

APPENDIX D: SECTION 106 OF NHPA

**FEDERAL HIGHWAY ADMINISTRATION'S
SECTION 4(F) COMPLIANCE REQUIREMENTS (for historic properties) AND
SECTION 106 FINDINGS AND DETERMINATIONS
AREA OF POTENTIAL EFFECT
ELIGIBILITY DETERMINATIONS
EFFECT FINDING**

**US 41 INTERSECTION IMPROVEMENT PROJECT
EVANSVILLE, VANDERBURGH COUNTY, INDIANA
DES. NO.: 2000186
DHPA NO.: 27933**

**AREA OF POTENTIAL EFFECTS
(Pursuant to 36 CFR Section 800.4(a)(1))**

Given the nature of the proposed project, the area of potential effects (APE) was determined to include the proposed project area and a buffer zone based on topography and vegetation surrounding the proposed intersection improvement location. Visual impacts were the driving factor in the creation of the APE, as the project area is visible from surrounding commercial buildings and residences. The APE takes into account the potential direct and indirect effects of the proposed project within the immediate contextual setting, which is comprised of a heavily urbanized area with commercial and residential lots with scattered trees. The project location and the APE can be found in Appendix A.

**ELIGIBILITY DETERMINATIONS
(Pursuant to 36 CFR 800.4(c)(2))**

Lincolnshire Historic District (NR-0908) – The Lincolnshire Historic District was listed in the NRHP in 1989 under Criteria A and C for its significance relating to architecture, commerce, and community planning and development. The district contains 97 “contributing” and 22 “non-contributing” resources that are a mix of Tudor and Colonial Revival commercial and residential properties from the early to mid-twentieth century, some of which were designed by Anderson & Veatch, an Evansville design firm (see Appendix A [maps] and Appendix B [photos]).

Akin Park Historic District (IHSSI No. 163-196-430[01-213]) – The Akin Park Historic District is an excellent example of community planning and development in Evansville during the early to mid-twentieth century. The architecturally diverse residences represent the desire to have a fashionable, well-planned residential neighborhood utilizing local architects. The Akin Park Historic District is comprised of approximately 190 previously surveyed resources ranging from “contributing” to “outstanding,” that are set within an urban landscape dotted with mature trees. The district also contains approximately 21 “non-contributing” resources. The resources are a mix of single-family and multifamily dwellings along with Akin Park. The district features an array of architectural styles such as Craftsman Bungalows, Colonial Revival, Contemporary, Tudor Revival, and vernacular (see Appendix A [maps] and Appendix B [photos]).

The Akin Park Historic District is recommended eligible under Criterion A as it illustrates a planned and meticulously designed neighborhood in Evansville during the early to mid-twentieth century. Evidence of a planned neighborhood include the boulevards and centrally located park of which the residential neighborhood is centered around.

The Akin Park Historic District is also recommended eligible under Criterion C. The district features a cohesive collection of representative structures that embody the distinctive characteristics of early and mid-twentieth century architectural styles with original materials and forms along with locally designed residences.

EFFECT FINDING

Lincolnshire Historic District (NR-0908) – “No Adverse Effect.”

Akin Park Historic District (IHSSI No. 163-196-430[01-213]) – “No Adverse Effect.”

The Indiana Department of Transportation (INDOT) acting on behalf of the Federal Highway Administration (FHWA) has determined a “No Adverse Effect” finding is appropriate for this undertaking.

INDOT respectfully requests the Indiana State Historic Preservation Officer provide a written concurrence with the Section 106 determination of “No Adverse Effect.”

SECTION 4(F) COMPLIANCE REQUIREMENTS (for historic properties)

Lincolnshire Historic District (NR-0908) – This undertaking will not convert property from any Section 4(f) historic property to a transportation use; the INDOT, acting on FHWA’s behalf, has determined the appropriate Section 106 finding is “No Adverse Effect”; therefore no Section 4(f) evaluation is required.

Akin Park Historic District (IHSSI No. 163-196-430[01-213]) – This undertaking will not convert property from any Section 4(f) historic property to a transportation use; the INDOT, acting on FHWA’s behalf, has determined the appropriate Section 106 finding is “No Adverse Effect”; therefore no Section 4(f) evaluation is required.



Matthew S. Coon, Acting Manager
Cultural Resource Offices
Environmental Services

July 21, 2022

Approved Date

**FEDERAL HIGHWAY ADMINISTRATION
DOCUMENTATION OF SECTION 106 FINDING OF
NO ADVERSE EFFECT
SUBMITTED TO THE STATE HISTORIC PRESERVATION OFFICER
PURSUANT TO 36 CFR Section 800.5(c)**

**US 41 INTERSECTION IMPROVEMENT PROJECT
EVANSVILLE, VANDERBURGH COUNTY, INDIANA
DES. NO.: 2000186
DHPA NO.: 27933**

1. DESCRIPTION OF THE UNDERTAKING

The Indiana Department of Transportation (INDOT), with funding from the Federal Highway Administration (FHWA), proposes to proceed with the US 41 intersection improvement project at the intersection of US 41 and Washington Avenue (Des. No. 200186) in the City of Evansville, Vanderburgh County. The proposed undertaking is located at the intersection of US 41 and Washington Avenue in the city of Evansville, Vanderburgh County, Indiana. It is within Knight Township, as shown on the Evansville, South, Indiana, USGS Topographic Quadrangle, in Sections 28 & 33, Township 6 South, Range 10 West (see Appendix A [maps]). The proposed project area begins on US 41 approximately 560 feet (0.12 mile) south of its intersection with Bellemeade Avenue and extends south along US 41 for approximately 2,600 feet (0.5 mile). The project also begins along Washington Avenue approximately 430 feet (0.08 mile) west of its intersection with US 41 and extends approximately 1,010 feet (0.19 mile) east along Washington Avenue (see Appendix A [maps] and Appendix F [plans]).

The purpose of this project is to improve the safety of the intersection. The need for the project is based on the existing crash rates and safety concerns with this intersection. Based on the abbreviated engineer report for the project completed by HNTB, Corporation (HNTB) in January 2020, between 2016 and 2018, there were 15 Fatal and/or Incapacitating Crashes, 15 Non-Incapacitating and Possibly Injury Crash and 85 Property Damage Only Crashes making this intersection a "high crash" location.

Currently, the intersection of US 41 and Washington Avenue consists of two northbound and two southbound US 41 lanes with a left-turn lane for each direction separated by a grassy median. Washington Avenue consists of one westbound and one eastbound lane with a right-turn lane and a left-turn lane in each direction.

In order to meet the project purpose and need, a reduced conflict intersection will be constructed at the intersection of US 41 and Washington Avenue. The preferred option is the Boulevard Left intersection type. The Boulevard Left intersection relocates left turns from all legs of the main intersection to median U-turns located approximately 700 feet north and south of Washington Avenue. This intersection type retains the east-west connectivity on Washington Avenue. The Boulevard Left intersection has fewer signal phases than other types of reduced conflict intersections and provides better mobility to eastbound-westbound traffic on Washington Avenue, which is a minor arterial. The project will also include directional signage, maintenance of drainage

within the project extents, replacement of traffic signals at the main intersection, and the installation of new traffic signals at the median U-turn locations. There is no permanent or temporary right-of-way (ROW) acquisition anticipated for this project (see Appendix F [plans]).

Any median crossing work will be constructed prior to the closing of the intersection to allow for traffic movements to be open during construction. US 41 will be reduced to one lane in each direction during construction. Pedestrian maintenance of traffic will be required.

There are no anticipated relocations for this project.

Federal funding from the FHWA will be utilized for this project.

According to 36 CFR Section 800.16(d), the APE is the geographic area or areas within which an undertaking may directly or indirectly cause alterations in the character or use of historic properties, if any such properties exist. The APE is influenced by the scale and nature of an undertaking and may be different for different kinds of effects caused by the undertaking. Given the nature of the proposed project, the APE was determined to include the proposed project area and a buffer zone based on topography and vegetation surrounding the proposed intersection improvement location. Visual impacts were the driving factor in the creation of the APE, as the project area is visible from surrounding commercial buildings and residences. The APE takes into account the potential direct and indirect effects of the proposed project within the immediate contextual setting, which is comprised of a heavily urbanized area with commercial and residential lots with scattered trees (Appendix A [maps] & B [photos]).

2. EFFORTS TO IDENTIFY HISTORIC PROPERTIES.

The National Register of Historic Places (NRHP), Indiana Register of Historic Sites and Structures (State Register), the State Historic Architectural and Archaeological Research Database (SHAARD), the 1994 *Vanderburgh County Interim Report*, and Indiana Historic Building, Bridges, and Cemeteries (IHBBC) Map were consulted. As a result of the historic property identification and evaluation efforts, the Lincolnshire Historic District (NR-0908) was identified as being listed in the NRHP. Additionally, the Akin Park Historic District (IHSSI No. 163-196-430[01-213]) was identified and evaluated. CRA recommends that the Akin Park Historic District is eligible for listing in the NRHP.

With regards to archaeological resources, Andrew Martin, a qualified professional archaeologist, reviewed the proposed project area and determined the proposed intersection improvement project in Vanderburgh County will not likely affect archaeological resources due to the project setting. The project be within existing disturbed right-of-way. Furthermore, the soils throughout the project area have likely been disturbed by earth moving associated with a series of construction and demolition episodes occurring around the intersection during the late twentieth and early twenty-first centuries, as well as previous maintenance and improvements to the intersection itself and the installation and maintenance of buried utilities. Surficial evidence of these disturbances were noted during a visual inspection of the project area.

The project area does not appear to have been previously surveyed for archaeological resources. However, according to the State Historic Architectural and Archaeological Database (SHAARD), two previously recorded archaeological sites (12Vg1824 and 12Vg1825) are located within or adjacent to the project area. Site 12Vg1824 is a historic industrial site associated with machinist, elevator, and hot water apparatus manufacturing operations. Site 12Vg1825 is a historic lumber mill site. Both of these sites were recorded by researchers at Indiana State University. Based solely on historic map data and given the extent of subsequent disturbances at this location, it is unlikely that any intact archaeological deposits associated with these sites remain.

A review of historic map data revealed that an abandoned interurban line may transect the project area. This line followed Washington Avenue, running east and west, and is mapped within the roadway. It was established by the Evansville Railroad Company in 1866 and operated until 1939. At the time of abandonment, the line was owned by Southern Indiana Gas and Electric Co. An abandoned segment of the Illinois Central Railroad also transects the project area, extending north-south along the west side of US 41. What remained of the track bed at this location has been repurposed as a trail for cyclist and pedestrian use.

Given the disturbed character of the project area and its correspondingly low potential to contain intact archaeological deposits, it is recommended that the project be allowed to proceed without additional archaeological study. However, state law (Indiana Code 14-21-1-27 and -29) requires that if any prehistoric or historic archaeological artifacts or human remains are uncovered during construction, demolition, or earth moving activities, that the discovery must be reported to the Department of Natural Resources within two (2) business days.

A Section 106 early coordination letter (ECL) was distributed on August 2, 2021. A list of the invited consulting parties is as follows and can be found in Appendix C (consulting party list):

- Indiana State Historic Preservation Office (SHPO)
- Benjamin Bosse High School
- Greater Lincolnshire Neighborhood Association
- Charles Farmer, Traffic Engineering Foreman
- Downtown Evansville, Inc.
- Economic Development Coalition of Southwest Indiana
- Evansville Historic Preservation Commission
- Evansville Metropolitan Planning Organization (EMPO)
- Evansville-Vanderburgh School Corporation
- Historic Southern Indiana
- Indiana Landmarks Southwest Field Office
- Old Evansville Historic Association
- Preservation Alliance of Evansville
- Presidents Neighborhood Association
- Vanderburgh County Commissioners
- Vanderburgh County Highway Engineer
- Vanderburgh County Highway Superintendent
- Vanderburgh County Historian

- Vanderburgh County Historical Society
- Lloyd Winnecke, Mayor of Evansville
- Absentee Shawnee Tribe of Oklahoma
- Eastern Shawnee Tribe of Oklahoma
- Delaware Tribe of Indians, Oklahoma
- Miami Tribe of Oklahoma
- Peoria Tribe of Indians of Oklahoma
- Pokagon Band of Potawatomi Indians
- Shawnee Tribe
- United Keetowah Band of Cherokee Indians

On September 9, 2021, SHPO staff responded to the ECL. A Dual Review has been commenced for this project (Appendix D [correspondence]).

The Eastern Shawnee Tribe responded to the ECL on October 4, 2021, accepting consulting party status. In addition, they stated, "...should this project inadvertently discover an archeological site or object(s) we request that you immediately contact the Eastern Shawnee Tribe, as well as the appropriate state agencies (within 24 hours). We also ask that all ground disturbing activity stop until the Tribe and State agencies are consulted. Please note that any future changes to this project will require additional consultation" (Appendix D [correspondence]).

[The Peoria Tribe of Indians of Oklahoma responded to the ECL on October 15, 2021. They had no objections to the proposed project and requested immediate notification and consultation should any cultural resources be found (Appendix D [correspondence]). SEE NOTE BELOW]

No further responses were received regarding the ECL.

In August 2021, Cultural Resource Analysts, Inc. (CRA) conducted a site visit of the APE and documented all above-ground resources that will be 50 years of age or older at the time of project letting (2024). The APE was investigated for the existence of any buildings, structures, objects, or districts listed in or eligible for listing in the NRHP.

A Historic Property Report (HPR) was completed (Reynolds, November 22, 2021) and sent to consulting parties on December 13, 2021. CRA recommended that the Lincolnshire Historic District (NR-0908) continue to be listed in the NRHP Criteria A and C for its significance relating to architecture, commerce, and community planning and development. CRA also recommended the Akin Park Historic District (IHSSI No. 163-196-430[01-213]) as eligible for listing in the NRHP under Criterion A and C as it illustrates a planned and meticulously designed neighborhood in Evansville during the early to mid-twentieth century with excellent examples of early and mid-twentieth century architectural styles shown on locally designed residences. The summary of the HPR is found in Appendix E.

The SHPO responded to the HPR in a letter dated January 7, 2022. They agreed with the size of the APE, the archaeological assessment, and the recommendation that the Akin Park Historic District was eligible for listing in the NRHP. They also agreed that the Lincolnshire Historic District remains eligible for listing in the NRHP. Additionally, they also stated, "In regard to the

**Peoria Tribe of Indians of
Oklahoma did not respond to the
ECL and no letter is included in the
attachments.**

residential area north of Washington Avenue, bounded on the west by South Kerth Avenue, on the north by East Gum Street and on the east by US 41 that was evaluated to determine its potential as a mid-century historic district, we agree with the conclusions of the HPR that this area is not eligible for inclusion in the NRHP under the Residential Planning and Development in Indiana, 1940-1973 Multiple Property Documentation Form (MPDF). However, we have taken note of the nearby Bayard Park Historic District (NR-0756), which was listed in the NRHP on June 27, 1985, but is located outside of this project's area of potential effects. In reviewing the area surrounding the district, we believe that if the nomination for the Bayard Park Historic District was ever updated and its period of significance expanded, it is probable that its eastern boundary would be extended farther to the east. However, for the purposes of the dual review of this undertaking, we agree that the Lincolnshire Historic District and the Akin Park Historic District are the only historic properties located within the project's APE" (Appendix D [correspondence]).

No further responses were received regarding the HPR.

An Effects Report was sent to consulting parties on May 16, 2022. CRA recommended that there would be a finding of "No Adverse Effect" to the Lincolnshire Historic District (NR-0908) and the Akin Park Historic District (IHSSI No. 163-196-430[01-213]).

SHPO staff responded to the Effects Report on June 7, 2022. They agreed with the "No Adverse Effect" finding. SHPO staff recommended that INDOT proceed with an 800.11 document.

No additional responses to the Effects Letter were received.

3. DESCRIBE AFFECTED HISTORIC PROPERTIES

Lincolnshire Historic District (NR-0908) – The Lincolnshire Historic District was listed in the NRHP in 1989 under Criteria A and C for its significance relating to architecture, commerce, and community planning and development. The district contains 97 "contributing" and 22 "non-contributing" resources that are a mix of Tudor and Colonial Revival commercial and residential properties from the early to mid-twentieth century, some of which were designed by Anderson & Veatch, an Evansville design firm (Appendix A [maps] and Appendix B [photos]).

Akin Park Historic District (IHSSI No. 163-196-430[01-213]) – The Akin Park Historic District is an excellent example of community planning and development in Evansville during the early to mid-twentieth century. The architecturally diverse residences represent the desire to have a fashionable, well-planned residential neighborhood utilizing local architects. Approximately 190 previously surveyed resources range from "contributing" to "outstanding," and are set within an urban landscape dotted with mature trees. The district also contains approximately 21 "non-contributing" resources. The resources are a mix of single-family and multifamily dwellings along with Akin Park. The district features an array of architectural styles such as Craftsman Bungalows, Colonial Revival, Contemporary, Tudor Revival, and vernacular (Appendix A [maps] and Appendix B [photos]).

The Akin Park Historic District is recommended eligible under Criterion A as it illustrates a planned and meticulously designed neighborhood in Evansville during the early to mid-twentieth

century. Evidence of a planned neighborhood include the boulevards and centrally located park of which the residential neighborhood is centered around.

The Akin Park Historic District is also recommended eligible under Criterion C. The district features a cohesive collection of representative structures that embody the distinctive characteristics of early and mid-twentieth century architectural styles with original materials and forms along with locally designed residences.

4. DESCRIBE THE UNDERTAKING'S EFFECTS ON HISTORIC PROPERTIES

Lincolnshire Historic District (NR-0908) - There will be no physical effects to the Lincolnshire Historic District. There is no permanent or temporary ROW acquisition associated with this project.

Akin Park Historic District (IHSSI No. 163-196-430[01-213]) - There will be no physical effects to the Lincolnshire Historic District. There is no permanent or temporary ROW acquisition associated with this project.

Overall, work associated with the project, including the reconfiguration of US 41 using a Boulevard Left intersection, along with new directional signage, and the installation of new traffic signals at the median U-turn locations, will be visible from the Lincolnshire Historic District and the Akin Park Historic District, but it will not alter the districts' ability to convey their significance relating to architecture, commerce, and community planning and development.

5. EXPLAIN APPLICATION OF CRITERIA OF ADVERSE EFFECT -- INCLUDE CONDITIONS OR FUTURE ACTIONS TO AVOID, MINIMIZE OR MITIGATE ADVERSE EFFECTS

According to 36 CFR 800.5(a)(1) "An adverse effect is found when an undertaking may alter, directly or indirectly, any of the characteristics of a historic property that qualify the property for inclusion in the National Register in a manner that would diminish the integrity of the property's location, design, setting, materials, workmanship, feeling or association."

Lincolnshire Historic District (NR-0908) – According to 36 CFR 800.5(a)(1) the criteria of adverse effect do not apply.

Per 36 CFR 800.5(a)(2)(i), the undertaking will not result in the "Physical destruction of or damage to all or part of the property." None of the NRHP-listed district resources will be damaged. No permanent or temporary ROW will be acquired from the historic district.

Per 36 CFR 800.5(a)2(ii), the undertaking will not cause "Alteration of a property, including restoration, rehabilitation, repair, maintenance, stabilization, hazardous material remediation and provision of handicapped access, that is not consistent with the Secretary's Standards for the Treatment of Historic Properties and/or other applicable guidelines."

Per 36 CFR 800.5(a)2(iii), the undertaking will not result in the "Removal of the property from its historic location."

Per 36 CFR 800.5(a)2(iv), the undertaking will not result in a “Change of the character of the property’s uses or of physical features within the property setting that contribute to the historic significance” of the property. The proposed project will not alter the Lincolnshire Historic District’s setting as none of the activities associated with the project occur within the district, only adjacent to it. The traffic signals and signage located at the intersection of Washington Avenue and US 41 will be replaced in-kind with modern materials of approximately the same height. Replacement in-kind of modern materials will not affect the district’s setting as a form of these utilities has existed at the intersection for over 50 years. Additionally, a Boulevard Left intersection will result in the construction of two U-turns and the addition of traffic signals at each U-turn, eliminating the left-turn lanes for northbound and southbound lanes of US 41, also near, but not within the historic district.

Per 36 CFR 800.5(a)2(v), the undertaking will not result in the “Introduction of visual, atmospheric, or audible elements that diminish the integrity of the property’s significant historic features.” The previously mentioned project activities are all located outside of the historic district. Work associated with the alteration of the intersection of Washington Avenue and US 41 will be minimally visible from the historic district, as the work will occur at or near surface grade or will involve the replacement of existing more visible items, such as traffic signals and signage. The traffic signals and signage will be replaced in-kind with modern materials of approximately the same height in approximately the same location. New traffic signals will be placed at each median U-turn and will be suspended from strain poles located within the grassed ROW between the edge of pavement and the ROW fence. However, the work will not alter the district’s ability to convey its significance.

Per 36 CFR 800.5(a)2(vi), the undertaking will not cause the “Neglect of a property which causes its deterioration...” The project will have no effect that will result in neglect.

Per 36 CFR 800.5(a)2(vii), the undertaking will not cause the “Transfer, lease, or sale of property out of Federal ownership or control...” There are no federally owned properties within the proposed project area.

Akin Park Historic District (IHSSI No. 163-196-430[01-213]) – According to 36 CFR 800.5(a)(1) the criteria of adverse effect do not apply.

Per 36 CFR 800.5(a)2(i), the undertaking will not result in the “Physical destruction of or damage to all or part of the property.” None of the resources within the proposed boundary for the Akin Park Historic District will be damaged. No permanent or temporary ROW will be acquired from the proposed historic district boundary.

Per 36 CFR 800.5(a)2(ii), the undertaking will not cause “Alteration of a property, including restoration, rehabilitation, repair, maintenance, stabilization, hazardous material remediation and provision of handicapped access, that is not consistent with the Secretary’s Standards for the Treatment of Historic Properties and/or other applicable guidelines.”

Per 36 CFR 800.5(a)2(iii), the undertaking will not result in the “Removal of the property from its historic location.”

Per 36 CFR 800.5(a)2(iv), the undertaking will not result in a “Change of the character of the property’s uses or of physical features within the property setting that contribute to the historic significance” of the property. The proposed project will not alter the Akin Park Historic District’s setting as none of the activities associated with the project occur within the district. The traffic signals and signage located at the intersection of Washington Avenue and US 41 will be replaced in-kind with modern materials of approximately the same height. Replacement in-kind of modern materials will not affect the district’s setting as a form of these utilities has existed at the intersection for over 50 years. Additionally, a Boulevard Left intersection will result in the construction of two U-turns and the addition of traffic signals at each U-turn, eliminating the left-turn lanes for northbound and southbound lanes of US 41, also near, but not within the historic district.

Per 36 CFR 800.5(a)2(v), the undertaking will not result in the “Introduction of visual, atmospheric, or audible elements that diminish the integrity of the property’s significant historic features.” The previously mentioned project activities are all located outside of the historic district. Work associated with the alteration of the intersection of Washington Avenue and US 41 will be minimally visible from the historic district, as the work will occur at or near surface grade or will involve the replacement of existing more visible items, such as traffic signals and signage. The traffic signals and signage will be replaced in-kind with modern materials of approximately the same height in approximately the same location. New traffic signals will be placed at each median U-turn and will be suspended from strain poles located within the grassed ROW between the edge of pavement and the ROW fence. However, the work will not alter the district’s ability to convey its significance.

Per 36 CFR 800.5(a)2(vi), the undertaking will not cause the “Neglect of a property which causes its deterioration...” The project will have no effect that will result in neglect.

Per 36 CFR 800.5(a)2(vii), the undertaking will not cause the “Transfer, lease, or sale of property out of Federal ownership or control...” There are no federally owned properties within the proposed project area.

FUTURE ACTIONS TO AVOID, MINIMIZE OR MITIGATE ADVERSE EFFECTS.

There will be no adverse effects as a result of this project.

6. SUMMARY OF CONSULTING PARTIES AND PUBLIC VIEWS

An early coordination letter was uploaded to INSCOPE (INDOT’s public Section 106 consultation website) on August 2, 2021 and released for viewing online the same day. Hard copies of these materials were mailed to SHPO on August 2, 2021. Please also see Appendix C for a list of consulting parties.

On September 9, 2021, SHPO staff responded to the ECL. A Dual Review was commenced for this project (Appendix D [correspondence]).

The Eastern Shawnee Tribe responded to the ECL on October 4, 2021, accepting consulting party status. They also asked to be notified in any archaeological sites or objects were discovered (Appendix D [correspondence]).

The Peoria Tribe of Indians of Oklahoma responded to the ECL on October 15, 2021. They had no objections to the proposed project and requested immediate notification and consultation should any cultural resources be found (Appendix D [correspondence]).

No further responses were received regarding the early coordination letter.

A Historic Property Report (HPR) was completed (Reynolds, November 22, 2021) and sent to consulting parties on December 13, 2021. CRA recommended that the Lincolnshire Historic District (NR-0908) continued to be listed in the NRHP Criteria A and C for its significance relating to architecture, commerce, and community planning and development. CRA also recommended the Akin Park Historic District (IHSSI No. 163-196-430[01-213]) as eligible for listing in the NRHP under Criterion A and C as it illustrates a planned and meticulously designed neighborhood in Evansville during the early to mid-twentieth century with excellent examples of early and mid-twentieth century architectural styles shown on locally designed residences. The summary of the HPR is found in Appendix E.

The SHPO responded to the HPR in a letter dated January 7, 2022. They agreed with the size of the APE, the archaeological assessment, and the NRHP recommendations. They also noted that the nearby Bayard Park Historic District's (NR-0756) nomination form was ever updated, that the boundary would be extended east into the current project area (Appendix D [correspondence]).

No further responses were received regarding the HPR.

An Effects Report was sent to consulting parties on May 16, 2022. CRA recommended that there would be a finding of "No Adverse Effect" to the Lincolnshire Historic District (NR-0908) and the Akin Park Historic District (IHSSI No. 163-196-430[01-213]).

SHPO staff responded to the Effects Report on June 7, 2022. They agreed with the "No Adverse Effect" finding. SHPO staff recommended that INDOT proceed with an 800.11 document.

No additional responses to the Effects Report were received.

A public notice of the "No Adverse Effect" will be published in the *Courier & Press* seeking the views of the public regarding the effects of the proposed project on the historic property within the APE. Comments from the public will be accepted for 30 days following the publications of the notice. If any substantive comments are received during this period, this document will be revised to include them.

Section 800 Documentation

APPENDIX A: MAPS

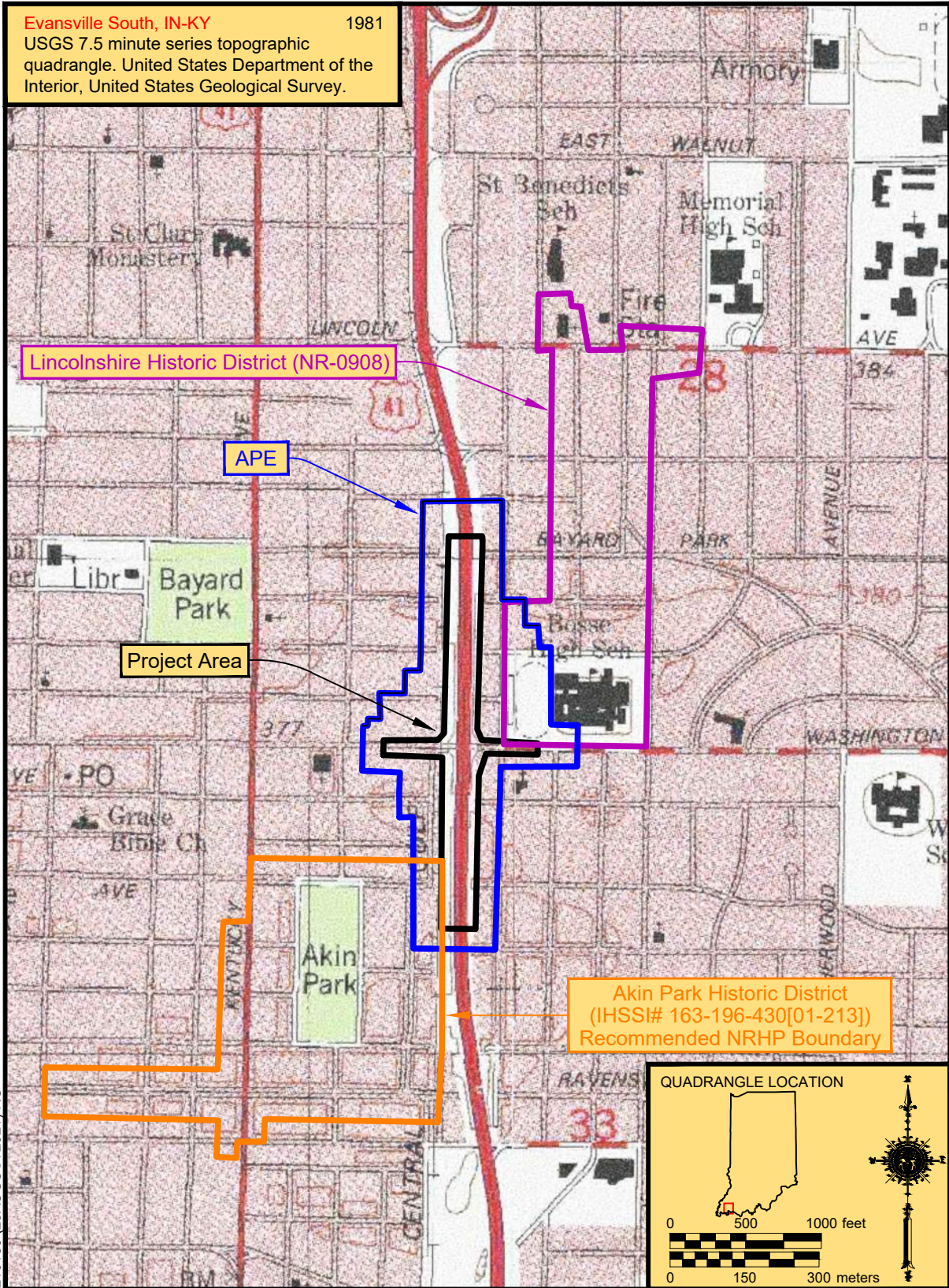


Figure 1. Topographic map showing the location of the proposed project, APE, and survey sites.

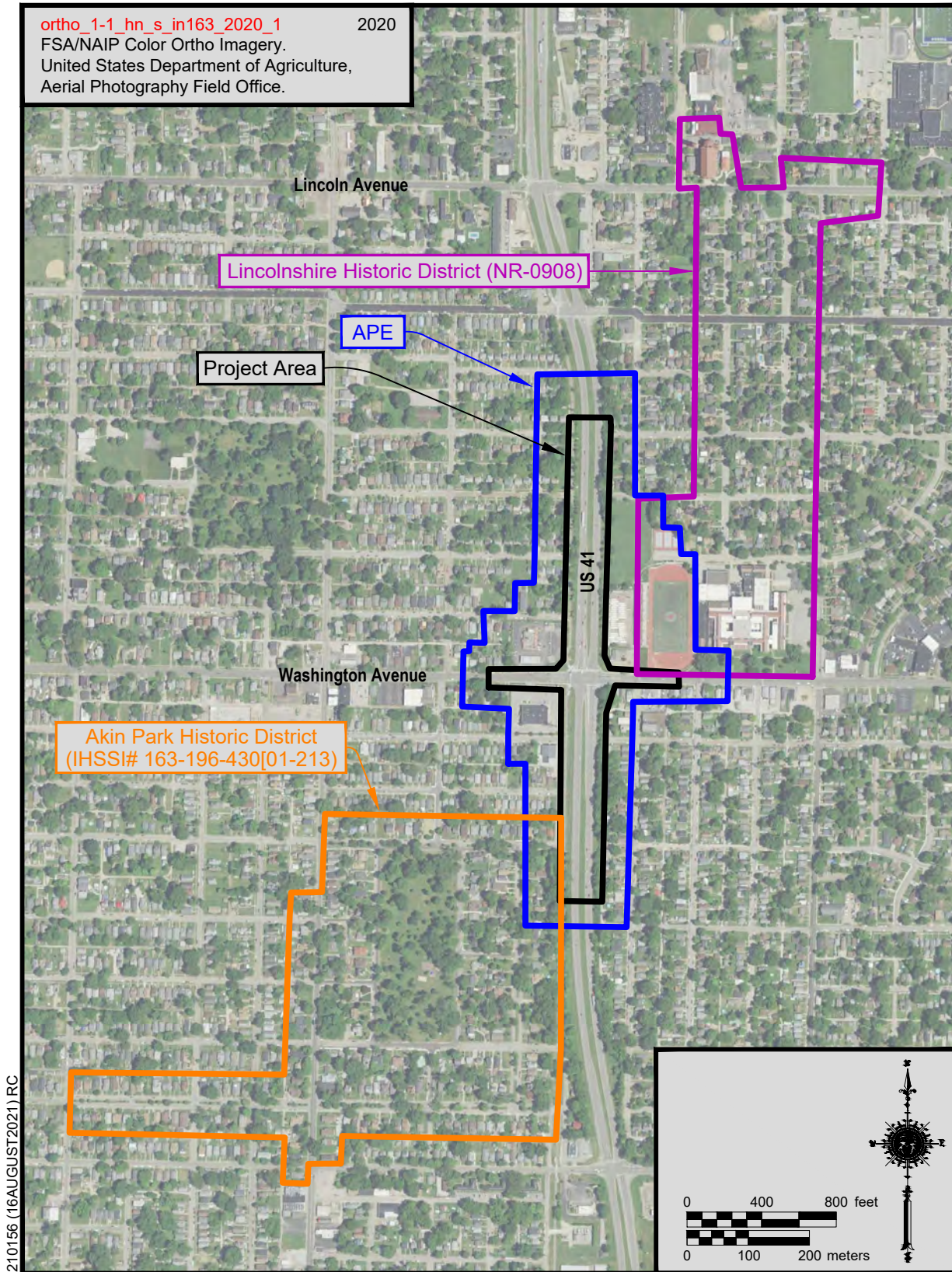


Figure 2. Aerial map showing the location of the proposed project, APE, and survey sites.

Section 800 Documentation

APPENDIX B. PHOTOS

ortho_1-1_hn_s_in163_2020_1 2020
FSA/NAIP Color Ortho Imagery.
United States Department of Agriculture,
Aerial Photography Field Office.

