be responsible for completion of Part II.*

Original Submission Date: 1/4/24 **Amended Submission Date*:**

**Consult with INDOT-CRO to determine whether an amendment is required. For revisions/updates to original form, please detail in applicable sections below. Please use red font to distinguish the revisions/updates.*

Submitted By (Provide Name and Firm/Organization):

Alyssa Reynolds
Cultural Resource Analysts, Inc.
201 NW 4th Street, Suite 204
Evansville, Indiana 47708
adreynolds@crai-ky.com
812.549.4503

Project Designation Number: 2100968 & 2100969

Route Number: SR 63

Feature crossed (if applicable): Abandoned Railroad

City/Township: Cayuga/Eugene

County: Vermillion

Project Description: The SR 63 bridge project is located 0.24 mile south of SR 234 in Vermillion County, Indiana. The purpose of this project is to improve user safety, improve transportation infrastructure reliability, provide the necessary geometric criteria for the roadway, and reduce the long-term maintenance costs at the project site. The need of this project is to address the deterioration of the existing structures that continues to worsen and compromises the safety and reliable transportation infrastructure for the motoring public. In addition, these bridges are nearing the end of their design life. Recent bridge inspection reports noted the structures were in overall “fair” condition (five out of nine). In addition, the October 20, 2021 bridge inspection report for the southbound bridge documented a critical find, resulting in emergency lane closures until repairs could be made.

The proposed work includes removal of the existing twin bridge superstructures and construction of a new structure over the abandoned railroad for a future pathway. The existing 174' x 43.4' structures (Bridge Number [No.] 063-83-02002 ANBL/ASBL; National Bridge Inventory [NBI] No.: 022727 northbound [NB] and 022729 southbound [SB]) consist of twin three-span continuous steel beam bridges built in 1976. The bridges had latex-modified concrete (LCM) overlays installed in 1987, the wearing surface was epoxy injected in 2019, and substantial beam and bearing repairs were completed in 2020.

The existing bridges will be removed. A 14-foot span, 13.5-foot rise, three-sided small structure will be installed. The proposed structure would be approximately 232 feet long with 2-foot, 6-inch minimum headwalls with precast concrete wingwalls, or mechanically stabilized earth (MSE) retaining walls. The MSE walls could be used in lieu of traditional cast-in-place concrete headwalls and wingwalls to shorten the proposed structure length. SR 63 would

Minor Projects PA Project Submittal and Assessment Form

be reconstructed on the same horizontal and vertical alignments and would provide two 12-foot travel lanes, one in each direction, bordered by a 4-foot inside and 10-foot outside paved shoulders in each direction.

The project limits will be approximately 250 feet long, measured 125 feet each way from the centerline of the existing bridges. Pavement within the project limits will be replaced, including the underdrains. Incidental construction limits would extend approximately 300 feet beyond the project limits for each approach. Work in the incidental construction limits would primarily include 50 feet of milling and resurfacing, guardrail removal, and linear grading. Median drainage structures would be reconstructed as required to maintain the existing drainage pattern.

Two temporary crossovers constructed under separate construction contracts (one north of SR 234 and one south of Maple Street) will be left in place for use on this project. They will be repaired and resurfaced as required and will be removed at the conclusion of construction.

The first phase of construction will involve constructing the full length of the 3-sided structure beneath the existing bridges while they remain in place. Backfill will be placed as high as practical to allow for compaction.

For the second phase of construction, each direction of SR 63 will be reduced to a single lane. The southbound lane will crossover to the northbound roadway. The existing northbound SR 63 roadway will carry one 11.5-foot lane in each direction separated by flexible tubular markers with lane separators. The exiting southbound structure will be removed, and the southbound roadway and embankment will be constructed.

For the third phase of construction, both lanes of traffic on the northbound roadway and structure will be crossed over to the southbound roadway. The southbound SR 63 roadway will carry one 11.5-foot lane in each direction separated by flexible tubular markers with lane separators. The existing northbound structure would be removed, and the northbound roadway and embankment will be constructed. Upon completion of this phase, the temporary crossovers will be removed, and the median restored to the preconstruction condition.

Approximately 1.3 acres of permanent right-of-way (ROW) will be acquired for the proposed project.

If the project includes any curb, curb ramp, or sidewalk work, please specify the location(s) of such work:
None.

For bridge or small structure projects, please list feature crossed, structure number, NBI number, and structure type:

The existing 174' x 43.4' structures (Bridge No. 063-83-02002 ANBL/ASBL; NBI No.: 022727 NB and 022729 SB) consist of twin three-span continuous steel beam bridges. The bridges carry the northbound and southbound lanes of SR 63 over an abandoned railroad.

**For bridge projects, is the bridge included in INDOT's Historic Bridge Inventory
(<https://www.in.gov/indot/2531.htm>)?**

Yes No

If yes, did the inventory determine the bridge eligible for or listed in the National Register of Historic Places? Please provide page # of entry in Historic Bridge Inventory.

Yes No

Inventory Page # _____

Will there be right-of-way acquisition as part of this project?

Yes No

Minor Projects PA Project Submittal and Assessment Form

If yes was checked above, please check all that apply:

Permanent Temporary Reacquisition

If applicable, identify right-of-way acquisition locations in text below and in attached mapping. Please specify how much (both temporary and permanent) and indicate what activities are included in the proposed right-of-way: The 1.3 acres of permanent ROW will be acquired for the bridge replacement, construction access, and the embankment.

Is there any potential for additional temporary right-of-way to be needed later for purposes such as access, staging, etc.?

Yes No

Archaeology (check one):

All proposed activities are presumed to occur in previously disturbed soils.
**INDOT-CRO will notify you if the project area includes undisturbed soils and requires an archaeological reconnaissance.*

Project takes place in undisturbed soils and the archaeology report is included in submission

**If an archaeology report is required, the Minor Projects PA Form will not be finalized until the report is reviewed and approved by INDOT-CRO. For INDOT-sponsored projects, INDOT-CRO may be able to complete the archaeological investigation. If you would like to request that INDOT-CRO complete an archaeological investigation, please contact the INDOT-CRO archaeology team lead. See CRM Pt. 1 Ch. 3 for current contact information.*

Please specify all applicable categories and condition(s) (highlight applicable conditions in yellow)*:

B-3. Construction of added travel, turning, or auxiliary lanes (e.g., bicycle, truck climbing, acceleration and deceleration lanes) and shoulder widening under the following conditions **[BOTH Condition A, which pertains to Archaeological Resources, and Condition B, which pertains to Above-Ground Resources, must be satisfied]**:

Condition A (Archaeological Resources)

One of the two conditions listed below must be met (*EITHER Condition i or Condition ii must be satisfied*):

- i. Work occurs in previously disturbed soils; *OR*
- ii. Work occurs in undisturbed soils and an archaeological investigation conducted by the applicant and reviewed by INDOT Cultural Resources Office determines that no National Register-listed or potentially National Register-eligible archaeological resources are present within the project area. If the archaeological investigation locates National Register-listed or potentially National Register-eligible archaeological resources, then full Section 106 review will be required. Copies of any archaeological reports prepared for the project will be provided to the DHPA and any archaeological site form information will be entered directly into the SHAARD by the applicant. The archaeological reports will also be available for viewing (by Tribes only) on INSCOPE.

Condition B (Above-Ground Resources)

Work does not occur adjacent to or within a National Register-listed or National Register-eligible district or individual above-ground resource.

B-12. Replacement, widening, or raising the elevation of the superstructure on existing bridges, and bridge replacement projects (when both the superstructure and substructure are removed), under the following conditions **[BOTH Condition A, which pertains to Archaeological Resources, and Condition B, which pertains to Above-Ground Resources, must be satisfied]**:

Minor Projects PA Project Submittal and Assessment Form

Condition A (Archaeological Resources)

One of the two conditions listed below must be met (*EITHER Condition i or Condition ii must be satisfied*):

- i. Work occurs in previously disturbed soils; *OR*
- ii. Work occurs in undisturbed soils and an archaeological investigation conducted by the applicant and reviewed by INDOT Cultural Resources Office determines that no National Register-listed or potentially National Register-eligible archaeological resources are present within the project area. If the archaeological investigation locates National Register-listed or potentially National Register-eligible archaeological resources, then full Section 106 review will be required. Copies of any archaeological reports prepared for the project will be provided to the DHPA and any archaeological site form information will be entered directly into the SHAARD by the applicant. The archaeological reports will also be available for viewing (by Tribes only) on INSCOPE.

Condition B (Above-Ground Resources)

The conditions listed below must be met (***BOTH Condition i and Condition ii must be satisfied***):

- i. Work does not occur adjacent to or within a National Register-listed or National Register-eligible district or individual above-ground resource; *AND*
- ii. With regard to the subject bridge, at least one of the conditions listed below is satisfied (*AT LEAST one of the conditions a, b or c, must be fulfilled*):
 - a. The latest Historic Bridge Inventory identified the bridge as non-historic (see <http://www.in.gov/indot/2531.htm>);
 - b. The bridge was built after 1945, and is a common type as defined in Section V. of the *Program Comment Issued for Streamlining Section 106 Review for Actions Affecting Post-1945 Concrete and Steel Bridges* issued by the Advisory Council on Historic Preservation on November 2, 2012 for so long as that Program Comment remains in effect AND the considerations listed in Section IV of the Program Comment do not apply.
 - c. The bridge is part of the Interstate system and was determined not eligible for the National Register under the Section 106 Exemption Regarding Effects to the Interstate Highway System adopted by the Advisory Council on Historic Preservation on March 10, 2005, for so long as that Exemption remains in effect.

Check if SECTION 2: Minor Projects PA Category B-1, Condition B-ii Submission is included.

Check if SECTION 3: Minor Projects PA Category B-9, Condition B-i-c-2 or B-ii-b-3 Submission is included.

Part II: Completed by INDOT-CRO

Amendments will be shown in red font.

Information reviewed (please check all that apply):

General project location map USGS map Aerial photograph Soil survey data

General project area photos Archaeology Reports Historic Property Reports

Indiana Historic Buildings, Bridges, and Cemeteries Map/Interim Report

Bridge inspection information/BIAS Historic Bridge Inventory Database

SHAARD SHAARD GIS Streetview Imagery County GIS Data/Property Cards

Minor Projects PA Project Submittal and Assessment Form

Other (please specify):

Rusche, Michael and Lisa J. Kelley

2024 *A Phase Ia Archaeological Survey for the Proposed SR 63 Bridge Removal and New Pedestrian Underpass over Abandoned Railroad in Vermillion County, Indiana (Des. No. 2100968 and 2100969).*

Archaeological Short Report prepared by Cultural Resources Analysts, Evansville, for Beam, Longest and Neff, Indianapolis. Document on file at INDOT-CRO.

Are there any commitments associated with this project? If yes, please explain in the Additional Comments Section below. yes no

Does the project result in a de minimis impact to a Section 4(f) protected historic resource? If yes, please explain in the Additional Comments Section below. yes no

Additional Comments:

Above-ground Resources

An INDOT-Cultural Resources Office (CRO) historian who meets the Secretary of the Interior's Professional Qualification Standards as per 36 CFR Part 61 first performed a desktop review, checking the Indiana Register of Historic Sites and Structures (State Register) and National Register of Historic Places (National Register) lists for Vermillion County. No listed resources are present within 0.25 mile of the project area, a distance that serves as an adequate area of potential effects given the project scope and terrain.

The National Register & IHSSI information for Vermillion County is available in the Indiana State Historic Architectural and Archaeological Research Database (SHAARD) and the Indiana Historic Buildings, Bridges, and Cemeteries Map (IHBBCM). The *Vermillion County Interim Report* (2000; Eugene Township, Cayuga Scattered Sites) of the Indiana Historic Sites and Structures Inventory (IHSSI) was also consulted. The SHAARD information was checked against the Interim Report hard copy maps. The IHBBCM contains the most up to date IHSSI information. No IHSSI documented properties rated above "Contributing" are located within 0.25 mile to the project area.

According to the IHSSI rating system, generally properties rated "Contributing" do not possess the level of historical or architectural significance necessary to be considered individually National Register eligible, although they would contribute to a historic district. If they retain material integrity, properties rated "Notable" might possess the necessary level of significance after further research. Properties rated "Outstanding" usually possess the necessary level of significance to be considered National Register eligible if they retain material integrity. Historic districts identified in the IHSSI are usually considered eligible for the National Register.

The INDOT-CRO historian reviewed structures adjacent to the project area utilizing online aerial, street-view photography, and the Vermillion County GIS website. The project area is located in a rural setting surrounded by dense vegetation and agricultural fields. The building stock within the 0.25-mile buffer consists of residential and commercial structures ranging from mid twentieth to early twenty-first century. None of the immediately adjacent building stock appears to possess the significance or integrity to be considered National Register-eligible.

The most recent inspection report (M. Hughes; 08/08/2022) from the Bridge Inspection Application Systems (BIAS) was referenced to review both subject structures. INDOT Bridge No. 063-83-02002 ANBL (NBI No.: 022727). Likewise, the most recent report (D. Bewley; 10/20/2021) was utilized to review INDOT Bridge No. 063-83-02002 ASBL (NBI No.: 022729). Both carry SR 63 over an abandoned railroad and are three-span concrete cast-in-place bridges that were constructed in 1976 and reconstructed in 1987. Structures built after 1965 were not included in the data-gathering conducted for the 2009 INDOT-sponsored Indiana Historic Bridge Inventory (HBI).

Minor Projects PA Project Submittal and Assessment Form

On November 12, 2012, the Advisory Council on Historic Preservation (AHP) issued the Program Comment for Streamlining Section 106 Review for Actions Affecting Post-1945 Concrete and Steel Bridges (*Program Comment*). The *Program Comment* relieves federal agencies from the Section 106 requirement to consider the effects of undertakings on most concrete and steel bridges built after 1945. On March 19, 2013, federal agencies were approved to use the *Program Comment* for Indiana projects.

The *Program Comment* applies for INDOT Bridge No. 063-83-02002 ANBL (NBI 022727) and for INDOT Bridge No. 063-83-02002 ASBL (NBI 022729) because they have not been previously listed in or determined eligible for listing in the National Register of Historic Places and are not located in or adjacent to a historic district (Section IV.A of the *Program Comment*). As an example of a concrete cast-in-place, the bridges were also not one of the types exempted from the *Program Comment* (arch bridges, truss bridges, bridges with movable spans, suspension bridges, cable-stayed bridges, or covered bridges [Section IV.B]). Additionally, the bridges have not been identified as having exceptional significance for association with a person or event, being a very early or particularly important example of their type in the state or the nation, having distinctive engineering or architectural features that depart from standard designs, or displaying other elements that were engineered to respond to a unique environmental context (Section IV.C). These bridges also have not been identified as having some exceptional quality. Based on consultation between FHWA, INDOT, SHPO and interested parties, no bridges with exceptional significance were identified in Indiana (Section IV.C). Because the above criteria from the *Program Comment* have been met, no individual consideration under Section 106 is required for INDOT Bridge No. 063-83-020020; ANBL/ASBL; NBI No.: 022727 [NB] and 022729 [SB].

Based on the available information, as summarized above, no above-ground concerns exist.

Archaeological Resources

An INDOT-CRO archaeologist who meets the Secretary of the Interior's Professional Qualification Standards as per 36 CFR Part 61 reviewed the Phase Ia field reconnaissance report completed for the project by Cultural Resource Analysts (CRA) (Rusche and Kelley 2024). A review of SHAARD records identified no archaeological sites have been previously recorded within or adjacent to the project area.

A 2.3-hectare (5.7-acre) survey area was investigated via a combination of shovel probing ($n=20$), and visual inspection of obviously disturbed areas. No archaeological resources were identified, and no additional work is recommended. It is our opinion that the report is acceptable, and we concur with the evaluations and recommendations made by CRA (Rusche and Kelley 2024).

Therefore, there are no archaeological concerns provided that the project scope and footprint do not change.

Accidental Discovery: If any archaeological artifacts or human remains are uncovered during construction, demolition, or earth moving activities, construction within 100 feet of the discovery will be stopped, and INDOT-CRO and the Indiana Division of Natural Resources-Division of Historic Preservation and Archaeology (IDNR-DHPA) will be notified immediately.

INDOT-CRO staff reviewer(s): Taylor Payne, Clint Kelly, and Dawn Alexander

INDOT Approval Date: June 14, 2024

Amendment Approval Date (if applicable):

****Be sure to attach this form to the National Environmental Policy Act documentation for this project. Also, the NEPA documentation shall reference and include the description of the specific stipulation in the PA that qualifies the project as exempt from further Section 106 review.*

Minor Projects PA Project Submittal and Assessment Form

SECTION 1

The submittal of this form is only required for projects where Category B applies. Projects qualifying under Category A do not require submitting this form. SECTION 2 (for Conditions of Category B.1 for curb/sidewalk) or SECTION 3 (for Conditions of Category B.9 for drainage structures) may be required as determined by INDOT-Cultural Resources Office (INDOT-CRO) review. INDOT-CRO will notify the applicant if the Minor Projects PA does not apply.

Part I: Project Information-Completed by Applicant (Consultant/PM/Project Sponsor/INDOT District Staff)*

**A qualified professional historian (QP) is not required to complete Part I. THE INDOT-Cultural Resources Office (INDOT-CRO) staff will be responsible for completion of Part II.*

Original Submission Date: 1/4/24 **Amended Submission Date*:** **10/23/24**

**Consult with INDOT-CRO to determine whether an amendment is required. For revisions/updates to original form, please detail in applicable sections below. Please use red font to distinguish the revisions/updates.*

Submitted By (Provide Name and Firm/Organization):

Alyssa Reynolds
Cultural Resource Analysts, Inc.
201 NW 4th Street, Suite 204
Evansville, Indiana 47708
adreynolds@crai-ky.com
812.549.4503

Project Designation Number: 2100968 & 2100969

Route Number: SR 63

Feature crossed (if applicable): Abandoned Railroad

City/Township: Cayuga/Eugene **County:** Vermillion

Project Description: The SR 63 bridge project is located 0.24 mile south of SR 234 in Vermillion County, Indiana. The purpose of this project is to improve user safety, improve transportation infrastructure reliability, provide the necessary geometric criteria for the roadway, and reduce the long-term maintenance costs at the project site. The need of this project is to address the deterioration of the existing structures that continues to worsen and compromises the safety and reliable transportation infrastructure for the motoring public. In addition, these bridges are nearing the end of their design life. Recent bridge inspection reports noted the structures were in overall “fair” condition (five out of nine). In addition, the October 20, 2021, bridge inspection report for the southbound bridge documented a critical find, resulting in emergency lane closures until repairs could be made.

The proposed work includes removal of the existing twin bridge superstructures. **A new structure will no longer be constructed** over the abandoned railroad for a future pathway. The existing 174' x 43.4' structures (Bridge Number [No.] 063-83-02002 ANBL/ASBL; National Bridge Inventory [NBI] No.: 022727 northbound [NB] and 022729 southbound [SB]) consist of twin three-span continuous steel beam bridges built in 1976. The bridges had latex-modified concrete (LCM) overlays installed in 1987, the wearing surface was epoxy injected in 2019, and substantial beam and bearing repairs were completed in 2020.

The existing bridges will be removed. **The end bents will be removed to a minimum depth of 2 feet below the proposed pavement subgrade. The bent caps will be removed from the interior bents, leaving the columns and foundations in place. The existing concrete slope walls will not be removed from the spill slopes at each end bent.**

Minor Projects PA Project Submittal and Assessment Form

SR 63 will be reconstructed on the same horizontal and vertical alignments and will provide two 12-foot travel lanes bordered by a 4-foot median and 10-foot outside paved shoulders in each direction. The existing median and side slopes meet current standards and will be maintained. Pavement within the project limits will be replaced and 50 feet of each approach will be milled and resurfaced. Incidental construction will include pavement milling and resurfacing, guardrail removal, and linear grading. The project will backfill the existing crossing with roadway embankment material.

Two temporary crossovers constructed under separate construction contracts (one north of SR 234 and one south of Maple Street) will be left in place for use on this project. They will be repaired and resurfaced as required and will be removed at the conclusion of construction.

The first phase of construction will involve constructing the full length of the 3-sided structure beneath the existing bridges while they remain in place. Backfill will be placed as high as practical to allow for compaction.

For the second phase of construction, each direction of SR 63 will be reduced to a single lane. The southbound lane will crossover to the northbound roadway. The existing northbound SR 63 roadway will carry one 11.5-foot lane in each direction separated by flexible tubular markers with lane separators. The exiting southbound structure will be removed, and the southbound roadway and embankment will be constructed.

For the third phase of construction, both lanes of traffic on the northbound roadway and structure will be crossed over to the southbound roadway. The southbound SR 63 roadway will carry one 11.5-foot lane in each direction separated by flexible tubular markers with lane separators. The existing northbound structure would be removed, and the northbound roadway and embankment will be constructed. Upon completion of this phase, the temporary crossovers will be removed, and the median restored to the preconstruction condition.

The project requires approximately 1.2 acres of permanent right-of-way (ROW), 210 feet west of the centerline of southbound SR 63 and 210 feet east of northbound SR 63. The ROW will be acquired from one parcel, formerly the Norfolk and Southern Railroad corridor, which is located within INDOT apparent ROW. All ROW being acquired is presently within INDOT's apparent ROW, and the acquisition of ROW is to establish legally documented ownership.

If the project includes any curb, curb ramp, or sidewalk work, please specify the location(s) of such work:
None.

For bridge or small structure projects, please list feature crossed, structure number, NBI number, and structure type:

The existing 174' x 43.4' structures (Bridge No. 063-83-02002 ANBL/ASBL; NBI No.: 022727 NB and 022729 SB) consist of twin three-span continuous steel beam bridges. The bridges carry the northbound and southbound lanes of SR 63 over an abandoned railroad.

For bridge projects, is the bridge included in INDOT's Historic Bridge Inventory (<https://www.in.gov/indot/2531.htm>)?

Yes No

If yes, did the inventory determine the bridge eligible for or listed in the National Register of Historic Places? Please provide page # of entry in Historic Bridge Inventory.

Yes No

Inventory Page # _____

Will there be right-of-way acquisition as part of this project?

Yes No

Minor Projects PA Project Submittal and Assessment Form

If yes was checked above, please check all that apply:

Permanent Temporary Reacquisition

If applicable, identify right-of-way acquisition locations in text below and in attached mapping. Please specify how much (both temporary and permanent) and indicate what activities are included in the proposed right-of-way: The 1.2 acres of permanent ROW will be acquired for the bridge replacement, construction access, and the embankment.

Is there any potential for additional temporary right-of-way to be needed later for purposes such as access, staging, etc.?

