Welcome to the 2023 INDOT Bridge Design Conference

The conference will start momentarily.

Welcome to the 2023 INDOT Bridge Design Conference

Stephanie Wagner, Bridge Engineering Director

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About today…

- Concept by the ASCE-INDOT Structures Committee
  - Completely “Volunteer”- Run Event
- Goals
  - Share Applicable Information
    - Policy Updates
    - Research
    - Practical Design Topics
    - Other Resources
  - Attendees Leave with Tangible Take-Aways

Thank You

- All the Presenters
- Executive Producer Jim Lesh and the production team
- Survey Responders

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Hybrid Format: How-To

Everyone

- [ ] Presentations Posted to Web
- [ ] CEU Certificates to be Emailed

LAST CALL: Week after email notification!

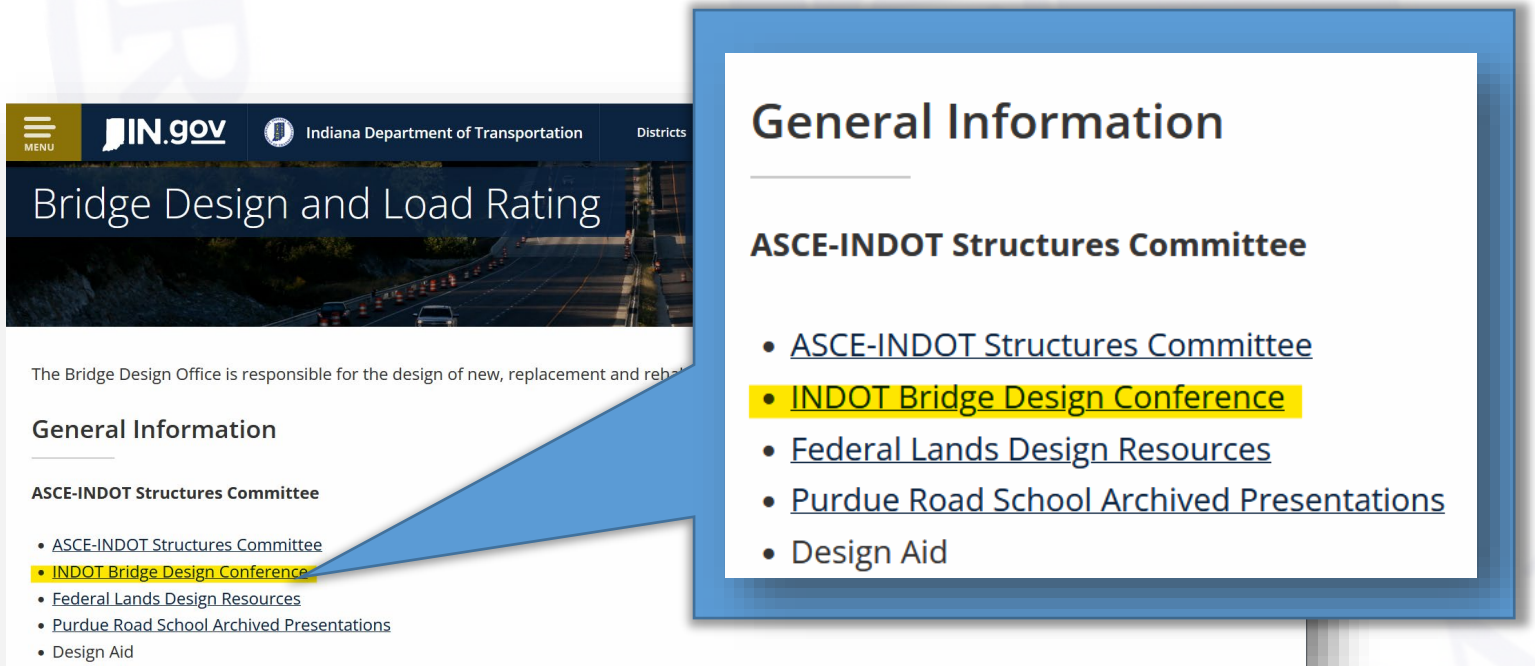
- [ ] PLEASE Complete Survey

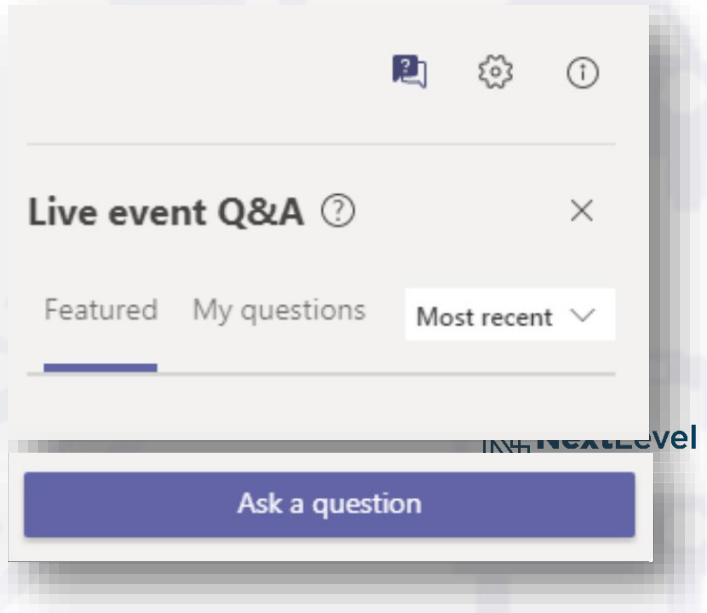
In-Person

- [ ] Paper Sign-In Sheets (AM & PM)
- [ ] Hold Questions until the End

Virtual

- [ ] Credit for Time in TEAMS Event
- [ ] Use Q&A Box (sooner the better)
- [ ] Sign-off Before Event Ends





What's the plan?

- Policy Update with Bonus: Pavement Design for Bridges

  Break: 9:35am—9:50am

- Lessons Learned
- NEXT Beams
- Deck Pour Sequences

  Lunch (Lunch is on your own): 11:40am—1:00pm

  JTRP Poster Session in Atrium

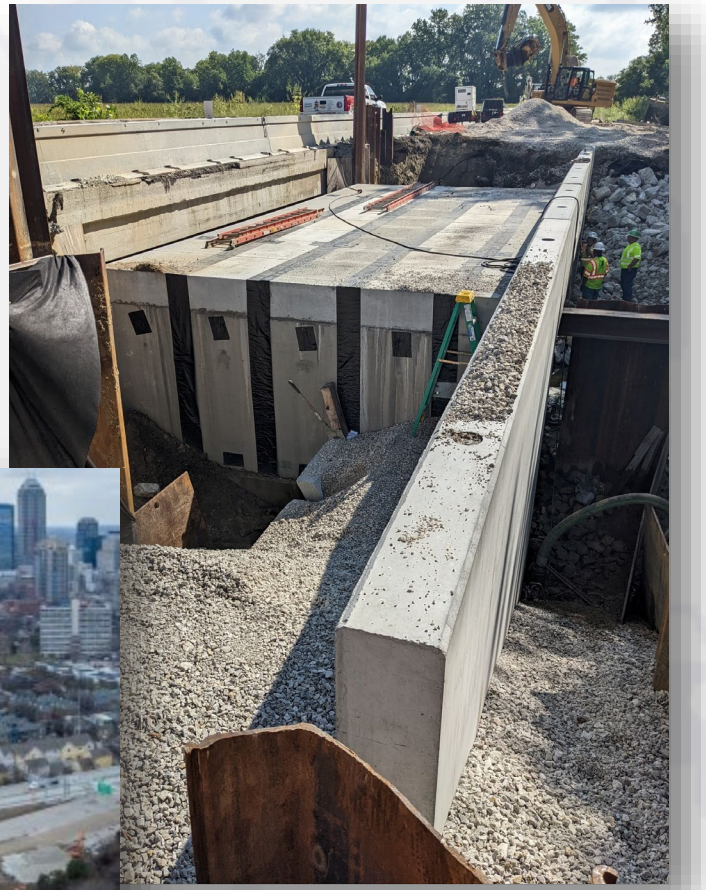
- Drilled Shafts
- Under-Fill Load Ratings

