

FHWA-Indiana Environmental Document
CATEGORICAL EXCLUSION LEVEL 1 FORM
GENERAL PROJECT INFORMATION

Road No./County:	SR 37/Monroe County
Designation Number(s):	1800371
Project Description/Termini:	Intersection Improvement at SR 37 and Dillman Road in Bloomington



CE Level 1 documentation for exempted projects



Additional Information to CE Level 1

Approval:

INDOT DE/ESD Signature and Date

DGD

2022.07.12 08:27:20

Release for Public Involvement:

-04'00'

INDOT DE/ESD Initials and Date

Certification of Public involvement:

INDOT Consultant Services Signature and Date

INDOT DE/ESD Reviewer:

Signature and Date

CE Preparer:

Mindy Baker, INDOT – Seymour District

Name and Organization

Indiana Department of Transportation

County Monroe Route SR 37 Des. No. 1800371

GENERAL PROJECT INFORMATION, DESCRIPTION, AND DESIGN INFORMATION	
Purpose and Need:	<p>Need: The intersection of Dillman Road and SR 37 has a crash rate that is higher than expected as analyzed over the preceding five-year period. 31% of crashes occurred from vehicles turning left off Dillman Road onto SR 37 in either direction.</p> <p>Purpose: The purpose of the project is to improve intersection safety for the traveling public by reducing conflict between SR 37 and Dillman Road traffic while also considering traffic delays and mobility.</p>
Project Description (Preferred Alternative):	<p>The project is located at the intersection of SR 37 and Dillman Road in Bloomington in Perry Township/Monroe County, Indiana. See Appendix B, page B-1 for a location map.</p> <p>Existing Conditions</p> <p>In the vicinity of the project, SR 37 runs north to south, and Dillman Road runs east to west. SR 37 is classified by INDOT as a rural principal arterial and is a Federal Highway Administration (FHWA) National Highway System (NHS) route. The posted speed limit on SR 37 is 55 mph. Through the project area SR 37 has two 12-foot through lanes in each direction with a 30-foot median. The cross section includes a 10-foot shoulder on both sides of the road. Dillman Road is classified by INDOT as a rural local road to the west and a rural minor arterial to the east of the intersection. The posted speed limit on Dillman Road is 30 mph. Through the project area Dillman Road has one 12-foot lane in each direction without paved shoulders.</p> <p>The existing intersection is unsignalized. SR 37 has two through lanes and dedicated left and right turn lanes for each approach. Dillman Road has one shared left/through/right turn lane for each approach. The traffic control at the intersection includes single stop signs on each Dillman Road approach, along with overhead one-section signal heads. The signal heads are set to flashing yellow for SR 37 and flashing red for Dillman Road. The signals are hung on span and catenary across strain poles in the northeast and southwest. There are no pedestrian or non-motorized facilities provided at the intersection or within the surrounding area. Land use in surrounding area is primarily commercial with some residential properties to the east.</p> <p>There was a total of 38 crashes at the intersection over the previous 5 years. An analysis of the crashes shows that 68% of the crashes have resulted in property damage, and 16% have resulted in an incapacitating injury. 31% of crashes occurred from vehicles turning left off Dillman Road onto SR 37 in either direction. This is a typical crash type at high speed rural divided highways, like this intersection, as motorists on the side road sometimes have difficulty judging the two-stage crossing through the median.</p>

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	<p><u>Preferred Alternative</u></p> <p>Based on the analysis of crash history, traffic operations, costs, and cost effectiveness related to mobility and safety, a Reduced Conflict Intersection (RCI) with Left Turns is the preferred alternative.</p> <p>Reduced Conflict Intersection (RCI) with left turns lanes: This alternative intersection design eliminates the Dillman Road through and left-turn movements at the main intersection. All Dillman Road traffic will turn right and traffic that previously turned left at the intersection will instead turn right and complete a U-turn at the crossovers. Left turns from SR 37 will be allowed and will yield to oncoming traffic. Traffic approaching from Dillman Road will be stop-controlled, and the crossovers will be yield controlled. Traffic approaching the intersection on SR 37 will not stop. The project has a length of approximately 2500 feet and SR 37 will be slightly widened at the crossovers (creating 'loons') to allow for longer trucks to complete the U-turn. Permanent lighting will be added at the intersection and the crossovers. Dillman Road will be temporarily closed in order to reconfigure the intersection to an RCI and access will be maintained using a detour.</p> <p>This alternative will increase safety by eliminating the Dillman Road direct left turns that account for 31% of crashes over the past five years while also allowing SR 37 traffic to freely flow without stopping. Based on the crash analysis, the preferred alternative has an annual expected crash reduction of 36.</p>
<p>Other Alternatives Considered:</p>	<p>No Build: This alternative involves doing nothing to the existing intersection. The intersection would still operate as a two-way stop-controlled intersection. The No Build alternative does not address the project's purpose and need and would allow safety issues to continue at the intersection. Therefore, the No Build alternative has been discarded.</p> <p>Boulevard left turn lane: This alternative intersection design is similar to the preferred alternative except the intersection of SR 37 and Dillman Road would be signalized while the crossovers would be stop-controlled for the indirect left-turn movements. This alternative is not ideal because the left-turn traffic may still attempt to make left-turns at the intersection. Based on the crash analysis, this alternative has an annual expected crash reduction of 36.</p> <p>RCI without left turn lanes: This alternative intersection design is similar to the preferred alternative except that it also prohibits the SR 37 direct left-turns and through movements at the main intersection. The median along SR 37 would be closed off. SR 37 traffic will not stop at any of the intersections. Based on the crash analysis, this alternative has an annual expected crash reduction of 29. Although this is a feasible alternative, the expected crash reduction is lower than the preferred alternative.</p> <p>Signalized Intersection: This alternative involves converting the existing two-way stop-controlled intersection into a signalized intersection. The traffic signal would operate as a three-phase signal with a protected left-turn phase</p>

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		<p>for SR 37 left-turns. The traffic signal would be similar to the intersection of SR 37 and Victor Pike, less than one mile north of Dillman Road. Based on the crash analysis, this alternative has an annual expected crash reduction of 36. Although this is a feasible alternative, it was not selected because of the low traffic volume on Dillman Road.</p> <p>Roundabout Intersection: This alternative involves converting the existing intersection into a roundabout. The roundabout would have two lanes on the SR 37 approaches and one lane on the Dillman Road approaches. The unbalanced traffic volumes between SR 37 and Dillman Road is not ideal for a roundabout intersection. Typically, a roundabout would have more balanced volumes on all approaches. Another consideration that makes this location not ideal is that drivers will not be expecting a roundabout on this stretch of the SR 37 corridor. Based on the crash analysis, this alternative has an annual expected crash reduction of 40.</p>			
Funding Source(s):		<input checked="" type="checkbox"/> Federal	<input checked="" type="checkbox"/> State	<input type="checkbox"/> Local	<input type="checkbox"/> Other
Project Sponsor:		INDOT			
Estimated Cost:		\$1,711,789 (FY2024)	Project Length:	2500 feet	
Public Involvement:				No:	Yes: X
<p>All work will be completed within the existing INDOT right of way; therefore, Notice of Entry letters were not required.</p> <p>Project Does Meet The project will meet the minimum requirements described in the current <i>Indiana Department of Transportation (INDOT) Public Involvement Manual</i> which requires the project sponsor to offer the public an opportunity to submit comments and/or request a public hearing. Therefore, a legal notice will appear in a local publication contingent upon the release of this document for public involvement. This document will be revised after the public involvement requirements are fulfilled.</p>					
Right-of-Way:				No: X	Yes:
<p>Right-of-Way (ROW) extends approximately 85 feet from the edge of pavement on either side of SR 37. This project will occur within existing right-of-way (ROW). No permanent or temporary ROW will be required for this project.</p> <p>If the scope of work or permanent or temporary right-of-way amounts change, the INDOT Environmental Services Division (ESD) and the INDOT District Environmental Section will be contacted immediately.</p>					
Maintenance of Traffic (MOT) During Construction:				No:	Yes: X
<p>The MOT for the project will require a temporary closure of Dillman Road. The project will be constructed in multiple phases. The crossover intersections north and south of the Dillman Road intersection will be constructed in the Phase 1 and concrete barriers will be used to protect the construction zone. During Phase 2, Dillman Road will be closed in order to reconfigure the intersection</p>					

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to an RCI. Access to Dillman Road will be maintained using a detour. The detour route will be Walnut Street for access east of SR 37 and Victor Pike for access west of SR 37.

The closures/lane restrictions will pose a temporary inconvenience to traveling motorists (including school buses and emergency services); however, no significant delays are anticipated, and all inconveniences and delays will cease upon project completion.

Bridge(s) and/or Small Structure(s) (include structure number(s)):	No: X	Yes:
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No bridges or small structures are located within the project area and impacts to bridges or small structures are not expected.

IDENTIFICATION AND EVALUATION OF IMPACTS

Early Coordination:

Early coordination letters were sent on November 4, 2021 (Appendix C, page C-1).

<u>Agency</u>	<u>Date Sent</u>	<u>Date Response Received</u>	<u>Appendix</u>
Forest Supervisor, US Forest Service	November 4, 2021	No Response	N/A
Indiana Geological and Water Survey	November 4, 2021	November 4, 2021	Appendix C, page 13
Environmental Coordinator, IDNR – DFW	November 4, 2021	December 3, 2021	Appendix C, page 4
IDEM	November 4, 2021	November 4, 2021	Appendix C, page 6
Bloomington/Monroe County Metropolitan Planning Organization	November 4, 2021	No Response	N/A
Erica Tait, FHWA	November 4, 2021	No Response	N/A
David Dye, INDOT – Seymour Environmental Section	November 4, 2021	No Response	N/A
Brad Williamson, INDOT – Seymour Project Manager	November 4, 2021	No Response	N/A
Monroe County MS4 Coordinator	November 13, 2020	November 16, 2020	Appendix C, page 16

All applicable recommendations are included in the Environmental Commitments section of this CE document.

Streams, Rivers, and Other Jurisdictional Features Impacted:	No: X	Yes:
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Based on the desktop review, the aerial map of the project area, and the Red Flag Investigation (RFI) report (Appendix E, page E-1 to 15) there are thirteen (13) streams, rivers, watercourse or other

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jurisdictional features within the 0.5 mile search radius. The site visit on June 14, 2021 by INDOT confirmed that there is one (1) stream, river, watercourse, or other jurisdictional features present within or adjacent to the project area.

Existing drainage within the project area is primarily maintained through an open drainage system on either side and in the median of SR 37. There is a small cross culvert that runs underneath Dillman Road just west of the intersection. This culvert carries drainage from the roadway ditch running alongside SR 37. A 9' x 7' pipe arch culvert runs from the southeast quadrant of the intersection to a point approximately 200' northwest of the intersection. The nearest jurisdictional stream feature, an unnamed tributary (UNT) to Clear Creek, flows northwest through this culvert and drains runoff from properties to the southeast. All of the work will occur along SR 37 and will not affect this stream or the culvert. Therefore, no impacts are expected.

IDNR-DFW responded on December 3, 2021 with recommendations to protect disturbed areas and prevent sediment from entering the stream (Appendix C, page C-4). All applicable recommendations are included in the Environmental Commitments section of this CE document.

Open Water Feature(s):	No: X	Yes:
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Based on the desktop review, the aerial map of the project area, and the RFI report (Appendix E, page E-1 to 15) there are eleven (11) open water feature(s) within the 0.5 mile search radius. That number was confirmed by aerial maps, and a site visit on June 14, 2021 by INDOT confirmed there are no open water features within or adjacent to the project area. Therefore, no impacts are expected.

Wetlands:	No: X	Yes:
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Based on the desktop review, the aerial map of the project area, and the RFI report (Appendix E, page E-1 to 15) there are eleven (11) wetlands within the 0.5 mile search radius. The site visit on June 14, 2021 by INDOT confirmed that there is one (1) wetland present within the project area. The wetland is a riverine wetland associated with the unnamed tributary to Clear Creek that flows beneath the intersection. All work is planned along SR 37 and will not affect the wetland or associated stream. Therefore, no impacts are expected.

Terrestrial Habitat:	No:	Yes: X
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Based on a desktop review, a site visit on June 14, 2021 by INDOT and the aerial map of the project area (Appendix B, page B-3), there are grassy areas and ditches located along SR 37. The median will be disturbed to install the northbound and southbound crossover intersections, and the shoulders will be disturbed for pavement widening to allow trucks to complete the turn. Land use in the vicinity of the project is primarily commercial with some residences to the east. No trees will be cleared.

IDNR-DFW responded on December 3, 2021 with recommendations to revegetate and protect disturbed areas to prevent sediment from leaving the project area (Appendix C, page C-4). All applicable recommendations are included in the Environmental Commitments section of this CE document.

Protected Species:	No: X	Yes:
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Based on a desktop review and the RFI report (Appendix E, page E-1 to 15), completed by INDOT-Seymour on April 27, 2021, the IDNR Monroe County Endangered, Threatened and Rare (ETR) Species List has been checked. According to the IDNR-DFW early coordination response letter dated

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December 3, 2021 (Appendix C, page C-4), the Natural Heritage Program’s Database has been checked and the following bat species have been identified within 0.5 mile of the project area: Little Brown Myotis, *Myotis lucifugus* (state endangered), Tricolored Bat, *Perimyotis subflavus* (state endangered), Eastern Red Bat, *Lasiurus borealis* (state special concern). No critical habitat was identified within 0.5 mile of the project area.

Indiana Bat and Northern Long-Eared Bat

Bats, Programmatic Informal Consultation – Not Likely to Adversely Affect

Project information was submitted through the USFWS’s Information for Planning and Consultation (IPaC) portal, and an official species list was generated (Appendix C, page C-28 to 33). The project is within range of the federally endangered Indiana bat (*Myotis sodalis*) and the federally threatened northern long-eared bat (NLEB) (*Myotis septentrionalis*). No additional species were generated in the IPaC species list other than the Indiana bat and northern long-eared bat.

The project qualifies for the *Range-wide Programmatic Informal Consultation for the Indiana bat and northern long-eared bat (NLEB)*, dated May 2016 (revised February 2018), between FHWA, Federal Railroad Administration (FRA), Federal Transit Administration (FTA), and USFWS. An effect determination key was completed on December 17, 2021, and based on the responses provided, the project was found to “may affect, not likely to adversely affect” the Indiana bat and/or the NLEB (Appendix C, page C-18 to 27). INDOT reviewed and verified the effect finding on December 17, 2021, and requested USFWS’s review of the finding. No response was received from USFWS within the 14-day review period; therefore, it was concluded they concur with the finding. General and Lighting Avoidance and Minimization Measures (AMMs) are included as firm commitments in the *Environmental Commitments* section of this document.

This precludes the need for further consultation on this project as required under Section 7 of the Endangered Species Act, as amended. If new information on endangered species at the site becomes available, or if project plans are changed, USFWS will be contacted for consultation.

Geological and Mineral Resources:	No: X	Yes:
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Inside karst area; no presence

Based on a desktop review, the project is located inside the designated karst region of Indiana as outlined in the *Protection of Karst Features during Project Development and Construction*. According to the topo map of the project area (Appendix B, page B-2) and the RFI report (Appendix E, page E-1 to 15) there are no karst features identified within or adjacent to the project area. In the early coordination response dated November 4, 2021, the Indiana Geological and Water Survey (IGWS) did not indicate that karst features exist in the project area (Appendix C, page C-13). Response from IGWS has been communicated to the designer on November 4, 2021. No impacts are expected.

Drinking Water Resources:	No: X	Yes:
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Outside of Sole Source Aquifer (SSA)

The project is located in Monroe County, which is not located within the area of the St. Joseph Sole Source Aquifer, the only legally designated sole source aquifer in the state of Indiana. Therefore, the FHWA/EPA Sole Source Aquifer Memorandum of Understanding (MOU) is not applicable to this project, a detailed groundwater assessment is not needed, and no impacts are expected.

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Wellhead Protection Area and Source Water

Not located in a Wellhead Protection Area or Source Water Area

The Indiana Department of Environmental Management's Wellhead Proximity Determinator website (<http://www.in.gov/idem/cleanwater/pages/wellhead/>) was accessed on July 21, 2020 by INDOT – Seymour District. This project is not located within a Wellhead Protection Area or Source Water Area. No impacts are expected.

Water Wells

No wells present, no impacts

The Indiana Department of Natural Resources Water Well Record Database website (<https://www.in.gov/dnr/water/3595.htm>) was accessed on November 4, 2021 by INDOT – Seymour District. No wells are located near this project. Therefore, no impacts are expected.

Urban Area Boundary

In an Urban Area Boundary Location

Based on a desktop review of the INDOT MS4 website (<https://entapps.indot.in.gov/MS4/>) by INDOT – Seymour District on June 4, 2020, and the RFI report; this project is located in an Urban Area Boundary (UAB) location. An early coordination letter was sent on November 13, 2020 to the Monroe County MS4 Coordinator. The MS4 coordinator responded that front slopes that are approximately 50' provide reasonable Filter Strips (Appendix C, Page C-16). Placement of check dams in the flowline with small outlets together with the Filter Strip should provide adequate control.

Public Water System

In a Public Water System Location

Based on a desktop review, a site visit on June 14, 2021, and the aerial map of the project area (Appendix B, page B-3) this project is located where there is a public water system. A public water line is located beneath SR 37 south of the intersection with Dillman Road. The public water system will not be affected because no excavation is planned in the vicinity of any public water lines.

Floodplains:	No: X	Yes:
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Not in Floodplain

The Indiana Department of Natural Resources Indiana Floodway Information Portal website (<http://dnrmmaps.dnr.in.gov/appsphp/fdms/>) was accessed on November 30, 2021 by INDOT. This project is not located in a regulatory floodplain as determined from approved IDNR floodplain maps (Appendix F, page F-2). Therefore, it does not fall within the guidelines for the implementation of 23 CFR 650, 23 CFR 771, and 44 CFR. No impacts are expected.

Farmland:	No: X	Yes:
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No presence, no impact

Based on a desktop review, a site visit on June 14, 2021 by INDOT, the aerial map of the project area (Appendix B, page B-3), there is no land that meets the definition of farmland under the Farmland

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Protection Policy Act (FPPA) within or adjacent to the project area. The requirements of the FPPA do not apply to this project; therefore, no impacts are expected.		
Cultural Resources:	No: X	Yes:
<p><u>Minor Project PA Category B projects</u></p> <p>On December 7, 2020, the INDOT Cultural Resource Office (CRO) determined that this project falls within the guidelines of Category B, Types 2 and 3 under the Minor Projects Programmatic Agreement, (Appendix D, page D-1). Category B Type 2 includes installation of new lighting, signals, signage and other traffic control devices in previously disturbed soils. Category B Type 3 includes construction of added travel, turning, or auxiliary lanes (e.g., bicycle, truck climbing, acceleration and deceleration lanes) and shoulder widening in previously disturbed soils.</p> <p>The current project will stay within the previously disturbed soils of the SR 37 corridor. Project construction will be confined to the existing pavement and the median. No archaeological investigation is necessary for this project because, as currently planned, there are no potential impacts to archaeological resources. No further consultation is required. This completes the Section 106 process and the responsibilities of the FHWA under Section 106 have been fulfilled.</p>		
Section 4(f) and Section 6(f) Resources:	No: X	Yes:
<p>Section 4(f)</p> <p><u>No presence, no impact</u></p> <p>Section 4(f) of the U.S. Department of Transportation Act of 1966 prohibits the use of certain public and historic lands for federally funded transportation facilities unless there is no feasible and prudent alternative. The law applies to significant publicly owned parks, recreation areas, wildlife / waterfowl refuges, and NRHP eligible or listed historic properties regardless of ownership. Lands subject to this law are considered Section 4(f) resources.</p> <p>Based on a desktop review, the aerial map of the project area (Appendix B, page B-3), and the RFI report (Appendix E, page E-1 to 15), there are two (2) potential 4(f) resources located within the 0.5 mile search radius. Based on the site visit on June 14, 2021 by INDOT, no 4(f) resources are located within or adjacent to the project area. Therefore, no use is expected.</p> <p>Section 6(f)</p> <p><u>No presence, no impact</u></p> <p>The U.S. Land and Water Conservation Fund Act of 1965 established the Land and Water Conservation Fund (LWCF), which was created to preserve, develop, and assure accessibility to outdoor recreation resources. Section 6(f) of this Act prohibits conversion of lands purchased with LWCF monies to a non-recreation use.</p> <p>A review of 6(f) properties on the INDOT ESD website revealed a total of 22 properties in Monroe County (Appendix H, page H-1). None of these properties are located within or adjacent to the project area. Therefore, there will be no impacts to 6(f) resources.</p>		

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Air Quality:	No: <input checked="" type="checkbox"/>	Yes: <input type="checkbox"/>
<p>This project is in the 2020-2026 Bloomington-Monroe County Metropolitan Planning Organization (BMCMPPO) TIP (Appendix G, page G-2), which has been directly incorporated into the 2022-2026 INDOT STIP (Appendix G, page G-1).</p> <p>This project is located in Monroe County, which is currently in attainment for all criteria pollutants according to IDEM (https://www.in.gov/idem/sips/air-quality-designations/). Therefore, the conformity procedures of 40 CFR Part 93 do not apply.</p> <p>This project is of a type qualifying as a categorical exclusion (Group 1) under 23 CFR 771.117(c), or exempt under the Clean Air Act conformity rule under 40 CFR 93.126, and as such, a Mobile Source Air Toxics analysis is not required.</p>		
Community Impacts:	No: <input checked="" type="checkbox"/>	Yes: <input type="checkbox"/>
<p><u>No EJ analysis required</u> Under FHWA Order 6640.23A, FHWA and the project sponsor, as a recipient of funding from FHWA, are responsible to ensure that their programs, policies, and activities do not have a disproportionately high and adverse effect on minority or low-income populations. This project will have no relocations and will require less than 0.5 acre of additional permanent right-of-way; therefore, an EJ analysis is not required per the current INDOT Categorical Exclusion Manual.</p>		
Public Facilities and Services (e.g. schools, emergency services):	No: <input checked="" type="checkbox"/>	Yes: <input type="checkbox"/>
<p><u>No presence, no impact</u> Based on a desktop review and the aerial map of the project area (Appendix B, page B-3), there are two (2) public facilities within the 0.5-mile search radius. That number was confirmed by a site visit by INDOT on June 14, 2021. The nearest public facility is a trail located mile 0.47 mile northwest of the project area. There are no public facilities within or adjacent to the project area. Access to all properties will be maintained during construction. Therefore, no impacts are expected. Access to all properties will be maintained during construction.</p> <p>It is the responsibility of the project sponsor to notify school corporations and emergency services at least two weeks prior to any construction that would block or limit access.</p>		
Hazardous Materials and Regulated Substances:	No: <input checked="" type="checkbox"/>	Yes: <input type="checkbox"/>
<p><u>Presence, no impact</u> Based on a review of GIS and available public records, the RFI was completed on April 27, 2021 by INDOT – Seymour District and INDOT SAM provided their concurrence on June 16, 2021 (Appendix E, Page E-1 to 15). A total of 25 hazardous material sites are located within 0.5 mile of the project area, but no sites are located within the project area. The nearest sites are a NPDES facility located in the northwest quadrant and a LUST site in the southwest quadrant of the intersection. One (1) RCRA Generator is located within 0.5 mile of the project area. One (1) Voluntary Remediation Program Site is located within 0.5 mile of the project area. One (1) Solid Waste Landfill is located within 0.5 mile of the project area. Three (3) Leaking Underground Storage Tank (LUST) Sites are located within 0.5 mile of the project area. One (1) Waste Transfer Station Site is located within 0.5 mile of the project area. Two (2) Brownfields Sites are located within 0.5 mile of the project area. One (1) Institutional Controls Site is located within 0.5 mile of the project area. Six (6) NPDES Facilities are located within 0.5 mile of the</p>		

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project area. Nine (9) NPDES Pipe Location is located within 0.5 mile of the project area. None of the hazmat sites identified will impact the project. Further investigation for hazardous material concerns is not required at this time.

