

CATEGORICAL EXCLUSION / ENVIRONMENTAL ASSESSMENT FORM
 GENERAL PROJECT INFORMATION

Road No./County:	SR 58, Bartholomew County, Indiana
Designation Number(s):	Des. No. 2100568
Project Description/Termini:	Bridge Replacement Project, 5.61 miles west of I-65

X	Categorical Exclusion, Level 2 – Required Signatories: INDOT DE and/or INDOT ESD
	Categorical Exclusion, Level 3 – Required Signatories: INDOT ESD
	Categorical Exclusion, Level 4 – Required Signatories: INDOT ESD and FHWA
	Environmental Assessment (EA) – Required Signatories: INDOT ESD and FHWA
	Additional Investigation (AI) – The proposed action included a design change from the original approved environmental document. Required Signatories must include the appropriate environmental approval authority

Approval

_____ INDOT DE Signature and Date _____ INDOT ESD Signature and Date

_____ FHWA Signature and Date

Release for Public Involvement


2024.03.26
14:34:33 -04'00'

_____ INDOT DE Initials and Date _____ INDOT ESD Initials and Date

Certification of Public Involvement

_____ INDOT Consultant Services Signature and Date

INDOT DE/ESD Reviewer Signature and Date: _____

Name and Organization of CE/EA Preparer: Rachel Pluckebaum and Kirk Roth, Corradino, LLC

Indiana Department of Transportation

County Bartholomew Route SR 58 Des. No. 2100568

Note: Refer to the most current INDOT CE Manual, guidance language, and other ESD resources for further guidance regarding any section of this form.

Part I – Public Involvement

Every Federal action requires some level of public involvement, providing for early and continuous opportunities throughout the project development process. The level of public involvement should be commensurate with the proposed action.

Does the project have a historic bridge processed under the Historic Bridges PA*?	Yes <input type="checkbox"/>	No <input checked="" type="checkbox"/>
If No, then: Opportunity for a Public Hearing Required?	<input checked="" type="checkbox"/>	<input type="checkbox"/>

*A public hearing is required for all historic bridges processed under the Historic Bridges Programmatic Agreement between INDOT, FHWA, SHPO, and the ACHP.

Discuss what public involvement activities (legal notices, letters to affected property owners and residents (i.e. notice of entry), meetings, special purpose meetings, newspaper articles, etc.) have occurred for this project.

Notice of Entry letters were mailed to potentially affected property owners near the project area on August 1, 2022 notifying them about the project and that individuals responsible for land surveying and field activities may be seen in the area. A sample copy of the Notice of Entry letter is included in Appendix G-2 to G-3.

To meet the public involvement requirements of Section 106, a legal notice of FHWA's finding of "No Historic Properties Affected" was published in *The Republic* on November 1, 2023 offering the public an opportunity to submit comment pursuant to 36 CFR 800.2(d), 800.3(e), and 800.6(a)(4). The public comment period closed 30 days later on December 1, 2023. The text of the public notice and the affidavit of publication appear in Appendix D-61 to D-63. No comments were received during the comment period.

The project will meet the minimum requirements described in the current *Indiana Department of Transportation (INDOT) Project Development Public Involvement Procedures Manual* which requires the project sponsor to offer the public an opportunity to submit comments and/or request a public hearing. Therefore, a legal notice will appear in a local publication contingent upon the release of this document for public involvement. This document will be revised after the public involvement requirements are fulfilled.

Public Controversy on Environmental Grounds

Discuss public controversy concerning community and/or natural resource impacts, including what is being done during the project to minimize impacts.

At this time, there is no substantial public controversy concerning impacts to the community or to natural resources.

Part II - General Project Identification, Description, and Design Information

Sponsor of the Project: Indiana Department of Transportation (INDOT) INDOT District: Seymour

Local Name of the Facility: SR 58

Funding Source (mark all that apply): Federal State Local Other*

*If other is selected, please identify the funding source: _____

PURPOSE AND NEED:

The need should describe the specific transportation problem or deficiency that the project will address. The purpose should describe the goal or objective of the project. The solution to the traffic problem should NOT be discussed in this section.

Need: The primary need for this project is due to the condition of the existing bridge (058-03-05882 B). There is light cracking and spalling in the curbs and there is longitudinal cracking on the deck. There is a 5-foot crack in a beam and efflorescence present

This is page 2 of 25 Project name: SR 58 Bridge Project Date: February 22, 2024

Indiana Department of Transportation

County Bartholomew Route SR 58 Des. No. 2100568

between all beams. Many of the "I" blocks are separating, and some have washed away. The structural evaluation rating from the bridge inspection report is a 5 (fair condition) on a scale from 0 (failed condition) to 9 (excellent condition). See the bridge inspection report dated January 3, 2022 for more details (Appendix I-2 to I-22). A secondary need for this project is due to the substandard roadway geometry. The existing roadway alignment at the structure consists of several small radii curves and short tangent sections, which do not meet the American Association of State and Highway and Transportation Officials (AASHTO) minimum requirements for roadway alignments.

Purpose: The purpose of this project is to provide a structure with a condition rating of good or better (7 or above) and to provide roadway geometry to meet current AASHTO standards.

PROJECT DESCRIPTION (PREFERRED ALTERNATIVE):

County: Bartholomew Municipality: N/A

Limits of Proposed Work: STA. 405+50.00 to STA. 421+00.00

Total Work Length: 0.293 Mile(s) Total Work Area: 6.75 Acre(s)

Is an Interstate Access Document (IAD)¹ required?
 If yes, when did the FHWA provide a Determination of Engineering and Operational Acceptability?

Yes ¹	No
<input type="checkbox"/>	<input checked="" type="checkbox"/>
Date: <input style="width: 100%;" type="text"/>	

¹If an IAD is required; a copy of the approved CE/EA document must be submitted to the FHWA with a request for final approval of the IAD.

Describe location of project including township, range, city, county, roads, etc. Existing conditions should include current conditions, current deficiencies, roadway description, surrounding features, etc. Preferred alternative should include the scope of work, anticipated impacts, and how the project will meet the Purpose and Need. Logical termini and independent utility also need discussed.

INDOT and the Federal Highway Administration (FHWA) intend to proceed with the small structure project.

Location: The structure is on SR 58, 5.61 miles west of Interstate 65 (I-65). The project is in Bartholomew County, Indiana, in Section 25 and Section 36, Township 8 North, Range 4 East (Appendix B-4).

Existing Conditions: The existing bridge structure on SR 58 is a single span prestressed concrete box beam structure with a 30-foot 5-inch span and a 67-foot length. As documented in the *Waters of the U.S. Determination* report, White Creek flows southeast under the structure (Appendix F-13). The surrounding land is rural. The existing bridge has longitudinal cracking on the top of the deck and spalling in the eastbound lane which is deteriorating despite previous patching efforts. There is a five-foot crack in the bottom of one of the beams and the west abutment, southwest wingwall, and southbound shoulder are being undermined. The existing SR 58 pavement section within the project area consists of pavement widths at approximately 24 feet wide with no aggregate shoulder outside the pavement. North of the bridge, travel lanes are 12 feet wide with no shoulder. Approximately 200 feet north of the bridge, the travel lanes transition to 11 feet wide with 1-foot usable shoulders. Travel lanes on the bridge consist of 11 feet wide with varying shoulder widths due to the curved roadway and tangent structure. This section of SR 58 is a *Rural Major Collector*.

A 12-inch diameter corrugated metal pipe 31 feet in length is situated in the CR 700 S approach. The existing condition of CR 700 S is a *Rural Local Road*. The surrounding land is rural. The existing CR 700 S pavement section within the project area consists of two 9-foot paved lanes and 1-foot usable shoulders.

Preferred Alternative: The preferred alternative is to replace the existing bridge with a three-span continuous concrete slab bridge with a 33-foot width and 100-foot length structure. The proposed structure will be realigned to the natural stream flow of White Creek approximately 50 foot downstream of the existing structure. The existing 24-foot-wide roadway approach will be replaced with a 26 to 30-foot paved roadway width.

SR 58 will be realigned for approximately 0.293 mile, 0.050 mile to the west and 0.243 mile to the north of the bridge. This alignment will remove the two substandard curves at the existing bridge and replace them with a single 315-foot radius curve with a full superelevation. The proposed alignment will shift the centerline approximately 150 feet to the east of the north approach of the

This is page 3 of 25 Project name: SR 58 Bridge Project Date: February 22, 2024

Indiana Department of Transportation

County Bartholomew Route SR 58 Des. No. 2100568

existing structure. Due to the realignment of SR 58, CR 700 S will be realigned with a perpendicular connection to the new SR 58 alignment to improve intersection sight distances. SR 58 will be raised slightly at the proposed bridge to increase clearance to the design storm event elevation. The existing lane and shoulder widths will be maintained. Milling and resurfacing will take place to connect the new pavement into the existing pavement. The SR 58 intersection with CR 700 S east of the structure will be reconstructed and shifted approximately 40 feet northeast of the existing intersection.

Two new structures will be placed as part of the project. A new culvert will be placed underneath SR 58 and the corrugated metal pipe underneath County Road 700 S will be replaced. New guardrail will be constructed in all four quadrants of the bridge. New ditches will be constructed along the new SR 58 alignment.

Construction impacts will be reduced to only the extent necessary to meet the project's purpose and need. Impact to trees and tributaries have been reduced to the extent practicable. See Appendix B-5 to B-17 for the design plans. The project will change the horizontal alignment of SR 58 and CR 700 S and the horizontal alignment of the SR 58 bridge. A wildlife crossing will be constructed under the bridge near the west end of the bridge. The wildlife crossing will run north and south (Appendix B-15 to B-16).

The maintenance of traffic (MOT) for this project will include a road closure on SR 58. SR 58 will be closed during construction and traffic will be detoured via SR 258, I-65, and SR 11. Refer to the Maintenance of Traffic section below for more details.

Logical Termini/Independent Utility: This alternative meets the project's purpose and need by providing a structure with a condition rating of best (9) and providing new roadway which meets current AASHTO standards. The project demonstrates independent utility because the purpose of maintaining the structure's integrity is not associated with any other projects, and it would be built regardless of any other projects in the area. Therefore, it is a single and complete project. The project termini are logical because they are limited to only that required to construct the project and fulfill the purpose of the project. Design plans provide details regarding the proposed project improvements (Appendix B-5 to B-17).

OTHER ALTERNATIVES CONSIDERED:

Provide a header for each alternative. Describe all discarded alternatives, including the No Build Alternative. Explain why each discarded alternative was not selected. Make sure to state how each alternative meets or does not meet the Purpose and Need and why.

Do Nothing Alternative: The no-build alternative was considered. This alternative has no costs and no environmental impacts. However, it does not meet the identified purpose of the project because it does not provide a sufficient structure with a condition rating of good or better (7 or above) and improve the roadway geometry to meet current AASHTO standards.

Structure Rehabilitation: The structure rehabilitation was considered. This alternative would extend the service life of the existing structure. However, the rehabilitation would not meet the purpose and need of the project since it does not improve the roadway geometry to meet current AASHTO standards.

Structure Replacement on Existing Alignment: A structure replacement on the existing roadway alignment was considered. This alternative would extend the service life of the existing structure. However, given the need to lengthen the existing structure over the existing conditions to construct the replacement structure, modern construction practices for bridges, and perpetuation of the existing alignment which does not meet current AASHTO standards, this alternative was determined to not be feasible.

The No Build Alternative is not feasible, prudent or practicable because (Mark all that apply)

- It would not correct existing capacity deficiencies;
- It would not correct existing safety hazards;
- It would not correct the existing roadway geometric deficiencies;
- It would not correct existing deteriorated conditions and maintenance problems; or
- It would result in serious impacts to the motoring public and general welfare of the economy.
- Other (Describe):

ROADWAY CHARACTER:

If the proposed action includes multiple roadways, complete and duplicate for each roadway.

Name of Roadway SR 58
 Functional Classification: Rural Major Collector
 Current ADT: 1715 VPD (2026) Design Year ADT: 1725 VPD (2046)

Indiana Department of Transportation

County Bartholomew Route SR 58 Des. No. 2100568

Design Hour Volume (DHV): 10.56% Truck Percentage (%) 5.0%
 45 mph (25 mph advisory at curve)
 Designed Speed (mph): 45 MPH Legal Speed (mph): 45 MPH

	Existing	Proposed
Number of Lanes:	2	2
Type of Lanes:	Two-lane segment	Two-lane segment
Pavement Width:	11 ft.	11 ft.
Shoulder Width:	1 ft.	3 ft.
Median Width:	0 ft.	0 ft.
Sidewalk Width:	0 ft.	0 ft.

Setting: Urban Suburban Rural
 Topography: Level Rolling Hilly

Name of Roadway CR 700 S
 Functional Classification: Rural Local Road
 Current ADT: 375 VPD (2026) Design Year ADT: 380 VPD (2046)
 Design Hour Volume (DHV): 38 Truck Percentage (%) 0.5
 Designed Speed (mph): 45 MPH Legal Speed (mph): 30 MPH

	Existing	Proposed
Number of Lanes:	2	2
Type of Lanes:	Two-lane segment	Two-lane segment
Pavement Width:	9 ft.	9 ft.
Shoulder Width:	1 ft.	3 ft.
Median Width:	0 ft.	0 ft.
Sidewalk Width:	0 ft.	0 ft.

Setting: Urban Suburban Rural
 Topography: Level Rolling Hilly

Indiana Department of Transportation

County Bartholomew Route SR 58 Des. No. 2100568

BRIDGES AND/OR SMALL STRUCTURE(S):

If the proposed action includes multiple structures, complete and duplicate for each bridge and/or small structure. Include both existing and proposed bridge(s) and/or small structure(s) in this section.

Structure/NBI Number(s): 058-03-05882 B (Structure #1) Sufficiency Rating: 76.0, Bridge Inspection (Appendix I-9)
021100 (Rating, Source of Information)

	Existing	Proposed
Bridge/Structure Type:	Box Beam	Reinforced Concrete Bridge
Number of Spans:	Single Span	Three Span
Weight Restrictions:	None	None
Height Restrictions:	None	None
Curb to Curb Width:	28'-6"	30'-0"
Outside to Outside Width:	30'-5"	33'-0"
Shoulder Width:	2'-0"	4'-0"

Structure/NBI Number(s): SR 58 Box Culvert (Structure #2) Sufficiency Rating: N/A
 (Rating, Source of Information)

	Existing	Proposed
Bridge/Structure Type:	N/A	Concrete Box Culvert
Number of Spans:	N/A	Single Span
Weight Restrictions:	N/A	None
Height Restrictions:	N/A	None
Curb to Curb Width:	N/A	30'-0"
Outside to Outside Width:	N/A	33'-0"
Shoulder Width:	N/A	4'-0"

Structure/NBI Number(s): CR 700 S Pipe (Structure #3) Sufficiency Rating: N/A
 (Rating, Source of Information)

	Existing	Proposed
Bridge/Structure Type:	Corrugated Metal Pipe	Smooth Circular Pipe
Number of Spans:	Single Span	Single Span
Weight Restrictions:	None	None
Height Restrictions:	None	None
Curb to Curb Width:	18'-0"	18'-0"
Outside to Outside Width:	31'-0"	116'-0"
Shoulder Width:	2'-0"	3'-0"

Structure/NBI Number(s): SR 58 Pipe Sufficiency Rating: N/A
 (Rating, Source of Information)

	Existing	Proposed
Bridge/Structure Type:	Corrugated Metal Pipe	Smooth Circular Pipe
Number of Spans:	Single Span	Single Span
Weight Restrictions:	None	N/A
Height Restrictions:	None	N/A
Curb to Curb Width:	28'-6"	N/A
Outside to Outside Width:	30'-5"	N/A
Shoulder Width:	2'-0"	N/A

Indiana Department of Transportation

County Bartholomew Route SR 58 Des. No. 2100568

Describe impacts and work involving bridge(s), culvert(s), pipe(s), and small structure(s). Provide details for small structure(s): structure number, type, size (length and dia.), location and impacts to water. Use a table if the number of small structures becomes large. If the table exceeds a complete page, put it in the appendix and summarize the information below with a citation to the table.

The existing structure (Structure #1; Appendix B-12) (058-03-05882 B) is comprised of a single span, prestressed concrete box beam with a 30-foot 5-inch width and a 67-foot length. The project will include the complete removal and replace the structure. The existing structure will be replaced with a 100-foot long, 33-foot wide three span reinforced concrete bridge. Scour protection (revetment riprap on geotextiles) will be placed at the inlet and outlet of the structure. Temporary dewatering measures will involve the installation of a cofferdam along the banks of White Creek for construction site dewatering.

The proposed structure (Structure #2; Appendix B-12) will be a 5-foot span, a 2-foot 6-inch rise, and a 75-foot length single span concrete box culvert. Scour protection (revetment riprap on geotextiles) will be placed at the outlet of the structure.

The existing structure (Structure #3; Appendix B-12) is comprised of a single span, corrugated metal pipe with 12-inch diameter and a 31-foot length. The project will include the complete removal and replace the structure. The existing structure will be replaced with a 36-inch diameter, 116-foot length, smooth circular pipe. Scour protection (revetment riprap on geotextiles) will be placed at the outlet of the structure.

The latest Historic Bridge Inventory (<http://www.in.gov/indot/2531.htm>) did not identify any historic structures on SR 58 at CR 700 S. The bridge (Bridge No. 058-05882 B) located within the project area is a single span prestressed concrete box beam. It was constructed in 1986. The superstructure and deck were replaced in 1980 (Appendix D-2). Structure #3 is a corrugated metal pipe with no wood, stone, or bricks parts. No additional structures are located within the project area.

MAINTENANCE OF TRAFFIC (MOT) DURING CONSTRUCTION:

	Yes	No
Is a temporary bridge proposed?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Is a temporary roadway proposed?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Will the project involve the use of a detour or require a ramp closure? (describe below)	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Provisions will be made for access by local traffic and so posted.	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Provisions will be made for through-traffic dependent businesses.	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Provisions will be made to accommodate any local special events or festivals.	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Will the proposed MOT substantially change the environmental consequences of the action?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Is there substantial controversy associated with the proposed method for MOT?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Will the project require a sidewalk, curb ramp, and/or bicycle lane closure? (describe below)	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Provisions will be made for access by pedestrians and/or bicyclist and so posted (describe below).	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Discuss closures, detours, and/or facilities (if any) that will be provided for maintenance of traffic. Any known impacts from these temporary measures should be quantified to the extent possible, particularly with respect to properties such as Section 4(f) resources and wetlands. Discuss any pedestrian/bicycle closures. Any local concerns about access and traffic flow should be detailed as well.

The MOT for this project will include a road closure during construction. SR 58 will be closed at the project area during construction and traffic will utilize SR 258, I-65, and SR 11. The detour is approximately 26 miles long and will be in place up to 45 days (Appendix B-10 to B-11).

The road closure will pose a temporary inconvenience to traveling motorists (including school buses and emergency services); however, no significant delays are anticipated, and all inconveniences and delays will cease upon project completion.

ESTIMATED PROJECT COST AND SCHEDULE:

Engineering: \$ \$388,363 (2024) Right-of-Way: \$ 50,000 (2025) Construction: \$ 3,700,000 (2027)

Anticipated Start Date of Construction: November 1, 2026

This is page 7 of 25 Project name: SR 58 Bridge Project Date: February 22, 2024

Indiana Department of Transportation

County Bartholomew Route SR 58 Des. No. 2100568

RIGHT OF WAY:

Land Use Impacts	Amount (acres)	
	Permanent	Temporary
Agricultural	3.02	N/A
Forest	1.62	N/A
Wetlands	0.056	N/A
Stream:	0.89	N/A
Grassy Roadside:	1.344	N/A
TOTAL	6.93	N/A

Describe both Permanent and Temporary right-of-way and describe their current use. Typical and Maximum right-of-way widths (existing and proposed) should also be discussed. Any advance acquisition, reacquisition or easements, either known or suspected, and their impacts on the environmental analysis should be discussed.

The existing right-of-way (ROW) along the approaches to the structure is approximately 11 feet to the east and west of the centerline of SR 58. Grants for the area cannot be found and the existing right of way was established as the edge of pavement.

The project requires approximately 6.93 acres of permanent ROW located in the northeast, northwest, and south quadrants. The ROW that will be acquired is rural farmland and wooded property.

If the scope of work or permanent or temporary ROW amounts change, the INDOT Environmental Services Division (ESD) and the INDOT District Environmental Section will be contacted immediately.

Part III – Identification and Evaluation of Impacts of the Proposed Action

SECTION A - EARLY COORDINATION:

List the date(s) coordination was sent and all resource agencies that were contacted as a part of the development of this Environmental Study. Also, include the date of their response or indicate that no response was received.

Early coordination letters were sent on February 7, 2023 unless otherwise indicated (Appendix C-2 to C-4).

Agency	Dates Sent	Date Response Received	Appendix
INDOT Environmental Policy Manager	February 8, 2023	No response received	N/A
Federal Highway Administration (FHWA)	February 7, 2023	No response received	N/A
Indiana Department of Natural Resources-Division of Fish and Wildlife (IDNR-DFW)	February 8, 2023	March 9, 2023	C-12 to C-15
INDOT - Seymour District Project Manager and Environmental Policy Manager	February 7, 2023	No response received	N/A
National Park Service	February 7, 2023	No response received	N/A
National Resource Conservation Service (NRCS)	February 8, 2023	February 15, 2023	C-16 to C-17
Bartholomew Consolidated School Corporation	February 7, 2023	No response received	N/A
U.S. Army Corps of Engineers (USACE)	February 7, 2023	No response received	N/A
U.S. Department of Housing & Urban Development (USDHUD)	February 7, 2023	No response received	N/A
U.S. Fish and Wildlife Service (USFWS)	February 8, 2023	February 15, 2023	C-5 to C-11
Bartholomew County Surveyor	February 7, 2023	No response received	N/A
Bartholomew County Highway Department	February 7, 2023	No response received	N/A
Indiana Geological and Water Survey (IGWS)	February 8, 2023	February 8, 2023 (Automated)	C-18 to C-20
USFWS Information for Planning and Consultation (IPaC) Portal	April 6, 2023	April 17, 2023	C-21 to C-46

This is page 8 of 25 Project name: SR 58 Bridge Project Date: February 22, 2024

Indiana Department of Transportation

County Bartholomew Route SR 58 Des. No. 2100568

Bartholomew County Commissioner	February 7, 2023	No response received	N/A
Bartholomew MS4 - Stormwater	February 7, 2023	No response received	N/A
Columbus Area Metropolitan Planning Organization (CAMPO)	November 20, 2023	No response received	N/A
Mt. Healthy Elementary School	February 12, 2024	February 20, 2024	C-50
Mt. Healthy Community Church (formerly Ogilville Methodist Church)	February 9, 2024	No response received	N/A

All applicable recommendations are included in the Environmental Commitments section of this CE document.

SECTION B – ECOLOGICAL RESOURCES:

Streams, Rivers, Watercourses & Other Jurisdictional Features	Presence	Impacts	
	Yes	Yes	No
Federal Wild and Scenic Rivers	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
State Natural, Scenic or Recreational Rivers	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Nationwide Rivers Inventory (NRI) listed	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Outstanding Rivers List for Indiana	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Navigable Waterways	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Total stream(s) in project area: 1765 Linear feet Total impacted stream(s): 370 Linear feet

Stream Name	Classification	Total Size in Project Area (linear feet)	Impacted linear feet	Comments (i.e. location, flow direction, likely Water of the U.S. appendix reference)
White Creek	Intermittent	1438	295 (permanent) 50 (temporary)	Project structure, flowing southeast, likely Water of the U.S.; Appendix F-13
UNT1 to White Creek	Ephemeral	165	15 (permanent) 10 (temporary)	Adjacent northwest of the project structure, flowing southeast, likely Water of the U.S. Appendix F-13
UNT2 to White Creek	Ephemeral	162	No impact.	0.23 mile north of the project structure at a culvert under SR 58, flowing west, likely Water of the U.S. Appendix F-13

Describe all streams, rivers, watercourses and other jurisdictional features adjacent or within the project area. Include whether or not impacts (both permanent and temporary) will occur to the features identified. Include if the streams or rivers are listed on any federal or state lists for Indiana. Include if features are likely subject to federal or state jurisdiction. Discuss measures to avoid, minimize, and mitigate if impacts will occur.

Based on the desktop review, the aerial map of the project area, and the RFI report (Appendix E-3 and E-4) there are seven streams, rivers, watercourse, or other jurisdictional features within the 0.5-mile search radius. There is one stream, river, watercourse, or other jurisdictional feature within or adjacent to the project area. That number was updated to three by the site visit on September 23, 2022 by Corradino, LLC.

A *Waters of the U.S. Determination/Wetland Delineation Report* was approved for the project on March 31, 2023. Please refer to Appendix F for the *Waters of the U.S. Determination/Wetland Delineation Report*. It was determined that three streams within or adjacent to the project are apparent Waters of the U.S. traceable to the navigable East Fork White River.

White Creek is an intermittent creek that flows southeast through the project bridge and has an ordinary high water mark (OHWM) 25 feet wide and 0.75 foot deep. The upstream drainage area is 6.97 square miles at the project location (Appendix F-17 to F-18). Up to 295 linear feet of permanent and 50 linear feet of temporary impacts to White Creek are anticipated. White Creek will have scour protection (revetment riprap on geotextiles) placed at the inlet and outlet of the structure. Temporary dewatering measures will involve the installation of a cofferdam along the banks for construction site dewatering (Appendix B-15).

UNT1 to White Creek is an ephemeral creek that flows southeast towards the project bridge from the northwest and has an OHWM 2 feet wide and 0.25 foot deep. The upstream drainage area is included in the 6.97-square mile watershed of White Creek (Appendix F-17 to F-18). Up to 15 linear feet of permanent and 10 linear feet of temporary impacts to UNT1 to White Creek are anticipated. UNT1 to White Creek will have scour protection (revetment riprap on geotextiles) and a cofferdam along the banks for construction site dewatering (Appendix B-15).

Indiana Department of Transportation

County Bartholomew Route SR 58 Des. No. 2100568

UNT2 to White Creek is an ephemeral creek that flows west under SR 58 at the north end of the project and has an OHWM 1.5 feet wide and 0.25 foot deep. The upstream drainage area is included in the 6.97-square mile watershed of White Creek (Appendix F-17 to F-18). Impacts to UNT2 to White Creek have been avoided during project design (Appendix B-14). No permanent or temporary impacts to UNT2 to White Creek are anticipated.

There are no Federal, Wild and Scenic Rivers, State Natural, Scenic and Recreational Rivers, Outstanding Rivers for Indiana, navigable waterways or National Rivers Inventory waterways present within or adjacent to the project area. Therefore, no impacts to these resources are expected. No mitigation is expected, but will be determined during permitting. Impacts to jurisdictional waterways has been reduced through project design to the extent practicable while still meeting the project purpose and need. A Section 404 Permit from USACE and a Section 401 Water Quality Certification from IDEM will be required for impacts to jurisdictional streams.

USFWS responded to early coordination on February 15, 2023 (Appendix C-5 to C-11) with recommendations to restrict low-water work, utilize natural substrate if possible, evaluate wildlife crossings, restrict channel work to the minimum necessary, minimize the extent of riprap, and avoid all work within the inundated part of the stream channel during the fish spawning season (April 1 through June 30).

IDNR-DFW responded to early coordination on March 9, 2023 (Appendix C-12 to C-15) with recommendations to avoid and minimize impacts to fish, wildlife, and botanical resources to the greatest extent possible, and compensate for impacts. The recommendations include: a structure that allows natural substrate to form; evaluate wildlife crossings; bank stabilization measures; minimization of channel disturbance due to tree and brush removal; minimization of excavation in low flow areas; minimum of 6 inch riprap grade for aquatic organism habitat; sediment control at streams; do not construct any temporary runarounds, access bridges, causeways, cofferdams, diversions, or pumparounds; do not allow construction materials or debris to enter the waterway; and avoidance of all work within the inundated part of the stream channel during the fish spawning season (April 1 through June 30).

All applicable recommendations are included in the Environmental Commitments section of this CE document.

Open Water Feature(s)	Presence	Impacts	
		Yes	No
Reservoirs	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Lakes	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Farm Ponds	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Retention/Detention Basin	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Storm Water Management Facilities	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Other: _____	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Describe all open water feature(s) identified adjacent or within the project area. Include whether or not impacts (both permanent and temporary) will occur to the features identified. Include if features are likely subject to federal or state jurisdiction. Discuss measures to avoid, minimize, and mitigate if impacts will occur.

Based on the desktop review, the aerial map of the project area, and the RFI report (Appendix E-3 to E-4) there are eleven open water features within the 0.5-mile search radius. There are no open water features within or adjacent to the project area. That number was confirmed by the site visit on September 23, 2022 by Corradino, LLC.

