

The physical disturbance of the stream and riparian vegetation, especially large trees overhanging any affected water bodies should be limited to only that which is absolutely necessary to complete the project. The shade provided by the large overhanging trees helps maintain proper stream temperatures and dissolved oxygen for aquatic life.

6. For projects involving construction activity (which includes clearing, grading, excavation and other land disturbing activities) that result in the disturbance of one (1), or more, acres of total land area, contact the Office of Water Quality – Watershed Planning Branch (317/233-1864) regarding the need for of a Rule 5 Storm Water Runoff Permit. Visit the following Web page
 - <http://www.in.gov/idem/4902.htm>

To obtain, and operate under, a Rule 5 permit you will first need to develop a Construction Plan (<http://www.in.gov/idem/4917.htm#constreq>), and as described in 327 IAC 15-5-6.5 (<http://www.in.gov/legislative/iac/T03270/A00150> [PDF], pages 16 through 19). Before you may apply for a Rule 5 Permit, or begin construction, you must submit your Construction Plan to your county Soil and Water Conservation District (SWCD) (<http://www.in.gov/isda/soil/contacts/map.html>).

Upon receipt of the construction plan, personnel of the SWCD or the Indiana Department of Environmental Management will review the plan to determine if it meets the requirements of 327 IAC 15-5. Plans that are deemed deficient will require re-submittal. If the plan is sufficient you will be notified and instructed to submit the verification to IDEM as part of the Rule 5 Notice of Intent (NOI) submittal. Once construction begins, staff of the SWCD or Indiana Department of Environmental Management will perform inspections of activities at the site for compliance with the regulation.

Please be mindful that approximately 149 Municipal Separate Storm Sewer System (MS4) areas are now being established by various local governmental entities throughout the state as part of the implementation of Phase II federal storm water requirements. All of these MS4 areas will eventually take responsibility for Construction Plan review, inspection, and enforcement. As these MS4 areas obtain program approval from IDEM, they will be added to a list of MS4 areas posted on the IDEM Website at: <http://www.in.gov/idem/4900.htm>.

If your project is located in an IDEM-approved MS4 area, please contact the local MS4 program about meeting their storm water requirements. Once the MS4 approves the plan, the NOI can be submitted to IDEM.

Regardless of the size of your project, or which agency you work with to meet storm water requirements, IDEM recommends that appropriate structures and techniques be utilized both during the construction phase, and after completion of the project, to minimize the impacts associated with storm water runoff. The use of appropriate planning and site development and appropriate storm water quality measures are recommended to prevent soil from leaving the construction site during active land disturbance and for post construction water quality concerns. Information and assistance regarding storm water related to construction activities are available from the Soil and Water Conservation District (SWCD) offices in each county or from IDEM.

7. For projects involving impacts to fish and botanical resources, contact the Department of Natural Resources - Division of Fish and Wildlife (317/232-4080) for addition project input.

8. For projects involving water main construction, water main extensions, and new public water supplies, contact the Office of Water Quality - Drinking Water Branch (317-308-3299) regarding the need for permits.
9. For projects involving effluent discharges to waters of the State of Indiana, contact the Office of Water Quality - Permits Branch (317-233-0468) regarding the need for a National Pollutant Discharge Elimination System (NPDES) permit.
10. For projects involving the construction of wastewater facilities and sewer lines, contact the Office of Water Quality - Permits Branch (317-232-8675) regarding the need for permits.

AIR QUALITY

The above-noted project should be designed to minimize any impact on ambient air quality in, or near, the project area. The project must comply with all federal and state air pollution regulations. Consideration should be given to the following:

1. Regarding open burning, and disposing of organic debris generated by land clearing activities; some types of open burning are allowed (<http://www.in.gov/idem/4148.htm>) under specific conditions. You also can seek an open burning variance from IDEM.

However, IDEM generally recommends that you take vegetative wastes to a registered yard waste composting facility or that the waste be chipped or shredded with composting on site (you must register with IDEM if more than 2,000 pounds is to be composted; contact 317/232-0066). The finished compost can then be used as a mulch or soil amendment. You also may bury any vegetative wastes (such as leaves, twigs, branches, limbs, tree trunks and stumps) onsite, although burying large quantities of such material can lead to subsidence problems, later on.

Reasonable precautions must be taken to minimize fugitive dust emissions from construction and demolition activities. For example, wetting the area with water, constructing wind barriers, or treating dusty areas with chemical stabilizers (such as calcium chloride or several other commercial products). Dirt tracked onto paved roads from unpaved areas should be minimized.

Additionally, if construction or demolition is conducted in a wooded area where blackbirds have roosted or abandoned buildings or building sections in which pigeons or bats have roosted for 3-5 years precautionary measures should be taken to avoid an outbreak of histoplasmosis. This disease is caused by the fungus *Histoplasma capsulatum*, which stems from bird or bat droppings that have accumulated in one area for 3-5 years. The spores from this fungus become airborne when the area is disturbed and can cause infections over an entire community downwind of the site. The area should be wetted down prior to cleanup or demolition of the project site. For more detailed information on histoplasmosis prevention and control, please contact the Acute Disease Control Division of the Indiana State Department of Health at (317) 233-7272.

2. The U.S. EPA and the Surgeon General recommend that people not have long-term exposure to radon at levels above 4 pCi/L. (For a county-by-county map of predicted radon levels in Indiana, visit: <http://www.in.gov/idem/4145.htm>.)

The U.S. EPA further recommends that all homes (and apartments within three stories of ground level) be tested for radon. If in-home radon levels are determined to be 4 pCi/L, or higher, EPA recommends a follow-up test. If the second test confirms that radon levels are 4 pCi/L, or higher,

EPA recommends the installation of radon-reduction measures. (For a list of qualified radon testers and radon mitigation (or reduction) specialists visit: http://www.in.gov/isdh/regsvcs/radhealth/pdfs/radon_testers_mitigators_list.pdf.) It also is recommended that radon reduction measures be built into all new homes, particularly in areas like Indiana that have moderate to high predicted radon levels.

To learn more about radon, radon risks, and ways to reduce exposure visit: <http://www.in.gov/isdh/regsvcs/radhealth/radon.htm>, <http://www.in.gov/idem/4145.htm>, or <http://www.epa.gov/radon/index.html>.

3. With respect to asbestos removal: all facilities slated for renovation or demolition (except residential buildings that have (4) four or fewer dwelling units and which will not be used for commercial purposes) must be inspected by an Indiana-licensed asbestos inspector prior to the commencement of any renovation or demolition activities. If regulated asbestos-containing material (RACM) that may become airborne is found, any subsequent demolition, renovation, or asbestos removal activities must be performed in accordance with the proper notification and emission control requirements.

If no asbestos is found where a renovation activity will occur, or if the renovation involves removal of less than 260 linear feet of RACM off of pipes, less than 160 square feet of RACM of other facility components, or less than 35 cubic feet of RACM off of all facility components, the owner or operator of the project does not need to notify IDEM before beginning the renovation activity.

For questions on asbestos demolition and renovation activities, you can also call IDEM's Lead/Asbestos section at 1-888-574-8150.

However, in all cases where a demolition activity will occur (even if no asbestos is found), the owner or operator must still notify IDEM 10 working days prior to the demolition, using the form found at <http://www.in.gov/icpr/webfile/formsdiv/44593.pdf>.

Anyone submitting a renovation/demolition notification form will be billed a notification fee based upon the amount of friable asbestos containing material to be removed or demolished. Projects that involve the removal of more than 2,600 linear feet of friable asbestos containing materials on pipes, or 1,600 square feet or 400 cubic feet of friable asbestos containing material on other facility components, will be billed a fee of \$150 per project; projects below these amounts will be billed a fee of \$50 per project. All notification remitters will be billed on a quarterly basis.

For more information about IDEM policy regarding asbestos removal and disposal, visit: <http://www.in.gov/idem/4983.htm>.

4. With respect to lead-based paint removal: IDEM encourages all efforts to minimize human exposure to lead-based paint chips and dust. IDEM is particularly concerned that young children exposed to lead can suffer from learning disabilities. Although lead-based paint abatement efforts are not mandatory, any abatement that is conducted within housing built before January 1, 1978, or a child-occupied facility is required to comply with all lead-based paint work practice standards, licensing and notification requirements. For more information about lead-based paint removal visit: <http://www.in.gov/isdh/19131.htm>.

5. Ensure that asphalt paving plants are permitted and operate properly. The use of cutback asphalt, or asphalt emulsion containing more than seven percent (7%) oil distillate, is prohibited during the months April through October. See 326 IAC 8-5-2 , Asphalt Paving Rule (<http://www.ai.org/legislative/iac/T03260/A00080.PDF>).
6. If your project involves the construction of a new source of air emissions or the modification of an existing source of air emissions or air pollution control equipment, it will need to be reviewed by the IDEM Office of Air Quality (OAQ). A registration or permit may be required under 326 IAC 2 (View at: www.ai.org/legislative/iac/t03260/a00020.pdf.) New sources that use or emit hazardous air pollutants may be subject to Section 112 of the Clean Air Act and corresponding state air regulations governing hazardous air pollutants.
7. For more information on air permits visit: <http://www.in.gov/idem/4223.htm>, or to initiate the IDEM air permitting process, please contact the Office of Air Quality Permit Reviewer of the Day at (317) 233-0178 or OAMPROD atdem.state.in.us.

LAND QUALITY

In order to maintain compliance with all applicable laws regarding contamination and/or proper waste disposal, IDEM recommends that:

1. If the site is found to contain any areas used to dispose of solid or hazardous waste, you need to contact the Office of Land Quality (OLQ) at 317-308-3103.
2. All solid wastes generated by the project, or removed from the project site, need to be taken to a properly permitted solid waste processing or disposal facility. For more information, visit <http://www.in.gov/idem/4998.htm>.
3. If any contaminated soils are discovered during this project, they may be subject to disposal as hazardous waste. Please contact the OLQ at 317-308-3103 to obtain information on proper disposal procedures.
4. If PCBs are found at this site, please contact the Industrial Waste Section of OLQ at 317-308-3103 for information regarding management of any PCB wastes from this site.
5. If there are any asbestos disposal issues related to this site, please contact the Industrial Waste Section of OLQ at 317-308-3103 for information regarding the management of asbestos wastes (Asbestos removal is addressed above, under Air Quality).
6. If the project involves the installation or removal of an underground storage tank, or involves contamination from an underground storage tank, you must contact the IDEM Underground Storage Tank program at 317/308-3039. See: <http://www.in.gov/idem/4999.htm>.

FINAL REMARKS

Should you need to obtain any environmental permits in association with this proposed project, please be mindful that IC 13-15-8 requires that you notify all adjoining property owners and/or occupants within ten days your submittal of each permit application. However, if you are seeking multiple permits you can still meet the notification requirement with a single notice if all required permit applications are submitted with the same ten day period.

Should the scope of the proposed project be expanded to the extent that a National Environmental Policy Act Environmental Assessment (EA) or Environmental Impact Statement (EIS) is required, IDEM will actively participate in any early interagency coordination review of the project.

Meanwhile, please note that this letter does not constitute a permit, license, endorsement or any other form of approval on the part of the Indiana Department of Environmental Management regarding any project for which a copy of this letter is used. Also note that it is the responsibility of the project engineer or consultant using this letter to ensure that the most current draft of this document, which is located at <http://www.in.gov/idem/5284.htm>, is used.

Sincerely,



Thomas W. Easterly
Commissioner

Signature(s) of the Applicant

I acknowledge that the following proposed roadway project will be financed in part, or in whole, by public monies.

Project Description

The Indiana Department of Transportation (INDOT) is planning an I-69 Interstate Expansion from 106th Street in Fishers to Exit 226 (SR 9 & 109 in Anderson), in Hamilton and Madison Counties. This expansion has been broken into multiple projects with independent utility and logical termini. Environmental analysis is being conducted for Project 1 (Des. No. 1383332), from 106th Street to 0.5 mi N of Campus Parkway, and Project 3 (Des. No. 1383336), from 0.5 mi N of Campus Parkway to 0.5 mi East of SR 13. This letter is part of the early coordination phase of the environmental review process. We are requesting comments from your area of expertise regarding any possible environmental effects associated with these projects. Please use the above designation numbers and descriptions in your reply. We will incorporate your comments into a study of the projects' environmental impacts. Purpose and Need: The need for these projects stems from traffic congestion issues that currently exist on these segments of I-69. Traffic data was analyzed using Highway Capacity Manual methodology in Highway Capacity Software (HCS). The data was collected by INDOT in 2011, and a 1.5% per year growth rate was applied to forecast the traffic for 2013 ("current year") and 2033 ("design year"). The adjusted and balanced data was then used to produce results in Level of Service (LOS). LOS is a rating for traffic congestion with LOS A being the least delay and LOS F being the most delay. I-69 between Exit 205 and SR 38 is currently operating at LOS E, which is characterized as "unstable flow". In 2033, I-69 from Exit 205 to SR 13 is predicted to experience "forced flow" (LOS F). This is likely to appear in the form of queuing upstream of ramp junctions (southbound at SR 13 in the AM peak hours and northbound at Exit 210 in the PM peak hours). I-69 is considered to be urban to Exit 210 from the south and rural from Exit 210 to the north, which means the minimally acceptable LOS's are D and C, respectively. The results show unacceptable LOS for both existing and future traffic in each direction for this section of I-69. The purpose of these projects is to improve overall traffic operation by reducing congestion on this

segment of I-69. Existing Conditions: The existing cross section of I-69 from Exit 205 to 0.5 mi E of SR 13 has 2 travel lanes in each direction. The northbound cross section of 3 lanes in each direction ends at Cumberland Rd. The southbound 3-lane section starts with the southbound SR 37 entrance ramps. A pavement resurfacing project (Des. No. 0900053) has recently been completed for this segment of I-69. The pavement condition in this area will be determined by INDOT Pavement Design and the ultimate decision on the level of pavement work required for the project will depend on the condition of the pavement. Proposed Projects: Project 1: I-69 from 106th Street to 0.5 mile north of Campus Parkway, Hamilton County The project would construct additional lanes from Exit 205 (116th Street and SR 37 in Fishers) to Exit 210 (Campus Parkway) in the form of median travel lanes. An outside auxiliary lane would be added on southbound I-69 from 106th Street to 116th Street. Existing pavement would be resurfaced. The cross section would have a 10-foot paved inside shoulder and a 10-foot paved outside shoulder. Double-sided guardrail would be installed. All mainline bridges would be widened in the median. There would be work on overhead structure at Cumberland Road. The structure at Brooks School Road over I-69 would have the bridge deck replaced. The overhead structure at 126th St would require no additional work. The interchange at Exit 210 would be modified as part of a separate project (Project 2). All small structures will be evaluated to determine if rehabilitation or replacement is necessary. Detention would likely be required at all legal drains. All detention basins would be constructed within existing right-of-way. Project 3: I-69 from 0.5 mile north of Campus Parkway to 0.5 mile east of SR 13, Hamilton and Madison Counties The project would construct additional lanes from Exit 210 to SR 13 in the form of median travel lanes. Existing pavement would be resurfaced. The cross section would have a 10-foot paved inside shoulder and a 10-foot paved outside shoulder. Double-sided guardrail would be installed in most areas, though not in wide median areas. All mainline bridges would be widened in the median. The overhead structures at Olio Road and Cyntheanne Road would require no additional work. The pavement on SR 13 under I-69 would be lowered to provide adequate bridge clearance. All small structures will be evaluated to determine if rehabilitation or replacement is necessary. Detention would likely be required at all legal drains within Hamilton County. Detention is not expected to be required in Madison County. All detention basins would be constructed within existing right-of-way. Right-of-Way (ROW): No new ROW would be required for either project. Environmental Concerns: Four U.S. Geological Survey (USGS) blue-lined streams (Cheeney Creek, Sand Creek, Mud Creek, and Thorpe Creek) lie within or adjacent to the project areas. Information from the National Wetland Inventory (NWI) map shows seven NWI points and thirty-six NWI-wetland polygons within a half-mile radius of the project areas; however, all are located outside of the projects limits. Three NWI line segments lie within the project area (along Sand Creek, Mud Creek, and Thorpe Creek). Several lakes lie adjacent to the projects limits. However, no lakes are expected to be impacted by the proposed projects. Four floodplains (Cheeney Creek, Sand Creek, Mud Creek, and Thorpe Creek) lie within a half-mile radius of the project areas. The Cheeney Creek Floodplain lies outside of the project areas and will not be impacted by the proposed projects. The other 3 floodplains lie within the project areas. See the attached Water Resources Map, Attachment A-5, for the NWI and FEMA layers. According to the Soil Survey Geographic (SSURGO) Database for Hamilton and Madison Counties, Indiana, majority of the project areas lie within nationally listed hydric soils (see Soils Map, Attachment A-8). Project 1 is located along an urbanized section of I-69, with land use within vicinity of the project consisting primarily of residential and commercial properties. Project 3 is located along a more rural section of I-69, with land use within vicinity of the project consisting primarily of agricultural properties. Four religious facilities, thirteen recreational facilities, two hospitals, and seven schools lie within a half-mile radius of the projects, but outside of the projects limits. Waters investigations, including wetland delineations, were conducted from May through July, 2014 by Parsons environmental staff to evaluate possible environmental impacts within the project areas. Coordination is ongoing with the U.S. Army Corps of Engineers and the Indiana Department of Environmental Management (IDEM). A waters report will be completed, and all applicable permits will be applied for and acquired before

construction can begin. See Attachment B for Project Area Photographs. These projects are Type I projects, and therefore Noise Analyses are currently being conducted to determine traffic noise levels, potential noise impacts, and the feasibility of traffic noise mitigation. If any facilities are determined to have traffic noise impacts, noise abatement measures will be considered and appropriate measures constructed to mitigate for these impacts. An Air Quality Analysis is currently being conducted as well. The results of this analysis will be included in the environmental document prepared for these projects.

With my signature, I do hereby affirm that I have read the letter from the Indiana Department of Environment that appears directly above. In addition, I understand that in order to complete that project in which I am interested, with a minimum of impact to the environment, I must consider all the issues addressed in the aforementioned letter, and further, that I must obtain any required permits.

Date: SEPT. 5, 2014

Signature of the INDOT
Project Engineer or Other Responsible Agent 
Tony Jones

Date: September 5, 2014

Signature of the
For Hire Consultant 
Daniel J. Miller

**Questionnaire for the Indiana Department of Transportation,
Office of Aviation**

Des/Bridge No: 1383332 & 1383336

Project Description:

I 69 Interstate Expansion Project 1 Added travel lanes from
106th Street Hamilton, Madison County, Indiana

Requested By:

Parsons

Are there any existing or proposed airports within or near the project limits? YES

If yes, describe any potential conflicts with air traffic during or after the construction of the project.

The Indianapolis Metropolitan Airport is located 9,400' North of the
project. If any permanent structures or equipment utilized for
the project penetrates the 100:1 slope from the airport FAA
Form 7460 (Notice of Proposed construction or alteration) must
be filed. For assistance contact Marcus Dial, INDOT Office of
Aviation, 317-232-1494.

This information was furnished by:

Name: James W. Kinder
Title: Chief Airport Inspector – INDOT Office of Aviation
Date: September 10, 2014



20 Years of Making Your Project, Our Priority

September 10, 2014

Mr. Daniel J. Miller
Senior Environmental Planner
Parsons
101 West Ohio Street, Suite 2121
Indianapolis, Indiana 46204

RE: Environmental Analysis
Design Nos.: 1383332 and 1383336
I-69 Interstate Expansion
Projects 1 and 3, Hamilton and Madison Counties, Indiana

Dear Mr. Miller:

This correspondence is in response to your request for comment on environmental impacts associated with the Design Nos. 1383332 and 1383336 in Hamilton and Madison Counties, Indiana. We represent the Madison County Drainage Board in dealing with the projects and their impacts to the regulated drains in Madison County. We met with Parsons, in regard to these projects, on April 3, 2014 and followed up with a letter, dated April 22, 2014, to Ms. Tina Murphy, PE, copy attached.

It should be noted that the Martha A. Ford Regulated Drain will be impacted by the proposed project and that nothing has changed in regard to our comments and opinions documented in the referenced letter. Please consider this correspondence, with the attachment, as the response to your comment request letter.

Please advise if you have questions relating to this correspondence or the attachment.

Regards,

A handwritten signature in blue ink, appearing to read "Jeff Healy".

Jeff Healy, PE
Vice president

cc: Madison County Drainage Board
Patrick Manship, Madison County Surveyor
Kent Ward, Hamilton County Surveyor
Madison County Soil and Water Conservation District



20 Years of Making Your Project, Our Priority

April 22, 2014

Ms. Tina Murphy, PE
Parsons
101 West Ohio Street, Suite 2121
Indianapolis, Indiana 46204

RE: I-69 Expansion Design-Build Projects
Madison County, Indiana

Dear Ms. Murphy:

We are writing to follow up on several items that we discussed during our meeting of April 3, 2014 and that I subsequently discussed with the Madison County Drainage Board on April 9, 2014. We have also discussed coordination of this project with the Hamilton County Surveyor's office and staff that are leading this effort for Hamilton County.

1. The Madison County Drainage Board is willing to work with your schedule and contract format regarding project review and approval. They are willing to provide a Preliminary Approval on your Hydraulics, Hydrology and Thirty Percent Plans so that you have some assurance on the acceptability of the design prior to the bidding process. A Final Approval submittal and review would be expected for the completed plans and specifications from the Design-Build contractor.
2. The submitted plans, computations and hydraulic models should be submitted to Banning Engineering (attn. Jeff Healy, PE) with a carbon copy of the transmittal to the Madison County Drainage Board.
3. The erosion and sediment control for the construction site (Rule 5) will be coordinated through the Madison County Soil and Water Conservation District. The Post-construction stormwater quality measures, practices and operation and maintenance methods and plans should be submitted to Banning Engineering (attn. Jeff Healy, PE) with a carbon copy of the transmittal to the Madison County Drainage Board.
4. The Madison County Drainage Ordinance (DRAFT – An Ordinance Pertaining to Erosion and Sediment Control; Storm Water Quality and Drainage; Regulated Drain Crossings; and Pond Construction, current revision 2-7-2014) and the Madison County Stormwater Technical Standards Manual (DRAFT) will be followed in so much as they are applicable. In other words, stormwater detention and treatment will be expected for the newly developed / expanded areas. Thorp Creek drains immediately into Hamilton County. The Madison County Surveyor has expressed concern that no additional peak discharge be directed into that conveyance.
5. There are no known flooding problems at this interchange although there is development proposed for the southwest quadrant of the interchange. This entire area is very flat and poorly drained. Great care should be taken to evaluate and accommodate right-of-way drainage as well as outlets from adjoining areas on all quadrants of the interchange.
6. It should be noted that the Martha A. Ford regulated drain was maintained by excavation from I-69 to the Madison County line during the Fall 2013. A concern was expressed by the Madison County Surveyor

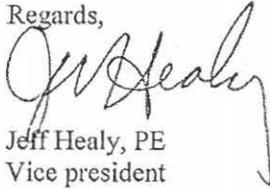
*Banning Engineering, P.C. • 853 Columbia Road, Suite 101 • Plainfield, IN 46168
Phone: (317) 707-3700 • Fax: (317) 707-3800 • E-mail: banning@BanningEngineering.com*

that the flow conveyance area through the existing right-of-way appears to be partly obstructed and in need of maintenance. This should be factored into the overall project expectations.

7. The Madison County Drainage Board is interested in establishing written expectations and understandings for and from the perspectives of both INDOT and the Drainage Board. This pertains primarily to operation and maintenance of the regulated drain once the project is completed.

Thank you for your early coordination efforts on this project. We will make every effort to be thorough, responsive and timely in our efforts to represent the interests and expectations of the Madison County Drainage Board.

Regards,



Jeff Healy, PE
Vice president

cc: Madison County Drainage Board c/o Anjie Cox
Patrick Manship, Madison County Surveyor
Kent Ward, Hamilton County Surveyor

Appendix E: Section 106

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**FEDERAL HIGHWAY ADMINISTRATION'S
SECTION 4(F) COMPLIANCE REQUIREMENTS (FOR HISTORIC PROPERTIES) AND
SECTION 106 FINDINGS AND DETERMINATIONS
AREA OF POTENTIAL EFFECTS
ELIGIBILITY DETERMINATIONS
EFFECT FINDING
I-69 ADDED TRAVEL LANES FROM 106TH STREET TO STATE ROAD 13
DELAWARE, FALL CREEK, AND WAYNE TOWNSHIPS, HAMILTON COUNTY
& GREEN TOWNSHIP, MADISON COUNTY, INDIANA
Des. Nos.: 1383332 & 1383336**

**AREA OF POTENTIAL EFFECTS
(Pursuant to 36 CFR § 800.4(a)(1))**

The Area of Potential Effects (APE) for this undertaking incorporates the project location and includes properties that may be impacted by project activities, such as noise and visual intrusions. Weintraut & Associates (W&A) initially drew an APE approximately 1,000 feet from the edge of the project location to take into account any potential for noise impacts. The APE was expanded at intersections and overpasses and also to the east where topography did not shield views to I-69 or in areas where detour may occur.

The APE for archaeological resources was defined as the project footprint. (See Appendix A: Plans and Appendix B: Maps.)

ELIGIBILITY DETERMINATIONS

(Pursuant to 36 CFR § 800.4(c)(2)). Four properties have been found to be eligible for the National Register of Historic Places (NRHP) as a result of this consultation.

Hamilton County Bridge No. 177 (S2-3)

Hamilton County Bridge No. 177 is a reinforced concrete girder bridge identified as a T-beam sub-type. The bridge was constructed circa 1912 and carries an abandoned portion of Prairie Baptist Road over Mud Creek. Hamilton County Bridge No. 177 is eligible for listing in the NRHP under Criterion C as an example of a concrete girder bridge that displays important, unusual, or highly important special features, and the period of significance is the date of construction.

Center School (095-343-65015)

The Center School, constructed in 1929, retains a high level of integrity and conveys the history of public education in Green Township and the state of Indiana. The Center School conveys its original use and meets the eligibility requirements established by the Indiana Public Common & High Schools Multiple Property Listing for the NRHP under Criteria A and C. The period of significance is 1929 to 1967.

Fishers Methodist Episcopal Church (057-206-51006)

Built around 1905, this frame church building sits on a concrete foundation. The steep cross-gable roof of this simple Gothic Revival-style edifice features a tower at its northeast corner and multiple additions to the rear. A large handicapped ramp marks the façade. This property is eligible under Criterion C, and its period of significance is circa 1905.

House at 7883 South State Road (SR) 13 (S2-8)

Built in the mid-twentieth century, this Contemporary-style dwelling sits on a concrete slab and is topped by a very low-pitched, gable-front roof with wide overhanging boxed eaves. This property is eligible under Criterion C, and its period of significance is circa 1950.

EFFECT FINDING

Hamilton County Bridge No. 177 – No Effect

Center School – No Adverse Effect

Fishers Methodist Episcopal Church – No Effect

House at 7883 SR 13 – No Adverse Effect

The Indiana Department of Transportation (INDOT), acting on behalf of the Federal Highway Administration (FHWA), has determined a finding of "No Adverse Effect" is appropriate for this undertaking. INDOT, acting on FHWA's behalf, respectfully requests the Indiana State Historic Preservation Officer provide written concurrence with the Section 106 determination of effect for each property and the project's overall effect finding of "Historic Properties Affected: No Adverse Effect."

SECTION 4(F) COMPLIANCE REQUIREMENTS (for historic properties)

Hamilton County Bridge No. 177 (S2-3) – This undertaking will not convert property from the Hamilton County Bridge No. 177, a Section 4(f) historic property, to a transportation use; INDOT, acting on FHWA's behalf, has determined the appropriate Section 106 finding is "No Effect;" therefore, no Section 4(f) evaluation is required for the Hamilton County Bridge No. 177.

Center School (095-343-65015) – This undertaking will not convert property from Center School, a Section 4(f) historic property, to a transportation use; INDOT, acting on FHWA's behalf, has determined the appropriate Section 106 finding is "No Adverse Effect;" therefore, no Section 4(f) evaluation is required for Center School.

Fishers Methodist Episcopal Church (057-206-51006) – This undertaking will not convert property from the Fishers Methodist Episcopal Church, a Section 4(f) historic property, to a transportation use; INDOT, acting on FHWA's behalf, has determined the appropriate Section 106 finding is "No Effect;" therefore, no Section 4(f) evaluation is required for the Fishers Methodist Episcopal Church.

House at 7883 South SR 13 (S2-8) – This undertaking will not convert property from the House at 7883 South SR 13, a Section 4(f) historic property, to a transportation use; INDOT, acting on FHWA's behalf, has determined the appropriate Section 106 finding is "No Adverse Effect;" therefore, no Section 4(f) evaluation is required for the House at 7883 South SR 13.



Patrick Carpenter for FHWA
Manager, INDOT Cultural Resources

10/30/2014

Approved Date

**FEDERAL HIGHWAY ADMINISTRATION
DOCUMENTATION OF SECTION 106 FINDING OF
NO ADVERSE EFFECT
SUBMITTED TO THE STATE HISTORIC PRESERVATION OFFICER
PURSUANT TO 36 CFR 800.5(c)
I-69 ADDED TRAVEL LANES FROM 106th STREET TO STATE ROAD 13
DELAWARE, FALL CREEK, AND WAYNE TOWNSHIPS, HAMILTON COUNTY & GREEN
TOWNSHIP, MADISON COUNTY, INDIANA
DES NOS.: 1383332 and 1383336**

1. DESCRIPTION OF THE UNDERTAKING

The Federal Highway Administration (FHWA) and the Indiana Department of Transportation (INDOT) are planning an I-69 Interstate Expansion from 106th Street in Fishers to Exit 226 [State Roads (SR) 9 & 109 in Anderson], in Hamilton and Madison counties. This expansion has been broken into multiple projects with independent utility and logical termini. This documentation of Section 106 finding of “No Adverse Effect” has been prepared for Project 1 (Des. No.: 1383332), from 106th Street to 0.5 mile (mi) north of Campus Parkway, and Project 3 (Des. No.: 1383336), from 0.5 mi north of Campus Parkway to 0.5 mi east of SR 13. The need for these projects stems from traffic congestion issues that currently exist on these segments of I-69. The purpose of these projects is to improve overall traffic operation by reducing congestion on this segment of I-69.