Permits:	No: X	Yes:
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No permits are required for this project since the total land disturbance is less than one acre and there are no impacts to streams, wetlands, or floodways.

Applicable recommendations provided by resource agencies are included in the Environmental Commitments section of this document. If permits are found to be necessary, the conditions of the permit will be requirements of the project and will supersede these recommendations.

It is the responsibility of the project sponsor to identify and obtain all required permits.

ENVIRONMENTAL COMMITMENTS:

Firm:

- 1) Any work in a wetland area within INDOT's right of way or in borrow/waste areas is prohibited unless specifically allowed in the US Army Corps of Engineers or IDEM permit. (INDOT ESD)
- 2) If the scope of work or permanent or temporary right-of-way amounts change, the INDOT Environmental Services Division (ESD) and the INDOT District Environmental Section will be contacted immediately. (INDOT ESD and INDOT District)
- 3) It is the responsibility of the project sponsor to notify school corporations and emergency services at least two weeks prior to any construction activity that would block or limit access. (INDOT ESD)
- 4) General AMM 1: Ensure all operators, employees, and contractors working in areas of known or presumed bat habitat are aware of all FHWA/FRA/FTA (Transportation Agencies) environmental commitments, including all applicable AMMs.
- 5) Lighting AMM 1: Direct temporary lighting away from suitable habitat during the active season.
- 6) Lighting AMM 2: When installing new or replacing existing permanent lights, use downward-facing, full cut-off lens lights (with same intensity or less for replacement lighting); or for those transportation agencies using the BUG system developed by the Illuminating Engineering Society, be as close to 0 for all three ratings with a priority of "uplight" of 0 and "backlight" as low as practical.
- 7) Hibernacula AMM1: For projects located within karst areas, on-site personnel will use best management practices, secondary containment measures, or other standard spill prevention and countermeasures to avoid impacts to possible hibernacula. Where practicable, a 300 foot buffer will be employed to separate fueling areas and other major containment risk activities from caves, sinkholes, losing streams, and springs in karst topography.

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- 8) The UNT to Clear Creek flows through the project area and is listed as impaired for PCBs in fish tissue. Exposure to PCBs in fish tissue is considered low, assuming workers are not eating biota surrounding or associated with the water body. Workers will be informed. If there will be sediment and/or soils disturbed by construction, additional investigation may be necessary. Coordination with INDOT ESD Site Assessment & Management (SAM) will occur.

For Consideration:

- 1) Revegetate all bare and disturbed areas with a mixture of grasses (excluding all varieties of tall fescue) and legumes as soon as possible upon completion; low endophyte tall fescue may be used in the ditch bottom and side slopes only.
- 2) Appropriately designed measures for controlling erosion and sediment must be implemented to prevent sediment from entering the stream or leaving the construction site; maintain these measures until construction is complete and all disturbed areas are stabilized.

Appendices

A: INDOT Supporting Documentation

- 1. Threshold Chart.....A-1

B: Graphics

- 1. Project Location Map..... B-1
- 2. Topographical Map..... B-2
- 3. Aerial Map.....B-3
- 4. Plans..... B-4 to B-36

C: Early Coordination

- 1. Early Coordination Letter.....C-1 to C-3
- 2. Early Coordination Response from IDNR.....C-4 to C-5
- 3. Early Coordination Automated Response from IDEM.....C-6 to C-12
- 4. Early Coordination Automated Response from IGWS.....C-13 to C-15
- 5. MS4 Early Coordination and Response from Monroe County.....C-16 to C-17
- 6. USFWS Concurrence Verification Letter and Species List.....C-18 to C-33

D: Cultural Resources

- 1. MPPA.....D-1 to D-3

E: Red Flag and Hazardous Materials

- 1. Red Flag Investigation.....E-1 to E-15

F: Water Resources

- 1. NWI Map..... F-1
- 2. DNR Floodplain Map..... F-2

G: Air Quality

- 1. STIPG-1
- 2. Bloomington-Monroe Co. Metro Planning Organization TIP.....G-2

H: Additional Studies

- 1. LWCF Resources.....H-1 to H-2

Appendix A
INDOT Supporting
Documentation

Categorical Exclusion Level Thresholds

	PCE	Level 1	Level 2	Level 3	Level 4 ¹
Section 106	Falls within guidelines of Minor Projects PA	“No Historic Properties Affected”	“No Adverse Effect”	-	“Adverse Effect” Or Historic Bridge involvement ²
Stream Impacts³	No construction in waterways or water bodies	< 300 linear feet of stream impacts	≥ 300 linear feet of stream impacts	-	USACE Individual 404 Permit ⁴
Wetland Impacts³	No adverse impacts to wetlands	< 0.1 acre	-	< 1.0 acre	≥ 1.0 acre
Right-of-way⁵	Property acquisition for preservation only or none	< 0.5 acre	≥ 0.5 acre	-	-
Relocations	None	-	-	< 5	≥ 5
Threatened/Endangered Species (Species Specific Programmatic for Indiana bat & northern long eared bat)*	“No Effect”, “Not likely to Adversely Affect” (With select AMMs ⁶)	“Not likely to Adversely Affect” (With any AMMs or commitments)	-	“Likely to Adversely Affect”	Project does not fall under Species Specific Programmatic ⁷
Threatened/Endangered Species (Any other species)*	Falls within guidelines of USFWS 2013 Interim Policy or “No Effect”	“Not likely to Adversely Affect”	-	-	“Likely to Adversely Affect”
Environmental Justice	No disproportionately high and adverse impacts	-	-	-	Potential ⁸
Sole Source Aquifer	No Detailed Groundwater Assessment	-	-	-	Detailed Groundwater Assessment
Floodplain	No Substantial Impacts	-	-	-	Substantial Impacts
Section 4(f) Impacts	None	-	-	-	Any ⁹
Section 6(f) Impacts	None	-	-	-	Any
Permanent Traffic Alteration	None	-	-	-	Any
Noise Analysis Required	No	-	-	-	Yes
Air Quality Analysis Required	No	-	-	-	Yes ¹⁰
Approval Level	Concurrence by DE or ESD	DE or ESD	DE or ESD	DE and/or ESD	DE and/or ESD; and FHWA
<ul style="list-style-type: none"> • District Env. (DE) • Env. Serv. Div. (ESD) • FHWA 					

¹ Coordinate with INDOT Environmental Services Division. INDOT will then coordinate with the appropriate FHWA Environmental Specialist.

² Any involvement with a bridge processed under the Historic Bridge Programmatic Agreement.

³ Total permanent impacts to streams (linear feet) and wetlands (acres).

⁴ US Army Corps of Engineers Individual 404 Permit

⁵ Total permanent and temporary right-of-way. This does not include reacquisition of existing apparent right-of-way.

⁶ Avoidance and Mitigation Measures (AMMs) determined by the IPAC determination key to be required that are not tree AMMs, bridge AMMs, or structure AMMs.

⁷ Projects that do not fall under a Species Specific Programmatic and results in a “Likely to Adversely Affect”. Other findings can be processed as a lower level CE.

⁸ Potential for causing a disproportionately high and adverse impact.

⁹ Section 4(f) use resulting in an Individual, Programmatic, or *de minimis* evaluation. The only exception is a *de minimis* evaluation for historic properties (Effective January 2, 2020). If a historic property *de minimis* and no other use, mark the *None* column.

¹⁰ Hot Spot Analysis and/or MSAT Quantitative Emission Analysis.

* Includes the threatened/endangered species critical habitat

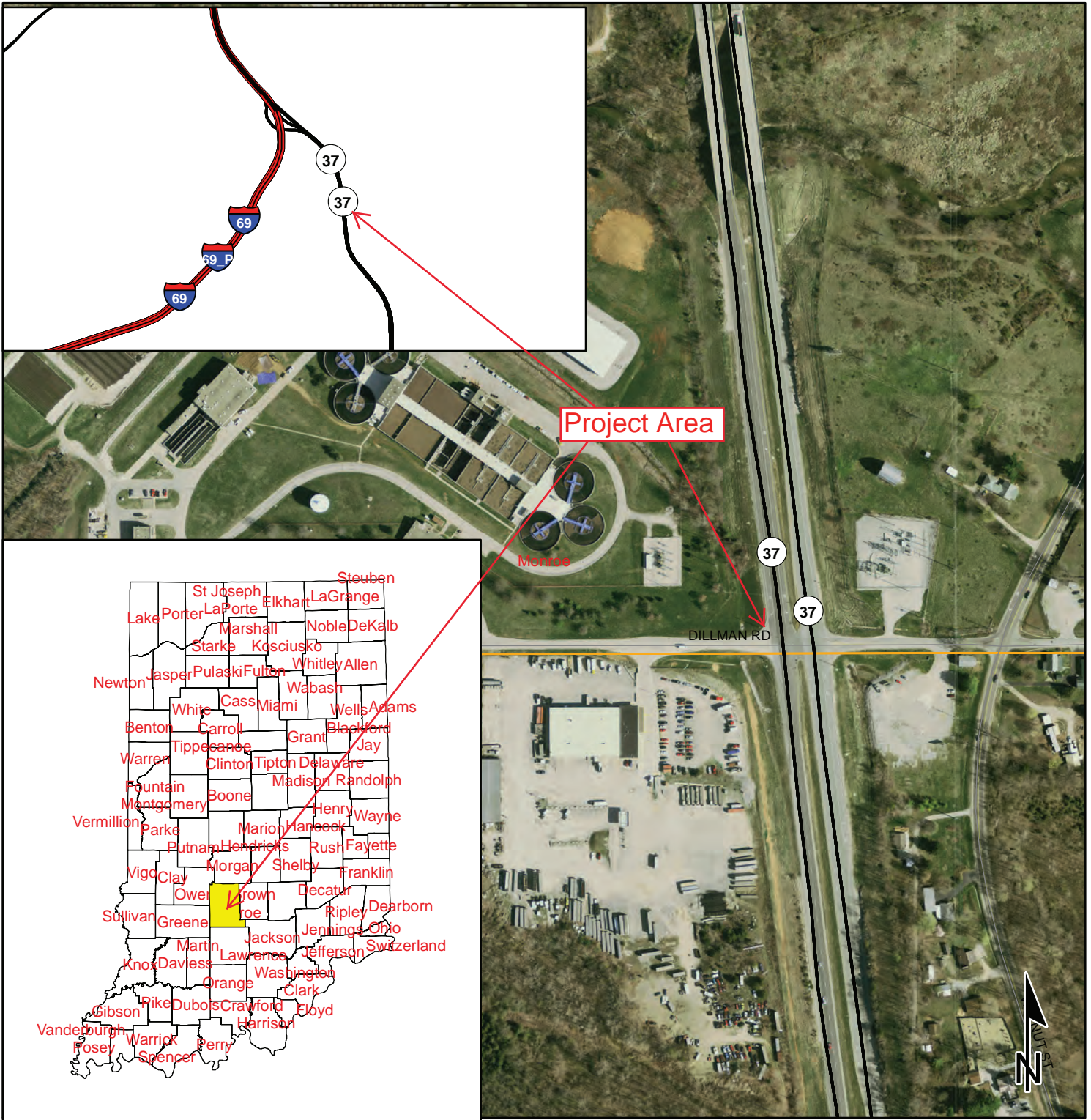
Note: Substantial public or agency controversy may require a higher-level NEPA document.

Note: The CE was elevated to Level 1 due to Public Involvement.

Appendix B

Graphics

SR 37 / Dillman Road in Bloomington SR 37, Monroe County, Indiana Des No. 1800371, Intersect. Improv. W/ Added Turn Lanes



This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.

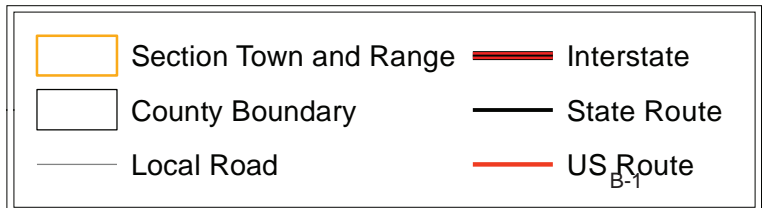
Sources: Non Orthophotography

Data - Obtained from the State of Indiana Geographical Information Office Library

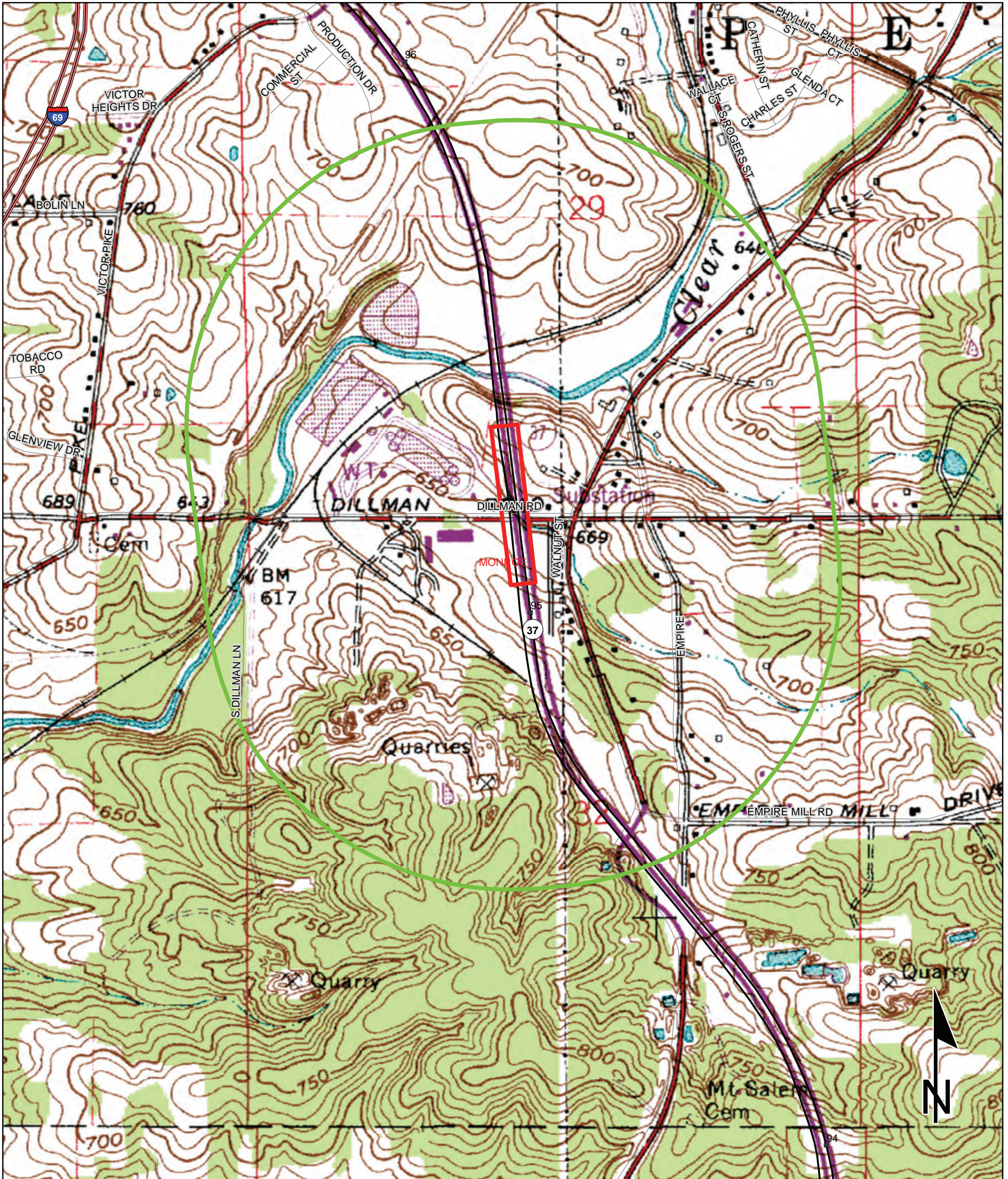
Orthophotography - Obtained from Indiana Map Framework Data (www.indianamap.org)

Map Projection: UTM Zone 16 N **Map Datum:** NAD83

Scale 1:4,099 Miles
0.06 0.03 0 0.06



Topographical Map
 SR 37 at Intersection of Dillman Road in Bloomington
 Des. No. 1800371, Intersection Improvement - Added Turn Lanes
 Monroe County, Indiana



Sources: 0.2 0.1 0 0.2
Miles
Non Orthophotography
 Data - Obtained from the State of Indiana Geographical Information Office Library
Orthophotography - Obtained from Indiana Map Framework Data (www.indianamap.org)
 Map Projection: UTM Zone 16 N Map Datum: NAD83
 This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.