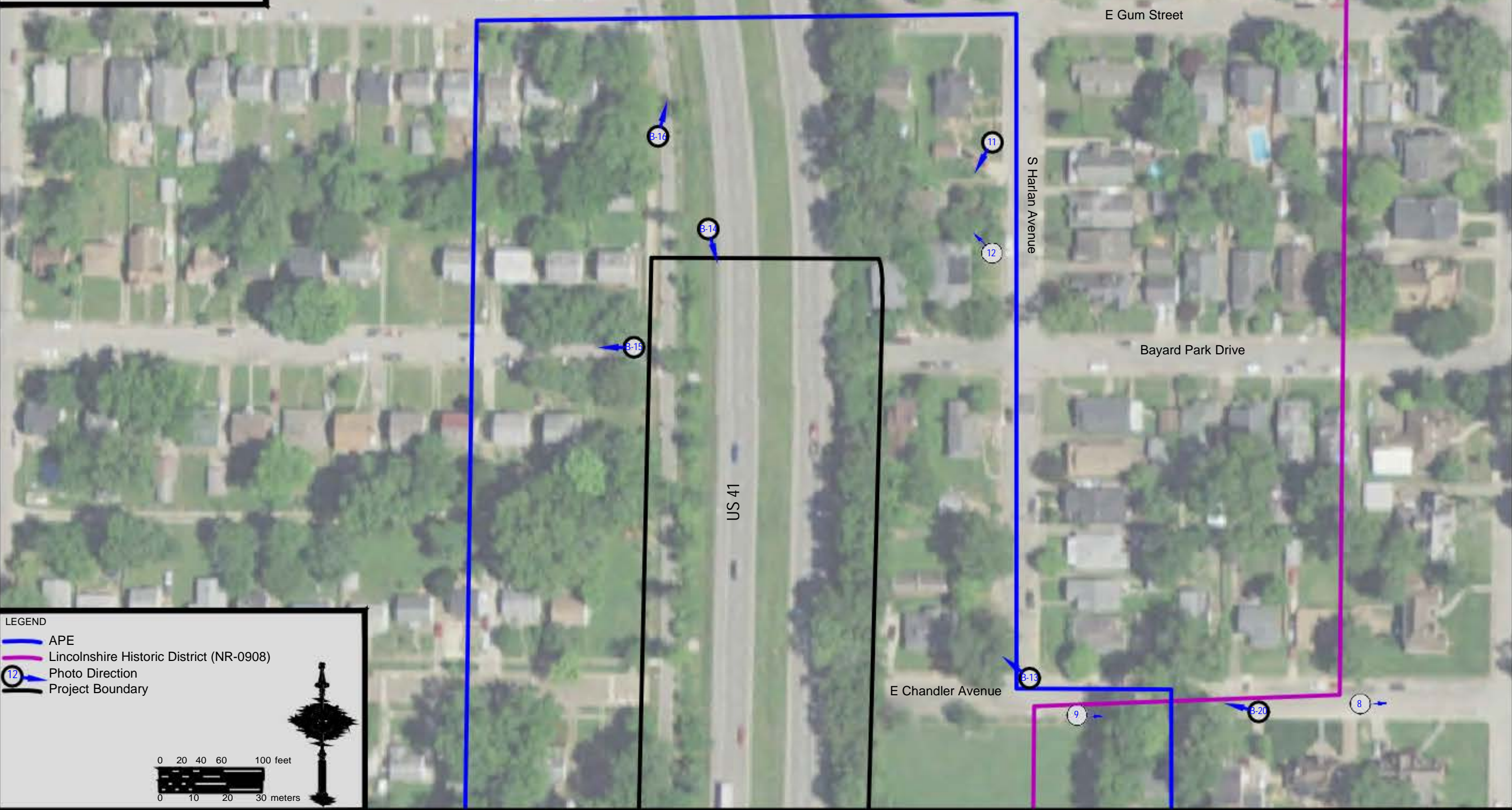
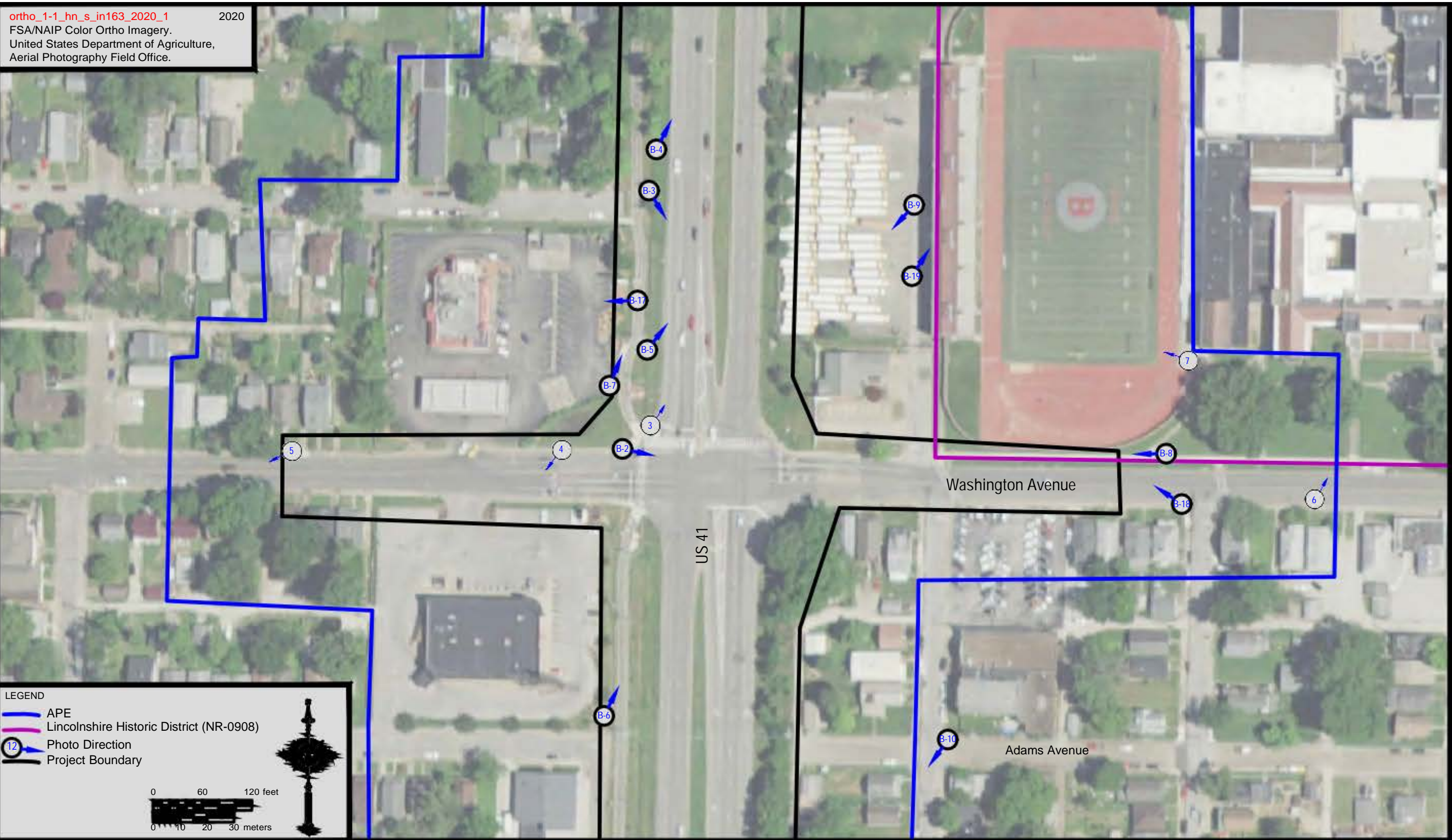


Figure B-1a. Washington Avenue and US 41 intersection improvement project photomap.

ortho_1-1_hn_s_in163_2020_1 2020
FSA/NAIP Color Ortho Imagery.
United States Department of Agriculture,
Aerial Photography Field Office.



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Figure B-1b. Washington Avenue and US 41 intersection improvement project photomap.

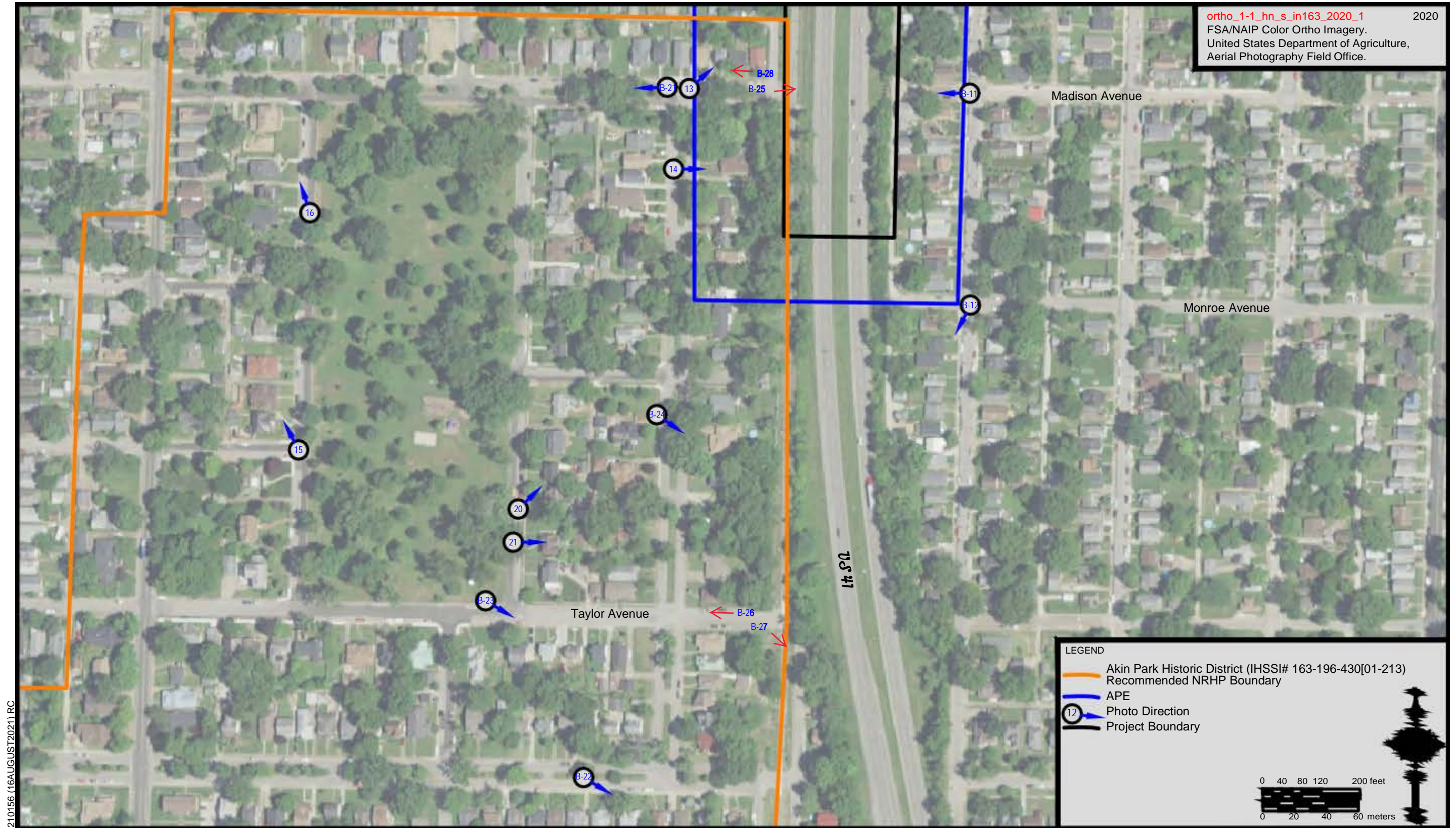


Figure B-1c. Washington Avenue and US 41 intersection improvement project photomap.

ortho_1-1_hn_s_in163_2020_1 2020
FSA/NAIP Color Ortho Imagery.
United States Department of Agriculture,
Aerial Photography Field Office.



Figure B-1d. Washington Avenue and US 41 intersection improvement project photomap.



Figure 1. Overview of project area at the intersection of Washington Avenue and US 41, looking northeast



Figure 2. Benjamin Bosse High School (IHSSI No. 163-196-41045), "Outstanding," located within the Lincolnshire Historic District (NR-0908), façade (south) elevation, facing northeast.



Figure 3. Overview of the interior of Enlow Field (IHSSI No. 163-196-41044), "Outstanding," located within the Lincolnshire Historic District (NR-0908), facing northwest.



Figure 4. Overview of the Akin Park Historic District at the intersection of Jackson Street and Park Drive, facing northwest.



Figure 5. Overview of the Akin Park Historic District along Park Drive, facing northwest.



Figure 6. Overview of the Akin Park Historic District at the intersection of South Bedford Avenue and Ravenswood Drive, facing northwest.



Figure 7. Overview of the Akin Park Historic District at the intersection of South Bedford Avenue and Ravenswood Drive, facing southwest.



Figure 8. Overview of the Akin Park Historic District at the intersection of South Bedford Avenue and Ravenswood Drive, facing southeast, showing the boulevard.



Figure 9. Façade (west) and south elevation of the Irma Schmidt House (IHSSI No. 163-196-430[168]; "Notable," designed by Alfred Neucks, facing northeast, outside of the APE.



Figure 10. Façade (west) elevation of the Dr. C. Gardner Johnson House (IHSSI No. 163-196-430[166]; "Notable"), designed by Walter K. Stuart, facing east, outside of the APE.



Figure 11. Overview of project area at the intersection of US 41 and Washington Avenue, facing southeast.



Figure 12. Overview of APE along the west side of US 41, facing southeast.



Figure 13. View of project area toward the Lincolnshire Historic District (NR-0908) with Enlow Field (IHSSI No. 163- 196-41044), "Outstanding," in the foreground, facing northeast.



Figure 14. Overview of APE along US 41, facing northeast.



Figure 15. Overview of APE along Washington Avenue, facing west.



Figure 16. Overview of project area from the Enlow Field parking lot, facing southwest.



Figure 17. Overview of the project area along US 41, facing southeast near the Akin Park Historic District.



Figure 18. Exterior of Enlow Field (IHSSI No. 163-196-41044), part of the Lincolnshire Historic District (NR-0908) along Washington Avenue, facing northwest.



Figure 19. Exterior of Enlow Field (IHSSI No. 163-196-41044), part of the Lincolnshire Historic District (NR-0908) from the parking lot, facing northeast.



Figure 20. Overview of the Akin Park Historic District (IHSSI No. 163-196-430[01-213]) at the intersection of Akin Drive and Madison Avenue, facing west.



Figure 21. Overview of the Akin Park Historic District (IHSSI No. 163-196-430[01-213]) along Ravenswood Drive, facing southeast.



Figure 22. Overview of Akin Park Historic District (IHSSI No. 163-196-430[01-213]) along Taylor Avenue, facing southeast.



Figure B-2. Overview of project area at the intersection of US 41 and Washington Avenue, facing southeast.



Figure B-3. Overview of APE along the west side of US 41, facing southeast.



Figure B-4. Overview of APE along the west side of US 41, facing northeast.



Figure B-5. View of project area toward the Lincolnshire Historic District (NR-0908) with Enlow Field (IHSSI No. 163-196-41044), "Outstanding," in the foreground, facing northeast.



Figure B-6. Overview of APE along US 41, facing northeast.



Figure B-7. Overview of project area along west side of US 41, facing northeast.



Figure B-8. Overview of APE along Washington Avenue, facing west.



Figure B-9. Overview of project area from the Enlow Field parking lot, facing southwest.



Figure B-10. Overview of the APE at the intersection of Adams Avenue and South Harlan Avenue, facing southwest.



Figure B-11. Overview of the project area from the intersection of South Harlan Avenue and Madison Avenue, facing west.



Figure B-12. Overview of resources located just outside of the APE at the intersection of South Harlan Avenue and Monroe Avenue, facing southwest.



Figure B-13. Overview of the APE at the intersection of South Harlan Avenue and East Chandler Avenue, facing northwest.



Figure B-14. Overview of the project area along US 41, facing southeast.



Figure B-15. Overview of the APE along Bayard Park Drive, facing west.



Figure B-16. Overview of the APE along US 41, facing northeast.



Figure B-17. Example of a property constructed during the early twenty-first century at the intersection of US 41 and Washington Avenue, facing northwest.



Figure B-18. Exterior of Enlow Field (IHSSI No. 163-196-41044), part of the Lincolnshire Historic District (NR-0908) along Washington Avenue, facing northwest.



Figure B-19. Exterior of Enlow Field (IHSSI No. 163-196-41044), part of the Lincolnshire Historic District (NR-0908) from the parking lot, facing northeast.



Figure B-20. Overview of a portion of the Lincolnshire Historic District (NR-0908) located within and just outside of the APE along East Powell Avenue, facing northwest.



Figure B-21. Overview of the Akin Park Historic District (IHSSI No. 163-196-430[01-213]) at the intersection of Akin Drive and Madison Avenue, facing west.



Figure B-22. Overview of the Akin Park Historic District (IHSSI No. 163-196-430[01-213]) along Ravenswood Drive, facing southeast.



Figure B-23. Overview of Akin Park Historic District (IHSSI No. 163-196-430[01-213]) along Taylor Avenue, facing southeast.



Figure B-24. Overview of Akin Park Historic District (IHSSI No. 163-196-430[01-213]) along Akin Drive, facing southeast.



Figure B-25. Overview of project area at the eastern end of Madison Avenue near the Akin Park Historic District looking toward the project area along US 41, facing east.



Figure B-26. Overview of the Akin Park Historic District on **Taylor Avenue** near the **eastern** edge of the project area along US 41, facing west.



Figure B-27. Overview of the project area from **Taylor Avenue** near the Akin Park Historic District, facing southeast.



Figure B-28. Overview of the Akin Park Historic District from the eastern end of **Madison Avenue** near the project area along US 41, facing southwest.

Section 800 Documentation

APPENDIX C: CONSULTING PARTY LIST

Accepted consulting parties highlighted

Automatic Section 106 Consulting Parties:
Indiana Department of Natural Resources,
Division of Historic Preservation &
Archaeology, Indiana State Historic
Preservation Office (SHPO)
402 W. Washington St., Room W274
Indianapolis, Indiana 46204

Invited Consulting Parties:

Benjamin Bosse High School

Aaron Huff, Principal
1300 Washington Avenue
Evansville, IN 47714
812.435.8889

Greater Lincolnshire Neighborhood Association

Betsy Pruitt
{glnaevansville@gmail.com}

Charles Farmer, Traffic Engineering Foreman

1 NW Martin Luther King, Jr. Boulevard, Room 302
Evansville, IN 47708
812.435.6003
{cmfarmer@evansville.in.gov}

Downtown Evansville, Inc.

Kathleen Lane
209 Main Street
Evansville, IN 47708
812.424.2986
{kathleenlane@downtownevansville.org}

Economic Development Coalition of Southwest Indiana

Greg Wathen, President
318 Main Street, Suite 400
Evansville, IN 47708
812.423.2020
{dbennett@southwestindiana.org}

Evansville Historic Preservation Commission

Matt Rowe, Chair
224 SE First Street
Evansville, IN 47706
812.426.1871
{mrowe@reitzhome.com}

Evansville Metropolitan Planning Organization (EMPO)

Seyed Shokouhzadeh, Executive Director
Civic Center Complex, Room 316
1 NW Martin Luther King Boulevard
Evansville, IN 47708
812.436.7833
{shokouhzadeh@evansvillempo.com}
{pdrach@evansvillempo.com}

Evansville-Vanderburgh School Corporation

David B. Smith, Superintendent
951 Walnut Street
Evansville, IN 47713

Historic Southern Indiana

Leslie Townsend, Director
University of Southern Indiana
8600 University South Boulevard
Evansville, IN 47712
812.465.7013
{ltownsen@usi.edu}

Indiana Landmarks Southwest Field Office

Candice Croix, Director
PO Box 297
Evansville, IN 47702
812.423.2988
{ccroix@indianalandmarks.org}

Old Evansville Historic Association

Cathie Hite
PO Box 1012
606 SE First Street
Evansville, IN 47706
812.425.9909
{pguth1950@aol.com}

Preservation Alliance of Evansville

Dennis Au
PO Box 1322
Evansville, IN 47706
812.426.1871
{info@PreserveEvansville.org}

Presidents Neighborhood Association

Brent Jackson, President
320 SE Martin Luther King, Jr Boulevard
Suite B
Evansville, IN 47713
812.428.4243
{brent@bdjackson.com}

Vanderburgh County Commissioner

Jeff Hatfield
1 NW Martin Luther King Jr. Boulevard
Civic Center Complex, Room 305
Evansville, IN 47708
812.435.5241
{jhatfield@vanderburghgov.org}

Vanderburgh County Commissioner

Cheryl Musgrave
1 NW Martin Luther King Jr. Boulevard
Civic Center Complex, Room 305
Evansville, IN 47708
812.435.5241
{cwmusgrave@vanderburghgov.org}

Vanderburgh County Commissioner

Ben Shoulders
1 NW Martin Luther King Jr. Boulevard
Civic Center Complex, Room 305
Evansville, IN 47708
812.435.5241
{bshoulders@vanderburghgov.org}

Vanderburgh County Highway Engineer

John Stoll, P.E.
201 NW 4th Street, Room 306
Evansville, IN 47708
812.435.5773
{jstoll@vanderburghgov.org}

Vanderburgh County Highway Superintendent

Scot Wichser
5105 N Saint Joseph Avenue
Evansville, IN 47720
812.435.5777
{swichser@vanderburghgov.org}

Vanderburgh County Historian

Stan Schmitt
2900 N Fulton, #F1
Evansville, IN 47710
812.423.6815
{stanleya53@juno.com}

Vanderburgh County Historical Society

Terry Hughes, President
PO Box 2626
Evansville, IN 47728
812.401.1112
{thughes5109@gmail.com}

Lloyd Winnecke, Mayor of Evansville

1 NW Martin Luther King, Jr. Boulevard, Room
302
Evansville, IN 47708
812.436.4962
{mayor@evansville.in.gov}

Absentee Shawnee Tribe of Oklahoma

Eastern Shawnee Tribe of Oklahoma

Delaware Tribe of Indians, Oklahoma

Miami Tribe of Oklahoma

Peoria Tribe of Indians of Oklahoma

Pokagon Band of Potawatomi Indians

Shawnee Tribe

United Keetowah Band of Cherokee Indians

Section 800 Documentation

APPENDIXD: CORRESPONDENCE

Alyssa Reynolds

From: Alyssa Reynolds <adreynolds@crai-ky.com>
Sent: Monday, August 2, 2021 2:51 PM
To: glnaevansville@gmail.com; cmfarmer@evansville.in.gov;
kathleenlane@downtownevansville.org; dbennett@southwestindiana.org;
mrowe@reitzhome.com; shokouhzadeh@evansvillempo.com;
pdrach@evansvillempo.com; ltownsen@usi.edu; 'Candice Croix'; pguth1950@aol.com;
info@PreserveEvansville.org; brent@bdjackson.com; jhatfield@vanderburghgov.org;
cwmusgrave@vanderburghgov.org; bshoulders@vanderburghgov.org;
jstoll@vanderburghgov.org; swichser@vanderburghgov.org; stanleya53@juno.com;
thughes5109@gmail.com; mayor@evansville.in.gov
Cc: Andrew Martin; Robert Ball; 'Christine Meador'; 'Carpenter, Patrick A'; 'Branigin, Susan';
'Kumar, Anuradha'; 'Miller, Shaun (INDOT)'; 'Tabitha Enyart'; 'Dan Thatcher'; 'Arnold,
Troy'
Subject: Dual Review FHWA Project: Des No 2000186; ECL; US 41 at Washington Avenue
Intersection Improvement, Vanderburgh County, Indiana
Attachments: US 41_Intersection Improvement_Des 2000186_ECL_2021_8_2.pdf

All,

Des. No.: 2000186

Project Description: Intersection Improvement at US 41 and Washington Avenue

Location: Evansville, Knight Township, Vanderburgh County, Indiana

The Indiana Department of Transportation, with funding from the Federal Highway Administration, proposes to proceed with the intersection improvement project at US 41 and Washington Avenue; Des. No. 2000186
Section 106 of the National Historic Preservation Act requires federal agencies to take into account the effects of their undertakings on historic properties. The following agencies/individuals are being invited to become consulting parties:

Indiana Department of Natural Resources
Division of Historic Preservation & Archaeology Indiana State Historic Preservation Office (SHPO)

Benjamin Bosse High School

Greater Lincolnshire Neighborhood Association

Charles Farmer, Traffic Engineering Foreman

Downtown Evansville, Inc.

Economic Development Coalition of Southwest Indiana

Evansville Historic Preservation Commission

Evansville Metropolitan Planning Organization (EMPO)

Evansville-Vanderburgh School Corporation

Historic Southern Indiana

Indiana Landmarks Southwest Field Office

Old Evansville Historic Association

Preservation Alliance of Evansville

Presidents Neighborhood Association

Vanderburgh County Commissioners

Vanderburgh County Highway Engineer

Vanderburgh County Highway Superintendent

Vanderburgh County Historian

Vanderburgh County Historical Society

Lloyd Winnecke, Mayor of Evansville

Absentee Shawnee Tribe of Oklahoma

Eastern Shawnee Tribe of Oklahoma

Delaware Tribe of Indians, Oklahoma

Miami Tribe of Oklahoma

Peoria Tribe of Indians of Oklahoma

Pokagon Band of Potawatomi Indians

Shawnee Tribe

United Keetowah Band of Cherokee Indians

This letter is part of the early coordination phase of the environmental review process requesting comments associated with this project. We are requesting comments from your area of expertise regarding any possible environmental effects associated with this project. Please use the above Des. Number and project description in your reply and your comments will be incorporated into the formal environmental study.

Please review the attached letter, which is also located in IN SCOPE at <http://erms.indot.in.gov/Section106Documents/> (the Des. No. is the most efficient search term, once in IN SCOPE), and respond with your comments on any historic resource impacts incurred as a result of this project so that an environmental report can be completed. We also welcome your related opinions and other input to be considered in the preparation of the environmental document. If a hard copy of the materials is needed, please respond to this email with your request within seven (7) days.

Consulting parties have thirty (30) calendar days from receipt of this information to review and provide comments. If we do not receive a response from an invited consulting party within the time allotted, the project will proceed consistent with the proposed design. **Therefore, if we do not receive a response within thirty (30) days, your agency or organization will not receive any further information on the project unless the scope of work changes.**

Tribal consulting parties may enter the process at any time and are encouraged to respond to this notification with any comments or concerns at their earliest convenience by contacting Shaun Miller at smiller@indot.in.gov or 317-416-0876 or Kari-Carmany-George at FHWA at K.CarmanyGeorge@dot.gov or 317-266-5629.

Thank you in advance for your input,

Alyssa Reynolds
Architectural Historian
adreynolds@crai-ky.com

Indiana Office
201 NW 4th Street, Suite 204
Evansville, Indiana 47708
812.253.3009 office
812.253.3010 fax
812.549.4503 cell
<http://www.crai-ky.com>



Celebrating 38 Years in Business!

Alyssa Reynolds

From: Alyssa Reynolds <adreynolds@crai-ky.com>
Sent: Monday, August 2, 2021 2:54 PM
To: 'Slider, Chad (DNR)'; 'McCord, Beth K'; 'Burkett, Miriam'; dhparchive
Cc: 'Carpenter, Patrick A'; 'Branigin, Susan'; 'Miller, Shaun (INDOT)'; 'Kumar, Anuradha'; Robert Ball; 'Christine Meador'; 'Dan Thatcher'; 'Tabitha Enyart'
Subject: Dual Review FHWA Project: Des No 2000186; ECL; US 41 at Washington Avenue Intersection Improvement, Vanderburgh County, Indiana
Attachments: US 41_Intersection Improvement_Des 2000186_ECL_2021_8_2.pdf; US 41_Intersection Improvement_Des 2000186_DHPA review form_2021_8_2.pdf

All,

A hard copy of the ECL will be mailed to you by the end of the week.

Des. No.: 2000186

Project Description: Intersection Improvement at US 41 and Washington Avenue

Location: Evansville, Knight Township, Vanderburgh County, Indiana

The Indiana Department of Transportation, with funding from the Federal Highway Administration, proposes to proceed with the intersection improvement project at US 41 and Washington Avenue; Des. No. 2000186

Section 106 of the National Historic Preservation Act requires federal agencies to take into account the effects of their undertakings on historic properties. The following agencies/individuals are being invited to become consulting parties:

Indiana Department of Natural Resources
Division of Historic Preservation & Archaeology Indiana State Historic Preservation Office (SHPO)

Benjamin Bosse High School

Greater Lincolnshire Neighborhood Association

Charles Farmer, Traffic Engineering Foreman

Downtown Evansville, Inc.

Economic Development Coalition of Southwest Indiana

Evansville Historic Preservation Commission

Evansville Metropolitan Planning Organization (EMPO)

Evansville-Vanderburgh School Corporation

Historic Southern Indiana

Indiana Landmarks Southwest Field Office

Old Evansville Historic Association

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Presidents Neighborhood Association

Vanderburgh County Commissioners

Vanderburgh County Highway Engineer

Vanderburgh County Highway Superintendent

Vanderburgh County Historian

Vanderburgh County Historical Society

Lloyd Winnecke, Mayor of Evansville

Absentee Shawnee Tribe of Oklahoma

Eastern Shawnee Tribe of Oklahoma

Delaware Tribe of Indians, Oklahoma

Miami Tribe of Oklahoma

Peoria Tribe of Indians of Oklahoma

Pokagon Band of Potawatomi Indians

Shawnee Tribe

United Keetowah Band of Cherokee Indians

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Tribal consulting parties may enter the process at any time and are encouraged to respond to this notification with any comments or concerns at their earliest convenience by contacting Shaun Miller at smiller@indot.in.gov or 317-416-0876 or Kari-Carmany-George at FHWA at K.CarmanyGeorge@dot.gov or 317-266-5629.

Thank you in advance for your input,

Alyssa Reynolds
Architectural Historian
adreynolds@crai-ky.com

Indiana Office
201 NW 4th Street, Suite 204
Evansville, Indiana 47708
812.253.3009 office
812.253.3010 fax
812.549.4503 cell
<http://www.crai-ky.com>



Celebrating 38 Years in Business!

Alyssa Reynolds

From: Carpenter, Patrick A <PACarpenter@indot.IN.gov>
Sent: Monday, August 2, 2021 4:00 PM
To: 'thpo@estoo.net'; Diane Hunter; Logan Pappenfort; matthew.bussler@pokagonband-nsn.gov; tonya@shawnee-tribe.com; snease@astribe.com; Larry Heady; Erica Gorsuch
Cc: Alyssa Reynolds; Miller, Shaun (INDOT); Carmany-George, Karstin (FHWA); Branigin, Susan
Subject: FHWA Project: Des No 2000186; ECL; US 41 at Washington Avenue Intersection Improvement, Vanderburgh County, Indiana

Dear consulting parties,

Des. No.: 2000186

Project Description: Intersection Improvement at US 41 and Washington Avenue

Location: Evansville, Knight Township, Vanderburgh County, Indiana

The Indiana Department of Transportation, with funding from the Federal Highway Administration, proposes to proceed with the intersection improvement project at US 41 and Washington Avenue; Des. No. 2000186. Section 106 of the National Historic Preservation Act requires federal agencies to take into account the effects of their undertakings on historic properties. The following agencies/individuals are being invited to become consulting parties:

Indiana Department of Natural Resources
Division of Historic Preservation & Archaeology Indiana State Historic Preservation Office (SHPO)

Benjamin Bosse High School

Greater Lincolnshire Neighborhood Association

Charles Farmer, Traffic Engineering Foreman

Downtown Evansville, Inc.

Economic Development Coalition of Southwest Indiana

Evansville Historic Preservation Commission

Evansville Metropolitan Planning Organization (EMPO)

Evansville-Vanderburgh School Corporation

Historic Southern Indiana

Indiana Landmarks Southwest Field Office

Old Evansville Historic Association

Preservation Alliance of Evansville

Presidents Neighborhood Association

Vanderburgh County Commissioners

Vanderburgh County Highway Engineer

Vanderburgh County Highway Superintendent

Vanderburgh County Historian

Vanderburgh County Historical Society

Lloyd Winnecke, Mayor of Evansville

Absentee Shawnee Tribe of Oklahoma

Eastern Shawnee Tribe of Oklahoma

Delaware Tribe of Indians, Oklahoma

Miami Tribe of Oklahoma

Peoria Tribe of Indians of Oklahoma

Pokagon Band of Potawatomi Indians

Shawnee Tribe

United Keetowah Band of Cherokee Indians

This letter is part of the early coordination phase of the environmental review process requesting comments associated with this project. We are requesting comments from your area of expertise regarding any possible environmental effects associated with this project. Please use the above Des. Number and project description in your reply and your comments will be incorporated into the formal environmental study.

Please review the attached letter, which is also located in IN SCOPE at <http://erms.indot.in.gov/Section106Documents/> (the Des. No. is the most efficient search term, once in IN SCOPE), and respond with your comments on any historic resource impacts incurred as a result of this project so that an environmental report can be completed. We also welcome your related opinions and other input to be considered in the preparation of the environmental document. If a hard copy of the materials is needed, please respond to this email with your request within seven (7) days.

Consulting parties have thirty (30) calendar days from receipt of this information to review and provide comments. If we do not receive a response from an invited consulting party within the time allotted, the project will proceed consistent with the proposed design. **Therefore, if we do not receive a response within thirty (30) days, your agency or organization will not receive any further information on the project unless the scope of work changes.**

Tribal consulting parties may enter the process at any time and are encouraged to respond to this notification with any comments or concerns at their earliest convenience by contacting Shaun Miller at smiller@indot.in.gov or 317-416-0876 or Kari-Carmany-George at FHWA at K.CarmanyGeorge@dot.gov or 317-266-5629.

Thank you in advance for your input,

Patrick Carpenter
Section 106 Specialist, Cultural Resources Office
Environmental Services
Indiana Department of Transportation
100 N Senate Ave., IGCN- Room N758-ES
Indianapolis, IN 46204-2216
317-416-7960*

*Please note new phone number.

August 2, 2021

This letter was sent to the listed parties.

RE: Dual Review FHWA Project: Des No 2000186; US 41 at Washington Avenue
Intersection Improvement, Vanderburgh County, Indiana

Dear Consulting Party (see attached list),

The Indiana Department of Transportation (INDOT), with funding from the Federal Highway Administration (FHWA), proposes to proceed with a project involving the construction of a Reduced Conflict Intersection (RCI) at US 41 and Washington Avenue (Des. No. 2000186), approximately 1.00 mile south of the intersection of US 41 and SR 66/SR 62 (Lloyd Expressway) in the city of Evansville, Vanderburgh County, Indiana. Cultural Resource Analysts, Inc. (CRA) is under contract with INDOT to advance the environmental documentation for the referenced project.

This letter is part of the early coordination phase of the environmental review process requesting comments associated with the project. We are requesting comments from your area of expertise regarding any possible environmental effects associated with this project. Please use the above Des. No. and project description in your reply and your comments will be incorporated into the formal environmental study.

The proposed undertaking is located at the intersection of US 41 and Washington Avenue in the city of Evansville, Vanderburgh County, Indiana. It is within Knight Township, as shown on the Evansville, South, Indiana, USGS Topographic Quadrangle, in Sections 28 & 33, Township 6 South, Range 10 West.