The existing cross section of I-69 from Exit 205 to 0.5 mi east of SR 13 has two travel lanes in each direction. The northbound cross section of three lanes in each direction ends at Cumberland Road. The southbound three-lane section starts with the southbound SR 37 ramps. A pavement resurfacing project (Des. No.: 0900053) has recently been completed for this segment of I-69. The pavement condition in this area will be determined by INDOT Pavement Design, and the ultimate decision on the future form of the roadway will depend on the condition of the pavement.

Project 1: I-69 from 106th Street to 0.5 mi North of Campus Parkway, Hamilton County

Project 1 is a federally funded undertaking designed to improve overall traffic operation by reducing congestion in the project area. The project would construct additional lanes from Exit 205 (116th Street and SR 37 in Fishers) to Exit 210 (Campus Parkway) in the form of median travel lanes. An outside auxiliary lane would be added on Southbound (SB) I-69 from 106th Street to 116th Street. Existing pavement would be resurfaced. The cross section would have a 10-foot (ft) paved inside shoulder and a 10-ft paved outside shoulder. Double-sided guardrail would be installed. All mainline bridges would be widened in the median. There would be work on the overhead structure at Cumberland Road. The structure at Brooks School Road over I-69 would have the bridge deck replaced. The overhead structure at 126th Street would require no additional work. The interchange at Exit 210 would be modified as part of a separate project (Project 2). All small structures will be evaluated to determine if rehabilitation or replacement is necessary. Detention would likely be required at all legal drains. All detention basins would be constructed within existing right-of-way (ROW).

Project 3: I-69 from 0.5 mi North of Campus Parkway to 0.5 mi East of SR 13, Hamilton and Madison Counties.

Project 3 is a federally funded undertaking designed to improve overall traffic operation by reducing congestion in the project area. The project would construct additional lanes from Exit 210 (Campus Parkway) to SR 13 in the form of median travel lanes. Existing pavement would be resurfaced. The cross section would have a 10-ft paved inside shoulder and a 10-ft paved outside shoulder. Double-sided guardrail would be installed in most areas, though not in wide median areas. All mainline bridges would be widened in the median. The overhead structures at Olio Road and Cyntheanne Road would require no additional work. The pavement on SR 13 under I-

69 would be lowered to provide adequate bridge clearance. All small structures will be evaluated to determine if rehabilitation or replacement is necessary. Detention would likely be required at all legal drains within Hamilton County. Detention is not expected to be required in Madison County. All detention basins would be constructed within existing ROW.

No new ROW would be required for Project 1 and Project 3.

36 CFR § 800.16(d) defines the Area of Potential Effects (APE) as the “geographic area or areas within which an undertaking may directly or indirectly cause alterations in the character or use of historic properties, if any such properties exist. The area of potential effects is influenced by the scale and nature of an undertaking and may be different for different kinds of effects caused by the undertaking.”

The APE for this undertaking incorporates the project location and includes properties that may be impacted by project activities, such as noise and visual intrusions. (See Appendix A: Maps.) Weintraut & Associates (W&A) initially drew an APE approximately 1,000 feet from the edge of the project location to take into account any potential for noise impacts. The APE was expanded at intersections and overpasses and also to the east where topography did not shield views to I-69. The APE for archaeological resources was defined as the project footprint. (See Appendix A: Plans and Appendix B: Maps.)

2. EFFORTS TO IDENTIFY HISTORIC PROPERTIES

Pursuant to 36 CFR § 800.4(b), W&A identified and evaluated historic properties. W&A initiated aboveground efforts by reviewing properties listed in the National Register of Historic Places (NRHP), National Historic Landmarks (NHL) Program, Indiana Register of Historic Sites and Structures (State Register), the State Historical Architectural and Archaeological Research Database (SHAARD), the Indiana Historic Sites and Structures Inventory (IHSSI) survey cards at the Indiana Department of Natural Resources (IDNR), Division of Historic Preservation and Archaeology (DHPA), the *Hamilton County Interim Report*, the *Madison County Interim Report*, and the *Indiana Historic Bridge Inventory* for previously identified properties.

In addition, W&A reviewed topographic quadrangle maps for Fishers, Ingalls, Lapel, Riverwood, and McCordsville (1952 through 1969); aerial photographs of the APE from 1961 and 1962; and plat maps from the late nineteenth and early twentieth centuries in order to establish baseline construction dates for modern resources. Historians examined other primary and secondary resources. Documentary research for the project included county and city histories and online resources. Historians consulted staff from the Anderson Public Library and Fortville-Vernon Township Public Library and bridge engineers from Hamilton County and the City of Noblesville for information on resources within the APE. W&A consulted with INDOT-Cultural Resources Office (INDOT-CRO) and requested copies of I-69 Engineering drawings and survey from the 1960s and recent Section 106 investigations that had been conducted within the APE.

Finally, historians reviewed prior Section 106 consultation for Des. No. 1298035, which overlapped with the project area, and they consulted previous investigations conducted by W&A for information relating to the history of the area and its properties. (See Appendix G: Section 106 Documentation and Additional Information Relating to the Flanagan House.)

W&A initiated archaeological identification and evaluation by conducting a records check on SHAARD and then reviewed files at the DHPA on March 6, 2014.

Historians for W&A conducted a field survey of aboveground resources on March 18, 2014; on April 1, 2014, W&A conducted site surveys of those properties that could not be seen from the public ROW and of those properties that needed additional scrutiny. Historians photographed and recorded survey notes for all properties greater than fifty years of age within the APE. Representative views and photographs of individual properties were taken, and historians scrutinized individual properties that possessed historic and/or architectural significance carefully. In addition, they carefully considered architectural and thematic continuity of properties while in the field. (See Appendix D: Photographs for representative photographs of the APE.)

For mid-century resources, W&A applied the evaluation standards established through conversations with the staff of the DHPA (in particular, those conversations related to I-69 Evansville to Indianapolis Tier 2 Studies: Additional Information survey of recent past properties). Recent past properties must have a “high level of integrity” to be recommended Contributing and must be “almost perfect” to be recommended eligible for listing in the NRHP. This methodology aided with the evaluation of the substantial number of recent past properties within this APE.

In May 2014, W&A completed its HPR, which included a historic context by which the historians evaluated resources for eligibility. Historians identified the Flanagan House (057-206-50019) as a property that SHPO had previously believed to be eligible (Des. No.1298035) and two other properties that they recommended eligible for listing in the NRHP: Hamilton County Bridge No. 177 (S2-3) and Center School (095-343-65015). (See Appendix E: Report Summaries.) The report was submitted to INDOT-Cultural Resources Office (CRO) on June 9, 2014, and the report was approved on June 16, 2014.

On July 24, 2014, W&A sent an early coordination letter, the HPR, and an invitation to join in consultation to the following potential consulting parties: Indiana Landmarks—Central Regional Office, Hamilton County Historian, Hamilton County Historical Society, Hamilton County Genealogy Society, Carmel-Clay Historical Society, Fishers Historic Preservation Committee, Noblesville Preservation Alliance, City of Noblesville, City of Fishers, Hamilton County Commissioners, Fishers Chamber of Commerce, Noblesville Chamber of Commerce, Madison County Historian, Madison County Historical Society, Madison County Commissioners, Hancock County Historical Society, Hancock County Historian, and the Indianapolis Metropolitan Planning Organization. The State Historic Preservation Officer (SHPO), who is always a consulting party, was provided the early coordination letter and HPR and was invited to comment on the list of invited consulting parties in a letter of the same date. INDOT and FHWA were copied on the correspondence. (See Appendix C: Consulting parties and Appendix F: Correspondence.)

On August 11, 2014, Indiana Landmarks Central Office accepted the invitation to become a consulting party and suggested that Indiana Landmarks Eastern Regional Office and Visit Hamilton County Indiana be invited to join in consultation. On August 12, 2014, W&A sent an early coordination letter, HPR, and an invitation to join in consultation to Indiana Landmarks Eastern Regional Office and Visit Hamilton County Indiana. (See Appendix F: Correspondence.)

In addition to Indiana Landmarks Central Office, the following also responded affirmatively to the invitation to join consultation: Madison County Historian, Hancock County Historian, Hamilton County Government, Indiana Landmarks Eastern Regional Office, Madison County Commissioners, and Hamilton County Tourism Inc. (Visit Hamilton County Indiana). The Hamilton County Historical Society, and the Carmel-Clay Historical Society declined the invitation. (See Appendix C: Consulting Parties, for consulting party post card responses.)

On August 22, 2014, the staff of the SHPO responded to the early coordination letter and the HPR. The staff agreed that the Flanagan House, Hamilton County Bridge No. 177 and the Center

School are “eligible for inclusion” in the NRHP. The staff, however, stated that the Fishers Methodist Episcopal Church and the mid-century House at 7883 South SR 13 “are potentially eligible for listing in the [NRHP], if the interiors are intact.” SHPO added, “We understand that it may not be possible to determine the condition of the interiors of these structures; therefore, we would be willing to consider them eligible for listing for the purposes of this review.” (See Appendix F: Correspondence.)

W&A completed the Archaeology Short Report on September 3, 2014, and transmitted it to INDOT-CRO for review on September 5, 2014. The archaeological records check “determined the project area did not have the potential to contain archaeological resources” and recommended no further work and project clearance. The report was approved and sent to SHPO on September 10, 2014. (See Appendix E: Report Summaries).

On September 18, 2014, staff of the SHPO responded via email to a phone message left by staff of W&A regarding the identification and eligibility of the Fishers Methodist Episcopal Church and the House at 7883 South SR 13. SHPO stated the Fishers Methodist Episcopal church would be eligible under Criterion C (Architecture), though “[a] case may also be able to be made for Religion depending on what additional information is available in the future.” The House at 7883 South SR 13 would be eligible under Criterion C (Architecture). (See Appendix F: Correspondence.)

W&A replied to SHPO’s comments on the HPR in an email dated September 23, 2014. W&A questioned the eligibility of the Fishers Methodist Episcopal Church given alterations observed in the field and based on previous consultation with SHPO on a similar structure. W&A also questioned the eligibility of the House at 7883 South SR 13 given previous consultation with SHPO. (See Appendix F: Correspondence.)

SHPO staff responded to W&A questions in an email dated September 29, 2014, and stated that staff believe both the Fishers Methodist Episcopal Church and House at 7883 South SR 13 to be “potentially eligible pending additional information.” Regarding the church, SHPO noted the “ongoing and dramatic loss of historic resources has focused attention on a limited pool of historic places that now stand out as representative to the history of the community.” SHPO attached an essay regarding mid-twentieth century resources. (See Appendix F: Correspondence.)

SHPO replied to the Archaeology Short Report in a letter dated October 3, 2014. SHPO stated that based on the submitted report and information available to the SHPO staff, “we have not identified any currently known archaeological resources listed in or eligible for inclusion in the NRHP within the proposed project area.” SHPO stated this conclusion was “subject to project activities remaining within areas disturbed by previous construction of a recent and non-historical nature.” SHPO also stated that any archaeological deposits encountered from the post-contact period would be subject to evaluation “regarding their eligibility for the NRHP in consultation with the staff of the Indiana SHPO. Please contact our office if such deposits are encountered.” Additionally, SHPO stated that Indiana state law requires the Department of Natural Resources be notified within two business days in the event that “archaeological artifacts or human remains are uncovered during construction, demolition, or earthmoving activities.” (See Appendix F: Correspondence.)

On October 9, 2014, INDOT-CRO sent a letter to the SHPO and to the Survey & Registration leader of the DHPA advising that a private entity had moved the Flanagan House to a new location, approximately 0.25 mi north of its previous location on 106th Street, and requesting an opinion of continued eligibility. [In prior consultation (Des. No.1298035) INDOT’s consultant had recommended the Flanagan House eligible, and SHPO had concurred with that

recommendation.] In the letter of October 9, 2014, INDOT-CRO expressed the opinion that the Flanagan House (sometimes referred to as the Kincaid House) would still be eligible since its significance is derived from its architecture (Criterion C). INDOT stated: "Its new setting, very close in proximity and character to its previous setting, does not detract from the house's features that made it National Register eligible." (See Appendix F: Correspondence and Appendix G: Section 106 Documentation and Additional Information Relating to the Flanagan House for newspaper articles relating to the house's relocation.)

On October 22, 2014, the Assistant Director of Preservation Services for DHPA, Paul Diebold, responded to the request for an opinion of eligibility on the Flanagan House: "[a]fter some debate, we have reached the conclusion that the house no longer meets the National Register criteria. In particular, the siting and orientation of the house render it incapable of conveying its architectural significance." Diebold noted, "Examples of vernacular architecture like the Kincaid House [Flanagan House] convey their sense of time and place, in good measure, by their orientation." (See Appendix F: Correspondence.) Therefore, for the purposes of this project, the Flanagan House is not considered eligible for listing in the NRHP.

No further efforts, including consultation, to identify historic archaeological and aboveground resources took place.

3. DESCRIBE AFFECTED HISTORIC PROPERTIES

There are four historic properties within the APE that have been found NRHP-eligible as a result of this consultation.

Hamilton County Bridge No. 177 (S2-3)

Hamilton County Bridge No. 177 is a reinforced concrete girder bridge identified as a T-beam sub-type. The bridge was constructed circa 1912 and carries an abandoned portion of Prairie Baptist Road over Mud Creek. Hamilton County Bridge No. 177 is eligible for listing in the NRHP under Criterion C as an example of a concrete girder bridge that displays important, unusual, or highly important special features, and the period of significance is the date of construction.

Center School (095-343-65015)

The Center School, constructed in 1929, retains a high level of integrity and conveys the history of public education in Green Township and the state of Indiana. The Center School conveys its original use, and meets the eligibility requirements established by the Indiana Public Common & High Schools Multiple Property Listing for the NRHP under Criteria A and C. The period of significance is 1929 to 1967.

Fishers Methodist Episcopal Church (057-206-51006)

Built around 1905, this frame church building sits on a concrete foundation. The steep cross-gable roof of this simple Gothic Revival-style edifice features a tower at its northeast corner; it has suffered multiple additions and the installation of a rather large handicap accessible ramp. Staff of the SHPO believe this church should be considered "potentially eligible" for the purposes of Section 106, if "the interiors are intact." Therefore, based on this consultation with the SHPO, the Fishers Methodist Episcopal Church is considered eligible for listing in the NRHP for this project.

House at 7883 South SR 13 (S2-8)

Built in the mid-twentieth century, this Contemporary-style dwelling sits on a concrete slab and is topped by a very low-pitched, gable-front roof with wide overhanging boxed eaves. While consultation with the Indiana SHPO has led the historians to believe that mid-century buildings must have an extremely high level of integrity to be considered eligible for Criterion C, staff of the

SHPO believe this church should be considered “potentially eligible” for the purposes of Section 106, if “the interiors are intact.” Therefore, based on this consultation with the SHPO, the House at 7883 South SR 13 is considered eligible for listing in the NRHP for this project.

4. DESCRIBE THE UNDERTAKING'S EFFECTS ON HISTORIC PROPERTIES

Project 1: I-69 from 106th Street to 0.5 mi North of Campus Parkway, Hamilton County

Project 1 is a federally funded undertaking designed to improve overall traffic operation by reducing congestion in the project area. The project would construct additional lanes from Exit 205 (116th Street and SR 37 in Fishers) to Exit 210 (Campus Parkway) in the form of median travel lanes. An outside auxiliary lane would be added on Southbound (SB) I-69 from 106th Street to 116th Street. Existing pavement would be resurfaced. The cross section would have a 12-ft paved inside shoulder and a 10-ft paved outside shoulder. Double-sided guardrail would be installed. All mainline bridges would be widened in the median. There would be work on the overhead structure at Cumberland Road. The structure at Brooks School Road over I-69 would have the bridge deck replaced. The overhead structure at 126th Street would require no additional work. The interchange at Exit 210 would be modified as part of a separate project (Project 2). All small structures will be evaluated to determine if rehabilitation or replacement is necessary. Detention would likely be required at all legal drains. All detention basins would be constructed within existing ROW.

No new ROW would be required for Project 1.

The **Fishers Methodist Episcopal Church** is located within the APE of Project 1 at 11425 Lantern Road west of the undertaking. No ROW will be taken from the resource and all improvements will occur within the ROW of I-69. Houses and trees block views to the interstate and the property is more than 800 feet from the undertaking. There will be no effect to the Fishers Methodist Episcopal Church as a result of this undertaking.

Project 3: I-69 from 0.5 mi North of Campus Parkway to 0.5 mi East of SR 13, Hamilton and Madison Counties.

Project 3 is a federally funded undertaking designed to improve overall traffic operation by reducing congestion in the project area. The project would construct additional lanes from Exit 210 (Campus Parkway) to SR 13 in the form of median travel lanes. Existing pavement would be resurfaced. The cross section would have a 12-ft paved inside shoulder and a 10-ft paved outside shoulder. Double-sided guardrail would be installed in most areas, though not in wide median areas. All mainline bridges would be widened in the median. The overhead structure at Olio Road would require no additional work. The overhead structures at Olio Road and Cyntheanne Road would require no additional work. The pavement on SR 13 under I-69 would be lowered to provide adequate bridge clearance. All small structures will be evaluated to determine if rehabilitation or replacement is necessary. Detention would likely be required at all legal drains within Hamilton County. Detention is not expected to be required in Madison County. All detention basins would be constructed within existing ROW.

No new ROW would be required for Project 3.

Hamilton County Bridge No. 177 is located within the APE of Project 3 on an abandoned section of Prairie Baptist Road. The road presently provides access for the Burk(e) Cemetery. Since the road is abandoned, traffic will not be added. The bridge is located in proximity to an existing interstate and the setting of the bridge will not be impacted by the additional lanes within existing ROW. A noise analysis was not conducted at this location because previous consultation with the SHPO had concluded that noise or lack thereof is not an aspect of this type of property's

setting that qualifies it for inclusion in the NRHP. The bridge will not be affected as a result of the undertaking.

Center School is located within the APE of Project 3 along SR 13 at the eastern edge of the APE and has a direct view of the undertaking. Traffic and subsequent development may increase as a result as the undertaking, but since this property is near the location of an existing interstate and interchange, it is already subject to traffic and development. Therefore, the undertaking would not adversely impact the property. The Center School is more than 800 feet from the undertaking; therefore, noise impacts were not analyzed. The property will not be affected adversely as a result of this undertaking.

The **House at 7883 South SR 13** is located along SR 13 and has a direct view to the undertaking, including the interchange modifications. The pavement on SR 13 will be lowered to provide bridge clearance. This will affect the view from the property but not adversely. Traffic and subsequent development may increase as a result as the undertaking, but since this property is near the location of an existing interstate and interchange, it is already subject to traffic and development. The house is nearly 1,000 feet from the interchange; therefore the added traffic noise that comes with added travel lanes should not be an issue. The property will be affected but not adversely as a result of the undertaking.

5. EXPLAIN APPLICATION OF CRITERIA OF ADVERSE EFFECT -- INCLUDE CONDITIONS OR FUTURE ACTIONS TO AVOID, MINIMIZE OR MITIGATE ADVERSE EFFECTS

As discussed above, the APE was established to take into account potential visual and audible impacts. Noise effects upon historic properties under Section 106 of the NHPA are assessed in the following manner: Noise effects are not considered adverse simply because the undertaking would result in a change in noise; predicted noise increases are not considered effects unless there is an increase of 15 A-weighted Decibels (dBA) and the predicted noise exceeds 67 dBA using the Traffic Noise Model (TNM). FHWA has not validated the TNM for accurate results beyond 800 feet, per FHWA's "Addendum to Validation of FHWA's Traffic Noise Model® TNM: Phase 1." According to the policy, therefore, noise receptor locations that would be located more than 800 feet from the project roadway are not evaluated for highway traffic noise effects. For purposes of this project, the 800-foot distance was used as a conservative measure to capture potential impacts, unless the setting was not a key component of the property's eligibility.

Noise effects were not applicable to this project once the historic properties were identified and evaluated for one of two reasons: the distance of the historic resource from the undertaking was greater than 800 feet, or due to the fact that, in previous consultation with the SHPO, noise, or lack thereof, was not an aspect of a property's setting that qualified it for inclusion in the NRHP

36 CFR § 800.5(a)(1) states: "An adverse effect is found when an undertaking may alter, directly or indirectly, any of the characteristics of a historic property that qualify the property for inclusion in the National Register in a manner that would diminish the integrity of the property's location, design, setting, materials, workmanship, feeling, or association. Consideration shall be given to all qualifying characteristics of a historic property, including those that may have been identified subsequent to the original evaluation of the property's eligibility for the National Register. Adverse effects may include reasonably foreseeable effects caused by the undertaking that may occur later in time, be farther removed in distance or be cumulative."

Fishers Methodist Episcopal Church -The criteria of adverse effect do not apply. The Fishers Methodist Episcopal Church will not be affected by the undertaking.

The criteria of adverse effect, as defined and described in 36 CFR 800.5(a)(1) and in 36 CFR 800.5(a)(2)(i) through (v), do not apply to the Fishers Methodist Episcopal Church.

Per 36 CFR 800.5(a)(2)(i), the undertaking will cause no “physical destruction of or damage to all or part of the property.” No ROW will be taken from the resource. Construction at this location will be limited to the current INDOT ROW and will not affect the Fishers Methodist Episcopal Church.

Per 36 CFR 800.5(a)(2)(ii), there will be no “restoration, rehabilitation, repair, maintenance, stabilization, hazardous material remediation and provision of handicapped access, that is not consistent with the Secretary’s Standards for the Treatment of Historic Properties (36 CFR part 68) and applicable guidelines.”

Per 36 CFR 800.5(a)(2)(iii), the property will not be removed from its historic location.

Per 36 CFR 800.5(a)(2)(iv), there will not be a change “of the character of the property’s use or of physical features within the property’s setting.” Project improvements will take place within the current roadway profile, which is located within the median of I-69. Intervening trees and buildings obstruct the view to the interstate and the setting of property will not be impacted.

Per 36 CFR 800.5(a)(2)(v), there will not be an “introduction of visual, atmospheric or audible elements that diminish the integrity of the property’s significant historic features.”

Per 36 CFR 800.5(a)(2)(vi), there will be no neglect or deterioration of the property.

Per 36 CFR 800.5(a)(2)(vii), there will be no “transfer, lease, or sale of the property out of Federal ownership or control.”

Hamilton County Bridge No. 177 - The criteria of adverse effect do not apply. Hamilton County Bridge No. 177 will not be affected by the undertaking.

The criteria of adverse effect, as defined and described in 36 CFR 800.5(a)(1) and in 36 CFR 800.5(a)(2)(i) through (v), do not apply to Hamilton County Bridge No. 177.

Per 36 CFR 800.5(a)(2)(i), the undertaking will cause no “physical destruction of or damage to all or part of the property.” Construction at this location will be limited to the current roadway profile and will not affect Hamilton County Bridge No. 177.

Per 36 CFR 800.5(a)(2)(ii), there will be no “restoration, rehabilitation, repair, maintenance, stabilization, hazardous material remediation and provision of handicapped access, that is not consistent with the Secretary’s Standards for the Treatment of Historic Properties (36 CFR part 68) and applicable guidelines.”

Per 36 CFR 800.5(a)(2)(iii), the property will not be removed from its historic location.

Per 36 CFR 800.5(a)(2)(iv), there will not be a change “of the character of the property’s use or of physical features within the property’s setting.” Project improvements will take place within the current roadway profile.

Per 36 CFR 800.5(a)(2)(v), there will not be an “introduction of visual, atmospheric or audible elements that diminish the integrity of the property’s significant historic features.” Consultation with SHPO had concluded that noise or lack thereof is not an aspect of this type of property’s setting that qualifies it for inclusion in the NRHP; therefore, noise impacts were not assessed. The undertaking will occur within the existing ROW, and the visual change will not be apparent from the resource.

Per 36 CFR 800.5(a)(2)(vi), there will be no neglect or deterioration of the property.

Per 36 CFR 800.5(a)(2)(vii), there will be no “transfer, lease, or sale of the property out of Federal ownership or control.”

Center School - The criteria of adverse effect do not apply. The Center School will be affected by the undertaking, but the effects of the undertaking will not be adverse.

The criteria of adverse effect, as defined and described in 36 CFR 800.5(a)(1) and in 36 CFR 800.5(a)(2)(i) through (v), do not apply to the Center School.

Per 36 CFR 800.5(a)(2)(i), the undertaking will cause no “physical destruction of or damage to all or part of the property.” Construction at this location will be limited to the current INDOT ROW and will not affect the Center School.

Per 36 CFR 800.5(a)(2)(ii), there will be no “restoration, rehabilitation, repair, maintenance, stabilization, hazardous material remediation and provision of handicapped access, that is not consistent with the Secretary’s Standards for the Treatment of Historic Properties (36 CFR part 68) and applicable guidelines.”

Per 36 CFR 800.5(a)(2)(iii), the property will not be removed from its historic location.

Per 36 CFR 800.5(a)(2)(iv), there will not be a change “of the character of the property’s use or of physical features within the property’s setting.” Project improvements will take place within the current roadway profile.

Per 36 CFR 800.5(a)(2)(v), there will not be an “introduction of visual, atmospheric or audible elements that diminish the integrity of the property’s significant historic features.” The undertaking will occur within the current roadway profile of the mainline, and while the view to the undertaking will change, it will not be an adverse effect. SR 13 will be lowered at its intersection with I-69 and additional lanes will be added within the current interstate ROW, but this visual change will likely not be apparent from the Center School. Therefore, there will be visual changes, but these changes will not diminish the architectural quality or further diminish the educational context of the Center School.

Per 36 CFR 800.5(a)(2)(vi), there will be no neglect or deterioration of the property.

Per 36 CFR 800.5(a)(2)(vii), there will be no “transfer, lease, or sale of the property out of Federal ownership or control.”

House at 7883 South SR 13 - The criteria of adverse effect do not apply. The House at 7883 South SR 13 will be affected by the undertaking, but the effects of the undertaking will not be adverse.

The criteria of adverse effect, as defined and described in 36 CFR 800.5(a)(1) and in 36 CFR 800.5(a)(2)(i) through (v), do not apply to the House at 7883 South SR 13.

Per 36 CFR 800.5(a)(2)(i), the undertaking will cause no “physical destruction of or damage to all or part of the property.” Construction at this location will be limited to the current INDOT ROW and will not affect the House at 7883 South SR 13.

Per 36 CFR 800.5(a)(2)(ii), there will be no “restoration, rehabilitation, repair, maintenance, stabilization, hazardous material remediation and provision of handicapped access, that is not consistent with the Secretary’s Standards for the Treatment of Historic Properties (36 CFR part 68) and applicable guidelines.”

Per 36 CFR 800.5(a)(2)(iii), the property will not be removed from its historic location.

Per 36 CFR 800.5(a)(2)(iv), there will not be a change “of the character of the property’s use or of physical features within the property’s setting.” Project improvements will take place within the current roadway profile.

Per 36 CFR 800.5(a)(2)(v), there will not be an “introduction of visual, atmospheric or audible elements that diminish the integrity of the property’s significant historic features.” The undertaking will occur within the current roadway profile of the mainline and while the view to the undertaking will change, it will not be an adverse effect. SR 13 will be lowered at its intersection with I-69 and additional lanes will be added within the current interstate ROW, but this visual change will likely not be apparent from the House at 7883 South SR 13. Therefore, there will be visual changes, but these changes will not diminish the architectural quality of the House at 7883 South SR 13.

Per 36 CFR 800.5(a)(2)(vi), there will be no neglect or deterioration of the property.

Per 36 CFR 800.5(a)(2)(vii), there will be no “transfer, lease, or sale of the property out of Federal ownership or control.”

EFFORTS TO AVOID, MINIMIZE, AND MITIGATE

INDOT and its project engineers have minimized effects on historic properties by making all improvements within INDOT ROW.

6. SUMMARY OF CONSULTING PARTIES AND PUBLIC VIEWS

The following organizations responded affirmatively to the invitation to join consultation, sent by W&A on July 24, 2014 and August 12, 2014: Indiana Landmarks Central Office, Madison County Historian, Hancock County Historian, Hamilton County Government, Indiana Landmarks Eastern Regional Office, Madison County Commissioners, and Hamilton County Tourism Inc. (Visit Hamilton County Indiana). The Hamilton County Historical Society and the Carmel Clay Historical Society declined the invitation. (See Appendix C: Consulting Parties, for consulting party post card responses.) The SHPO is always considered a consulting party; INDOT and FHWA were participating agencies.

On August 11, 2014, Indiana Landmarks Central Office accepted the invitation to become a consulting party and suggested that Indiana Landmarks Eastern Regional Office and Visit Hamilton County Indiana be invited to join in consultation. (Staff of W&A invited both organizations to participate in the consultation under a letter dated August 12, 2014). The Indiana Landmarks Central Office inquired about the decision “To separate the overall I-69 Expansion. . .into several, individual projects with separate environmental analysis,” and asked, “will any of these projects occurring between Exit 205 and Exit 226 be completed concurrently?” (See Appendix F: Correspondence.) On August 15, 2014, W&A replied that “it is my understanding that construction will likely occur on Projects 1 and 3 at the same time but that the interchange may be occur at a later date. All of these projects are part of the ‘Major Moves 2020’ program; each of these corridors associated with this project has independent utility and logical termini so that if one project is delayed it will not affect the funding for other projects.” (Appendix F: Correspondence.)

On August 22, 2014, the staff of the SHPO responded to the early coordination letter and the HPR. The staff agreed that the Flanagan House, Hamilton County Bridge No. 177 and the Center School are “eligible for inclusion” in the NRHP. The staff, however, stated that the Fishers Methodist Episcopal Church and the mid-century House at 7883 South SR 13 “are potentially eligible for listing in the [NRHP], if the interiors are intact.” SHPO added, “We understand that it may not be possible to determine the condition of the interiors of these structures; therefore, we would be willing to consider them eligible for listing for the purposes of this review.” (See Appendix F: Correspondence.)