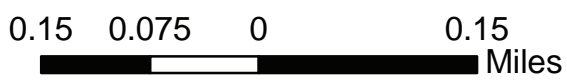
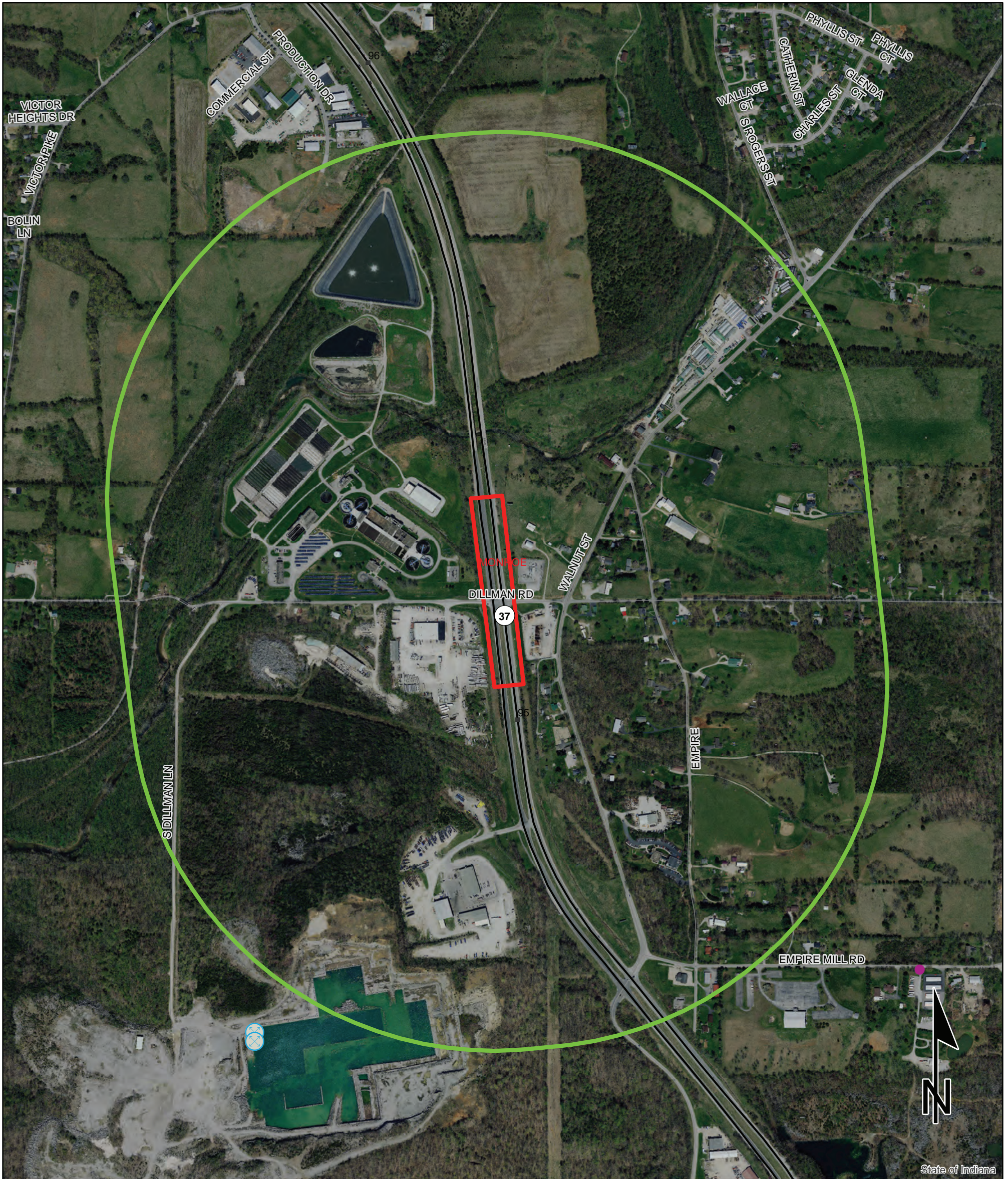
**CLEAR CREEK QUADRANGLE
 INDIANA
 7.5 MINUTE SERIES
 (TOPOGRAPHIC)**

Aerial Map

SR 37 at Intersection of Dillman Road in Bloomington

Des. No. 1800371, Intersection Improvement - Added Turn Lanes

Monroe County, Indiana

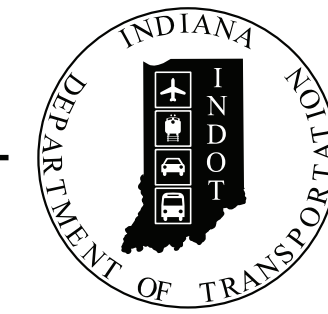


Sources:
Non Orthophotography Data - Obtained from the State of Indiana Geographical Information Office Library
Orthophotography - Obtained from Indiana Map Framework Data (www.indianamap.org)
Map Projection: UTM Zone 16 N **Map Datum:** NAD83
This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.

Oil and Gas Wells	County Boundary	Toll
Mineral Resources	Project Area	Interstate
Mine - Surface	Half Mile Radius	State Route
Mine - Underground		US Route
		Local Road

PROJECT	DESIGNATION
1800371	1800371
CONTRACT	
R-42414	

INDIANA DEPARTMENT OF TRANSPORTATION



ROAD PLANS

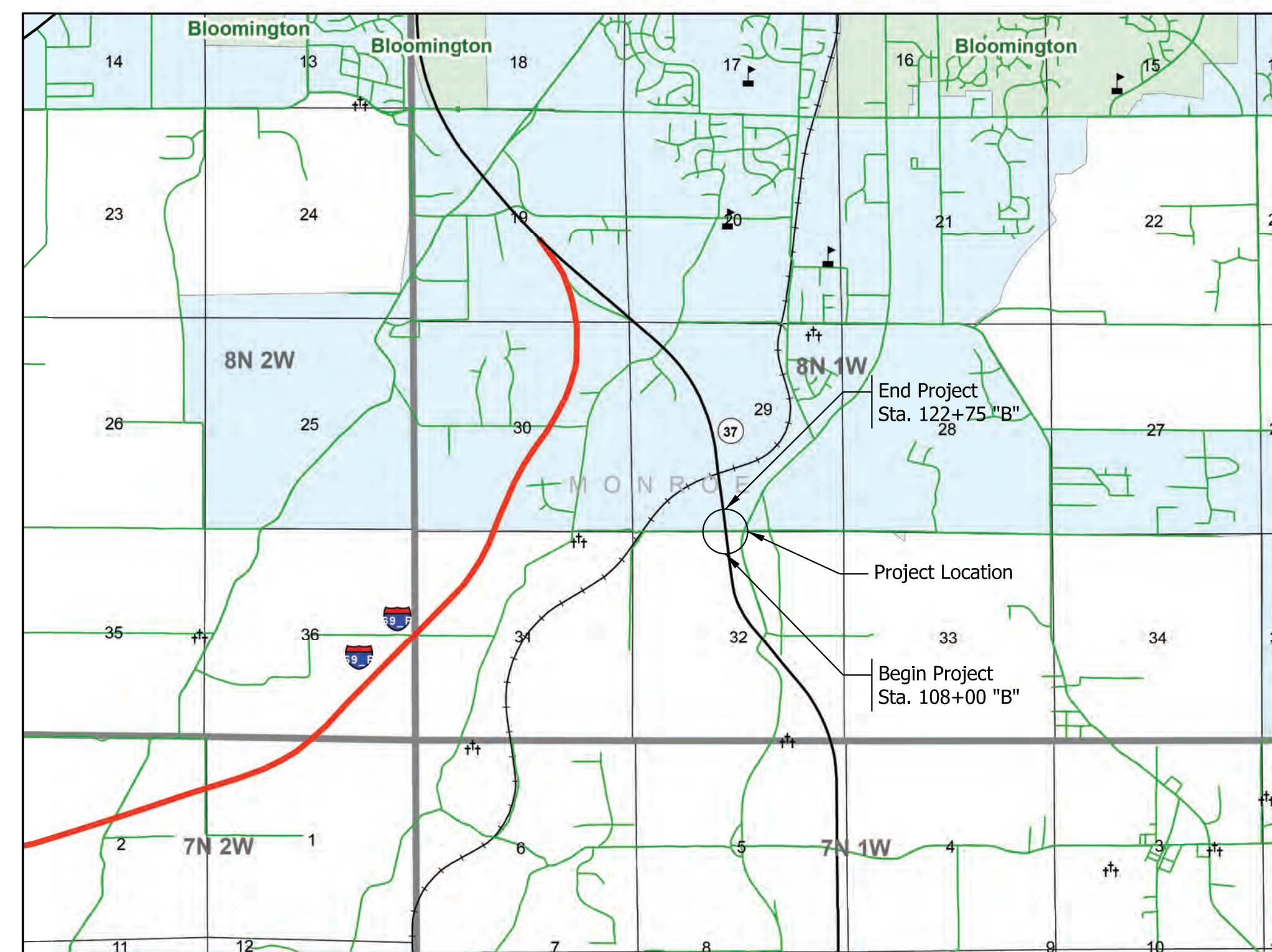
ROUTE: SR 37 FROM: RP 95+16 TO: RP 95+16
 PROJECT NO. 180027300ST5 P.E.

NO ADDITIONAL RIGHT OF WAY
 REQUIRED FOR THIS PROJECT

R/W

180027300ST5 CONST.

Intersection Improvement with Added Turn Lanes on SR 37
 Located at intersection of Dillman Road in Bloomington
 Section 29 & 32, T-8-N, R-1-W, Perry Township, Monroe County, Indiana



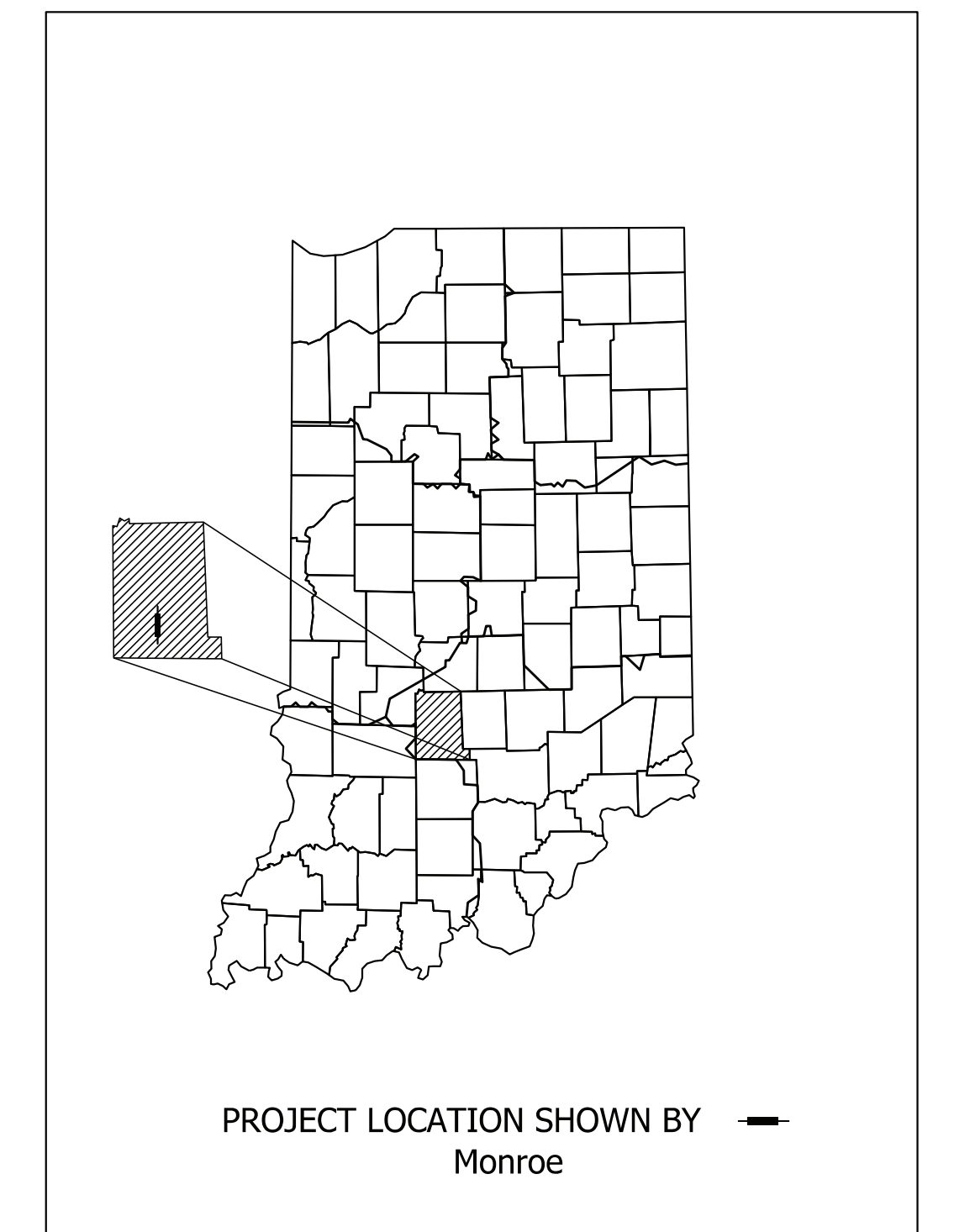
SCALE: 1" = 3,000'

TRAFFIC DATA - SR 37		
A.A.D.T.	(2024)	24,595 V.P.D.
A.A.D.T.	(2044)	33,294 V.P.D.
D.H.V	(2044)	2,548 V.P.H.
DIRECTIONAL DISTRIBUTION		50 %
TRUCKS		9.07 % A.A.D.T. 3.77 % D.H.V.

DESIGN DATA	
DESIGN SPEED	55 M.P.H.
PROJECT DESIGN CRITERIA	3R (NON-FREEWAY)
FUNCTIONAL CLASSIFICATION	PRINCIPAL ARTERIAL
RURAL/URBAN	RURAL
TERRAIN	LEVEL
ACCESS CONTROL	PARTIAL

TRAFFIC DATA - W DILLMAN RD		
A.A.D.T.	(2024)	1789 V.P.D.
A.A.D.T.	(2044)	1789 V.P.D.
D.H.V	(2044)	156 V.P.H.
DIRECTIONAL DISTRIBUTION		36 %
TRUCKS		11.41 % A.A.D.T. 2.56 % D.H.V.

DESIGN DATA	
DESIGN SPEED	55 M.P.H.
PROJECT DESIGN CRITERIA	3R (NON-FREEWAY)
FUNCTIONAL CLASSIFICATION	MINOR COLLECTOR
RURAL/URBAN	RURAL
TERRAIN	LEVEL
ACCESS CONTROL	NONE



LATITUDE: N 39°05'34" LONGITUDE: W 86°32'44"

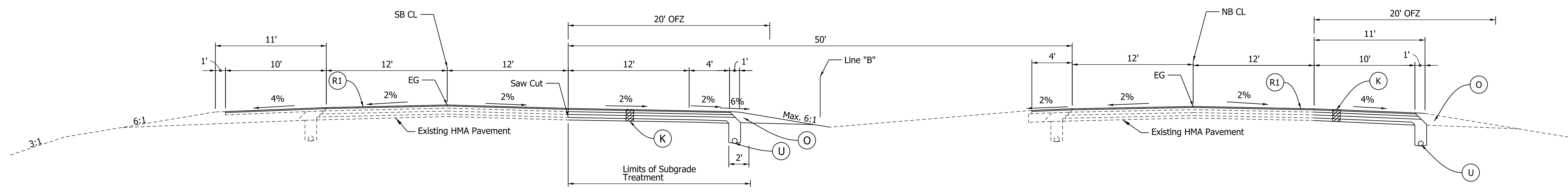
GROSS LENGTH: 0.28 MI.
 NET LENGTH: 0.28 MI.
 MAX. GRADE: 1.21 %

INDIANA DEPARTMENT OF TRANSPORTATION
 STANDARD SPECIFICATIONS DATED 2022
 TO BE USED WITH THESE PLANS

PLANS PREPARED BY: _____ PHONE NUMBER _____
THIS AREA SHOULD NOT BE CONSIDERED A CERTIFIED DOCUMENT
 CERTIFIED BY: _____ DATE _____
 RECOMMENDED FOR LETTING: _____ INDIANA DEPARTMENT OF TRANSPORTATION DATE _____

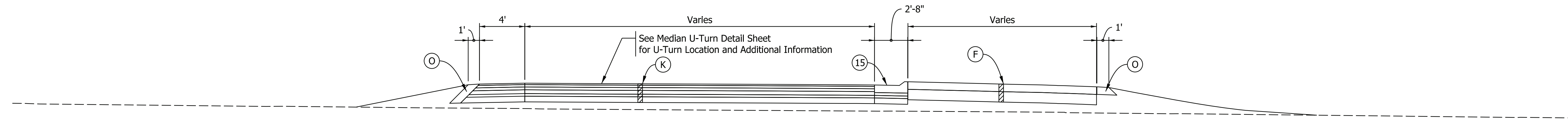
DESIGNATION	
1800371	
SHEETS	
1	of 51
CONTRACT	PROJECT
R-42414	1800371

Plot: 4/14/2022 8:56 PM



SOUTHBOUND ADDED INSIDE TURN LANE AND SHOULDER REPLACEMENT

Scale: 3/16"=1'-0"
 Sta. 108+75 "B" to Sta. 114+50 "B" - SB SR 37 Lt-Turn Lane and J-Turn Lane
 Sta. 108+25 to Sta. 110+75 "B" - NB Shoulder Replacement



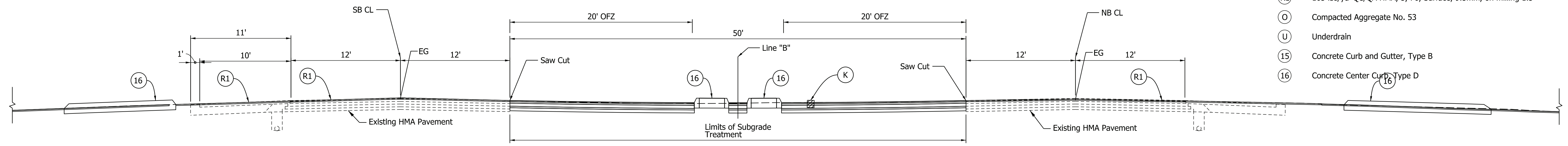
SOUTHBOUND U-TURN SECTION

Scale: 1"=5'
 Sta. 108+50 to Sta. 109+50

* Note: Pavement Design is assumed at this time

LEGEND

- (F) PCCP, 7 inches, using 1.25 inch Dowel Bars D-1 Joints spaced no more than 15 feet, on 10 inches of Compacted Aggregate, No. 53, on Subgrade Treatment Type IC
- (K) 165 lbs/yd' QC/QA HMA, 3, 70, Surface, 9.5mm, on 275 lbs/yd' QC/QA HMA, 3, 70, Intermediate, 19.0mm, on 330 lbs/yd' QC/QA HMA, 3, 64, Base, 19.0mm, on 400 lbs/yd' QC/QA HMA, 4, 76, Base OG, 19.0mm, on 4 in of Compacted Aggregate No. 53, on Subgrade Treatment Type IC
- (R1) 165 lbs/yd' QC/QA HMA, 3, 70, Surface, 9.5mm, on milling 1.5"
- (O) Compacted Aggregate No. 53
- (U) Underdrain
- (15) Concrete Curb and Gutter, Type B
- (16) Concrete Center Curb, Type D



MEDIAN U-TURN AREA

Scale: 3/16"=1'-0"
 Sta. 114+15 "B" to Sta. 116+00 "B"
 (Shown at 115+10)

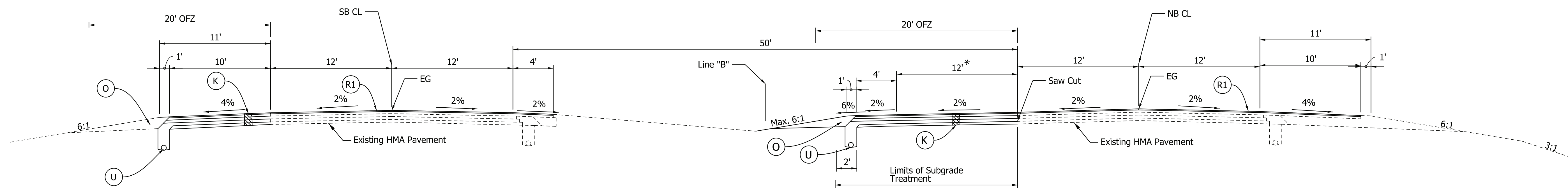
Note: Underdrain locations are estimated at this time

Plot: 4/14/2022 8:56 PM

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
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CHECKED: AKB	April 2022	CHECKED: AKB
	April 2022	

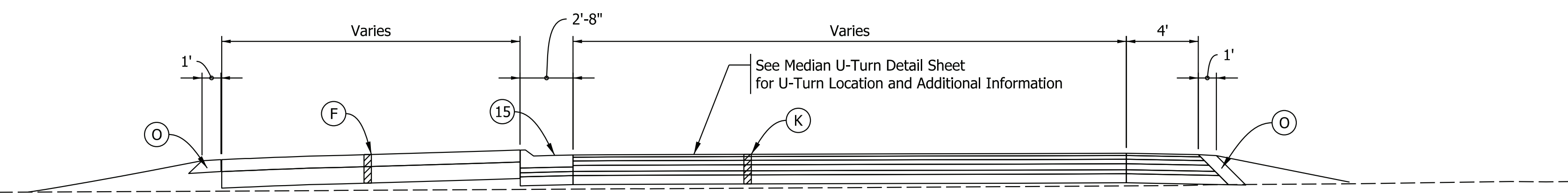
INDIANA DEPARTMENT OF TRANSPORTATION	
TYPICAL SECTIONS	
Page 1 OF 2	

SCALE AS NOTED	BRIDGE FILE
	DESIGNATION
	1800371
	SHEETS
3	of 51
CONTRACT R-42414	PROJECT 1800371



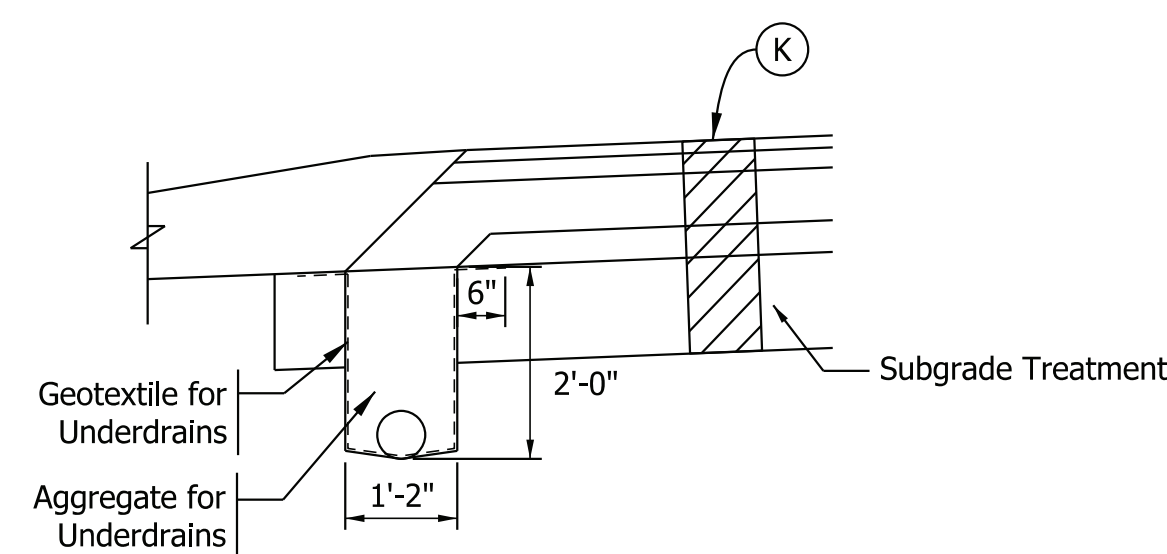
NORTHBOUND ADDED INSIDE TURN LANE AND SHOULDER REPLACEMENT

Scale: 3/16"=1'-0"
 Sta. 115+75 "B" to Sta. 122+47 "B" - NB SR 37 Lt-Turn Lane and J-Turn Lane
 Sta. 120+19 to Sta. 122+69 "B" - SB Shoulder Replacement



NORTHBOUND U-TURN SECTION

Scale: 1"=5'
 Sta. 121+60 to Sta. 122+50



UNDERDRAIN DETAIL

Scale: 1/2"=1'-0"
 Underdrain installation shall be done prior to subgrade treatment installation.