Yes No

Archaeology (check one):

All proposed activities are presumed to occur in previously disturbed soils.

**INDOT-CRO will notify you if the project area includes undisturbed soils and requires an archaeological reconnaissance.*

Project takes place in undisturbed soils and the archaeology report is included in submission

**If an archaeology report is required, the Minor Projects PA Form will not be finalized until the report is reviewed and approved by INDOT-CRO. For INDOT-sponsored projects, INDOT-CRO may be able to complete the archaeological investigation. If you would like to request that INDOT-CRO complete an archaeological investigation, please contact the INDOT-CRO archaeology team lead. See CRM Pt. 1 Ch. 3 for current contact information.*

Please specify all applicable categories and condition(s) (highlight applicable conditions in yellow) *:

B-3. Construction of added travel, turning, or auxiliary lanes (e.g., bicycle, truck climbing, acceleration and deceleration lanes) and shoulder widening under the following conditions **[BOTH Condition A, which pertains to Archaeological Resources, and Condition B, which pertains to Above-Ground Resources, must be satisfied]**:

Condition A (Archaeological Resources)

One of the two conditions listed below must be met (EITHER Condition i or Condition ii must be satisfied):

i. Work occurs in previously disturbed soils; OR

ii. Work occurs in undisturbed soils and an archaeological investigation conducted by the applicant and reviewed by INDOT Cultural Resources Office determines that no National Register-listed or potentially National Register-eligible archaeological resources are present within the project area. If the archaeological investigation locates National Register-listed or potentially National Register-eligible archaeological resources, then full Section 106 review will be required. Copies of any archaeological reports prepared for the project will be provided to the DHPA and any archaeological site form information will be entered directly into the SHAARD by the applicant. The archaeological reports will also be available for viewing (by Tribes only) on INSCOPE.

Condition B (Above-Ground Resources)

Work does not occur adjacent to or within a National Register-listed or National Register-eligible district or individual above-ground resource.

B-12. Replacement, widening, or raising the elevation of the superstructure on existing bridges, and bridge replacement projects (when both the superstructure and substructure are removed), under the following conditions **[BOTH Condition A, which pertains to Archaeological Resources, and Condition B, which pertains to Above-Ground Resources, must be satisfied]**:

Minor Projects PA Project Submittal and Assessment Form

Condition A (Archaeological Resources)

One of the two conditions listed below must be met (*EITHER Condition i or Condition ii must be satisfied*):

- i. Work occurs in previously disturbed soils; *OR*
- ii. Work occurs in undisturbed soils and an archaeological investigation conducted by the applicant and reviewed by INDOT Cultural Resources Office determines that no National Register-listed or potentially National Register-eligible archaeological resources are present within the project area. If the archaeological investigation locates National Register-listed or potentially National Register-eligible archaeological resources, then full Section 106 review will be required. Copies of any archaeological reports prepared for the project will be provided to the DHPA and any archaeological site form information will be entered directly into the SHAARD by the applicant. The archaeological reports will also be available for viewing (by Tribes only) on INSCOPE.

Condition B (Above-Ground Resources)

The conditions listed below must be met (*BOTH Condition i and Condition ii must be satisfied*):

- i. Work does not occur adjacent to or within a National Register-listed or National Register-eligible district or individual above-ground resource; *AND*
- ii. With regard to the subject bridge, at least one of the conditions listed below is satisfied (*AT LEAST one of the conditions a, b or c, must be fulfilled*):
 - a. The latest Historic Bridge Inventory identified the bridge as non-historic (see <http://www.in.gov/indot/2531.htm>);
 - b. The bridge was built after 1945, and is a common type as defined in Section V. of the *Program Comment Issued for Streamlining Section 106 Review for Actions Affecting Post-1945 Concrete and Steel Bridges* issued by the Advisory Council on Historic Preservation on November 2, 2012 for so long as that Program Comment remains in effect AND the considerations listed in Section IV of the Program Comment do not apply.
 - c. The bridge is part of the Interstate system and was determined not eligible for the National Register under the Section 106 Exemption Regarding Effects to the Interstate Highway System adopted by the Advisory Council on Historic Preservation on March 10, 2005, for so long as that Exemption remains in effect.

Check if SECTION 2: Minor Projects PA Category B-1, Condition B-ii Submission is included.

Check if SECTION 3: Minor Projects PA Category B-9, Condition B-i-c-2 or B-ii-b-3 Submission is included.

Part II: Completed by INDOT-CRO

Amendments will be shown in red font.

Information reviewed (please check all that apply):

General project location map USGS map Aerial photograph Soil survey data

General project area photos Archaeology Reports Historic Property Reports

Indiana Historic Buildings, Bridges, and Cemeteries Map/Interim Report

Bridge inspection information/BIAS Historic Bridge Inventory Database

SHAARD SHAARD GIS Streetview Imagery County GIS Data/Property Cards

Minor Projects PA Project Submittal and Assessment Form

Other (please specify):

Rusche, Michael and Lisa J. Kelley

2024 *A Phase Ia Archaeological Survey for the Proposed SR 63 Bridge Removal and New Pedestrian Underpass over Abandoned Railroad in Vermillion County, Indiana (Des. No. 2100968 and 2100969).*

Archaeological Short Report prepared by Cultural Resources Analysts, Evansville, for Beam, Longest and Neff, Indianapolis. Document on file at INDOT-CRO.

Rusche, Michael

2024 *A Phase Ia Archaeological Addendum Survey for the Proposed SR 63 Bridge Removal Over an Abandoned Railroad in Vermillion County, Indiana (Des. Nos. 2100968 and 2100969). Cultural Resource Analysts, Inc., Evansville. Prepared for Egis BLN USA, Inc., Indianapolis. Document on file at INDOT-CRO.*

Are there any commitments associated with this project? If yes, please explain in the Additional Comments Section below. yes no

Does the project result in a de minimis impact to a Section 4(f) protected historic resource? If yes, please explain in the Additional Comments Section below. yes no

Additional Comments:

Above-ground Resources

An INDOT-Cultural Resources Office (CRO) historian who meets the Secretary of the Interior's Professional Qualification Standards as per 36 CFR Part 61 first performed a desktop review, checking the Indiana Register of Historic Sites and Structures (State Register) and National Register of Historic Places (National Register) lists for Vermillion County. No listed resources are present within 0.25 mile of the project area, a distance that serves as an adequate area of potential effects given the project scope and terrain.

The National Register & IHSSI information for Vermillion County is available in the Indiana State Historic Architectural and Archaeological Research Database (SHAARD) and the Indiana Historic Buildings, Bridges, and Cemeteries Map (IHBBCM). The *Vermillion County Interim Report* (2000; Eugene Township, Cayuga Scattered Sites) of the Indiana Historic Sites and Structures Inventory (IHSSI) was also consulted. The SHAARD information was checked against the Interim Report hard copy maps. The IHBBCM contains the most up to date IHSSI information. No IHSSI documented properties rated above "Contributing" are located within 0.25 mile to the project area.

According to the IHSSI rating system, generally properties rated "Contributing" do not possess the level of historical or architectural significance necessary to be considered individually National Register eligible, although they would contribute to a historic district. If they retain material integrity, properties rated "Notable" might possess the necessary level of significance after further research. Properties rated "Outstanding" usually possess the necessary level of significance to be considered National Register eligible if they retain material integrity. Historic districts identified in the IHSSI are usually considered eligible for the National Register.

The INDOT-CRO historian reviewed structures adjacent to the project area utilizing online aerial, street-view photography, and the Vermillion County GIS website. The project area is located in a rural setting surrounded by dense vegetation and agricultural fields. The building stock within the 0.25-mile buffer consists of residential and commercial structures ranging from mid twentieth to early twenty-first century. None of the immediately adjacent building stock appears to possess the significance or integrity to be considered National Register-eligible.

Minor Projects PA Project Submittal and Assessment Form

The most recent inspection report (M. Hughes; 08/08/2022) from the Bridge Inspection Application Systems (BIAS) was referenced to review both subject structures. INDOT Bridge No. 063-83-02002 ANBL (NBI No.: 022727). Likewise, the most recent report (D. Bewley; 10/20/2021) was utilized to review INDOT Bridge No. 063-83-02002 ASBL (NBI No.: 022729). Both carry SR 63 over an abandoned railroad and are three-span concrete cast-in-place bridges that were constructed in 1976 and reconstructed in 1987. Structures built after 1965 were not included in the data-gathering conducted for the 2009 INDOT-sponsored Indiana Historic Bridge Inventory (HBI).

On November 12, 2012, the Advisory Council on Historic Preservation (AChP) issued the Program Comment for Streamlining Section 106 Review for Actions Affecting Post-1945 Concrete and Steel Bridges (*Program Comment*). The *Program Comment* relieves federal agencies from the Section 106 requirement to consider the effects of undertakings on most concrete and steel bridges built after 1945. On March 19, 2013, federal agencies were approved to use the *Program Comment* for Indiana projects.

The *Program Comment* applies for INDOT Bridge No. 063-83-02002 ANBL (NBI 022727) and for INDOT Bridge No. 063-83-02002 ASBL (NBI 022729) because they have not been previously listed in or determined eligible for listing in the National Register of Historic Places and are not located in or adjacent to a historic district (Section IV.A of the *Program Comment*). As an example of a concrete cast-in-place, the bridges were also not one of the types exempted from the *Program Comment* (arch bridges, truss bridges, bridges with movable spans, suspension bridges, cable-stayed bridges, or covered bridges [Section IV.B]). Additionally, the bridges have not been identified as having exceptional significance for association with a person or event, being a very early or particularly important example of their type in the state or the nation, having distinctive engineering or architectural features that depart from standard designs, or displaying other elements that were engineered to respond to a unique environmental context (Section IV.C). These bridges also have not been identified as having some exceptional quality. Based on consultation between FHWA, INDOT, SHPO and interested parties, no bridges with exceptional significance were identified in Indiana (Section IV.C). Because the above criteria from the *Program Comment* have been met, no individual consideration under Section 106 is required for INDOT Bridge No. 063-83-020020; ANBL/ASBL; NBI No.: 022727 [NB] and 022729 [SB].

November 2024 update

In October 2024, INDOT-CRO was notified of project scope changes requiring CRO to review these changes. There is a slight enlargement of the project area, however the original review buffer of adjacent above-ground structures was sufficient for this change. No above-ground concerns exist, and no further evaluation is needed.

Based on the available information, as summarized above, no above-ground concerns exist.

Archaeological Resources

An INDOT-CRO archaeologist who meets the Secretary of the Interior's Professional Qualification Standards as per 36 CFR Part 61 reviewed the Phase Ia field reconnaissance report completed for the project by Cultural Resource Analysts (CRA) (Rusche and Kelley 2024). A review of SHAARD records identified no archaeological sites have been previously recorded within or adjacent to the project area.

A 2.3-hectare (5.7-acre) survey area was investigated via a combination of shovel probing ($n=20$), and visual inspection of obviously disturbed areas. No archaeological resources were identified, and no additional work is recommended. It is our opinion that the report is acceptable, and we concur with the evaluations and recommendations made by CRA (Rusche and Kelley 2024).

Therefore, there are no archaeological concerns provided that the project scope and footprint do not change.

October 2024 Update

Minor Projects PA Project Submittal and Assessment Form

An INDOT-CRO archaeologist who meets the Secretary of the Interior's Professional Qualifications Standards as per 36 CFR Part 61 reviewed the proposed project area amendment and accompanying addendum Phase Ia field reconnaissance survey report completed for the project by Cultural Resource Analysts, Inc. (Rusche 2024). There are no previously recorded archaeological sites within or adjacent to the adjusted project area. The addendum survey covered an additional 1.1 hectares (2.7 acres), which was subjected to visual inspection of areas with obvious disturbance at 30-m intervals, systematic shovel probing (n= 22) of areas with suspected undisturbed soils at 15-m intervals, and auger probing (n= 2) to investigate the potential for the adjusted project area to contain deeply buried archaeological resources. No archaeological resources were documented as a result of the survey, and no additional investigation is recommended (Rusche 2024).

Therefore, there remain no archaeological concerns provided that the project scope and footprint do not change.

Accidental Discovery: If any archaeological artifacts or human remains are uncovered during construction, demolition, or earth moving activities, construction within 100 feet of the discovery will be stopped, and INDOT-CRO and the Indiana Division of Natural Resources-Division of Historic Preservation and Archaeology (IDNR-DHPA) will be notified immediately.

INDOT-CRO staff reviewer(s): Taylor Payne, Clint Kelly, Dawn Alexander, and David Walton

INDOT Approval Date: June 14, 2024

Amendment Approval Date (if applicable): November 18, 2024

****Be sure to attach this form to the National Environmental Policy Act documentation for this project. Also, the NEPA documentation shall reference and include the description of the specific stipulation in the PA that qualifies the project as exempt from further Section 106 review.*



INDIANA ARCHAEOLOGICAL SHORT REPORT

State Form 54566 (R3 / 3-22)

INDIANA DEPARTMENT OF NATURAL RESOURCES
DIVISION OF HISTORIC PRESERVATION AND ARCHAEOLOGY
402 West Washington Street, Room W274
Indianapolis, Indiana 46204-2739
Telephone Number: (317) 232-1646
Fax Number: (317) 232-0693
E-mail: dhpain.dnr.IN.gov

Where applicable, the use of this form is recommended but not required by the Division of Historic Preservation and Archaeology (DHPA).

Name(s) of author(s) Michael Rusche	Date (month, day, year) April 10, 2024
Title of project A Phase Ia Archaeological Survey for the Proposed SR 63 Bridge Removal and New Pedestrian Underpass over Abandoned Railroad in Vermillion County, Indiana (Des. No. 2100968 and 2100969)	
This document is being used to report on the results of: <input type="checkbox"/> Records check only <input checked="" type="checkbox"/> Records check and Phase Ia archaeological reconnaissance <input type="checkbox"/> An addendum to a previous archaeological report. For an addendum, provide the following information.	
Name(s) of author(s) of previous report N/A	
Title of previous report N/A	
Date of previous report (month, day, year) N/A	DHPA number N/A

PROJECT OVERVIEW

Description of project

The SR 63 bridge project is located 386 m (1,267 ft) south of SR 234 east of the town of Cayuga in Vermillion County, Indiana (Figures 1 and 2). The purpose of this project is to provide structurally sufficient bridges to convey traffic on SR 63 over the abandoned railroad. The need for this project is to address the existing structures' worsening, deteriorating conditions. In addition, these bridges are nearing the end of their design life.

The proposed work includes removal of the existing twin 53 m (174 ft) by 13 m (43 ft) bridge superstructures and construction of new structures over the abandoned railroad for a future pathway. The existing 53 m (174 ft) by 13 m (43 ft) structures consist of twin three-span continuous steel beam bridges, originally built in 1976. The bridges had latex-modified concrete (LMC) overlays installed in 1987, the wearing surface was epoxy-injected in 2019, and substantial beam and bearing repairs were completed in 2020.

The existing bridges will be removed. A 4.3 m (14.0 ft) span, 4.1 m (13.5 ft) rise, three-sided small structure will be installed. The proposed structure would be approximately 70.7 m (232.0 ft) long with 0.8 m (2.5 ft) minimum headwalls with precast concrete wingwalls, or mechanically stabilized earth (MSE) retaining walls. The MSE walls could be used in lieu of traditional cast-in-place concrete headwalls and wingwalls to shorten the proposed structure length. SR 63 would be reconstructed on the same horizontal and vertical alignments and would provide two 3.7 m (12.0 ft) travel lanes, one in each direction, bordered by a 1.2 m (4.0 ft) inside and 3.0 m (10.0 ft) outside paved shoulders in each direction.

The project limits will be approximately 250 ft long, measured 125 ft each way from the centerline of the existing bridges. Pavement within the project limits will be replaced, including the underdrains. Incidental construction limits would extend approximately 300 ft beyond the project limits for each approach. Work in the incidental construction limits would primarily include 50 ft of milling and resurfacing, guardrail removal, and linear grading. Median drainage structures would be reconstructed as required to maintain the existing drainage pattern.

Approximately 1.3 acres of new permanent right-of-way (ROW) will be acquired for the project. All ROW needs and construction limits for the proposed project are enclosed within the current survey area, which encompasses 2.3 ha (5.7 acres) (Figure 3).

INDOT designation number(s) 2100968 & 2100969	Project number CRA Project No. I230427; CRA Publication Series No. 23-447	DHPA number N/A	DHPA plan number N/A
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Prepared for: (Company / Institution / Agency)

Beam, Longest & Neff, LLC

Name of contact

Brian Shaw

Address (number and street, city, state, and ZIP code)

8320 Craig Street, Suite 204, Indianapolis, IN 46250

Telephone number
(317) 849-5832

E-mail address
bshaw@b-l-n.com

Explanation / justification

There are no previously recorded archaeological sites within the survey area; however, there appeared to be undisturbed landforms that had not been subjected to previous archaeological investigations.

Phase 1a archaeological reconnaissance (Check all that apply)

- No Phase 1a reconnaissance was conducted.
- Phase 1a reconnaissance located no archaeological resources.
- Previously recorded sites were in the project area.
 - Artifacts and/or features at a previously recorded site(s) within the project area were not discovered. List the site(s) below.
- Phase 1a reconnaissance has identified landforms conducive to buried archaeological deposits. Describe below.

List sites.

N/A

Describe landforms.

N/A

Number of shovel probes excavated 20	Number of cores / auger probes 0
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Describe disturbances. Attach photographs documenting disturbances.

Disturbances were prevalent throughout the survey area. As discussed above, primary causes include road and bridge construction within the existing ROW, buried utilities, sloping embankments, drainage improvements, and a dirt road where the abandoned railroad is shown on historic maps (Figures 6–8).

Actual area surveyed (hectares) 2.3	Actual area surveyed (acres) 5.7
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Explain results of fieldwork.

As previously mentioned, the majority of the survey area was on a sloping embankment located on the east and west sides of SR 63 (see Figure 7) and was unable to be tested. The only portions of the survey area that were able to be shovel tested were the median between the north and southbound lanes of SR 63 (Figure 9) and underneath the bridges (see Figure 8). The primary soil series within this portion of the survey area is the Armiesburg silty clay loam, with a small portion towards the south being mapped within the Genesee series.

Throughout the shovel test survey, a single profile was observed throughout the survey area. This consisted of a dark brown (10YR 3/3) silt loam with abundant gravel inclusions to approximately 7 cm below ground surface. This was underlain by impenetrable gravel fill.

No archaeological materials were found in the survey area.

RECOMMENDATIONS

Records check (Check all that apply)

- No archaeological investigation is recommended before the project is allowed to proceed because the records check has determined that the project area does not have the potential to contain archaeological resources.
- A Phase 1a archaeological reconnaissance is recommended.
- Based upon the records check results, a Phase 1a archaeological reconnaissance was recommended and has been conducted.
- A cemetery development plan may be required under Indiana Code 14-21-1-26.5 because project ground disturbance will be within 100 feet of a cemetery.

Phase 1a archaeological reconnaissance (Check all that apply)

- It is recommended that the project be allowed to proceed as planned because the Phase 1a archaeological reconnaissance has located no archaeological sites within the project area and/or previously recorded sites that were investigated warrant no additional investigation.
- It is recommended that Phase 1c archaeological subsurface reconnaissance be conducted before the project is allowed to proceed. The Phase 1a archaeological reconnaissance has determined that the project area includes landforms which have the potential to contain buried archaeological deposits.

Other recommendations / commitments

The survey did not locate any archaeological sites or the potential for buried archaeological deposits. Based on this evidence, archaeological clearance is recommended.

Pursuant to IC-14-21-1, if any archaeological artifacts or human remains are uncovered during construction, demolition, or earthmoving activities, state law (Indiana Code 14-21-1-27 and 29) requires that the discovery must be reported to the Department of Natural Resources within two (2) business days. In that event, please call (317) 232-1646.

REQUIRED ATTACHMENTS



INDIANA ARCHAEOLOGICAL SHORT REPORT

State Form 54566 (R3 / 3-22)

INDIANA DEPARTMENT OF NATURAL RESOURCES
DIVISION OF HISTORIC PRESERVATION AND ARCHAEOLOGY
402 West Washington Street, Room W274
Indianapolis, Indiana 46204-2739
Telephone Number: (317) 232-1646
Fax Number: (317) 232-0693
E-mail: dhpia@dnr.IN.gov

Where applicable, the use of this form is recommended but not required by the Division of Historic Preservation and Archaeology (DHPA).

Name(s) of author(s) Michael Rusche, RPA 5909	Date (month, day, year) October 14, 2024
Title of project A Phase Ia Archaeological Addendum Survey for the Proposed SR 63 Bridge Removal Over an Abandoned Railroad in Vermillion County, Indiana (Des. Nos. 2100968 and 2100969)	
This document is being used to report on the results of: <input type="checkbox"/> Records check only <input type="checkbox"/> Records check and Phase 1a archaeological reconnaissance <input checked="" type="checkbox"/> An addendum to a previous archaeological report. For an addendum, provide the following information.	
Name(s) of author(s) of previous report Michael Rusche	
Title of previous report A Phase Ia Archaeological Survey for the Proposed SR 63 Bridge Removal and New Pedestrian Underpass over Abandoned Railroad in Vermillion County, Indiana (Des. No. 2100968 and 2100969)	
Date of previous report (month, day, year) April 10, 2024	DHPA number N/A

PROJECT OVERVIEW

Description of project

The SR 63 bridge project is located 386.0 m (1,266.4 ft) south of SR 234 in Vermillion County, Indiana (Figures 1 and 2). The purpose of this project is to improve user safety and transportation infrastructure reliability, provide the necessary geometric criteria for the roadway, and reduce the long-term maintenance costs at the project site. The need for this project is to address the deterioration of the existing structures that continue to worsen and compromise the safety and reliable transportation infrastructure for the motoring public. In addition, these bridges are nearing the end of their design life. Recent bridge inspection reports noted the structures were in overall "fair" condition (five out of nine). In addition, the October 20, 2021, bridge inspection report for the southbound bridge documented a critical find, resulting in emergency lane closures until repairs could be made.

The proposed work includes removal of the existing twin bridge superstructures and construction of a new structure over the abandoned railroad for a future pathway. The existing 53.0 m (174.0 ft) by 13.2 m (43.4 ft) structures consist of twin three-span, continuous steel beam bridges built in 1976. The bridges had latex-modified concrete overlays installed in 1987, the wearing surface was epoxy injected in 2019, and substantial beam and bearing repairs were completed in 2020.

The existing bridges will be removed. The end bents will be removed to a minimum depth of 0.6 m (2.0 ft) below the proposed pavement subgrade. The bent caps will be removed from the interior bents, leaving the columns and foundations in place. The existing concrete slope walls will not be removed from the spill slopes at each end bent.