  Break: 2:45pm—3:00pm

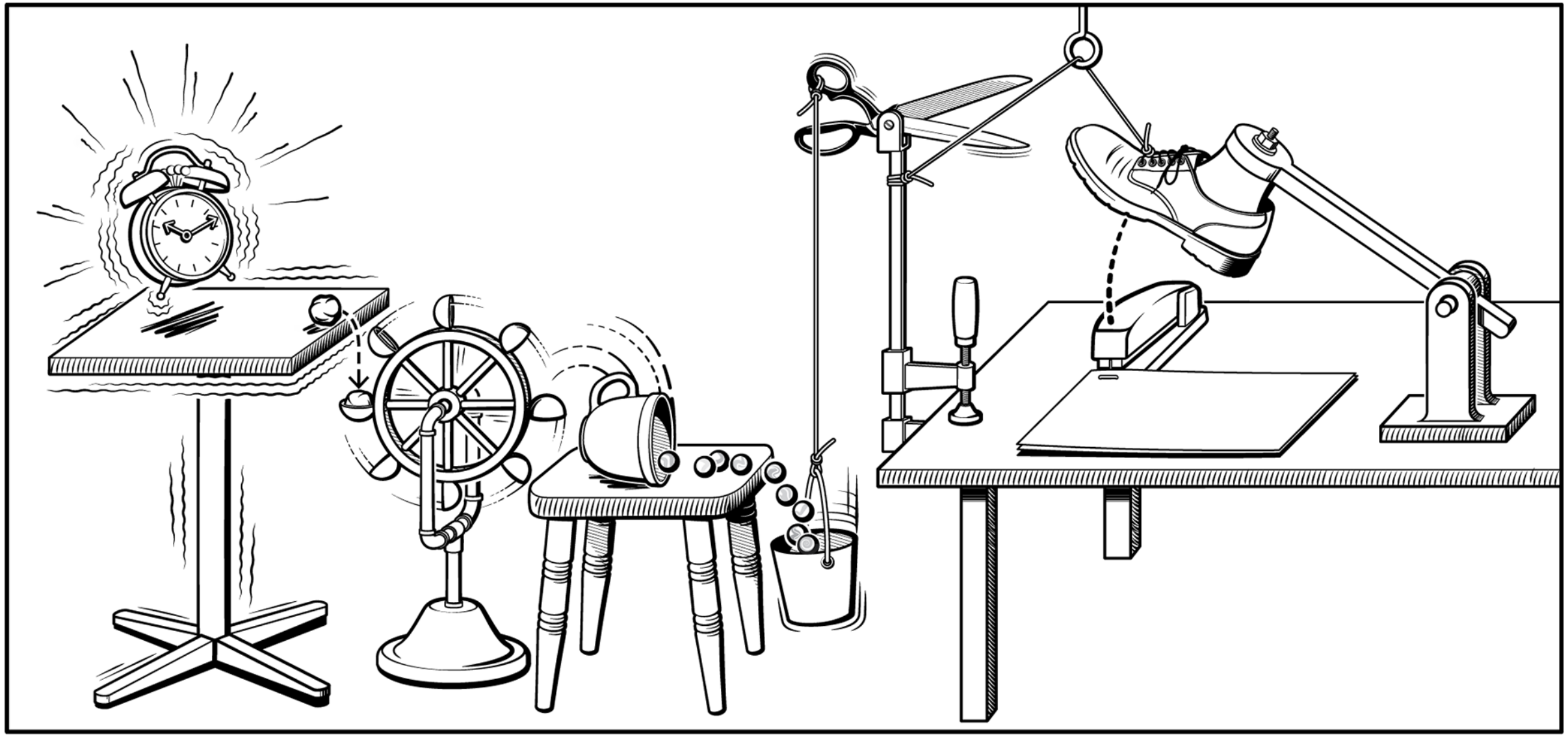
- SR62 Case Study
- Bridge Asset Management