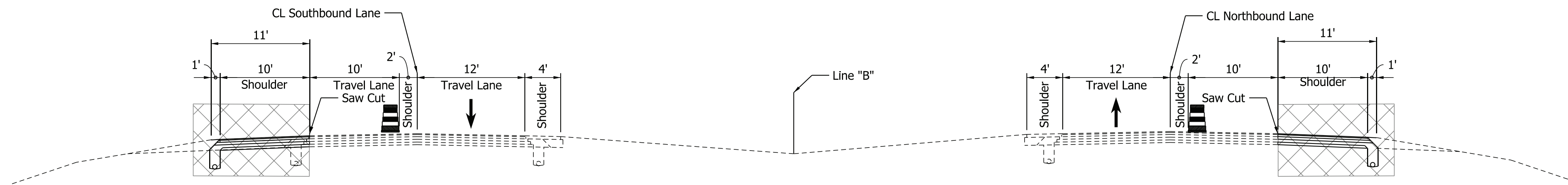
* Note: Pavement Design is assumed at this time

LEGEND

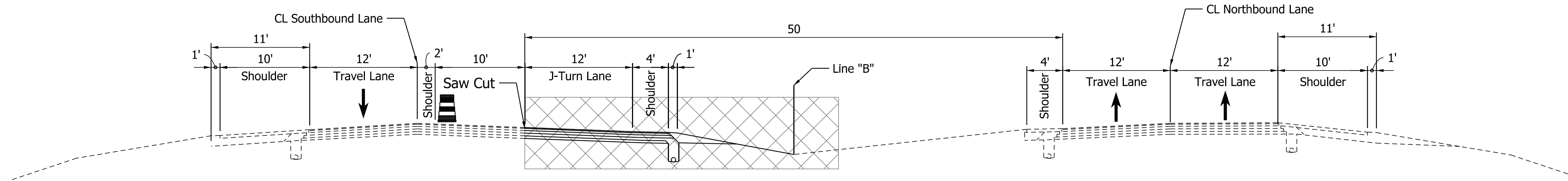
- (F) PCCP, 7 inches, using 1.25 inch Dowel Bars
D-1 Joints spaced no more than 15 feet, on 10 inches of Compacted Aggregate, No. 53, on Subgrade Treatment Type IC
- (K) 165 lbs/yd' QC/QA HMA, 3, 70, Surface, 9.5mm, on 275 lbs/yd' QC/QA HMA, 3, 70, Intermediate, 19.0mm, on 330 lbs/yd' QC/QA HMA, 3, 64, Base, 19.0mm, on 400 lbs/yd' QC/QA HMA, 4, 76, Base OG, 19.0mm, on 4 in of Compacted Aggregate No. 53, on Subgrade Treatment Type IC
- (R1) 165 lbs/yd' QC/QA HMA, 3, 70, Surface, 9.5mm, on milling 1.5"
- (O) Compacted Aggregate No. 53
- (U) Underdrain
- (15) Concrete Curb and Gutter, Type B
- (16) Concrete Center Curb, Type D

Note: Underdrain locations are estimated at this time

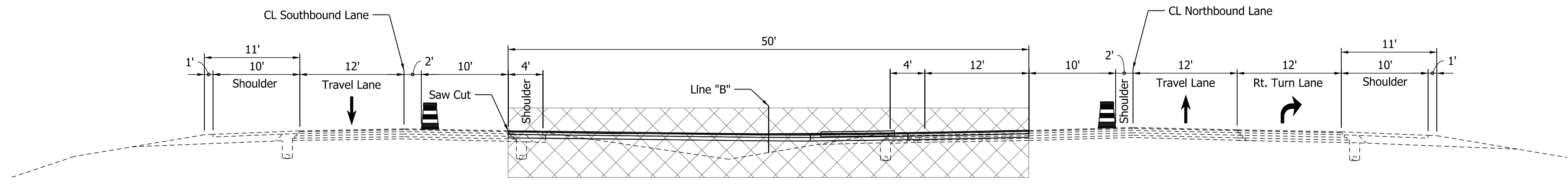
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			AS NOTED	DESIGNATION	
	DESIGNED: MRS _____ MAR 2021	DRAWN: MRS _____ MAR 2021	TYPICAL SECTIONS		SHEETS
	CHECKED: AKB _____ April 2022	CHECKED: AKB _____ April 2022			4 of 51
		Page 2 Of 2		CONTRACT	
				R-42414	
				PROJECT	
				1800371	



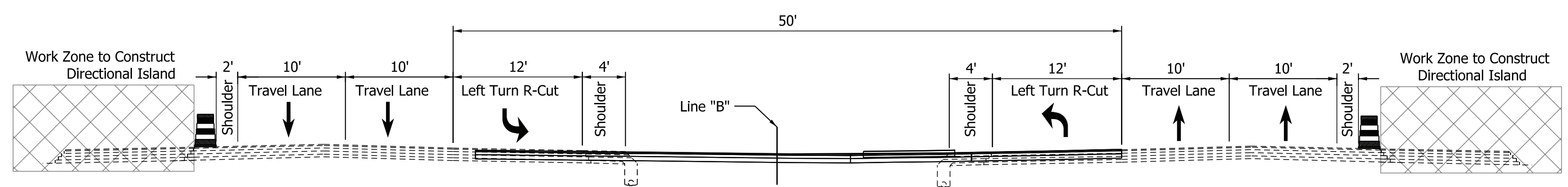
PHASE 1 TYPICAL SECTION
 Scale: 1/8" = 1'
 Section Shown at Sta. 109+00 "B" Southbound
 Section Shown at Sta. 122+00 "B" Northbound



PHASE 2 TYPICAL SECTION
 Scale: 1/8" = 1'
 Section Shown at Sta. 110+00 "B"



PHASE 3 TYPICAL SECTION
 Scale: 1/8" = 1'
 Section Shown at Sta. 114+50 "B"



PHASE 4 TYPICAL SECTION
 Scale: 1/8" = 1'
 Section Shown at Sta. 115+10 "B"

MOT SCHEME:

Phase One:

- A. Traffic shall be maintained with a single lane closure. Shift Northbound and Southbound SR 37 traffic to the inside travel lanes. Intersection with W Dillman Rd. will remain open
- B. Construct the new full depth outside loons across from the median J-Turn crossover areas.

Phase 1-B:

- A. Keep existing Phase 1 MOT devices in place. Place additional barrels and flagging devices.
- B. Sawcut existing edge of pavement. Construct new full depth widening for turning radii.

Phase Two:

- A. Traffic shall be maintained with a single lane closure. Shift Northbound and Southbound SR 37 traffic to the outside travel lanes. Intersection with W Dillman Rd. will remain open.
- B. Construct the new Northbound and Southbound J-turn auxiliary lanes and J-turn median crossover areas in the SR 37 median.

Phase Three:

- A. Close SR 37 intersection with W Dillman Rs. Maintain Northbound and Southbound SR 37 traffic in the outside travel lanes. W Dillman Rd. through traffic, W Dillman Rd. left turn movements onto SR 37 and SR 37 left turn movements onto W Dillman Rd. will have to utilize the new J-turn auxiliary lanes and J-turn median crossover areas.
- B. Construct new intersection of SR 37 and W Dillman Rd.
- C. Install final permanent pavement markings and signage.

Phase Four:

- A. Maintain Northbound and Southbound SR 37 traffic in shifted/narrowed travel lanes. E Dillman Rd. right turns onto SR 37 N, and SR 37 N right turns onto E Dillman Rd. will remain open under flaggers; W Dillman Rd. right onto SR 37 S, and SR 37 S right turns onto W Dillman Rd. will remain open under flaggers; SR 37 NB and SB left turn will utilize the new J-turn auxiliary lanes and J-turn median crossover areas.
- B. Construct directional islands on E and W Dillman Rds.
- C. Install final permanent pavement markings and signage.

CONSTRUCTION ZONE SPEED = 45 MPH

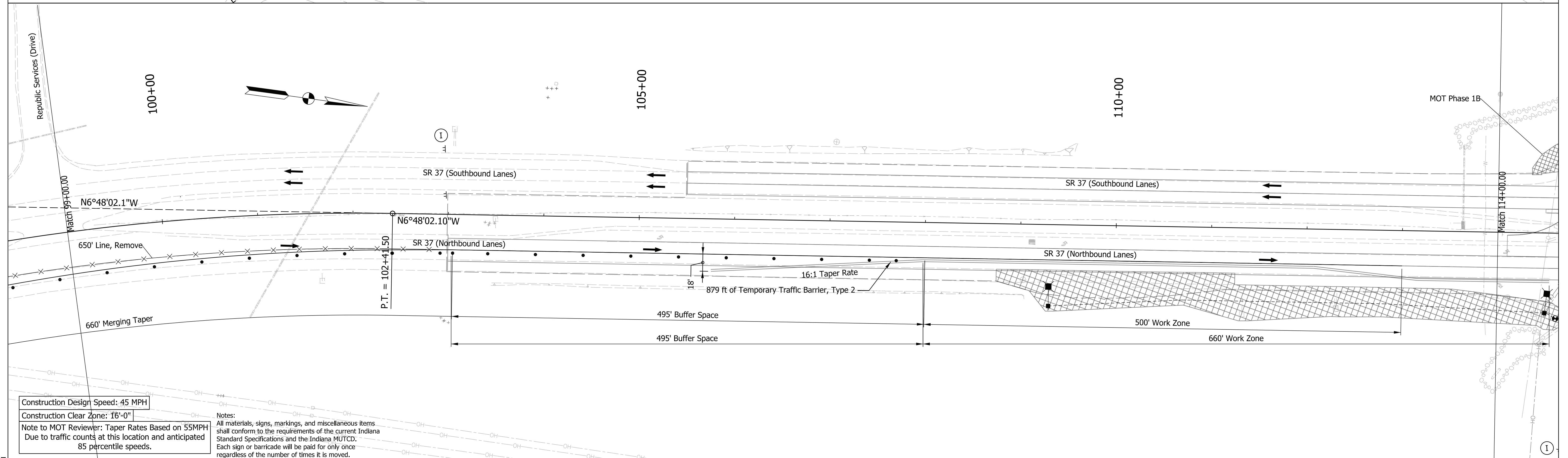
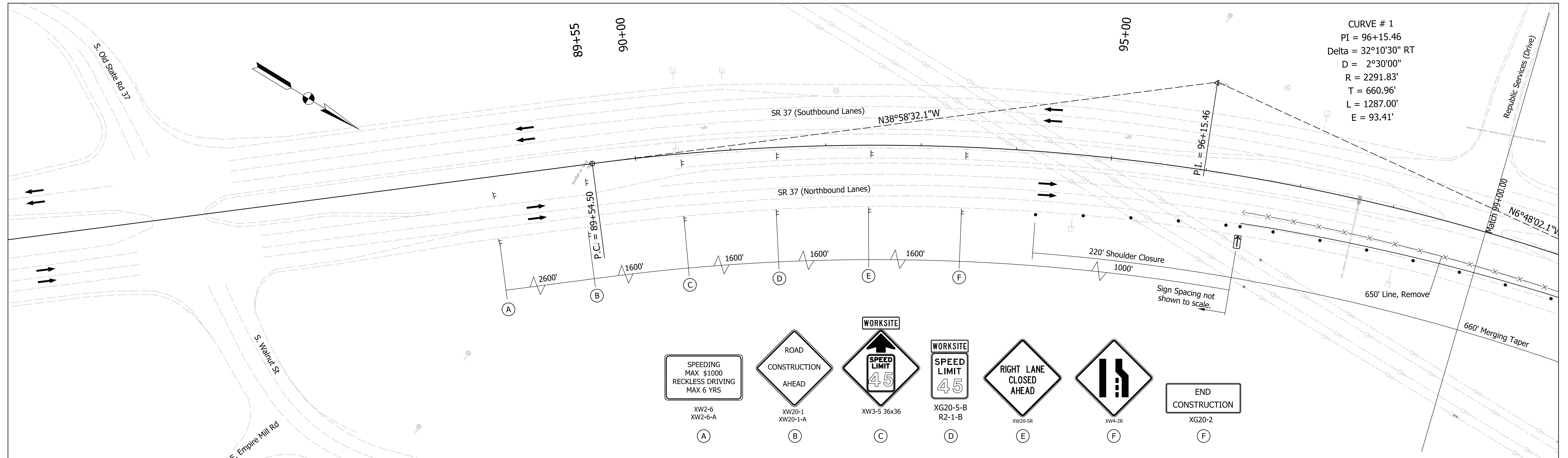
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 Model:RD_Detail Sheet

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: MRS	MAR 2021	DRAWN: MRS
CHECKED: CPM	MAR 2021	CHECKED: CPM
	MAR 2021	

INDIANA DEPARTMENT OF TRANSPORTATION	
MAINTENANCE OF TRAFFIC	

SCALE	BRIDGE FILE
1/8" = 1'	
	DESIGNATION
	1800371
	SHEETS
	5 of 51
CONTRACT	PROJECT
R-42414	1800371



Maintenance of Traffic Legend

	Construction Area
	Construction Sign
	Barricade Type III-A
	Channelizing Device
	Temporary Pavement Marking, 5 in (Black)
	Direction of Traffic

Temporary Pavement Marking Legend

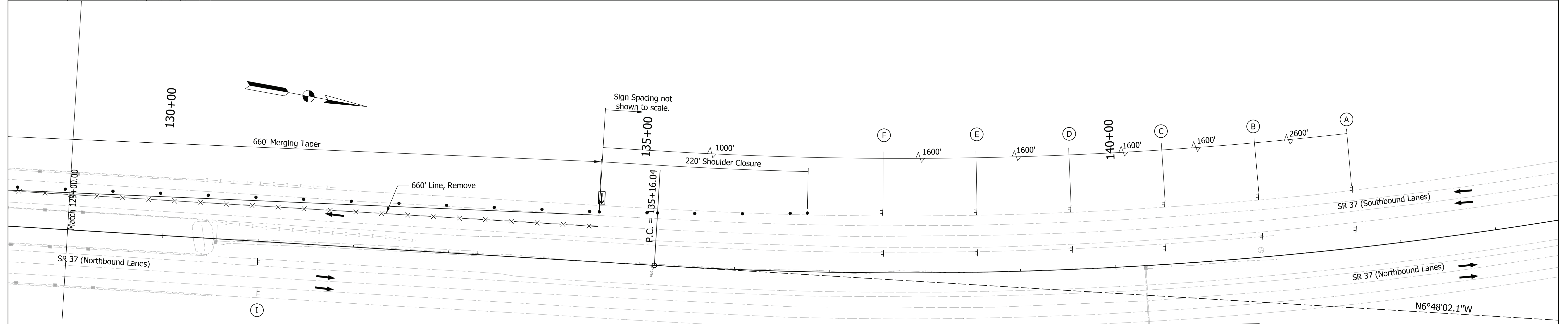
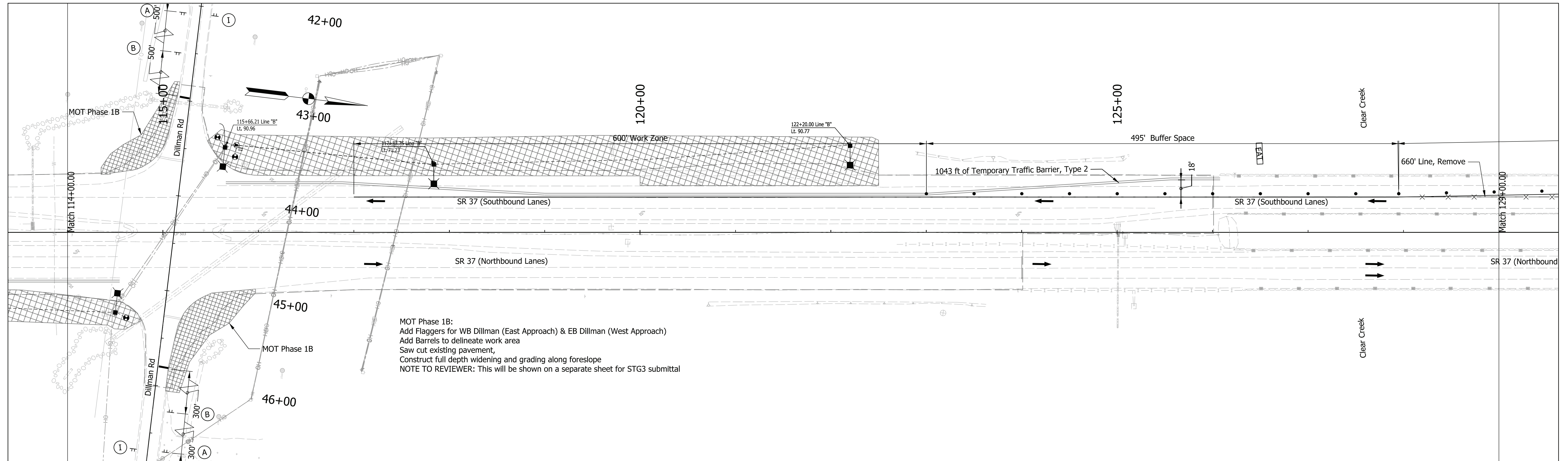
①	Temporary Pavement Marking, 4in. (White)
②	Temporary Pavement Marking, 4in. (Yellow)
③	Temporary Transverse Pavement Marking, 24in. (Stop Bar)
④	Temporary Pavement Marking, 5in. (Black)

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: RRC	April 2022	DRAWN: RRC
April 2022		April 2022
CHECKED: AKB	April 2022	CHECKED: AKB
April 2022		April 2022

INDIANA DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC PHASE 1

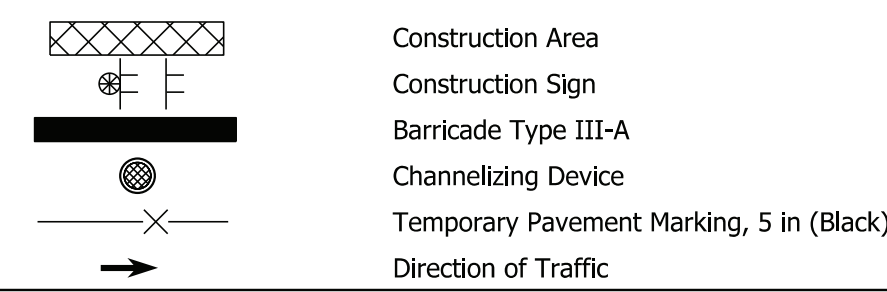
SCALE	BRIDGE FILE
1"=50'	
	DESIGNATION
	1800371
	SHEETS
	6 of 51
CONTRACT	PROJECT
R-42414	1800371



Construction Design Speed: 45 MPH
 Construction Clear Zone: 16'-0"
 Note to MOT Reviewer: Taper Rates Based on 55MPH
 Due to traffic counts at this location and anticipated 85 percentile speeds.