SR 63 will be reconstructed on the same horizontal and vertical alignments and will provide two 3.7 m (12.0 ft) travel lanes bordered by a 1.2 m (4.0 ft) median and 3.0 m (10.0 ft) outside paved shoulders in each direction. The existing median and sideslopes meet current standards and will be maintained. Pavement within the project limits will be replaced and 15.2 m (50.0 ft) of each approach will be milled and resurfaced. Incidental construction will include pavement milling and resurfacing, guardrail removal, and linear grading. The project will backfill the existing crossing with roadway embankment material.

The project requires approximately 0.5 ha (1.2 acres) of permanent right-of-way (ROW), 64.0 m (210.0 ft) west of the centerline of southbound SR 63 and 64.0 m (210.0 ft) east of northbound SR 63. The ROW will be acquired from one parcel, formerly the Norfolk and Southern Railroad corridor, which is located within Indiana Department of Transportation (INDOT) apparent ROW. All ROW being acquired is presently within INDOT's apparent ROW, and the acquisition of ROW is to establish legally documented ownership. The potential ROW needs encompass 3.4 ha (8.4 acres). A survey (Rusche 2024) was previously conducted in 2023 that covered 2.3 ha (5.7 acres); however, since the time of this survey, project plans were changed to include additional areas that had not been subjected to a survey. These new addendum areas cover an additional 1.1 ha (2.7 acres) (Figure 3).

INDOT designation number(s) 2100968 & 2100969	Project number CRA Project No. I240401; Contract Publication Series 24-472	DHPA number N/A	DHPA plan number N/A
--	---	--------------------	-------------------------

List sites.
N/A

Describe landforms.
N/A

Number of shovel probes excavated 22	Number of cores / auger probes 2
---	-------------------------------------

Describe disturbances. Attach photographs documenting disturbances.
Disturbances throughout the survey area were prevalent and consisted of dirt/gravel access roads, sloping embankment, and overhead utility lines (Figures 4–6).

Actual area surveyed (hectares) 1.1	Actual area surveyed (acres) 2.7
--	-------------------------------------

Explain results of fieldwork.
A total of 1.1 ha of grassy fields, access roads, sloping embankment, and secondary-growth forests were subjected to investigation during the addendum survey (see Figures 4–6; Figures 7 and 8). Portions of the current addendum survey area were located along gravel/dirt access roads, sloping embankment, or underneath overhead utility lines, and disturbances related to these factors prevented testing in these portions (see Figure 3). In addition, there were no areas of the current investigation that were suitable for pedestrian survey.

The portions of the addendum survey area that were investigated with systematic shovel testing were located along a narrow strip at the base of a sloping embankment from SR 63, and were located primarily within the Armiesburg and Genesee soil series. Throughout the investigated area, three common soil profiles were revealed.

The first profile, recorded west of SR 63, consisted of a dark brown (10YR 3/3) silty clay loam from the ground surface to 21 cm below ground surface (bgs). This was underlain by a brown (10YR 5/3) silty clay loam to 35 cm bgs. This profile was recorded in portions of the survey area mapped within the Armiesburg soil series, and is consistent with the expected range of attributes and characteristics. Additionally, within the portions of the survey area directly north or south of the abandoned railroad corridor, in the current location of a dirt access road, there were common unnatural gravel inclusions in the top 10 cm bgs of shovel tests with this profile.

The second profile, recorded east of SR 63, consisted of a dark grayish-brown (10YR 4/2) silty clay loam (Zone I) with common gray (10YR 5/1) mottles from the ground surface to 26 cm bgs. This was underlain by a gray (10YR 5/1) silty clay loam (Zone II) to 36 cm bgs. A bucket auger (BA 1) was excavated in this shovel test, since this portion of the survey area was mapped within the Genesee series and could contain deeply buried cultural deposits. This bucket auger revealed that the aforementioned Zone II, consisting of a gray (10YR 5/1) silty clay loam from 26 to 36 cm bgs, was underlain by a yellowish-brown (10YR 5/4) silty clay loam to 66 cm bgs. At this depth, small and common natural gravels became apparent and continued to increase in density until the auger reached an impenetrable layer of gravel at 94 cm bgs.

The third soil profile, recorded near the southeast corner of the survey area, consisted of a brown (10YR 5/3) silt loam from the ground surface to 19 cm bgs. This was underlain by a dark yellowish-brown (10YR 4/6) silt loam to 40 cm bgs and a yellowish-brown (10YR 5/4) silt loam to 56 cm bgs. The base of this shovel test was further excavated with a bucket auger (BA 2). The second auger (BA 2) revealed that the yellowish-brown (10YR 5/4) silt loam continued to 88 cm bgs. At this depth, the texture of the soils became more representative of a silty clay loam, and common small natural gravels were observed. This continued to a depth of 115 cm bgs, when a gravel impasse was encountered.

Neither bucket auger and none of the shovel tests located an archaeological site or indicated a potential for unlocated deeply buried archaeological deposits.

RECOMMENDATIONS	
Records check (Check all that apply)	
<input type="checkbox"/>	No archaeological investigation is recommended before the project is allowed to proceed because the records check has determined that the project area does not have the potential to contain archaeological resources.
<input type="checkbox"/>	A Phase 1a archaeological reconnaissance is recommended.
<input checked="" type="checkbox"/>	Based upon the records check results, a Phase 1a archaeological reconnaissance was recommended and has been conducted.
<input type="checkbox"/>	A cemetery development plan may be required under Indiana Code 14-21-1-26.5 because project ground disturbance will be within 100 feet of a cemetery.

Phase 1a archaeological reconnaissance (Check all that apply)

It is recommended that the project be allowed to proceed as planned because the Phase 1a archaeological reconnaissance has located no archaeological sites within the project area and/or previously recorded sites that were investigated warrant no additional investigation.

It is recommended that Phase 1c archaeological subsurface reconnaissance be conducted before the project is allowed to proceed. The Phase 1a archaeological reconnaissance has determined that the project area includes landforms which have the potential to contain buried archaeological deposits.

Other recommendations / commitments

None

Pursuant to IC-14-21-1, if any archaeological artifacts or human remains are uncovered during construction, demolition, or earthmoving activities, state law (Indiana Code 14-21-1-27 and 29) requires that the discovery must be reported to the Department of Natural Resources within two (2) business days. In that event, please call (317) 232-1646.

REQUIRED ATTACHMENTS

Figure showing project location within Indiana

USGS topographic map showing the project area (1:24,000 scale)

Aerial photograph showing the project area, land use and survey methods

Photographs of the project area, including, if applicable, photographs documenting disturbances

Project plans (*if available*)

Other attachments

Figures 1–8; Tables 1 and 2; References Cited

References cited (See short report instructions for required references to be consulted)

See attachments

Comments

N/A

CURATION

Location of project documentation

Survey notes and photographs will be retained at the office of CRA in Evansville, Indiana.

Appendix E:

Red Flag and Hazardous Materials



INDIANA DEPARTMENT OF TRANSPORTATION

100 North Senate Avenue
Room N758-ES
Indianapolis, Indiana 46204

PHONE: (855) 463-6848
(855) INDOT4U

Eric Holcomb, Governor
Michael Smith, Commissioner

Date: December 7, 2023

To: Site Assessment & Management (SAM)
Environmental Policy Office - Environmental Services Division (ESD)
Indiana Department of Transportation (INDOT)
100 N Senate Avenue, Room N758-ES
Indianapolis, IN 46204

From: Raquel Walker
Beam, Longest and Neff (BLN)
8320 Craig Street
Indianapolis, Indiana
rwalker@b-l-n.com

Re: RED FLAG INVESTIGATION
DES 2100968 & 2100969, State Project
Bridge Removal Project
State Road (SR) 63 over Abandoned Railroad, 0.24 Mile South of SR 234
Vermillion County, Indiana

PROJECT DESCRIPTION

Brief Description of Project: This project is located on SR 63 over an abandoned railroad (Bridge No. 063-83-02002 ANBL/ASBL (NBI: 022727/ 022729), approximately 0.24 mile south of SR 234 in Cayuga, Vermillion County, Indiana. The existing structures consist of twin three-span continuous steel beam bridges, 175.5 feet long, originally built in 1976. The bridges had latex-modified concrete (LCM) overlays installed in 1987, the wearing surface was epoxy injected in 2019, and substantial beam and bearing repairs were completed in 2020.

Currently, two alternatives are being proposed. The preferred is to remove the existing structures and install a 14-foot span, 13.5-foot rise, three-sided small structure. The proposed structure would be approximately 232 feet long and would have 2-foot, 6-inch minimum headwalls with precast concrete wingwalls, or mechanically stabilized earth (MSE) retaining walls could be used as headwalls and wing walls to shorten the length of the proposed structure. SR 63 would be reconstructed on the same horizontal and vertical alignments and would provide two 12 foot travel lanes bordered by a 4 foot median and 10-foot outside paved shoulders in each direction. Project limits would extend approximately 250 feet long by 125 feet out each way from the centerline of the existing bridges. Pavement within the project limits would be replaced, including the underdrains. Incidental construction would extend approximately 300 feet beyond the project limits for each approach and would include 50 feet of pavement milling and resurfacing, guardrail removal, and linear grading.

The second alternative would be to remove the existing bridges and backfill the existing crossing with roadway embankment material. SR 63 would be reconstructed on the same horizontal and vertical alignment and would provide two 12-foot travel lanes bordered by a 4-foot median and 10-foot outside paved shoulders. The existing median and side

slopes meet current standards and would be maintained. Project limits would be approximately 250 feet long, measured 125 feet each way from the centerline of the existing bridge. Pavement within the project limits would be replaced, including the underdrains. Incidental construction limits would extend approximately 300 feet beyond the project limits for each approach. Work in the incidental construction limits would primarily include 50 feet of milling and resurfacing, guardrail removal, and linear grading. Median drainage structures would be reconstructed as required to maintain the existing drainage pattern. This alternative meets the purpose and need of the project; however, it would limit the use of the abandoned railroad corridor in the future.

Bridge Work Included in Project: Yes No Structure #(s): 063-83-02002 ANBL (NBI: 022727) and ASBL (NBI: 022729)

If this is a bridge project, is the bridge Historical? Yes No , Select Non-Select

(Note: If the project involves a historical bridge, please include the bridge information in the Recommendations Section of the report).

Culvert Work Included in Project: Yes No Structure #(s) _____

Proposed right of way: Temporary # Acres _____ Permanent # Acres 1.3 _____, Not Applicable

Type and proposed depth of excavation: Excavation will be necessary to remove the existing bridges and construct the new 14-foot span, 13-foot rise, three-sided small structure, and wingwalls or MSE walls. The depth of excavation will not exceed 8 feet.

Maintenance of traffic (MOT): MOT is anticipated to utilize phased construction using a temporary runaround in the median. The full length of the 3-sided structure would be constructed beneath the existing bridges while they remain in place. Temporary pavement will be constructed in the median and across the three-sided structure that will accommodate two 11-foot travel lanes with two 2-foot paved shoulders. Upon completion of this initial phase, one bridge will be closed, traffic from that structure will be diverted onto the runaround, and then that bridge will be removed, and the new roadway will be constructed. This pattern will be reversed for the bridge in the opposite direction.

Work in waterway: Yes No Below ordinary high water mark: Yes No

State Project: LPA:

Any other factors influencing recommendations: N/A

INFRASTRUCTURE TABLE AND SUMMARY

Infrastructure			
Indicate the number of items of concern found within the 0.5 mile search radius. If there are no items, please indicate N/A:			
Religious Facilities	2*	Recreational Facilities	2
Airports ¹	N/A	Pipelines	N/A
Cemeteries	N/A	Railroads	1
Hospitals	N/A	Trails	1
Schools	N/A	Managed Lands	N/A

¹In order to complete the required airport review, a review of public-use airports within 3.8 miles (20,000 feet) is required.

Explanation:

Religious Facilities*: Although not mapped on the GIS layer, two (2) religious facilities were identified within the 0.5 mile search radius. The nearest facility, Cayuga Christian Church Youth Center, is located approximately 0.37 mile southwest of the project area. No impact is expected.

Recreational Facilities: Two (2) recreational facilities are located within the 0.5 mile search radius. The nearest facility, Vermillion County Fairgrounds, is located approximately 0.23 mile southwest of the project area. No impact is expected.

Railroads: One (1) railroad segment is located within the 0.5 mile search radius. This railroad segment, an abandoned railroad, crosses the project area. However, no information could be found about the property owner. Standard coordination will occur with INDOT Utilities and Railroads by the Project Management Team or their consultant no later than the Ready for Contracts (RFC) date.

Trails: One (1) trail segment is located within the 0.5 mile search radius. This trail segment, Cayuga Park Pathway, is located approximately 0.23 mile southwest of the project area. No impact is expected.

WATER RESOURCES TABLE AND SUMMARY

Water Resources			
Indicate the number of items of concern found within the 0.5 mile search radius. If there are no items, please indicate N/A:			
NWI - Points	N/A	Canal Routes - Historic	N/A
Karst Springs	N/A	NWI - Wetlands	11
Canal Structures – Historic	N/A	Lakes	4
NPS NRI Listed	N/A	Floodplain - DFIRM	3
NWI-Lines	1	Cave Entrance Density	N/A
IDE� 303d Listed Streams and Lakes (Impaired)	1	Sinkhole Areas	N/A
Rivers and Streams	2	Sinking-Stream Basins	N/A

If unmapped water features are identified that might impact the project area, direct coordination with INDOT ESD Ecology and Waterway Permitting will occur.

Explanation:

NWI-Lines: One (1) NWI-line segment is located within the 0.5 mile search radius. This segment is located approximately 0.41 mile south of the project area. No impact is expected.

IDE� 303d Listed Streams and Lakes*: One (1) unmapped 303d Listed Stream is located within the 0.5 mile search radius. Vermillion River is located approximately 0.49 mile northwest of the project area. No impact is expected.

Rivers and Streams: Two (2) stream segments are located within the 0.5 mile search radius. The nearest stream segment, Dry Branch, is located approximately 0.21 mile east of the project area. No impact is expected.

NWI-Wetlands: Eleven (11) wetland polygons are located within the 0.5 mile search radius. One (1) wetland polygon is located within the project area. A Waters of the U.S. Report is recommended based on mapped features, and coordination with INDOT ESD Ecology and Waterway Permitting will occur.

Lakes: Four (4) lake polygons are located within the 0.5 mile search radius. The nearest lake polygon is located approximately 0.04 mile east of the project area. No impact is expected.

Floodplains: Three (3) floodplain polygons are located within the 0.5 mile search radius. The project area is located within one (1) of the floodplain polygons. Coordination with INDOT ESD Ecology and Waterway Permitting will occur.

MINING AND MINERAL EXPLORATION TABLE AND SUMMARY

Mining/Mineral Exploration			
Indicate the number of items of concern found within the 0.5 mile search radius. If there are no items, please indicate N/A:			
Petroleum Wells	N/A	Mineral Resources	N/A
Mines – Surface	N/A	Mines – Underground	N/A

Explanation: No mining and mineral resources were identified within the 0.5 mile search radius.

HAZARDOUS MATERIAL CONCERNS TABLE AND SUMMARY

Hazardous Material Concerns			
Indicate the number of items of concern found within the 0.5 mile search radius. If there are no items, please indicate N/A:			
Superfund	N/A	Manufactured Gas Plant Sites	N/A
RCRA Generator/ TSD	1	Open Dump Waste Sites	N/A
RCRA Corrective Action Sites	N/A	Restricted Waste Sites	N/A
State Cleanup Sites	N/A	Waste Transfer Stations	N/A
Septage Waste Sites	N/A	Tire Waste Sites	N/A
Underground Storage Tank (UST) Sites	2	Confined Feeding Operations (CFO)	N/A
Voluntary Remediation Program	N/A	Brownfields	3
Construction Demolition Waste	N/A	Institutional Controls	N/A
Solid Waste Landfill	N/A	NPDES Facilities	N/A
Infectious/Medical Waste Sites	N/A	NPDES Pipe Locations	N/A
Leaking Underground Storage (LUST) Sites	4	Notice of Contamination Sites	N/A

Unless otherwise noted, site-specific details presented in this section were obtained from documents reviewed on the Indiana Department of Environmental Management (IDEM) Virtual File Cabinet (VFC).

Explanation:

Underground Storage Tank (UST) Sites: Two (2) UST sites are located within the 0.5 mile search radius. The nearest UST site, Albert Clark, 100 N Division Street, Agency Interest Identification (AI ID) # 56087, is located approximately 0.5 mile west of the project area. According to the UST Notification Form dated August 18, 1987, the site formerly housed one (1) 500-gallon and two (2) 3,000-gallon USTs that were empty and last used in 1980. The notification form indicates that the USTs were filled with an inert material. No impact is expected.

RCRA Generator/TSD Site: One (1) RCRA Generator/TSD site is located within the 0.5 mile search radius. Clark Chevrolet, 105 North Division Street, AI ID# 58464, is located approximately 0.5 mile west of the project area. According to the IDEM letter dated June 6, 2002, this site is no longer in need of a US EPA Identification number. No impact is expected.

Leaking Underground Storage (LUST) Sites: Four (4) LUST sites are located within the 0.5 mile search radius. The nearest site, Stans Highway Shell (also known as McClure Store 73), 999 E State Road 234, AI ID# 55650, is located approximately 0.11 mile northwest of the project area. The site is an operating service station. According to the No Further Action (NFA) Determination Pursuant to Remediation Closure Guide (RCG) issued by IDEM on September 13, 2022, all chemicals of concern (COCs) in the soil and groundwater were below the Remediation Closure Guide (RCG) residential screening levels. The closure was unconditional for soil, groundwater, and vapor intrusion exposures. No impact is expected.

Brownfields: Three (3) brownfield sites are located within the 0.5 mile search radius. The nearest site, Hubbard Property 4211009, 106 South Division Street, AI ID# 129436, is located approximately 0.46 mile west of the project area. The site is a commercial building. A 2021 Phase I ESA identified three (3) adjacent recognized environmental conditions; however, none of them impact the project area. No impact is expected.

ECOLOGICAL INFORMATION SUMMARY

The Vermillion County listing of the Indiana Natural Heritage Data Center information on endangered, threatened, or rare (ETR) species and high-quality natural communities is provided at https://www.in.gov/dnr/nature-preserves/files/np_vermillion.pdf. A preliminary review of the Indiana Natural Heritage Database by INDOT ESD did not indicate the presence of ETR species within the 0.5 mile search radius. Coordination with USFWS and IDNR will occur.

A review of the USFWS database did not indicate the presence of endangered bat species in or within 0.5 mile of the project area. The project area is located in a rural area surrounded by farm fields. The October 20, 2021 inspection report for Bridge 063-83-02002 ASBL, and the August 8, 2022 inspection report for Bridge 063-83-02002 ANBL state that no evidence of bats was seen or heard under the bridges. The range-wide programmatic consultation for the Indiana Bat and Northern Long-eared Bat will be completed according to the most recent “Using the USFWS’s IPaC System for Listed Bat Consultation for INDOT Projects”.

RECOMMENDATIONS SECTION

Include recommendations from each section. If there are no recommendations, please indicate N/A:

INFRASTRUCTURE: One (1) railroad segment is located within the project area. Standard coordination will occur with INDOT Utilities and Railroads by the Project Management Team or their consultant no later than the Ready for Contracts (RFC) date.

WATER RESOURCES:

NWI-Wetlands: One (1) wetland is located within the project area. A Waters of the U.S. Report is recommended based on mapped features, and coordination with INDOT ESD Ecology and Waterway Permitting will occur.

Floodplains: The project area is located within one (1) of the floodplain polygons. Coordination with INDOT ESD Ecology and Waterway Permitting will occur.

MINING/MINERAL EXPLORATION: N/A

HAZARDOUS MATERIAL CONCERNS: N/A

ECOLOGICAL INFORMATION: Coordination with USFWS and IDNR will occur. The range-wide programmatic consultation for the Indiana Bat and Northern Long-eared Bat will be completed according to the most recent “Using the USFWS’s IPaC System for Listed Bat Consultation for INDOT Projects”.

Peter
Washburn
INDOT ESD concurrence: _____

Digitally signed by Peter
Washburn
Date: 2023.12.13 12:18:37
-05'00'
(Signature)

Prepared by:
Raquel Walker
Senior Environmental Analyst
Beam, Longest and Neff, LLC.