The need for the project is based on the existing crash rates and safety concerns with this intersection. Based on the abbreviated engineer report for the project completed by HNTB in January 2020, between 2016 and 2018, there were 15 Fatal and/or Incapacitating Crashes, 15 Non-Incapacitating and Possibly Injury Crash and 85 Property Damage Only Crashes making this intersection a "high crash" location. The purpose of this project is to improve the safety of the intersection.

In order to meet the project purpose and need, a reduced conflict intersection will be constructed at the intersection of US 41 and Washington Avenue. The project will reduce conflict points at the intersection by considering eliminating left turns in the intersection and evaluating through movements of Washington Avenue across US 41. Details will be finalized

during design; however, preliminary studies have shown the use of a boulevard left to be a feasible solution. The project will also include directional signage, maintenance of drainage within the project extents, and replacement of traffic signals. Utilities may be relocated if in conflict with the project. The project is anticipated to require less than 0.5 acre of permanent right-of-way.

Any median crossing work will be constructed prior to the closing of the intersection to allow for traffic movements to be open during construction. US 41 will be reduced to one lane in each direction during construction. Pedestrian maintenance of traffic will be required.

The proposed project area for the intersection improvement project, located at the intersection of US 41 and Washington Avenue, begins on US 41 approximately 560 feet (0.12 mile) south of its intersection with Bellemeade Avenue and extends south along US 41 for approximately 2,600 feet (0.5 mile). The project also begins along Washington Avenue approximately 430 feet (0.08 mile) west of its intersection with US 41 and extends approximately 1,010 feet (0.19 mile) east along Washington Avenue.

Section 106 of the National Historic Preservation Act requires federal agencies to take into account the effects of their undertakings on historic and archaeological properties. In accordance with 36 CFR 800.2 (c), you are hereby requested to be a consulting party to participate in the Section 106 process. Entities that have been invited to participate in the Section 106 consultation process for this project are identified in the attached list. Per 36 CFR 800.3(f), we hereby request that the Indiana State Historic Preservation Officer (SHPO) notify this office if the SHPO staff is aware of any other parties that may be entitled to be consulting parties or should be contacted as potential consulting parties for the project.

The Section 106 process involves efforts to identify historic properties potentially affected by the undertaking, assess its effects and seek ways to avoid, minimize or mitigate any adverse effects on historic properties. For more information regarding the protection of historic resources, please see the Advisory Council on Historic Preservation's guide: *Protecting Historic Properties: A Citizen's Guide to Section 106 Review* available online at <https://www.achp.gov/sites/default/files/documents/2017-01/CitizenGuide.pdf>.

Please note that per the permanent rule issued by the Indiana Department of Natural Resources effective August 14, 2013 (312 IAC 20-4-11.5), INDOT is requesting that this project be subjected to "dual review"; that is, reviewed by the Division of Historic Preservation and Archaeology simultaneously under 54 U.S.C. 306108 (Section 106) and IC 14-21-1-18 (Indiana Preservation and Archaeology Law dealing with alterations of historic sites and structures requiring a Certificate of Approval). Pursuant to Section 11.5(f) of this rule, at the conclusion of the review process we anticipate that the Division Director would issue a letter of clearance exempting this project from obtaining a Certificate of Approval under IC 14-21-1-18. Enclosed with this letter is a detailed list of the consulting parties, including contact information including email addresses, for processing the dual review submission.

The Area of Potential Effects (APE) is the area in which the proposed project may cause alterations in the character or use of historic resources. At this time, no cultural resource

investigations have occurred; however, the results of cultural resource above-ground identification and evaluation efforts will be forthcoming. Consulting parties will receive notification when these reports are completed.

With regards to archaeological resources, Andrew Martin, a qualified professional archaeologist, reviewed the proposed project area and determined the proposed intersection improvement project in Vanderburgh County will not likely affect archaeological resources due to the project setting. The project will largely be within existing disturbed right-of-way, with less than 0.5 acres of new right-of-way expected to be needed. Furthermore, the soils throughout the project area have likely been disturbed by earth moving associated with a series of construction and demolition episodes occurring around the intersection during the late twentieth and early twenty-first centuries, as well as previous maintenance and improvements to the intersection itself and the installation and maintenance of buried utilities. Surficial evidence of these disturbances were noted during a visual inspection of the project area.

The project area does not appear to have been previously surveyed for archaeological resources. However, according to the State Historic Architectural and Archaeological Database (SHAARD), two previously recorded archaeological sites (12Vg1824 and 12Vg1825) are located within or adjacent to the project area. Site 12Vg1824 is a historic industrial site associated with machinist, elevator, and hot water apparatus manufacturing operations. Site 12Vg1825 is a historic lumber mill site. Both of these sites were recorded by researchers at Indiana State University. And, based solely on historic map data and given the extent of subsequent disturbances at this location, it is unlikely that any intact archaeological deposits associated with these sites remain.

A review of historic map data revealed that an abandoned interurban line may transect the project area. This line followed Washington Avenue, running east and west, and is mapped within the roadway. It was established by the Evansville Railroad Company in 1866 and operated until 1939. At the time of abandonment, the line was owned by Southern Indiana Gas and Electric Co. An abandoned segment of the Illinois Central Railroad also transects the project area, extending north-south along the west side of US 41. What remained of the track bed at this location has been repurposed as a trail for cyclist and pedestrian use.

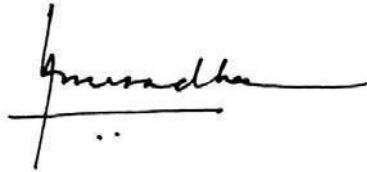
Given the disturbed character of the project area and its correspondingly low potential to contain intact archaeological deposits, it is recommended that the project be allowed to proceed without additional archaeological study. However, state law (Indiana Code 14-21-1-27 and -29) requires that if any prehistoric or historic archaeological artifacts or human remains are uncovered during construction, demolition, or earth moving activities, that the discovery must be reported to the Department of Natural Resources within two (2) business days.

For questions concerning specific project details, you may contact Alyssa Reynolds of Cultural Resource Analysts, Inc. (CRA) at (812) 253-3009 or adreynolds@crai-ky.com. All future responses regarding the proposed project should be forwarded to CRA at the following address:

Alyssa Reynolds
Architectural Historian
Cultural Resource Analysts, Inc. (CRA)
201 NW 4th Street, Suite 204
Evansville, Indiana 47708
adreynolds@crai-ky.com

Tribal contacts may contact Shaun Miller at smiller@indot.in.gov or 317-416-0876 Kari Carmany-George at FHWA at K.CarmanyGeorge@dot.gov or 317-226-5629.

Sincerely,

A handwritten signature in black ink, appearing to read "Anuradha", written over a horizontal line. There are two small dots below the line.

Anuradha V. Kumar, Manager
Cultural Resources Office
Environmental Services

Enclosures: Distribution List, USGS Topographic map, and Aerial View Map.

Consulting Party Early Coordination

Automatic Section 106 Consulting Parties:
Indiana Department of Natural Resources,
Division of Historic Preservation &
Archaeology, Indiana State Historic
Preservation Office (SHPO)
402 W. Washington St., Room W274
Indianapolis, Indiana 46204

August 2, 2021

Invited Consulting Parties:

Benjamin Bosse High School

Aaron Huff, Principal
1300 Washington Avenue
Evansville, IN 47714
812.435.8889

Greater Lincolnshire Neighborhood Association

Betsy Pruitt
{glnaevansville@gmail.com}

Charles Farmer, Traffic Engineering Foreman

1 NW Martin Luther King, Jr. Boulevard, Room 302
Evansville, IN 47708
812.435.6003
{cmfarmer@evansville.in.gov}

Downtown Evansville, Inc.

Kathleen Lane
209 Main Street
Evansville, IN 47708
812.424.2986
{kathleenlane@downtownevansville.org}

Economic Development Coalition of Southwest Indiana

Greg Wathen, President
318 Main Street, Suite 400
Evansville, IN 47708
812.423.2020
{dbennett@southwestindiana.org}

Evansville Historic Preservation Commission

Matt Rowe, Chair
224 SE First Street
Evansville, IN 47706
812.426.1871
{mrowe@reitzhome.com}

Evansville Metropolitan Planning Organization (EMPO)

Seyed Shokouhzadeh, Executive Director
Civic Center Complex, Room 316
1 NW Martin Luther King Boulevard
Evansville, IN 47708
812.436.7833
{shokouhzadeh@evansvillempo.com}
{pdrach@evansvillempo.com}

Evansville-Vanderburgh School Corporation

David B. Smith, Superintendent
951 Walnut Street
Evansville, IN 47713

Historic Southern Indiana

Leslie Townsend, Director
University of Southern Indiana
8600 University South Boulevard
Evansville, IN 47712
812.465.7013
{ltownsen@usi.edu}

Indiana Landmarks Southwest Field Office

Candice Croix, Director
PO Box 297
Evansville, IN 47702
812.423.2988
{ccroix@indianalandmarks.org}

Old Evansville Historic Association

Cathie Hite
PO Box 1012
606 SE First Street
Evansville, IN 47706
812.425.9909
{pguth1950@aol.com}

Preservation Alliance of Evansville

Dennis Au
PO Box 1322
Evansville, IN 47706
812.426.1871
{info@PreserveEvansville.org}

Presidents Neighborhood Association

Brent Jackson, President
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812.428.4243
{brent@bdjackson.com}

Vanderburgh County Commissioner

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Vanderburgh County Commissioner

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{cwmusgrave@vanderburghgov.org}

Vanderburgh County Commissioner

Ben Shoulders
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Civic Center Complex, Room 305
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{bshoulders@vanderburghgov.org}

Vanderburgh County Highway Engineer

John Stoll, P.E.
201 NW 4th Street, Room 306
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{jstoll@vanderburghgov.org}

Vanderburgh County Highway Superintendent

Scot Wichser
5105 N Saint Joseph Avenue
Evansville, IN 47720
812.435.5777
{swichser@vanderburghgov.org}

Vanderburgh County Historian

Stan Schmitt
2900 N Fulton, #F1
Evansville, IN 47710
812.423.6815
{stanleya53@juno.com}

Vanderburgh County Historical Society

Terry Hughes, President
PO Box 2626
Evansville, IN 47728
812.401.1112
{thughes5109@gmail.com}

Lloyd Winnecke, Mayor of Evansville

1 NW Martin Luther King, Jr. Boulevard, Room
302
Evansville, IN 47708
812.436.4962
{mayor@evansville.in.gov}

Absentee Shawnee Tribe of Oklahoma

Eastern Shawnee Tribe of Oklahoma

Delaware Tribe of Indians, Oklahoma

Miami Tribe of Oklahoma

Peoria Tribe of Indians of Oklahoma

Pokagon Band of Potawatomi Indians

Shawnee Tribe

United Keetowah Band of Cherokee Indians

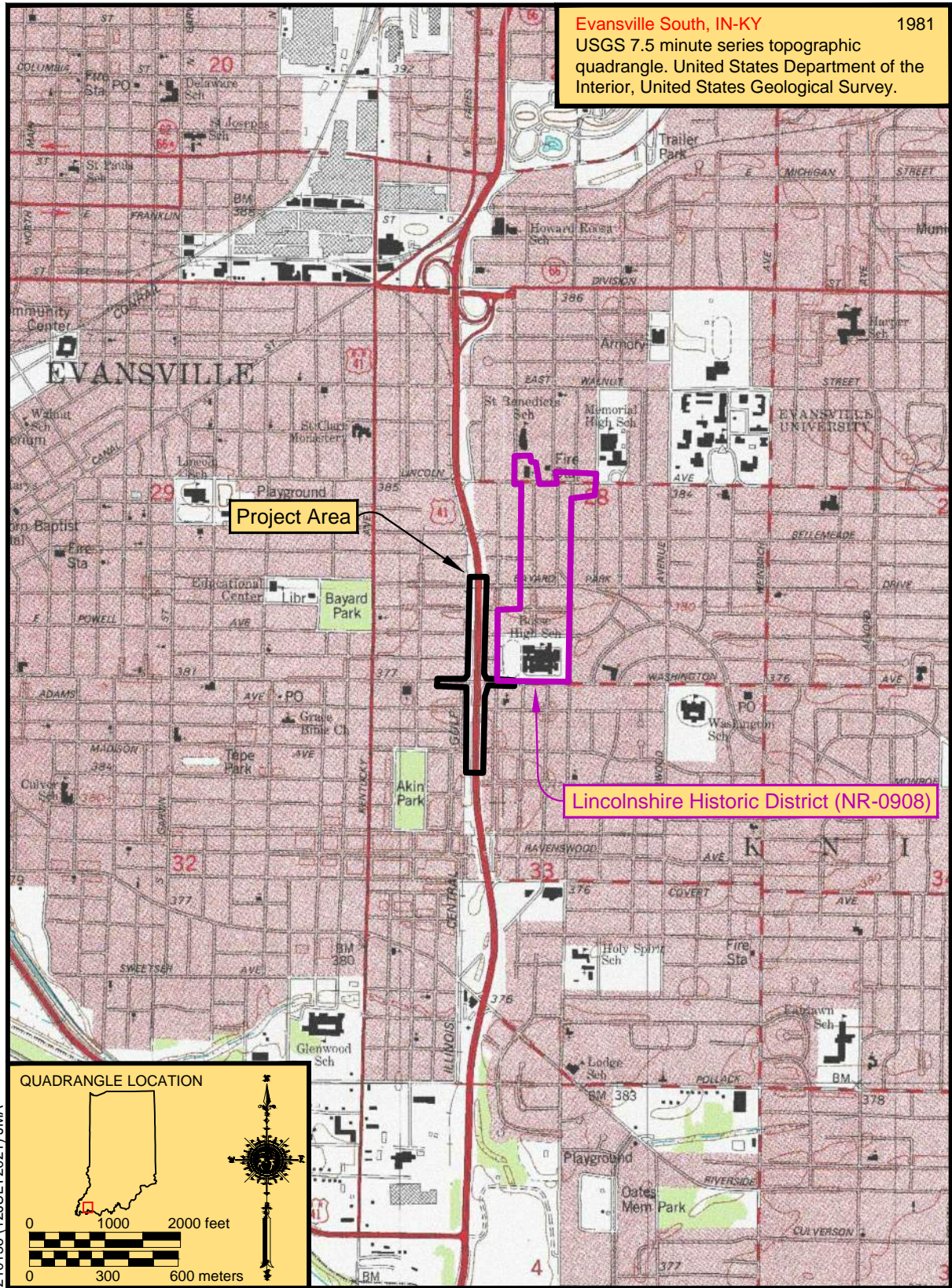


Figure 1. Topographic quadrangle showing the location of the project area and the Lincolnshire Historic District (NR-0908).

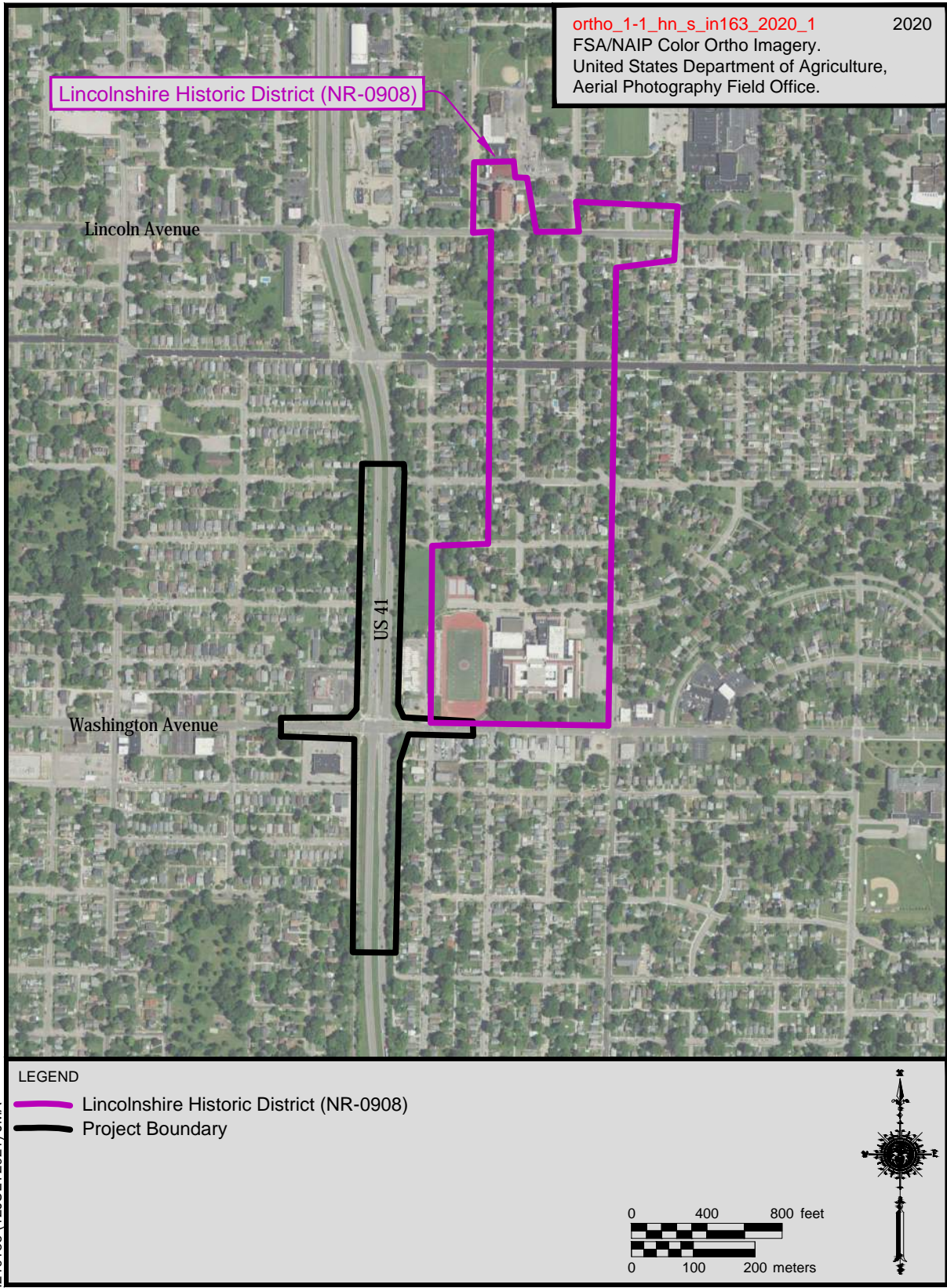


Figure 2. Aerial map showing the location of the project area and the Lincolnshire Historic District (NR-0908).

Division of Historic Preservation & Archaeology · 402 W. Washington Street, W274 · Indianapolis, IN 46204-2739
Phone 317-232-1646 · Fax 317-232-0693 · dhp@dnr.IN.gov



September 9, 2021

Alyssa Reynolds
Architectural Historian
Cultural Resources Analysts, Inc.
201 NW 4th Street, Suite 204
Evansville, Indiana 47708

State Agency: Indiana Department of Transportation (“INDOT”),
Federal Agency: Federal Highway Administration, Indiana Division (“FHWA”)

Re: DUAL REVIEW: Early coordination letter, and proposal for Dual Review for the US 41 at Washington
Avenue Intersection Improvement project (Des. No. 2000186; DHPA No. 27933)

Dear Ms. Reynolds:

The Indiana Department of Natural Resources, Division of Historic Preservation and Archaeology (“DNR-DHPA”), which also serves as the staff of the Indiana State Historic Preservation Officer (“Indiana SHPO”), is in receipt of INDOT’s early coordination letter, dated August 2, 2021, transmitting your proposal for a dual review, pursuant to 312 Indiana Administrative Code (“IAC”) 20-4-11.5, of the aforementioned project in Evansville, Knight Township, Vanderburgh County. We received this submission on August 2, 2021.

The Indiana SHPO/DNR-DHPA will review the information submitted under Section 106 of the National Historic Preservation Act of 1966, as amended (54 U.S.C. § 306108), implementing regulations at 36 C.F.R. Part 800, as well as Indiana Code 14-21-1-18 and 312 IAC 20-4. By copy of this letter, DNR-DHPA is providing notification of the commencement of the dual review to interested persons and members of the Indiana Historic Preservation Review Board (“Review Board”). Notice of the commencement will also be posted on the division’s website (<https://www.in.gov/dnr/historic-preservation/help-for-professionals/check-project-status/>).

For the purposes of Indiana Code 14-21-1-18 and 312 IAC 20-4, we have added the members of the Review Board and additional, potentially interested parties to the list of parties we intend to copy with our comment letters. Anyone receiving an e-mailed copy of this letter who *does not wish to receive future copies of our correspondence about this project* is asked to reply by e-mail to dkauffmann@dnr.in.gov and advise us that he or she does not wish to receive any further copies of our e-mails on this project.

In your next regular submission, please include which consulting parties agree to participate in the consultation of this dual review.

As INDOT’s August 2, 2021, letter indicates, additional information regarding above-ground historic resources is forthcoming. Once the indicated information is received, the Indiana SHPO will resume identification and evaluation procedures for this project. Please keep in mind that additional information may be requested in the future. We note that another dual review project, Des. No. 1902709 (DHPA No. 26884) is currently past the effects analysis stage of review in our office for the construction of a pedestrian bridge over US 41 at this same intersection.

In terms of archaeological resources, based on the submitted information and the documentation available to the staff of the Indiana SHPO, it is our understanding that portions of the proposed project area lie within portions of two previously recorded industrial archaeological sites--12-Vg-1824 (a historical industrial site associated with machinist, elevator, and hot water apparatus manufacturing operations) and 12-Vg-1825 (a historical lumber mill site)--and within portions of an abandoned interurban line (Evansville Railroad Company/Southern Indiana Gas and Electric Co.; established in 1866, and operated until 1939) and an

abandoned segment of the Indiana Central Railroad. However, it is our understanding, from the submission and from additional/clarifying information contained in the September 8, 2021, e-mail message from Andy V. Martin (CRA, Inc.) to Wade T. Tharp (Indiana DNR-DHPA) that modern development (i.e., recent building construction, utilities installation, and grading related to road and ditch construction) within the areas of the previously recorded industrial archaeological sites are likely to have negatively affected the integrity of any extant archaeological deposits.

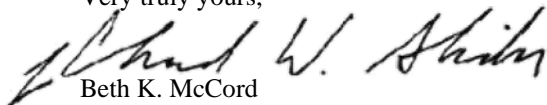
If any prehistoric or historic archaeological artifacts or human remains are uncovered during construction, demolition, or earthmoving activities, state law (Indiana Code 14-21-1-27 and Indiana Code 14-21-1-29) requires that the discovery be reported to the Indiana Department of Natural Resources, Division of Historic Preservation and Archaeology within two (2) business days. In that event, please call (317) 232-1646. Be advised that adherence to Indiana Code 14-21-1-27 and Indiana Code 14-21-1-29 does not obviate the need to adhere to applicable federal statutes and regulations, including but not limited to 36 C.F.R. Part 800.

For the benefit of those recipients of a copy of this letter who are not Section 106 consulting parties, please be aware that a copy of INDOT's August 2, 2021, letter can be found online at <http://erms.indot.in.gov/Section106Documents/>. From there, search by this project's designation number: 2000186.

If you have questions regarding our dual review of the aforementioned project, please contact DNR-DHPA. Questions about archaeological issues should be directed to contact Wade T. Tharp at (317) 232-1650 or wtharo1@dnr.in.gov. Questions about historic buildings or structures pertaining to this review should be directed to Caitlin Lehman at (317) 232-0461 or clehman1@dnr.IN.gov or Danielle Kauffmann at (317) 232-0582 or dkauffmann@dnr.IN.gov.

In all future correspondence regarding the dual review of this US 41 at Washington Avenue Intersection Improvement project in Evansville, Knight Township, Vanderburgh County (Des. No. 2000186), please refer to DHPA No. 27933.

Very truly yours,



Beth K. McCord
Deputy State Historic Preservation Officer
Director, Division of Historic Preservation and Archaeology

BKM:CML:DMK:WTT:wtt

EMC to federal and state agency or consultant staff members:

Kari Carmany-George, FHWA
Anuradha Kumar, INDOT
Shaun Miller, INDOT
Susan Branigin, INDOT
Anthony Ross, INDOT
Alyssa Reynolds, Cultural Resources Analysts, Inc.
Wade Tharp, Indiana DNR-DHPA
Danielle Kauffmann, Indiana DNR-DHPA
Caitlin Lehman, Indiana DNR-DHPA

EMC to Indiana Historic Preservation Review Board Members:

J. Scott Keller, Review Board
Daniel Kloc, AIA, Review Board
Jason Larrison, AIA, Review Board
Chandler Lighty, Review Board
Beth K. McCord, Indiana DNR-DHPA, Review Board
Ryan Mueller, Deputy Director, DNR, and Chairman, Review Board
Anne Shaw, Review Board
April Sievert, Ph.D., Review Board

EMC to potentially interested persons:

Absentee Shawnee Tribe of Oklahoma
Eastern Shawnee Tribe of Oklahoma
Delaware Tribe of Indians, Oklahoma
Miami Tribe of Oklahoma
Peoria Tribe of Indians of Oklahoma
Pokagon Band of Potawatomi Indians
Shawnee Tribe
United Keetowah Band of Cherokee Indians
Benjamin Bosse High School
Greater Lincolnshire Neighborhood Association
Charles Farmer, Traffic Engineering Foreman

Downtown Evansville, Inc.
Economic Development Coalition of Southwest Indiana
Evansville Historic Preservation Commission
Evansville Metropolitan Planning Organization
Evansville-Vanderburgh School Corporation
City of Evansville Historic Preservation Officer
Historic Southern Indiana
Indiana Landmarks Southwest Field Office
Old Evansville Historic Association
Dennis Au, Preservation Alliance of Evansville
Brent Jackson, Presidents Neighborhood Association
Jeff Hatfield, Vanderburgh County Commissioner
Cheryl Musgrave, Vanderburgh County Commissioner
Ben Shoulders, Vanderburgh County Commissioner
Vanderburgh County Highway Engineer
Vanderburgh County Highway Superintendent
Vanderburgh County Historian
Vanderburgh County Historical Society
Honorable Lloyd Winnecke, Mayor of Evansville



**EASTERN SHAWNEE
CULTURAL PRESERVATION DEPARTMENT**

70500 East 128 Road, Wyandotte, OK 74370

October 4, 2021

INDOT - Indiana Department of Transportation
100 N. Senate Ave. IGCN642
Indianapolis, IN 46201

RE: Des. No. 2000186, Vanderburgh County, Indiana

Dear Mr. Miller,

The Eastern Shawnee Tribe has received your letter regarding the above referenced project(s) within Vanderburgh County, Indiana. The Eastern Shawnee Tribe is committed to protecting sites important to Tribal Heritage, Culture and Religion. Furthermore, the Tribe is particularly concerned with historical sites that may contain but not limited to the burial(s) of human remains and associated funerary objects.

As described in your correspondence, and upon research of our database(s) and files, we find our people occupied these areas historically and/or prehistorically. However, the project proposes **NO Adverse Effect** or endangerment to known sites of interest to the Eastern Shawnee Tribe. Please continue project as planned. However, should this project inadvertently discover an archeological site or object(s) we request that you immediately contact the Eastern Shawnee Tribe, as well as the appropriate state agencies (within 24 hours). We also ask that all ground disturbing activity stop until the Tribe and State agencies are consulted. Please note that any future changes to this project will require additional consultation.

In accordance with the NHPA of 1966 (16 U.S.C. § 470-470w-6), federally funded, licensed, or permitted undertakings that are subject to the Section 106 review process must determine effects to significant historic properties. As clarified in Section 101(d)(6)(A-B), historic properties may have religious and/or cultural significance to Indian Tribes. Section 106 of NHPA requires Federal agencies to consider the effects of their actions on all significant historic properties (36 CFR Part 800) as does the National Environmental Policy Act of 1969 (43 U.S.C. § 4321-4347 and 40 CFR § 1501.7(a)). This letter evidences NHPA and NEPA historic properties compliance pertaining to consultation with this Tribe regarding the referenced proposed projects.

Thank you, for contacting the Eastern Shawnee Tribe, we appreciate your cooperation. Should you have any further questions or comments please contact our Office.

Sincerely,

Paul Barton, Tribal Historic Preservation Officer (THPO)
Eastern Shawnee Tribe of Oklahoma
(918) 666-5151 Ext:1833

Alyssa Reynolds

From: Alyssa Reynolds <adreynolds@crai-ky.com>
Sent: Monday, December 13, 2021 8:22 AM
To: dhpareview
Cc: Andrew Martin; Robert Ball; 'Carpenter, Patrick A'; 'Branigin, Susan'; 'Arnold, Troy'; 'Christine Meador'; 'tenyart'; 'Falls, Ryan G'
Subject: Dual Review FHWA Project: Des No 2000186; DHPA No 27933; HPR; US 41 at Washington Avenue Intersection Improvement, Vanderburgh County, Indiana
Attachments: US 41_Intersection Improvement_Des 2000186_DHPA review form_2021_12_13.pdf; US 41_Intersection Improvement_Des 2000186_HPR_2021_12_13.pdf; US 41_Intersection Improvement_Des 2000186_RDL_2021_12_13.pdf

Hello,

A hard copy of the attached documents will be mailed to you by the end of the week.

Des. No.: 2000186

Project Description: Intersection Improvement at US 41 and Washington Avenue

Location: Evansville, Knight Township, Vanderburgh County, Indiana

The Indiana Department of Transportation, with funding from the Federal Highway Administration, proposes to proceed with the intersection improvement project at US 41 and Washington Avenue; Des. NO. 2000186.

The Section 106 Early Coordination Letter for this project was originally distributed on August 2, 2021.

As part of Section 106 of the National Historic Preservation Act, a Historic Property Report has been prepared and is ready for review and comment by consulting parties. In addition, an archaeological assessment has been prepared and is included in the attached Report Distribution Letter.

Please review this documentation located in IN SCOPE at <http://erms.indot.in.gov/Section106Documents/> (the Des. No. is the most efficient search term, once in IN SCOPE), and respond with any comments that you may have. If a hard copy of the materials is needed, please respond to this email with your request as soon as you can.

Consulting parties have thirty (30) calendar days from receipt of this information to review and provide comment. Tribal consulting parties may enter the process at any time and are encouraged to respond to this notification with any comments or concerns at their earliest convenience. Tribal contacts may contact Shaun Miller at smiller@indot.in.gov or 317-416-0876 or Kari-Carmany-George at FHWA at K.CarmanyGeorge@dot.gov or 317-226-5629.

Thank you in advance for your input,

Alyssa Reynolds
Architectural Historian
adreynolds@crai-ky.com

Indiana Office
201 NW 4th Street, Suite 204
Evansville, Indiana 47708

812.253.3009 office
812.253.3010 fax
812.549.4503 cell
<http://www.crai-ky.com>



Celebrating 38 Years in Business!

December 13, 2021

This letter was sent to the listed parties.

RE: Dual Review FHWA Project: Des No 2000186; DHPA No 27933; US 41 at Washington Avenue Intersection Improvement, Vanderburgh County, Indiana

Dear Consulting Party (see attached list),

The Indiana Department of Transportation (INDOT), with funding from the Federal Highway Administration (FHWA), proposes to proceed with a project involving the construction of a Reduced Conflict Intersection (RCI) at US 41 and Washington Avenue (Des. No. 2000186), approximately 1.00 mile south of the intersection of US 41 and SR 66/SR 62 (Lloyd Expressway) in the city of Evansville, Vanderburgh County, Indiana.

This letter is part of the early coordination phase of the environmental review process requesting comments associated with the project. We are requesting comments from your area of expertise regarding any possible environmental effects associated with this project. Please use the above Des. No. and project description in your reply and your comments will be incorporated into the formal environmental study.

A Section 106 early coordination letter (ECL) was distributed on August 2, 2021.

The proposed undertaking is located at the intersection of US 41 and Washington Avenue in the city of Evansville, Vanderburgh County, Indiana. It is within Knight Township, as shown on the Evansville, South, Indiana, USGS Topographic Quadrangle, in Sections 28 & 33, Township 6 South, Range 10 West.

The need for the project is based on the existing crash rates and safety concerns with this intersection. Based on the abbreviated engineer report for the project completed by HNTB in January 2020, between 2016 and 2018, there were 15 Fatal and/or Incapacitating Crashes, 15 Non-Incapacitating and Possibly Injury Crash and 85 Property Damage Only Crashes making this intersection a "high crash" location. The purpose of this project is to improve the safety of the intersection.

In order to meet the project purpose and need, a reduced conflict intersection will be constructed at the intersection of US 41 and Washington Avenue. The project will reduce conflict points at the intersection by considering eliminating left turns in the intersection and evaluating through movements of Washington Avenue across US 41. Details will be finalized during design; however, preliminary studies have shown the use of a boulevard left to be a feasible solution. The project will also include directional signage, maintenance of drainage within the project extents, and replacement of traffic signals. Utilities may be relocated if in conflict with the project. The project is anticipated to require less than 0.5 acre of permanent right-of-way.

Any median crossing work will be constructed prior to the closing of the intersection to allow for traffic movements to be open during construction. US 41 will be reduced to one lane in each direction during construction. Pedestrian maintenance of traffic will be required.

The proposed project area for the intersection improvement project, located at the intersection of US 41 and Washington Avenue, begins on US 41 approximately 560 feet (0.12 mile) south of its intersection with Bellemeade Avenue and extends south along US 41 for approximately 2,600 feet (0.5 mile). The project also begins along Washington Avenue approximately 430 feet (0.08 mile) west of its intersection with US 41 and extends approximately 1,010 feet (0.19 mile) east along Washington Avenue.

Cultural Resource Analysts (CRA) is acting on behalf of HNTB Corporation (HNTB), who is under contract with INDOT to advance the environmental documentation for the referenced project.

Section 106 of the National Historic Preservation Act requires federal agencies to take into account the effects of their undertakings on historic and archaeological properties. In accordance with 36 CFR 800.2 (c), you are hereby requested to be a consulting party to participate in the Section 106 process. Entities that have been invited to participate in the Section 106 consultation process for this project are identified in the attached list. Per 36 CFR 800.3(f), we hereby request that the Indiana State Historic Preservation Officer (SHPO) notify this office if the SHPO staff is aware of any other parties that may be entitled to be consulting parties or should be contacted as potential consulting parties for the project.

The Section 106 process involves efforts to identify historic properties potentially affected by the undertaking, assess its effects and seek ways to avoid, minimize or mitigate any adverse effects on historic properties. For more information regarding the protection of historic resources, please see the Advisory Council on Historic Preservation's guide: *Protecting Historic Properties: A Citizen's Guide to Section 106 Review* available online at <https://www.achp.gov/sites/default/files/documents/2017-01/CitizenGuide.pdf>.