On September 18, 2014, staff of the SHPO responded via email to a phone message left by staff of W&A regarding the identification and eligibility of the Fishers Methodist Episcopal Church and the House at 7883 South SR 13. SHPO stated the Fishers Methodist Episcopal church would be eligible under Criterion C (Architecture), though “[a] case may also be able to be made for Religion depending on what additional information is available in the future.” The House at 7883 South SR 13 would be eligible under Criterion C (Architecture). (See Appendix F: Correspondence.)

In an email dated September 29, 2014, SHPO staff responded to additional questions asked by W&A on September 23, 2014. SHPO stated that staff believe both the Fishers Methodist Episcopal Church and House at 7883 South SR 13 are “potentially eligible pending additional information.” Regarding the church, SHPO noted the “ongoing and dramatic loss of historic resources has focused attention on a limited pool of historic places that now stand out as representative to the history of the community.” SHPO also provided draft guidelines regarding the eligibility of mid-twentieth century resources. (See Appendix F: Correspondence.)

SHPO replied to the Archaeology Short Report in a letter dated October 3, 2014. SHPO stated that based on the submitted report and information available to the SHPO staff, “we have not identified any currently known archaeological resources listed in or eligible for inclusion in the NRHP within the proposed project area.” SHPO stated this conclusion was “subject to project activities remaining within areas disturbed by previous construction of a recent and non-historical nature.” SHPO also stated that any archaeological deposits encountered from the post-contact period would be subject to evaluation “regarding their eligibility for the NRHP in consultation with the staff of the Indiana SHPO. Please contact our office if such deposits are encountered.” Additionally, SHPO stated that Indiana state law requires the Department of Natural Resources be notified within two business days in the event that “archaeological artifacts or human remains are uncovered during construction, demolition, or earthmoving activities.” (See Appendix F: Correspondence.)

On October 22, 2014, the Assistant Director of Preservation Services for DHPA, Paul Diebold, responded to a request for an opinion of eligibility made by INDOT-CRO in regard to the Flanagan House. “After some debate, we have reached the conclusion that the house no longer meets the National Register criteria. In particular, the siting and orientation of the house render it incapable of conveying its architectural significance.” Diebold noted, “Examples of vernacular architecture like the Kincaid House convey their sense of time and place, in good measure, by their orientation.” (See Appendix F: Correspondence.)

No other comments were received.

A public notice of No Adverse Effect will be posted in a local newspaper(s) and the public will be afforded thirty (30) days to respond. In addition, the public will have the opportunity to comment on section 106 at a hearing held for the larger environmental document. If appropriate, this document will be revised after the public has had the opportunity to comment.

APPENDIX

Appendix A: Plans

Appendix B: Maps

Appendix C: Consulting Parties

Appendix D: Photographs

Appendix E: Report Summaries

Appendix F: Correspondence

Appendix G: Section 106 Documentation and Additional Information Relating to the Flanagan House

APPENDIX A. Plans

PROJECT	DESIGNATION
1383332, 1383336	1383332, 1383336
CONTRACT	
R-37053	

INDIANA DEPARTMENT OF TRANSPORTATION



ROAD PLANS

ROUTE: I-69 FROM: RP 205+40 TO: RP 214+95

PROJECT NO. 1383332, 1383336 P.E.

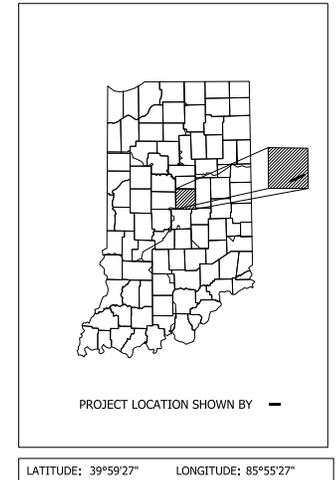
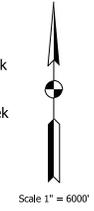
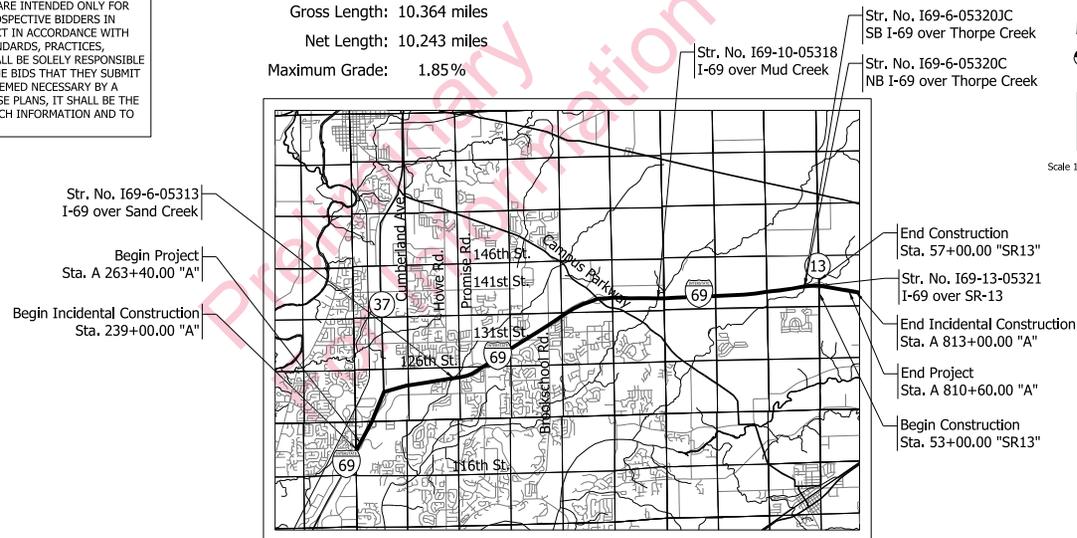
The project begins approximately 0.30 miles north of 106th Street and follows the existing Interstate 69 up to 0.46 miles east of SR-13. The project is in Section 1 in T-17-N, R-4-E, Section 6 in T-17-N, R-5-E, Sections 23, 24, 26, 27, 28, 31, 32, and 33 in T-18-N, R-5-E, and Sections 19, 20, 21, and 22 in T-18-N, R-6-E, in Delaware, Fall Creek, and Green Townships in Hamilton and Madison Counties, Indiana.

R/W
1383332, 1383336 CONST.

WARNING:

THESE PLANS ARE CERTIFIED ONLY FOR COMPLIANCE WITH INDOT LEVEL ONE DESIGN CRITERIA EXCEPT AS NOTED IN THE "SCOPE OF SERVICES" SECTION OF THE CONTRACT DOCUMENTS AND THEY ARE NOT COMPLETE AND ARE NOT ADEQUATE FOR THE PREPARATION OF DESIGN/BUILD BIDS OR CONSTRUCTION OF THE PROJECT. THEY ARE INTENDED ONLY FOR THE LIMITED PURPOSE OF PROVIDING INFORMATION TO ASSIST PROSPECTIVE BIDDERS IN DEVELOPING THEIR BIDS FOR THE PROPOSED DESIGN/BUILD PROJECT IN ACCORDANCE WITH INDOT REQUIREMENTS AND APPLICABLE RULES, REGULATIONS, STANDARDS, PRACTICES, POLICIES, GUIDELINES AND SPECIFICATIONS. DESIGN/BUILDERS SHALL BE SOLELY RESPONSIBLE FOR THE ACCURACY AND COMPLETENESS OF THEIR DESIGNS AND THE BIDS THAT THEY SUBMIT AND IF PROJECT INFORMATION IN ADDITION TO THESE PLANS IS DEEMED NECESSARY BY A DESIGN/BUILDER, OR IF AN ERROR OR INACCURACY OCCURS IN THESE PLANS, IT SHALL BE THE SOLE RESPONSIBILITY OF THE DESIGN/BUILDER TO ACQUIRE ALL SUCH INFORMATION AND TO DETECT AND CORRECT ANY SUCH ERROR OR INACCURACY.

Gross Length: 10.364 miles
Net Length: 10.243 miles
Maximum Grade: 1.85%



LOCATION MAP
HAMILTON & MADISON COUNTY

INDIANA DEPARTMENT OF TRANSPORTATION
STANDARD SPECIFICATIONS DATED 2014
TO BE USED WITH THESE PLANS

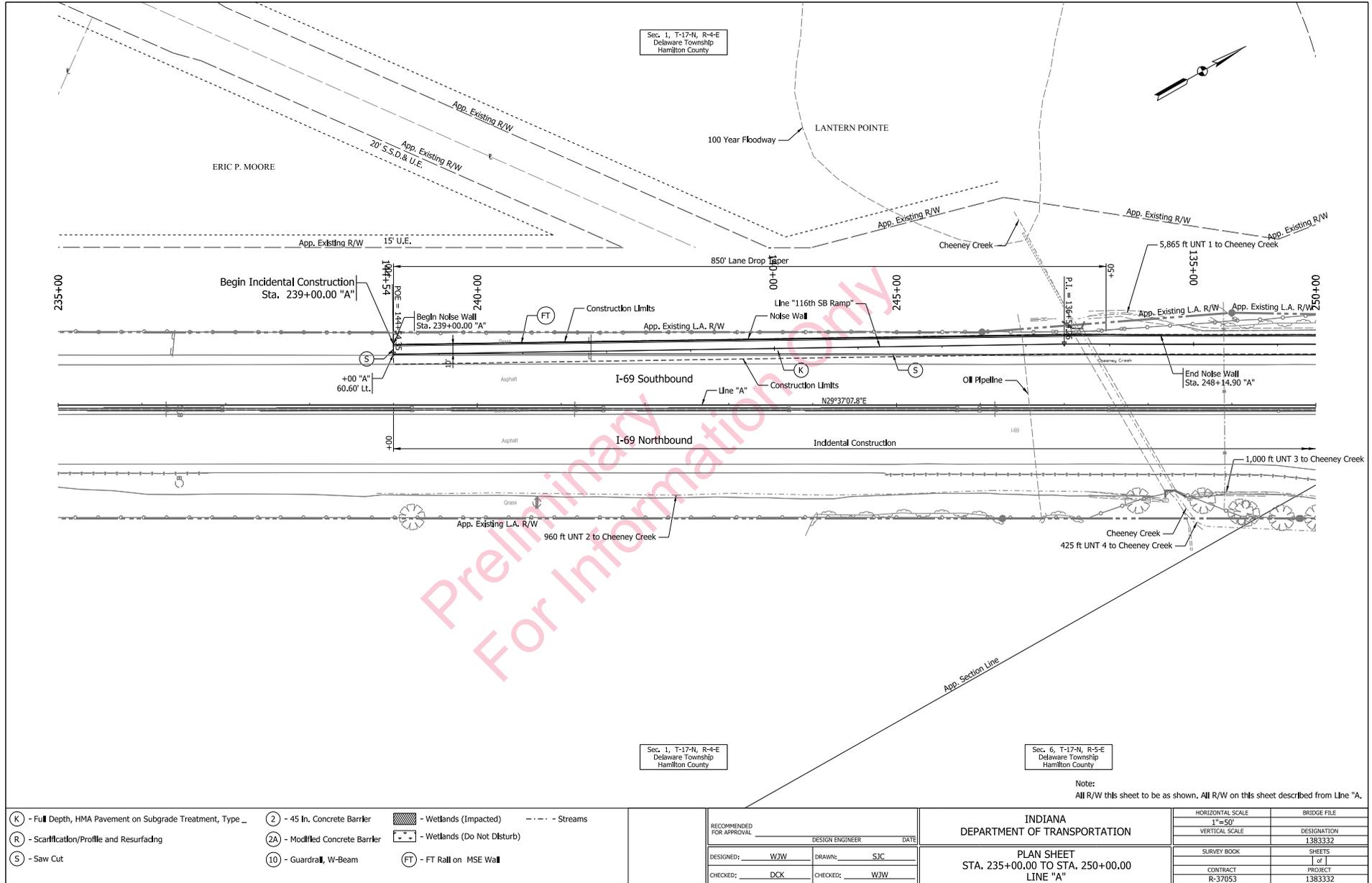
PARSONS
101 W. Ohio St., Suite 2121
Indianapolis, IN 46204
Bus (317) 616-1000
Fax (317) 616-1033

PLANS PREPARED BY: _____	PHONE NUMBER _____
CERTIFIED BY: _____	DATE _____
APPROVED FOR LETTING: _____	DATE _____

INDIANA DEPARTMENT OF TRANSPORTATION

	DESIGNATION
	1383332, 1383336
SURVEY BOOK	SHEETS
	1 of 1
CONTRACT	PROJECT
R-37053	1383332, 1383336

23-SEP-2014



- (K) - Full Depth, HMA Pavement on Subgrade Treatment, Type _
- (R) - Scarification/Profile and Resurfacing
- (S) - Saw Cut
- (2) - 45 In. Concrete Barrier
- (2A) - Modified Concrete Barrier
- (10) - Guardrail, W-Beam
- (FT) - FT Fall on MSE Wall
- ▨ - Wetlands (Impacted)
- ▨ - Wetlands (Do Not Disturb)
- - Streams

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
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CHECKED: DCK	CHECKED: WJW	

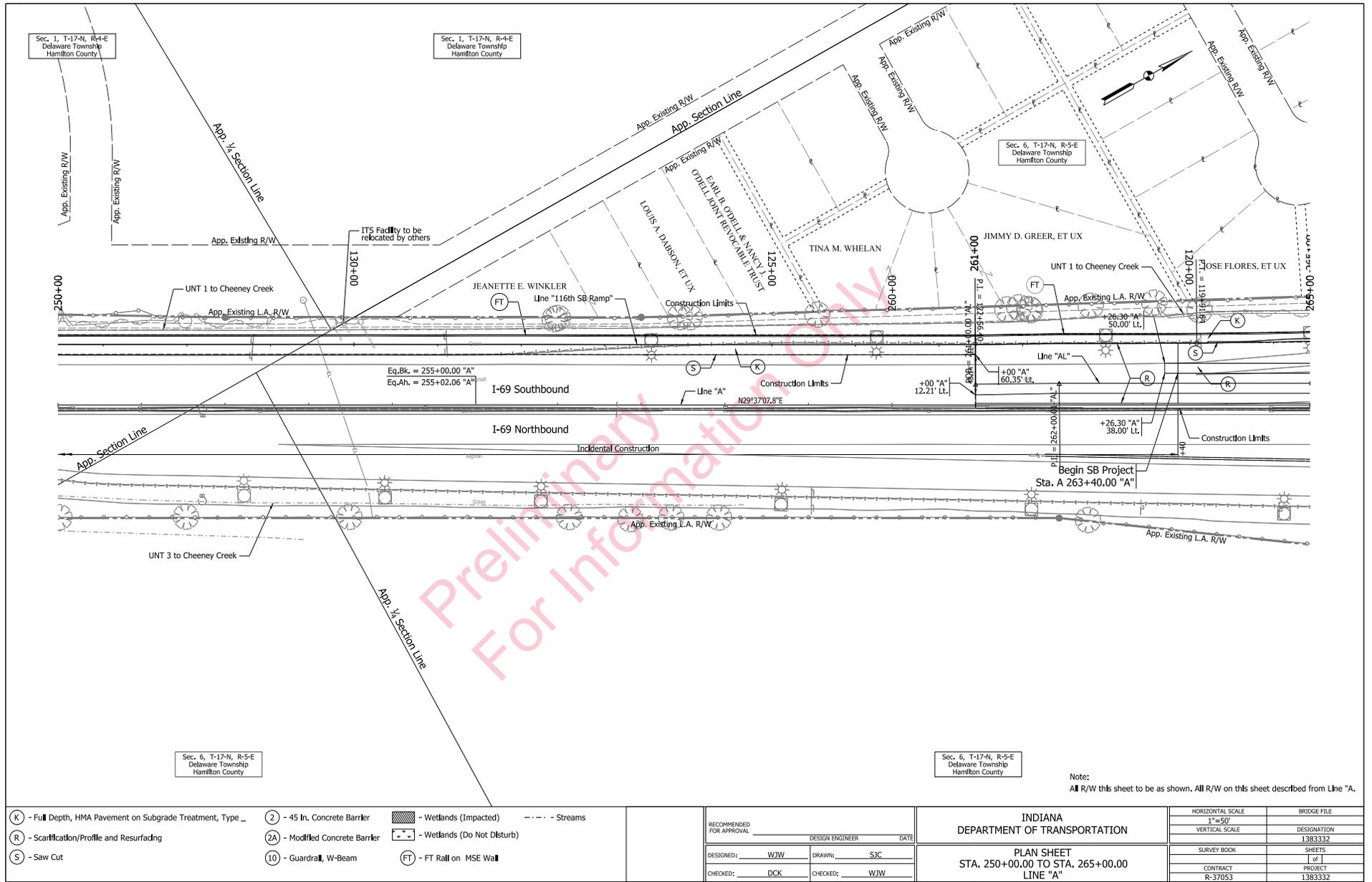
INDIANA
DEPARTMENT OF TRANSPORTATION

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VERTICAL SCALE	DESIGNATION
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SURVEY BOOK	SHEETS
	of 1
CONTRACT	PROJECT
R-37053	1383332

Note:
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23-OCT-2014



Note:
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(S) - Saw Cut	(10) - Guardrail, W-Beam	(FT) - FT Wall on MSE Wall	

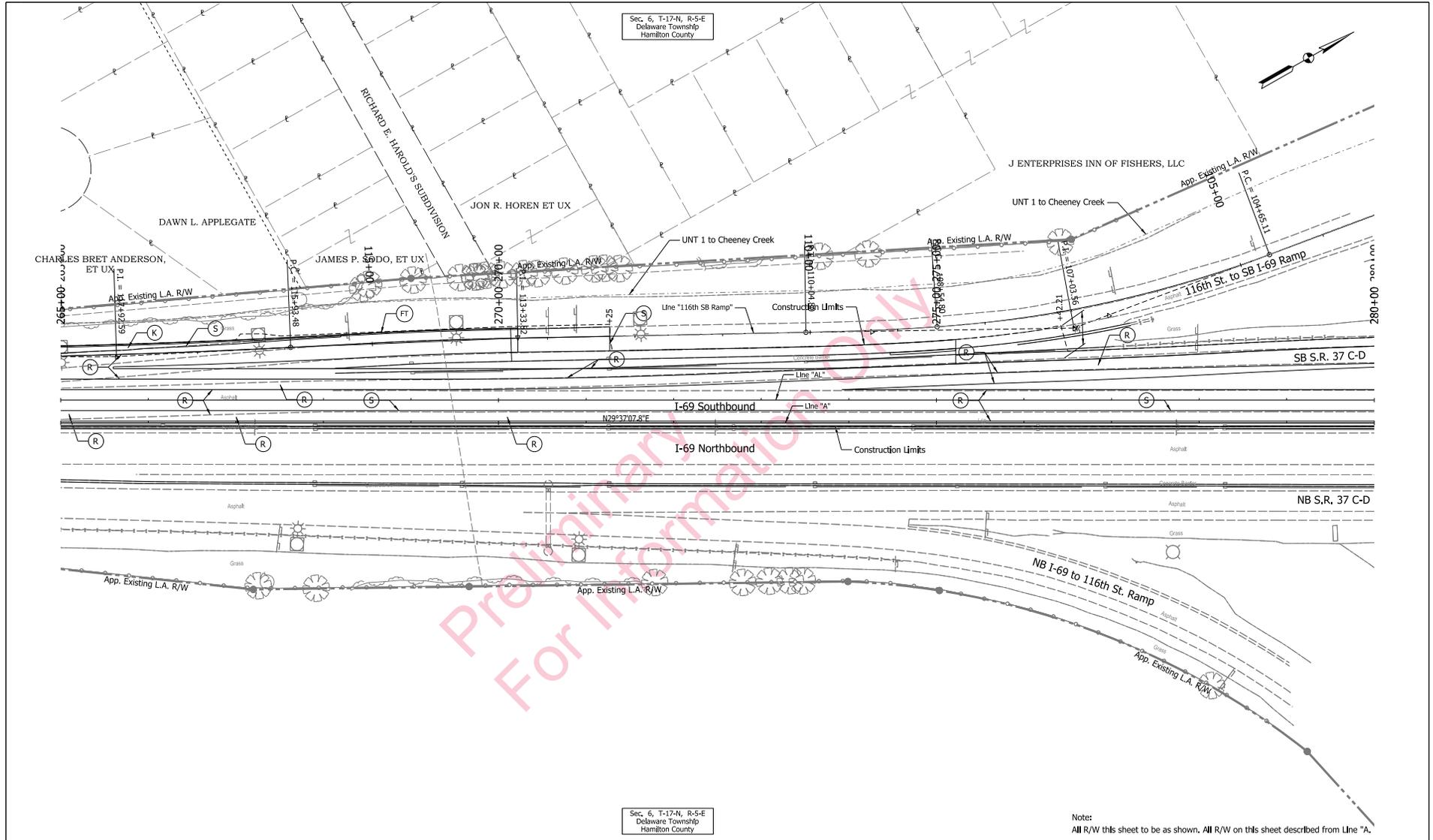
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INDIANA
 DEPARTMENT OF TRANSPORTATION

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CONTRACT	PROJECT
R-37053	1383332

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 23-OCT-2014



Sec. 6, T-17-N, R-5-E
Delaware Township
Hamilton County

Sec. 6, T-17-N, R-5-E
Delaware Township
Hamilton County

Note:
All R/W this sheet to be as shown. All R/W on this sheet described from Line "A".

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- (R) - Scarification/Profile and Resurfacing
- (S) - Saw Cut
- (2) - 45 In. Concrete Barrier
- (2A) - Modified Concrete Barrier
- (10) - Guardrail, W-Beam
- (FT) - FT Rail on MSE Wall
- [Hatched] - Wetlands (Impacted)
- [Dashed] - Wetlands (Do Not Disturb)
- [Dotted] - Streams

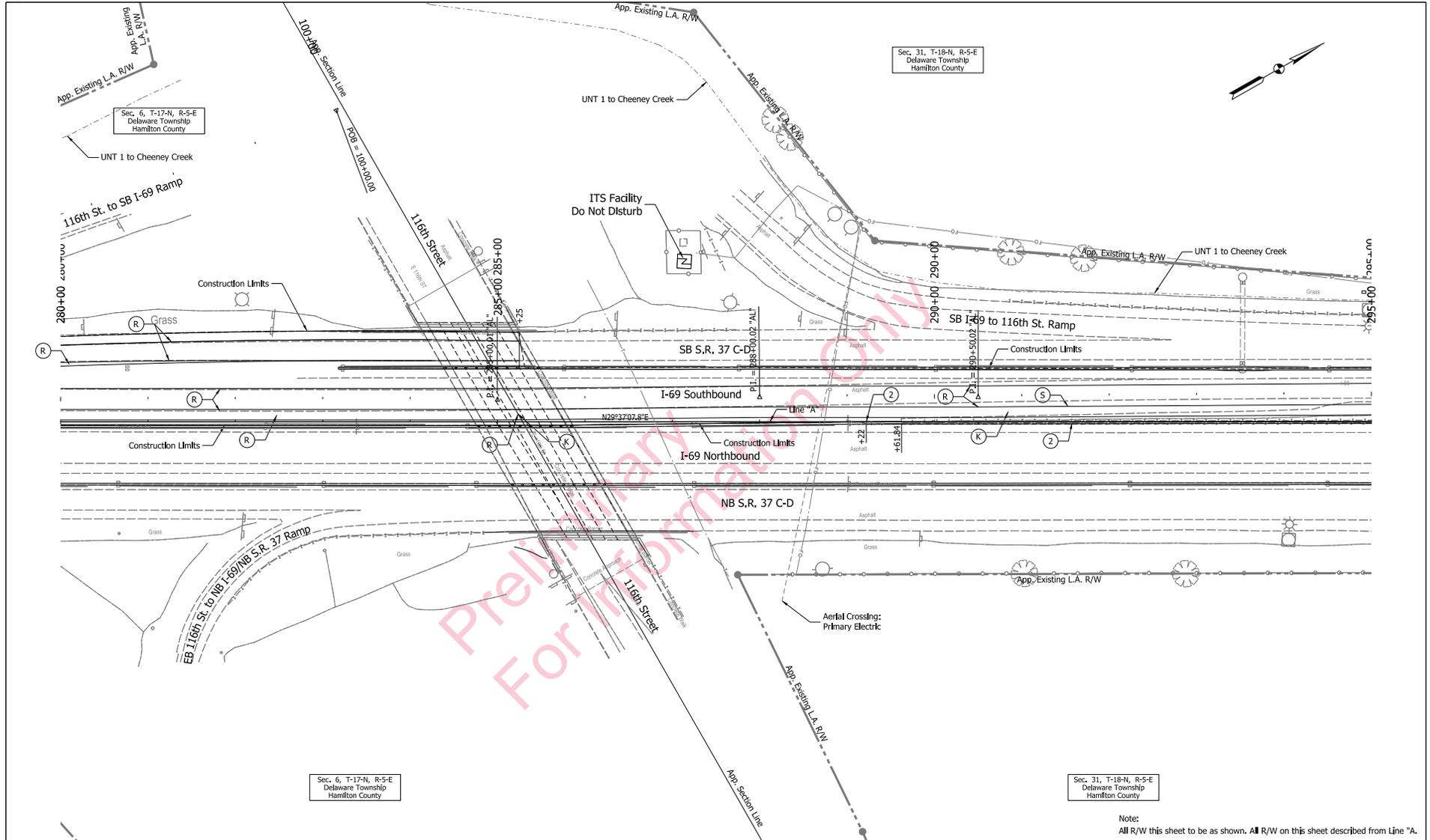
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CHECKED: DCK	CHECKED: WJW	

INDIANA
DEPARTMENT OF TRANSPORTATION

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CONTRACT	PROJECT
R-37053	1383332

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23-OCT-2014



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(R) - Scarification/Profile and Resurfacing	(2A) - Modified Concrete Barrier	[Dotted] - Wetlands (Do Not Disturb)	
(S) - Saw Cut	(10) - Guardrail, W-Beam	(FT) - FT Fall on MSE Wall	

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CHECKED: DCK	CHECKED: WJW	

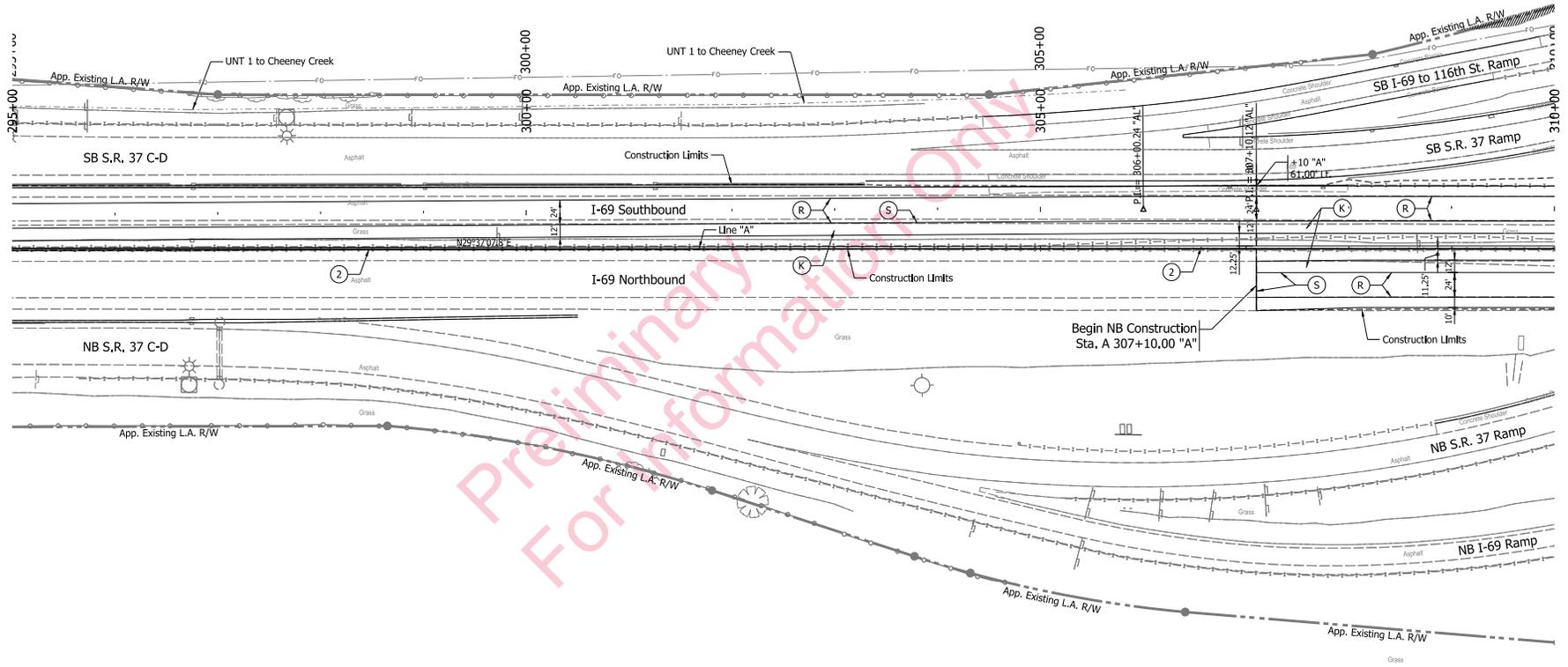
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DEPARTMENT OF TRANSPORTATION

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23-OCT-2014

Sec. 31, T-18-N, R-5-E
Delaware Township
Hamilton County



Sec. 31, T-18-N, R-5-E
Delaware Township
Hamilton County

Note:
All R/W this sheet to be as shown. All R/W on this sheet described from Line "A".

