

**Indiana Department of Transportation**

County Hendricks and Marion Route US 36/Rockville Road Des. No. 1601072, 1901655, 1901657

**FHWA-Indiana Environmental Document  
CATEGORICAL EXCLUSION / ENVIRONMENTAL ASSESSMENT FORM  
GENERAL PROJECT INFORMATION**

<b>Road No./County:</b>	United States Highway (US) 36/Rockville Road / Hendricks and Marion Counties
<b>Designation Number:</b>	1601072, 1901655, and 1901657
<b>Project Description/Termini:</b>	Added travel lanes project along US 36/Rockville Road, beginning at Shiloh Park Drive (Reference Post [RP] 64+67) and terminating approximately 1,500 feet east of Raceway Road (RP 65+68), for a total length of approximately 1.1 miles.

After completing this form, I conclude that this project qualifies for the following type of Categorical Exclusion (FHWA must review/approve if Level 4 CE):

<input type="checkbox"/>	<b>Categorical Exclusion, Level 2</b> – The proposed action meets the criteria for Categorical Exclusion Manual Level 2 - table 1, CE Level Thresholds. Required Signatories: ESM (Environmental Scoping Manager)
<input type="checkbox"/>	<b>Categorical Exclusion, Level 3</b> – The proposed action meets the criteria for Categorical Exclusion Manual Level 3 - table 1, CE Level Thresholds. Required Signatories: ESM, ES (Environmental Services Division)
<input type="checkbox"/>	<b>Categorical Exclusion, Level 4</b> – The proposed action meets the criteria for Categorical Exclusion Manual Level 4 - table 1, CE Level Thresholds. Required Signatories: ESM, ES, FHWA
<input checked="" type="checkbox"/>	<b>Environmental Assessment (EA)</b> – EAs require a separate FONSI. Additional research and documentation is necessary to determine the effects on the environment. Required Signatories: ES, FHWA

Note: For documents prepared by or for Environmental Services Division, it is not necessary for the ESM of the district in which the project is located to release for public involvement or sign for approval.

**Release for Public Involvement**

Ronald E. Bala      8-6-19      M. Allen      8/2/19  
 ES Signature                      Date                      FHWA Signature                      Date

**Certification of Public Involvement**

\_\_\_\_\_  
 Office of Public Involvement                      Date

Note: Do not approve until after Section 106 public involvement and all other environmental requirements have been satisfied.

INDOT ESD/District Env. Reviewer Signature: Meghan Stubble      Date: 8/6/19  
 Name and Organization of CE/EA Preparer: Jaime Byerly / RQAW Corporation

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County

Hendricks and Marion

Route

US 36/Rockville Road

Des. No.

1601072, 1901655,  
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### Part I - PUBLIC INVOLVEMENT

Every Federal action requires some level of public involvement, providing for early and continuous opportunities throughout the project development process. **The level of public involvement should be commensurate with the proposed action.**

	<b>Yes</b>	<b>No</b>
Does the project have a historic bridge processed under the Historic Bridges PA*? If No, then:	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Opportunity for a Public Hearing Required?	<input checked="" type="checkbox"/>	<input type="checkbox"/>

*\*A public hearing is required for all historic bridges processed under the Historic Bridges Programmatic Agreement between INDOT, FHWA, SHPO, and the ACHP.*

*Discuss what public involvement activities (legal notices, letters to affected property owners and residents (i.e. notice of entry), meetings, special purpose meetings, newspaper articles, etc.) have occurred for this project.*

Remarks: The project has included several public involvement efforts. These efforts include Notice of Entry for Survey or Investigation letters, a Section 106 legal notice, a public information meeting, and a Community Advisory Committee (CAC) meeting. A public hearing will also be held prior to the issuance of the Finding of No Significant Impact (FONSI). The public involvement activities to date are summarized below.

Notice of Entry for Survey or Investigation Letters  
Notice of Entry letters were mailed to potentially affected property owners near the project area on September 20, 2018 notifying them about the project and that individuals responsible for land surveying and field activities may be seen in the area (Appendix F, pages F-1 to F-3).

Section 106 Public Notice  
To meet the public involvement requirements of Section 106, FHWA's finding of *No Adverse Effect*, a notice was advertised in the *Indianapolis Star* on March 21, 2019. The public comment period closed 30 days later on April 20, 2019. No public comments were received. The text of the public notice and the affidavit of publication appear in Appendix C, pages C-78 to C-80.

Public Information Meeting  
Invitation letters were mailed to nearby property owners on September 11, 2018 (Appendix F, pages F-4 to F-7). A public information meeting was held on October 2, 2018 at the Avon Town Hall located at 6570 East US 36 in Avon. The doors opened at 5:30 pm and the presentation began at 6:00 pm. A sign-in sheet was offered, and the public was provided an informational handout upon signing in (Appendix F, pages F-8 to F-12). The project was introduced to the public, and the purpose and need, preliminary design plans, project schedule, maintenance of traffic, and environmental process were described (Appendix F, pages F-13 to F-22). Meeting participants were asked to discuss their concerns and provide written comments on the project. Oral comments/questions were received and generally pertained to maintenance of traffic during construction and the construction time frame. Generally, written comments/questions pertained to sequencing of traffic lights, use of U-turns at intersections, installing frontage roads along US 36, and "personalizing" US 36 within the project area (Appendix F, pages F-23 to F-28). During the meeting, interested CAC volunteers were asked to sign up and provide contact details for upcoming meeting(s) (Appendix F, page F-29).

CAC Meeting  
The CAC is comprised of a small group of volunteers representing a broad segment of the population within an affected community. The CAC provides opportunities for citizens and other interested groups to participate in current transportation planning efforts such as the preliminary design, environmental studies, maintenance

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of traffic, etc. The role of a CAC member is to provide information about the project to the groups they represent. In this role, CAC members can bring information from the public to the project team (e.g. project sponsors and designers). A CAC was established for the project to gather additional feedback and concerns.

Invitation e-mails were sent to CAC members on December 6, 2018 (Appendix F, page F-30). The CAC list included business leaders, community leaders, and emergency response facilities (Appendix F, page F-31). A CAC meeting was held on December 14, 2018 at the Avon Town Hall from 6:00 pm to 7:30 pm. A sign-in sheet was offered, and CAC members were provided an informational handout upon signing in (Appendix F, pages F-31 and F-32). The role of the CAC was discussed, and the purpose and need, preliminary design plans, project schedule, and maintenance of traffic were described (Appendix F, pages F-33 to F-40). Meeting participants were asked to discuss concerns of the community. Generally, comments/questions pertained to maintenance of traffic during construction, construction time frame, use of a mountable median, and the importance of disseminating information to the public about construction and traffic pattern changes prior to construction (Appendix F, page F-41).

Invitation e-mails were sent to CAC members on April 30, 2019 (Appendix F, page F-43). A second CAC meeting was held on May 21, 2019 at the Avon Town Hall from 3:00 pm to 5:00 pm. A sign-in sheet was offered (Appendix F, page F-44), and the status of the project, environmental document, and Stage 2 design plans were discussed (Appendix F, pages F-45 to F-49). Meeting participants were asked to discuss concerns of the community. Generally, comments/questions pertained to the aesthetics of the proposed concrete median, ways to identify where mountable curbs are in place post construction, and coordinating the timing of traffic signals for emergency response vehicles within the corridor (Appendix F, page F-50).

### Fire Chief Meeting

A meeting was held with RQAW, the Town of Avon, and the Assistant Fire Chief of the Town of Avon on February 27, 2019. The primary purpose of the meeting was to discuss concerns about emergency response vehicles maneuvering around traffic during construction. The Assistant Fire Chief expressed concerns about navigating through stopped traffic during an emergency run especially since the two-way-left-turn-lane will be replaced with a curbed median. It was decided that specifying mountable curbs at strategic locations (i.e. not directly across from right in, right out access points) to allow emergency response vehicles to cross over the median to the other side of the road is the solution. These locations will need to be delineated to allow drivers of the emergency response vehicles to easily identify the locations. RQAW is currently identifying these locations (Appendix F, page F-42).

### Public Hearing

The project is being processed as an Environmental Assessment (EA). Per the current *Indiana Department of Transportation (INDOT) Public Involvement Manual* the project sponsor is required to hold a public hearing. Upon release of the EA for public involvement, a legal advertisement will be placed in a local publication notifying the public of the public hearing and availability of the EA for review. The public will be provided a 30-day comment period. Following the public hearing, a request for a Finding of No Significant Impact (FONSI) will be submitted to the Federal Highway Administration (FHWA). All comments received during this period will be addressed and attached to the FONSI request. If any comments require a change to the EA, an Additional Information document may be prepared and approved by FHWA prior to the submission of the FONSI request to FHWA. The approval of the FONSI by FHWA will indicate the NEPA process for this project has been completed. Once the NEPA process is completed, a public notice announcing the availability of the FONSI will be advertised in local publications of general circulation.

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Public Controversy on Environmental Grounds

Will the project involve substantial controversy concerning community and/or natural resource impacts? Yes No X

Remarks: Currently, there is no substantial public controversy concerning impacts to the or to natural resources.

Part II - General Project Identification, Description, and Design Information

Sponsor of the Project: Indiana Department of Transportation INDOT District: Crawfordsville
Local Name of the Facility: US 36 (Rockville Road)

Funding Source (mark all that apply): Federal X State X Local Other\*

\*If other is selected, please identify the funding source:

PURPOSE AND NEED:

Describe the transportation problem that the project will address. The solution to the traffic problem should NOT be discussed in this section. (Refer to the CE Manual, Section IV.B.2. Purpose and Need)

The primary need for the project is due to the existing level of service motorists experience when traveling through the project area. Two major intersections (Ronald Reagan Parkway and Raceway Road) are within the project area and both intersections operate at a Level of Service (LOS) of E (unstable flow, operating at capacity) during AM and PM peak hours. Per a Traffic Operations Analysis (EMCS, 2018), between 2016 and 2017, traffic volumes along US 36 have increased approximately 2.5% which is likely due to new developments within the corridor from 2016 to 2017, especially within the western project area. A secondary need for the project is due to the number of accidents within the project area. Per crash records between January 2010 and December 2014, at least 45 accidents have occurred along the roadway within project area and 311 crashes have occurred at intersections within the project area. The type of collisions consisted of head-on, left turn, left/right turn, opposite direction sideswipe, ran off road, right angle, right turn, and same direction sideswipe; the majority consisted of rear end collisions.

The primary purpose of the project is to reduce vehicular delays along US 36, with an emphasis on the heavily congested portion of US 36 located within the project area (i.e. between Ronald Reagan Parkway and Raceway Road). Post construction (design year 2041), the LOS is anticipated to improve to a LOS of D (approaching unstable flow) for the US 36/Ronald Reagan Parkway Intersection and to a LOS of C (stable flow, at or near free flow) for the US 36/Raceway Road Intersection during AM and PM peak hours. A secondary purpose of the project is to reduce the crash rate within the project area.

PROJECT DESCRIPTION (PREFERRED ALTERNATIVE):

County: Hendricks and Marion Municipality: Town of Avon

Limits of Proposed Work: The project limits along US 36/Rockville Road begin at Shiloh Park Drive (RP 64+67) and terminate approximately 1,500 feet east of Raceway Road (RP 65+68), for a total length of approximately 1.1 miles.

Total Work Length: 1.1 Mile(s) Total Work Area: 21.5 Acre(s)

Is an Interchange Modification Study / Interchange Justification Study (IMS/IJS) required? Yes1 No X



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If yes, when did the FHWA grant a conditional approval for this project?

Date:

<sup>1</sup>If an IMS or IJS is required; a copy of the approved CE/EA document must be submitted to the FHWA with a request for final approval of the IMS/IJS.

In the remarks box below, describe existing conditions, provide in detail the scope of work for the project, including the preferred alternative. Include a discussion of logical termini. Discuss any major issues for the project and how the project will improve safety or roadway deficiencies if these are issues.

The Federal Highway Administration (FHWA) and the INDOT Crawfordsville District propose to proceed with an added travel lanes project located on US 36, known locally as Rockville Road, through the Town of Avon in Hendricks and Marion counties, Indiana. The project limits on US 36 extend east from Shiloh Park Drive (RP 64+67) to approximately 1,500 feet east of Raceway Road (65+68), for a total length of approximately 1.1 miles. The termini for the project are logical because this portion of US 36 (i.e. between Ronald Reagan Parkway and Raceway Road) is, overall, a heavily congested portion of the US 36 corridor. Project limits extend approximately 1,500 feet east of Raceway Road to fully incorporate the proposed left-turn-lane for both east and westbound traffic at the US 36/Raceway Road Intersection. The project is within Washington Township (Hendricks County), Wayne Township (Marion County), Clermont U.S. Geological Survey (USGS) Quadrangle, Township 15 North, Range 2 East, and Sections 4, 5, 8 and 9. The project area is in a developed area surrounded by residential and commercial properties (Appendix A, pages A-1 to A-5).

**Existing Conditions:** US 36 is functionally classified as a Principal Arterial. US 36, within the project area (i.e. between Ronald Reagan Parkway and Raceway Road), is a heavily congested portion of the overall US 36 corridor. Within the project area, US 36 generally consists of two 12-foot wide travel lanes, one 16-foot wide two-way center left-turn lane and up to 11-foot wide paved shoulders for east and westbound directions. A discontinuous 12-foot wide right-turn lane is present at the intersection approaches within project area. Existing drainage within the project area is adequate. Existing roadway drainage is handled via a combination of open ditches, driveway/approach pipe culverts (generally 15 to 30 inches in diameter) and an enclosed drainage system consisting of roadway inlets and yard catch basins. Within the project area, the existing right-of way width along US 36 varies from approximately 75 to 135 from the roadway centerline. Existing 10-foot wide trails are located north of US 36, both east and west of Ronald Reagan Parkway; both trails are part of the Ronald Reagan Parkway Corridor (Appendix A, pages A-3 to A-5).

**Preferred Alternative:** The preferred alternative will involve milling, resurfacing and widening the existing US 36 roadway (Des. Number 1601072). The proposed cross section will consist of one continuous 11-foot wide travel lane and two continuous 12-foot wide travel lanes with a discontinuous 11-foot wide right-turn-lane bordered by curb and gutter with a 2-foot offset for east and westbound directions. An 11-foot wide left-turn-lane will be provided at each signalized intersection for east and westbound directions. The existing curb ramps in the northeast and northwest quadrants of the US 36/Ronald Reagan Parkway Intersection will be replaced to meet Americans with Disabilities (ADA) requirements. The portion of roadway between the curb ramps will be striped to indicate a trail crossing. An at-grade pedestrian crossing, and median pedestrian refuge will be installed at the US 36 and Ronald Reagan Parkway Intersection to connect the existing trail located north of US 36, along the west side of Ronald Reagan Parkway, to the proposed trail located south of US 36, along the west side of Ronald Reagan Parkway (Des. Number 1601121). Des. Number 1601121 will be constructed by the Town of Avon prior to this added travel lanes project (Des. Number 1601072); therefore, coordination between both designers has been occurring during design of both projects. Des. Number 1601121 will terminate just south of the added travel lanes project; the added travel lanes project will fill in this gap by constructing the remainder of the trail to provide connectivity of the trail along both sides of US 36. Four curb ramps will be constructed (two in the median pedestrian refuge, one north of the median pedestrian refuge, and one south of the median pedestrian refuge). The portion of roadway between these four curb ramps will be striped to indicate a trail crossing. There is no existing curb ramp in the southeast quadrant of the US 36/Ronald Reagan Parkway Intersection. This project will not include installation of a new curb ramp at this location as there are no pedestrian facilities in this quadrant. Per coordination with the Town of Avon

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Assistant Fire Chief, mountable curbs (i.e. rolling curbs) will be installed at strategic locations to allow emergency response vehicles to cross over the median to the other side of the road. RQAW is currently identifying these locations and these locations will be incorporated into the final design plans. Existing traffic signals will also be replaced (Des. Number 1901655).

The existing horizontal alignment of US 36 will remain unchanged. The profile grade will match the existing profile grade. Because of the widened roadway, drainage will be addressed. Where needed, existing driveway/approach drainage pipe culverts will be replaced, some structures will have a larger diameter. Curb turnouts with ditches and driveway/approach pipe culverts are proposed to handle drainage between Shiloh Park Drive and Raceway Road. Between Raceway Road and the east end of the project, an enclosed drainage system is proposed to match the existing drainage system; some inlets may be moved farther from the road in this section. Work to the existing 36-inch diameter corrugated metal pipe (CMP) east of the US 36/Ronald Reagan Parkway Intersection (identified as Structures 26 and 27 on current plan sheets) will involve extending the length of the pipe approximately 11 feet north of US 36 and approximately 17 feet south of US 36. Work to Structure CV 036-032-64.80 that conveys Avon Creek under US 36 will involve extending the length of the pipe approximately 10 feet north of US 36 and approximately 22 feet south of US 36 (Des. Number 1901657) (Appendix A, pages A-45 to A-49).

Within the project area, an additional travel lane will be added in each direction (eastbound and westbound) which will reduce delays along the corridor. Within the project area, left turn lanes will be extended the length of the through lane queue to allow for left turning vehicles to move out of the through lane into the left turn lane. Within the project area, a raised median will be constructed to prevent vehicles from turning left across traffic. Per an Interactive Highway Safety Design Model (IHSDM) analysis, the inclusion of an additional through travel lane in each direction and the addition of a raised median is predicted to reduce the crash within the project area by 45%.

The maximum depth of excavation will be up to approximately eight feet below ground surface (bgs). Approximately 0.20 acre of permanent right-of-way and 0.45 acre of temporary right-of-way will be required for the project. No residences or businesses will be relocated. Construction will be phased, and two lanes of traffic will always be maintained in each direction. Access to all properties will be maintained during construction (Appendix A, pages A-22 to A-44). Refer to the *Maintenance of Traffic (MOT) During Construction* section of this document for further details on the proposed MOT. The estimated project cost is \$9,414,000 (fiscal year [FY] 2021). The project is currently scheduled to let in early 2021 with construction anticipated to begin in the spring of 2021.

This project demonstrates independent utility because it is a "stand alone" project; it is not an interdependent part of a larger action and it does not require prior or simultaneous actions to be taken for this project to proceed. The project has been designed to specifically reduce vehicular delays and crash rates within the project area. The project can be constructed regardless if any other projects are currently or will be planned in the area.

Post construction, traffic is not expected to back up west of the project area. Per coordination with the project designer, an entire traffic analysis and simulation was developed to determine the optimum location to eliminate the third westbound lane.

The preferred alternative satisfies the purpose and need of the project by reducing vehicular delays along US 36, with an emphasis on the heavily congested portion of US 36 located within the project area (i.e. between Ronald Reagan Parkway and Raceway Road) via improving the LOS at both intersections and reducing the crash rate within the project area and at both intersections.

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### OTHER ALTERNATIVES CONSIDERED:

Describe all discarded alternatives, including the Do-Nothing Alternative and an explanation of why each discarded alternative was not selected.

**Do Nothing Alternative:** This alternative would not involve any improvements to the existing roadway. This alternative would require no additional permanent or temporary right-of-way, not involve any cost, and would not result in any impacts to the surrounding environment. The Do Nothing Alternative would not reduce vehicular delays or crash rates within the project area. This alternative does not meet the purpose and need of the project and was therefore dismissed from further consideration.

**Added Travel Lanes from CR 900 East to 1,500 feet east of Raceway Road with Dual Left Turn Lanes at Ronald Reagan Parkway and Raceway Road:** This alternative would involve adding travel lanes from CR 900 East and to 1,500 feet east of Raceway Road. Dual left turn lanes would be added at Ronald Regan Parkway and Raceway Road. This alternative would address the purpose and need of the project via reducing vehicular delays at the Ronald Reagan Parkway and Raceway Road intersections; however, this alternative would involve greater wetland impacts at the Ronald Regan Parkway intersection and result in higher costs that exceed the programmed budget. There would not be any other increased impacts over the preferred alternative. In addition, the traffic operation analysis report demonstrates that the US 36/Ronald Reagan Parkway and US 36/Raceway Road intersections would continue to operate at an acceptable LOS of D (approaching unstable flow) with a single left turn lane along the US 36 eastbound and westbound approaches through 2041. Therefore, this alternative was not selected.

No other alternatives were considered.

**The Do Nothing Alternative is not feasible, prudent or practicable because (Mark all that apply):**

- It would not correct existing capacity deficiencies;
- It would not correct existing safety hazards;
- It would not correct the existing roadway geometric deficiencies;
- It would not correct existing deteriorated conditions and maintenance problems; or
- It would result in serious impacts to the motoring public and general welfare of the economy.
- Other (Describe)

<input checked="" type="checkbox"/>
<input checked="" type="checkbox"/>
<input type="checkbox"/>
<input type="checkbox"/>
<input type="checkbox"/>
<input type="checkbox"/>

### ROADWAY CHARACTER:

**US 36/Rockville Road**

Functional Classification:	Urban Principal Arterial				
Current ADT:	44,398	VPD (2021)	Design Year ADT:	48,838	VPD (2041)
Design Hour Volume (DHV):	4,900	Truck Percentage (%)	4		
Designed Speed (mph):	45	Legal Speed (mph):	45		

	Existing	Proposed
Number of Lanes:	3-5	8-10
Type of Lanes:	Two 12-foot wide travel lanes One 16-foot wide center two-way left turn lane Two 12-foot wide right turn lanes (various locations)	Four 12-foot wide travel lanes Two 11-foot wide travel lanes Two 11-foot wide center left turn lanes Two 11-foot wide right turn lanes (various locations)
Pavement Width:	40-64	92-114
Shoulder Width:	11	N/A

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Median Width:	4	ft.		7	ft.
Sidewalk Width:	0	ft.		0	ft.

Setting:  Urban  Suburban  Rural  
 Topography:  Level  Rolling  Hilly

**This project will not require any work to Ronald Reagan Parkway or Raceway Road. As such, some of the below design data is not available (N/A).**

### Ronald Reagan Parkway

Functional Classification: Principal Arterial  
 Current ADT: N/A VPD (20xx)          Design Year ADT: N/A VPD (20xx)           
 Design Hour Volume (DHV): N/A Truck Percentage (%) N/A  
 Designed Speed (mph): N/A Legal Speed (mph): 45

	Existing		Proposed
Number of Lanes:	7	ft.	Same as existing (N/A)
Type of Lanes:	Four 12-foot wide travel lanes Two 12-foot wide left turn lanes One 12-foot wide right turn lane		N/A
Pavement Width:	72	ft.	N/A
Shoulder Width:	0	ft.	N/A
Median Width:	0	ft.	N/A
Sidewalk Width:	0	ft.	N/A

Setting:  Urban  Suburban  Rural  
 Topography:  Level  Rolling  Hilly

### Raceway Road

Functional Classification: Minor Arterial  
 Current ADT: N/A VPD (20xx)          Design Year ADT: N/A VPD (20xx)           
 Design Hour Volume (DHV): N/A Truck Percentage (%) N/A  
 Designed Speed (mph): N/A Legal Speed (mph): 40

	Existing		Proposed
Number of Lanes:	5	ft.	Same as existing (N/A)
Type of Lanes:	Three 12-foot wide travel lanes One 12-foot wide way left turn lane One 12-foot wide right turn lane		N/A
Pavement Width:	60	ft.	N/A
Shoulder Width:	0	ft.	N/A
Median Width:	0	ft.	N/A
Sidewalk Width:	0	ft.	N/A

Setting:  Urban  Suburban  Rural  
 Topography:  Level  Rolling  Hilly

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### DESIGN CRITERIA FOR BRIDGES:

Structure/NBI Number(s): N/A Sufficiency Rating: N/A  
 (Rating, Source of Information)

	Existing		Proposed
Bridge Type:	192-inch concrete arch top box culvert		N/A
Number of Spans:	1		1
Weight Restrictions:	N/A	ton	N/A
Height Restrictions:	N/A	ft.	N/A
Curb to Curb Width:	N/A	ft.	N/A
Outside to Outside Width:	N/A	ft.	N/A
Shoulder Width:	N/A	ft.	N/A
Length of Channel Work:			N/A

*Describe bridges and structures; provide specific location information for small structures.*

Remarks: No bridges are located within the project area. A 192-inch concrete arch top box culvert (unknown structure number and not given a structure number on current plan sheets) transports Avon Creek under Shiloh Crossing Drive, north of US 36 and west of the US 36/Ronald Reagan Parkway Intersection. This structure is located within the construction limits; however, it will not be modified per this project (Appendix A, page A-46). Work will be restricted to Shiloh Crossing Drive and will not impact Avon Creek.

Will the structure be rehabilitated or replaced as part of the project?  Yes  No  N/A  
*If the proposed action has multiple bridges or small structures, this section should be filled out for each structure.*

Structure/NBI Number(s): CV 036-032-64.80 Sufficiency Rating: Condition Rating 7 (INDOT Culvert Inspection Report, dated December 24, 2015) (Appendix H, page H-7)  
 (Rating, Source of Information)

	Existing		Proposed
Bridge Type:	108-inch diameter CMP		108-inch diameter CMP
Number of Spans:	1		1
Weight Restrictions:	N/A	ton	N/A
Height Restrictions:	N/A	ft.	N/A
Curb to Curb Width:	N/A	ft.	N/A
Outside to Outside Width:	N/A	ft.	N/A
Shoulder Width:	N/A	ft.	N/A
Length of Channel Work:			32

*Describe bridges and structures; provide specific location information for small structures.*

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Remarks: A 108-inch diameter and 183-foot long CMP (CV 036-032-64.80 and identified as Structures 15 and 17 on current design plan sheets) transports Avon Creek under US 36, west of the US 36/Ronald Reagan Parkway Intersection. Work to the structure includes extending the length of the pipe approximately 10 feet north of US 36 and approximately 22 feet south of US 36 (Des. Number 1901657) (Appendix A, page A-46). This will impact approximately 32 linear feet of Avon Creek below the ordinary high water mark (OHWM).

Will the structure be rehabilitated or replaced as part of the project?  Yes  No  N/A  
*If the proposed action has multiple bridges or small structures, this section should be filled out for each structure.*

Structure/NBI Number(s): N/A Sufficiency Rating: N/A  
 (Rating, Source of Information)

	Existing		Proposed
Bridge Type:	30-inch diameter CMP		N/A
Number of Spans:	1		N/A
Weight Restrictions:	N/A	ton	N/A
Height Restrictions:	N/A	ft.	N/A
Curb to Curb Width:	N/A	ft.	N/A
Outside to Outside Width:	N/A	ft.	N/A
Shoulder Width:	N/A	ft.	N/A
Length of Channel Work:			220
			ft.

*Describe bridges and structures; provide specific location information for small structures.*

Remarks: One 30-inch diameter CMP (unknown structure number and not given a structure number on current design plan sheets) transports unnamed tributary (UNT) 2 to Avon Creek, south of US 36 and west of the US 36/Ronald Reagan Parkway Intersection. Work to this structure includes its removal (Appendix A, page A-46). Due to the roadway widening, approximately 220 linear feet of UNT 2 to Avon Creek will be shifted approximately 15 feet south of the roadway. This will result in approximately 220 linear feet (0.01 acre) of stream impacts to UNT 2 of Avon Creek.

Will the structure be rehabilitated or replaced as part of the project?  Yes  No  N/A  
*If the proposed action has multiple bridges or small structures, this section should be filled out for each structure.*

Structure/NBI Number(s): N/A Sufficiency Rating: N/A  
 (Rating, Source of Information)

	Existing		Proposed
Bridge Type:	30-inch diameter CMP		36-inch diameter CMP
Number of Spans:	1		1
Weight Restrictions:	N/A	ton	N/A
Height Restrictions:	N/A	ft.	N/A
Curb to Curb Width:	N/A	ft.	N/A
Outside to Outside Width:	N/A	ft.	N/A
Shoulder Width:	N/A	ft.	N/A
Length of Channel Work:			X
			ft.

*Describe bridges and structures; provide specific location information for small structures.*

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Remarks: One 30-inch diameter CMP (unknown structure number and not given a structure number on current design plan sheets) transports a roadside ditch located just west of the UNT to Shiloh Creek, north of US 36. Work to the structure includes replacing it with a 36-inch diameter CMP and extending it 44 feet to the northeast to redirect the flow of the roadside ditch into the UNT of Shiloh Creek. Work also includes placing riprap within the stream channel to prevent erosion of the stream channel (Appendix A, page A-49). This will result in approximately 20 linear feet (0.0009 acre) of stream impacts to UNT to Shiloh Creek.

Will the structure be rehabilitated or replaced as part of the project?  Yes  No  N/A  
*If the proposed action has multiple bridges or small structures, this section should be filled out for each structure.*

Structure/NBI Number(s): N/A Sufficiency Rating: N/A  
 (Rating, Source of Information)

	Existing		Proposed
Bridge Type:	30 to 36-inch diameters CMPs		30 to 36-inch diameters CMPs
Number of Spans:	1		1
Weight Restrictions:	N/A	ton	N/A
Height Restrictions:	N/A	ft.	N/A
Curb to Curb Width:	N/A	ft.	N/A
Outside to Outside Width:	N/A	ft.	N/A
Shoulder Width:	N/A	ft.	N/A
Length of Channel Work:			N/A

*Describe bridges and structures; provide specific location information for small structures.*

Remarks: 

One 30-inch diameter CMP (unknown structure number and not given a structure number on current design plan sheets) transports a roadside ditch under Shiloh Crossing Drive, north of US 36 and west of the US 36/Ronald Reagan Parkway Intersection. Work to this structure includes replacing it with a 36-inch diameter CMP (Appendix A, page A-45). This structure will not impact any jurisdictional waterways.

One 30-inch diameter CMP (unknown structure number and not given a structure number on current design plan sheets) transports a roadside ditch under Shiloh Park Drive, north of US 36 and west of the US 36/Ronald Reagan Parkway Intersection. This structure is located outside the construction limits and will not be modified per this project. Impacts to this structure are not expected (Appendix A, page A-45).

One 30-inch diameter CMP (unknown structure number and identified as Structures 10, 11 and 12 on current design plan sheets) transports a roadside ditch under Shiloh Crossing Drive, south of US 36 and west of the US 36/Ronald Reagan Parkway Intersection. Work to this structure includes replacing it with 36-inch diameter CMP (Appendix A, page A-45). This structure will not impact any jurisdictional waterways.

A 36-inch diameter CMP (unknown structure number and identified as Structures 26 and 27 on current plan sheets) transports roadside drainage under US 36, east of the US 36/Ronald Reagan Parkway Intersection (Appendix A, page A-47). Work to this structure includes extending the length of the pipe approximately 28 feet (11 feet north of US 36 and 17 feet south of US 36). This structure will not impact any jurisdictional waterways.

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Several small storm sewer drain pipes and drain inlets will be installed and/or replaced beneath and along the roadway to convey storm water (Appendix A, pages A-45 to A-49). The new storm sewer drain pipes and drain inlets will maintain the existing drainage pattern and convey storm water runoff to Avon Creek and Shiloh Creek (outside the project area), which are jurisdictional waterways. Where needed, existing driveway/approach drainage pipe culverts will be replaced; none of these structures are associated with jurisdictional waterways.

Will the structure be rehabilitated or replaced as part of the project? **Yes** **No** **N/A**  
    
*If the proposed action has multiple bridges or small structures, this section should be filled out for each structure.*

**MAINTENANCE OF TRAFFIC (MOT) DURING CONSTRUCTION:**

	Yes	No
Is a temporary bridge proposed?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Is a temporary roadway proposed?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Will the project involve the use of a detour or require a ramp closure? (describe in remarks)	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Provisions will be made for access by local traffic and so posted.	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Provisions will be made for through-traffic dependent businesses.	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Provisions will be made to accommodate any local special events or festivals.	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Will the proposed MOT substantially change the environmental consequences of the action?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Is there substantial controversy associated with the proposed method for MOT	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Remarks: The MOT for the project will be phased and two lanes of traffic will always be maintained in each direction (Appendix A, pages A-22 to A-44). The lane restrictions will pose a temporary inconvenience to traveling motorists (including school buses and emergency services); however, no significant delays are anticipated, and all inconveniences will cease upon project completion. Access to all businesses will be maintained during construction.

**ESTIMATED PROJECT COST AND SCHEDULE:**

Engineering: \$ 240,000 (2018) Right-of-Way: \$ 25,000 (2020) Construction: \$ 9,149,000 (2021)

Anticipated Start Date of Construction: Spring of 2021

Date project incorporated into STIP July 2, 2019 (Appendix G, page G-3). The STIP amount (\$9,414,000) and TIP amount (\$16,052,366) do not agree. An amendment to update the STIP/TIP will be needed to account for this discrepancy.

Is the project in an MPO Area? **Yes** **No**  
   
 If yes,

Name of MPO Indianapolis Metropolitan Planning Organization (IMPO)

Location of Project in TIP <https://mitip.indympo.org/>

Date of incorporation by reference into the STIP July 2, 2019 (Appendix G, page G-5)

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<b>RIGHT OF WAY:</b>
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Land Use Impacts	Amount (acres)	
	Permanent	Temporary
<b>Residential</b>	<b>0</b>	<b>0</b>
<b>Commercial</b>	<b>0.20</b>	<b>0.45</b>
<b>Agricultural</b>	<b>0</b>	<b>0</b>
<b>Forest</b>	<b>0</b>	<b>0</b>
<b>Wetlands</b>	<b>0</b>	<b>0</b>
<b>Other:</b>	<b>0</b>	<b>0</b>
<b>TOTAL</b>	<b>0.20</b>	<b>0.45</b>

*Describe both Permanent and Temporary right-of-way and describe their current use. Typical and Maximum right-of-way widths (existing and proposed) should also be discussed. Any advance acquisition or reacquisition, either known or suspected, and there impacts on the environmental analysis should be discussed.*

Remarks:

The current existing right-of-way along US 36 varies from approximately 75 to 135 feet from the roadway centerline; the proposed right-of-way along US 36 will vary from approximately 70 to 100 feet from the roadway centerline. The current existing right-of-way along Ronald Reagan Parkway varies from approximately 110 to 200 feet from the roadway centerline; the proposed right-of-way along Ronald Reagan Parkway will remain the same. The current existing right-of-way along Raceway Road varies from approximately 25 to 80 feet from the roadway centerline; the proposed right-of-way along Raceway Road will remain the same.

The project will require approximately 0.20 acre of permanent right-of-way from commercial land; this right-of-way consists of maintained lawn. The commercial land is along US 36, primarily near the US 36/Ronald Reagan Parkway Intersection and the US 36/Raceway Road Intersection. The project will require approximately 0.45 acre of temporary right-of-way from commercial land for driveway reconstruction and grading. The commercial land is along US 36, primarily between and near the US 36/Ronald Reagan Parkway Intersection and the US 36/Raceway Road Intersection (Appendix A, pages A-3 to A-5).

Please note that the early coordination letter stated approximately 0.25 acre of permanent right-of-way and 0.75 acre of temporary right-of-way would be needed (Appendix B, pages B-1 to B-4). Since that time, design has been refined and right-of-way amounts have decreased.

If the scope of work or permanent or temporary right-of-way amounts change, the INDOT Environmental Services Division and the INDOT District Environmental Section will be contacted immediately.

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**Part III – Identification and Evaluation of Impacts of the Proposed Action**

**SECTION A – ECOLOGICAL RESOURCES**

**Streams, Rivers, Watercourses & Jurisdictional Ditches**

- Federal Wild and Scenic Rivers
- State Natural, Scenic or Recreational Rivers
- Nationwide Rivers Inventory (NRI) listed
- Outstanding Rivers List for Indiana
- Navigable Waterways

	<u>Presence</u>	<u>Impacts</u>	
		<u>Yes</u>	<u>No</u>
	X	X	

Remarks:

Per a desktop review, a field visit conducted on May 25, 2018 and April 3, 2019 by RQAW (Appendix A, pages A-6 to A-15), an aerial photograph of the project area (Appendix A, pages A-3 to A-5), USGS topographic map (Appendix A, page A-2), and the water resources map in the Red Flag Investigation (RFI) report (Appendix D, page D-8), there are four stream segments located within 0.5 mile of the project area. A *Waters of the U.S. Determination Report* was completed for the project by RQAW on May 1, 2019 (Appendix E, pages E-1 to E-42). It was determined five streams, Avon Creek, UNTs 1 and 2 to Avon Creek, Shiloh Creek, and UNT to Shiloh Creek, are located within or adjacent to the project area. Shiloh Creek is not described in the *Waters of the U.S. Determination Report* but is located approximately 90 feet east of the construction limits. As such, Shiloh Creek is included in this section. The U.S. Army Corps of Engineers (USACE) makes all determinations regarding jurisdiction.

Per the field visit, and as described in the *Waters of the U.S. Determination Report*, 13 roadside ditches were identified within the project area (Appendix A, pages A-3 to A-5). The ditches are along the north and south sides of US 36 and convey stormwater drainage from the existing roadway to Avon Creek, UNT 1 to Avon Creek, and UNT 2 to Avon Creek. The roadside ditches did not exhibit OHWM characteristics, a defined bed and bank, and are not captured streams. Therefore, the roadside ditches are not likely to be considered jurisdictional (i.e. a Waters of the United States).

**Avon Creek** is an intermittent stream that flows in a northwest to southeast direction under US 36 (Appendix A, page A-4). The predominant substrate consisted of gravel and sand; crayfish were also observed within the stream. The upstream drainage area is approximately 1.03 square miles (Appendix E, page E-14). The stream exhibited a defined bed and bank, had OHWM characteristics of 6.83 feet in width and 0.66 feet in depth, and empties into the East Fork of White Lick Creek which flows into the White River, a Traditionally Navigable Waterway (TNW). Based on these criteria, this stream is likely to be considered jurisdictional (i.e. a Waters of the United States). Avon Creek is not listed as a Federal Wild and Scenic River; State Natural, Scenic and Recreational River; or as an Indiana Department of Natural Resources (IDNR) Outstanding River.

A 108-inch diameter and 183-foot long CMP (CV 036-032-64.80 and identified as Structures 15 and 17 on current design plan sheets) transports Avon Creek under US 36, west of the US 36/Ronald Reagan Parkway Intersection. Work to the structure includes extending the length of the pipe approximately 10 feet north of US 36 and approximately 22 feet south of US 36 (Des. Number 1601657) (Appendix A, page A-46). This will impact approximately 32 linear feet (0.005 acre) of Avon Creek below the OHWM.

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Avon Creek is impaired with *Escherichia coli* (E. coli). Workers who will be in or near water with E. coli should take care to wear appropriate personal protective equipment (PPE), observe proper hygiene procedures, including regular hand washing, and limit personal exposure.

**UNT 1 to Avon Creek** flows in a northeast to southwest direction on the north side of US 36 (Appendix A, page A-4). The predominant substrate consisted of gravel and sand. The upstream drainage area is less than 0.1 square mile. The stream exhibited a defined bed and bank, had OHWM characteristics of 3.5 feet in width and 0.25 feet in depth, and empties into Avon Creek. Based on these criteria, this stream is likely to be considered jurisdictional (i.e. a Waters of the United States). UNT 1 to Avon Creek is not listed as a Federal Wild and Scenic River; State Natural, Scenic and Recreational River; or as an IDNR Outstanding River. Due to the roadway widening, approximately 625 linear feet of UNT 1 to Avon Creek will be shifted approximately 10 feet north of the roadway. This will result in approximately 625 linear feet (0.05 acre) of stream impacts to UNT 1.

**UNT 2 to Avon Creek** flows in a northwest to southeast direction on the south side of US 36; it begins at the concrete channel of a roadside ditch (Appendix A, page A-4). UNT 2 to Avon Creek is also impaired with E. coli. The predominant substrate consisted of silt. The upstream drainage area is less than 0.1 square mile. The stream exhibited a defined bed and bank, had OHWM characteristics of 2.08 feet in width and 0.17 feet in depth, and empties into Avon Creek. Based on these criteria, this stream is likely to be considered jurisdictional (i.e. a Waters of the United States). UNT 2 to Avon Creek is not listed as a Federal Wild and Scenic River; State Natural, Scenic and Recreational River; or as an IDNR Outstanding River.

One 30-inch diameter CMP (unknown structure number and not given a structure number on current design plan sheets) transports UNT 2 to Avon Creek, south of US 36 and west of the US 36/Ronald Reagan Parkway Intersection. Work to this structure includes its removal (Appendix A, page A-46). Due to the roadway widening, approximately 220 linear feet of UNT 2 to Avon Creek will be shifted approximately 15 feet south of the roadway. This will result in approximately 220 linear feet (0.01 acre) of stream impacts to UNT 2 of Avon Creek.

**Shiloh Creek** flows in a northwest to south direction under US 36 (Appendix A, page A-5). The predominant substrate consisted of silt and sand. Shiloh Creek is impaired with E. coli and impaired biotic communities (IBC). The upstream drainage area is approximately 0.65 square mile (Appendix E, page E-44). The stream exhibited a defined bed and bank, had OHWM characteristics of 7.25 feet in width and 1.16 feet in depth, and empties into the East Fork of White Lick Creek which flows into the White River, a TNW. Based on these criteria, this stream is likely to be considered jurisdictional (i.e. a Waters of the United States). Shiloh Creek is not listed as a Federal Wild and Scenic River; State Natural, Scenic and Recreational River; or as an IDNR Outstanding River. The stream is approximately 90 feet east of the construction limits and will not be impacted. Because this stream is outside the project area, structure information is not available per the designer.

**UNT to Shiloh Creek** flows in a southwest to northeast direction on the north side of US 36; it begins where a roadside ditch ends (Appendix A, page A-5). The predominant substrate consisted of silt and sand. The upstream drainage area is less than 0.1 square mile. The stream exhibited a defined bed and bank, had OHWM characteristics of 2.33 feet in width and 0.92 feet in depth, and empties into Shiloh Creek. Based on these criteria, this stream is likely to be considered jurisdictional (i.e. a Waters of the United States). UNT to Shiloh Creek is not listed as a Federal Wild and Scenic River; State Natural, Scenic and Recreational River; or

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as an IDNR Outstanding River. An existing 30-inch diameter CMP (unknown structure number and identified as Structure 72 on current design plans) transports a roadside ditch located just west of the UNT to Shiloh Creek on the north side of US 36. Work to the structure includes replacing it with a 36-inch diameter CMP and extending it 44 feet to the northeast to redirect the flow of the roadside ditch into the UNT of Shiloh Creek. Work also includes placing riprap within the stream channel to prevent erosion of the stream channel (Appendix A, page A-49). This will impact up to approximately 20 linear feet (0.0009 acre) of UNT to Shiloh Creek below the OHWM.

The project will impact approximately 897 linear feet (0.07 acre) of streams. Mitigation will likely be needed due to these stream impacts. Mitigation will be determined during permitting. If possible, the IDNR In-Lieu Fee Program will be utilized for stream mitigation. A USACE Section 404 Permit and Indiana Department of Environmental Management (IDEM) Section 401 Water Quality Certification will be required due to these stream impacts.

Early coordination letters were sent to the USACE, IDNR Division of Fish and Wildlife, and IDEM on January 28, 2019 (Appendix B, pages B-1 to B-4). The USACE did not respond to the early coordination letter. The automatic generated response letter from IDEM was received that same day (Appendix B, pages B-5 to B-13). The IDEM automatic response letter included recommendations pertaining to streams. Recommendations regarding streams include restricting channel disturbance.

The IDNR Division of Fish and Wildlife responded to early coordination efforts on February 28, 2019 with recommendations to avoid or minimize impacts to streams and terrestrial habitat (Appendix B, pages B-15 and B-16). Recommendations regarding streams generally include implementing erosion and sediment control measures, not working within the stream channel from April 1 through June 30, and placement of riprap.

The U.S. Fish and Wildlife Service (USFWS) was not consulted during early coordination because the project meets the criteria for programmatic coordination under the U.S. Fish and Wildlife Service Interim Policy for the Review of Highway Transportation Projects in Indiana dated May 29, 2013. Applicable recommendations include measures similar to the IDNR recommendations. All applicable agency recommendations are included in the Environmental Commitments section of this Categorical Exclusion (CE) document.

**Other Surface Waters**

- Reservoirs
- Lakes
- Farm Ponds
- Detention Basins
- Storm Water Management Facilities
- Other: \_\_\_\_\_

	<u>Presence</u>	<u>Impacts</u>	
		Yes	No
Reservoirs	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Lakes	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Farm Ponds	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Detention Basins	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Storm Water Management Facilities	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Other:	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Remarks: Per a desktop review, a field visit conducted on May 25, 2018 and April 3, 2019 by RQAW (Appendix A, pages A-6 to A-15), an aerial photograph of the project area (Appendix A, pages A-3 to A-5), USGS topographic map (Appendix A, page A-2), and the water resources map in the RFI report (Appendix D, page D-8), there are 19 ponds or lakes (detention basins) located within 0.5 mile of the project area. Of the 19 ponds or lakes, 17 ponds or lakes are unmapped but are shown in the aerial. The nearest surface water is mapped adjacent to the project area; however, per the aerial and field visit, the surface water is not present. Therefore, impacts to other surface waters are not expected. A Waters of the U.S. Determination Report was completed for the

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project by RQAW on May 1, 2019 (Appendix E, pages E-1 to E-42). The USACE makes all determinations regarding jurisdiction.

Early coordination letters were sent to the USACE, IDNR Division of Fish and Wildlife, and the IDEM on January 28, 2019 (Appendix B, pages B-1 to B-4). The USACE did not respond to the early coordination letter. The automatic generated response letter from IDEM was received that same day (Appendix B, pages B-5 to B-13). The IDEM automatic response letter did not include any recommendations pertaining to other surface waters.

The IDNR Division of Fish and Wildlife responded to early coordination efforts on February 28, 2019 with recommendations to avoid or minimize impacts to streams and terrestrial habitat only (Appendix B, pages B-15 and B-16).

**Wetlands**

Presence

Impacts

Yes

No

Total wetland area: 0.25 acre(s)

Total wetland area impacted: 0.13 acre(s)

(If a determination has not been made for non-isolated/isolated wetlands, fill in the total wetland area impacted above.)

Wetland No.	Classification	Total Size (Acres)	Impacted Acres	Comments
Wetland A	PEM	0.03	0.01	Located on the north side of US 36 just west of the US/Ronald Reagan Parkway Intersection; it is within a low-lying swale on the south bank of Avon Creek that holds floodwater during storm events and drains southeast into Avon Creek.
Wetland B	PEM	0.02	0	Located just east of Shiloh Crossing Drive, on the north side of US 36; it is within a low-lying swale on the south bank of Avon Creek that holds floodwater during storm events and drains east into Avon Creek.
Wetland C	PEM	0.01	0	Located on the east side of Ronald Reagan Parkway just north of US 36; it is within a widened section of a roadside ditch and drains via south via a drain inlet near just northeast of the US 36/Ronald Reagan Parkway Intersection. It then flows west into UNT 1 to Avon Creek, which then flows into Avon Creek.
Wetland D	PEM	0.05	0.005	Located on the south side of US 36 just west of the US 36/Ronald Reagan Parkway Intersection; it is within a depression that receives roadside drainage before draining to Avon Creek.
Wetland E	PEM	0.12	0.11	Located on the south side of US 36 just west of the US 36/Ronald Reagan Parkway Intersection; it is within a widened section of UNT 2 and drains east into Avon Creek.

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Wetland F	PEM	0.02	0	Located on the south side of US 36 just east of the US 36/Ronald Reagan Parkway Intersection; it is within a widened section of a roadside ditch.
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### Documentation

### ES Approval Dates

**Wetlands** (Mark all that apply)

Wetland Determination  
 Wetland Delineation  
 USACE Isolated Waters Determination  
 Mitigation Plan

X
X

May 1, 2019
May 1, 2019

**Improvements that will not result in any wetland impacts are not practicable because such avoidance would result in** (Mark all that apply and explain):

- Substantial adverse impacts to adjacent homes, business or other improved properties;
- Substantially increased project costs;
- Unique engineering, traffic, maintenance, or safety problems;
- Substantial adverse social, economic, or environmental impacts, or
- The project not meeting the identified needs.

X

*Measures to avoid, minimize, and mitigate wetland impacts need to be discussed in the remarks box.*

Remarks:

Per a review of the National Wetlands Inventory (NWI) online mapper (<https://www.fws.gov/wetlands/data/mapper.html>), USGS topographic map (Appendix A, page A-2), and the water resources map in the RFI report (Appendix D, page D-8), there are 18 wetlands located within 0.5 mile of the project area. The nearest wetland is mapped adjacent to the project area. A field visit was conducted on May 25, 2018 and April 3, 2019 by RQAW and it was determined six wetlands are located within the project area. A Waters of the U.S. Determination Report was completed for the project by RQAW on May 1, 2019 (Appendix E, pages E-1 to E-42). The USACE makes all determinations regarding jurisdiction.

**Wetland A** is approximately 0.03 acre in size and is a palustrine emergent wetland (PEM) located on the north side of US 36 just west of the US 36/Ronald Reagan Parkway Intersection. It is located within a low-lying swale on the south bank of Avon Creek that holds floodwater during storm events and drains southeast into Avon Creek. Two data points (A1 and A2) were taken to determine the boundary of Wetland A. Data point A1 was taken within Wetland A and exhibited hydrophytic vegetation, hydric soils, and wetland hydrology; thus, meeting the three criteria to be classified as a wetland per the *Corps of Engineers Wetland Delineated Manual* (1987) and the *Regional Supplement to the Corps of Engineers Wetland Delineation Manual: Midwest Region Version 2.0* (2010). Data point A2 was taken approximately 50 feet west of data point A1 and did not meet all three of the criteria to be considered within a wetland (Appendix E, pages E-15 to E-18). This wetland is likely to be considered jurisdictional (i.e. a Waters of the United States) due to its connectivity to Avon Creek, a tributary to the White River, a TNW. The project will impact approximately 0.01 acre of Wetland A within the construction limits.

**Wetland B** is approximately 0.02 acre in size and is a PEM wetland located just east of Shiloh Crossing Drive, on the north side of US 36. It is located within a low-lying swale on the south bank of Avon Creek that holds floodwater during storm events and drains east into Avon Creek. Two data points (B1 and B2) were taken to determine the boundary of Wetland B. Data point B1 was taken within Wetland B and exhibited hydrophytic vegetation, hydric soils, and wetland hydrology; thus, meeting the three criteria to be classified as a wetland. Data point B2 was taken approximately 15 feet east of data point B1 and did not meet all three of the criteria to be considered within a wetland (Appendix E, pages E-19 to E-22). This wetland is likely to be considered

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jurisdictional (i.e. a Waters of the United States) due to its connectivity to Avon Creek, a tributary to the White River, a TNW. Construction limits are approximately 7 feet outside the boundary of Wetland B; therefore, the project will not impact Wetland B.

**Wetland C** is approximately 0.01 acre in size and is a PEM wetland located on the east side of Ronald Reagan Parkway just north of US 36. It is located within a widened section of a roadside ditch and drains south via a drain inlet near just northeast of the US 36/Ronald Reagan Parkway Intersection. It then flows west into UNT 1 to Avon Creek, which then flows into Avon Creek. Two data points (C1 and C2) were taken to determine the boundary of Wetland C. Data point C1 was taken within Wetland C and exhibited hydrophytic vegetation, hydric soils, and wetland hydrology; thus, meeting the three criteria to be classified as a wetland. Data point C2 was taken approximately 10 feet east of data point C1 and did not meet all three of the criteria to be considered within a wetland (Appendix E, pages E-23 to E-26). This wetland is likely to be considered jurisdictional (i.e. a Waters of the United States) due to its connectivity to Avon Creek, a tributary to the White River, a TNW. Construction limits are approximately 3 feet outside the boundary of Wetland C; therefore, the project will not impact Wetland C.

**Wetland D** is approximately 0.05 acre in size and is a PEM wetland located on the south side of US 36 just west of the US 36/Ronald Reagan Parkway Intersection. It is located within a depression that receives roadside drainage before draining to Avon Creek. Two data points (D1 and D2) were taken to determine the boundary of Wetland D. Data point D1 was taken within Wetland D and exhibited hydrophytic vegetation, hydric soils, and wetland hydrology; thus, meeting the three criteria to be classified as a wetland. Data point D2 was taken approximately 20 feet west of data point D1 and did not meet all three of the criteria to be considered within a wetland (Appendix E, pages E-27 to E-30). This wetland is likely to be considered jurisdictional (i.e. a Waters of the United States) due to its connectivity to Avon Creek, a tributary to the White River, a TNW.