Special Guest

Jessica Miller,
Managing Director of Project Delivery



Every Step Matters

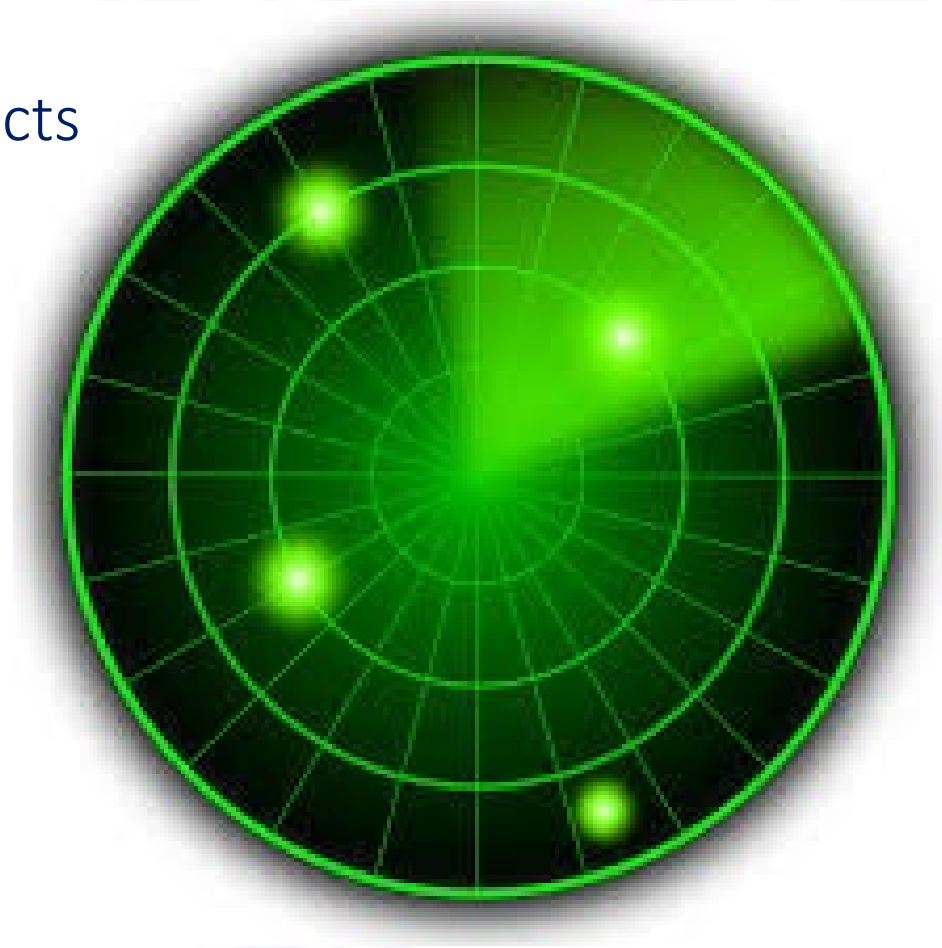


On the Radar:

Challenge:

Accounting for Utility Relocation in Environmental Impacts

- Environmental Document @ STG2
- Relocation Details @ STG3 (🤞)



Complication:

Standard Spec Requirements for Construction Limits

Clearing for Utilities is Project Specific

Solution:

Utility Relocation Area (as seen on major projects)