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- (R) - Scarification/Profile and Resurfacing
- (S) - Saw Cut
- (2) - 45 In. Concrete Barrier
- (2A) - Modified Concrete Barrier
- (10) - Guardrail, W-Beam
- (FT) - FT Rail on MSE Wall
- [Hatched] - Wetlands (Impacted)
- [Dashed] - Wetlands (Do Not Disturb)
- [Dotted] - Streams

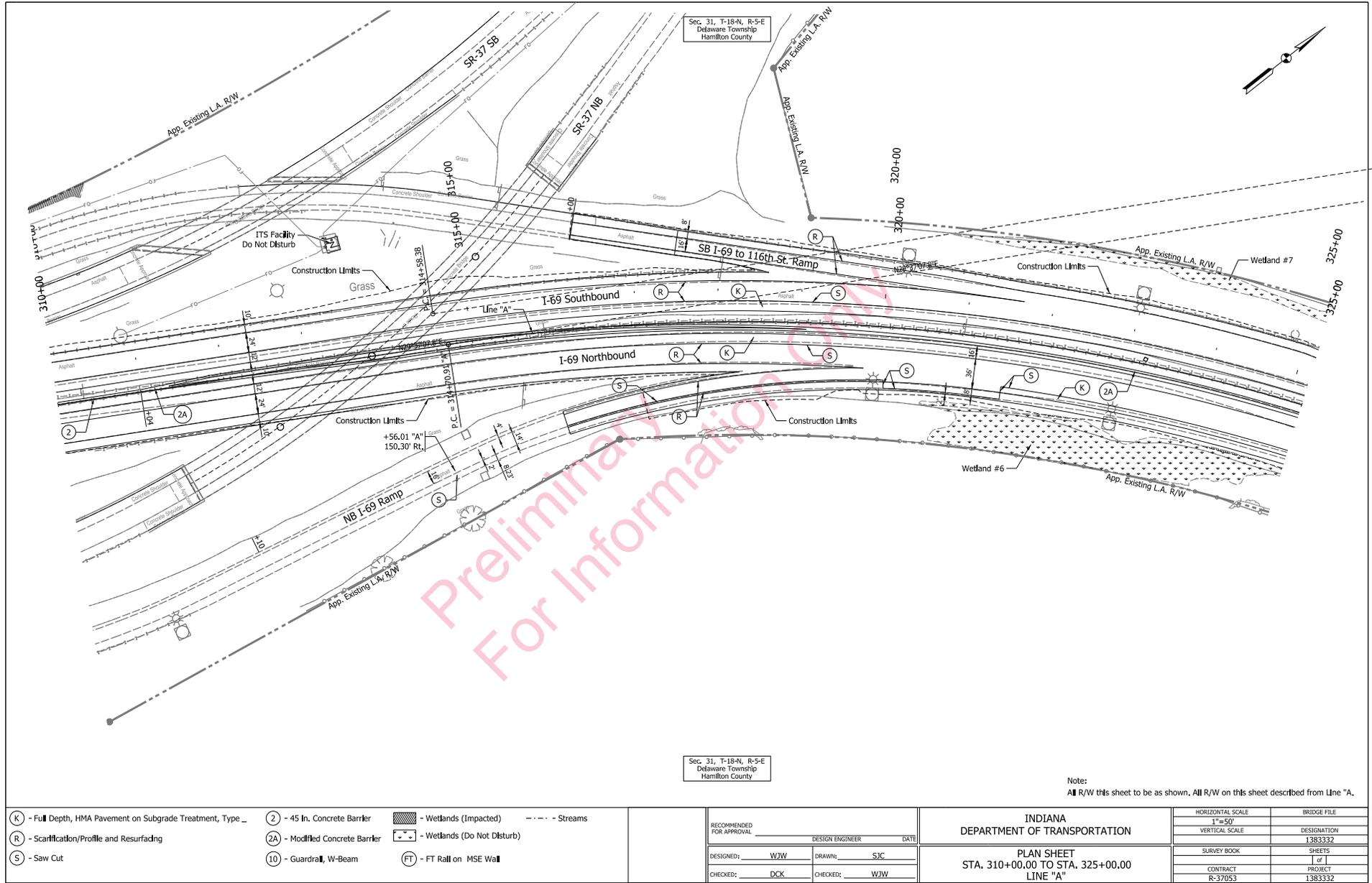
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INDIANA
DEPARTMENT OF TRANSPORTATION

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1"=50'	DESIGNATION
VERTICAL SCALE	1383332
SURVEY BOOK	SHEETS
CONTRACT	of 1
R-37053	PROJECT
	1383332

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23-OCT-2014



Sec. 31, T-18-N, R-5-E
Delaware Township
Hamilton County

Sec. 31, T-18-N, R-5-E
Delaware Township
Hamilton County

Note:
All R/W this sheet to be as shown. All R/W on this sheet described from Line "A".

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- (S) - Saw Cut
- (2) - 45 In. Concrete Barrier
- (2A) - Modified Concrete Barrier
- (10) - Guardrail, W-Beam
- (FT) - FT Rail on MSE Wall
- [Hatched] - Wetlands (Impacted)
- [Dotted] - Wetlands (Do Not Disturb)
- [Dashed] - Streams

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
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INDIANA
DEPARTMENT OF TRANSPORTATION

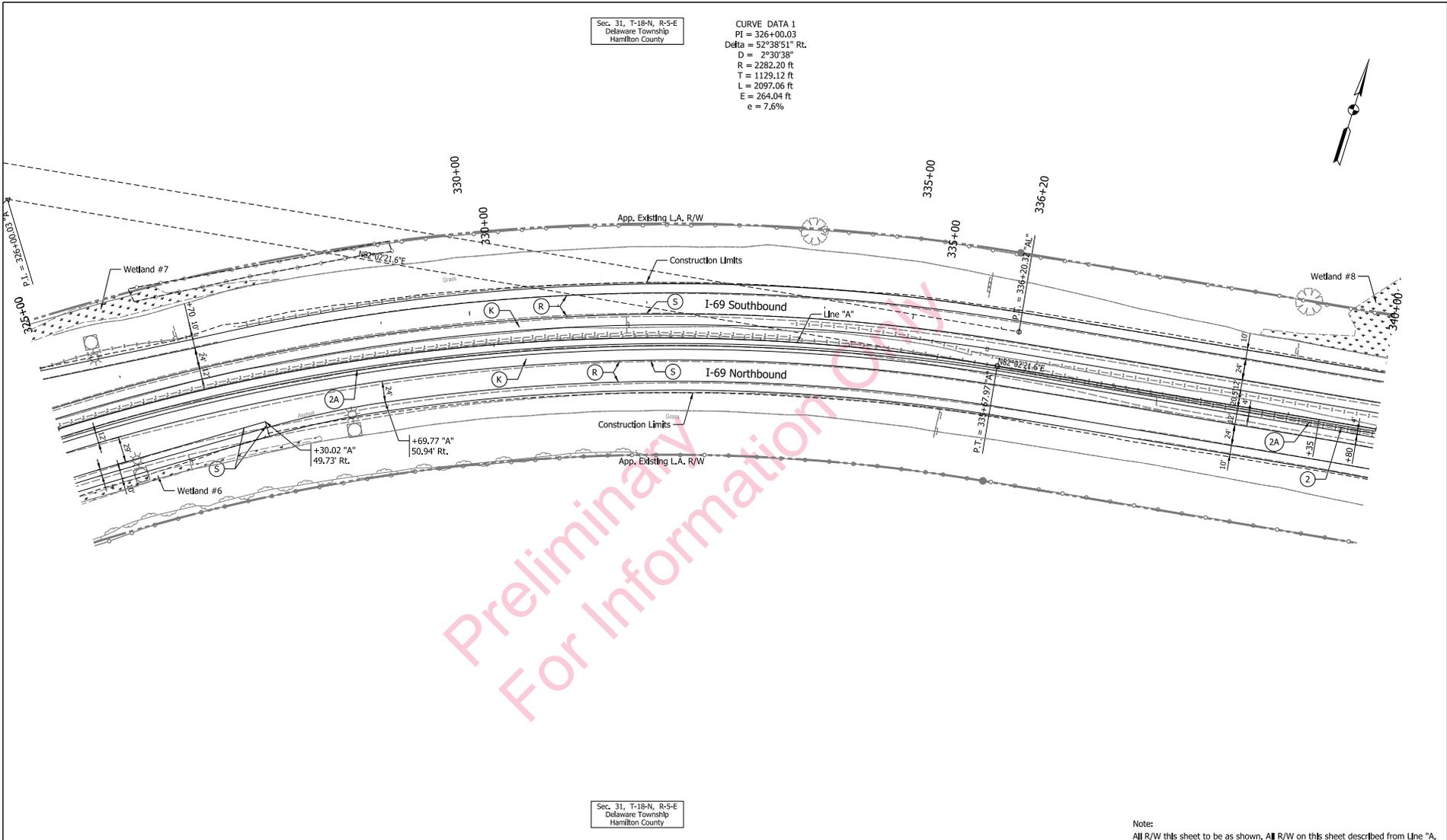
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23-OCT-2014

Sec. 31, T-18-N, R-5-E
Delaware Township
Hamilton County

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L = 2097.06 ft
E = 264.04 ft
e = 7.6%



Sec. 31, T-18-N, R-5-E
Delaware Township
Hamilton County

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- (2A) - Modified Concrete Barrier
- (10) - Guardrail, W-Beam
- (FT) - FT Fall on MSE Wall
- Wetlands (Impacted)
- Wetlands (Do Not Disturb)
- Streams

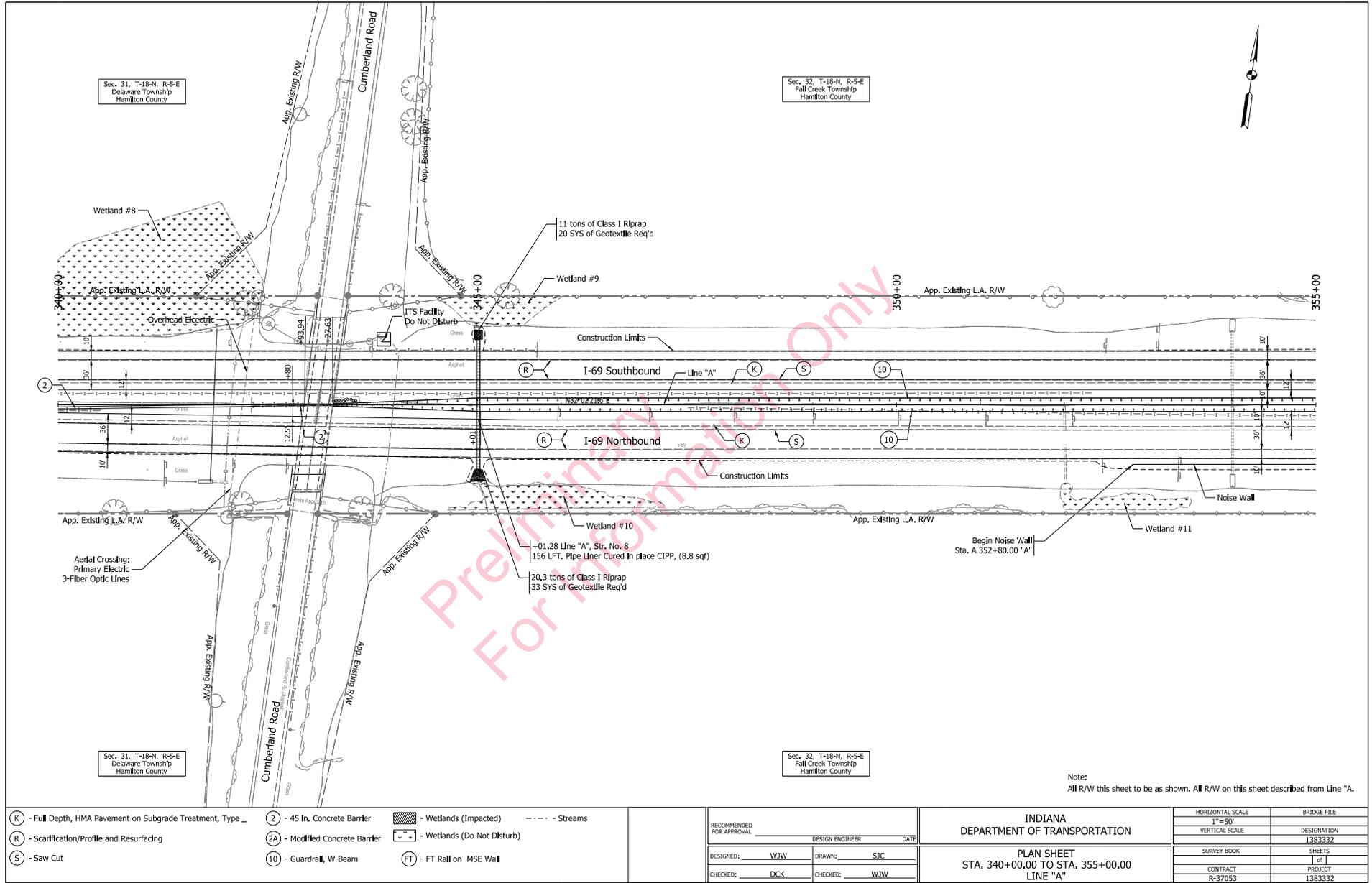
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INDIANA
DEPARTMENT OF TRANSPORTATION

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CONTRACT	PROJECT
R-37053	1383332

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23-OCT-2014



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(S) - Saw Cut	(10) - Guardrail, W-Beam	(FT) - FT Rall on MSE Wall	

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
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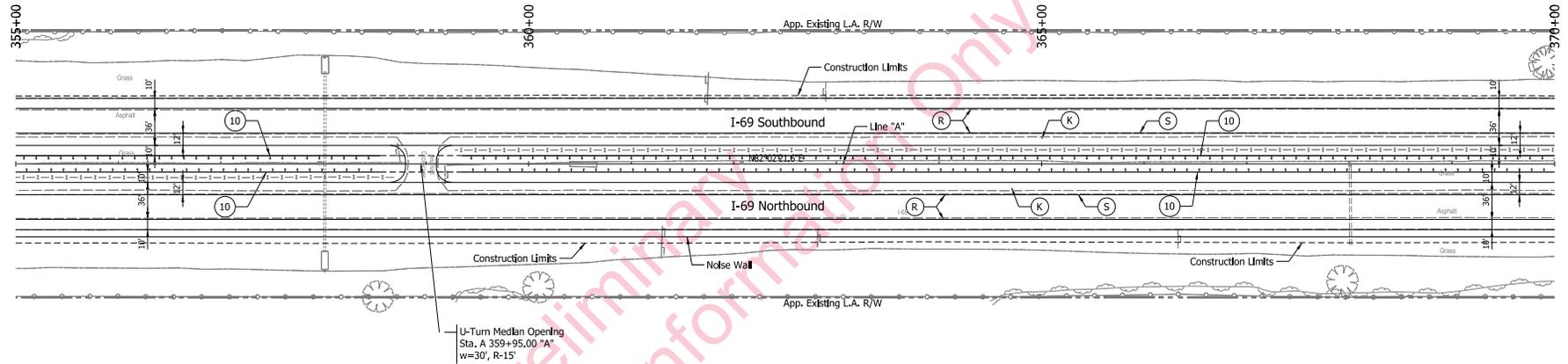
INDIANA
DEPARTMENT OF TRANSPORTATION

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	of 1
CONTRACT	PROJECT
R-37053	1383332

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23-OCT-2014

Sec. 32, T-18-N, R-5-E
Fall Creek Township
Hamilton County



Preliminary
For Information Only

Sec. 32, T-18-N, R-5-E
Fall Creek Township
Hamilton County

Note:
All R/W this sheet to be as shown. All R/W on this sheet described from Line "A".

- (K) - Full Depth, HMA Pavement on Subgrade Treatment, Type _
- (R) - Scarification/Profile and Resurfacing
- (S) - Saw Cut
- (2) - 45 In. Concrete Barrier
- (2A) - Modified Concrete Barrier
- (10) - Guardrail, W-Beam
- (FT) - FT Fall on MSE Wall
- [Hatched] - Wetlands (Impacted)
- [Dashed] - Wetlands (Do Not Disturb)
- [Dotted] - Streams

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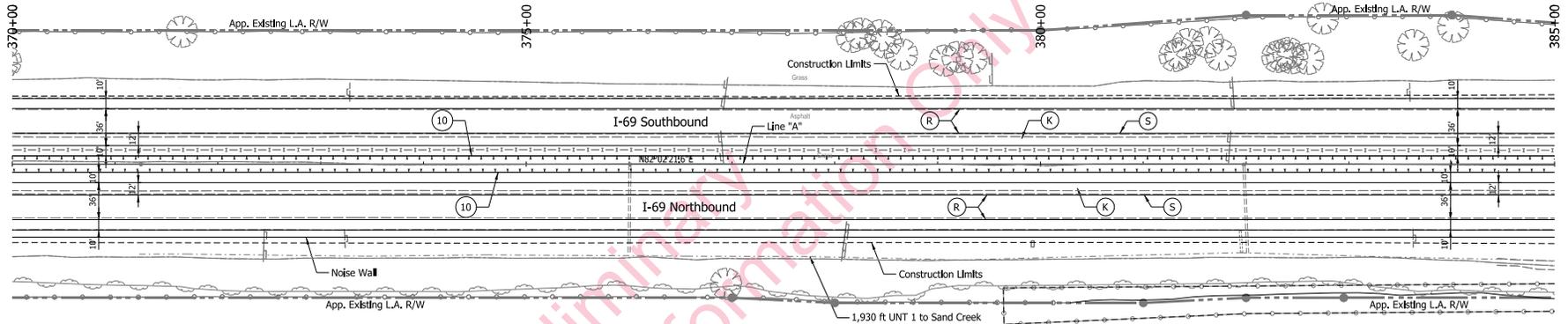
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DEPARTMENT OF TRANSPORTATION

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VERTICAL SCALE	DESIGNATION
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SURVEY BOOK	SHEETS
	of 1
CONTRACT	PROJECT
R-37053	1383332

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23-OCT-2014

Sec. 32, T-18-N, R-5-E
Fall Creek Township
Hamilton County



Preliminary
For Information Only

Sec. 32, T-18-N, R-5-E
Fall Creek Township
Hamilton County

100 Year Floodway

Note:
All R/W this sheet to be as shown. All R/W on this sheet described from Line "A".

- (K) - Full Depth, HMA Pavement on Subgrade Treatment, Type _
- (R) - Scarification/Profile and Resurfacing
- (S) - Saw Cut
- (2) - 45 In. Concrete Barrier
- (2A) - Modified Concrete Barrier
- (10) - Guardrail, W-Beam
- (FT) - FT Fall on MSE Wall
- [Hatched] - Wetlands (Impacted)
- [Dashed] - Wetlands (Do Not Disturb)
- [Dotted] - Streams

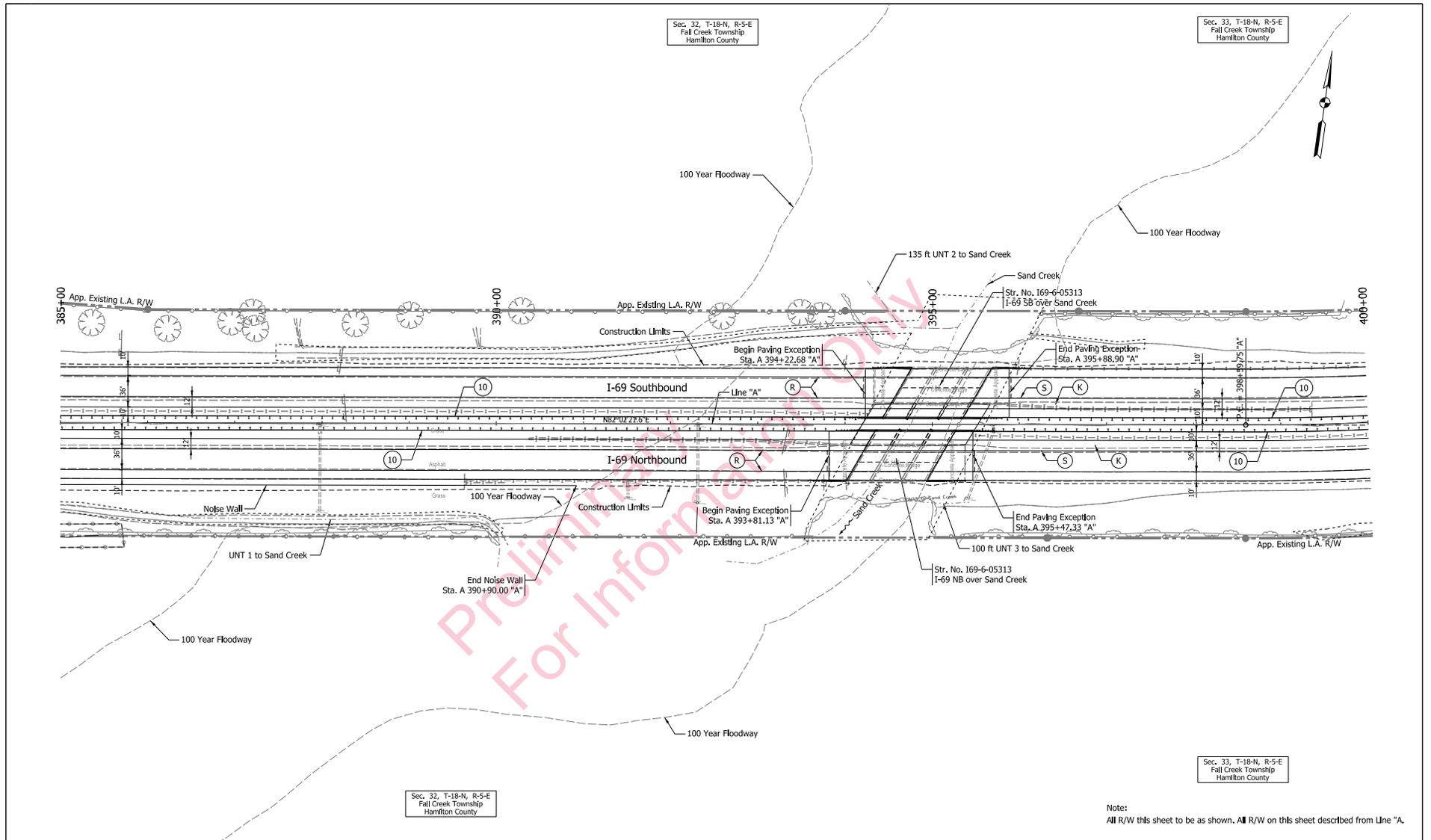
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INDIANA
DEPARTMENT OF TRANSPORTATION

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VERTICAL SCALE	DESIGNATION
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SURVEY BOOK	SHEETS
	of 1
CONTRACT	PROJECT
R-37053	1383332

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23-OCT-2014



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(S) - Saw Cut	(10) - Guardrail, W-Beam	(FT) - FT Fall on MSE Wall	

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: WJW	DRAWN: SJC	
CHECKED: DCK	CHECKED: WJW	

INDIANA
DEPARTMENT OF TRANSPORTATION

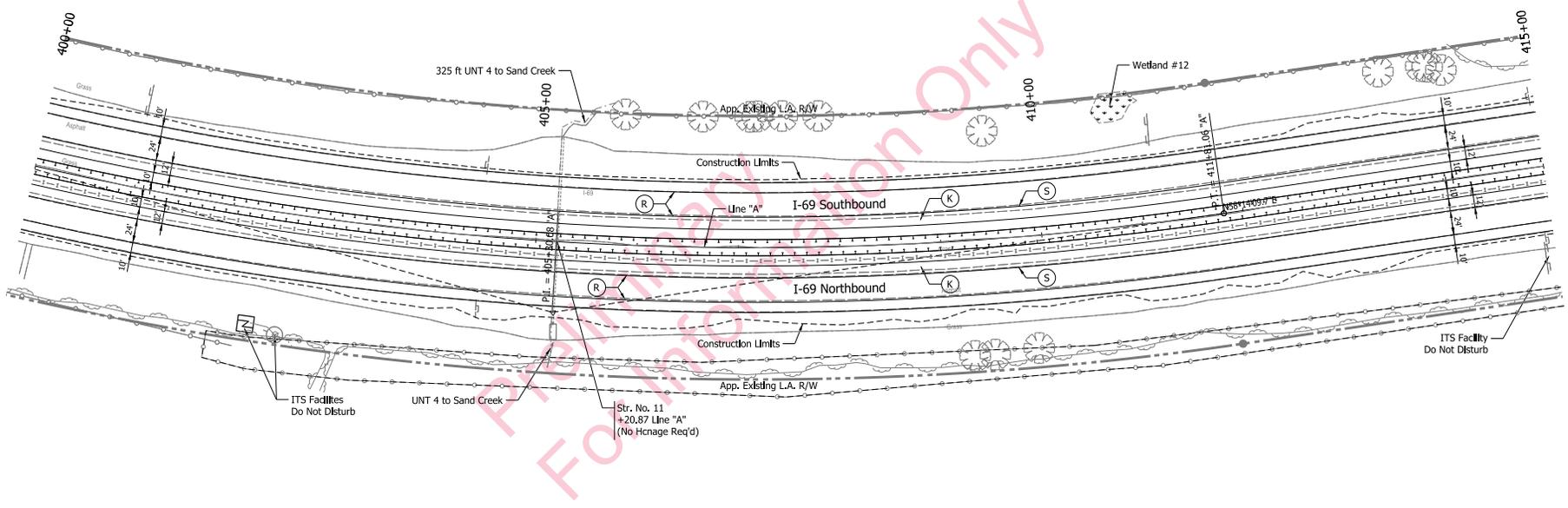
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SURVEY BOOK	SHEETS
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CONTRACT	PROJECT
R-37053	1383332

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23-OCT-2014

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 E = 69.90 ft

Sec. 33, T-18-N, R-9-E
 Fall Creek Township
 Hamilton County



Sec. 33, T-18-N, R-9-E
 Fall Creek Township
 Hamilton County

Note:
 All R/W this sheet to be as shown. All R/W on this sheet described from Line "A".

- (K) - Full Depth, HMA Pavement on Subgrade Treatment, Type _
- (R) - Scarification/Profile and Resurfacing
- (S) - Saw Cut
- (2) - 45 In. Concrete Barrier
- (2A) - Modified Concrete Barrier
- (10) - Guardrail, W-Beam
- (FT) - FT Fall on MSE Wall
- [Hatched] - Wetlands (Impacted)
- [Dashed] - Wetlands (Do Not Disturb)
- [Dotted] - Streams

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
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INDIANA
 DEPARTMENT OF TRANSPORTATION

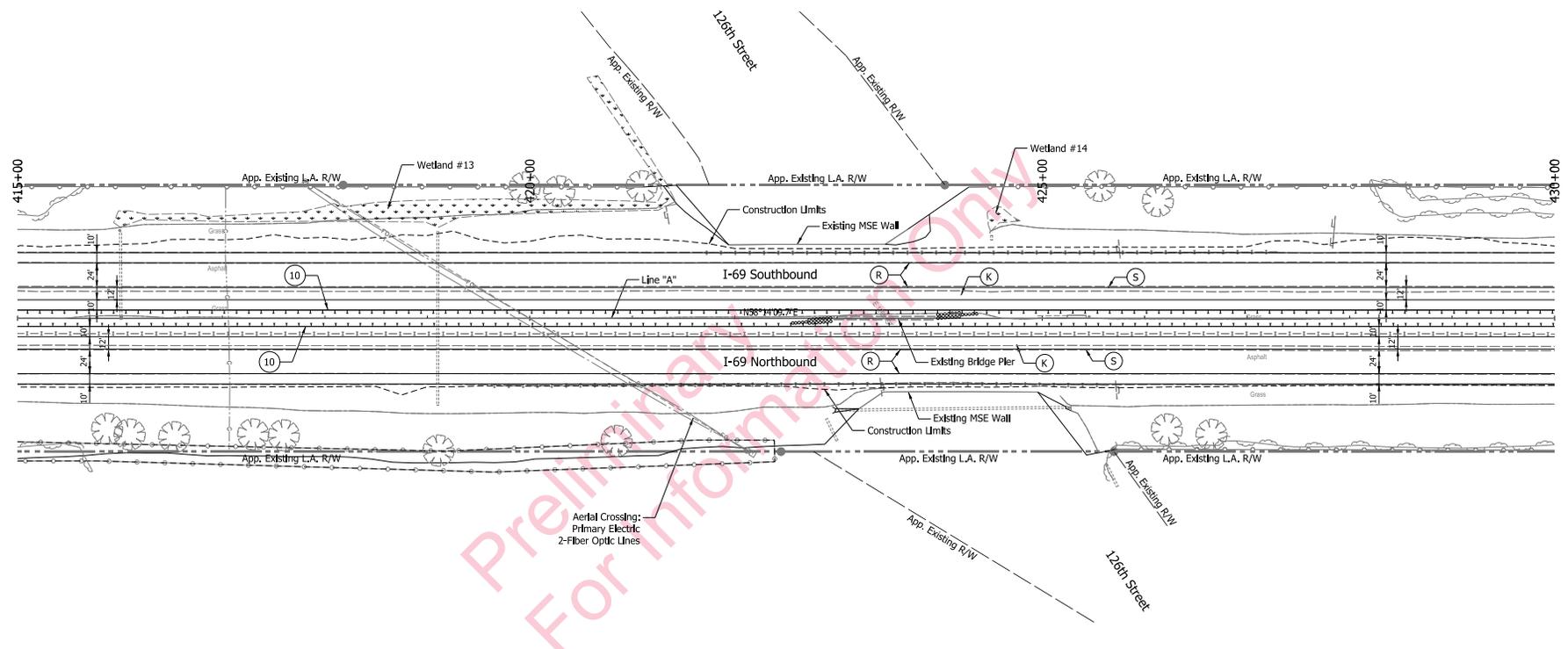
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1"=50'	
VERTICAL SCALE	DESIGNATION
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SURVEY BOOK	SHEETS
	of 1
CONTRACT	PROJECT
R-37053	1383332

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 23-OCT-2014

Sec. 33, T-18-N, R-5-E
Fall Creek Township
Hamilton County

Sec. 28, T-18-N, R-5-E
Fall Creek Township
Hamilton County



Sec. 33, T-18-N, R-5-E
Fall Creek Township
Hamilton County

Sec. 28, T-18-N, R-5-E
Fall Creek Township
Hamilton County

Note:
All R/W this sheet to be as shown. All R/W on this sheet described from Line "A".