Please note that per the permanent rule issued by the Indiana Department of Natural Resources effective August 14, 2013 (312 IAC 20-4-11.5), INDOT is requesting that this project be subjected to "dual review"; that is, reviewed by the Division of Historic Preservation and Archaeology simultaneously under 54 U.S.C. 306108 (Section 106) and IC 14-21-1-18 (Indiana Preservation and Archaeology Law dealing with alterations of historic sites and structures requiring a Certificate of Approval). Pursuant to Section 11.5(f) of this rule, at the conclusion of the review process we anticipate that the Division Director would issue a letter of clearance exempting this project from obtaining a Certificate of Approval under IC 14-21-1-18. Enclosed with this letter is a detailed list of the consulting parties, including contact information including email addresses, for processing the dual review submission.

The Area of Potential Effects (APE) is the area in which the proposed project may cause alterations in the character or use of historic resources. The APE contains one resource listed in the National Register of Historic Places (NRHP), the Lincolnshire Historic District (NR-0908) (IHSSI No. 41000-106).

A historian who meets the Secretary of the Interior's Professional Qualification Standards identified and evaluated above-ground resources within the APE for potential eligibility for the NRHP. As a result of the historic property identification and evaluation efforts, one previously surveyed above-ground resource, the Akin Park Historic District (IHSSI No. 163-196-430[01-213]) is recommended as eligible for listing in the NRHP.

With regards to archaeological resources, Andrew Martin, a qualified professional archaeologist, reviewed the proposed project area and determined the proposed intersection improvement project in Vanderburgh County will not likely affect archaeological resources due to the project setting. The project will largely be within existing disturbed right-of-way, with less than 0.5 acres of new right-of-way expected to be needed. Furthermore, the soils throughout the project area have likely been disturbed by earth moving associated with a series of construction and demolition episodes occurring around the intersection during the late twentieth and early twenty-first centuries, as well as previous maintenance and improvements to the intersection itself and the installation and maintenance of buried utilities. Surficial evidence of these disturbances were noted during a visual inspection of the project area.

The project area does not appear to have been previously surveyed for archaeological resources. However, according to the State Historic Architectural and Archaeological Database (SHAARD), two previously recorded archaeological sites (12Vg1824 and 12Vg1825) are located within or adjacent to the project area. Site 12Vg1824 is a historic industrial site associated with machinist, elevator, and hot water apparatus manufacturing operations. Site 12Vg1825 is a historic lumber mill site. Both of these sites were recorded by researchers at Indiana State University. Based solely on historic map data and given the extent of subsequent disturbances at this location, it is unlikely that any intact archaeological deposits associated with these sites remain.

A review of historic map data revealed that an abandoned interurban line may transect the project area. This line followed Washington Avenue, running east and west, and is mapped within the roadway. It was established by the Evansville Railroad Company in 1866 and operated until 1939. At the time of abandonment, the line was owned by Southern Indiana Gas and Electric Co. An abandoned segment of the Illinois Central Railroad also transects the project area, extending north-south along the west side of US 41. What remained of the track bed at this location has been repurposed as a trail for cyclist and pedestrian use.

Given the disturbed character of the project area and its correspondingly low potential to contain intact archaeological deposits, it is recommended that the project be allowed to proceed without additional archaeological study. However, state law (Indiana Code 14-21-1-27 and -29) requires that if any prehistoric or historic archaeological artifacts or human remains are uncovered during construction, demolition, or earth moving activities, that the discovery must be reported to the Department of Natural Resources within two (2) business days.

On September 9, 2021, SHPO staff responded to the ECL. A Dual Review has been commenced for this project.

The Eastern Shawnee Tribe responded to the ECL on October 4, 2021, accepting consulting party status. In addition, they stated, "...should this project inadvertently discover an archeological site or object(s) we request that you immediately contact the Eastern Shawnee Tribe, as well as the appropriate state agencies (within 24 hours). We also ask that all ground disturbing activity stop until the Tribe and State agencies are consulted. Please note that any future changes to this project will require additional consultation."

The Historic Property Short Report is available for review in IN SCOPE at <http://erms.indot.in.gov/Section106Documents/> (the Des. No. is the most efficient search term, once in IN SCOPE). You are invited to review this document and to respond with comments on any historic resource impacts incurred as a result of this project so that an environmental report can be completed. We also welcome your related opinions and other input to be considered in the preparation of the

environmental document. If you prefer a hard-copy of this material, please respond to this email with your request as soon as you can.

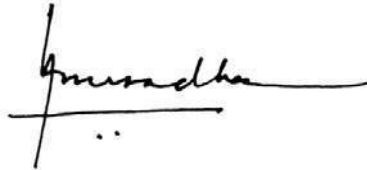
Please review the information and comment within thirty (30) calendar days of receipt. If you indicate that you do not desire to be a consulting party or if you have not previously accepted consulting party status and you do not respond to this letter, you will not be included on the list of consulting parties for this project and will not receive further information about the project unless the design changes. Tribal consulting parties may enter the process at any time and are encouraged to respond to this notification with any comments or concerns at their earliest convenience.

For questions concerning specific project details, you may contact Alyssa Reynolds of Cultural Resource Analysts, Inc. (CRA) at (812) 253-3009 or adreynolds@crai-ky.com. All future responses regarding the proposed project should be forwarded to CRA at the following address:

Alyssa Reynolds
Architectural Historian
Cultural Resource Analysts, Inc. (CRA)
201 NW 4th Street, Suite 204
Evansville, Indiana 47708
adreynolds@crai-ky.com

Tribal contacts may contact Shaun Miller at smiller@indot.in.gov or 317-416-0876 Kari Carmany-George at FHWA at K.CarmanyGeorge@dot.gov or 317-226-5629.

Sincerely,



Anuradha V. Kumar, Manager
Cultural Resources Office
Environmental Services

Enclosures: Distribution List, USGS Topographic map, and Aerial View Map.

The topographic map and aerial view map have been removed from the RDL. Please see Appendix A: Maps of the the 800.11. The SHPO's response to the ECL has been removed from the RDL. Please see Appendix D: Correspondence for the SHPO response.

Accepted Consulting Parties

**Indiana Department of Natural Resources, Division of Historic Preservation & Archaeology,
Indiana State Historic Preservation Office (SHPO)**

402 W. Washington St., Room W274
Indianapolis, Indiana 46204

Eastern Shawnee Tribe



January 7, 2022

Alyssa Reynolds
Architectural Historian
Cultural Resource Analysts, Inc.
201 NW 4th Street, Suite 204
Evansville, Indiana 47708

Federal Agency: Indiana Department of Transportation (“INDOT”),
on behalf of Federal Highway Administration, Indiana Division (“FHWA”)

Re: DUAL REVIEW: Historic property report (Reynolds, 11/22/2021) for the US 41 at Washington Avenue
Intersection Improvement (Des. No. 2000186; DHPA No. 27933)

Dear Ms. Reynolds:

Pursuant to Section 106 of the National Historic Preservation Act of 1966, as amended (54 U.S.C. § 306108); implementing regulations at 36 C.F.R. Part 800; the “Programmatic Agreement (PA) Among the Federal Highway Administration, the Indiana Department of Transportation, the Advisory Council on Historic Preservation and the Indiana State Historic Preservation Officer Regarding the Implementation of the Federal Aid Highway Program In the State of Indiana” (“Indiana Minor Projects PA”); and also pursuant to Indiana Code 14-21-1-18 and 312 Indiana Administrative Code (“IAC”) 20-4, the staff of the Indiana State Historic Preservation Officer (“Indiana SHPO”) has reviewed your December 13, 2021, submission which enclosed the historic property report (“HPR”; Reynolds, 11/22/2021) for the aforementioned project in Evansville, Vanderburgh County, Indiana.

The area of potential effects (“APE”) proposed in the HPR appears to be of adequate size to encompass the geographic area in which direct and indirect effects of a project of this nature could occur.

We agree with the conclusions of the HPR that the Lincolnshire Historic District (NR-0908) is listed in the National Register of Historic Places (“NRHP”) and is located within the project’s APE. We also agree that the Akin Park Historic District (Indiana Historic Sites & Structures Inventory # 163-196-43001-213) is eligible for inclusion in the NRHP.

In regard to the residential area north of Washington Avenue, bounded on the west by South Kerth Avenue, on the north by East Gum Street and on the east by US 41 that was evaluated to determine its potential as a mid-century historic district, we agree with the conclusions of the HPR that this area is not eligible for inclusion in the NRHP under *the Residential Planning and Development in Indiana, 1940-1973 Multiple Property Documentation Form* (MPDF). However, we have taken note of the nearby Bayard Park Historic District (NR-0756), which was listed in the NRHP on June 27, 1985, but is located outside of this project’s area of potential effects. In reviewing the area surrounding the district, we believe that if the nomination for the Bayard Park Historic District was ever updated and its period of significance expanded, it is probable that its eastern boundary would be extended farther to the east. However, for the purposes of the dual review of this undertaking, we agree that the Lincolnshire Historic District and the Akin Park Historic District are the only historic properties located within the project’s APE.

As previously indicated, in terms of archaeological resources, based on the submitted information and the documentation available to the staff of the Indiana SHPO, it is our understanding that portions of the proposed project area lie within portions of two previously recorded industrial archaeological sites--12-Vg-1824 (a historical industrial site associated with machinist, elevator, and hot water apparatus manufacturing operations) and 12-Vg-1825 (a historical lumber mill site)--and within portions of an abandoned interurban line (Evansville Railroad Company/Southern Indiana Gas and Electric Co.; established in 1866, and operated until 1939) and an abandoned segment of the Indiana Central Railroad. However, it is our understanding, from the submission and from

additional/clarifying information contained in the September 8, 2021, e-mail message from Andy V. Martin (CRA, Inc.) to Wade T. Tharp (Indiana DNR-DHPA) that modern development (i.e., recent building construction, utilities installation, and grading related to road and ditch construction) within the areas of the previously recorded industrial archaeological sites are likely to have negatively affected the integrity of any extant archaeological deposits.

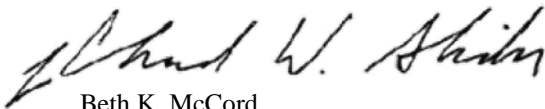
If any prehistoric or historic archaeological artifacts or human remains are uncovered during construction, demolition, or earthmoving activities, state law (Indiana Code 14-21-1-27 and -29) requires that the discovery be reported to the Indiana SHPO within two (2) business days. In that event, please call (317) 232-1646. Be advised that adherence to Indiana Code 14-21-1-27 and -29 does not obviate the need to adhere to applicable federal statutes and regulations, including but not limited to 36 C.F.R. Part 800.

For the benefit of those recipients of a copy of this letter who are not Section 106 consulting parties, please be aware that a copy of this submission can be found online at <http://erms.indot.in.gov/Section106Documents/>. From there, search by this project's designation number: 2000186. We note that a different project (Des. No. 1902709; DHPA No. 26884) involving the construction of a pedestrian bridge over US 41 at the same intersection has recently concluded the dual review process.

If you have questions regarding our dual review of the aforementioned project, please contact DNR-DHPA. Questions about archaeological issues should be directed to contact Wade T. Tharp at (317) 232-1650 or wtharp1@dnr.in.gov. Questions about historic buildings or structures pertaining to this review should be directed to Caitlin Lehman at (317) 232-0461 or clehman1@dnr.IN.gov or Danielle Kauffmann at (317) 232-0582 or dkauffmann@dnr.in.gov.

In all future correspondence about the US 41 at Washington Avenue Intersection Improvement project in Vanderburgh County (Des. No. 2000186), please refer to DHPA No. 27933.

Very truly yours,



Beth K. McCord
Deputy State Historic Preservation Officer

BKM:CML:DMK:cml

EMC to federal/state agency or consultant staff members:

Kari Carmany-George, FHWA
Anuradha Kumar, INDOT
Shaun Miller, INDOT
Susan Branigin, INDOT
Anthony Ross, INDOT
Alyssa Reynolds, CRA Inc.
Wade Tharp, Indiana DNR-DHPA
Danielle Kauffmann, Indiana DNR-DHPA
Caitlin Lehman, Indiana DNR-DHPA

EMC to Review Board Members:

J. Scott Keller, Review Board
Daniel Kloc, AIA, Review Board
Jason Larrison, AIA, Review Board
Chandler Lighty, Review Board
Anne Shaw, Review Board
April Sievert, Ph.D., Review Board
Beth K. McCord, DNR-DHPA, Review Board
Ryan Mueller, Deputy Director, DNR, and Chairman, Review Board

EMC to Potentially Interested Persons:

Absentee Shawnee Tribe of Oklahoma
Eastern Shawnee Tribe of Oklahoma
Delaware Tribe of Indians, Oklahoma
Miami Tribe of Oklahoma
Peoria Tribe of Indians of Oklahoma
Pokagon band of Potawatomi Indians
Shawnee Tribe
United Keetowah Band of Cherokee Indians
Benjamin Bosse HS
Greater Lincolnshire Neighborhood Association
Charles Farmer, Traffic Engineering Foreman

Downtown Evansville, Inc
Economic Development Coalition of Southwest Indiana
Evansville Historic Preservation Commission
Evansville Metropolitan Planning Organization
Evansville Vanderburgh School Corporation
Historic Southern Indiana, Leslie Townsend
Indiana Landmarks Southwest Field Office
City of Evansville Historic Preservation Officer
Old Evansville Historic Association
Dennis Au, Preservation Alliance of Evansville
Brent Jackson, Presidents Neighborhood Association
Jeff Hatfield, Vanderburgh County Commissioner
Cheryl Musgrave, Vanderburgh County Commissioner
Ben Shoulders, Vanderburgh County Commissioner
Vanderburgh County Highway Engineer
Vanderburgh County Highway Superintendent
Vanderburgh County Historian
Vanderburgh County Historical Society
Honorable Lloyd Winnecke. Mayor of Evansville

Alyssa Reynolds

From: Alyssa Reynolds <adreynolds@crai-ky.com>
Sent: Monday, May 16, 2022 3:22 PM
To: dhpareview
Cc: Robert Ball; Andrew Martin; 'Ross Nelson'; 'Christine Meador'; 'tenyart'; 'Arnold, Troy'; 'Falls, Ryan G'; 'Branigin, Susan'; 'Carpenter, Patrick A'; Robert Ball; 'Coon, Matthew'
Subject: Dual Review FHWA Project: Des No 2000186; DHPA No 27933; Effects Report; US 41 at Washington Avenue Intersection Improvement, Vanderburgh County, Indiana
Attachments: US 41_Intersection Improvement_Des 2000186_ER_Part 1_2022_5_16.pdf; US 41_Intersection Improvement_Des 2000186_DHPA review form_2022_5_16.pdf

All,

Hard copies of the attached will be mailed to you by the end of the week. A second email will follow with Part 2 of the ER as it was too large to send all together.

Des. No.: 2000186

Project Description: Intersection Improvement at US 41 and Washington Avenue

Location: Evansville, Knight Township, Vanderburgh County, Indiana

The Indiana Department of Transportation, with funding from the Federal Highway Administration, proposes to proceed with the intersection improvement project at US 41 and Washington Avenue; Des. No. 2000186.

The Section 106 Early Coordination Letter for this project was originally distributed on August 2, 2021. Additionally, a Historic Property Report (HPR) was distributed on December 13, 2021.

As part of Section 106 of the National Historic Preservation Act, an Effects Report has been prepared and is ready for review and comment by consulting parties.

Please review the documentation, which is also located in IN SCOPE at <https://erms.indot.in.gov/Section106Documents/> (the Des. No. is the most efficient search term, once in IN SCOPE), and respond with your comments on any historic resource impacts incurred as a result of this project so that an environmental report can be completed. We also welcome your related opinions and other input to be considered in the preparation of the environmental document. If a hard copy of the materials is needed, please respond to this email with your request within seven (7) days.

Consulting parties have thirty (30) calendar days from receipt of this information to review and provide comments. Tribal consulting parties may enter the process at any time and are encouraged to respond to this notification with any comments or concerns at their earliest convenience. If we do not receive a response from an invited consulting party within the time allotted, the project will proceed consistent with the proposed design.

Tribal contacts may contact Patty Jo Korzeniewski at PKorzeniewski@indot.IN.gov or 317-416-4377 or Kari Carmany-George at FHWA at K.CarmanyGeorge@dot.gov or 317-226-5629.

Thanks,

Alyssa Reynolds
Architectural Historian
adreynolds@crai-ky.com

Indiana Office
201 NW 4th Street, Suite 204
Evansville, Indiana 47708
812.253.3009 office
812.253.3010 fax
812.549.4503 cell
<http://www.crai-ky.com>



Celebrating 39 Years in Business!

Alyssa Reynolds

From: Alyssa Reynolds <adreynolds@crai-ky.com>
Sent: Monday, May 16, 2022 3:23 PM
To: 'dhparchive'
Cc: 'Robert Ball'; 'Andrew Martin'; 'Ross Nelson'; 'Christine Meador'; 'tenyart'; 'Arnold, Troy'; 'Falls, Ryan G'; 'Branigin, Susan'; 'Carpenter, Patrick A'; 'Robert Ball'; 'Coon, Matthew'
Subject: RE: Dual Review FHWA Project: Des No 2000186; DHPA No 27933; Effects Report; US 41 at Washington Avenue Intersection Improvement, Vanderburgh County, Indiana
Attachments: US 41_Intersection Improvement_Des 2000186_ER_Part 2_2022_5_16.pdf

All,

Attached above is Part 2 of the ER.

Thank you,

Alyssa Reynolds
Architectural Historian
adreynolds@crai-ky.com

Indiana Office
201 NW 4th Street, Suite 204
Evansville, Indiana 47708
812.253.3009 office
812.253.3010 fax
812.549.4503 cell
<http://www.crai-ky.com>



Celebrating 39 Years in Business!

From: Alyssa Reynolds <adreynolds@crai-ky.com>
Sent: Monday, May 16, 2022 3:22 PM
To: dhparchive <dhparchive@dnr.in.gov>
Cc: Robert Ball <rball@crai-ky.com>; Andrew Martin <amartin@crai-ky.com>; 'Ross Nelson' <rnelson@crai-ky.com>; 'Christine Meador' <CMEador@HNTB.com>; 'tenyart' <tenyart@HNTB.com>; 'Arnold, Troy' <TArnold1@indot.IN.gov>; 'Falls, Ryan G' <RFalls@indot.IN.gov>; 'Branigin, Susan' <SBranigin@indot.IN.gov>; 'Carpenter, Patrick A' <PACarpenter@indot.IN.gov>; Robert Ball <rball@crai-ky.com>; 'Coon, Matthew' <mcoon@indot.IN.gov>
Subject: Dual Review FHWA Project: Des No 2000186; DHPA No 27933; Effects Report; US 41 at Washington Avenue Intersection Improvement, Vanderburgh County, Indiana

All,

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Thanks,

Alyssa Reynolds
Architectural Historian
adreynolds@crai-ky.com

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<http://www.crai-ky.com>



Celebrating 39 Years in Business!



June 7, 2022

Alyssa Reynolds
Architectural Historian
Cultural Resources Associates, Inc. (CRA)
201 NW 4th Street, Suite 204
Evansville, Indiana 47708

State Agency: Indiana Department of Transportation (“INDOT”),
Federal Agency: Federal Highway Administration, Indiana Division (“FHWA”)

Re: DUAL REVIEW: Effects report for the US 41 at Washington Avenue Intersection Improvement
(Des. No. 2000186; DHPA No. 27933)

Dear Ms. Reynolds:

Pursuant to Section 106 of the National Historic Preservation Act of 1966, as amended (54 U.S.C. § 306108); implementing regulations at 36 C.F.R. Part 800; the “Programmatic Agreement (PA) Among the Federal Highway Administration, the Indiana Department of Transportation, the Advisory Council on Historic Preservation and the Indiana State Historic Preservation Officer Regarding the Implementation of the Federal Aid Highway Program In the State of Indiana” (“Indiana Minor Projects PA”); and also pursuant to Indiana Code 14-21-1-18 and 312 Indiana Administrative Code (“IAC”) 20-4, the staff of the Indiana State Historic Preservation Officer (“Indiana SHPO”) has reviewed your May 16, 2022 effects report, received by our office the same day, for the aforementioned project in Evansville, Vanderburgh County, Indiana.

As previously indicated, we agree that the Lincolnshire Historic District (NR-0908) and the Akin Park Historic District (Indiana Historic Sites and Structures Inventory [“IHSSI”] # 163-196-43001-213) are the only historic properties located within the project’s area of potential effects. Additionally, we agree with the conclusions in the effects report that the project as proposed will not adversely affect either of these historic properties.

Furthermore, as previously indicated, in terms of archaeological resources, based on the submitted information and the documentation available to the staff of the Indiana SHPO, it is our understanding that portions of the proposed project area lie within portions of two previously recorded industrial archaeological sites--12-Vg-1824 (a historical industrial site associated with machinist, elevator, and hot water apparatus manufacturing operations) and 12-Vg-1825 (a historical lumber mill site)--and within portions of an abandoned interurban line (Evansville Railroad Company/Southern Indiana Gas and Electric Co.; established in 1866, and operated until 1939) and an abandoned segment of the Indiana Central Railroad. However, it is our understanding, from the submission and from additional/clarifying information contained in the September 8, 2021, e-mail message from Andy V. Martin (CRA, Inc.) to Wade T. Tharp (Indiana DNR-DHPA) that modern development (i.e., recent building construction, utilities installation, and grading related to road and ditch construction) within the areas of the previously recorded industrial archaeological sites are likely to have negatively affected the integrity of any extant archaeological deposits.

If any prehistoric or historic archaeological artifacts or human remains are uncovered during construction, demolition, or earthmoving activities, state law (Indiana Code 14-21-1-27 and Indiana Code 14-21-1-29) requires that the discovery be reported to the Indiana Department of Natural Resources, Division of Historic Preservation and Archaeology within two (2) business days. In that event, please call (317) 232-1646. Be advised that adherence to Indiana Code 14-21-1-27 and Indiana Code 14-21-1-29 does not obviate the need to adhere to applicable federal statutes and regulations, including but not limited to 36 C.F.R. Part 800.

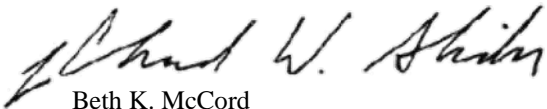
Unless another consulting party expresses a different opinion about this project's effects, it might now be appropriate for INDOT to make a finding for this undertaking, at which time we will decide whether it is appropriate to issue a Director's Letter of Clearance for the project, indicating compliance with Indiana Code 14-21-1-18.

For the benefit of those recipients of a copy of this letter who are not Section 106 consulting parties, please be aware that a copy of this submission can be found online at <http://erms.indot.in.gov/Section106Documents/>. From there, search by this project's designation number: 2000186. Anyone receiving an e-mailed copy of this letter who does not wish to receive future copies of our correspondence about this project is asked to reply to clehman1@dnr.IN.gov and so advise us.

If you have questions regarding our dual review of the aforementioned project, please contact DNR-DHPA. Questions about archaeological issues should be directed to contact Wade T. Tharp at (317) 232-1650 or wtharp1@dnr.IN.gov. Questions about historic buildings or structures pertaining to this review should be directed to Caitlin Lehman at (317) 232-0461 or clehman1@dnr.IN.gov or Danielle Kauffmann at (317) 232-0582 or dkauffmann@dnr.IN.gov.

In all future correspondence regarding the dual review of this project in Evansville, Vanderburgh County (Des. No. 2000186), please refer to DHPA No. 27933.

Very truly yours,



Beth K. McCord
Deputy State Historic Preservation Officer
Director, Division of Historic Preservation and Archaeology

BKM:CML:DMK:WTT:cml

EMC to federal and state agency or consultant staff members:

Kari Carmany-George, FHWA
Matt Coon, INDOT
Susan Branigin, INDOT
Alyssa Reynolds, CRA, Inc.
Danielle Kauffmann, Indiana DNR-DHPA
Caitlin Lehman, Indiana DNR-DHPA
Wade T. Tharp, Indiana DNR-DHPA

EMC to Indiana Historic Preservation Review Board Members:

J. Scott Keller, Review Board
Daniel Kloc, AIA, Review Board
Jason Larrison, AIA, Review Board
Chandler Lighty, Review Board
Beth K. McCord, Indiana DNR-DHPA, Review Board
Ryan Mueller, Deputy Director, Indiana DNR, and Chairman, Review Board
Anne Shaw, Review Board
April Sievert, Ph.D., Review Board

EMC to potentially interested persons:

Absentee Shawnee Tribe of Oklahoma
Eastern Shawnee Tribe of Oklahoma
Delaware Tribe of Indians, Oklahoma
Miami Tribe of Oklahoma
Peoria Tribe of Indians of Oklahoma
Pokagon band of Potawatomi Indians
Shawnee Tribe
United Keetowah Band of Cherokee Indians
Benjamin Bosse HS
Greater Lincolnshire Neighborhood Association
Charles Farmer, Traffic Engineering Foreman
Downtown Evansville, Inc.
Economic Development Coalition of Southwest Indiana
Evansville Historic Preservation Commission
Evansville Metropolitan Planning Organization
Evansville Vanderburgh School Corporation
Historic Southern Indiana, Leslie Townsend
Danielle Bachant-Bell, Indiana Landmarks Southwest Field Office
City of Evansville Historic Preservation Officer

Alyssa Reynolds

June 7, 2022

Page 3

Old Evansville Historic Association
Dennis Au, Preservation Alliance of Evansville
Brent Jackson, Presidents Neighborhood Association
Jeff Hatfield, Vanderburgh County Commissioner
Cheryl Musgrave, Vanderburgh County Commissioner
Ben Shoulders, Vanderburgh County Commissioner
Vanderburgh County Highway Engineer
Vanderburgh County Highway Superintendent
Vanderburgh County Historian Stan Schmitt
Vanderburgh County Historical Society - Terry Hughes
Honorable Lloyd Winnecke, Mayor of Evansville

Section 800 Documentation

APPENDIX E: REPORT ABSTRACT

HISTORIC PROPERTY REPORT FOR THE PROPOSED
US 41 AT WASHINGTON AVENUE INTERSECTION
IMPROVEMENT PROJECT IN KNIGHT TOWNSHIP,
VANDERBAUGH COUNTY, INDIANA
(INDOT DES. NO. 2000186; DHPA NO. 27933)



by
Alyssa Reynolds, MS

Prepared for

HNTB Corporation



Prepared by



Kentucky | West Virginia | Wyoming
Indiana | Louisiana | Tennessee | Virginia

**HISTORIC PROPERTY REPORT FOR THE PROPOSED
US 41 AND WASHINGTON AVENUE INTERSECTION
IMPROVEMENT PROJECT IN KNIGHT TOWNSHIP,
VANDERBURGH COUNTY, INDIANA
(INDOT DES. NO. 2000186; DHPA NO. 27933)**

by

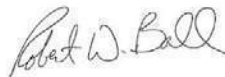
Alyssa Reynolds, MS

Prepared for

Christine Meador
HNTB Corporation
111 Monument Circle, Suite 1200
Indianapolis, Indiana
Phone: (317) 917-5338
Email: cmeador@hntb.com

Prepared by

Cultural Resource Analysts, Inc.
201 NW 4th Street, Suite 204
Evansville, Indiana 47708
E-mail: amartin@crai-ky.com
Phone: (812) 253-3009
Fax: (812) 253-3010
CRA Project No.: I210156



Robert Ball, MHP
Principal Investigator

November 22, 2021

INDOT Des. No.: 2000186
DHPA No.: 27933

ABSTRACT

This report documents the identification and evaluation efforts for properties included in the Area of Potential Effects (APE) for the proposed US 41 and Washington Avenue Intersection Improvement project in Vanderburgh County, Indiana (Indiana Department of Transportation [INDOT] Des. No. 2000186). Above-ground resources located within the project APE were identified and evaluated in accordance with Section 106 of the National Historic Preservation Act (NHPA) of 1966, as amended, and the regulations implementing Section 106 (36 CFR Part 800).

As a result of the NHPA, as amended, and CFR Part 800, federal agencies are required to take into account the impact of federal undertakings upon historic properties in the area of the undertaking. Historic properties include buildings, structures, sites, objects, and/or districts that are eligible for or listed in the National Register of Historic Places (NRHP). As this project is receiving funding from the Federal Highway Administration (FHWA), it is subject to a Section 106 review.

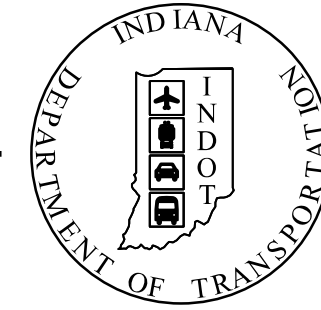
The APE contains one historic district listed in the NRHP, the Lincolnshire Historic District (NR-0908). The APE contains one additional historic district that is recommended eligible for listing in the NRHP – the Akin Park Historic District (IHSSI No. 163-169-430[01-213]).

Section 800 Documentation

APPENDIX F: PLANS

PROJECT	DESIGNATION
2000186	2000186
CONTRACT	
B-42876	

INDIANA DEPARTMENT OF TRANSPORTATION



TRAFFIC DATA	U.S. 41	WASHINGTON AVE.
A.A.D.T. (2023)	30,200 V.P.D.	12,900 V.P.D.
A.A.D.T. (2044)	37,100 V.P.D.	13,100 V.P.D.
D.H.V. (2044)	2,970 V.P.H.	1,180 V.P.H.
DIRECTIONAL DISTRIBUTION	59.00%	57.00%
TRUCKS	11.00% OF AADT	2.00% OF AADT
	1.00% OF DHV	1.00% OF DHV

DESIGN DATA	U.S. 41	WASHINGTON AVE.
DESIGN SPEED	40 MPH	30 MPH
PROJECT DESIGN CRITERIA	3R NON-FREEWAY	3R NON-FREEWAY
FUNCTIONAL CLASSIFICATION	PRINCIPAL ARTERIAL	MINOR ARTERIAL
RURAL/URBAN	URBAN	URBAN
TERRAIN	LEVEL	LEVEL
ACCESS CONTROL	PARTIAL	PARTIAL

ROAD PLANS

US 41 AND WASHINGTON AVENUE INTERSECTION IMPROVEMENT

ROUTE: U.S. 41 FROM: RP 2+82 TO: RP 2+89

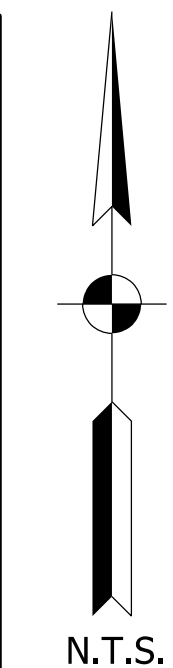
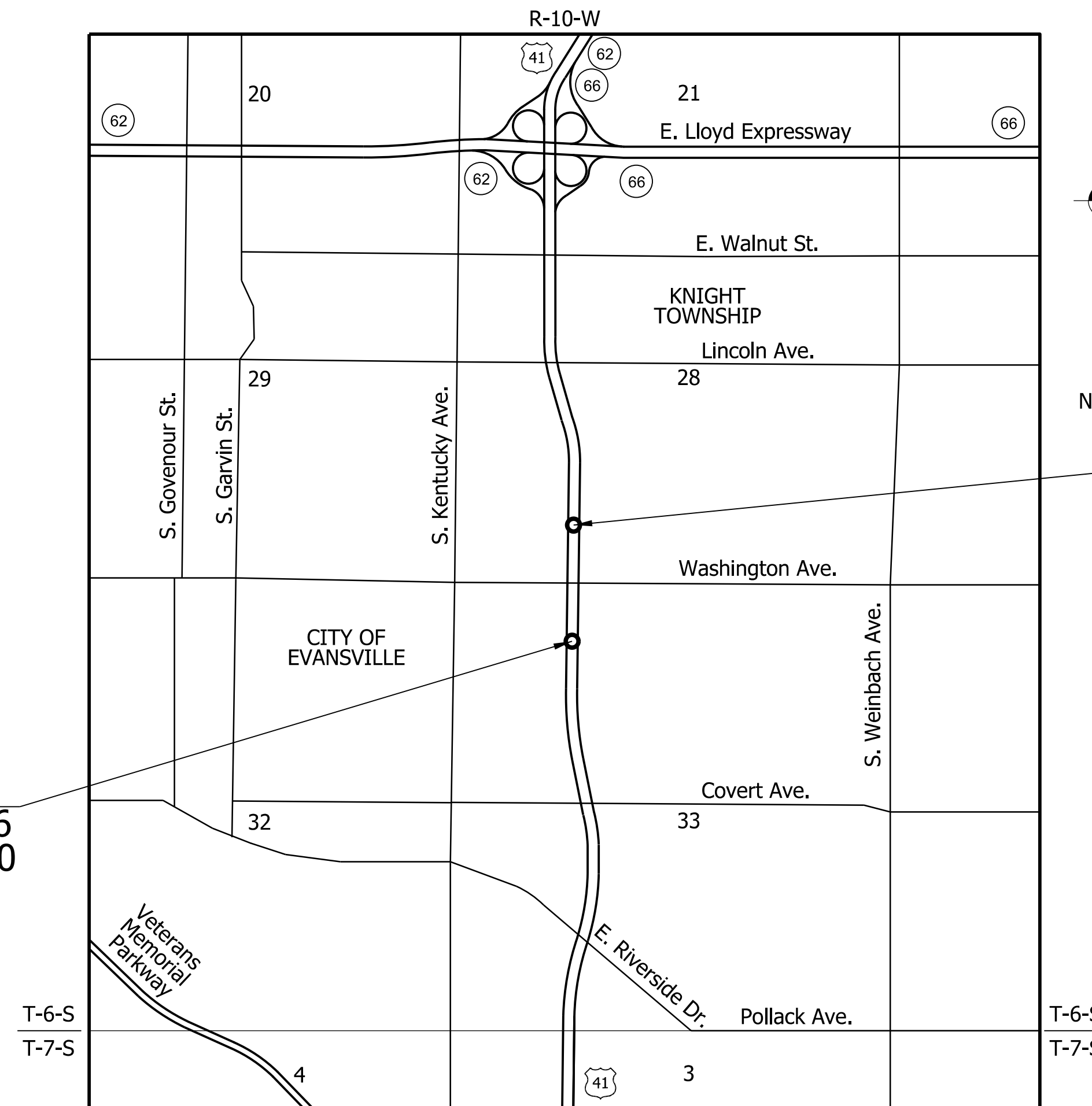
PROJECT NO. 2000186 P.E.
2000186 CONST.