Construction limits are approximately 3 feet outside the boundary of Wetland D; however, Wetland D will be impacted by construction activities associated with Des. Number 1601121 prior to this added travel lanes project (Des. Number 1601072). Des. Number 1601121 will involve constructing a segment of the Ronald Reagan Parkway Trail south of US 36, along the west side of Ronald Reagan Parkway. That trail will terminate just south of this added travel lanes project which will fill in this gap by constructing the remainder of the trail to provide connectivity of the trail along both sides of US 36. Des. Number 1601121 will impact approximately 0.005 acre of Wetland D within the construction limits. Mitigation associated with Des. Number 1601121 will be incorporated into this added travel lanes project (Des. Number 1601072).

**Wetland E** is approximately 0.12 acre in size and is a PEM wetland located on the south side of US 36 just west of the US 36/Ronald Reagan Parkway Intersection. It is located within a widened section of UNT 2 and drains east into Avon Creek. Two data points (E1 and E2) were taken to determine the boundary of Wetland E. Data point E1 was taken within Wetland E and exhibited hydrophytic vegetation, hydric soils, and wetland hydrology; thus, meeting the three criteria to be classified as a wetland. Data point E2 was taken approximately 10 feet southeast of data point E1 and did not meet all three of the criteria to be considered within a wetland (Appendix E, pages E-31 to E-34). This wetland is likely to be considered jurisdictional (i.e. a Waters of the United States) due to its connectivity to Avon Creek, a tributary to the White River, a TNW. The project will impact approximately 0.11 acre of Wetland E within the construction limits.

**Wetland F** is approximately 0.02 acre in size and is a PEM wetland located on the south side of US 36 just east of the US 36/Ronald Reagan Parkway Intersection. It is located within a widened section of a roadside

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ditch. Two data points (F1 and F2) were taken to determine the boundary of Wetland F. Data point F1 was taken within Wetland F and exhibited hydrophytic vegetation, hydric soils, and wetland hydrology; thus, meeting the three criteria to be classified as a wetland. Data point F2 was taken approximately 20 feet north of data point F1 and did not meet all three of the criteria to be considered within a wetland (Appendix E, pages E-35 to E-38). This wetland is likely to be considered jurisdictional (i.e. a Waters of the United States) due to its connectivity to Avon Creek, a tributary to the White River, a TNW. Construction limits are approximately 60 feet outside the boundary of Wetland F; therefore, the project will not impact Wetland F.

Please note that coordination has been ongoing with the project designer to ensure wetland impacts are avoided and minimized as much as possible. Avoidance alternatives would not be practicable because the project would not meet the purpose and need. Wetland boundaries will be identified as "Wetland—Do Not Disturb Outside Construction Limits" on the final design plans. Wetland boundaries will be identified in the field prior to construction to avoid indirect impacts. INDOT recommends orange fencing and signs to separate construction activities from all wetlands.

The project will impact approximately 0.13 acre of wetlands. Mitigation will likely be needed due to these wetland impacts. Mitigation will be determined during permitting. If possible, the IDNR In-Lieu Fee Program will be utilized for wetland mitigation. A USACE Section 404 Permit and IDEM Section 401 Water Quality Certification will be required due to these wetland impacts.

Early coordination letters were sent to the USACE, IDNR Division of Fish and Wildlife, and IDEM on January 28, 2019 (Appendix B, pages B-1 to B-4). The USACE did not respond to the early coordination letter. The automatic generated response letter from IDEM was received that same day (Appendix B, pages B-5 to B-13). The IDEM automatic response letter did not include any recommendations pertaining to wetlands.

The IDNR Division of Fish and Wildlife responded to early coordination efforts on February 28, 2019 with recommendations to avoid or minimize impacts to streams and terrestrial habitat only (Appendix B, pages B-15 and B-16).

Per the U.S. Fish and Wildlife Service Interim Policy for the Review of Highway Transportation Projects in Indiana dated May 29, 2013, the project meets the criteria for programmatic coordination. The Interim Policy does not provide any recommendations pertaining to wetlands.

**Terrestrial Habitat**

Unique or High Quality Habitat

**Presence**

<b>X</b>

**Impacts**

<b>Yes</b>	<b>No</b>
<b>X</b>	

*Use the remarks box to identify each type of habitat and the acres impacted (i.e. forested, grassland, farmland, lawn, etc).*

Remarks:

Per a desktop review, a field visit conducted on May 25, 2018 and April 3, 2019 by RQAW (Appendix A, pages A-6 to A-15), an aerial photograph of the project area (Appendix A, pages A-3 to A-5), and USGS topographic map (Appendix A, page A-2), adjacent land use consists of commercial properties in a heavily developed area. The commercial properties are associated with maintained lawns. Dominant vegetation included tall fescue (*Schedonorus arundinaceus*), Canada thistle (*Cirsium arvense*), white clover (*Trifolium repens*), and great ragweed (*Ambrosia trifida*). Although no animals were observed, it is assumed that certain common animals are likely present within the project area (e.g. squirrels, raccoons, birds, etc.).

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The project will require approximately 0.20 acre of permanent right-of-way from commercial land consisting of maintained lawn and approximately 0.12 acre of wetlands. Trees greater than three inches in diameter-at-breast-height (dbh) are within the project area; however, trees will not be cleared. Mitigation is not anticipated because trees will not be impacted.

Early coordination letters were sent to the USACE, IDNR Division of Fish and Wildlife, and IDEM on January 28, 2019 (Appendix B, pages B-1 to B-4). The USACE did not respond to the early coordination letter. The automatic generated response letter from IDEM was received that same day (Appendix C, pages B-5 to B-13). The IDEM automatic response letter included recommendations pertaining to terrestrial habitat. These measures generally include revegetating disturbed areas.

The IDNR Division of Fish and Wildlife responded to early coordination efforts on February 28, 2019 with recommendations to avoid or minimize impacts to streams and terrestrial habitat (Appendix B, pages B-15 and B-16). Recommendations regarding terrestrial habitat include revegetating disturbed areas and minimizing tree and brush clearing.

Per the U.S. Fish and Wildlife Service Interim Policy for the Review of Highway Transportation Projects in Indiana dated May 29, 2013, the project meets the criteria for programmatic coordination. Recommendations from the Interim Policy include restricting clearing outside the construction zone boundaries. All applicable agency recommendations are included in the Environmental Commitments section of this CE document.

*If there are high incidences of animal movements observed in the project area, or if bridges and other areas appear to be the sole corridor for animal movement, consideration of utilizing wildlife crossings should be taken.*

**Karst**

Is the proposed project located within or adjacent to the potential Karst Area of Indiana?  
 Are karst features located within or adjacent to the footprint of the proposed project?

Yes	No
<input type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>

If yes, will the project impact any of these karst features?

*Use the remarks box to identify any karst features within the project area. (Karst investigation must comply with the Karst MOU, dated October 13, 1993)*

Remarks: Per a desktop review, a field visit conducted on May 25, 2018 and April 3, 2019 by RQAW (Appendix A, pages A-6 to A-15), USGS topographic map (Appendix A, page A-2), and the water resources map in the RFI report (Appendix D, page D-8), the proposed project is located outside the designated karst region of Indiana, as outlined in the October 13, 1993 Memorandum of Understanding (MOU). There are no karst features identified within the project area.

Early coordination was conducted electronically with the Indiana Geological Survey (IGS) on January 28, 2019. In their early coordination response, the IGS did not indicate that karst features may exist in the project area (Appendix B, pages B-17 to B-19). Therefore, impacts to karst features are not expected. The IGS stated the 0.5 mile search radius is located within an area with moderate liquefaction potential, floodway, moderate bedrock resource potential, and low sand and gravel resource potential. This information has been conveyed to the project designer and will be noted as the design plans are further developed.

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Threatened or Endangered Species	<u>Presence</u>	<u>Impacts</u>	
		<u>Yes</u>	<u>No</u>
Within the known range of any federal species	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Any critical habitat identified within project area	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Federal species found in project area (based upon informal consultation)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
State species found in project area (based upon consultation with IDNR)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Is Section 7 formal consultation required for this action?  Yes  No

Remarks: Per a desktop review and the RFI report completed by RQAW on February 15, 2019 (Appendix D, pages D-1 to D-15), the IDNR Endangered, Threatened and Rare (ETR) Species List has been checked (Appendix D, pages D-12 to D-15). The highlighted species on the list reflect the federal and state identified ETR species located within Hendricks and Marion counties. Per the IDNR Division of Fish and Wildlife early coordination response letter dated February 28, 2019, the Natural Heritage Program's database has been checked, and to date, no plant or animal species listed as state or federally threatened, endangered, or rare have been reported to occur in the project vicinity (Appendix B, pages B-15 and B-16).

Hendricks and Marion counties are within the range of the federally endangered Indiana bat (*Myotis sodalis*) and the federally threatened northern long-eared bat (*Myotis septentrionalis*). On March 15, 2019, RQAW submitted project information through the USFWS Information for Planning and Consultation (IPaC) website (<https://ecos.fws.gov/ipac/>). An official species list was generated; no additional species were found within the project area (Appendix B, pages B-31 to B-36).

The project qualifies for the *Range-wide Programmatic Informal Consultation for the Indiana bat and northern long-eared bat*, dated May 2016 (revised February 2018), between FHWA, Federal Railroad Administration, Federal Transit Administration, and the USFWS. On March 15, 2019, RQAW submitted project information through the USFWS IPaC website. Per the USFWS IPaC website, a Concurrence Verification Letter, dated March 15, 2019, determined the project *May Affect, Not Likely to Adversely Affect* the Indiana bat and northern long-eared bat (Appendix B, pages B-37 to B-47). The INDOT Crawfordsville District reviewed and verified the effect finding on March 15, 2019 and requested USFWS review of the finding on March 15, 2019 (Appendix B, page B-29). No response was received from the USFWS within the 14-day review period; therefore, it was concluded the USFWS concurs with the finding. See Avoidance and Minimization Measures (AMMs) in the *Environmental Commitments* section of this document.

On March 15, 2019, RQAW reviewed the USFWS Range Map for the rusty patched bumble bee (*Bombus affinis*) (<https://www.fws.gov/midwest/endangered/insects/rpbb/rpbbmap.html>). The project area is outside a High Potential Zone for rusty patched bumble bee habitat. The RFI report was approved on February 15, 2019 and INDOT confirmed the project area is outside a High Potential Zone for the rusty patched bumble bee (Appendix D, page D-5).

This precludes the need for further consultation on this project under Section 7 of the Endangered Species Act of 1973, as amended. If new information on endangered species at the site becomes available, or if project plans are changed, the USFWS will be contacted for consultation.

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### SECTION B – OTHER RESOURCES

#### Drinking Water Resources

Wellhead Protection Area  
Public Water System(s)  
Residential Well(s)  
Source Water Protection Area(s)  
Sole Source Aquifer (SSA)

	<u>Presence</u>	<u>Impacts</u>	
		Yes	No
Wellhead Protection Area	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Public Water System(s)	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Residential Well(s)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Source Water Protection Area(s)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Sole Source Aquifer (SSA)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

If a SSA is present, answer the following:

	<u>Yes</u>	<u>No</u>
Is the Project in the St. Joseph Aquifer System?	<input type="checkbox"/>	<input type="checkbox"/>
Is the FHWA/EPA SSA MOU Applicable?	<input type="checkbox"/>	<input type="checkbox"/>
Initial Groundwater Assessment Required?	<input type="checkbox"/>	<input type="checkbox"/>
Detailed Groundwater Assessment Required?	<input type="checkbox"/>	<input type="checkbox"/>

#### Remarks:

The project is located within Hendricks and Marion counties, which are not located within the St. Joseph Sole Source Aquifer, the only legally designated sole source aquifer in Indiana. Therefore, the FHWA/EPA Sole Source Aquifer Memorandum of Understanding (MOU) is not applicable to this project. Impacts are not expected.

In an early coordination letter dated February 1, 2019, the IDEM Groundwater Section stated the project is not located within a Wellhead Protection Area (Appendix B, page B-14). Impacts are not expected.

Per a review of the IDNR Water Well Record Database website (<https://www.in.gov/dnr/water/3595.htm>) on April 24, 2019 by RQAW, three unconsolidated water wells are located adjacent to the project area. Impacts are not expected because water wells are not identified within or adjacent to the project area per the design plan sheets. Apparent water wells were not observed during the field visit. Should it be determined during the right-of-way phase that these wells are affected, a cost to cure will likely be included in the appraisal to restore the wells.

Per a desktop review of the INDOT Separate Storm Sewer Systems (MS4) website (<https://entapps.indot.in.gov/MS4/>) on April 24, 2019 by RQAW, and the urbanized area boundary map in the RFI report (Appendix D, page D-9), the project area is within an Urbanized Area Boundary. Early coordination was conducted with the Town of Avon MS4 Coordinator and the Hendricks County MS4 Coordinator on January 28, 2019. The Town of Avon and Hendricks County MS4 coordinators did not respond to the early coordination letter. Impacts are not expected.

Per coordination with the designer, there are underground public water utility lines present throughout the entire project area; these utility lines will be temporarily impacted to install the new storm sewer lines and drain inlets. Utility coordination is ongoing.

#### Flood Plains

Longitudinal Encroachment  
Transverse Encroachment  
Project located within a regulated floodplain

	<u>Presence</u>	<u>Impacts</u>	
		Yes	No
Longitudinal Encroachment	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Transverse Encroachment	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Project located within a regulated floodplain	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

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Homes located in floodplain within 1000' up/downstream from project

*Discuss impacts according to classification system described in the "Procedural Manual for Preparing Environmental Studies".*

Remarks: Per review of the IDNR Indiana Floodway Information Portal website (<https://dnrmaps.dnr.in.gov/appsphp/fdms/>) on April 24, 2019 by RQAW, and the water resources map in the RFI report (Appendix D, page D-8), the project area is located within a regulatory floodplain as determined by approved IDNR floodplain maps. The project qualifies as a Category 3 per the current INDOT *Categorical Exclusion Manual*.

Category 3 – The modifications to drainage structures included in this project will result in an insubstantial change in their capacity to carry flood water. This change could cause a minimal increase in flood heights and flood limits. These minimal increases will not result in any substantial adverse impacts on the natural and beneficial floodplain values; they will not result in substantial change in flood risks or damage; and they do not have substantial potential for interruption or termination of emergency service or emergency routes; therefore, it has been determined that this encroachment is not substantial.

An early coordination letter was sent to the Local Floodplain Administrator on January 28, 2019 (Appendix B, pages B-1 to B-4). The Local Floodplain Administrator did not respond to the early coordination letter.

Farmland	<u>Presence</u>	<u>Impacts</u>	
		Yes	No
Agricultural Lands	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Prime Farmland (per NRCS)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Total Points (from Section VII of CPA-106/AD-1006\* N/A  
*\*If 160 or greater, see CE Manual for guidance.*

*See CE Manual for guidance to determine which NRCS form is appropriate for your project.*

Remarks: Per a field visit conducted on May 25, 2018 and April 3, 2019 by RQAW (Appendix A, pages A-6 to A-15), an aerial photograph of the project area (Appendix A, pages A-3 to A-5), and early coordination with the Natural Resources Conservation Service (NRCS) (Appendix B, page B-20), the project will not cause a conversion of prime farmland. As such, there is no land that meets the definition of farmland under the Farmland Protection Policy Act (FPPA) within or adjacent to the project area and the requirements of the FPPA do not apply. Impacts are not expected.

### SECTION C – CULTURAL RESOURCES

Minor Projects PA Clearance	<u>Category</u>	<u>Type</u>	<u>INDOT Approval Dates</u>	<u>N/A</u>
	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Eligible and/or Listed  
Resource Present

**Results of Research**

Archaeology		<input type="checkbox"/>
NRHP Buildings/Site(s)		<input checked="" type="checkbox"/>
NRHP District(s)		<input type="checkbox"/>
NRHP Bridge(s)		<input type="checkbox"/>

**Project Effect**

No Historic Properties Affected  No Adverse Effect  Adverse Effect

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### Documentation Prepared

Documentation (mark all that apply)

	<b>ES/FHWA Approval Date(s)</b>	<b>SHPO Approval Date(s)</b>
Historic Properties Short Report		
Historic Property Report	X	X
Archaeological Records Check/ Review	X	X
Archaeological Phase Ia Survey Report	X	X
Archaeological Phase Ic Survey Report		
Archaeological Phase II Investigation Report		
Archaeological Phase III Data Recovery		
APE, Eligibility and Effect Determination	X	X
800.11 Documentation	X	X

Memorandum of Agreement (MOA)  **MOA Signature Dates** (List all signatories)

*Describe all efforts to document cultural resources, including a detailed summary of the Section 106 process, using the categories outlined in the remarks box. The completion of the Section 106 process requires that a Legal Notice be published in local newspapers. Please indicate the publication date, name of paper(s) and the comment period deadline. Likewise include any further Section 106 work which must be completed at a later date, such as mitigation or deep trenching.*

Remarks: Because this is a federal aid highway project, a Section 106 evaluation is required as mandated by the National Historic Preservation Act of 1966, as amended (54 USC § 306108) and as governed by the process established by 36 CFR Part 800.

**Area of Potential Effect (APE):** The APE is the area in which the proposed project may cause alterations in the character or use of historic resources. The APE for this project is an irregular polygon that generally encompasses the areas adjacent to the proposed work where ground-disturbing activity may occur, areas within a viewshed of the proposed work, and areas where other non-visual direct or indirect effects such as audible effects may occur. Most of the APE extends approximately 700 feet wide and it expands to approximately 900 feet wide where open viewsheds occur (Appendix C, pages C-17 to C-20).

**Coordination with Consulting Parties:** Early coordination was initiated with potential consulting parties by RQAW on October 1, 2018 with an e-mail inviting organizations and individuals to be consulting parties (Appendix C, pages C-29 and C-30). Early coordination was initiated with tribal contacts by the INDOT Cultural Resources Office (CRO) on October 1, 2018 (Appendix C, pages C-31 and C-32). A weblink to the Historic Property Report (HPR) was also provided in the early coordination. The following is a list of organizations and individuals that were sent e-mails. Those who indicated they wished to be consulting parties are in bold. [Note: The Indiana State Historic Preservation Officer (SHPO) is an automatic consulting party. The FHWA is the federal agency undertaking the project with INDOT acting on behalf of the FHWA]. See the example of the early coordination letter, dated September 26, 2018, in Appendix C, pages C-26 to C-28. See the list of consulting parties in Appendix C, pages C-23 and C-24 and consulting party correspondence in Appendix C, pages C-26 to C-71.

In a letter dated November 1, 2018, the Indiana SHPO did not have any additional recommendations for consulting parties (Appendix C, pages C-34 and C-35).

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Section 106 Consulting Parties	Date of Response
<b>1. Indiana Landmarks Central Regional Office</b>	<b>October 30, 2018 (C-33)</b> <b>January 11, 2019 (C-48)</b> <b>March 14, 2019 (C-68)</b> <b>March 15, 2019 (C-71)</b>
2. Hendricks County Historical Society/Hendricks County Museum	No response received
3. Hendricks County Heritage Alliance	No response received
4. Hendricks County Historian	No response received
5. Marion County Historian	No response received
6. Wayne Township Historical Society	No response received
7. Indianapolis Metropolitan Planning Organization	No response received
8. Avon Town Manager	No response received
9. Hendricks County Commissioner's Office	No response received
10. Hendricks County Engineer	No response received
11. Hendricks County Highway Office Manager	No response received
12. Hendricks County Plan Commission Chair	No response received
13. Indianapolis Mayor	No response received
14. City of Indianapolis Department of Public Works Director	No response received
15. Marion County Commissioner's Office	No response received
<b>16. Delaware Nation of Oklahoma</b>	<b>November 6, 2018 (C-36)</b>
17. Eastern Shawnee Tribe of Oklahoma	No response received
18. Forest County Potawatomi Community	No response received
<b>19. Miami Tribe of Oklahoma</b>	<b>November 7, 2018 (C-39)</b>
20. Peoria Tribe of Indians of Oklahoma	No response received
21. Pokagon Band of Potawatomi Indians	No response received
22. *Rockville Marketplace LLC	No response received
<b>23. **Andrew Vinson</b>	<b>January 8, 2019 (C-51 and C-52)</b>

\* Historic property owner for David Faucett House, 9055 Rockville Road.  
 \*\* Historic property owner for Charles McLain House, 10664 East US 36.

**Archaeology:** A *Phase Ia Archaeological Reconnaissance Survey* was completed by a qualified professional from Cultural Resource Analysis on October 9, 2018 (Martin, 2018). The reconnaissance identified no archaeological resources within the project area and it was recommended the project be allowed to proceed. The report describing these findings was sent to the INDOT CRO on November 2, 2018 and was approved by INDOT CRO on November 15, 2018. The archaeological report was sent to the Indiana SHPO on December 11, 2018; in a letter dated January 11, 2019, the Indiana SHPO concurred with the findings of the archaeological report and stated no additional archaeological assessment is necessary. See Appendix C, pages C-49 and C-50, for the Indiana SHPO concurrence letter and Appendix C, pages C-75 and C-76 for the archaeological report summary.

In an e-mail dated November 6, 2018, the Delaware Nation concurred with the project. The Delaware Nation also asked to be kept up to date as the project progresses (Appendix C, page C-36). In a letter dated November 7, 2018, the Miami Tribe of Oklahoma offered no objection to the project. The Miami Tribe of Oklahoma also noted that if any human remains or Native American cultural items falling under the Native American Graves

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Protection and Repatriation Act (NAGPRA) or archaeological evidence is discovered during any phase of this project, the Miami Tribe requests immediate consultation with the entity of jurisdiction for the location of discovery (Appendix C, page C-39).

**Historic Properties:** The APE was investigated for the existence of any historic properties and/or structures by a qualified professional from RQAW on August 24, 2018 (Appendix C, pages C-17 to C-20). Per the field visit and associated documentary research, the historian recommended two properties as eligible for listing on the NRHP:

- Charles McLain House (IHSSI Number 063-117-45024)
- David Faucett House (IHSSI Number 097-117-56005)

The HPR (Boot, 2018) describing these findings was sent to INDOT CRO on November 2, 2018 and was approved by INDOT CRO on December 10, 2018. The HPR was sent to consulting parties, including the Indiana SHPO, on December 11, 2018 (Appendix C, pages C-40 to C-47). In a letter dated January 11, 2019, the Indiana SHPO concurred with the recommendations that both properties are eligible for the NRHP. In the same letter, the Indiana SHPO requested additional information regarding the assessment of effects to historic properties. A Potential Effects Letter (see discussion in following paragraph) was sent to the Indiana SHPO on February 11, 2019 to fulfill the request for additional information. Refer to Appendix C, pages C-49 and C-50, for the Indiana SHPO concurrence letter and Appendix C, pages C-73 and C-74 for the HPR summary. In a letter dated January 11, 2019, the Indiana Landmarks also concurred with the recommendations of the HPR (Appendix C, page C-48).

Per request by the Indiana SHPO, a Potential Effects letter, dated February 11, 2019, was sent to consulting parties. The letter explained that any remnants from the Terre Haute, Indianapolis, and Eastern Traction Company line were demolished during the replacement and widening of US 36 during the 1970s. Additionally, the letter described the undertaking's potential effects to both historic resources and that a *No Adverse Effect* finding is anticipated. The letter requested any comments within 15 calendar days (Appendix C, pages C-53 to C-62). In a letter dated February 19, 2019, the Indiana SHPO concurred with the recommendations and the anticipated *No Adverse Effect* finding as described in the Potential Effects letter (Appendix C, pages C-65 to C-67). In a letter dated March 15, 2019, the Indiana Landmarks stated they did not anticipate the project will result in adverse effects to either the Charles McLain House or David Faucett House. The Indiana Landmarks also requested that the 15-day review request "be added to the public record" (Appendix C, page C-71). In an e-mail dated March 15, 2019, INDOT CRO stated the comment period was extended to 30 days. The letter emphasized that the 15-day comment period in the Potential Effects letter did not constitute a Section 106 finding of effect, therefore, a 30-day comment period is not required per 36 CFR Part 800 (Appendix C, page C-68).

**Documentation, Findings:** The 800.11(e) documentation for the *No Adverse Effect* was sent to INDOT CRO on February 28, 2019 and was signed by INDOT CRO, on behalf of FHWA, on March 18, 2019 (Appendix C, pages C-1 to C-4). The 800.11(e) documentation was sent to consulting parties, including the Indiana SHPO, on March 18, 2019. The Indiana SHPO concurred with the *No Adverse Effect* Section 106 finding on April 12, 2019 (Appendix C, pages C-81 and C-82).

**Public Involvement:** To meet the public involvement requirements of Section 106, FHWA's finding of *No Adverse Effect*, a notice was advertised in the *Indianapolis Star* on March 21, 2019. The public comment period closed 30 days later on April 20, 2019. No public comments were received. The text of the public

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---------------------------------------	--------------------------------------	--

notice and the affidavit of publication appear in Appendix C, pages C-78 to C-80. The Section 106 process has been completed and the responsibilities of the FHWA under Section 106 have been fulfilled.

### SECTION D – SECTION 4(f) RESOURCES/ SECTION 6(f) RESOURCES

**Section 4(f) Involvement** (mark all that apply)

**Parks & Other Recreational Land**

- Publicly owned park
- Publicly owned recreation area
- Other (school, state/national forest, bikeway, etc.)

Presence

X

Use

Yes	No
	X

Evaluations

Prepared

- Programmatic Section 4(f)\*
- “De minimis” Impact\*
- Individual Section 4(f)


FHWA  
Approval date

--

**Wildlife & Waterfowl Refuges**

- National Wildlife Refuge
- National Natural Landmark
- State Wildlife Area
- State Nature Preserve

Presence


Use

Yes	No

Evaluations

Prepared

- Programmatic Section 4(f)\*
- “De minimis” Impact\*
- Individual Section 4(f)


FHWA  
Approval date

--

**Historic Properties**

- Sites eligible and/or listed on the NRHP

Presence

X
---

Use

Yes	No
	X

Evaluations

Prepared

- Programmatic Section 4(f)\*
- “De minimis” Impact\*
- Individual Section 4(f)


FHWA  
Approval date

--

\*FHWA approval of the environmental document also serves as approval of any Section 4f Programmatic and/or De minimis evaluation(s) discussed below.

Discuss Programmatic Section 4(f) and “de minimis” Section 4(f) impacts in the remarks box below. Individual Section 4(f) documentation must be separate Draft and Final documents. For further discussions on Programmatic, “de minimis” and Individual Section 4(f) evaluations please refer to the “Procedural Manual for the Preparation of Environmental Studies”. Discuss proposed alternatives that satisfy the requirements of Section 4(f).

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Remarks: Section 4(f) of the U.S. Department of Transportation Act of 1966 prohibits the use of certain public and historic lands for federally transportation facilities unless there is no feasible and prudent alternative. The law applies to significant publicly owned parks, recreational areas, wildlife/waterfowl refuges, and NRHP eligible or listed historic properties regardless of ownership. Lands subject to this law are considered Section 4(f) resources.

Per a desktop review, a field visit conducted on May 25, 2018 and April 3, 2019 by RQAW (Appendix A, pages A-6 to A-15), an aerial photograph of the project area (Appendix A, pages A-3 to A-5), the infrastructure map in the RFI report (Appendix D, page D-7), and the APE maps (Appendix C, pages C-17 to C-20), there are five Section 4(f) resources located within 0.5 mile of the project area:

- Ronald Reagan Parkway Trail (three recreational trail segments), two recreational trail segments are open on the north side of US 36 at the US 36/Ronald Reagan Parkway Intersection, and one recreational trail segment is currently under development on the south side of US 36 at the US 36/Ronald Reagan Parkway Intersection (Des. Number 1601121).
- Charles McLain House (IHSSI Number 063-117-45024), historic resource recommended eligible for the NRHP, adjacent to the project area.
- David Faucett House (IHSSI Number 097-117-56005), historic resource recommended eligible for the NRHP, adjacent to the project area.

Note that Steeplechase Apartments, and associated recreational facilities, is mapped approximately 0.15 mile north of project area and the Avon Recreation Center is mapped approximately 0.27 mile southwest of project area. However, both properties are privately-owned and are not Section 4(f) properties. No temporary or permanent right-of-way will be needed from either property. No use is expected.

**Ronald Reagan Parkway Trail:** These publicly owned trail segments are at the US 36/Ronald Reagan Parkway Intersection. Two segments are currently open on the north side of US 36, one along the west side of Ronald Reagan Parkway and one along the east side of Ronald Reagan Parkway. One recreational trail segment is currently under development on the south side of US 36, on the west side of Ronald Reagan Parkway (Des. Number 1601121). The trail segment currently under construction will terminate south of this added travel lanes project; the full trail (i.e. across US 36) will not be open prior to or during construction of this added travel lanes project. Permanent or temporary right-of-way will not be needed in the northeast, northwest, or southwest quadrant for construction of the project.

An at-grade pedestrian crossing, and median pedestrian refuge will be installed to connect the existing trail located north of US 36, along the west side of Ronald Reagan Parkway, to the proposed trail located south of US 36, along the west side of Ronald Reagan Parkway (Des. Number 1601121). Des. Number 1601121 will be constructed by the Town of Avon prior to this added travel lanes project (Des. Number 1601072). Des. Number 1601121 will terminate just south of the added travel lanes project; the added travel lanes project will fill in this gap by constructing the remainder of the trail to provide connectivity of the trail along both sides of US 36 (note: during construction of this added travel lanes project, a "Trail Ends Here" sign will be posted where the southern trail segment terminates). The existing curb ramps in the northeast and northwest quadrants of the US 36/Ronald Reagan Parkway Intersection will be replaced to meet ADA requirements. The portion of roadway between the curb ramps will be striped to indicate a trail crossing. Four curb ramps will be constructed (two in the median pedestrian refuge, one north of the median pedestrian refuge, and one south of the median pedestrian refuge). The portion of roadway between these four curb ramps will be striped to indicate a trail crossing. All curb ramps will meet ADA requirements. There would not be any

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temporary occupancy or use of the existing trails located north of US 36 or the proposed trail located south of US 36 during construction. The existing or proposed trails would not need to be moved or require a detour during construction because both existing trails north of US 36 currently terminate at US 36. The proposed trail south of US 36 will terminate several feet south of US 36 and this gap will be filled in as part of the added travel lanes project; as such, that proposed trail will also terminate south of US 36. The project will not use this resource by taking permanent or temporary right-of-way and will not alter the environment in such a way as to constitute use of this resource. Impacts are not expected.

In an e-mail dated January 17, 2019, INDOT Environmental Services Division concurred there would not be a Section 4(f) use of the trails. INDOT Environmental Services Division also recommended coordination with the \*parks department to make sure they are aware of the project. Lastly, INDOT Environmental Services Division stated if the scope of work changes, the project will need to be reassessed to determine if Section 4(f) temporary occupancy may need to be evaluated (Appendix H, page H-2).

During the early coordination phase of the project, the Hendricks County Planning and Building Commission (\*managing entity of the Ronald Reagan Parkway Trail) was consulted on January 28, 2019 (Appendix B, pages B-1 to B-4). The Hendricks County Planning and Building Commission did not respond to early coordination letter.

**Charles McLain House (IHSSI Number 063-117-45024):** The house is located on the north side of US 36 and east of Ronald Reagan Parkway (Appendix C, page C-19). The house is recommended eligible for the NRHP under Criterion C because it is a good example of a circa 1904 Free Classic Queen Ann style house with original architectural features in Washington Township, Hendricks County.

Impacts adjacent to the historic resource boundary will include roadway widening and reconstruction, driveway reconstruction, and limited sidewalk reconstruction to tie the existing sidewalk into the new driveway. No permanent right-of-way will be needed from the property; however, approximately 0.02 acre of temporary right-of-way will be needed from the property for driveway and adjacent sidewalk reconstruction. The existing roadway, driveway, parking lot, and sidewalk are modern concrete construction. The historic resource boundary goes to the back (north edge) of the existing sidewalk. The anticipated construction limits will not extend into the historic resource boundary.

Construction in this area will widen the US 36 pavement by approximately 17 feet to add a travel lane, reconstruct and extend the existing right turn lane, and install a curb and gutter. The added travel lane near the property is anticipated to provide only a 10% increase in capacity from 2021 to 2041. The property's setting has been drastically altered by development over recent decades from the expansion of US 36 and the property's own conversion to commercial use (i.e. parking lot and sidewalk construction).

The minimal introduction of visual and audible elements to the property's setting will not constitute a significant alteration to historic features that contribute to the Charles McLain House historic significance because of the already transformed, suburban landscape. Although permanent work is proposed near the Charles McLain House property, it is anticipated that there will be no adverse effect to the house or carriage house because no contributing historic features will be impacted directly. Therefore, the alterations adjacent to the historic resource boundary and its setting are not anticipated to reduce the significance or impact any of the characteristics that qualify the Charles McLain House for the NRHP in a manner that would diminish the integrity of the property's location, design, setting, materials, workmanship, feeling, or association. As a result, the project is anticipated to have *No Adverse Effect* to the Charles McLain House.

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The project will temporarily occupy land from the Charles McLain House, a Section 4(f) historic property. The temporary occupancy will not constitute a Section 4(f) use because all of the conditions listed in 23 CFR 774.13(d) are satisfied:

1. Duration must be temporary, i.e., less than the time needed for construction of the project, and there should be no change in ownership of the land;
2. Scope of the work must be minor, i.e., both the nature and the magnitude of the changes to the Section 4(f) property are minimal;
3. There are no anticipated permanent adverse physical impacts, nor will there be interference with the protected activities, features, or attributes of the property, on either a temporary or permanent basis;
4. The land being used must be fully restored, i.e., the property must be returned to a condition which is at least as good as that which existed prior to the project; and
5. There must be documented agreement of the official(s) with jurisdiction over the Section 4(f) resource regarding the above conditions.

INDOT CRO determined the appropriate Section 106 finding is *No Adverse Effect*; the temporary occupancy will not constitute a Section 4(f) use for the Charles McLain House because all of the conditions listed in 23 CFR 774.13(d) are satisfied (Appendix C, pages C-1 to C-77). The Indiana SHPO concurred with the *No Adverse Effect* Section 106 finding in a letter dated April 12, 2019 (Appendix C, pages C-81 and C-82).

**David Faucett House (IHSSI Number 097-117-56005):** The house is located on the south side of US 36 and east of Ronald Reagan Parkway (Appendix C, page C-20). The house is recommended eligible for the NRHP under Criterion C because it is a good example of a circa 1846 I-house with Greek Revival architectural features in Wayne Township, Marion County.

Impacts adjacent to the historic resource boundary will be strictly visual and limited to the existing right-of-way. Permanent or temporary right-of-way will not be needed from the property. In terms of the property's setting, the east end of new construction will occur approximately 99 feet northwest of the historic property boundary; while the east end of the incidental construction, consisting of pavement milling to transition the new construction to the existing pavement, will occur approximately 35 feet northwest of the historic property boundary. No work will occur within the historic boundary or immediately adjacent to the frontage.

The property has been drastically altered by development over recent decades from the expansion of US 36. The project's minimal introduction of visual and audible elements to the property's setting will not constitute a significant alteration from this already transformed, suburban landscape. Thus, the alterations near the historic property boundary and neighboring visible changes, will have no direct impact on the property other than a viewshed change approximately 99 feet away for pavement replacement and the addition of a curb and gutter. This minor viewshed alteration is not anticipated to reduce the significance or impact any of the characteristics that qualify the property for the NRHP in a manner that would diminish the integrity of the property's location, design, setting, materials, workmanship, feeling, or association. As a result, the project is anticipated to have *No Adverse Effect* to the David Faucett House.

INDOT CRO determined the appropriate Section 106 finding is *No Adverse Effect*; therefore, no Section 4(f) evaluation is required for the David Faucett House (Appendix C, pages C-1 to C-77). The Indiana SHPO

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concurred with the *No Adverse Effect* Section 106 finding in a letter dated April 12, 2019 (Appendix C, pages C-81 and C-82).

Impacts to Section 4(f) resources are not anticipated as a result of this project. All contributing items to these historic properties will be marked as "do not disturb" on the final design plans.

**Section 6(f) Involvement** Presence Use

*Discuss proposed alternatives that satisfy the requirements of Section 6(f). Discuss any Section 6(f) involvement.*

Remarks: The U.S. Land and Water Conservation Fund Act of 1965 established the Land and Water Conservation Fund (LWCF), which was created to preserve, develop and assure accessibility to outdoor recreation resources. Section 6(f) of this Act prohibits conversion of lands purchased with LWCF monies to a non-recreation use.

Per the LWCF website (<https://www.lwcfcoalition.com/tools>) accessed on March 14, 2019 by RQAW, there are 24 LWCF properties within Hendricks and Marion counties (Appendix H, page H-1). None of these properties are located within or adjacent to the project area. In addition, an early coordination letter was sent to the National Park Service on January 28, 2019 (Appendix B, pages B-1 to B-4). The National Park Service did not respond to the early coordination letter. Impacts are not expected.

### SECTION E – Air Quality

**Air Quality**

**Conformity Status of the Project**

	Yes	No
Is the project in an air quality non-attainment or maintenance area?	<input checked="" type="checkbox"/>	<input type="checkbox"/>
If YES, then:		
Is the project in the most current MPO TIP?	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Is the project exempt from conformity?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
If the project is NOT exempt from conformity, then:		
Is the project in the Transportation Plan (TP)?	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Is a hot spot analysis required (CO/PM)?	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Level of MSAT Analysis required?

Level 1a  Level 1b  Level 2  Level 3  Level 4  Level 5

Remarks: The project is included in the FY 2018 to 2021 and FY 2020 to 2023 Indianapolis Metropolitan Planning Organization Transportation Improvement Program (IMPO TIP) and FY 2018 to 2021 and FY 2020 to 2024 INDOT Statewide Transportation Improvement Program (INDOT STIP) (Appendix G, pages G-1 to G-5). Note the STIP amount (\$9,414,000) and TIP amount (\$16,052,366) do not agree. An amendment to update the STIP/TIP will be needed to account for this discrepancy.

The project is in Hendricks and Marion counties which are currently in a maintenance area for 8-hour ozone per the IDEM Office of Air Quality website ([https://www.in.gov/idem/airquality/files/nonattainment\\_county\\_list.pdf](https://www.in.gov/idem/airquality/files/nonattainment_county_list.pdf)), accessed on April 25, 2019 by RQAW. It is in a maintenance area under the 1997 Ozone and 8-hour standard that was revoked in 2015 and is being evaluated for conformity due to the February 16, 2018, South Coast Air Quality Management District V. Environmental Protection Agency (EPA), et. al. Decision. The project

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scope and concept are accurately in the INDOT STIP and IMPO TIP and both conform to the State Implementation Plan (SIP). Therefore, the conformity requirements of 40 CFR 93 have been met.

The purpose of the project is to reduce vehicular delays along US 36, with an emphasis on the heavily congested portion of US 36 located within the project area (i.e. between Ronald Reagan Parkway and Raceway Road) and reduce the crash rate within the project area by constructing additional travel lanes. The project has been determined to generate minimal air quality impacts for Clean Air Act criteria pollutants and has not been linked with any special mobile source air toxic (MSAT) concerns. As such, the project will not result in changes in traffic volumes, vehicle mix, basic project location, or any other factors that would cause a meaningful increase in MSAT impacts from that of the Do Nothing Alternative.

Moreover, EPA regulations for vehicle engines and fuels will cause overall MSAT emissions to decline significantly over the next several decades. Based on regulations now in effect, an analysis of national trends with EPA's MOVES2014 model forecasts a combined reduction of over 90% in the total annual emissions rate.

### SECTION F - NOISE

**Noise** **Yes**  **No**   
 Is a noise analysis required in accordance with FHWA regulations and INDOT's traffic noise policy?

	No	Yes/ Date
<b>ES Review of Noise Analysis</b>	<input type="checkbox"/>	April 23, 2019 (Appendix H, page H-6)

Remarks: Per the Noise Analysis completed by Metric Environmental, dated April 22, 2019, 70 noise receptor locations were identified within the project area (37 residential receptors [Category B], 3 medical facility receptors [Category D], 27 restaurant/bar receptors [Category E], and 3 receptors associated with the Ronald Reagan Parkway Trail [Category C]). Noise levels were modeled for areas with activity Categories B, C, D, and E. Noise abatement measures for the impacted receptors were investigated based on feasibility and reasonableness.

Per the Noise Analysis, the predicted noise levels did not have a substantial increase of at least 15 A-weighted decibels (dBA) at any of the receptors. However, five receptors were found to be impacted in the Future Build condition based on reaching or exceeding their Noise Abatement Criteria (NAC) sound criteria levels. Impacted receptors include one residential receptor (receptor 11), three receptors associated with Ronald Reagan Parkway Trail (receptors 67, 68, and 69), and one medical facility receptor (receptor 13). The existing noise levels for (Appendix H, pages H-3 to H-5).

Receptor Number	Existing Noise Levels (dBA)	Predicted Noise Levels (dBA)
11	65.6	67.1
13	65.2	66.7
67	68.7	69.3
68	70.3	70.9
69	70.2	70.8

Per the Noise Analysis, INDOT has not identified any locations where noise abatement is likely (Appendix H, page H-6). Noise abatement at these five locations is based on preliminary design costs and design

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criteria. Noise abatement has been not been found to be feasible and reasonable. This is due to engineering feasibility restrictions (e.g. length of restrictions from drives and located off US 36, roadside ditches along the north side of US 36, and safety and line of sight considerations that would prevent the installation of noise barrier walls that can effectively provide noise abatement to the impacted receptors. Additionally, noise barriers along Ronald Reagan Parkway to abate noise impacts to the Ronald Reagan Parkway Trail receptors are not feasible because the project is a state-funded project along a state highway and Ronald Reagan Parkway is a local roadway outside the project limits or managed by a project sponsor. Therefore, abatement measures for impacted receptors within the project area are not feasible.

A reevaluation of the noise analysis will occur during final design. If during final design it has been determined that conditions have changed such that noise abatement is feasible and reasonable, the abatement measures might be provided. The final decision on the installation of any abatement measure(s) will be made upon the completion of the project's final design and the public involvement processes.

### SECTION G – COMMUNITY IMPACTS

#### Regional, Community & Neighborhood Factors

- Will the proposed action comply with the local/regional development patterns for the area?
- Will the proposed action result in substantial impacts to community cohesion?
- Will the proposed action result in substantial impacts to local tax base or property values?
- Will construction activities impact community events (festivals, fairs, etc.)?
- Does the community have an approved transition plan?
- If No, are steps being made to advance the community's transition plan?
- Does the project comply with the transition plan? (explain in the remarks box)

Yes	No
<input checked="" type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input checked="" type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>
<input checked="" type="checkbox"/>	<input type="checkbox"/>

Remarks:

The project will comply with the local/regional development patterns for the area. The project is not anticipated to result in substantial impacts to community cohesion, because it will not change access to properties within the area or divide existing communities. The proposed project is not expected to impact the surrounding community or cause economic impacts to the surrounding area. Therefore, the project will have minimal or no negative impacts to the community or local economy.

There are several businesses within the project area. Per the Fairs and Festivals website ([www.fairsandfestivals.net](http://www.fairsandfestivals.net)), accessed on April 15, 2019 by RQAW, 15 fairs/festivals are currently scheduled within 10 miles of zip code 46123. These or any future fairs/festivals that may be planned are unlikely to be impacted by the project since fair or festival goers can utilize US 36 during construction.

Per the Hendricks County, Indiana government website ([https://www.co.hendricks.in.us/egov/documents/1495197073\\_66107.pdf](https://www.co.hendricks.in.us/egov/documents/1495197073_66107.pdf)), Hendricks County has an Implementation and Transition Plan, dated December 2011. Per coordination with the Department of Public Works-City of Indianapolis Office of Disability Affairs on April 30, 2019, Marion County has an approved Americans with Disabilities Act (ADA) Transition Plan. The Indianapolis Department of Public Works-City of Indianapolis Office of Disability then coordinated with The Fehribach Group to determine the approval date of the plan. It was determined that the ADA voluntary compliance and implementation of all the ADA requirements within Marion County fall under the City of Indianapolis Office of Disability Affairs. The most recent plan was implemented in September 2016. The project will comply with the ADA Transition Plans by providing ADA compliant curb ramps and a median pedestrian refuge. The existing curb ramps in the northeast and northwest quadrants of the US 36/Ronald

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Reagan Parkway Intersection will be replaced to meet ADA requirements. An at-grade pedestrian crossing and median pedestrian refuge will be installed at the US 36 and Ronald Reagan Parkway Intersection to connect the existing trail located north of US 36, along the west side of Ronald Reagan Parkway, to the proposed trail located south of US 36, along the west side of Ronald Reagan Parkway (Des. Number 1601121). Four curb ramps will be constructed (two in the median pedestrian refuge, one north of the median pedestrian refuge, and one south of the median pedestrian refuge). There is no existing curb ramp in the southeast quadrant of the US 36/Ronald Reagan Parkway Intersection. This project will not include installation of a new curb ramp at this location as there are no pedestrian facilities in this quadrant. The project is also in compliance with INDOT's Draft ADA Transition Plan, dated June 1, 2018 (<https://www.in.gov/indot/files/2018%20Transition%20Plan%20Draft.pdf>).

**Indirect and Cumulative Impacts**

Will the proposed action result in substantial indirect or cumulative impacts?

Yes <input type="checkbox"/>	No <input checked="" type="checkbox"/>
---------------------------------	---

Remarks:

Indirect impacts are effects caused by the action (project) and later in time, or farther removed in distance, but are still reasonably foreseeable. Indirect effects may include growth inducing effects and other effects related to induced changes in the pattern of land use, population density, or growth rate. Cumulative impacts affect the environment which result from the incremental impact of the action when added to other past, present, and reasonably foreseeable future actions regardless of what agency (Federal or non-Federal) or person undertakes such actions.

Due to the scope of the project and limited impacts, it is not expected to result in any substantial indirect or cumulative impacts. The improvement of the roadway will allow more efficient travel for vehicles along US 36. However, the improved efficiency for vehicles is not expected to increase development in the area beyond what is already planned. The project will add capacity to the existing roadway network; however, it will not provide additional access to any currently undeveloped area. Post construction, traffic is not expected to back up west of the project area. Per coordination with the project designer, an entire traffic analysis and simulation was developed to determine the optimum location to eliminate the third westbound lane.

**Public Facilities & Services**

Will the proposed action result in substantial impacts on health and educational facilities, public and private utilities, emergency services, religious institutions, airports, public transportation or pedestrian and bicycle facilities? *Discuss how the maintenance of traffic will affect public facilities and services.*

Yes <input type="checkbox"/>	No <input checked="" type="checkbox"/>
---------------------------------	---

Remarks:

Per a desktop review, a field visit conducted on May 25, 2018 and April 3, 2019 by RQAW (Appendix A, pages A-6 to A-15), an aerial photograph of the project area (Appendix A, pages A-3 to A-5), and the infrastructure map in the RFI report (Appendix D, page D-7), there are seven public facilities (one church, two recreational facilities, three trail segments, and one airport) located within 0.5 mile of the project area. There does not appear to be any schools (educational facilities), hospitals or police stations (emergency services), or public transportation stations located within the 0.5 mile search radius. The Washington Township Avon Fire Department Station Number 2 is located approximately 0.46 mile north of the US 36/Ronald Reagan Parkway Intersection. Impacts are not expected. The Rainbow Acres Church of God is mapped adjacent to the project area; however, per the field visit, the church is not located within or adjacent to the project area. Per coordination with the designer, there are underground public water utility lines present throughout the entire project area; these utility lines will be temporarily impacted to install the new storm sewer lines and drain inlets. Utility coordination is ongoing.

Three trail segments associated with the Ronald Reagan Parkway Trail (two segments are open on the north side of US 36 at the US 36/Ronald Reagan Parkway Intersection, and one segment is currently under

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development on the south side of US 36 at the US 36/Ronald Reagan Parkway Intersection) are adjacent to the project area. Refer to the *Section 4(f) Involvement* section of this document for further details on the trail segments. The project will allow reduced vehicular delays along US 36, with an emphasis on the heavily congested portion of US 36 located within the project area (i.e. between Ronald Reagan Parkway and Raceway Road) and reduce the crash rate within the project area.

Per the RFI report, one public airport, Speedway Public Airport, is located within 3.8 miles (20,000 feet) of the project area (Appendix D, page D-2). An early coordination was sent to the INDOT Office of Aviation on January 28, 2019 (Appendix B, page B-1 to B-4). Per the INDOT Office of Aviation early coordination response letter, dated February 4, 2019, an Indiana Tall Structure Permit would not be required unless the project involves the construction of a temporary (e.g., crane) or permanent structure that penetrates a 100:1 slope from the nearest point of the Indianapolis International Airport runways (Appendix B, page B-22). An Indiana Tall Structure Permit will not be required.

Per the Hendricks County Engineer early coordination response e-mail, dated February 13, 2019, Hendricks County does not foresee any adverse effects to county facilities due to the project. The e-mail also indicated that all drainage structures not located under US 36 and that are 36 inches or greater in diameter or span length are under County jurisdiction. Any proposed modifications to such structures would need to be reviewed during the design process by the Hendricks County Engineer (Appendix B, page B-23).

Per the Marion County Surveyor's Office early coordination response e-mail, dated February 4, 2019, Marion County has one monument located within the right-of-way limits at the county line (i.e. at Raceway Road). If the monument is disturbed, it will need to be replaced under the supervision of the Marion County Surveyor's Office per IC 8-23-9-24. The Marion County Surveyor's Office indicated it can provide a cast iron Harrison monument to replace the current Harrison monument if it is desired. The Marion County Surveyor's Office would like to be present if/when the corner is excavated (Appendix B, pages B-24 to B-26).

A meeting was held with RQAW, the Town of Avon, and the Assistant Fire Chief of the Town of Avon on February 27, 2019. It was decided that specifying mountable curbs at strategic locations (i.e. not directly across from right in, right out access points) to allow emergency response vehicles to cross over the median to the other side of the road is the solution. These locations will need to be delineated to allow drivers of the emergency response vehicles to easily identify the locations. RQAW is currently identifying these locations (Appendix F, page F-42).

Early coordination letters were also sent to the U.S. Department of Housing and Urban Development, Indianapolis Metropolitan Planning Organization, Hendricks County Planning and Building Commission, Rainbow Acres Church of God, Hendricks County Council Members, Marion County Council members, Hendricks County Commissioner members, Marion County Commissioner members, Hendricks Highway Supervisor, Hendricks County Surveyor, Marion County Department of Public Works, Town of Avon Council members, and Town of Avon Public Works on January 28, 2019 (Appendix B, pages B-1 to B-4). These organizations did not respond to the early coordination letter.

It is the responsibility of the project sponsor to notify school corporations and emergency services at least two weeks prior to any construction that would block or limits access.

**Environmental Justice (EJ)** (Presidential EO 12898)

During the development of the project were EJ issues identified?  
Does the project require an EJ analysis?

Yes	No
	X
X	

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If YES, then:

Are any EJ populations located within the project area?



Will the project result in adversely high or disproportionate impacts to EJ populations?



Remarks:

Under FHWA Order 6640.23A, FHWA and INDOT, as a recipient of funding from FHWA, are responsible to ensure that their programs, policies, and activities do not have a disproportionately high and adverse effect on minority or low-income populations. Per the current INDOT *Environmental Justice NEPA Documentation Process*, an Environmental Justice (EJ) Analysis is required for all EA and Environmental Impact Statement (EIS) level documents.

Potential EJ impacts are detected by locating minority populations and low-income populations relative to a reference population to determine if populations of EJ concern exists and whether there could be disproportionately high and adverse impacts to them. The reference population may be a county, city, or town and is called the community of comparison (COC). In this project, the COC is Washington Township (Hendricks County) and Wayne Township (Marion County). The community that overlaps the project limits is called the affected community (AC). In this project, the ACs are Census Tract 2106.04, Census Tract 2106.06 (Washington Township), and Census Tract 3401.01 (Wayne Township).