A Waters of the U.S. Determination/Wetland Delineation Report was completed for the project on March 3, 2023. Please refer to Appendix F for the Waters of the U.S. Determination/Wetland Delineation Report. It was determined that no open water features are present within the investigative area. The USACE makes all final determinations regarding jurisdiction.

Wetlands	Presence	Impacts	
		Yes	No
	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

Total wetland area: 0.108 Acre(s) Total wetland area impacted: 0.051 Acre(s)

(If a determination has not been made for non-isolated/isolated wetlands, fill in the total wetland area impacted above.)

This is page 10 of 25 Project name: SR 58 Bridge Project Date: February 22, 2024

Indiana Department of Transportation

County Bartholomew Route SR 58 Des. No. 2100568

Wetland No.	Classification	Total Size (Acres)	Impacted Acres	Comments (i.e. location, likely Water of the US, appendix reference)
Wetland 1	PEM	0.051	0.051	Ditch east of SR 58, likely Water of the US; Appendix F-8
Wetland 2	PFO	0.057	No impact.	Flatwoods west of project structure, likely Water of the US; Appendix F-8

Wetlands (Mark all that apply)

Wetland Determination
 Wetland Delineation
 USACE Isolated Waters Determination

Documentation

X
X

ESD Approval Dates

March 31, 2023
March 31, 2023

Improvements that will not result in any wetland impacts are not practicable because such avoidance would result in (Mark all that apply and explain):

- Substantial adverse impacts to adjacent homes, business or other improved properties;
- Substantially increased project costs;
- Unique engineering, traffic, maintenance, or safety problems;
- Substantial adverse social, economic, or environmental impacts, or
- The project not meeting the identified needs.

X

Describe all wetlands identified adjacent or within the project area. Include whether or not impacts (both permanent and temporary) will occur to the features identified. Include if features are likely subject to federal or state jurisdiction. Discuss measures to avoid, minimize, and mitigate if impacts will occur.

Based on the desktop review, the aerial map of the project area, and the RFI report (Appendix E-3 to E-4) there are twenty wetlands within the 0.5-mile search radius. There is one wetland within or adjacent to the project area. That number was updated to two by the site visit on September 23, 2022 by Corradino, LLC.

A *Waters of the U.S. Determination/Wetland Delineation Report* was completed for the project on March 3, 2023. Please refer to Appendix F for the *Waters of the U.S. Determination/Wetland Delineation Report*. It was determined that two wetlands within or adjacent to the project are apparent Waters of the U.S. traceable to the navigable East Fork White River.

Wetland 1 is a poor quality palustrine emergent wetland occupying the bed of the ditch east of SR 58. Wetland 1 is 0.051 acre and 0.051 acre may be impacted by the reconstruction of SR 58.

Wetland 2 is an average quality palustrine forested wetland west of the project bridge. Wetland 2 is 0.057 acre and impact has been entirely avoided during the design phase.

Wetland impacts were reduced to the extent practicable during the design phase of the project. Total avoidance of wetland impacts is not practicable because SR 58 may not be relocated without impacting its ditch area that includes Wetland 1, and therefore the purpose and need of the project cannot be met without impact to that wetland. Mitigation is not anticipated. The USACE makes all final determinations regarding jurisdiction.

USFWS responded to early coordination on February 15, 2023 (Appendix C-5 to C-11) and IDNR-DFW responded on March 9, 2023 (Appendix C-12 to C-15). USFWS did not include recommendations for wetlands. IDNR-DFW recommended coordination with IDEM regarding wetland impacts. All applicable recommendations are included in the Environmental Commitments section of this CE document.

Terrestrial Habitat

Presence

X

Impacts

Yes	NO
X	

Total terrestrial habitat in project area: 2.964 Acre(s) Total tree clearing: 1.0 Acre(s)

This is page 11 of 25 Project name: SR 58 Bridge Project Date: February 22, 2024

Indiana Department of Transportation

County Bartholomew Route SR 58 Des. No. 2100568

Describe types of terrestrial habitat (i.e. forested, grassland, farmland, lawn, etc) adjacent or within the project area. Include whether or not impacts will occur to habitat identified. Include total terrestrial habitat impacted and total tree clearing that will occur. Discuss measure to avoid, minimize, and mitigate if impacts will occur.

Based on a desktop review, a site visit on September 23, 2022 by Corradino, LLC, the aerial map of the project area (Appendix B-3), there is 1,344 acre of grassy roadside habitat within the project area. Dominant species include reed canary grass (*Phalaris arundinacea*) and Japanese bristlegrass (*Setaria faberi*). There is also 1.62 acre of upland wooded habitat within the project area. Dominant species include eastern sycamore (*Platanus occidentalis*), American elm (*Ulmus americana*), black walnut (*Juglans nigra*), eastern redcedar (*Juniperus virginiana*), and red maple (*Acer rubrum*). Land use in the surrounding area is rural. The project will disturb approximately 6.68 acres of soil and 1.00 acre of tree removal is required at the outlet of the project structure. Impacts to terrestrial habitat were reduced during the design phase of the project. 0.62 acre of wooded habitat is planned to be acquired west of White Creek, along SR 58 for future maintenance needs, but tree clearing will not occur in this area for this project.

IDNR-DFW responded on March 9, 2023 with recommendations regarding wildlife passage, tree clearing, mitigation for riparian habitat, revegetation with native species, and erosion control (Appendix C-12 to C-15). All applicable recommendations are included in the Environmental Commitments section of this CE document.

Protected Species

Federally Listed Bats

	Yes	No
Information for Planning and Consultation (IPaC) determination key completed	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Section 7 informal consultation completed (IPaC cannot be completed)	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Section 7 formal consultation Biological Assessment (BA) required	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Determination Received for Listed Bats from USFWS: NE NLAA LAA

Other Species not included in IPaC

	Yes	No
Additional federal species found in project area (based on IPaC species list)	<input type="checkbox"/>	<input checked="" type="checkbox"/>
State species (not bird) found in project area (based upon consultation with IDNR)	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Migratory Birds

	Yes	No
Known usage or presence of birds (i.e. nests)	<input type="checkbox"/>	<input checked="" type="checkbox"/>
State bird species based upon coordination with IDNR	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Discuss IDNR coordination and species identified. Describe USFWS Section 7 consultation and determination received for Indiana bat and northern long-eared bat impacts. Discuss if other federally listed species were identified. If so, include consultation that has occurred and the determination that was received. Discuss if migratory birds have been observed and any impacts.

Based on a desktop review and the RFI report (Appendix E-6), completed by Corradino, LLC on April 28, 2023, the IDNR Bartholomew County Endangered, Threatened and Rare (ETR) Species List has been checked. According to the IDNR-DFW early coordination response letter dated March 9, 2023 (Appendix C-12 to C-15), the Natural Heritage Program's Database has been checked and no presence of ETR species is known within the 0.5-mile search radius. An INDOT 0.5-mile bat review occurred on February 9, 2023. There are no documented sites within 0.5-mile of the project area (Appendix I-23 to I-24).

Project information was submitted through the USFWS's Information for Planning and Consultation (IPaC) portal, and an official species list was generated on November 29, 2023 (Appendix C-21 to C-32). The project is within range of federally endangered Indiana bat (*Myotis sodalis*) and the federally endangered northern long-eared bat (NLEB) (*Myotis septentrionalis*). Other species were generated in the IPaC species list along with the Indiana bat and northern long-eared bat. Refer to paragraph below.

The official species list generated from IPaC also indicated one other species present within the project area, the candidate species monarch butterfly (*Danaus plexippus*). Because the monarch butterfly does not have endangered or threatened status, it is not federally protected under the Endangered Species Act and no further coordination is required.

The project qualifies for the Range-wide Programmatic Informal Consultation for the Indiana bat and Northern long-eared bat (NLEB), dated May 2016 (revised February 2018), between FHWA, Federal Railroad Administration (FRA), Federal Transit Administration (FTA), and USFWS. INDOT conducted a culvert inspection on January 3, 2022 and Corradino, LLC conducted a bat inspection on September 23, 2022. Neither inspection identified signs of bats/birds using the structures (Appendix I-19; C-47). An

Indiana Department of Transportation

County Bartholomew Route SR 58 Des. No. 2100568

effect determination key was completed on April 17, 2023, and based on the responses provided, the project was found to "may affect – not likely to adversely affect" the Indiana bat and/or the NLEB (Appendix C-33 to C-46). INDOT reviewed and verified the effect finding on April 17, 2203 and requested USFWS's review of the finding. No response was received from USFWS within the 14-day review period; therefore, it was concluded they concur with the finding. Avoidance and Minimization Measures (AMMs) include the following: General AMM1, Lighting AMM1, Tree Removal AMM1, Tree Removal AMM2, Tree Removal AMM3, Three Removal AMM4.

AMMs are included as firm commitments in the Environmental Commitments section of this document.

Bridge 058-03-05882 B, SR 58 over White Creek in Bartholomew County, Indiana, and the project's surrounding habitat are conducive for use (i.e. nests) by a bird species protected under the Migratory Bird Treaty Act (MBTA). Prior to the start of nesting season (May 1) the structure must be inspected for birds or signs of birds. If birds or signs of birds are found during the inspection avoidance and minimization measures must be implemented prior to the start of and during the nesting season. Nests without eggs or young should be removed prior to construction during the non-nesting season (September 8 – April 30) and during the nesting season if no eggs or young are present. Nests with eggs or young should be screened or buffered from active construction. Details of the required procedures are outlined in the "Potential Migratory Bird on Structure" RSP.

Bridge inspections occurred on September 23, 2022 (Appendix C-47 to C-49). USFWS Bridge/Structure Assessments are only valid for two years. If construction will begin after September 23, 2024 an inspection of the structure by a qualified individual, must be performed. Inspection of the structure should check for presence of bats/bat indicators and/or presence of birds. The results of the inspection must indicate no signs of bats or birds. If signs of bats or birds are documented during this inspection, the INDOT District Environmental Manager must be contacted immediately (INDOT ESD)

This precludes the need for further consultation on this project as required under Section 7 of the Endangered Species Act, as amended. If new information on endangered species at the site becomes available, or if project plans are changed, USFWS will be contacted for consultation.

Geological and Mineral Resources

- Project located within the Indiana Karst Region
- Karst features identified within or adjacent to the project area
- Oil/gas or exploration/abandoned wells identified in the project area

Yes	No
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>

Date Karst Evaluation reviewed by INDOT EWPO (if applicable): _____

Discuss if project is located in the Indiana Karst Region and if any karst features have been identified in the project area (from RFI). Discuss response received from IGWS coordination. Discuss if any mines, oil/gas, or exploration/abandoned wells were identified and if impacts will occur. Include discussion of karst study/report was completed and results. (Karst investigation must comply with the current Protection of Karst Features during Planning and Construction guidance and coordinated and reviewed by INDOT EWPO)

Based on a desktop review and the Indiana Karst Region map, the project is located in the designated Indiana Karst Region as outlined in the most current Protection of Karst Features during Project Development and Construction. According to the topographic map of the project area (Appendix B-4), the RFI report (Appendix E-4), there are no karst features identified within or adjacent to the project area. In the early coordination response February 8, 2023, the Indiana Geological and Water Survey (IGWS) did not indicate that karst features exist in the project area (Appendix C-18 to C-20). IGWS identified the project area as having high liquefaction potential, moderate potential for a bedrock resource, low potential as a sand and gravel resources, and 1% annual chance flood hazard. The features will not be affected because the project does not have excavation deep enough to impact bed rock or liquefaction potential. Response from IGWS has been communicated to the designer on March 2, 2023. No impacts are expected.

Indiana Department of Transportation

County Bartholomew Route SR 58 Des. No. 2100568

SECTION C – OTHER RESOURCES

Drinking Water Resources	Presence	Impacts	
		Yes	No
Wellhead Protection Area(s)	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Source Water Protection Area(s)	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Water Well(s)	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Urbanized Area Boundary	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Public Water System(s)	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Is the project located in the St. Joseph Sole Source Aquifer (SSA):		<input type="checkbox"/>	<input checked="" type="checkbox"/>
If Yes, is the FHWA/EPA SSA MOU Applicable?		<input type="checkbox"/>	<input type="checkbox"/>
If Yes, is a Groundwater Assessment Required?		<input type="checkbox"/>	<input type="checkbox"/>

Check the appropriate boxes and discuss each topic below. Provide details about impacts and summarize resource-specific coordination responses and any mitigation commitments. Reference responses in the Appendix.

The project is located in Bartholomew County, Indiana which is not located within the St. Joseph Sole Source Aquifer, the only legally designated sole source aquifer in the state of Indiana. Therefore, the FHWA/EPA/INDOT Sole Source Aquifer Memorandum of Understanding (MOU) is not applicable to this project, a detailed groundwater assessment is not needed, and no impacts are expected.

The Indiana Department of Environmental Management's Wellhead Proximity Determinator website (<http://www.in.gov/idem/cleanwater/pages/wellhead/>) was accessed on March 2, 2023 by Corradino, LLC. This project is not located within a Wellhead Protection Area or Source Water Area. No impacts are expected.

Based on a desktop review of, a site visit on September 23, 2022 by Corradino, LLC, the aerial map of the project area (Appendix B-3), this project is not located in an Urban Area Boundary. No impacts are expected.

The Indiana Department of Natural Resources Water Well Record Database website (<https://www.in.gov/dnr/water/3595.htm>) was accessed on March 2, 2023 by Corradino, LLC. No wells are located near this project. Therefore, no impacts are expected.

Based on a desktop review, a site visit on September 23, 2022 by Corradino, LLC, the aerial map of the project area (Appendix B-3), no public water systems were identified. Therefore, no impacts are expected.

Floodplains	Presence	Impacts	
		Yes	No
Project located within a regulated floodplain	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Longitudinal encroachment	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Transverse encroachment	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Homes located in floodplain within 1000' up/downstream from project	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

If applicable, indicate the Floodplain Level?

Level 1 Level 2 Level 3 Level 4 Level 5

Use the IDNR Floodway Information Portal to help determine potential impacts. Include floodplain map in appendix. Discuss impacts according to the classification system. If encroachment on a flood plain will occur, coordinate with the Local Flood Plain Administrator during design to insure consistency with the local flood plain planning.

Based on a desktop review of The Indiana Department of Natural Resources Indiana Floodway Information portal website ([Indiana Floodplain Information Portal 2.0 \(arcgis.com\)](http://Indiana Floodplain Information Portal 2.0 (arcgis.com))) by Corradino, LLC on March 2, 2023 and the RFI report, this project is located in a regulatory floodplain as determined from approved IDNR floodplain maps (Appendix F-14). An early coordination letter was sent on February 7, 2023, to the local Bartholomew County MS4-Stormwater Director. The Bartholomew County MS4-Stormwater Director did not respond within the 30-day time frame. This project qualifies as a Category 4 per the current INDOT CE Manual, which states,

Indiana Department of Transportation

County Bartholomew Route SR 58 Des. No. 2100568

"0 homes are located within the base floodplain within 1,000 feet upstream and 0 homes are located within the base floodplain within 1,000 feet downstream. The proposed structure will have an effective capacity such that backwater surface elevations are not expected to substantially increase. As a result, there will be no substantial adverse impacts on natural and beneficial floodplain values; there will be no substantial change in flood risks; and there will be no substantial increase in potential for interruption or termination of emergency service or emergency evacuation routes; therefore, it has been determined that this encroachment is not substantial."

Farmland	Presence	Impacts	
		Yes	No
Agricultural Lands	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Prime Farmland (per NRCS)	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Total Points (from Section VII of CPA-106/AD-1006*)	<u>136</u>		

*If 160 or greater, see CE Manual for guidance.

Discuss existing farmland resources in the project area, impacts that will occur to farmland, and mitigation and minimization measures considered.

Based on a desktop review, a site visit on September 23, 2022 by Corradino, LLC, the aerial map of the project area (Appendix B-3), the project will convert 3.02 acres of farmland as defined by the Farmland Protection Policy Act (FPPA). An early coordination letter was sent on February 8, 2023, to Natural Resources Conservation Service (NRCS). Coordination with NRCS resulted in a score of 136 on the AD 1006 Form (Appendix C-16 to C-17). This score listed was based on 7.50 acres of impact, which has been reduced to 3.02 acres after coordination with NRCS. NRCS's threshold score for significant impacts to farmland that result in the consideration of alternatives is 160. Since this project score is less than the threshold, no significant loss of prime, unique, statewide, or local important farmland will result from this project. No alternatives other than those previously discussed in this document will be investigated without reevaluating impacts to prime farmland.

SECTION D – CULTURAL RESOURCES

Minor Projects PA	Category(ies) and Type(s)	INDOT Approval Date(s)	N/A
	<input type="text"/>	<input type="text"/>	<input checked="" type="checkbox"/>
Full 106 Effect Finding			
No Historic Properties Affected	<input checked="" type="checkbox"/>	No Adverse Effect	<input type="checkbox"/>
		Adverse Effect	<input type="checkbox"/>
Eligible and/or Listed Resources Present			
NRHP Building/Site/District(s)	<input type="checkbox"/>	Archaeology	<input type="checkbox"/>
		NRHP Bridge(s)	<input type="checkbox"/>

Indiana Department of Transportation

County Bartholomew Route SR 58 Des. No. 2100568

Documentation Prepared (mark all that apply)

- APE, Eligibility and Effect Determination
- 800.11 Documentation
- Historic Properties Report or Short Report
- Archaeological Records Check and Assessment
- Archaeological Phase Ia Survey Report
- Archaeological Phase Ic Survey Report
- Other:

	ESD Approval Date(s)	SHPO Approval Date(s)
<input checked="" type="checkbox"/>	10-26-23	11-27-23
<input checked="" type="checkbox"/>	10-26-23	11-27-23
<input checked="" type="checkbox"/>	6-12-23	7-6-23
<input checked="" type="checkbox"/>	6-9-23	7-6-23
<input checked="" type="checkbox"/>	6-9-23	7-6-23

Memorandum of Agreement (MOA)

MOA Signature Dates (List all signatories)

--	--

If the project falls under the MPPA, describe the category(ies) that the project falls under and any approval dates. If the project requires full Section 106, use the headings provided. The completion of the Section 106 process requires that a Legal Notice be published in local newspapers. Please indicate the publication date, name of the paper(s) and the comment period deadline. Include any further Section 106 work which must be completed at a later date, such as mitigation from a MOA or avoidance commitments.

Area of Potential Effect (APE): The APE for archaeology is the project footprint. For above-ground structures, the APE was defined as extending at least 500 feet beyond the project end points, and at least one property deep on each side of SR 58 and CR 700 South (Appendix D-14).

Coordination with Consulting Parties: Per the National Historic Preservation Act of 1966, the State Historic Preservation Officer (SHPO) is entitled to participate in the Section 106 process as a consulting party. The following other individuals and organizations were invited by letter or email dated November 29, 2022 (Appendix D-8):

Consulting Parties	Response Date	Appendix Reference
Department of Natural Resources - Division of Historic Preservation and Archaeology	December 29, 2022	Appendix D-51
Indiana Landmarks, Central Regional Office	November 29, 2022	Appendix D-44
Landmark Columbus Foundation	No Response	N/A
Bartholomew County Historian, Dr. Tamara Stone Iorio	No Response	N/A
Bartholomew County Genealogical Society	No Response	N/A
Bartholomew County Historical Society	No Response	N/A
Columbus Area Metropolitan Planning Organization	No Response	N/A
Bartholomew County Commissioners: Carl H. Leinhoop, Larry Kleinhenz, Tony London	No Response	N/A
Bartholomew County Engineer, Danny Hollander, P.E.	No Response	N/A
Bartholomew County Highway Superintendent, Dwight D. Smith	No Response	N/A
Eastern Shawnee Tribe of Oklahoma	January 4, 2023	Appendix D-48
Miami Tribe of Oklahoma	December 2, 2022	Appendix D-45
Peoria Tribe of Indians of Oklahoma	No Response	N/A
Pokagon Band of Potawatomi Indians	No Response	N/A
Shawnee Tribe	No Response	N/A
Delaware Nation of Oklahoma	January 16, 2023	Appendix D-49
Delaware Tribe of Indians	No Response	N/A

Archaeology and Historic Properties: Efforts to identify properties in the APE included a check of data available online at the Indiana State Historic Architectural and Archaeological Research Database (SHAARD), the Bartholomew County Interim Report: Indiana Historic Sites and Structures Inventory (2012, Historic Landmarks Foundation of Indiana), the Indiana Historic Buildings, Bridges, and Cemeteries Map (IHBBM), the INDOT-Cultural Resources Office (CRO) Public Web Map App, and communication with consulting parties. Sources of information examined included National Register of Historic Places (NRHP) listings, Indiana Register of Historic Sites and Structures (IRHSS) listings, the Indiana Historic Bridge Inventory, archaeological site maps, cultural resources management reports, and cemetery records (Appendix D-4).

Indiana Department of Transportation

County Bartholomew Route SR 58 Des. No. 2100568

The literature review determined there are no NRHP-listed properties within the APE. No previously inventoried archaeological sites are located within the project area.

The results of field surveys were reported in a Historic Property Short Report (HPSR) (Vorndran, 6/12/2023) and an Archaeological Short Report (ASR) (Copenhaver, 5/27/2023). The principal investigators for these reports meet the Secretary of the Interior's Professional Qualification Standards as per 36 CFR Part 61. The HPSR and ASR identified no properties listed or eligible for listing in the NRHP. In a letter dated November 27, 2023, the Indiana SHPO stated concurrence that no further archaeological investigations appear necessary at the proposed project area (Appendix D-59). Additionally, the SHPO concurred with the find of "No Historic Properties Affected" for this undertaking.

Documentation Findings: No consulting parties offered an objection to the proposed APEs and NRHP eligibility recommendations of both the project historian and archaeologist. Thus, since no historic properties are present within the APE, a finding of "No Historic Properties Affected" has been made for this undertaking.

Public Involvement: A notice of FHWA's finding of "No Historic Properties Affected" was published in *The Republic* on November 1, 2023. No public comments were received. The text of the public notice and the affidavit of publication appear in Appendix D-62 to D-63. The Section 106 process has been completed and the responsibilities of the FHWA under Section 106 have been fulfilled.

SECTION E -- SECTION 4(f) RESOURCES/ SECTION 6(f) RESOURCES

	<u>Presence</u>	<u>Use</u>	
		<u>Yes</u>	<u>No</u>
Parks and Other Recreational Land			
Publicly owned park			
Publicly owned recreation area			
Other (school, state/national forest, bikeway, etc.)			
Wildlife and Waterfowl Refuges			
National Wildlife Refuge			
National Natural Landmark			
State Wildlife Area			
State Nature Preserve			
Historic Properties			
Site eligible and/or listed on the NRHP			
<u>Evaluations</u>			
	<u>Prepared</u>		
Programmatic Section 4(f)			
"De minimis" Impact			
Individual Section 4(f)			
Any exception included in 23 CFR 774.13			

Discuss Programmatic Section 4(f) and "de minimis" Section 4(f) impacts in the discussion below. Individual Section 4(f) documentation must be included in the appendix and summarized below. Discuss proposed alternatives that satisfy the requirements of Section 4(f). FHWA has identified various exceptions to the requirement for Section 4(f) approval. Refer to 23 CFR § 774.13 - Exceptions.

Section 4(f) of the U.S. Department of Transportation Act of 1966 prohibits the use of certain public and historic lands for federally funded transportation facilities unless there is no feasible and prudent alternative. The law applies to significant publicly owned parks, recreation areas, wildlife/waterfowl refuges, and NRHP eligible or listed historic properties regardless of ownership. Lands subject to this law are considered Section 4(f) resources.

Based on a desktop review, the aerial map of the project area (Appendix B-3), and the RFI report (Appendix E-3), there are no potential 4(f) resources located within the 0.5-mile search radius. According to additional research, and by the site visit on September 23, 2022 by Corradino, LLC, there are no Section 4(f) resources within or adjacent to the project area. Therefore, no use is expected.

Indiana Department of Transportation

County Bartholomew Route SR 58 Des. No. 2100568

Section 6(f) Involvement	<u>Presence</u>	<u>Use</u>	
Section 6(f) Property	<input type="checkbox"/>	Yes <input type="checkbox"/>	No <input type="checkbox"/>

Discuss Section 6(f) resources present or not present. Discuss if any conversion would occur as a result of this project. If conversion will occur, discuss the conversion approval.

The U.S. Land and Water Conservation Fund Act of 1965 established the Land and Water Conservation Fund (LWCF), which was created to preserve, develop, and assure accessibility to outdoor recreation resources. Section 6(f) of this Act prohibits conversion of lands purchased with LWCF monies to a non-recreation use.

A review of 6(f) properties on the INDOT ESD website revealed a total of five properties in Bartholomew County (Appendix I-25). None of these properties are located within or adjacent to the project area. Therefore, there will be no impacts to 6(f) resources.

SECTION F – Air Quality

STIP/TIP and Conformity Status of the Project

	Yes	No
Is the project in the most current STIP/TIP?	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Is the project located in an MPO Area?	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Is the project in an air quality non-attainment or maintenance area?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
If Yes, then:		
Is the project in the most current MPO TIP?	<input type="checkbox"/>	<input type="checkbox"/>
Is the project exempt from conformity?	<input type="checkbox"/>	<input type="checkbox"/>
If No, then:		
Is the project in the Transportation Plan (TP)?	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Is a hot spot analysis required (CO/PM)?	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Location in STIP: 2024-2028 STIP
 Name of MPO (if applicable): Columbus Area Metropolitan Planning Organization (CAMPO)
 Location in TIP (if applicable): FY 2024 – FY 2028

Level of MSAT Analysis required?

Level 1a Level 1b Level 2 Level 3 Level 4 Level 5

Describe if the project is listed in the STIP and if it is in a TIP. Describe the attainment status of the county(ies) where the project is located. Indicate whether the project is exempt from a conformity determination. If the project is not exempt, include information about the TP and TIP. Describe if a hot spot analysis is required and the MSAT Level.

This project is included in the CAMPO Fiscal Year (FY) 2024-2028 Transportation Improvement Program (TIP) which has been directly incorporated into the Fiscal Year (FY) 2024-2028 Statewide Transportation Improvement Program (STIP) (Appendix H-2).

This project is located in Bartholomew County, which is currently in attainment for all criteria pollutants according to IDEM (<https://www.in.gov/idem/airmonitoring/air-quality-data/>). Therefore, the conformity procedures of 40 CFR Part 93 do not apply.

This project is of a type qualifying as a categorical exclusion (Group 1) under 23 CFR 771.117(c), or exempt under the Clean Air Act conformity rule under 40 CFR 93.126, and as such, a Mobile Source Air Toxics analysis is not required.

Indiana Department of Transportation

County Bartholomew Route SR 58 Des. No. 2100568

SECTION G - NOISE

Noise Yes No
 Is a noise analysis required in accordance with FHWA regulations and INDOT's traffic noise policy?

Date Noise Analysis was approved/technically sufficient by INDOT ESD: _____

Describe if the project is a Type I or Type III project. If it is a Type I project, describe the studies completed to date and if noise impacts were identified. If noise impacts were identified, describe if abatement is feasible and reasonable and include a statement of likelihood.

This project is a Type III project. In accordance with 23 CFR 772 and the current Indiana Department of Transportation Traffic Noise Analysis Procedure, this action does not require a formal noise analysis.

SECTION H – COMMUNITY IMPACTS

Regional, Community & Neighborhood Factors

Will the proposed action comply with the local/regional development patterns for the area?
 Will the proposed action result in substantial impacts to community cohesion?
 Will the proposed action result in substantial impacts to local tax base or property values?
 Will construction activities impact community events (festivals, fairs, etc.)?
 Does the community have an approved transition plan?
 If No, are steps being made to advance the community's transition plan?
 Does the project comply with the transition plan? (explain in the discussion below)

Yes	No
X	
	X
	X
	X
X	
X	

Discuss how the project complies with the area's local/regional development patterns; whether the project will impact community cohesion; and impact community events. Discuss how the project conforms with the ADA Transition Plan.

The proposed action is not expected to conflict with development patterns or have substantial impacts to property values. The project is not expected to affect American Disabilities Act (ADA) facilities in any way. The City of Columbus ADA Transition plan does not identify this rural highway as an ADA corridor.

Public Facilities and Services

Discuss what public facilities and services are present in the project area and impacts (such as MOT) that will occur to them. Include how the impacts have been minimized and what coordination has occurred. Some examples of public facilities and services include health facilities, educational facilities, public and private utilities, emergency services, religious institutions, airports, transportation or public pedestrian and bicycle facilities.

Based on a desktop review, the aerial map of the project area (Appendix B-3), and the RFI report (Appendix E-3), there is one religious facility located within the 0.5-mile of the project. There is one school, Mount Healthy Elementary School, within the 0.5-mile of the project. There is one recreational facility located within the 0.5-mile of the project. There is one trail within the 0.5-mile of the project. There are no public facilities within or adjacent to the project area. That number was confirmed by the site visit on September 22, 2023 by Corradino, LLC. Therefore, no impacts are expected. Access to all properties will be maintained during construction.