KIN PROJECT INFORMATION		
DESIGNATION	PROJECT DESCRIPTION	LEAD DES
1902709	PEDESTRIAN BRIDGE OVER US 41	
2000186	US 41 AND WASHINGTON AVENUE INTERSECTION IMPROVEMENT	

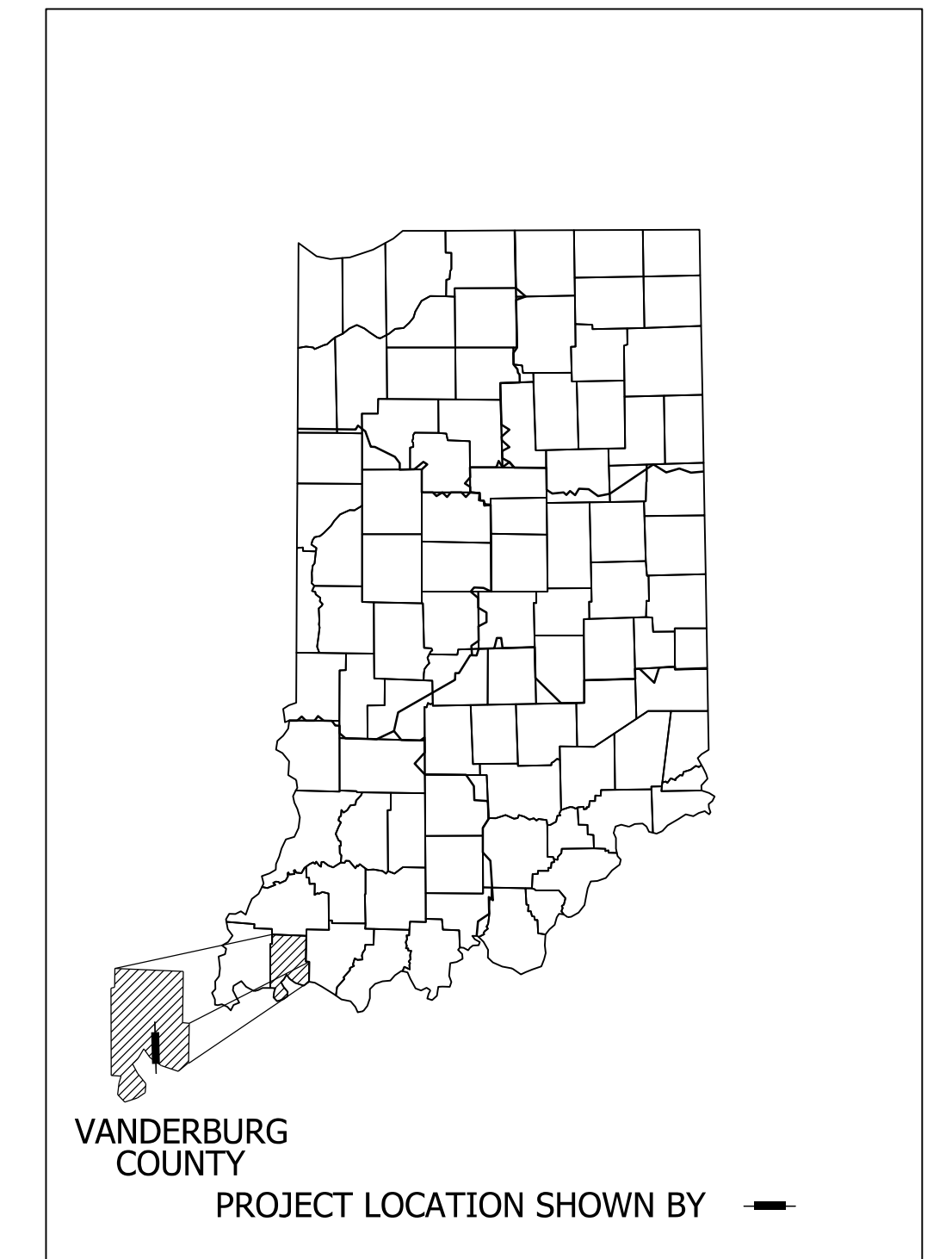
PROJECT DESCRIPTION
INTERSECTION IMPROVEMENTS AND ROADWAY MODIFICATIONS ON U.S. 41 AND WASHINGTON AVENUE APPROXIMATELY 1.00 MILES SOUTH OF THE U.S. 41/E. LLOYD EXPRESSWAY IN SECTION 28 & 33, T-6-S, R-10-W, CITY OF EVANSVILLE, KNIGHT TOWNSHIP, VANDERBURGH COUNTY, INDIANA.

STAGE 2 REVIEW PLANS
APRIL 1, 2022

BEGIN PROJECT
PROJECT NO. 2000186
P.O.C. STA. 273+00.00
LINE "B"



END PROJECT
PROJECT NO. 2000186
P.O.T. STA. 293+95.00
LINE "B"



LATITUDE: 37°57'46"N LONGITUDE: 87°32'34"W

GROSS LENGTH: 0.397 MI.
NET LENGTH: 0.397 MI.
MAX. GRADE: 0.73%

HYDROLOGIC UNIT CODE: 05140202010030

INDIANA DEPARTMENT OF TRANSPORTATION
STANDARD SPECIFICATIONS DATED 2022
TO BE USED WITH THESE PLANS

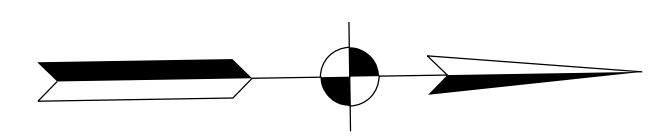


THE HNTB COMPANIES
INFRASTRUCTURE SOLUTIONS
111 MONUMENT CIRCLE
SUITE 1200
INDIANAPOLIS, IN 46204-5178

DRAFT
NOT FOR CONSTRUCTION

PLANS PREPARED BY: HNTB INDIANA, INC. (317) 636-4682 PHONE NUMBER
CERTIFIED BY: _____ DATE
APPROVED FOR LETTING: _____ DATE
INDIANA DEPARTMENT OF TRANSPORTATION

DESIGNATION	
2000186	
SURVEY BOOK	SHEETS
ELECTRONIC	1 of 45
CONTRACT	PROJECT
B-42876	2000186



270+00

275+00

280+00

285+00

290+00

295+00

INDEX OF OWNERS

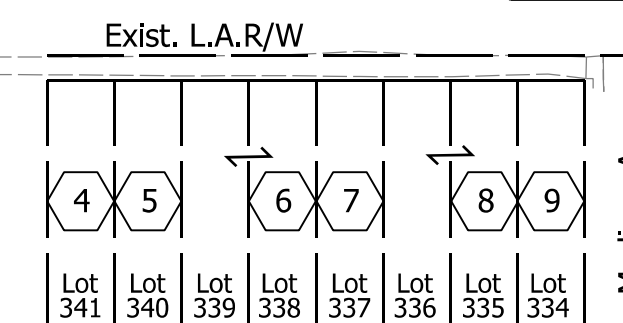
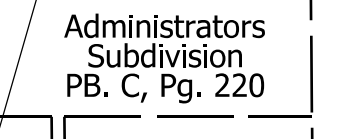
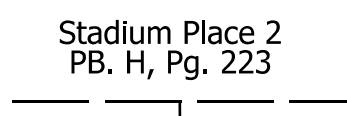
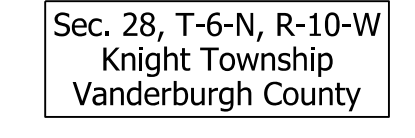
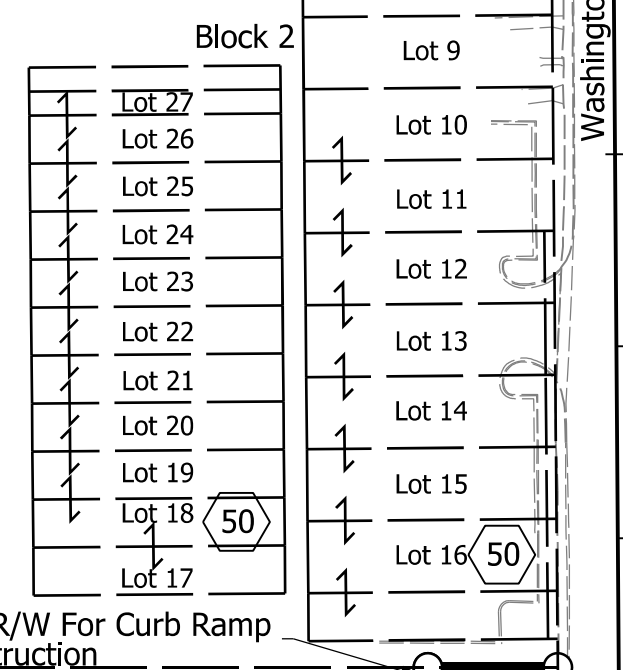
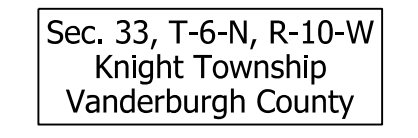
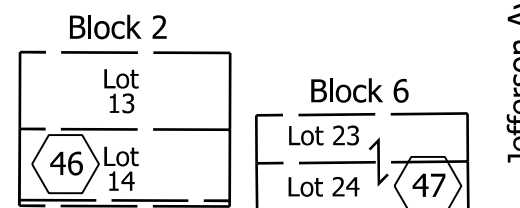
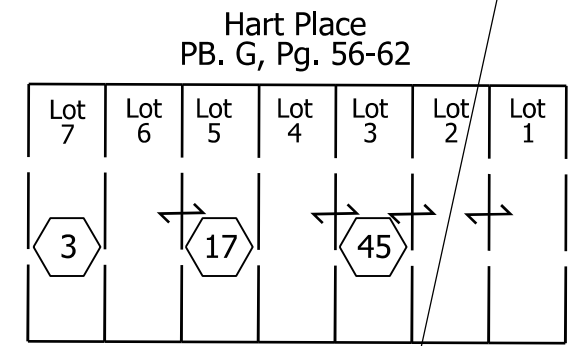
- 3 LIBS, LAURA B.
- 17 SEWARD, ELIZABETH
- 45 LEWIS, MARVIN E. & TRACY Y.
- 46 JOHNSON, JOHN & COOPER, FARESTINE
- 47 HAWKINS, LAWANDA J.
- 48 RHODES, JOYCE
- 49 WHEELER, STANLEY
- 50 WARREN WASHINGTON AVENUE PROPERTY LLC

INDEX OF OWNERS

- 36 SYSTEM CAPITAL REAL PROPERTY CORPORATION 13-0681
- 37 FLINT CREEK PROPERTIES LLC GOEBEL COMMERCIAL REALTY
- 38 MCGLOTHEN, KELLY
- 39 WADDELL, EVIE C.
- 40 OCHOA, MIGUEL & NERI, NICOLASA H/W
- 41 M A VALLEY INC.
- 42 SOUTHERN INDIANA GAS & ELECTRIC CO
- 43 CHAVOUS, STEPHANIE

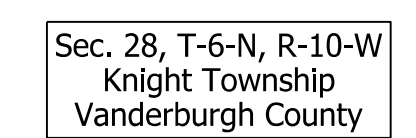
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P.O.C. STA. 273+00.00
LINE "B"
N: 176331.32
E: 789440.14

END PROJECT
PROJECT NO. 1800226
P.O.T. STA. 293+95.00
LINE "B"
N: 178426.20
E: 789457.38



INDEX OF OWNERS

- 4 TENNYSON, THOMAS K.
- 5 GIBSON, RATHEN
- 6 REISINGER, RONALD E. & MARY LOU
- 7 REISINGER, RONALD E. & MARY LOU
- 8 REISINGER, RONALD E. & MARY LOU
- 9 REISINGER, RONALD E. & MARY LOU
- 10 JONES, BRITTANY HOLLOWAY & KILEY HILTON W/H
- 11 HADFIELD HOLDINGS LLC
- 12 SHREE LAXME LLC
- 13 BROCKMAN RANDY J.
- 14 LOWE, W WALT & D LYNN TRUSTEES T/C
- 15 SHARYGIN, ANDREI
- 16 WELCOME MANAGEMENT LLC
- 18 EFFINGER, ANGELA G.
- 19 YODER, WALTER & DEBORAH T/E
- 20 BARRON, THOMAS J. & MARY E.
- 21 PADDOCK, DANNY RAY SR.
- 22 ALEXANDER, JANE E.
- 23 MCDANIEL, KATHY L.
- 24 AMERSON, ELBERT D.
- 25 BONNIE, GENE PATRICK JR. & SHERWOOD, RACHEL
- 26 RUFF, TAMMY
- 27 THOMAS, GORDON S. & BRADFORD, DIXIE K.
- 28 HUSK, KIP N. & SANDRA L. H/W
- 29 MOORE, ROBERT C.
- 2 GRACE OF CHRIST INC.
- 1 IRWIN, LEATRICE & ROBINSON, RAPINA



INDEX OF OWNERS

- 30 SPIRIT MASTER FUNDING VI LLC C/O SPIRIT REALTY CAPITAL
- 31 EVANSVILLE VANDERBURGH SCHOOL CORPORATION
- 32 GLOVER, KATHERINE F.
- 33 SCHLECHTE, KATHLEEN M.
- 34 HODNETT, JAMES C.
- 35 SANDEFUR, MICHAEL & APRIL H/W

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DRAFT
NOT FOR CONSTRUCTION

RECOMMENDED FOR APPROVAL _____	DESIGN ENGINEER _____	DATE _____
DESIGNED: _____ NSX _____	DRAWN: _____ CPH _____	
CHECKED: _____ TNE _____	CHECKED: _____ NSX _____	

INDIANA
DEPARTMENT OF TRANSPORTATION

PLAT NO. 1

HORIZONTAL SCALE	BRIDGE FILE
1"=100'	N/A
VERTICAL SCALE	DESIGNATION
N/A	2000186
SURVEY BOOK	SHEETS PLT-01
ELECTRONIC	5 of 45
CONTRACT	PROJECT
B-42876	2000186

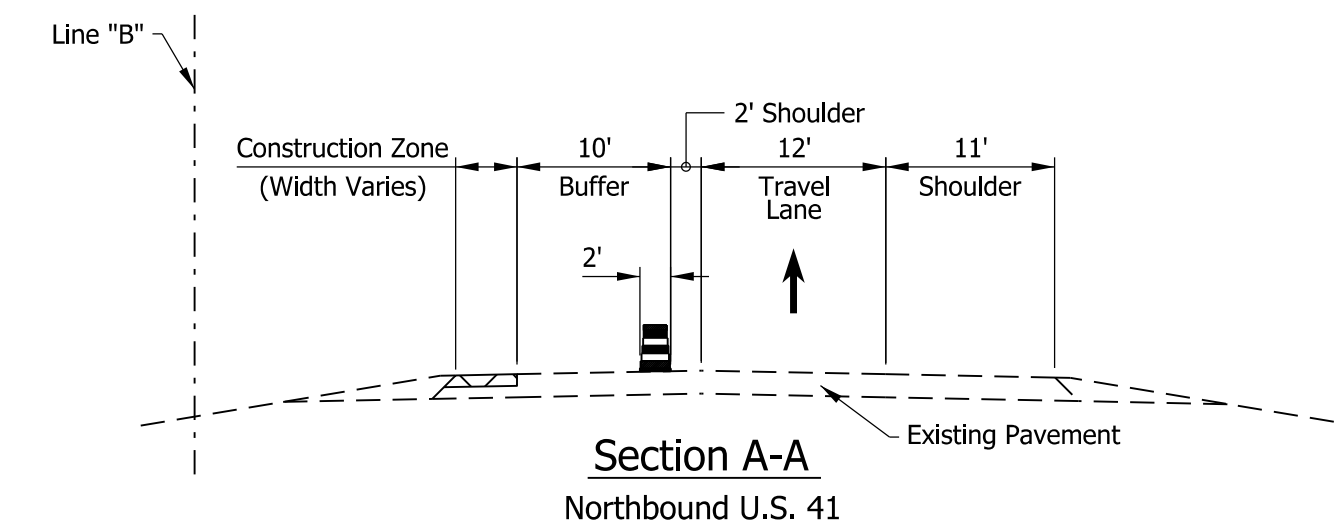
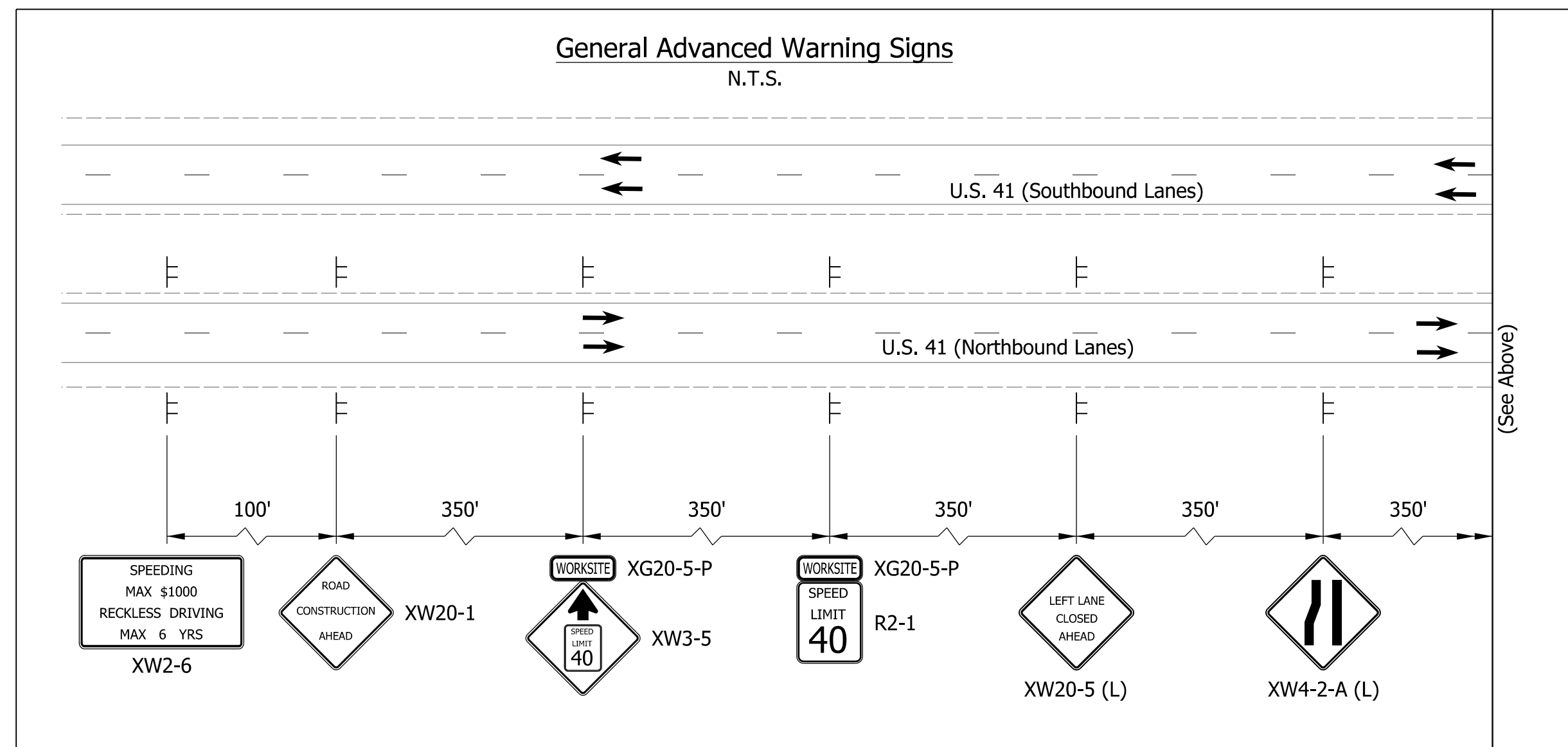
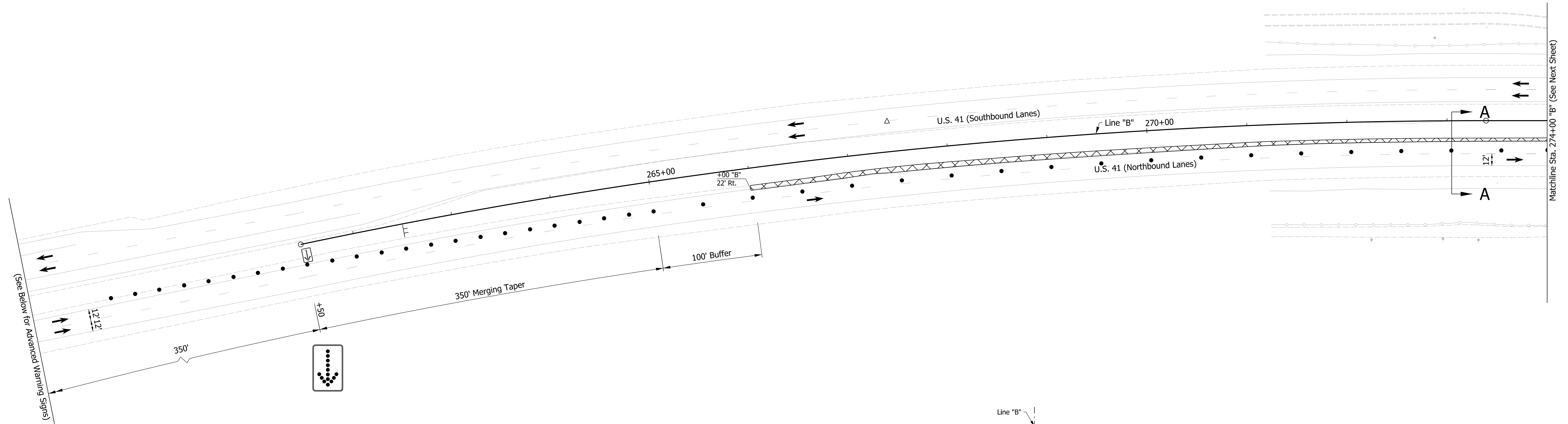
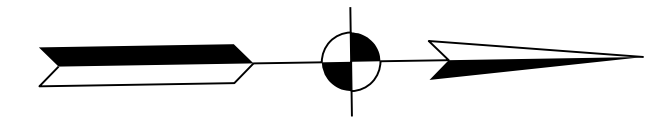
CONSTRUCTION SEQUENCING

PHASE 1:

1. Install Advanced Warning Signs.
2. Place temporary Traffic Drums adjacent to US 41 workzones.
3. Construct the Northbound and Southbound Shoulder Strengthening along US 41.

Construction Design Speed: 40 MPH
 Construction Clear Zone: 13'-0"

Maintenance of Traffic Quantity Summary - Phase 1		
Item	Amount	Unit
Construction Sign A	20	EACH
Construction Sign B	6	EACH
Temporary Worksite Speed Limit Sign Assembly	8	EACH
Flashig Arrow Sign	14	DAY



Notes:

1. Shoulder Strengthening to be completed by Single Lane Closure per SDD E 801-TCLC-11
2. Contractor shall maintain a 1' Minimum Shoulder at all times
3. All Sign Spacing shall be 350' Minimum unless otherwise noted
3. Traffic Drum Spacing along Tapers shall be at 25'
4. Traffic Drum Spacing along Tangents shall be at 50'
5. All temporary Pavement Markings shall be Removable
6. Existing Transverse Rumble Strips to remain in place
7. No changes to the Traffic Signal at US 41 and Washington Ave. Intersection

LEGEND

- | | | | |
|---------------------------|--------------------------------------|---|--|
| Construction Zone | Energy Absorbing Terminal CZ, Type 2 | Temporary Pavement Markings, 6 In., Solid, White | Temporary Transverse Pavement Markings, 24 In., Solid, Stop Line |
| Temporary Pavement | Construction Sign | Temporary Pavement Markings, 6 In., Solid, Yellow | Temporary Transverse Pavement Markings, 24 In., Solid, White |
| Direction of Traffic | Traffic Drum | Temporary Pavement Markings, 6 In., Broken, White | Temporary Pavement Message Marking, Lane Indication Arrow |
| Temporary Traffic Barrier | | | |

DRAFT
NOT FOR CONSTRUCTION

RECOMMENDED FOR APPROVAL _____	DESIGN ENGINEER _____	DATE _____
DESIGNED: _____ NSX _____	DRAWN: _____ HDW _____	
CHECKED: _____ TNE _____	CHECKED: _____ NSX _____	

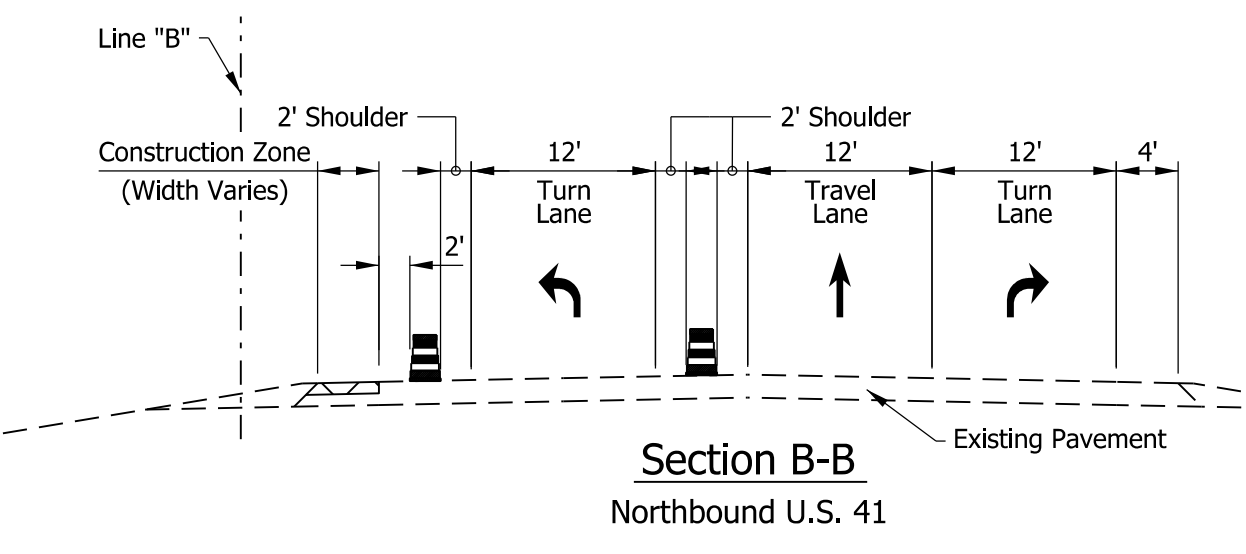
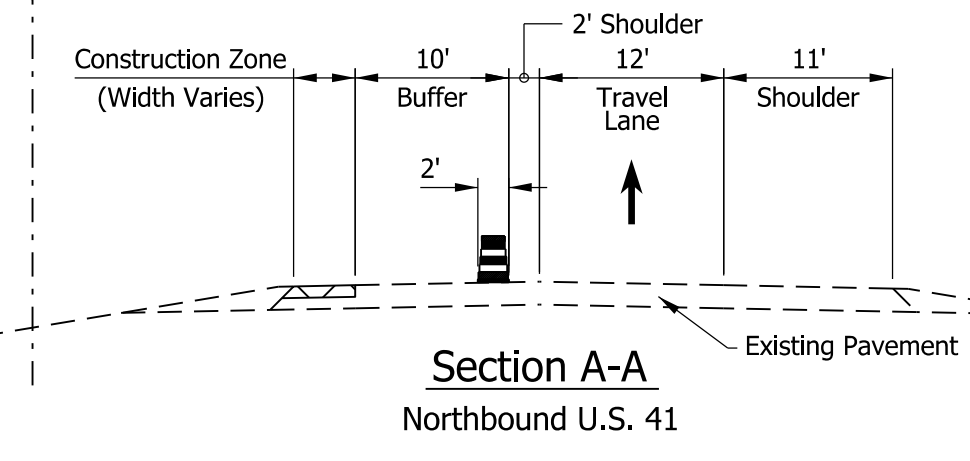
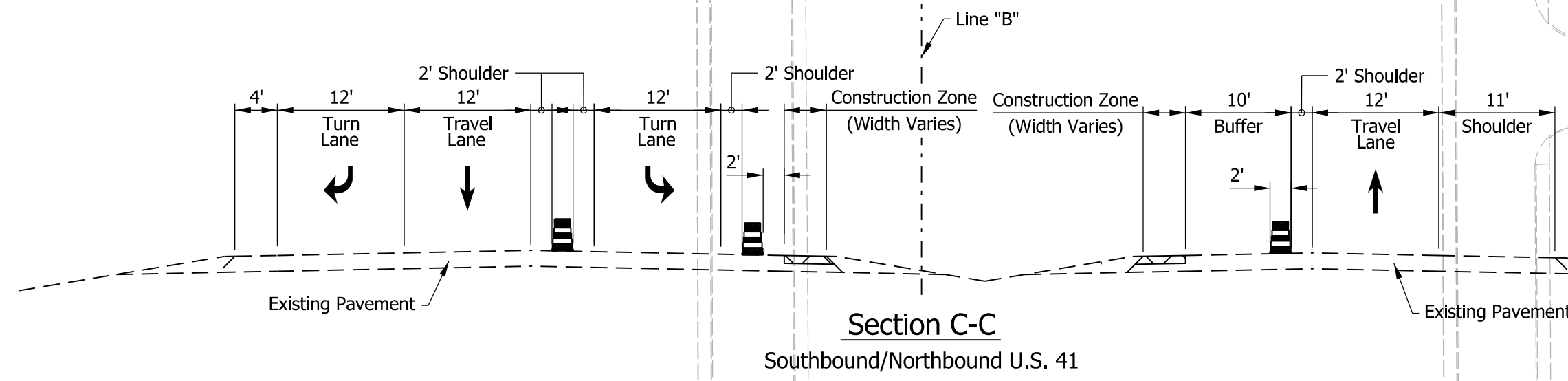
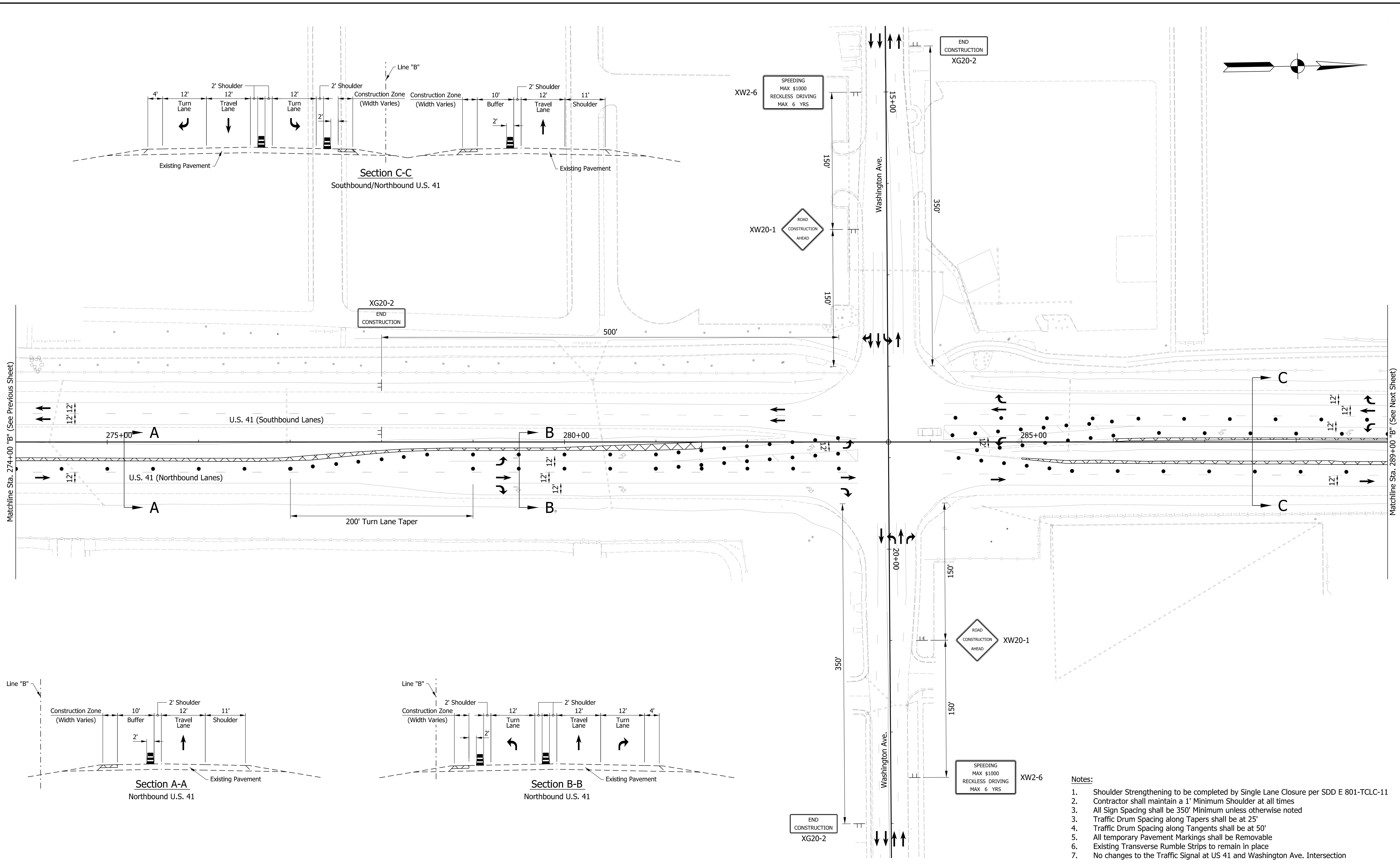
INDIANA DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC PHASE 1

HORIZONTAL SCALE	BRIDGE FILE
1"=50'	N/A
VERTICAL SCALE	DESIGNATION
N/A	2000186
SURVEY BOOK	SHEETS MOT-01
ELECTRONIC	6 of 54
CONTRACT	PROJECT
B-42876	2000186

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 file:c:\adidib\p\wilhelm\greatakes\0286823179067-rd-s-mto1.dgn



- Notes:**
- Shoulder Strengthening to be completed by Single Lane Closure per SDD E 801-TCLC-11
 - Contractor shall maintain a 1' Minimum Shoulder at all times
 - All Sign Spacing shall be 350' Minimum unless otherwise noted
 - Traffic Drum Spacing along Tapers shall be at 25'
 - Traffic Drum Spacing along Tangents shall be at 50'
 - All temporary Pavement Markings shall be Removable
 - Existing Transverse Rumble Strips to remain in place
 - No changes to the Traffic Signal at US 41 and Washington Ave. Intersection

LEGEND	
	Construction Zone
	Temporary Pavement
	Direction of Traffic
	Temporary Traffic Barrier
	Energy Absorbing Terminal CZ, Type 2
	Construction Sign
	Traffic Drum
	Temporary Pavement Markings, 6 In., Solid, White
	Temporary Pavement Markings, 6 In., Solid, Yellow
	Temporary Pavement Markings, 6 In., Broken, White
	Temporary Transverse Pavement Markings, 24 In., Solid, Stop Line
	Temporary Transverse Pavement Markings, 24 In., Solid, White
	Temporary Pavement Message Marking, Lane Indication Arrow