An AC has a population of concern for EJ if the population is more than 50% low-income or minority or if the low-income or minority population is 125% of the COC. American Community Survey 5-year estimates data (2013 through 2017) was obtained from the U.S. Census Bureau website (<https://factfinder.census.gov/faces/nav/jsf/pages/searchresults.xhtml?refresh=t>) on June 4, 2019 by RQAW. The data collected for low-income and minority populations within the ACs are summarized in the table below.

Table: Low-income and Minority Data (American Community Survey, 2013 through 2017)				
	COC: Washington Township (Hendricks County) + Wayne Township (Marion County)	AC 1 (Census Tract 2106.04)	AC 2 (Census Tract 2106.06)	AC 3 (Census Tract 3401.01)
Percent Low-income	20.5%	2.9%	8.9%	9.9%
125% of COC	25.7%	AC ≥ 125% of COC	AC ≥ 125% of COC	AC ≥ 125% of COC
EJ Population of Concern		No	No	No
Percent Minority	45.9%	24.8%	30.3%	18.1%
125% of COC	57.4%	AC ≥ 125% of COC	AC ≥ 125% of COC	AC ≥ 125% of COC
EJ Population of Concern		No	No	No

AC 1 has a percent low-income of 2.9% which is below 50% and is below the 125% COC threshold (25.7%). AC 2 has a percent low-income of 8.9% which is below 50% and is below the 125% COC threshold (25.7%). AC 3 has a percent low-income of 9.9% which is below 50% and is below the 125% COC threshold (25.7%). Therefore, there are no low-income populations of EJ concern.

AC 1 has a percent minority of 24.8% which is below 50% and is below the 125% COC threshold (57.4%). AC 2 has a percent minority of 30.3% which is below 50% and is below the 125% COC threshold (57.4%). AC 3

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has a percent minority of 18.1% which is below 50% and is below the 125% COC threshold (57.4%). Therefore, there are no minority populations of EJ concern. The census data sheets, map, and calculations can be found in Appendix H, pages H-8 to H-13. No further EJ analysis is warranted.

### Relocation of People, Businesses or Farms

Will the proposed action result in the relocation of people, businesses or farms?  
 Is a Business Information Survey (BIS) required?  
 Is a Conceptual Stage Relocation Study (CSRS) required?  
 Has utility relocation coordination been initiated for this project?

Yes	No
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input checked="" type="checkbox"/>	<input type="checkbox"/>

Number of relocations: Residences: 0 Businesses: 0 Farms: 0 Other: 0

If a BIS or CSRS is required, discuss the results in the remarks box.

Remarks: No relocations of people, businesses, or farms will take place because of this project. Utility coordination has been initiated for the project. Several utilities are present within the project area (electric, water, telephone, gas, cable, etc.), some of which will be impacted for relocation. Per coordination with the designer, there are underground public water utility lines present throughout the entire project area; these utility lines will be temporarily impacted to install the new storm sewer lines and drain inlets. Utility coordination is ongoing.

### SECTION H – HAZARDOUS MATERIALS & REGULATED SUBSTANCES

#### Hazardous Materials & Regulated Substances (Mark all that apply)

Red Flag Investigation

Phase I Environmental Site Assessment (Phase I ESA)

Phase II Environmental Site Assessment (Phase II ESA)

Design/Specifications for Remediation required?

#### Documentation

<input checked="" type="checkbox"/>
<input type="checkbox"/>
<input type="checkbox"/>
<input type="checkbox"/>

**No Yes/ Date**

<b>ES Review of Investigations</b>		February 15, 2019
------------------------------------	--	-------------------

Include a summary of findings for each investigation.

Remarks: Per a review of geographic information system (GIS) and available public records, a RFI report was completed on February 15, 2019 by RQAW (Appendix D, pages D-1 to D-15). Ten hazardous material concerns (one state cleanup site, three leaking underground storage tanks [LUSTs], five National Pollutant Discharge Elimination (NPDES) facilities, and one NPDES pipe location are located within 0.5-mile of the project area. The state cleanup site, two LUSTs, one NPDES facility are mapped within or adjacent to the project area. INDOT Site Assessment and Management approved the Red Flag Investigation on February 15, 2019.

Avon Creek, UNT 2 to Avon Creek, and Shiloh Creek (outside the project area) are impaired with E. coli. Workers who will be in or near water with E. coli should take care to wear appropriate PPE, observe proper hygiene procedures, including regular hand washing, and limit personal exposure.

Per the RFI, one state cleanup site, Coach and Horses Restaurant, 9251 Rockville Road, Agency Interest ID 21024, is located within 0.5 mile of the project area. The state cleanup site is located within the project area. No impacts are expected because a No Further Action for this site was issued on July 20, 2004. Per the IDEM Virtual File Cabinet (VFC), residual petroleum impacts were left in place and extend under Rockville Road. If excavation occurs in this area, proper handling, removal, and disposal of soil and/or groundwater will be necessary.

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Hendricks and Marion	US 36/Rockville Road		

Per the RFI, three leaking underground storage tank (LUST) sites, are located within 0.5 mile of the project area. The nearest LUST site, Speedway/Sm #6125, 10908 East US 36, Facility ID 6682, is mapped within the project area. No impacts are expected because a No Further Action for this site was issued on March 7, 2007. Per the IDEM VFC, residual chemicals of concern (CoCs) remain on-site and do not appear to extend to the project area. No impact is expected.

One LUST site, Autobahn Inc., 8921 East 116<sup>th</sup> Street, Agency Interest ID 21603, is mapped adjacent to the project area. However, per the IDEM VFC, the LUST icon is misplaced; it is in Fishers, Indiana. There is a Meijer Gas Station there now and it does not appear they have a release. No impacts are expected.

One LUST site, Eldron Byrd Property, 8751 Rockville Road, Agency Interest ID 20889, is mapped approximately 0.29 mile east of the project area. No impacts are expected due to the distance from the project area.

Per the RFI, five National Pollutant Discharge Elimination System (NPDES) facilities are located within 0.5 mile of the project area. The nearest NPDES facility, LA-Z-Boy Home Furnishings and Décor, Rockville Road and Raceway Road, Permit Number INR 10K783, is located adjacent to the project area. Information was not found in the IDEM VFC; however, no impacts are expected.

Per the RFI, one NPDES pipe location is located within 0.5 mile of the project area. The NPDES pipe location is located approximately 0.24 mile south of the project area. No impacts are expected due to the distance from the project area.

In addition, no additional hazardous material concerns were observed within or adjacent to the project area during a field visit conducted on May 25, 2018 and April 3, 2019 by RQAW (Appendix D, pages D-16 and D-17). Further investigation for hazardous material concerns is not required at this time.

### SECTION I – PERMITS CHECKLIST

Permits (mark all that apply)

Likely Required

**Army Corps of Engineers (404/Section10 Permit)**

Individual Permit (IP)	<input type="checkbox"/>
Nationwide Permit (NWP)	<input type="checkbox"/>
Regional General Permit (RGP)	<input checked="" type="checkbox"/>
Pre-Construction Notification (PCN)	<input type="checkbox"/>
Other	<input type="checkbox"/>
Wetland Mitigation required	<input type="checkbox"/>
Stream Mitigation required	<input type="checkbox"/>

**IDEM**

Section 401 WQC	<input checked="" type="checkbox"/>
Isolated Wetlands determination	<input type="checkbox"/>
Rule 5	<input checked="" type="checkbox"/>
Other	<input type="checkbox"/>
Wetland Mitigation required	<input checked="" type="checkbox"/>
Stream Mitigation required	<input checked="" type="checkbox"/>

**IDNR**

Construction in a Floodway	<input checked="" type="checkbox"/>
Navigable Waterway Permit	<input type="checkbox"/>

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Lake Preservation Permit Other Mitigation Required <b>US Coast Guard Section 9 Bridge Permit</b> <b>Others (Please discuss in the remarks box below)</b>	<table border="1" style="margin: auto;"> <tr><td style="height: 15px;"> </td></tr> <tr><td style="height: 15px;"> </td></tr> <tr><td style="height: 15px;"> </td></tr> <tr><td style="height: 15px;"> </td></tr> <tr><td style="height: 15px;"> </td></tr> </table>					

Remarks: A USACE Section 404 Permit and IDEM Section 401 Water Quality Certification will be required due to stream and wetland impacts. The total area of land disturbance is approximately 21.50 acres. Because the project will result in one acre or more of land disturbance, an IDEM Rule 5 Notice of Intent will be required. An IDNR Construction in a Floodway will be needed.

The project will impact approximately 897 linear feet (0.07 acre) of streams. The project will impact approximately 0.13 acre of wetlands. Mitigation will be determined during permitting. If possible, the IDNR In-Lieu Fee Program will be utilized for mitigation.

Per the IDNR Division of Fish and Wildlife early coordination response letter, dated February 28, 2019, the project will require formal approval for construction in a floodway under the Flood Control Act, IC 14-28-1, for any proposal to construct, excavate, or fill in or on the floodway of a stream or other flowing waterbody which has a drainage area greater than one square mile (Appendix B, pages B-15 and B-16). Because Avon Creek has an upstream drainage area of approximately 1.03 square miles (Appendix E, page E-14), an IDNR Construction in a Floodway Permit will be required.

Per the INDOT Office of Aviation early coordination response letter, dated February 4, 2019, an Indiana Tall Structure Permit would not be required unless the project involves the construction of a temporary (e.g., crane) or permanent structure that penetrates a 100:1 slope from the nearest point of the Indianapolis International Airport runways (Appendix B, page B-22). An Indiana Tall Structure Permit will not be required.

It is the responsibility of the project sponsor to identify and obtain all required permits.

### SECTION J- ENVIRONMENTAL COMMITMENTS

The following information should be provided below: List all commitments, name of agency/organization requesting the commitment(s) and indicating which are firm and which are for further consideration. The commitments should be numbered.

Remarks: **Firm:**

1. If the scope of work or permanent or temporary right-of-way amounts change, the INDOT Environmental Services Division and the INDOT District Environmental Section will be contacted immediately. (INDOT Environmental Services Division)
2. A state cleanup site, Coach and Horses Restaurant, 9251 Rockville Road, Agency Interest ID 21024, is located within the project area. If excavation occurs in this area, proper handling, removal, and disposal of soil and/or groundwater will be necessary. (INDOT Environmental Services Division)
3. Avon Creek and UNT 2 to Avon Creek are impaired with E. coli. Workers who will be in or near water with E. coli should take care to wear appropriate PPE, observe proper hygiene procedures, including regular hand washing, and limit personal exposure. (INDOT Environmental Services Division)
4. Wetland boundaries will be identified as "Wetland—Do Not Disturb Outside Construction Limits" on the final design plans. Wetland boundaries will be identified in the field prior to construction to avoid indirect impacts. INDOT recommends orange fencing and signs to separate construction activities from all wetlands. (INDOT Environmental Services Division)

## Indiana Department of Transportation

County Hendricks and Marion	Route US 36/Rockville Road	Des. No.	1601072, 1901655, 1901657
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5. It is the responsibility of the project sponsor to notify school corporations and emergency services at least two weeks prior to any construction that would block or limits access. (INDOT Environmental Services Division)
  6. Mountable curbs will be installed at strategic locations to allow emergency response vehicles to cross over the median to the other side of the road. RQAW is currently identifying these locations and these locations will be incorporated into the final design plans. (Town of Avon)
  7. All drainage structures not located under US 36 and are 36 inches or greater in diameter or span length are under County jurisdiction. Any proposed modifications to such structures would need to be reviewed during the design process by the Hendricks County Engineer. (Hendricks County Engineer)
  8. Marion County has one monument located within the right-of-way limits at the US 36/Raceway Road Intersection. If the monument is disturbed, it will need to be replaced under the supervision of the Marion County Surveyor's Office per IC 8-23-9-24. The Marion County Surveyor's Office indicated it can provide a cast iron Harrison monument to replace the current Harrison monument if it is desired. The Marion County Surveyor's Office would like to be present if/when the corner is excavated (Marion County Surveyor's Office).
  9. If any prehistoric or historic archaeological artifacts or human remains are uncovered during construction, demolition, or earthmoving activities, state law (Indiana Code 14-21-1-27 and -29) requires the discovery be reported to the IDNR Division of Historic Preservation and Archaeology within two business days. In this event, please call 317-232-1646. (Indiana SHPO)
  10. General AMM 1: Ensure all operators, employees, and contractors working in areas of known or presumed bat habitat are aware of all FHWA/FRA/FTA (Transportation Agencies) environmental commitments, including all applicable AMMs. (USFWS)
  11. Lighting AMM 1: Direct temporary lighting away from suitable habitat during the active season. (USFWS)
- For Further Consideration:**
1. Appropriate structures and techniques should be utilized both during the construction phase, and after completion of the project, to minimize the impacts associated with storm water runoff. (IDEM)
  2. The use of appropriate planning and site development and appropriate storm water quality measures are recommended to prevent soil from leaving the construction site during active land disturbance and for post construction water quality concerns. (IDEM)
  3. Vegetative wastes should be taken to a registered yard waste composting facility or be chipped or shredded with composting on site (you must register with IDEM if more than 2,000 pounds is to be composted); contact 317-232-0066. (IDEM)
  4. The physical disturbance of the stream and riparian vegetation, especially large trees overhanging any affected water bodies should be limited to only that which is absolutely necessary to complete the project. (IDEM)
  5. Reasonable precautions must be taken to minimize fugitive dust emissions from construction and demolition activities. For example, wetting the area with water, constructing wind barriers, or treating dusty areas with chemical stabilizers (such as calcium chloride or several other commercial products). Dirt tracked onto paved roads from unpaved areas should be minimized. (IDEM)
  6. Ensure that asphalt paving plants are permitted and operate properly. The use of cutback asphalt, or asphalt emulsion containing more than 7% oil distillate, is prohibited during the months April through October. (IDEM)

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7. If the site is found to contain any areas used to dispose of solid or hazardous waste, you need to contact the Office of Land Quality (OLQ) at 317-308-3103. (IDEM)
8. All solid wastes generated by the project, or removed from the project site, need to be taken to a properly permitted solid waste processing or disposal facility. For more information, visit <http://www.in.gov/idem/4998.htm>. (IDEM)
9. If any contaminated soils are discovered during this project, they may be subject to disposal as hazardous waste. Please contact the OLQ at 317-308-3103 to obtain information on proper disposal procedures. (IDEM)
10. Revegetate all bare and disturbed areas that will not be mowed and maintained with a mixture of grasses, sedges, and wildflowers native to central Indiana and specifically for stream bank/floodway stabilization purposes as soon as possible upon completion; turf-type grasses (including low endophyte, friendly endophyte, and endophyte free tall fescue but excluding all other varieties of tall fescue) may be used in regularly mowed areas only. (IDNR Division of Fish and Wildlife)
11. Minimize and contain within the project limits inchannel disturbance and the clearing of trees and brush. (IDNR Division of Fish and Wildlife)
12. Do not work in the waterway from April 1 through June 30 within the prior written approval of the IDNR Division of Fish and Wildlife. (IDNR Division of Fish and Wildlife)
13. Do not construct any temporary runarounds, access bridges, causeways, cofferdams, diversions, or pumparounds. (IDNR Division of Fish and Wildlife)
14. Use minimum average 6-inch graded riprap stone extended below the normal water level to provide habitat for aquatic organisms in the voids. (IDNR Division of Fish and Wildlife)
15. Do not use broken concrete as riprap. (IDNR Division of Fish and Wildlife)
16. Underlay the riprap with a bedding layer of well graded aggregate or a geotextile to prevent piping of soil underneath the riprap. (IDNR Division of Fish and Wildlife)
17. Do not deposit or allow demolition/construction materials or debris to fall or otherwise enter the waterway. (IDNR Division of Fish and Wildlife)
18. Appropriately designed measures for controlling erosion and sediment must be implemented to prevent sediment from entering the stream or leaving the construction site; maintain these measures until construction is complete and all disturbed areas are stabilized. (IDNR Division of Fish and Wildlife)
19. Seed and protect all disturbed streambanks and slopes not protected by other methods that are 3:1 or steeper with erosion control blankets that are heavy-duty, biodegradable, and net free or that use loose-woven/Leno-woven netting to minimize the entrapment and snaring of small-bodied wildlife such as snakes and turtles (follow manufacturer's recommendations for selection and installation); seed and apply mulch on all other disturbed areas. (IDNR Division of Fish and Wildlife)
20. Do not clear trees or understory vegetation outside the construction zone boundaries. (This restriction is not related to the "tree clearing" restriction for potential Indiana bat habitat). (USFWS Standard Recommendation)
21. Restrict below low-water work in streams to placement of culverts, piers, pilings and/or footings, shaping of the spill slopes around the bridge abutments, and placement of riprap. (USFWS Standard Recommendation)
22. Restrict channel work and vegetation clearing to the minimum necessary for installation of the stream crossing structure. (USFWS Standard Recommendation)
23. Minimize the extent of hard armor (riprap) in bank stabilization by using bioengineering techniques whenever possible. If riprap is utilized for bank stabilization, extend it below low-water elevation to provide aquatic habitat. (USFWS Standard Recommendation)
24. Implement temporary erosion and sediment erosion control methods within areas of disturbed soils.

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All disturbed soil areas upon project completion will be vegetated following INDOT's Standard Specification. (USFWS Standard Recommendation)

25. Avoid all work within the inundated part of the stream channel (in perennial streams and larger intermittent streams) during the fish spawning season (April 1 through June 30), except for work within sealed structures such as caissons or cofferdams that were installed prior to the spawning season. No equipment shall be operated below Ordinary High Water Mark during this time unless the machinery is within the caissons or on the cofferdams. (USFWS Standard Recommendation)

26. Evaluate wildlife crossings under bridge/culverts projects in appropriate situations. Suitable crossings include flat areas below bridge abutments with suitable ground cover, high water shelves in culverts, amphibian tunnels and diversion fencing. (USFWS Standard Recommendation)

### SECTION K- EARLY COORDINATION

*Please list the date coordination was sent and all agencies that were contacted as a part of the development of this Environmental Study. Also, include the date of their response or indicate that no response was received. INDOT and FHWA are automatically considered early coordination participants and should only be listed if a response is received.*

Remarks:

Early coordination letters were sent to agencies on January 28, 2019 (Appendix B, pages B-1 to B-4). If a response was not received, it was assumed the agency did not feel the project would result in substantial impacts. See responding agency correspondences in Appendix B, pages B-5 through B-53. The below agencies/individuals were contacted during early coordination. After early coordination was initiated, slight changes in the project design and construction limits occurred that resulted in impacts to an additional stream. A letter describing these changes was sent to the USACE, IDNR Division of Fish and Wildlife, IDEM, Marion County Surveyor's Office, and the Hendricks County Surveyor's Office on June 27, 2019 (Appendix B, pages B-48 to B-50).

Agency	Date of Response(s)
1. Natural Resources Conservation Service (electronic coordination)	February 13, 2019
2. IDNR Division of Fish and Wildlife (electronic coordination)	February 28, 2019
3. IDEM Groundwater Section (electronic coordination)	February 1, 2019
4. INDOT Office of Aviation (electronic coordination)	February 4, 2019
5. INDOT Office of Public Involvement (electronic coordination)	January 29, 2019
6. U.S. Department of Housing and Urban Development (electronic coordination)	No response received
7. Indiana Geological Survey (electronic submission)	January 28, 2019
8. IDEM (electronic submission)	January 28, 2019 June 27, 2019
9. National Park Service, Midwest Regional Office	No response received
10. Indianapolis Metropolitan Planning Organization	No response received
11. U.S. Army Corps of Engineers, Louisville District (electronic coordination)	No response received
12. Hendricks County MS4 Coordinator	No response received
13. Hendricks County Council Members	No response received
14. Hendricks County Board of Commissioners	No response received
15. Hendricks County Engineer	February 13, 2019
16. Hendricks County Highway Supervisor	No response received
17. Hendricks County Surveyor	No response received

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18. Marion County Council Members	No response received
19. Marion County Board of Commissioners	No response received
20. Marion County Department of Public Works	No response received
21. Marion County Surveyor	February 4, 2019
22. Town of Avon Council	No response received
23. Town of Avon Public Works	No response received
24. Hendricks County Planning and Building Commission	No response received
25. Rainbow Acres Church of God	No response received
26. Town of Avon MS4 Coordinator	No response received
27. U.S. Fish and Wildlife Service (IPaC electronic coordination)	March 15, 2019

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US 36/Rockville Road Added Travel Lanes Project

Date: August 5, 2019



# Designation (Des.) Number 1601072

## US 36 Added Travel Lane Project – Avon, Hendricks and Marion Counties, Indiana

### Appendix A: Graphics

General Location Map .....	A-1
Topographic Map .....	A-2
Aerial Photograph Key Maps showing Project Limits.....	A-3
Photographs .....	A-6
Preliminary Design Plan Sheets .....	A-16

### Appendix B: Early Coordination

Sample Early Coordination Letter Sent to Resource Agencies ( <i>some graphics omitted</i> ) .....	B-1
Indiana Department of Environmental Management (IDEM)	
Standard Electronic Response Letter .....	B-5
Groundwater Section Response Letter .....	B-14
Indiana Department of Natural Resources (IDNR) Division of Fish and Wildlife	
Response Letter .....	B-15
Indiana Geological Survey	
Electronic Response .....	B-17
Natural Resources Conservation Service	
Response Letter .....	B-20
Indiana Department of Transportation (INDOT) Office of Public Involvement	
Standard Response E-mail .....	B-21
INDOT Office of Aviation	
Response Letter .....	B-22
Hendricks County Engineer	
Response E-mail .....	B-23
Marion County Surveyors Office	
Response E-mail .....	B-24
U.S. Fish and Wildlife Service (USFWS)	
INDOT Crawfordsville District Coordination E-mail .....	B-27
INDOT Crawfordsville District Coordination E-mail .....	B-28
Information for Planning and Consultation (IPaC) Species List Letter .....	B-31
IPaC Concurrence Verification Letter.....	B-37
Sample Re-coordination Letter Sent to Water Resource Agencies.....	B-48
IDEM Standard Electronic Response Letter .....	B-51

### Appendix C: Cultural Resources

800.11 (e) Documentation ( <i>some graphics omitted</i> ) .....	C-1
Affidavit and Public Notice .....	C-78
State Historic Preservation Officer (SHPO) 800.11 (e) Concurrence Letter .....	C-81

### Appendix D: Red Flag Investigation and Hazardous Materials

Red Flag Investigation ( <i>some graphics omitted</i> ) .....	D-1
Hazardous Materials Site Visit Form .....	D-16

### Appendix E: Water Resources

Waters of the U.S. Determination/Wetland Delineation Report ( <i>some graphics removed</i> ) .....	E-1
Waters of the U.S. Determination/Wetland Delineation Report INDOT Approval E-mail .....	E-43
StreamStats Report (Shiloh Creek).....	E-44

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## US 36 Added Travel Lane Project – Avon, Hendricks and Marion Counties, Indiana

### Appendix F: Public Involvement

Sample Notice of Entry for Survey or Investigation Letter .....	F-1
Public Information Meeting Invitees .....	F-4
Sample Public Information Meeting Invite Letter .....	F-6
Public Information Meeting Sign-in Sheet (October 21, 2018) .....	F-8
Public Information Meeting Handout .....	F-11
Public Information Meeting Presentation .....	F-13
Public Information Meeting Written Comments .....	F-23
Public Information Comments and Responses Table .....	F-28
CAC Members Sign-up Sheet (after public information meeting) .....	F-29
CAC Meeting Invite E-mail .....	F-30
CAC Meeting Sign-in Sheet (December 14, 2019) .....	F-31
CAC Meeting Handout .....	F-32
CAC Meeting Presentation .....	F-33
CAC Meeting Notes .....	F-41
Assistant Fire Chief Meeting Notes (February 27, 2019) .....	F-42
CAC Meeting Invite E-mail .....	F-43
CAC Meeting Sign-in Sheet (May 21, 2019) .....	F-44
CAC Meeting Presentation .....	F-45
CAC Meeting Notes .....	F-50

### Appendix G: Air Quality

Statewide Transportation Improvement Program ( <i>relevant pages only</i> ) .....	G-1
Transportation Improvement Program (TIP) ( <i>relevant page only</i> ) .....	G-4

### Appendix H: Other Information

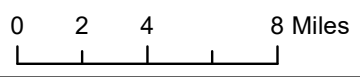
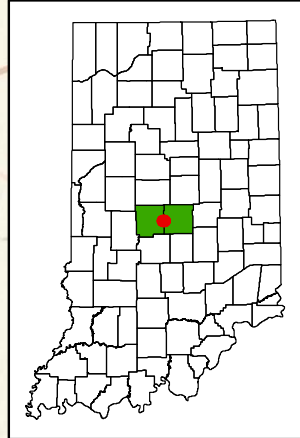
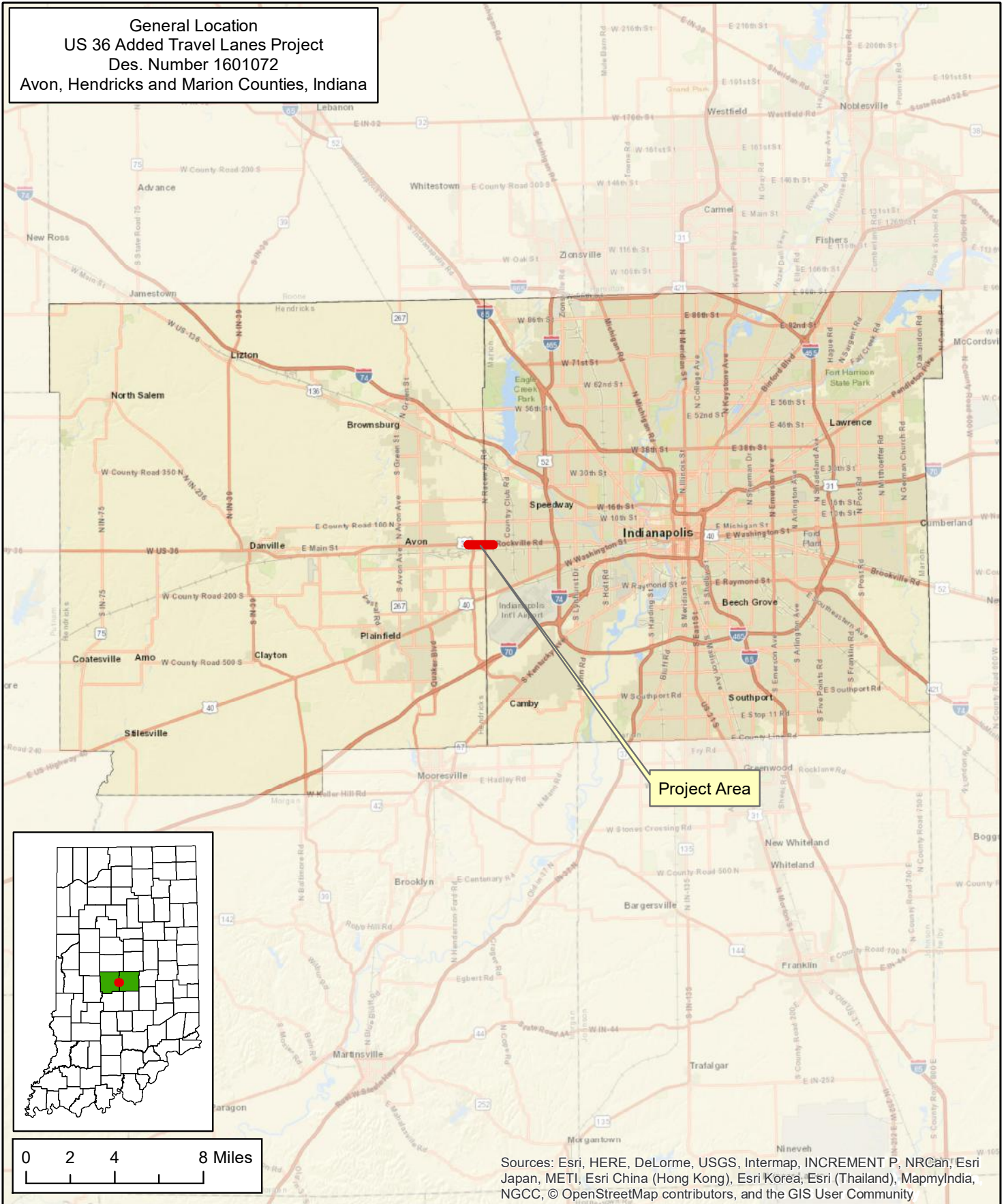
National Park Service Land and Water Conservation Fund List .....	H-1
INDOT Section 4(f) Coordination E-mail .....	H-2
Noise Analysis ( <i>Conclusion only, graphics omitted</i> ) .....	H-3
Noise Analysis INDOT Approval E-mail .....	H-6
Large Culvert Inspection Report ( <i>relevant page only</i> ) .....	H-7
Environmental Justice (EJ) Analysis .....	H-8

**Environmental Assessment**

# **Appendix A**

**Graphics**

General Location  
 US 36 Added Travel Lanes Project  
 Des. Number 1601072  
 Avon, Hendricks and Marion Counties, Indiana



Sources: Esri, HERE, DeLorme, USGS, Intermap, INCREMENT P, NRCan, Esri Japan, METI, Esri China (Hong Kong), Esri Korea, Esri (Thailand), MapmyIndia, NGCC, © OpenStreetMap contributors, and the GIS User Community

**RQAW**  
 ENVIRONMENTAL  
 8770 North Street; Suite 110  
 Fishers, IN 46038

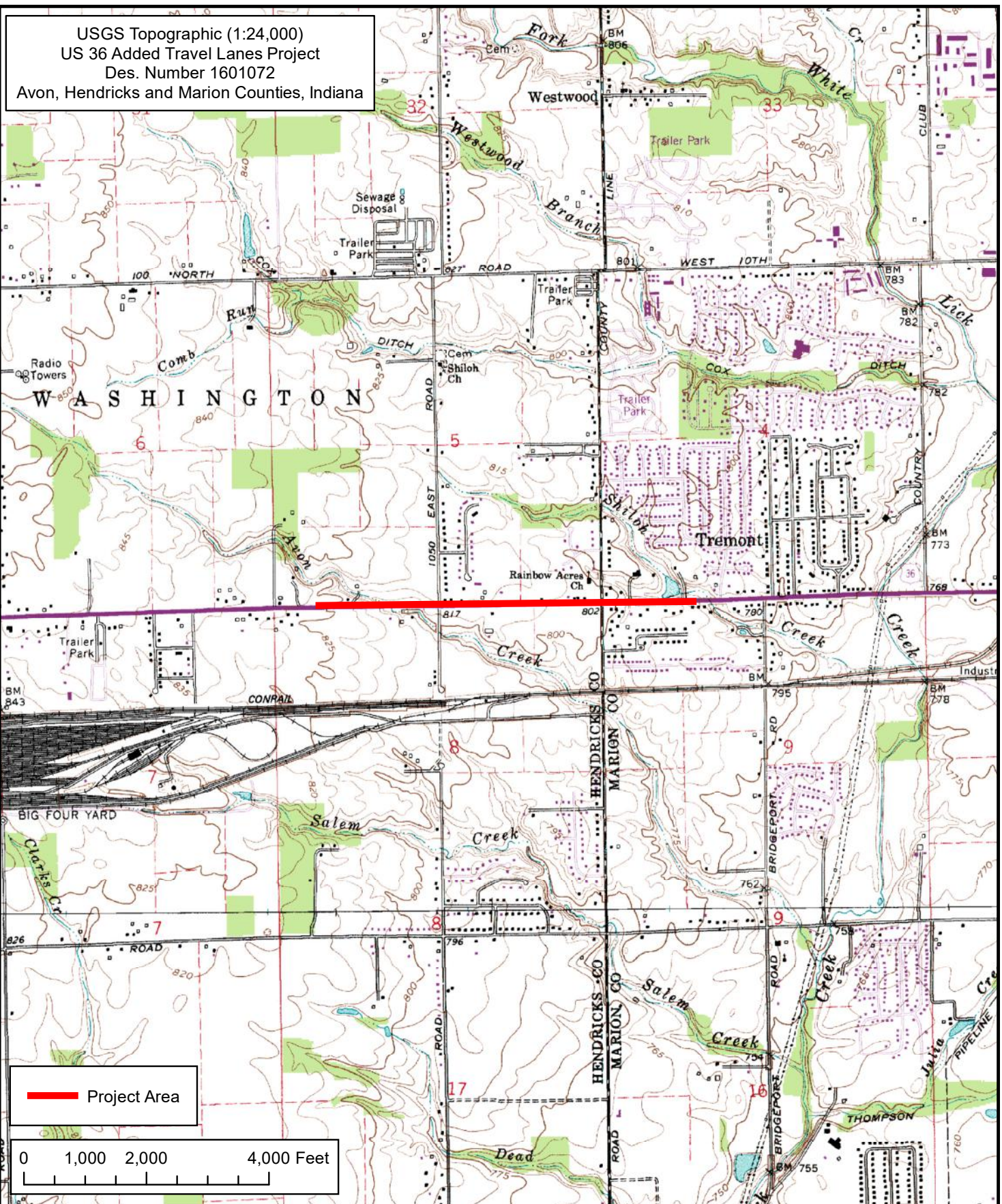
This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.

Map Datum: NAD 83  
 Map Projection: UTM Zone 16 North





USGS Topographic (1:24,000)  
 US 36 Added Travel Lanes Project  
 Des. Number 1601072  
 Avon, Hendricks and Marion Counties, Indiana



8770 North Street; Suite 110  
 Fishers, IN 46038

This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.

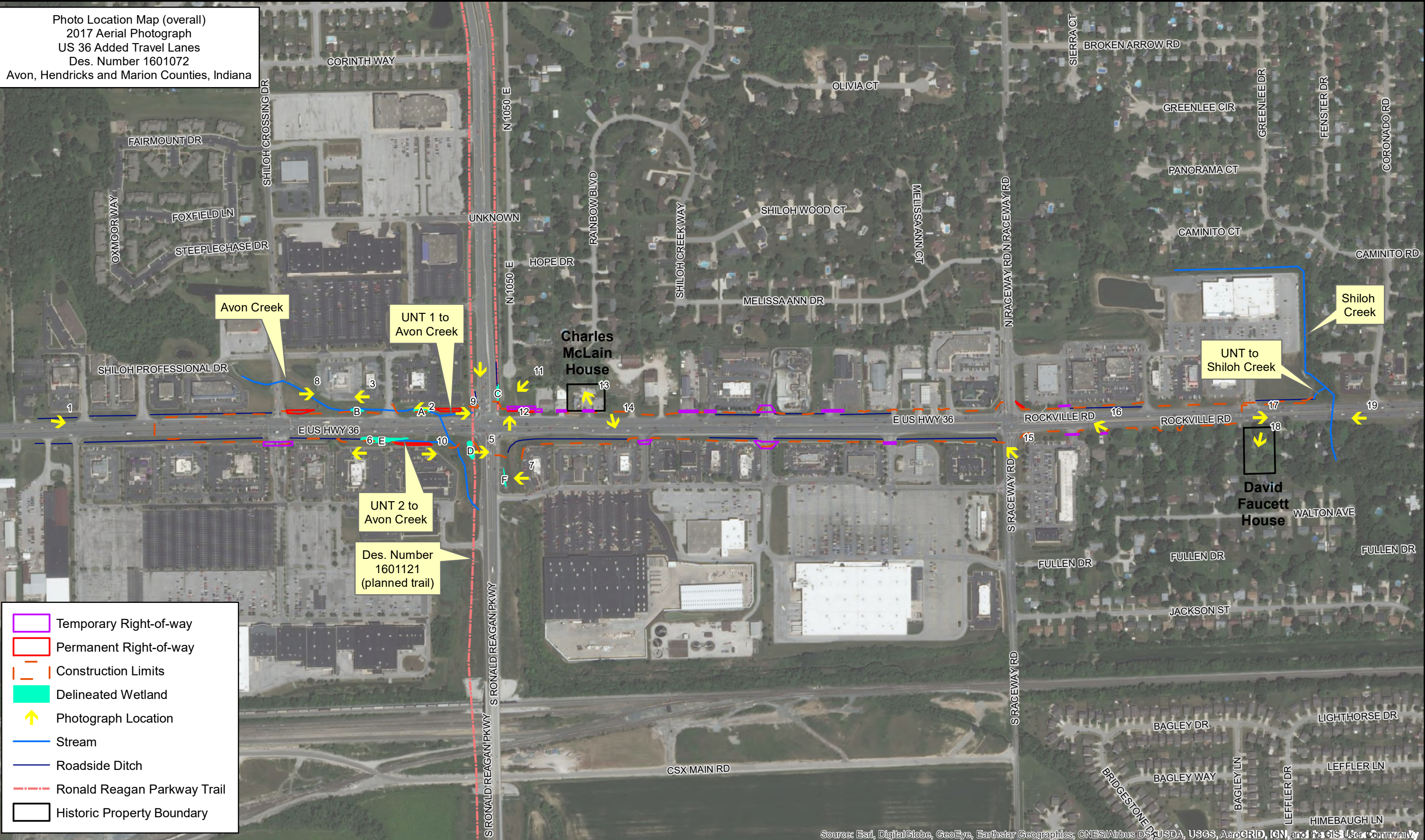
Topographic Quadrangle obtained from USGS (Clermont Quadrangle)

Map Datum: NAD 83  
 Map Projection: UTM Zone 16 North





Photo Location Map (overall)  
 2017 Aerial Photograph  
 US 36 Added Travel Lanes  
 Des. Number 1601072  
 Avon, Hendricks and Marion Counties, Indiana



Source: Esri, DigitalGlobe, GeoEye, Earthstar Geographics, CNES/Airbus DS, USDA, USGS, AeroGRID, IGN, and the GIS User Community

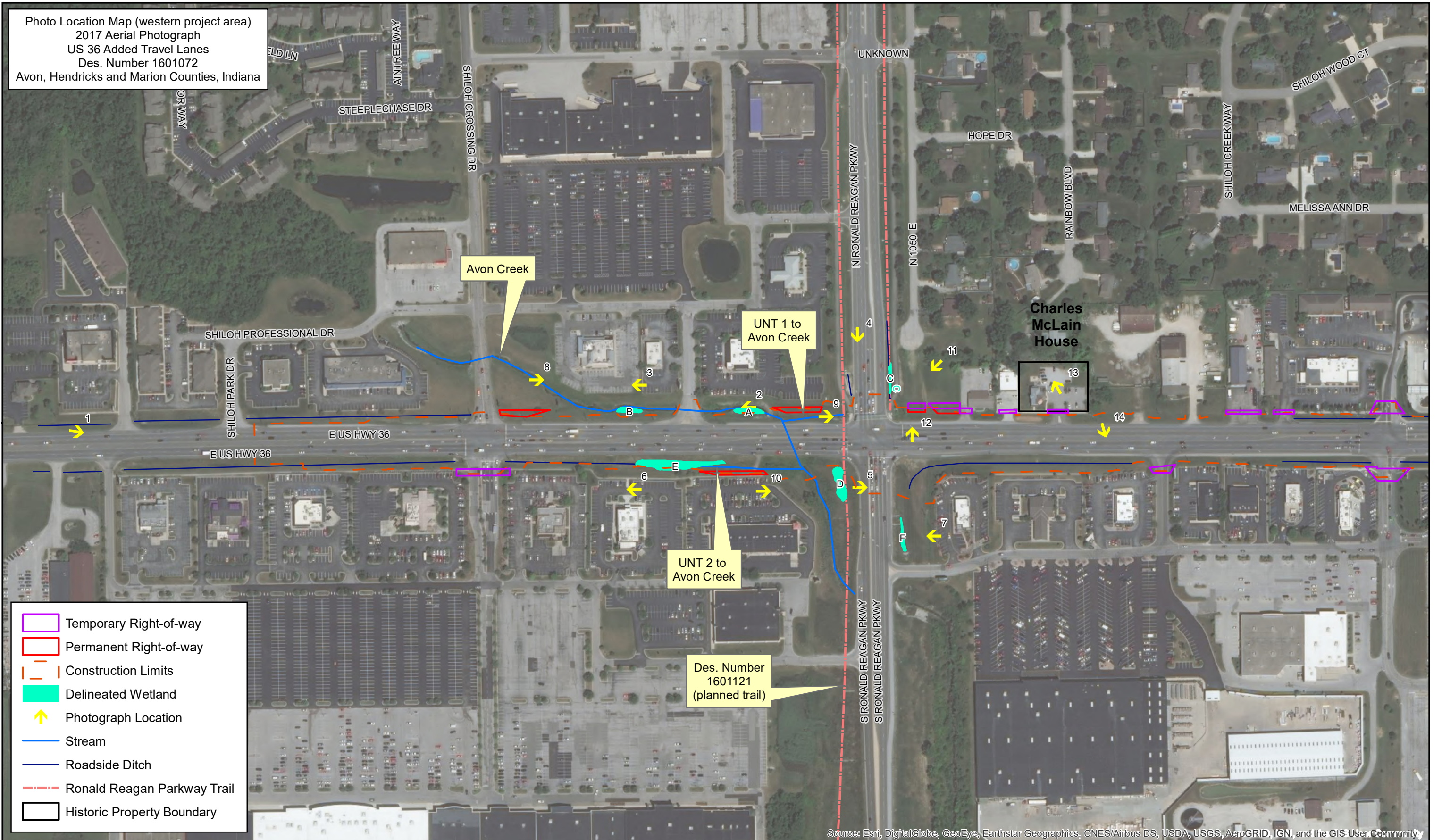


This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.  
 Map Datum: NAD 83  
 Map Projection: UTM Zone 16 North





Photo Location Map (western project area)  
 2017 Aerial Photograph  
 US 36 Added Travel Lanes  
 Des. Number 1601072  
 Avon, Hendricks and Marion Counties, Indiana



Source: Esri, DigitalGlobe, GeoEye, Earthstar Geographics, CNES/Airbus DS, USDA, USGS, AeroGRID, IGN, and the GIS User Community



This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.

Map Datum: NAD 83  
 Map Projection: UTM Zone 16 North

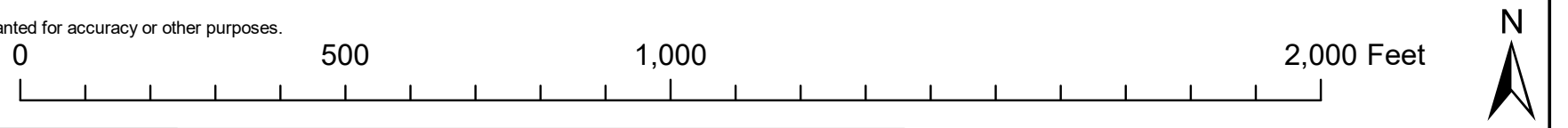
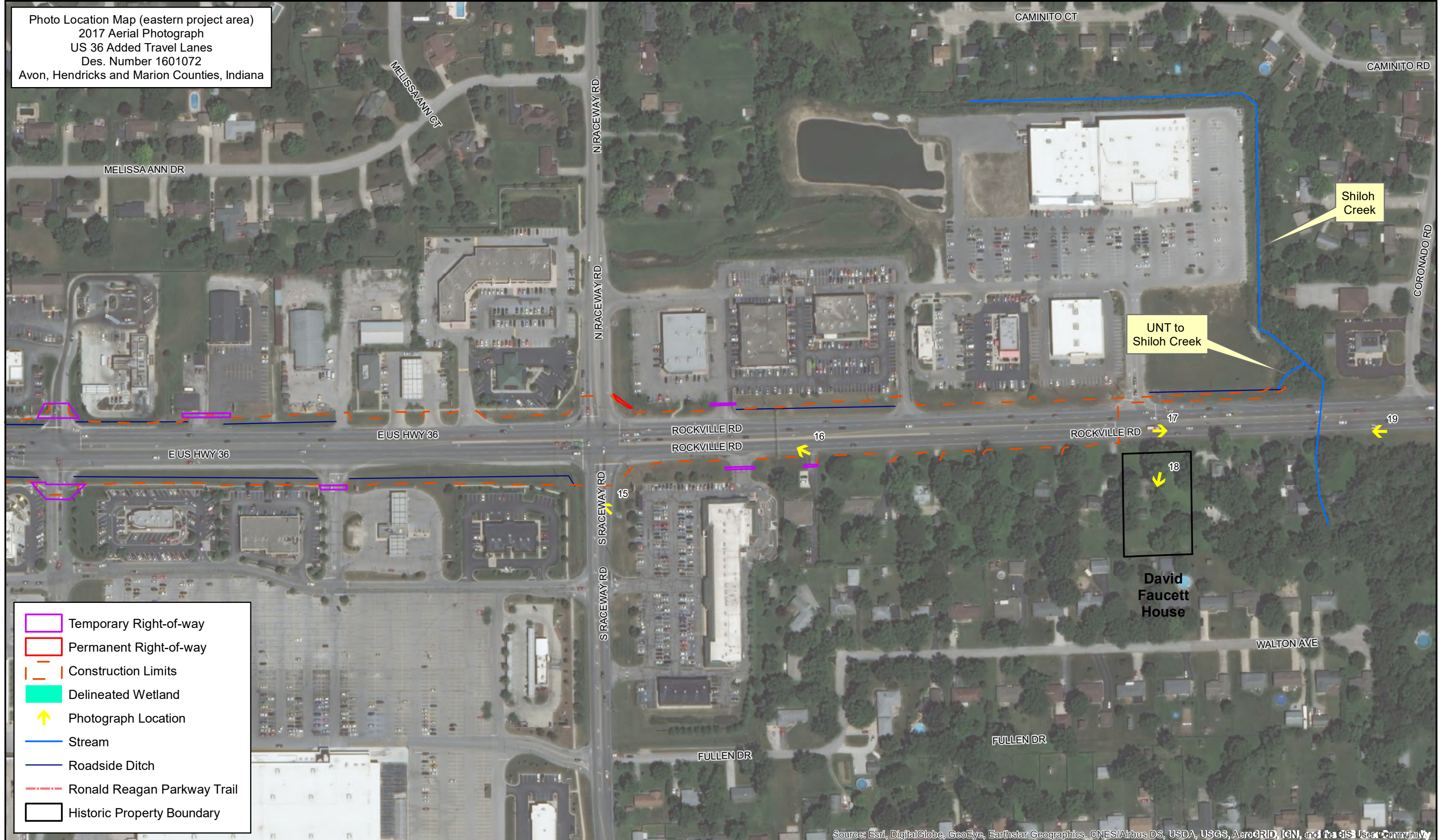




Photo Location Map (eastern project area)  
 2017 Aerial Photograph  
 US 36 Added Travel Lanes  
 Des. Number 1601072  
 Avon, Hendricks and Marion Counties, Indiana



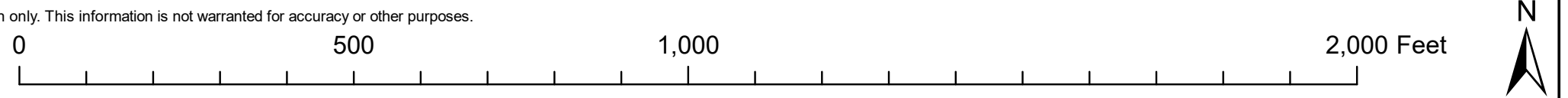
- Temporary Right-of-way
- Permanent Right-of-way
- Construction Limits
- Delineated Wetland
- ↑ Photograph Location
- Stream
- Roadside Ditch
- Ronald Reagan Parkway Trail
- Historic Property Boundary

Source: Esri, DigitalGlobe, GeoEye, Earthstar Geographics, CNES/Airbus DS, USDA, USGS, AeroGRID, IGN, and the GIS User Community



This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.

Map Datum: NAD 83  
 Map Projection: UTM Zone 16 North



















9 Looking east at UNT 1 to Avon Creek



10 Looking east at UNT 2 to Avon Creek



11 Looking southwest at the US 36/Ronald Reagan Parkway Intersection



12 Looking north at trail that intersects project area and is adjacent to Ronald Reagan Parkway





13 Looking northwest at the recommended NRHP-eligible McClain House



14 Looking southeast within project area



15 Looking northwest at the US 36/Raceway Road Intersection



16 Looking northwest within project area





17 Looking east at eastern project area



18 Looking southwest at the recommended NRHP-eligible David Faucett House



19 Looking west at eastern project area

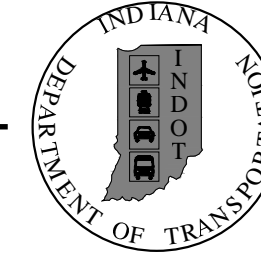


PROJECT	DESIGNATION
1601072	1601072
CONTRACT	BRIDGE FILE
R-39814	N/A

CULVERT ASSETS		
DES. NO.	CULVERT ASSET ID	WORK TYPE
0000000	CV 036-032-064.80	-

Note to the Reviewer: The des.nos. for the traffic signals and culvert are currently being obtained.

# INDIANA DEPARTMENT OF TRANSPORTATION



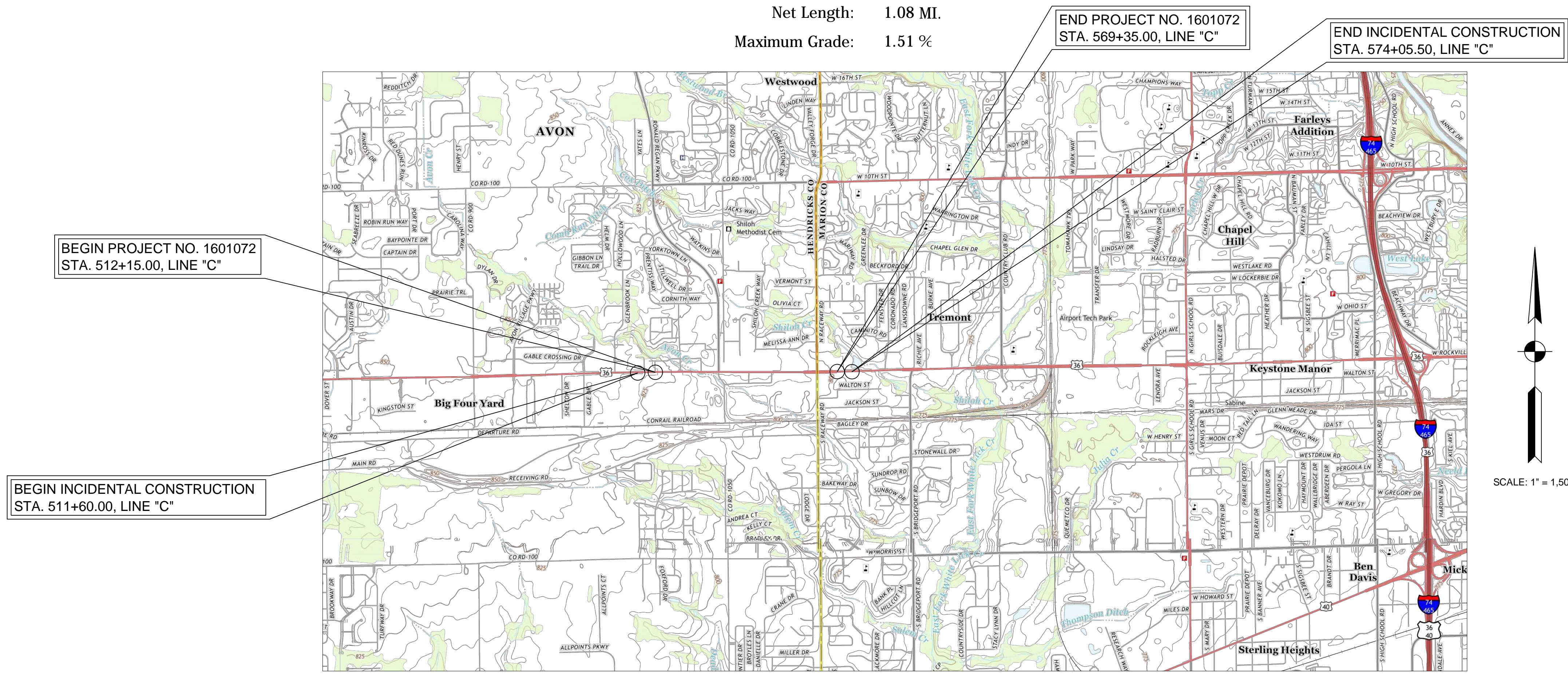
## ROAD PLANS

### US 36 ADDED TRAVEL LANES

PROJECT NO. 1601072 P.E.  
 PROJECT NO. 1601072 R/W  
 PROJECT NO. 1601072 CONST.