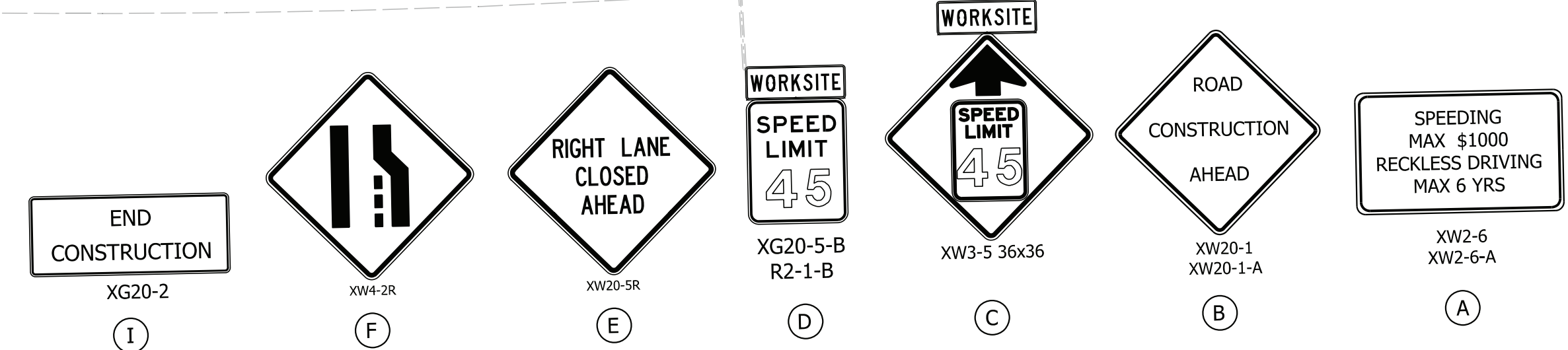
Notes:
 All materials, signs, markings, and miscellaneous items shall conform to the requirements of the current Indiana Standard Specifications and the Indiana MUTCD.
 Each sign or barricade will be paid for only once regardless of the number of times it is moved.

Maintenance of Traffic Legend



Temporary Pavement Marking Legend

- ① Temporary Pavement Marking, 4in. (White)
- ② Temporary Pavement Marking, 4in. (Yellow)
- ③ Temporary Transverse Pavement Marking, 24in. (Stop Bar)
- ④ Temporary Pavement Marking, 5in. (Black)



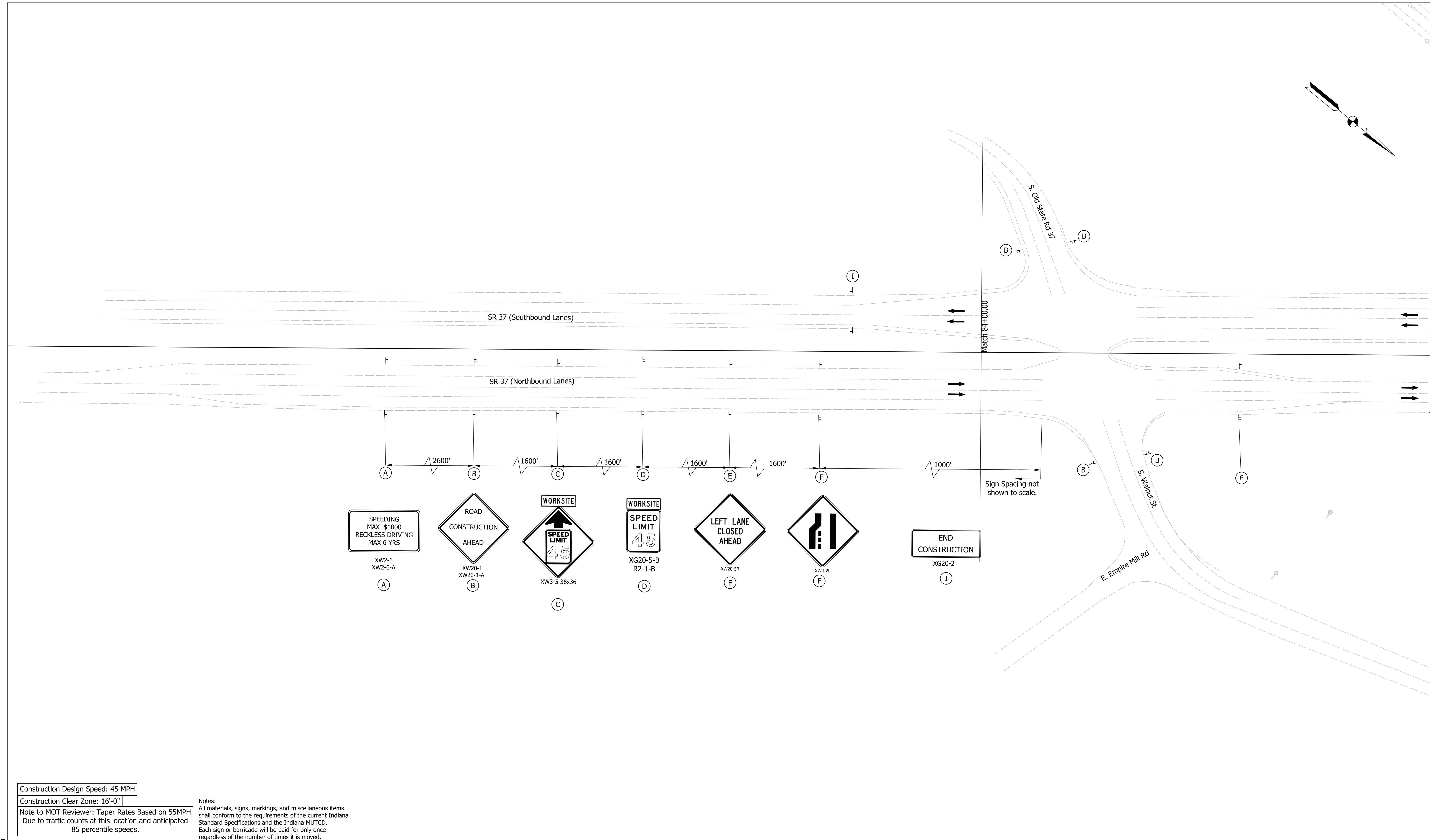
RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: RRC	April 2022	DRAWN: RRC
CHECKED: AKB	April 2022	CHECKED: AKB
	April 2022	

INDIANA
DEPARTMENT OF TRANSPORTATION

**MAINTENANCE OF TRAFFIC
PHASE 1**

SCALE	BRIDGE FILE
1"=50'	
	DESIGNATION
	1800371
	SHEETS
	7 of 51
CONTRACT	PROJECT
R-42414	1800371

Plot: 4/14/2022 10:05 PM



Construction Design Speed: 45 MPH
 Construction Clear Zone: 16'-0"
 Note to MOT Reviewer: Taper Rates Based on 55MPH
 Due to traffic counts at this location and anticipated
 85 percentile speeds.

Notes:
 All materials, signs, markings, and miscellaneous items
 shall conform to the requirements of the current Indiana
 Standard Specifications and the Indiana MUTCD.
 Each sign or barricade will be paid for only once
 regardless of the number of times it is moved.

Maintenance of Traffic Legend

- Construction Area
- Construction Sign
- Barricade Type III-A
- Channelizing Device
- Temporary Pavement Marking, 5 in (Black)
- Direction of Traffic

Temporary Pavement Marking Legend

- ① Temporary Pavement Marking, 4in. (White)
- ② Temporary Pavement Marking, 4in. (Yellow)
- ③ Temporary Transverse Pavement Marking, 24in. (Stop Bar)
- ④ Temporary Pavement Marking, 5in. (Black)

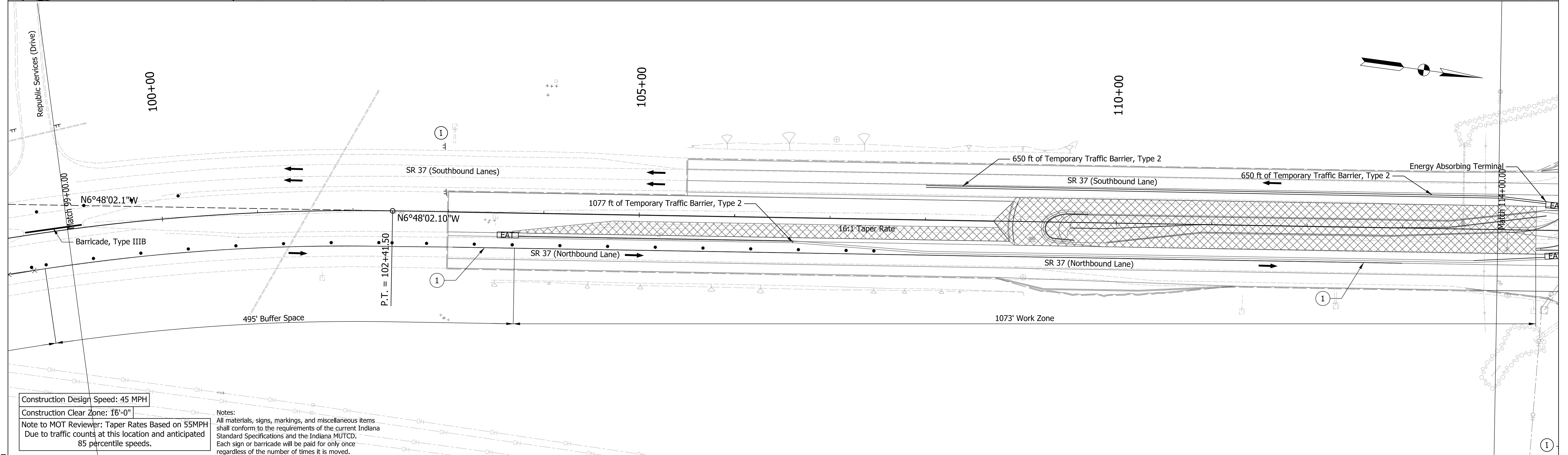
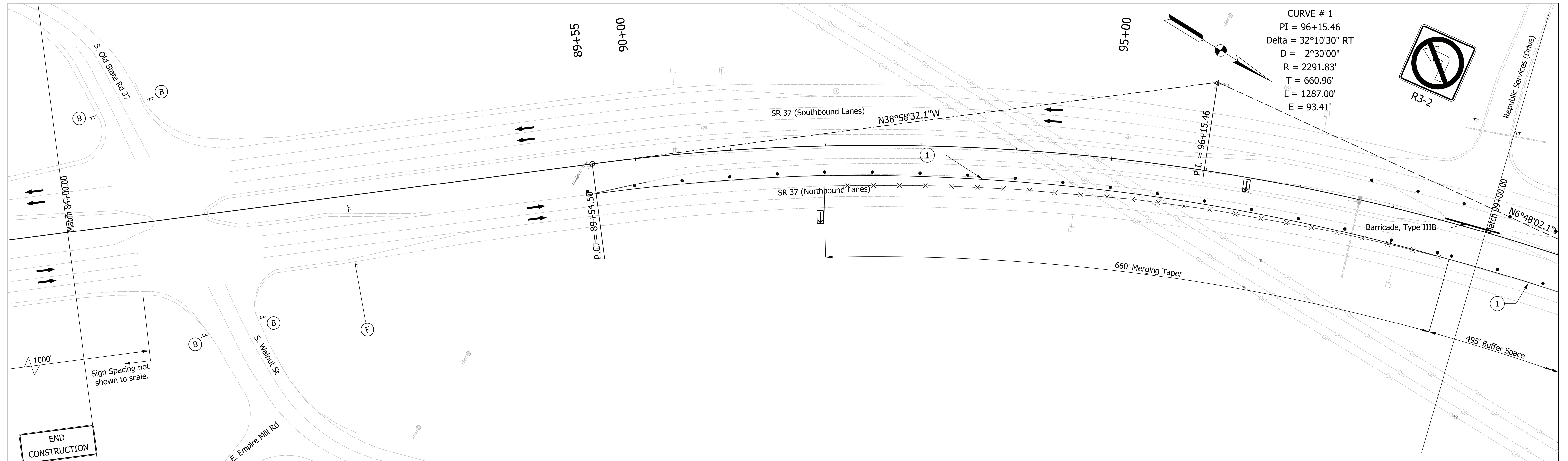
RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: RRC	April 2022	DRAWN: RRC
April 2022	April 2022	
CHECKED: AKB	April 2022	CHECKED: AKB
April 2022	April 2022	

INDIANA
DEPARTMENT OF TRANSPORTATION

**MAINTENANCE OF TRAFFIC
PHASE 2**

SCALE	BRIDGE FILE
1"=50'	
	DESIGNATION
	1800371
	SHEETS
	8 of 51
CONTRACT	PROJECT
R-42414	1800371

Plot: 4/14/2022 10:06 PM



Construction Design Speed: 45 MPH
 Construction Clear Zone: 16'-0"
 Note to MOT Reviewer: Taper Rates Based on 55MPH
 Due to traffic counts at this location and anticipated
 85 percentile speeds.

Notes:
 All materials, signs, markings, and miscellaneous items
 shall conform to the requirements of the current Indiana
 Standard Specifications and the Indiana MUTCD.
 Each sign or barricade will be paid for only once
 regardless of the number of times it is moved.

Maintenance of Traffic Legend

- Construction Area
- Construction Sign
- Barricade Type III-A
- Channelizing Device
- Temporary Pavement Marking, 5 in (Black)
- Direction of Traffic

Temporary Pavement Marking Legend

- ① Temporary Pavement Marking, 4in. (White)
- ② Temporary Pavement Marking, 4in. (Yellow)
- ③ Temporary Transverse Pavement Marking, 24in. (Stop Bar)
- ④ Temporary Pavement Marking, 5in. (Black)

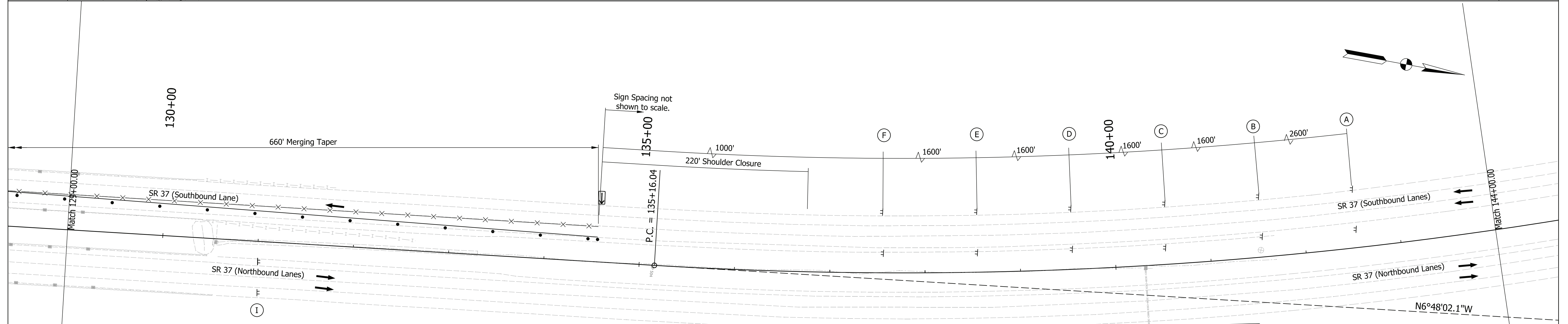
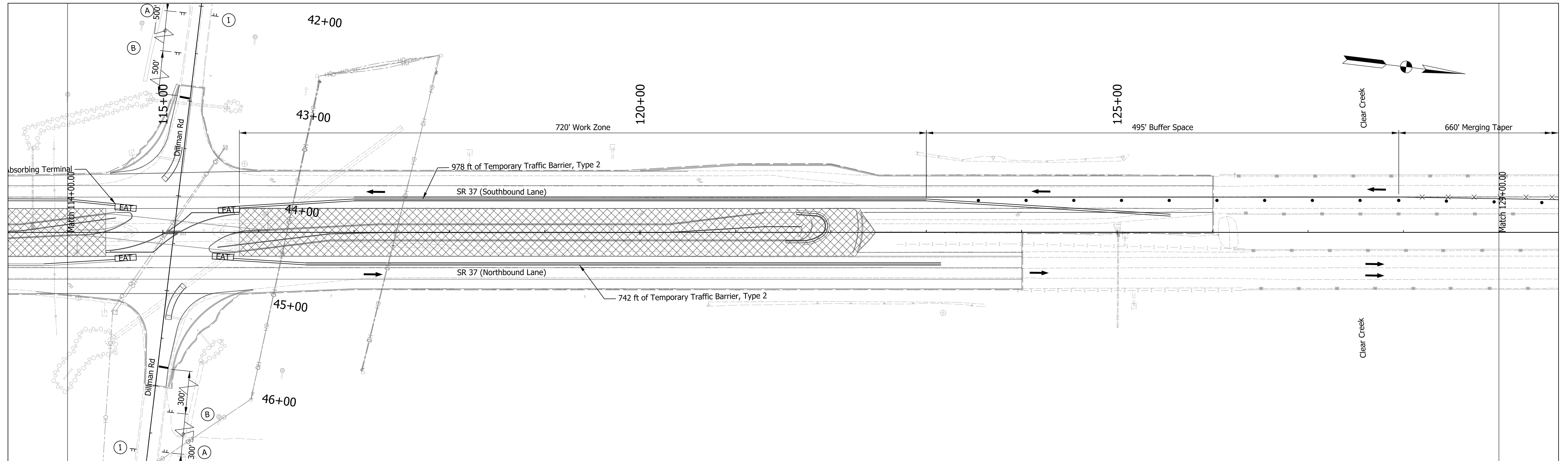
RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: RRC	April 2022	DRAWN: RRC
April 2022		April 2022
CHECKED: AKB	April 2022	CHECKED: AKB
		April 2022

INDIANA
DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC
PHASE 2

SCALE 1"=50'	BRIDGE FILE
	DESIGNATION 1800371
	SHEETS 9 of 51
CONTRACT R-42414	PROJECT 1800371

Plot: 4/14/2022 10:06 PM



Construction Design Speed: 45 MPH
 Construction Clear Zone: 16'-0"
 Note to MOT Reviewer: Taper Rates Based on 55MPH
 Due to traffic counts at this location and anticipated 85 percentile speeds.

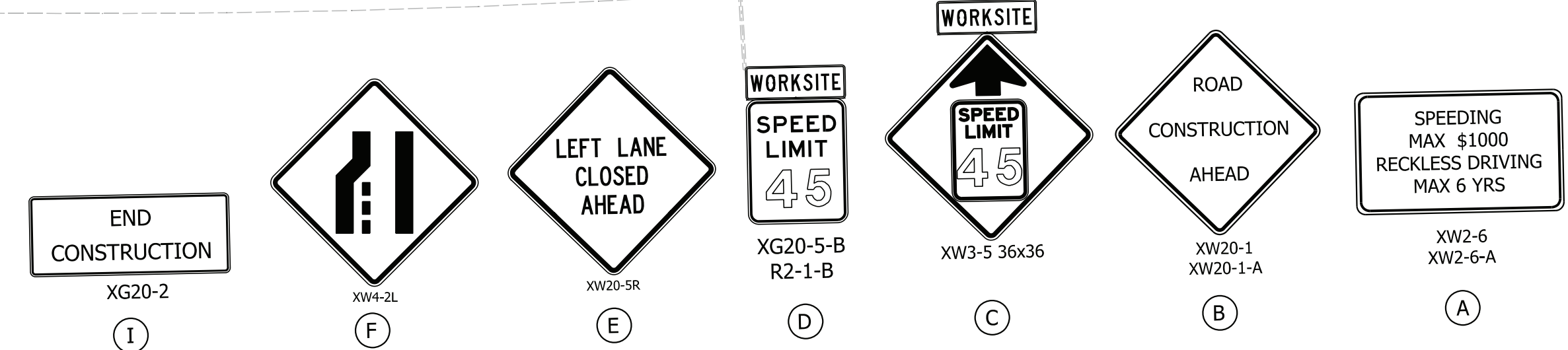
Notes:
 All materials, signs, markings, and miscellaneous items shall conform to the requirements of the current Indiana Standard Specifications and the Indiana MUTCD. Each sign or barricade will be paid for only once regardless of the number of times it is moved.