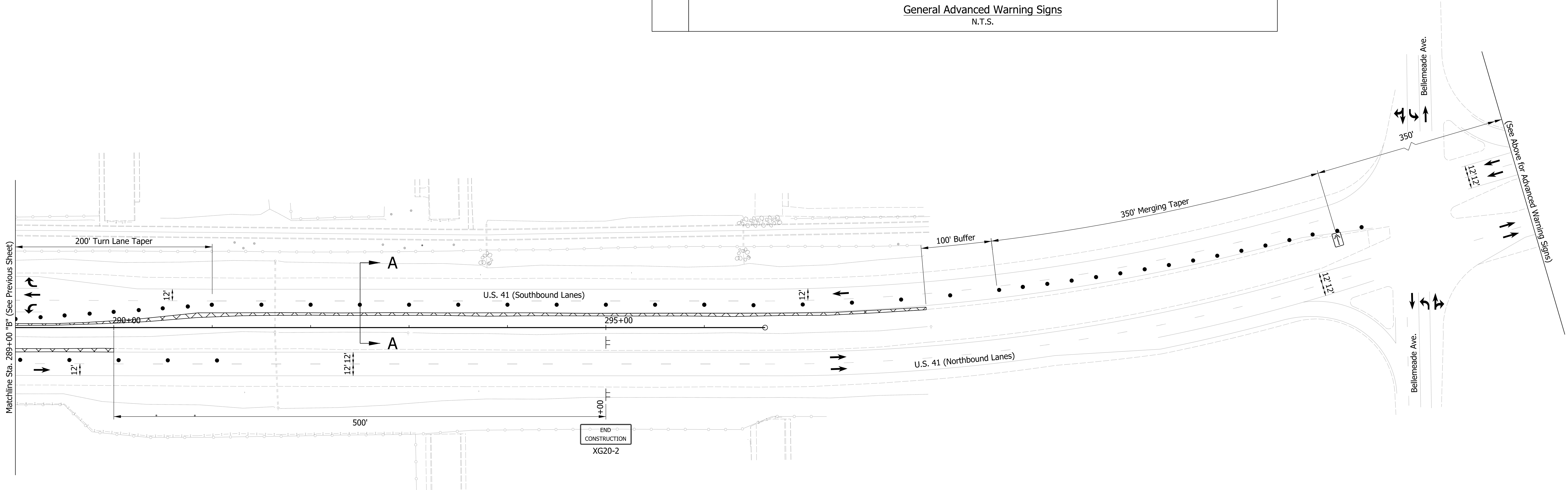
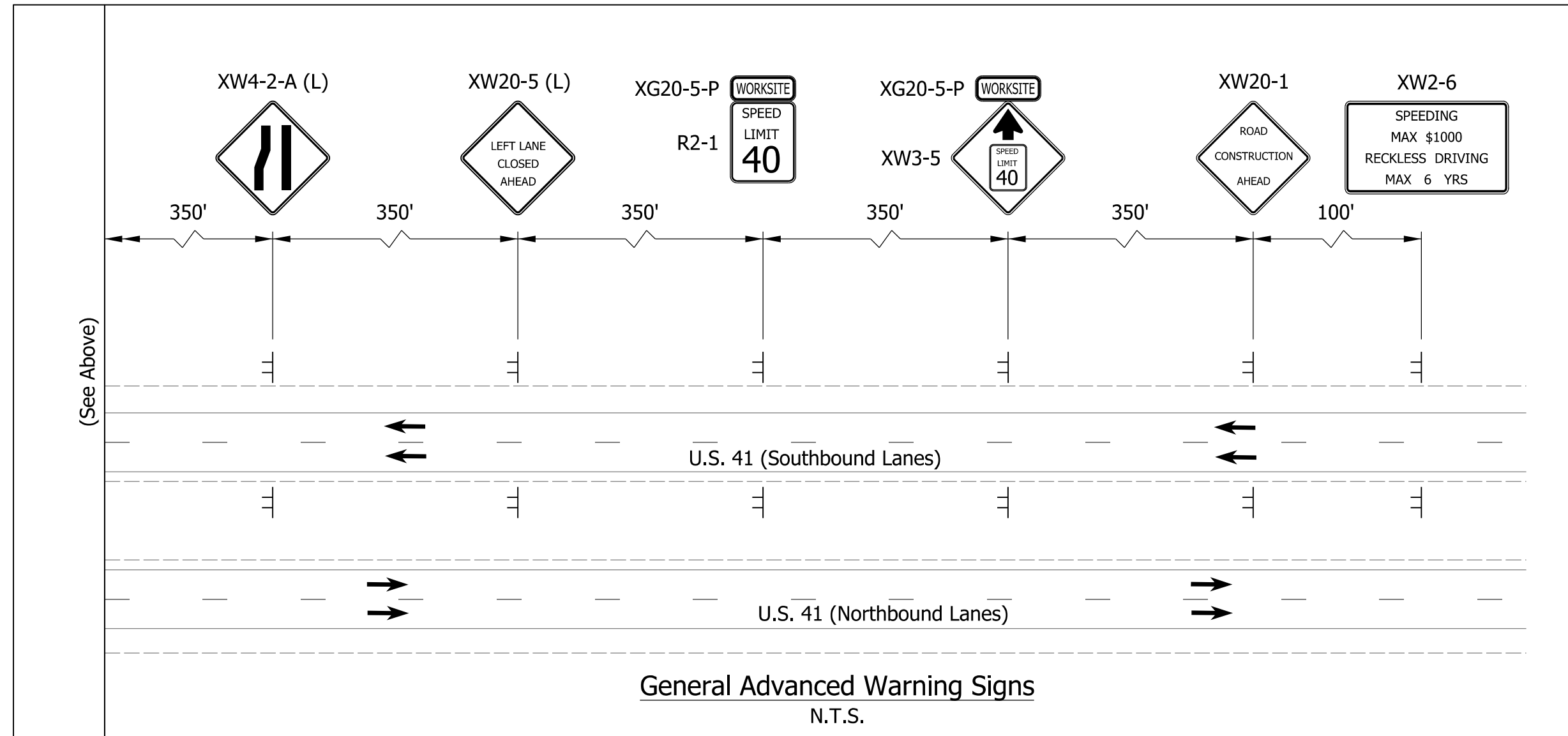
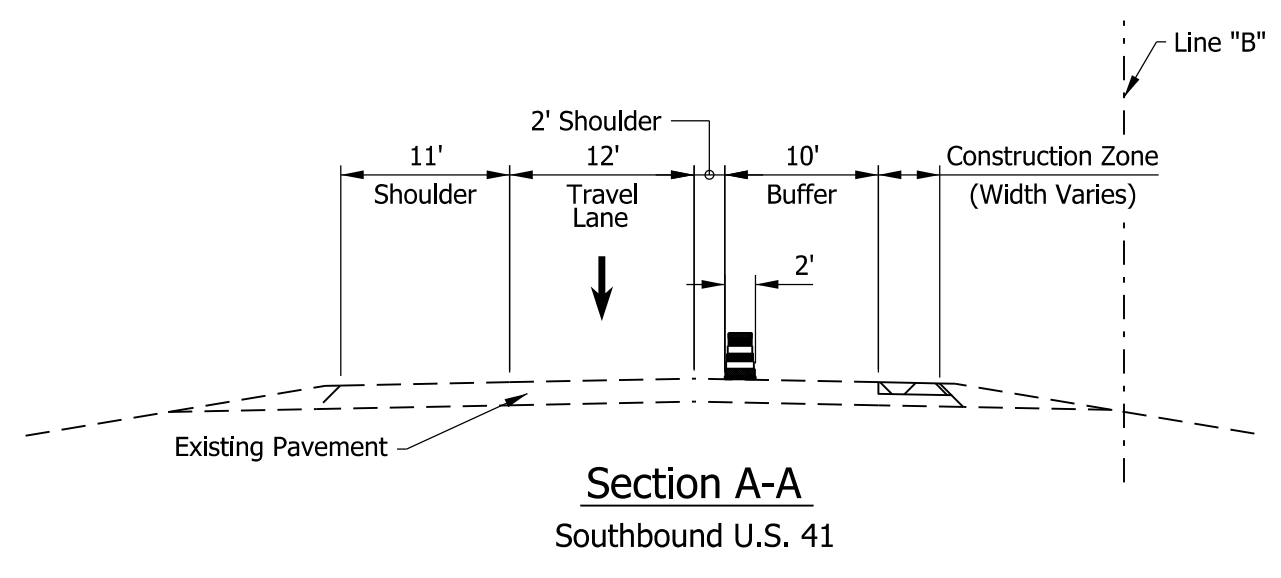
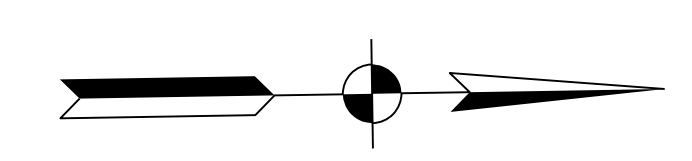
DRAFT
 NOT FOR CONSTRUCTION

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: NSX	DRAWN: HDW	
CHECKED: TNE	CHECKED: NSX	

INDIANA
 DEPARTMENT OF TRANSPORTATION

 MAINTENANCE OF TRAFFIC
 PHASE 1

HORIZONTAL SCALE	BRIDGE FILE
1"=50'	N/A
VERTICAL SCALE	DESIGNATION
N/A	2000186
SURVEY BOOK	SHEETS
ELECTRONIC	7 of 54
CONTRACT	PROJECT
B-42876	2000186



- Notes:**
- Shoulder Strengthening to be completed by Single Lane Closure per SDD E 801-TCLC-11
 - Contractor shall maintain a 1' Minimum Shoulder at all times
 - All Sign Spacing shall be 350' Minimum unless otherwise noted
 - Traffic Drum Spacing along Tapers shall be at 25'
 - Traffic Drum Spacing along Tangents shall be at 50'
 - All temporary Pavement Markings shall be Removable
 - Existing Transverse Rumble Strips to remain in place
 - No changes to the Traffic Signal at US 41 and Washington Ave. Intersection

LEGEND	
	Construction Zone
	Temporary Pavement
	Direction of Traffic
	Temporary Traffic Barrier
	Energy Absorbing Terminal CZ, Type 2
	Construction Sign
	Traffic Drum
	1 Temporary Pavement Markings, 6 In., Solid, White
	2 Temporary Pavement Markings, 6 In., Solid, Yellow
	3 Temporary Pavement Markings, 6 In., Broken, White
	5 Temporary Transverse Pavement Markings, 24 In., Solid, Stop Line
	6 Temporary Transverse Pavement Markings, 24 In., Solid, White
	7 Temporary Pavement Message Marking, Lane Indication Arrow

DRAFT
NOT FOR CONSTRUCTION

RECOMMENDED FOR APPROVAL _____	DESIGN ENGINEER _____	DATE _____
DESIGNED: _____ NSX _____	DRAWN: _____ HDW _____	
CHECKED: _____ TNE _____	CHECKED: _____ NSX _____	

INDIANA
DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC
PHASE 1

HORIZONTAL SCALE	BRIDGE FILE	
1"=50'	N/A	
VERTICAL SCALE	DESIGNATION	
N/A	2000186	
SURVEY BOOK	SHEETS	MOT-03
ELECTRONIC	8	of 54
CONTRACT	PROJECT	
B-42876	2000186	

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CONSTRUCTION SEQUENCING

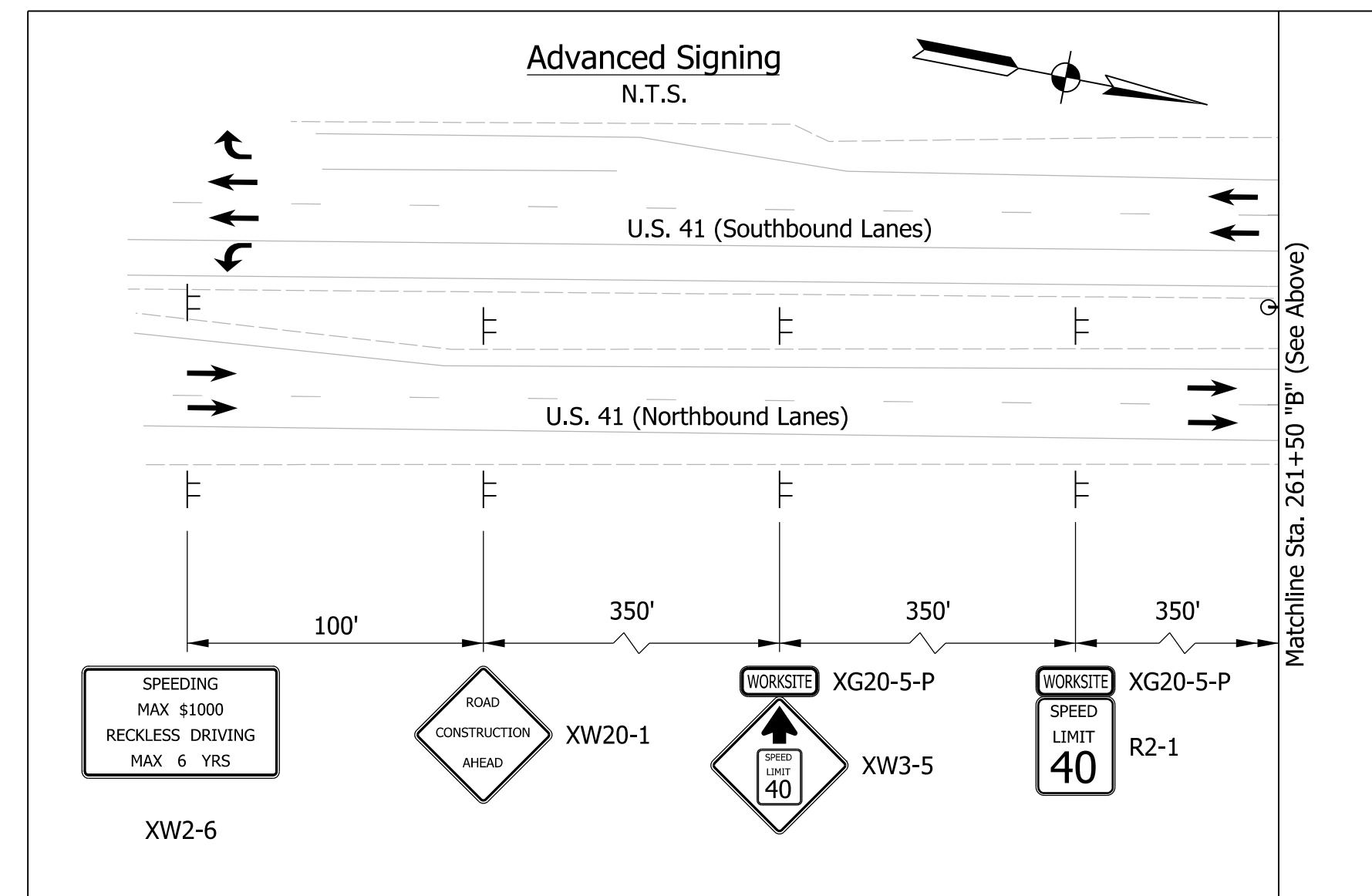
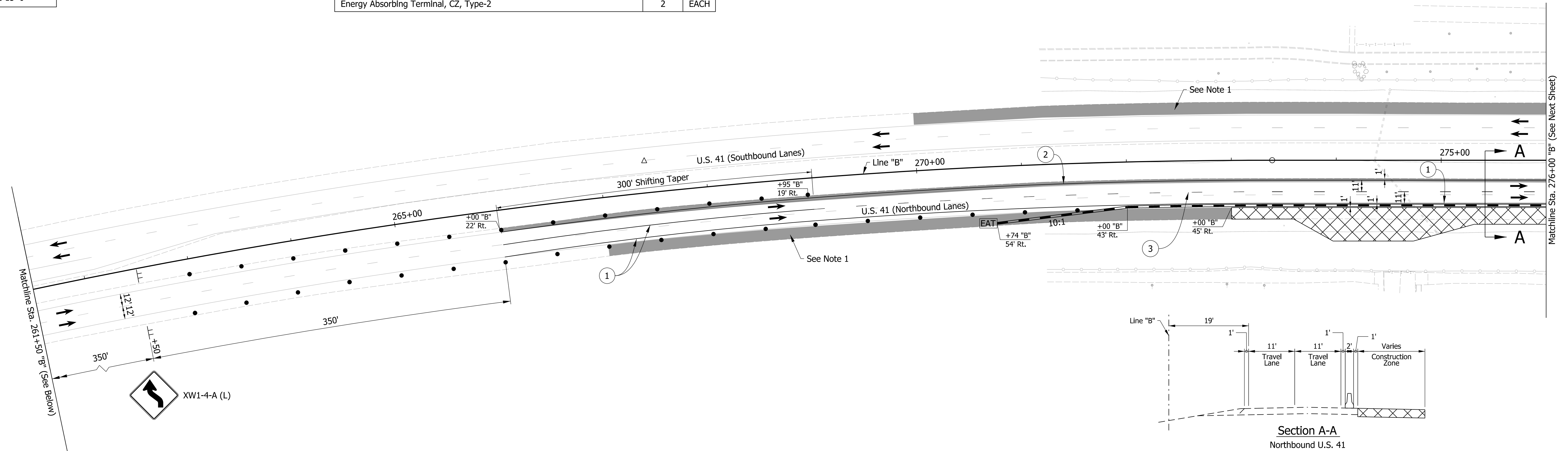
PHASE 2:

1. Install Advanced Warning Signs.
2. Remove existing Pavement Markings along US 41.
3. Install temporary Pavement Markings and Traffic Drums.
4. Place temporary Traffic Barrier adjacent to US 41 workzones.
5. Construct the Northbound and Southbound along US 41 Outside Shoulder Loops.

Construction Design Speed: 40 MPH

Construction Clear Zone: 13'-0"

Maintenance of Traffic Quantity Summary - Phase 2		
Item	Amount	Unit
Construction Sign A	18	EACH
Construction Sign B	4	EACH
Temporary Worksite Speed Limit Sign Assembly	8	EACH
Temporary Pavement Marking, Removable, 6 In.	10,518	LFT
Temporary Transverse Pavement Marking, Removable, Lane Indication Arrow	4	EACH
Temporary Transverse Pavement Marking, Removable, 24 In.	100	LFT
Temporary Traffic Barrier, Type 2	1,614	LFT
Energy Absorbing Terminal, CZ, Type-2	2	EACH



Notes:

1. Shoulder Strengthening to be completed by Single Lane Closure per SDD E 801-TCLC-11
2. Screened Signs were placed in Previous Construction Phase
3. Contractor shall maintain a 1' Minimum Shoulder at all times
4. All Sign Spacing shall be 350' Minimum unless otherwise noted
5. Traffic Drum Spacing along Tapers shall be at 25'
6. Traffic Drum Spacing along Tangents shall be at 50'
7. All temporary Pavement Markings shall be Removable
8. Existing Transverse Rumble Strips to remain in place
9. No changes to the Traffic Signal at US 41 and Washington Ave. Intersection

LEGEND

- | | | | |
|---------------------------|--------------------------------------|---|--|
| Construction Zone | Energy Absorbing Terminal CZ, Type 2 | Temporary Pavement Markings, 6 In., Solid, White | Temporary Transverse Pavement Markings, 24 In., Solid, Stop Line |
| Temporary Pavement | Construction Sign | Temporary Pavement Markings, 6 In., Solid, Yellow | Temporary Transverse Pavement Markings, 24 In., Solid, White |
| Direction of Traffic | Traffic Drum | Temporary Pavement Markings, 6 In., Broken, White | Temporary Pavement Message Marking, Lane Indication Arrow |
| Temporary Traffic Barrier | | | |

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NOT FOR CONSTRUCTION

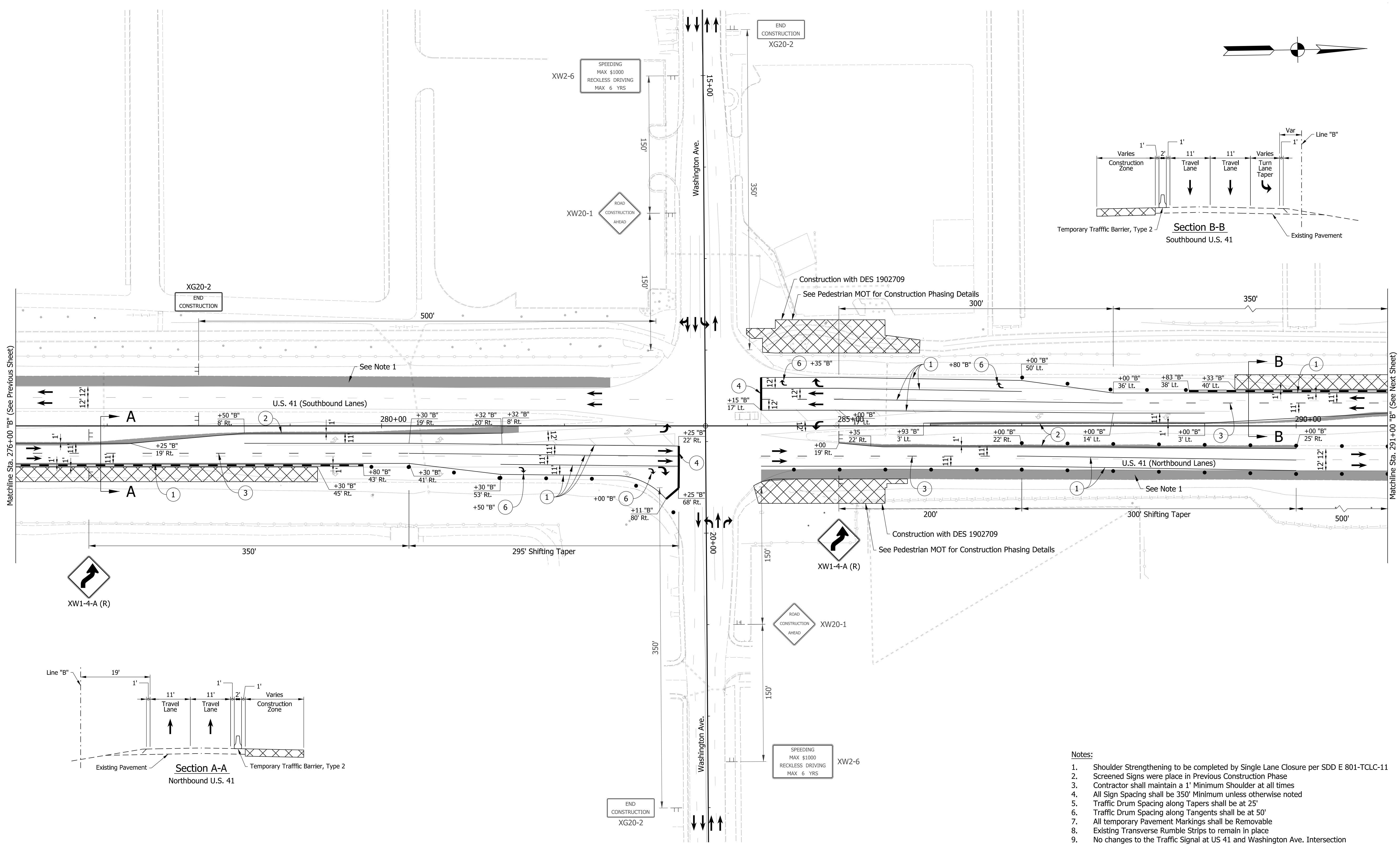
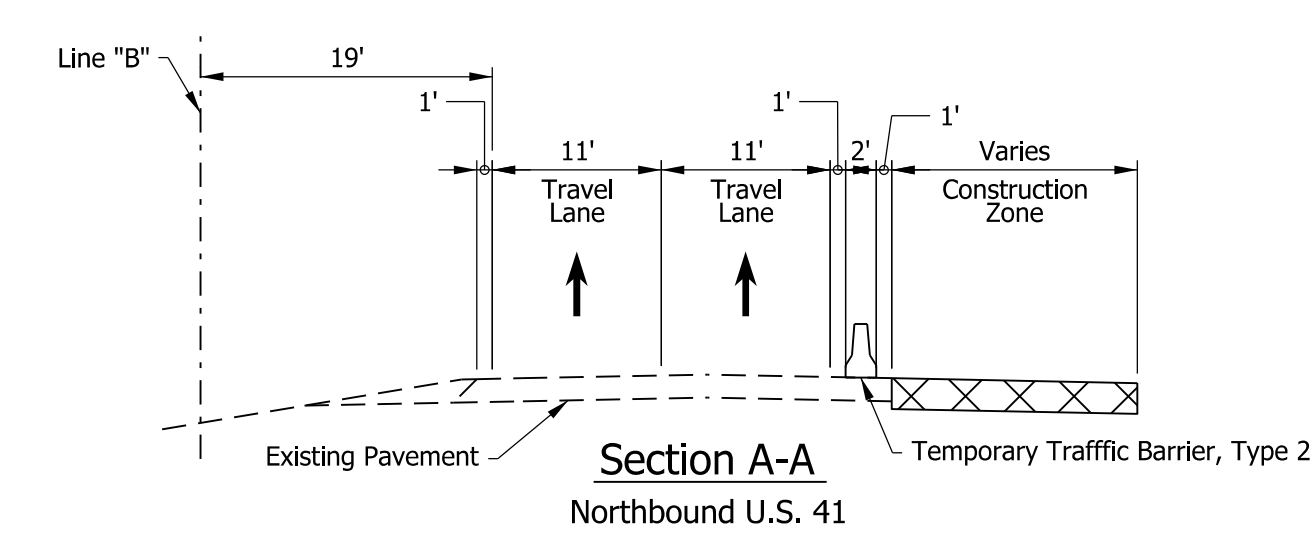
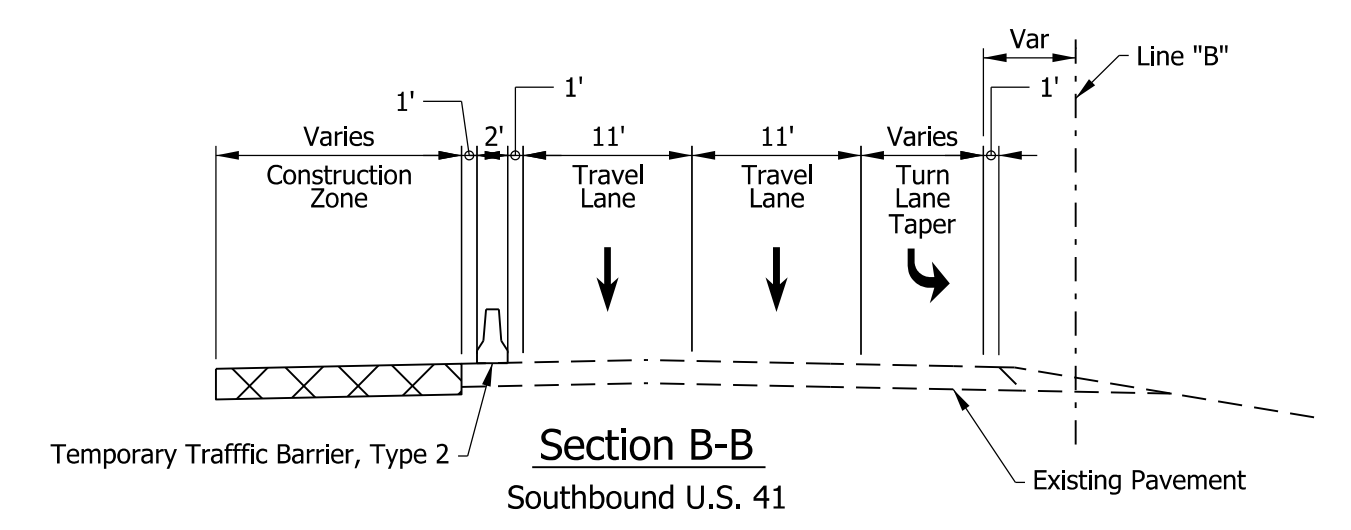
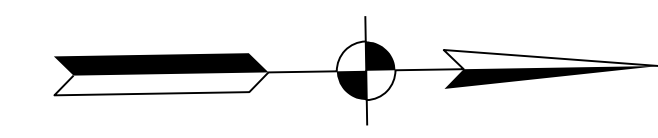
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DESIGNED: _____ NSX _____	DRAWN: _____ HDW _____	
CHECKED: _____ TNE _____	CHECKED: _____ NSX _____	

INDIANA
DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC
PHASE 2

HORIZONTAL SCALE 1"=50'	BRIDGE FILE N/A
VERTICAL SCALE N/A	DESIGNATION 2000186
SURVEY BOOK ELECTRONIC	SHEETS MOT-04 9 of 54
CONTRACT B-42876	PROJECT 2000186

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- Notes:**
- Shoulder Strengthening to be completed by Single Lane Closure per SDD E 801-TCLC-11
 - Screened Signs were placed in Previous Construction Phase
 - Contractor shall maintain a 1' Minimum Shoulder at all times
 - All Sign Spacing shall be 350' Minimum unless otherwise noted
 - Traffic Drum Spacing along Tapers shall be at 25'
 - Traffic Drum Spacing along Tangents shall be at 50'
 - All temporary Pavement Markings shall be Removable
 - Existing Transverse Rumble Strips to remain in place
 - No changes to the Traffic Signal at US 41 and Washington Ave. Intersection

LEGEND			
	Construction Zone		Energy Absorbing Terminal CZ, Type 2
	Temporary Pavement		Construction Sign
	Direction of Traffic		Traffic Drum
	Temporary Traffic Barrier		Temporary Pavement Markings, 6 In., Solid, White
			Temporary Pavement Markings, 6 In., Solid, Yellow
			Temporary Pavement Markings, 6 In., Broken, White
			Temporary Transverse Pavement Markings, 24 In., Solid, Stop Line
			Temporary Transverse Pavement Markings, 12 In., Solid, Yellow
			Temporary Pavement Message Marking, Lane Indication Arrow

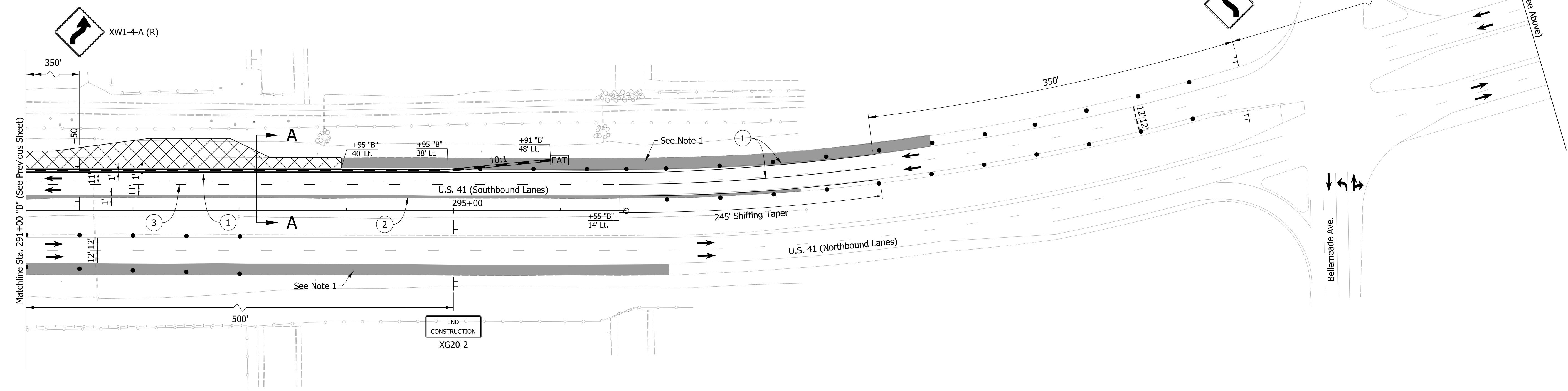
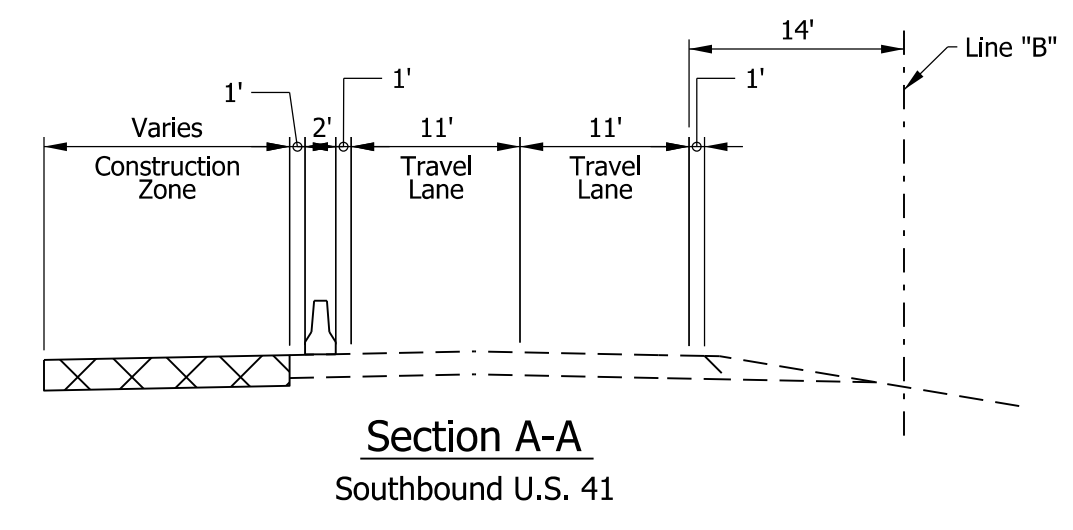
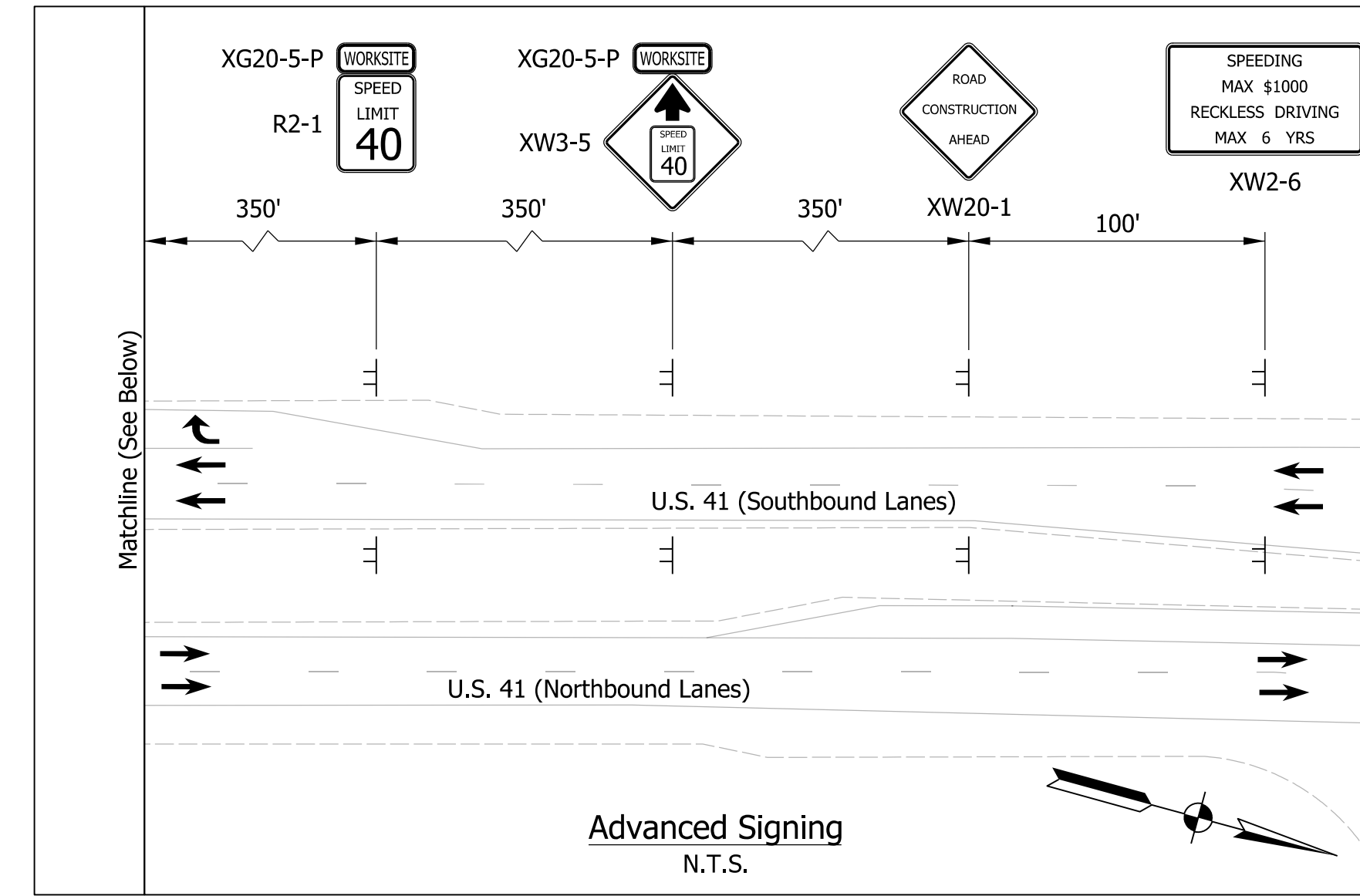
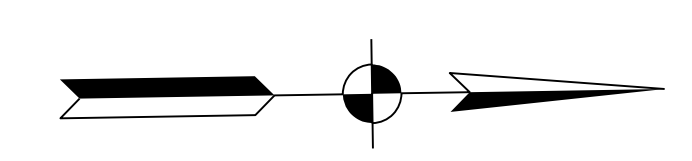
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NOT FOR CONSTRUCTION