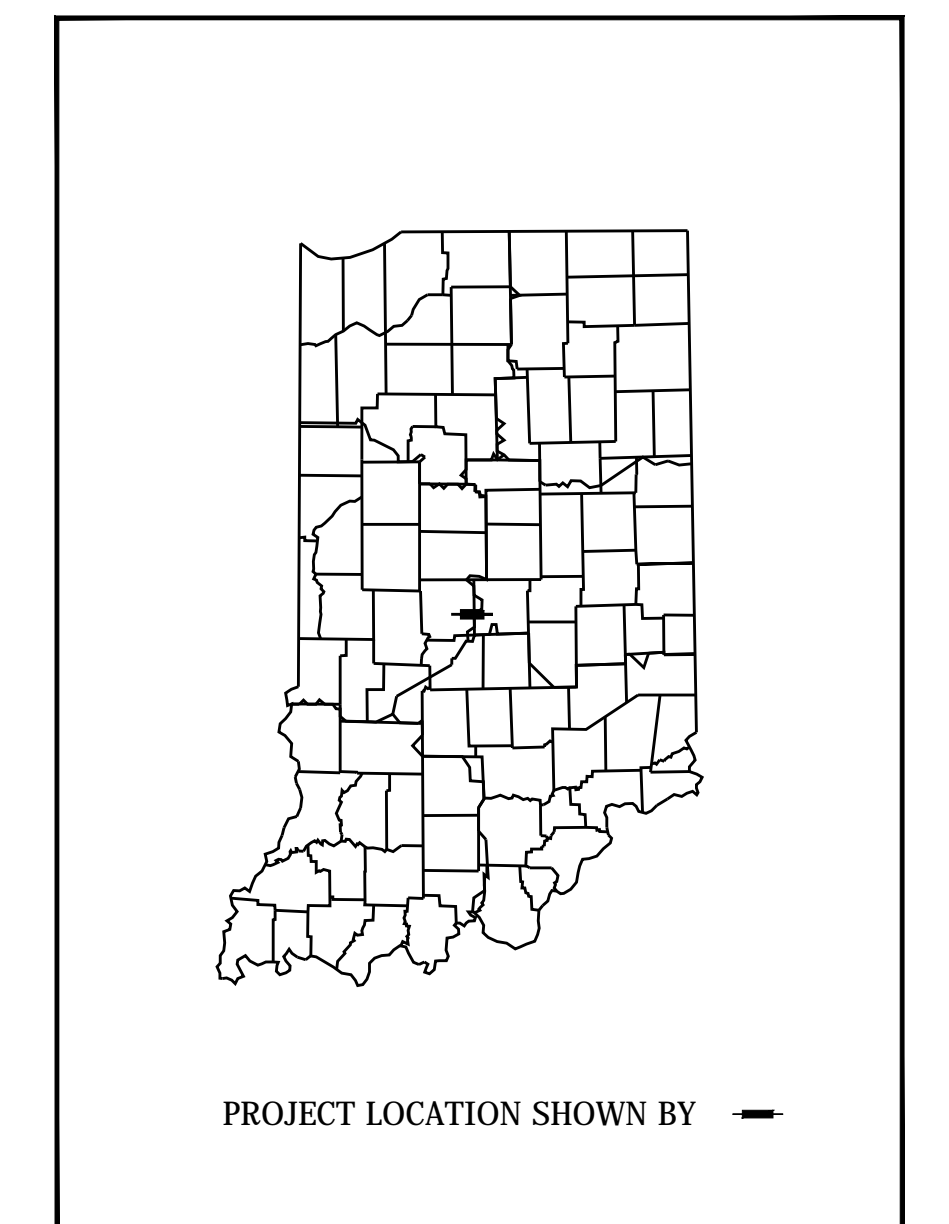
ROAD RECONSTRUCTION AND ADDED TRAVEL LANES BEGINNING AT SHILOH PARK DRIVE (RP 64+67) ON U.S. 36 (ROCKVILLE ROAD) TO 1500 FT EAST OF RACEWAY ROAD (RP 65+68) IN SECTIONS 5 & 8, TOWNSHIP 15 NORTH, RANGE 2 EAST IN WASHINGTON TOWNSHIP, IN HENDRICKS COUNTY, AND SECTIONS 4 & 9, TOWNSHIP 15 NORTH, RANGE 2 EAST IN WAYNE TOWNSHIP, IN MARION COUNTY, INDIANA.

Gross Length: 1.08 MI.  
 Net Length: 1.08 MI.  
 Maximum Grade: 1.51 %

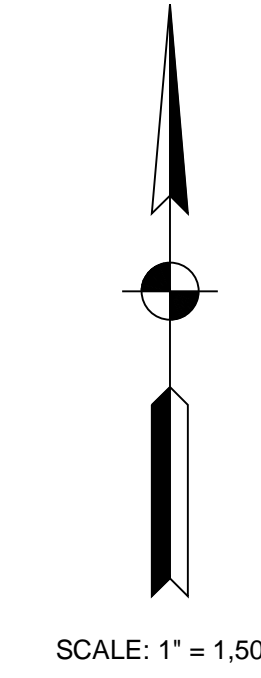


HENDRICKS COUNTY/MARION COUNTY

TRAFFIC DATA		LINE "C"
A.A.D.T. (2021)	44,398	V.P.D.
A.A.D.T. (2041)	48,838	V.P.D.
D.H.V. (2041)	4,900	V.P.H.
DIRECTIONAL DISTRIBUTION		50 %
TRUCKS		4 % A.A.D.T. 4 % D.H.V.
DESIGN DATA		
DESIGN SPEED	45 M.P.H.	
PROJECT DESIGN CRITERIA	RECONSTRUCTION (NON-FREEWAY)	
FUNCTIONAL CLASSIFICATION	PRINCIPAL ARTERIAL	
RURAL/URBAN	URBAN (INTERMEDIATE)	
TERRAIN	LEVEL	
ACCESS CONTROL	NONE	



BEGIN:	LATITUDE: 39° 45' 49" N	LONGITUDE: 86° 20' 36" W
END:	LATITUDE: 39° 45' 50" N	LONGITUDE: 86° 19' 20" W



Stage 2 Plans  
 MARCH 20, 2019

INDIANA DEPARTMENT OF TRANSPORTATION  
 STANDARD SPECIFICATIONS DATED 2018  
 TO BE USED WITH THESE PLANS.

PRINT DATE: 3/20/19  
 PLOT SCALE: 1:1  
 EDIT DATE: 7/12/11 - 1:51 PM  
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 DRAWN BY: KUSHAW  
 EDITED BY: KUSHAW

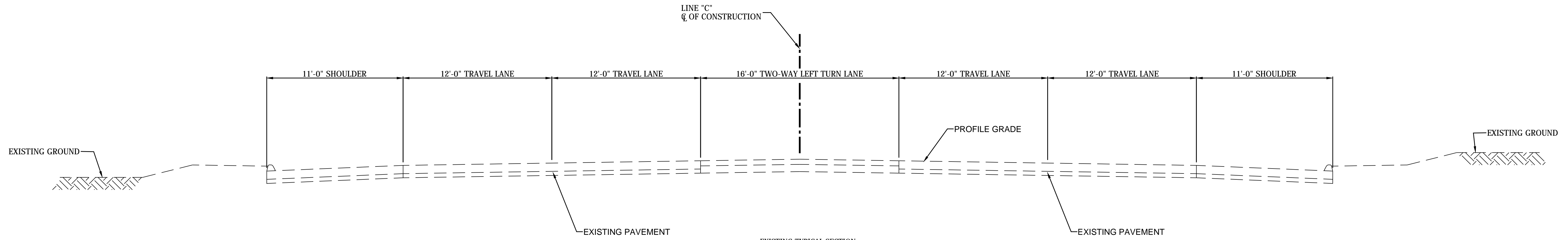
**RQAW**  
 INTENTIONAL INNOVATION

8770 NORTH ST., STE. 110  
 FISHERS, IN 46038  
 P: 317.588.1798  
 F: 317.588.1799  
 WWW.RQAW.COM

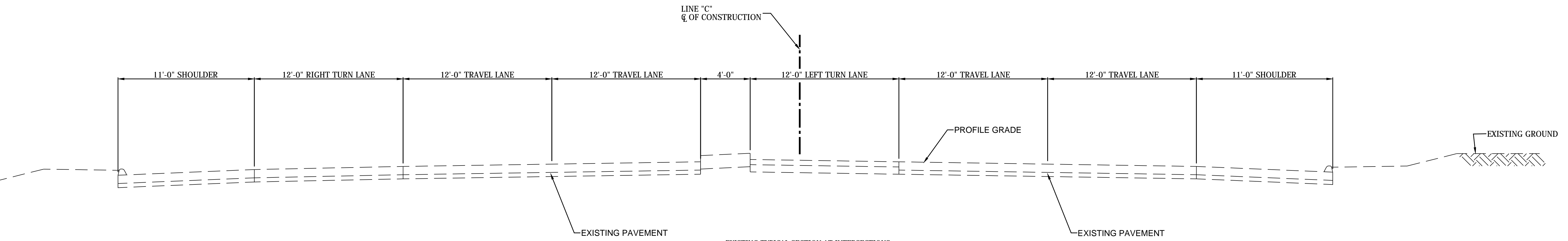
PLANS PREPARED BY:	RQAW Corporation	317-588-1798
		PHONE NUMBER
CERTIFIED BY:		DATE
APPROVED FOR LETTING:		DATE
INDIANA DEPARTMENT OF TRANSPORTATION		

SURVEY BOOK	SHEET
CONTRACT	1 of 96
R-39814	PROJECT
	1601072





EXISTING TYPICAL SECTION



EXISTING TYPICAL SECTION AT INTERSECTIONS

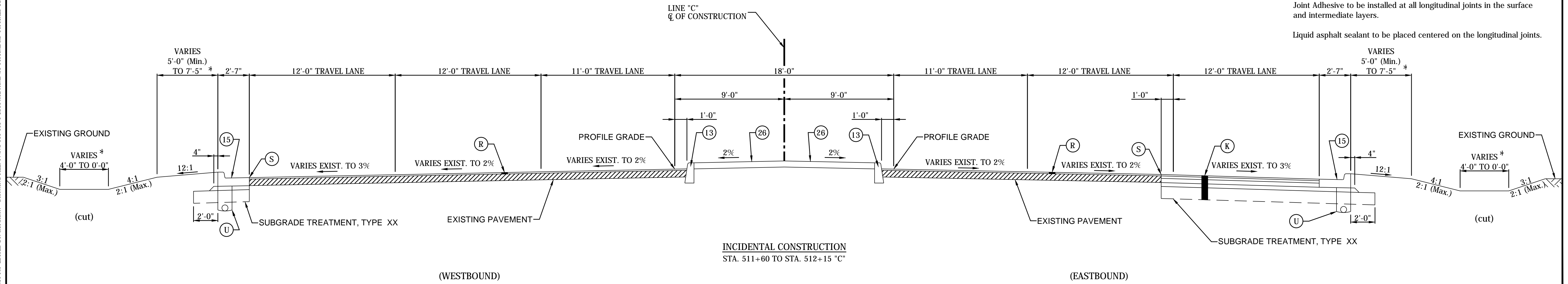
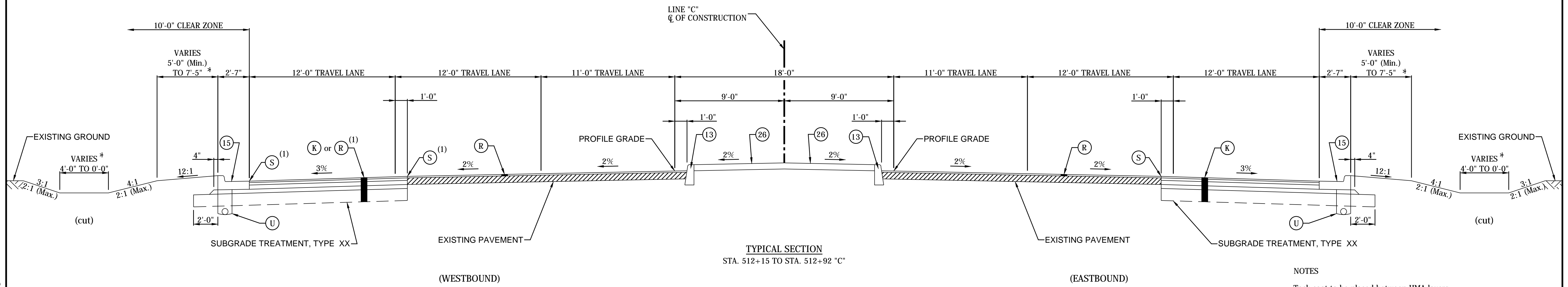
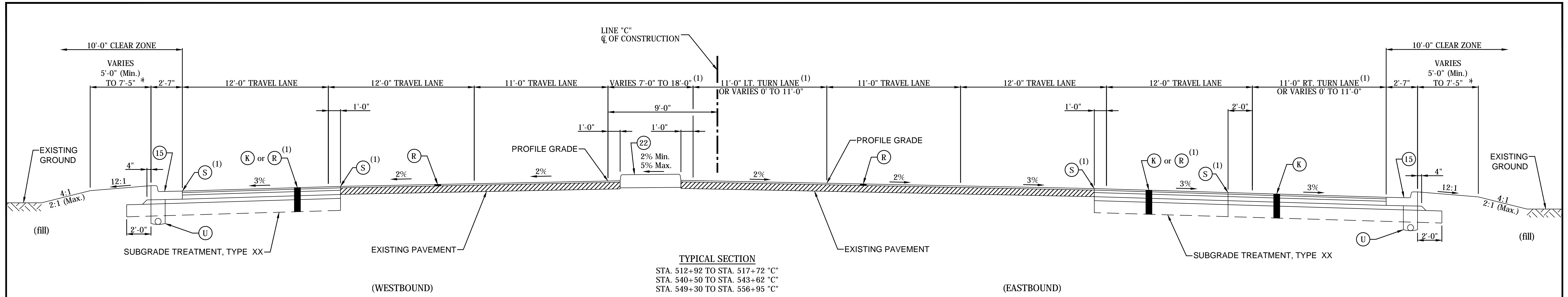
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 EDITED BY: KUPSIKAW  
 DRAWING FILE: P:\175-500-005-1 US36.ATI.MCAD\03 TYPICALS\RD-TYPICALS-US36.DWG

LEGEND		
(K) XXX lb/SYS QC/QA HMA, X, XX, Surface XX mm on XXX lb/SYS QC/QA HMA, X, XX, Intermediate XX mm on XXX lb/SYS QC/QA HMA, X, XX, Base XX mm on X in. Compacted Aggregate, No. 53 on Subgrade Treatment, Type XX	(C) Compacted Aggregate, No. 53, Base	(22) Concrete Center Curb Type D
(R) Milling, Asphalt, 1 1/2 in. (Minimum) 165 lb/SYS QC/QA HMA, 2, 64, Surface 9.5 mm on Variable Depth QC/QA HMA, 2, 64, Intermediate 19.0 mm (275 lb/SYS Minimum)	(S) Saw Cut	(26) Sodding
	(U) Underdrain	(1) See Construction Details
	(13) Concrete Curb	* See Cross Sections
	(15) Combined Concrete Curb & Gutter	

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: <u>K.R.U.</u>	DRAWN: <u>K.R.U.</u>	
CHECKED: <u>L.L.C.</u>	CHECKED: <u>R.D.S.</u>	

**INDIANA**  
**DEPARTMENT OF TRANSPORTATION**  
**EXISTING TYPICAL CROSS SECTIONS**  
**U.S. 36 (ROCKVILLE ROAD)**

HORIZONTAL SCALE	BRIDGE FILE
1/4" = 1'-0"	N/A
VERTICAL SCALE	DESIGNATION
N/A	1601072
SURVEY BOOK	SHEET
	3 of 96
CONTRACT	PROJECT
R-39814	1601072



**NOTES**

- Tack coat to be placed between HMA layers.
- Joint Adhesive to be installed at all longitudinal joints in the surface and intermediate layers.
- Liquid asphalt sealant to be placed centered on the longitudinal joints.

LEGEND			
(K)	XXX lb/SYS QC/QA HMA, X, XX, Surface XX mm on	(22)	Concrete Center Curb Type D
(S)	XXX lb/SYS QC/QA HMA, X, XX, Intermediate XX mm on	(26)	Sodding
(U)	XXX lb/SYS QC/QA HMA, X, XX, Base XX mm on	(1)	See Construction Details
(R)	X in. Compacted Aggregate, No. 53 on	*	See Cross Sections
(R)	Milling, Asphalt, 1 1/2 in. (Minimum)		
(15)	165 lb/SYS QC/QA HMA, 2, 64, Surface 9.5 mm on		
(13)	Variable Depth QC/QA HMA, 2, 64, Intermediate 19.0 mm (275 lb/SYS Minimum)		
(15)	Combined Concrete Curb & Gutter		
(15)	Compacted Aggregate, No. 53, Base		
(S)	Saw Cut		
(U)	Underdrain		

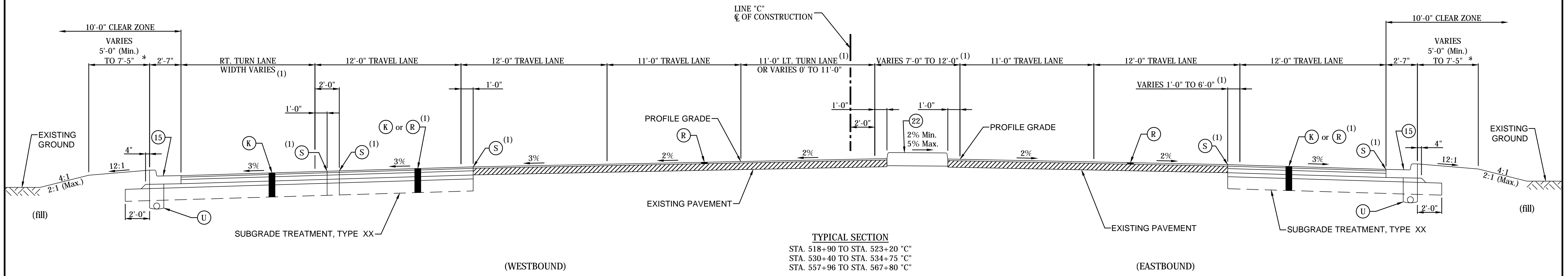
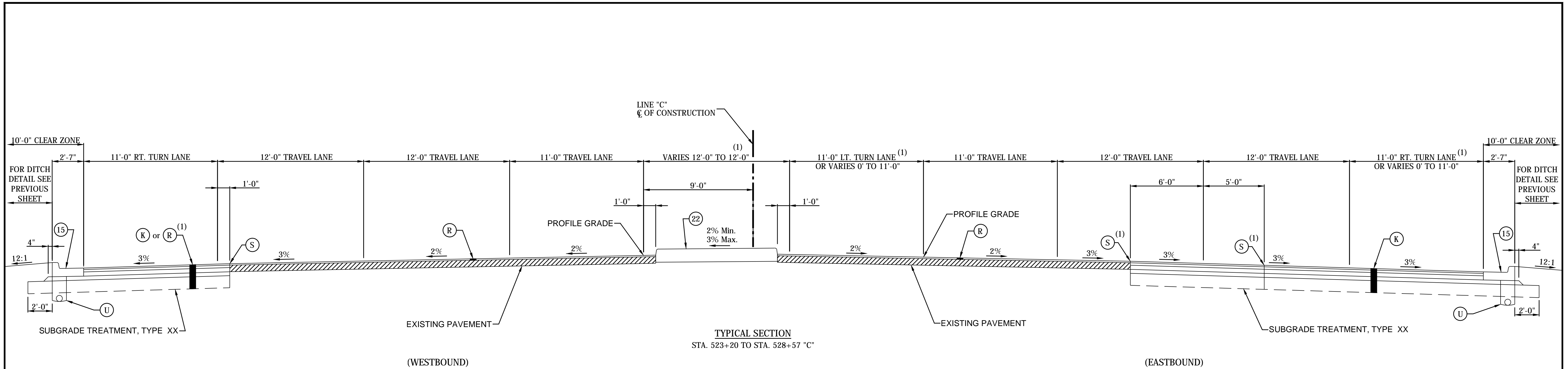
RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: K.R.U.	DRAWN: K.R.U.	
CHECKED: L.L.C.	CHECKED: R.D.S.	

**INDIANA**  
**DEPARTMENT OF TRANSPORTATION**

**TYPICAL CROSS SECTIONS**  
**U.S. 36 (ROCKVILLE ROAD)**

HORIZONTAL SCALE	BRIDGE FILE
1/4" = 1'-0"	N/A
VERTICAL SCALE	DESIGNATION
N/A	1601072
SURVEY BOOK	SHEET
	4 of 96
CONTRACT	PROJECT
R-39814	1601072

PRINT DATE: 3/20/19  
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 EDIT DATE: 2/15/19 - 1:50 PM  
 EDITOR: KUPSHAW  
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 EDIT DATE: 2/15/19 - 1:59 PM  
 EDITED BY: KUPSIKAW  
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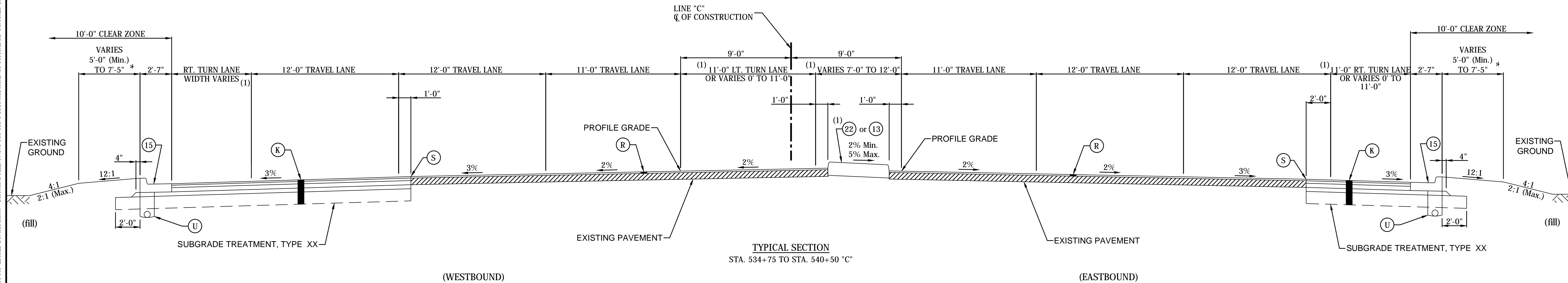
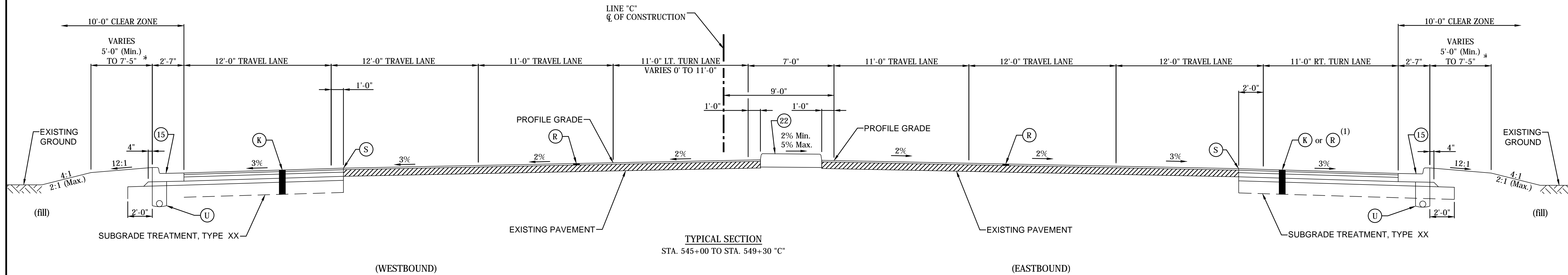
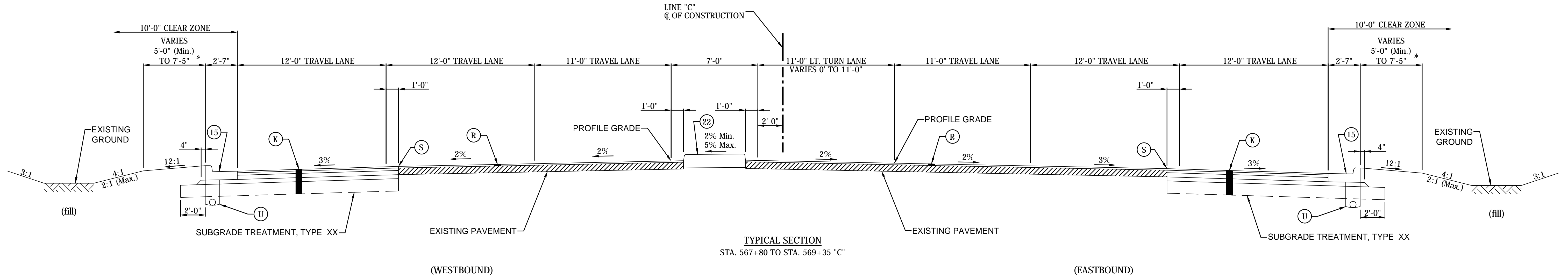
LEGEND		
(K) XXX lb/SYS QC/QA HMA, X, XX, Surface XX mm on XXX lb/SYS QC/QA HMA, X, XX, Intermediate XX mm on XXX lb/SYS QC/QA HMA, X, XX, Base XX mm on X in. Compacted Aggregate, No. 53 on Subgrade Treatment, Type XX	(20) Compacted Aggregate, No. 53, Base	(22) Concrete Center Curb Type D
(R) Milling, Asphalt, 1 1/2 in. (Minimum) 165 lb/SYS QC/QA HMA, 2, 64, Surface 9.5 mm on Variable Depth QC/QA HMA, 2, 64, Intermediate 19.0 mm (275 lb/SYS Minimum)	(S) Saw Cut	(26) Sodding
(U) Underdrain	(13) Concrete Curb	(1) See Construction Details
(15) Combined Concrete Curb & Gutter		* See Cross Sections

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: <u>K.R.U.</u>	DRAWN: <u>K.R.U.</u>	
CHECKED: <u>L.L.C.</u>	CHECKED: <u>R.D.S.</u>	

**INDIANA**  
**DEPARTMENT OF TRANSPORTATION**  
**TYPICAL CROSS SECTIONS**  
**U.S. 36 (ROCKVILLE ROAD)**

HORIZONTAL SCALE	BRIDGE FILE
1/4" = 1'-0"	N/A
VERTICAL SCALE	DESIGNATION
N/A	1601072
SURVEY BOOK	SHEET
	5 of 96
CONTRACT	PROJECT
R-39814	1601072

PRINT DATE: 3/20/19  
 PLOT SCALE: 1/4" = 1'-0"  
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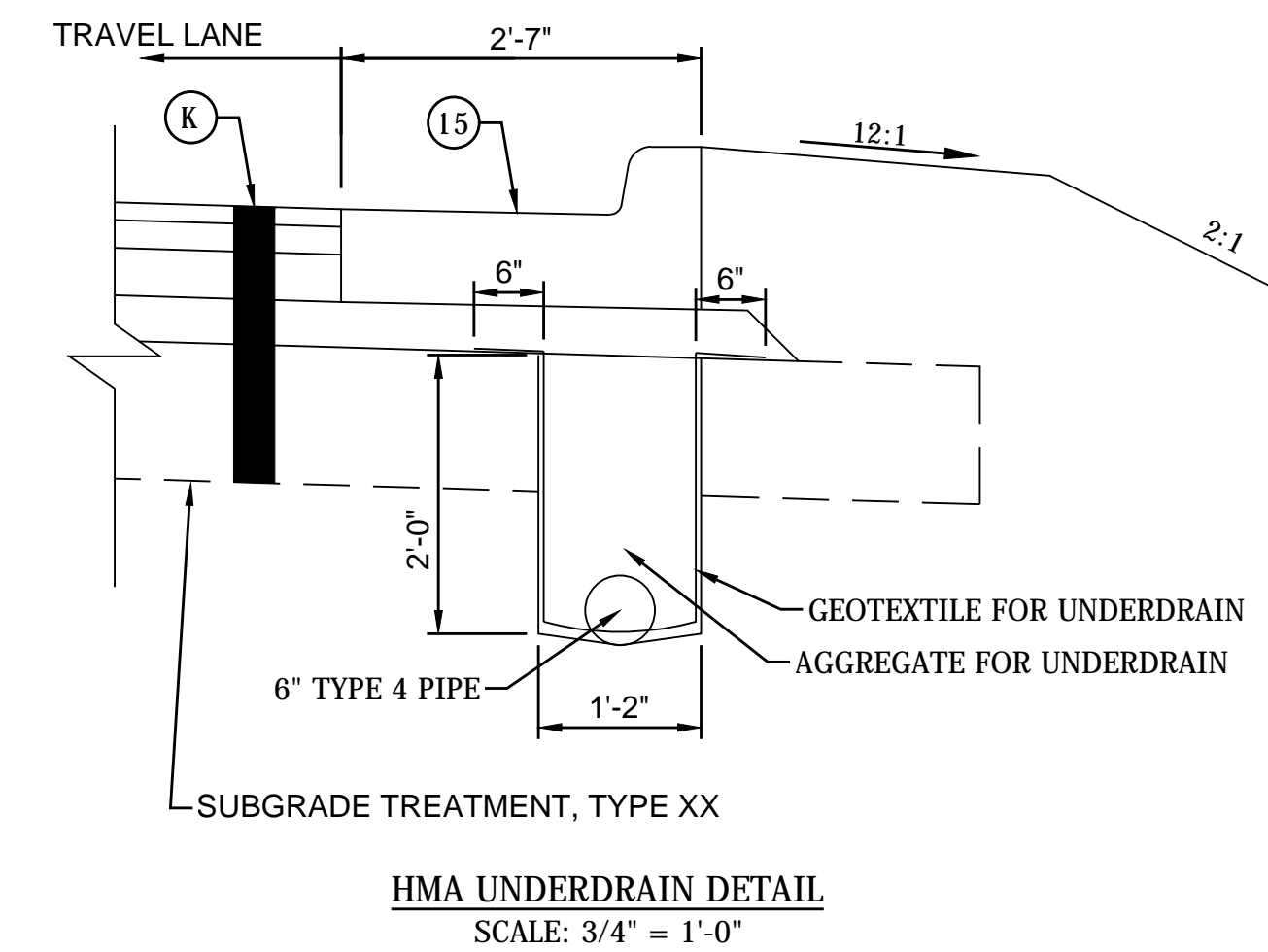
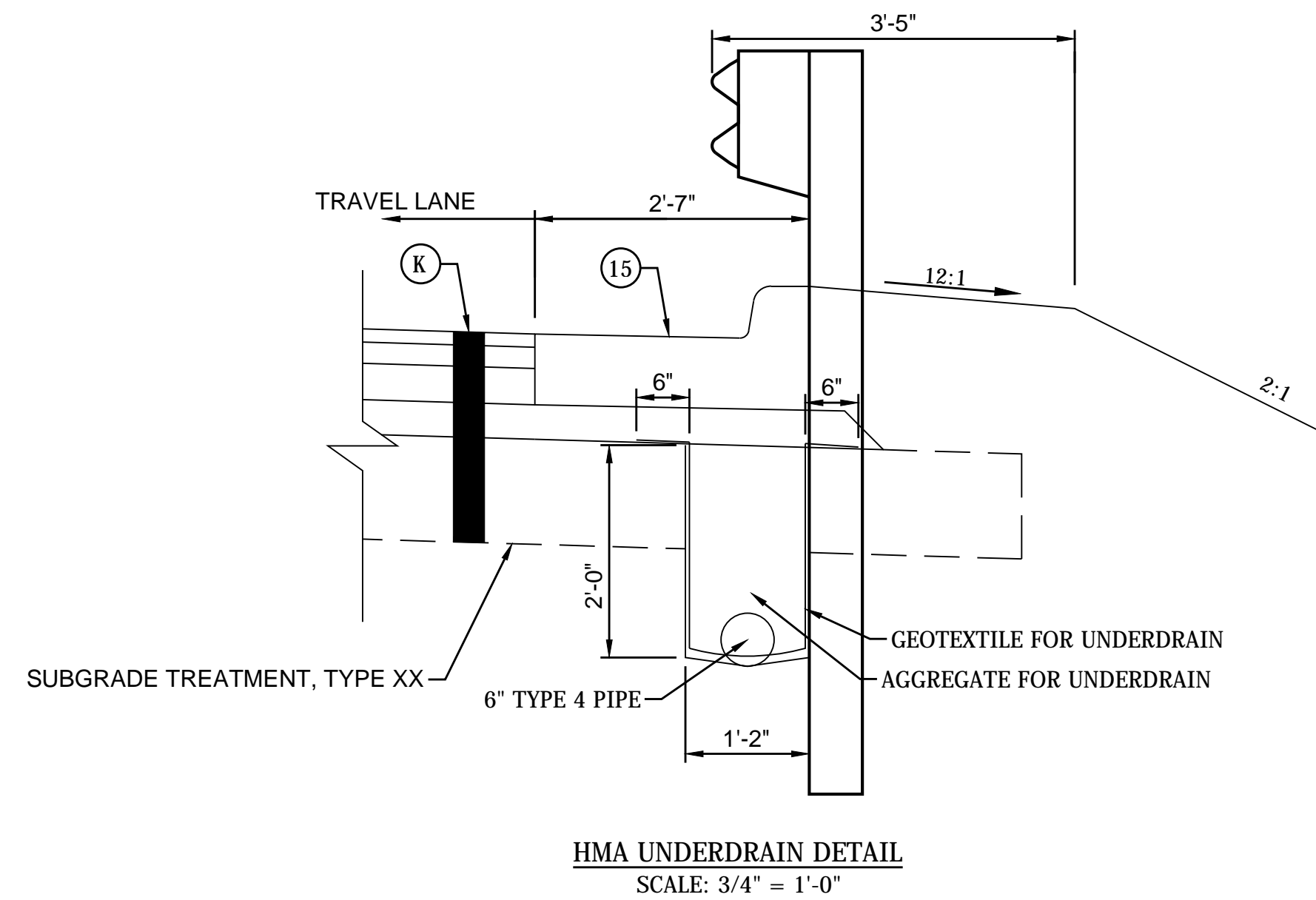
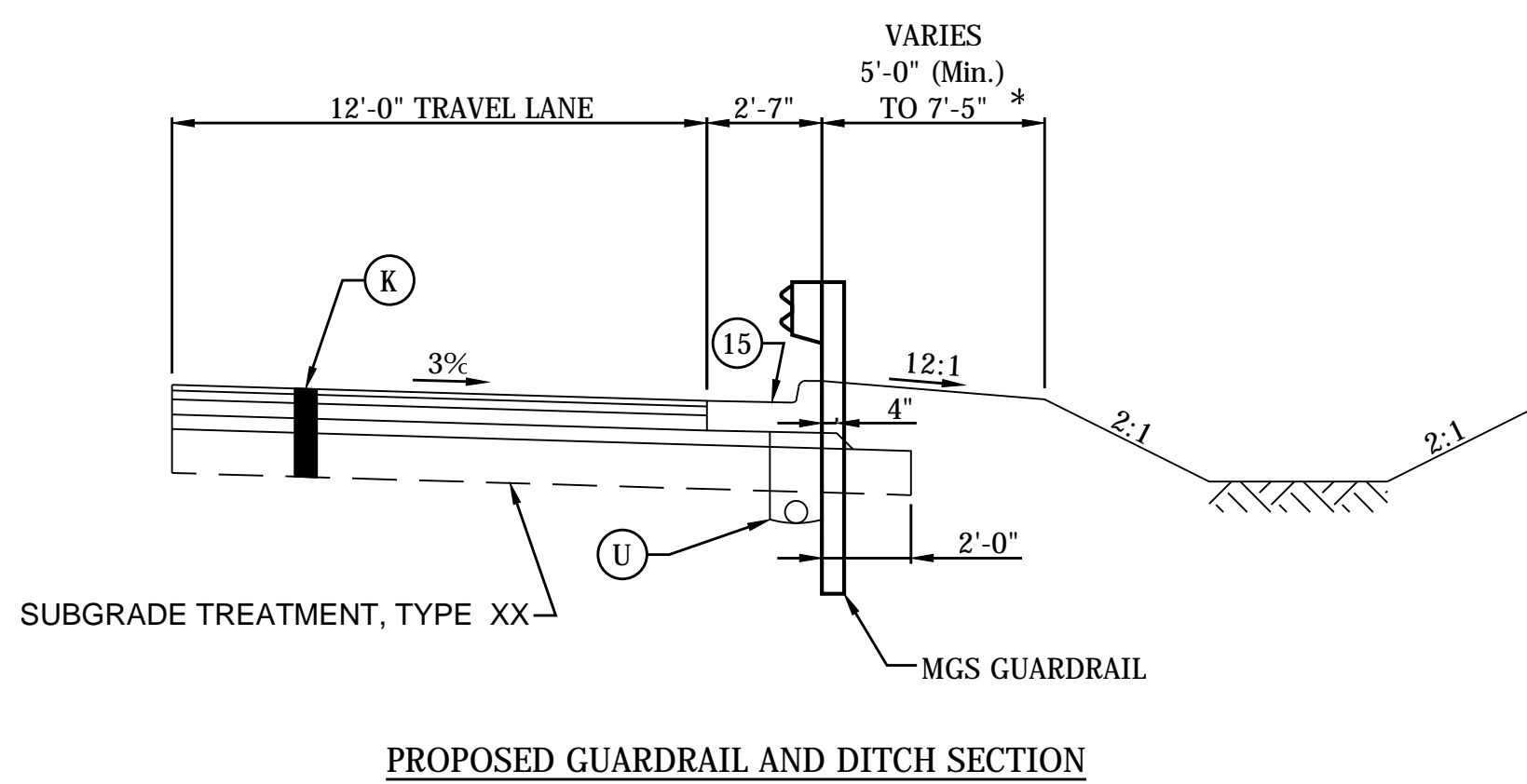
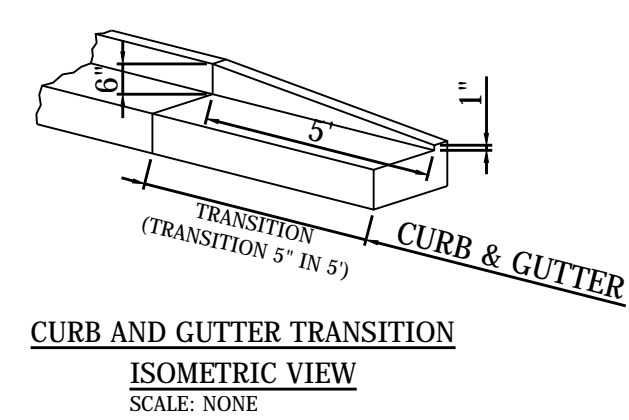
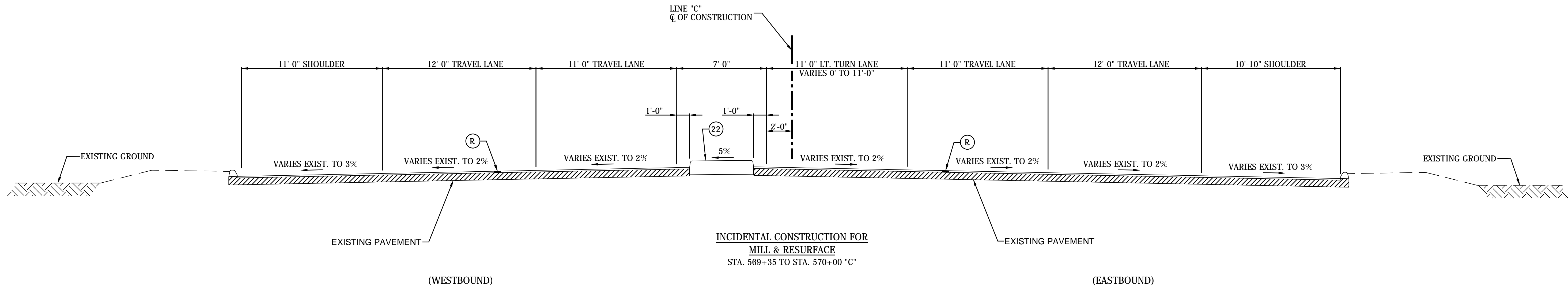


LEGEND		
(K) XXX lb/SYS QC/QA HMA, X, XX, Surface XX mm on XXX lb/SYS QC/QA HMA, X, XX, Intermediate XX mm on XXX lb/SYS QC/QA HMA, X, XX, Base XX mm on X in. Compacted Aggregate, No. 53 on Subgrade Treatment, Type XX	(O) Compacted Aggregate, No. 53, Base	(22) Concrete Center Curb Type D
(R) Milling, Asphalt, 1 1/2 in. (Minimum) 165 lb/SYS QC/QA HMA, 2, 64, Surface 9.5 mm on Variable Depth QC/QA HMA, 2, 64, Intermediate 19.0 mm (275 lb/SYS Minimum)	(S) Saw Cut	(26) Sodding
	(U) Underdrain	(1) See Construction Details
	(13) Concrete Curb	* See Cross Sections
	(15) Combined Concrete Curb & Gutter	

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: K.R.U.	DRAWN: K.R.U.	
CHECKED: L.L.C.	CHECKED: R.D.S.	

**INDIANA**  
**DEPARTMENT OF TRANSPORTATION**  
  
**TYPICAL CROSS SECTIONS**  
**U.S. 36 (ROCKVILLE ROAD)**

HORIZONTAL SCALE	BRIDGE FILE
1/4" = 1'-0"	N/A
VERTICAL SCALE	DESIGNATION
N/A	1601072
SURVEY BOOK	SHEET
	6 of 96
CONTRACT	PROJECT
R-39814	1601072



LEGEND		
(K) XXX lb/SYS QC/QA HMA, X, XX, Surface XX mm on XXX lb/SYS QC/QA HMA, X, XX, Intermediate XX mm on XXX lb/SYS QC/QA HMA, X, XX, Base XX mm on X in. Compacted Aggregate, No. 53 on Subgrade Treatment, Type XX	(O) Compacted Aggregate, No. 53, Base	(22) Concrete Center Curb Type D
(R) Milling, Asphalt, 1 1/2 in. (Minimum) 165 lb/SYS QC/QA HMA, 2, 64, Surface 9.5 mm on Variable Depth QC/QA HMA, 2, 64, Intermediate 19.0 mm (275 lb/SYS Minimum)	(S) Saw Cut	(26) Sodding
	(U) Underdrain	(1) See Construction Details
	(13) Concrete Curb	* See Cross Sections
	(15) Combined Concrete Curb & Gutter	

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: K.R.U.	DRAWN: K.R.U.	
CHECKED: L.L.C.	CHECKED: R.D.S.	

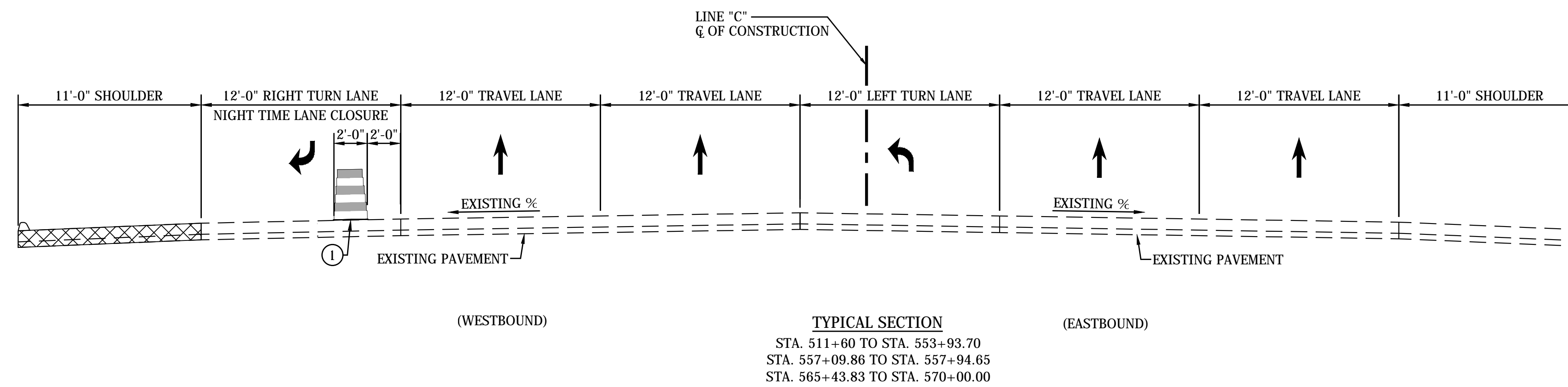
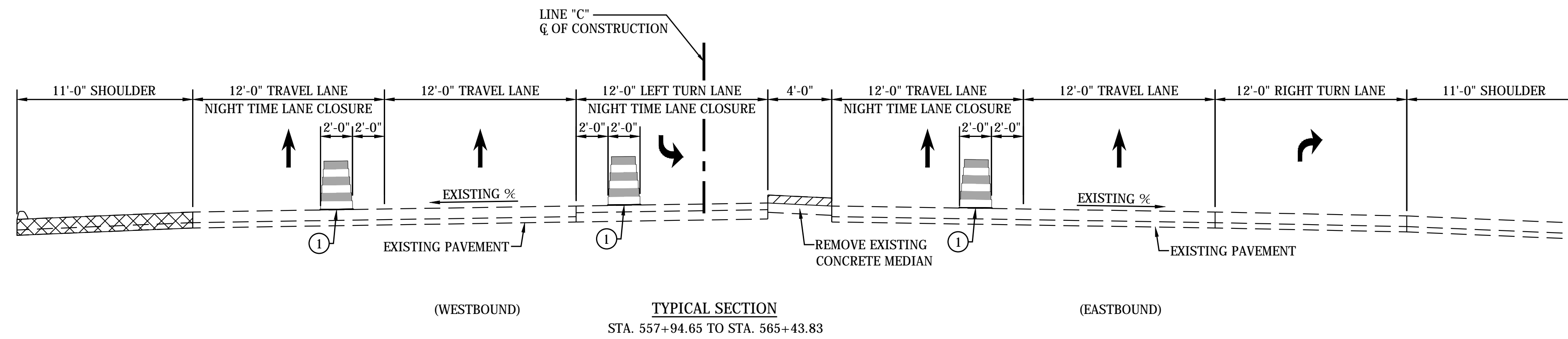
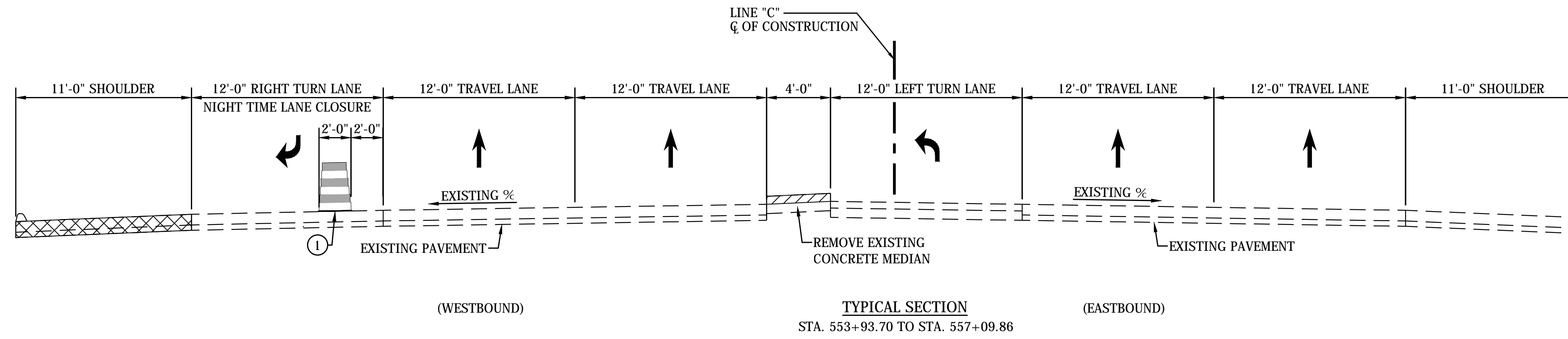
INDIANA  
DEPARTMENT OF TRANSPORTATION

TYPICAL CROSS SECTIONS  
U.S. 36 (ROCKVILLE ROAD)

HORIZONTAL SCALE	BRIDGE FILE
1/4" = 1'-0"	N/A
VERTICAL SCALE	DESIGNATION
N/A	1601072
SURVEY BOOK	SHEET
	7 of 96
CONTRACT	PROJECT
R-39814	1601072

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- NOTES FOR ALL PHASES:**
1. ALL CONSTRUCTION TO OCCUR DURING NIGHT TIME HOURS ONLY.
  2. THE CONTRACTOR SHALL NOT CLOSE MORE THAN ONE LANE IN EACH DIRECTION AT THE SAME TIME.
  3. DRUMS TO BE PLACED ON CONSTRUCTION SHOULDER DURING DAY TIME.

WORK ZONE DESIGN SPEED = 35 MPH

**LEGEND**

	AREA OF CONSTRUCTION
	SHOULDER STRENGTHENING & PATCHING LOCATIONS TO BE DETERMINED (WORK TO BE COMPLETED AT NIGHT)
	CONSTRUCTION DRUM

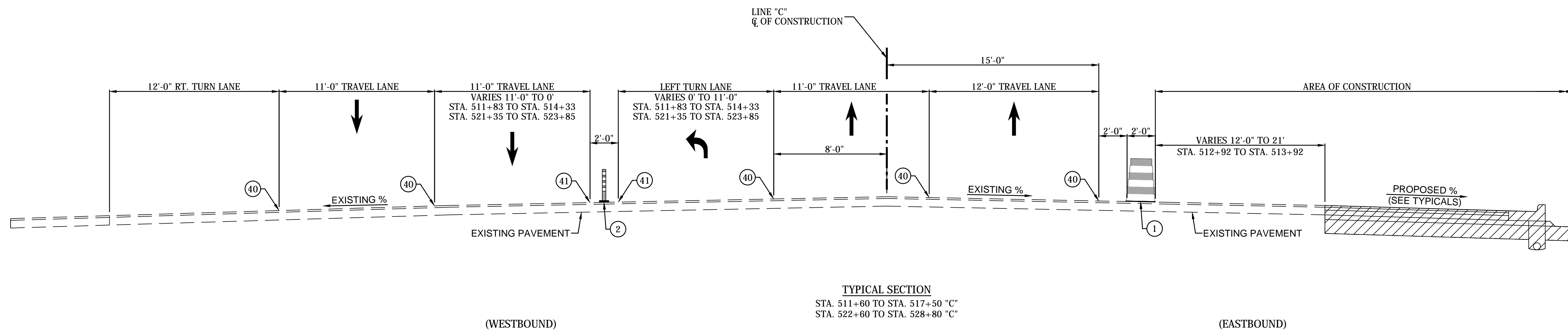
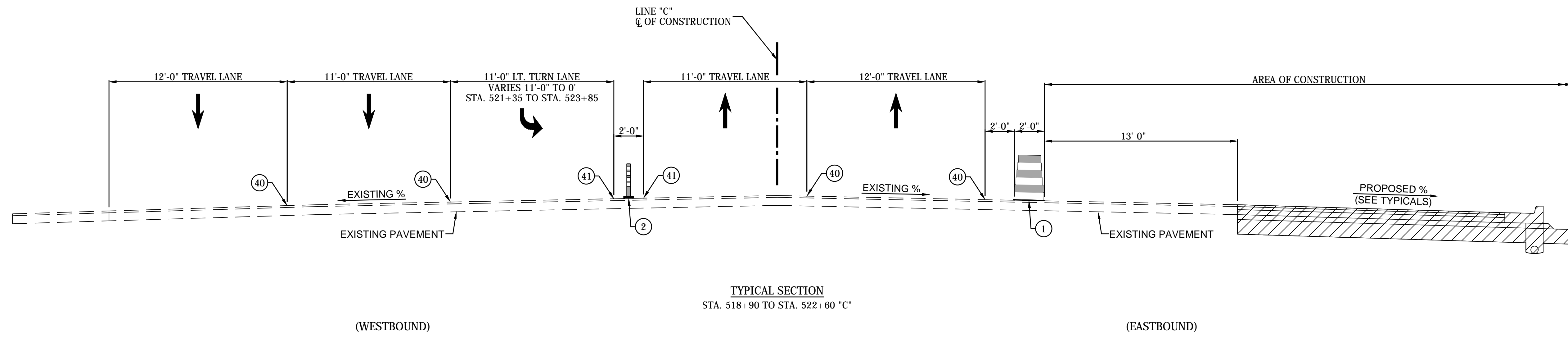
RECOMMENDED FOR APPROVAL \_\_\_\_\_  
DESIGN ENGINEER DATE

DESIGNED: R.D.S. DRAWN: K.R.U.  
CHECKED: L.L.C. CHECKED: L.L.C.