- USP for Clearing Required by Contractor

Project Area = INDOT Construction Limits + Utility Relocation Area

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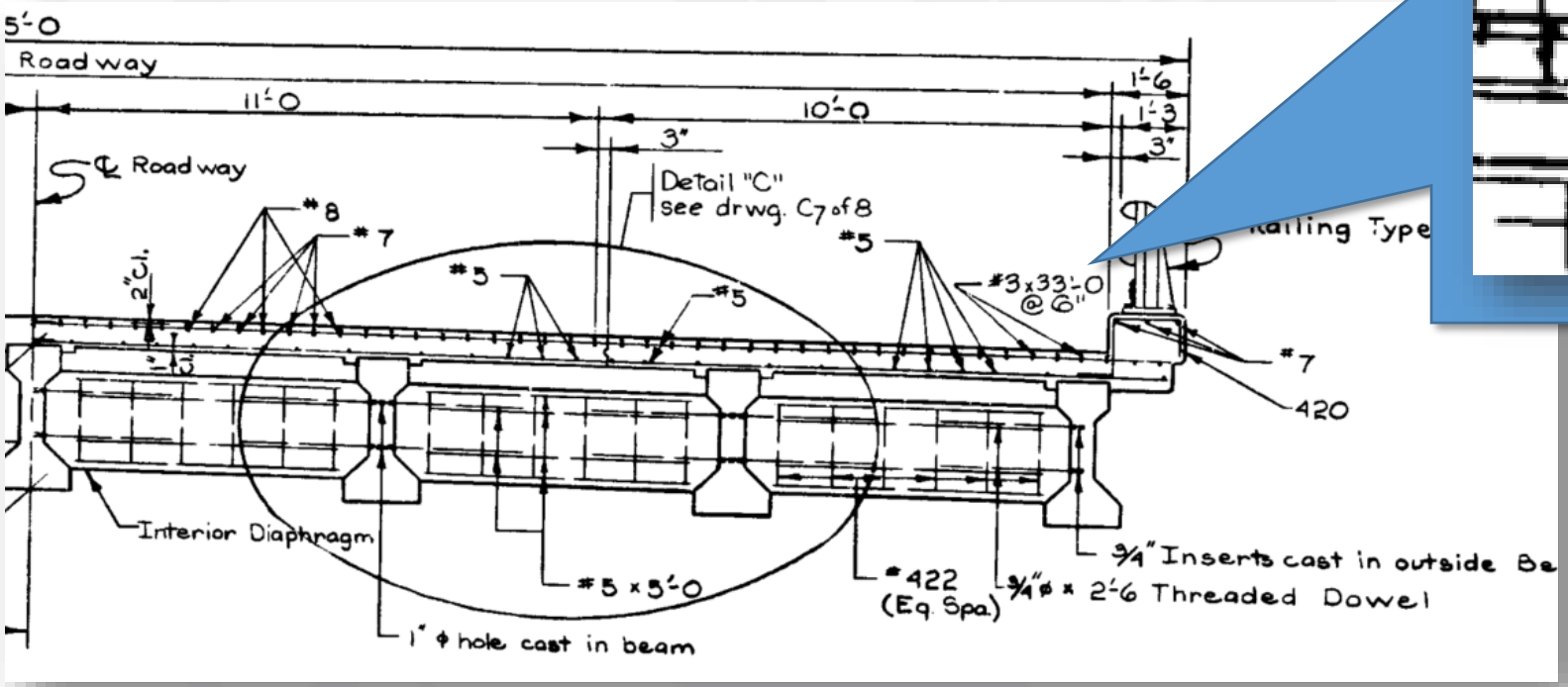
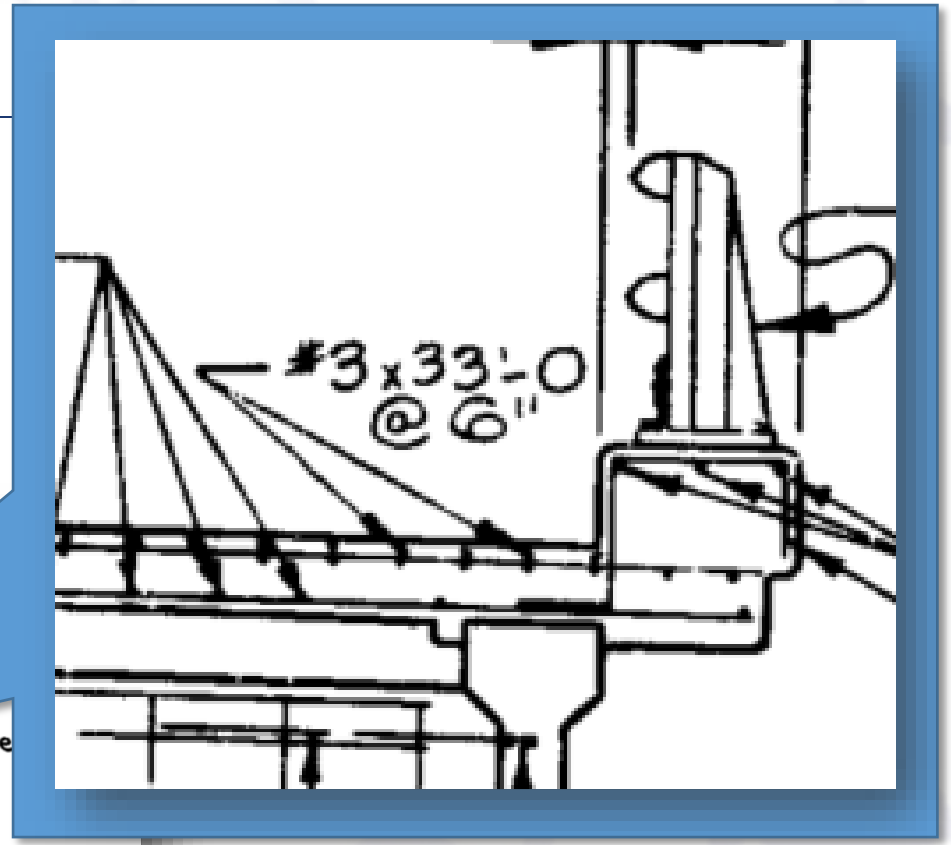
MORE TO COME ON THIS…

Beware of the #3's

- Additional Longitudinal Top Mat
- Short Window during the 1960's





Beware of the #3’s

- Potential Delays
- Costly Change Orders

- Bring it up!
  - BAE’s
  - STG1

MOT: Where the rubber (boots) meets the road.

MENU

Indiana Department of Labor

Search DOL

DOL / IOSHA / WORKER SAFETY INITIATIVES / WORK ZONE SAFETY

Work Zone Safety

TRANSPORTATION-RELATED INCIDENTS CONTINUE TO BE THE SINGLE LEADING CAUSE OF DEATH FOR WORKS IN THE STATE OF INDIANA.

fact, transportation-related fatalities are the most common cause of death for workers across the
y. Nationally, three people are killed every day in a work zone. The Indiana Department of