- (K) - Full Depth, HMA Pavement on Subgrade Treatment, Type _
- (R) - Scarification/Profile and Resurfacing
- (S) - Saw Cut
- (2) - 45 In. Concrete Barrier
- (2A) - Modified Concrete Barrier
- (10) - Guardrail, W-Beam
- (FT) - FT Fall on MSE Wall
- [Hatched Box] - Wetlands (Impacted)
- [Dashed Box] - Wetlands (Do Not Disturb)
- [Dotted Line] - Streams

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: WJW	DRAWN: SJC	
CHECKED: DCK	CHECKED: WJW	

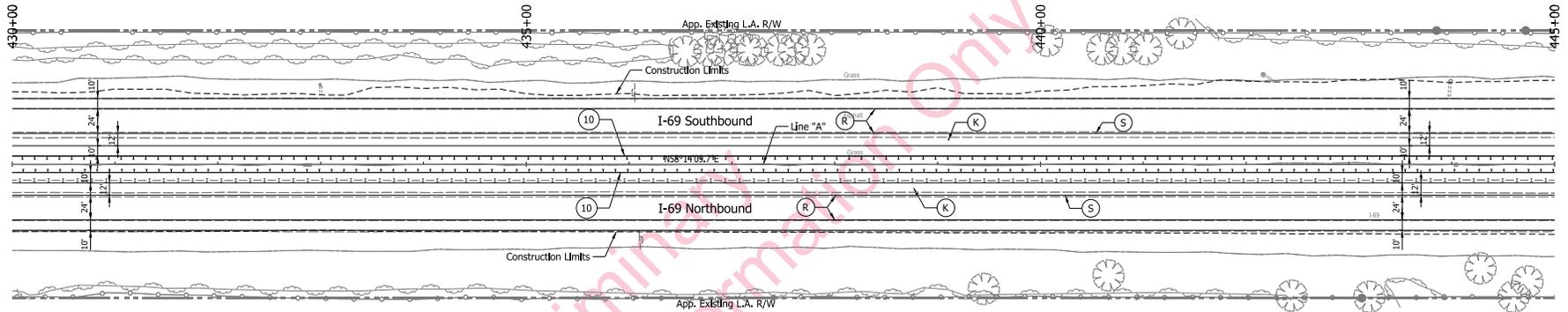
INDIANA
DEPARTMENT OF TRANSPORTATION

PLAN SHEET
STA. 415+00.00 TO STA. 430+00.00
LINE "A"

HORIZONTAL SCALE	BRIDGE FILE
1"=50'	
VERTICAL SCALE	DESIGNATION
	1383332
SURVEY BOOK	SHEETS
	of 1
CONTRACT	PROJECT
R-37053	1383332

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23-OCT-2014

Sec. 28, T-18-N, R-5-E
Fall Creek Township
Hamilton County



Preliminary
For Information Only

Sec. 28, T-18-N, R-5-E
Fall Creek Township
Hamilton County

Note:
All R/W this sheet to be as shown. All R/W on this sheet described from Line "A".

- (K) - Full Depth, HMA Pavement on Subgrade Treatment, Type _
- (R) - Scarification/Profile and Resurfacing
- (S) - Saw Cut
- (2) - 45 In. Concrete Barrier
- (2A) - Modified Concrete Barrier
- (10) - Guardrail, W-Beam
- (FT) - FT Rail on MSE Wall
- [Hatched Box] - Wetlands (Impacted)
- [Dashed Box] - Wetlands (Do Not Disturb)
- [Dashed Line] - Streams

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: WJW	DRAWN: SJC	
CHECKED: DCK	CHECKED: WJW	

INDIANA
DEPARTMENT OF TRANSPORTATION

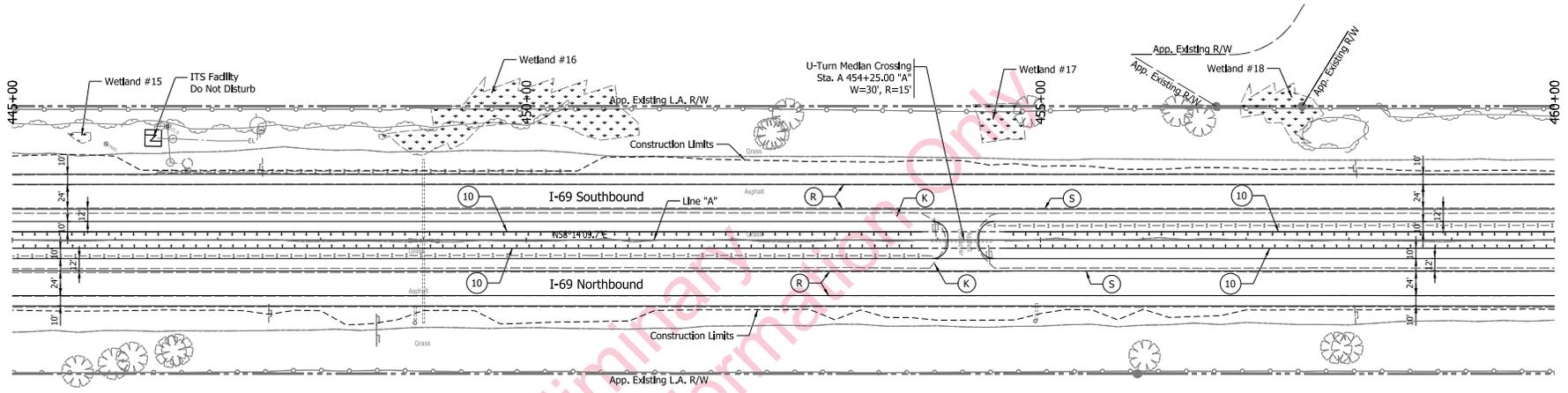
PLAN SHEET
STA. 430+00.00 TO STA. 445+00.00
LINE "A"

HORIZONTAL SCALE 1"=50'	BRIDGE FILE
VERTICAL SCALE	DESIGNATION 1383332
SURVEY BOOK	SHEETS of 1
CONTRACT R-37053	PROJECT 1383332

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23-OCT-2014

Sec. 28, T-18-N, R-5-E
Fall Creek Township
Hamilton County

Sec. 27, T-18-N, R-5-E
Fall Creek Township
Hamilton County



Sec. 27, T-18-N, R-5-E
Fall Creek Township
Hamilton County

Sec. 28, T-18-N, R-5-E
Fall Creek Township
Hamilton County

Note:
All R/W this sheet to be as shown, All R/W on this sheet described from Line "A".

- (K) - Full Depth, HMA Pavement on Subgrade Treatment, Type _
- (R) - Scarification/Profile and Resurfacing
- (S) - Saw Cut
- (2) - 45 In. Concrete Barrier
- (2A) - Modified Concrete Barrier
- (10) - Guardrail, W-Beam
- (FT) - FT Rail on MSE Wall
- ▨ - Wetlands (Impacted)
- ▨ - Wetlands (Do Not Disturb)
- - Streams

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: WJW	DRAWN: SJC	
CHECKED: DCK	CHECKED: WJW	

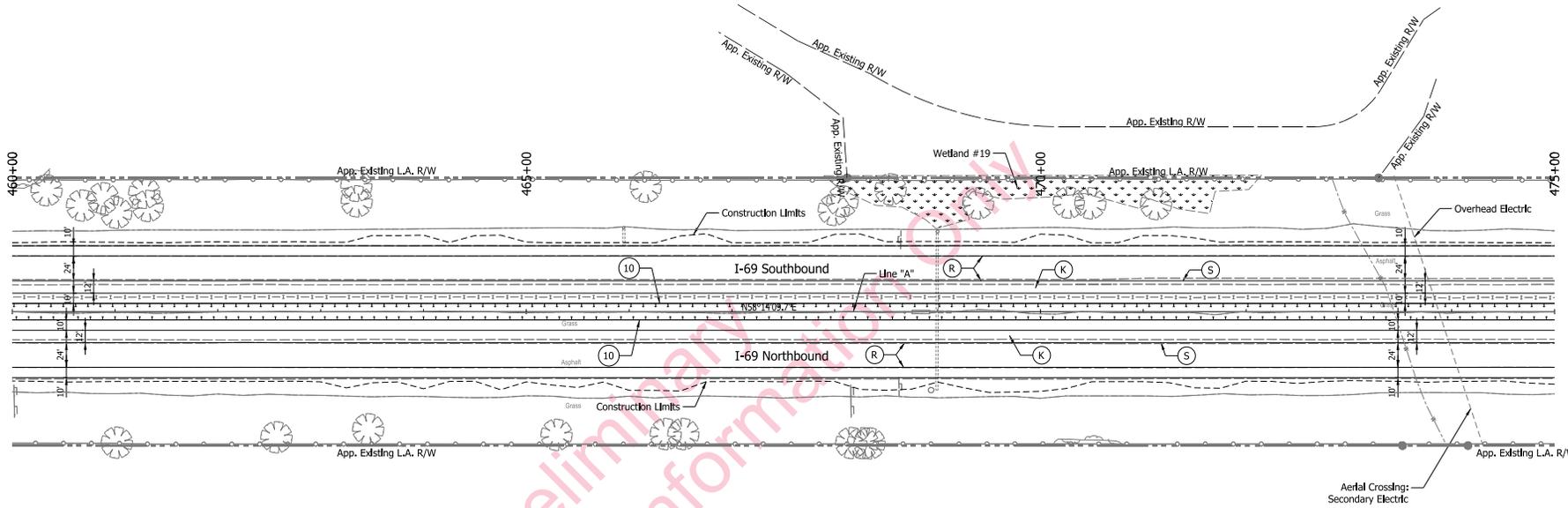
INDIANA
DEPARTMENT OF TRANSPORTATION

PLAN SHEET
STA. 445+00.00 TO STA. 460+00.00
LINE "A"

HORIZONTAL SCALE	BRIDGE FILE
1"=50'	
VERTICAL SCALE	DESIGNATION
	1383332
SURVEY BOOK	SHEETS
	of 1
CONTRACT	PROJECT
R-37053	1383332

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23-OCT-2014

Sec. 27, T-18-N, R-5-E
Fall Creek Township
Hamilton County



Sec. 27, T-18-N, R-5-E
Fall Creek Township
Hamilton County

Note:
All R/W this sheet to be as shown. All R/W on this sheet described from Line "A".

- (K) - Full Depth, HMA Pavement on Subgrade Treatment, Type _
- (R) - Scarification/Profile and Resurfacing
- (S) - Saw Cut
- (2) - 45 In. Concrete Barrier
- (2A) - Modified Concrete Barrier
- (10) - Guardrail, W-Beam
- (FT) - FT Wall on MSE Wall
- [Hatched] - Wetlands (Impacted)
- [Dotted] - Wetlands (Do Not Disturb)
- [Dashed] - Streams

RECOMMENDED FOR APPROVAL		DESIGN ENGINEER	DATE
DESIGNED: WJW	DRAWN: SJC		
CHECKED: DCK	CHECKED: WJW		

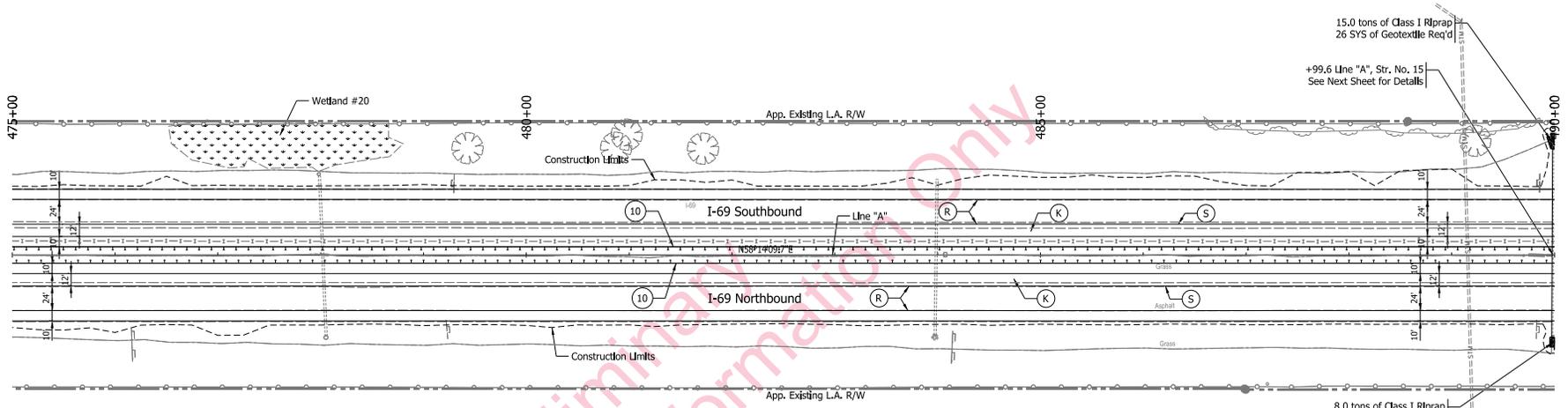
INDIANA
DEPARTMENT OF TRANSPORTATION

PLAN SHEET
STA. 460+00.00 TO STA. 475+00.00
LINE "A"

HORIZONTAL SCALE	BRIDGE FILE
1"=50'	
VERTICAL SCALE	DESIGNATION
	1383332
SURVEY BOOK	SHEETS
	of 1
CONTRACT	PROJECT
R-37053	1383332

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23-OCT-2014

Sec. 27, T-18-N, R-5-E
Fall Creek Township
Hamilton County



Sec. 27, T-18-N, R-5-E
Fall Creek Township
Hamilton County

Note:
All R/W this sheet to be as shown. All R/W on this sheet described from Line "A".

- (K) - Full Depth, HMA Pavement on Subgrade Treatment, Type _
- (R) - Scarification/Profile and Resurfacing
- (S) - Saw Cut
- (2) - 45 In. Concrete Barrier
- (2A) - Modified Concrete Barrier
- (10) - Guardrail, W-Beam
- (FT) - FT Fall on MSE Wall
- Wetlands (Impacted)
- Wetlands (Do Not Disturb)
- Streams

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE	
DESIGNED: WJW	DRAWN: SJC		
CHECKED: DCK	CHECKED: WJW		

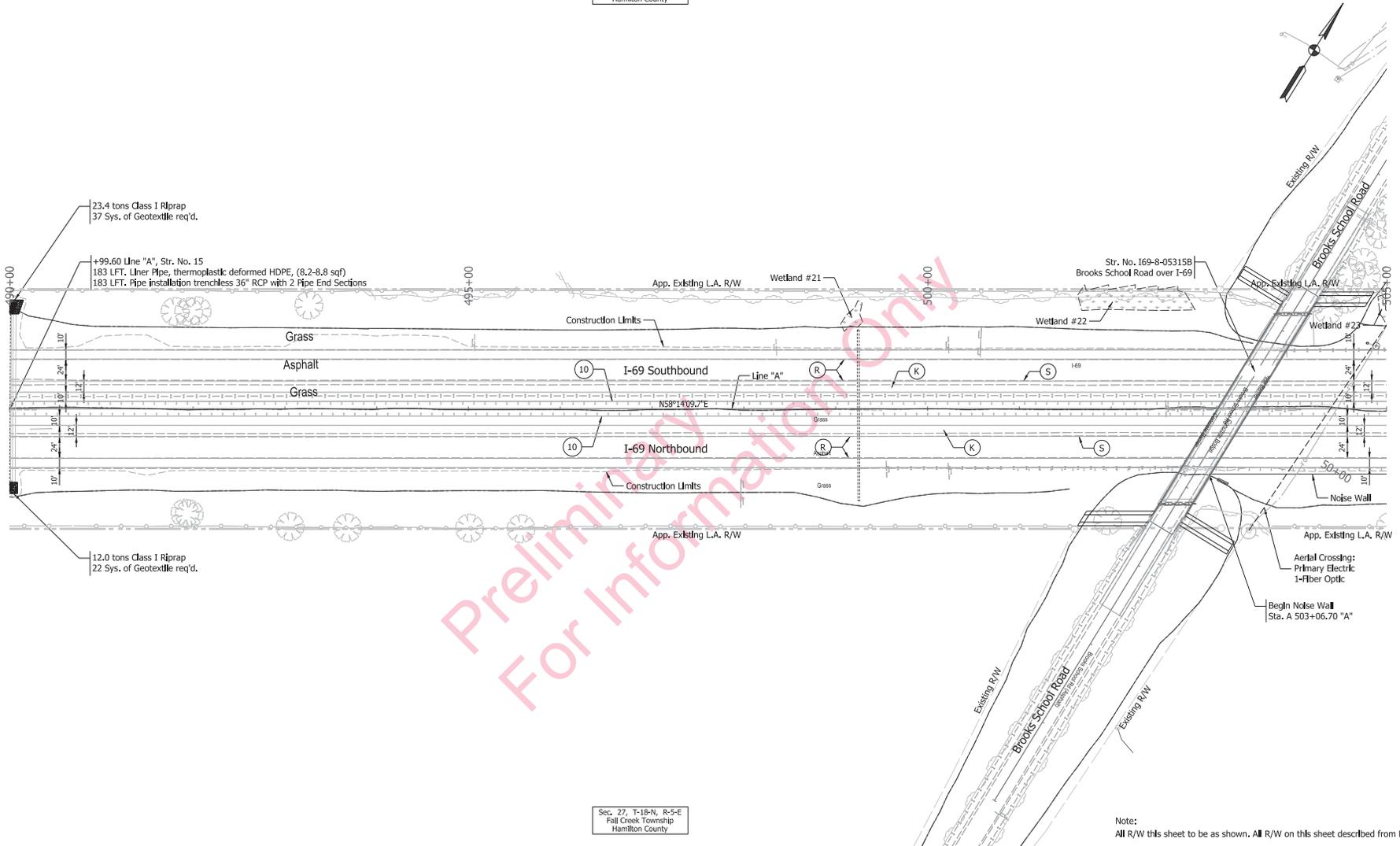
INDIANA
DEPARTMENT OF TRANSPORTATION

PLAN SHEET
STA. 475+00.00 TO STA. 490+00.00
LINE "A"

HORIZONTAL SCALE	BRIDGE FILE
1"=50'	
VERTICAL SCALE	DESIGNATION
	1383332
SURVEY BOOK	SHEETS
	of 1
CONTRACT	PROJECT
R-37053	1383332

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23-OCT-2014

Sec. 27, T-18-N, R-5-E
Fall Creek Township
Hamilton County



Preliminary
For Information Only

Sec. 27, T-18-N, R-5-E
Fall Creek Township
Hamilton County

Note:
All R/W this sheet to be as shown. All R/W on this sheet described from Line "A".

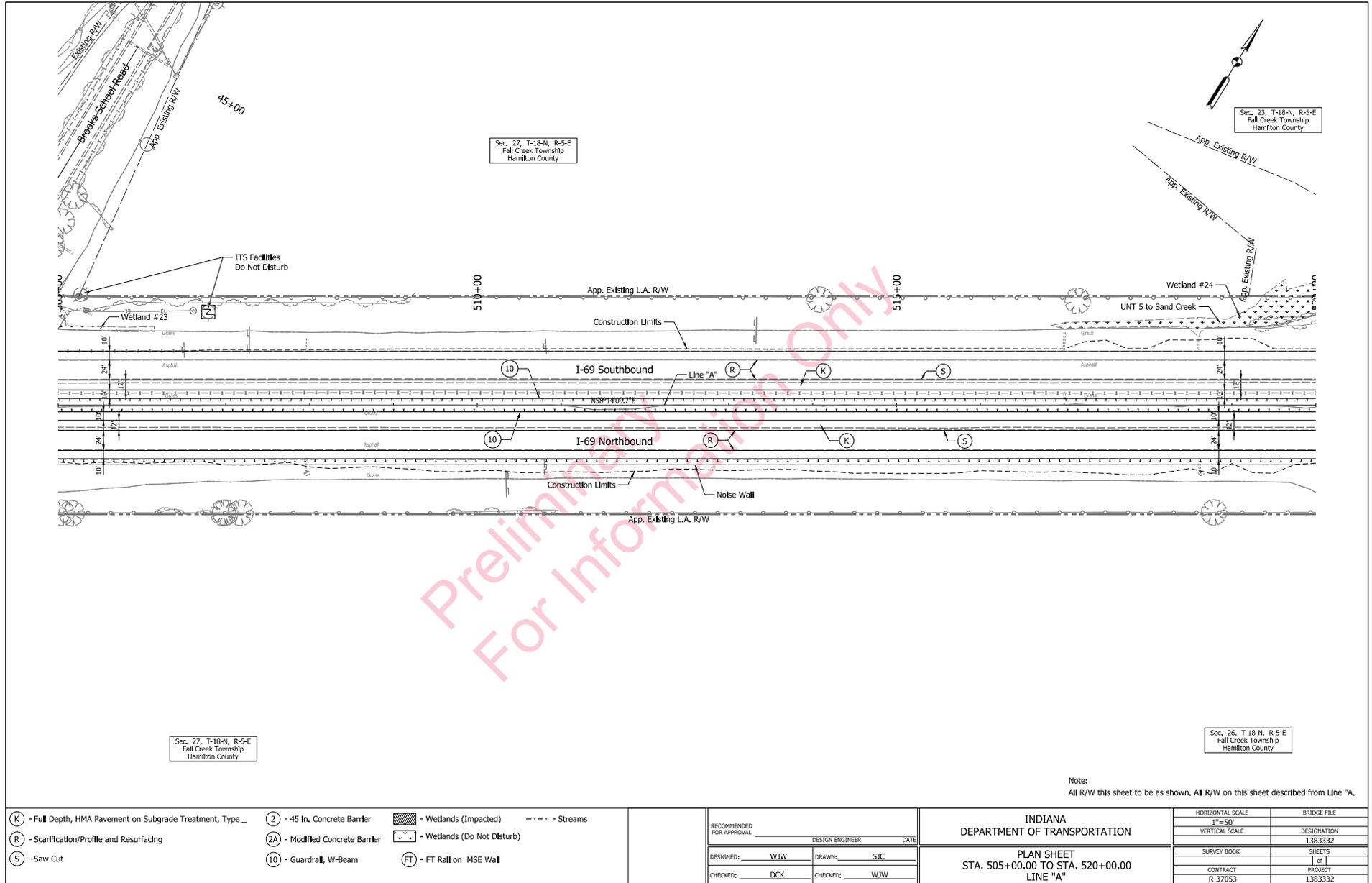
- | | | | |
|--|----------------------------------|--------------------------------------|-------------------|
| (K) - Full Depth, HMA Pavement on Subgrade Treatment, Type _ | (2) - 45 In. Concrete Barrier | [Hatched] - Wetlands (Impacted) | - - - - - Streams |
| (R) - Scarification/Profile and Resurfacing | (2A) - Modified Concrete Barrier | [Dotted] - Wetlands (Do Not Disturb) | |
| (S) - Saw Cut | (10) - Guardrail, W-Beam | (FT) - FT Rail on MSE Wall | |

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: WJW	DRAWN: SJC	
CHECKED: DCK	CHECKED: WJW	

INDIANA
DEPARTMENT OF TRANSPORTATION

PLAN SHEET
STA. 490+00.00 TO STA. 505+00.00
LINE "A"

HORIZONTAL SCALE	BRIDGE FILE
1" = 50'	
VERTICAL SCALE	DESIGNATION
	1383332
SURVEY BOOK	SHEETS
	of
CONTRACT	PROJECT
R-37053	1383332



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23-OCT-2014

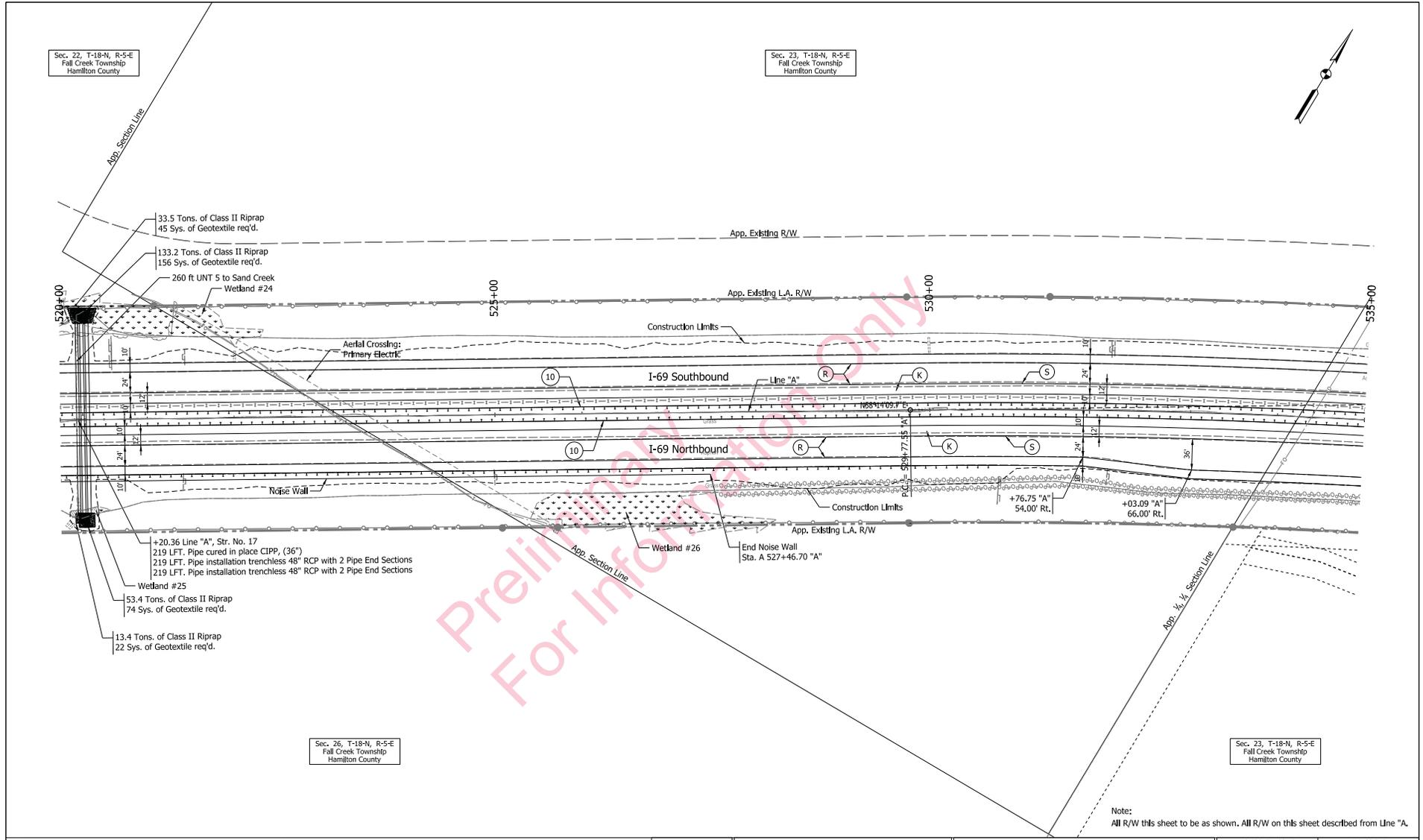
- | | | | |
|--|----------------------------------|-------------------------------|-------------------|
| (K) - Full Depth, HMA Pavement on Subgrade Treatment, Type _ | (2) - 45 In. Concrete Barrier | ▨ - Wetlands (Impacted) | - - - - - Streams |
| (R) - Scarification/Profile and Resurfacing | (2A) - Modified Concrete Barrier | ▨ - Wetlands (Do Not Disturb) | |
| (S) - Saw Cut | (10) - Guardrail, W-Beam | (FT) - FT Fall on MSE Wall | |

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: WJW	DRAWN: SJC	
CHECKED: DCK	CHECKED: WJW	

INDIANA
DEPARTMENT OF TRANSPORTATION

PLAN SHEET
STA. 505+00.00 TO STA. 520+00.00
LINE "A"

HORIZONTAL SCALE	BRIDGE FILE
1"=50'	
VERTICAL SCALE	DESIGNATION
	1383332
SURVEY BOOK	SHEETS
	of 1
CONTRACT	PROJECT
R-37053	1383332



Note:
All R/W this sheet to be as shown. All R/W on this sheet described from Line "A".