**INDIANA  
DEPARTMENT OF TRANSPORTATION**

**MAINTENANCE OF TRAFFIC  
TYPICAL CROSS SECTIONS - PHASE 1**

HORIZONTAL SCALE	BRIDGE FILE
1" = 5'	N/A
VERTICAL SCALE	DESIGNATION
1" = 5'	1601072
SURVEY BOOK	SHEET
	11 of 96
CONTRACT	PROJECT
R-39814	1601072



WORK ZONE DESIGN SPEED = 35 MPH

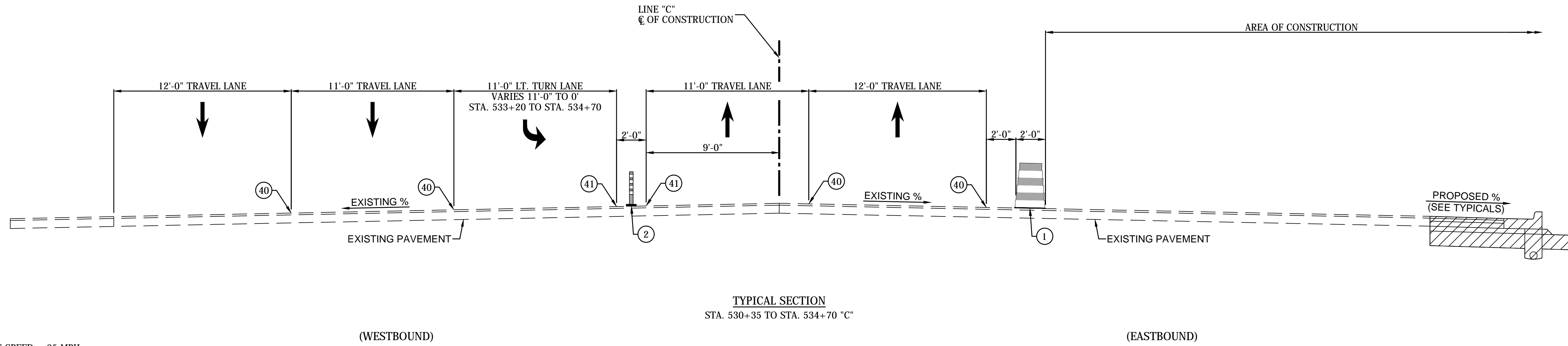
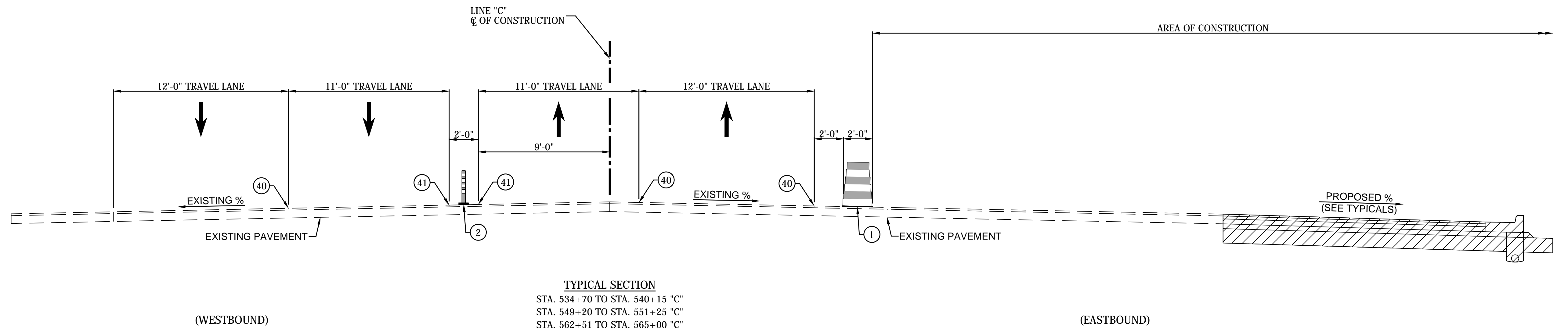
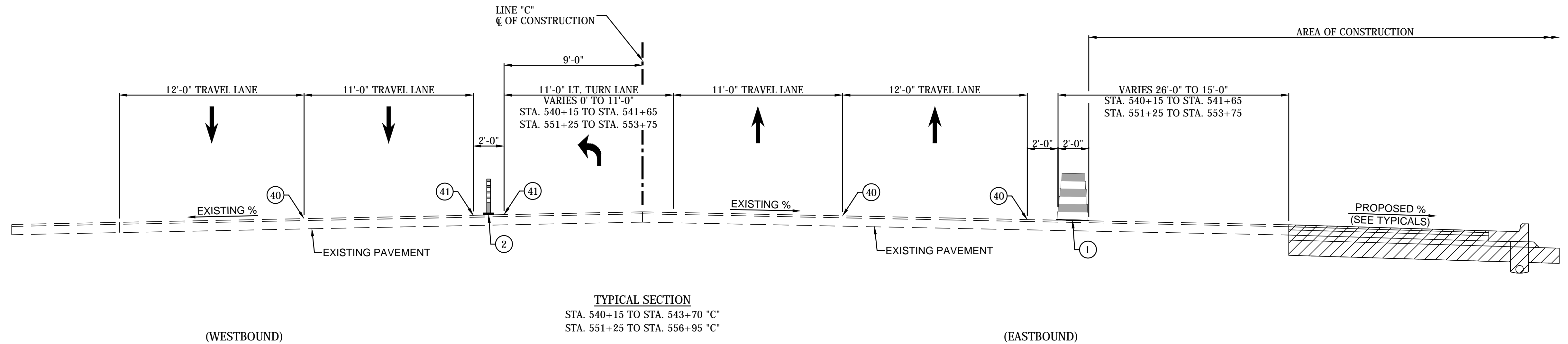
LEGEND	
AREA OF CONSTRUCTION	TEMP. PAVEMENT MARKINGS, SOLID WHITE, 4"
CONSTRUCTION DRUM	TEMP. PAVEMENT MARKINGS, SOLID YELLOW 4"
TUBULAR MARKER	

RECOMMENDED FOR APPROVAL _____	
DESIGNED: <u>R.D.S.</u>	DRAWN: <u>K.R.U.</u>
CHECKED: <u>L.L.C.</u>	CHECKED: <u>L.L.C.</u>

<b>INDIANA DEPARTMENT OF TRANSPORTATION</b>	
<b>MAINTENANCE OF TRAFFIC TYPICAL CROSS SECTIONS - PHASE 2</b>	

HORIZONTAL SCALE	BRIDGE FILE
1/4" = 1'	N/A
VERTICAL SCALE	DESIGNATION
1/4" = 1'	1601072
SURVEY BOOK	SHEET
	12 of 96
CONTRACT	PROJECT
R-39814	1601072

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WORK ZONE DESIGN SPEED = 35 MPH

LEGEND	
	AREA OF CONSTRUCTION
	CONSTRUCTION DRUM
	TUBULAR MARKER
	TEMP. PAVEMENT MARKINGS, SOLID WHITE, 4"
	TEMP. PAVEMENT MARKINGS, SOLID YELLOW 4"

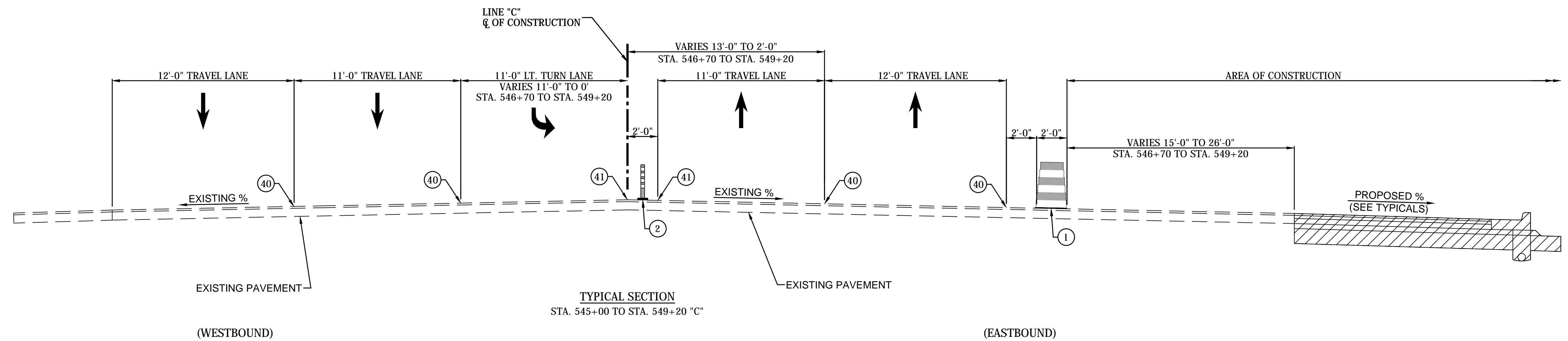
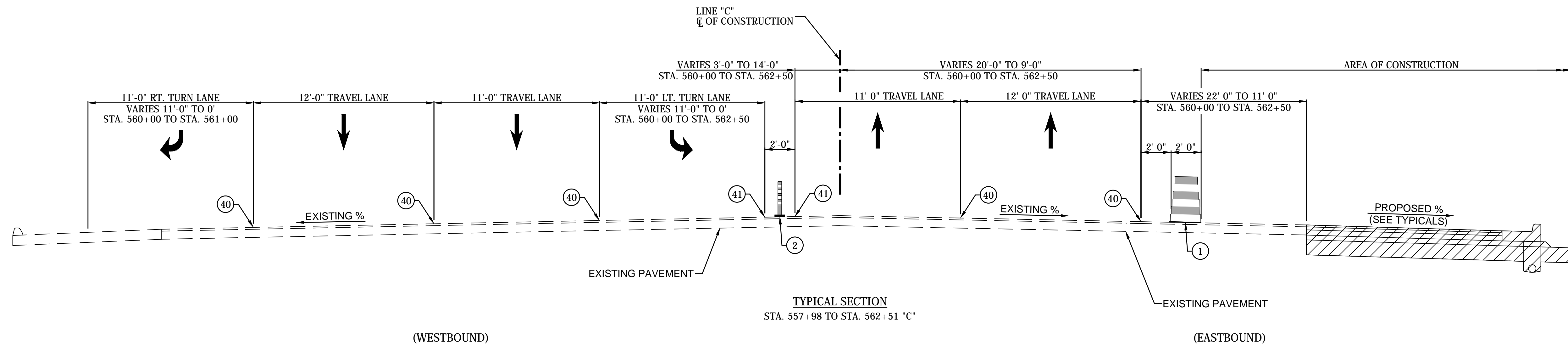
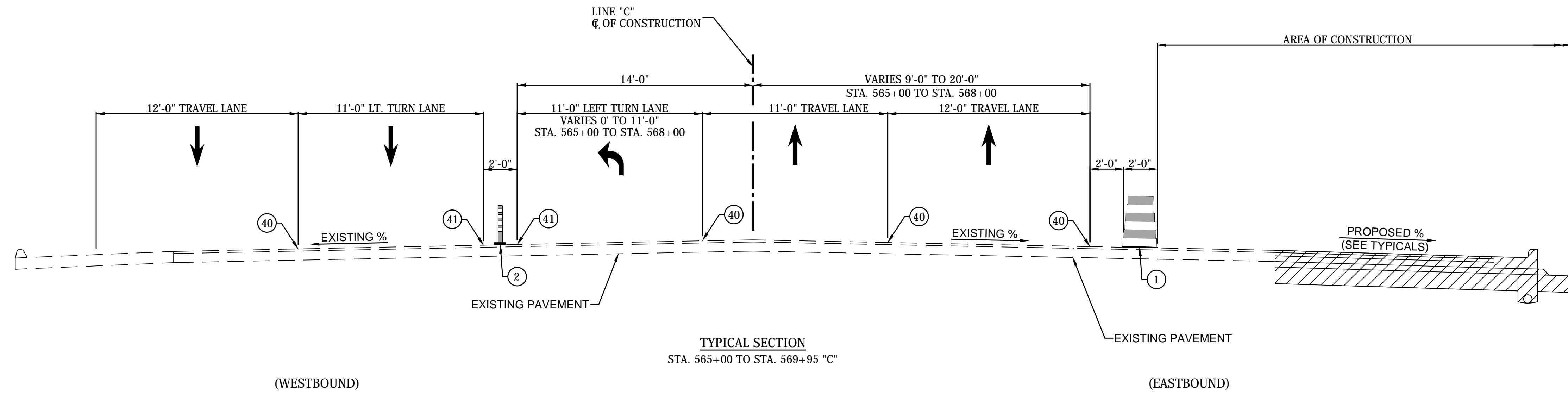
RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: R.D.S.	DRAWN: K.R.U.	
CHECKED: L.L.C.	CHECKED: L.L.C.	

**INDIANA DEPARTMENT OF TRANSPORTATION**

**MAINTENANCE OF TRAFFIC TYPICAL CROSS SECTIONS - PHASE 2**

HORIZONTAL SCALE	BRIDGE FILE
1/4" = 1'	N/A
VERTICAL SCALE	DESIGNATION
1/4" = 1'	1601072
SURVEY BOOK	SHEET
	13 of 96
CONTRACT	PROJECT
R-39814	1601072

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WORK ZONE DESIGN SPEED = 35 MPH

LEGEND	
	AREA OF CONSTRUCTION
	CONSTRUCTION DRUM
	TUBULAR MARKER
	TEMP. PAVEMENT MARKINGS, SOLID WHITE, 4"
	TEMP. PAVEMENT MARKINGS, SOLID YELLOW 4"

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: R.D.S.	DRAWN: K.R.U.	
CHECKED: L.L.C.	CHECKED: L.L.C.	

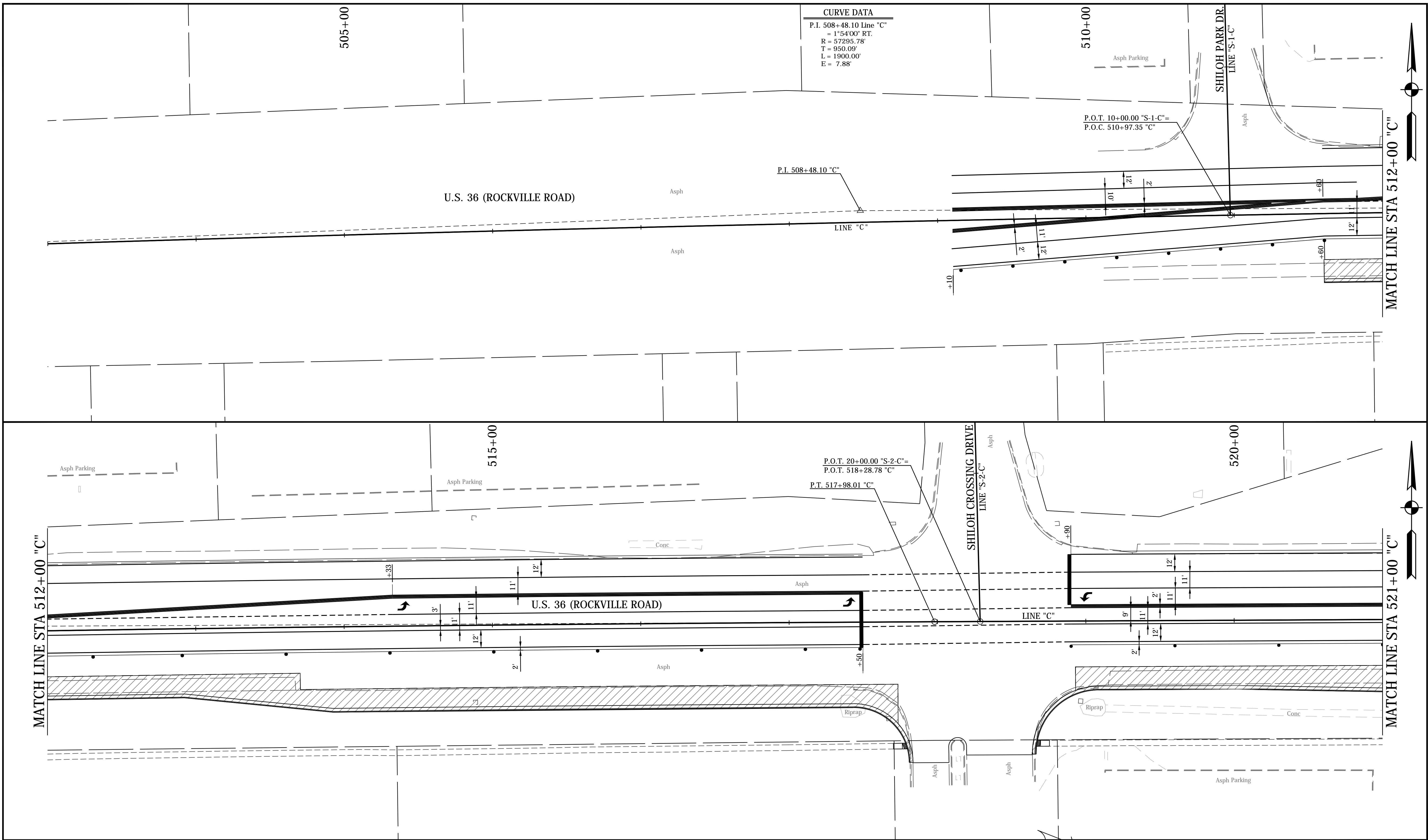
INDIANA DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC  
TYPICAL CROSS SECTIONS - PHASE 2

HORIZONTAL SCALE	BRIDGE FILE
1/4" = 1'	N/A
VERTICAL SCALE	DESIGNATION
1/4" = 1'	1601072
SURVEY BOOK	SHEET
	14 of 96
CONTRACT	PROJECT
R-39814	1601072

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 EDITED BY: MISELS  
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**CURVE DATA**  
 P.I. 508+48.10 Line "C"  
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 R = 57295.78'  
 T = 950.09'  
 L = 1900.00'  
 E = 7.88'

**LEGEND**

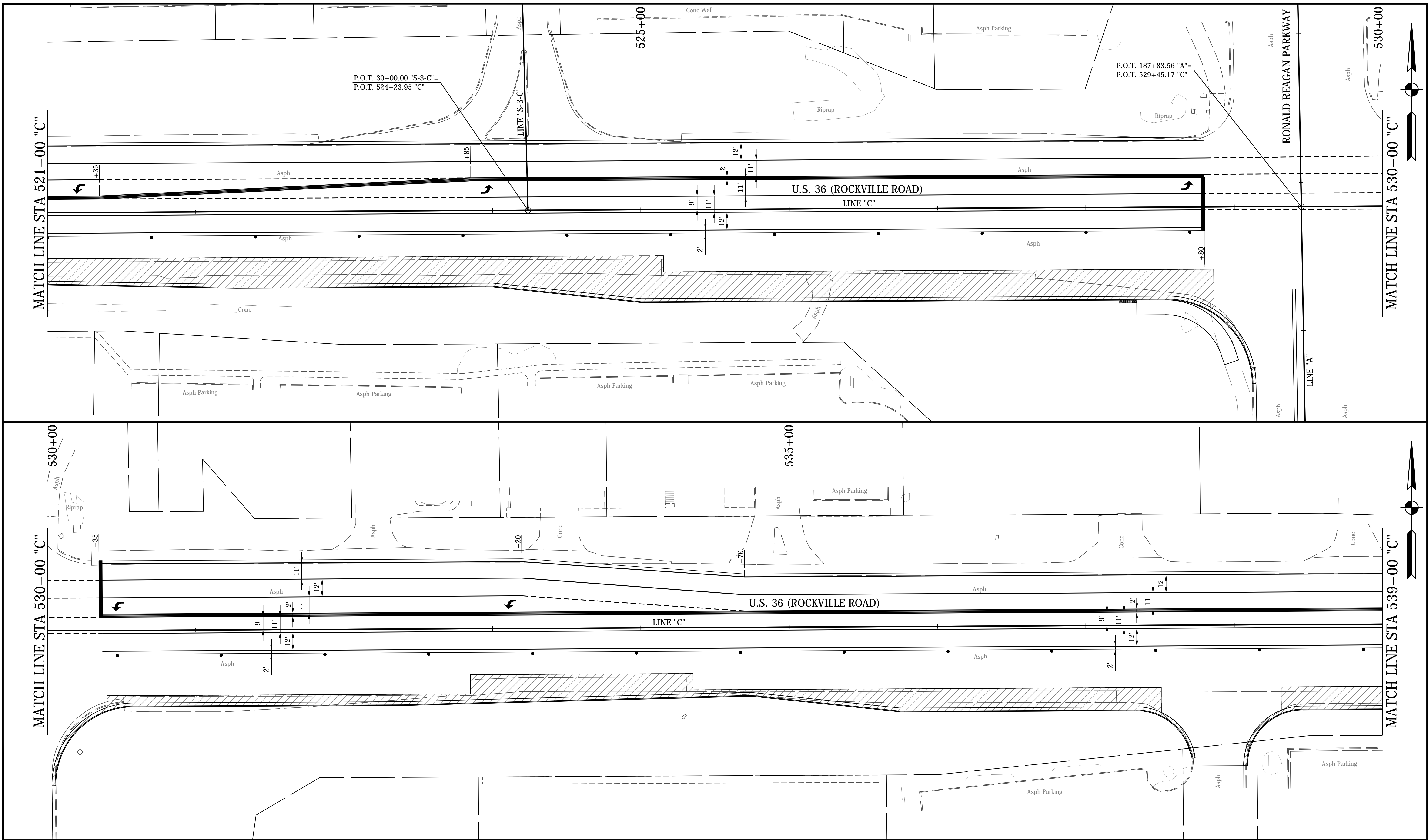
- AREA OF CONSTRUCTION
  - DRUM w/ WARNING LIGHT
- WORK ZONE DESIGN SPEED = 35 MPH

RECOMMENDED FOR APPROVAL _____		DESIGN ENGINEER _____	DATE _____
DESIGNED: <u>R.D.S.</u>	DRAWN: <u>M.S.S.</u>		
CHECKED: <u>L.L.C.</u>	CHECKED: <u>L.L.C.</u>		

**INDIANA**  
**DEPARTMENT OF TRANSPORTATION**  
  
**MAINTENANCE OF TRAFFIC**  
**PHASE 2**

HORIZONTAL SCALE	BRIDGE FILE
1" = 30'	N/A
VERTICAL SCALE	DESIGNATION
N/A	1601072
SURVEY BOOK	SHEET
	15 of 96
CONTRACT	PROJECT
R-39814	1601072

PRINT DATE: 3/20/19  
 PLOT SCALE: 1:1  
 EDIT DATE: 7/12/11 - 9:18 AM  
 EDITED BY: MISELS  
 DRAWING FILE: P:\75-500-003\15536\ATL\CAD\05\MOTRD\_MOTPH2\_C\_30\_01.DWG



P.O.T. 30+00.00 "S-3-C"=  
 P.O.T. 524+23.95 "C"

P.O.T. 187+83.56 "A"=  
 P.O.T. 529+45.17 "C"

MATCH LINE STA 521+00 "C"

MATCH LINE STA 530+00 "C"

MATCH LINE STA 530+00 "C"

MATCH LINE STA 539+00 "C"

**LEGEND**

- AREA OF CONSTRUCTION
  - DRUM w/ WARNING LIGHT
- WORK ZONE DESIGN SPEED = 35 MPH

RECOMMENDED FOR APPROVAL \_\_\_\_\_  
 DESIGN ENGINEER DATE

DESIGNED: R.D.S. DRAWN: M.S.S.  
 CHECKED: L.L.C. CHECKED: L.L.C.

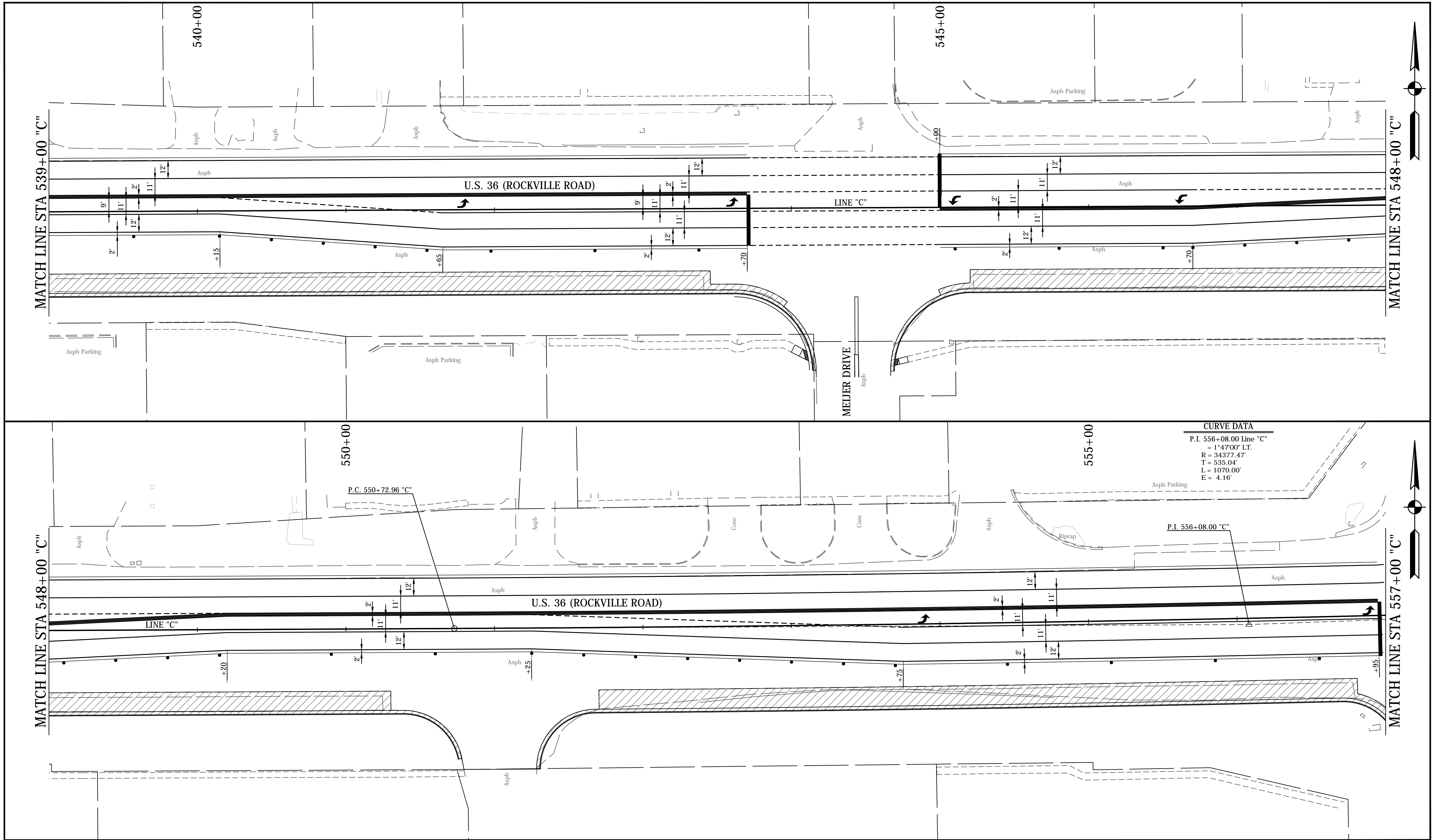
**INDIANA DEPARTMENT OF TRANSPORTATION**

**MAINTENANCE OF TRAFFIC PHASE 2**

HORIZONTAL SCALE	BRIDGE FILE
1" = 30'	N/A
VERTICAL SCALE	DESIGNATION
N/A	1601072
SURVEY BOOK	SHEET
	16 of 96
CONTRACT	PROJECT
R-39814	1601072



PRINT DATE: 3/20/19  
 PLOT SCALE: 1:1  
 EDIT DATE: 7/12/11 - 9:18 AM  
 EDITED BY: MISELS  
 DRAWING FILE: P:\175-500-003 - US36 ATLACAD\05 MOTTRD\_MOTPH2\_C\_30\_01.DWG



**LEGEND**

AREA OF CONSTRUCTION

DRUM w/ WARNING LIGHT

WORK ZONE DESIGN SPEED = 35 MPH

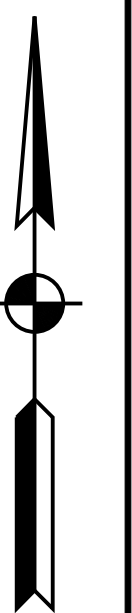
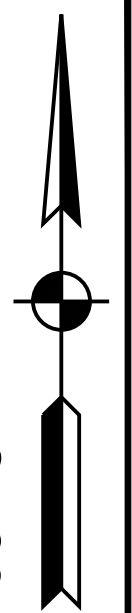
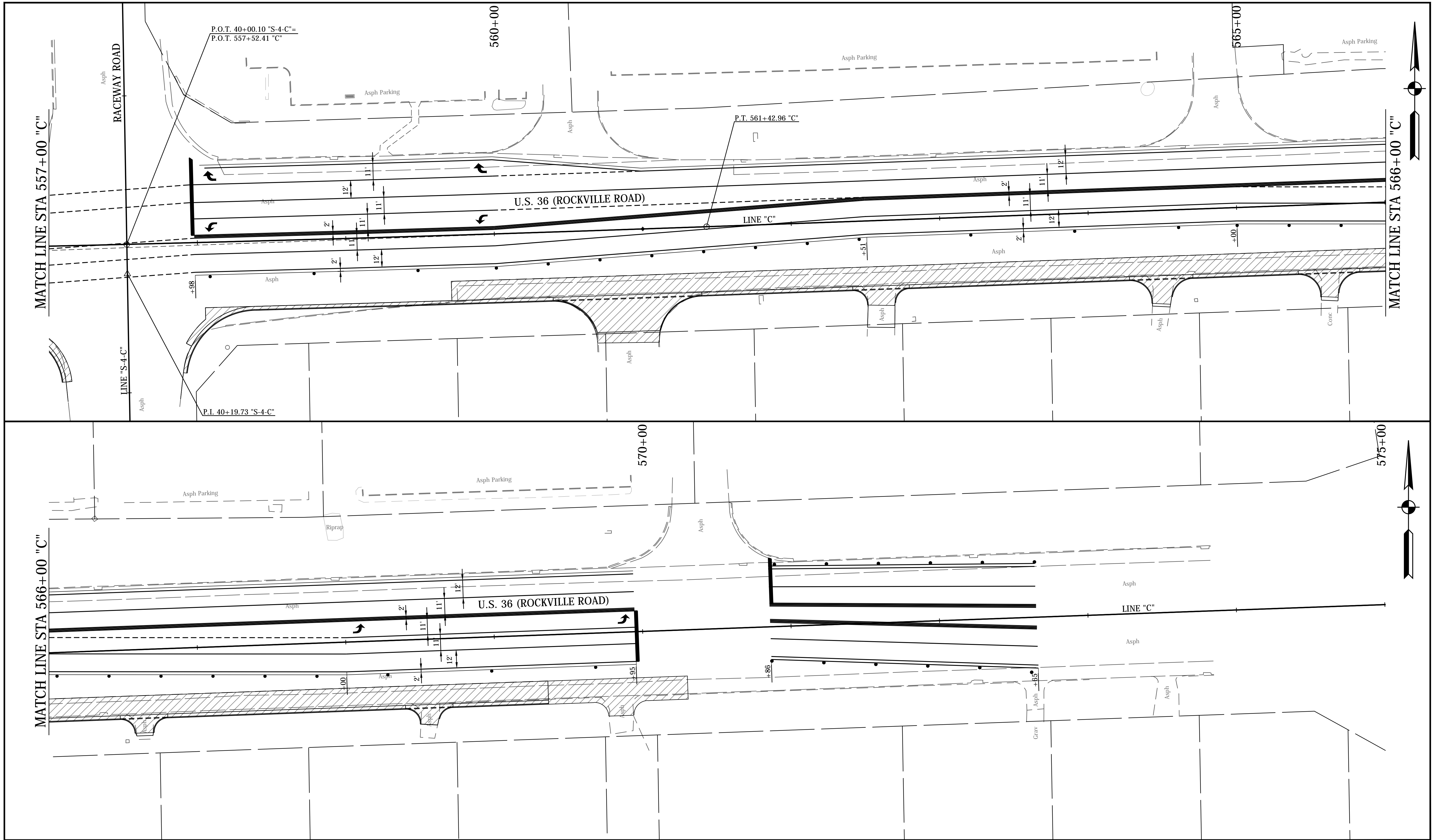
RECOMMENDED FOR APPROVAL		DESIGN ENGINEER	DATE
DESIGNED: R.D.S.	DRAWN: M.S.S.		
CHECKED: L.L.C.	CHECKED: L.L.C.		

**INDIANA DEPARTMENT OF TRANSPORTATION**

**MAINTENANCE OF TRAFFIC PHASE 2**

HORIZONTAL SCALE	BRIDGE FILE
1" = 30'	N/A
VERTICAL SCALE	DESIGNATION
N/A	1601072
SURVEY BOOK	SHEET
	17 of 96
CONTRACT	PROJECT
R-39814	1601072

PRINT DATE: 3/20/19  
 PLOT SCALE: 1:1  
 EDIT DATE: 7/12/11 - 9:18 AM  
 EDITED BY: MISELLS  
 DRAWING FILE: P:\175-500-003\175500-003\ISS56\ATL\CAD\05\MOTR\RD\_MOTR\PH2\_C\_30\_01.DWG



**LEGEND**

- AREA OF CONSTRUCTION
  - DRUM w/ WARNING LIGHT
- WORK ZONE DESIGN SPEED = 35 MPH

RECOMMENDED FOR APPROVAL \_\_\_\_\_  
 DESIGN ENGINEER DATE

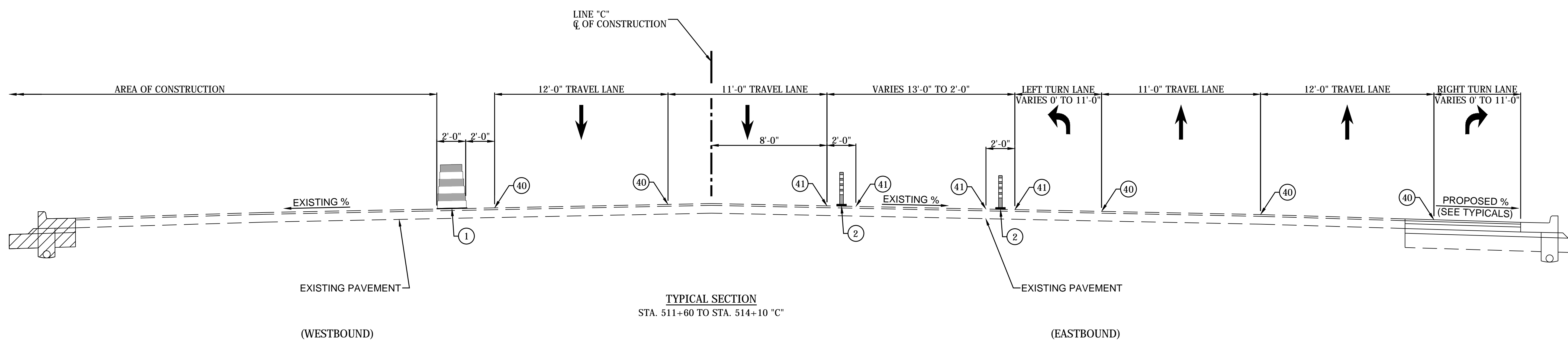
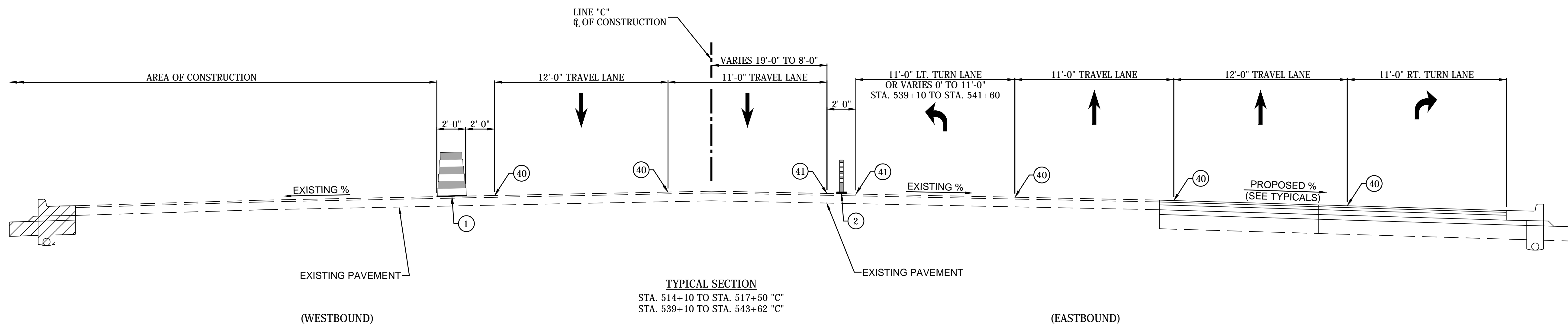
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 CHECKED: L.L.C. CHECKED: L.L.C.

INDIANA  
DEPARTMENT OF TRANSPORTATION

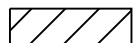
MAINTENANCE OF TRAFFIC  
PHASE 2

HORIZONTAL SCALE	BRIDGE FILE
1" = 30'	N/A
VERTICAL SCALE	DESIGNATION
N/A	1601072
SURVEY BOOK	SHEET
	18 of 96
CONTRACT	PROJECT
R-39814	1601072





WORK ZONE DESIGN SPEED = 35 MPH

LEGEND	
	AREA OF CONSTRUCTION
①	CONSTRUCTION DRUM
②	TUBULAR MARKER
④①	TEMP. PAVEMENT MARKINGS, SOLID WHITE, 4"
④②	TEMP. PAVEMENT MARKINGS, SOLID YELLOW 4"

RECOMMENDED FOR APPROVAL \_\_\_\_\_  
DESIGN ENGINEER DATE

DESIGNED: R.D.S. DRAWN: K.R.U.  
CHECKED: L.L.C. CHECKED: L.L.C.

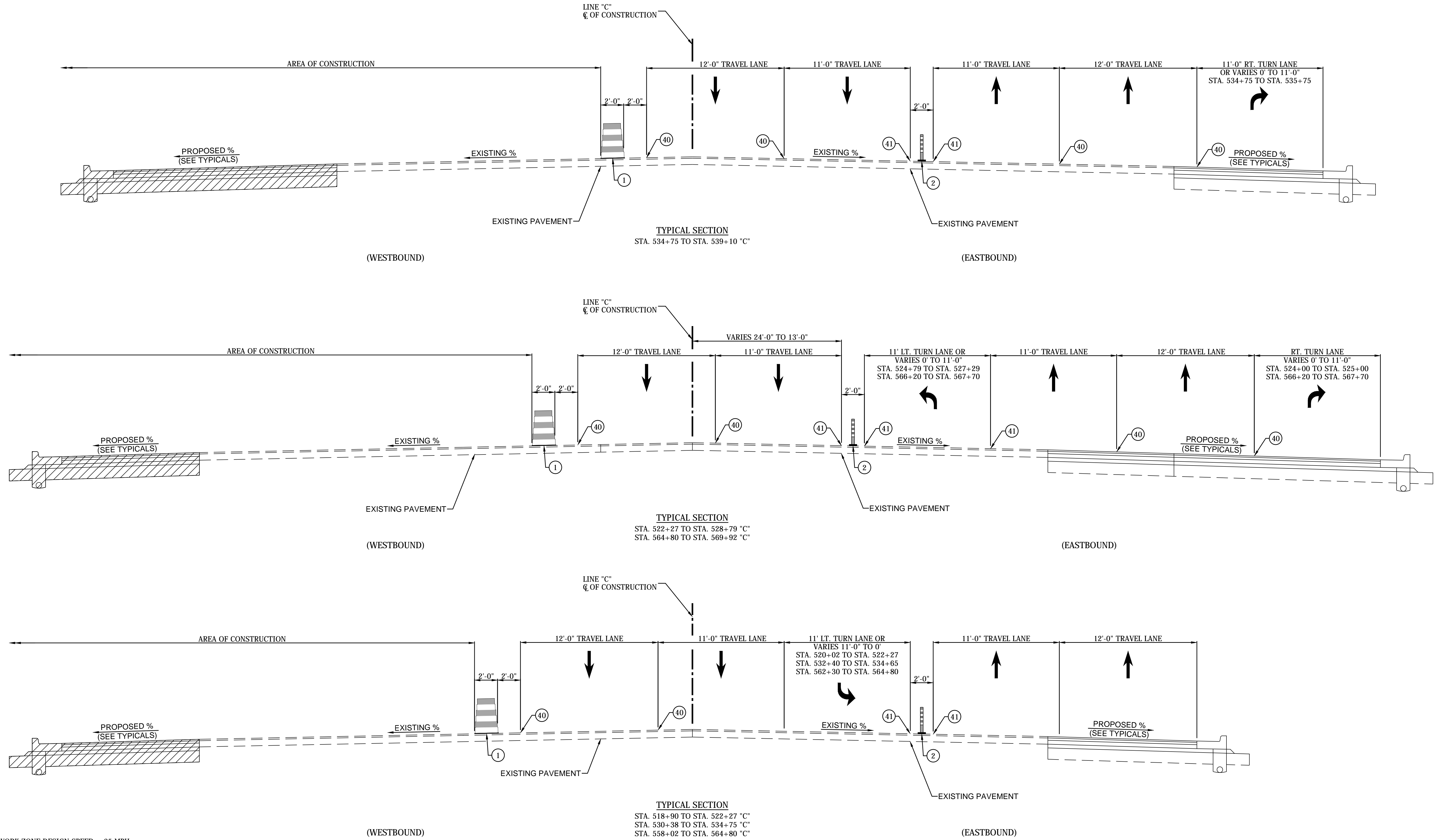
INDIANA  
DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC  
TYPICAL CROSS SECTIONS - PHASE 3

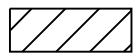




HORIZONTAL SCALE	BRIDGE FILE
1/4" = 1'	N/A
VERTICAL SCALE	DESIGNATION
1/4" = 1'	1601072
SURVEY BOOK	SHEET
	19 of 96
CONTRACT	PROJECT
R-39814	1601072

PRINT DATE: 3/20/19 PLOT SCALE: 1:1 EDIT DATE: 3/20/19 8:58 AM EDITED BY: KUPSHAW DRAWING FILE: P:\175-900-005-1 US96 ATLACAD\05 MOTOR\RD\_NOT\_TYP\_P18.DWG

PRINT DATE: 3/20/19  
 PLOT SCALE: 1:1  
 EDIT DATE: 2/20/19 - 8:58 AM  
 EDITED BY: KUSHAW  
 DRAWING FILE: P:\175-300-005-1 US96 ATLACAD\05 MOTORV.MOT\_TYP\_BILDING



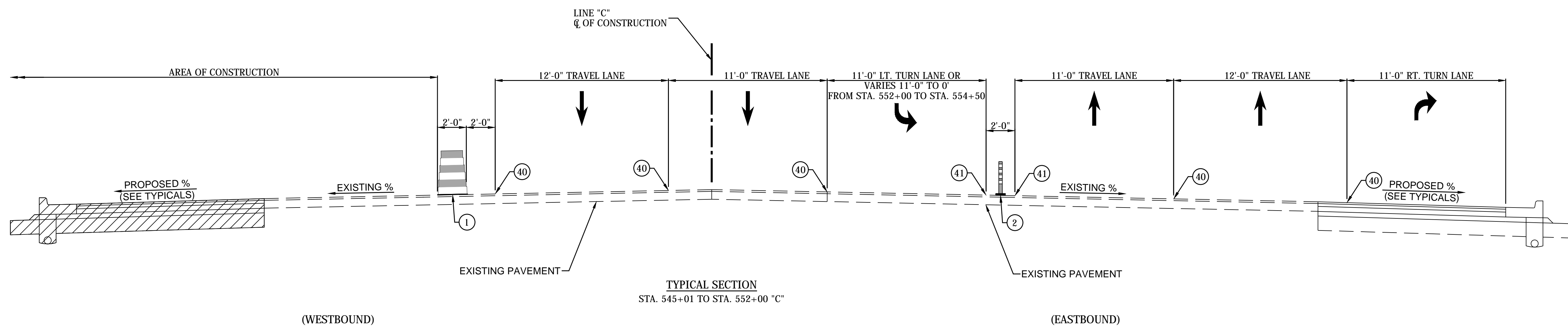
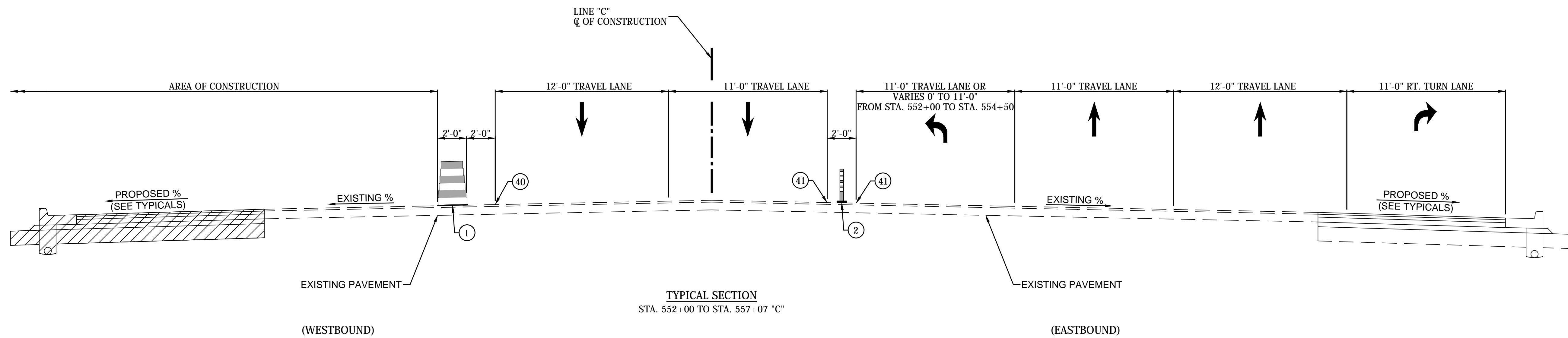
WORK ZONE DESIGN SPEED = 35 MPH

LEGEND	
	AREA OF CONSTRUCTION
	CONSTRUCTION DRUM
	TUBULAR MARKER
	TEMP. PAVEMENT MARKINGS, SOLID WHITE, 4"
	TEMP. PAVEMENT MARKINGS, SOLID YELLOW 4"

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: R.D.S.	DRAWN: K.R.U.	
CHECKED: L.L.C.	CHECKED: L.L.C.	

**INDIANA DEPARTMENT OF TRANSPORTATION**  
**MAINTENANCE OF TRAFFIC**  
**TYPICAL CROSS SECTIONS - PHASE 3**

HORIZONTAL SCALE	BRIDGE FILE
1/4" = 1'	N/A
VERTICAL SCALE	DESIGNATION
1/4" = 1'	1601072
SURVEY BOOK	SHEET
	20 of 96
CONTRACT	PROJECT
R-39814	1601072



WORK ZONE DESIGN SPEED = 35 MPH

LEGEND	
	AREA OF CONSTRUCTION
①	CONSTRUCTION DRUM
②	TUBULAR MARKER
④①	TEMP. PAVEMENT MARKINGS, SOLID WHITE, 4"
④②	TEMP. PAVEMENT MARKINGS, SOLID YELLOW 4"

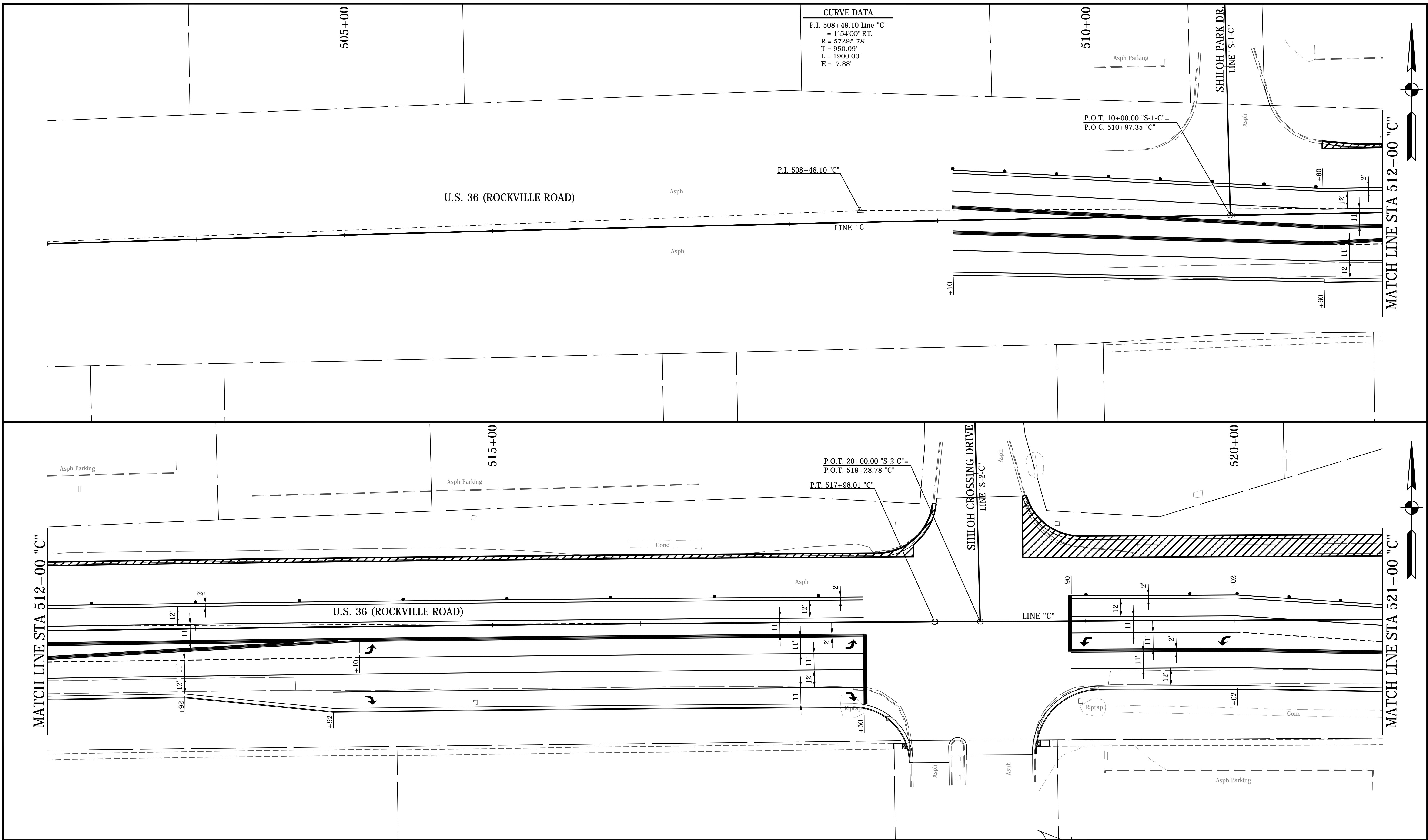
RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: R.D.S.	DRAWN: K.R.U.	
CHECKED: L.L.C.	CHECKED: L.L.C.	