Corradino, LLC sent an early coordination letter to Bartholomew Consolidated School Corporation on February 7, 2023 and did not receive a response.

It is the responsibility of the project sponsor to notify school corporations and emergency services at least two weeks prior to any construction that would block or limit access.

Indiana Department of Transportation

County Bartholomew Route SR 58 Des. No. 2100568

Environmental Justice (EJ) (Presidential EO 12898)

During the development of the project were EJ issues identified?

Does the project require an EJ analysis?

If YES, then:

Are any EJ populations located within the project area?

Will the project result in adversely high and disproportionate impacts to EJ populations?

	Yes	No
During the development of the project were EJ issues identified?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Does the project require an EJ analysis?	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Are any EJ populations located within the project area?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Will the project result in adversely high and disproportionate impacts to EJ populations?	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Indicate if EJ issues were identified during project development. If an EJ analysis was not required, discuss why. If an EJ analysis was required, describe how the EJ population was identified. Include if the project has a disproportionately high or adverse effect on EJ populations and explain your reasoning. If yes, describe actions to avoid, minimize and mitigate these effects.

Under FHWA Order 6640.23A, FHWA and the project sponsor, as a recipient of funding from FHWA, are responsible to ensure that their programs, policies, and activities do not have a disproportionately high and adverse effect on minority or low-income populations. Per the current INDOT Categorical Exclusion Manual, an Environmental Justice (EJ) Analysis is required for any project that has two or more relocations or 0.5 acre of additional permanent right-of-way. The project will require 6.93 acre of additional permanent ROW and no relocations. Therefore, an EJ Analysis is required.

Potential EJ impacts are detected by locating minority and low-income populations relative to a reference population to determine if populations of EJ concern exists and whether there could be disproportionately high and adverse impacts to them. The reference population may be a county, city or town and is called the community of comparison (COC). In this project, the COC is Bartholomew County. The community that overlaps the project area is called the affected community (AC). In this project, the AC is Census Tract 115. An AC has a population of concern for EJ if the population is more than 50% minority or low-income or if the low-income or minority population is 125% of the COC. Data from the U.S. Census Bureau 2021 American Community Survey 5-year Estimates was obtained from <https://data.census.gov/cedsci/> on April 6, 2023 by Corradino, LLC. The data collected for minority and low-income populations within the AC are summarized in the below table.

	COC –Bartholomew County	AC – Census Tract 115
Percent Minority	19.88%	22.43%
125% of COC	24.85%	AC < 125% COC
EJ Population of Concern		No
Percent Low-Income	11.62%	9.50%
125% of COC	14.52%	AC < 125% COC
EJ Population of Concern		No

The AC Census Tract 115 has a percent minority of 22.43% which is below 50% and is below the 125% COC threshold. Therefore, the AC does not contain minority populations of EJ concern.

The AC Census Tract 115 has a percent low-income of 9.50% which is below 50% and is below the 125% COC threshold. Therefore, the AC does not contain low-income populations of EJ concern.

The census data sheets, map, and calculations can be found in Appendix I-26 to I-29. The project does not cause a disproportionately high and adverse effect on minority and/or low-income populations of EJ concern relative to non EJ populations in accordance with the provisions of Executive Order 12898 and FHWA Order 6640.23a. No further EJ Analysis is warranted.

Relocation of People, Businesses or Farms

Will the proposed action result in the relocation of people, businesses or farms?

Is a BIS or CSRS required?

	Yes	No
Will the proposed action result in the relocation of people, businesses or farms?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Is a BIS or CSRS required?	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Number of relocations: Residences: 0 Businesses: 0 Farms: 0 Other: 0

Discuss any relocations that will occur due to the project. If a BIS or CSRS is required, discuss the results in the discussion below.

No relocation of people, businesses, or farms will take place as a result of this project.

Indiana Department of Transportation

County Bartholomew Route SR 58 Des. No. 2100568

SECTION I – HAZARDOUS MATERIALS & REGULATED SUBSTANCES

Hazardous Materials & Regulated Substances (Mark all that apply)

Documentation

Red Flag Investigation (RFI)	<input checked="" type="checkbox"/>
Phase I Environmental Site Assessment (Phase I ESA)	<input type="checkbox"/>
Phase II Environmental Site Assessment (Phase II ESA)	<input type="checkbox"/>
Design/Specifications for Remediation required?	<input type="checkbox"/>

Date RFI concurrence by INDOT SAM (if applicable): April 28, 2023

Include a summary of the potential hazardous material concerns found during review. Discuss in depth sites found within, directly adjacent to, or ones that could impact the project area. Refer to current INDOT SAM guidance. If additional documentation (special provisions, pay quantities, etc.) will be needed, include in discussion. Include applicable commitments.

Based on a review of GIS and available public records, the RFI was completed on April 28, 2023 by Corradino, LLC and INDOT SAM provided their concurrence on April 28, 2023 (Appendix E-4 to E-5). Two hazardous waste sites (hazmat sites) are located within 0.5-mile of the project area. One underground storage tank site is located within 0.5-mile of the project area. One NPDES facility is located within 0.5-mile of the project area. None of the hazmat sites identified will impact the project. Further investigation for hazardous material concerns is not required at this time.

Part IV – Permits and Commitments

PERMITS CHECKLIST

Permits (mark all that apply)	<u>Likely Required</u>
Army Corps of Engineers (404/Section 10 Permit)	
Nationwide Permit (NWP)	<input type="checkbox"/>
Regional General Permit (RGP)	<input checked="" type="checkbox"/>
Individual Permit (IP)	<input type="checkbox"/>
Other	<input type="checkbox"/>
IN Department of Environmental Management (401/Rule 5)	
Nationwide Permit (NWP)	<input type="checkbox"/>
Regional General Permit (RGP)	<input checked="" type="checkbox"/>
Individual Permit (IP)	<input type="checkbox"/>
Isolated Wetlands	<input type="checkbox"/>
Construction Stormwater General Permit	<input checked="" type="checkbox"/>
Other	<input type="checkbox"/>
IN Department of Natural Resources	
Construction in a Floodway	<input checked="" type="checkbox"/>
Navigable Waterway Permit	<input type="checkbox"/>
Other	<input type="checkbox"/>
Mitigation Required	
US Coast Guard Section 9 Bridge Permit	<input type="checkbox"/>
Others (Please discuss in the discussion below)	<input type="checkbox"/>

List the permits likely required for the project and summarize why the permits are needed, including permits designated as "Other."

White Creek, UNT 1 to White Creek, UNT 2 to White Creek, Wetland 1, and Wetland 2 were identified as waters of the U.S. in the Waters of the U.S. Determination Report. For impacts to waters of the U.S., a Section 404 Regional General Permit from USACE and a Section 401 Water Quality Certification from IDEM will be required for approximately 370 linear feet of stream impact and 0.051 acre of wetland impact.

Indiana Department of Transportation

County Bartholomew Route SR 58 Des. No. 2100568

Total disturbed area of soil will be 6.68 acres, which is above the 1.0 acre threshold for an IDEM Construction Stormwater General Permit (GSGP), formerly Rule 5 Storm Water Runoff Permit.

A Construction in a Floodway Permit is required and a rural bridge exemption does not apply.

No public airports are within 3.8 miles of the project area, and an Indiana Tall Structure Permit is not required.

Applicable recommendations provided by resource agencies are included in the Environmental Commitments section of this document. If permits are found to be necessary, the conditions of the permit will be requirements of the project and will supersede these recommendations.

It is the responsibility of the project sponsor to identify and obtain all required permits.

ENVIRONMENTAL COMMITMENTS

List all commitments and include the name of agency/organization requesting/requiring the commitment(s). Listed commitments should be numbered.

Firm:

1. If the scope of work or permanent or temporary right-of-way amounts change, INDOT Environmental Services Division (ESD) and the Seymour District Design/Environmental Manager will be contacted immediately. (INDOT ESD and INDOT Seymour District)
2. It is the responsibility of the project sponsor to notify school corporations and emergency services at least two weeks prior to any construction activity that would block or limit access. (INDOT ESD)
3. USFWS Bridge/Structure Assessment shall take place no earlier than two (2) years prior to the start of construction. If construction will begin after September 23, 2024 an inspection of the structure by a qualified individual, must be performed. Inspection of the structure should check for presence of bats/bat indicators and/or presence of birds. The results of the inspection must indicate no signs of bats or birds. If signs of bats or birds are documented during this inspection, the INDOT District Environmental Manager must be contacted immediately (INDOT ESD)
4. General AMM1 – Ensure all employees, and contractors working in areas of known or presumed bat habitat are aware of all FHWA/FRA/FTA (Transportation Agencies) environmental commitments, including all applicable AMMs. (USFWS)
5. Lighting AMM1 – Direct temporary lighting away from suitable habitat during the active season (April 1 to September 30). (USFWS)
6. Tree AMM1 - Modify all phases/aspects of the project (e.g., temporary work areas, alignments) to the extent practicable to avoid tree removal in excess of what is required to implement the project safely. (USFWS)
7. Tree Removal AMM2 - Apply time of year restrictions for tree removal when bats are not likely to be present (October 1 to March 31), or limit tree removal to 10 or fewer trees per project at any time of year within 100 feet of existing road/rail surface and outside of documented roosting/foraging habitat or travel corridors; visual emergence survey must be conducted with no bats observed. (USFWS and IDNR-DFW)
8. Tree Removal AMM3 - Ensure tree removal is limited to that specified in project plans and ensure that contractors understand clearing limits and how they are marked in the field (e.g., install bright colored flagging/fencing prior to any tree clearing to ensure contractors stay within clearing limits). (USFWS)
9. Tree Removal AMM4 - Do not remove documented Indiana bat or NLEB roosts that are still suitable for roosting, or trees within 0.25 miles of roosts, or documented foraging habitat any time of year. (USFWS)
10. Any work in a wetland area within right-of-way or in borrow/waste areas is prohibited unless specifically allowed in the U.S. Army Corps of Engineers permit. (INDOT ESD)

For Further Consideration:

1. Restrict below low-water work in streams to placement of culverts, piers, pilings and/or footings, shaping of the spill slopes around the bridge abutments, and placement of riprap (USFWS).
2. Culverts should span the active stream channel, should be either embedded or a 3-sided or open-arch culvert, and be installed where practicable on an essentially flat slope. When an open-bottomed culvert or arch is used in a stream, which has a good natural bottom substrate, such as gravel, cobbles and boulders, the existing substrate should be left undisturbed beneath the culvert to provide natural habitat for the aquatic community. (USFWS)
3. Minimize the extent of hard armor (riprap) in bank stabilization by using bioengineering techniques whenever possible. If riprap is utilized for bank stabilization, extend it below low-water elevation to provide aquatic habitat. (USFWS).

This is page 22 of 25 Project name: SR 58 Bridge Project Date: February 22, 2024

Indiana Department of Transportation

County Bartholomew Route SR 58 Des. No. 2100568

- 4. Evaluate wildlife crossings under bridge/culverts projects in appropriate situations. Suitable crossings include flat areas below bridge abutments with suitable ground cover, high water shelves in culverts, amphibian tunnels and diversion fencing. (USFWS)
- 5. Avoid all work within the inundated part of the stream channel (in perennial streams and larger intermittent streams) during the fish spawning season (April 1 through June 30), except for work within sealed structures such as caissons or cofferdams that were installed prior to the spawning season. No equipment shall be operated below the Ordinary High Water Mark during this time unless the machinery is within the caissons or on the cofferdams. (USFWS and IDNR-DFW)
- 6. If box or pipe culverts are used, the bottoms should be buried to a minimum of 6" (or 20% of the culvert height/pipe diameter, whichever is greater up to a maximum of 2') below the stream bed elevation to allow a natural streambed to form within or under the crossing structure. Crossings should: span the entire channel width (a minimum of 1.2 times the bank full width); maintain the natural stream substrate within the structure; have a minimum openness ratio (height x width/length) of 0.25; and have stream depth and water velocities during low-flow conditions that are approximate to those in the natural stream channel. The new, replacement, or rehabbed structure should not create conditions that are less favorable for wildlife passage under the structure compared to the current conditions. (IDNR-DFW)
- 7. Do not place riprap in the bed of the channel (unless sumped across the bed to avoid creating a fish passage obstruction) and use alternative erosion protection materials whenever possible. From the OHWM to the top of the banks, heavy duty erosion control blankets or turf reinforcement mats or a similar bioengineering method should be used and these materials should be seeded with native plants to allow a natural, vegetated stream bank to develop. (IDNR-DFW)
- 8. Impacts to non-wetland forest of one (1) acre or more should be mitigated at a minimum 2:1 ratio. (IDNR-DFW)
- 9. Use minimum average 6 inch graded riprap stone extended below the normal water level to provide habitat for aquatic organisms in the voids. (IDNR-DFW)
- 10. Do not construct any temporary runarounds, access bridges, causeways, cofferdams, diversions, or pumparounds (IDNR-DFW).

Indiana Department of Transportation

County Bartholomew Route SR 58 Des. No. 2100568

Table of Contents for Appendix Items

Appendix A: INDOT Supporting Documentation... A-1
Threshold Document... (A-2)
Appendix B: Graphics... B-1
Project Location Map... (B-2)
Aerial Map... (B-3)
USGS Topographic Map... (B-4)
Plan Sheets... (B-5 to B-17)
Photo Key and Photos... (B-18 to B-20)
Appendix C: Early Coordination... C-1
Example Early Coordination Letter... (C-2 to C-4)
Early Coordination Responses... (C-5 to C-49)
USFWS... (C-5 to C-11)
IDNR-DFW... (C-12 to C-15)
NRCS... (C-16 to C-17)
IGWS... (C-18 to C-20)
IPaC Species List... (C-21 to C-32)
IPaC Consistency Letter... (C-33 to C-46)
Bridge/Structure Bat Assessment Form... (C-47 to C-49)
Mt. Healthy Elementary School... (C-50)
Appendix D: Section 106 of the NHPA... D-1
800.11 Distribution Letter... (D-2 to D-5)
800.11 Documentation... (D-6 to D-60)
Area of Potential Effects... (D-6)
Eligibility Determination... (D-6)
Section 4(f) Compliance Requirements... (D-6)
Effect Finding... (D-7 to D-10)
Graphics... (D-11 to D-25)
Historic Property and Archaeology Short Report... (D-26 to D-33)
Correspondence with Consulting Parties... (D-34 to D-60)
Legal Notice... (D-61 to D-63)
Appendix E: Red Flag and Hazardous Materials... E-1
Red Flag Investigation... (E-2 to E-11)
Appendix F: Water Resources... F-1
Waters Report... (F-2 to F-9)
Supporting Maps... (F-10 to F-18)
Photos... (F-19 to F-43)

Indiana Department of Transportation

County Bartholomew Route SR 58 Des. No. 2100568

Wetland Data Forms (F-44 to F-57)
Preliminary Jurisdictional Determination Form..... (F-58 to F-61)
Appendix G: Public Involvement **G-1**
Notice of Survey Letter..... (G-2 to G-3)
Appendix H: Air Quality **H-1**
Page from TIP with Project Listed..... (H-2)
INDOT Letter to FHWA(H-3 to H-4)
FHWA/FTA Letter to INDOT.....(H-5 to H-6)
Appendix I: Additional Studies **I-1**
Bridge Inspection Report.....(I-2 to I-22)
Bat Heritage Database Check.....(I-23 to I-24)
Section 6(f) List (I-25)
Environmental Justice Analysis.....(I-26 to I-29)

Appendix A

INDOT Supporting Documentation

Des. No. 2100568

Categorical Exclusion Level Thresholds

	PCE	Level 1	Level 2	Level 3	Level 4 ¹
Section 106	Falls within guidelines of Minor Projects PA	"No Historic Properties Affected"	"No Adverse Effect"	-	"Adverse Effect" Or Historic Bridge involvement ²
Stream Impacts³	No construction in waterways or water bodies	< 300 linear feet of stream impacts	≥ 300 linear feet of stream impacts	-	USACE Individual 404 Permit ⁴
Wetland Impacts³	No adverse impacts to wetlands	< 0.1 acre	-	< 1.0 acre	≥ 1.0 acre
Right-of-way⁵	Property acquisition for preservation only or none	< 0.5 acre	≥ 0.5 acre	-	-
Relocations	None	-	-	< 5	≥ 5
Threatened/Endangered Species (Species Specific Programmatic for Indiana bat & northern long eared bat)*	"No Effect", "Not likely to Adversely Affect" (With select AMMs ⁶)	"Not likely to Adversely Affect" (With any AMMs or commitments)	-	"Likely to Adversely Affect"	Project does not fall under Species Specific Programmatic ⁷
Threatened/Endangered Species (Any other species)*	Falls within guidelines of USFWS 2013 Interim Policy or "No Effect"	"Not likely to Adversely Affect"	-	-	"Likely to Adversely Affect"
Environmental Justice	No disproportionately high and adverse impacts	-	-	-	Potential ⁸
Sole Source Aquifer	No Detailed Groundwater Assessment	-	-	-	Detailed Groundwater Assessment
Floodplain	No Substantial Impacts	-	-	-	Substantial Impacts
Section 4(f) Impacts	None	-	-	-	Any ⁹
Section 6(f) Impacts	None	-	-	-	Any
Permanent Traffic Alteration	None	-	-	-	Any
Noise Analysis Required	No	-	-	-	Yes
Air Quality Analysis Required	No	-	-	-	Yes ¹⁰
Approval Level	Concurrence by DE or ESD	DE or ESD	DE or ESD	DE and/or ESD	DE and/or ESD; and FHWA

¹ Coordinate with INDOT Environmental Services Division. INDOT will then coordinate with the appropriate FHWA Environmental Specialist.

² Any involvement with a bridge processed under the Historic Bridge Programmatic Agreement.

³ Total permanent impacts to streams (linear feet) and wetlands (acres).

⁴ US Army Corps of Engineers Individual 404 Permit

⁵ Total permanent and temporary right-of-way. This does not include reacquisition of existing apparent right-of-way.

⁶ Avoidance and Mitigation Measures (AMMs) determined by the IPAC determination key to be required that are not tree AMMs, bridge AMMs, or structure AMMs.

⁷ Projects that do not fall under a Species Specific Programmatic and results in a "Likely to Adversely Affect". Other findings can be processed as a lower level CE.

⁸ Potential for causing a disproportionately high and adverse impact.

⁹ Section 4(f) use resulting in an Individual, Programmatic, or *de minimis* evaluation. The only exception is a *de minimis* evaluation for historic properties (Effective January 2, 2020). If a historic property *de minimis* and no other use, mark the *None* column.

¹⁰ Hot Spot Analysis and/or MSAT Quantitative Emission Analysis.

* Includes the threatened/endangered species critical habitat

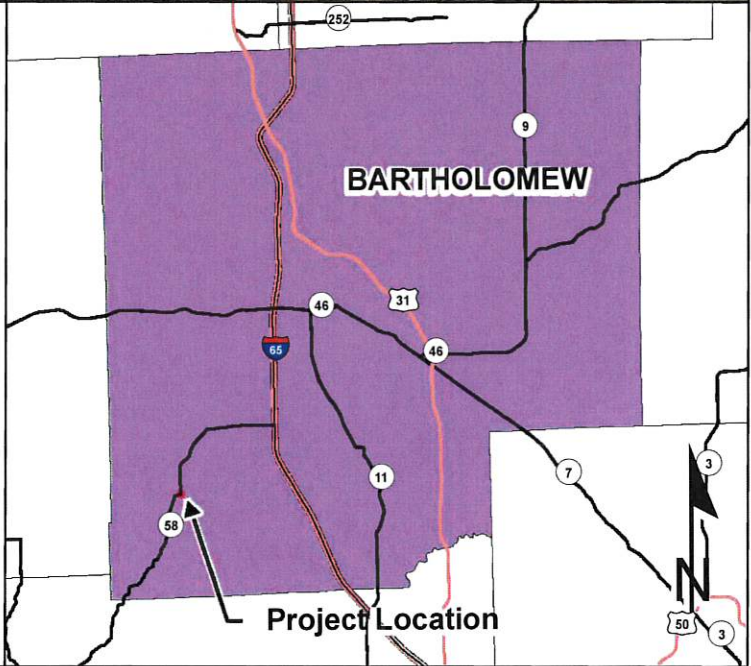
Note: Substantial public or agency controversy may require a higher-level NEPA document.

APPENDIX B

Graphics

Des. No. 2100568

Project Location Map
SR 58, 5.61 Miles West of I-65
Des. No. 1902833, Bridge Replacement
Bartholomew County, Indiana



Sources:
Non Orthophotography
Data - Obtained from the State of Indiana Geographical Information Office Library
Orthophotography - Obtained from Indiana Map Framework Data (www.indianamap.org)
Map Projection: UTM Zone 16 N **Map Datum:** NAD83
 This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.



INDIANA STATEWIDE GIS DATA

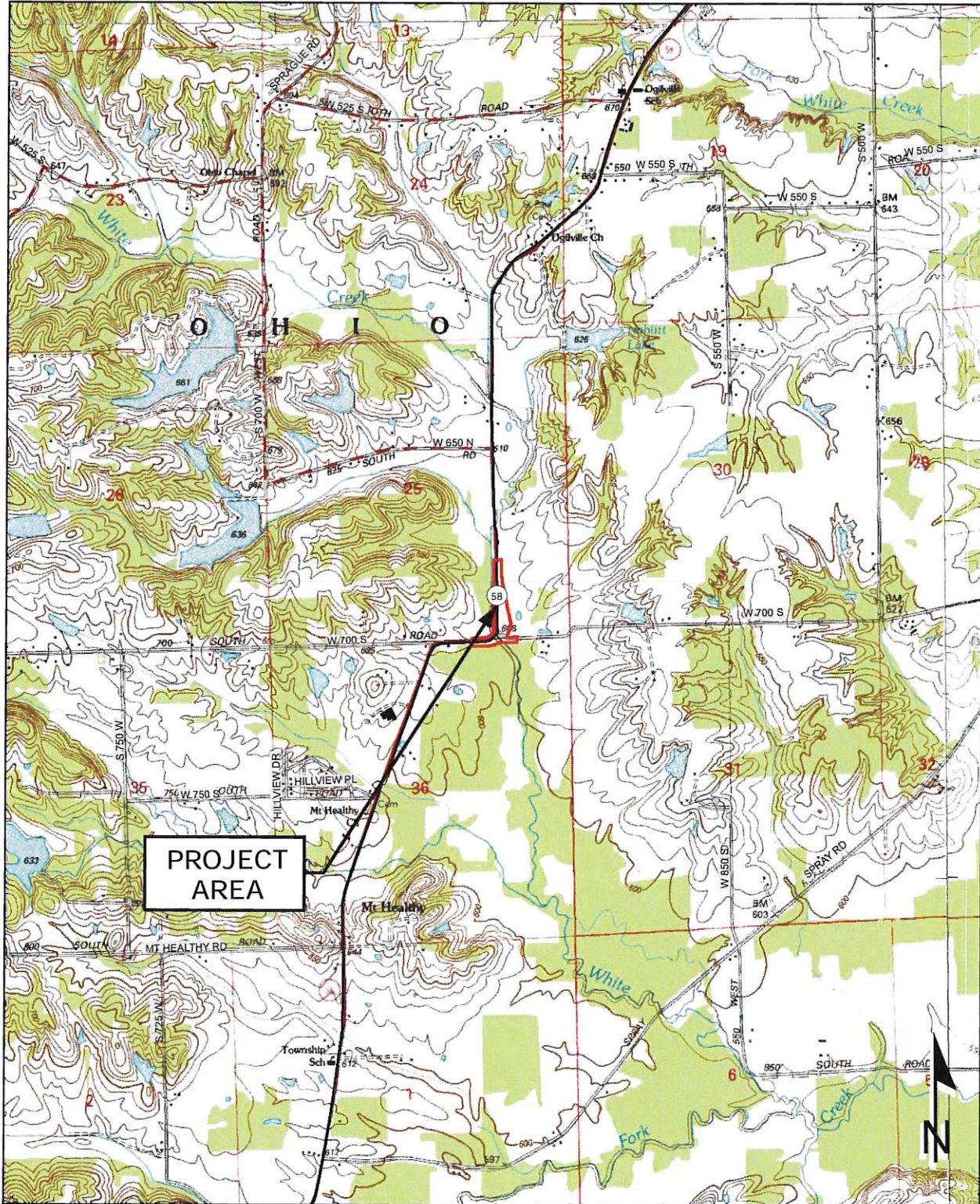
Aerial Map
SR 58, 5.61 Miles West of I-65
Des. No. 2100568, Bridge Replacement Project
Bartholomew County, Indiana



Sources: 240 120 0 240 Feet
- Non-Orthophotography
- Data - Obtained from the State of Indiana Geographical Information Office Library
- Orthophotography - Obtained from Indiana Map Framework Data (www.indianamap.org)
- Map Projection: UTM Zone 16 N Map Datum: NAD83
This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.

INDIANA STATEWIDE
AERIAL IMAGERY
FLOWN 2016

Topographic Map
 SR 58, 5.61 Miles West of I-65
 Des. No. 2100568, Bridge Replacement Project
 Bartholomew County, Indiana



PROJECT AREA

Sources:
 Non-Orthophotography
 Data - Obtained from the State of Indiana Geographical Information Office Library
 Orthophotography - Obtained from Indiana Map Framework Data (www.indianamap.org)
 Map Projection: UTM Zone 16 N Map Datum: NAD83

This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.

**WAYMANSVILLE
 QUADRANGLE INDIANA
 7.5 MINUTE SERIES
 (TOPOGRAPHIC)**

INDIANA DEPARTMENT OF TRANSPORTATION



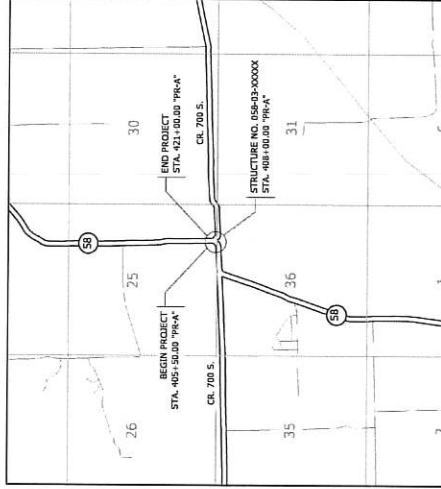
BRIDGE PLANS

FOR SPANS OVER 20 FEET

ROUTE: S.R. 58 AT: RP 116+60

PROJECT NO. 2100568 P.E.
2100568 R/W
2100568 CONST.

BRIDGE REPLACEMENT ON S.R. 58 OVER WHITE CREEK 5.61 MILES WEST OF I-65 IN SECTION 36, T-8-N, R-4-E, JACKSON TOWNSHIP AND SECTION 25, T-8-N, R-4-E, OHIO TOWNSHIP, BARTHOLOMEW COUNTY, INDIANA



SCALE: 1" = 200'

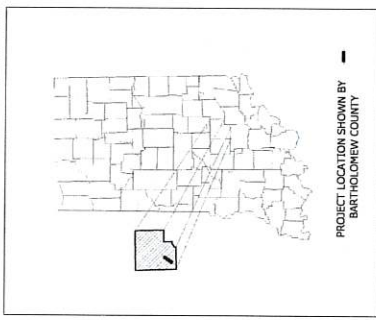
STAGE 1 PLANS
OCTOBER 2023

PROJECT	DESIGNATION
Z100568	Z100568
CONTRACT	BRIDGE FILE
R-93771	058-03-0000X

STRUCTURE INFORMATION			
STRUCTURE	TYPE	SPAN AND SKEW	OVER
058-03-0000X	REINFORCED CONCRETE SLAB BRIDGE	3 SPANS @ 30'-0", 50' AND 30'-0" SKEW: RURAL	WHITE CREEK
			STATION 408+00.00 "R-R-A"

TRAFFIC DATA		S.R. 58	C.R. 700 S.
A.A.D.T. (2026)	1715 V.P.D.	375 V.P.D.	375 V.P.D.
A.A.D.T. (2046)	1725 V.P.D.	386 V.P.D.	386 V.P.D.
D.H.V. (2046)	182 V.P.H.	38 V.P.H.	38 V.P.H.
DIRECTIONAL DISTRIBUTION	50.12% (EAST)	50%	50%
TRUCKS	4.97% D.H.V.	3.7% D.H.V.	3.7% D.H.V.
	4.93% A.A.D.T.	0.59% A.A.D.T.	0.59% A.A.D.T.