Graphics:

A map for each report section with a 0.5 mile search radius buffer around all project area(s) showing all items identified as possible items of concern is attached. If there is not a section map included, please change the YES to N/A:

SITE LOCATION: YES

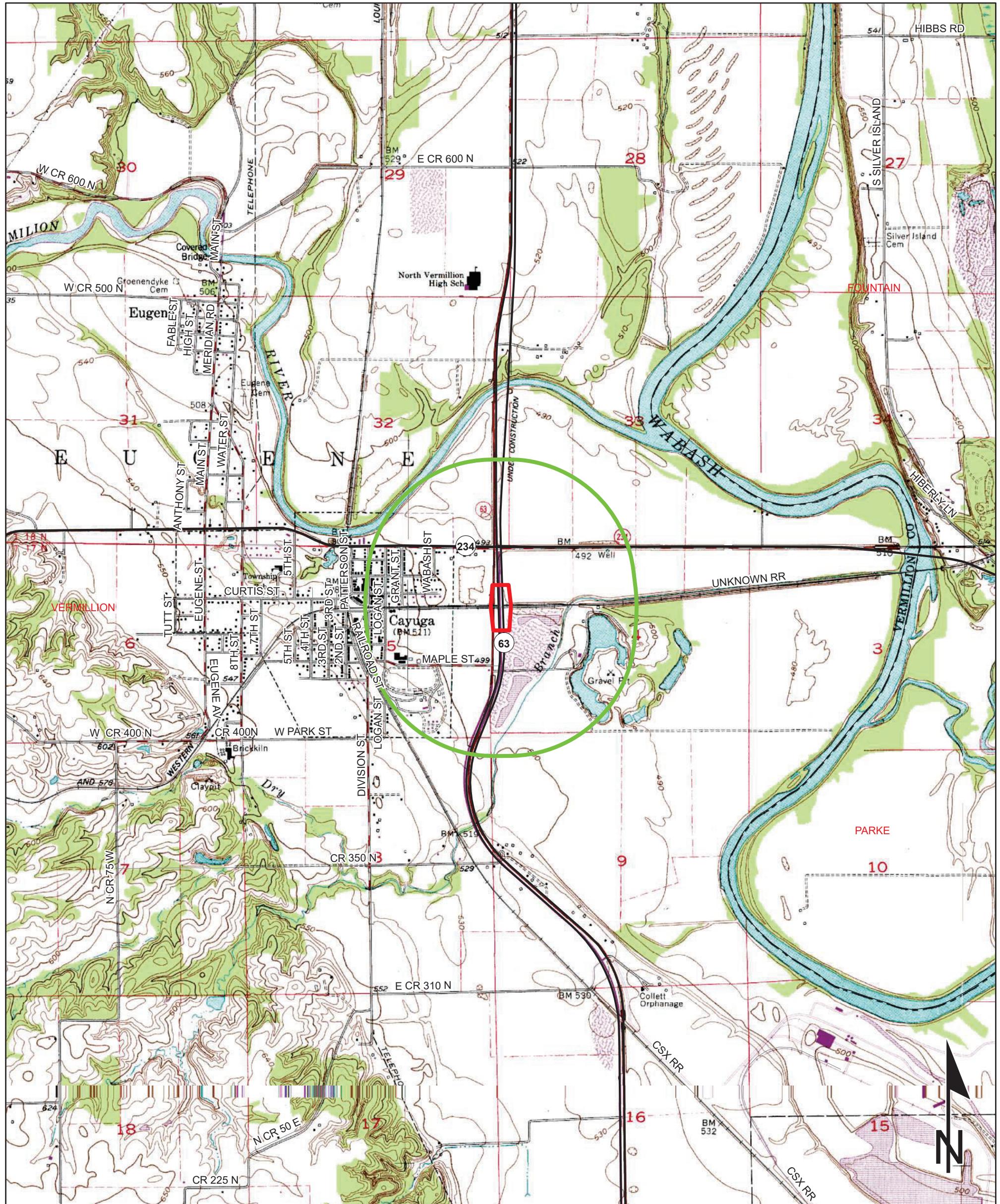
INFRASTRUCTURE: YES

WATER RESOURCES: YES

MINING/MINERAL EXPLORATION: N/A

HAZARDOUS MATERIAL CONCERNS: YES

Red Flag Investigation - Site Location
SR 63 Over Abandoned Railroad, 0.24 Mile South of SR 234
Des. No. 2100968/2100969, Bridge Removal Project
Vermillion County, Indiana



Sources: 0.5 0.25 0 0.5 Miles
Non Orthophotography

Data - Obtained from the State of Indiana Geographical Information Office Library

Orthophotography - Obtained from Indiana Map Framework Data (www.indianamap.org)

Map Projection: UTM Zone 16 N Map Datum: NAD83

This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.

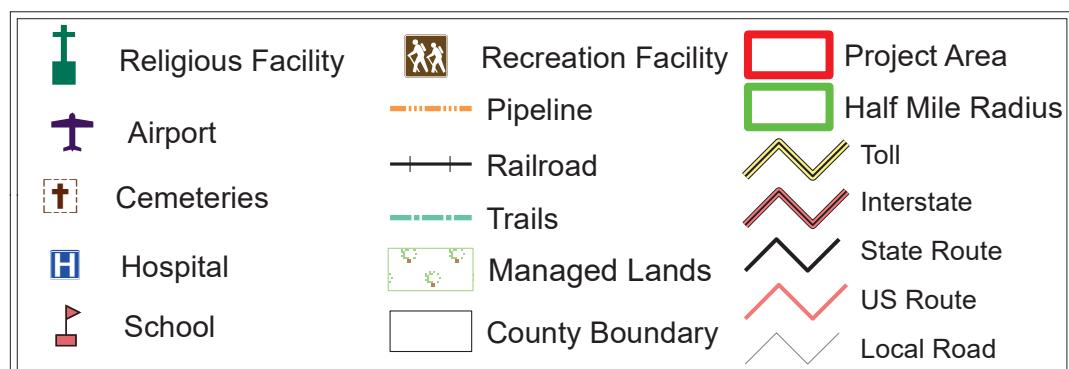
NEWPORT QUADRANGLE
INDIANA
7.5 MINUTE SERIES
(TOPOGRAPHIC)

Red Flag Investigation - Infrastructure
SR 63 Over Abandoned Railroad, 0.24 Mile South of SR 234
Des. No. 2100968/2100969, Bridge Removal Project
Vermillion County, Indiana

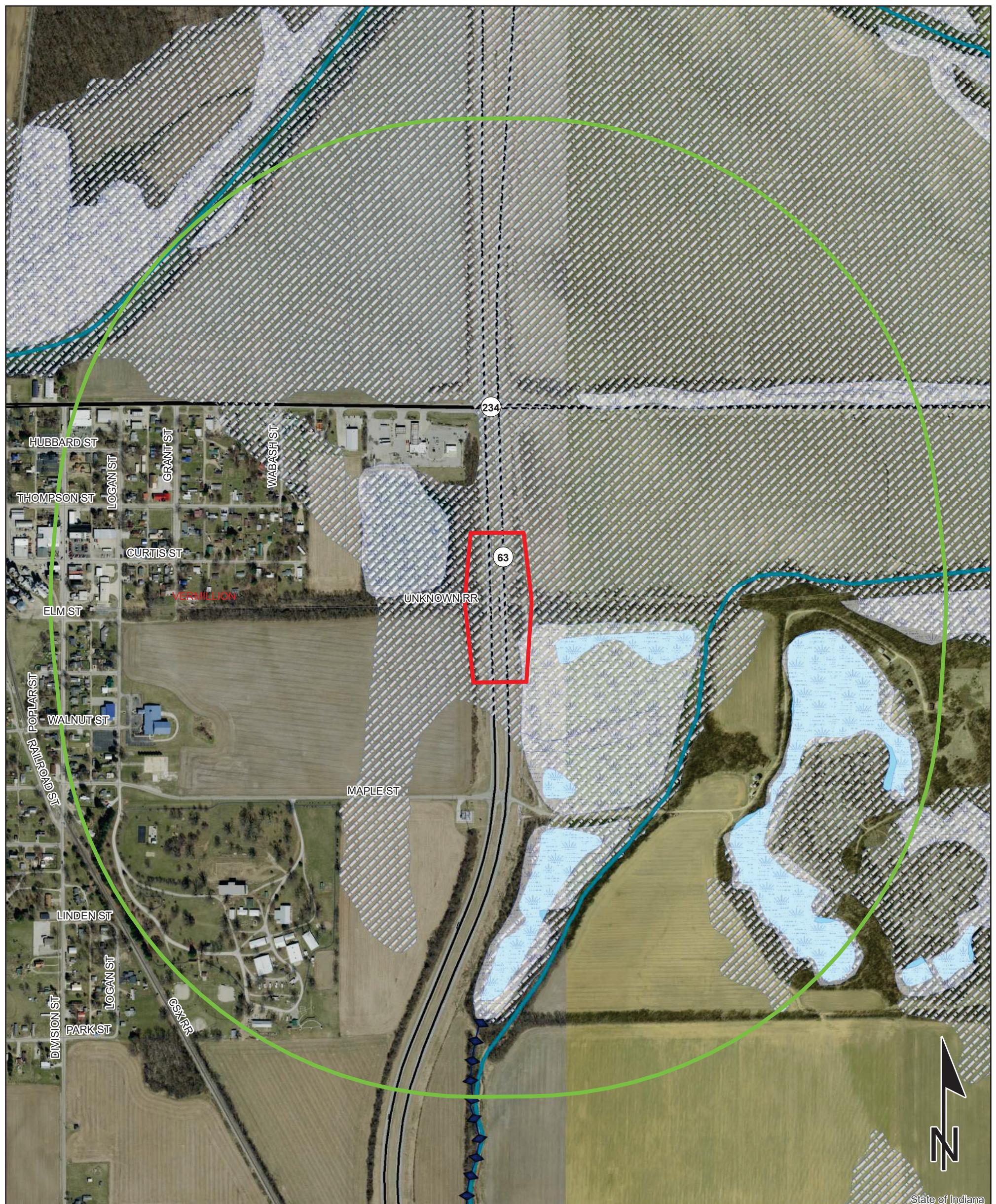


Sources:
Non Orthophotography 
Data - Obtained from the State of Indiana Geographical Information Office Library
Orthophotography - Obtained from Indiana Map Framework Data (www.indianamap.org)
Map Projection: UTM Zone 16 N **Map Datum:** NAD83

This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.



Red Flag Investigation - Water Resources
SR 63 Over Abandoned Railroad, 0.24 Mile South of SR 234
Des. No. 2100968/2100969, Bridge Removal Project
Vermillion County, Indiana



Sources:
Non Orthophotography

Data - Obtained from the State of Indiana Geographical Information Office Library

Orthophotography - Obtained from Indiana Map Framework Data (www.indianamap.org)

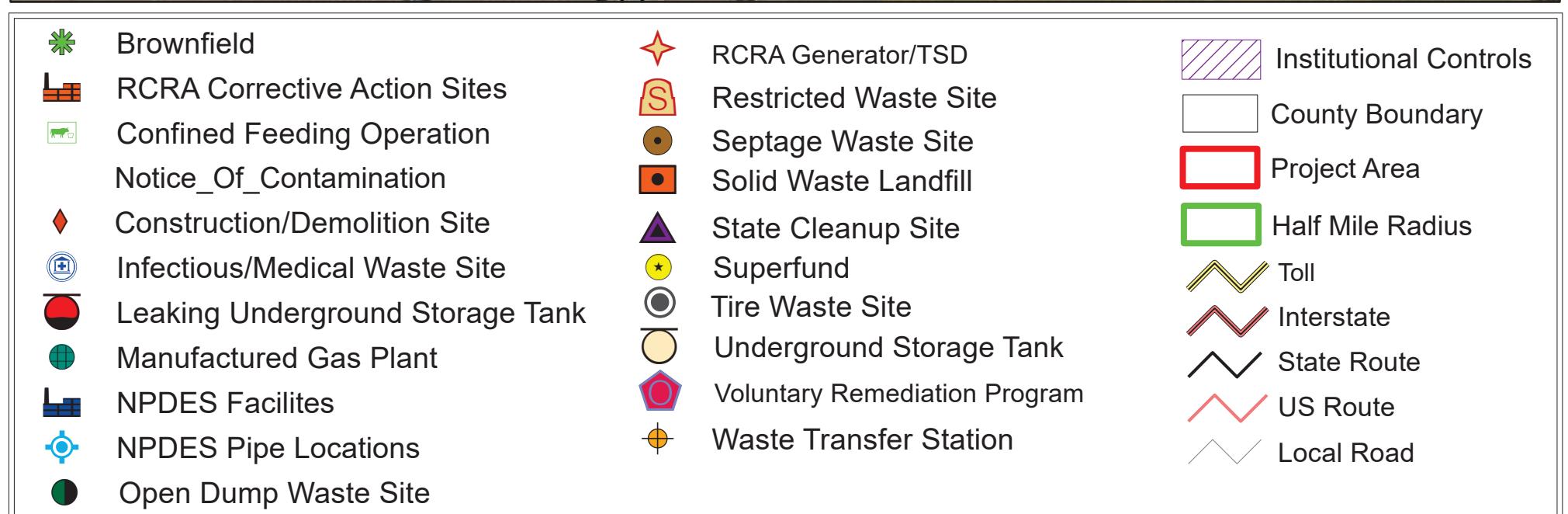
Map Projection: UTM Zone 16 N **Map Datum:** NAD83

This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.

0.15 0.075 0 0.15 Miles

NWI - Point	Wetlands	Project Area
Karst Spring	Lake	Half Mile Radius
NWI- Line	Floodplain - DFIRM	Toll
Impaired_Stream_Lake	Cave Entrance Density	Interstate
NPS NRI listed	Sinkhole Area	State Route
River	Sinking-Stream Basin	US Route
Canal Structure - Historic	County Boundary	Local Road
Canal Route - Historic		

Red Flag Investigation - Hazardous Material Concerns
SR 63 Over Abandoned Railroad, 0.24 Mile South of SR 234
Des. No. 2100968/2100969, Bridge Removal Project
Vermillion County, Indiana



0.15 0.075 0 0.15
Miles

This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.

E-10

Sources:

Non Orthophotography

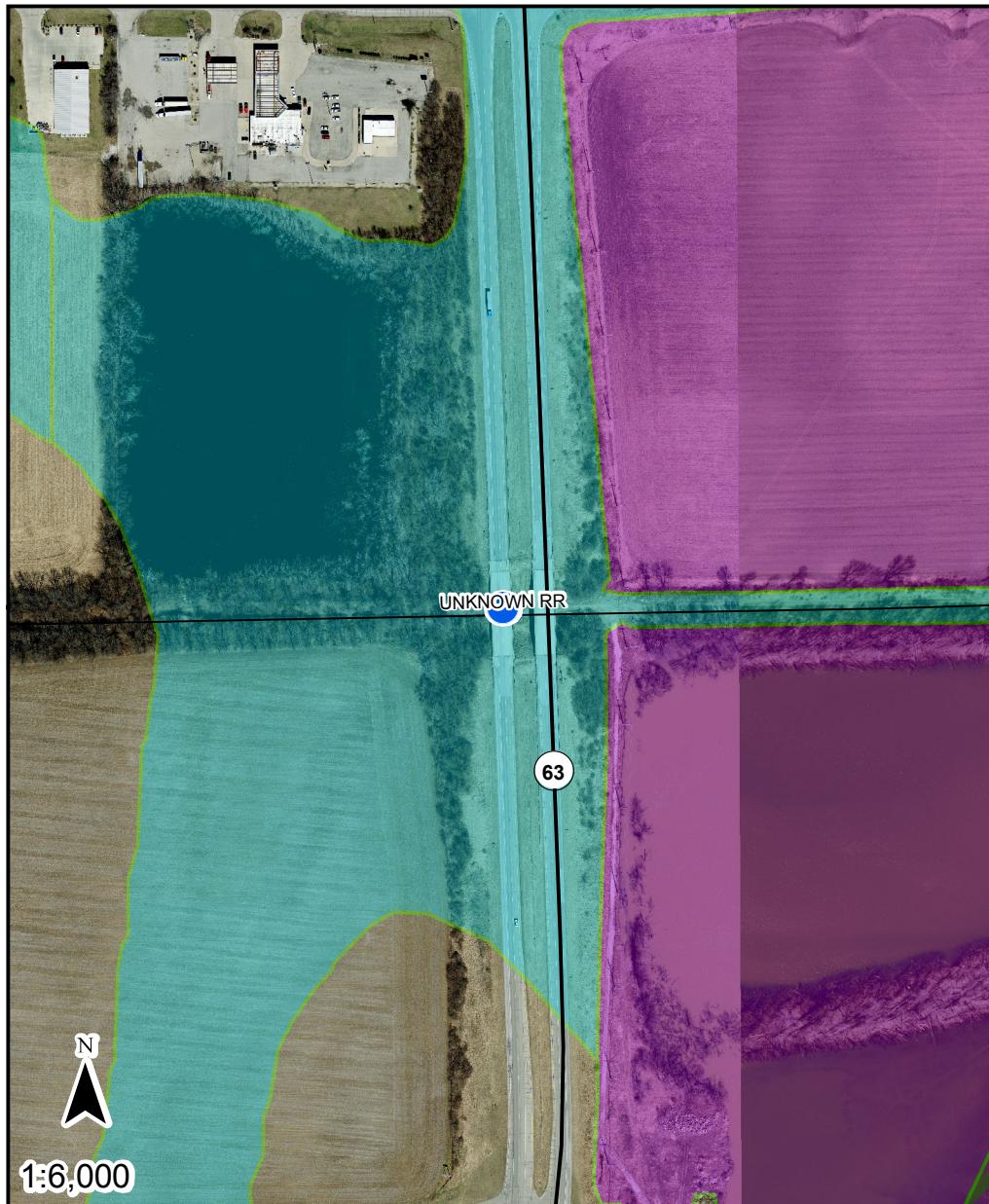
Data - Obtained from the State of Indiana Geographical Information Office Library

Orthophotography - Obtained from Indiana Map Framework Data (www.indianamap.org)

Map Projection: UTM Zone 16 N Map Datum: NAD83

Appendix F:

Water Resources



The information provided below is based on the point of interest shown in the map above.

County: **Vermillion**

Approximate Ground Elevation: **497.8 feet (NAVD88)**

Stream Name:

Dry Branch

Base Flood Elevation: **505.8 feet (NAVD88)**

Drainage Area: **Not available**

Best Available Flood Hazard Zone: **FEMA Zone A**

National Flood Hazard Zone: **FEMA Zone A**

Is a Flood Control Act permit from the DNR needed for this location? **See following pages**

Is a local floodplain permit needed for this location? **yes-**

Floodplain Administrator: **Penney Carpenter**

Community Jurisdiction: **Vermillion County, County proper**

Phone: **(765) 492-5343**

Email: **penney.carpenter@vermillioncounty.in.gov**

Katie Finney

From: Mcgill, Justus <JMcgill@indot.IN.gov>
Sent: Tuesday, October 31, 2023 2:00 PM
To: Raquel Walker; Rehder, Crystal
Cc: Katie Finney; Brian Shaw; Miller, Jessica
Subject: RE: Des 2100968 & 2100969 Waters Report - Vermillion County

This Message Is From an External Sender

This message came from outside your organization.

[Report Suspicious](#)

Hello Raquel,

Thank you for the information. I have completed a desktop review and I would agree that there is likely not any jurisdictional features within the project limits. Since no impacts below the OHWM, Q100, or wetland are anticipated, no waters report is required for **Des 2100968 & 2100969 SR 63 Bridge Removal**.

It is still recommend that the designer provide EWPO the permit determination checklist information to verify that no additional environmental permits are needed for the project. Contact me with any questions.

Thanks,

Justus McGill, WPIT

Ecology and Waterway Permitting Office (Crawfordsville District)
100 N Senate Ave. Indianapolis Rm N758-ES, IN 46204

Office: (317)-509-7296

Email: jmcgill@indot.in.gov



From: Raquel Walker <rwalker@b-l-n.com>
Sent: Monday, October 9, 2023 9:24 AM
To: Mcgill, Justus <JMcgill@indot.IN.gov>; Rehder, Crystal <CRehder@indot.IN.gov>
Cc: Katie Finney <kfinney@b-l-n.com>; Brian Shaw <bshaw@b-l-n.com>
Subject: Des 2100968 & 2100969 Waters Report - Vermillion County

**** This is an EXTERNAL email. Exercise caution. DO NOT open attachments or click links from unknown senders or unexpected email. ****

Justus & Crystal,

We are working on a project on SR 63 over an abandoned railroad in Vermillion County. During the site visit no water resources were identified; however, there is one pond that is mapped on the NWI layer adjacent to the project area. I've attached the early coordination letter with maps and pictures for reference. I wanted to check and see if a waters report would need to be prepared. A proof of absence sample point was not taken as the portion that encroaches in the project area consists of a gravel road and fence row.

Appendix G:

Public Involvement

NOTICE OF SURVEY

January 30, 2023

Sample Survey Letter

Re: SR 63 Bridge Deck Replacement
Vermillion County, Indiana
Des. No. 2100968 & 2100969

Dear Property Owner:

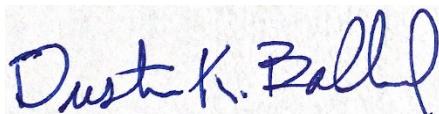
Our information indicates that you own or occupy property near this proposed bridge deck replacement project. Our employees will be doing a survey of the project area in the near future. It may be necessary for them to come onto your property to complete this work. This is permitted by law per Indiana Code IC 8-23-7-26. They will show you their identification, if you are available, before coming onto your property. If you have sold this property, or it is occupied by someone else, please let us know the name and address of the new owner or current occupant so we can contact them about the survey.

The survey work will include mapping the location of features such as the existing bridge structure, roadway, utilities, buildings, fences, drives, creeks, ditches and property corners. This work is needed for the proper planning and design of this bridge deck replacement project.

Please be assured of our sincere desire to cause you as little inconvenience as possible during this survey. If any problems do occur, please speak to our field crew or contact me at the telephone number or address shown above.

Sincerely,

BEAM, LONGEST AND NEFF, L.L.C.



Dustin Ballard, PS
Director of Survey Services
xc: 220083

Appendix H:

Air Quality

Indiana Department of Transportation (INDOT)
State Preservation and Local Initiated Projects FY 2024 - 2028

SPONSOR	CONTR ACT # / LEAD DES	STIP NAME	ROUTE	WORK TYPE	DISTRICT	MILES	FEDERAL CATEGORY	Total Cost of Project*	PROGRAM	PHASE	FEDERAL	MATCH	2024	2025	2026	2027	2028
Indiana Department of Transportation	43686 / 2100188	Init.	SR 63	HMA Overlay Minor Structural	Crawfordsville	8.264	NHPP	\$40,447,677.50	Road Construction	CN	\$23,465,600.00	\$5,866,400.00	\$40,000.00	\$40,000.00	\$29,252,000.00		
									Road Consulting	PE	\$1,600,000.00	\$400,000.00	\$2,000,000.00				
									Road ROW	RW	\$180,000.00	\$45,000.00	\$150,000.00	\$75,000.00			
									Bridge Construction	CN	\$5,632,000.00	\$1,408,000.00			\$7,040,000.00		
Performance Measure Impacted: Pavement Condition																	
Location: SR-63, From 0.1 mi S of US 36 to 0.07 mi S of SR 71 and from 0.07 mi S of SR 71 to 0.08 mi S of SR 234, bridges NB and SB over Little Vermillion River, 1.10 mi S of SR 71 and NB and SB over abandoned RR, 0.24 mi S of SR 234																	
Comments:Include DES 2100188, 2100190, 2100966, 2100967, 2100968, 2100969																	
Indiana Department of Transportation	43686 / 2100188	M 45	SR 63	HMA Overlay Minor Structural	Crawfordsville	8.264	NHPP	\$40,447,678.00	Road ROW	RW	\$0.00	\$0.00	(\$150,000.00)	\$150,000.00			
Performance Measure Impacted: Pavement Condition																	
Location: SR-63, From 0.1 mi S of US 36 to 0.07 mi S of SR 71 = (1) HMA Overlay, Minor Structural & (1) HMA Overlay, Preventive Maintenance, (2) Bridge Deck Replacements & (2) Bridge Removals																	
Comments:Move RW from FY 25 to FY 26																	
Indiana Department of Transportation	43686 / 2100188	M 53	SR 63	HMA Overlay Minor Structural	Crawfordsville	8.264	NHPP	\$47,242,221.00	Road Construction	CN	\$0.00	\$0.00	(\$4,074,000.00)	\$4,074,000.00			
									Bridge Construction	CN	\$0.00	\$0.00	(\$1,933,000.00)	\$1,933,000.00			
Performance Measure Impacted: Pavement Condition																	
Location: SR-63, From 0.1 mi S of US 36 to 0.07 mi S of SR 71 = (1) HMA Overlay, Minor Structural & (1) HMA Overlay, Preventive Maintenance, (2) Bridge Deck Replacements & (2) Bridge Removals																	
Comments:move FY25 CN \$6,007,000 to FY26, include DES 2100188, 2001776, 2100966, 2100967, 2100968, 2100969																	
Vermillion County	44257 / 2101716	Init.	IR 1896	Bridge Replacement	Crawfordsville	.132	STBG	\$2,164,000.00	Local Bridge Program	CN	\$1,454,000.00	\$0.00				\$1,454,000.00	
									Local Funds	RW	\$0.00	\$10,000.00		\$10,000.00			
									Local Bridge Program	RW	\$38,000.00	\$0.00		\$38,000.00			
									Local Funds	CN	\$0.00	\$363,000.00				\$363,000.00	
Performance Measure Impacted: Bridge Condition																	
Location: Bridge 37 Vermillion County , CR 200E over Norton Creek																	
Comments:Include DES 2101716																	
Indiana Department of Transportation	44377 / 2200675	Init.	I 74	Small Structure Pipe Lining	Crawfordsville	.1	NHPP	\$2,993,130.64	Bridge Consulting	PE	\$504,000.00	\$56,000.00	\$560,000.00				
									Bridge Construction	CN	\$2,189,700.00	\$243,300.00			\$100,000.00	\$2,333,000.00	

Appendix I:

Additional Studies

Land and Water Conservation Fund (LWCF) County Property List for Indiana (Last Updated March 2022)

ProjectNumber	SubProjectCode	County	Property
1800103	1800103	Vermillion	Blanford Community Park
1800144	1800144	Vermillion	Fairview Park Ballfield
1800208	1800208	Vermillion	Millers Park, Miller Community Park
1800286	1800286	Vermillion	Perrysville Park

*Park names may have changed. If acquisition of publically owned land or impacts to publically owned land is anticipated, coordination with IDNR, Division of Outdoor Recreation, should occur.