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: NSX	DRAWN: HDW	
CHECKED: TNE	CHECKED: NSX	

INDIANA DEPARTMENT OF TRANSPORTATION
MAINTENANCE OF TRAFFIC PHASE 2

HORIZONTAL SCALE	BRIDGE FILE
1"=50'	N/A
VERTICAL SCALE	DESIGNATION
N/A	2000186
SURVEY BOOK	SHEETS
ELECTRONIC	10 of 54
CONTRACT	PROJECT
B-42876	2000186

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- Notes:**
- Shoulder Strengthening to be completed by Single Lane Closure per SDD E 801-TCLC-11
 - Screened Signs were placed in Previous Construction Phase
 - Contractor shall maintain a 1' Minimum Shoulder at all times
 - All Sign Spacing shall be 350' Minimum unless otherwise noted
 - Traffic Drum Spacing along Tapers shall be at 25'
 - Traffic Drum Spacing along Tangents shall be at 50'
 - All temporary Pavement Markings shall be Removable
 - Existing Transverse Rumble Strips to remain in place
 - No changes to the Traffic Signal at US 41 and Washington Ave. Intersection

LEGEND	
	Construction Zone
	Energy Absorbing Terminal CZ, Type 2
	Temporary Pavement
	Direction of Traffic
	Temporary Traffic Barrier
	Temporary Pavement Markings, 6 In., Solid, White
	Temporary Pavement Markings, 6 In., Solid, Yellow
	Temporary Pavement Markings, 6 In., Broken, White
	Temporary Transverse Pavement Markings, 24 In., Solid, Stop Line
	Temporary Transverse Pavement Markings, 24 In., Solid, White
	Temporary Pavement Message Marking, Lane Indication Arrow

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NOT FOR CONSTRUCTION

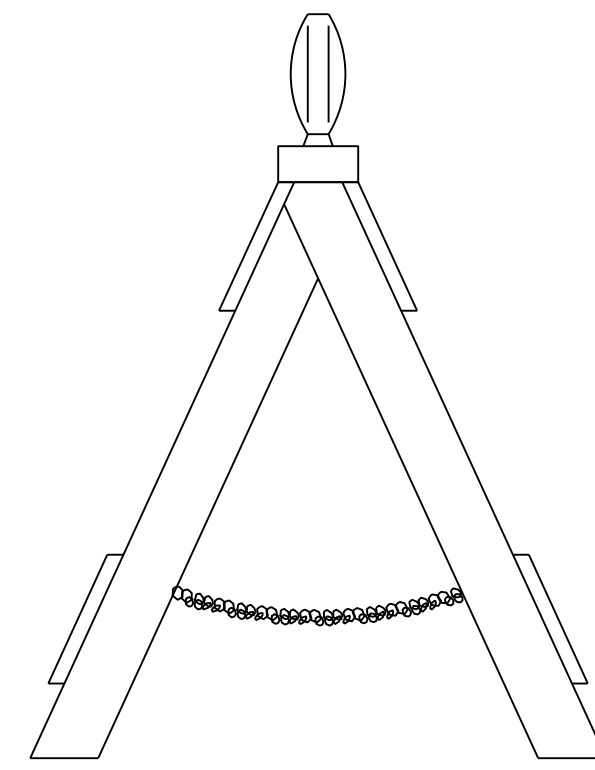
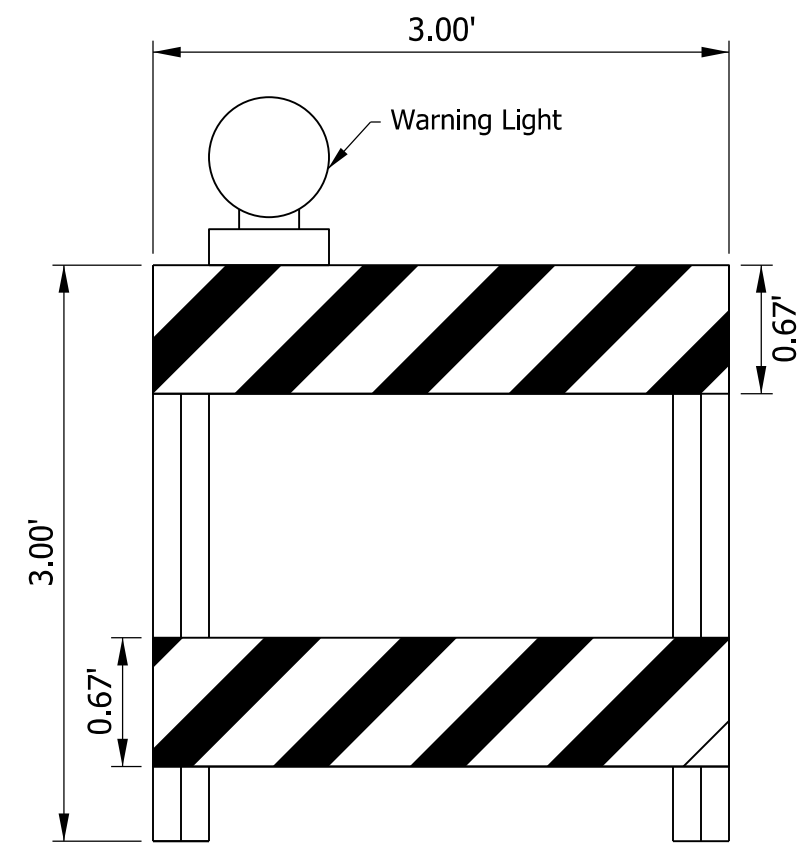
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INDIANA
DEPARTMENT OF TRANSPORTATION

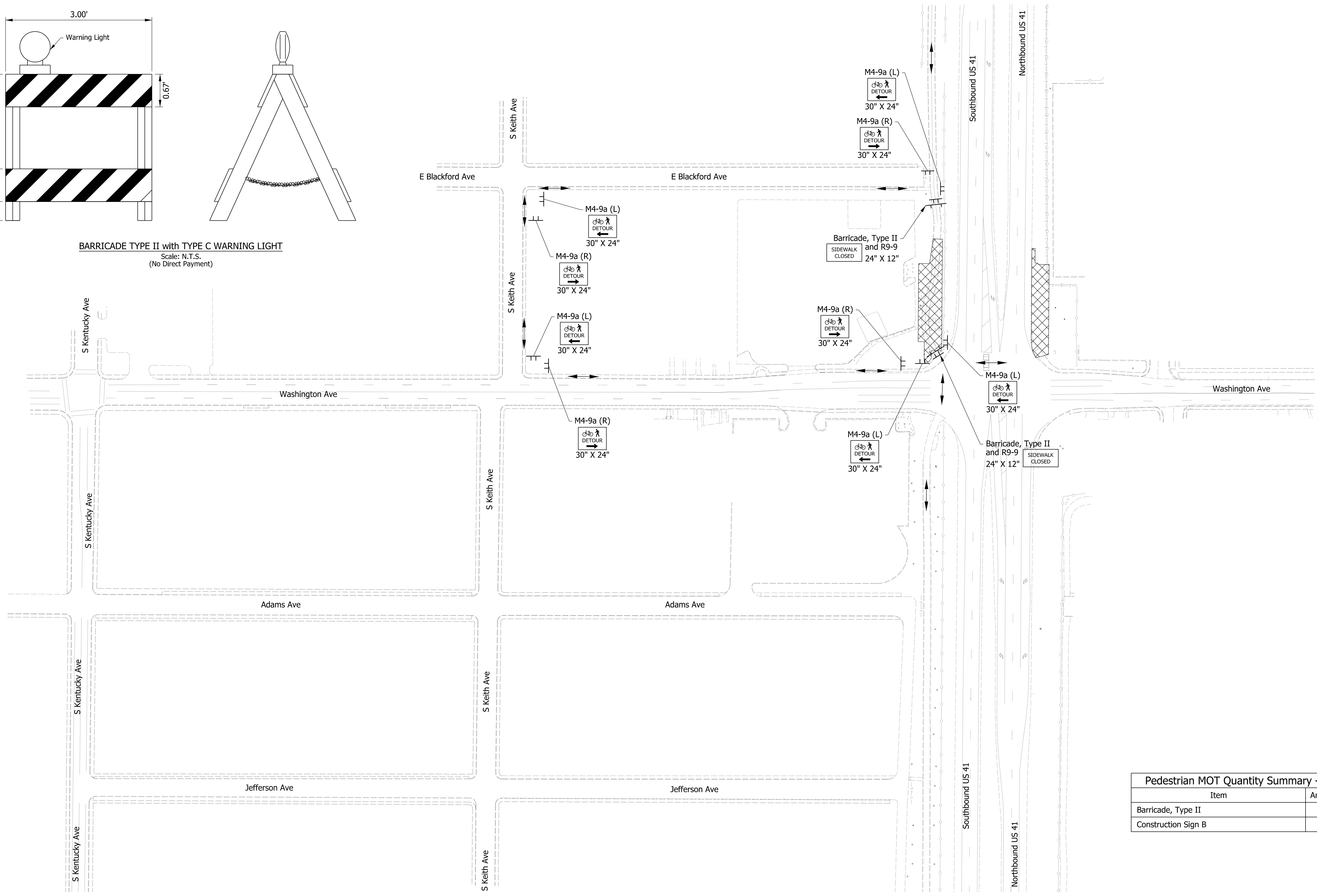
MAINTENANCE OF TRAFFIC
PHASE 2

HORIZONTAL SCALE	BRIDGE FILE
1"=50'	N/A
VERTICAL SCALE	DESIGNATION
N/A	2000186
SURVEY BOOK	SHEETS MOT-06
ELECTRONIC	11 of 54
CONTRACT	PROJECT
B-42876	2000186

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BARRICADE TYPE II with TYPE C WARNING LIGHT
Scale: N.T.S.
(No Direct Payment)



Item	Amount	Unit
Barricade, Type II	2	EACH
Construction Sign B	11	EACH

Construction Sign, B	Direction of Pedestrian Travel
Barricade, Type II with Type C Warning Light	Construction Area
Drum	

DRAFT
NOT FOR CONSTRUCTION

RECOMMENDED FOR APPROVAL _____	DESIGN ENGINEER _____	DATE _____
DESIGNED: _____ HDW _____	DRAWN: _____ HDW _____	
CHECKED: _____ TNE _____	CHECKED: _____ AJP _____	

INDIANA DEPARTMENT OF TRANSPORTATION

PEDESTRIAN MAINTENANCE OF TRAFFIC PHASE 2

HORIZONTAL SCALE	BRIDGE FILE
CUSTOM 75	N/A
VERTICAL SCALE	DESIGNATION
N/A	2000186
SURVEY BOOK	SHEETS MOT-07
ELECTRONIC	12 of 56
CONTRACT	PROJECT
B-42876	2000186

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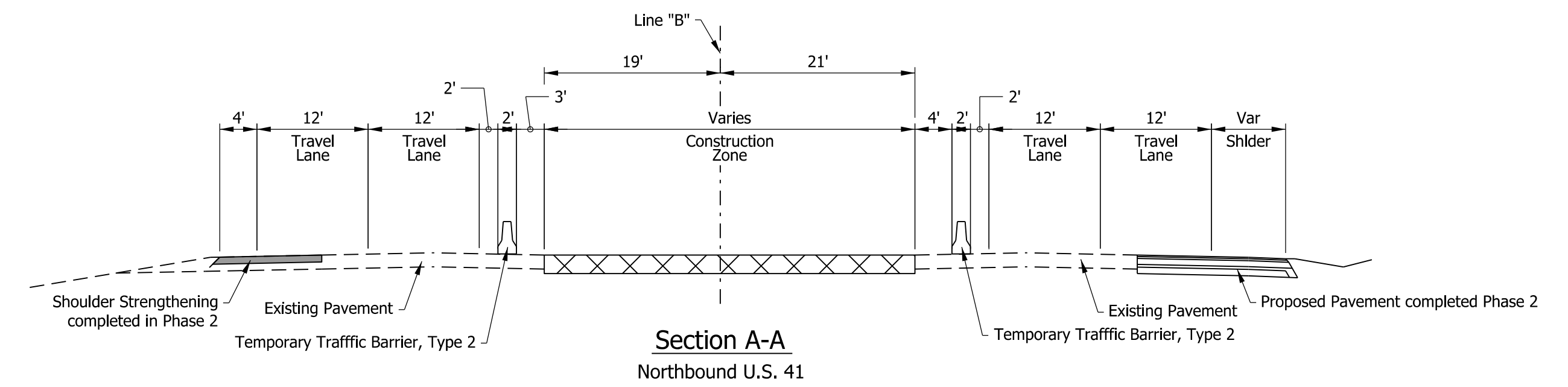
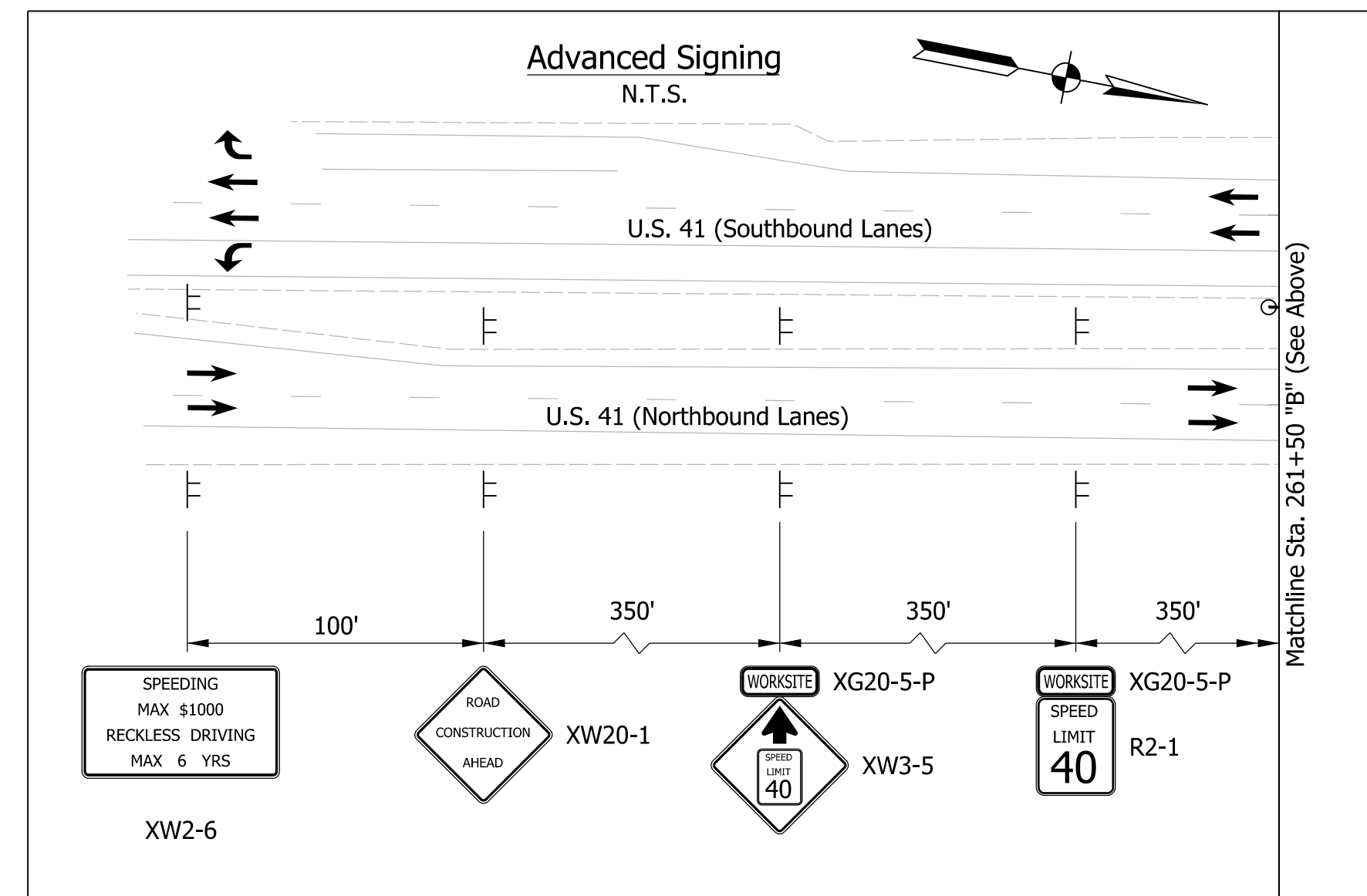
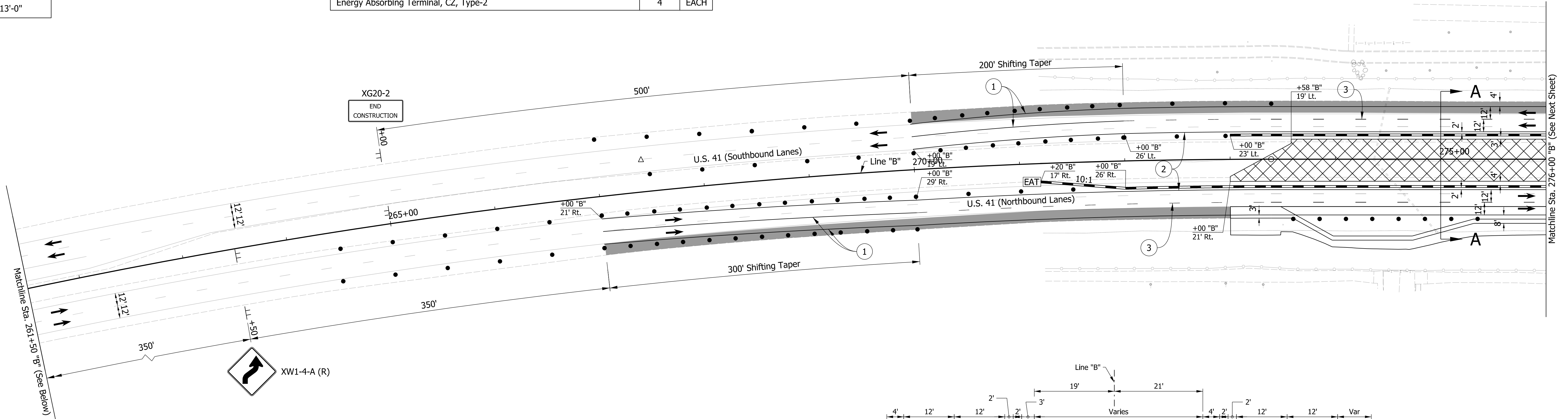
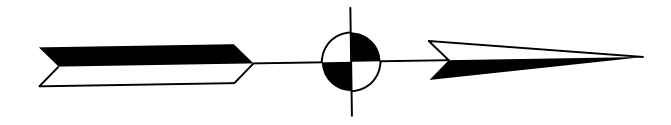
PHASE 3A:

1. Install Advanced Warning Signs.
2. Remove existing Pavement Markings and temporary Pavement Markings from Phase 2 along US 41.
3. Install temporary Pavement Markings and Traffic Drums.
4. Place temporary Traffic Barrier adjacent to US 41 workzones.
5. Construct the Northbound and Southbound Median U-Turn Lanes and Median Widening along US 41.

Construction Design Speed: 40 MPH

Construction Clear Zone: 13'-0"

Maintenance of Traffic Quantity Summary - Phase 3A		
Item	Amount	Unit
Construction Sign A	18	EACH
Construction Sign B	6	EACH
Temporary Worksite Speed Limit Sign Assembly	8	EACH
Temporary Pavement Marking, Removable, 6 In.	15,285	LFT
Temporary Transverse Pavement Marking, Removable, Lane Indication Arrow	2	EACH
Temporary Transverse Pavement Marking, Removable, 24 In.	113	LFT
Temporary Traffic Barrier, Type 2	4,020	LFT
Energy Absorbing Terminal, CZ, Type-2	4	EACH



Notes:

1. Contractor shall maintain a 2' Minimum Shoulder at all times
2. All Sign Spacing shall be 350' Minimum unless otherwise noted
3. Traffic Drum Spacing along Tapers shall be at 25'
4. Traffic Drum Spacing along Tangents shall be at 50'
5. All temporary Pavement Markings shall be Removable
6. Existing Transverse Rumble Strips to remain in place
7. No changes to the Traffic Signal at US 41 and Washington Ave. Intersection

LEGEND

- | | | | | | | | |
|--|---------------------------|--|--------------------------------------|--|---|--|--|
| | Construction Zone | | Energy Absorbing Terminal CZ, Type 2 | | 1 Temporary Pavement Markings, 6 In., Solid, White | | 5 Temporary Transverse Pavement Markings, 24 In., Solid, Stop Line |
| | Temporary Pavement | | Construction Sign | | 2 Temporary Pavement Markings, 6 In., Solid, Yellow | | 6 Temporary Transverse Pavement Markings, 24 In., Solid, White |
| | Direction of Traffic | | Traffic Drum | | 3 Temporary Pavement Markings, 6 In., Broken, White | | 7 Temporary Pavement Message Marking, Lane Indication Arrow |
| | Temporary Traffic Barrier | | | | | | |

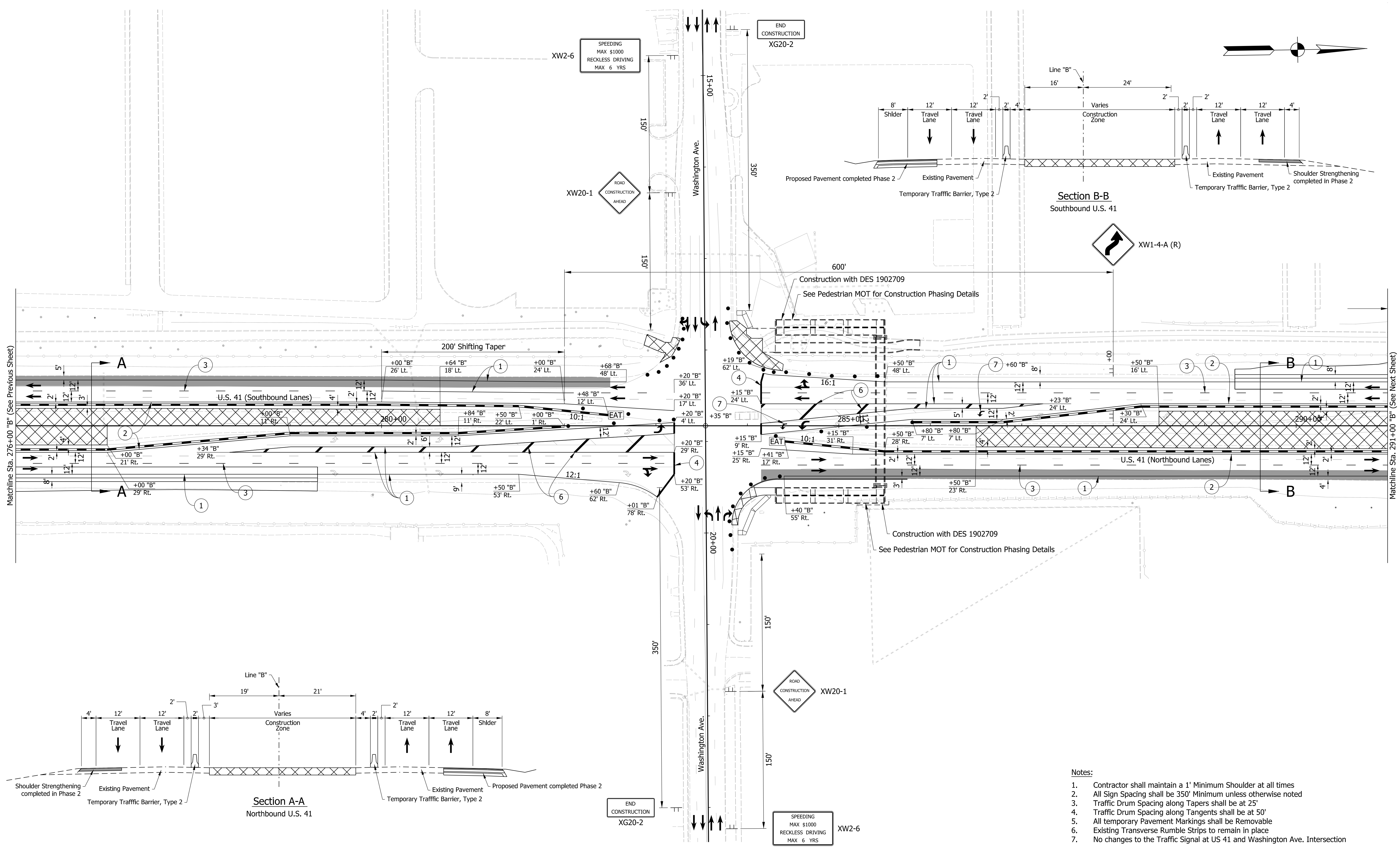
DRAFT
NOT FOR CONSTRUCTION

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: NSX	DRAWN: HDW	
CHECKED: TNE	CHECKED: NSX	

INDIANA
DEPARTMENT OF TRANSPORTATION

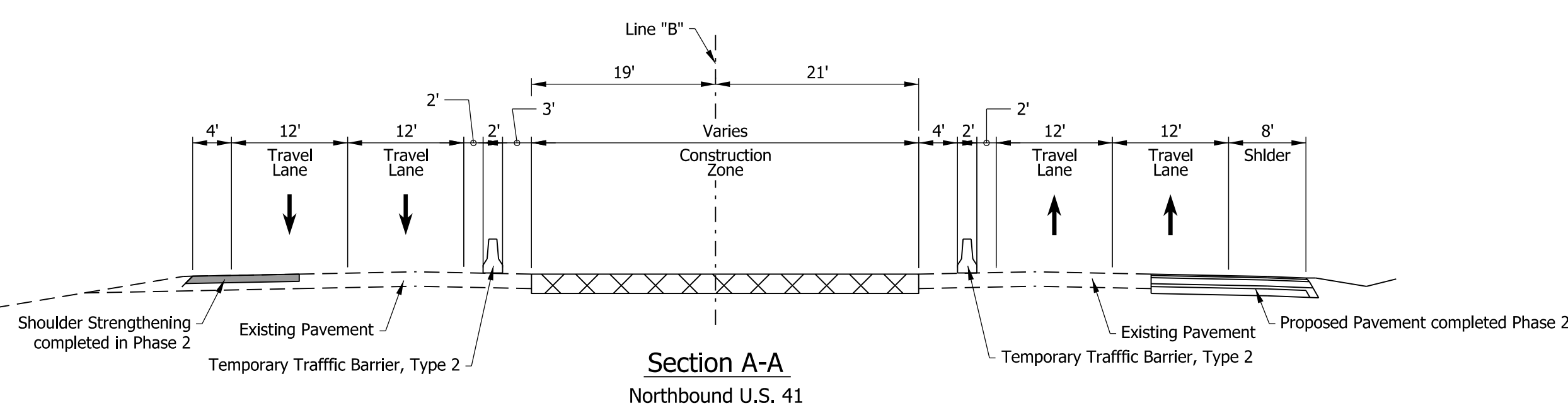
MAINTENANCE OF TRAFFIC
PHASE 3A

HORIZONTAL SCALE	BRIDGE FILE
1"=50'	N/A
VERTICAL SCALE	DESIGNATION
N/A	2000186
SURVEY BOOK	SHEETS MOT-07
ELECTRONIC	12 of 54
CONTRACT	PROJECT
B-42876	2000186

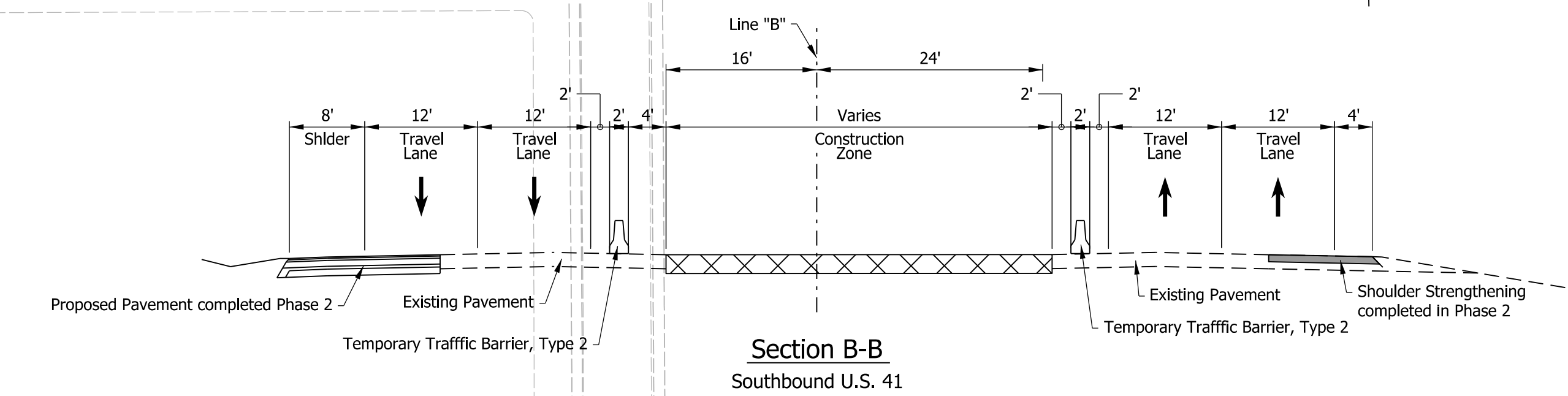


Matchline Sta. 276+00 "B" (See Previous Sheet)

Matchline Sta. 291+00 "B" (See Next Sheet)



Section A-A
Northbound U.S. 41



Section B-B
Southbound U.S. 41

- Notes:
1. Contractor shall maintain a 1' Minimum Shoulder at all times
 2. All Sign Spacing shall be 350' Minimum unless otherwise noted
 3. Traffic Drum Spacing along Tapers shall be at 25'
 4. Traffic Drum Spacing along Tangents shall be at 50'
 5. All temporary Pavement Markings shall be Removable
 6. Existing Transverse Rumble Strips to remain in place
 7. No changes to the Traffic Signal at US 41 and Washington Ave. Intersection

LEGEND	
	Construction Zone
	Temporary Pavement
	Direction of Traffic
	Temporary Traffic Barrier
	Energy Absorbing Terminal CZ, Type 2
	Construction Sign
	Traffic Drum
	1 Temporary Pavement Markings, 6 In., Solid, White
	2 Temporary Pavement Markings, 6 In., Solid, Yellow
	3 Temporary Pavement Markings, 6 In., Broken, White
	5 Temporary Transverse Pavement Markings, 24 In., Solid, Stop Line
	6 Temporary Transverse Pavement Markings, 24 In., Solid, White
	7 Temporary Pavement Message Marking, Lane Indication Arrow

