




GENERAL NOTES :

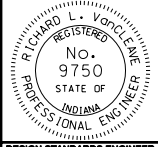

- ① These notes apply to Standard Drawings 610-DRIV-01 through 12.
- ② If a PCCP approach is designed for a class II or class IV drive, the radii shall be constructed using ear construction type C as detailed on Standard Drawing 605-ERCN-02.
- ③ When the maximum approach grade of $\pm 10\%$ does not meet the grade of the existing drive before the R/W line, the approach grade of $\pm 10\%$ shall extend beyond the R/W to the point of intersection with the existing driveway grade. Construction beyond the R/W line shall be done in temporary R/W.
- ④ The appropriate pipe end treatment should be provided for pipes located either inside the clear zone or outside the clear zone.
- ⑦ The maximum algebraic difference in grades shall not exceed 8% for crested grade nor 12% for sagged grades for Types I and III drives, nor 11% for crested grade and 14% for sagged grades for Types II, IV, and V drives.
- ⑧ The minimum driveway pavement sections for Class VI and Class VII Drives have been designed for 200 trucks per day. If the truck traffic count is greater than 200 per day, the required pavement section shall be as shown elsewhere on the plans.
- ⑪ Hc - earth cover over culvert or pipe shall be 300 or greater.
- ⑫ Curb ramp type H, as shown on Standard Drawing 604-SWCR-09, when the approach is signalized, or a sidewalk elevation transition as shown on Standard Drawing 604-SDWK-02 shall be used when sidewalk is adjacent to curb.
- ⑬ When X is equal to or greater than 600 but less than 1800, either a curb ramp type G as shown on Standard Drawing 604-SWCR-09, when the approach is signalized, or a sidewalk elevation transition as shown on Standard Drawing 604-SDWK-01 shall be used.
- ⑭ When X is equal to or greater than 1800, no curb ramp or sidewalk elevation transition is required unless the curb height is in excess of 150.
- ⑮ Driveway embankment slope within the clear zone for a road functionally classified as follows shall be:
 - a.) 6:1 for an arterial or a high speed (80km/h or greater design speed) collector.
 - b.) 4:1 for a local road or a low speed (less than 80 km/h design speed) collector.

⑪ See Standard Drawing 610-DRIV-14 for shoulder treatment at driveways.

LEGEND

- ⑤ ½ in. preformed joint filler
 - ⑥ Monolithic curb
 - ⑨ Longitudinal joint
 - ⓕ Concrete sidewalk
 - Ⓢ For type and thickness equivalent to surface in place, see plans.
- X = Distance between face of curb and sidewalk
 - W = Width of sidewalk
 -  PCCP
 -  Curb ramp, if signalized, or typically, sidewalk elevation transition.
 -  Curb ramp or sidewalk elevation transition section view.

All Dimension are in mm unless otherwise specified

INDIANA DEPARTMENT OF TRANSPORTATION	
DRIVES GENERAL NOTES AND LEGEND	
MARCH 2006	
STANDARD DRAWING NO. 610-DRIV-13	
	/s/ Richard L. VanCleave 3-01-06 DESIGN STANDARDS ENGINEER DATE
	/s/ Richard K. Smutzer 3-01-06 CHIEF HIGHWAY ENGINEER DATE
DESIGN STANDARDS ENGINEER	