Land and Water Conservation Fund Summary Report

Indiana; Vermillion County

(Unofficial report; contact us to learn where to find official information: <https://lwcf.tplgis.org/contact>)

January 10, 2024

Number of projects funded:

4

Year range of funding:

1972 - 1977

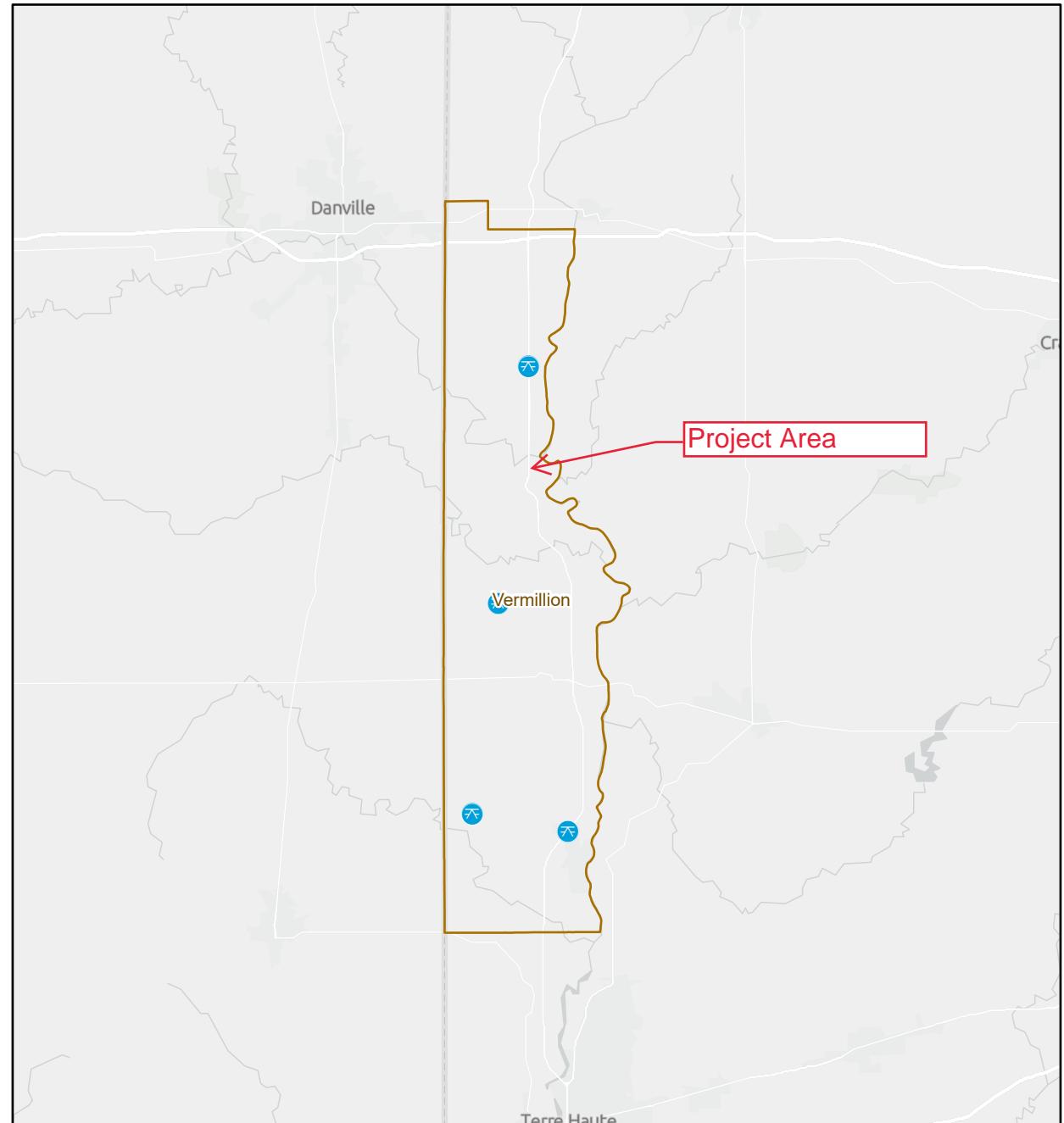
Total funding received (estimate):

\$41,000

Legend

Project funded by LWCF

State and Local Assistance Program (4)



This report was created on January 10, 2024 using the Land and Water Conservation Fund interactive mapping site. It is for informational purposes only. The providers of this report disclaim any and all warranties, express or implied, including fitness for a particular purpose or merchantability, and make no representation that the report is complete, accurate, or error free. Use and reliance on this report is at the sole risk of the party using same.

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Routine Bridge Inspection Report



Structure Information

Structure:	063-83-02002 ANBL	Facility Carried:	SR 63 NB
NBI Number:	022727	Features Intersected:	ABANDONED RR

Inspection Information

Inspection Date:	08/07/2024	Lead Inspector:	Melvin Hughes
Inspection Type:	Routine	Additional Inspectors:	

Condition Ratings Summary

(58) Deck:	7	(60) Substructure:	7
(58.01) Wearing Surface:	7	(61) Channel / Channel Protection:	N
(58.02) Joints:	4	(62) Culverts:	N
(58.05) Approach Slabs:	5	(71) Waterway Adequacy:	N
(59) Superstructure:	5	(72) Approach Roadway Alignment:	8
(59.01) Paint:	5	(113) Scour Critical Bridge:	N

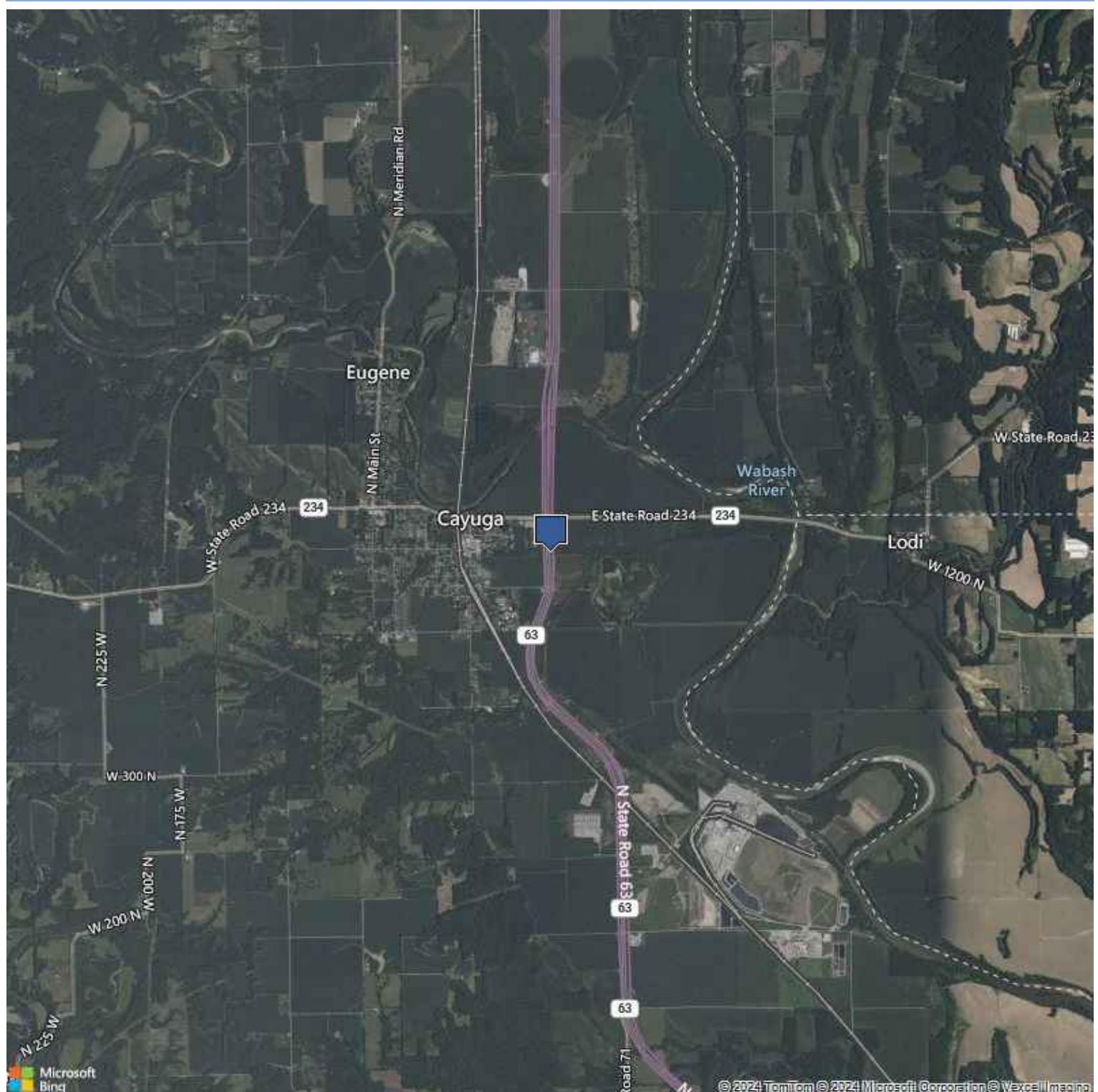


Structure: 063-83-02002
ANBL
NBI Number: 022727

Facility Carried: SR 63 NB
Features Intersected: ABANDONED RR

Inspector: Melvin Hughes
Inspection Date: 08/07/2024

Location Map



Location: 00.24 S SR 234
County: Vermillion

Latitude: 39.9488
Longitude: -87.45124

Structure: 063-83-02002
ANBL
NBI Number: 022727

Facility Carried: SR 63 NB
Features Intersected: ABANDONED RR

Inspector: Melvin Hughes
Inspection Date: 08/07/2024

Routine Inspection Summary

2024 Inspection, The structure is in overall fair condition. Both joints leak, material missing. There are transverse, longitudinal and diagonal cracks in the wearing surface ranging from hairline to wide width, most of these cracks have been sealed. There is a minor spall at the north joint in the driving lane. There are hair line to medium width transverse cracks with white efflorescence in span B and span C. The north and south copings in span B have hair line to medium width cracking with efflorescence. Span C has a moderate spall between beams 6 and 7 near pier 3. Both approach slabs have been paved over. Span A beam 2 bent 1 the web was crushing and out of plane there is now a retro-fit in place. beam 3 at bent 1 bottom flange west side has 1/4" section loss above the bearing. There is scale rust with some section loss at the beam ends at both end bents. There is severe rust with advanced section loss on all of the end diaphragms at both end bents. At bent 1, Bearing 1 fell out reported critical find on 8-7-2024 and was repaired on 8-8-2024. bearing 5 is loose. All of the bearings at bent 1 have some degree of rust, pack rust, and minor section loss. At bent 4, bearings 1, 3 and 5 are loose. All of the bearings at bent 4 have some degree of rust, pack rust, and retro-fits on bearings 3 and 7. At bent 1, beam 1 bearing fell out critical find reported, bearing 5, is loose. All of the bearings at bent 1 have rust, bent 4 1 and 6, rust, and minor section loss. At bent 4, bearings 1, 3, 5, are loose, retro-fits on bearings 3 and 7. There are hairline vertical cracks with white efflorescence in both end bents, the south bent cap has a spall at bearing 1. Pier 2 column 1 has a minor spall on the north side. Pier 3 column 3 has spall on the north side. Has maintenance needs reported for deck spall and back wall spalling at the north joint.

Has work scheduled in SPMS / Des # 2100968 / Contract # R-43686 / Letting date 10/08/2025 / Bridge removal / Program year 2026 / Active.

(1975) New Bridge / Contract# R-10176

Rehab A / 1987 / Bridge deck overlay / Des#8347870 / Contract# B-16772

Rehab P / 2002 / Bridge painting / Contract# B-25662

Programmed / 2020 / Bridge maintenance and repair / DES#1901577 / Contract# B-42091

Programmed / 2020 / Bridge thin deck overlay / Des#1602045 / Contract# Unknown

Programmed / 2021 / Bridge maintenance and repair / Des#2001627 / Contract#B-42091

Structure: 063-83-02002
ANBL
NBI Number: 022727

Facility Carried: SR 63 NB
Features Intersected: ABANDONED RR

Inspector: Melvin Hughes
Inspection Date: 08/07/2024

Identification

(1) State Code:	185 - Indiana	(12) Base Highway Network:	Inventory Route is on the Base Network
(8) Structure:	022727	(13A) Inventory Route:	0000000001
(5) Inv. Route:	1 - 3 - 1 - 00063 - 0	(13B) Subroute Number:	01
(2) Highway Agency District:	1 - Crawfordsville	(16) Latitude:	39.9488
(3) County Code:	083 - Vermillion	(17) Longitude:	-87.45124
(4) Place Code:	00000 - N/A	(98) Border	
(6) Features Intersected:	ABANDONED RR	(A) State Name:	
(7) Facility Carried:	SR 63 NB	(B) Percent:	
(9) Location:	00.24 S SR 234	(99) Border Bridge Struct. No:	
(11) Milepoint:	0025.060		

Age Of Service

(27) Year Built:	1976	(19) Bypass Detour Length:	001
(106) Year Reconstructed:	1987	(29) ADT:	003820
(42) Type Of Service		(30) Year Of ADT:	2006
(A) On Bridge:	1 - Highway	(109) ADTT:	33
(B) Under Bridge:	0 - Other	(114) Future ADT:	005290
(28) Lanes		(115) Year Of Future ADT:	2033
(A) On Bridge:	02		
(B) Under Bridge:	00		

Structure Type And Material

(43) Main Spans:		(45) No. Of Spans In Main Unit:	003
(A) Kind Of Material:	4 - Steel Continuous	(46) No. Of Approach Spans:	0000
(B) Type Of Design:	2 - Stringer/Multibeam or Girder	(107) Deck Structure Type:	1 - Concrete Cast-In-Place
(44) Approach Spans		(108) Wearing Surface	
(A) Kind Of Material:	0 - Other	A) Wearing Surface:	3 - Latex Concrete or similar additive
(B) Type Of Design:	00 - Other	B) Deck Membrane:	0 - None
		C) Deck Protection:	0 - None

Structure: 063-83-02002
ANBL
NBI Number: 022727

Facility Carried: SR 63 NB
Features Intersected: ABANDONED RR

Inspector: Melvin Hughes
Inspection Date: 08/07/2024

Classification

(20) Toll:	3 - On Free Road. The structure is toll-free and carries a toll-free highway.	(21) Maint Responsibility:	01
(22) Owner:	01 - State Highway Agency	(26) Functional Class:	02
(37) Historical Significance:	5 - Not eligible	(100) Strahnet Highway:	0 - The inventory route is not a STRAHNET route.
(101) Parallel Structure:	R - The right structure of parallel bridges carrying the roadway in the direction of the inventory. (For a STRAHNET highway, this is west to east and south to north.)	(102) Direction Of Traffic:	1 - 1-Way Traffic
(103) Temporary Structure:		(104) NHS Inventory:	1 - Inventory Route is on the NHS
(105) Federal Lands Highways:	0 - Not Applicable	(110) DES National Network:	Inventory route on National Truck Network
(112) NBIS Bride Length:	Yes		

Geometric Data

(48) Length Of Max Span:	0070.0	(35) Structure Flared:	0 - No Flare
(49) Structure Length:	00174.0	(10) Inv Rte, Min Vert Clearance:	99.99
(50) Curb/Sidewalk Widths		(47) Tot Horiz Clearance:	039.9
(A) Left:	00.0	(53) Vert Clear Over Br Rdwy:	99.99
(B) Right:	00.0	(54) Min Vertical Underclearance:	
(51) Brdg Rdwy Width Curb- To-Curb:	039.9	A) Reference Feature:	N
(52) Deck Width, Out-To-Out:	043.4	B) Min Vert Underclear:	00.00
(32) Approach Roadway:	038.0	(55) Lateral Underclearance Right:	
(33) Bridge Median:	0 - No Median	A) Reference Feature:	N
(34) Skew:	00	B) Min Lateral Underclear:	000.0
		(56) Min Lateral Underclear On Left:	00.0

Structure: 063-83-02002
ANBL
NBI Number: 022727

Facility Carried: SR 63 NB
Features Intersected: ABANDONED RR

Inspector: Melvin Hughes
Inspection Date: 08/07/2024

Inspections

(90) Inspection Date:	08/08/2022	(91) Designated Inspection Frequency:	24
(92) Critical Feature Inspection		(93) Critical Feature Inspection Date	
A) NSTM Insp Req / Freq:	N	A) NSTM Date:	
B) Underwater Insp Req / Freq:	N	B) Underwater Insp Date:	
C) Special Insp Req / Freq:	N	C) Special Insp Date:	

Proposed Improvements

(75A) Type Of Work:			
(75B) Work Done By:			
(76) Length Of Improvement:	000000	(94) Bridge Improvement Cost:	000000
(97) Year Of Improvement Cost Estimate:		(95) Roadway Improvement Cost:	000000
Comments:		(96) Total Project Cost:	000000

Navigation Data

(38) Navigation Control:	N	(39) Navigation Vertical Clear:	000.0
(111) Pier Or Abutment Protection:		(116) Minimum Navigation Verti.Clearance, Vert. Lift Bridge:	
		(40) Nav Horizontal Clearance:	0000.0

Structure: 063-83-02002
ANBL
NBI Number: 022727

Facility Carried: SR 63 NB
Features Intersected: ABANDONED RR

Inspector: Melvin Hughes
Inspection Date: 08/07/2024

Load Rating & Posting

5.1 – Loads And Load Ratings		Legacy Coding	
B.LR.01 - Design Load	HS20	(65) Inventory Rating Method	8
B.LR.02 - Design Method	LFD	(66) Inventory Rating	1.09
B.LR.03 - Load Rating Date		(63) Operating Rating Method	8
B.LR.04 - Load Rating Method	LRFR	(64) Operating Rating	1.48
B.LR.05 - Inventory Load Rating Factor	1.09	(31) Design Load	5
B.LR.06 - Operating Load Rating Factor	1.48	(70) Bridge Posting	5
B.LR.07 - Controlling Legal Load Rating Factor	1.48	(41) Structure Open/Posted/Closed	A
B.LR.08 - Routine Permit Loads	C	Tons Posted	
		Date Posted/Closed	

5.2 – Load Posting Status		Posting – Emergency Vehicles (Ton)
B.PS.01 - Load Posting Status	PO	Emergency Vehicle Sign
B.PS.02 - Posting Status Change Date		Posted Tonnage (Single Axle) EV Posted Tonnage (Tandem) EV Posted Tonnage (Gross) EV

Posting – Commercial Vehicle (Ton)	Maximum Allowable Tonnages
*Actual posted values may not exceed those as shown below	
Commercial Vehicle Sign	Weight Limit
Posted Tonnage (Single Axle) CV	Tons
Posted Tonnage (Gross) CV	Emergency Vehicle Weight Limit
Posted Tonnage (2-axle) CV	Single Axle T
Posted Tonnage (3-axle) CV	Tandem T
Posted Tonnage (4-axle) CV	Gross T
Posted Tonnage (5-axle) CV	
Posted Tonnage (6-axle) CV	

Structure: 063-83-02002 ANBL	Facility Carried: SR 63 NB	Inspector: Melvin Hughes
NBI Number: 022727	Features Intersected: ABANDONED RR	Inspection Date: 08/07/2024

National Bridge Inventory Condition Ratings

(58) Deck:	7 - Good Condition (some minor problems)
------------	--

There are hairline to medium width transverse cracks with white efflorescence in span B and span C. The north and south copings in span B have hair line to medium width cracking with efflorescence. Span C has a moderate spall between beams 6 and 7 near pier 3.

(58.01) Wearing Surface:	7 - Good Condition
--------------------------	--------------------

There are transverse, longitudinal and diagonal cracks in the wearing surface ranging from hairline to wide width, most of these cracks have been sealed. there is a minor spall at the north joint in the driving lane.

(58.02) Joints:	4 - Poor Condition, leaking, noising damage, areas of adhesion loss
-----------------	---

Joint Type: A - BS	Joint Location: Transverse North/East
--------------------	---------------------------------------

Both joints leak, material missing.

(58.05) Approach Slabs:	5 - Fair condition, no settlement, moderate cracking and spalls, crack spacing > .5'
-------------------------	--

Both approach slabs have been paved over.

(58.06) Terminal Joints:	N - No terminal joint
--------------------------	-----------------------

N

(59) Superstructure:	5 - Fair Condition (minor section loss)
----------------------	---

Span A beam 2 bent 1 the web was crushing and out of plane there is now a reto-fit in place. beam 3 at bent 1 bottom flange west side has 1/4" section loss above the bearing.