(K) - Full Depth, HMA Pavement on Subgrade Treatment, Type _	(2) - 45 In. Concrete Barrier	Wetlands (Impacted)	Streams
(R) - Scarification/Profile and Resurfacing	(2A) - Modified Concrete Barrier	Wetlands (Do Not Disturb)	
(S) - Saw Cut	(10) - Guardrail, W-Beam	(FT) - FT Fall on MSE Wall	

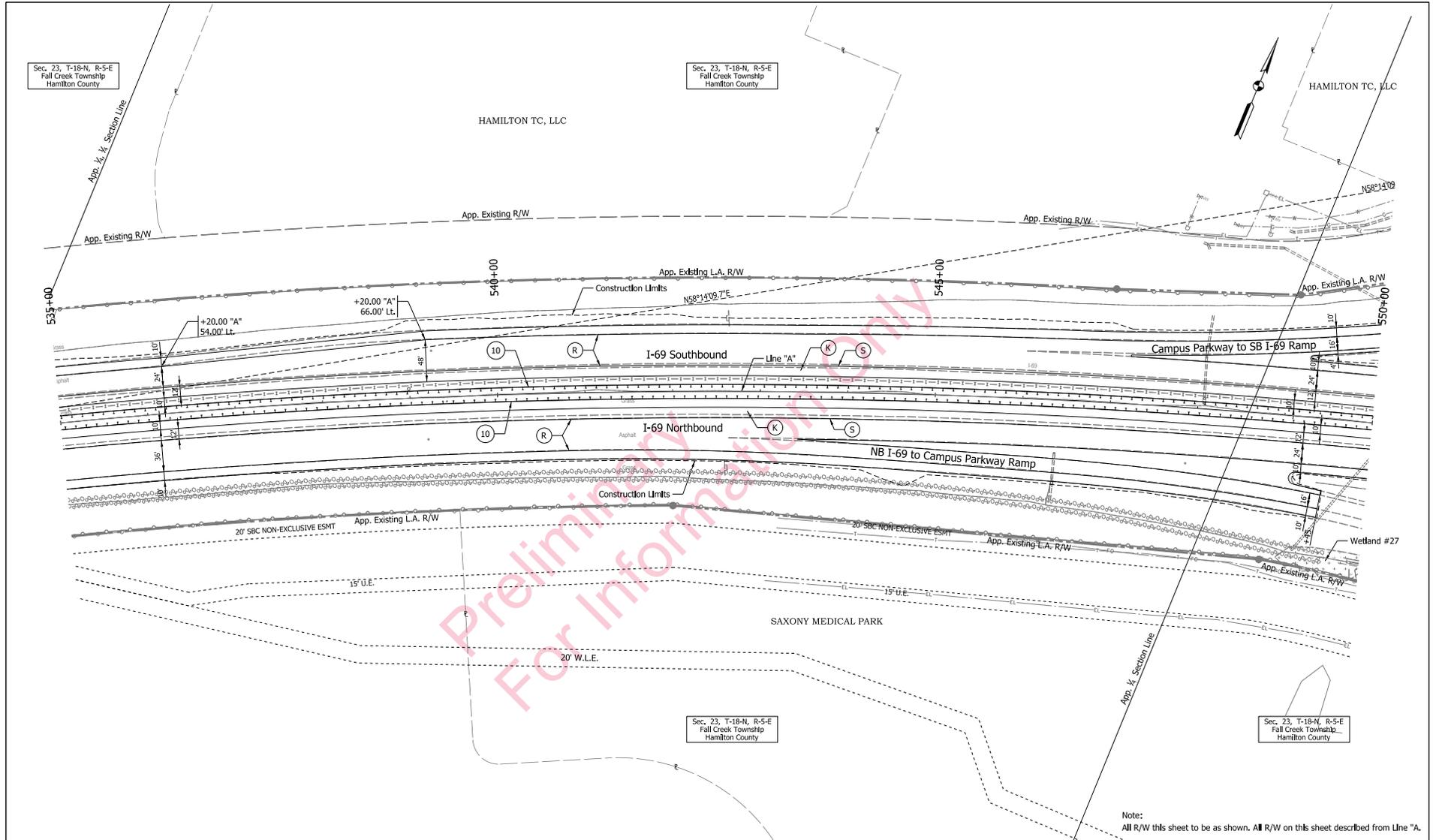
RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: WJW	DRAWN: SJC	
CHECKED: DCK	CHECKED: WJW	

INDIANA
DEPARTMENT OF TRANSPORTATION

PLAN SHEET
STA. 520+00.00 TO STA. 535+00.00
LINE "A"

HORIZONTAL SCALE	BRIDGE FILE
1"=50'	
VERTICAL SCALE	DESIGNATION
	1383332
SURVEY BOOK	SHEETS
	of 1
CONTRACT	PROJECT
R-37053	1383332

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23-OCT-2014



Note:
All R/W this sheet to be shown. All R/W on this sheet described from Line "A".

(K) - Full Depth, HMA Pavement on Subgrade Treatment, Type ...	(2) - 45 In. Concrete Barrier	Wetlands (Impacted)	Streams
(R) - Scarification/Profile and Resurfacing	(2A) - Modified Concrete Barrier	Wetlands (Do Not Disturb)	
(S) - Saw Cut	(10) - Guardrail, W-Beam	(FT) - FT Wall on MSE Wall	

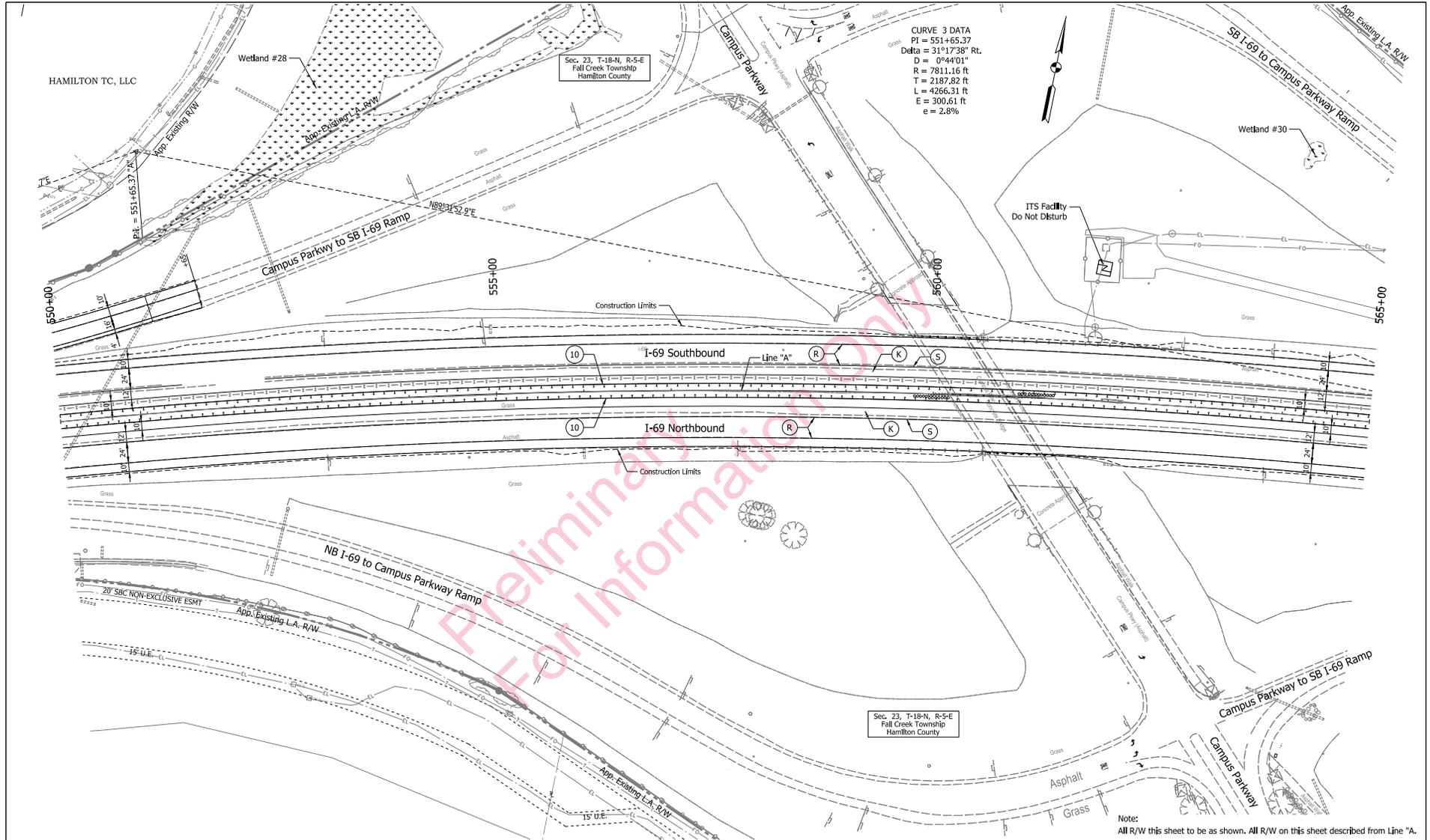
RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: WJW	DRAWN: SJC	
CHECKED: DCK	CHECKED: WJW	

INDIANA
DEPARTMENT OF TRANSPORTATION

PLAN SHEET
STA. 535+00.00 TO STA. 550+00.00
LINE "A"

HORIZONTAL SCALE	BRIDGE FILE
1"=50'	
VERTICAL SCALE	DESIGNATION
	1383332
SURVEY BOOK	SHEETS
	of 1
CONTRACT	PROJECT
R-37053	1383332

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23-OCT-2014



(K) - Full Depth, HMA Pavement on Subgrade Treatment, Type _	(2) - 45 In. Concrete Barrier	[Hatched] - Wetlands (Impacted)	- - - - - Streams
(R) - Scarification/Profile and Resurfacing	(2A) - Modified Concrete Barrier	[Dotted] - Wetlands (Do Not Disturb)	
(S) - Saw Cut	(10) - Guardrail, W-Beam	(FT) - FT Wall on MSE Wall	

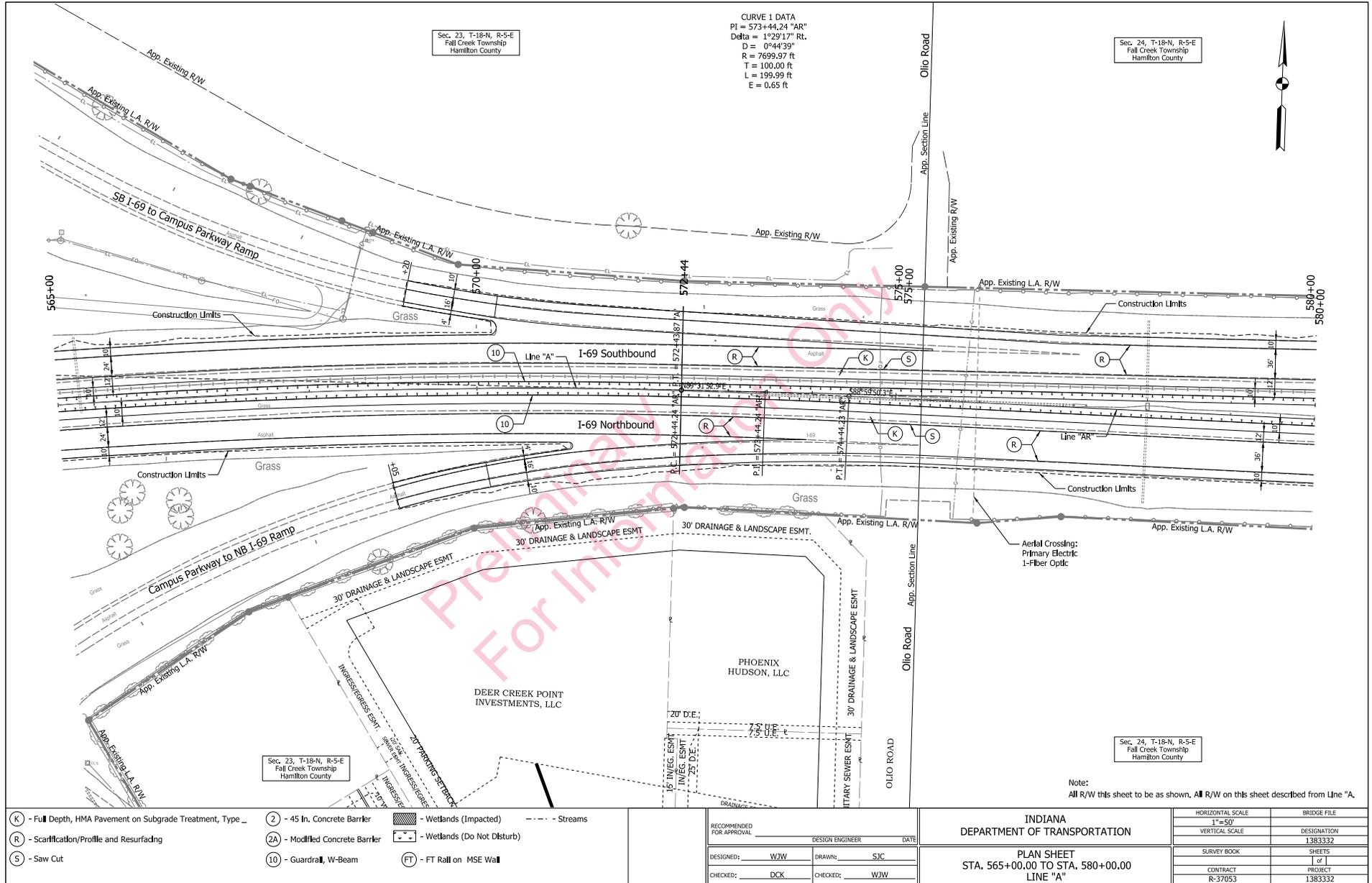
RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: WJW	DRAWN: SJC	
CHECKED: DCK	CHECKED: WJW	

INDIANA
DEPARTMENT OF TRANSPORTATION

PLAN SHEET
STA. 550+00.00 TO STA. 565+00.00
LINE "A"

HORIZONTAL SCALE	BRIDGE FILE
VERTICAL SCALE	DESIGNATION
SURVEY BOOK	SHEETS
CONTRACT	PROJECT
R-37053	1383332

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23-OCT-2014



CURVE 1 DATA
 PI = 573+44.24 "AR"
 Delta = 1°29'17"
 D = 0°44'39"
 R = 7699.97 ft
 T = 100.00 ft
 L = 199.99 ft
 E = 0.65 ft

Sec. 23, T-18-N, R-5-E
 Fall Creek Township
 Hamilton County

Sec. 24, T-18-N, R-5-E
 Fall Creek Township
 Hamilton County



Note:
 All R/W this sheet to be as shown, All R/W on this sheet described from Line "A".

- (K) - Full Depth, HMA Pavement on Subgrade Treatment, Type ...
- (R) - Scarification/Profile and Resurfacing
- (S) - Saw Cut
- (2) - 45 In. Concrete Barrier
- (2A) - Modified Concrete Barrier
- (10) - Guardrail, W-Beam
- (FT) - FT Fall on MSE Wall
- Wetlands (Impacted)
- Wetlands (Do Not Disturb)
- Streams

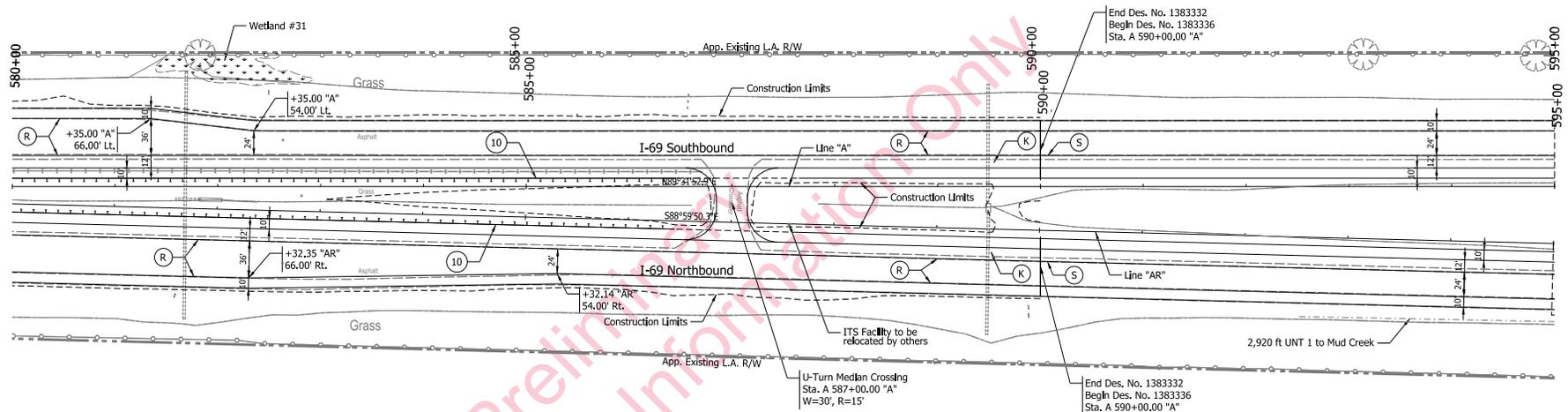
RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: WJW	DRAWING: SJC	
CHECKED: DCK	CHECKED: WJW	

INDIANA
 DEPARTMENT OF TRANSPORTATION
 PLAN SHEET
 STA. 565+00.00 TO STA. 580+00.00
 LINE "A"

HORIZONTAL SCALE	BRIDGE FILE
1"=50'	
VERTICAL SCALE	DESIGNATION
	1383332
SURVEY BOOK	SHEETS
	of 1
CONTRACT	PROJECT
R-37053	1383332

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 23-OCT-2014

Sec. 24, T-18-N, R-5-E
Fall Creek Township
Hamilton County



Sec. 24, T-18-N, R-5-E
Fall Creek Township
Hamilton County

Note:
All R/W ths sheet to be as shown. All R/W on this sheet described from Line "A".

- (K) - Full Depth, HMA Pavement on Subgrade Treatment, Type _
- (R) - Scarification/Profile and Resurfacing
- (S) - Saw Cut
- (2) - 45 In. Concrete Barrier
- (2A) - Modified Concrete Barrier
- (10) - Guardrail, W-Beam
- (FT) - FT Fall on MSE Wall
- [Hatched] - Wetlands (Impacted)
- [Dashed] - Wetlands (Do Not Disturb)
- [Dotted] - Streams

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: WJW	DRAWN: SJC	
CHECKED: DCK	CHECKED: WJW	

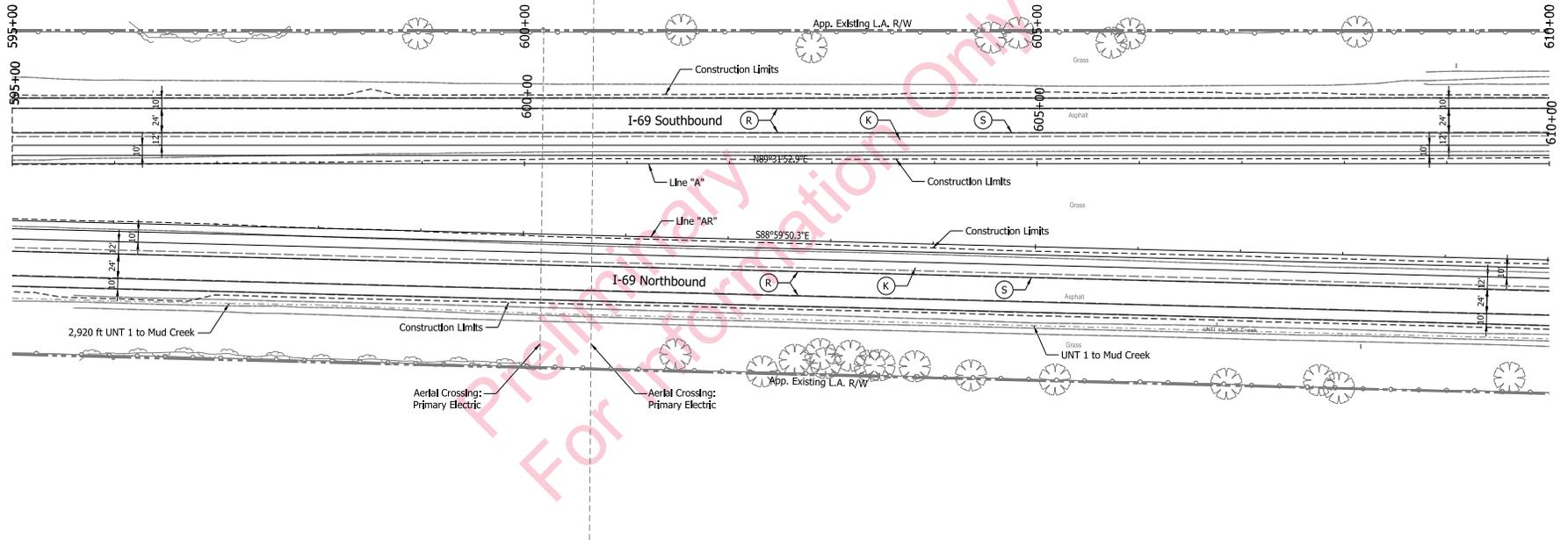
INDIANA
DEPARTMENT OF TRANSPORTATION

PLAN SHEET
STA. 580+00.00 TO STA. 595+00.00
LINE "A"

HORIZONTAL SCALE	BRIDGE FILE
1"=50'	
VERTICAL SCALE	DESIGNATION
	1383332, 1383336
SURVEY BOOK	SHEETS
	of 1
CONTRACT	PROJECT
R-37053	1383332, 1383336

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23-OCT-2014

Sec. 24, T-18-N, R-5-E
Fall Creek Township
Hamilton County



Sec. 24, T-18-N, R-5-E
Fall Creek Township
Hamilton County

Note:
All R/W this sheet to be as shown. All R/W on this sheet described from Line "A".

- (K) - Full Depth, HMA Pavement on Subgrade Treatment, Type _
- (R) - Scarification/Profile and Resurfacing
- (S) - Saw Cut
- (2) - 45 In. Concrete Barrier
- (2A) - Modified Concrete Barrier
- (10) - Guardrail, W-Beam
- (FT) - FT Fall on MSE Wall
- [Hatched Box] - Wetlands (Impacted)
- [Dashed Box] - Wetlands (Do Not Disturb)
- [Dotted Line] - Streams

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: WJW	DRAWN: SJC	
CHECKED: DCK	CHECKED: WJW	

INDIANA
DEPARTMENT OF TRANSPORTATION

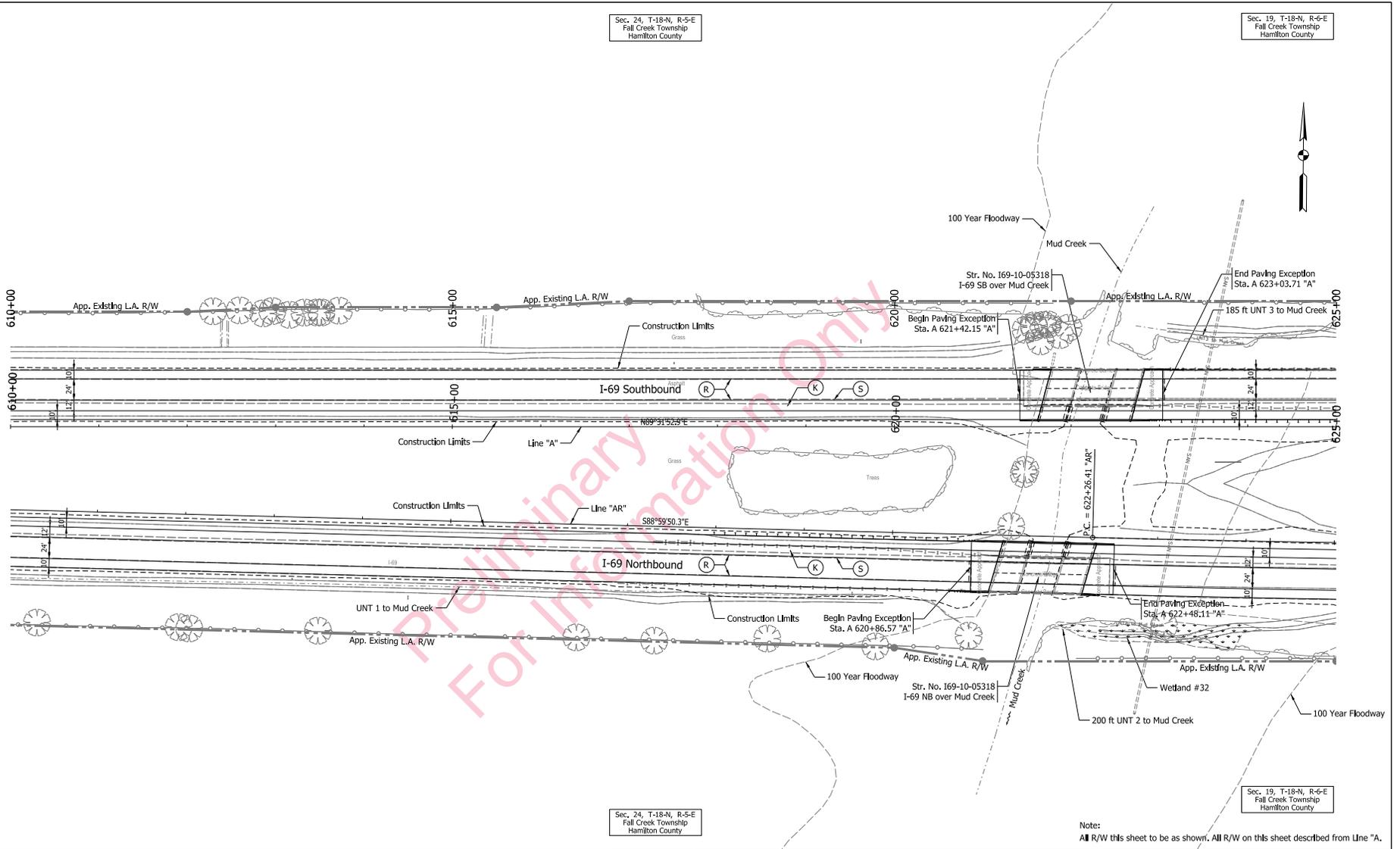
PLAN SHEET
STA. 595+00.00 TO STA. 610+00.00
LINE "A"

HORIZONTAL SCALE	BRIDGE FILE
1"=50'	DESIGNATION
VERTICAL SCALE	1383336
SURVEY BOOK	SHEETS
	of 1
CONTRACT	PROJECT
R-37053	1383336

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23-OCT-2014

Sec. 24, T-18-N, R-5-E
Fall Creek Township
Hamilton County

Sec. 19, T-18-N, R-6-E
Fall Creek Township
Hamilton County



Sec. 24, T-18-N, R-5-E
Fall Creek Township
Hamilton County

Sec. 19, T-18-N, R-6-E
Fall Creek Township
Hamilton County

Note: All R/W this sheet to be as shown. All R/W on this sheet described from Line "A".

(K) - Full Depth, HMA Pavement on Subgrade Treatment, Type ...	(2) - 45 In. Concrete Barrier	Wetlands (Impacted)	Streams
(R) - Scarification/Profile and Resurfacing	(2A) - Modified Concrete Barrier	Wetlands (Do Not Disturb)	
(S) - Saw Cut	(10) - Guardrail, W-Beam	(FT) - FT Roll on MSE Wall	

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: WJW	DRAWN: SJC	
CHECKED: DCK	CHECKED: WJW	

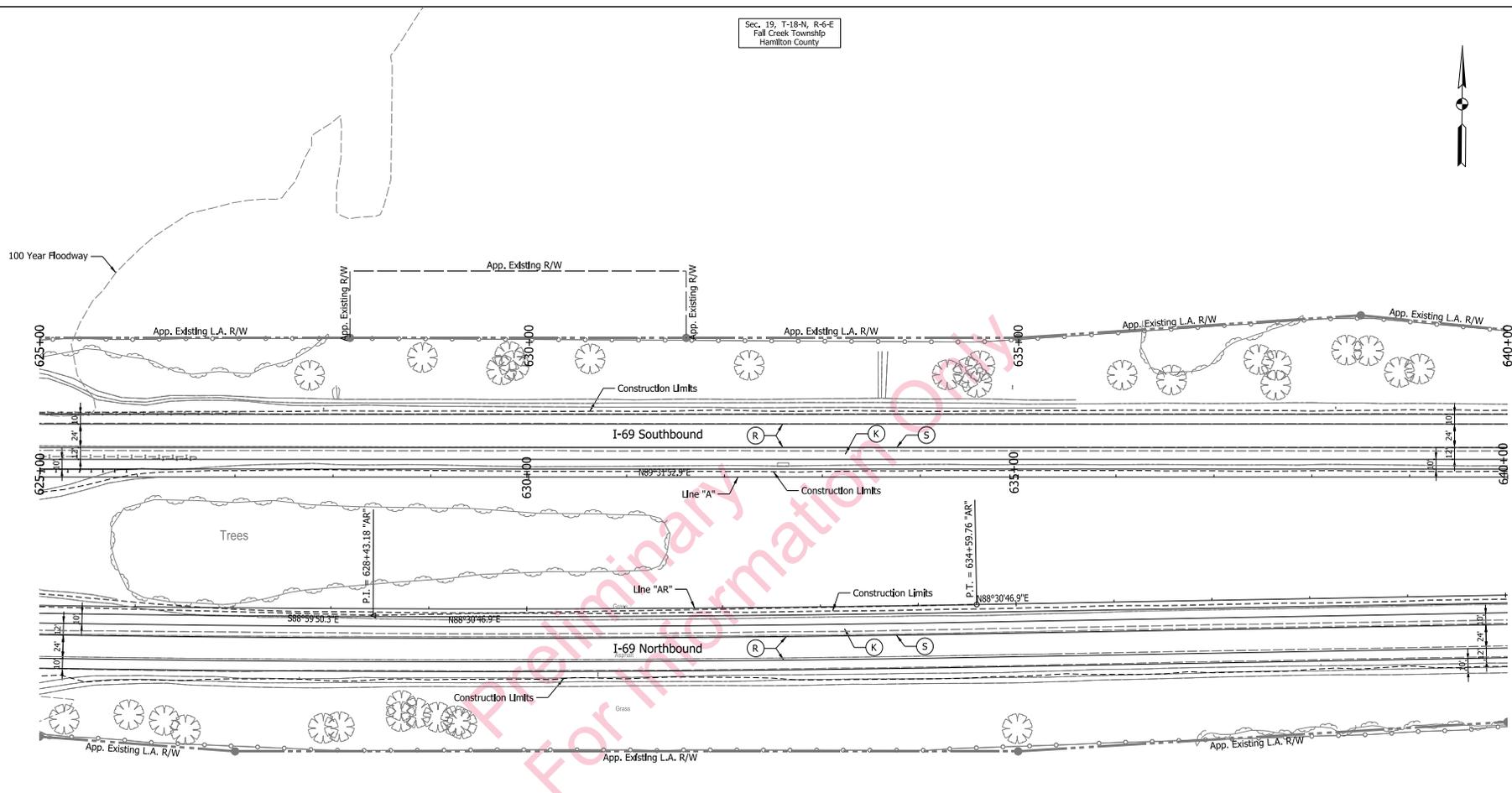
INDIANA
DEPARTMENT OF TRANSPORTATION

PLAN SHEET
STA. 610+00.00 TO STA. 625+00.00
LINE "A"

HORIZONTAL SCALE	BRIDGE FILE
1"=50'	
VERTICAL SCALE	DESIGNATION
	1383336
SURVEY BOOK	SHEETS
	of 1
CONTRACT	PROJECT
R-37053	1383336

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23-OCT-2014

Sec. 19, T-18-N, R-6-E
Fall Creek Township
Hamilton County



CURVE 2 DATA
PI = 628+43.18 "AR"
Delta = 2°29'34" Lt.
D = 0°12'08"
R = 28348.19 ft
T = 616.77 ft
L = 1233.35 ft
E = 6.71 ft

Sec. 19, T-18-N, R-6-E
Fall Creek Township
Hamilton County

Note:
All R/W this sheet to be as shown. All R/W on this sheet described from Line "A".