INDIANA DEPARTMENT OF TRANSPORTATION	
MAINTENANCE OF TRAFFIC TYPICAL CROSS SECTIONS - PHASE 3	

HORIZONTAL SCALE	BRIDGE FILE
1/4" = 1'	N/A
VERTICAL SCALE	DESIGNATION
1/4" = 1'	1601072
SURVEY BOOK	SHEET
	21 of 96
CONTRACT	PROJECT
R-39814	1601072

PRINT DATE: 3/20/19  
 PLOT SCALE: 1:1  
 EDIT DATE: 3/20/19 - 8:58 AM  
 EDITED BY: KUPSHAW  
 DRAWING FILE: P:\175-900-005-1 US96 ATLACAD\05 MOTORRD\_NOT\_TYP\_BILD.DWG

PRINT DATE: 3/20/19  
 PLOT SCALE: 1:1  
 EDIT DATE: 7/12/11 - 9:35 AM  
 EDITED BY: MISELLS  
 DRAWING FILE: P:\175-500-003 - US36 ATLACAD\05 MOTTRD\_MOTPH3\_C\_30\_01.DWG



**CURVE DATA**  
 P.I. 508+48.10 Line "C"  
 = 1°54'00" RT.  
 R = 57295.78'  
 T = 950.09'  
 L = 1900.00'  
 E = 7.88'

P.O.T. 20+00.00 "S-2-C"=  
 P.O.T. 518+28.78 "C"  
 P.T. 517+98.01 "C"

**LEGEND**

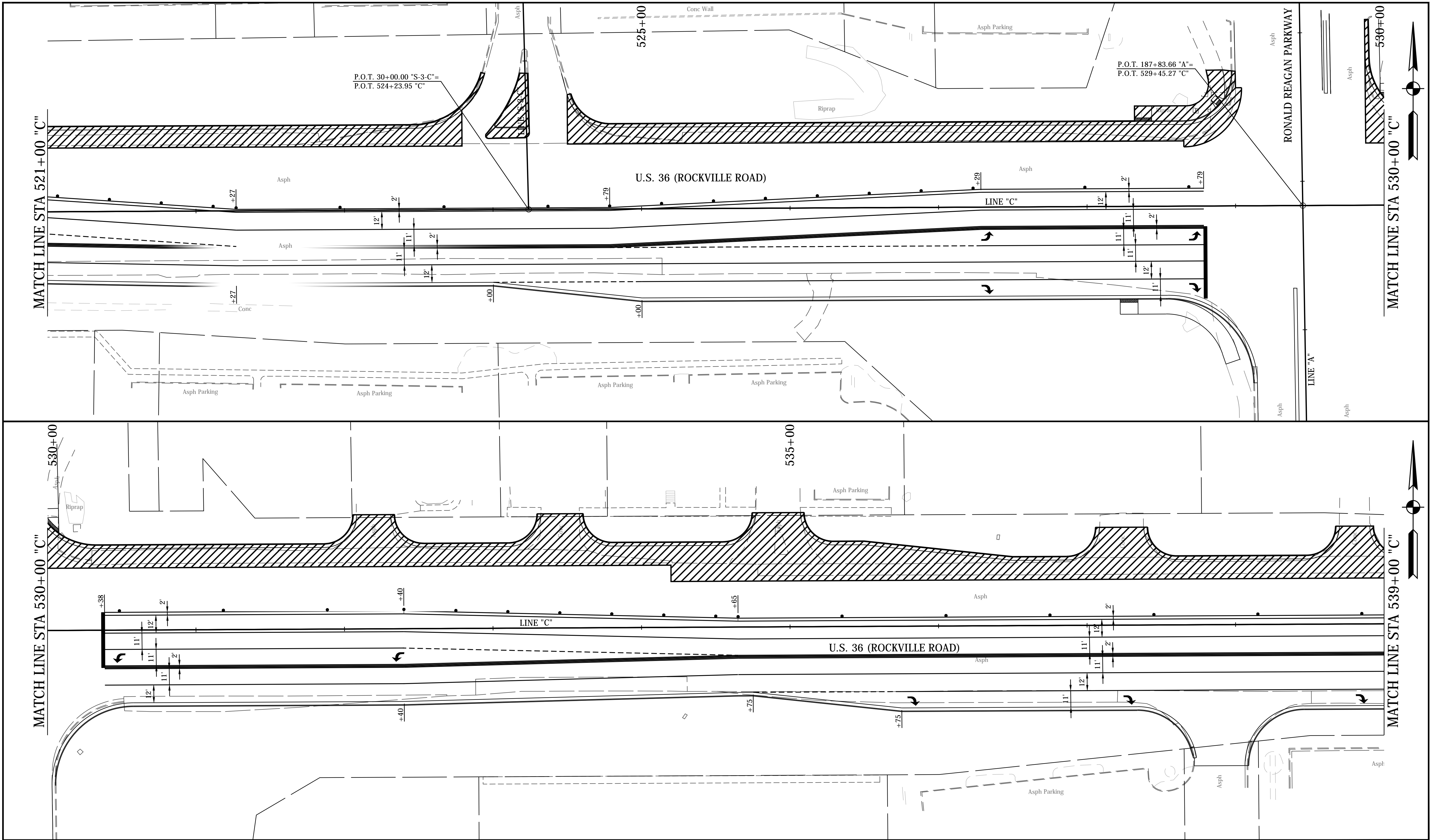
- AREA OF CONSTRUCTION
  - DRUM w/ WARNING LIGHT
- WORK ZONE DESIGN SPEED = 35 MPH

RECOMMENDED FOR APPROVAL		DESIGN ENGINEER	DATE
DESIGNED: R.D.S.	DRAWN: M.S.S.		
CHECKED: L.L.C.	CHECKED: L.L.C.		

**INDIANA**  
**DEPARTMENT OF TRANSPORTATION**  
  
**MAINTENANCE OF TRAFFIC**  
**PHASE 3**

HORIZONTAL SCALE	BRIDGE FILE
1" = 30'	N/A
VERTICAL SCALE	DESIGNATION
N/A	1601072
SURVEY BOOK	SHEET
	22 of 96
CONTRACT	PROJECT
R-39814	1601072

PRINT DATE: 3/20/19  
 PLOT SCALE: 1:1 EDIT DATE: 7/12/11 - 9:35 AM EDITED BY: MISELLS DRAWING FILE: P:\175-500-003 - US36 ATLACAD\05 MOTTRD\_MOTPH3\_C\_30\_01.DWG



**LEGEND**

- AREA OF CONSTRUCTION
  - DRUM w/ WARNING LIGHT
- WORK ZONE DESIGN SPEED = 35 MPH

RECOMMENDED FOR APPROVAL \_\_\_\_\_  
 DESIGN ENGINEER DATE

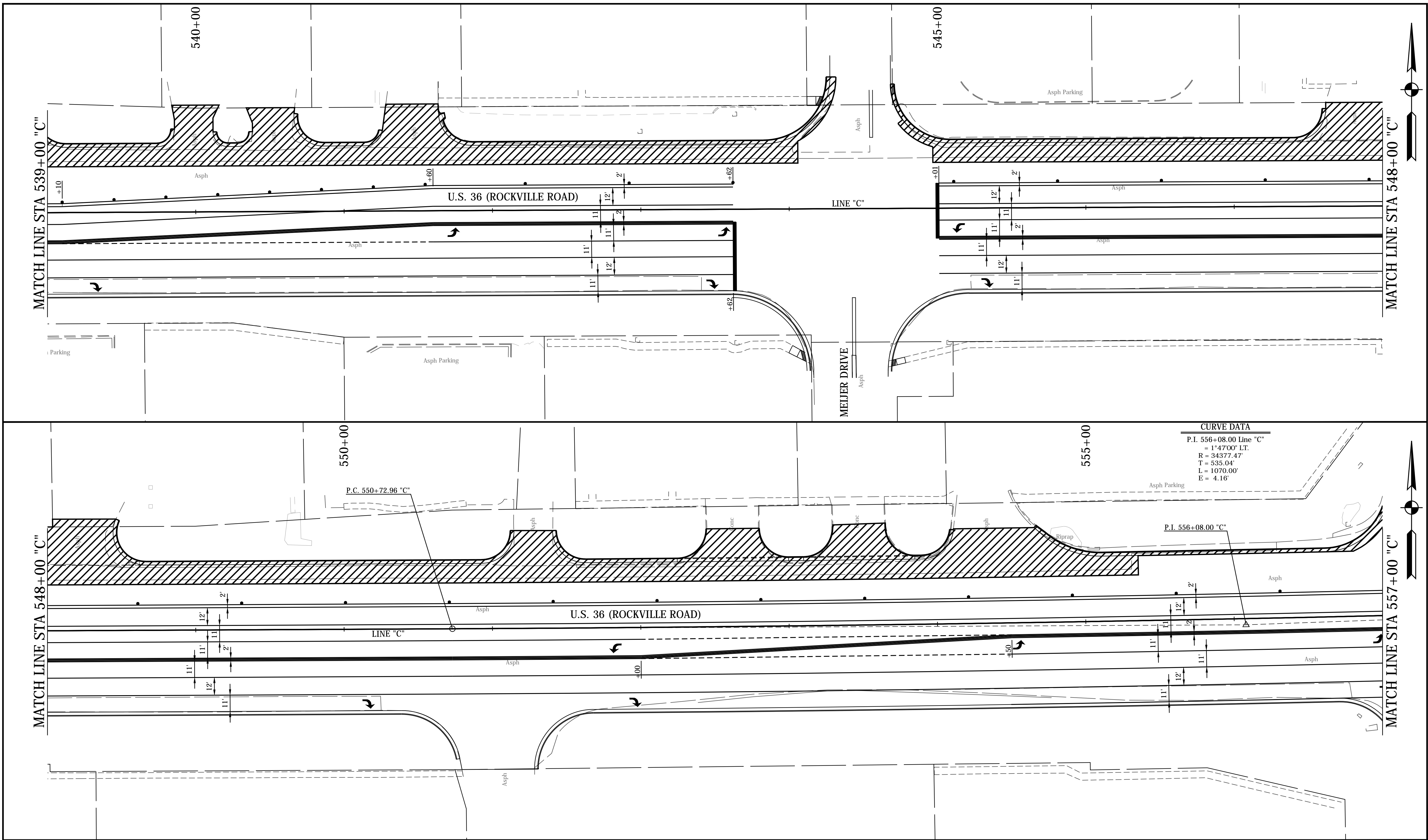
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 CHECKED: L.L.C. CHECKED: L.L.C.

INDIANA  
 DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC  
 PHASE 3

HORIZONTAL SCALE	BRIDGE FILE
1" = 30'	N/A
VERTICAL SCALE	DESIGNATION
N/A	1601072
SURVEY BOOK	SHEET
	23 of 96
CONTRACT	PROJECT
R-39814	1601072

PRINT DATE: 3/20/19  
 PLOT SCALE: 1:1  
 EDIT DATE: 7/12/11 - 9:35 AM  
 EDITED BY: MISELS  
 DRAWING FILE: P:\175-500-003\15536 ATLA CAD\05 MOTTRD\_MOTTRD\_PHE\_C\_30\_01.DWG



**CURVE DATA**

P.I. 556+08.00 Line "C"
= 1°47'00" LT.
R = 34377.47'
T = 535.04'
L = 1070.00'
E = 4.16'

**LEGEND**

- AREA OF CONSTRUCTION
  - DRUM w/ WARNING LIGHT
- WORK ZONE DESIGN SPEED = 35 MPH

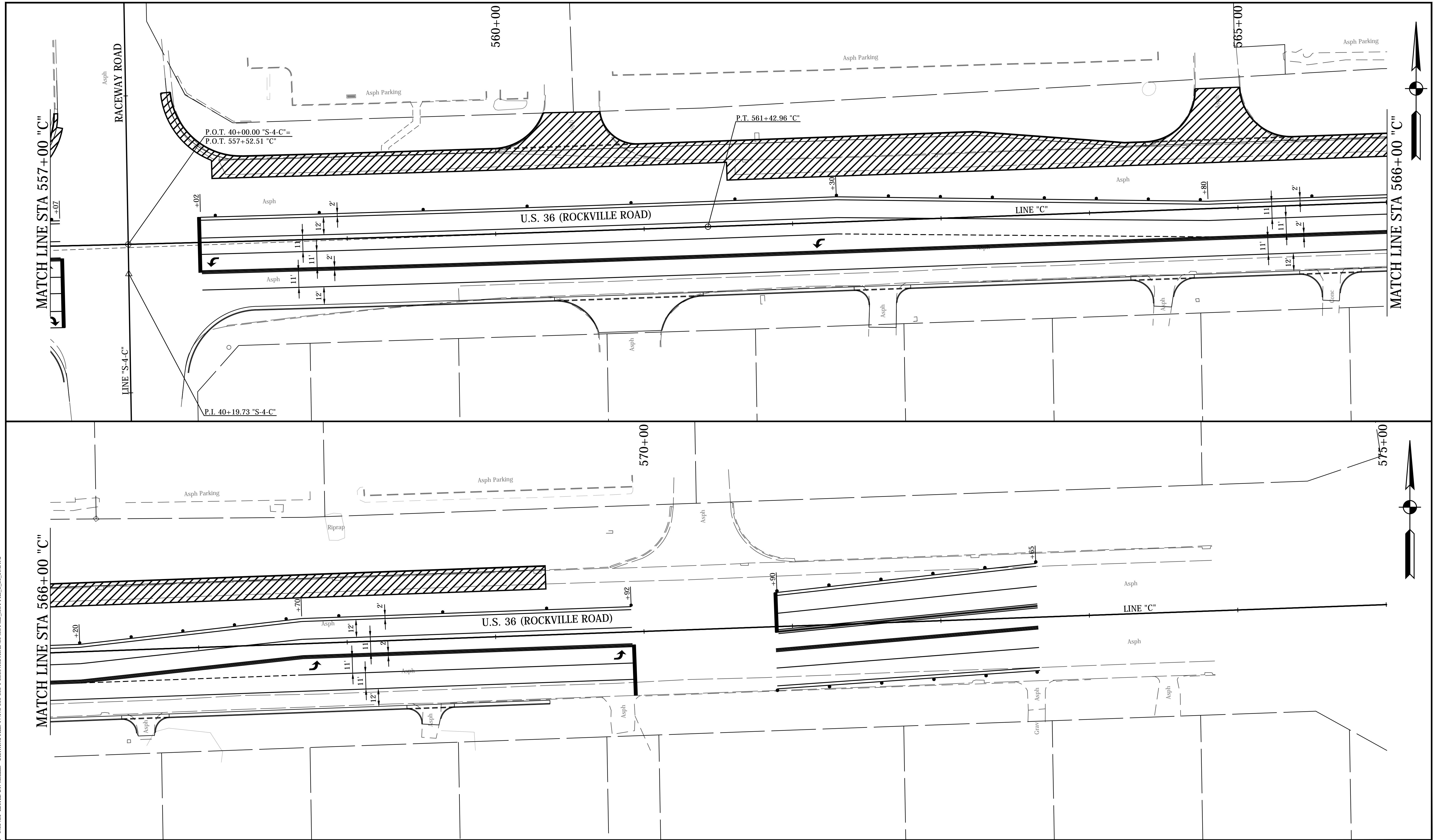
RECOMMENDED FOR APPROVAL		DESIGN ENGINEER	DATE
DESIGNED: <u>R.D.S.</u>	DRAWN: <u>M.S.S.</u>		
CHECKED: <u>L.L.C.</u>	CHECKED: <u>L.L.C.</u>		

**INDIANA**  
**DEPARTMENT OF TRANSPORTATION**  
  
**MAINTENANCE OF TRAFFIC**  
**PHASE 3**

HORIZONTAL SCALE	BRIDGE FILE
1" = 30'	N/A
VERTICAL SCALE	DESIGNATION
N/A	1601072
SURVEY BOOK	SHEET
	24 of 96
CONTRACT	PROJECT
R-39814	1601072



PRINT DATE: 3/20/19  
 PLOT SCALE: 1:1 EDIT DATE: 7/12/11 - 9:35 AM EDITED BY: MISELLS DRAWING FILE: P:\75-500-003 - US36 ATLACAD\05 MOTR\RD\_MOTR\FIE\_C\_39\_01.DWG



**LEGEND**

- AREA OF CONSTRUCTION
- DRUM w/ WARNING LIGHT

WORK ZONE DESIGN SPEED = 35 MPH

RECOMMENDED FOR APPROVAL \_\_\_\_\_  
 DESIGN ENGINEER DATE

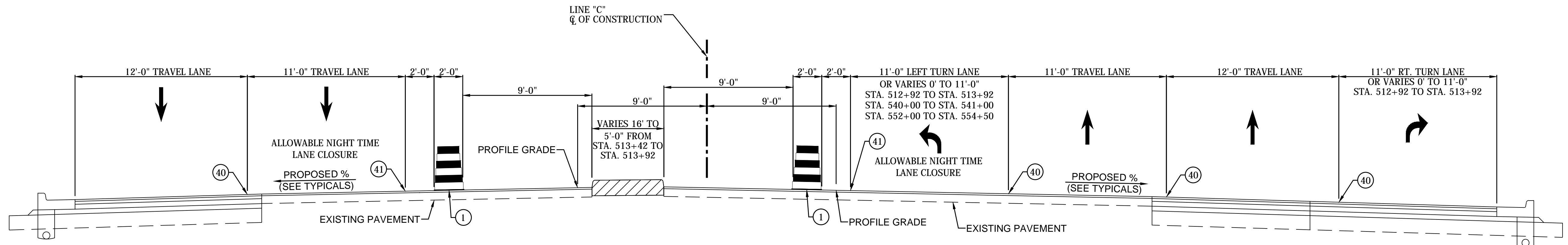
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INDIANA DEPARTMENT OF TRANSPORTATION

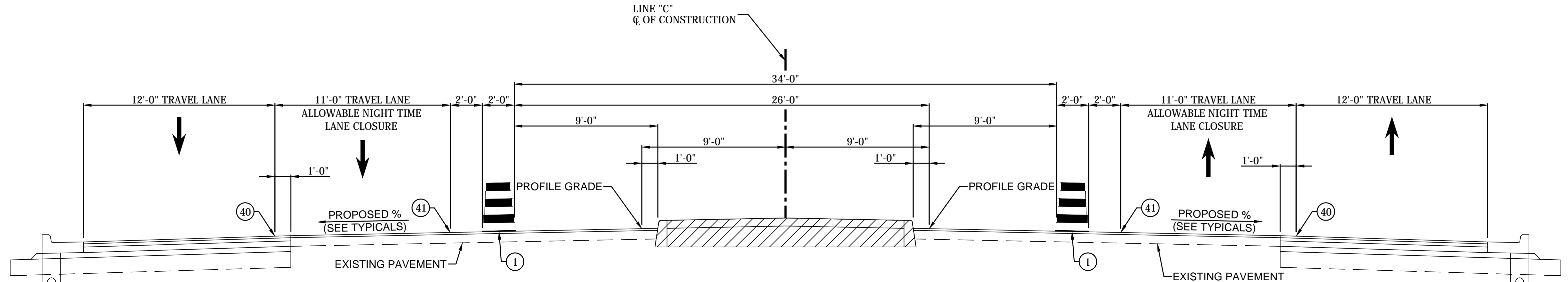
MAINTENANCE OF TRAFFIC PHASE 3

HORIZONTAL SCALE	BRIDGE FILE
1" = 30'	N/A
VERTICAL SCALE	DESIGNATION
N/A	1601072
SURVEY BOOK	SHEET
	25 of 96
CONTRACT	PROJECT
R-39814	1601072





**TYPICAL SECTION**  
 STA. 512+92 TO STA. 517+72 "C"  
 STA. 540+50 TO STA. 543+62 "C"  
 STA. 549+30 TO STA. 556+95 "C"



**TYPICAL SECTION**  
 STA. 511+60 TO STA. 512+92 "C"

PRINT DATE: 3/20/19  
 PLOT SCALE: 1:1  
 EDIT DATE: 3/20/19 - 9:03 AM  
 EDITED BY: KUPSHAW  
 DRAWING FILE: P:\175 300-005-1 US96 ATLACAD\05 MOTOR\RD\_NOT\_TYP\_P4L.DWG

WORK ZONE DESIGN SPEED = 35 MPH

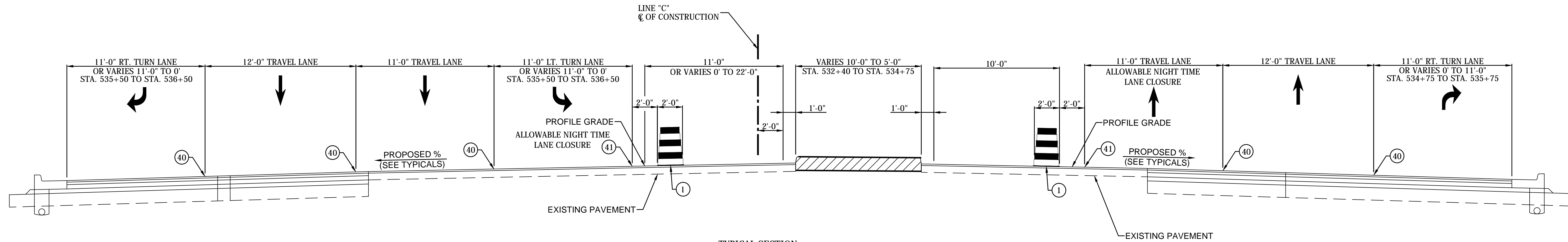
LEGEND	
	AREA OF CONSTRUCTION
	CONSTRUCTION DRUM
	TUBULAR MARKER
	TEMP. PAVEMENT MARKINGS, SOLID WHITE, 4"
	TEMP. PAVEMENT MARKINGS, SOLID YELLOW 4"

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: R.D.S.	DRAWN: K.R.U.	
CHECKED: L.L.C.	CHECKED: L.L.C.	

**INDIANA**  
**DEPARTMENT OF TRANSPORTATION**

**MAINTENANCE OF TRAFFIC**  
**TYPICAL CROSS SECTIONS - PHASE 4**

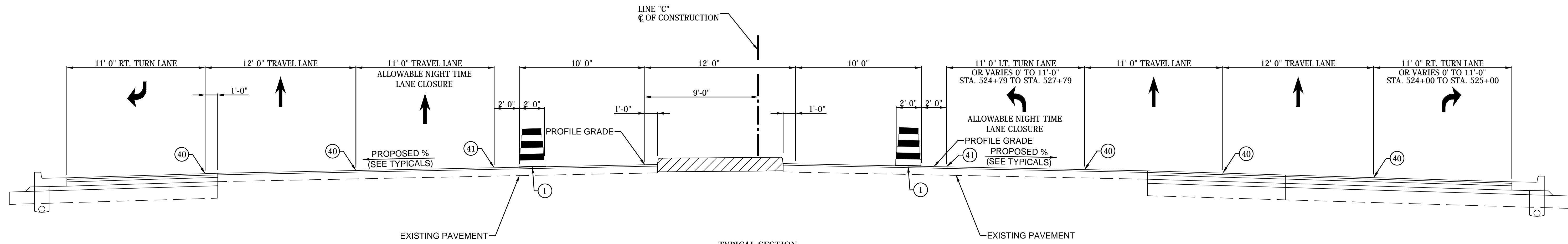
HORIZONTAL SCALE	BRIDGE FILE
1/4" = 1'	N/A
VERTICAL SCALE	DESIGNATION
1/4" = 1'	1601072
SURVEY BOOK	SHEET
	26 of 96
CONTRACT	PROJECT
R-39814	1601072



TYPICAL SECTION  
STA. 530+40 TO STA. 536+50 "C"

(WESTBOUND)

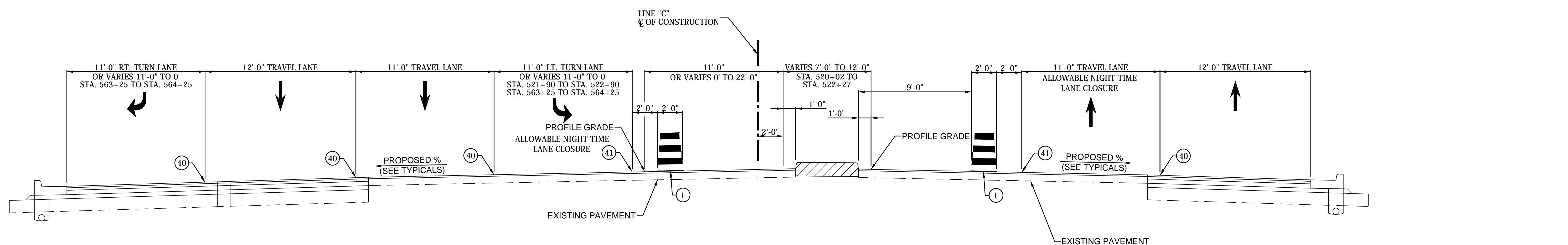
(EASTBOUND)



TYPICAL SECTION  
STA. 523+20 TO STA. 528+79 "C"

(WESTBOUND)

(EASTBOUND)



TYPICAL SECTION  
STA. 518+90 TO STA. 523+20 "C"  
STA. 557+96 TO STA. 567+80 "C"

(WESTBOUND)

(EASTBOUND)

WORK ZONE DESIGN SPEED = 35 MPH

LEGEND	
	AREA OF CONSTRUCTION
	CONSTRUCTION DRUM
	TUBULAR MARKER
	TEMP. PAVEMENT MARKINGS, SOLID WHITE, 4"
	TEMP. PAVEMENT MARKINGS, SOLID YELLOW 4"

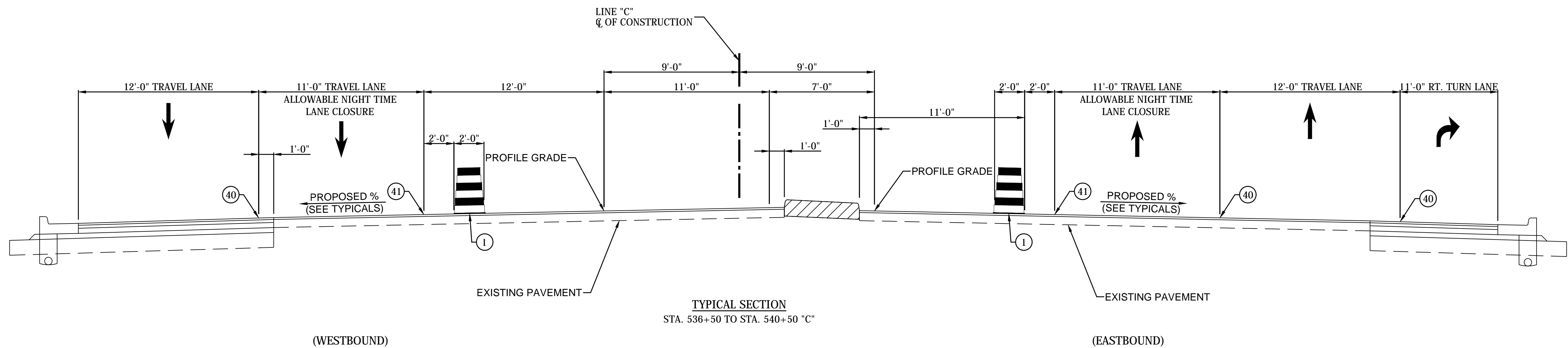
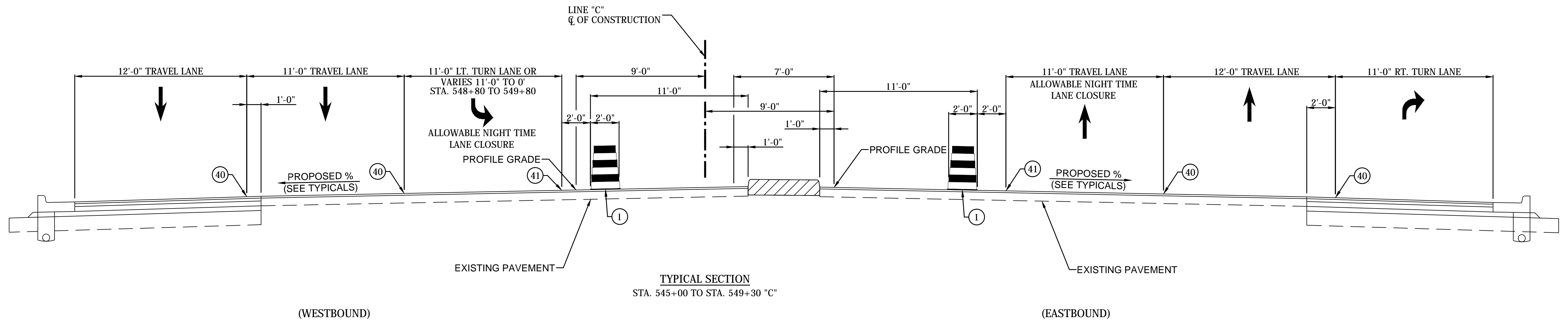
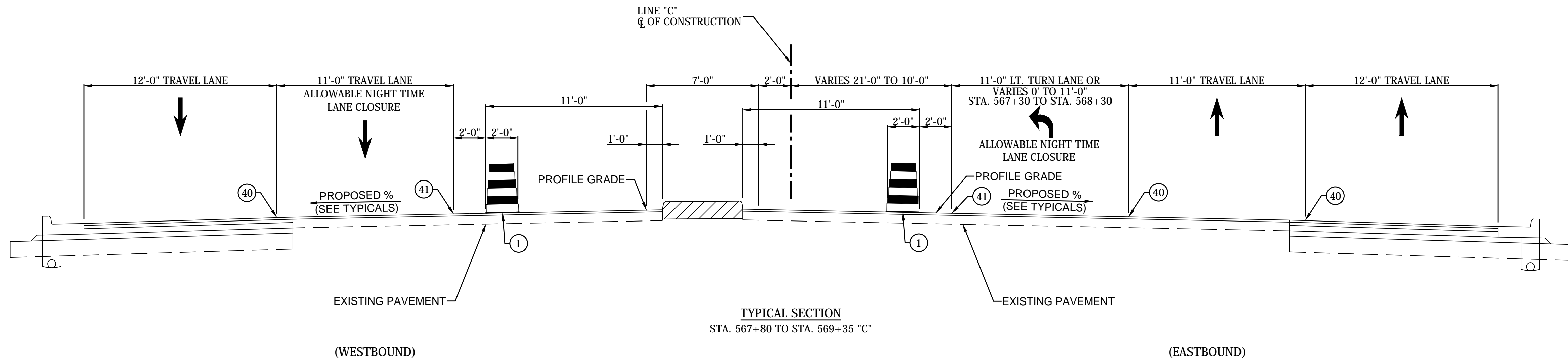
RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: R.D.S.	DRAWN: K.R.U.	
CHECKED: L.L.C.	CHECKED: L.L.C.	

**INDIANA DEPARTMENT OF TRANSPORTATION**

**MAINTENANCE OF TRAFFIC TYPICAL CROSS SECTIONS - PHASE 4**

HORIZONTAL SCALE	BRIDGE FILE
1/4" = 1'	N/A
VERTICAL SCALE	DESIGNATION
1/4" = 1'	1601072
SURVEY BOOK	SHEET
	27 of 96
CONTRACT	PROJECT
R-39814	1601072

PRINT DATE: 3/20/19 PLOT SCALE: 1:1 EDIT DATE: 3/20/19 - 4:06 PM DRAWING FILE: P:\175-500-005-1 US96-ATT-ACAD\05 MOTORV-MOT-TYP\_BH4.DWG



WORK ZONE DESIGN SPEED = 35 MPH

LEGEND	
	AREA OF CONSTRUCTION
	CONSTRUCTION DRUM
	TUBULAR MARKER
	TEMP. PAVEMENT MARKINGS, SOLID WHITE, 4"
	TEMP. PAVEMENT MARKINGS, SOLID YELLOW 4"

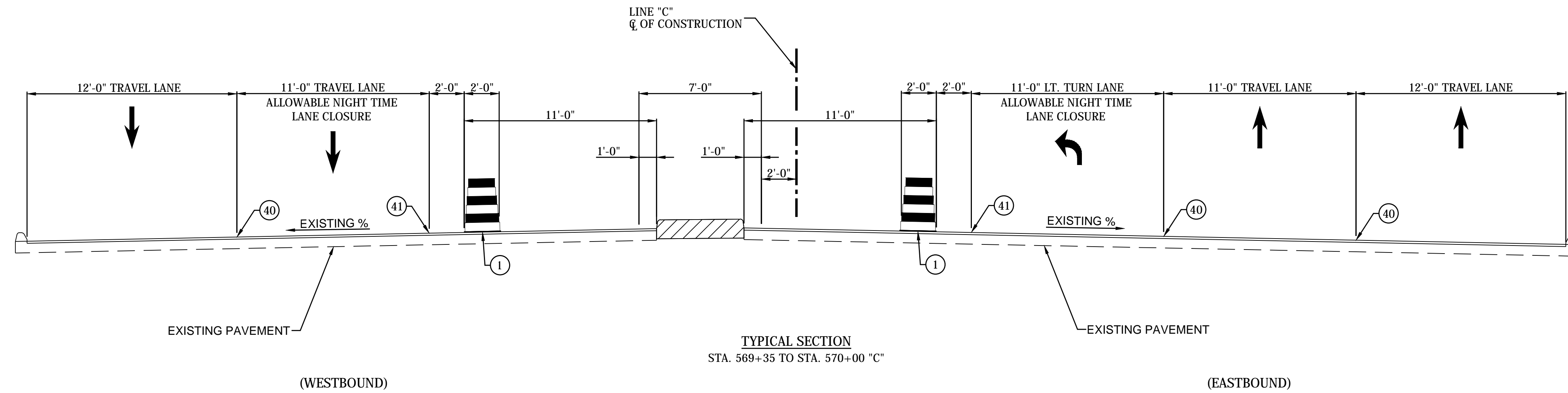
RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: R.D.S.	DRAWN: K.R.U.	
CHECKED: L.L.C.	CHECKED: L.L.C.	

INDIANA  
DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC  
TYPICAL CROSS SECTIONS - PHASE 4

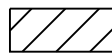




HORIZONTAL SCALE	BRIDGE FILE
1/4" = 1'	N/A
VERTICAL SCALE	DESIGNATION
1/4" = 1'	1601072
SURVEY BOOK	SHEET
	28 of 96
CONTRACT	PROJECT
R-39814	1601072

PRINT DATE: 3/20/19  
 PLOT SCALE: 1:1  
 EDIT DATE: 3/20/19 - 4:06 PM  
 EDITED BY: KUPSIKAW  
 DRAWING FILE: P:\175-500-005-1 US36-ATI-MCD-015 MOT-TRD\_MOT\_TYP\_P44.DWG



TYPICAL SECTION  
 STA. 569+35 TO STA. 570+00 "C"

WORK ZONE DESIGN SPEED = 35 MPH

LEGEND	
	AREA OF CONSTRUCTION
	CONSTRUCTION DRUM
	TUBULAR MARKER
	TEMP. PAVEMENT MARKINGS, SOLID WHITE, 4"
	TEMP. PAVEMENT MARKINGS, SOLID YELLOW 4"

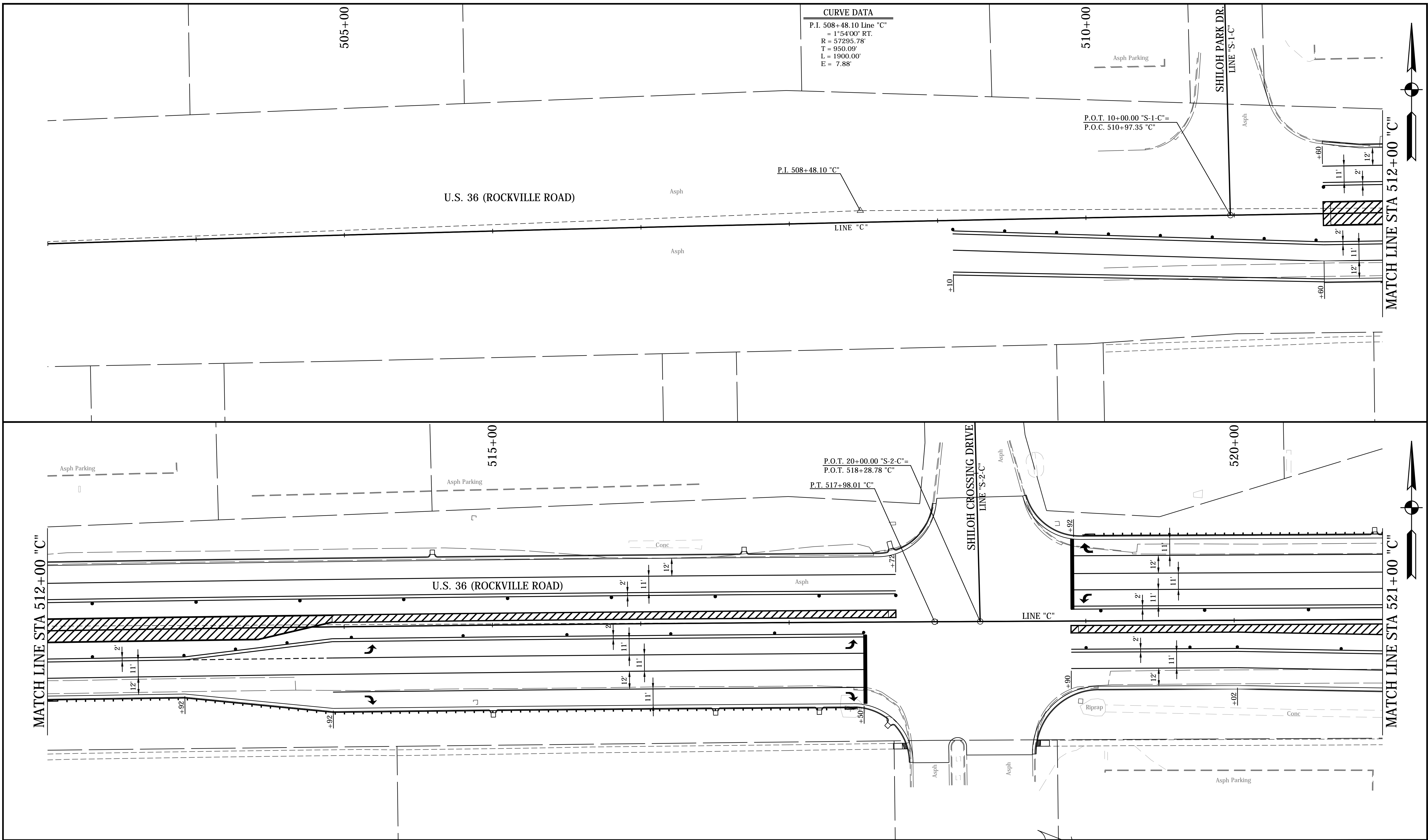
RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: R.D.S.	DRAWN: K.R.U.	
CHECKED: L.L.C.	CHECKED: L.L.C.	

<b>INDIANA</b> <b>DEPARTMENT OF TRANSPORTATION</b>	
<b>MAINTENANCE OF TRAFFIC</b> <b>TYPICAL CROSS SECTIONS - PHASE 4</b>	

HORIZONTAL SCALE	BRIDGE FILE
1/4" = 1'	N/A
VERTICAL SCALE	DESIGNATION
1/4" = 1'	1601072
SURVEY BOOK	SHEET
	29 of 96
CONTRACT	PROJECT
R-39814	1601072



PRINT DATE: 3/20/19  
 PLOT SCALE: 1:1  
 EDIT DATE: 7/12/11 - 2:17 PM  
 EDITED BY: KUPSHAW  
 DRAWING FILE: P:\15-500-003-1 US36 ATJAC\05 NOTRD\_MOT PHL\_C\_30\_01.DWG



**LEGEND**

- AREA OF CONSTRUCTION
  - DRUM w/ WARNING LIGHT
- WORK ZONE DESIGN SPEED = 35 MPH

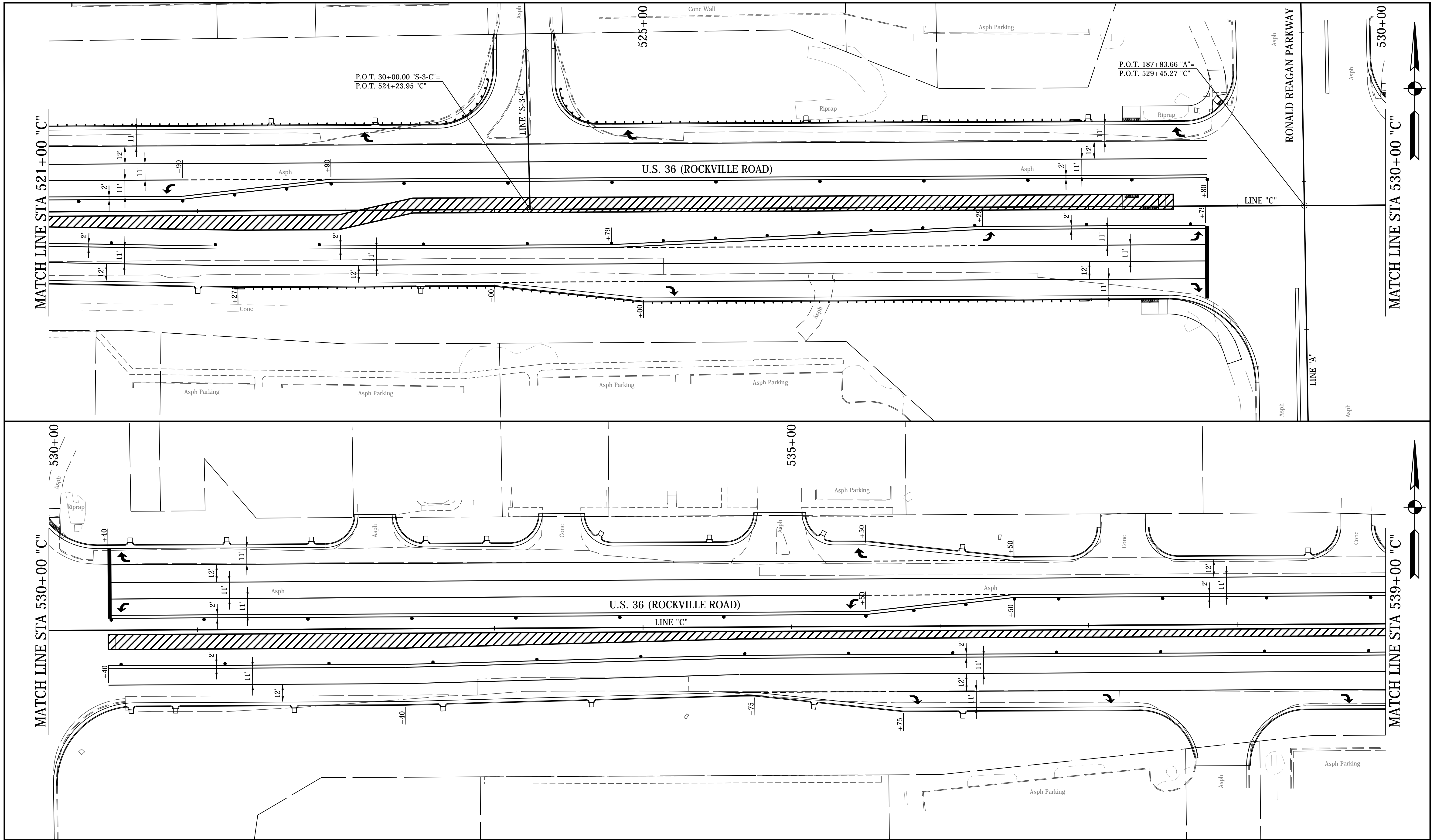
RECOMMENDED FOR APPROVAL		DESIGN ENGINEER	DATE
DESIGNED: <u>R.D.S.</u>	DRAWN: <u>M.S.S.</u>		
CHECKED: <u>L.L.C.</u>	CHECKED: <u>L.L.C.</u>		

**INDIANA DEPARTMENT OF TRANSPORTATION**



**MAINTENANCE OF TRAFFIC PHASE 4**

HORIZONTAL SCALE	BRIDGE FILE
1" = 30'	N/A
VERTICAL SCALE	DESIGNATION
N/A	1601072
SURVEY BOOK	SHEET
	30 of 96
CONTRACT	PROJECT
R-39814	1601072

PRINT DATE: 3/20/19  
 PLOT SCALE: 1:1 EDIT DATE: 7/12/11 - 2:17 PM EDITED BY: KUPSHAW DRAWING FILE: P:\175-500-003-1 US36 ATTAC\0405 NOTRD\_MOT PHL\_C\_30\_01.DWG



**LEGEND**

-  AREA OF CONSTRUCTION
  -  DRUM w/ WARNING LIGHT
- WORK ZONE DESIGN SPEED = 35 MPH

RECOMMENDED FOR APPROVAL \_\_\_\_\_  
 DESIGN ENGINEER DATE

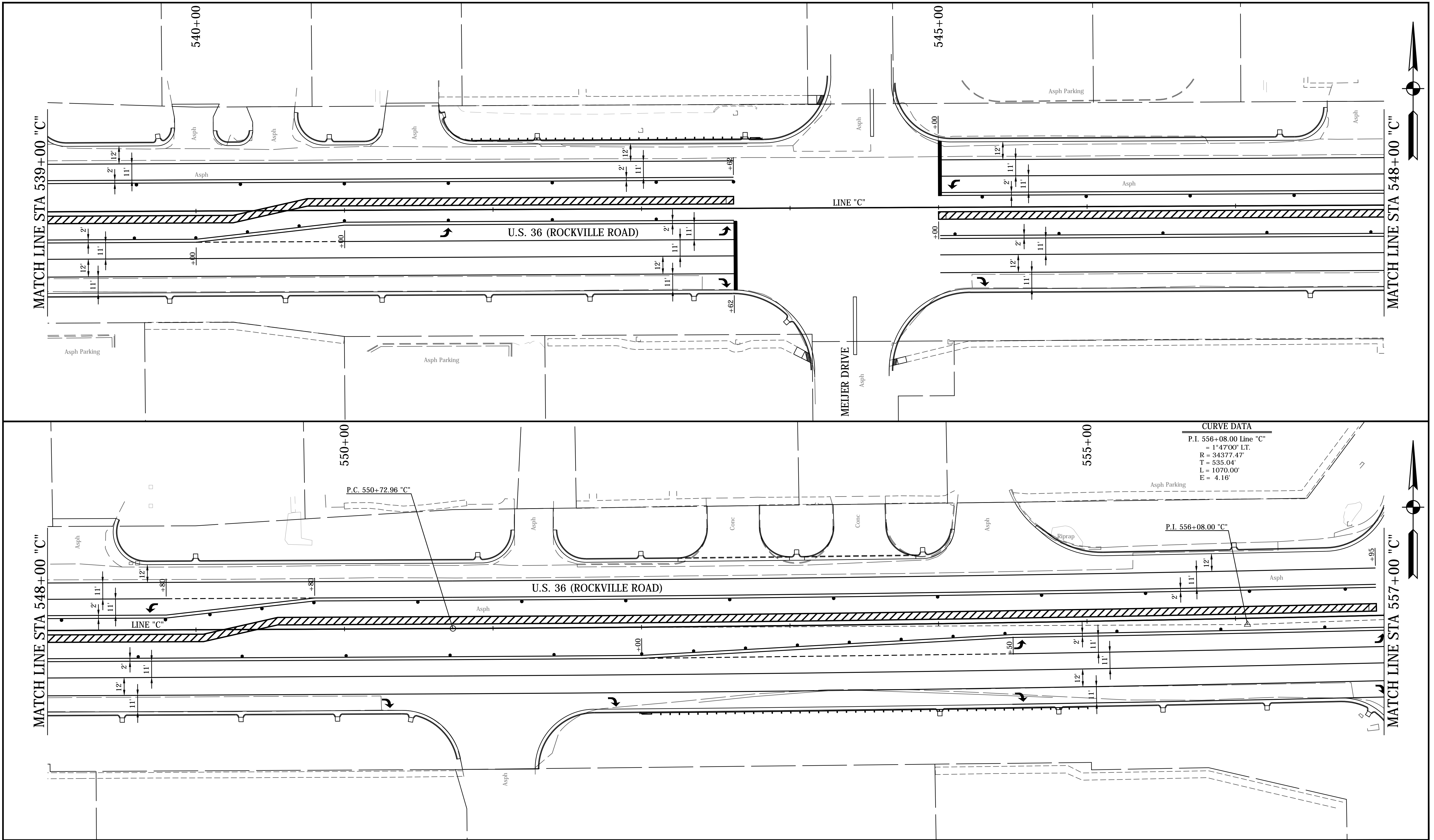
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 CHECKED: L.L.C. CHECKED: L.L.C.

INDIANA  
 DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC  
 PHASE 4

HORIZONTAL SCALE	BRIDGE FILE
1" = 30'	N/A
VERTICAL SCALE	DESIGNATION
N/A	1601072
SURVEY BOOK	SHEET
	31 of 96
CONTRACT	PROJECT
R-39814	1601072

PRINT DATE: 3/20/19  
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**CURVE DATA**

P.I. 556+08.00 Line "C"
= 1°47'00" LT.
R = 34377.47'
T = 535.04'
L = 1070.00'
E = 4.16'

**LEGEND**

- AREA OF CONSTRUCTION
  - DRUM w/ WARNING LIGHT
- WORK ZONE DESIGN SPEED = 35 MPH

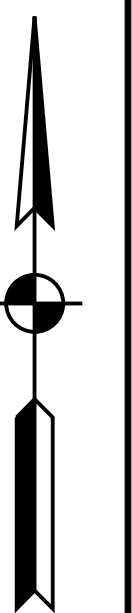
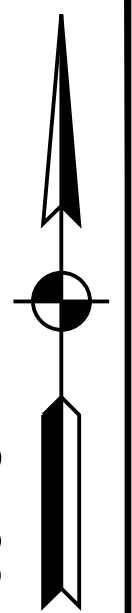
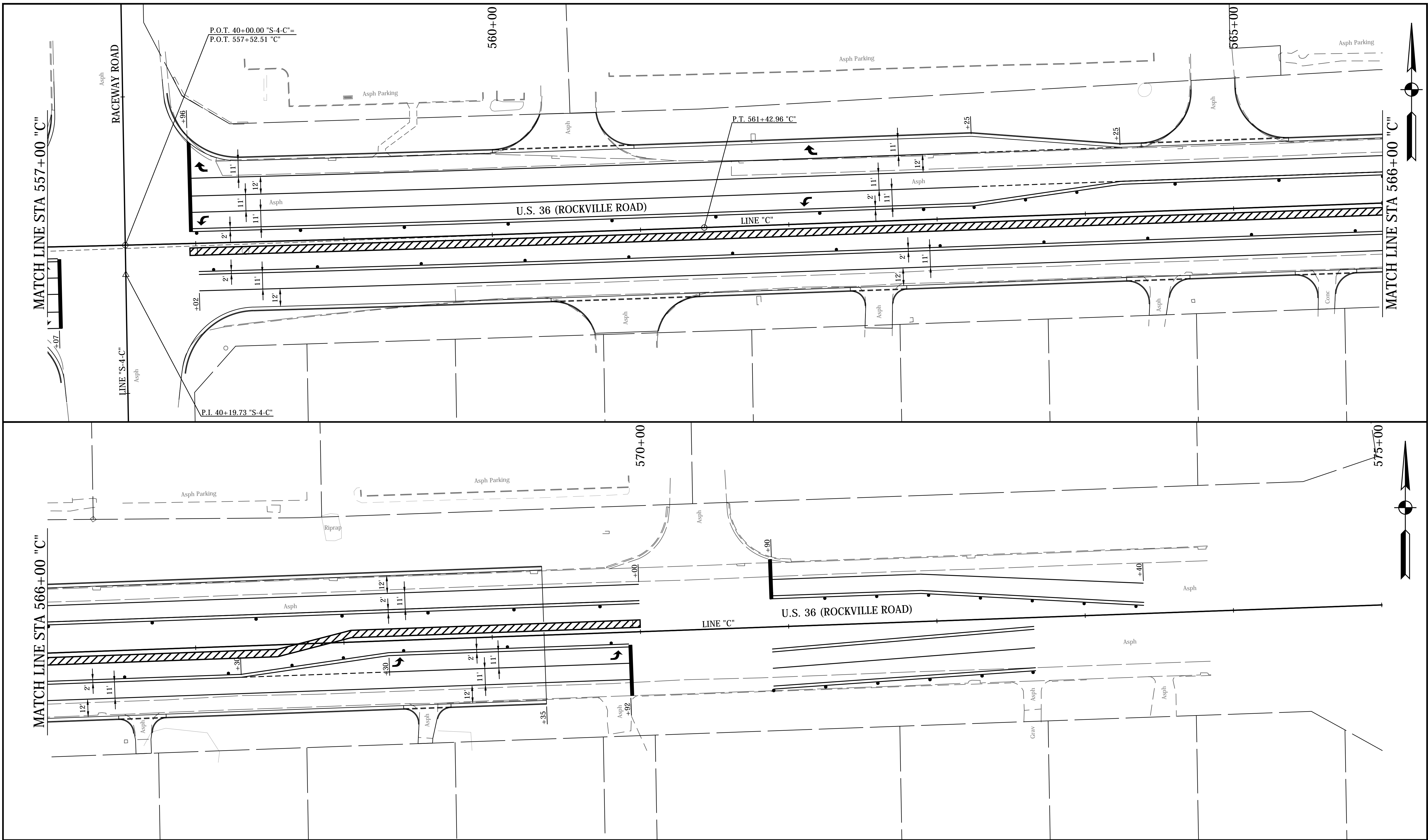
RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: R.D.S.	DRAWN: M.S.S.	
CHECKED: L.L.C.	CHECKED: L.L.C.	