There is scale rust with some section loss at the beam ends at both end bents. There is severe rust with advanced section loss on all of the end diaphragms at both end bents. At bent 1, bearing 5 is loose. All of the bearings at bent 1 have some degree of rust, pack rust, and minor section loss. At bent 4, bearings 1, 3 and 5 are loose. All of the bearings at bent 4 have some degree of rust, pack rust, and reto-fits on bearings 3 and 7.

(59.01) Paint:	5 - Fair Condition – areas of light rust and minor peeling	Paint Year: 2002
----------------	--	------------------

There is severe rust at the beam ends and diaphragms near both end bents. This bridge was last painted green under contract B-25662

(59.02) Bearings:	4 - Poor Condition, section loss, pack rust > 1/4", bulging, loose, misalignment/slippage	Bearing Type: 1
-------------------	---	-----------------

At bent 1,beam 1 bearing fell out critical find reported, bearing 5, is loose. All of the bearings at bent 1 have rust, bent 4 1 and 6, rust, and minor section loss. At bent 4, bearings 1, 3, 5, are loose, reto-fits on bearings 3 and 7.

(60) Substructure:	7 - Good Condition (some minor problems)
--------------------	--

There are hairline vertical cracks with white efflorescence in both end bents, the south bent cap has a spall at bearing 1.

Pier 2 column 1 has a minor spall on the north side.

Pier 3 column 3 has spall on the north side.

(61) Channel / Channel Protection:	N - Use when Bridge is not over a waterway (channel).
------------------------------------	---

N

(62) Culverts:	N - Not applicable. Use if structure is not a culvert.
----------------	--

N

INDOT Defined Condition Ratings

Concrete Slopewall:	7 - Good Condition
---------------------	--------------------

Good condition.

Birds Present?:	Yes
-----------------	-----

Structure:	063-83-02002 ANBL	Facility Carried:	SR 63 NB	Inspector:	Melvin Hughes
NBI Number:	022727	Features Intersected:	ABANDONED RR	Inspection Date:	08/07/2024

Bats Present?: No

Structure: 063-83-02002 ANBL	Facility Carried: SR 63 NB	Inspector: Melvin Hughes
NBI Number: 022727	Features Intersected: ABANDONED RR	Inspection Date: 08/07/2024

Appraisal

(71) Water Adequacy: N - NOT APPLICABLE

N

(72) Approach Roadway Alignment: 8 - Equal to present desirable criteria

No speed reduction needed when approaching the structure at the current speed limit.

36A) Bridge Rails: 1 - Meets acceptable standards

36B) Transitions: 0 - Does not meet acceptable standards/safety feature is required

36D) Approach Guardrail Ends: 1 - Meets acceptable standards

Sufficiency Rating:	88.6	(67) Structural Evaluation:	5
Status:	0	(68) Deck Geometry:	7

(69) Underclearances, Vertical & Horizontal N

Scour Critical Bridge Appraisal

Bridge Inspection Scour Appraisal

(113) Scour Critical Bridges: N - Not over waterway

Scour Critical Safety Status: N/A - Bridge not over water

Countermeasures Placed/Verified:

Bridge Inspection Comments:

Hydraulic Scour Analysis Determination

Scour Analysis Status: N/A - Bridge not over water

Scour Analysis Date:

Scour Analysis Determination:

Hydraulics Comments:

Structure: 063-83-02002
ANBL
NBI Number: 022727

Facility Carried: SR 63 NB
Features Intersected: ABANDONED RR

Inspector: Melvin Hughes
Inspection Date: 08/07/2024

Element Level Condition Ratings

Component	Total Quantity	Units	CS1	CS2	CS3	CS4	Comments
12 - Reinforced Concrete Deck	7605	SF	7395	210	0	0	
510 - Wearing Surfaces	7020	SF	6380	555	85	0	
107 - Steel Open Girder/Beam	1204	LF	1160	0	43	1	CS4: Span A beam 2 bent 1 the web was crushing and out of plane there is now a retro-fit in place.
515 - Steel Protective Coating	10058	SF	9748	0	310	0	
205 - Reinforced Concrete Column	6	EA	4	1	1	0	
215 - Reinforced Concrete Abutment	91	LF	85	6	0	0	
234 - Reinforced Concrete Pier Cap	85	LF	85	0	0	0	
302 - Compression Joint Seal	160	LF	0	136	0	24	CS4: Both joints leak, material missing.
311 - Movable Bearing	14	EA	0	0	14	0	
313 - Fixed Bearing	14	EA	14	0	0	0	
321 - Reinforced Concrete Approach Slab	793	SF	793	0	0	0	
331 - Reinforced Concrete Bridge Railing	351	LF	202	134	15	0	

Structure: 063-83-02002
ANBL
NBI Number: 022727

Facility Carried: SR 63 NB
Features Intersected: ABANDONED RR

Inspector: Melvin Hughes
Inspection Date: 08/07/2024



PHOTO #:



PHOTO #:

Structure: 063-83-02002
ANBL
NBI Number: 022727

Facility Carried: SR 63 NB
Features Intersected: ABANDONED RR

Inspector: Melvin Hughes
Inspection Date: 08/07/2024



PHOTO #:



PHOTO #:

Structure: 063-83-02002
ANBL
NBI Number: 022727

Facility Carried: SR 63 NB
Features Intersected: ABANDONED RR

Inspector: Melvin Hughes
Inspection Date: 08/07/2024



PHOTO #:

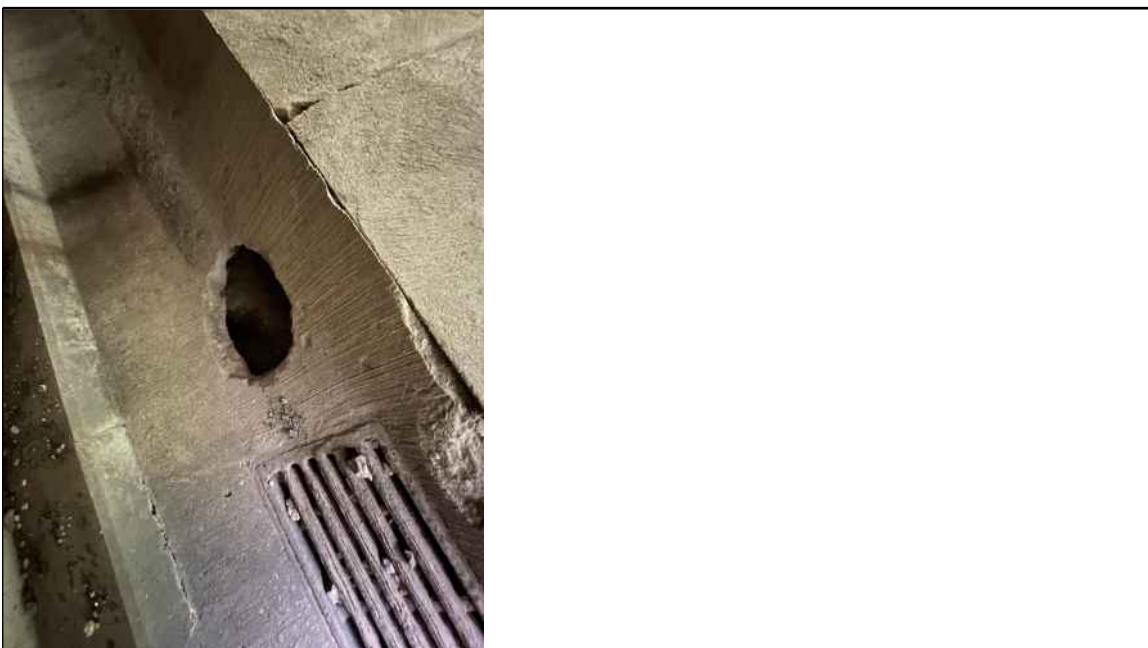


PHOTO #:

Structure: 063-83-02002
ANBL
NBI Number: 022727

Facility Carried: SR 63 NB
Features Intersected: ABANDONED RR

Inspector: Melvin Hughes
Inspection Date: 08/07/2024

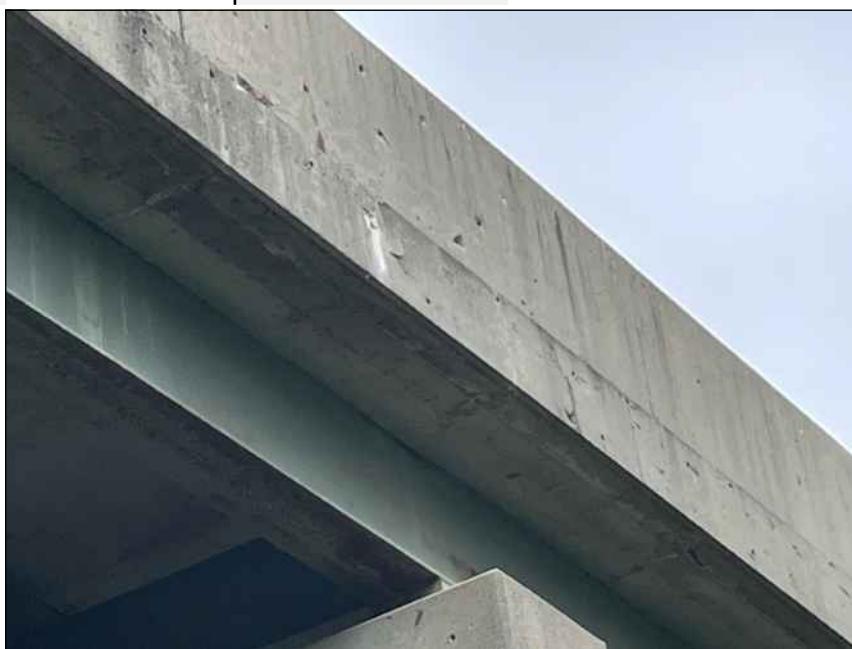


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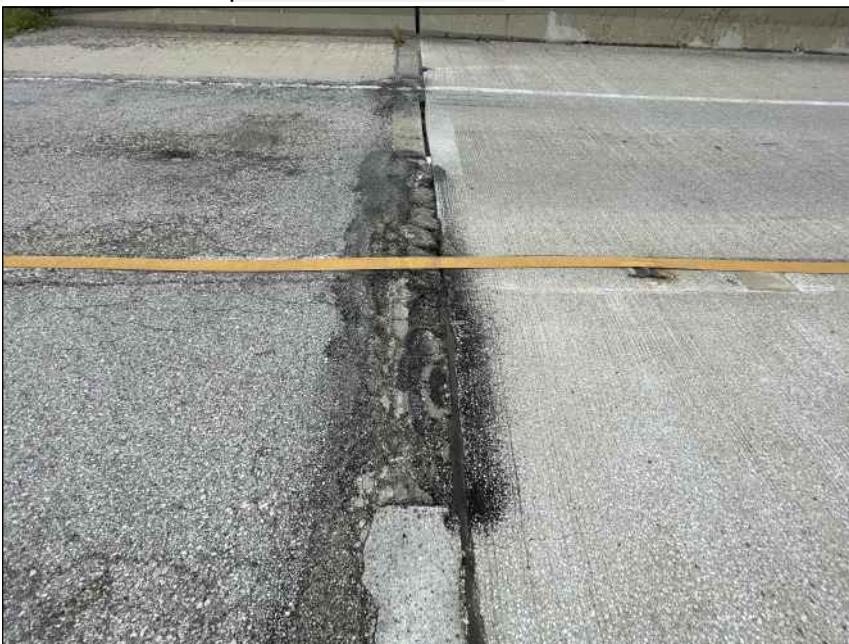


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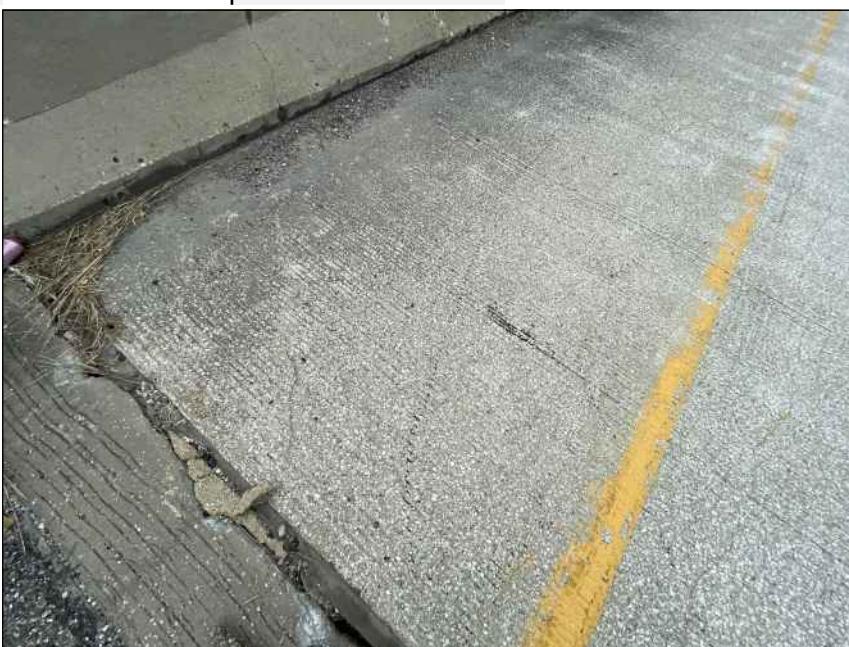


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Inspection Date: 08/07/2024



PHOTO #:



PHOTO #:

Structure: 063-83-02002
ANBL
NBI Number: 022727

Facility Carried: SR 63 NB
Features Intersected: ABANDONED RR

Inspector: Melvin Hughes
Inspection Date: 08/07/2024



PHOTO #:



PHOTO #:

Structure:	063-83-02002	Facility Carried:	SR 63 NB	Inspector:	Melvin Hughes
NBI Number:	022727	Features Intersected:	ABANDONED RR	Inspection Date:	08/07/2024



PHOTO #:



PHOTO #:

Structure: 063-83-02002
ANBL
NBI Number: 022727

Facility Carried: SR 63 NB
Features Intersected: ABANDONED RR

Inspector: Melvin Hughes
Inspection Date: 08/07/2024



PHOTO #:



PHOTO #:

Structure: 063-83-02002
ANBL
NBI Number: 022727

Facility Carried: SR 63 NB
Features Intersected: ABANDONED RR

Inspector: Melvin Hughes
Inspection Date: 08/07/2024

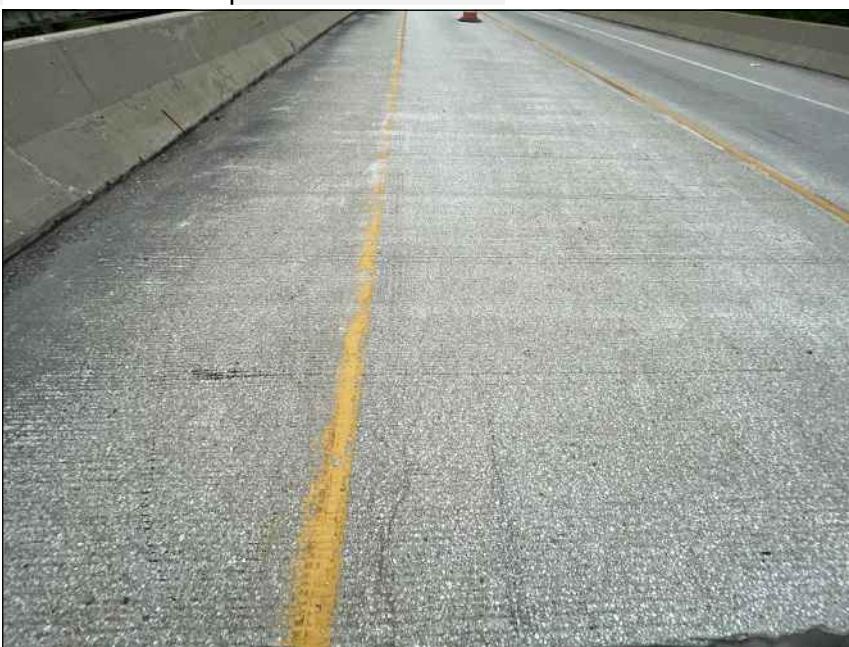


PHOTO #:



PHOTO #:

Structure:	063-83-02002 ANBL	Facility Carried:	SR 63 NB	Inspector:	Melvin Hughes
NBI Number:	022727	Features Intersected:	ABANDONED RR	Inspection Date:	08/07/2024



PHOTO #:



PHOTO #:

Structure: 063-83-02002
ANBL
NBI Number: 022727

Facility Carried: SR 63 NB
Features Intersected: ABANDONED RR

Inspector: Melvin Hughes
Inspection Date: 08/07/2024



PHOTO #:



PHOTO #:

Structure: 063-83-02002
ANBL
NBI Number: 022727

Facility Carried: SR 63 NB
Features Intersected: ABANDONED RR

Inspector: Melvin Hughes
Inspection Date: 08/07/2024



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Structure: 063-83-02002
ANBL
NBI Number: 022727

Facility Carried: SR 63 NB
Features Intersected: ABANDONED RR

Inspector: Melvin Hughes
Inspection Date: 08/07/2024



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Facility Carried: SR 63 NB
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Inspection Date: 08/07/2024

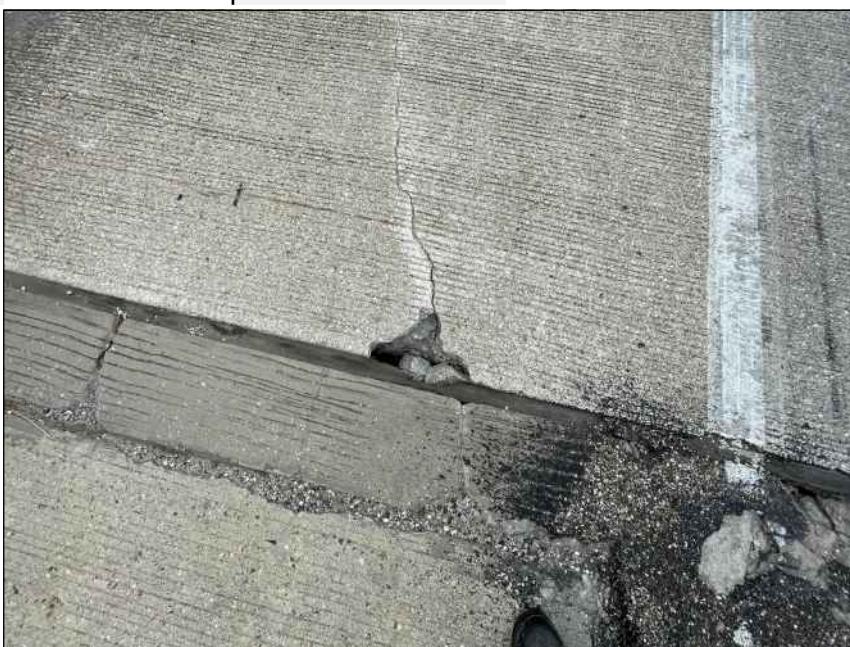


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Inspection Date: 08/07/2024



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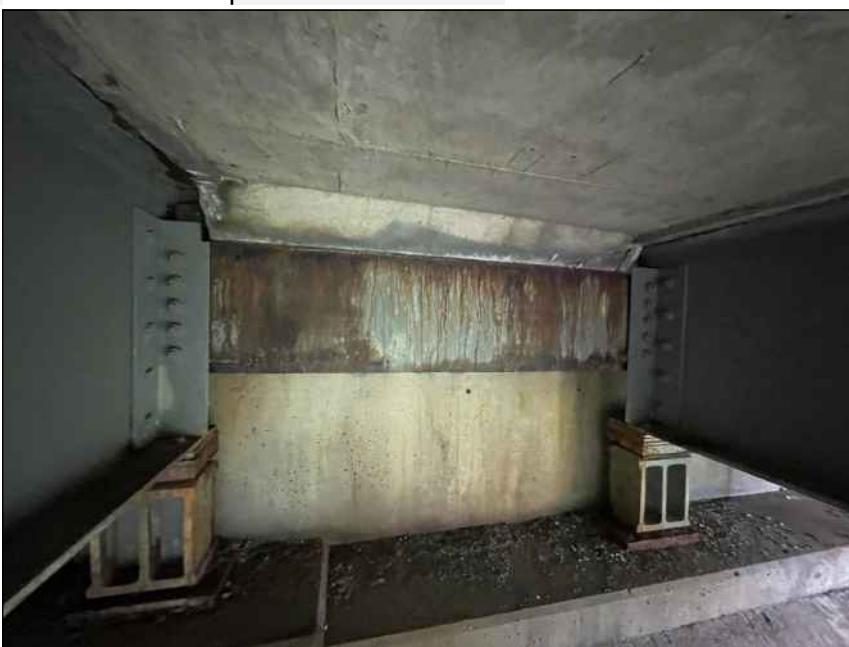


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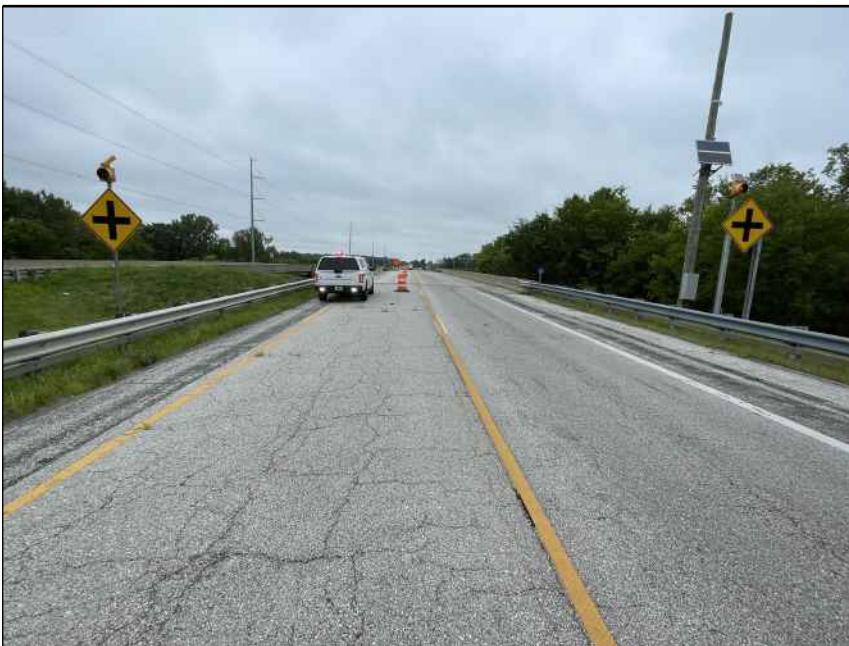


PHOTO #:

Structure Information

Structure Number:	063-83-02002 ANBL	Facility Carried:	SR 63 NB
NBI Number:	022727	Features Intersected:	ABANDONED RR
County / District:	Vermillion	Location:	00.24 S SR 234

Maintenance Needs Reported

Date Reported:	08/07/2024	Priority:	3-Green
Work Code:			
Deficiency Description:			
Recommendation:	Patch with concrete		
Maintenance Action Status:		Work Order Number:	
Maintenance Action Executed:			

Maintenance Item Priority Criteria

The Summary of Maintenance Items is a tool the county can use to determine action items to complete based on the priority guiding principal colors, which are described below, and the deficiency description.