- (K) - Full Depth, HMA Pavement on Subgrade Treatment, Type ...
- (R) - Scarification/Profile and Resurfacing
- (S) - Saw Cut
- (2) - 45 In. Concrete Barrier
- (2A) - Modified Concrete Barrier
- (10) - Guardrail, W-Beam
- (FT) - FT Fall on MSE Wall
- Wetlands (Impacted)
- Wetlands (Do Not Disturb)
- Streams

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: WJW	DRAWN: SJC	
CHECKED: DCK	CHECKED: WJW	

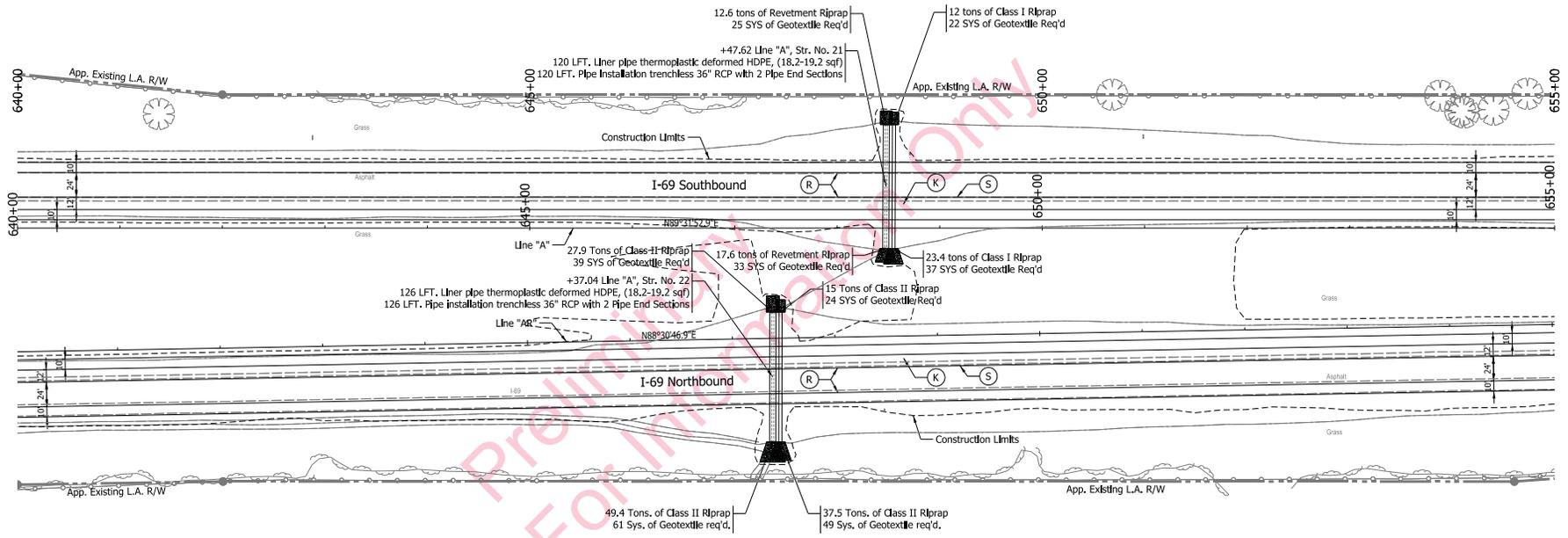
INDIANA
DEPARTMENT OF TRANSPORTATION

PLAN SHEET
STA. 625+00.00 TO STA. 640+00.00
LINE "A"

HORIZONTAL SCALE	BRIDGE FILE
1"=50'	
VERTICAL SCALE	DESIGNATION
	1383336
SURVEY BOOK	SHEETS
	of 1
CONTRACT	PROJECT
R-37053	1383336

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Sec. 19, T-18-N, R-6-E
Fall Creek Township
Hamilton County



Sec. 19, T-18-N, R-6-E
Fall Creek Township
Hamilton County

Note:
All R/W this sheet to be as shown. All R/W on this sheet described from Line "A".

- (K) - Full Depth, HMA Pavement on Subgrade Treatment, Type ...
- (R) - Scarification/Profile and Resurfacing
- (S) - Saw Cut
- (2) - 45 In. Concrete Barrier
- (2A) - Modified Concrete Barrier
- (10) - Guardrail, W-Beam
- (FT) - FT Fall on MSE Wall
- Wetlands (Impacted)
- Wetlands (Do Not Disturb)
- Streams

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: WJW	DRAWING: SJC	
CHECKED: DCK	CHECKED: WJW	

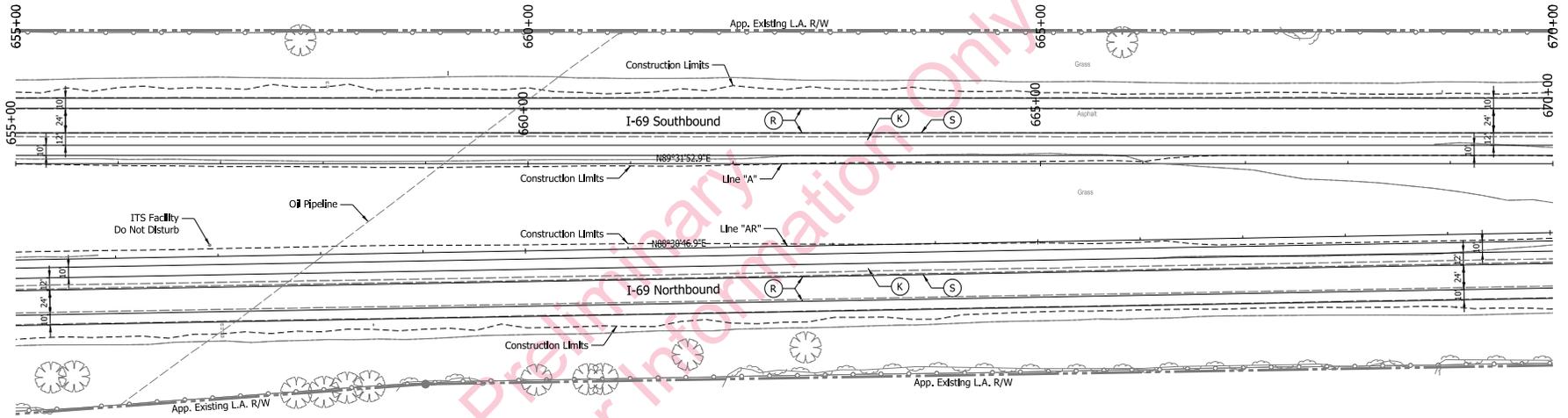
INDIANA
DEPARTMENT OF TRANSPORTATION

PLAN SHEET
STA. 640+00.00 TO STA. 655+00.00
LINE "A"

HORIZONTAL SCALE	BRIDGE FILE
1"=50'	
VERTICAL SCALE	DESIGNATION
	1383336
SURVEY BOOK	SHEETS
	of 1
CONTRACT	PROJECT
R-37053	1383336

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23-OCT-2014

Sec. 19, T-18-N, R-6-E
Fall Creek Township
Hamilton County



Sec. 19, T-18-N, R-6-E
Fall Creek Township
Hamilton County

Note:
All R/W this sheet to be as shown. All R/W on this sheet described from Line "A".

- (K) - Full Depth, HMA Pavement on Subgrade Treatment, Type _
- (R) - Scarification/Profile and Resurfacing
- (S) - Saw Cut
- (2) - 45 In. Concrete Barrier
- (2A) - Modified Concrete Barrier
- (10) - Guardrail, W-Beam
- (FT) - FT Fall on MSE Wall
- [Hatched] - Wetlands (Impacted)
- [Dashed] - Wetlands (Do Not Disturb)
- [Dotted] - Streams

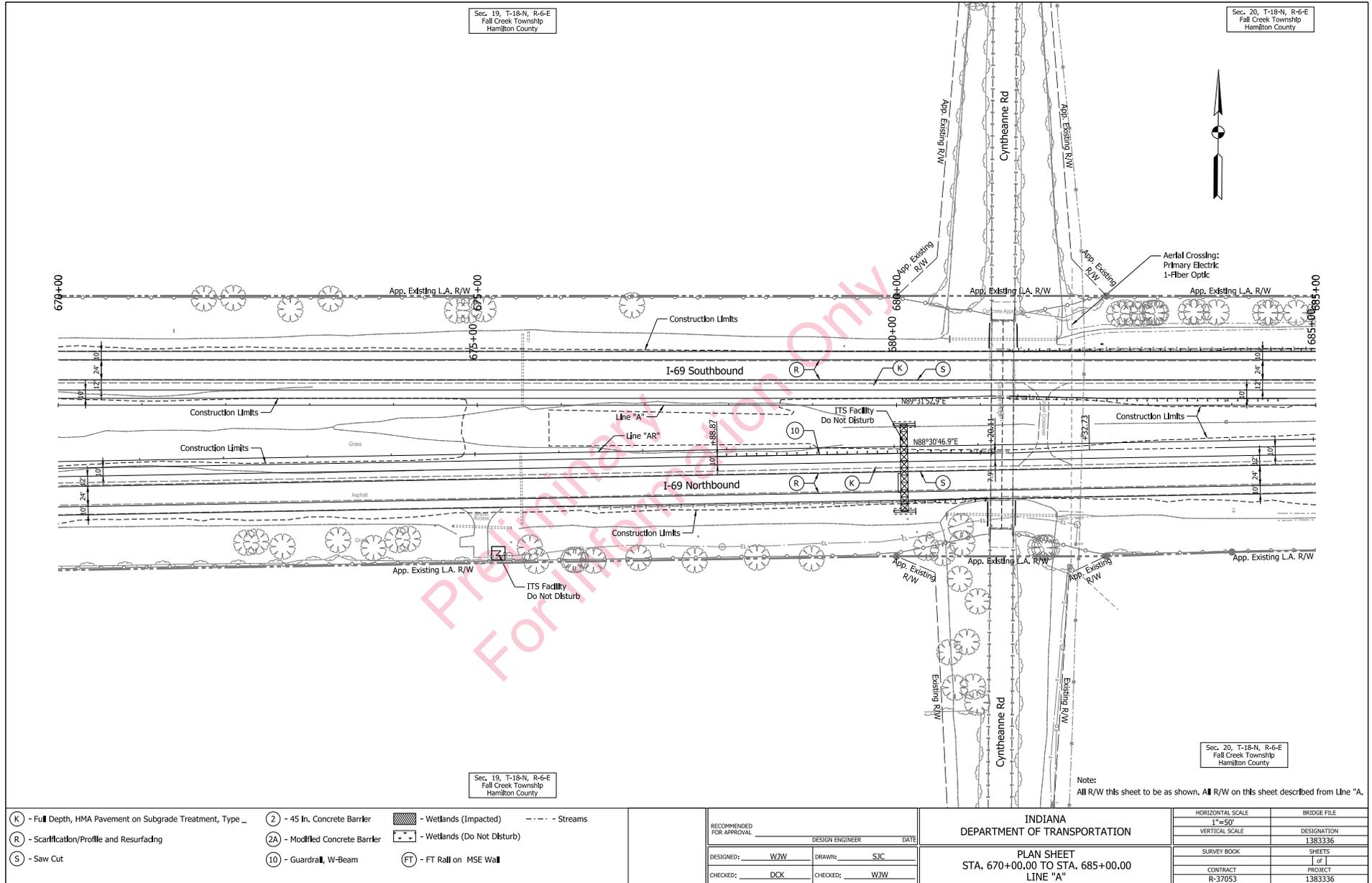
RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: WJW	DRAWN: SJC	
CHECKED: DCK	CHECKED: WJW	

INDIANA
DEPARTMENT OF TRANSPORTATION

PLAN SHEET
STA. 655+00.00 TO STA. 670+00.00
LINE "A"

HORIZONTAL SCALE	BRIDGE FILE
1"=50'	
VERTICAL SCALE	DESIGNATION
	1383336
SURVEY BOOK	SHEETS
	of 1
CONTRACT	PROJECT
R-37053	1383336

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23-OCT-2014



Sec. 19, T-18-N, R-6-E
Fall Creek Township
Hamilton County

Sec. 20, T-18-N, R-6-E
Fall Creek Township
Hamilton County

Sec. 19, T-18-N, R-6-E
Fall Creek Township
Hamilton County

Sec. 20, T-18-N, R-6-E
Fall Creek Township
Hamilton County

Note:
All R/W this sheet to be as shown. All R/W on this sheet described from Line "A".

- (K) - Full Depth, HMA Pavement on Subgrade Treatment, Type _
- (R) - Scarification/Profile and Resurfacing
- (S) - Saw Cut
- (2) - 45 In. Concrete Barrier
- (2A) - Modified Concrete Barrier
- (10) - Guardrail, W-Beam
- (FT) - FT Roll on MSE Wall
- [Hatched] - Wetlands (Impacted)
- [Dashed] - Wetlands (Do Not Disturb)
- [Dotted] - Streams

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: WJW	DRAWN: SJC	
CHECKED: DCK	CHECKED: WJW	

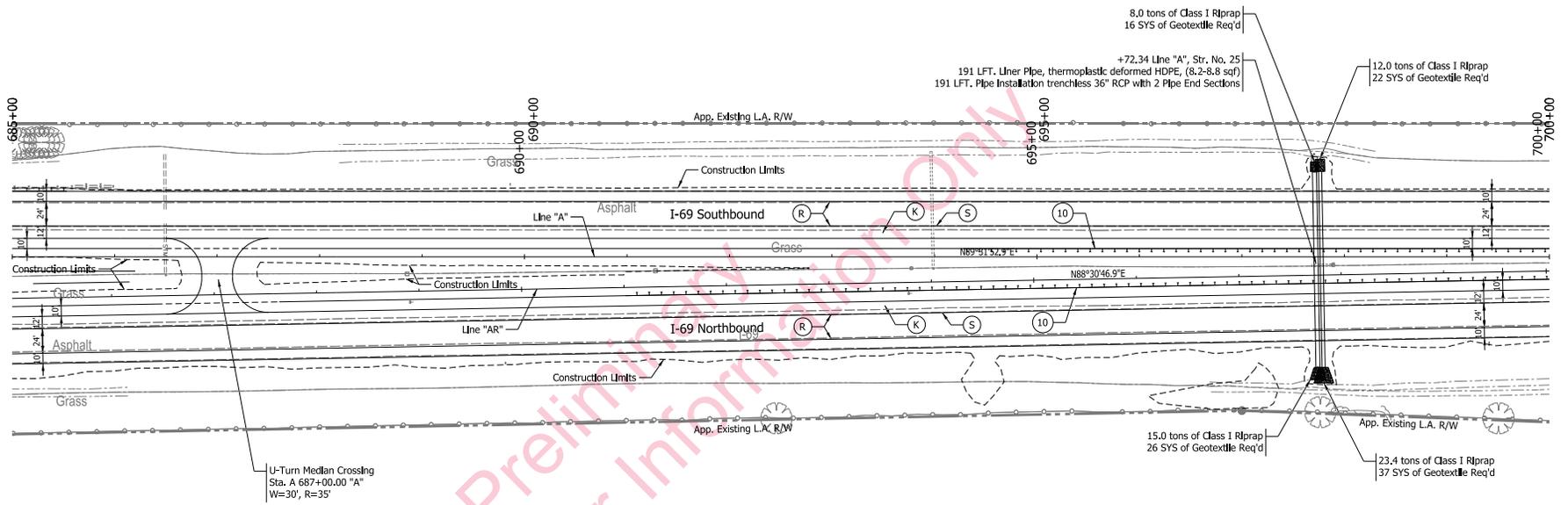
INDIANA
DEPARTMENT OF TRANSPORTATION

PLAN SHEET
STA. 670+00.00 TO STA. 685+00.00
LINE "A"

HORIZONTAL SCALE	BRIDGE FILE
1"=50'	
VERTICAL SCALE	DESIGNATION
	1383336
SURVEY BOOK	SHEETS
	of 1
CONTRACT	PROJECT
R-37053	1383336

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23-OCT-2014

Sec. 20, T-18-N, R-6-E
Fall Creek Township
Hamilton County



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Sec. 20, T-18-N, R-6-E
Fall Creek Township
Hamilton County

Note:
All R/W this sheet to be as shown. All R/W on this sheet described from Line "A".

- (K) - Full Depth, HMA Pavement on Subgrade Treatment, Type _
- (R) - Scarification/Profile and Resurfacing
- (S) - Saw Cut
- (2) - 45 In. Concrete Barrier
- (2A) - Modified Concrete Barrier
- (10) - Guardrail, W-Beam
- (FT) - FT Fall on MSE Wall
- [Hatched] - Wetlands (Impacted)
- [Dashed] - Wetlands (Do Not Disturb)
- [Dotted] - Streams

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: WJW	DRAWING: SJC	
CHECKED: DCK	CHECKED: WJW	

INDIANA
DEPARTMENT OF TRANSPORTATION

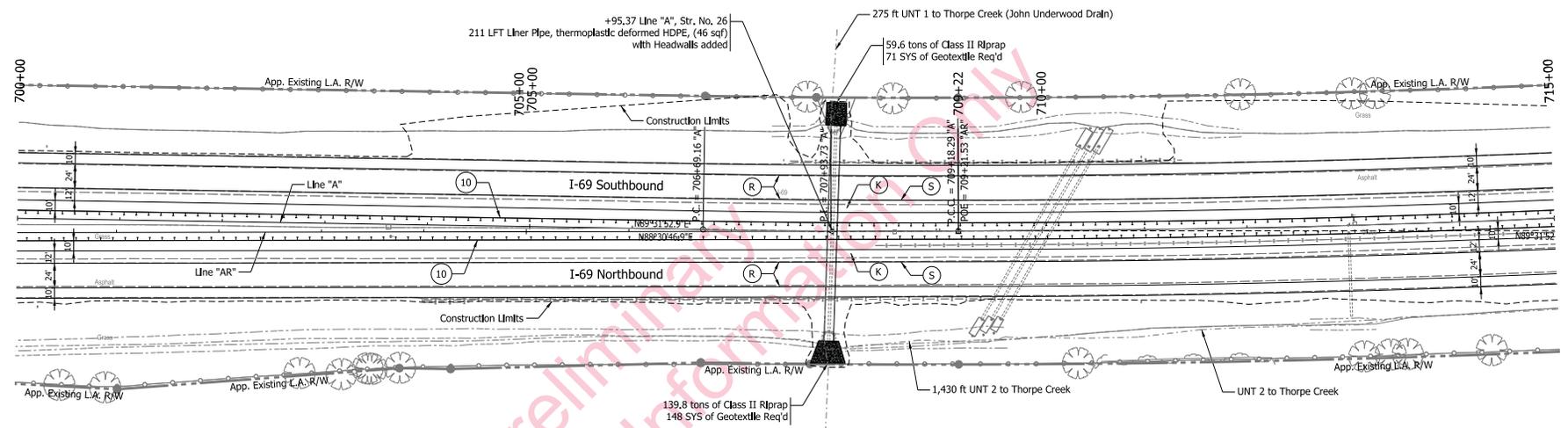
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STA. 685+00.00 TO STA. 700+00.00
LINE "A"

HORIZONTAL SCALE	BRIDGE FILE
1"=50'	
VERTICAL SCALE	DESIGNATION
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SURVEY BOOK	SHEETS
	of 1
CONTRACT	PROJECT
R-37053	1383336

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23-OCT-2014

CURVE 4 DATA
 PI = 707+93.73
 Delta = 1°13'16" Lt.
 D = 0°29'25"
 R = 11688.54 ft
 T = 124.57 ft
 L = 249.13 ft
 E = 0.66 ft
 e = 2.0%

Sec. 20, T-18-N, R-6-E
 Fall Creek Township
 Hamilton County



Sec. 20, T-18-N, R-6-E
 Fall Creek Township
 Hamilton County

Note:
 All R/W this sheet to be as shown. All R/W on this sheet described from Line "A".

- (K) - Full Depth, HMA Pavement on Subgrade Treatment, Type ...
- (R) - Scarification/Profile and Resurfacing
- (S) - Saw Cut
- (2) - 45 In. Concrete Barrier
- (2A) - Modified Concrete Barrier
- (10) - Guardrail, W-Beam
- (FT) - FT Fall on MSE Wall
- [Hatched] - Wetlands (Impacted)
- [Dashed] - Wetlands (Do Not Disturb)
- [Dotted] - Streams

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: WJW	DRAWING: SJC	
CHECKED: DCK	CHECKED: WJW	

INDIANA
 DEPARTMENT OF TRANSPORTATION

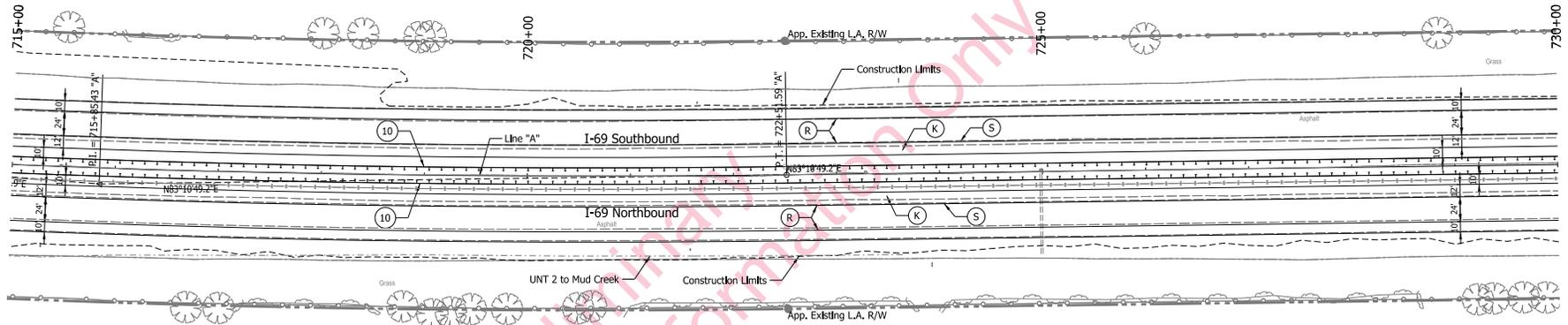
PLAN SHEET
 STA. 700+00.00 TO STA. 715+00.00
 LINE "A"

HORIZONTAL SCALE	BRIDGE FILE
1"=50'	
VERTICAL SCALE	DESIGNATION
	1383336
SURVEY BOOK	SHEETS
	of 1
CONTRACT	PROJECT
R-37053	1383336

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 23-OCT-2014

CURVE 5 DATA
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 Delta = 5°23'04" LT.
 D = 0°24'14"
 R = 14187.59 ft
 T = 667.14 ft
 L = 1333.29 ft
 E = 15.68 ft

Sec. 20, T-18-N, R-6-E
 Fall Creek Township
 Hamilton County



Sec. 20, T-18-N, R-6-E
 Fall Creek Township
 Hamilton County

Note:
 All R/W this sheet to be as shown. All R/W on this sheet described from Line "A".

- (K) - Full Depth, HMA Pavement on Subgrade Treatment, Type _
- (R) - Scarification/Profile and Resurfacing
- (S) - Saw Cut
- (2) - 45 In. Concrete Barrier
- (2A) - Modified Concrete Barrier
- (10) - Guardrail, W-Beam
- (FT) - FT Fall on MSE Wall
- Wetlands (Impacted)
- Wetlands (Do Not Disturb)
- Streams

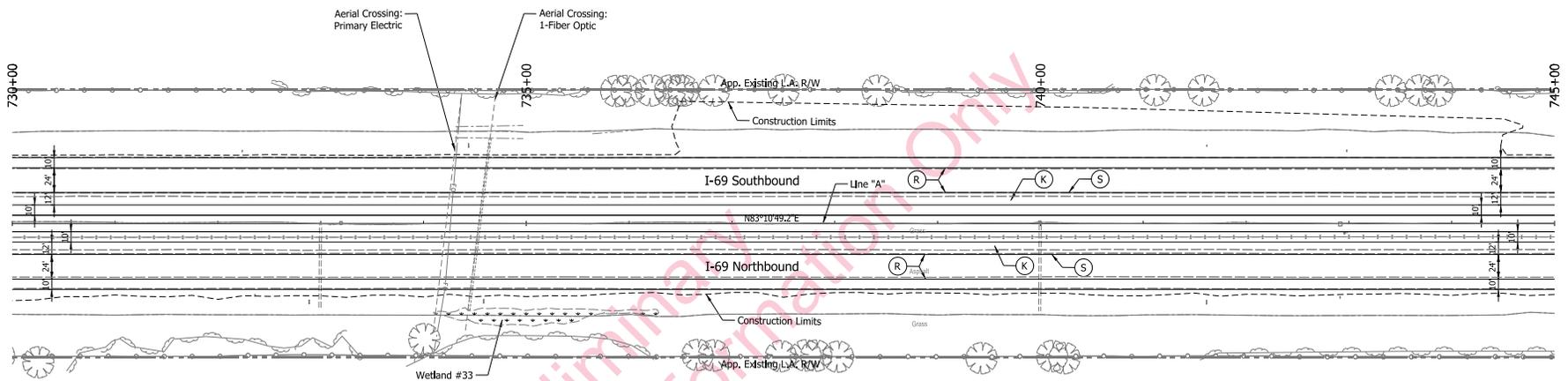
RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: WJW	DRAWN: SJC	
CHECKED: DCK	CHECKED: WJW	

INDIANA
 DEPARTMENT OF TRANSPORTATION
 PLAN SHEET
 STA. 715+00.00 TO STA. 730+00.00
 LINE "A"

HORIZONTAL SCALE	BRIDGE FILE
1"=50'	
VERTICAL SCALE	DESIGNATION
	1383336
SURVEY BOOK	SHEETS
	of 1
CONTRACT	PROJECT
R-37053	1383336

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Sec. 21, T-18-N, R-6-E
Fall Creek Township
Hamilton County



Preliminary
For Information Only

Sec. 21, T-18-N, R-6-E
Fall Creek Township
Hamilton County

Note:
All R/W this sheet to be as shown. All R/W on this sheet described from Line "A".

- (K) - Full Depth, HMA Pavement on Subgrade Treatment, Type _
- (R) - Scarification/Profile and Resurfacing
- (S) - Saw Cut
- (2) - 45 In. Concrete Barrier
- (2A) - Modified Concrete Barrier
- (10) - Guardrail, W-Beam
- (FT) - FT Fall on MSE Wall
- [Hatched] - Wetlands (Impacted)
- [Dashed] - Wetlands (Do Not Disturb)
- [Dotted] - Streams

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: WJW	DRAWN: SJC	
CHECKED: DCK	CHECKED: WJW	

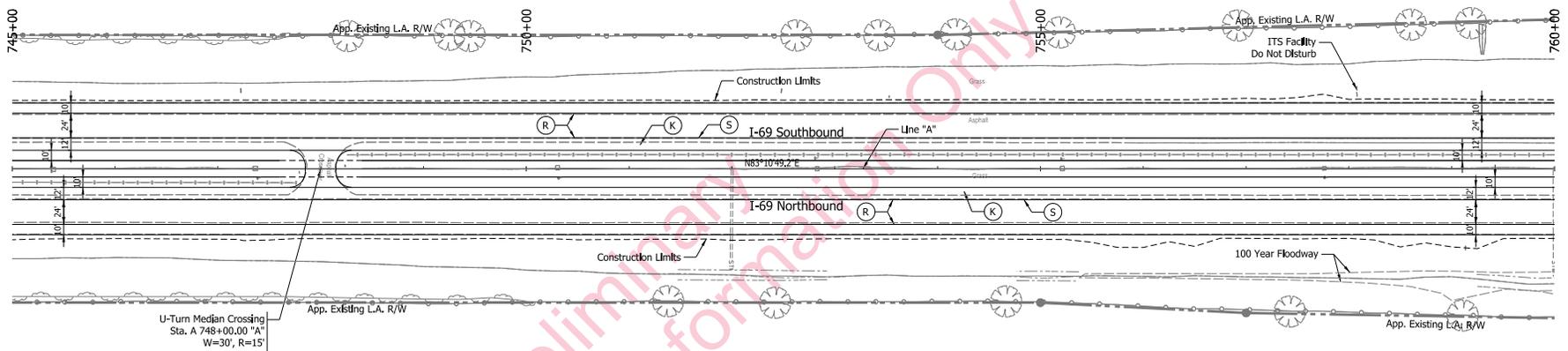
INDIANA
DEPARTMENT OF TRANSPORTATION

PLAN SHEET
STA. 730+00.00 TO STA. 745+00.00
LINE "A"

HORIZONTAL SCALE	BRIDGE FILE
1"=50'	
VERTICAL SCALE	DESIGNATION
	1383336
SURVEY BOOK	SHEETS
	of 1
CONTRACT	PROJECT
R-37053	1383336

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23-OCT-2014

Sec. 21, T-18-N, R-6-E
Green Township
Madison County



Preliminary
For Information Only

Sec. 21, T-18-N, R-6-E
Green Township
Madison County

Note:
All R/W this sheet to be as shown. All R/W on this sheet described from Line "A".