Maintenance of Traffic Legend

- Construction Area
- Construction Sign
- Barricade Type III-A
- Channelizing Device
- Temporary Pavement Marking, 5 in (Black)
- Direction of Traffic

Temporary Pavement Marking Legend

- ① Temporary Pavement Marking, 4in. (White)
- ② Temporary Pavement Marking, 4in. (Yellow)
- ③ Temporary Transverse Pavement Marking, 24in. (Stop Bar)
- ④ Temporary Pavement Marking, 5in. (Black)



Plot: 4/14/2022 10:07 PM

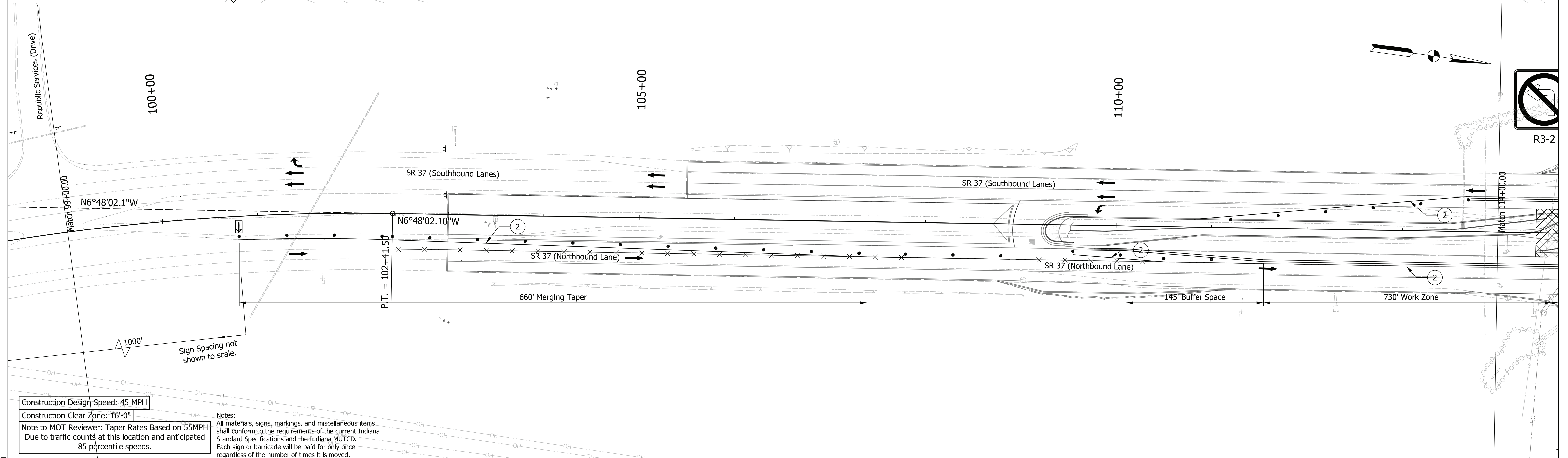
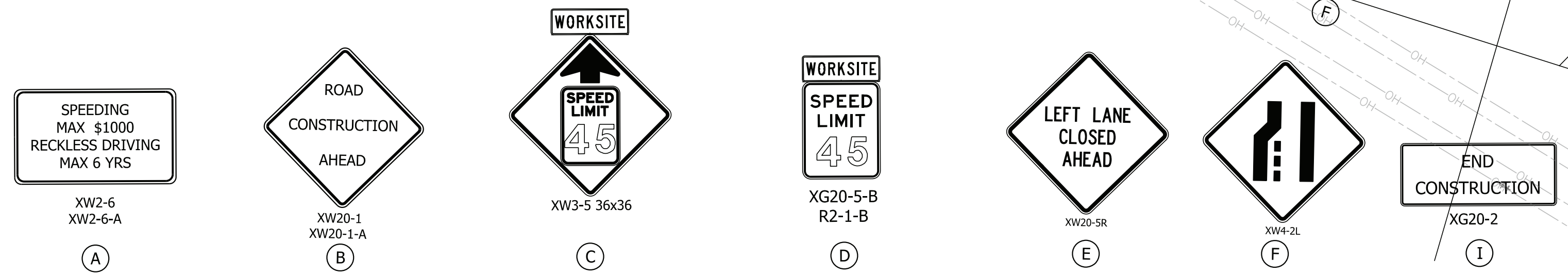
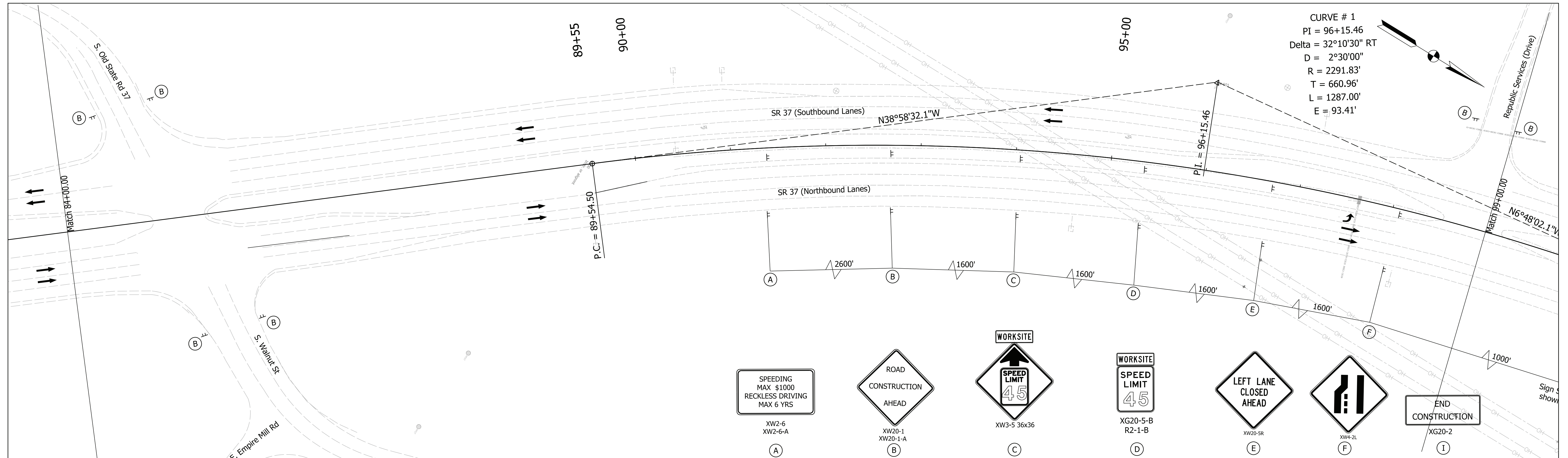
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RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: RRC	April 2022	DRAWN: RRC
April 2022		April 2022
CHECKED: AKB	April 2022	CHECKED: AKB
		April 2022

INDIANA
DEPARTMENT OF TRANSPORTATION

**MAINTENANCE OF TRAFFIC
PHASE 2**

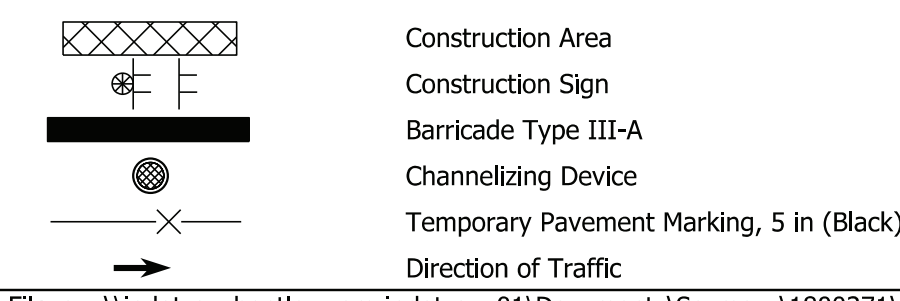
SCALE 1"=50'	BRIDGE FILE
	DESIGNATION 1800371
	SHEETS 10 of 51
CONTRACT R-42414	PROJECT 1800371



Construction Design Speed: 45 MPH
 Construction Clear Zone: 16'-0"
 Note to MOT Reviewer: Taper Rates Based on 55MPH Due to traffic counts at this location and anticipated 85 percentile speeds.

Notes:
 All materials, signs, markings, and miscellaneous items shall conform to the requirements of the current Indiana Standard Specifications and the Indiana MUTCD. Each sign or barricade will be paid for only once regardless of the number of times it is moved.

Maintenance of Traffic Legend



Temporary Pavement Marking Legend

- ① Temporary Pavement Marking, 4in. (White)
- ② Temporary Pavement Marking, 4in. (Yellow)
- ③ Temporary Transverse Pavement Marking, 24in. (Stop Bar)
- ④ Temporary Pavement Marking, 5in. (Black)

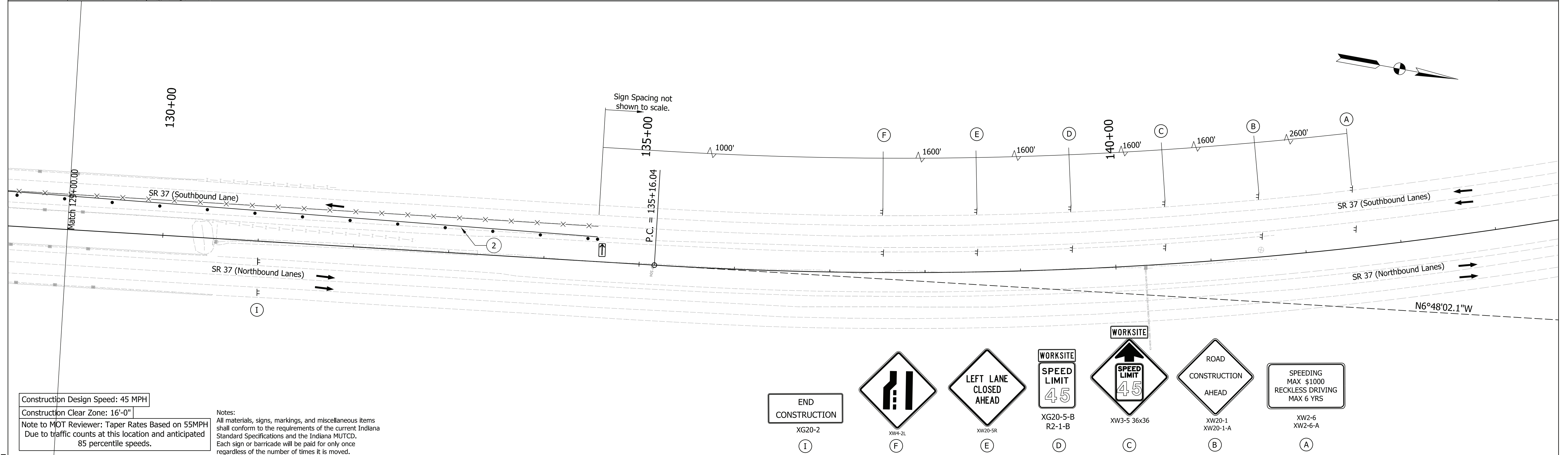
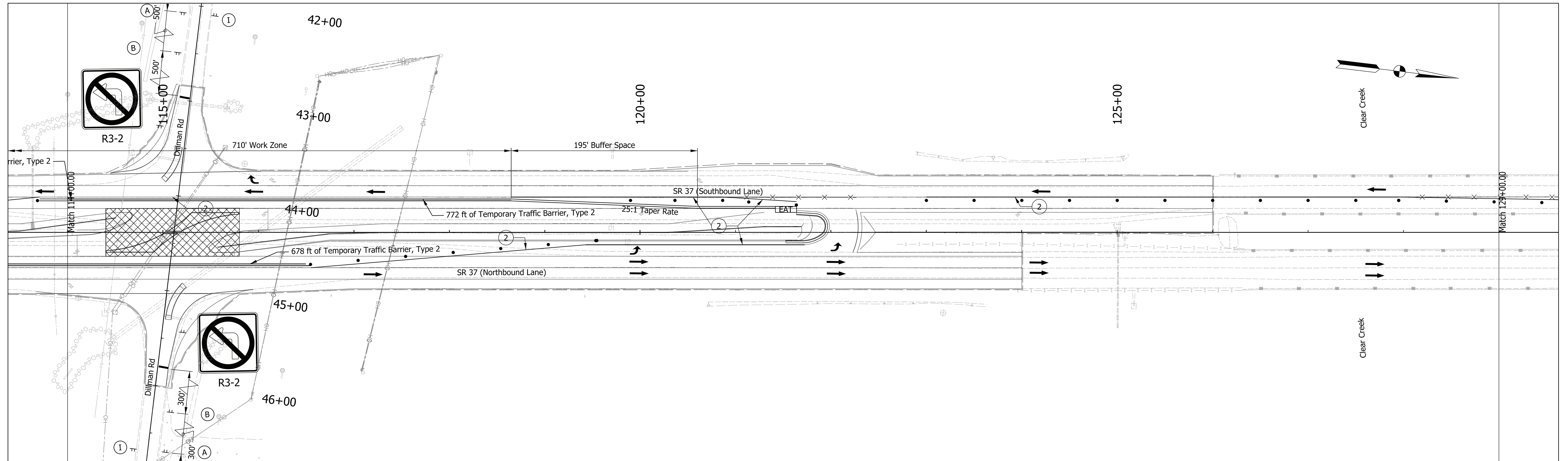
RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: RRC	April 2022	DRAWN: RRC April 2022
CHECKED: AKB	April 2022	CHECKED: AKB April 2022

INDIANA DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC PHASE 3

SCALE	BRIDGE FILE
1"=50'	DESIGNATION
	1800371
	SHEETS
	11 of 51
CONTRACT	PROJECT
R-42414	1800371

Plot: 4/14/2022 10:07 PM



Construction Design Speed: 45 MPH
 Construction Clear Zone: 16'-0"
 Note to MOT Reviewer: Taper Rates Based on 55MPH
 Due to traffic counts at this location and anticipated 85 percentile speeds.

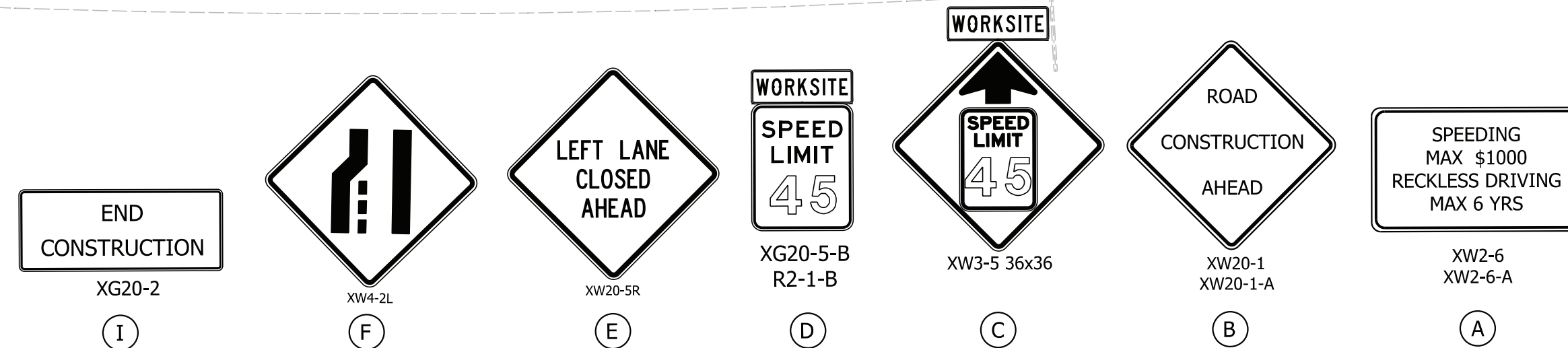
Notes:
 All materials, signs, markings, and miscellaneous items shall conform to the requirements of the current Indiana Standard Specifications and the Indiana MUTCD. Each sign or barricade will be paid for only once regardless of the number of times it is moved.

Maintenance of Traffic Legend

- Construction Area
- Construction Sign
- Barricade Type III-A
- Channelizing Device
- Temporary Pavement Marking, 5 in (Black)
- Direction of Traffic

Temporary Pavement Marking Legend

- ① Temporary Pavement Marking, 4in. (White)
- ② Temporary Pavement Marking, 4in. (Yellow)
- ③ Temporary Transverse Pavement Marking, 24in. (Stop Bar)
- ④ Temporary Pavement Marking, 5in. (Black)



Plot: 4/14/2022 10:08 PM

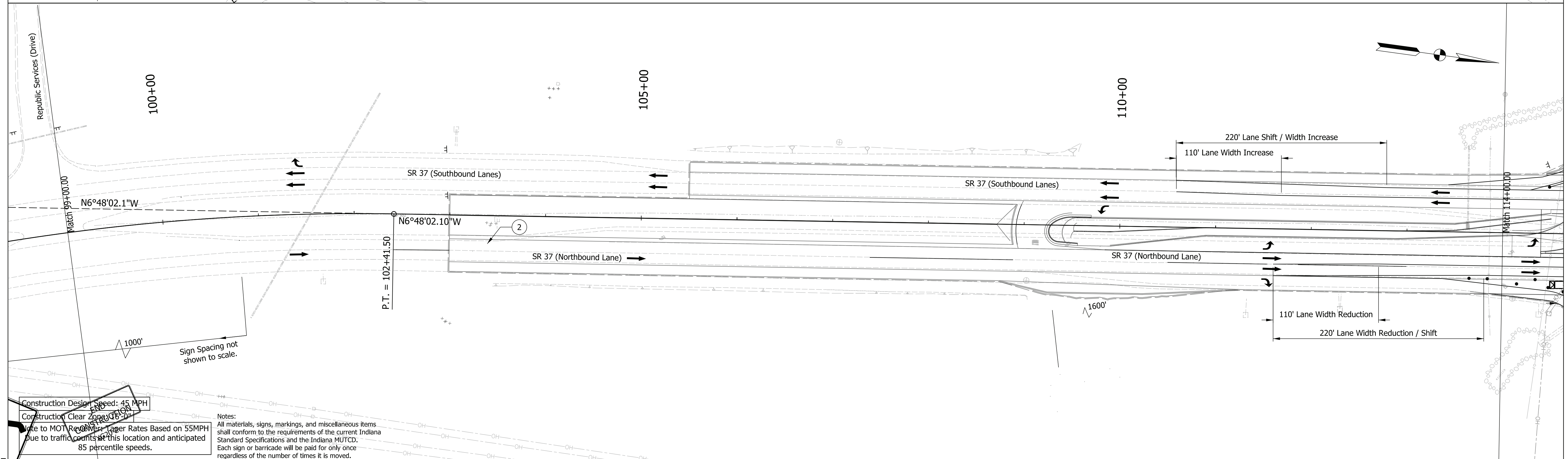
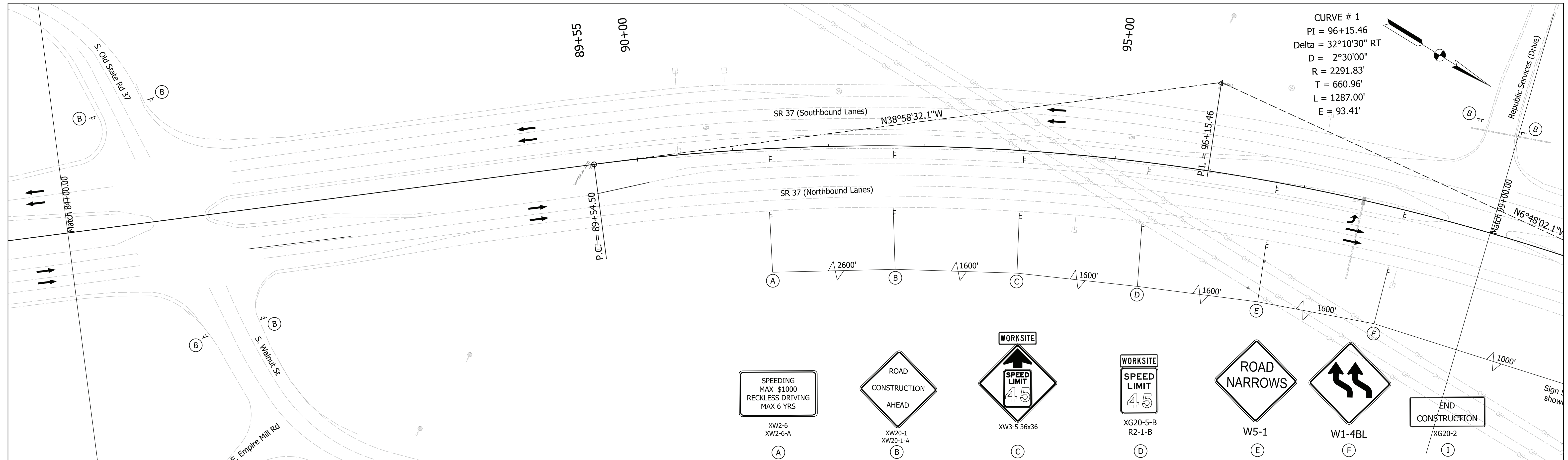
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RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: RRC	April 2022	DRAWN: RRC
CHECKED: AKB	April 2022	CHECKED: AKB
	April 2022	

INDIANA
DEPARTMENT OF TRANSPORTATION

**MAINTENANCE OF TRAFFIC
PHASE 3**

SCALE	BRIDGE FILE
1"=50'	
	DESIGNATION
	1800371
	SHEETS
	12 of 51
CONTRACT	PROJECT
R-42414	1800371



Maintenance of Traffic Legend

	Construction Area
	Construction Sign
	Barricade Type III-A
	Channelizing Device
	Temporary Pavement Marking, 5 in (Black)
	Direction of Traffic

Temporary Pavement Marking Legend

①	Temporary Pavement Marking, 4in. (White)
②	Temporary Pavement Marking, 4in. (Yellow)
③	Temporary Transverse Pavement Marking, 24in. (Stop Bar)
④	Temporary Pavement Marking, 5in. (Black)