Red	This priority is for issues that could cause the failure of all or part of the bridge or a serious traffic safety hazard if not resolved. It is recommended that repairs be completed in 4 weeks (1 month) from date of report based on the engineering judgments of the Bridge Inspection Team Leader.
Yellow	This priority is for issues that are showing signs of progression and may result in extensive deterioration, significant loss in integrity of a structural component, or may impose a traffic safety hazard if not resolved. It is recommended that repairs be completed in 12 weeks (3 months) from date of report based on the engineering judgments of the Bridge Inspection Team Leader.
Green	This priority is for issues that may result in minor component deterioration or other safety concerns. It is recommended that repairs be scheduled for repair and resolved in 26 weeks (6 months) from date of report based on the engineering judgments of the Bridge Inspection Team Leader.
Grey	This priority is for issues that is recommended to be resolved during the next cyclical preventative maintenance activity. It is recommended that repairs be completed in 56 weeks (13 months) from date of report based on the engineering judgments of the Bridge Inspection Team Leader.



Wearing surface spalled at the north joint * PHOTO

Maintenance Needs Reported

Date Reported:	08/07/2024	Priority:	3-Green
Work Code:			
Deficiency Description:			
Recommendation:	Patch with concrete.		
Maintenance Action Status:		Work Order Number:	
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Bent 4 back wall spalling * PHOTO

Bridge Inspection Report



Structure Information

Structure:	063-83-02002 ASBL	Facility Carried:	SR 63 SB
NBI Number:	022729	Features Intersected:	ABANDONED RR

Inspection Information

Inspection Date:	10/12/2023	Lead Inspector:	Melvin Hughes
Inspection Type:	Routine	Additional Inspectors:	

Condition Ratings Summary

(58) Deck:	6	(60) Substructure:	7
(58.01) Wearing Surface:	6	(61) Channel / Channel Protection:	N
(58.02) Joints:	2 - Critical Condition, severe leaking	(62) Culverts:	N
(58.03) Approach Slabs:	6 - Satisfactory condition, mild crack, wide spacing	(71) Waterway Adequacy:	N
(59) Superstructure:	5	(72) Approach Roadway Alignment:	8

Bridge Inspection Report

(59.01) Paint:

5 - Fair
Condition
n – areas
of light
rust and
minor
peeling

(113) Scour Critical Bridge:

N

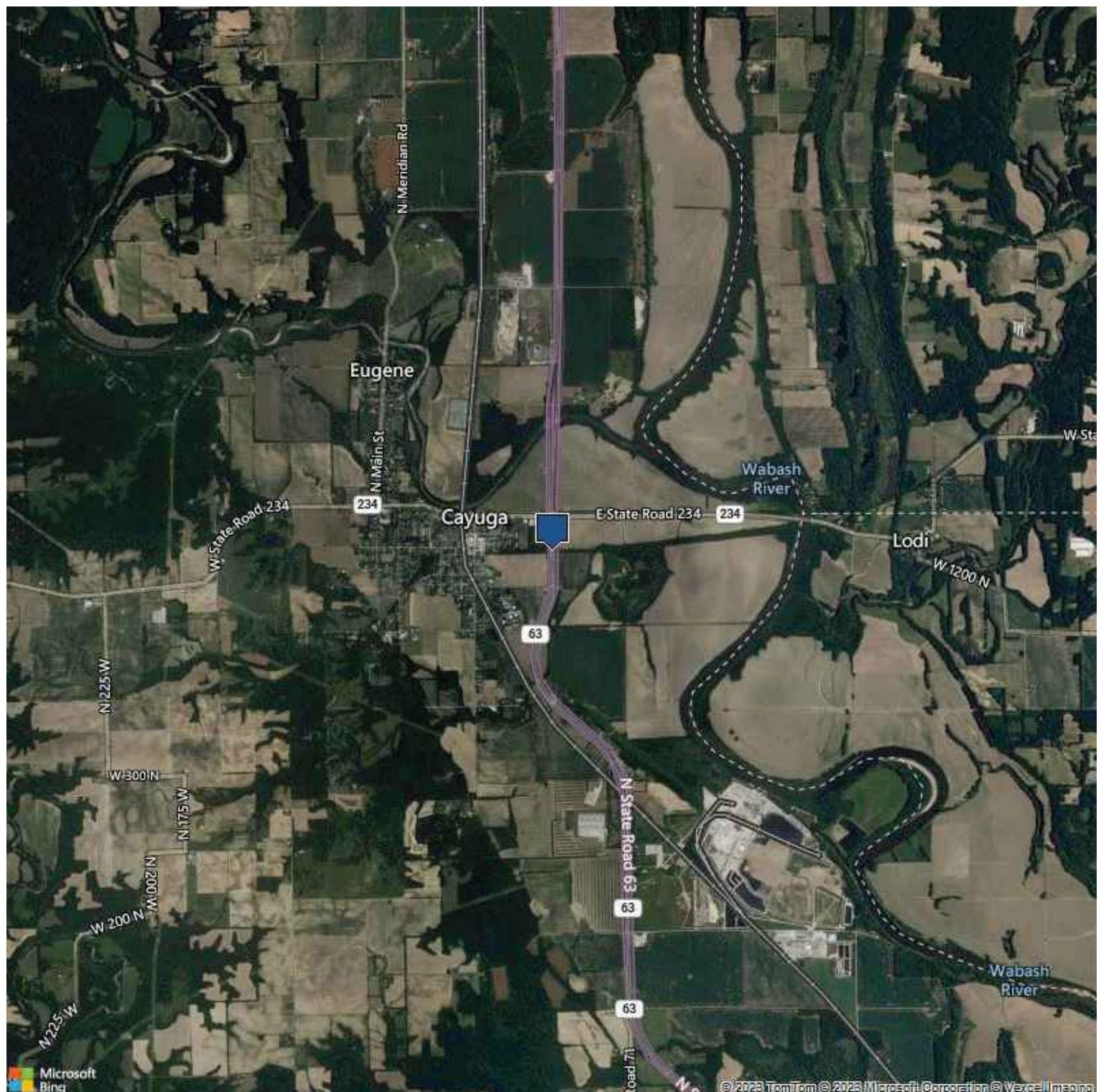


Structure: 063-83-02002
ASBL
NBI Number: 022729

Facility Carried: SR 63 SB
Features Intersected: ABANDONED RR

Inspector: Melvin Hughes
Inspection Date: 10/12/2023

LOCATION MAP



Location: 00.24 S SR 234
County: 083 Ohio

Latitude: 39.94882
Longitude: -87.4516

Structure: 063-83-02002
ASBL
NBI Number: 022729

Facility Carried: SR 63 SB
Features Intersected: ABANDONED RR

Inspector: Melvin Hughes
Inspection Date: 10/12/2023

IDENTIFICATION

(1) STATE CODE:	185 - Indiana	(12) BASE HIGHWAY NETWORK:	1
(8) STRUCTURE:	022729	(13A) INVENTORY ROUTE:	0000000001
(5) INV. ROUTE:	1 - Route carried "on" the structure - 3 - State Highway - 1 - Mainline - 00063 - 0	(13B) SUBROUTE NUMBER:	01
(2) HIGHWAY AGENCY DISTRICT:		(16) LATITUDE:	39.94882
(3) COUNTY CODE:	083 - Vermillion	(17) LONGITUDE:	-87.45160
(4) PLACE CODE:	00000	(98) BORDER	
(6) FEATURES INTERSECTED:	ABANDONED RR	(A) STATE NAME:	
(7) FACILITY CARRIED:	SR 63 SB	(B) PERCENT:	
(9) LOCATION:	00.24 S SR 234	(99) BORDER BRIDGE STRUCT. NO:	
(11) MILEPOINT:	0025.060		

AGE OF SERVICE

(27) YEAR BUILT:	1976	(19) BYPASS DETOUR LENGTH:	001
(106) YEAR RECONSTRUCTED:	1987	(29) ADT:	006793
(42) TYPE OF SERVICE		(30) YEAR OF ADT:	2021
(A) ON BRIDGE:	1 - Highway	(109) ADTT:	28
(B) UNDER BRIDGE:	0 - Other	(114) FUTURE AVERAGE DAILY TRAFFIC:	006253
(28) LANES		(115) YEAR OF FUTURE ADT:	2034
(A) ON BRIDGE:	02		
(B) UNDER BRIDGE:	00		

STRUCTURE TYPE AND MATERIAL

(43) STRUCTURE TYPE, MAIN:		(45) NUMBER OF SPANS IN MAIN UNIT:	003
(A) KIND OF MATERIAL:	4 - Steel continuous	(46) NUMBER OF APPROACH SPANS:	0000
(B) TYPE OF DESIGN:	02 - Stringer/Multibeam or Girder	(107) DECK STRUCTURE TYPE:	1 - Concrete Cast-in-Place
(44) STRUCTURE TYPE, APPROACH SPANS		(108) WEARING SURFACE PROTECTION SYSTEM	
(A) KIND OF MATERIAL:	0 - Other	A) WEARING SURFACE:	3 - Latex Concrete
(B) TYPE OF DESIGN:	00 - Other	B) DECK MEMBRANE:	0 - None
		C) DECK PROTECTION:	0 - None

Structure:	063-83-02002	Facility Carried:	SR 63 SB	Inspector:	Melvin Hughes
NBI Number:	ASBL 022729	Features Intersected:	ABANDONED RR	Inspection Date:	10/12/2023

GEOMETRIC DATA

(48) LENGTH OF MAX SPAN:	0070.0	(35) STRUCTURE FLARED:	0 - No
(49) STRUCTURE LENGTH:	00174.0	(10) INV RTE, MIN VERT CLEARANCE:	99.99
(50) CURB/SIDEWALK WIDTHS		(47) TOT HORIZ CLEARANCE:	039.8
A) LEFT:	00.0	(53) VERT CLEAR OVER BR RDWY:	99.99
B) RIGHT:	00.0	(54) MIN VERTICAL UNDERCLEARANCE:	
(51) BRDG RDWY WIDTH CURB- TO-CURB:	039.8	A) REFERENCE FEATURE:	N - Feature not a highway or a railroad
(52) DECK WIDTH, OUT-TO-OUT:	043.3	B) MIN VERT UNDERCLEAR:	00.00
(32) APPROACH ROADWAY:	038.0	(55) LATERAL UNDERCLEARANCE RIGHT:	
(33) BRIDGE MEDIAN:	0 - No Median	A) REFERENCE FEATURE:	N - Feature not a highway or a railroad
(34) SKEW:	00	B) MIN LATERAL UNDERCLEAR:	00.00
		(56) MIN LATERAL UNDERCLEAR ON LEFT:	00.0

CLASSIFICATION

(20) TOLL:	3 - On free road	(21) MAINT RESPONSIBILITY:	01 - State Highway Administration
(22) OWNER:	01 - State Highway Administration	(26) FUNCTIONAL CLASS OF INVENTORY RTE:	02 - Rural Principal Arterial - Other
(37) HISTORICAL SIGNIFICANCE:	5 - Not eligible	(100) STRAHNET HIGHWAY:	0 - Not a defense highway
(101) PARALLEL STRUCTURE:	L - Left structure (South or West)	(102) DIRECTION OF TRAFFIC:	1 - 1-way traffic
(103) TEMPORARY STRUCTURE:		(104) HIGHWAY SYSTEM OF INVENTORY ROUTE:	1 - Structure/Route is on NHS
(105) FEDERAL LANDS HIGHWAYS:	0	(110) DESIGNATED NATIONAL NETWORK:	1 - The inventory route is part of the national network for trucks
(112) NBIS BRIDE LENGTH:	Y - Yes		

Structure: 063-83-02002
ASBL
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Inspection Date: 10/12/2023

LOAD RATING & POSTING

5.1 – LOADS AND LOAD RATINGS		LEGACY CODING	
B.LR.01 - Design Load		(65) Inventory Rating Method	8
B.LR.02 - Design Method		(66) Inventory Rating	1.095
B.LR.03 - Load Rating Date		(63) Operating Rating Method	8
B.LR.04 - Load Rating Method		(64) Operating Rating	1.483
B.LR.05 - Inventory Load Rating Factor		(31) Design Load	5
B.LR.06 - Operating Load Rating Factor		(70) Bridge Posting	5 - Equal to or above legal loads
B.LR.07 - Controlling Legal Load Rating Factor		(41) Structure Open/Posted/Closed	A - Open
B.LR.08 - Routine Permit Loads		Tons Posted	
		Date Posted/Closed	

5.2 – LOAD POSTING STATUS		POSTING – EMERGENCY VEHICLES (TON)	
B.PS.01 - Load Posting Status		Emergency Vehicle Sign	
B.PS.02 - Posting Status Change Date		Posted Tonnage (Single Axle) EV	
		Posted Tonnage (Tandem) EV	
		Posted Tonnage (Gross) EV	

POSTING – COMMERCIAL VEHICLE (TON)		MAXIMUM ALLOWABLE TONNAGES	
*Actual posted values may not exceed those as shown below			
Commercial Vehicle Sign		WEIGHT LIMIT	EMERGENCY VEHICLE WEIGHT LIMIT
Posted Tonnage (Single Axle) CV		TONS	SINGLE AXLE T
Posted Tonnage (Gross) CV			TANDEM T
Posted Tonnage (2-axle) CV			GROSS T
Posted Tonnage (3-axle) CV			
Posted Tonnage (4-axle) CV			
Posted Tonnage (5-axle) CV			
Posted Tonnage (6-axle) CV			

Structure:	063-83-02002	Facility Carried:	SR 63 SB	Inspector:	Melvin Hughes
NBI Number:	022729	Features Intersected:	ABANDONED RR	Inspection Date:	10/12/2023

NAVIGATION DATA

(38) NAVIGATION CONTROL:	N - Not applicable, no waterway	(39) NAVIGATION VERTICAL CLEAR:	000.0
(111) PIER OR ABUTMENT PROTECTION:		(116) MINIMUM NAVIGATION VERTI.CLEARANCE, VERT. LIFT BRIDGE:	
		(40) NAV HORIZONTAL CLEARANCE:	0000.0

INSPECTIONS

(90) INSPECTION DATE:	(91) DESIGNATED INSPECTION FREQUENCY:
(92) CRITICAL FEATURE INSPECTION	(93) CRITICAL FEATURE INSPECTION DATE
A) NSTM INSP REQ / FREQ:	A) NSTM DATE:
B) UNDERWATER INSP REQ / FREQ:	B) UNDERWATER INSP DATE:
C) SPECIAL INSP REQ / FREQ:	C) SPECIAL INSP DATE:
D) INSPECTION EQUIPMENT NEEDED:	D) SPECIAL INSP DATE:

PROPOSED IMPROVEMENTS

(75A) TYPE OF WORK:	(76) LENGTH OF IMPROVEMENT:	(94) BRIDGE IMPROVEMENT COST:
(75B) WORK DONE BY:	000000	(95) ROADWAY IMPROVEMENT COST:
(97) YEAR OF IMPROVEMENT COST ESTIMATE:		(96) TOTAL PROJECT COST:
COMMENTS:		

Structure:	063-83-02002	Facility Carried:	SR 63 SB	Inspector:	Melvin Hughes
NBI Number:	ASBL 022729	Features Intersected:	ABANDONED RR	Inspection Date:	10/12/2023

NATIONAL BRIDGE INVENTORY CONDITION RATINGS

(58) DECK:

(58.01) WEARING SURFACE: 6 - SATISFACTORY CONDITION - structural elements show some minor deterioration.

(58.02) JOINTS:

(58.03) APPROACH SLABS:

(58.04) TERMINAL JOINTS:

(59) SUPERSTRUCTURE:

(59.01) PAINT:

(59.02) BEARINGS:

(60) SUBSTRUCTURE:

(60.1) RETAINING WALLS:

(61) CHANNEL / CHANNEL PROTECTION:

(62) CULVERTS:

(71) WATERWAY ADEQUACY:

(72) APPROACH ROADWAY ALIGNMENT:

(113) SCOUR CRITICAL BRIDGES:

Structure: 063-83-02002
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Inspection Date: 10/12/2023

INDOT DEFINED CONDITION RATINGS

CONCRETE SLOPEWALL:

BIRDS PRESENT?:

BATS PRESENT?:

APPRAISAL

36A) BRIDGE RAILS: 1

36B) TRANSITIONS: 0

36C) APPROACH GUARDRAIL: 1

36D) APPROACH GUARDRAIL ENDS: 1

SUFFICIENCY RATING:	(67) STRUCTURAL EVALUATION:	5 - Somewhat better than minimum adequacy
STATUS:	(68) DECK GEOMETRY: (69) UNDERCLEARANCES, VERTICAL & HORIZONTAL	7 - Better than present minimum criteria N - Not applicable

Structure: 063-83-02002
ASBL
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Facility Carried: SR 63 SB
Features Intersected: ABANDONED RR

Inspector: Melvin Hughes
Inspection Date: 10/12/2023

SCOUR CRITICAL BRIDGE APPRAISAL

HYDRAULIC DETERMINATION BY ANALYSIS

Scour Analysis Status:

Scour Analysis Date:

Scour Analysis Determination:

Hydraulics Comments:

BRIDGE INSPECTION FOLLOW-UP

Scour Critical Safety Status:

Date of Countermeasures Placed
or Field Verified:

Bridge Inspection Comments:

Structure: 063-83-02002
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NBI Number: 022729

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Inspector: Melvin Hughes
Inspection Date: 10/12/2023

ELEMENT LEVEL CONDITION RATINGS

Component	Total Quantity	Units	CS1	CS2	CS3	CS4	Comments
12 - Reinforced Concrete Deck	7605	SF	7446	156	3	0	
510 - Wearing Surfaces	7020	SF	6897	120	3	0	
107 - Steel Open Girder/Beam	1204	LF	1150	50	4	0	
515 - Steel Protective Coating	10058	SF	9258	800	0	0	
205 - Reinforced Concrete Column	6	EA	5	1	0	0	
215 - Reinforced Concrete Abutment	91	LF	90	1	0	0	
234 - Reinforced Concrete Pier Cap	85	LF	85	0	0	0	
301 - Pourable Joint Seal	160	LF	77	10	3	70	
311 - Movable Bearing	14	EA	2	10	2	0	
313 - Fixed Bearing	14	EA	14	0	0	0	
321 - Reinforced Concrete Approach Slab	1216	SF	1155	60	1	0	
331 - Reinforced Concrete Bridge Railing	351	LF	289	60	2	0	

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Typical cracking in bridge rail

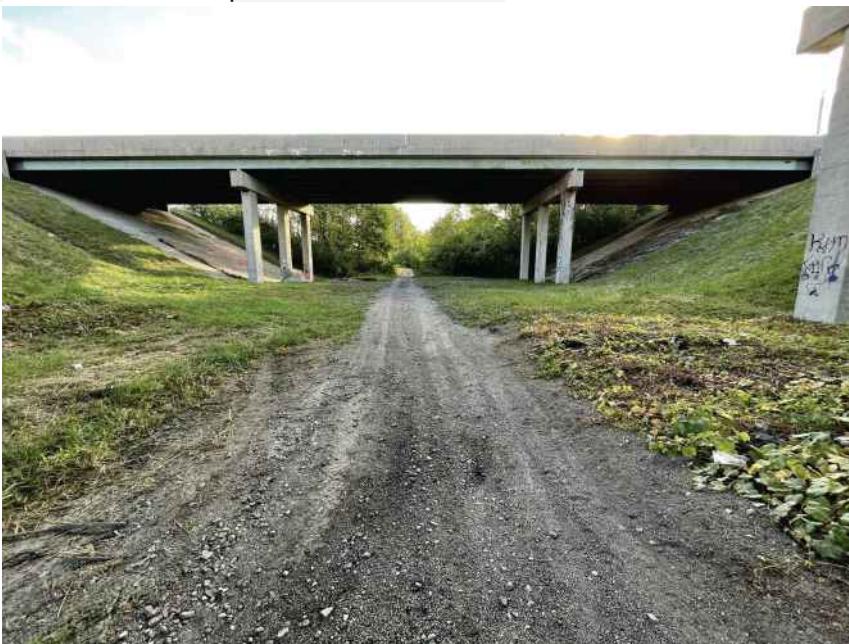


Span C full depth patch near bent 4

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Looking east on old railroad bed

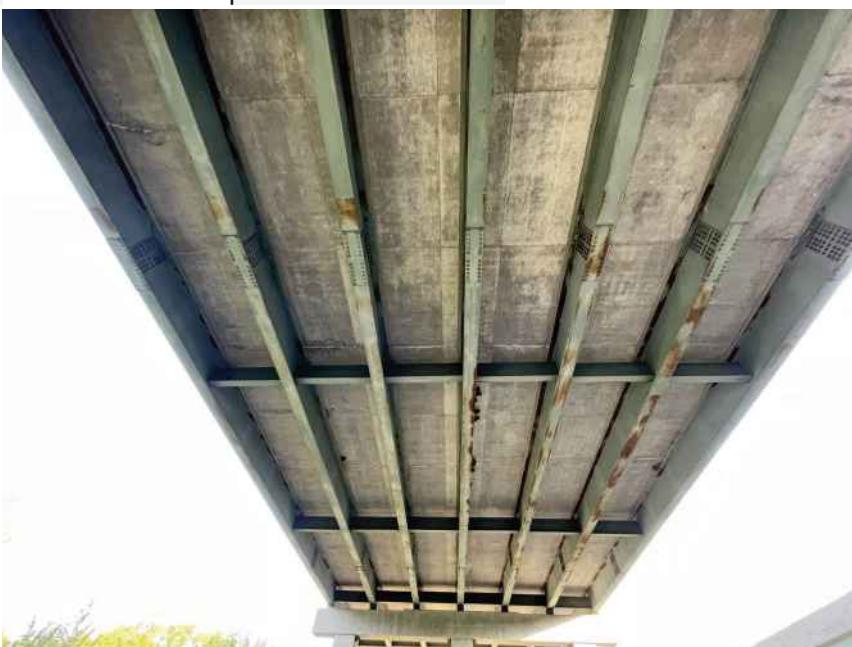


West bridge rail condition at top of rail

Structure: 063-83-02002
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Inspection Date: 10/12/2023