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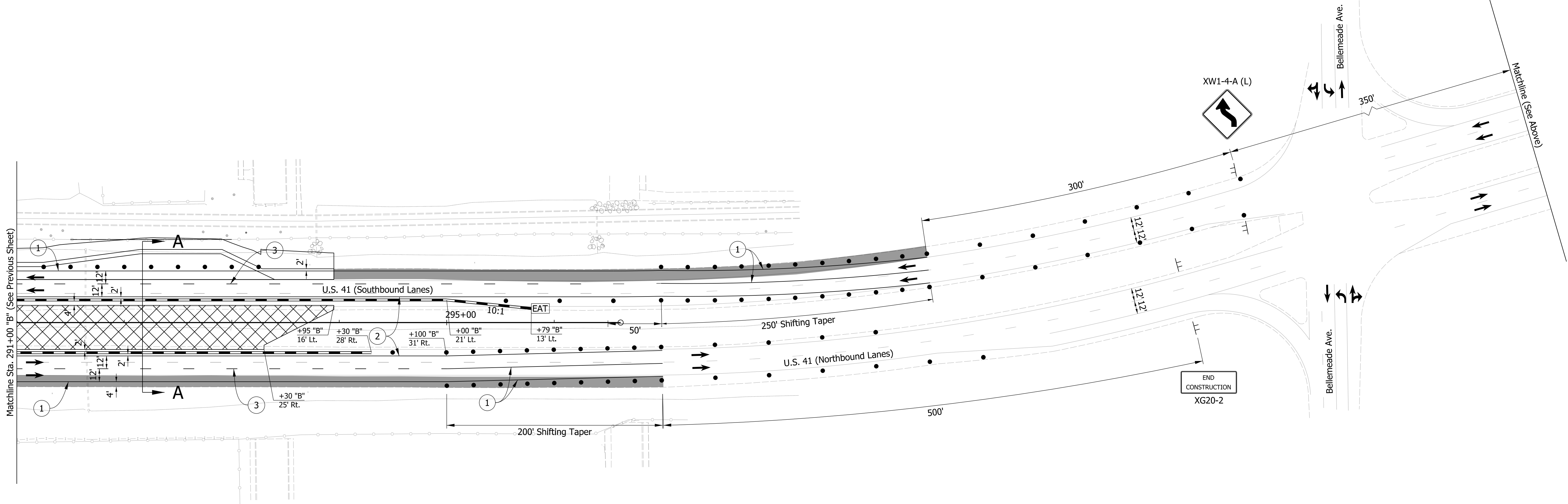
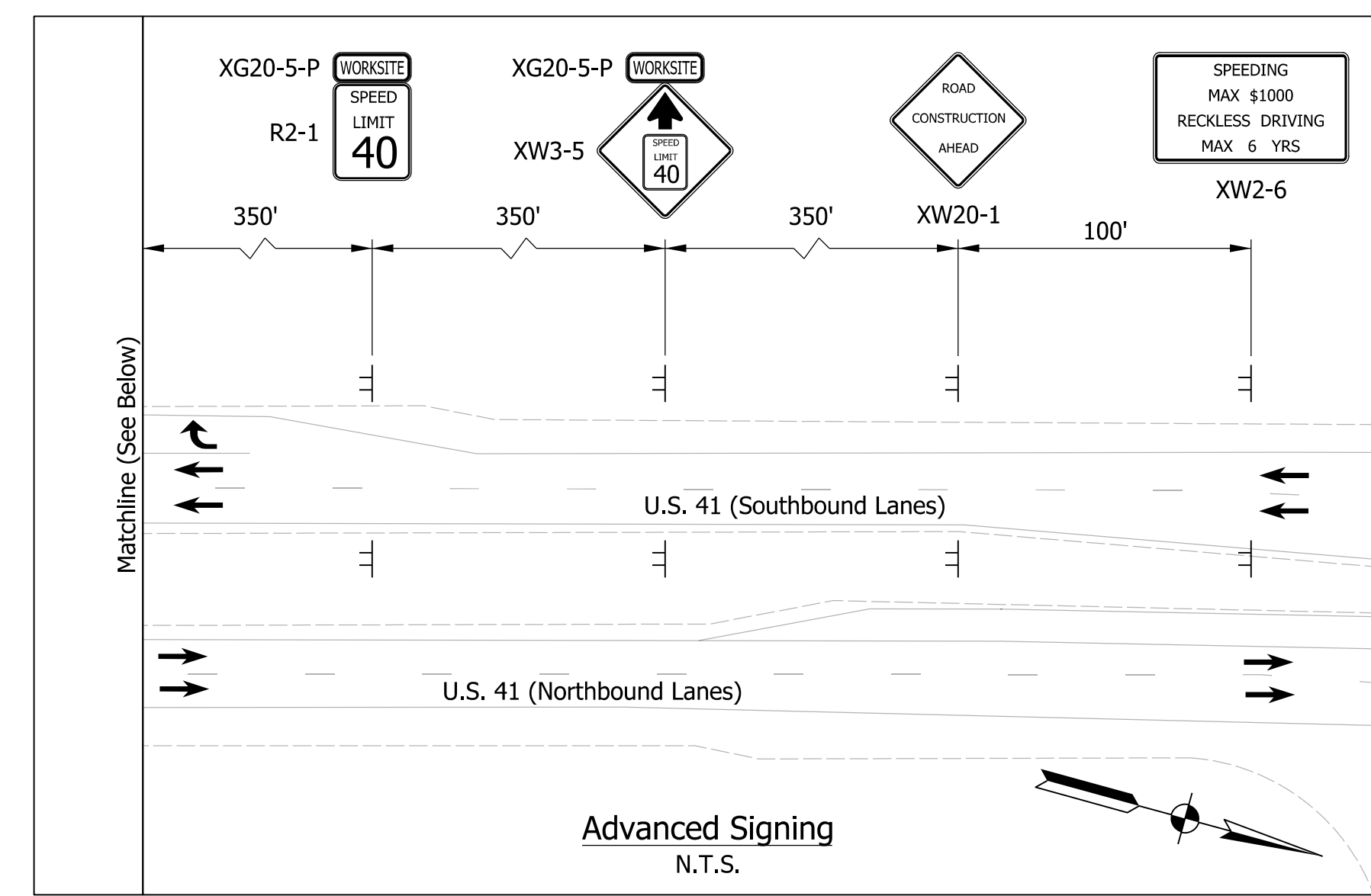
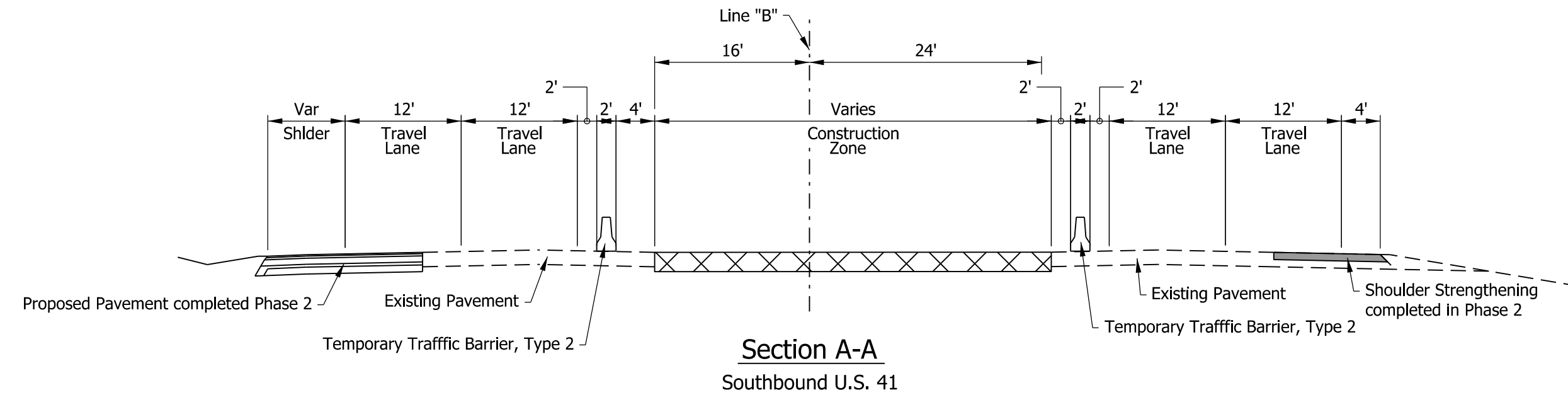
RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: NSX	DRAWN: HDW	
CHECKED: TNE	CHECKED: NSX	

INDIANA
DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC
PHASE 3A

HORIZONTAL SCALE	BRIDGE FILE
1"=50'	N/A
VERTICAL SCALE	DESIGNATION
N/A	2000186
SURVEY BOOK	SHEETS
ELECTRONIC	13 of 54
CONTRACT	PROJECT
B-42876	2000186

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 model:Sheet2
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- Notes:**
- Shoulder Strengthening to be completed by Single Lane Closure per SDD E 801-TCLC-11
 - Contractor shall maintain a 1' Minimum Shoulder at all times
 - All Sign Spacing shall be 350' Minimum unless otherwise noted
 - Traffic Drum Spacing along Tapers shall be at 25'
 - Traffic Drum Spacing along Tangents shall be at 50'
 - All temporary Pavement Markings shall be Removable
 - Existing Transverse Rumble Strips to remain in place
 - No changes to the Traffic Signal at US 41 and Washington Ave. Intersection

LEGEND

	Construction Zone		Energy Absorbing Terminal CZ, Type 2	①	Temporary Pavement Markings, 6 In., Solid, White	⑤	Temporary Transverse Pavement Markings, 24 In., Solid, Stop Line
	Temporary Pavement		Construction Sign	②	Temporary Pavement Markings, 6 In., Solid, Yellow	⑥	Temporary Transverse Pavement Markings, 24 In., Solid, White
	Direction of Traffic		Traffic Drum	③	Temporary Pavement Markings, 6 In., Broken, White	⑦	Temporary Pavement Message Marking, Lane Indication Arrow
	Temporary Traffic Barrier						

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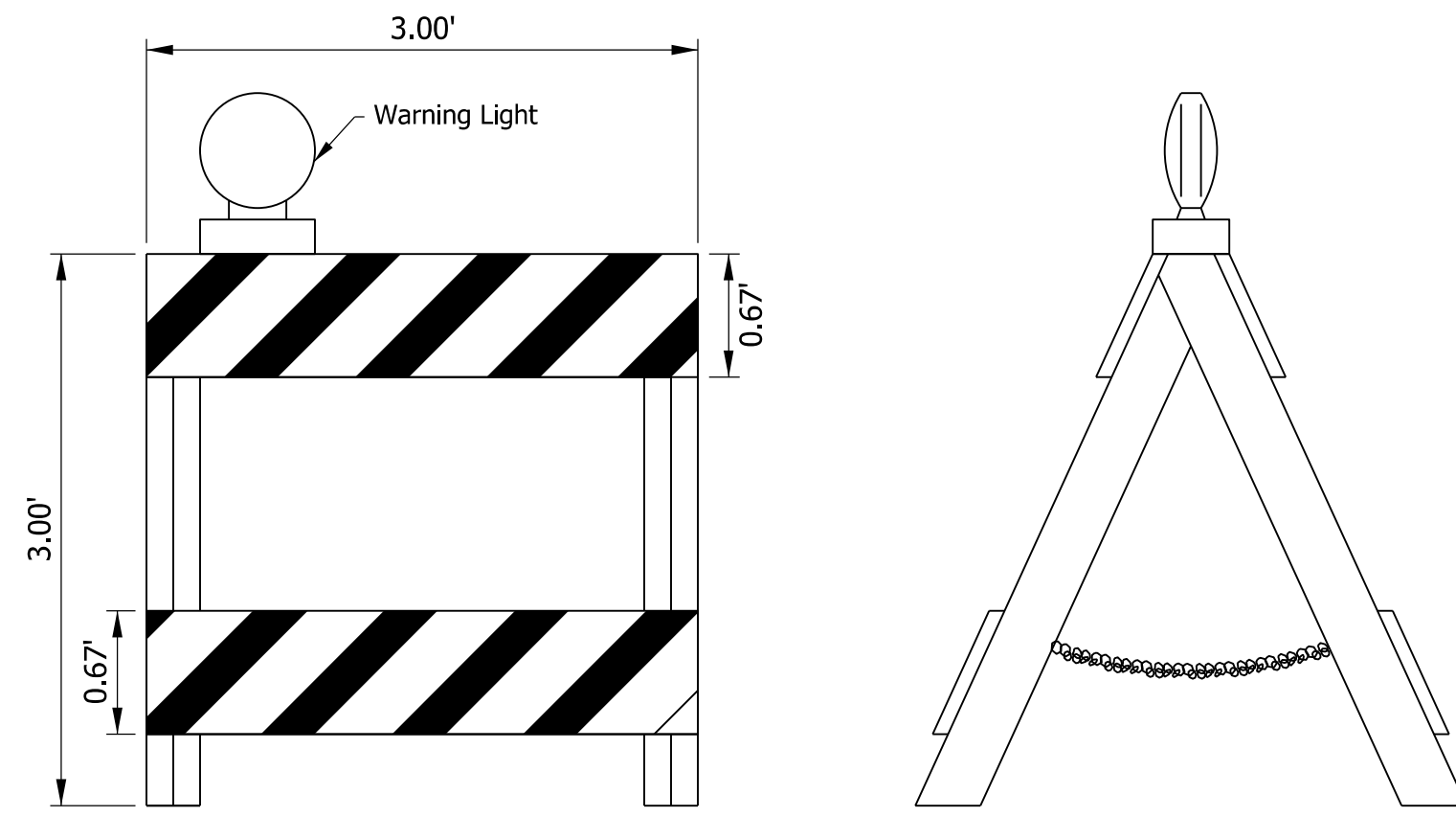
RECOMMENDED FOR APPROVAL _____	DESIGN ENGINEER _____	DATE _____
DESIGNED: _____ NSX _____	DRAWN: _____ HDW _____	
CHECKED: _____ TNE _____	CHECKED: _____ NSX _____	

INDIANA
DEPARTMENT OF TRANSPORTATION

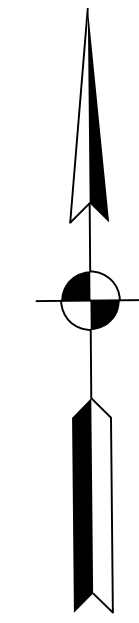
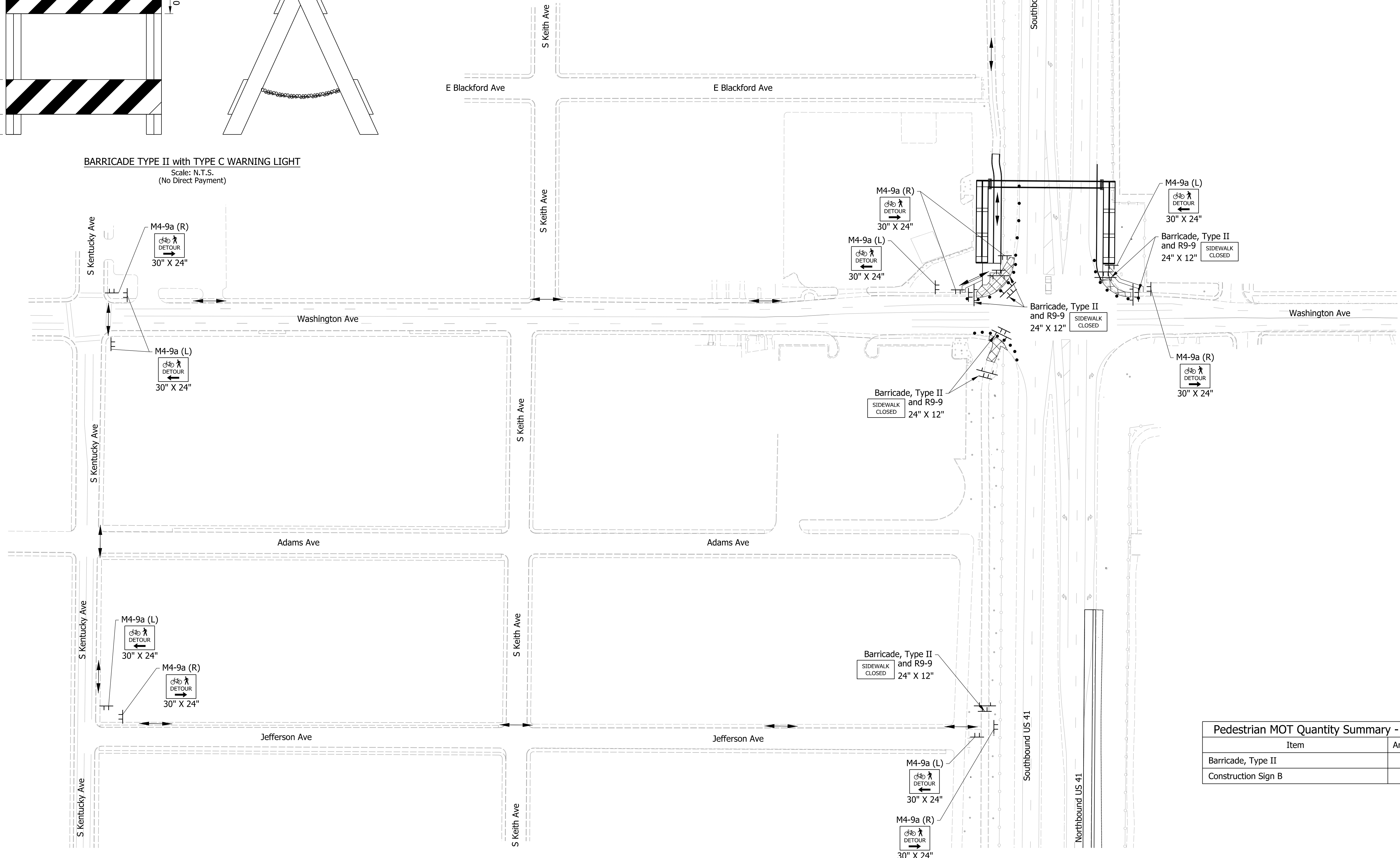
MAINTENANCE OF TRAFFIC
PHASE 3A

HORIZONTAL SCALE	BRIDGE FILE
1"=50'	N/A
VERTICAL SCALE	DESIGNATION
N/A	2000186
SURVEY BOOK	SHEETS MOT-09
ELECTRONIC	14 of 54
CONTRACT	PROJECT
B-42876	2000186

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BARRICADE TYPE II with TYPE C WARNING LIGHT
 Scale: N.T.S.
 (No Direct Payment)



Pedestrian MOT Quantity Summary - Phase 3A		
Item	Amount	Unit
Barricade, Type II	8	EACH
Construction Sign B	20	EACH

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LEGEND	
	Construction Sign, B
	Barricade, Type II with Type C Warning Light
	Drum
	Direction of Pedestrian Travel
	Construction Area

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RECOMMENDED FOR APPROVAL _____	DESIGN ENGINEER _____	DATE _____
DESIGNED: _____ HDW	DRAWN: _____ HDW	
CHECKED: _____ TNE	CHECKED: _____ AJP	

INDIANA DEPARTMENT OF TRANSPORTATION
PEDESTRIAN MAINTENANCE OF TRAFFIC PHASE 3A

HORIZONTAL SCALE	BRIDGE FILE
CUSTOM 75	N/A
VERTICAL SCALE	DESIGNATION
N/A	2000186
SURVEY BOOK	SHEETS MOT-11
ELECTRONIC	16 of 56
CONTRACT	PROJECT
B-42876	2000186

CONSTRUCTION SEQUENCING

PHASE 3B:

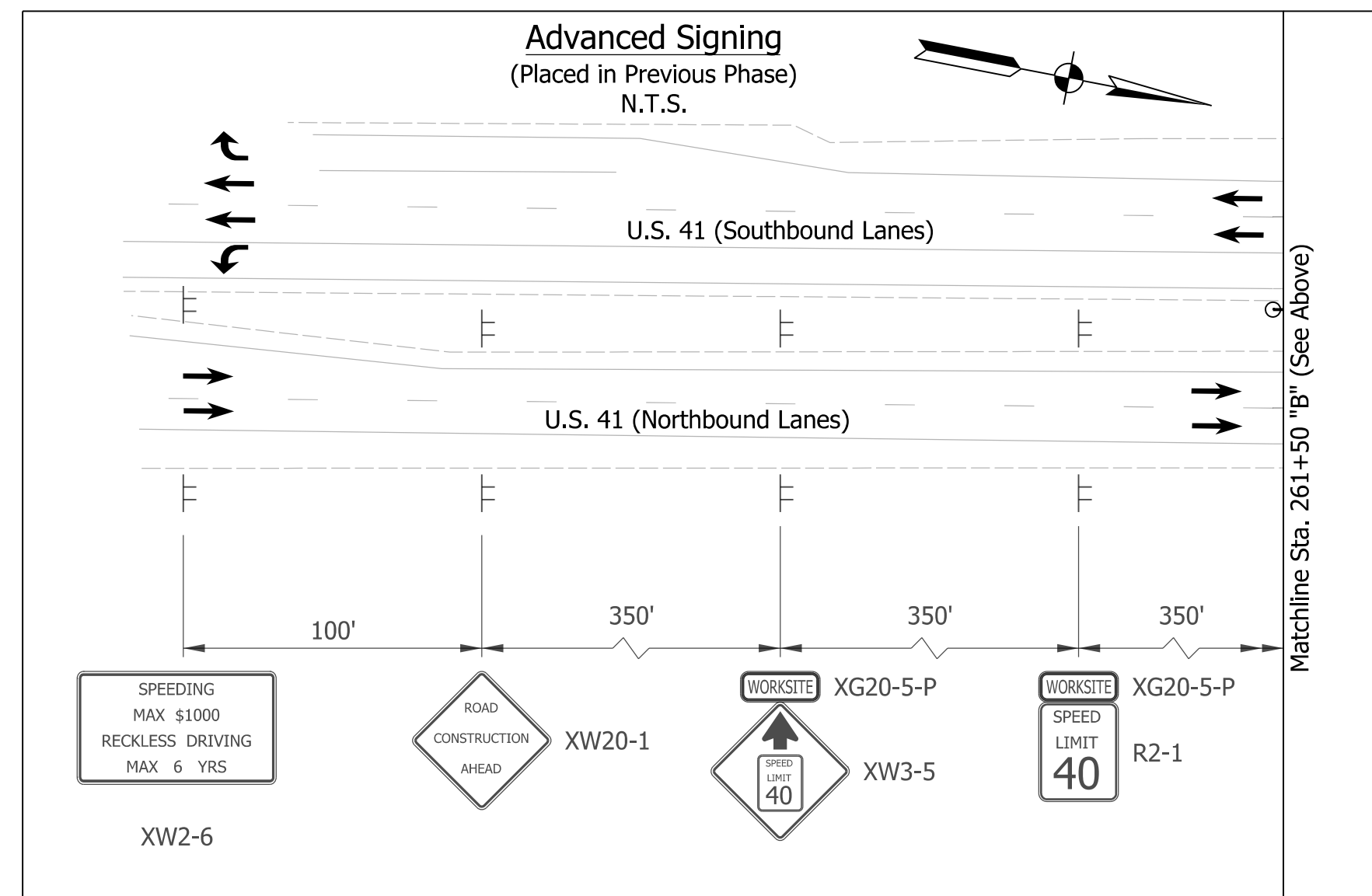
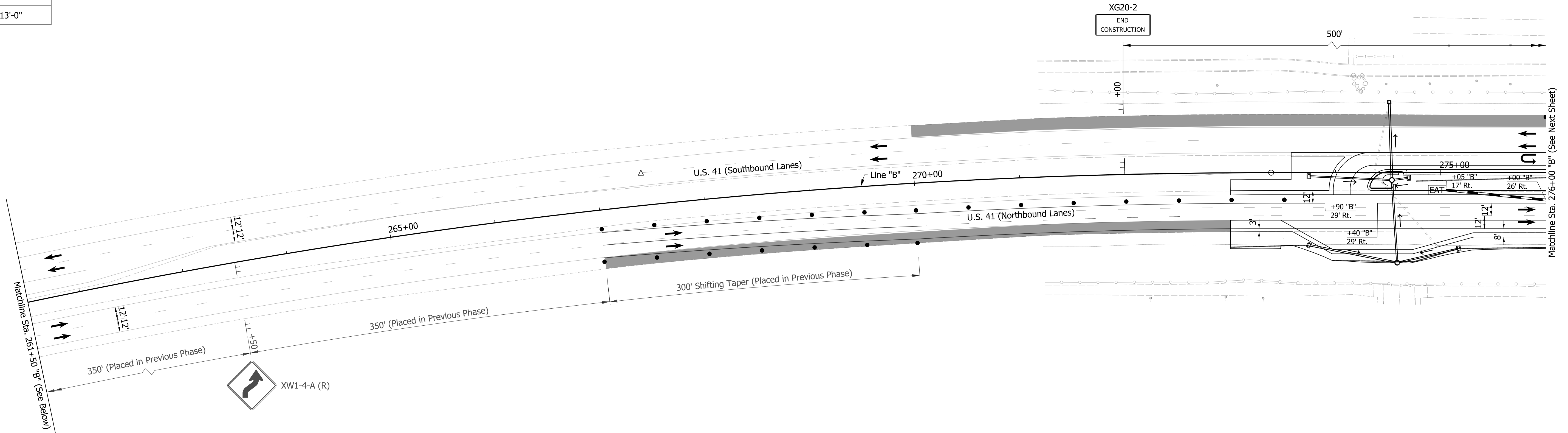
1. Remove any conflicting temporary Pavement Markings from Phase 3A along US 41.
2. Install temporary Pavement Markings and Traffic Drums.
3. Install Permanent Pavement Markings on and adjacent to Completed U-Turn Lanes along US 41.
4. Place temporary Traffic Barrier adjacent to US 41 workzones.
5. Finish constructing the Northbound and Southbound Median U-Turn Lanes and Median Widening along US 41 and Remove Existing Left Turn Lanes and Intersection Pavement.

Construction Design Speed: 40 MPH

Construction Clear Zone: 13'-0"

Maintenance of Traffic Quantity Summary - Phase 3B

Item	Amount	Unit
Construction Sign A	4	EACH
Construction Sign B	4	EACH
Temporary Pavement Marking, Removable, 6 In.	2,877	LFT
Temporary Traffic Barrier, Type 2	2,743	LFT
Energy Absorbing Terminal, CZ, Type-2	2	EACH



Notes:

1. Screened Pavement Markings and Signs were placed in Construction Phase 3A
2. Contractor shall maintain a 2' Minimum Shoulder at all times
3. All Sign Spacing shall be 350' Minimum unless otherwise noted
4. Traffic Drum Spacing along Tapers shall be at 25'
5. Traffic Drum Spacing along Tangents shall be at 50'
6. All temporary Pavement Markings shall be Removable
7. Existing Transverse Rumble Strips to remain in place
8. No changes to the Traffic Signal at US 41 and Washington Ave. Intersection

LEGEND

- | | | | | | | | |
|--|---------------------------|--|--------------------------------------|--|---|--|--|
| | Construction Zone | | Energy Absorbing Terminal CZ, Type 2 | | 1 Temporary Pavement Markings, 6 In., Solid, White | | 5 Temporary Transverse Pavement Markings, 24 In., Solid, Stop Line |
| | Temporary Pavement | | Construction Sign | | 2 Temporary Pavement Markings, 6 In., Solid, Yellow | | 6 Temporary Transverse Pavement Markings, 24 In., Solid, White |
| | Direction of Traffic | | Traffic Drum | | 3 Temporary Pavement Markings, 6 In., Broken, White | | 7 Temporary Pavement Message Marking, Lane Indication Arrow |
| | Temporary Traffic Barrier | | | | | | |

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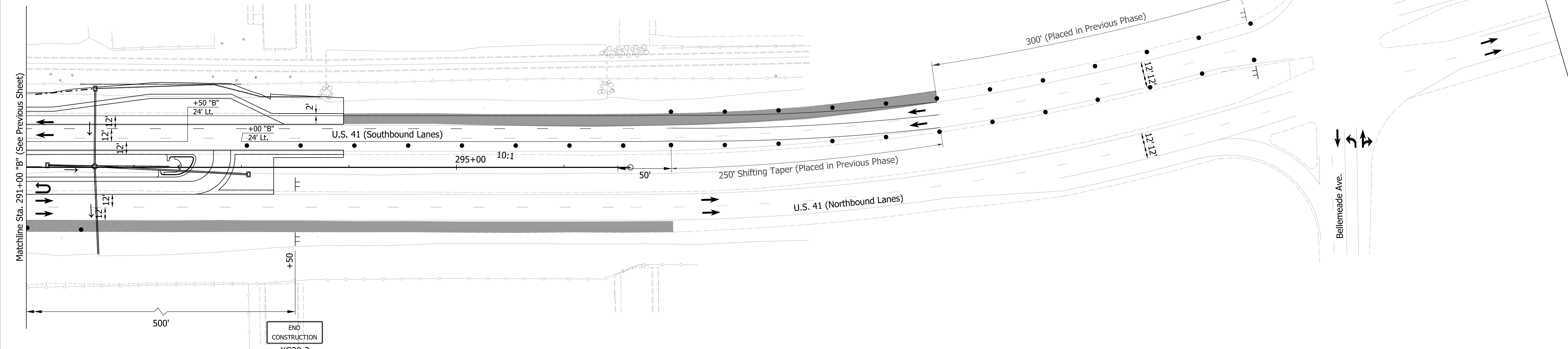
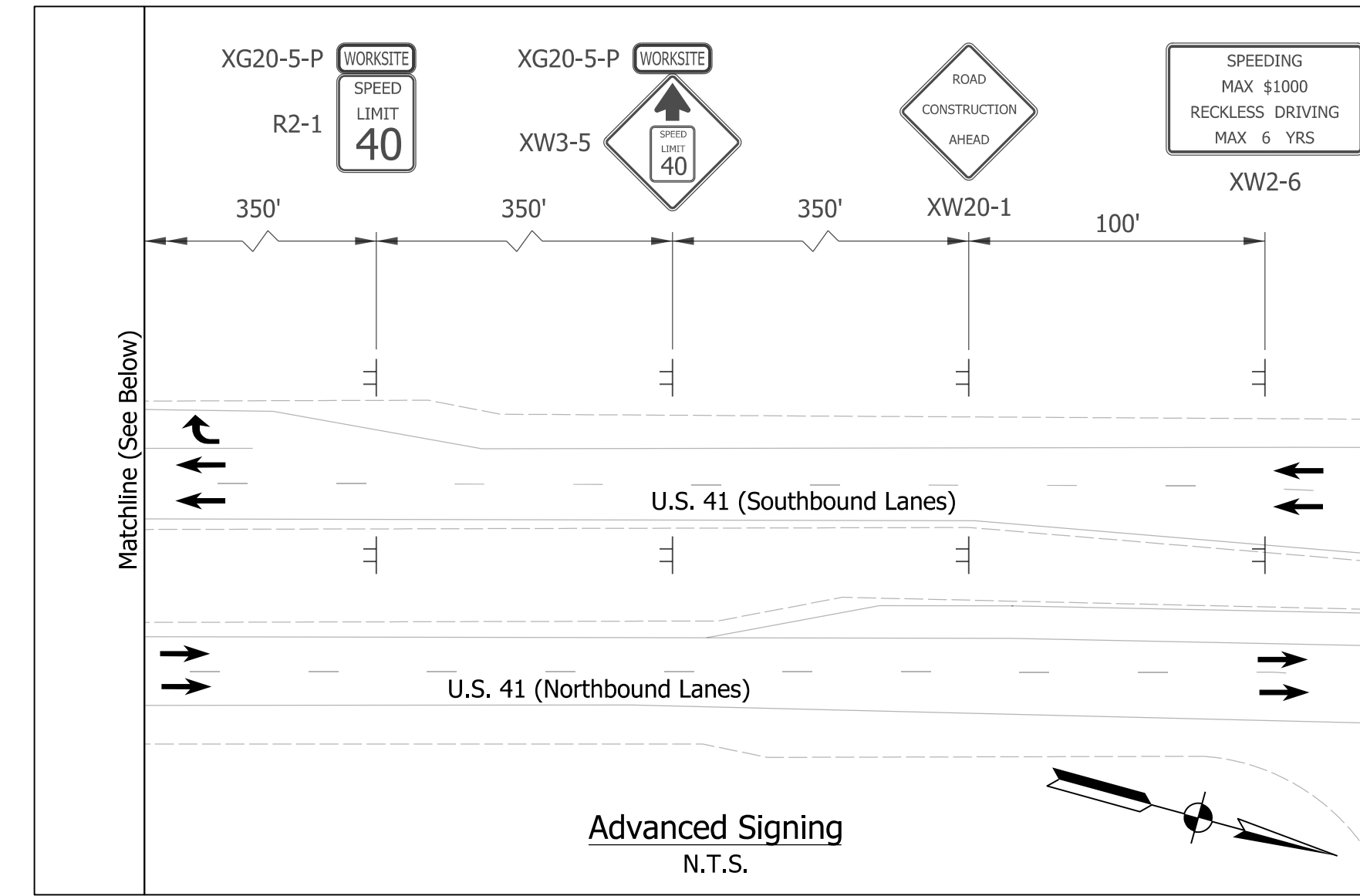
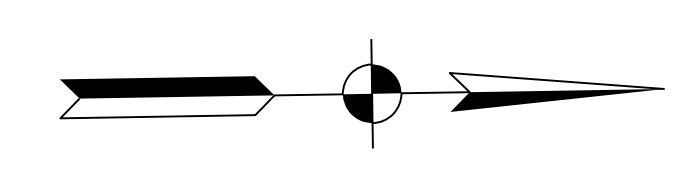
RECOMMENDED FOR APPROVAL _____	DESIGN ENGINEER _____	DATE _____
DESIGNED: _____ NSX _____	DRAWN: _____ HDW _____	
CHECKED: _____ TNE _____	CHECKED: _____ NSX _____	

INDIANA DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC PHASE 3B

HORIZONTAL SCALE	BRIDGE FILE
1"=50'	N/A
VERTICAL SCALE	DESIGNATION
N/A	2000186
SURVEY BOOK	SHEETS MOT-10
ELECTRONIC	15 of 54
CONTRACT	PROJECT
B-42876	2000186

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LEGEND	
	Construction Zone
	Temporary Pavement
	Direction of Traffic
	Temporary Traffic Barrier
	Energy Absorbing Terminal CZ, Type 2
	Construction Sign
	Traffic Drum
	1 Temporary Pavement Markings, 6 In., Solid, White
	2 Temporary Pavement Markings, 6 In., Solid, Yellow
	3 Temporary Pavement Markings, 6 In., Broken, White
	5 Temporary Transverse Pavement Markings, 24 In., Solid, Stop Line
	6 Temporary Transverse Pavement Markings, 24 In., Solid, White
	7 Temporary Pavement Message Marking, Lane Indication Arrow

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NOT FOR CONSTRUCTION

RECOMMENDED FOR APPROVAL _____	DESIGN ENGINEER _____	DATE _____
DESIGNED: _____ NSX _____	DRAWN: _____ HDW _____	
CHECKED: _____ TNE _____	CHECKED: _____ NSX _____	

INDIANA
DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC
PHASE 3B

HORIZONTAL SCALE	BRIDGE FILE
1"=50'	N/A
VERTICAL SCALE	DESIGNATION
N/A	2000186
SURVEY BOOK	SHEETS MOT-12
ELECTRONIC	17 of 54
CONTRACT	PROJECT
B-42876	2000186

- Notes:**
1. Screened Pavement Markings and Signs were placed in Construction Phase 3A
 2. Contractor shall maintain a 2' Minimum Shoulder at all times
 3. All Sign Spacing shall be 350' Minimum unless otherwise noted
 4. Traffic Drum Spacing along Tapers shall be at 25'
 5. Traffic Drum Spacing along Tangents shall be at 50'
 6. All temporary Pavement Markings shall be Removable
 7. Existing Transverse Rumble Strips to remain in place
 8. No changes to the Traffic Signal at US 41 and Washington Ave. Intersection

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