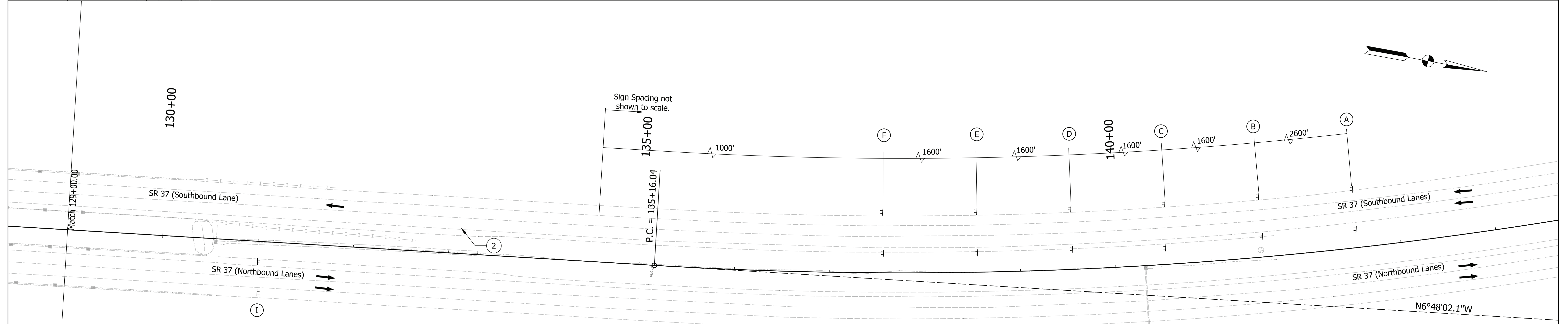
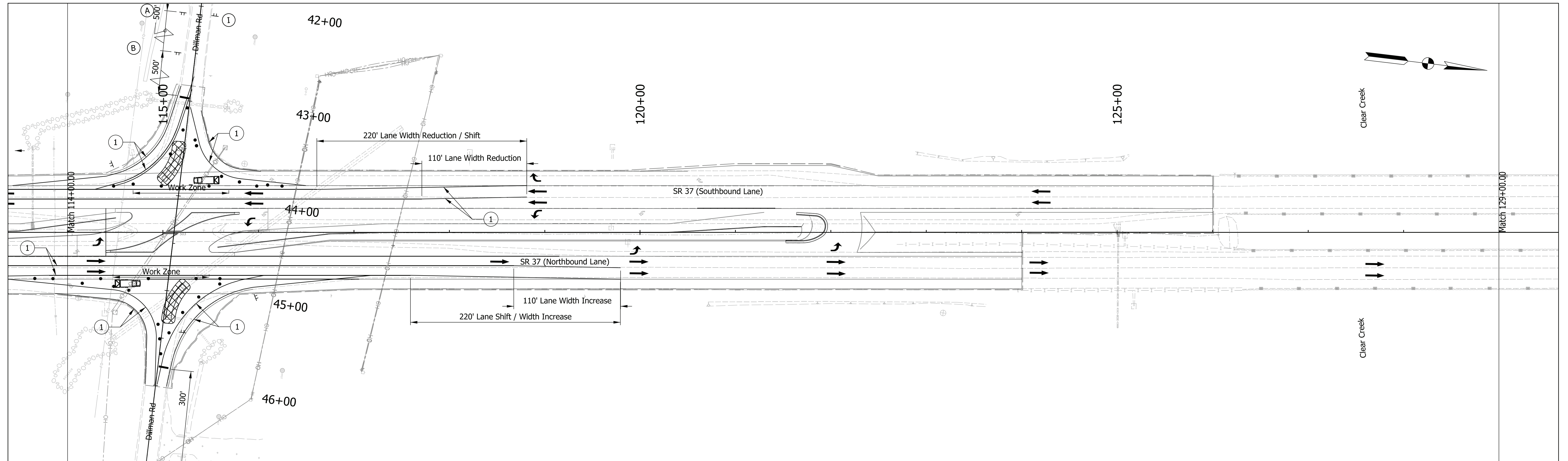
RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: RRC	April 2022	DRAWN: RRC
April 2022		April 2022
CHECKED: AKB	April 2022	CHECKED: AKB
April 2022		April 2022

INDIANA DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC PHASE 4

SCALE	BRIDGE FILE
1"=50'	
	DESIGNATION
	1800371
	SHEETS
	13 of 51
CONTRACT	PROJECT
R-42414	1800371

File: pw:\indot-pw.bentley.com\indot-pw-01\Documents\Seymour\1800371\Design\M5\ShT MOT PH4 PlanDetails_50_1.dgn
 Model: Default



Construction Design Speed: 45 MPH
 Construction Clear Zone: 16'-0"
 Note to MOT Reviewer: Taper Rates Based on 55MPH
 Due to traffic counts at this location and anticipated 85 percentile speeds.

Notes:
 All materials, signs, markings, and miscellaneous items shall conform to the requirements of the current Indiana Standard Specifications and the Indiana MUTCD. Each sign or barricade will be paid for only once regardless of the number of times it is moved.

Maintenance of Traffic Legend

- Construction Area
- Construction Sign
- Barricade Type III-A
- Channelizing Device
- Temporary Pavement Marking, 5 in (Black)
- Direction of Traffic

Temporary Pavement Marking Legend

- ① Temporary Pavement Marking, 4in. (White)
- ② Temporary Pavement Marking, 4in. (Yellow)
- ③ Temporary Transverse Pavement Marking, 24in. (Stop Bar)
- ④ Temporary Pavement Marking, 5in. (Black)

END CONSTRUCTION
 XG20-2
 ①

W1-4BL
 ⑥

W5-1
 ⑤

XG20-5-B
 R2-1-B
 ④

XW3-5 36x36
 ③

XW20-1
 XW20-1-A
 ②

XW2-6
 XW2-6-A
 ①

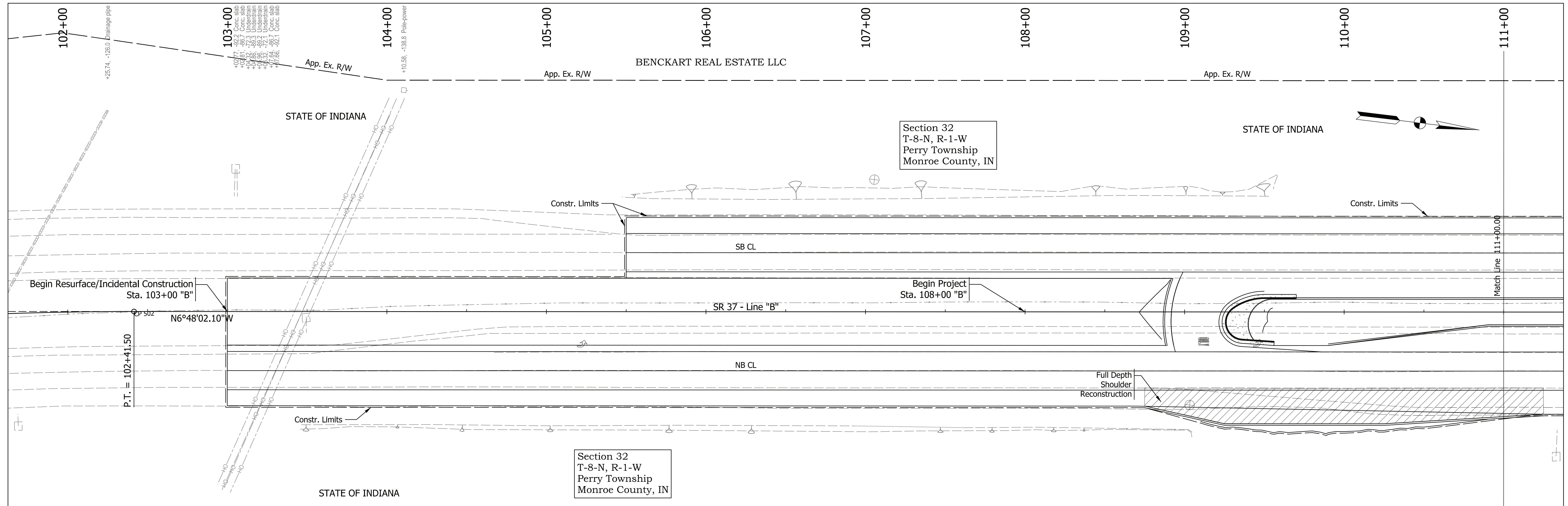
RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: RRC	April 2022	DRAWN: RRC
April 2022		April 2022
CHECKED: AKB	April 2022	CHECKED: AKB
April 2022		April 2022

INDIANA DEPARTMENT OF TRANSPORTATION
 MAINTENANCE OF TRAFFIC PHASE 4

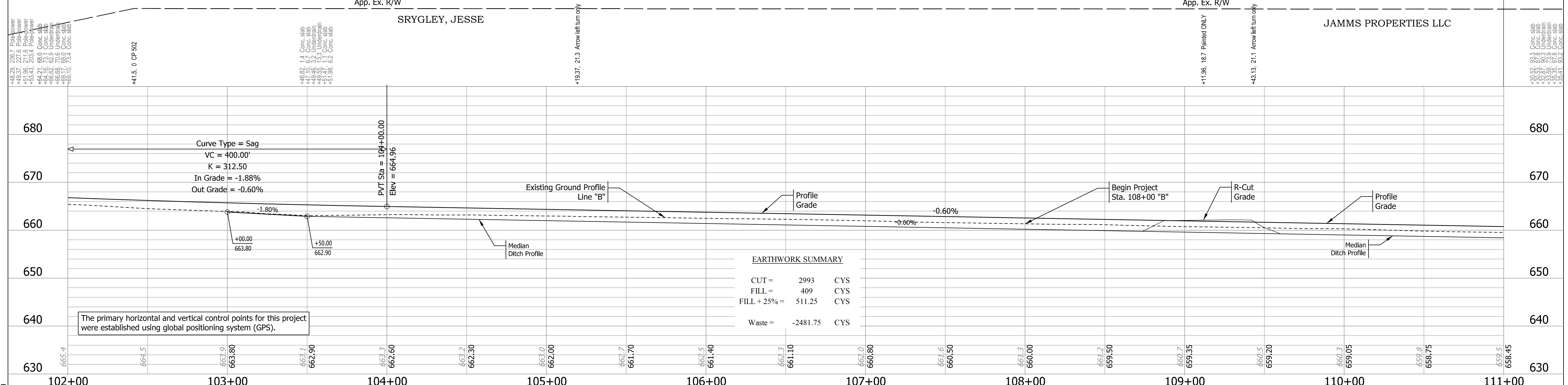
SCALE	BRIDGE FILE
1"=50'	
	DESIGNATION
	1800371
	SHEETS
	14 of 51
CONTRACT	PROJECT
R-42414	1800371

Plot: 4/14/2022 10:09 PM

File: pw:\indot-pw.bentley.com\indot-pw-01\Documents\Seymour\1800371\Design\M5\ShT MOT PH4 PlanDetails_50_2.dgn
 Model: Default



All topographic information is referenced from Line "B".
Line "B" to be constructed.



The primary horizontal and vertical control points for this project were established using global positioning system (GPS).

Control Points and Benchmarks:
 CP 501: PT Sta 95+98.00 (Survey Monument), Offset: 93.407, Northing: 168274.3322, Easting: 774598.8838
 CP 502: PT Sta 102+41.50 Offset: 0.00, Northing: 168930.6419, Easting: 774520.6168
 CP 503: Sta 115+11.51, Offset: 0.00, Northing: 170191.7165, Easting: 774370.2297
 TBM 1: Sta: 126+23.20, Offset: -61.10 (SW corner of SR 37 bridge SB), Northing: 171288.310, Easting: 774177.580, Elev: 652.212'

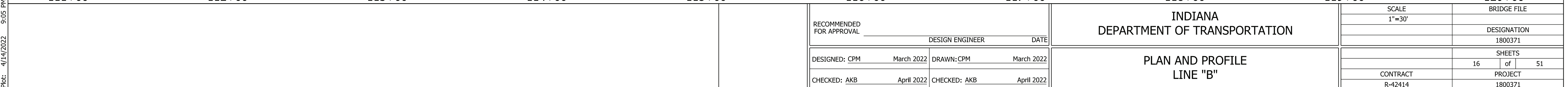
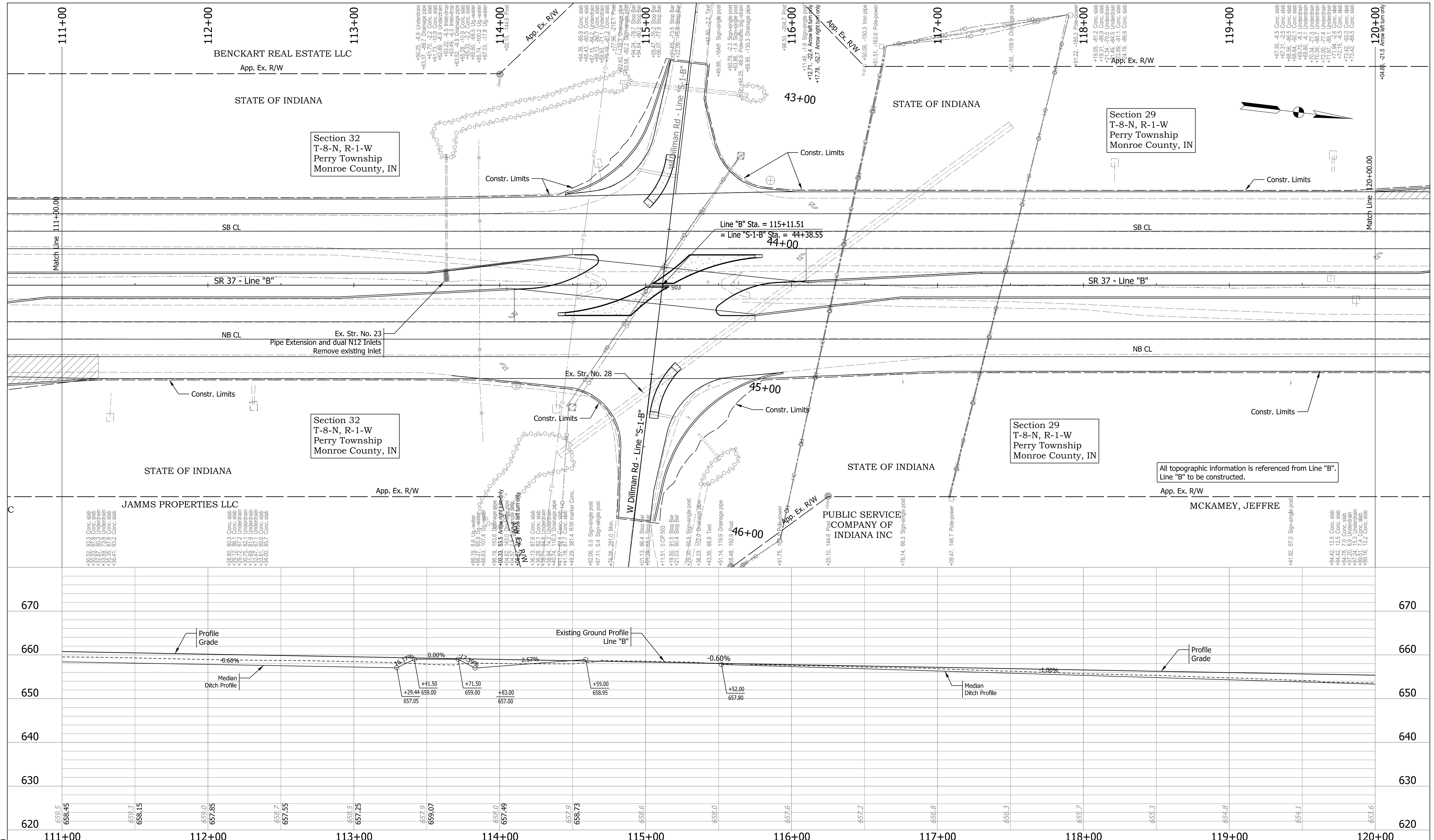
RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: CPM	March 2022	DRAWN: CPM
March 2022		March 2022
CHECKED: AKB	April 2022	CHECKED: AKB
April 2022		April 2022

INDIANA DEPARTMENT OF TRANSPORTATION

PLAN AND PROFILE LINE "B"

SCALE	BRIDGE FILE
1"=30'	
DESIGNATION	
1800371	
SHEETS	
15 of 51	
CONTRACT	PROJECT
R-42414	1800371

Plot: 4/14/2022 11:06 PM



Plot: 4/14/2022 9:05 PM

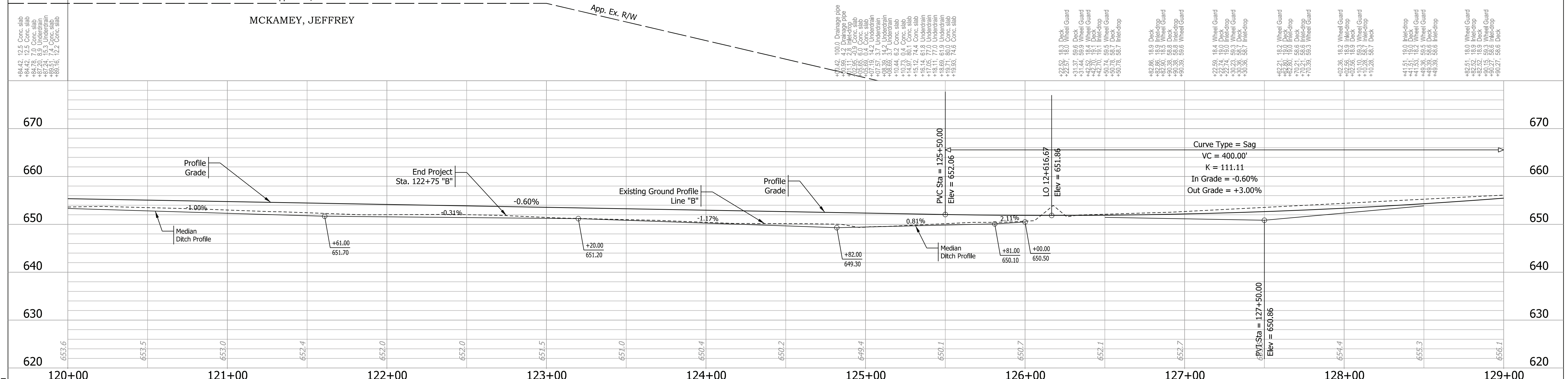
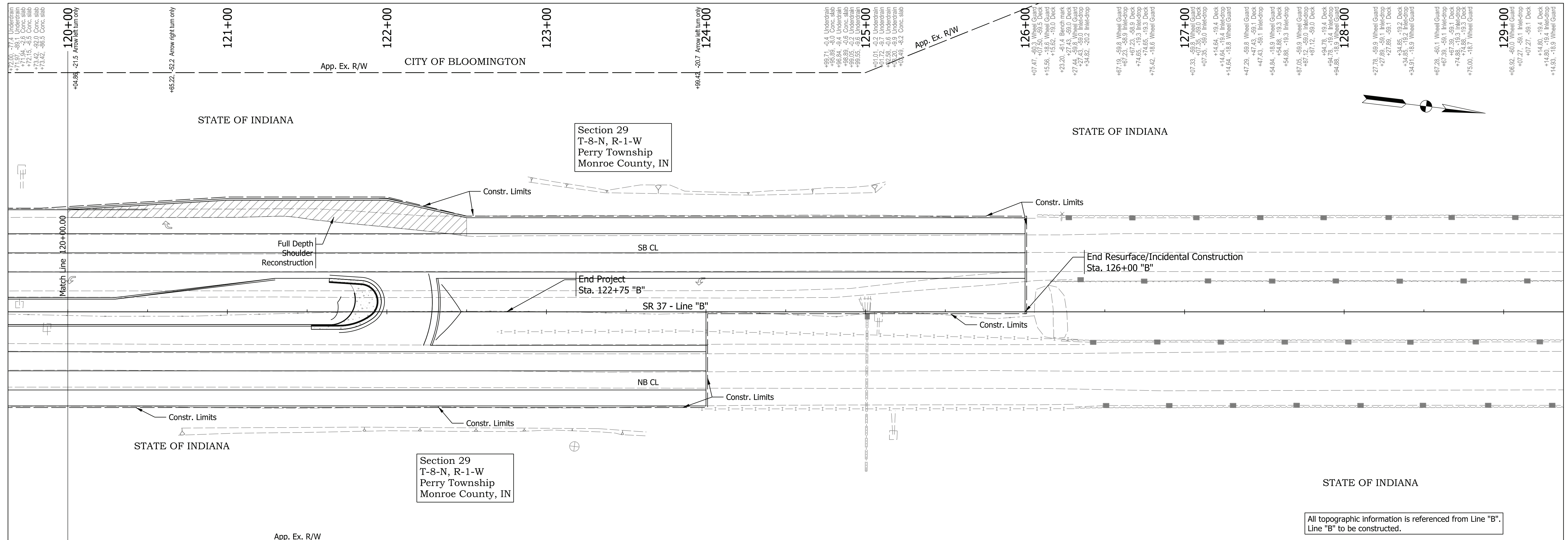
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RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: CPM	March 2022	DRAWN: CPM
CHECKED: AKB	April 2022	CHECKED: AKB
	April 2022	

INDIANA
DEPARTMENT OF TRANSPORTATION

PLAN AND PROFILE
LINE "B"