DESIGN DATA	
DESIGN SPEED	45 M.P.H.
DESIGN CONTROL	3R (NON-FREEWAY)
FUNCTIONAL CLASSIFICATION	3R (NON-FREEWAY)
RURAL/URBAN	RURAL
TERMINI	RURAL
ACCESS CONTROL	NONE



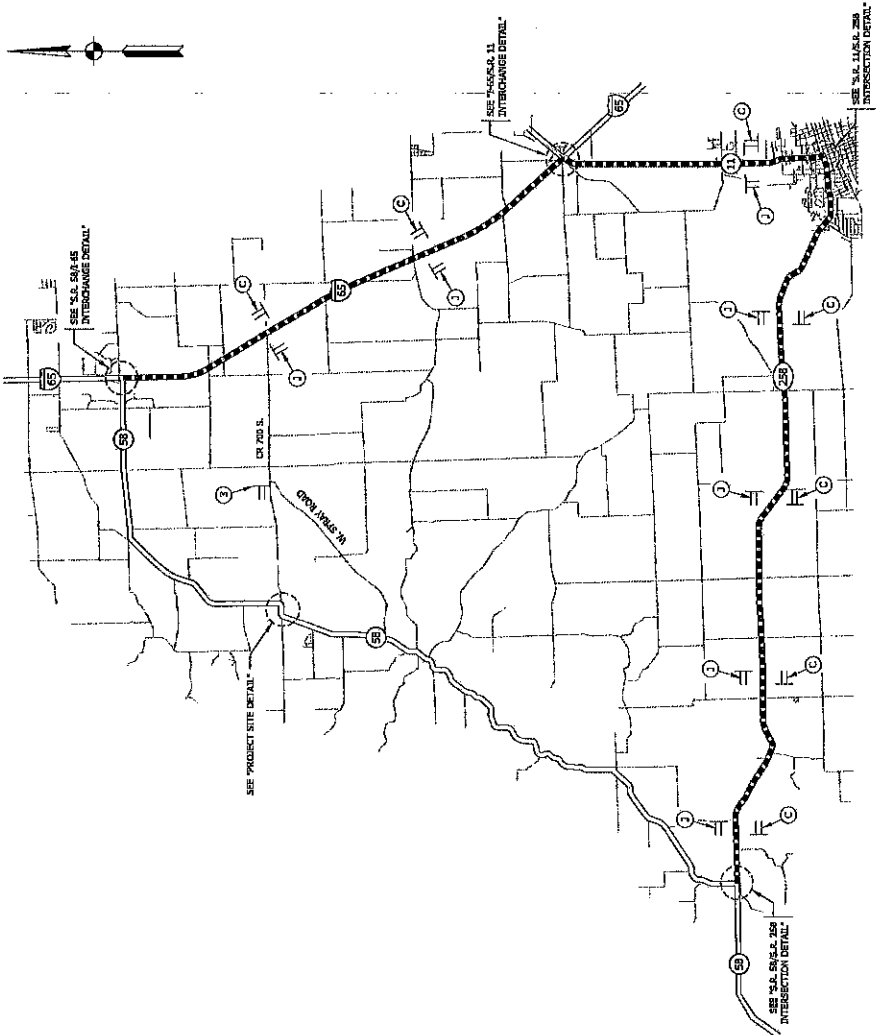
LATITUDE: 39°05'43" N	LONGITUDE: 85°01'41" W
BRIDGE LENGTH: 0.019 MI	
ROADWAY LENGTH: 0.274 MI	
TOTAL LENGTH: 0.293 MI	
MAX. GRADE: 3.21 %	

H.U.C. 051202060404

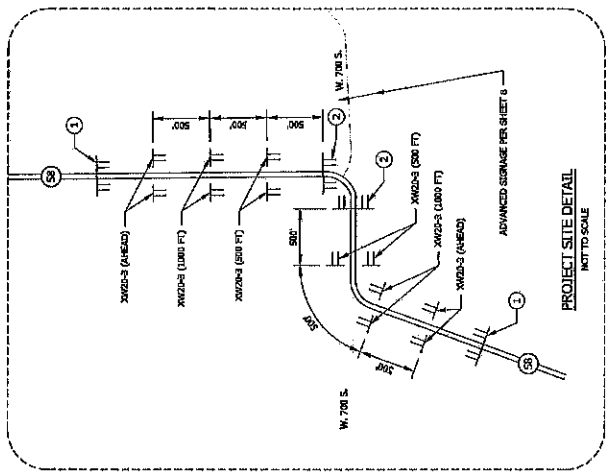


INDIANA DEPARTMENT OF TRANSPORTATION
PLANS AND SPECIFICATIONS DATED 2024 TO
BE USED WITH THESE PLANS.

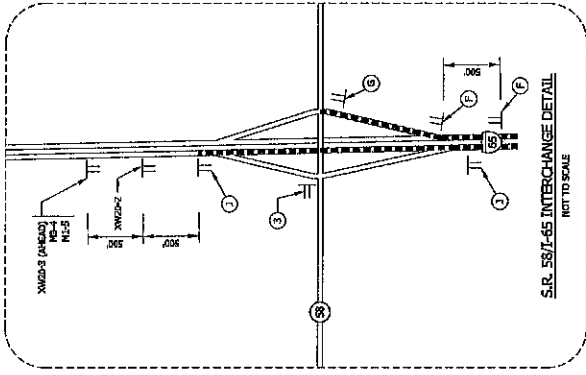
PLANS PREPARED BY: CORRADINO, LLC	317-488-2303	BRIDGE FILE	058-03-0000X
CERTIFIED BY:	DATE:	DESIGNATION	058-03-0000X
APPROVED FOR LETTING:	DATE:	PROJECT	058-03-0000X
		FILE #	058-03-0000X
		PROJECT	058-03-0000X
		FILE #	058-03-0000X
		PROJECT	058-03-0000X
		FILE #	058-03-0000X



- LEGEND**
- ① 24 LFT. OF TYPE 27A BARBICLOS, STAGGERED WITH ROAD CLOSURE SIGN ASSEMBLY RLL-4.
 - ② 24 LFT. OF TYPE 27A BARBICLOS WITH ROAD CLOSURE SIGN ASSEMBLY RLL-2.
 - ③ 24 LFT. OF TYPE 27A BARBICLOS WITH ROAD CLOSURE SIGN ASSEMBLY CONSISTING OF RLL-3A AND 27A-10.
 - DETOUR ROUTE
 - TT SIGN ASSEMBLY

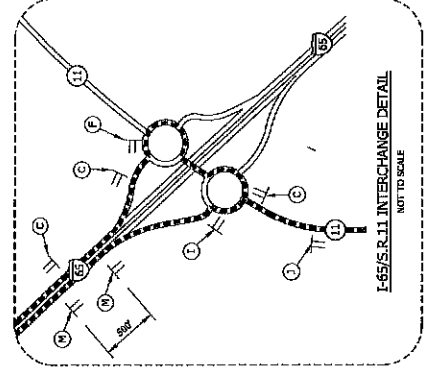
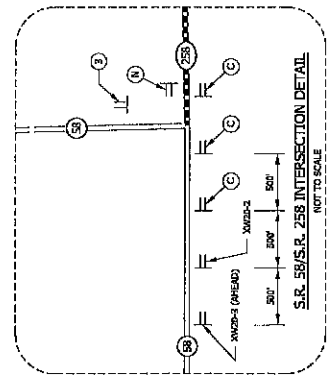
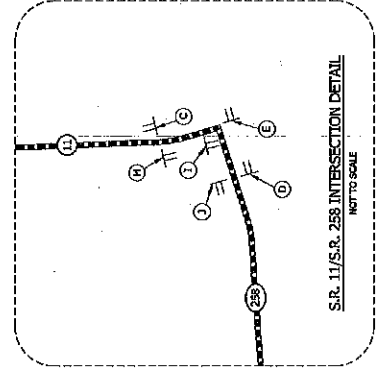


PRELIMINARY	DESIGNED BY: <u>BM</u> CHECKED BY: <u>MT</u>	DRAWN BY: <u>SD</u> CHECKED BY: <u>DM</u>	ORDER NUMBER: <u> </u> DATE: <u> </u>	INSTRUCTIONS FOR APPROVAL: <u> </u>	HORIZONTAL SCALE: <u> </u> VERTICAL SCALE: <u> </u>	SHEET NO.: <u> </u> PROJECT NO.: <u> </u>
	INDIANA DEPARTMENT OF TRANSPORTATION MAINTENANCE OF TRAFFIC DETOUR	CONTRACT NO.: <u> </u> DATE: <u> </u>	SHEET NO.: <u> </u> PROJECT NO.: <u> </u>	HORIZONTAL SCALE: <u> </u> VERTICAL SCALE: <u> </u>	SHEET NO.: <u> </u> PROJECT NO.: <u> </u>	SHEET NO.: <u> </u> PROJECT NO.: <u> </u>



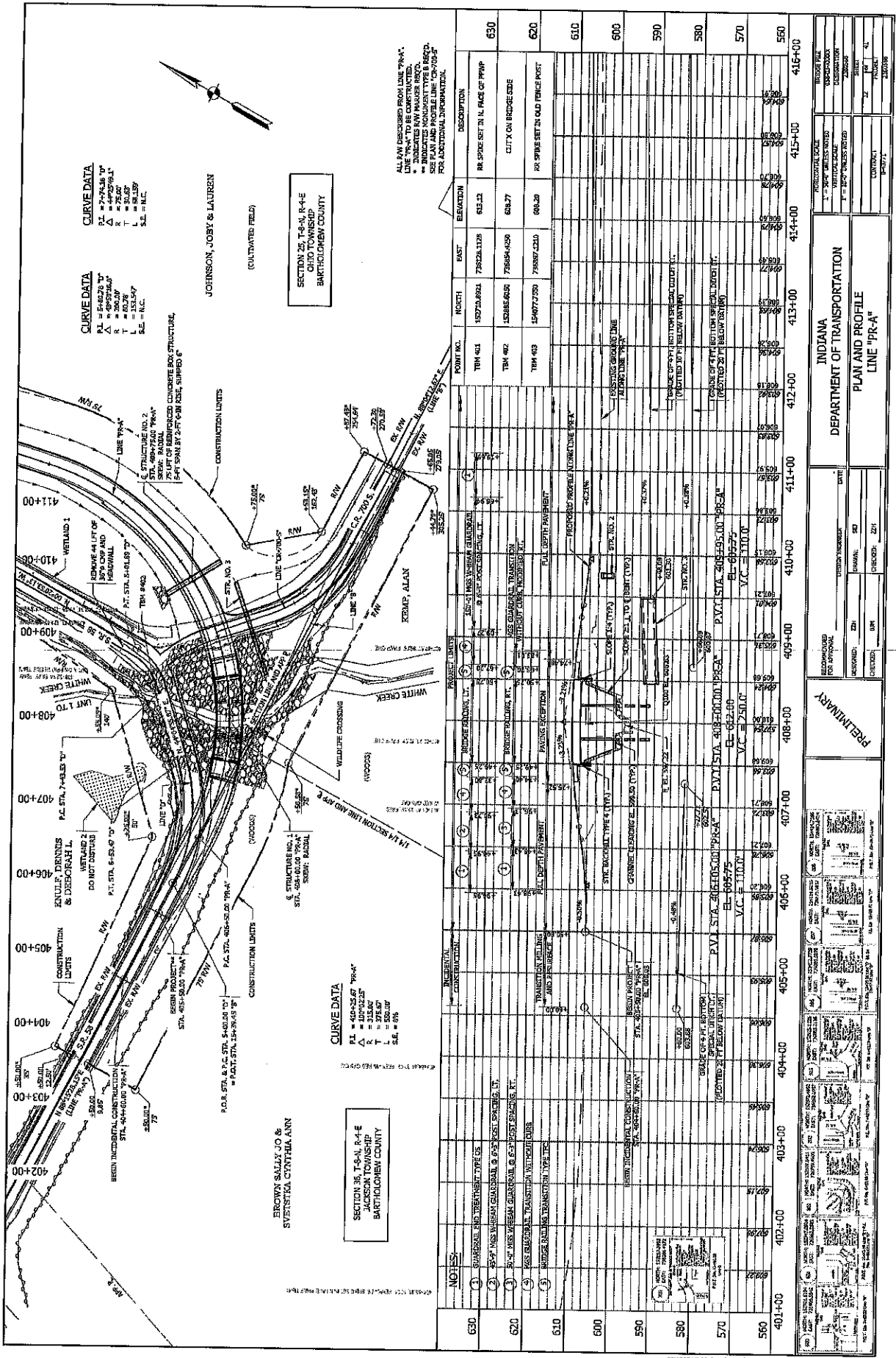
LEGEND

- ① 24' LFT. OF TYPE III-A BARRICADES, STAGGERED WITH ROAD CLOSURE SIGN ASSEMBLY PCL-4.
- ② 24' LFT. OF TYPE III-A BARRICADES WITH ROAD CLOSURE SIGN ASSEMBLY PCL-2.
- ③ 24' LFT. OF TYPE III-A BARRICADES WITH ROAD CLOSURE SIGN ASSEMBLY, CONSISTING OF PCL-1 AND PCL-2.
- DETOUR ROUTE
- TT SIGN ASSEMBLY



PRELIMINARY FOR APPROVAL	DESIGN ENGINEER: _____ DATE: _____	INDIANA DEPARTMENT OF TRANSPORTATION MAINTENANCE OF TRAFFIC DETOUR	SHEET FILE NO.: 2023-03-0000 REVISION: 210508H SHEET NO.: 10 PROJECT: 4-4377 CONTRACT: 4-4377 PROJECT: 210508H
	CHECKED: _____ DATE: _____		

I-1731 - 08 08/2019 (Revised) Standard Plans for Design of Interchanges and Intersections
 October 11, 2023 1:52:58 PM / 2nd Print
 Produced by: _____
 Checked by: _____



CURVE DATA
 P.L. = 7+91.68 "W"
 Δ = 44°25'01.1"
 T = 72.62'
 L = 58.159'
 S.E. = N.C.

CURVE DATA
 P.L. = 6+42.76 "W"
 Δ = 69°37'43.6"
 T = 80.78'
 L = 133.547'
 S.E. = N.C.

CURVE DATA
 P.L. = 408+36.07 "W"
 Δ = 100°02'28.2"
 T = 315.66'
 L = 378.807'
 S.E. = 4.0%

SECTION 36, T-9-N, R-4-E
 JACKSON TOWNSHIP
 BARTHOLOMEW COUNTY

SECTION 25, T-9-N, R-4-E
 CHIO TOWNSHIP
 BARTHOLOMEW COUNTY

ALL PLAN DIMENSIONS FROM LINE "W"
 LINE "W" TO BE CONSTRUCTED.
 * INDICATES NEW PAVEMENT RECD.
 * INDICATES EXISTING PAVEMENT TYPE & RECD.
 * SEE PLAN FOR DIMENSIONS & RECD.
 * FOR ADDITIONAL INFORMATION.

STATION	ELEVATION	DESCRIPTION
630	631.2	RR SPIKE SET IN N. FACE OF P/WP
620	621.7	CUT X ON BRIDGE SIDE
610	612.9	RR SPIKE SET IN OLD FENCE POST
600		
590		
580		
570		
560		
401+00	413+00	414+00
402+00	413+00	414+00
403+00	413+00	414+00
404+00	413+00	414+00
405+00	413+00	414+00
406+00	413+00	414+00
407+00	413+00	414+00
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410+00	413+00	414+00
411+00	413+00	414+00

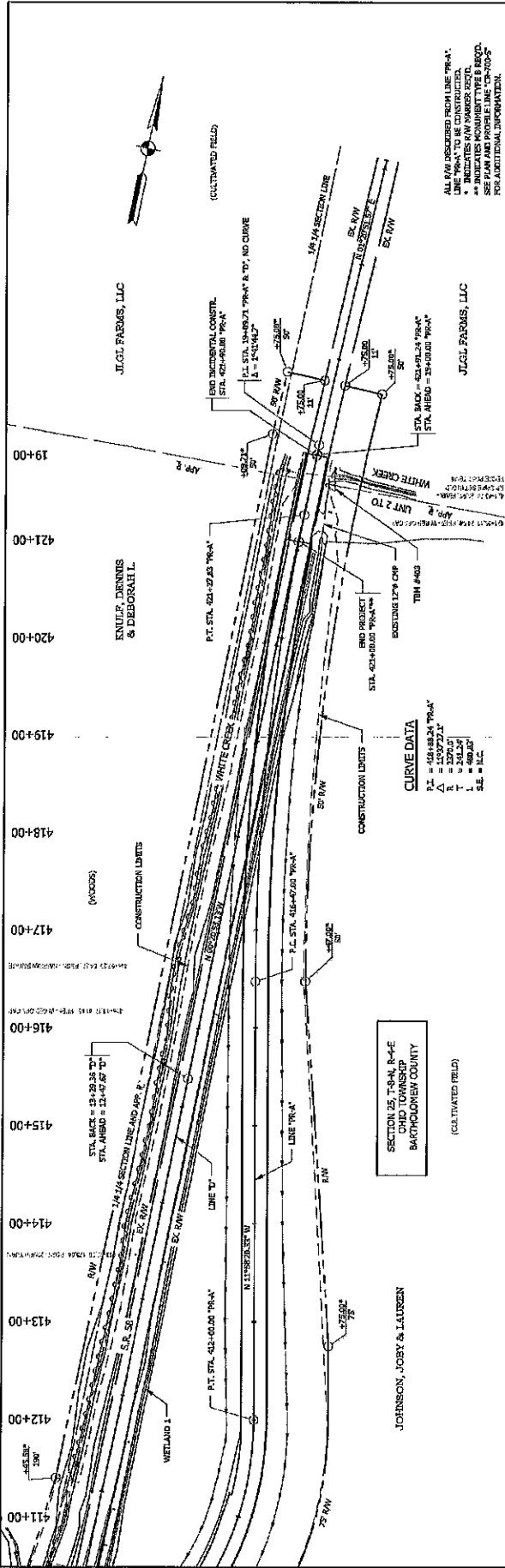
INDIANA DEPARTMENT OF TRANSPORTATION
PLAN AND PROFILE
LINE "P-R-A"

REVISIONS FOR APPROVAL: [] DATE: []
 REVISIONS FOR RECORD: [] DATE: []

DESIGNED BY: [] DRAWN BY: [] CHECKED BY: []
 IN CHARGE: []

CONTRACT NO. 200808

File Name: I:\2008\200808\200808_Plan_Profile\200808_Plan_Profile.dwg
 Date: 12/22/08 12:50:53 PM
 User: jay



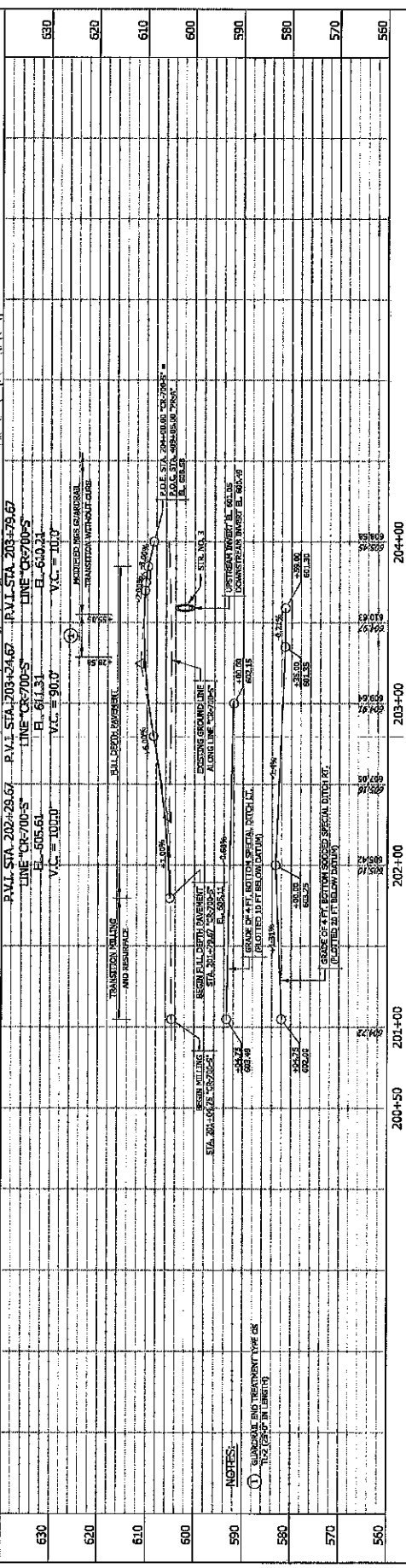
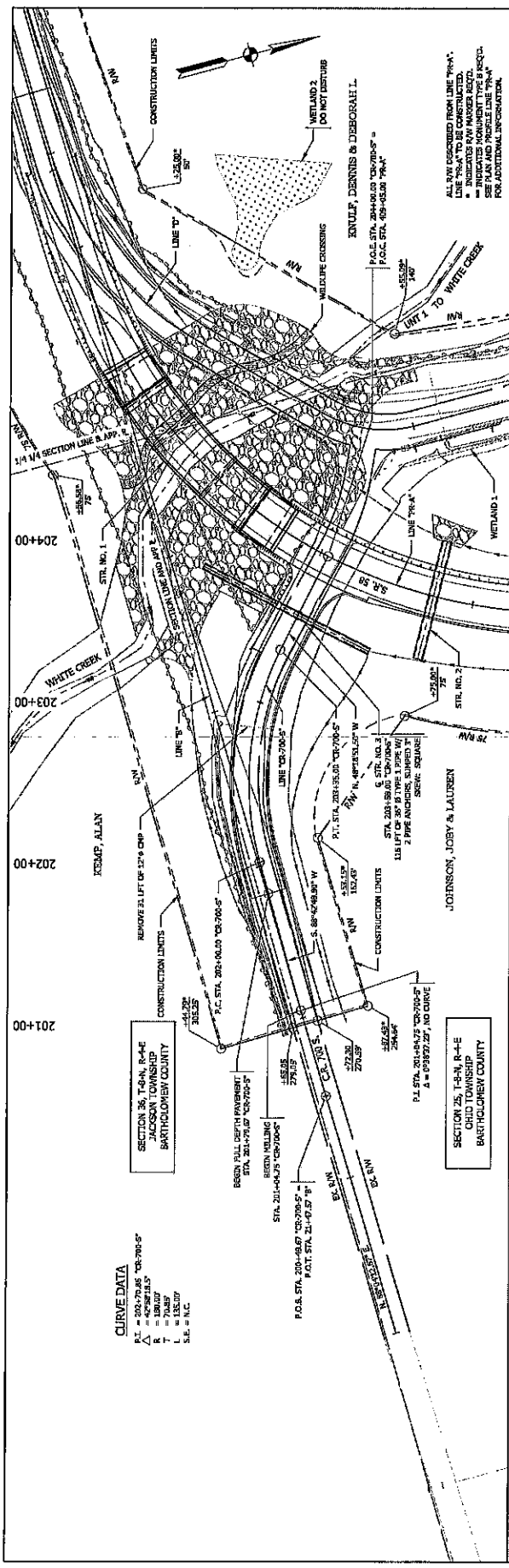
PORT NO.	NORTH	EAST	ELEVATION	DESCRIPTION
630	15731.4612	72628.1158	643.13	RR SPIKE SET IN N. FACE OF RWYP
620	15738.6260	72684.4250	648.77	CUT X ON BRIDGE SIDE
610	15407.7250	72682.1200	650.20	RR SPIKE SET IN OLD FENCE POST
600				
590				
580				
570				
560				

INDIANA
DEPARTMENT OF TRANSPORTATION
PLAN AND PROFILE
LINE "PRA"

DATE: 11/20/2013
PROJECT: 13-00000000
SHEET: 13-00000000

INDIANA SCALE
1" = 20' HORIZONTAL
1" = 10' VERTICAL

DATE: 11/20/2013
PROJECT: 13-00000000
SHEET: 13-00000000



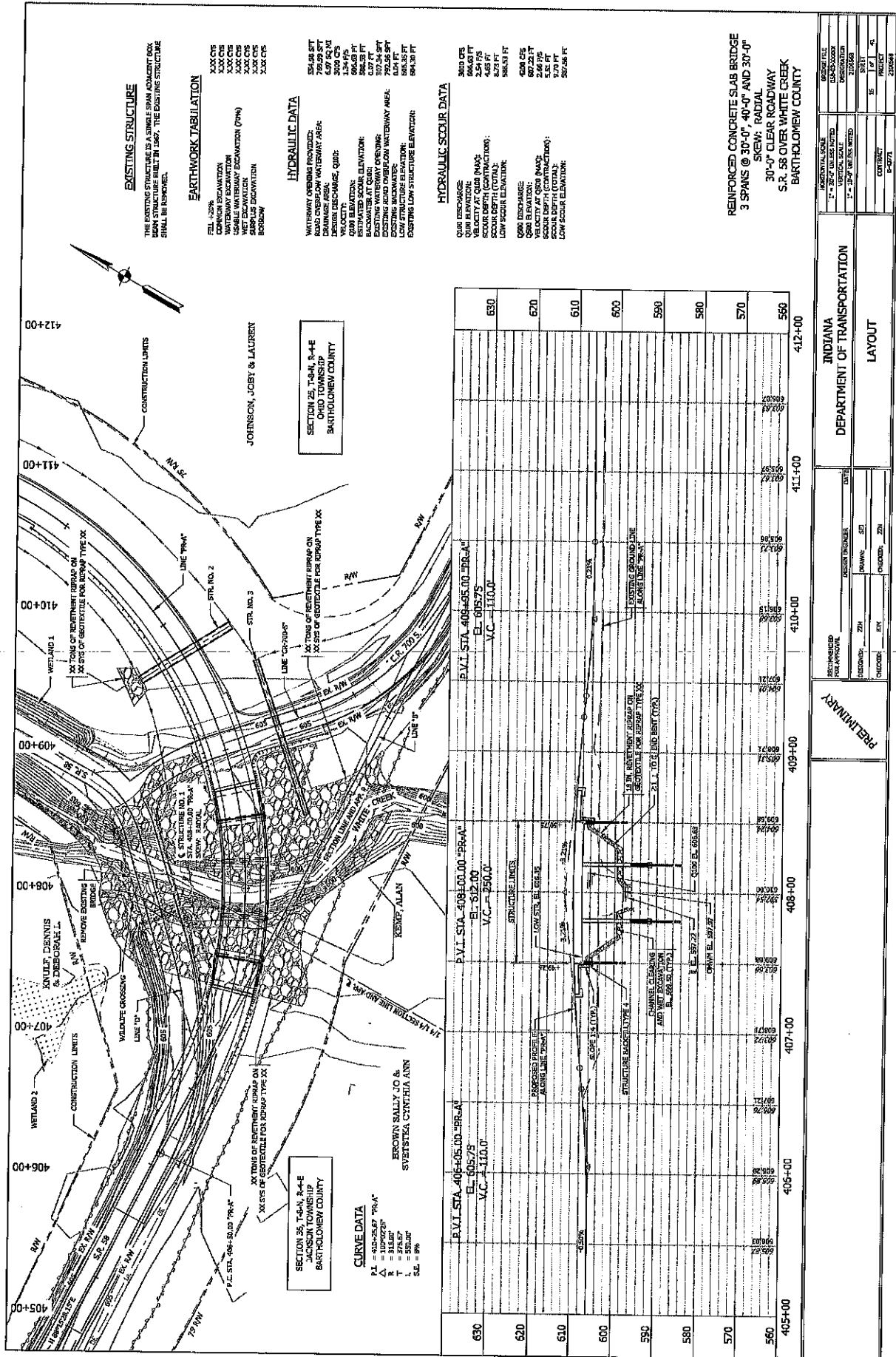
STATION	ELEVATION	REMARKS
201+00	560.00	EXISTING GROUND
201+00	565.00	PROPOSED GRADE
201+50	570.00	PROPOSED GRADE
202+00	575.00	PROPOSED GRADE
202+50	580.00	PROPOSED GRADE
203+00	585.00	PROPOSED GRADE
203+50	590.00	PROPOSED GRADE
204+00	595.00	PROPOSED GRADE

INDIANA DEPARTMENT OF TRANSPORTATION
PLAN AND PROFILE
LINE "CR-700-S"

PRELIMINARY

SEE PLAN AND PROFILE FOR LINE "CR-700-S" FOR ALIGNMENT, REFERENCE TIES AND VERTICAL CONTROL.