**INDIANA DEPARTMENT OF TRANSPORTATION**

**MAINTENANCE OF TRAFFIC PHASE 4**

HORIZONTAL SCALE	BRIDGE FILE
1" = 30'	N/A
VERTICAL SCALE	DESIGNATION
N/A	1601072
SURVEY BOOK	SHEET
	32 of 96
CONTRACT	PROJECT
R-39814	1601072

PRINT DATE: 3/20/19  
 PLOT SCALE: 1:1  
 EDIT DATE: 7/12/11 - 2:17 PM  
 EDITED BY: KUPSHAW  
 DRAWING FILE: P:\175-500-003-1 US36 ATTAC\05 NOTRD\_MOT PHL\_C\_30\_01.DWG



**LEGEND**

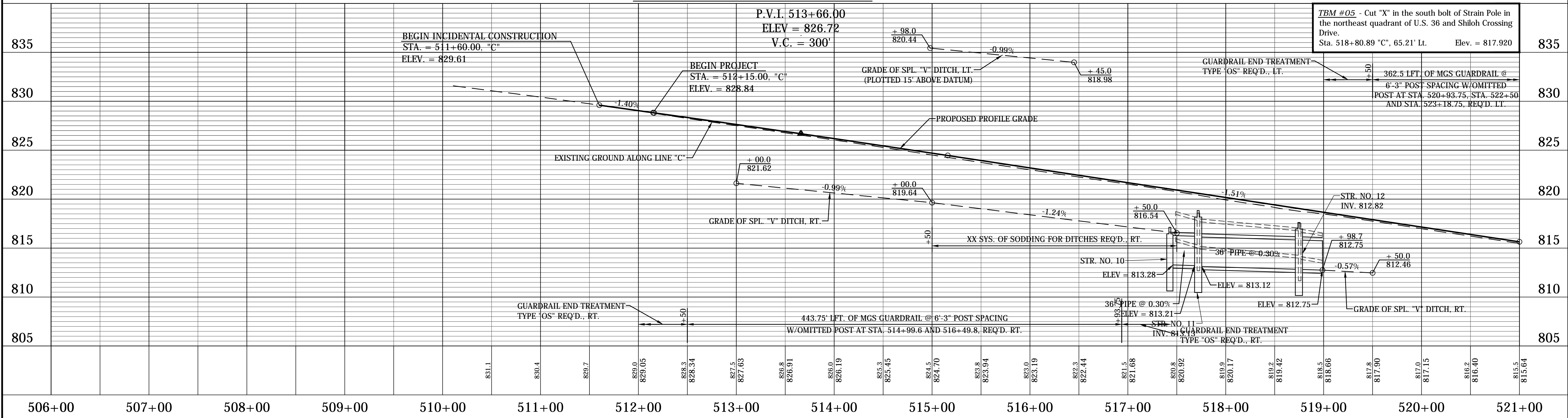
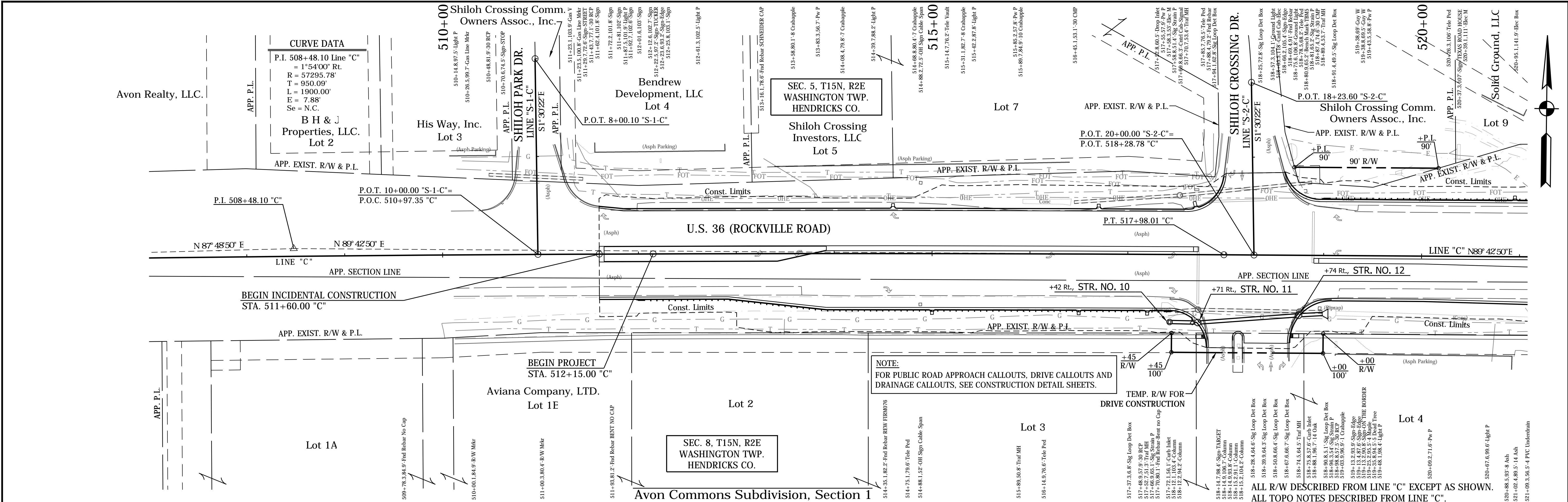
- AREA OF CONSTRUCTION
  - DRUM w/ WARNING LIGHT
- WORK ZONE DESIGN SPEED = 35 MPH

RECOMMENDED FOR APPROVAL		DESIGN ENGINEER	DATE
DESIGNED: <u>R.D.S.</u>	DRAWN: <u>M.S.S.</u>		
CHECKED: <u>L.L.C.</u>	CHECKED: <u>L.L.C.</u>		

**INDIANA**  
**DEPARTMENT OF TRANSPORTATION**  
  
**MAINTENANCE OF TRAFFIC**  
**PHASE 4**

HORIZONTAL SCALE	BRIDGE FILE
1" = 30'	N/A
VERTICAL SCALE	DESIGNATION
N/A	1601072
SURVEY BOOK	SHEET
	33 of 96
CONTRACT	PROJECT
R-39814	1601072





100	NORTH: 50167.7237 EAST: 47110.3281	117	NORTH: 50008.9257 EAST: 47379.6353	118	NORTH: 49991.1066 EAST: 46436.3862
516+59.78 "C", 83.36' Lt.	519+28.09 "C", 76.94' Rt.	509+83.54 "C", 84.27' Rt.			

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: Y.Z.	DRAWN: K.R.U.	
CHECKED: L.L.C.	CHECKED: L.L.C.	

**INDIANA DEPARTMENT OF TRANSPORTATION**

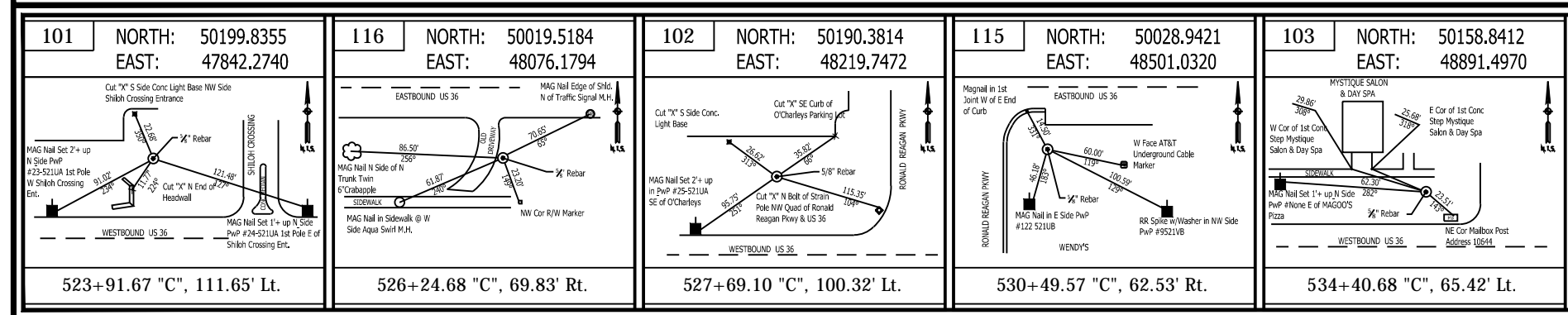
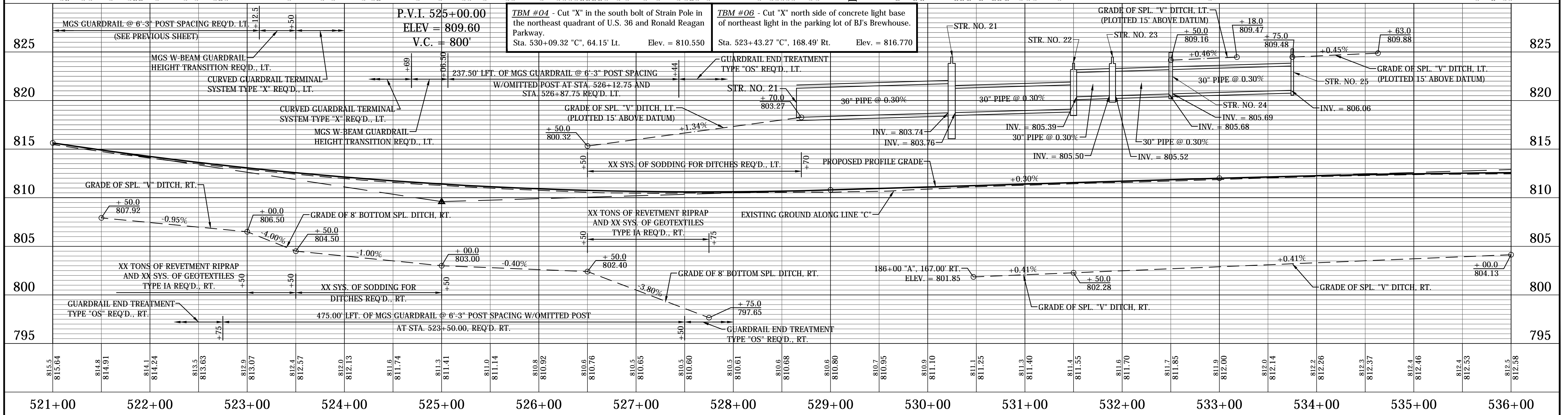
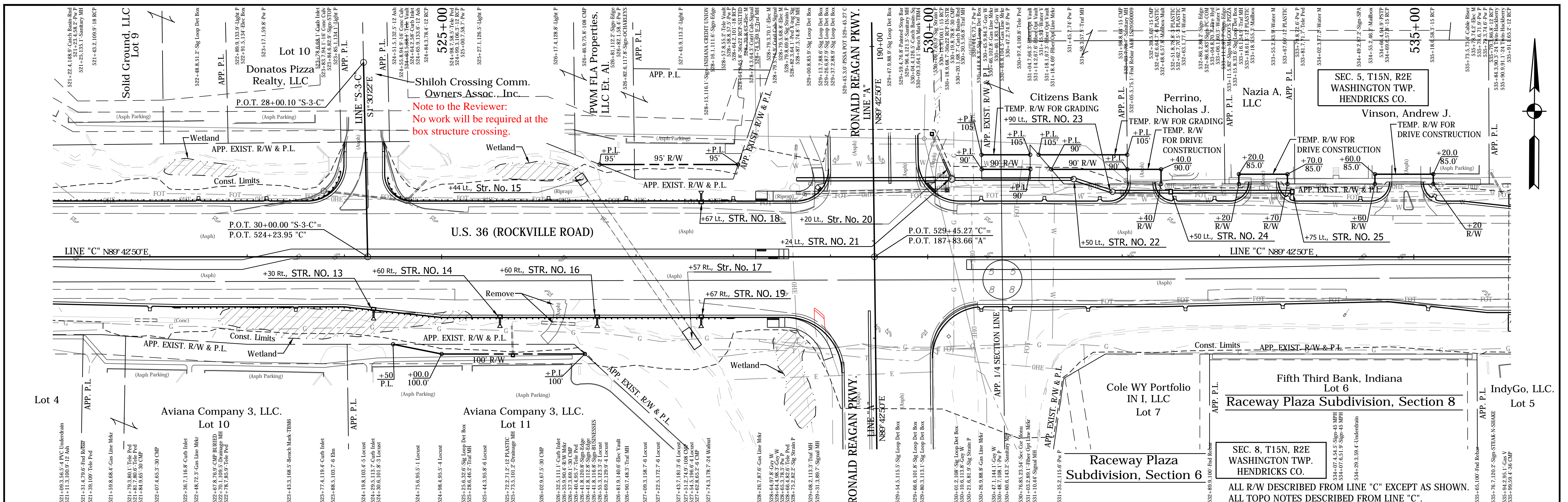
**PLAN AND PROFILE**

**STA. 508+00.00 TO 521+00.00 LINE "C"**

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VERTICAL SCALE	DESIGNATION
1" = 5'	1601072
SURVEY BOOK	SHEET
R-39814	34 of 96
CONTRACT	PROJECT
R-39814	1601072

PRINT DATE: 9/20/19  
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 DRAWING FILE: P:\75-500-003-1 US36 AT LACAD08 PLAN PROF.DWG  
 PLAN PROFILE SHEET: C\_50\_01.DWG  
 DESIGNED BY: KUPSHAW





101	NORTH: 50199.8355 EAST: 47842.2740	116	NORTH: 50019.5184 EAST: 48076.1794	102	NORTH: 50190.3814 EAST: 48219.7472	115	NORTH: 50028.9421 EAST: 48501.0320	103	NORTH: 50158.8412 EAST: 48891.4970
523+91.67 "C", 111.65' Lt.		526+24.68 "C", 69.83' Rt.		527+69.10 "C", 100.32' Lt.		530+49.57 "C", 62.53' Rt.		534+40.68 "C", 65.42' Lt.	

**INDIANA DEPARTMENT OF TRANSPORTATION**

**PLAN AND PROFILE**

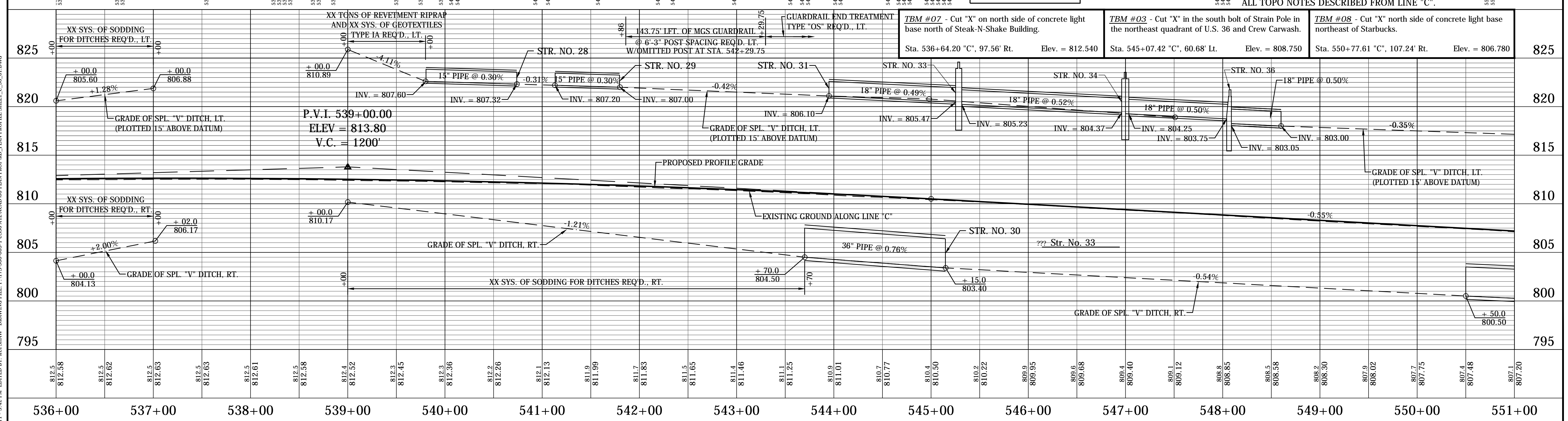
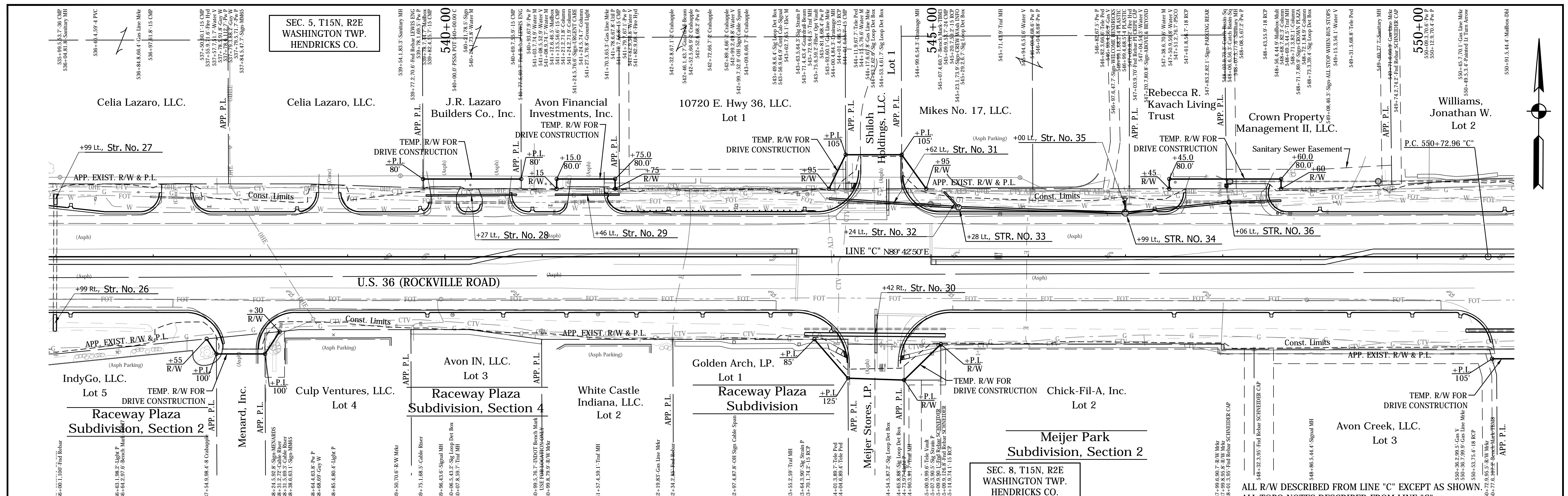
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RECOMMENDED FOR APPROVAL	DESIGNED ENGINEER	DATE
DESIGNED: <u>Y.Z.</u>	DRAWN: <u>K.R.U.</u>	
CHECKED: <u>L.L.C.</u>	CHECKED: <u>L.L.C.</u>	

HORIZONTAL SCALE	BRIDGE FILE
1" = 50'	N/A
VERTICAL SCALE	DESIGNATION
1" = 5'	1601072
SURVEY BOOK	SHEET
	35 of 96
CONTRACT	PROJECT
R-39814	1601072

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 DES. NUMBER 1601072



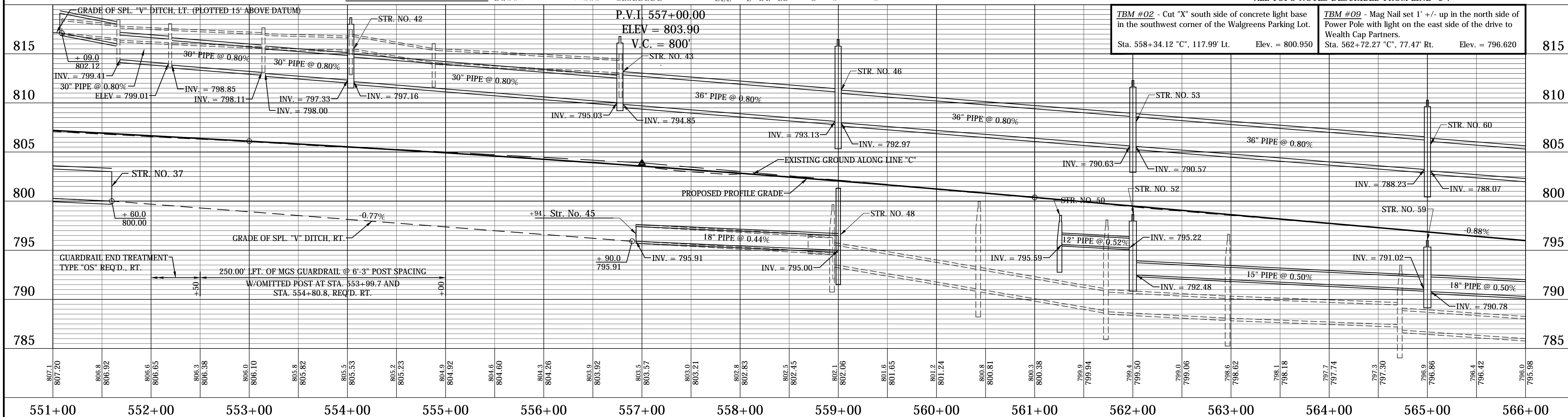
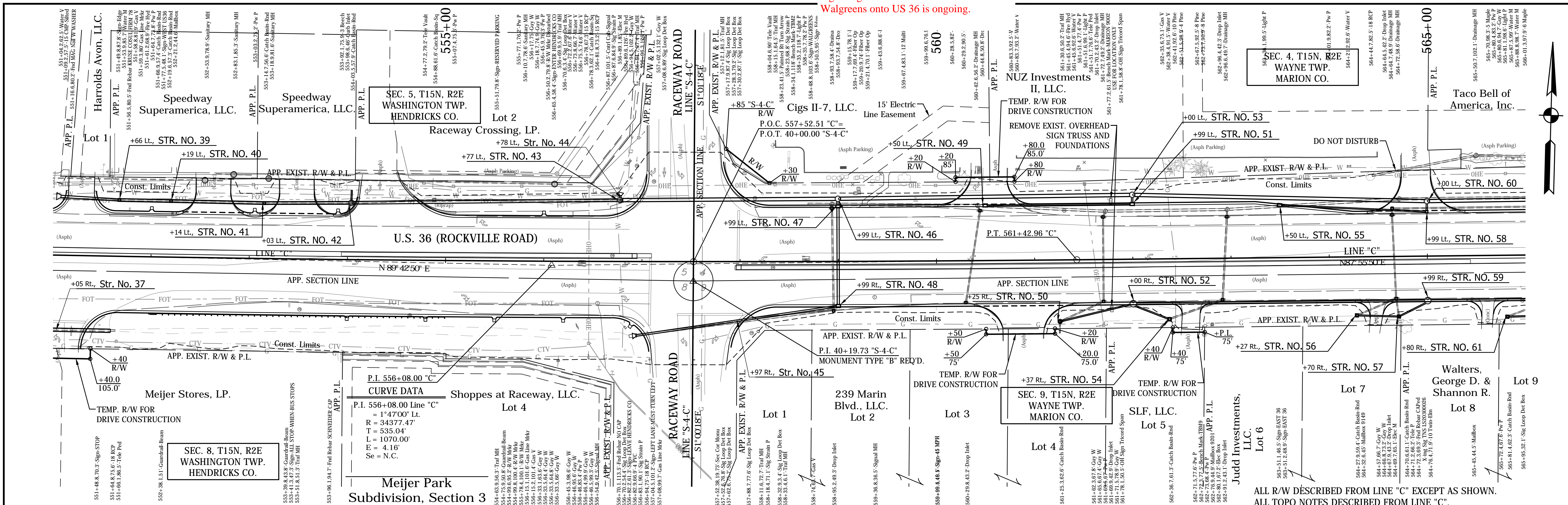


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PRINT DATE: 9/20/19  
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 Des. Number 1601072



Note to the Reviewer: Coordination regarding the sidewalk connection from Walgreens onto US 36 is ongoing.



SEC. 4, T15N, R2E  
WAYNE TWP.  
MARION CO.

SEC. 8, T15N, R2E  
WASHINGTON TWP.  
HENDRICKS CO.

SEC. 5, T15N, R2E  
WASHINGTON TWP.  
HENDRICKS CO.

NUZ Investments  
II, LLC.

Taco Bell of  
America, Inc.

PRINT DATE: 3/20/19 PLOT SCALE: 1:1 EDIT DATE: 7/12/11 3:42 PM EDITED BY: KUPHAW DRAWING FILE: P:\75-500-003-1\SS36\ATLACAD\06 PLAN PROF\RD\_PLAN PROFILE SHEET\_C\_50\_01.DWG

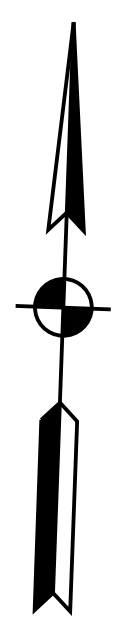
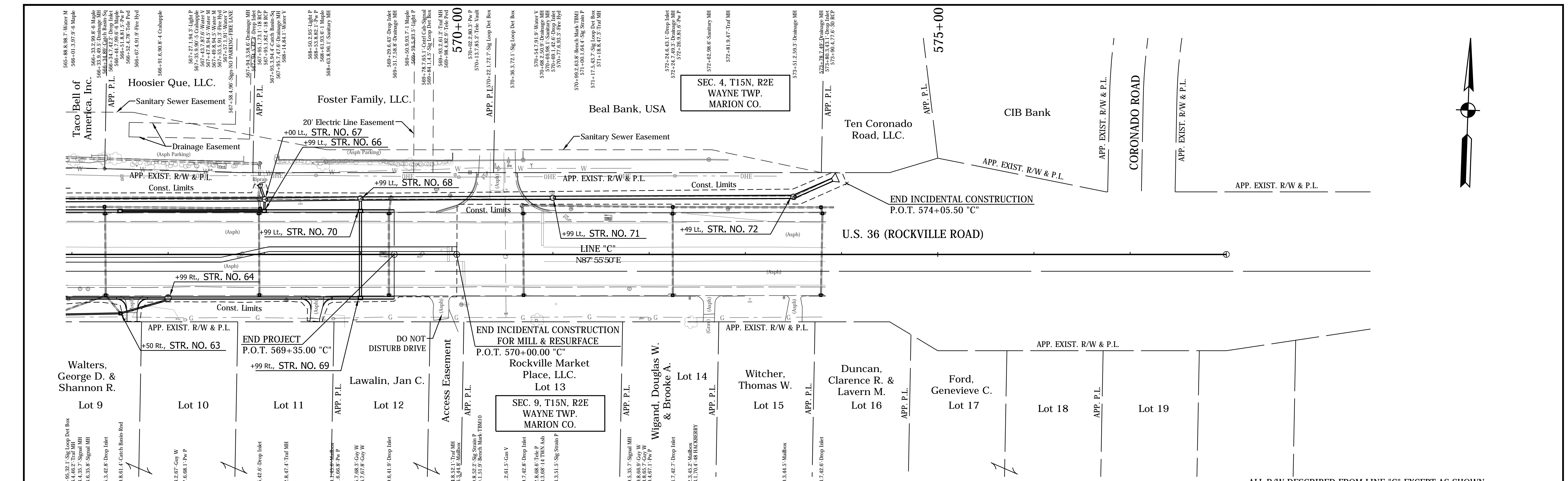
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552+45.18 "C", 91.62 Rt.	555+55.78 "C", 58.36 Ll.	556+56.78 "C", 108.70 Rt.	559+79.20 "C", 62.70 Ll.	563+86.73 "C", 47.76 Rt.

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: Y.Z.	DRAWN: K.R.U.	
CHECKED: L.L.C.	CHECKED: L.L.C.	

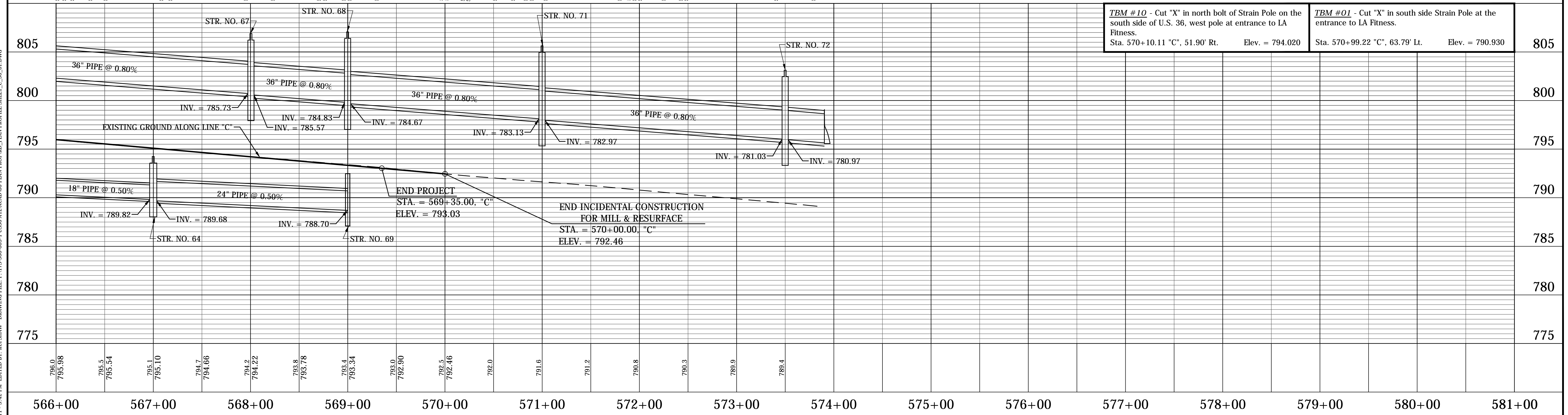
INDIANA DEPARTMENT OF TRANSPORTATION	
PLAN AND PROFILE STA. 551+00.00 TO 566+00.00 LINE "C"	

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VERTICAL SCALE 1" = 5'	DESIGNATION 1601072
SURVEY BOOK	SHEET 37 of 96
CONTRACT R-39814	PROJECT 1601072

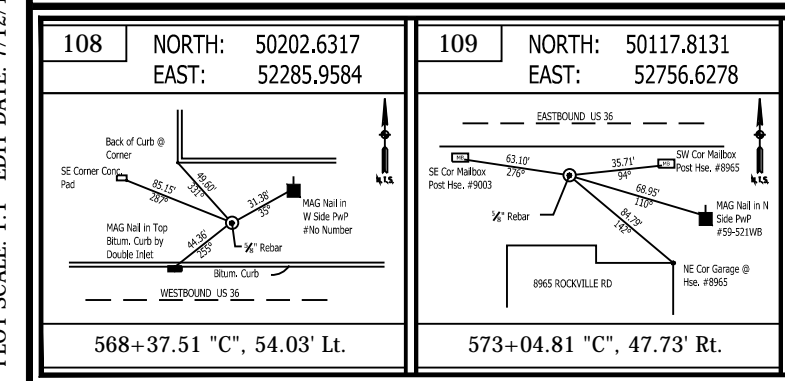




ALL R/W DESCRIBED FROM LINE "C" EXCEPT AS SHOWN.  
ALL TOPO NOTES DESCRIBED FROM LINE "C".



<b>TBM #10</b> - Cut "X" in north bolt of Strain Pole on the south side of U.S. 36, west pole at entrance to LA Fitness. Sta. 570+10.11 "C", 51.90' Rt. Elev. = 794.020	<b>TBM #01</b> - Cut "X" in south side Strain Pole at the entrance to LA Fitness. Sta. 570+99.22 "C", 63.79' Lt. Elev. = 790.930
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108 NORTH: 50202.6317 EAST: 52285.9584 568+37.51 "C", 54.03' Lt.	109 NORTH: 50117.8131 EAST: 52756.6278 573+04.81 "C", 47.73' Rt.
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RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: <u>Y.Z.</u>	DRAWN: <u>K.R.U.</u>	
CHECKED: <u>L.L.C.</u>	CHECKED: <u>L.L.C.</u>	

INDIANA  
DEPARTMENT OF TRANSPORTATION

PLAN AND PROFILE  
STA. 566+00.00 TO 577+99.88 LINE "C"

HORIZONTAL SCALE 1" = 50'	BRIDGE FILE N/A
VERTICAL SCALE 1" = 5'	DESIGNATION 1601072
SURVEY BOOK	SHEET 38 of 96
CONTRACT R-39814	PROJECT 1601072



**Environmental Assessment**

# **Appendix B**

**Early Coordination**

January 28, 2019

«Agency\_1»  
«Agency\_2»  
«Address\_1»  
«Address\_2»«  
City», «State» «Zip»

**Example Early Coordination Letter**

Re: Agencies Early Coordination  
Des. Number 1601072  
US 36 Added Travel Lane Project  
Avon, Hendricks and Marion Counties, Indiana

Dear «Position»,

The Federal Highway Administration (FHWA) and the Indiana Department of Transportation (INDOT) Crawfordsville District propose to proceed with an added travel lane project located on US 36 through the Town of Avon in Hendricks and Marion Counties, Indiana (Des. Number 1601072). The FHWA is providing funds and is designated as the lead Federal agency. This letter is part of the early coordination phase of the environmental review process. We are requesting comments from your area of expertise regarding any possible environmental effects associated with this project. Please use the above Des. Number and project description in your reply and your comments will be incorporated into the formal environmental study.

The project begins at Shiloh Park Drive, continues east for approximately 1.1 miles and ends approximately 1,500 feet east of Raceway Road. The project is in Washington Township (Hendricks County), Wayne Township (Marion County), Clermont U.S. Geological Survey (USGS) Quadrangle, Township 15 North, Range 2 East, and Sections 4, 5, 8 and 9. The project area is in a developed area surrounded by residential and commercial properties.

US 36 is a Principal Arterial. Within the project area, US 36 consists of two 12-foot wide travel lanes, one 16-foot wide two-way-left-turn-lane and up to a maximum 11-foot wide paved shoulder for east and westbound directions. A discontinuous 12-foot wide right-turn lane is present within portions of the project area. The posted speed limit is 45 miles per hour. Existing right-of way varies from approximately 75 to 135 from the roadway centerline. Two major intersections (Ronald Reagan Parkway and Raceway Road) are within the project area and both intersections operate at a Level of Service (LOS) of E (unstable flow, operating at capacity) during AM and PM peak hours. Per a Traffic Operations Analysis (EMCS, 2018), between 2016 and 2017, traffic volumes along US 36 have increased by approximately 2.5% which is likely due to new developments within along the corridor from 2016 to 2017, especially within the western project area. Per crash records between January 2010 and December 2014, at least 45 accidents have occurred along the segment of roadway within project area and 311 crashes have occurred at intersections within the project area.

Existing roadway drainage is handled via a combination of open ditches, driveway/approach pipe culverts (generally 15 to 30 inches in diameter) and an enclosed drainage system consisting of roadway inlets and yard catch basins. A 36-inch corrugated metal pipe (CMP) structure (unknown structure number) is approximately 650 feet east of the US 36/Ronald Reagan Parkway Intersection; this structure is not associated with jurisdictional waters. Structure CV 036-032-64.80 is approximately 230 feet west of the US/36 Ronald Reagan Parkway Intersection; the 108-inch diameter and 183-foot long CMP conveys Avon Creek under US 36. An existing 16-foot span and 70.5-foot long concrete arch top box culvert with 15-foot long wingwalls (unknown structure number) is under Shiloh Crossing Drive, approximately 550 feet northwest of the US 36/Ronald Reagan Parkway Intersection; the structure conveys Avon Creek under Shiloh Crossing Drive. Existing 10-foot wide trails are located north of US 36, both east and west of Ronald Reagan Parkway; both trails are part of the Ronald Reagan Parkway Corridor.

The preferred alternative will involve milling, resurfacing and widening of the existing roadway. The proposed cross section will consist of one 11-foot wide travel lane and two continuous 12-foot wide travel lanes with a discontinuous 11-foot wide right-turn-lane bordered by curb and gutter with a 2-foot offset for east and westbound directions. A 11-foot wide left-turn-lane will be provided for at each signalized intersection for east and westbound directions. An at-grade pedestrian crossing, and median pedestrian refuge will be installed to connect the existing trail located north of US 36, along the west side of Ronald Reagan Parkway, to the proposed trail located south of US 36, along the west side of Ronald Reagan Parkway (Des. Number 1601121). Des. Number 1601121 will be constructed prior to this added travel lanes project (Des. Number 1601072); therefore, coordination between both designers has been occurring during design of both projects. Des. Number 1601121 will terminate just south of the added travel lanes project; the added travel lanes project will fill in this gap to provide connectivity of the trail along both sides of US 36. Work will be needed to the curb ramp in the northeast quadrant of the US 36/Ronald Reagan Parkway Intersection to accommodate the existing trail located north of US 36, along the east side of Ronald Reagan Parkway. It will be determined during the design phase if existing signals will be modified or replaced.

The existing horizontal alignment of US 36 will remain unchanged. The profile grade will match the existing profile grade. Where needed, existing driveway/approach pipe culverts drainage will be replaced, some structures will have a larger diameter. Curb turnouts with ditches and driveway/approach pipe culverts are proposed to handle drainage between Shiloh Park Drive and Raceway Road. Between Raceway Road and the end of the project, an enclosed drainage system is proposed to match the existing drainage system; some inlets may be moved farther from the road in this section. Work to the CMP east of the US 36/Ronald Reagan Parkway Intersection will involve extending the length of the pipe approximately six feet north of US 36 and approximately 15 feet south of US 36. Work to Structure CV 036-032-64.80 will involve extending the length of the pipe approximately 10 feet north of US 36 and approximately 22 feet south of US 36. Work to the structure that conveys Avon Creek under Shiloh Crossing Drive may involve work to the southern wingwalls.

Construction will be phased, and two lanes of traffic will be maintained in each direction. Access to all properties will be maintained during construction. Most of the construction is anticipated to occur within the existing right-of-way. Approximately 0.25 acre of permanent and 0.75 acre of temporary right-of-way is anticipated to be required. The maximum depth of excavation is anticipated to be approximately eight feet below ground surface. Construction is anticipated to begin in 2021 and last for nine months.

To identify potential environmental concerns within the project vicinity, a Red Flag Investigation was performed for a 0.5-mile radius of the project area by RQAW. The Red Flag Investigation noted:

- Rainbow Acres Church of God is adjacent to the project area.
- Speedway Public Airport is located within 3.8 miles (20,000 feet) of the project area.
- Two trail segments transect the project area on the west side of Ronald Reagan Parkway. One trail segment abuts the north project area along the east side of Ronald Reagan Parkway. All trail segments are part of the Ronald Reagan Parkway Corridor.
- Avon Creek is within the project area and is listed as impaired for *Escherichia coli* (E. coli).
- The project area is within an Urbanized Area Boundary.
- Hazardous material concerns are mapped within and/or adjacent to the project area. Two gas stations are adjacent to the project area. A home furnishings store is adjacent to the project area and is mapped as a National Pollutant Discharge Elimination System (NPDES) facility. A car detailing store is adjacent to the project area and is mapped as a state cleanup site.

Coordination with respective agencies/owners of the above is occurring. See Appendix A for the Red Flag Investigation Maps.

RQAW performed site visits on May 25, 2018 and August 30, 2018 to identify any ecological resources present. Avon Creek and two unnamed tributaries (UNTS) of Avon Creek were observed within the project area. Six wetlands were also delineated within the project area. Permits will likely be needed. Tree clearing will not be needed. See Appendix B for the photograph key map and photographs.

The project is expected to qualify for the application of the U.S. Fish and Wildlife Service (USFWS) range-wide programmatic informal consultation for the Indiana bat and northern long-eared bat. Project information will be provided to the USFWS separately. RQAW is also investigating the Area of Potential Effect (APE) for archaeological and historic resources for compliance with Section 106. The results of these investigations will be forwarded to the State Historic Preservation Officer (SHPO) for review and concurrence.

If we do not receive your response within 30 calendar days from the date of this letter, it will be assumed your agency feels there will be no adverse effects incurred because of the project. However, if you feel an extension to the response time is necessary, a reasonable amount may be granted upon request. If a questionnaire follows this letter, please complete. If you have any questions regarding this matter, please contact Jaime Byerly of the Environmental Department at RQAW, at 317.588.1798 or at [jbyerly@rqaw.com](mailto:jbyerly@rqaw.com), or the INDOT Project Manager, Richard Gilyeat, at 765.361.5684, or at [rgilyeat@indot.in.gov](mailto:rgilyeat@indot.in.gov). Thank you in advance for your input.

Sincerely,



Jaime Byerly  
Environmental Department  
RQAW Corporation

Appendices removed to avoid duplication. See graphics in Appendices A and D of this EA document.

- Appendices:
- Appendix A: Red Flag Investigation Maps
  - Appendix B: Photograph Key and Photographs

Cc:

- INDOT Crawfordsville District (electronic coordination)
- Federal Highway Administration (electronic coordination)
- Natural Resources Conservation Service (electronic coordination)
- Indiana Geological Survey (electronic coordination)
- Indiana Department of Natural Resources (IDNR) Division of Fish and Wildlife (electronic coordination)
- Indiana Department of Environmental Management (IDEM) (electronic coordination)
- IDEM Ground Water Section (electronic coordination)
- INDOT Office of Public Involvement (electronic coordination)
- INDOT Office of Aviation (electronic coordination)
- U.S. Department of Housing and Urban Development (electronic coordination)
- U.S. Army Corps of Engineers, Louisville District (electronic coordination)
- National Park Service, Midwest Regional Office
- Indianapolis Metropolitan Planning Organization
- Hendricks County Planning and Building Commission
- Rainbow Acres Church of God
- Town of Avon MS4 Coordinator



- Hendricks County MS4 Coordinator
- Hendricks County Council
- Hendricks County Board of Commissioners
- Hendricks County Engineer
- Hendricks County Highway Supervisor
- Hendricks County Surveyor
- Marion County Council
- Marion County Board of Commissioners
- Marion County Department of Public Works
- Marion County Surveyor
- Town of Avon Council
- Town of Avon Public Works



# Indiana Department of Environmental Management

*We Protect Hoosiers and Our Environment.*

100 North Senate Avenue - Indianapolis, IN 46204  
(800) 451-6027 - (317) 232-8603 - [www.idem.IN.gov](http://www.idem.IN.gov)

RQAW

8770 North Street., Suite 110  
Fishers , IN 46038  
Date

RQAW

Ben DeMaria  
8770 North Street., Suite 110  
Fishers , IN 46038

To Engineers and Consultants Proposing Roadway Construction Projects:

RE: US 36 is a Principal Arterial. Within the project area, US 36 consists of two 12-foot wide travel lanes, one 16-foot wide two-way-left-turn-lane and up to a maximum 11-foot wide paved shoulder for east and westbound directions. A discontinuous 12-foot wide right-turn lane is present within portions of the project area. The posted speed limit is 45 miles per hour. Existing right-of way varies from approximately 75 to 135 from the roadway centerline. The preferred alternative will involve milling, resurfacing and widening of the existing roadway. The proposed cross section will consist of one 11-foot wide travel lane and two continuous 12-foot wide travel lanes with a discontinuous 11-foot wide right-turn-lane bordered by curb and gutter with a 2-foot offset for east and westbound directions. A 11-foot wide left-turn-lane will be provided for at each signalized intersection for east and westbound directions. An at-grade pedestrian crossing, and median pedestrian refuge will be installed to connect the existing trail located north of US 36, along the west side of Ronald Reagan Parkway, to the proposed trail located south of US 36, along the west side of Ronald Reagan Parkway (Des. Number 1601121). Des. Number 1601121 will be constructed prior to this added travel lanes project (Des. Number 1601072); therefore, coordination between both designers has been occurring during design of both projects. Des. Number 1601121 will terminate just south of the added travel lanes project; the added travel lanes project will fill in this gap to provide connectivity of the trail along both sides of US 36. Work will be needed to the curb ramp in the northeast quadrant of the US 36/Ronald Reagan Parkway Intersection to accommodate the existing trail located north of US 36, along the east side of Ronald Reagan Parkway. It will be determined during the design phase if existing signals will be modified or replaced.

This letter from the Indiana Department of Environmental Management (IDEM) serves as a standardized response to enquiries inviting IDEM comments on roadway construction, reconstruction, or other improvement projects within existing roadway corridors when the proposed scope of the project is beneath the threshold requiring a formal National Environmental Policy Act-mandated Environmental Assessment or Environmental Impact Statement. As the letter attempts to address all roadway-related environmental topics of potential concern, it is possible that not every topic addressed in the letter will be applicable to your particular roadway project.

For additional information on specific roadway-related topics of interest, please visit the appropriate Web pages cited below, many of which provide contact information for persons within the various program areas who can answer questions not fully addressed in this letter. Also please be mindful that some environmental requirements may be subject to change and so each person intending to include a copy of this letter in their project documentation packet is advised to download the most recently revised version of the letter; found at: <http://www.in.gov/idem/5283.htm> (<http://www.in.gov/idem/5283.htm>).

To ensure that all environmentally-related issues are adequately addressed, IDEM recommends that you read this letter in its entirety, and consider each of the following issues as you move forward with the planning of your proposed roadway construction, reconstruction, or improvement project:

## WATER AND BIOTIC QUALITY

1. Section 404 of the Clean Water Act requires that you obtain a permit from the U.S. Army Corps of Engineers (USACE) before discharging dredged or fill materials into any wetlands or other waters, such as rivers, lakes, streams, and ditches. Other activities regulated include the relocation, channelization, widening, or other such alteration of a stream, and the mechanical clearing (use of heavy construction equipment) of wetlands. Thus, as a project owner or sponsor, it is your responsibility to ensure that no wetlands are disturbed without the proper permit. Although you may initially refer to the U.S. Fish and Wildlife Service National Wetland Inventory maps as a means of identifying potential areas of concern, please be mindful that those maps do not depict jurisdictional wetlands regulated by the USACE or the Department of Environmental Management. A valid jurisdictional wetlands determination can only be made by the USACE, using the 1987 Wetland Delineation Manual.

USACE recommends that you have a consultant check to determine whether your project will abut, or lie within, a wetland area. To view a list of consultants that have requested to be included on a list posted by the USACE on their Web site, see USACE Permits and Public Notices (<http://www.lrl.usace.army.mil/orf/default.asp>) (<http://www.lrl.usace.army.mil/orf/default.asp>) and then click on "Information" from the menu on the right-hand side of that page. Their "Consultant List" is the fourth entry down on the "Information" page. Please note that the USACE posts all consultants that request to appear on the list, and that inclusion of any particular consultant on the list does not represent an endorsement of that consultant by the USACE, or by IDEM.

Much of northern Indiana (Newton, Lake, Porter, LaPorte, St. Joseph, Elkhart, LaGrange, Steuben, and Dekalb counties; large portions of Jasper, Starke, Marshall, Noble, Allen, and Adams counties; and lesser portions of Benton, White, Pulaski, Kosciusko, and Wells counties) is served by the USACE District Office in Detroit (313-226-6812). The central and southern portions of the state (large portions of Benton, White, Pulaski, Kosciusko, and Wells counties; smaller portions of Jasper, Starke, Marshall, Noble, Allen, and Adams counties; and all other Indiana counties located in north-central, central, and southern Indiana ) are served by the USACE Louisville District Office (502-315-6733).

Additional information on contacting these U.S. Army Corps of Engineers (USACE) District Offices, government agencies with jurisdiction over wetlands, and other water quality issues, can be found at <http://www.in.gov/idem/4396.htm> (<http://www.in.gov/idem/4396.htm>). IDEM recommends that impacts to wetlands and other water resources be avoided to the fullest extent.

2. In the event a Section 404 wetlands permit is required from the USACE, you also must obtain a Section 401 Water Quality Certification from the IDEM Office of Water Quality Wetlands Program. To learn more about the Wetlands Program, visit: <http://www.in.gov/idem/4384.htm> (<http://www.in.gov/idem/4384.htm>).
3. If the USACE determines that a wetland or other water body is isolated and not subject to Clean Water Act regulation, it is still regulated by the state of Indiana . A State Isolated Wetland permit from IDEM's Office of Water Quality (OWQ) is required for any activity that results in the discharge of dredged or fill materials into isolated wetlands. To learn more about isolated wetlands, contact the OWQ Wetlands Program at 317-233-8488.
4. If your project will involve over a 0.5 acre of wetland impact, stream relocation, or other large-scale alterations to water bodies such as the creation of a dam or a water diversion, you should seek additional input from the OWQ Wetlands Program staff. Consult the Web at: <http://www.in.gov/idem/4384.htm> (<http://www.in.gov/idem/4384.htm>) for the appropriate staff contact to further discuss your project.
5. Work within the one-hundred year floodway of a given water body is regulated by the Department of Natural Resources, Division of Water. The Division issues permits for activities regulated under the follow statutes:
  - IC 14-26-2 Lakes Preservation Act 312 IAC 11
  - IC 14-26-5 Lowering of Ten Acre Lakes Act No related code
  - IC 14-28-1 Flood Control Act 310 IAC 6-1
  - IC 14-29-1 Navigable Waterways Act 312 IAC 6
  - IC 14-29-3 Sand and Gravel Permits Act 312 IAC 6
  - IC 14-29-4 Construction of Channels Act No related code

For information on these Indiana (statutory) Code and Indiana Administrative Code citations, see the DNR Web site at: <http://www.in.gov/dnr/water/9451.htm> (<http://www.in.gov/dnr/water/9451.htm>) . Contact the DNR Division of Water at 317-232-4160 for further information.

The physical disturbance of the stream and riparian vegetation, especially large trees overhanging any affected water bodies should be limited to only that which is absolutely necessary to complete the project. The shade provided by the large overhanging trees helps maintain proper stream temperatures and dissolved oxygen for aquatic life.

6. For projects involving construction activity (which includes clearing, grading, excavation and other land disturbing activities) that result in the disturbance of one (1), or more, acres of total land area, contact the Office of Water Quality – Watershed Planning Branch (317/233-



1864) regarding the need for of a Rule 5 Storm Water Runoff Permit. Visit the following Web page

- <http://www.in.gov/idem/4902.htm> (<http://www.in.gov/idem/4902.htm>)

To obtain, and operate under, a Rule 5 permit you will first need to develop a Construction Plan (<http://www.in.gov/idem/4917.htm#constreq>) (<http://www.in.gov/idem/4917.htm#constreq>), and as described in 327 IAC 15-5-6.5 (<http://www.in.gov/legislative/iac/T03270/A00150> [PDF] (<http://www.in.gov/legislative/iac/T03270/A00150.PDF>), pages 16 through 19). Before you may apply for a Rule 5 Permit, or begin construction, you must submit your Construction Plan to your county Soil and Water Conservation District (SWCD) (<http://www.in.gov/isda/soil/contacts/map.html> (<http://www.in.gov/isda/soil/contacts/map.html>)).