Source: https://www.in.gov/dol/iosha/worker-safety-initiatives/work-zone-safety/

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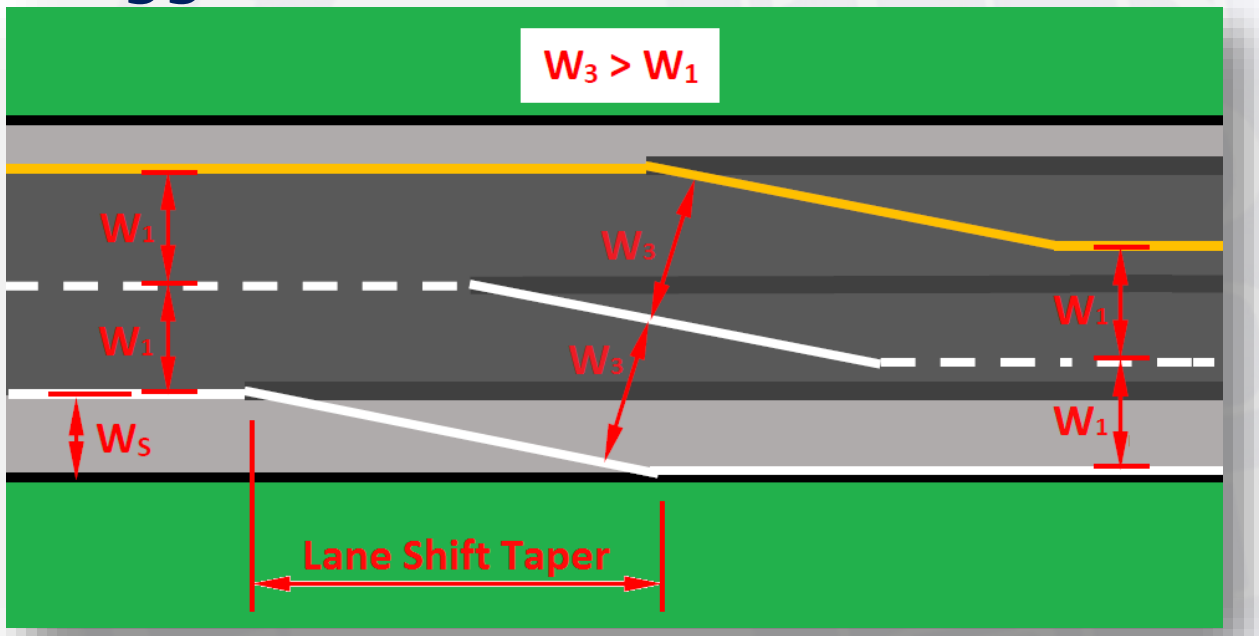
MOT: Phases for Temporary Pavement

- Need Phase for MOT Pvt
  - No Shoulder Work Adjacent to Live Traffic
  - Must Shift Lanes
- Note a Standard Drawing
  - Only if Appropriate!
- Detail Layout
  - Provide Width for Barrels/Barrier
  - Protect Drop-Offs
  - IHCP Impacts

Source: https://www.agpeltz.com/rcc-projects/285-shoulder-replacement/

MOT: Best Practices (Design Memo 21-05)