DATE: _____
DESIGN ENGINEER: _____
CHECKED: _____
PROJECT: _____
SHEET: _____
CONTRACT: _____
PROJECT: _____
SHEET: _____

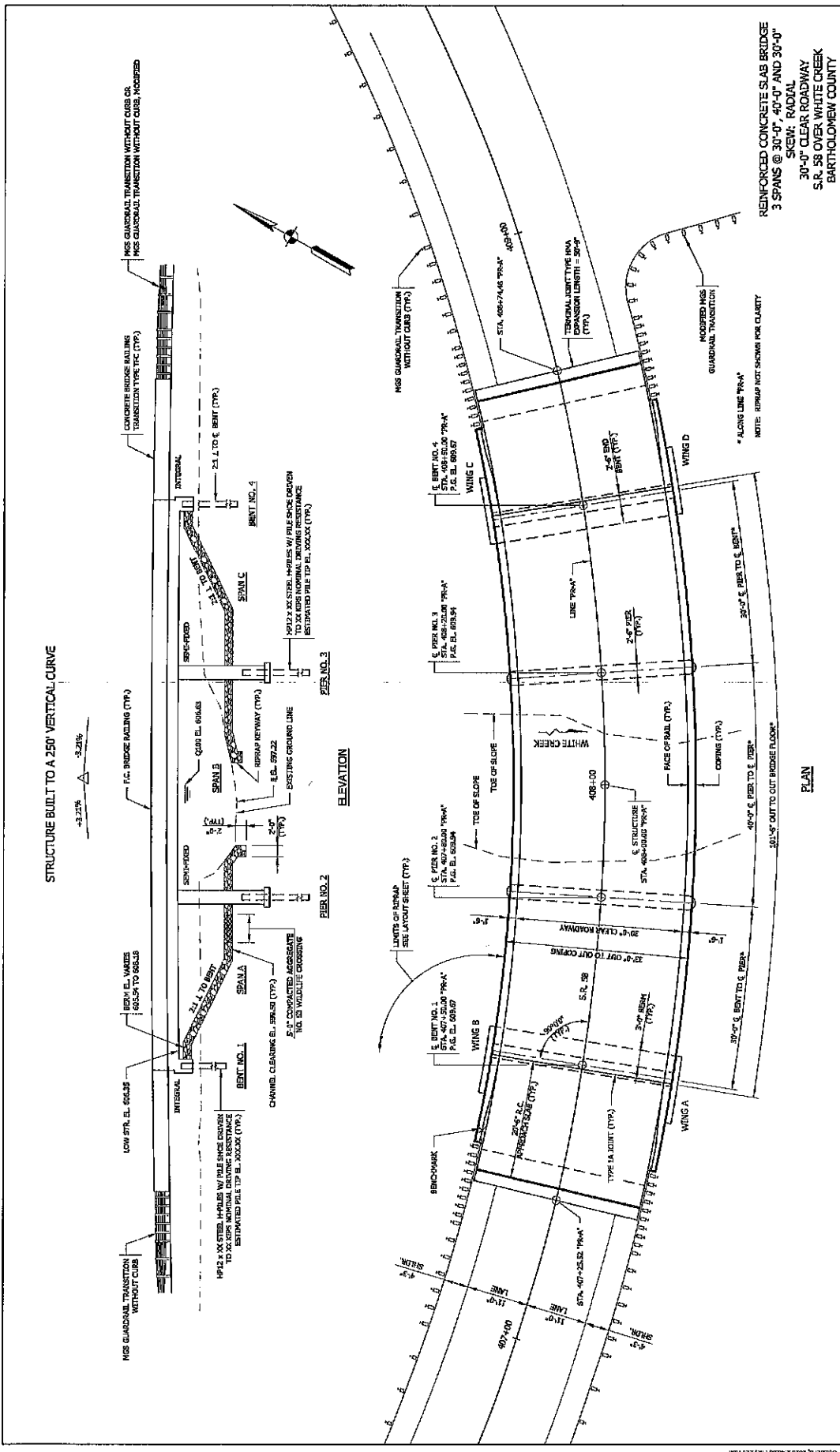


PRELIMINARY

INDIANA
 DEPARTMENT OF TRANSPORTATION
 LAYOUT

DESIGNER	DATE
PROJECT NO.	PROJECT
CONTRACT	CONTRACT
PROJECT	PROJECT
PROJECT	PROJECT
PROJECT	PROJECT

File Name: P:\ATD - 01-20\Browns Bridge\Drawings\10-Design\Bridges\A\Browns Bridge.dwg
 Date: 12/22/2011 10:28:58 AM
 User: jay
 Scale: 1:1
 Plot: 12/22/2011 10:28:58 AM
 Plotter: HP DesignJet 5000 Series
 Plot Size: 36x48
 Plot Orientation: Landscape
 Plot Scale: 1:1
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 Plot Lineweight: 0.25
 Plot Linetype: Solid
 Plot Font: Arial, 10
 Plot Font Color: Black
 Plot Font Size: 10
 Plot Font Weight: Normal
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 Plot Font Size: 10
 Plot Font Weight: Normal
 Plot Font Style: Normal



STRUCTURE BUILT TO A 250' VERTICAL CURVE

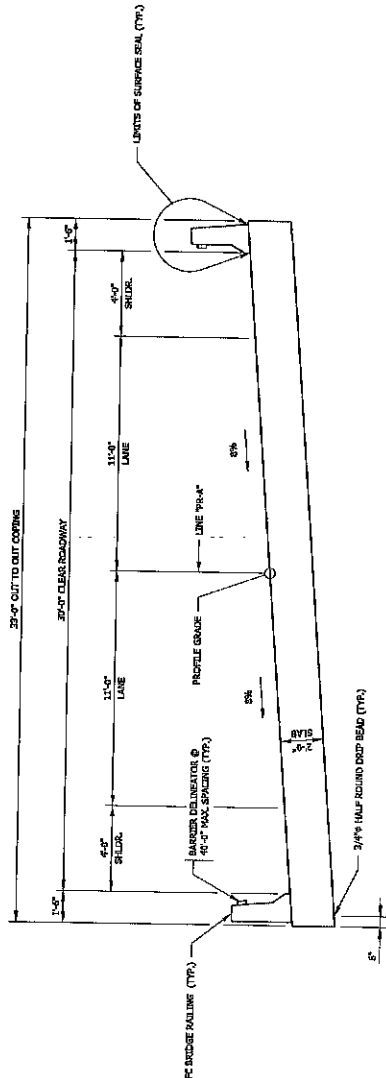
REINFORCED CONCRETE SLAB BRIDGE
 3 SPANS @ 30'-0", 40'-0" AND 30'-0"
 SKEW: RADIAL
 S.R. 58 OVER WHITE CREEK
 BARTHOLOMEW COUNTY

GENERAL PLAN

PROJECT NO.	2022-2-6288	DATE	12/12/22
DESIGNER	BRUNNEN	CHECKED	ZPH
DATE	12/12/22	DATE	12/12/22
SCALE	AS SHOWN	SCALE	AS SHOWN
SHEET	18	TOTAL SHEETS	41
CONTRACT	8-5271	PROJECT	2022-2-6288

INDIANA
 DEPARTMENT OF TRANSPORTATION
 GENERAL PLAN

PRELIMINARY



TYPICAL SECTION

GENERAL NOTES

REINFORCING STEEL COVERS SHALL BE 2.0\"/>

DESIGN DATA

LIVE LOAD
 ASSIGNED PER FHWA LOADING, IN ACCORDANCE WITH AASHTO STANDARD SPECIFICATIONS FOR BRIDGES, WITH SECTION AND SUBSEQUENT INTERIM SPECIFICATIONS.

DEAD LOAD
 ACTUAL WEIGHT PLUS RESISTANCE FOR FUTURE WEARING SURFACE.

FLOOR SLAB
 SLAB DESIGNED WITH A 12\"/>

DESIGN STRESSES

CONCRETE
 CLASS C
 CLASS B
 CLASS A

REINFORCING STEEL
 GRADE 60 F_y = 60,000 PSI

SEISMIC DESIGN DATA
 SEISMIC PERFORMANCE CLASS
 ACCIDENTAL COEFFICIENT
 SEISMIC SOIL PROFILE TYPE
 CLASS XX

WIND LOAD
 AS APPLICABLE PER HORIZONTAL WIND LOADING IN ACCORDANCE WITH LRFD 3.2.1.1.

REINFORCED CONCRETE SLAB BRIDGE
 3 SPANS @ 30'-0\"/>

HORIZONTAL SCALE	BRIDGE TITLE
30' = 1\"/>	
VERTICAL SCALE	DATE
1\"/>	
PROJECT	200808

INDIANA	INDIANA
DEPARTMENT OF TRANSPORTATION	DEPARTMENT OF TRANSPORTATION
GENERAL PLAN	GENERAL PLAN

DESIGNED BY	DATE
2/21/08	
DRAWN BY	DATE
2/21/08	
CHECKED BY	DATE
2/21/08	

PRELIMINARY

File Name: P:\1739 - IIS 08 System Upgrade\1739\Drawings\1739\1739-1739-1739-1739.dwg
 Plot Date: 2/21/08 10:00 AM
 Plot Path: P:\1739 - IIS 08 System Upgrade\1739\Drawings\1739\1739-1739-1739-1739.dwg - Layout1

Photo Key
SR 58, 5.61 Miles West of I-65
Des. No. 2100568, Bridge Replacement Project
Bartholomew County, Indiana

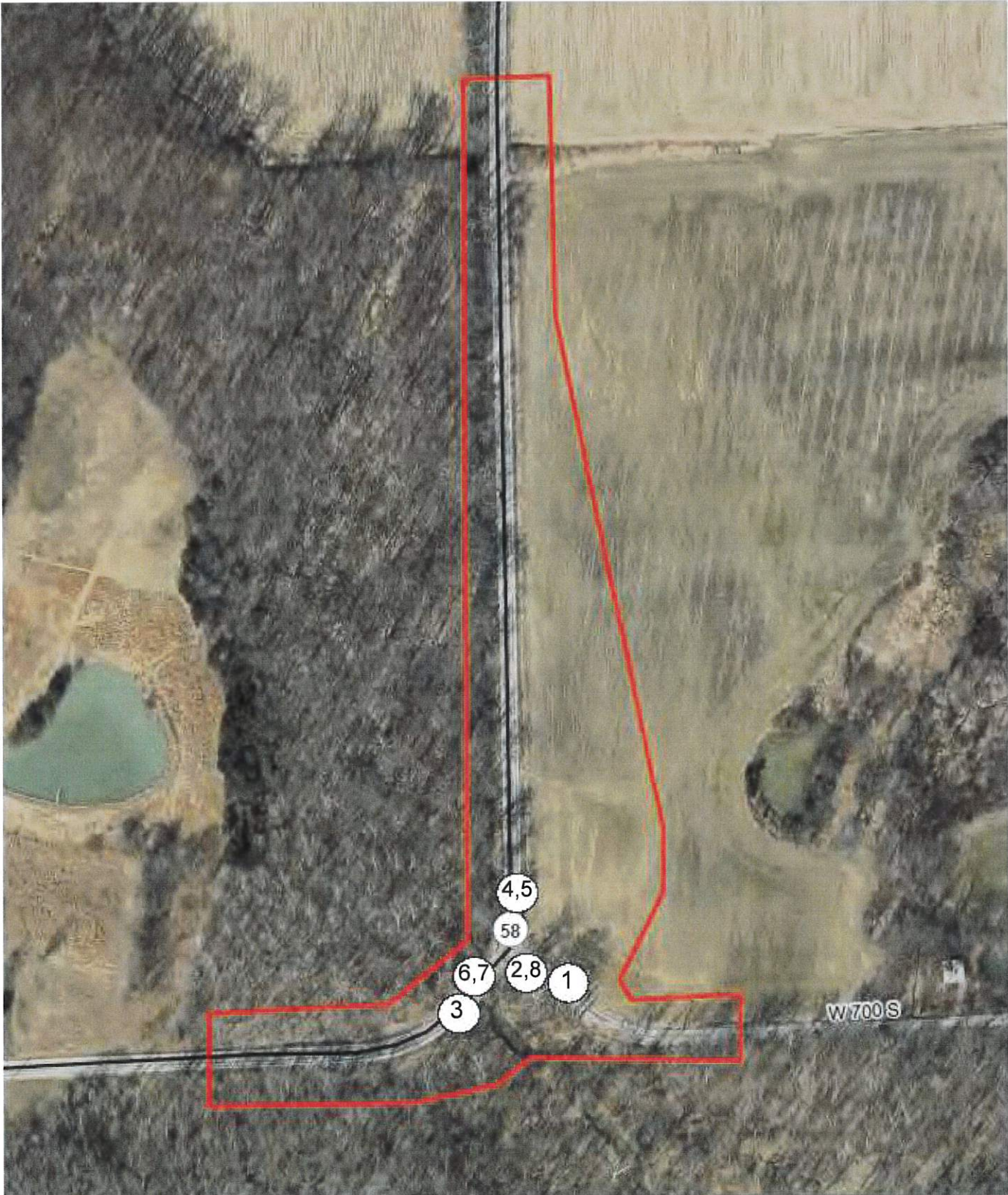


Photo Log
Photos taken on September 22, 2023



Photo 1: CR 700 S at SR 58 looking west.



Photo 2: CR 700 looking south at the SR 58 bridge.



Photo 3: SR 58 just south of the bridge.



Photo 4: View of the 36-inch corrugated metal pipe.

Photo Log Continued
Photos taken on September 22, 2023



Photo 5: View of inside the corrugated metal pipe.



Photo 6: Side view of the SR 58 bridge.

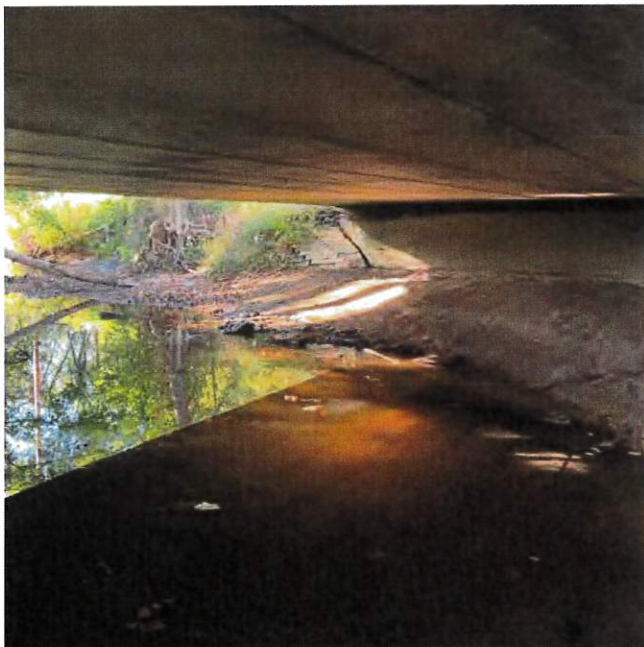


Photo 7: View of under the SR 58 bridge.



Photo 8: 12-inch corrugated metal pipe located under CR 700 S.

APPENDIX C

Early Coordination

Des. No. 2100568



INDIANA DEPARTMENT OF TRANSPORTATION

100 North Senate Avenue
Room N758-ES
Indianapolis, Indiana 46204

PHONE: (855) 463-6848
(855) INDOT4U

Eric Holcomb, Governor
Mike Smith, Commissioner

February 7, 2023

Indiana Department of Transportation - Seymour District
157 Agrico Ln.,
Seymour, IN 47274

Re: Early Coordination Letter, Des. No.: 2100568, SR 58, Small Structure Project 5.61 miles west of I-65,
Bartholomew County, Indiana
Environmental Early Coordination

Dear Environmental Coordinator:

The Indiana Department of Transportation (INDOT), with federal funding, intends to proceed with the small structure project in Benton County, Indiana. This letter is part of the early coordination phase of the environmental review process. We are requesting comments from your area of expertise regarding any possible environmental effects associated with this project. **Please use the above designation number and description in your reply.** We will incorporate your comments into a study of the project's environmental impacts.

This project is located on SR 58, 5.61 miles west of Interstate 65 (I-65), in Bartholomew County, Indiana. This section of SR 58 is a two-lane *Rural Principal Collector*. The existing SR 58 has two 12.0-foot-wide travel lanes, a 3.0-foot-wide shoulder. The existing structure is a 70-foot single span bridge. The draft need is due to the deterioration of the structure (rating 5 out of 9) which is in good condition, as well as poor roadway geometry. The draft purpose is to have a structure with a condition rating of at least 7 (good condition) out of 9, improve the hydraulic condition of the cross, and improve roadside safety at the project site. The approximate existing right-of-way is 30 feet each side of the centerline throughout the project.

The proposed project is anticipated to be a bridge replacement with a new roadway alignment to better align with the stream. The country road southeast of the structure will be regraded to tie into the new roadway centerline. Riprap will be placed for scour protection along the abutments of the bridge and along the banks near the existing structure where White Creek turns. The new structure will be built on a new horizontal alignment to better align with the stream and improve roadside safety at the project site. The new alignment will shift SR 58 to the east into an existing agricultural field. The project requires the acquisition of up to 7.50 acre of permanent right-of-way. Proposed right-of-way widths along SR 58 are 75 feet from the new centerline. The project will be approximately 2500 feet in length. Maintenance of traffic (MOT) is anticipated to be a road closure with a detour route of I-65 to SR 11 to SR 258. Approximately 3.5 acres of tree clearing will be required for this project. The project is anticipated to begin construction in Fall of 2025 and be completed in the fall of 2026.

Land use in the vicinity of the project area is primarily forest and agricultural. The project is anticipated to qualify for the Rangewide Programmatic Agreement for the Indiana bat and Northern Long-eared bat by completing the Information for Planning and Consultation (IPaC). Coordination will occur with INDOT Cultural Resources Office (CRO) to evaluate the project area for archaeological and historic resources and for Section 106 compliance. The results of this will be forwarded to the State Historic Preservation Officer (SHPO) for review and concurrence as appropriate.

Should we not receive your response **within thirty (30) calendar days** from the date of this letter, it will be assumed that your agency feels that there will be no adverse effects incurred as a result of the proposed project. However, should you find that an extension to the response time is necessary, a reasonable amount may be granted upon request. If you have any questions regarding this matter, please feel free to contact Zed Hott of Corradino LLC, at 317-488-2363 or zhott@corradino.com and or the Project Manager, Brad Williamson, of INDOT at bwilliamson@indot.in.gov. Thank you in advance for your input.

Sincerely,



Zed Hott
Corradino LLC
200 South Meridian Street, Suite 330
Indianapolis, IN 46225

Attachments:

- A. Project Location Maps
- B. Site Photos

The Following agencies received Early Coordination Letters:

Federal Highway Administration
Federal Office Building, Room 254
575 North Pennsylvania Street
Indianapolis, Indiana 46204

State Conservationist
Natural Resource Conservation Service
6013 Lakeside Boulevard
Indianapolis, IN 46278

Indiana Geological Survey
611 North Walnut Grove
Bloomington, IN 47405

Environmental Coordinator
Indiana Department of Natural Resources
Division of Fish and Wildlife
402 West Washington Street, Rm. W273
Indianapolis, IN 46204

Field Environmental Officer
Chicago Regional Office
US Department of Housing & Urban Development
Metcalf Fed. Bldg.
77 W. Jackson Blvd. Room 2401
Chicago, IL 60604

Regional Environmental Coordinator
Midwest Regional Office
National Park Service
601 Riverfront Drive
Omaha, Nebraska 68102

U.S. Army Corps of Engineers
Louisville District
ATTN: CELRL-RDN
P.O. Box 59
Louisville, KY 40201-0059

Indiana Department of Transportation
Seymour District
157 Agrico Ln.,
Seymour, IN 47274

IDEM
Automatic coordination website

IDEM – Groundwater Section
Electronic Submittal

Bartholomew County Commissioners
440 Third Street
Columbus, IN 47201

Bartholomew County Highway Department
ATTN: Danny Hollander
10150 E. 25th Street
Columbus, IN 47203

Bartholomew County MS4- Stormwater
1040 Second Street
Columbus, IN 47201

Bartholomew County Surveyor
ATTN: E.R. Gray III
440 Third Street Suite 400
Columbus, IN 47201

Bartholomew City Council
440 Third Street
Columbus, IN 47201

Bartholomew Consolidated School Corp.
ATTN: Transportation Department
1260 N. Marr Rd.
Columbus, IN 47201

Environmental Policy Manager
100 N Senate Ave N758-ES
Indianapolis, IN 46204

From: [McWilliams, Robin](#)
To: [Rachel Pluckebaum](#)
Subject: Re: [EXTERNAL] ECL Des. No. 2100568, SR 58 over White Lick Creek, Bridge Project, Bartholomew County
Date: Wednesday, February 15, 2023 1:43:19 PM

Ignore the part about project being within a karst area. That was an error.

Robin

Robin McWilliams Munson
Fish and Wildlife Biologist
U.S. Fish and Wildlife Service
620 South Walker Street
Bloomington, IN 47403
812-334-4261

[Mon-Tues 8-3:30p](#)

[Wed-Thurs 8:30-3p Telework](#)

From: McWilliams, Robin <robin_mcwilliams@fws.gov>
Sent: Wednesday, February 15, 2023 1:42 PM
To: Rachel Pluckebaum <rpluckebaum@CORRADINO.com>
Subject: Fw: [EXTERNAL] ECL Des. No. 2100568, SR 58 over White Lick Creek, Bridge Project, Bartholomew County

Dear Rachel,

This responds to your recent letter requesting our comments on the aforementioned projects.

These comments have been prepared under the authority of the Fish and Wildlife Coordination Act (16 U.S.C. 661 et. seq.) and are consistent with the intent of the National Environmental Policy Act of 1969, the Endangered Species Act of 1973, and the U. S. Fish and Wildlife Service's Mitigation Policy.

The project is within the range of the Indiana bat (*Myotis sodalis*) and northern long-eared bat (*Myotis septentrionalis*; NLEB) and should follow the Indiana bat/NLEB Federal Highway Administration, Federal Rail Administration, and Federal Transit Administration's programmatic consultation process, if applicable (*i.e.* a federal transportation nexus is established). As discussed below, the NLEB reclassification rule was finalized on November 30, 2022, and will now go into effect on March 31, 2023. For projects that are "no effect" or "not likely to adversely affect" the NLEB (per the programmatic consultation), the current determination key in IPAC may continue to be used. The Service has 14 days after a "not likely

to adversely affect" determination letter is generated to review the project and provide additional comments or request additional information (there is no review for projects with a "no effect" determination); if you do not receive a response from us within 14 days, we have no additional comments. **Please note**, if tree-clearing extends beyond 100 feet of the edge of pavement, compensatory mitigation may be required, and, if beyond 300 feet, a stand alone informal consultation may be needed.

The Service is working on an updated determination key that will incorporate forthcoming updates to the 2018 programmatic consultation, including the new listing status for the NLEB. This key should be in place early 2023.

Notice of Proposed ESA Listing Changes

Northern Long-eared Bat

In March 2022, the Service proposed to reclassify the NLEB from its current status as federally threatened to federally endangered. The NLEB original listing and current reclassification proposal are due to sharp population declines associated with white-nose syndrome (WNS), a deadly fungal disease affecting hibernating bats such as the NLEB. On November 30, 2022, the reclassification action was finalized and the new listing will now go into effect March 31, 2023 (delayed from January 30, 2023). At that time, the current 4(d) rule for the NLEB will no longer apply as these types of rules are only applicable to threatened species (not endangered ones). If no form of take of NLEBs is anticipated for this project (*i.e.* the project is determined to be "no effect" or "may affect, not likely to adversely affect"), no reinitiation of this consultation will be necessary once the status change goes into effect.

Tricolored Bat

On September 14, 2022, the Service published a proposal in the Federal Register to list the tricolored bat (*Perimyotis subflavus*; TCB) as endangered under the Endangered Species Act (ESA). The Service has up to 12 months from the date the proposal was published to make a final determination, either to list the tricolored bat under the Act or to withdraw the proposal. The Service determined the bat faces extinction primarily due to the range-wide impacts of WNS. Because TCB populations have been greatly reduced due to WNS, surviving bat populations are now more vulnerable to other stressors such as human disturbance and habitat loss. Species proposed for listing are not afforded protection under the Act; however, as soon as a listing becomes effective (typically 30 days after publication of the final rule in the Federal Register), the prohibitions against jeopardizing its continued existence and "take" will apply. Therefore, if this project or other future or existing projects have the potential to adversely affect the TCB after the potential new listing goes into effect, we recommend that the effects of the project on TCBs and their habitat be analyzed to determine whether

authorization under ESA section 7 or 10 is necessary. Projects or programs with an existing section 7 biological opinion may require reinitiation of consultation, and projects with an existing section 10 incidental take permit may require an amendment to provide uninterrupted authorization for covered activities. Contact your local U.S. Fish and Wildlife Service Ecological Services Office for assistance.

The following is an excerpt from the Service's Section 7 Handbook...

Conference - a process of early interagency cooperation involving informal or formal discussions between a Federal agency and the Services pursuant to section 7(a)(4) of the Act regarding the likely impact of an action on proposed species or proposed critical habitat. **Conferences are: (1) required for proposed Federal actions likely to jeopardize proposed species, or destroy or adversely modify proposed critical habitat;**

The Service has not yet developed any guidelines regarding what level of impact may jeopardize the TCB at the species level. Therefore, in the interim, the Indiana Field Office recommends that any project that does not result in adverse impacts to Indiana bat and/or NLEB (i.e., "no effect" or "may affect, not likely to adversely affect" determinations) would not rise to the level of jeopardy for TCB. The INFO also recommends that action agencies include a written jeopardy analysis (including a conceptual logic path) for the TCB in their administrative record for each project that may affect the species.

The TCB is a small insectivorous bat that typically overwinters in caves, abandoned mines and tunnels, and road-associated culverts (southern portion of the range) and spends the rest of the year in forested habitats, typically roosting among live and dead leaf clusters in tree branches. For more information on TCB and the proposed rule, please see: <https://www.fws.gov/species/tricolored-bat-perimyotis-subflavus> and for more information on WNS, please see: <https://www.whitenosesyndrome.org/>

Wetland and stream impacts may require permits from the U.S. Army Corps of Engineers, the Indiana Department of Environmental Management's Water Quality Certification program, and the Indiana Department of Natural Resources. Wetland impacts should be avoided, and any unavoidable impacts should be compensated for in accordance with agency mitigation guidelines.

The project is located within the karst region of Indiana. If any karst features are encountered or impacted, please re-coordinate with us.

Based on a review of the information you provided, the U.S. Fish and Wildlife Service has no other comments on the projects as currently proposed. However, should new information

arise pertaining to project plans or a revised species list be published, it will be necessary for the Federal agency to reinitiate consultation. Standard recommendations are provided below.

We appreciate the opportunity to comment at this early stage of project planning. If you have any questions about our recommendations, please contact me at robin_mcwilliams@fws.gov or you may call 812-334-4261 x. 207.

Sincerely,
Robin McWilliams Munson

Standard Recommendations:

1. Do not clear trees or understory vegetation outside the construction zone boundaries. **(This restriction is not related to the "tree clearing" restriction for potential Indiana Bat habitat.)**
2. Restrict below low-water work in streams to placement of culverts, piers, pilings and/or footings, shaping of the spill slopes around the bridge abutments, and placement of riprap. Culverts should span the active stream channel, should be either embedded or a 3-sided or open-arch culvert, and be installed where practicable on an essentially flat slope. When an open-bottom culvert or arch is used in a stream, which has a good natural bottom substrate, such as gravel, cobbles and boulders, the existing substrate should be left undisturbed beneath the culvert to provide natural habitat for the aquatic community.
3. Restrict channel work and vegetation clearing to the minimum necessary for installation of the stream crossing structure.
4. Minimize the extent of hard armor (riprap) in bank stabilization by using bioengineering techniques whenever possible. If riprap is utilized for bank stabilization, extend it below low-water elevation to provide aquatic habitat.
5. Implement temporary erosion and sediment control methods within areas of disturbed soil. All disturbed soil areas upon project completion will be vegetated following INDOT's standard specifications.
6. Avoid all work within the inundated part of the stream channel (in perennial streams and larger intermittent streams) during the fish spawning season (April 1 through June 30), except for work within sealed structures such as caissons or cofferdams that were installed prior to the spawning season. No equipment shall be operated below Ordinary High-Water Mark during this time unless the machinery is within the caissons or on the cofferdams.
7. Evaluate wildlife crossings under bridge/culverts projects in appropriate situations. Suitable crossings include flat areas below bridge abutments with suitable ground cover, high water shelves in culverts, amphibian tunnels and diversion fencing

Robin McWilliams Munson
Fish and Wildlife Biologist
U.S. Fish and Wildlife Service

620 South Walker Street
Bloomington, IN 47403
812-334-4261

Mon-Tues 8-3:30p

Wed-Thurs 8:30-3p Telework

From: Rachel Pluckebaum <rpluckebaum@CORRADINO.com>
Sent: Wednesday, February 15, 2023 12:49 PM
To: McWilliams, Robin <robin_mcwilliams@fws.gov>
Subject: RE: [EXTERNAL] ECL Des. No. 2100568, SR 58 over White Lick Creek, Bridge Project, Bartholomew County

I'm sorry. I didn't realize I didn't add the maps or photos. My apologies! See attached.

From: McWilliams, Robin <robin_mcwilliams@fws.gov>
Sent: Wednesday, February 15, 2023 12:32 PM
To: Rachel Pluckebaum <rpluckebaum@CORRADINO.com>
Subject: Re: [EXTERNAL] ECL Des. No. 2100568, SR 58 over White Lick Creek, Bridge Project, Bartholomew County

That helps. Also just a map of the project location is helpful so I can find it on my maps. And any photos of the site if available. Just so I'm sure to be looking at the same site!