Span B underside condition looking north



Bent 4 bearing 4 loose

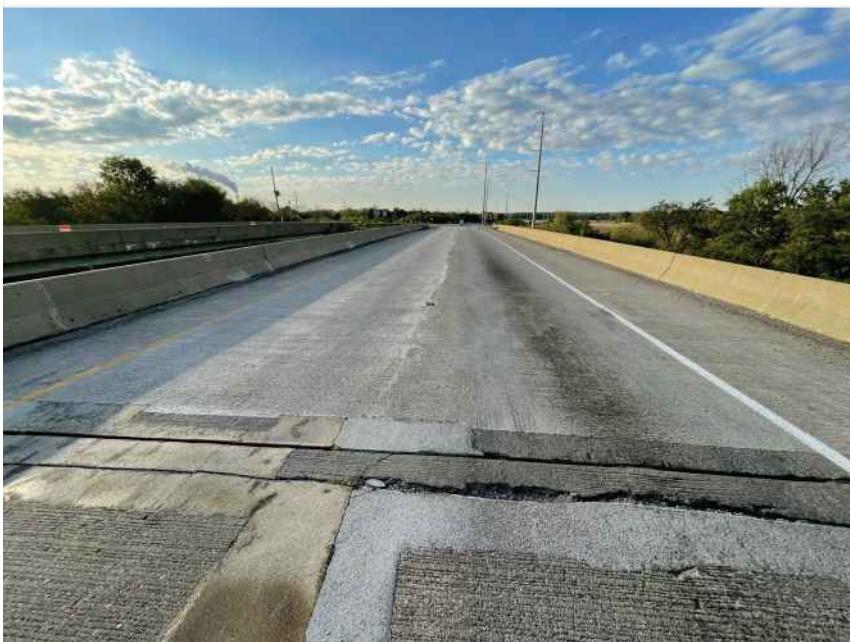
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ASBL
NBI Number: 022729

Facility Carried: SR 63 SB
Features Intersected: ABANDONED RR

Inspector: Melvin Hughes
Inspection Date: 10/12/2023



Wearing surface has a asphalt patch on the west shoulder

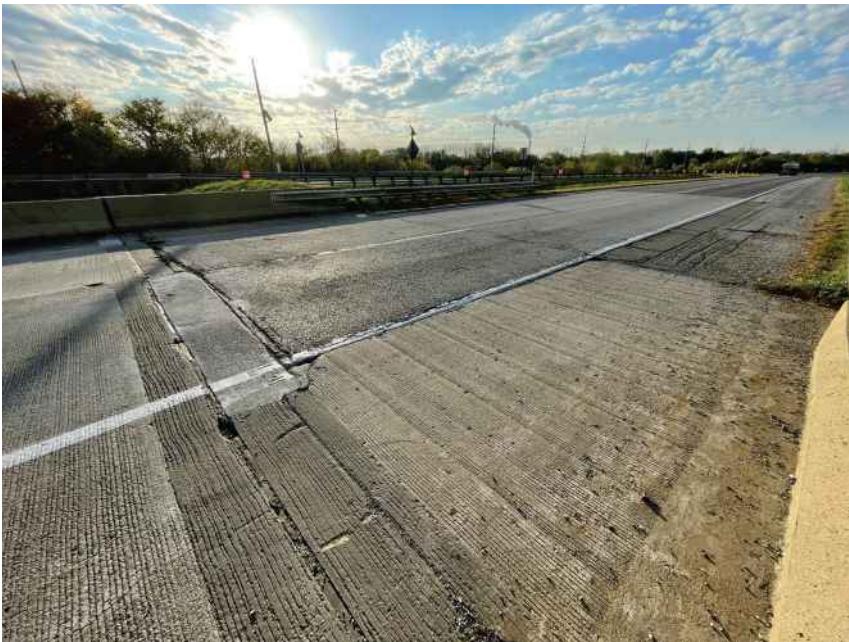


Wearing surface condition looking south

Structure: 063-83-02002
ASBL
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Facility Carried: SR 63 SB
Features Intersected: ABANDONED RR

Inspector: Melvin Hughes
Inspection Date: 10/12/2023



South joint and approach slab condition



East profile

Structure: 063-83-02002
ASBL
NBI Number: 022729

Facility Carried: SR 63 SB
Features Intersected: ABANDONED RR

Inspector: Melvin Hughes
Inspection Date: 10/12/2023



South slope condition has cracking



Bent 4 diaphragm between beams 6 and 7 rotted off

Structure: 063-83-02002
ASBL
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Inspector: Melvin Hughes
Inspection Date: 10/12/2023



Bent 4 bearing 3 is loose

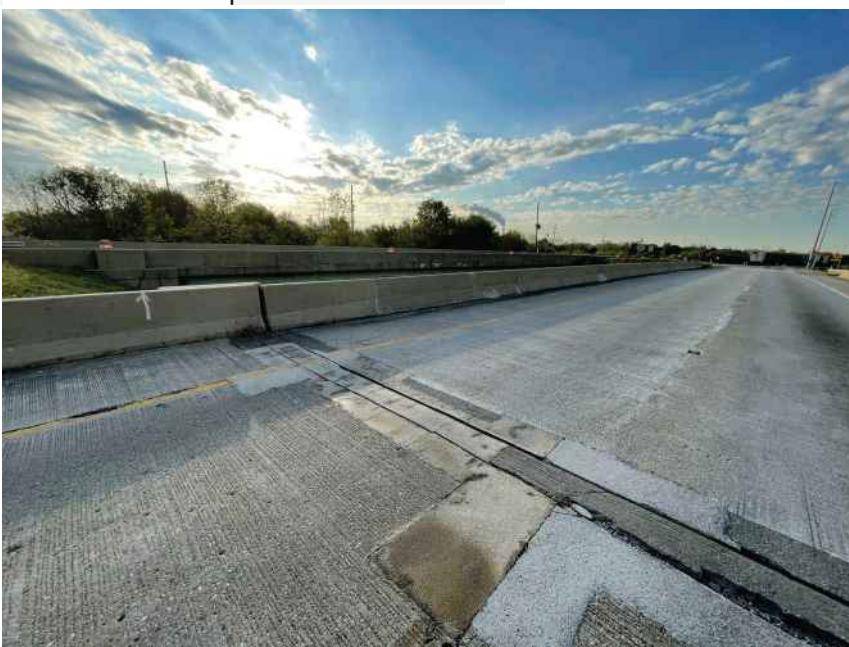


Road alignment looking north

Structure: 063-83-02002
ASBL
NBI Number: 022729

Facility Carried: SR 63 SB
Features Intersected: ABANDONED RR

Inspector: Melvin Hughes
Inspection Date: 10/12/2023



East bridge rail condition

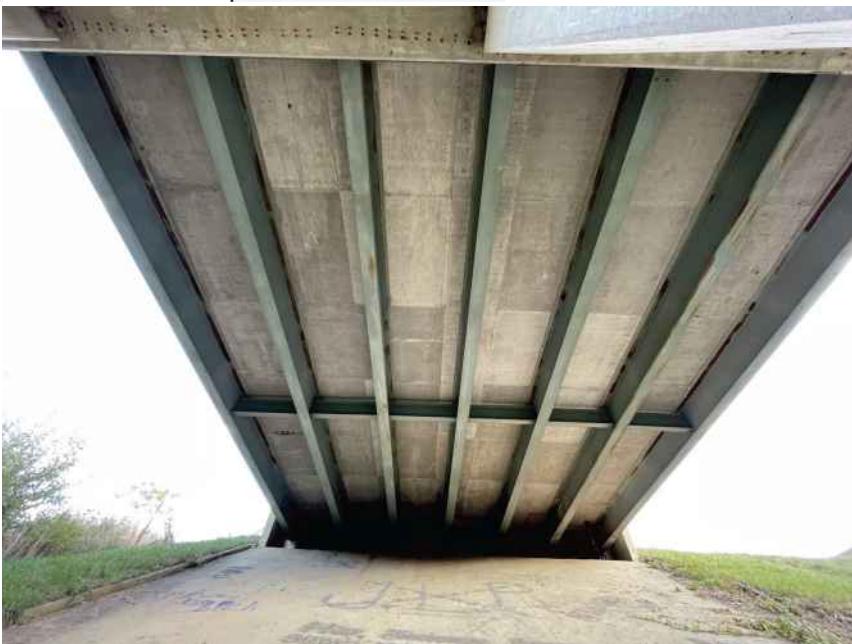


Span A underside condition

Structure: 063-83-02002
ASBL
NBI Number: 022729

Facility Carried: SR 63 SB
Features Intersected: ABANDONED RR

Inspector: Melvin Hughes
Inspection Date: 10/12/2023



Span C underside condition



Pier 3 south face condition

Structure: 063-83-02002
ASBL
NBI Number: 022729

Facility Carried: SR 63 SB
Features Intersected: ABANDONED RR

Inspector: Melvin Hughes
Inspection Date: 10/12/2023



Bent 4 beam 6 and bearing fix condition



Pier 3 south face column 1 minor spalling

Structure: 063-83-02002
ASBL
NBI Number: 022729

Facility Carried: SR 63 SB
Features Intersected: ABANDONED RR

Inspector: Melvin Hughes
Inspection Date: 10/12/2023



North slope wall condition has cracking



North concrete drain trough spalled

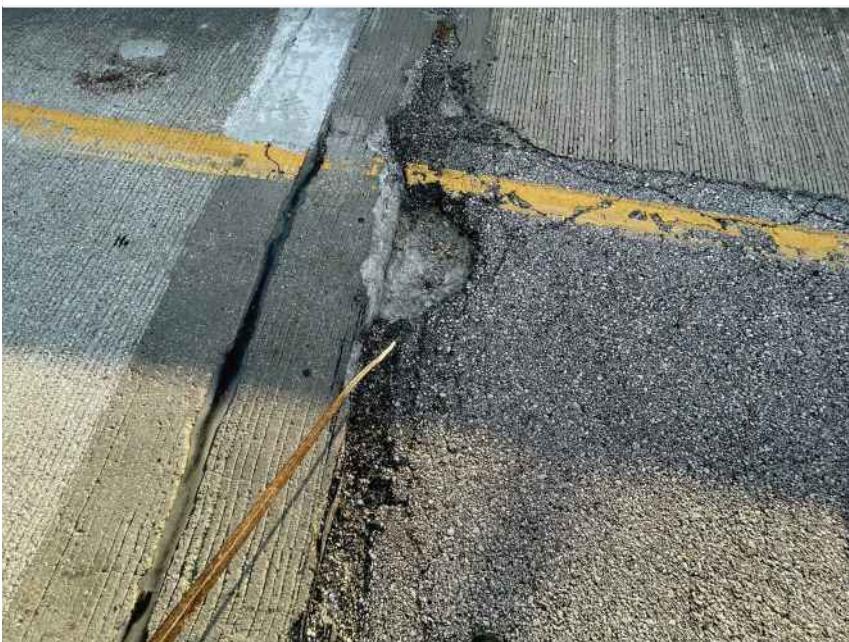
Structure: 063-83-02002
ASBL
NBI Number: 022729

Facility Carried: SR 63 SB
Features Intersected: ABANDONED RR

Inspector: Melvin Hughes
Inspection Date: 10/12/2023



Bent 4 bearing 1 condition



South approach slab has a spall in the passing lane

Structure: 063-83-02002
ASBL
NBI Number: 022729

Facility Carried: SR 63 SB
Features Intersected: ABANDONED RR

Inspector: Melvin Hughes
Inspection Date: 10/12/2023



Bent 1 beam 6 condition

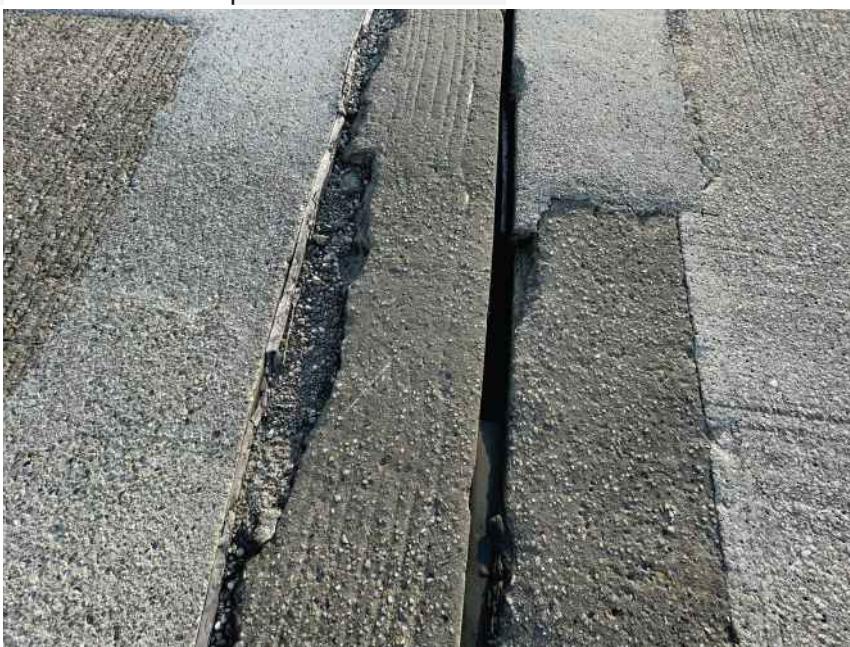


Wearing surface has hairline to wide cracking

Structure: 063-83-02002
ASBL
NBI Number: 022729

Facility Carried: SR 63 SB
Features Intersected: ABANDONED RR

Inspector: Melvin Hughes
Inspection Date: 10/12/2023



North joint failed and bent 4 is spalled



Bent 1 condition

Structure: 063-83-02002
ASBL
NBI Number: 022729

Facility Carried: SR 63 SB
Features Intersected: ABANDONED RR

Inspector: Melvin Hughes
Inspection Date: 10/12/2023



Road alignment looking south



West bridge rail condition

Structure: 063-83-02002
ASBL
NBI Number: 022729

Facility Carried: SR 63 SB
Features Intersected: ABANDONED RR

Inspector: Melvin Hughes
Inspection Date: 10/12/2023



Looking west on old railroad bed



Bent 1 has vertical cracking

Structure: 063-83-02002
ASBL
NBI Number: 022729

Facility Carried: SR 63 SB
Features Intersected: ABANDONED RR

Inspector: Melvin Hughes
Inspection Date: 10/12/2023



West profile



East deck coping and beam 7 condition

Structure: 063-83-02002
ASBL
NBI Number: 022729

Facility Carried: SR 63 SB
Features Intersected: ABANDONED RR

Inspector: Melvin Hughes
Inspection Date: 10/12/2023



Pier 2 south face condition



Bent 4 condition

Structure: 063-83-02002
ASBL
NBI Number: 022729

Facility Carried: SR 63 SB
Features Intersected: ABANDONED RR

Inspector: Melvin Hughes
Inspection Date: 10/12/2023



North approach slab and joint condition



Wearing surface has hairline to wide cracks some have been sealed

Structure: 063-83-02002
ASBL
NBI Number: 022729

Facility Carried: SR 63 SB
Features Intersected: ABANDONED RR

Inspector: Melvin Hughes
Inspection Date: 10/12/2023



Bent 1 bearing 7 condition



Span C underside spalled between beams 1 and 2

Katie Finney

From: Ervin, Brock <BERvin@indot.IN.gov>
Sent: Friday, July 19, 2024 12:50 PM
To: Brian Shaw
Cc: Miller, Jessica; Jeff Parke; Katie Finney; Kopetski, Pete
Subject: RE: Des 2100968 & 2100969 SR 63 Bridge Removal, Vermillion County

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Hi, Brian.

Regarding the SR 63 bridge removals and the PI needs associated with the railroad, I had some back and forth with Ron Bales on this issue today.

Regarding the trail, I'd like more information on its current status. If the trail is currently planned by a public entity and there's been formal action in that direction, we'll need to consider whether or not the project warrants 4(f) consideration. Please let us know what you've learned about the planned trail, ownership, incorporation into their fiscal plans. Any IDNR involvement with trail funding. If that's happening, then we'd need to address the 4(f) aspects. This might be something that dictates the CE level, and even the need for PI – some of the 4(f) options require PI. If it's to the point where Cayuga is willing to pay for the culvert, I suspect 4(f) likely will need addressed. If that's true, it may qualify for a net-benefit.

Disregarding any other thresholds, we are satisfied that the r/w needs would fall under the PI Manual's exemption for "Acquiring R/W presently within INDOT apparent ROW to establish legal documented ownership". And as this would be acquisition to acquire land that is within the apparent existing r/w, it would not need to be included in the right-of-way quantities toward the CE level threshold. But the term "reacquisition" is not applicable.

Also, during this project, I've been guided to ask that we not refer to the railroad as being abandoned. Long story short, there are apparently legal reasons. So moving forward, please refer to it as the "former" railroad corridor or similar. It's safe to say that the RR company has abandoned it for its previous use, but as a general descriptor, avoid the "abandoned RR" phrase.

Hope this helps. Let me know if you have any other questions.

Brock Ervin (He/Him/His)
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From: Ervin, Brock
Sent: Thursday, July 18, 2024 4:44 PM
To: Brian Shaw <bshaw@b-l-n.com>
Cc: Miller, Jessica <JeMiller1@indot.IN.gov>; Jeff Parke <jparke@b-l-n.com>; Katie Finney <kfinney@b-l-n.com>; Kopetski, Pete <PKOPETSKI@indot.IN.gov>
Subject: RE: Des 2100968 & 2100969 SR 63 Bridge Removal, Vermillion County

Hi, Brian.

FYI... Zane is no longer with INDOT. Pete Kopetski, our design manager, is the acting ESM. Any questions should generally go through the PM, who can then direct it to the appropriate person. But as this question has made it to us, I'll answer it as best I can.

As you say, while Norfolk and Southern no longer uses the property as a railroad and it is technically within the apparent existing r/w, they still own it, so it would be a change of use, precluding them from using it again or any other purpose. It is not land that we've ever thought we've owned, for example, like with the untimely recorded deeds or similar. The land is pretty obviously not for transportation, so it's not reacquisition of apparent existing r/w, and the first option would not apply.

As for establishing legal ownership, this seems to be an appropriate use of this exemption, but I've not actually seen it used before, so it's not entirely clear. If it is applicable, it is an exemption from the requirement for PI, but the CE threshold table does not list this type of PI exception to the r/w threshold in the way that it does for reacquisition. Of the four PI exceptions from needing PI for r/w acquisition, this one and donated r/w are not listed as exceptions from the CE-2 threshold.

So, as you can see, I can't give you a clear answer at this time. I've already messaged some of my peers to see if this is an appropriate use for the exemption and if it'd allow the CE level to be reduced. Given that the acquired r/w is flush and continuous with the r/w on either side of the railroad, this seems an appropriate use of the exemption, but I need to confirm this. I'll let you know as soon as I hear something. Based on the response, the project team can determine if a meeting is necessary.

Thanks for checking in on this project.

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From: Brian Shaw <bshaw@b-l-n.com>
Sent: Thursday, July 18, 2024 3:22 PM
To: Kurtz, Randy <RKurtz@indot.IN.gov>
Cc: Ervin, Brock <BErvin@indot.IN.gov>; Miller, Jessica <JeMiller1@indot.IN.gov>; Jeff Parke <jparke@b-l-n.com>; Katie Finney <kfinney@b-l-n.com>
Subject: Des 2100968 & 2100969 SR 63 Bridge Removal, Vermillion County

**** This is an EXTERNAL email. Exercise caution. DO NOT open attachments or click links from unknown senders or unexpected email. ****

Zane,

Good afternoon. We are currently working with the INDOT Crawfordsville District on development of design plans and a draft environmental documentation for Des 2100968 & 2100969 SR 63 Bridge Removal over Abandoned Railroad (N & S), 0.24 mile south of SR 234 near Cayuga in Vermillion County. We had a question concerning the right-of-way acquisition and public involvement requirement for the project.

Currently the project preferred alternative includes the following:

- Removing the bridge superstructure
- End bents will be removed to a minimum depth of 2 feet below the proposed pavement subgrade
- Bent caps will be removed from the interior bents leaving the columns and foundations in place.
- Existing concrete slope walls will not be removed from the spill slopes at each end bent.
- SR 63 will be reconstructed on the same horizontal and vertical alignments and guardrail will be installed along the outside shoulders.
- Existing median and side slopes will be maintained
- There will be backfill of the existing crossing with roadway embankment material and installation of a single span three-sided underfill tunnel (14-foot span, 14.5-foot rise) to allow for the option of a future 10-foot pedestrian path (by others) to cross under the SR 63 roadway.

Please note the inclusion of the three-sided structure would be funded by Vermillion County, not INDOT and coordination by INDOT with Vermillion County concerning inclusion of the structure/underfill tunnel as part of this project is ongoing. If the underfill tunnel structure is removed as part of the final design, the environmental documentation will be updated as appropriate to reflect that change and any additional revisions prior to final approval.

The area under the existing SR 63 bridge is an abandoned Norfolk and Southern Railroad corridor with the tracks and ties removed. We checked with our ROW Engineering staff and the property although abandoned by Norfolk and Southern Railroad in 1987, the property is still owned by the Norfolk and Southern Railroad and there would be acquisition of 1.3 acre of permanent ROW from that area. I have attached plan sheet excerpts, parcel map and some ground level photographs as reference.

We had anticipated a CE level 2 document with opportunity for public hearing as a result of the right-of-way being over 0.5 acre. In our discussions with the INDOT Crawfordsville project management team there were questions about this right-of-way possibly falling under one of the following exemptions for triggering public involvement:

1. The project does not involve new right-of-way but rather reacquires past prescriptions (i.e., existing, or apparent existing right-of-way).
2. Acquiring right-of-way presently within INDOT apparent right-of-way to establish legal documented ownership.

One additional item. There will be some type of public involvement for the project due to the discussions with Vermillion County about the three-side structure to provide access under SR 63 and the location of the project near the Town of Cayuga. If the right-of-way acquisition does not fall under one of the exemptions listed above, then we would proceed with a Level 2 CE with opportunity for hearing. The INDOT PM has indicated if that is the situation then we would go ahead and schedule a public hearing after the draft environmental is released for public involvement. If the right-of-way does fall under the exemption, we can have a Level 1 CE that does not require opportunity for hearing, but INDOT would hold a public information meeting at a later time.

We discussed this with the INDOT team yesterday and we wanted to check with you and get your take on the situation before moving forward.

Let me know if you have questions or need more information. It may be advantageous to set up a Teams Meeting and discuss, if needed.

Thank you

BRIAN SHAW

Director

Environmental Services

o: 317-806-3028

c: 317-709-3440

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