- (K) - Full Depth, HMA Pavement on Subgrade Treatment, Type _
- (R) - Scarification/Profile and Resurfacing
- (S) - Saw Cut
- (2) - 45 In. Concrete Barrier
- (2A) - Modified Concrete Barrier
- (10) - Guardrail, W-Beam
- (FT) - FT Rail on MSE Wall
- [Hatched] - Wetlands (Impacted)
- [Dashed] - Wetlands (Do Not Disturb)
- [Dotted] - Streams

RECOMMENDED FOR APPROVAL		DESIGN ENGINEER	DATE
DESIGNED BY: WJW	DRAWN BY: SJC		
CHECKED BY: DCK	CHECKED BY: WJW		

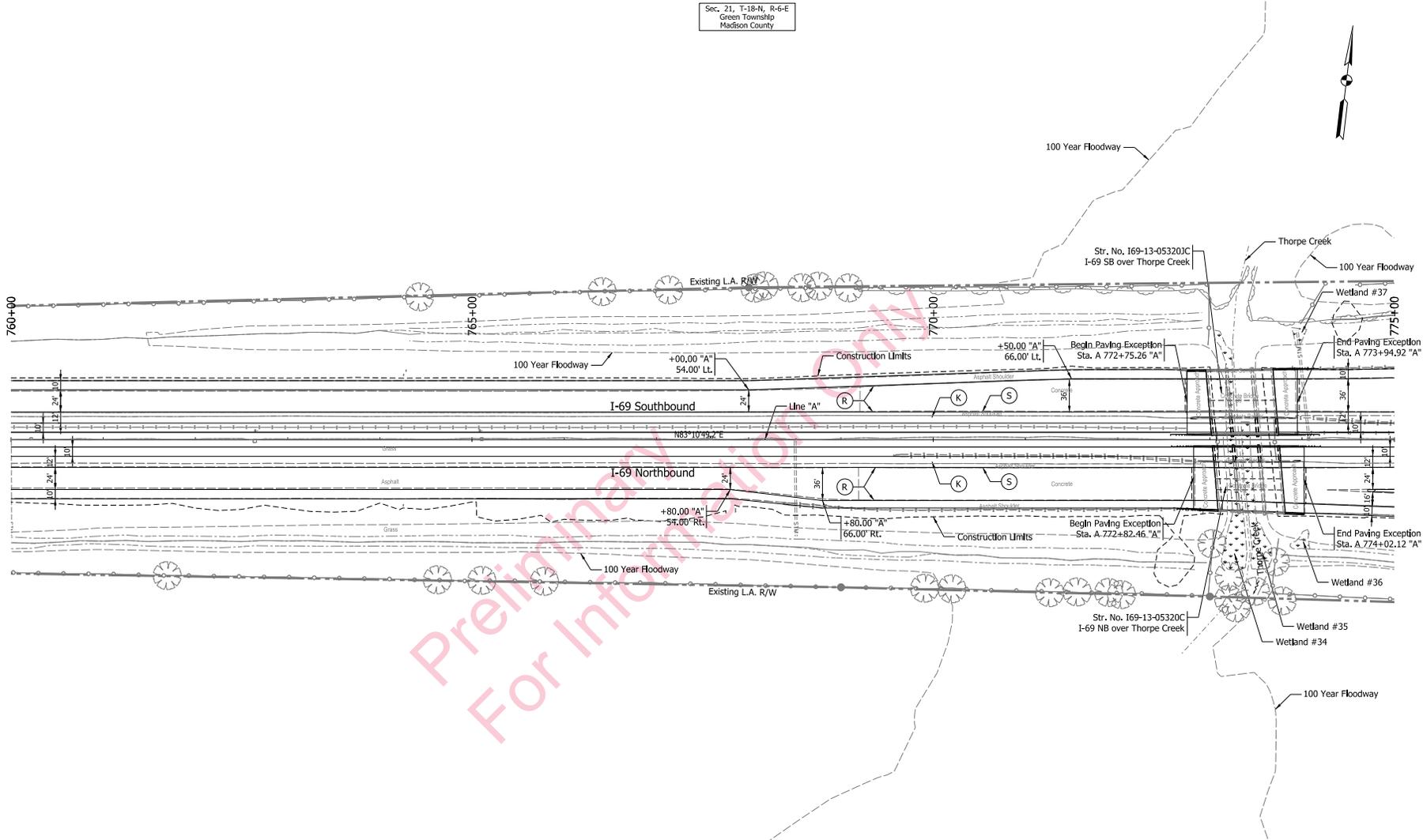
INDIANA
DEPARTMENT OF TRANSPORTATION

PLAN SHEET
STA. 745+00.00 TO STA. 760+00.00
LINE "A"

HORIZONTAL SCALE	BRIDGE FILE
1"=50'	
VERTICAL SCALE	DESIGNATION
	1383336
SURVEY BOOK	SHEETS
	of 1
CONTRACT	PROJECT
R-37053	1383336

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23-OCT-2014

Sec. 21, T-18-N, R-6-E
Green Township
Madison County



Sec. 21, T-18-N, R-6-E
Green Township
Madison County

Note:
All R/W this sheet to be as shown. All R/W on this sheet described from Line "A".

- (K) - Full Depth, HMA Pavement on Subgrade Treatment, Type _
- (R) - Scarification/Profile and Resurfacing
- (S) - Saw Cut
- (2) - 45 In. Concrete Barrier
- (2A) - Modified Concrete Barrier
- (10) - Guardrail, W-Beam
- (FT) - FT Fall on MSE Wall
- ▨ - Wetlands (Impacted)
- ▨ - Wetlands (Do Not Disturb)
- - Streams

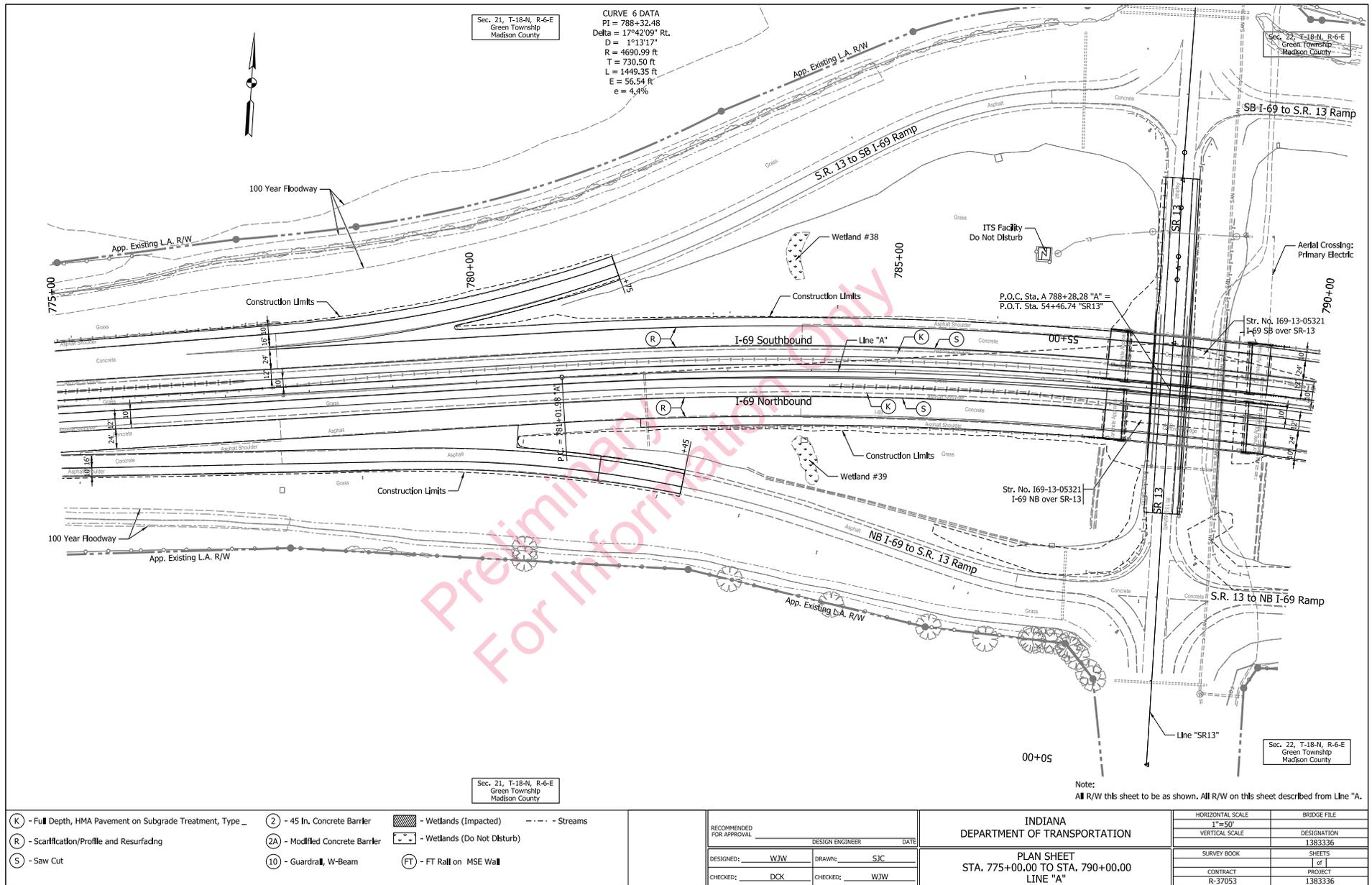
RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: WJW	DRAWN: SJC	
CHECKED: DCK	CHECKED: WJW	

INDIANA
DEPARTMENT OF TRANSPORTATION

PLAN SHEET
STA. 760+00.00 TO STA. 775+00.00
LINE "A"

HORIZONTAL SCALE	BRIDGE FILE
1"=50'	
VERTICAL SCALE	DESIGNATION
	1383336
SURVEY BOOK	SHEETS
	of 1
CONTRACT	PROJECT
R-37053	1383336

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23-OCT-2014



Sec. 21, T-18-N, R-6-E
Green Township
Madison County

CURVE 6 DATA
PI = 788+32.48
Delta = 17°42'09" Rt.
D = 1'13.17"
R = 4690.99 ft
T = 730.50 ft
L = 1449.35 ft
E = 56.54 ft
e = 4.4%

Sec. 22, T-18-N, R-6-E
Green Township
Madison County

Sec. 21, T-18-N, R-6-E
Green Township
Madison County

Sec. 22, T-18-N, R-6-E
Green Township
Madison County

Note:
All R/W this sheet to be as shown. All R/W on this sheet described from Line "A".

(K) - Full Depth, HMA Pavement on Subgrade Treatment, Type _	(2) - 45 In. Concrete Barrier	[Hatched] - Wetlands (Impacted)	- - - - - Streams
(R) - Scarification/Profile and Resurfacing	(2A) - Modified Concrete Barrier	[Dotted] - Wetlands (Do Not Disturb)	
(S) - Saw Cut	(10) - Guardrail, W-Beam	(FT) - FT Rail on MSE Wall	

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: WJW	DRAWING: SJC	
CHECKED: DCK	CHECKED: WJW	

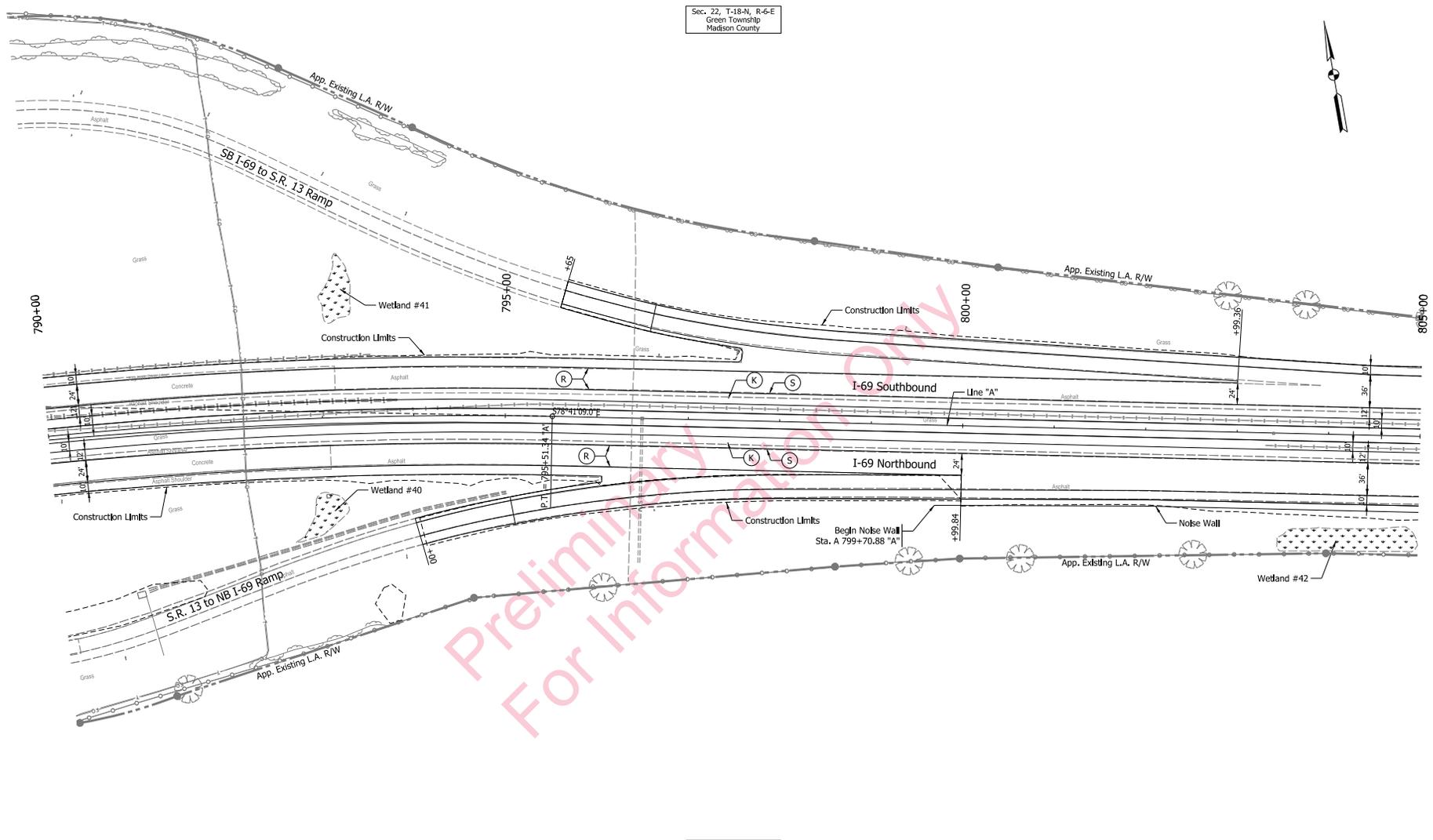
INDIANA
DEPARTMENT OF TRANSPORTATION

PLAN SHEET
STA. 775+00.00 TO STA. 790+00.00
LINE "A"

HORIZONTAL SCALE	BRIDGE FILE
VERTICAL SCALE	DESIGNATION
	1383336
SURVEY BOOK	SHEETS
	of 1
CONTRACT	PROJECT
R-37053	1383336

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23-OCT-2014

Sec. 22, T-18-N, R-6-E
Green Township
Madison County



Preliminary
For Information Only

Sec. 22, T-18-N, R-6-E
Green Township
Madison County

- (K) - Full Depth, HMA Pavement on Subgrade Treatment, Type _
- (R) - Scarification/Profile and Resurfacing
- (S) - Saw Cut
- (2) - 45 In. Concrete Barrier
- (2A) - Modified Concrete Barrier
- (10) - Guardrail, W-Beam
- (FT) - FT Rail on MSE Wall
- [Hatched] - Wetlands (Impacted)
- [Dotted] - Wetlands (Do Not Disturb)
- [Dashed] - Streams

RECOMMENDED FOR APPROVAL		DESIGN ENGINEER	DATE
DESIGNED BY: WJW	DRAWN BY: SJC		
CHECKED BY: DCK	CHECKED BY: WJW		

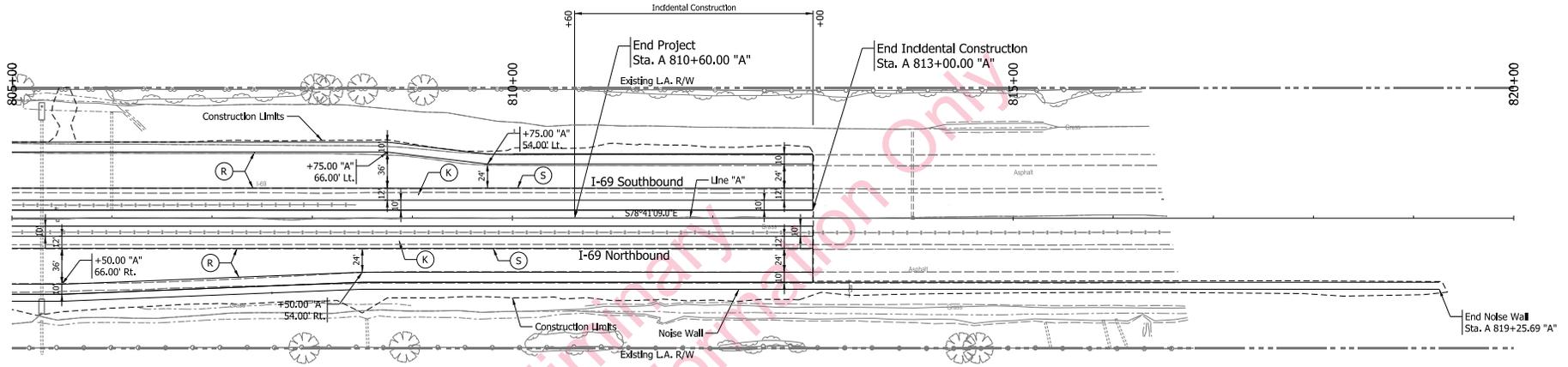
INDIANA
DEPARTMENT OF TRANSPORTATION

PLAN SHEET
STA. 790+00.00 TO STA. 805+00.00
LINE "A"

HORIZONTAL SCALE	BRIDGE FILE
1"=50'	
VERTICAL SCALE	DESIGNATION
	1383336
SURVEY BOOK	SHEETS
	of 1
CONTRACT	PROJECT
R-37053	1383336

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23-OCT-2014

Sec. 22, T-18-N, R-6-E
Green Township
Madison County



Preliminary
For Information Only

Sec. 22, T-18-N, R-6-E
Green Township
Madison County

Note:
All R/W this sheet to be as shown. All R/W on this sheet described from Line "A".

- (K) - Full Depth, HMA Pavement on Subgrade Treatment, Type _
- (R) - Scarification/Profile and Resurfacing
- (S) - Saw Cut
- (2) - 45 In. Concrete Barrier
- (2A) - Modified Concrete Barrier
- (10) - Guardrail, W-Beam
- (FT) - FT Rail on MSE Wall
- [Hatched] - Wetlands (Impacted)
- [Dashed] - Wetlands (Do Not Disturb)
- [Dotted] - Streams

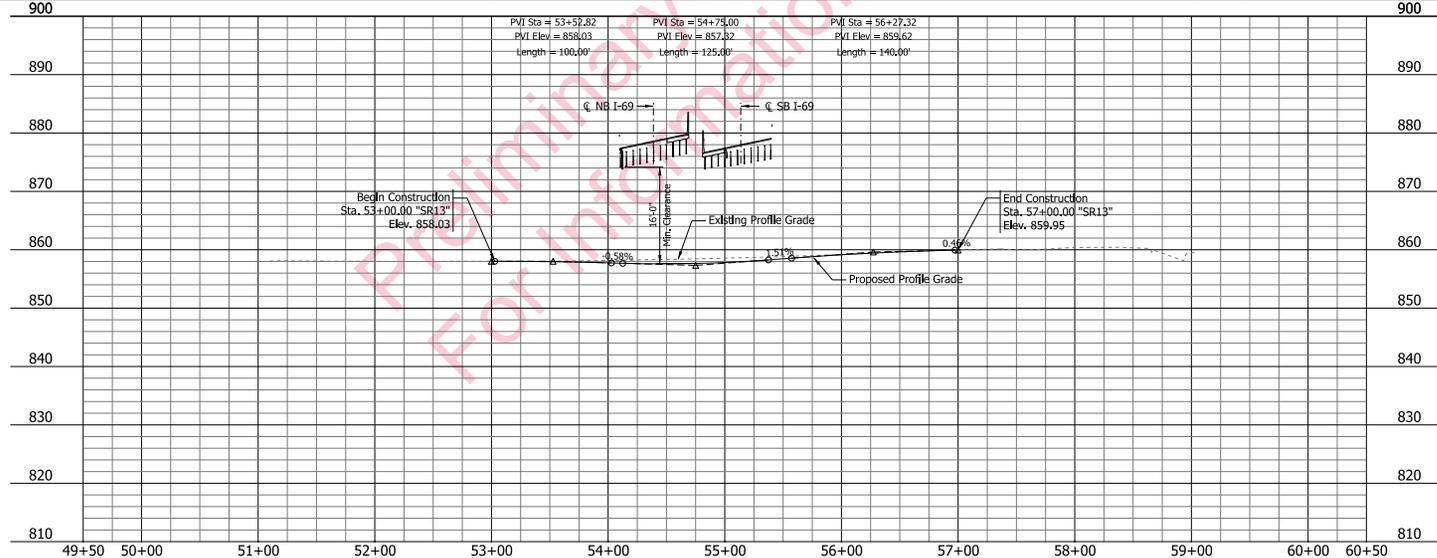
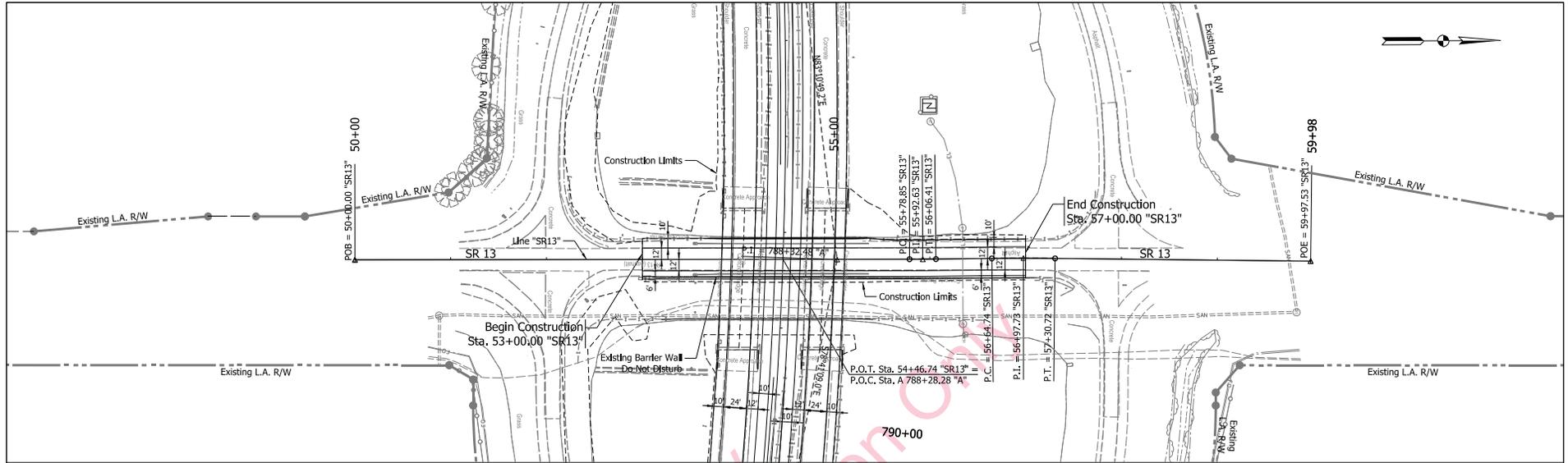
RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: WJW	DRAWN: SJC	
CHECKED: DCK	CHECKED: WJW	

INDIANA
DEPARTMENT OF TRANSPORTATION

PLAN SHEET
STA. 805+00.00 TO STA. 813+00.00
LINE "A"

HORIZONTAL SCALE	BRIDGE FILE
1"=50'	
VERTICAL SCALE	DESIGNATION
	1383336
SURVEY BOOK	SHEETS
	of 1
CONTRACT	PROJECT
R-37053	1383336

P:\PWP\dm9647\SPC_Plan_50_50.dgn
23-OCT-2014



(K) - Full Depth, HMA Pavement on Subgrade Treatment, Type _	(2) - 45 In. Concrete Barrier	Wetlands (Impacted)	Streams	RECOMMENDED FOR APPROVAL _____ DESIGN ENGINEER _____ DATE _____ DESIGNED: WJW DRAWING: SJC CHECKED: DCK CHECKED: WJW	INDIANA DEPARTMENT OF TRANSPORTATION PLAN & PROFILE SHEET STA. 53+00.00 TO STA. 57+00.00 LINE "SR13"	HORIZONTAL SCALE 1"=50'	BRIDGE FILE
(R) - Scarification/Profile and Resurfacing	(2A) - Modified Concrete Barrier	Wetlands (Do Not Disturb)	DESIGNATION 1383336				
(S) - Saw Cut	(10) - Guardrail, W-Beam	FT - FT Rail on MSE Wall	SURVEY BOOK			SHEETS of 1	
			CONTRACT R-37053			PROJECT 1383336	

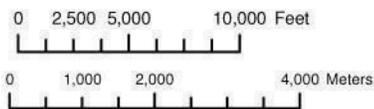
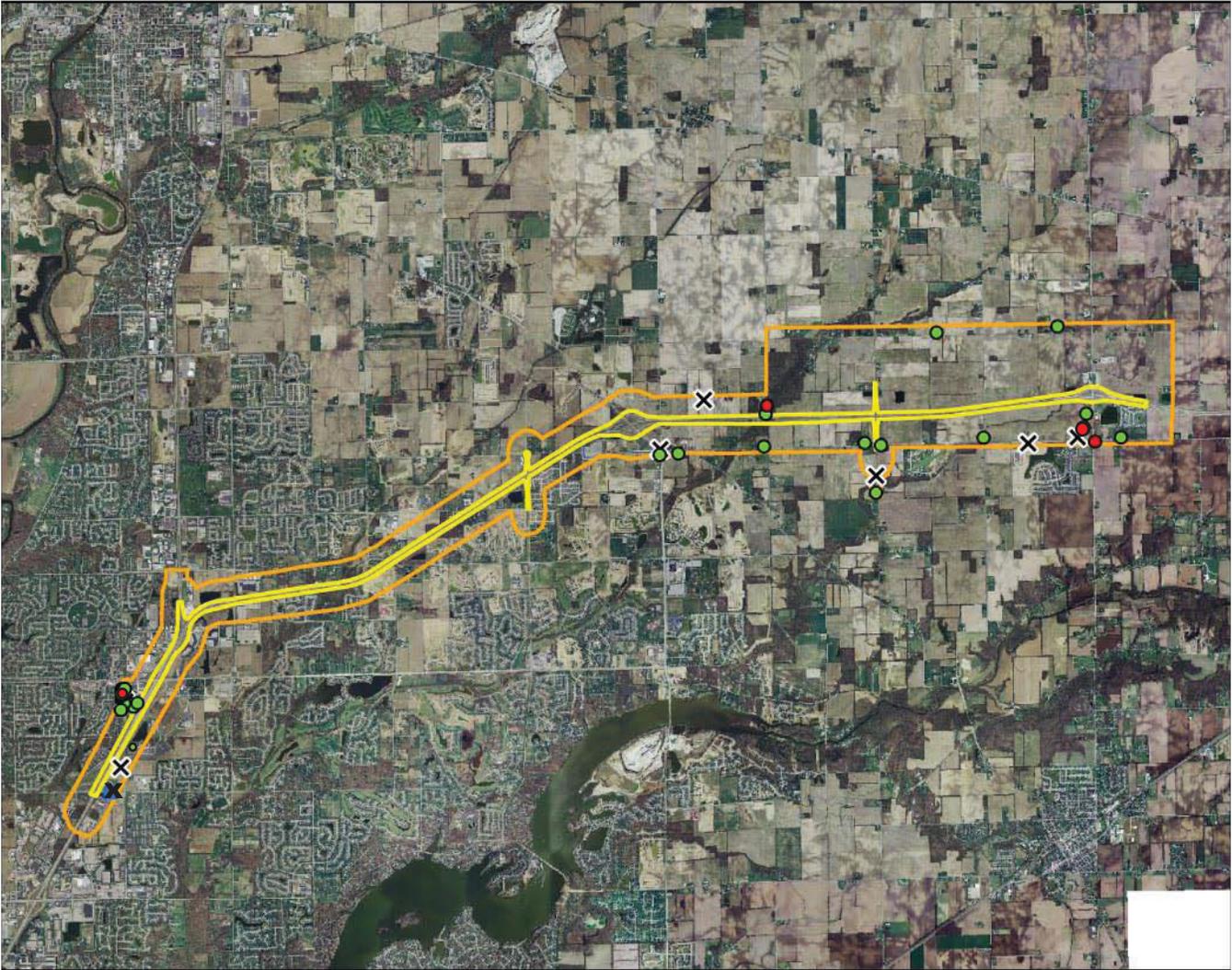
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23-OCT-2014

APPENDIX B. Maps

I-69 Added Travel Lanes from 106th Street to State Road 13
 Delaware, Fall Creek, & Wayne Townships, Hamilton County, Indiana
 Green Township, Madison County, Indiana
 Des. Nos.: 1383332 & 1383336
 2005 Aerial Photo



April 15, 2014



	Project Location
	APE
	Previously Determined NRHP-Eligible
	NRHP-Eligible
	Contributing Property
	No Longer Extant



WEINTRAUT & ASSOCIATES, INC.

Note: Information shown on this map is not warranted for accuracy or merchantability. GIS data used to create this map are from the best known sources existing at this time. However, experience shows that many national datasets are not all inclusive. Use of this map should be limited to planning, and should not replace field review or background checks with other sources. It is intended to serve as an aid in graphic representation only. This map does not represent a legal document.

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