Robin

Robin McWilliams Munson
Fish and Wildlife Biologist
U.S. Fish and Wildlife Service
620 South Walker Street
Bloomington, IN 47403
812-334-4261

Mon-Tues 8-3:30p

Wed-Thurs 8:30-3p Telework

From: Rachel Pluckebaum <rpluckebaum@CORRADINO.com>
Sent: Wednesday, February 15, 2023 12:19 PM
To: McWilliams, Robin <robin_mcwilliams@fws.gov>
Subject: RE: [EXTERNAL] ECL Des. No. 2100568, SR 58 over White Lick Creek, Bridge Project, Bartholomew County

Hi Robin,

We do not have Stage 1 Plans submitted yet. Should I wait until we have those submitted or show

you what we have at the moment? I have attached a working sheet. Let me know if you need more information.

Green Line: Proposed ROW

Yellow Line: Construction Limits

Thin Blue Line: Stream

Thanks,
Rachel

From: McWilliams, Robin <robin_mcwilliams@fws.gov>
Sent: Wednesday, February 15, 2023 10:22 AM
To: Rachel Pluckebaum <rpluckebaum@CORRADINO.com>
Subject: Re: [EXTERNAL] ECL Des. No. 2100568, SR 58 over White Lick Creek, Bridge Project, Bartholomew County

Hi Rachel,

Do you have any project maps and diagrams showing construction area, water, etc.?

Thanks
Robin

Robin McWilliams Munson
Fish and Wildlife Biologist
U.S. Fish and Wildlife Service
620 South Walker Street
Bloomington, IN 47403
812-334-4261

Mon-Tues 8-3:30p

Wed-Thurs 8:30-3p Telework

From: Rachel Pluckebaum <rpluckebaum@CORRADINO.com>
Sent: Wednesday, February 8, 2023 9:36 AM
To: McWilliams, Robin <robin_mcwilliams@fws.gov>
Subject: [EXTERNAL] ECL Des. No. 2100568, SR 58 over White Lick Creek, Bridge Project, Bartholomew County

This email has been received from outside of DOI - Use caution before clicking on links, opening attachments, or responding.

Hello,

Attached for your review is the Early Coordination Letter for Des. No. 2100568. The project is located on SR 58, 5.61 miles west of I-65, over White Lick Creek. It is a bridge replacement project in Bartholomew County, Indiana. If you have comments or commitments for this project, please respond within 30 days. Thank you in advance.

Sincerely,

Rachel Pluckebaum Frasier

The Corradino Group

w. 317.488.2363

c. 317.518.9890

THIS IS NOT A PERMIT

State of Indiana
DEPARTMENT OF NATURAL RESOURCES
Division of Fish and Wildlife
Early Coordination/Environmental Assessment

DNR #: ER-25352

Request Received: February 7, 2023

Requestor: The Corradino Group, Inc.
Zed Z Hott
200 South Meridian Street, Suite 330
Indianapolis, IN 46225

Project: SR 58 bridge replacement with new roadway alignment over White Creek, 5.61 miles west of I-65; Des #2100568

County/Site info: Bartholomew

The Indiana Department of Natural Resources has reviewed the above referenced project per your request. Our agency offers the following comments for your information and in accordance with the National Environmental Policy Act of 1969.

If our agency has regulatory jurisdiction over the project, the recommendations contained in this letter may become requirements of any permit issued. If we do not have permitting authority, all recommendations are voluntary.

Regulatory Assessment: This proposal will require the formal approval of our agency for construction in a floodway pursuant to the Flood Control Act (IC 14-28-1), unless it qualifies for a bridge exemption (see enclosure). Please include a copy of this letter with the permit application if the project does not meet the bridge exemption criteria.

Natural Heritage Database: The Natural Heritage Program's data have been checked. To date, no plant or animal species listed as state or federally threatened, endangered, or rare have been reported to occur in the project vicinity.

Fish & Wildlife Comments: Avoid and minimize impacts to fish, wildlife, and botanical resources to the greatest extent possible, and compensate for impacts. The following are recommendations that address potential impacts identified in the proposed project area:

1) Crossing Structure:

Maintaining or improving fish and wildlife passage at existing or proposed crossing locations is a priority for the Division of Fish & Wildlife (DFW) to reduce wildlife mortality along roadways. The DFW has outlined different requirements for different types of crossing structure impacts. For crossing replacements, the new structure must include wildlife passage appropriate for the type of replacement structure being proposed. If the replacement structure is sized to accommodate white-tailed deer passage then it should be included in the design of the new structure. If white-tailed deer passage is not possible with the existing structure, deer passage still needs to be considered in the design and at minimum the bank lines must be restored within structures to allow for smaller wildlife passage above the ordinary high water mark. All wildlife passage designs must include a smooth level pathway a minimum of 1-2 feet in width composed of natural substrate (soil, sand, gravel, etc.) or compacted aggregate fill over riprap (#2, #53, #73, etc.) tied into existing elevations both upstream and downstream. The stream crossing repairs or modifications, and any bank stabilization under or around the structure, must not create conditions that are less favorable for wildlife passage when compared to existing conditions. Upgrading wildlife passage for rehabilitated/modified structures is encouraged whenever possible to improve wildlife/vehicle safety.

There are a number of techniques and materials for incorporating wildlife passage into the design of a crossing structure. Coordination with a Regional Environmental Biologist to address wildlife passage issues before submitting a permit application (if required) is

Attachments: A - Bridge Exemption Criteria

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DEPARTMENT OF NATURAL RESOURCES
Division of Fish and Wildlife
Early Coordination/Environmental Assessment

encouraged to avoid delays in the permitting process. The following links are good resources to consider in the design of stream crossing structures to maintain fish and wildlife passage:

<https://www.fs.usda.gov/ccrc/tool/fishxing-fish-passage-learning-systems>,
<https://www.fs.usda.gov/wildlifecrossings/library/index.php>,
https://www.fhwa.dot.gov/clas/ctip/wildlife_crossing_structures/,
<https://www.fhwa.dot.gov/engineering/hydraulics/pubs/11008/hif11008.pdf>.

For purposes of maintaining fish and wildlife passage through a crossing structure, the Environmental Unit recommends bridges rather than culverts and bottomless culverts rather than box or pipe culverts. Wide culverts are better than narrow culverts, and culverts with shorter through lengths are better than culverts with longer through lengths. If box or pipe culverts are used, the bottoms should be buried a minimum of 6" (or 20% of the culvert height/pipe diameter, whichever is greater up to a maximum of 2') below the stream bed elevation to allow a natural streambed to form within or under the crossing structure. Crossings should: span the entire channel width (a minimum of 1.2 times the OHWM width); maintain the natural stream substrate within the structure; and have stream depth, channel width, and water velocities during low-flow conditions that are approximate to those in the natural stream channel.

2) Bank Stabilization:

Limit the use of riprap on the channel banks, if needed, to toe protection extending up to the ordinary high water mark (OHWM). Do not place riprap in the bed of the channel (unless sumped across the bed to avoid creating a fish passage obstruction) and use alternative erosion protection materials whenever possible. From the OHWM to the top of the banks, heavy duty erosion control blankets or turf reinforcement mats or a similar bioengineering method should be used and these materials should be seeded with native plants to allow a natural, vegetated stream bank to develop.

Information about bioengineering techniques can be found at <http://www.in.gov/legislative/iac/20120404-IR-312120154NRA.xml.pdf>. Also, the following is a USDA/NRCS document that outlines many different bioengineering techniques for streambank stabilization: https://efotg.sc.egov.usda.gov/references/public/IA/Chapter-16_Streambank_and_Shoreline_Protection.pdf.

3) Riparian Habitat:

We recommend a mitigation plan be developed (and submitted with the permit application, if required) for any unavoidable habitat impacts that will occur. The DNR's Habitat Mitigation Guidelines (and plant lists) can be found online at: <https://www.in.gov/nrc/files/IB-17.pdf>.

Impacts to non-wetland forest of one (1) acre or more in a rural or urban area should be mitigated at a minimum 2:1 ratio based on area of impact. Impacts to non-wetland forest under one (1) acre but at least 0.10 acre in a rural or urban area should be mitigated at a minimum 1:1 ratio based on area of impact. Impacts under 0.10 acre in a rural area typically do not require mitigation or additional plantings beyond seeding and stabilizing disturbed areas, though there are exceptions for high quality habitat sites. Impacts under 0.10 acre in an urban area should be mitigated by replacing trees that are 10" diameter-at-breast height (dbh) or greater by planting five trees, 1" to 2" in dbh, for each tree which is removed that is 10" dbh or greater. Seeding and stabilizing disturbed areas is required regardless of the impact amount and location. If floodway impacts to forested wetland and non-wetland habitat areas combine to be 0.10 acres or more, mitigation should be done and coordinated with the biologist, as needed.

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Division of Fish and Wildlife
Early Coordination/Environmental Assessment

The mitigation site should be located in the floodway, downstream of the one (1) square mile drainage area of that stream (or another stream within the 8-digit HUC, preferably as close to the impact site as possible) and adjacent to existing forested riparian habitat.

4) Wetland Habitat:

Due to the presence or potential presence of wetland habitat on site, we recommend contacting and coordinating with the Indiana Department of Environmental Management (IDEM) 401 program and also the US Army Corps of Engineers (USACE) 404 program. Impacts to wetland habitat should be mitigated at the appropriate ratio according to the 1991 INDOT/IDNR/USFWS Memorandum of Understanding.

The additional measures listed below should be implemented to avoid, minimize, or compensate for impacts to fish, wildlife, and botanical resources:

1. Revegetate all bare and disturbed areas with a mixture of native grasses, sedges, wildflowers, and also native hardwood trees and shrubs if any woody plants are disturbed during construction as soon as possible upon completion. Do not use any varieties of Tall Fescue or other non-native plants, including prohibited invasive species (see 312 IAC 18-3-25).
2. Minimize and contain within the project limits inchannel disturbance and the clearing of trees and brush.
3. Do not work in the waterway from April 1 through June 30 without the prior written approval of the Division of Fish and Wildlife.
4. Do not cut any trees suitable for Indiana bat or Northern Long-eared bat roosting (greater than 5 inches dbh, living or dead, with loose hanging bark, or with cracks, crevices, or cavities) from April 1 through September 30.
5. Do not excavate in the low flow area except for the placement of piers, foundations, and riprap, or removal of the old structure.
6. Do not construct any temporary runarounds, access bridges, causeways, cofferdams, diversions, or pumparounds.
7. Use minimum average 6 inch graded riprap stone extended below the normal water level to provide habitat for aquatic organisms in the voids.
8. Do not deposit or allow construction/demolition materials or debris to fall or otherwise enter the waterway. Any incidental fallen material or debris in the waterway must be removed within 24 hours using best management practices, particularly lifting material out of the waterway and not dragging it across the streambed whenever possible.
9. Appropriately designed measures for controlling erosion and sediment must be implemented to prevent sediment from entering the waterbody or leaving the construction site; maintain these measures until construction is complete and all disturbed areas are stabilized.
10. Seed and protect all disturbed streambanks and slopes not protected by other methods that are 3:1 or steeper with erosion control blankets that are heavy-duty, biodegradable, and net free or that use loose-woven / Leno-woven netting to minimize the entrapment and snaring of small-bodied wildlife such as snakes and turtles (follow manufacturer's recommendations for selection and installation); seed and apply mulch on all other disturbed areas.

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State of Indiana
DEPARTMENT OF NATURAL RESOURCES
Division of Fish and Wildlife
Early Coordination/Environmental Assessment

Contact Staff: Christie L. Stanifer, Environ. Coordinator, Fish & Wildlife
Our agency appreciates this opportunity to be of service. Please contact the above staff member at (317) 232-4080 if we can be of further assistance.

Christie L. Stanifer

Date: March 9, 2023

Christie L. Stanifer
Environ. Coordinator
Division of Fish and Wildlife



United States
Department of
Agriculture

Farm
Production
and
Conservation

Natural
Resources
Conservation
Service

Indiana State Office
6013 Lakeside Boulevard
Indianapolis, Indiana 46278
317-295-5800

February 15, 2023

Rachel Pluckebaum
Corradino, LLC
200 South Meridian Street, Suite 330
Indianapolis, Indiana 46225

Dear Ms. Pluckebaum:

The proposed bridge project, SR 58 over White Lick Creek, Bartholomew County, Indiana (Des. No. 2100568), as referred to in your letter received February 8, 2023, will cause a conversion of prime farmland.

The attached packet of information is for your use competing Parts VI and VII of the AD-1006. After completion, the federal funding agency needs to forward one copy to NRCS for our records.

If you need additional information, please contact John Allen at 317-295-5859 or john.allen@usda.gov

Sincerely,

JOHN ALLEN

Digitally signed by JOHN ALLEN
Date: 2023.02.15 09:32:06 -05'00'

JOHN ALLEN
State Soil Scientist

Enclosers

FARMLAND CONVERSION IMPACT RATING

PART I (To be completed by Federal Agency)		Date Of Land Evaluation Request February 8, 2023			
Name of Project DES2100568 SR58 5.61 miles west of I		Federal Agency Involved FHWA			
Proposed Land Use Bridge Project with Road Realignment		County and State Bartholomew County, Indiana			
PART II (To be completed by NRCS)		Date Request Received By NRCS		Person Completing Form: JRA	
Does the site contain Prime, Unique, Statewide or Local Important Farmland? (If no, the FPPA does not apply - do not complete additional parts of this form)		YES <input checked="" type="checkbox"/>	NO <input type="checkbox"/>	Acres Irrigated	Average Farm Size 284 ac
Major Crop(s) Corn	Farmable Land In Govt. Jurisdiction Acres: 224432 % 86	Amount of Farmland As Defined in FPPA Acres: 186938 % 71			
Name of Land Evaluation System Used LESA	Name of State or Local Site Assessment System	Date Land Evaluation Returned by NRCS 2/15/23			
PART III (To be completed by Federal Agency)		Alternative Site Rating			
		Site A	Site B	Site C	Site D
A. Total Acres To Be Converted Directly		7.50			
B. Total Acres To Be Converted Indirectly		0.00			
C. Total Acres In Site		7.50			
PART IV (To be completed by NRCS) Land Evaluation Information					
A. Total Acres Prime And Unique Farmland		7.07			
B. Total Acres Statewide Important or Local Important Farmland		0.00			
C. Percentage Of Farmland In County Or Local Govt. Unit To Be Converted		0.003			
D. Percentage Of Farmland In Govt. Jurisdiction With Same Or Higher Relative Value		60			
PART V (To be completed by NRCS) Land Evaluation Criterion Relative Value of Farmland To Be Converted (Scale of 0 to 100 Points)		71			
PART VI (To be completed by Federal Agency) Site Assessment Criteria (Criteria are explained in 7 CFR 658.5 b. For Corridor project use form NRCS-CPA-106)		Maximum Points	Site A	Site B	Site C
1. Area In Non-urban Use		(15)	15		
2. Perimeter In Non-urban Use		(10)	10		
3. Percent Of Site Being Farmed		(20)	11		
4. Protection Provided By State and Local Government		(20)	0		
5. Distance From Urban Built-up Area		(15)	15		
6. Distance To Urban Support Services		(15)	10		
7. Size Of Present Farm Unit Compared To Average		(10)	4		
8. Creation Of Non-farmable Farmland		(10)	0		
9. Availability Of Farm Support Services		(5)	0		
10. On-Farm Investments		(20)	0		
11. Effects Of Conversion On Farm Support Services		(10)	0		
12. Compatibility With Existing Agricultural Use		(10)	0		
TOTAL SITE ASSESSMENT POINTS		160	65	0	0
PART VII (To be completed by Federal Agency)					
Relative Value Of Farmland (From Part V)		100	71	0	0
Total Site Assessment (From Part VI above or local site assessment)		160	65	0	0
TOTAL POINTS (Total of above 2 lines)		260	136	0	0
Site Selected: Site A		Date Of Selection February 8, 2023		Was A Local Site Assessment Used? YES <input type="checkbox"/> NO <input checked="" type="checkbox"/>	
Reason For Selection: Missing farmland is unavoidable					
Name of Federal agency representative completing this form: Rachel Pluckebaum				Date: 2/8/2023	

(See Instructions on reverse side)



INDIANA GEOLOGICAL & WATER SURVEY

INDIANA UNIVERSITY

Organization and Project Information

Project ID: SR 58 over White Lick Creek, Bridge Project
Des. ID: Des. No. 2100568
Project Title: SR 58, 5.61 miles west of I-65
Name of Organization: Corradino, LLC
Requested by: Rachel Pluckebaum

Environmental Assessment Report

1. Geological Hazards:

- High liquefaction potential
- 1% Annual Chance Flood Hazard

2. Mineral Resources:

- Bedrock Resource: Moderate Potential
- Sand and Gravel Resource: Low Potential

3. Active or abandoned mineral resources extraction sites:

- None documented in the area

*All map layers from Indiana Map (maps.indiana.edu)

DISCLAIMER:

This document was compiled by Indiana University, Indiana Geological Survey, using data believed to be accurate; however, a degree of error is inherent in all data. This product is distributed "AS-IS" without warranties of any kind, either expressed or implied, including but not limited to warranties of suitability to a particular purpose or use. No attempt has been made in either the design or production of these data and document to define the limits or jurisdiction of any federal, state, or local government. The data used to assemble this document are intended for use only at the published scale of the source data or smaller (see the metadata links below) and are for reference purposes only. They are not to be construed as a legal document or survey instrument. A detailed on-the-ground survey and historical analysis of a single site may differ from these data and this document.

This information was furnished by Indiana Geological Survey

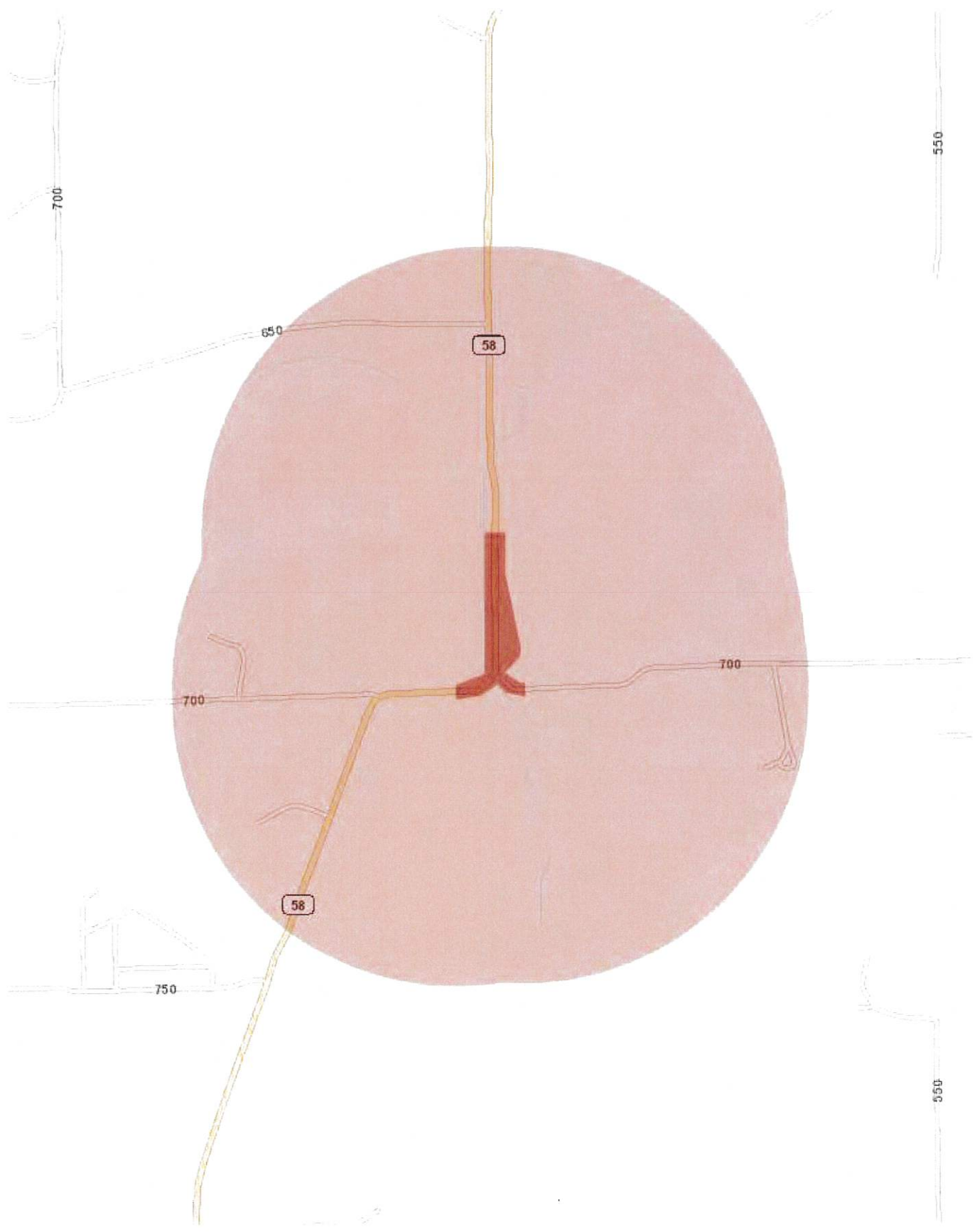
Address: 1001 E. 10th St., Bloomington, IN 47405

Email: IGSEnvir@indiana.edu

Phone: 812 855-7428

Date: February 08, 2023

Appendix C-18



Metadata:

- https://maps.indiana.edu/metadata/Geology/Seismic_Earthquake_Liquefaction_Potential.html
- https://maps.indiana.edu/metadata/Geology/Industrial_Minerals_Sand_Gravel_Resources.html
- https://maps.indiana.edu/metadata/Hydrology/Floodplains_FIRM.html
- https://maps.indiana.edu/metadata/Geology/Bedrock_Geology.html



United States Department of the Interior



FISH AND WILDLIFE SERVICE
Indiana Ecological Services Field Office
620 South Walker Street
Bloomington, IN 47403-2121
Phone: (812) 334-4261 Fax: (812) 334-4273

In Reply Refer To:

November 29, 2023

Project Code: 2023-0043654

Project Name: Des. No. 2100568, SR 58, 5.61 Miles West of I-65, Bartholomew County

Subject: List of threatened and endangered species that may occur in your proposed project location or may be affected by your proposed project

To Whom It May Concern:

The enclosed species list identifies threatened, endangered, proposed and candidate species, as well as proposed and final designated critical habitat, that may occur within the boundary of your proposed project and/or may be affected by your proposed project. The species list fulfills the requirements of the U.S. Fish and Wildlife Service (Service) under section 7(c) of the Endangered Species Act (Act) of 1973, as amended (16 U.S.C. 1531 *et seq.*).

New information based on updated surveys, changes in the abundance and distribution of species, changed habitat conditions, or other factors could change this list. Please feel free to contact us if you need more current information or assistance regarding the potential impacts to federally proposed, listed, and candidate species and federally designated and proposed critical habitat. Please note that under 50 CFR 402.12(e) of the regulations implementing section 7 of the Act, the accuracy of this species list should be verified after 90 days. This verification can be completed formally or informally as desired. The Service recommends that verification be completed by visiting the IPaC website at regular intervals during project planning and implementation for updates to species lists and information. An updated list may be requested through the IPaC system by completing the same process used to receive the enclosed list.

The purpose of the Act is to provide a means whereby threatened and endangered species and the ecosystems upon which they depend may be conserved. Under sections 7(a)(1) and 7(a)(2) of the Act and its implementing regulations (50 CFR 402 *et seq.*), Federal agencies are required to utilize their authorities to carry out programs for the conservation of threatened and endangered species and to determine whether projects may affect threatened and endangered species and/or designated critical habitat.

Please use the species list provided and visit the U.S. Fish and Wildlife Service's Region 3 Section 7 Technical Assistance website at - <http://www.fws.gov/midwest/endangered/section7/s7process/index.html>. This website contains step-by-step instructions which will help you

determine if your project will have an adverse effect on listed species and will help lead you through the Section 7 process. For all **wind energy projects and projects that include installing towers that use guy wires or are over 200 feet in height**, please contact this field office directly for assistance, even if no federally listed plants, animals or critical habitat are present within your proposed project or may be affected by your proposed project.

A Biological Assessment is required for construction projects (or other undertakings having similar physical impacts) that are major Federal actions significantly affecting the quality of the human environment as defined in the National Environmental Policy Act (42 U.S.C. 4332(2)(c)). For projects other than major construction activities, the Service suggests that a biological evaluation similar to a Biological Assessment be prepared to determine whether the project may affect listed or proposed species and/or designated or proposed critical habitat. Recommended contents of a Biological Assessment are described at 50 CFR 402.12.

If a Federal agency determines, based on the Biological Assessment or biological evaluation, that listed species and/or designated critical habitat may be affected by the proposed project, the agency is required to consult with the Service pursuant to 50 CFR 402. In addition, the Service recommends that candidate species, proposed species and proposed critical habitat be addressed within the consultation. More information on the regulations and procedures for section 7 consultation, including the role of permit or license applicants, can be found in the "Endangered Species Consultation Handbook" at:

<https://www.fws.gov/sites/default/files/documents/endangered-species-consultation-handbook.pdf>

Migratory Birds: In addition to responsibilities to protect threatened and endangered species under the Endangered Species Act (ESA), there are additional responsibilities under the Migratory Bird Treaty Act (MBTA) and the Bald and Golden Eagle Protection Act (BGEPA) to protect native birds from project-related impacts. Any activity, intentional or unintentional, resulting in take of migratory birds, including eagles, is prohibited unless otherwise permitted by the U.S. Fish and Wildlife Service (50 C.F.R. Sec. 10.12 and 16 U.S.C. Sec. 668(a)). For more information regarding these Acts, see <https://www.fws.gov/program/migratory-bird-permit/what-we-do>.

The MBTA has no provision for allowing take of migratory birds that may be unintentionally killed or injured by otherwise lawful activities. It is the responsibility of the project proponent to comply with these Acts by identifying potential impacts to migratory birds and eagles within applicable NEPA documents (when there is a federal nexus) or a Bird/Eagle Conservation Plan (when there is no federal nexus). Proponents should implement conservation measures to avoid or minimize the production of project-related stressors or minimize the exposure of birds and their resources to the project-related stressors. For more information on avian stressors and recommended conservation measures, see <https://www.fws.gov/library/collections/threats-birds>.

In addition to MBTA and BGEPA, Executive Order 13186: *Responsibilities of Federal Agencies to Protect Migratory Birds*, obligates all Federal agencies that engage in or authorize activities that might affect migratory birds, to minimize those effects and encourage conservation measures that will improve bird populations. Executive Order 13186 provides for the protection of both migratory birds and migratory bird habitat. For information regarding the implementation of

Executive Order 13186, please visit <https://www.fws.gov/partner/council-conservation-migratory-birds>.

We appreciate your concern for threatened and endangered species. The Service encourages Federal agencies to include conservation of threatened and endangered species into their project planning to further the purposes of the Act. **Please include the Consultation Code in the header of this letter with any request for consultation or correspondence about your project that you submit to our office.**

Attachment(s):

- Official Species List
- Bald & Golden Eagles
- Migratory Birds
- Wetlands

OFFICIAL SPECIES LIST

This list is provided pursuant to Section 7 of the Endangered Species Act, and fulfills the requirement for Federal agencies to "request of the Secretary of the Interior information whether any species which is listed or proposed to be listed may be present in the area of a proposed action".

This species list is provided by:

Indiana Ecological Services Field Office
620 South Walker Street
Bloomington, IN 47403-2121
(812) 334-4261

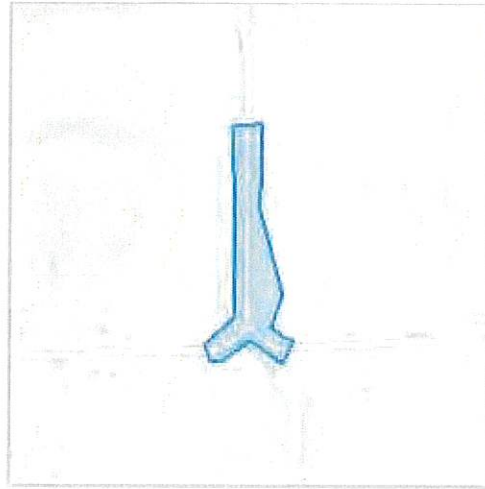
PROJECT SUMMARY

Project Code: 2023-0043654
Project Name: Des. No. 2100568, SR 58, 5.61 Miles West of I-65, Bartholomew County
Project Type: Bridge - Replacement
Project Description: The project is located on SR 58, 5.61 miles west of I-65 in Bartholomew County, IN. The structure (058-03-05882 B) carries SR 58 over White Creek. The existing structure is a 70-foot single span bridge. The structural evaluation rating from the INDOT Bridge Inspection Report is a 5 (fair condition) out of 9 (excellent condition). The existing structure is a 70-foot single span bridge. There is light cracking and spalling in the curbs and there is longitudinal cracking on the wearing surface. There is a 5-foot crack in a beam and efflorescence and minor staining are present between beams throughout. Many of the "I" blocks are separating, and some have washed away. Due to the severity of the deterioration of the structure and roadway geometry, the proposed scope for this project is a bridge replacement with a horizontal alignment to better align the roadway with the stream. The proposed bridge is a 100-foot long three span reinforced concrete slab top structure. The new alignment will shift SR 58 approximately 150-feet to the east, in an agricultural field. The country road southeast of the structure will be regraded to tie into the new roadway centerline. The slopes of the existing SR 58 will be removed as part of this project.