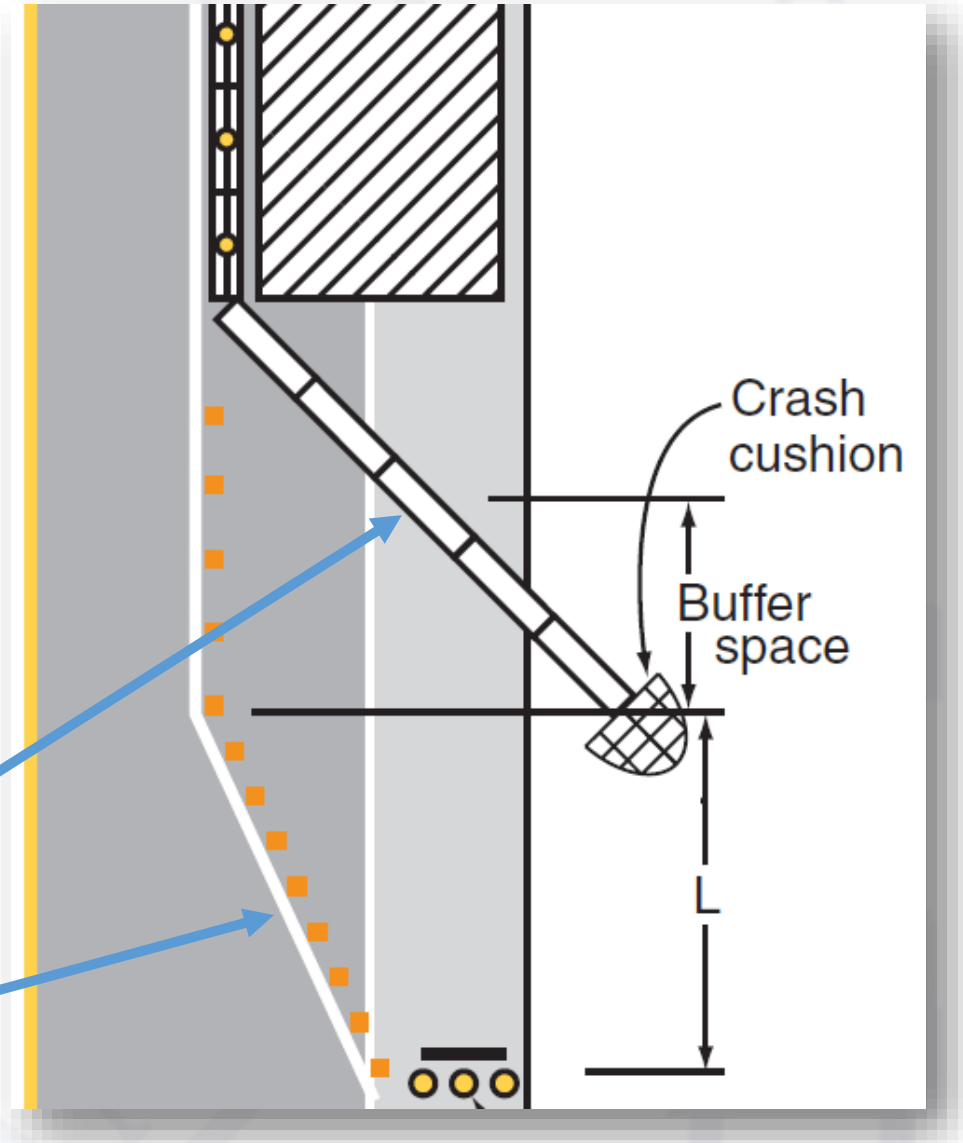
Staggered Shifts



Delineation vs. Protection

Temporary Traffic Barrier = Protection

Lines and Barrels = Delineation



MOT: Protecting TTB Ends

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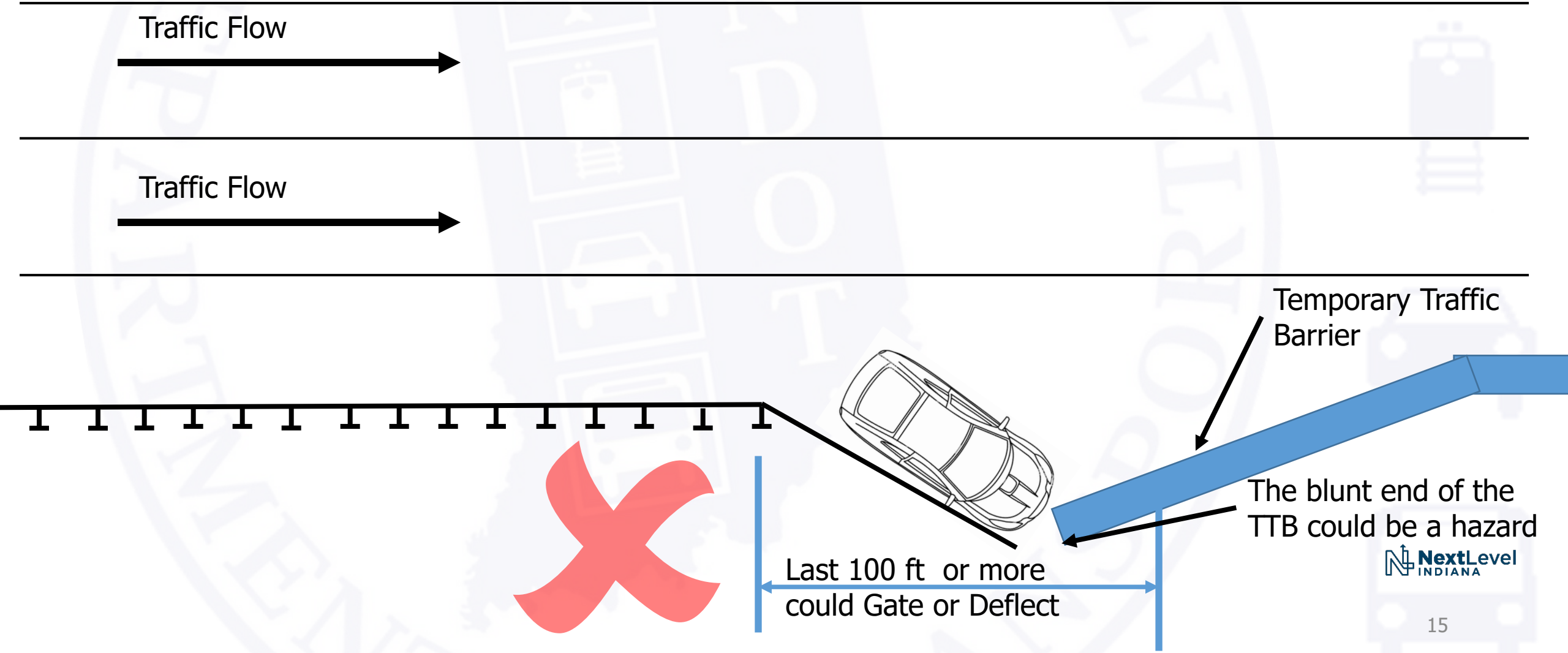