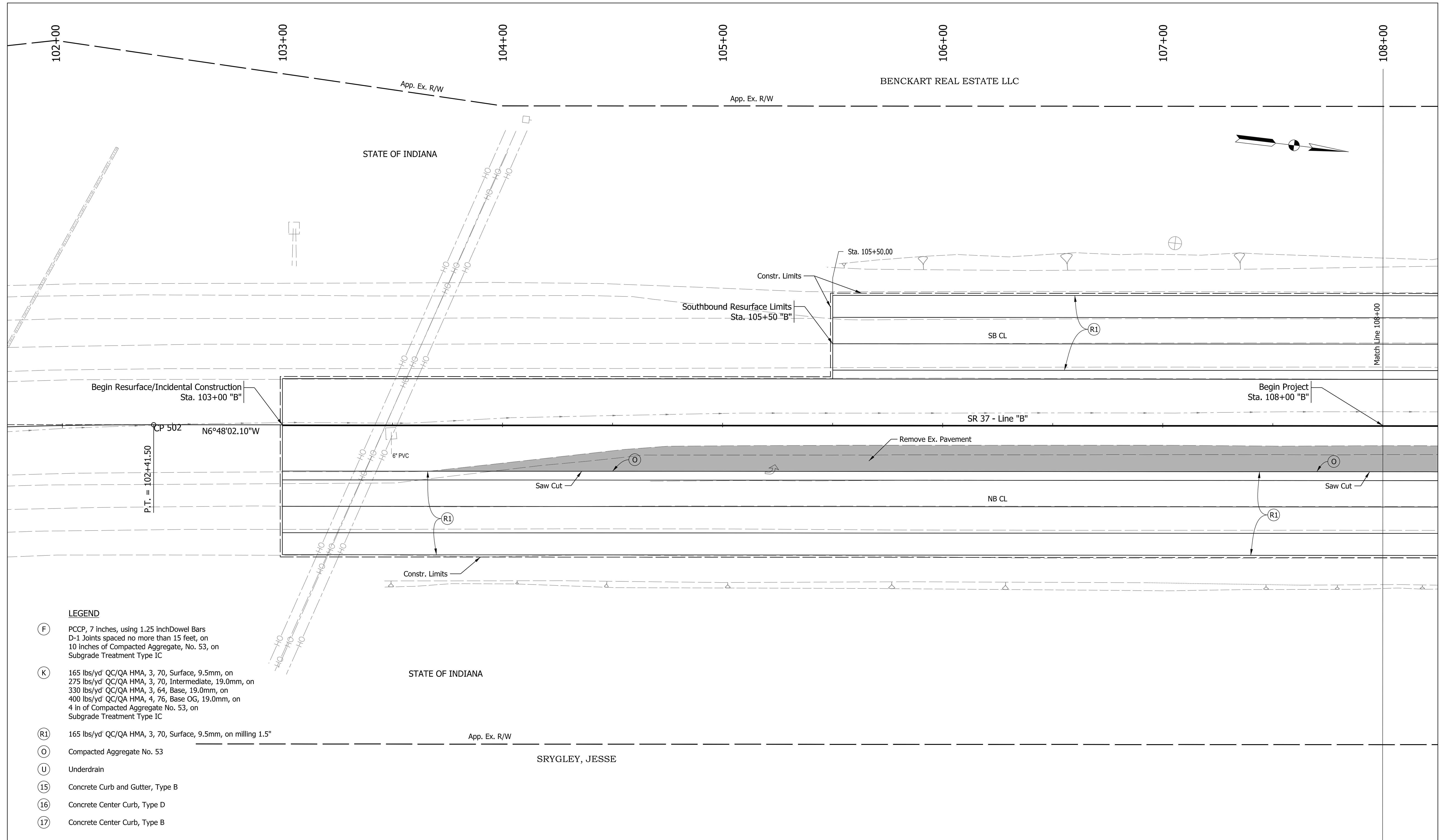
SCALE 1"=30'	BRIDGE FILE
	DESIGNATION 1800371
	SHEETS 16 of 51
CONTRACT R-42414	PROJECT 1800371



RECOMMENDED FOR APPROVAL		DESIGN ENGINEER		DATE	INDIANA DEPARTMENT OF TRANSPORTATION			SCALE 1"=30'	BRIDGE FILE	
DESIGNED: CPM	March 2022	DRAWN: CPM	March 2022		PLAN AND PROFILE LINE "B"			DESIGNATION		
CHECKED: MRS	March 2022	CHECKED: MRS	March 2022	1800371						
				SHEETS						
					CONTRACT			17 of 51		
					R-42414			PROJECT		
								1800371		

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 Model: Default

Plot: 4/14/2022 9:05 PM



LEGEND

- (F) PCCP, 7 inches, using 1.25 inch Dowel Bars
D-1 Joints spaced no more than 15 feet, on
10 inches of Compacted Aggregate, No. 53, on
Subgrade Treatment Type IC
- (K) 165 lbs/yd' QC/QA HMA, 3, 70, Surface, 9.5mm, on
275 lbs/yd' QC/QA HMA, 3, 70, Intermediate, 19.0mm, on
330 lbs/yd' QC/QA HMA, 3, 64, Base, 19.0mm, on
400 lbs/yd' QC/QA HMA, 4, 76, Base OG, 19.0mm, on
4 in of Compacted Aggregate No. 53, on
Subgrade Treatment Type IC
- (R1) 165 lbs/yd' QC/QA HMA, 3, 70, Surface, 9.5mm, on milling 1.5"
- (O) Compacted Aggregate No. 53
- (U) Underdrain
- (15) Concrete Curb and Gutter, Type B
- (16) Concrete Center Curb, Type D
- (17) Concrete Center Curb, Type B

Plot: 4/14/2022 9:05 PM

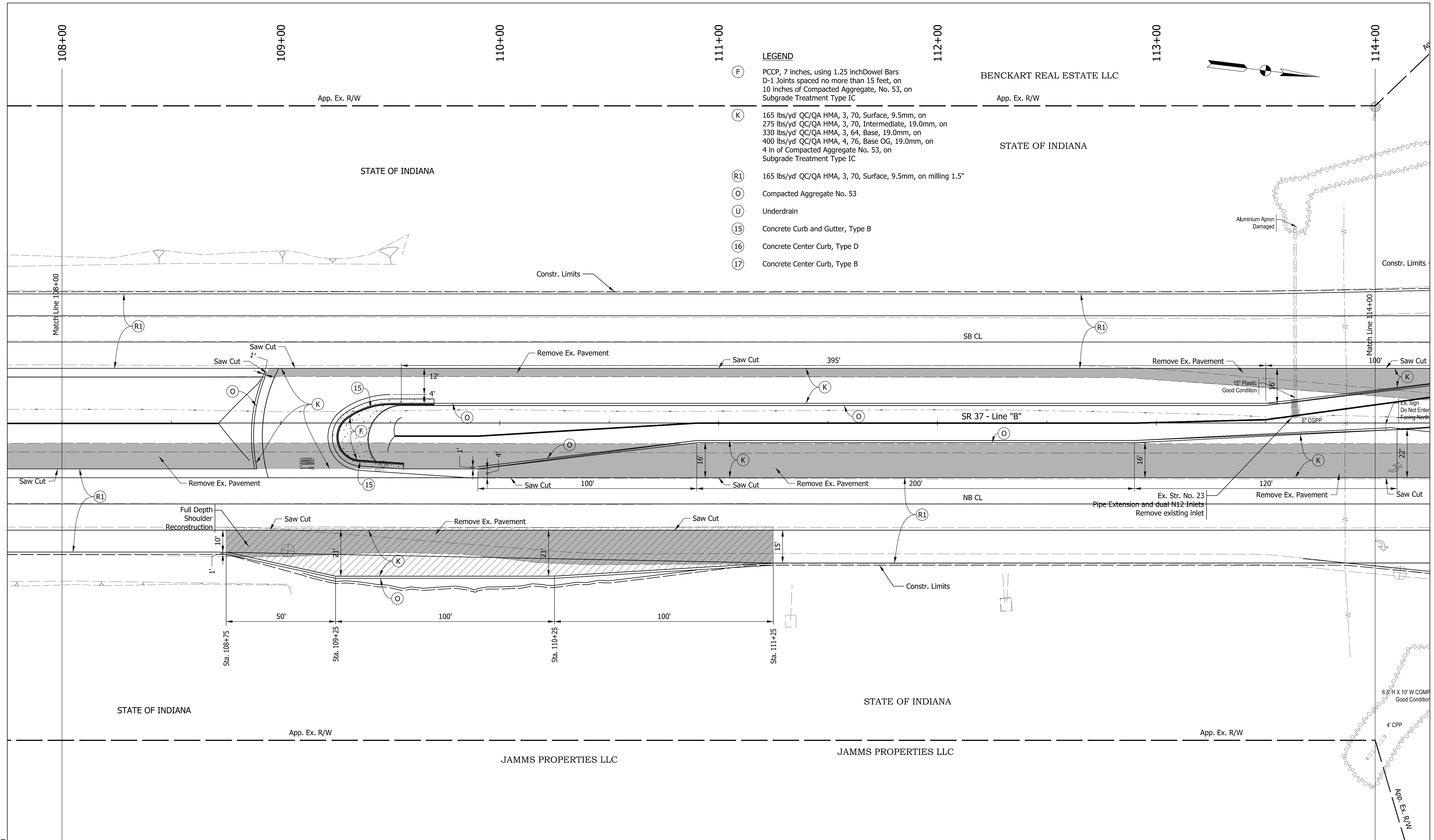
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RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: CPM	March 2022	DRAWN: CPM
CHECKED: MRS	March 2022	CHECKED: MRS
	March 2022	

**INDIANA
DEPARTMENT OF TRANSPORTATION**

**CONSTRUCTION DETAIL
LINE "B"**

SCALE 1"=20'	BRIDGE FILE
	DESIGNATION 1800371
	SHEETS 18 of 51
CONTRACT R-42414	PROJECT 1800371



LEGEND

- (F) PCCP, 7 inches, using 1.25 inch Dowel Bars D-1 Joints spaced no more than 15 feet, on 10 inches of Compacted Aggregate, No. 53, on Subgrade Treatment Type IC
- (K) 165 lbs/yd' QC/QA HMA, 3, 70, Surface, 9.5mm, on 275 lbs/yd' QC/QA HMA, 3, 70, Intermediate, 19.0mm, on 330 lbs/yd' QC/QA HMA, 3, 64, Base, 19.0mm, on 400 lbs/yd' QC/QA HMA, 4, 76, Base OG, 19.0mm, on 4 in of Compacted Aggregate No. 53, on Subgrade Treatment Type IC
- (R1) 165 lbs/yd' QC/QA HMA, 3, 70, Surface, 9.5mm, on milling 1.5"
- (O) Compacted Aggregate No. 53
- (U) Underdrain
- (15) Concrete Curb and Gutter, Type B
- (16) Concrete Center Curb, Type D
- (17) Concrete Center Curb, Type B

BENCKART REAL ESTATE LLC

STATE OF INDIANA

JAMMS PROPERTIES LLC

JAMMS PROPERTIES LLC

INDIANA DEPARTMENT OF TRANSPORTATION

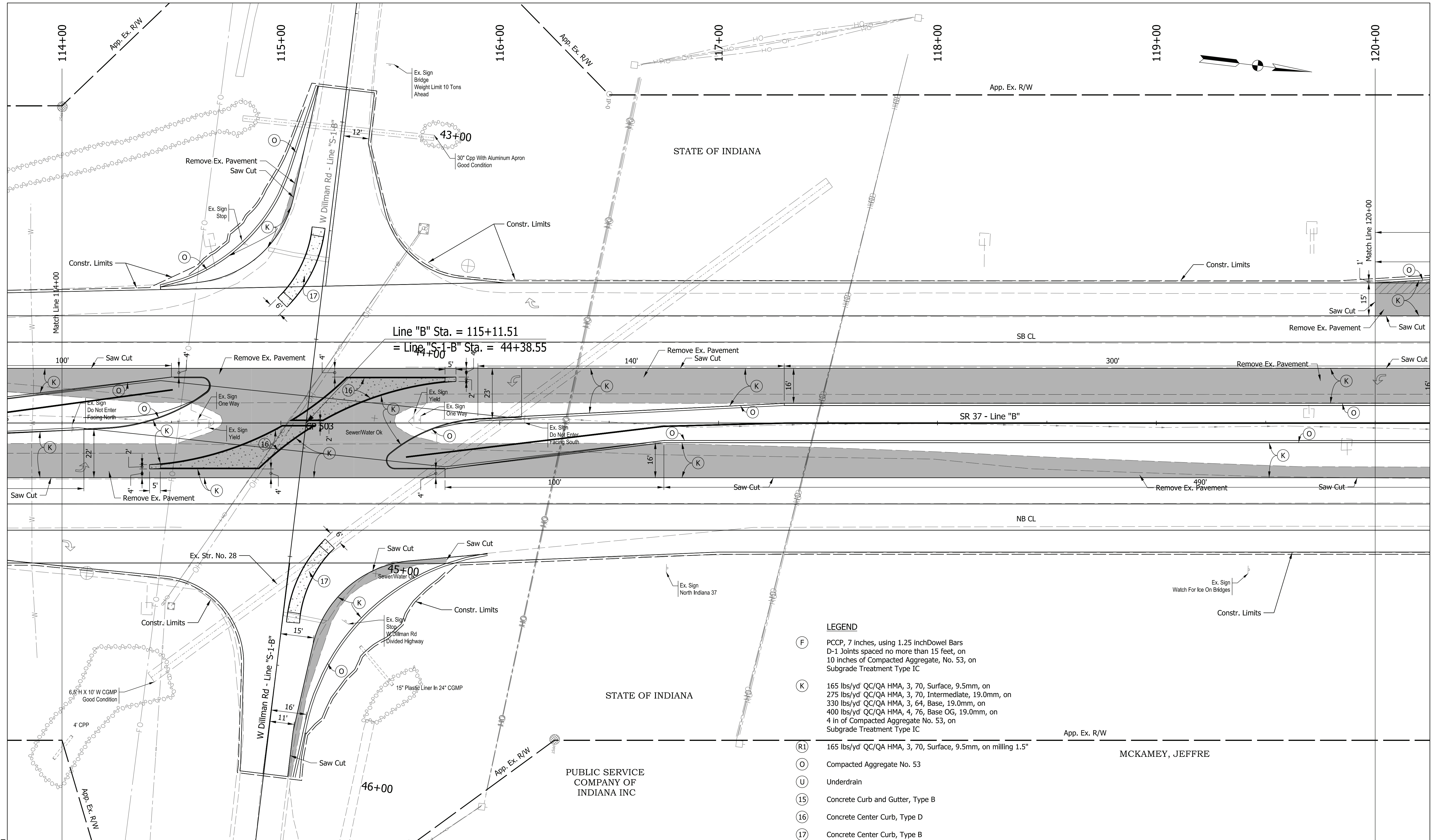
CONSTRUCTION DETAIL LINE "B"

SCALE 1"=20'	BRIDGE FILE
	DESIGNATION 1800371
	SHEETS 19 of 51
CONTRACT R-42414	PROJECT 1800371

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: CPM	March 2022	DRAWN: CPM
CHECKED: MRS	March 2022	CHECKED: MRS

Plot: 4/14/2022 9:05 PM

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Model: Default



- LEGEND**
- (F) PCCP, 7 inches, using 1.25 inch Dowel Bars D-1 Joints spaced no more than 15 feet, on 10 inches of Compacted Aggregate, No. 53, on Subgrade Treatment Type IC
 - (K) 165 lbs/yd' QC/QA HMA, 3, 70, Surface, 9.5mm, on 275 lbs/yd' QC/QA HMA, 3, 70, Intermediate, 19.0mm, on 330 lbs/yd' QC/QA HMA, 3, 64, Base, 19.0mm, on 400 lbs/yd' QC/QA HMA, 4, 76, Base OG, 19.0mm, on 4 in of Compacted Aggregate No. 53, on Subgrade Treatment Type IC
 - (R1) 165 lbs/yd' QC/QA HMA, 3, 70, Surface, 9.5mm, on milling 1.5"
 - (O) Compacted Aggregate No. 53
 - (U) Underdrain
 - (15) Concrete Curb and Gutter, Type B
 - (16) Concrete Center Curb, Type D
 - (17) Concrete Center Curb, Type B

MCKAMEY, JEFFRE

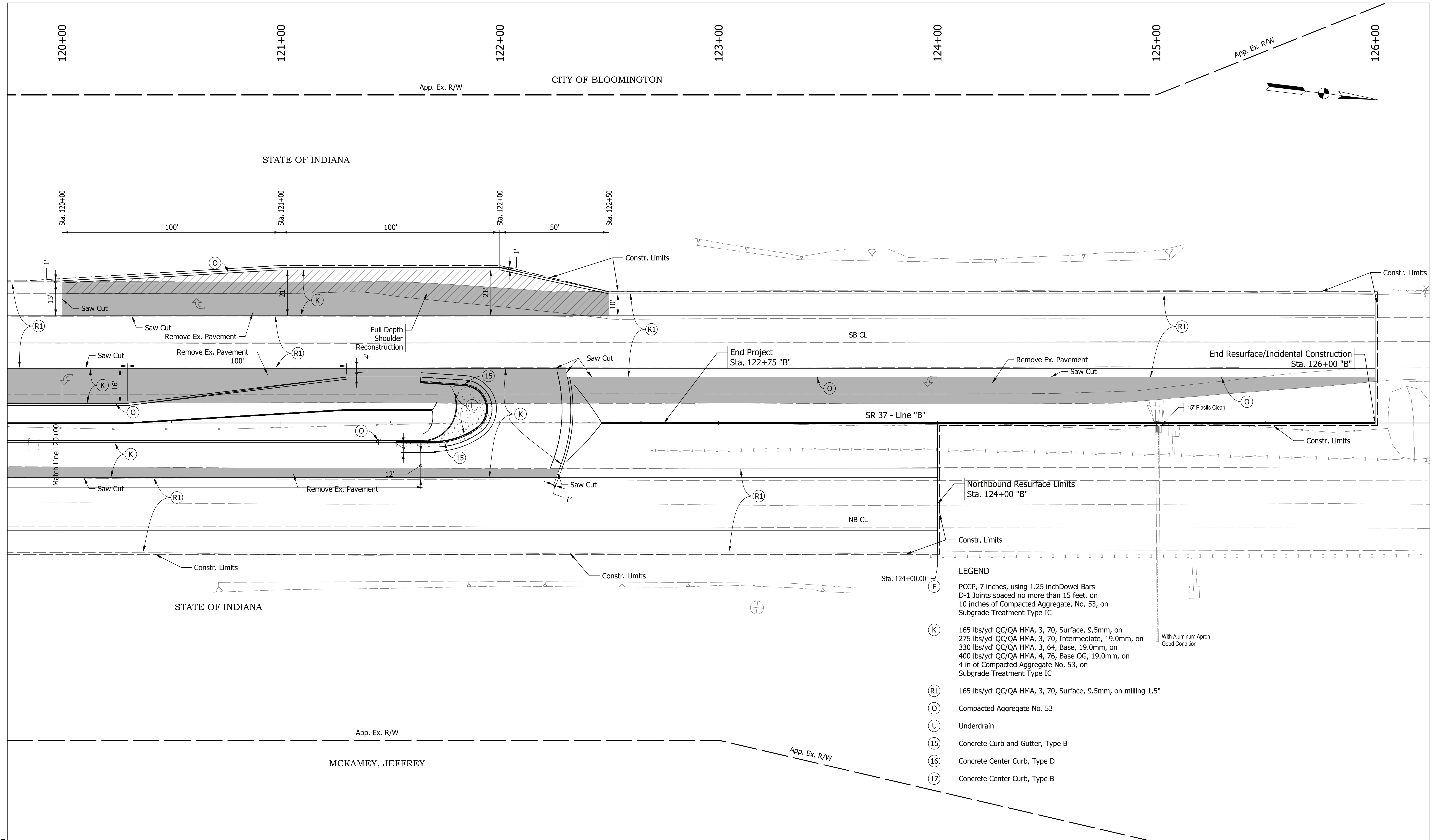
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RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: CPM	March 2022	DRAWN: CPM
CHECKED: MRS	March 2022	CHECKED: MRS
	March 2022	

INDIANA DEPARTMENT OF TRANSPORTATION	
CONSTRUCTION DETAIL LINE "B"	

SCALE 1"=20'	BRIDGE FILE
	DESIGNATION 1800371
	SHEETS 20 of 51
CONTRACT R-42414	PROJECT 1800371



- LEGEND**
- (F) PCCP, 7 inches, using 1.25 inch Dowel Bars
D-1 Joints spaced no more than 15 feet, on
10 inches of Compacted Aggregate, No. 53, on
Subgrade Treatment Type IC
 - (K) 165 lbs/yd QC/QA HMA, 3, 70, Surface, 9.5mm, on
275 lbs/yd QC/QA HMA, 3, 70, Intermediate, 19.0mm, on
330 lbs/yd QC/QA HMA, 3, 64, Base, 19.0mm, on
400 lbs/yd QC/QA HMA, 4, 76, Base OG, 19.0mm, on
4 in of Compacted Aggregate No. 53, on
Subgrade Treatment Type IC
 - (R1) 165 lbs/yd QC/QA HMA, 3, 70, Surface, 9.5mm, on milling 1.5"
 - (O) Compacted Aggregate No. 53
 - (U) Underdrain
 - (15) Concrete Curb and Gutter, Type B
 - (16) Concrete Center Curb, Type D
 - (17) Concrete Center Curb, Type B

Plot: 4/14/2022 9:05 PM

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RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: CPM	March 2022	DRAWN: CPM
March 2022		March 2022
CHECKED: MRS	March 2022	CHECKED: MRS
		March 2022

**INDIANA
DEPARTMENT OF TRANSPORTATION**

**CONSTRUCTION DETAIL
LINE "B"**

SCALE	BRIDGE FILE
1"=20'	
DESIGNATION	
1800371	
SHEETS	
21 of 51	
CONTRACT	PROJECT
R-42414	1800371