3.5 acre of tree clearing is expected. All tree clearing will be within 100 feet of the roadway. Construction is expected to begin Fall 2025 and be completed in Fall 2026. The INDOT Seymour District responded on February 9, 2023 indicating no presence of federally endangered species within 0.5 mile of the project area. The INDOT bridge inspection, done on January 3, 2022 did not find evidence of bat use. The Corradino bridge/structure bat assessment form dated September 23, 2022 did not find evidence of bat use. No permanent lighting will be installed, and it is unknown whether temporary lighting will be needed, thus temporary lighting will be assumed.

Project Location:

The approximate location of the project can be viewed in Google Maps: <https://www.google.com/maps/@39.0966582,-86.02783817548604,14z>



Counties: Bartholomew County, Indiana

ENDANGERED SPECIES ACT SPECIES

There is a total of 5 threatened, endangered, or candidate species on this species list.

Species on this list should be considered in an effects analysis for your project and could include species that exist in another geographic area. For example, certain fish may appear on the species list because a project could affect downstream species.

IPaC does not display listed species or critical habitats under the sole jurisdiction of NOAA Fisheries¹, as USFWS does not have the authority to speak on behalf of NOAA and the Department of Commerce.

See the "Critical habitats" section below for those critical habitats that lie wholly or partially within your project area under this office's jurisdiction. Please contact the designated FWS office if you have questions.

-
1. [NOAA Fisheries](#), also known as the National Marine Fisheries Service (NMFS), is an office of the National Oceanic and Atmospheric Administration within the Department of Commerce.

MAMMALS

NAME	STATUS
Indiana Bat <i>Myotis sodalis</i> There is final critical habitat for this species. Your location does not overlap the critical habitat. Species profile: https://ecos.fws.gov/ecp/species/5949	Endangered
Northern Long-eared Bat <i>Myotis septentrionalis</i> No critical habitat has been designated for this species. Species profile: https://ecos.fws.gov/ecp/species/9045	Endangered
Tricolored Bat <i>Perimyotis subflavus</i> No critical habitat has been designated for this species. Species profile: https://ecos.fws.gov/ecp/species/10515	Proposed Endangered

BIRDS

NAME	STATUS
Whooping Crane <i>Grus americana</i> Population: U.S.A. (AL, AR, CO, FL, GA, ID, IL, IN, IA, KY, LA, MI, MN, MS, MO, NC, NM, OH, SC, TN, UT, VA, WI, WV, western half of WY) No critical habitat has been designated for this species. Species profile: https://ecos.fws.gov/ecp/species/758	Experimental Population, Non- Essential

INSECTS

NAME	STATUS
Monarch Butterfly <i>Danaus plexippus</i> No critical habitat has been designated for this species. Species profile: https://ecos.fws.gov/ecp/species/9743	Candidate

CRITICAL HABITATS

THERE ARE NO CRITICAL HABITATS WITHIN YOUR PROJECT AREA UNDER THIS OFFICE'S JURISDICTION.

YOU ARE STILL REQUIRED TO DETERMINE IF YOUR PROJECT(S) MAY HAVE EFFECTS ON ALL ABOVE LISTED SPECIES.

BALD & GOLDEN EAGLES

Bald and golden eagles are protected under the Bald and Golden Eagle Protection Act¹ and the Migratory Bird Treaty Act².

Any person or organization who plans or conducts activities that may result in impacts to bald or golden eagles, or their habitats³, should follow appropriate regulations and consider implementing appropriate conservation measures, as described below.

1. The [Bald and Golden Eagle Protection Act](#) of 1940.
2. The [Migratory Birds Treaty Act](#) of 1918.
3. 50 C.F.R. Sec. 10.12 and 16 U.S.C. Sec. 668(a)

There are bald and/or golden eagles in your project area.

For guidance on when to schedule activities or implement avoidance and minimization measures to reduce impacts to migratory birds on your list, click on the PROBABILITY OF PRESENCE SUMMARY at the top of your list to see when these birds are most likely to be present and breeding in your project area.

NAME	BREEDING SEASON
Bald Eagle <i>Haliaeetus leucocephalus</i> This is not a Bird of Conservation Concern (BCC) in this area, but warrants attention because of the Eagle Act or for potential susceptibilities in offshore areas from certain types of development or activities. https://ecos.fws.gov/ecp/species/1626	Breeds Sep 1 to Jul 31

PROBABILITY OF PRESENCE SUMMARY

The graphs below provide our best understanding of when birds of concern are most likely to be present in your project area. This information can be used to tailor and schedule your project activities to avoid or minimize impacts to birds. Please make sure you read the supplemental

information and specifically the FAQ "Proper Interpretation and Use of Your Migratory Bird Report" before using or attempting to interpret this report.

Probability of Presence (■)

Green bars; the bird's relative probability of presence in the 10km grid cell(s) your project overlaps during that week of the year.

Breeding Season (■)

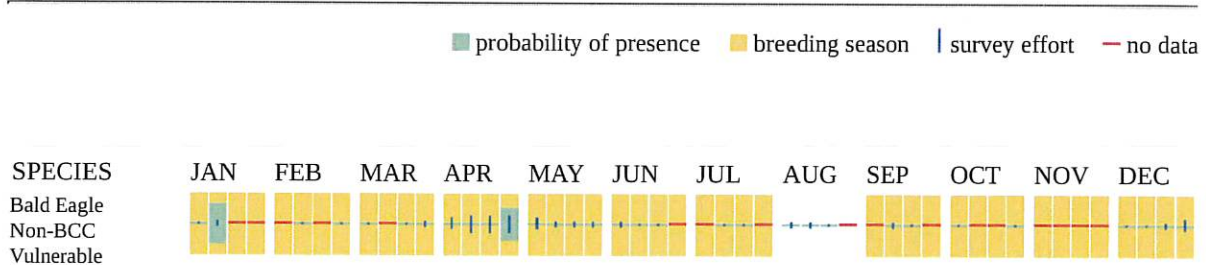
Yellow bars; liberal estimate of the timeframe inside which the bird breeds across its entire range.

Survey Effort (|)

Vertical black lines; the number of surveys performed for that species in the 10km grid cell(s) your project area overlaps.

No Data (-)

A week is marked as having no data if there were no survey events for that week.



Additional information can be found using the following links:

- Eagle Management <https://www.fws.gov/program/eagle-management>
- Measures for avoiding and minimizing impacts to birds <https://www.fws.gov/library/collections/avoiding-and-minimizing-incident-take-migratory-birds>
- Nationwide conservation measures for birds <https://www.fws.gov/sites/default/files/documents/nationwide-standard-conservation-measures.pdf>
- Supplemental Information for Migratory Birds and Eagles in IPaC <https://www.fws.gov/media/supplemental-information-migratory-birds-and-bald-and-golden-eagles-may-occur-project-action>

MIGRATORY BIRDS

Certain birds are protected under the Migratory Bird Treaty Act¹ and the Bald and Golden Eagle Protection Act².

Any person or organization who plans or conducts activities that may result in impacts to migratory birds, eagles, and their habitats³ should follow appropriate regulations and consider implementing appropriate conservation measures, as described below.

1. The [Migratory Birds Treaty Act](#) of 1918.
2. The [Bald and Golden Eagle Protection Act](#) of 1940.
3. 50 C.F.R. Sec. 10.12 and 16 U.S.C. Sec. 668(a)

For guidance on when to schedule activities or implement avoidance and minimization measures to reduce impacts to migratory birds on your list, click on the PROBABILITY OF PRESENCE SUMMARY at the top of your list to see when these birds are most likely to be present and breeding in your project area.

NAME	BREEDING SEASON
Bald Eagle <i>Haliaeetus leucocephalus</i> This is not a Bird of Conservation Concern (BCC) in this area, but warrants attention because of the Eagle Act or for potential susceptibilities in offshore areas from certain types of development or activities. https://ecos.fws.gov/ecp/species/1626	Breeds Sep 1 to Jul 31
Cerulean Warbler <i>Dendroica cerulea</i> This is a Bird of Conservation Concern (BCC) throughout its range in the continental USA and Alaska. https://ecos.fws.gov/ecp/species/2974	Breeds Apr 23 to Jul 20
Chimney Swift <i>Chaetura pelagica</i> This is a Bird of Conservation Concern (BCC) throughout its range in the continental USA and Alaska. https://ecos.fws.gov/ecp/species/9406	Breeds Mar 15 to Aug 25
Field Sparrow <i>Spizella pusilla</i> This is a Bird of Conservation Concern (BCC) only in particular Bird Conservation Regions (BCRs) in the continental USA https://ecos.fws.gov/ecp/species/9446	Breeds Mar 1 to Aug 15
Kentucky Warbler <i>Oporornis formosus</i> This is a Bird of Conservation Concern (BCC) throughout its range in the continental USA and Alaska. https://ecos.fws.gov/ecp/species/9443	Breeds Apr 20 to Aug 20
Red-headed Woodpecker <i>Melanerpes erythrocephalus</i> This is a Bird of Conservation Concern (BCC) throughout its range in the continental USA and Alaska. https://ecos.fws.gov/ecp/species/9398	Breeds May 10 to Sep 10
Wood Thrush <i>Hyllocichla mustelina</i> This is a Bird of Conservation Concern (BCC) throughout its range in the continental USA and Alaska. https://ecos.fws.gov/ecp/species/9431	Breeds May 10 to Aug 31

PROBABILITY OF PRESENCE SUMMARY

The graphs below provide our best understanding of when birds of concern are most likely to be present in your project area. This information can be used to tailor and schedule your project activities to avoid or minimize impacts to birds. Please make sure you read the supplemental information and specifically the FAQ "Proper Interpretation and Use of Your Migratory Bird Report" before using or attempting to interpret this report.

Probability of Presence (■)

Green bars; the bird's relative probability of presence in the 10km grid cell(s) your project overlaps during that week of the year.

Breeding Season (■)

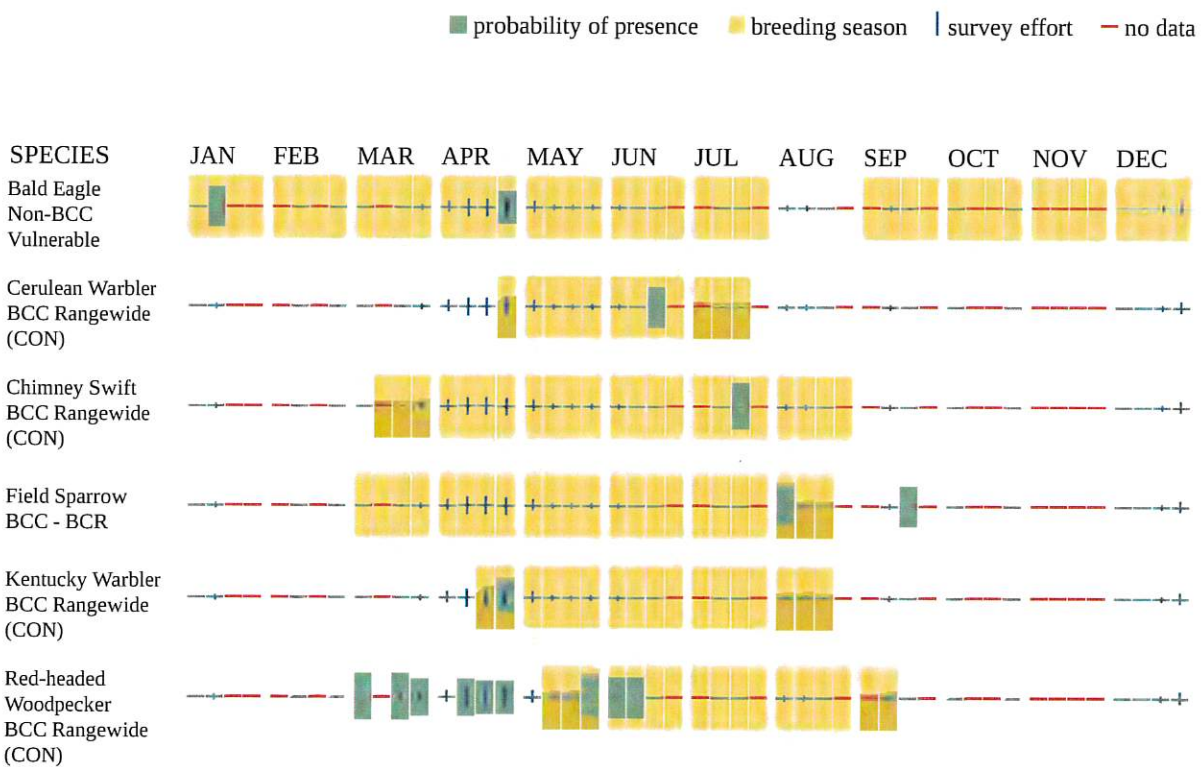
Yellow bars; liberal estimate of the timeframe inside which the bird breeds across its entire range.

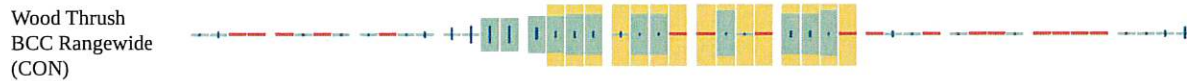
Survey Effort (|)

Vertical black lines; the number of surveys performed for that species in the 10km grid cell(s) your project area overlaps.

No Data (-)

A week is marked as having no data if there were no survey events for that week.





Additional information can be found using the following links:

- Eagle Management <https://www.fws.gov/program/eagle-management>
- Measures for avoiding and minimizing impacts to birds <https://www.fws.gov/library/collections/avoiding-and-minimizing-incident-take-migratory-birds>
- Nationwide conservation measures for birds <https://www.fws.gov/sites/default/files/documents/nationwide-standard-conservation-measures.pdf>
- Supplemental Information for Migratory Birds and Eagles in IPaC <https://www.fws.gov/media/supplemental-information-migratory-birds-and-bald-and-golden-eagles-may-occur-project-action>

WETLANDS

Impacts to [NWI wetlands](#) and other aquatic habitats may be subject to regulation under Section 404 of the Clean Water Act, or other State/Federal statutes.

For more information please contact the Regulatory Program of the local [U.S. Army Corps of Engineers District](#).

Please note that the NWI data being shown may be out of date. We are currently working to update our NWI data set. We recommend you verify these results with a site visit to determine the actual extent of wetlands on site.

FRESHWATER FORESTED/SHRUB WETLAND

- PFO1A

RIVERINE

- R5UBH
- R2UBH

IPAC USER CONTACT INFORMATION

Agency: Corradino LLC
Name: Rachel Pluckebaum
Address: 200 South Meridian Street Suite 330
City: Indianapolis
State: IN
Zip: 46225
Email: rpluckebaum@corradino.com
Phone: 3174882363

LEAD AGENCY CONTACT INFORMATION

Lead Agency: Department of Transportation



United States Department of the Interior

FISH AND WILDLIFE SERVICE
Indiana Ecological Services Field Office
620 South Walker Street
Bloomington, IN 47403-2121
Phone: (812) 334-4261 Fax: (812) 334-4273



In Reply Refer To:

April 17, 2023

Project code: 2023-0043654

Project Name: Des. No. 2100568, SR 58, 5.61 Miles West of I-65, Bartholomew County

Subject: Concurrence verification letter for the 'Des. No. 2100568, SR 58, 5.61 Miles West of I-65, Bartholomew County' project under the amended February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion (dated March 23, 2023) for Transportation Projects within the Range of the Indiana Bat and Northern Long-eared Bat (NLEB).

To whom it may concern:

The U.S. Fish and Wildlife Service (Service) has received your request dated April 17, 2023 to verify that the **Des. No. 2100568, SR 58, 5.61 Miles West of I-65, Bartholomew County** (Proposed Action) may rely on the concurrence provided in the amended February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion (dated March 23, 2023) for Transportation Projects within the Range of the Indiana Bat and Northern Long-eared Bat (PBO) to satisfy requirements under Section 7(a)(2) of the Endangered Species Act of 1973 (ESA) (87 Stat.884, as amended; 16 U.S.C. 1531 *et seq.*).

Based on the information you provided (Project Description shown below), you have determined that the Proposed Action is within the scope and adheres to the criteria of the PBO, including the adoption of applicable avoidance and minimization measures, and may affect, but is not likely to adversely affect (NLAA) the endangered Indiana bat (*Myotis sodalis*) and/or the endangered northern long-eared bat (*Myotis septentrionalis*). Consultation with the Service pursuant to section 7(a)(2) of ESA (87 Stat. 884, as amended; 16 U.S.C. 1531 *et seq.*) is required.

The Service has 14 calendar days to notify the lead Federal action agency or designated non-federal representative if we determine that the Proposed Action does not meet the criteria for a NLAA determination under the PBO. If we do not notify the lead Federal action agency or designated non-federal representative within that timeframe, you may proceed with the Proposed Action under the terms of the NLAA concurrence provided in the PBO. This verification period allows Service Field Offices to apply local knowledge to implementation of the PBO, as we may identify a small subset of actions having impacts that were unanticipated. In such instances,

Service Field Offices may request additional information that is necessary to verify inclusion of the proposed action under the PBO.

For Proposed Actions that include bridge/culvert or structure removal, replacement, and/or maintenance activities: If your initial bridge/culvert or structure assessment documented signs of bat use or occupancy, or an assessment failed to detect Indiana bats and/or NLEBs, yet are later detected prior to, or during construction, please submit the Post Assessment Discovery of Bats at Bridge/Culvert or Structure Form (User Guide Appendix E) to this Service Office within 2 working days of any potential take. In these instances, potential incidental take of Indiana bats and/or NLEBs is covered under the Incidental Take Statement in the 2018 FHWA, FRA, FTA PBO (provided that the take is reported to the Service).

If the Proposed Action is modified, or new information reveals that it may affect the Indiana bat and/or northern long-eared bat in a manner or to an extent not considered in the PBO, further review to conclude the requirements of ESA Section 7(a)(2) may be required.

For Proposed Actions that include bridge/culvert or structure removal, replacement, and/or maintenance activities:

If your initial bridge/culvert or structure assessments failed to detect Indiana bats and/or NLEB use or occupancy, yet bats are later detected prior to, or during construction, please submit the Post Assessment Discovery of Bats at Bridge/Culvert or Structure Form (User Guide Appendix E) to this Service Office within 2 working days of the incident. In these instances, potential incidental take of Indiana bats and/or NLEBs may be exempted provided that the take is reported to the Service.

If the Proposed Action may affect any other federally-listed or proposed species, and/or any designated critical habitat, additional consultation between the lead Federal action agency and this Service Office is required. If the proposed action has the potential to take bald or golden eagles, additional coordination with the Service under the Bald and Golden Eagle Protection Act may also be required. In either of these circumstances, please contact this Service Office.

The following species may occur in your project area and **are not** covered by this determination:

- Monarch Butterfly *Danaus plexippus* Candidate
- Tricolored Bat *Perimyotis subflavus* Proposed Endangered
- Whooping Crane *Grus americana* Experimental Population, Non-Essential

PROJECT DESCRIPTION

The following project name and description was collected in IPaC as part of the endangered species review process.

NAME

Des. No. 2100568, SR 58, 5.61 Miles West of I-65, Bartholomew County

DESCRIPTION

The project is located on SR 58, 5.61 miles west of I-65 in Bartholomew County, IN. The structure (058-03-05882 B) carries SR 58 over White Creek. The existing structure is a 70-foot single span bridge. The structural evaluation rating from the INDOT Bridge Inspection Report is a 5 (fair condition) out of 9 (excellent condition). The existing structure is a 70-foot single span bridge. There is light cracking and spalling in the curbs and there is longitudinal cracking on the wearing surface. There is a 5-foot crack in a beam and efflorescence and minor staining are present between beams throughout. Many of the "I" blocks are separating, and some have washed away. Due to the severity of the deterioration of the structure and roadway geometry, the proposed scope for this project is a bridge replacement with a horizontal alignment to better align the roadway with the stream. The proposed bridge is a 100-foot long three span reinforced concrete slab top structure. The new alignment will shift SR 58 approximately 150-feet to the east, in an agricultural field. The country road southeast of the structure will be regraded to tie into the new roadway centerline. The slopes of the existing SR 58 will be removed as part of this project.

3.5 acre of tree clearing is expected. All tree clearing will be within 100 feet of the roadway. Construction is expected to begin Fall 2025 and be completed in Fall 2026. The INDOT Seymour District responded on February 9, 2023 indicating no presence of federally endangered species within 0.5 mile of the project area. The INDOT bridge inspection, done on January 3, 2022 did not find evidence of bat use. The Corradino bridge/structure bat assessment form dated September 23, 2022 did not find evidence of bat use. No permanent lighting will be installed, and it is unknown whether temporary lighting will be needed, thus temporary lighting will be assumed.

Two other pipes are located within the project area. One 31-foot in length, 12-inch in diameter corrugated metal pipe is located on CR 700 S. This structure will be removed and replaced with a 116-foot in length, 36-inch diameter smooth circular pipe. A second pipe is located on SR 58 and is 44-feet in length, 36-inch diameter corrugated metal pipe, and will be removed when replacing the bridge.

DETERMINATION KEY RESULT

Based on your answers provided, this project(s) may affect, but is not likely to adversely affect the endangered Indiana bat and/or the endangered northern long-eared bat, therefore, consultation with the U.S. Fish and Wildlife Service pursuant to Section 7(a)(2) of the Endangered Species Act of 1973 (ESA) (87 Stat. 884, as amended 16 U.S.C. 1531 *et seq.*) is required. However, also based on your answers provided, this project may rely on the concurrence provided in the amended February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion (dated March 23, 2023) for Transportation Projects within the Range of the Indiana Bat and Northern Long-eared Bat.

QUALIFICATION INTERVIEW

1. Is the project within the range of the Indiana bat^[1]?

[1] See [Indiana bat species profile](#)

Automatically answered

Yes

2. Is the project within the range of the northern long-eared bat^[1]?

[1] See [northern long-eared bat species profile](#)

Automatically answered

Yes

3. Which Federal Agency is the lead for the action?

A) Federal Highway Administration (FHWA)

4. Are *all* project activities limited to non-construction^[1] activities only? (examples of non-construction activities include: bridge/abandoned structure assessments, surveys, planning and technical studies, property inspections, and property sales)

[1] Construction refers to activities involving ground disturbance, percussive noise, and/or lighting.

No

5. Does the project include *any* activities that are **greater than** 300 feet from existing road/rail surfaces^[1]?

[1] Road surface is defined as the actively used [e.g. motorized vehicles] driving surface and shoulders [may be pavement, gravel, etc.] and rail surface is defined as the edge of the actively used rail ballast.

No

6. Does the project include *any* activities **within** 0.5 miles of a known Indiana bat and/or NLEB hibernaculum^[1]?

[1] For the purpose of this consultation, a hibernaculum is a site, most often a cave or mine, where bats hibernate during the winter (see suitable habitat), but could also include bridges and structures if bats are found to be hibernating there during the winter.

No

7. Is the project located **within** a karst area?

No

8. Is there *any* suitable^[1] summer habitat for Indiana Bat or NLEB **within** the project action area^[2]? (includes any trees suitable for maternity, roosting, foraging, or travelling habitat)

[1] See the Service's [summer survey guidance](#) for our current definitions of suitable habitat.

[2] The action area is defined as all areas to be affected directly or indirectly by the Federal action and not merely the immediate area involved in the action (50 CFR Section 402.02). Further clarification is provided by the [User's Guide for the Range-wide Programmatic Consultation for Indiana Bat and Northern Long-eared Bat](#).

Yes

9. Will the project remove *any* suitable summer habitat^[1] and/or remove/trim any existing trees **within** suitable summer habitat?

[1] See the Service's [summer survey guidance](#) for our current definitions of suitable habitat.

Yes

10. Will the project clear more than 20 acres of suitable habitat per 5-mile section of road/rail?

No

11. Have presence/probable absence (P/A) summer surveys^{[1][2]} been conducted^{[3][4]} **within** the suitable habitat located within your project action area?

[1] See the Service's [summer survey guidance](#) for our current definitions of suitable habitat.

[2] Presence/probable absence summer surveys conducted within the fall swarming/spring emergence home range of a documented Indiana bat hibernaculum (contact local Service Field Office for appropriate distance from hibernacula) that result in a negative finding requires additional consultation with the local Service Field Office to determine if clearing of forested habitat is appropriate and/or if seasonal clearing restrictions are needed to avoid and minimize potential adverse effects on fall swarming and spring emerging Indiana bats.

[3] For projects within the range of either the Indiana bat or NLEB in which suitable habitat is present, and no bat surveys have been conducted, the transportation agency will assume presence of the appropriate species. This assumption of presence should be based upon the presence of suitable habitat and the capability of bats to occupy it because of their mobility.

[4] Negative presence/probable absence survey results obtained using the [summer survey guidance](#) are valid for a minimum of two years from the completion of the survey unless new information (e.g., other nearby surveys) suggest otherwise.

No

12. Does the project include activities **within documented Indiana bat habitat**^{[1][2]}?

[1] Documented roosting or foraging habitat – for the purposes of this consultation, we are considering documented habitat as that where Indiana bats and/or NLEB have actually been captured and tracked using (1) radio telemetry to roosts; (2) radio telemetry triangulation/triangulation to estimate foraging areas; or (3) foraging areas with repeated use documented using acoustics. Documented roosting habitat is also considered as suitable summer habitat within 0.25 miles of documented roosts.)

[2] For the purposes of this key, we are considering documented corridors as that where Indiana bats and/or NLEB have actually been captured and tracked to using (1) radio telemetry; or (2) treed corridors located directly between documented roosting and foraging habitat.

No

13. Will the removal or trimming of habitat or trees occur **within** suitable but **undocumented Indiana bat** roosting/foraging habitat or travel corridors?

Yes

14. What time of year will the removal or trimming of habitat or trees **within** suitable but **undocumented Indiana bat** roosting/foraging habitat or travel corridors occur^[1]?

[1] Coordinate with the local Service Field Office for appropriate dates.

B) During the inactive season

15. Does the project include activities **within documented NLEB habitat**^{[1][2]}?

[1] Documented roosting or foraging habitat – for the purposes of this consultation, we are considering documented habitat as that where Indiana bats and/or NLEB have actually been captured and tracked using (1) radio telemetry to roosts; (2) radio telemetry triangulation/triangulation to estimate foraging areas; or (3) foraging areas with repeated use documented using acoustics. Documented roosting habitat is also considered as suitable summer habitat within 0.25 miles of documented roosts.)

[2] For the purposes of this key, we are considering documented corridors as that where Indiana bats and/or NLEB have actually been captured and tracked to using (1) radio telemetry; or (2) treed corridors located directly between documented roosting and foraging habitat.

No

16. Will the removal or trimming of habitat or trees occur **within** suitable but **undocumented NLEB** roosting/foraging habitat or travel corridors?

Yes

17. What time of year will the removal or trimming of habitat or trees **within** suitable but **undocumented NLEB** roosting/foraging habitat or travel corridors occur?

B) During the inactive season

18. Will *any* tree trimming or removal occur **within** 100 feet of existing road/rail surfaces?

Yes

19. Will *any* tree trimming or removal occur **between** 100-300 feet of existing road/rail surfaces?

No

20. Are *all* trees that are being removed clearly demarcated?
Yes
21. Will the removal of habitat or the removal/trimming of trees include installing new or replacing existing **permanent** lighting?
No
22. Does the project include wetland or stream protection activities associated with compensatory wetland mitigation?
No
23. Does the project include slash pile burning?
No
24. Does the project include *any* bridge removal, replacement, and/or maintenance activities (e.g., any bridge repair, retrofit, maintenance, and/or rehabilitation work)?
Yes
25. Is there *any* suitable habitat^[1] for Indiana bat or NLEB **within** 1,000 feet of the bridge? (includes any trees suitable for maternity, roosting, foraging, or travelling habitat)
[1] See the Service's current [summer survey guidance](#) for our current definitions of suitable habitat.
Yes
26. Has a bridge assessment^[1] been conducted **within** the last 24 months^[2] to determine if the bridge is being used by bats?
[1] See [User Guide Appendix D](#) for bridge/structure assessment guidance
[2] Assessments must be completed no more than 2 years prior to conducting any work below the deck surface on all bridges that meet the physical characteristics described in the Programmatic Consultation, regardless of whether assessments have been conducted in the past. Due to the transitory nature of bat use, a negative result in one year does not guarantee that bats will not use that bridge/structure in subsequent years.
Yes

SUBMITTED DOCUMENTS

- *Bridge Inspection Report 010322.pdf* <https://ipac.ecosphere.fws.gov/project/E6B3V6TIWNDMNFKE5HDPD7TVBY/projectDocuments/122234186>
- *2100568 Bat Inspection.pdf* <https://ipac.ecosphere.fws.gov/project/E6B3V6TIWNDMNFKE5HDPD7TVBY/projectDocuments/125097595>

27. Did the bridge assessment detect *any* signs of Indiana bats and/or NLEBs roosting in/under the bridge (bats, guano, etc.)^[1]?