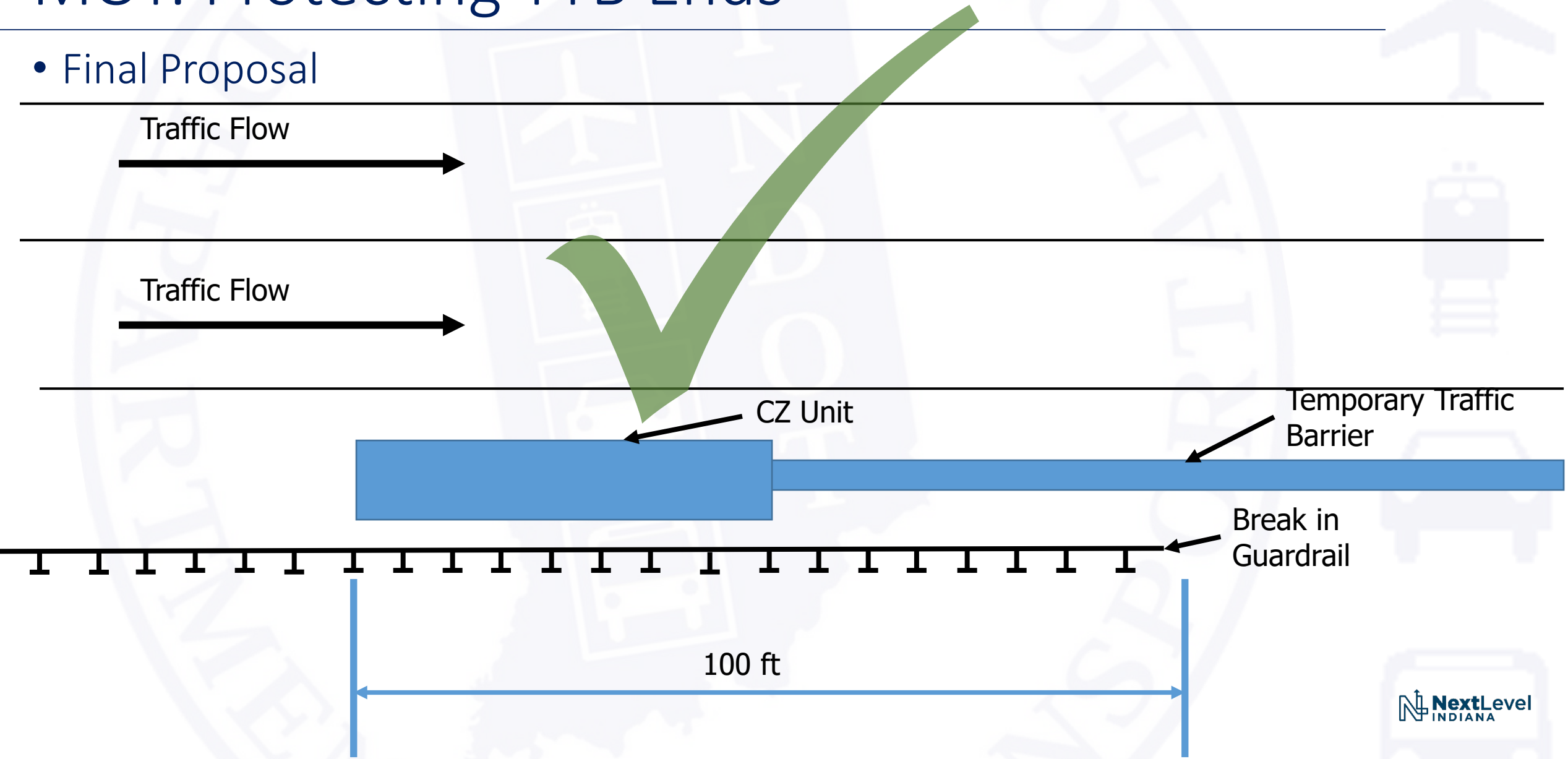
- Possible Gating or Deflection of W-Beam Guardrail Exposing the Blunt End or the TTB



MOT: Protecting TTB Ends

- Final Proposal



General Questions

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