[1] If bridge assessment detects signs of *any* species of bats, coordination with the local FWS office is needed to identify potential threatened or endangered bat species. Additional studies may be undertaken to try to identify which bat species may be utilizing the bridge prior to allowing *any* work to proceed.

Note: There is a small chance bridge assessments for bat occupancy do not detect bats. Should a small number of bats be observed roosting on a bridge just prior to or during construction, such that take is likely to occur or does occur in the form of harassment, injury or death, the PBO requires the action agency to report the take. Report all unanticipated take within 2 working days of the incident to the USFWS. Construction activities may continue without delay provided the take is reported to the USFWS and is limited to 5 bats per project.

No

28. Will the bridge removal, replacement, and/or maintenance activities include installing new or replacing existing **permanent** lighting?

No

29. Does the project include the removal, replacement, and/or maintenance of *any* structure other than a bridge? (e.g., rest areas, offices, sheds, outbuildings, barns, parking garages, etc.)

No

30. Will the project involve the use of **temporary** lighting *during* the active season?

Yes

31. Is there *any* suitable habitat **within** 1,000 feet of the location(s) where **temporary** lighting will be used?

Yes

32. Will the project install new or replace existing **permanent** lighting?

No

33. Does the project include percussives or other activities (**not including tree removal/trimming or bridge/structure work**) that will increase noise levels above existing traffic/background levels?

No

34. Are *all* project activities that are **not associated with** habitat removal, tree removal/trimming, bridge and/or structure activities, temporary or permanent lighting, or use of percussives, limited to actions that DO NOT cause any additional stressors to the bat species?

Examples: lining roadways, unlighted signage, rail road crossing signals, signal lighting, and minor road repair such as asphalt fill of potholes, etc.

Yes

35. Will the project raise the road profile **above the tree canopy**?

No

36. Are the project activities that are not associated with habitat removal, tree removal/trimming, bridge and/or structure activities, temporary or permanent lighting, or use of percussives consistent with a No Effect determination in this key?

Automatically answered

Yes, other project activities are limited to actions that DO NOT cause any additional stressors to the bat species as described in the BA/BO

37. Is the habitat removal portion of this project consistent with a Not Likely to Adversely Affect determination in this key?

Automatically answered

Yes, because the tree removal/trimming that occurs outside of the Indiana bat's active season occurs greater than 0.5 miles from the nearest hibernaculum, is less than 100 feet from the existing road/rail surface, includes clear demarcation of the trees that are to be removed, and does not alter documented roosts and/or surrounding summer habitat within 0.25 miles of a documented roost.

38. Is the habitat removal portion of this project consistent with a Not Likely to Adversely Affect determination in this key?

Automatically answered

Yes, because the tree removal/trimming that occurs outside of the NLEB's active season occurs greater than 0.5 miles from the nearest hibernaculum, is less than 100 feet from the existing road/rail surface, includes clear demarcation of the trees that are to be removed, and does not alter documented roosts and/or surrounding summer habitat within 0.25 miles of a documented roost.

39. Is the bridge removal, replacement, or maintenance activities portion of this project consistent with a No Effect determination in this key?

Automatically answered

Yes, because the bridge has been assessed using the criteria documented in the BA and no signs of bats were detected

40. **General AMM 1**

Will the project ensure *all* operators, employees, and contractors working in areas of known or presumed bat habitat are aware of *all* FHWA/FRA/FTA (Transportation Agencies) environmental commitments, including all applicable Avoidance and Minimization Measures?

Yes

41. Tree Removal AMM 1

Can *all* phases/aspects of the project (e.g., temporary work areas, alignments) be modified, to the extent practicable, to avoid tree removal^[1] in excess of what is required to implement the project safely?

Note: Tree Removal AMM 1 is a minimization measure, the full implementation of which may not always be practicable. Projects may still be NLAA as long as Tree Removal AMMs 2, 3, and 4 are implemented and LAA as long as Tree Removal AMMs 3, 5, 6, and 7 are implemented.

[1] The word "trees" as used in the AMMs refers to trees that are suitable habitat for each species within their range. See the USFWS' current summer survey guidance for our latest definitions of suitable habitat.

Yes

42. Tree Removal AMM 3

Can tree removal be limited to that specified in project plans and ensure that contractors understand clearing limits and how they are marked in the field (e.g., install bright colored flagging/fencing prior to any tree clearing to ensure contractors stay within clearing limits)?

Yes

43. Tree Removal AMM 4

Can the project avoid cutting down/removal of *all* (1) **documented**^[1] Indiana bat or NLEB roosts^[2] (that are still suitable for roosting), (2) trees **within** 0.25 miles of roosts, and (3) documented foraging habitat any time of year?

[1] The word documented means habitat where bats have actually been captured and/or tracked.

[2] Documented roosting or foraging habitat – for the purposes of this consultation, we are considering documented habitat as that where Indiana bats and/or NLEB have actually been captured and tracked using (1) radio telemetry to roosts; (2) radio telemetry triangulation/triangulation to estimate foraging areas; or (3) foraging areas with repeated use documented using acoustics. Documented roosting habitat is also considered as suitable summer habitat within 0.25 miles of documented roosts.)

Yes

44. Lighting AMM 1

Will *all* **temporary** lighting be directed away from suitable habitat during the active season?

Yes

PROJECT QUESTIONNAIRE

1. Have you made a No Effect determination for *all* other species indicated on the FWS IPaC generated species list?

N/A

2. Have you made a May Affect determination for *any* other species on the FWS IPaC generated species list?

N/A

3. How many acres^[1] of trees are proposed for removal between 0-100 feet of the existing road/rail surface?

[1] If described as number of trees, multiply by 0.09 to convert to acreage and enter that number.

3.5

4. Please describe the proposed bridge work:

Due to the severity of the deterioration of the structure and roadway geometry, the proposed scope for this project is a bridge replacement with a horizontal alignment to better align the roadway with the stream. The proposed bridge is a 100-foot long three span reinforced concrete slab top structure. The new alignment will shift SR 58 approximately 150-feet to the east, in an agricultural field. The country road southeast of the structure will be regraded to tie into the new roadway centerline. The slopes of the existing SR 48 will be removed as part of this project.

5. Please state the timing of all proposed bridge work:

Fall 2025 - Fall 2026

6. Please enter the date of the bridge assessment:

January 3, 2022

AVOIDANCE AND MINIMIZATION MEASURES (AMMS)

This determination key result includes the commitment to implement the following Avoidance and Minimization Measures (AMMs):

TREE REMOVAL AMM 3

Ensure tree removal is limited to that specified in project plans and ensure that contractors understand clearing limits and how they are marked in the field (e.g., install bright colored flagging/fencing prior to any tree clearing to ensure contractors stay within clearing limits).

TREE REMOVAL AMM 4

Do not remove **documented** Indiana bat or NLEB roosts that are still suitable for roosting, or trees within 0.25 miles of roosts, or **documented** foraging habitat any time of year.

GENERAL AMM 1

Ensure all operators, employees, and contractors working in areas of known or presumed bat habitat are aware of all FHWA/FRA/FTA (Transportation Agencies) environmental commitments, including all applicable AMMs.

TREE REMOVAL AMM 1

Modify all phases/aspects of the project (e.g., temporary work areas, alignments) to avoid tree removal.

LIGHTING AMM 1

Direct temporary lighting away from suitable habitat during the active season.

TREE REMOVAL AMM 2

Apply time of year restrictions for tree removal when bats are not likely to be present, or limit tree removal to 10 or fewer trees per project at any time of year within 100 feet of existing road/rail surface and **outside of documented** roosting/foraging habitat or travel corridors; visual emergence survey must be conducted with no bats observed.

DETERMINATION KEY DESCRIPTION: FHWA, FRA, FTA PROGRAMMATIC CONSULTATION FOR TRANSPORTATION PROJECTS AFFECTING NLEB OR INDIANA BAT

This key was last updated in IPaC on April 03, 2023. Keys are subject to periodic revision.

This decision key is intended for projects/activities funded or authorized by the Federal Highway Administration (FHWA), Federal Railroad Administration (FRA), and/or Federal Transit Administration (FTA), which may require consultation with the U.S. Fish and Wildlife Service (Service) under Section 7 of the Endangered Species Act (ESA) for the endangered **Indiana bat** (*Myotis sodalis*) and the endangered **northern long-eared bat** (NLEB) (*Myotis septentrionalis*).

This decision key should only be used to verify project applicability with the Service's [February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion for Transportation Projects](#). The programmatic biological opinion covers limited transportation activities that may affect either bat species, and addresses situations that are both likely and not likely to adversely affect either bat species. This decision key will assist in identifying the effect of a specific project/activity and applicability of the programmatic consultation. The programmatic biological opinion is not intended to cover all types of transportation actions. Activities outside the scope of the programmatic biological opinion, or that may affect ESA-listed species other than the Indiana bat or NLEB, or any designated critical habitat, may require additional ESA Section 7 consultation.

IPAC USER CONTACT INFORMATION

Agency: Indiana Department of Transportation

Name: Erin Carleton

Address: 185 Agrico Ln

City: Seymour

State: IN

Zip: 47274

Email: ecarleton@indot.in.gov

Phone: 8125243988

LEAD AGENCY CONTACT INFORMATION

Lead Agency: Department of Transportation

Bridge/Structure Bat Assessment Form

Date & Time of Assessment: 9/23/22; 11:00 am	DOT Project Number: 2100568	Route/Facility Carried: SR 58	County: Bartholomew
Federal Structure ID: 058-03-05882 B	Structure Coordinates (latitude and longitude): 39.095065; -86.028053	Structure Height (approximate): 12 feet	Structure Length: 67 feet
Structure Type (check one)		Structure Material (check all that apply)	
Bridge Construction Style		Deck Material	Beam Material
<input type="radio"/> Cast-in-place	<input type="radio"/> Pre-stressed Girder	<input type="checkbox"/> Metal	<input type="checkbox"/> None
<input checked="" type="radio"/> Flat Slab/Box	<input type="radio"/> Steel I-beam	<input checked="" type="checkbox"/> Concrete	<input checked="" type="checkbox"/> Concrete
<input type="radio"/> Truss	<input type="radio"/> Covered	<input type="checkbox"/> Timber	<input type="checkbox"/> Steel
<input type="radio"/> Parallel Box Beam	<input type="radio"/> Other:	<input type="checkbox"/> Open grid	<input type="checkbox"/> Timber
		<input type="checkbox"/> Other:	<input type="checkbox"/> Other:
Culvert Type	Other Structure	Culvert Material	
<input checked="" type="radio"/> Box		<input type="checkbox"/> Metal	<input checked="" type="checkbox"/> Concrete
<input type="radio"/> Pipe/Round		<input type="checkbox"/> Concrete	<input type="checkbox"/> Plastic
<input type="radio"/> Other:		<input type="checkbox"/> Stone/Masonry	<input type="checkbox"/> Other:
		<input type="checkbox"/> Other:	<input type="checkbox"/> Other:
Crossings Traversed (check all that apply)		Surrounding Habitat (check all that apply)	
<input checked="" type="checkbox"/> Bare ground	<input type="checkbox"/> Open vegetation	<input checked="" type="checkbox"/> Agricultural	<input type="checkbox"/> Grassland
<input type="checkbox"/> Rip-rap	<input type="checkbox"/> Closed vegetation	<input type="checkbox"/> Commercial	<input type="checkbox"/> Ranching
<input type="checkbox"/> Flowing water	<input type="checkbox"/> Railroad	<input type="checkbox"/> Residential-urban	<input checked="" type="checkbox"/> Riparian/wetland
<input checked="" type="checkbox"/> Standing water	<input type="checkbox"/> Road/trail - Type: <i>Inhabited</i>	<input type="checkbox"/> Residential-rural	<input type="checkbox"/> Mixed use
<input type="checkbox"/> Seasonal water	<input type="checkbox"/> Other:	<input checked="" type="checkbox"/> Woodland/forested	<input type="checkbox"/> Other:
Areas Assessed (check all that apply)			
Check all areas that apply. If an area is not present in the structure, check the "not present" box.			
Document all bat indicators observed during the assessment. Include the species present, if known, and provide photo documentation as indicated.			
Area (check if assessed)	Assessment Notes	Evidence of Bats (include photos if present)	
<input type="checkbox"/> All crevices and cracks:	<input type="checkbox"/> Not present	<input type="checkbox"/> Visual - live #	<input type="checkbox"/> dead #
<input checked="" type="checkbox"/> Bridges/culverts: rough surfaces or imperfections in concrete	some rough surfaces	<input type="checkbox"/> Audible	<input type="checkbox"/> Species
<input type="checkbox"/> Other structures: soffits, rafters, attic areas		<input type="checkbox"/> Odor	
		<input type="checkbox"/> Guano	<input type="checkbox"/> Photos
		<input type="checkbox"/> Staining	
<input checked="" type="checkbox"/> Concrete surfaces (open roosting on concrete)	<input type="checkbox"/> Not present	<input type="checkbox"/> Visual - live #	<input type="checkbox"/> dead #
		<input type="checkbox"/> Audible	<input type="checkbox"/> Species
		<input type="checkbox"/> Odor	
		<input type="checkbox"/> Guano	<input type="checkbox"/> Photos
		<input type="checkbox"/> Staining	
<input checked="" type="checkbox"/> Spaces between concrete end walls and the bridge deck	<input type="checkbox"/> Not present	<input type="checkbox"/> Visual - live #	<input type="checkbox"/> dead #
		<input type="checkbox"/> Audible	<input type="checkbox"/> Species
		<input type="checkbox"/> Odor	
		<input type="checkbox"/> Guano	<input type="checkbox"/> Photos
		<input type="checkbox"/> Staining	
<input checked="" type="checkbox"/> Crack between concrete railings on top of the bridge deck	<input type="checkbox"/> Not present	<input type="checkbox"/> Visual - live #	<input type="checkbox"/> dead #
		<input type="checkbox"/> Audible	<input type="checkbox"/> Species
		<input type="checkbox"/> Odor	
		<input type="checkbox"/> Guano	<input type="checkbox"/> Photos
		<input type="checkbox"/> Staining	
<input type="checkbox"/> Vertical surfaces on concrete I-beams	<input type="checkbox"/> Not present	<input type="checkbox"/> Visual - live #	<input type="checkbox"/> dead #
		<input type="checkbox"/> Audible	<input type="checkbox"/> Species
		<input type="checkbox"/> Odor	
		<input type="checkbox"/> Guano	<input type="checkbox"/> Photos
		<input type="checkbox"/> Staining	
<input checked="" type="checkbox"/> Spaces between walls, ceiling joists	<input type="checkbox"/> Not present	<input type="checkbox"/> Visual - live #	<input type="checkbox"/> dead #
		<input type="checkbox"/> Audible	<input type="checkbox"/> Species
		<input type="checkbox"/> Odor	
		<input type="checkbox"/> Guano	<input type="checkbox"/> Photos
		<input type="checkbox"/> Staining	
<input checked="" type="checkbox"/> Weep holes, scupper drains, and inlets/pipes	<input type="checkbox"/> Not present	<input type="checkbox"/> Visual - live #	<input type="checkbox"/> dead #
		<input type="checkbox"/> Audible	<input type="checkbox"/> Species
		<input type="checkbox"/> Odor	
		<input type="checkbox"/> Guano	<input type="checkbox"/> Photos
		<input type="checkbox"/> Staining	
<input checked="" type="checkbox"/> All gulleralls	<input type="checkbox"/> Not present	<input type="checkbox"/> Visual - live #	<input type="checkbox"/> dead #
		<input type="checkbox"/> Audible	<input type="checkbox"/> Species
		<input type="checkbox"/> Odor	
		<input type="checkbox"/> Guano	<input type="checkbox"/> Photos
		<input type="checkbox"/> Staining	
<input checked="" type="checkbox"/> All expansion joints	<input type="checkbox"/> Not present	<input type="checkbox"/> Visual - live #	<input type="checkbox"/> dead #
		<input type="checkbox"/> Audible	<input type="checkbox"/> Species
		<input type="checkbox"/> Odor	
		<input type="checkbox"/> Guano	<input type="checkbox"/> Photos
		<input type="checkbox"/> Staining	
Name: Kirk Roth		Signature:	

Bridge/Structure Bat Assessment Form

Date & Time of Assessment 9/23/22; 9:20 am	DOT Project Number 2100568	Route/Facility Carried SR 58	County Bartholomew
Federal Structure ID N/A	Structure Coordinates (latitude and longitude) 39.095498; -88.027953	Structure Height (approximate) 3 feet	Structure Length 44 feet
Structure Type (check one)		Structure Material (check all that apply)	
Bridge Construction Style		Deck Material	Beam Material
<input type="radio"/> Cast-in-place	<input type="radio"/> Pre-stressed Girder	<input type="checkbox"/> Metal	<input type="checkbox"/> None
<input type="radio"/> Flat Slab/Box	<input type="radio"/> Steel I-beam	<input type="checkbox"/> Concrete	<input type="checkbox"/> Concrete
<input type="radio"/> Truss	<input type="radio"/> Covered	<input type="checkbox"/> Timber	<input type="checkbox"/> Steel
<input type="radio"/> Parallel Box Beam	<input type="radio"/> Other:	<input type="checkbox"/> Open grid	<input type="checkbox"/> Timber
		<input type="checkbox"/> Other:	<input type="checkbox"/> Other:
Culvert Type	Other Structure	Culvert Material	
<input type="radio"/> Box	<input type="radio"/>	<input checked="" type="checkbox"/> Metal	Creosote Evidence <input type="checkbox"/> Yes <input checked="" type="checkbox"/> No <input type="checkbox"/> Unknown
<input type="radio"/> Pipe/Round	<input type="radio"/>	<input type="checkbox"/> Concrete	
<input type="radio"/> Other:	<input type="radio"/>	<input type="checkbox"/> Plastic	
		<input type="checkbox"/> Stone/Masonry	
		<input type="checkbox"/> Other:	Notes:
Crossings Traversed (check all that apply)		Surrounding Habitat (check all that apply)	
<input type="checkbox"/> Bare ground	<input type="checkbox"/> Open vegetation	<input type="checkbox"/> Agricultural	<input type="checkbox"/> Grassland
<input type="checkbox"/> Rip-rap	<input type="checkbox"/> Closed vegetation	<input type="checkbox"/> Commercial	<input type="checkbox"/> Ranching
<input type="checkbox"/> Flowing water	<input type="checkbox"/> Railroad	<input type="checkbox"/> Residential-urban	<input type="checkbox"/> Riparian/wetland
<input type="checkbox"/> Standing water	<input type="checkbox"/> Road/trail - Type:	<input type="checkbox"/> Residential-rural	<input type="checkbox"/> Mixed use
<input type="checkbox"/> Seasonal water	<input type="checkbox"/> Other:	<input type="checkbox"/> Woodland/forested	<input type="checkbox"/> Other:
Areas Assessed (check all that apply)			
Check all areas that apply. If an area is not present in the structure, check the "not present" box.			
Document all bat indicators observed during the assessment. Include the species present, if known, and provide photo documentation as indicated.			
Area (check if assessed)	Assessment Notes	Evidence of Bats (include photos if present)	
<input type="checkbox"/> All crevices and cracks: Bridges/culverts: rough surfaces or imperfections in concrete Other structures: soffits, rafters, attic areas	<input checked="" type="checkbox"/> Not present Entirely corrugated metal - no deterioration.	Visual - live #	Species
		Visual - dead #	Audible
		Guano	Odor
		Staining	Photos
<input type="checkbox"/> Concrete surfaces (open roosting on concrete)	<input checked="" type="checkbox"/> Not present	Visual - live #	Species
		Visual - dead #	Audible
		Guano	Odor
		Staining	Photos
<input type="checkbox"/> Spaces between concrete end walls and the bridge deck	<input checked="" type="checkbox"/> Not present	Visual - live #	Species
		Visual - dead #	Audible
		Guano	Odor
		Staining	Photos
<input type="checkbox"/> Crack between concrete railings on top of the bridge deck 	<input checked="" type="checkbox"/> Not present	Visual - live #	Species
		Visual - dead #	Audible
		Guano	Odor
		Staining	Photos
<input type="checkbox"/> Vertical surfaces on concrete I-beams	<input checked="" type="checkbox"/> Not present	Visual - live #	Species
		Visual - dead #	Audible
		Guano	Odor
		Staining	Photos
<input type="checkbox"/> Spaces between walls, ceiling joists	<input checked="" type="checkbox"/> Not present	Visual - live #	Species
		Visual - dead #	Audible
		Guano	Odor
		Staining	Photos
<input type="checkbox"/> Weep holes, scupper drains, and inlets/pipes	<input checked="" type="checkbox"/> Not present	Visual - live #	Species
		Visual - dead #	Audible
		Guano	Odor
		Staining	Photos
<input type="checkbox"/> All gulleralls	<input checked="" type="checkbox"/> Not present	Visual - live #	Species
		Visual - dead #	Audible
		Guano	Odor
		Staining	Photos
<input type="checkbox"/> All expansion joints	<input checked="" type="checkbox"/> Not present	Visual - live #	Species
		Visual - dead #	Audible
		Guano	Odor
		Staining	Photos
Name: Kirk Roth		Signature:	

Bridge/Structure Bat Assessment Form

Date & Time of Assessment: 9/23/22; 9:05 am	DOT Project Number: 2100568	Route/Facility Carried: SR 58	County: Bartholomew
Federal Structure ID: N/A	Structure Coordinates (latitude and longitude): 39.0951; -88.0274	Structure Height (approximate): 1 foot	Structure Length: 31 feet

Structure Type (check one)		Structure Material (check all that apply)		
<i>Bridge Construction Style</i>		<i>Deck Material</i>	<i>Beam Material</i>	<i>End/Back Wall Material</i>
<input type="radio"/> Cast-in-place	<input type="radio"/> Pre-stressed Girder	<input type="checkbox"/> Metal	<input type="checkbox"/> None	<input type="checkbox"/> Concrete
<input type="radio"/> Flat Slab/Box	<input type="radio"/> Steel I-beam	<input type="checkbox"/> Concrete	<input type="checkbox"/> Concrete	<input type="checkbox"/> Timber
<input type="radio"/> Truss	<input type="radio"/> Covered	<input type="checkbox"/> Timber	<input type="checkbox"/> Steel	<input type="checkbox"/> Stone/Masonry
<input type="radio"/> Parallel Box Beam	<input type="radio"/> Other: _____	<input type="checkbox"/> Open grid	<input type="checkbox"/> Timber	<input type="checkbox"/> Other: _____
<i>Culvert Type</i>		<i>Culvert Material</i>		<i>Creosote Evidence</i>
<input type="radio"/> Box	<input type="radio"/> Other Structure	<input checked="" type="checkbox"/> Metal	<input type="checkbox"/> Concrete	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No
<input checked="" type="radio"/> Pipe/Round		<input type="checkbox"/> Plastic	<input type="checkbox"/> Stone/Masonry	<input checked="" type="checkbox"/> Unknown
<input type="radio"/> Other: _____		<input type="checkbox"/> Other: _____		<i>Notes:</i>

Crossings Traversed (check all that apply)		Surrounding Habitat (check all that apply)	
<input type="checkbox"/> Bare ground	<input type="checkbox"/> Open vegetation	<input type="checkbox"/> Agricultural	<input type="checkbox"/> Grassland
<input type="checkbox"/> Rip-rap	<input type="checkbox"/> Closed vegetation	<input type="checkbox"/> Commercial	<input type="checkbox"/> Ranching
<input type="checkbox"/> Flowing water	<input type="checkbox"/> Railroad	<input type="checkbox"/> Residential-urban	<input type="checkbox"/> Riparian/wetland
<input type="checkbox"/> Standing water	<input type="checkbox"/> Road/trail - Type: _____	<input type="checkbox"/> Residential-rural	<input type="checkbox"/> Mixed use
<input type="checkbox"/> Seasonal water	<input type="checkbox"/> Other: _____	<input type="checkbox"/> Woodland/forested	<input type="checkbox"/> Other: _____

Areas Assessed (check all that apply)
 Check all areas that apply. If an area is not present in the structure, check the "not present" box.
 Document all bat indicators observed during the assessment. Include the species present, if known, and provide photo documentation as indicated.

Area (check if assessed)	Assessment Notes	Evidence of Bats (include photos if present)		
<input type="checkbox"/> All crevices and cracks: Bridges/culverts: rough surfaces or imperfections in concrete Other structures: soffits, rafters, attic areas	<input checked="" type="checkbox"/> Not present Entirely corrugated metal - no deterioration.	<input type="checkbox"/> Visual - live #	<input type="checkbox"/> dead #	<input type="checkbox"/> Audible <input type="checkbox"/> Odor <input type="checkbox"/> Photos
<input type="checkbox"/> Concrete surfaces (open roosting on concrete)	<input checked="" type="checkbox"/> Not present	<input type="checkbox"/> Visual - live #	<input type="checkbox"/> dead #	<input type="checkbox"/> Audible <input type="checkbox"/> Odor <input type="checkbox"/> Photos
<input type="checkbox"/> Spaces between concrete end walls and the bridge deck	<input checked="" type="checkbox"/> Not present	<input type="checkbox"/> Visual - live #	<input type="checkbox"/> dead #	<input type="checkbox"/> Audible <input type="checkbox"/> Odor <input type="checkbox"/> Photos
<input type="checkbox"/> Crack between concrete railings on top of the bridge deck 	<input checked="" type="checkbox"/> Not present	<input type="checkbox"/> Visual - live #	<input type="checkbox"/> dead #	<input type="checkbox"/> Audible <input type="checkbox"/> Odor <input type="checkbox"/> Photos
<input type="checkbox"/> Vertical surfaces on concrete I-beams	<input checked="" type="checkbox"/> Not present	<input type="checkbox"/> Visual - live #	<input type="checkbox"/> dead #	<input type="checkbox"/> Audible <input type="checkbox"/> Odor <input type="checkbox"/> Photos
<input type="checkbox"/> Spaces between walls, ceiling joists	<input checked="" type="checkbox"/> Not present	<input type="checkbox"/> Visual - live #	<input type="checkbox"/> dead #	<input type="checkbox"/> Audible <input type="checkbox"/> Odor <input type="checkbox"/> Photos
<input type="checkbox"/> Weep holes, scupper drains, and inlets/pipes	<input checked="" type="checkbox"/> Not present	<input type="checkbox"/> Visual - live #	<input type="checkbox"/> dead #	<input type="checkbox"/> Audible <input type="checkbox"/> Odor <input type="checkbox"/> Photos
<input type="checkbox"/> All guiderails	<input checked="" type="checkbox"/> Not present	<input type="checkbox"/> Visual - live #	<input type="checkbox"/> dead #	<input type="checkbox"/> Audible <input type="checkbox"/> Odor <input type="checkbox"/> Photos
<input type="checkbox"/> All expansion joints	<input checked="" type="checkbox"/> Not present	<input type="checkbox"/> Visual - live #	<input type="checkbox"/> dead #	<input type="checkbox"/> Audible <input type="checkbox"/> Odor <input type="checkbox"/> Photos

Name: Kirk Roth	Signature:
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Rachel Pluckebaum

From: Brett Boezeman <boezemanb@bcsc.k12.in.us>
Sent: Tuesday, February 20, 2024 3:41 PM
To: Rachel Pluckebaum
Subject: Re: ECL Des. No. 2100568, SR 58 over White Lick Creek, Bridge Project, Bartholomew County

No environmental concerns, our only concerns were due to timing, which have been addressed.

Thank you.

On Tue, Feb 20, 2024 at 3:32 PM Rachel Pluckebaum <rpluckebaum@corradino.com> wrote:

Hi Brett,

This is the environmental coordination. We are reaching out to find out if you have any environmental concerns. There will be no other coordination. Do you have any environmental concerns that the district would like to express?

Thank you,

Rachel

From: Brett Boezeman <boezemanb@bcsc.k12.in.us>
Sent: Monday, February 12, 2024 1:47 PM
To: Rachel Pluckebaum <rpluckebaum@CORRADINO.com>
Cc: Zed Hott <zhott@CORRADINO.com>
Subject: Re: ECL Des. No. 2100568, SR 58 over White Lick Creek, Bridge Project, Bartholomew County

Thanks. Will another early coordination letter be sent in 2026, or will this serve as that?