Upon receipt of the construction plan, personnel of the SWCD or the Indiana Department of Environmental Management will review the plan to determine if it meets the requirements of 327 IAC 15-5. Plans that are deemed deficient will require re-submittal. If the plan is sufficient you will be notified and instructed to submit the verification to IDEM as part of the Rule 5 Notice of Intent (NOI) submittal. Once construction begins, staff of the SWCD or Indiana Department of Environmental Management will perform inspections of activities at the site for compliance with the regulation.

Please be mindful that approximately 149 Municipal Separate Storm Sewer System (MS4) areas are now being established by various local governmental entities throughout the state as part of the implementation of Phase II federal storm water requirements. All of these MS4 areas will eventually take responsibility for Construction Plan review, inspection, and enforcement. As these MS4 areas obtain program approval from IDEM, they will be added to a list of MS4 areas posted on the IDEM Website at: <http://www.in.gov/idem/4900.htm> (<http://www.in.gov/idem/4900.htm>).

If your project is located in an IDEM-approved MS4 area, please contact the local MS4 program about meeting their storm water requirements. Once the MS4 approves the plan, the NOI can be submitted to IDEM.

Regardless of the size of your project, or which agency you work with to meet storm water requirements, IDEM recommends that appropriate structures and techniques be utilized both during the construction phase, and after completion of the project, to minimize the impacts associated with storm water runoff. The use of appropriate planning and site development and appropriate storm water quality measures are recommended to prevent soil from leaving the construction site during active land disturbance and for post construction water quality concerns. Information and assistance regarding storm water related to construction activities are available from the Soil and Water Conservation District (SWCD) offices in each county or from IDEM.

7. For projects involving impacts to fish and botanical resources, contact the Department of Natural Resources - Division of Fish and Wildlife (317/232-4080) for addition project input.

8. For projects involving water main construction, water main extensions, and new public water supplies, contact the Office of Water Quality - Drinking Water Branch (317-308-3299) regarding the need for permits.
9. For projects involving effluent discharges to waters of the State of Indiana , contact the Office of Water Quality - Permits Branch (317-233-0468) regarding the need for a National Pollutant Discharge Elimination System (NPDES) permit.
10. For projects involving the construction of wastewater facilities and sewer lines, contact the Office of Water Quality - Permits Branch (317-232-8675) regarding the need for permits.

## AIR QUALITY

The above-noted project should be designed to minimize any impact on ambient air quality in, or near, the project area. The project must comply with all federal and state air pollution regulations. Consideration should be given to the following:

1. Regarding open burning, and disposing of organic debris generated by land clearing activities; some types of open burning are allowed (<http://www.in.gov/idem/4148.htm>) under specific conditions. You also can seek an open burning variance from IDEM.

However, IDEM generally recommends that you take vegetative wastes to a registered yard waste composting facility or that the waste be chipped or shredded with composting on site (you must register with IDEM if more than 2,000 pounds is to be composted; contact 317/232-0066). The finished compost can then be used as a mulch or soil amendment. You also may bury any vegetative wastes (such as leaves, twigs, branches, limbs, tree trunks and stumps) onsite, although burying large quantities of such material can lead to subsidence problems, later on.

Reasonable precautions must be taken to minimize fugitive dust emissions from construction and demolition activities. For example, wetting the area with water, constructing wind barriers, or treating dusty areas with chemical stabilizers (such as calcium chloride or several other commercial products). Dirt tracked onto paved roads from unpaved areas should be minimized.

Additionally, if construction or demolition is conducted in a wooded area where blackbirds have roosted or abandoned buildings or building sections in which pigeons or bats have roosted for 3-5 years precautionary measures should be taken to avoid an outbreak of histoplasmosis. This disease is caused by the fungus *Histoplasma capsulatum*, which stems from bird or bat droppings that have accumulated in one area for 3-5 years. The spores from this fungus become airborne when the area is disturbed and can cause infections over an entire community downwind of the site. The area should be wetted down prior to cleanup or demolition of the project site. For more detailed information on histoplasmosis prevention and control, please contact the Acute Disease Control Division of the Indiana State Department of Health at (317) 233-7272.

2. The U.S. EPA and the Surgeon General recommend that people not have long-term exposure to radon at levels above 4 pCi/L. (For a county-by-county map of predicted radon levels in Indiana, visit: <http://www.in.gov/idem/4145.htm> (<http://www.in.gov/idem/4145.htm>).

The U.S. EPA further recommends that all homes (and apartments within three stories of ground level) be tested for radon. If in-home radon levels are determined to be 4 pCi/L, or higher, EPA recommends a follow-up test. If the second test confirms that radon levels are 4 pCi/L, or higher, EPA recommends the installation of radon-reduction measures. (For a list of qualified radon testers and radon mitigation (or reduction) specialists visit: [http://www.in.gov/isdh/regsvcs/radhealth/pdfs/radon\\_testers\\_mitigators\\_list.pdf](http://www.in.gov/isdh/regsvcs/radhealth/pdfs/radon_testers_mitigators_list.pdf) ([http://www.in.gov/isdh/regsvcs/radhealth/pdfs/radon\\_testers\\_mitigators\\_list.pdf](http://www.in.gov/isdh/regsvcs/radhealth/pdfs/radon_testers_mitigators_list.pdf))). It also is recommended that radon reduction measures be built into all new homes, particularly in areas like Indiana that have moderate to high predicted radon levels.

To learn more about radon, radon risks, and ways to reduce exposure visit: <http://www.in.gov/isdh/regsvcs/radhealth/radon.htm> (<http://www.in.gov/isdh/regsvcs/radhealth/radon.htm>), <http://www.in.gov/idem/4145.htm> (<http://www.in.gov/idem/4145.htm>), or <http://www.epa.gov/radon/index.html> (<http://www.epa.gov/radon/index.html>).

3. With respect to asbestos removal: all facilities slated for renovation or demolition (except residential buildings that have (4) four or fewer dwelling units and which will not be used for commercial purposes) must be inspected by an Indiana-licensed asbestos inspector prior to the commencement of any renovation or demolition activities. If regulated asbestos-containing material (RACM) that may become airborne is found, any subsequent demolition, renovation, or asbestos removal activities must be performed in accordance with the proper notification and emission control requirements.

If no asbestos is found where a renovation activity will occur, or if the renovation involves removal of less than 260 linear feet of RACM off of pipes, less than 160 square feet of RACM off of other facility components, or less than 35 cubic feet of RACM off of all facility components, the owner or operator of the project does not need to notify IDEM before beginning the renovation activity.

For questions on asbestos demolition and renovation activities, you can also call IDEM's Lead/Asbestos section at 1-888-574-8150.

However, in all cases where a demolition activity will occur (even if no asbestos is found), the owner or operator must still notify IDEM 10 working days prior to the demolition, using the form found at <http://www.in.gov/icpr/webfile/formsdiv/44593.pdf> (<http://www.in.gov/icpr/webfile/formsdiv/44593.pdf>).

Anyone submitting a renovation/demolition notification form will be billed a notification fee based upon the amount of friable asbestos containing material to be removed or demolished. Projects that involve the removal of more than 2,600 linear feet of friable asbestos containing materials on pipes, or 1,600 square feet or 400 cubic feet of friable

asbestos containing material on other facility components, will be billed a fee of \$150 per project; projects below these amounts will be billed a fee of \$50 per project. All notification remitters will be billed on a quarterly basis.

For more information about IDEM policy regarding asbestos removal and disposal, visit: <http://www.in.gov/idem/4983.htm> (<http://www.in.gov/idem/4983.htm>).

4. With respect to lead-based paint removal: IDEM encourages all efforts to minimize human exposure to lead-based paint chips and dust. IDEM is particularly concerned that young children exposed to lead can suffer from learning disabilities. Although lead-based paint abatement efforts are not mandatory, any abatement that is conducted within housing built before January 1, 1978, or a child-occupied facility is required to comply with all lead-based paint work practice standards, licensing and notification requirements. For more information about lead-based paint removal visit: <http://www.in.gov/isdh/19131.htm> (<http://www.in.gov/isdh/19131.htm>).
5. Ensure that asphalt paving plants are permitted and operate properly. The use of cutback asphalt, or asphalt emulsion containing more than seven percent (7%) oil distillate, is prohibited during the months April through October. See 326 IAC 8-5-2, Asphalt Paving Rule (<http://www.ai.org/legislative/iac/T03260/A00080.PDF> (<http://www.ai.org/legislative/iac/T03260/A00080.PDF>)).
6. If your project involves the construction of a new source of air emissions or the modification of an existing source of air emissions or air pollution control equipment, it will need to be reviewed by the IDEM Office of Air Quality (OAQ). A registration or permit may be required under 326 IAC 2 (View at: [www.ai.org/legislative/iac/t03260/a00020.pdf](http://www.ai.org/legislative/iac/t03260/a00020.pdf) (<http://www.ai.org/legislative/iac/t03260/a00020.pdf>)). New sources that use or emit hazardous air pollutants may be subject to Section 112 of the Clean Air Act and corresponding state air regulations governing hazardous air pollutants.
7. For more information on air permits visit: <http://www.in.gov/idem/4223.htm> (<http://www.in.gov/idem/4223.htm>), or to initiate the IDEM air permitting process, please contact the Office of Air Quality Permit Reviewer of the Day at (317) 233-0178 or OAMPROD atdem.state.in.us.

## LAND QUALITY

In order to maintain compliance with all applicable laws regarding contamination and/or proper waste disposal, IDEM recommends that:

1. If the site is found to contain any areas used to dispose of solid or hazardous waste, you need to contact the Office of Land Quality (OLQ) at 317-308-3103.
2. All solid wastes generated by the project, or removed from the project site, need to be taken to a properly permitted solid waste processing or disposal facility. For more information, visit <http://www.in.gov/idem/4998.htm> (<http://www.in.gov/idem/4998.htm>).



3. If any contaminated soils are discovered during this project, they may be subject to disposal as hazardous waste. Please contact the OLQ at 317-308-3103 to obtain information on proper disposal procedures.
4. If PCBs are found at this site, please contact the Industrial Waste Section of OLQ at 317-308-3103 for information regarding management of any PCB wastes from this site.
5. If there are any asbestos disposal issues related to this site, please contact the Industrial Waste Section of OLQ at 317-308-3103 for information regarding the management of asbestos wastes (Asbestos removal is addressed above, under Air Quality).
6. If the project involves the installation or removal of an underground storage tank, or involves contamination from an underground storage tank, you must contact the IDEM Underground Storage Tank program at 317/308-3039. See:  
<http://www.in.gov/idem/4999.htm> (<http://www.in.gov/idem/4999.htm>).

## FINAL REMARKS

Should you need to obtain any environmental permits in association with this proposed project, please be mindful that IC 13-15-8 requires that you notify all adjoining property owners and/or occupants within ten days your submittal of each permit application. However, if you are seeking multiple permits, you can still meet the notification requirement with a single notice if all required permit applications are submitted with the same ten day period.

Should the scope of the proposed project be expanded to the extent that a National Environmental Policy Act Environmental Assessment (EA) or Environmental Impact Statement (EIS) is required, IDEM will actively participate in any early interagency coordination review of the project.

Meanwhile, please note that this letter does not constitute a permit, license, endorsement or any other form of approval on the part of the Indiana Department of Environmental Management regarding any project for which a copy of this letter is used. Also note that is it the responsibility of the project engineer or consultant using this letter to ensure that the most current draft of this document, which is located at <http://www.in.gov/idem/5284.htm> (<http://www.in.gov/idem/5284.htm>), is used.

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## Signature(s) of the Applicant

I acknowledge that the following proposed roadway project will be financed in part, or in whole, by public monies.

# Project Description

US 36 is a Principal Arterial. Within the project area, US 36 consists of two 12-foot wide travel lanes, one 16-foot wide two-way-left-turn-lane and up to a maximum 11-foot wide paved shoulder for east and westbound directions. A discontinuous 12-foot wide right-turn lane is present within portions of the project area. The posted speed limit is 45 miles per hour. Existing right-of way varies from approximately 75 to 135 from the roadway centerline. The preferred alternative will involve milling, resurfacing and widening of the existing roadway. The proposed cross section will consist of one 11-foot wide travel lane and two continuous 12-foot wide travel lanes with a discontinuous 11-foot wide right-turn-lane bordered by curb and gutter with a 2-foot offset for east and westbound directions. A 11-foot wide left-turn-lane will be provided for at each signalized intersection for east and westbound directions. An at-grade pedestrian crossing, and median pedestrian refuge will be installed to connect the existing trail located north of US 36, along the west side of Ronald Reagan Parkway, to the proposed trail located south of US 36, along the west side of Ronald Reagan Parkway (Des. Number 1601121). Des. Number 1601121 will be constructed prior to this added travel lanes project (Des. Number 1601072); therefore, coordination between both designers has been occurring during design of both projects. Des. Number 1601121 will terminate just south of the added travel lanes project; the added travel lanes project will fill in this gap to provide connectivity of the trail along both sides of US 36. Work will be needed to the curb ramp in the northeast quadrant of the US 36/Ronald Reagan Parkway Intersection to accommodate the existing trail located north of US 36, along the east side of Ronald Reagan Parkway. It will be determined during the design phase if existing signals will be modified or replaced.


With my signature, I do hereby affirm that I have read the letter from the Indiana Department of Environment that appears directly above. In addition, I understand that in order to complete that project in which I am interested, with a minimum of impact to the environment, I must consider all the issues addressed in the aforementioned letter, and further, that I must obtain any required permits.

Date: March 12, 2019

Signature of the INDOT  
Project Engineer or Other Responsible Agent 

Date: \_\_\_\_\_

Signature of the  
For Hire Consultant \_\_\_\_\_

 Ben DeMaria



# Indiana Department of Environmental Management

*We Protect Hoosiers and Our Environment.*

100 N. Senate Avenue • Indianapolis, IN 46204  
(800) 451-6027 • (317) 232-8603 • [www.idem.IN.gov](http://www.idem.IN.gov)

Eric J. Holcomb  
Governor

Bruno Pigott  
Commissioner

February 1, 2019

66-33  
RQAW Corporation  
Attention: Jaime Byerly  
8770 North Street, Suite 110  
Fishers, Indiana 46038

RE: Wellhead Protection Area  
Proximity Determination  
Des No 1601072  
US 36 Added Travel Lane Project  
Avon, Hendricks and Marion  
Counties, Indiana

Dear Jaime Byerly,

Upon review of the above referenced project site, it has been determined that the proposed project area **is not located within** a Wellhead Protection Area. The information is accurate to the best of our knowledge; however, there are in some cases a few factors that could impact the accuracy of this determination. Some Wellhead Protection Area Delineations have not been submitted, and many have not been approved by this office. In these cases we use a 3,000 foot fixed radius buffer to make the proximity determination. To find the status of a Public Water Supply System's (PWSS's) Wellhead Protection Area Delineation please visit our tracking database at <http://www.in.gov/idem/cleanwater/2456.htm> and scroll to the bottom of the page.

Note: the Drinking Water Branch has launched a new self service feature which allows one to determine wellhead proximity without submitting the application form. Use the following instructions:

1. Go to <http://idemmaps.idem.in.gov/whpa2/>
2. Use the search tool located in the upper left hand corner of the application to zoom to your site of interest by way of city, county, or address; or use the mouse to click on the site of interest displayed on the map.
3. Once the site of interest has been located and selected, use the print tool to create a .pdf of a wellhead protection area proximity determination response.

In the future please consider using this self service feature if it suits your needs.

If you have any additional questions please feel free to contact me at the address above or at (317) 233-9158 and [aturnbow@idem.in.gov](mailto:aturnbow@idem.in.gov).

Sincerely,

Alisha Turnbow,  
Environmental Manager  
Ground Water Section  
Drinking Water Branch  
Office of Water Quality



Please Reduce, Reuse, Recycle

**THIS IS NOT A PERMIT**

**State of Indiana**  
**DEPARTMENT OF NATURAL RESOURCES**  
**Division of Fish and Wildlife**  
**Early Coordination/Environmental Assessment**

**DNR #:** ER-21182

**Request Received:** January 28, 2019

**Requestor:** RQAW Corporation  
Jaime Byerly  
10401 North Meridian Street, Suite 401  
Indianapolis, IN 46290

**Project:** US 36 added travel lanes from Shiloh Professional Drive to LA Fitness entrance, small structure (CV 036-032-64.80) pipe extension over Avon Creek, CMP pipe extension east of Ronald Reagan Parkway, and Shiloh Drive small structure wingwall rehabilitation over Avon Creek, Avon; Des #1601072

**County/Site info:** Hendricks - Marion

The Indiana Department of Natural Resources has reviewed the above referenced project per your request. Our agency offers the following comments for your information and in accordance with the National Environmental Policy Act of 1969.

If our agency has regulatory jurisdiction over the project, the recommendations contained in this letter may become requirements of any permit issued. If we do not have permitting authority, all recommendations are voluntary.

**Regulatory Assessment:** This proposal will require the formal approval for construction in a floodway under the Flood Control Act, IC 14-28-1 for any proposal to construct, excavate, or fill in or on the floodway of a stream or other flowing waterbody which has a drainage area greater than one square mile. Please submit a copy of this letter with the permit application.

**Natural Heritage Database:** The Natural Heritage Program's data have been checked. To date, no plant or animal species listed as state or federally threatened, endangered, or rare have been reported to occur in the project vicinity.

**Fish & Wildlife Comments:** Any modified stream crossing structure, and any bank stabilization under or around the structure, must not create conditions that are less favorable for wildlife passage when compared to current conditions. The Division of Fish and Wildlife would like to emphasize the importance of wildlife passage issues and transportation infrastructure projects. The following is a good place to start in terms of resources to consider in the design of stream crossing structures: <http://www.fs.fed.us/wildlifecrossings/library/>.

The additional measures listed below should be implemented to avoid, minimize, or compensate for impacts to fish, wildlife, and botanical resources:

1. Revegetate all bare and disturbed areas that will not be mowed and maintained with a mixture of grasses, sedges, and wildflowers native to Central Indiana and specifically for stream bank/floodway stabilization purposes as soon as possible upon completion; turf-type grasses (including low-endophyte, friendly endophyte, and endophyte free tall fescue but excluding all other varieties of tall fescue) may be used in regularly mowed areas only.
2. Minimize and contain within the project limits inchannel disturbance and the clearing of trees and brush.
3. Do not work in the waterway from April 1 through June 30 without the prior written approval of the Division of Fish and Wildlife.
4. Do not construct any temporary runarounds, access bridges, causeways, cofferdams, diversions, or pumpharounds.
5. Use minimum average 6 inch graded riprap stone extended below the normal water level to provide habitat for aquatic organisms in the voids.
6. Do not use broken concrete as riprap.



**THIS IS NOT A PERMIT**

**State of Indiana  
DEPARTMENT OF NATURAL RESOURCES  
Division of Fish and Wildlife  
Early Coordination/Environmental Assessment**

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7. Underlay the riprap with a bedding layer of well graded aggregate or a geotextile to prevent piping of soil underneath the riprap.
8. Minimize the movement of resuspended bottom sediment from the immediate project area.
9. Do not deposit or allow demolition/construction materials or debris to fall or otherwise enter the waterway.
10. Appropriately designed measures for controlling erosion and sediment must be implemented to prevent sediment from entering the stream or leaving the construction site; maintain these measures until construction is complete and all disturbed areas are stabilized.
11. Seed and protect all disturbed streambanks and slopes not protected by other methods that are 3:1 or steeper with erosion control blankets that are heavy-duty, biodegradable, and net free or that use loose-woven / Leno-woven netting to minimize the entrapment and snaring of small-bodied wildlife such as snakes and turtles (follow manufacturer's recommendations for selection and installation); seed and apply mulch on all other disturbed areas.

**Contact Staff:**

Christie L. Stanifer, Environ. Coordinator, Fish & Wildlife  
Our agency appreciates this opportunity to be of service. Please contact the above staff member at (317) 232-4080 if we can be of further assistance.



**Date:** February 28, 2019

Christie L. Stanifer  
Environ. Coordinator  
Division of Fish and Wildlife



## Organization and Project Information

**Project ID:**  
**Des. ID:** 1601072  
**Project Title:** US 36 Added Travel Lanes  
**Name of Organization:** RQAW  
**Requested by:** Ben DeMaria

## Environmental Assessment Report

1. Geological Hazards:
  - Moderate liquefaction potential
  - Floodway
2. Mineral Resources:
  - Bedrock Resource: Moderate Potential
  - Sand and Gravel Resource: Low Potential
3. Active or abandoned mineral resources extraction sites:
  - None documented in the area

\*All map layers from Indiana Map ([maps.indiana.edu](http://maps.indiana.edu))

### **DISCLAIMER:**

This document was compiled by Indiana University, Indiana Geological Survey, using data believed to be accurate; however, a degree of error is inherent in all data. This product is distributed "AS-IS" without warranties of any kind, either expressed or implied, including but not limited to warranties of suitability to a particular purpose or use. No attempt has been made in either the design or production of these data and document to define the limits or jurisdiction of any federal, state, or local government. The data used to assemble this document are intended for use only at the published scale of the source data or smaller (see the metadata links below) and are for reference purposes only. They are not to be construed as a legal document or survey instrument. A detailed on-the-ground survey and historical analysis of a single site may differ from these data and this document.

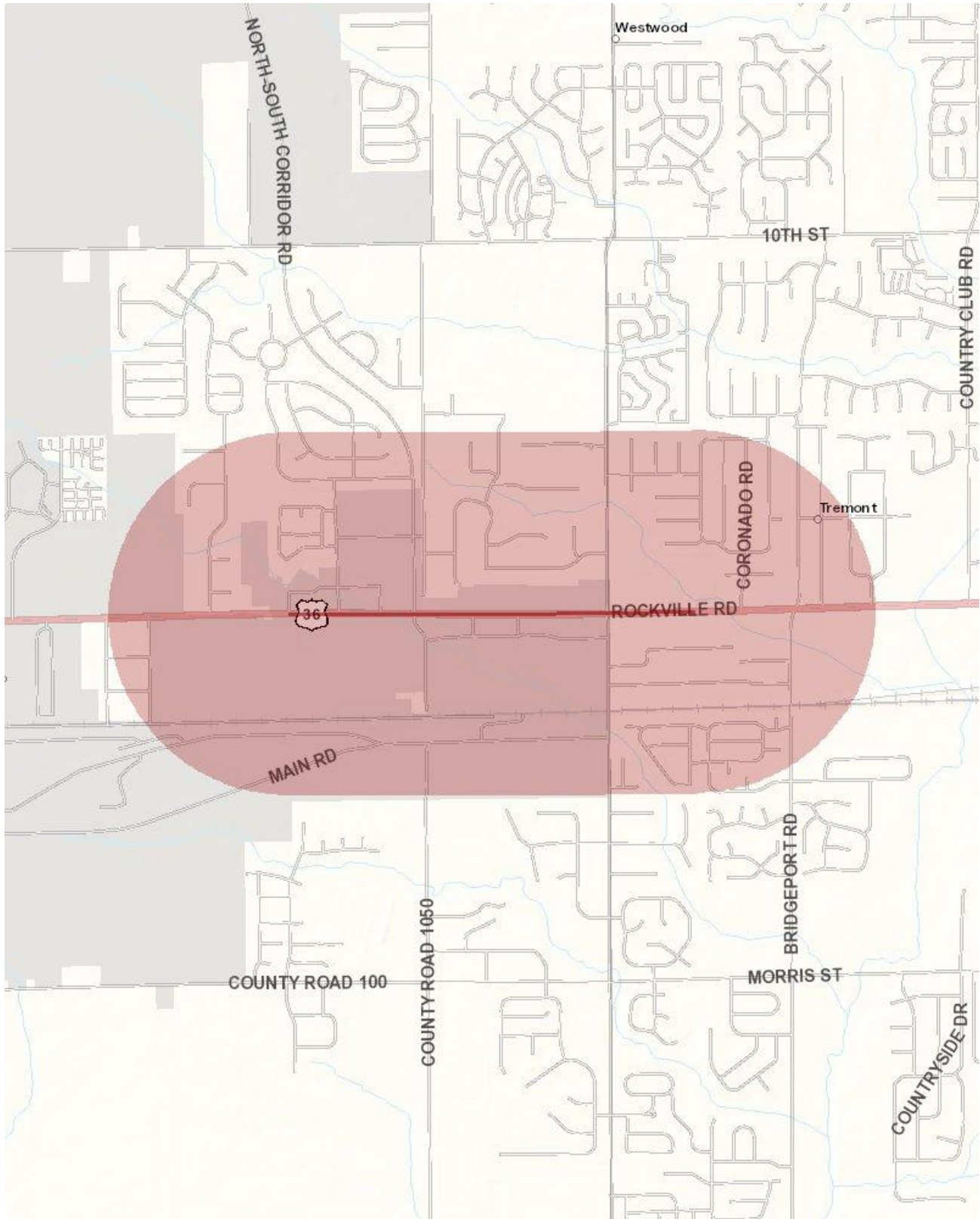
This information was furnished by Indiana Geological Survey

Address: 420 N. Walnut St., Bloomington, IN 47404

Email: IGSEnvir@indiana.edu

Phone: 812 855-7428

Date: January 28, 2019





## Metadata:

- [https://maps.indiana.edu/metadata/Geology/Seismic\\_Earthquake\\_Liquefaction\\_Potential.html](https://maps.indiana.edu/metadata/Geology/Seismic_Earthquake_Liquefaction_Potential.html)
- [https://maps.indiana.edu/metadata/Geology/Industrial\\_Minerals\\_Sand\\_Gravel\\_Resources.html](https://maps.indiana.edu/metadata/Geology/Industrial_Minerals_Sand_Gravel_Resources.html)
- [https://maps.indiana.edu/metadata/Hydrology/Floodplains\\_FIRM.html](https://maps.indiana.edu/metadata/Hydrology/Floodplains_FIRM.html)
- [https://maps.indiana.edu/metadata/Geology/Bedrock\\_Geology.html](https://maps.indiana.edu/metadata/Geology/Bedrock_Geology.html)





United States Department of Agriculture

February 13, 2019

Jaime Byerly  
RQAW Corporation  
8770 North Street, Suite 110  
Fishers, Indiana 46038

Dear Ms. Byerly:

The proposed project to add travel lanes on US 36 in Avon, Hendricks and Marion Counties, Indiana (Des No. 1601072), as referred to in your letter received on January 28, 2019, will not cause a conversion of prime farmland.

If you need additional information, please contact Daniel Phillips at 317-295-5871.

Sincerely,

JERRY RAYNOR  
State Conservationist

Enclosure

## Benjamin DeMaria

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**From:** Wright, Mary <MWRIGHT@indot.IN.gov>  
**Sent:** Tuesday, January 29, 2019 7:27 AM  
**To:** Benjamin DeMaria  
**Subject:** RE: Agencies Early Coordination/US 36 Added Travel Lanes (Des. No. 1601072) Marion and Hendricks Counties

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### Early Coordination and Creating a Public Involvement Plan (PIP)

We have received your early coordination notification packet for the above referenced project(s). Our office prefers to be notified at the early coordination stage in order to encourage early and ongoing public involvement aside from the specific legal requirements as outlined in our Public Involvement Manual <http://www.in.gov/indot/2366.htm>. Seeking the public's understanding of transportation improvement projects early in the project development stage can allow the opportunity for the public to express their concerns, comments, and to seek buy-in. Early coordination is the perfect opportunity to examine the proposed project and its impacts to the community along with the many ways and or tools to inform the public of the improvements and seek engagement. A good public involvement plan, or PIP, should consider the type, scope, impacts, and the level of public awareness that should, or could, be implemented. In other words, although there are cases where no public involvement is legally required, sometimes it is simply the right thing to do in order to keep the public informed.

The public involvement office is always available to provide support and resources to bolster any public involvement activities you may wish to implement or discuss. Please feel free to contact our office anytime should you have any questions or concerns. Thank you for notifying our office about your proposed project. We trust you will not only analyze the appropriate public involvement required, but also consider the opportunity to do go above and beyond those requirements in creating a good PIP.

Rickie Clark, Manager  
100 North Senate Avenue, Room N642  
Indianapolis, IN 46204  
Phone: 317-232-6601  
Email: [rclark@indot.in.gov](mailto:rclark@indot.in.gov)

Mary Wright, Hearing Examiner  
Phone: 317-234-0796  
Email: [mwright@indot.in.gov](mailto:mwright@indot.in.gov)

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**From:** Benjamin DeMaria [mailto:[bdemaria@rqaw.com](mailto:bdemaria@rqaw.com)]  
**Sent:** Monday, January 28, 2019 12:42 PM  
**To:** Clark, Rickie <[RCLARK@indot.IN.gov](mailto:RCLARK@indot.IN.gov)>  
**Cc:** Wright, Mary <[MWRIGHT@indot.IN.gov](mailto:MWRIGHT@indot.IN.gov)>  
**Subject:** Agencies Early Coordination/US 36 Added Travel Lanes (Des. No. 1601072) Marion and Hendricks Counties

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Mr. Rickie Clark,

Attached, please find an early coordination letter and appendices regarding the above project. These materials are for your review and comment for the environmental document.

Thank you,



# INDIANA DEPARTMENT OF TRANSPORTATION

100 North Senate Avenue  
Room N955  
Indianapolis, Indiana 46204

PHONE: (317) 232-1477  
FAX: (317) 232-1499

**Eric Holcomb, Governor**  
**Joe McGuinness, Commissioner**

February 4, 2019

Ms. Jaime Byerly, NEPA Specialist  
RQAW Environmental  
10401 N. Meridian St., Ste. 401  
Indianapolis, IN 46290

*Subject: Early Coordination Review (Des. No. 1601072)*

Dear Ms. Byerly,

In response to your request on January 28, 2019 for early coordination review of an added travel lane project located on US 36 through the Town of Avon in Hendricks and Marion Counties, Indiana; the Indiana Department of Transportation, Office of Aviation has reviewed the information and provides the following:

**Are there any existing or proposed public-use airports within 5 nautical miles of the project limits (IC 8-21-10-6)?**

The Indianapolis International Airport is located approximately 2.4 nautical miles northwest of the proposed project corridor.

**Will an Indiana Tall Structure permit (IC 8-21-10-3-a) and/or Noise Sensitive (IC 8-21-10-3-b) permit be required?**

Based upon the provided information, an Indiana Tall Structure permit would not be required unless the project involves the construction of a temporary (e.g., crane) or permanent structure that penetrates a 100:1 slope from the nearest point of the Indianapolis International Airport runways.

For any questions related to Indiana Tall Structure and/or Noise Sensitive permitting, please contact James Kinder at (317) 232-1485 or [jkinder2@indot.in.gov](mailto:jkinder2@indot.in.gov).

Sincerely,

Adam French, MPA  
Chief Airport Inspector, Office of Aviation  
Indiana Department of Transportation

## Jaime Byerly

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**From:** John Ayers <jayers@co.hendricks.in.us>  
**Sent:** Wednesday, February 13, 2019 1:27 PM  
**To:** Gilyeat, Richard; Jaime Byerly  
**Cc:** Ryan Cannon  
**Subject:** Des 1601072 US 36 Added Travel Lanes

Richard/Jaime:

Hendricks County has received the Early Coordination Letter regarding the above project. We do not foresee any adverse effects to County facilities due to this project.

Please remember that drainage structures not located under US 36 and 36" or greater diameter or span length are under County jurisdiction. We need to review any proposed modifications to such structures during the design process.

Thank you for including us in this effort.

John Ayers

*John E. Ayers, P.E.*  
*Hendricks County Engineer*  
*317-745-9236*



## Jaime Byerly

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**From:** Catlin, Bryan F. <Bryan.Catlin@indy.gov>  
**Sent:** Monday, February 4, 2019 1:53 PM  
**To:** Jaime Byerly; rgilyeat@indot.in.gov  
**Cc:** Jenkins, Debra S.; Black, Dana; Pangelinan, Robert S.; Catlin, Bryan F.; 'dgaston@co.hendricks.in.us'  
**Subject:** Des. Number 1601072 US 36 Added Travel Lane Project  
**Attachments:** 15020901.pdf; MCSO Rockville-Raceway Map.pdf

Dear Jaime and Richard:

The Marion County Surveyor's Office has one monument located in the right-of-way of the project area at the county line. I have attached the tie sheet for section corner monument that will be affected for your convenience as well as a drawing showing the general location of the monument. This monument will need to be replaced by INDOT under the supervision of our office per IC 8-23-9-24 if it is disturbed. Our office can provide a cast iron Harrison monument to replace the current Harrison if you desire. Since this is a common corner with Hendricks County, I am sure the Hendricks County Surveyor's Office will want to be present if/when this corner is excavated also.

However, I assume we were notified under the assumption our office is responsible for legal drains. Since Marion County was reorganized under Unigov, the responsibilities for legal drains the Marion County Surveyor's Office once had are now part of the responsibilities of the Indianapolis Department of Public Works. This was apparently included in the Unigov enabling legislation so there would only be one agency responsible for county wide drainage. Any drainage questions should be directed to DPW.

Please feel free to contact me if you have any further questions,

**Bryan F. Catlin, PS**

Technical Supervisor  
Marion County Surveyor's Office  
City-County Building  
200 East Washington St. Suite 742  
Indianapolis, Indiana 46204-3327  
Office (317) 327-4150  
Fax (317) 327-4146  
[Bryan.Catlin@indy.gov](mailto:Bryan.Catlin@indy.gov)

# MARION COUNTY SURVEYOR'S OFFICE

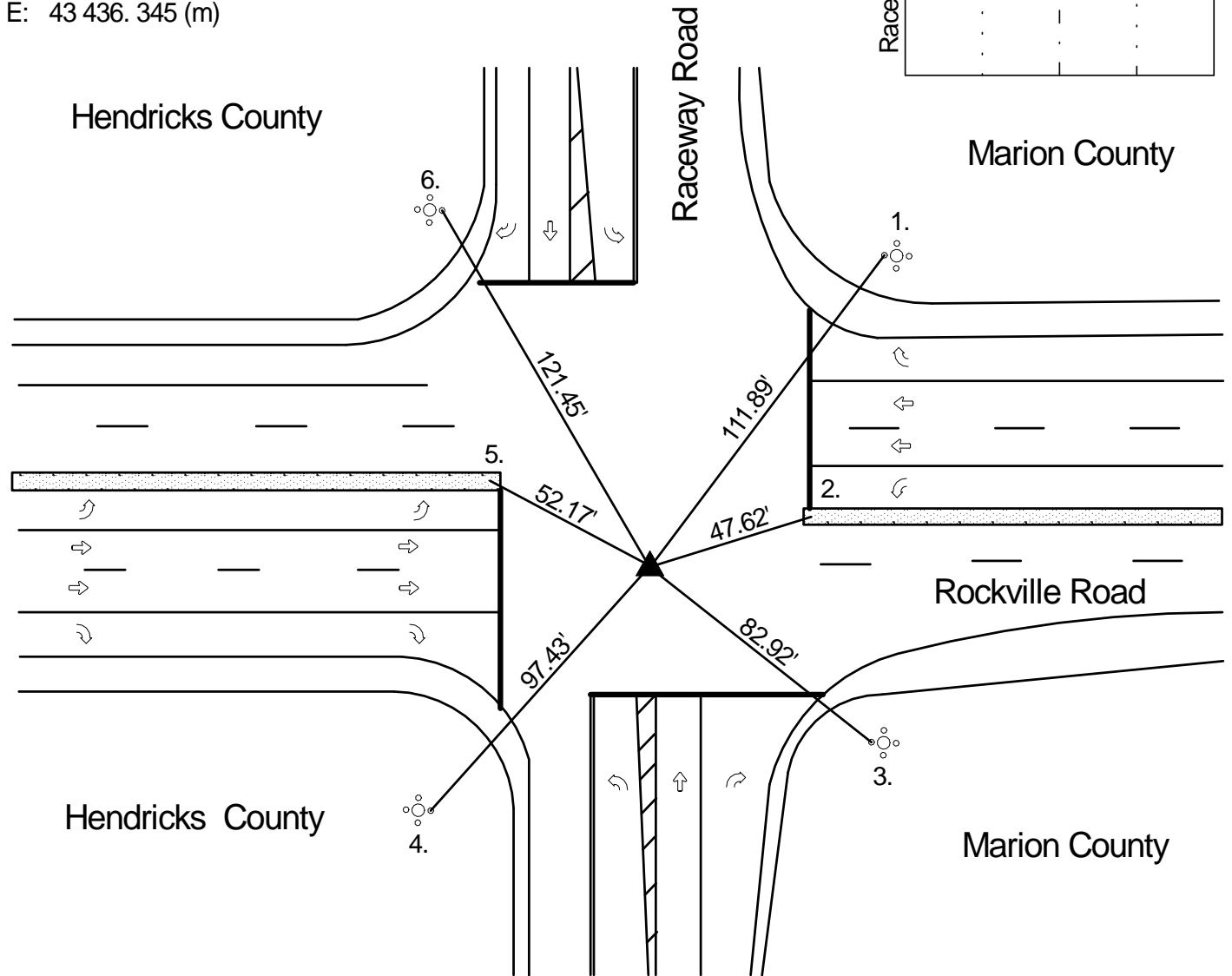
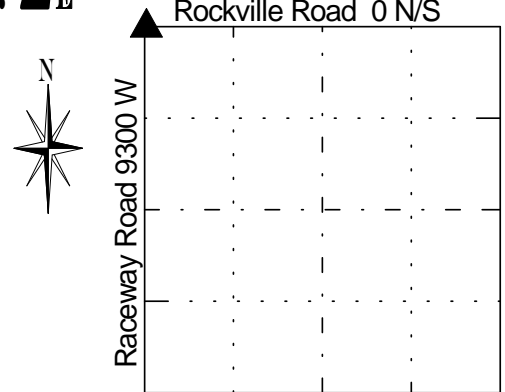
## SECTION CORNER RECORD

DEBRA S. JENKINS  
MARION COUNTY SURVEYOR

SEC. 9 TWN. 15<sub>N</sub> RNG. 2<sub>E</sub>

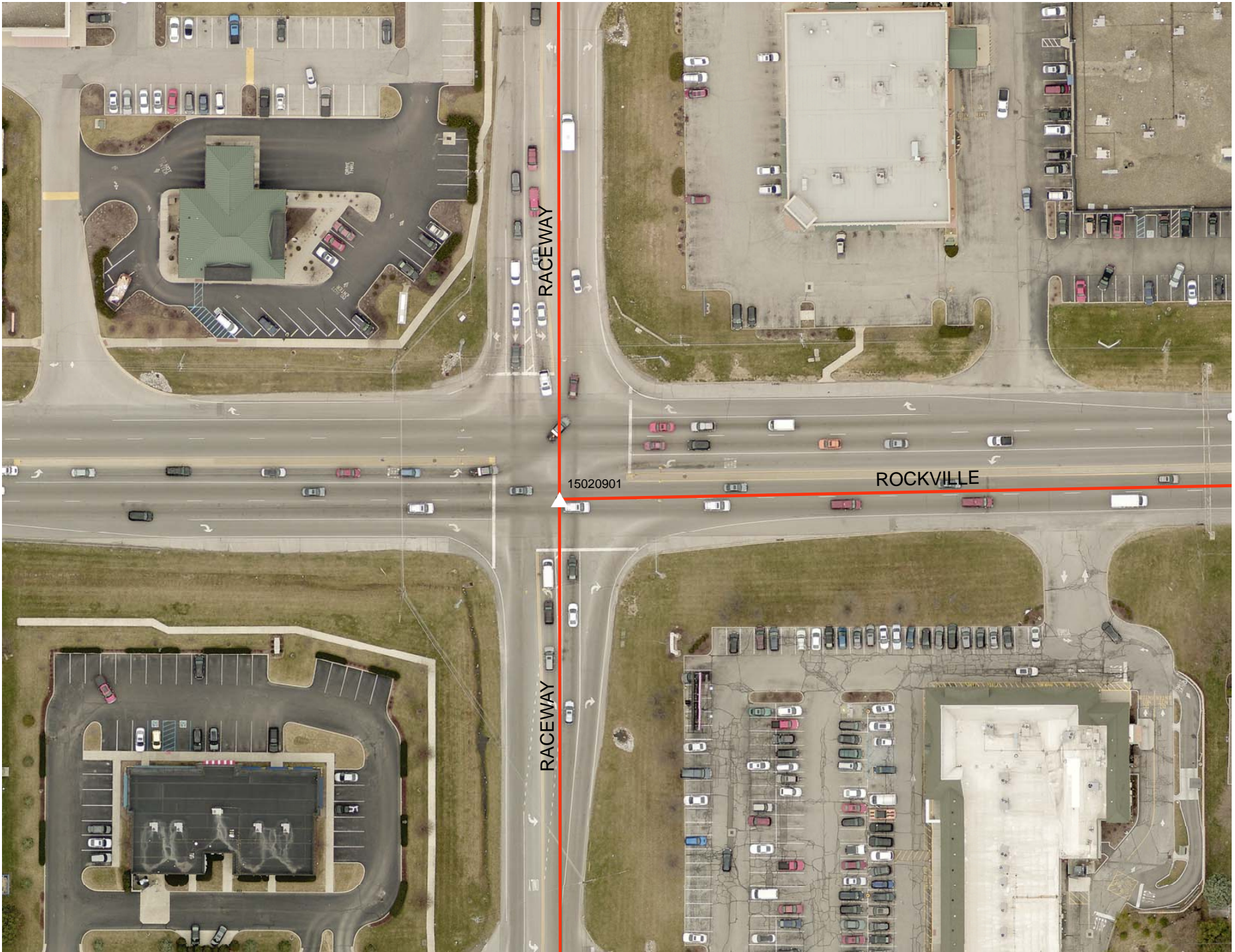
BRYAN F. CATLIN, PS  
TECHNICAL SUPERVISOR

Date: 3/10/2017  
Party Chief: Wilburn  
Crew: Bronson/Pangelinan  
Type of Marker: Harrison  
Method of Establishment: County: 7/13/1997  
IN S.P.C. E Zone, NAD 83 (1986)  
Name: 15020901  
N: 501 484. 420 (m)  
E: 43 436. 345 (m)



Original post set in 1820 by Wm. B. Laughlin with witnesses: 14" Sugar N 87°W 12 links, 12" Sugar S 71°E 9 links (Marion County Atlas pg. 2). R. Mayhew 9/1858 noted 'Stone in Road' (Wayne Twp. Legal Survey Book pg. 25). John V. Coyner 4/19 & 21/1900 'Found the original rock set years ago by Former Surveyors' (Wayne Twp. Legal Survey Book pg. 88). MCSO undated called 6" C.I. Plate under Black Top. MCSO undated called 8" Steel Plate. Stephen W. Burres 6/18/1963 called 8" Steel Plate (Wayne Twp. Legal Survey Book pg. 152). R. Straber 12/2/1971 called 8" Steel Plate. J.A. McLand 3/4/1975 called 8" Steel Plate 5" down. R. Miller 2/26/1976 called Brass Plate. Greene 6/30/1986 called Brass Plate. J.A.D. 8/15/1986 called Brass Plate. Wilcoxon 11/25/1995 found Brass Plug. Wilcoxon 6/3/1997 excavated, replaced Brass Plug with Harrison. Wilcoxon 7/13/1997 called Harrison. Wilcoxon 8/10/2002 called Harrison. Greene 5/20/2009 called Harrison. Wilburn 3/10/2017 found Harrison 0.1' down.

REF	DISTANCE	DESCRIPTION
1.	111.89'	Cut "X" W bolt traffic pole base
2.	47.62'	Cut "X" W end concrete median
3.	82.92'	Cut "X" W bolt traffic pole base
4.	97.43'	Cut "X" E bolt traffic pole base
5.	52.17'	Cut "X" E end concrete median
6.	121.45'	Cut "X" E bolt traffic pole base
		RP



## Jaime Byerly

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**From:** Krzton-Presson, Amy M <AKrztonPresson@indot.IN.gov>  
**Sent:** Thursday, November 1, 2018 11:58 AM  
**To:** Jaime Byerly  
**Subject:** RE: USFWS and IDNR Database Review: Des. Number 1601072

Nothing within 0.5 mile of your project area!

---

**From:** Jaime Byerly [mailto:jbyerly@RQAW.com]  
**Sent:** Thursday, November 01, 2018 11:53 AM  
**To:** Krzton-Presson, Amy M <AKrztonPresson@indot.IN.gov>  
**Subject:** RE: USFWS and IDNR Database Review: Des. Number 1601072

\*\*\*\* This is an EXTERNAL email. Exercise caution. DO NOT open attachments or click links from unknown senders or unexpected email. \*\*\*\*

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Amy,

Thank you! Did anything show up in Indiana Natural Heritage Database?

**Jaime Byerly | NEPA Specialist**

O: 317.588.1798

[www.rqaw.com](http://www.rqaw.com)

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**From:** Krzton-Presson, Amy M <[AKrztonPresson@indot.IN.gov](mailto:AKrztonPresson@indot.IN.gov)>  
**Sent:** Thursday, November 1, 2018 11:47 AM  
**To:** Jaime Byerly <[jbyerly@RQAW.com](mailto:jbyerly@RQAW.com)>  
**Subject:** RE: USFWS and IDNR Database Review: Des. Number 1601072

Hi Jaime:

1601072: A review of the USFWS database **did not** indicate the presence of endangered bat species in or within 0.5 mile of the project area.

**Amy Krzton-Presson**

*Environmental Manager*

41 West 300 North

Crawfordsville, IN 47933

**Office:** (765) 361-5253

**Email:** [akrztonpresson@indot.in.gov](mailto:akrztonpresson@indot.in.gov)





## Jaime Byerly

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**From:** Gina Zaccagnini  
**Sent:** Wednesday, March 20, 2019 8:16 AM  
**To:** Jaime Byerly; Aaron Lawson  
**Subject:** FW: IPaC Determination Key - US 36 Added Travel Lanes in Hendricks and Marion Counties (Des. 1601072)

---

**From:** Hinkle, Meghan <MHinkle@indot.IN.gov>  
**Sent:** Wednesday, March 20, 2019 8:14 AM  
**To:** Gina Zaccagnini <gzaccagnini@rqaw.com>  
**Cc:** Bales, Ronald <rbales@indot.IN.gov>; Miller, Brandon <BraMiller1@indot.IN.gov>; Krzton-Presson, Amy M <AKrztonPresson@indot.IN.gov>  
**Subject:** FW: IPaC Determination Key - US 36 Added Travel Lanes in Hendricks and Marion Counties (Des. 1601072)

Good Morning Gina,

Based on a meeting we had this week I would say no that commitment does not need added to the document.

With our current history the likelihood bats will be present in these types of structures is low. Current USFWS guidance states inspections can be completed in the winter. The two larger structures were inspected during bat season and during the winter and it was determined bat are not present on those structures.

This guidance may change in the future but for now that commitment is not needed in the CE document unless received from USFWS.

Meghan Hinkle  
Major Projects / LPA Review Liaison  
Environmental Services Division  
Indiana Department of Transportation  
100 N Senate Ave N 642  
Indianapolis, IN 46204-2216  
317-232-1490  
Email: [MHinkle@indot.IN.gov](mailto:MHinkle@indot.IN.gov)



To ensure that all NEPA documents are submitted appropriately in ERMS to the NEPA Document Review Unit, please be sure to include the following:

- The document type (CE/EA/EIS/PCE for ITS/Noise Analysis/ECF/AI/NTF/Bat Language) within the subject line and the body of the text.
- State in the body of the email who the document is intended for based on the CE Manual
  - PCE and State projects that are a CE-2 or lower to the appropriate district environmental supervisor/team lead
  - LPA and State projects that are a CE-3 and above or EA/EIS to the INDOT ESD Document Team Lead at Central Office.

- Specify the name and email address of the recipient who should get the final document (e.g. Brandon Miller, NEPA Document Team Lead at Central Office; email: [bramiller1@indot.in.gov](mailto:bramiller1@indot.in.gov))

---

**From:** Gina Zaccagnini [<mailto:gzaccagnini@rqaw.com>]

**Sent:** Monday, March 18, 2019 9:11 AM

**To:** Bales, Ronald <[rbales@indot.IN.gov](mailto:rbales@indot.IN.gov)>

**Cc:** Joseph Dabkowski <[jdabkowski@RQAW.com](mailto:jdabkowski@RQAW.com)>

**Subject:** FW: IPaC Determination Key - US 36 Added Travel Lanes in Hendricks and Marion Counties (Des. 1601072)

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Good Morning Ron,

Please see below response from the Crawfordsville District regarding the IPaC Determination Key, as well as the attached concurrence letter.

I am emailing you in regards to the statement that is highlighted below. Based on what the District told us, what would you prefer us to do for the US 36 Added Travel Lanes Project regarding bat inspections? In the past, we typically listed this as an environmental commitment; however, I wanted to reach out and ask what do you prefer since the District suggested to do so!

I also attached within this email the site photos and the table we put together for all of the pipes/structures that are within the project area for your reference. This bat inspection was conducted on February 12, 2019 by RQAW.

If you could get back to me whenever you can about this, I would greatly appreciate any help!

Thank you,

Gina Zaccagnini

---

**From:** Krzton-Presson, Amy M <[AKrztonPresson@indot.IN.gov](mailto:AKrztonPresson@indot.IN.gov)>

**Sent:** Friday, March 15, 2019 11:57 AM

**To:** Gina Zaccagnini <[gzaccagnini@rqaw.com](mailto:gzaccagnini@rqaw.com)>

**Subject:** RE: IPaC Determination Key - US 36 Added Travel Lanes in Hendricks and Marion Counties (Des. 1601072)

Hi Gina,

I have reviewed the USFWS consistency letter for Des No 1601072. I concur with the NLAA finding. If any changes to the scope should occur that would change the answers provided in the determination key, the key will have to be revised and the new effect determination reviewed by district environmental staff prior to NEPA approval.

The concurrence verification letter for the project is attached. USFWS has 14 days to review the finding and provide any comments. If USFWS does not provide any comments within that timeframe, you may proceed with the proposed action under the terms of the NLAA concurrence.

Additionally, it may be good to conduct a second structure inspection on all the pipes once it is actually bat season. The inspection in February is fine for getting IPaC done. Since Central is reviewing this document, I would check with them to see if they want an in-season bat inspection done. I don't want to speak on their behalf, but I thought it would be better to give you a heads up about this now instead of finding out during the document review. It's also possible that they'll just want a commitment saying the inspection will be done before the project goes to letting. You'll just have to see what they prefer.

**Amy Krzton-Presson**  
**Environmental Manager**

41 West 300 North  
Crawfordsville, IN 47933

**Office:** (765) 361-5253

**Email:** [akrztonpresson@indot.in.gov](mailto:akrztonpresson@indot.in.gov)



---

**From:** Gina Zaccagnini [<mailto:gzaccagnini@rgaw.com>]

**Sent:** Friday, March 15, 2019 10:43 AM

**To:** Krzton-Presson, Amy M <[AKrztonPresson@indot.IN.gov](mailto:AKrztonPresson@indot.IN.gov)>

**Subject:** RE: IPaC Determination Key - US 36 Added Travel Lanes in Hendricks and Marion Counties (Des. 1601072)

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Hi Amy,

I completed the IPaC determination again for the above referenced project and got a NLAA w/ AMMs. I believe you are still added as a member to review this key online- if not, please let me know and I can add you again! After field visits conducted on May 25, 2018 and February 12, 2019, all structures were investigated and evidence of bats were not found. The project designer stated that there will not be any tree clearing, and permanent lighting is not needed for this project. Hopefully the attached table with the labeled structures is detailed enough and provides all of the necessary information you need! I re-attached that same document again within this email just in case!

Let me know of any questions/concerns regarding the key.

Thank you!

Gina



# United States Department of the Interior



## FISH AND WILDLIFE SERVICE

Indiana Ecological Services Field Office

620 South Walker Street

Bloomington, IN 47403-2121

Phone: (812) 334-4261 Fax: (812) 334-4273

<http://www.fws.gov/midwest/Endangered/section7/s7process/step1.html>

In Reply Refer To:

March 15, 2019

Consultation Code: 03E12000-2019-SLI-0357

Event Code: 03E12000-2019-E-03343

Project Name: US 36 Added Travel Lanes in the Town of Avon, Hendricks and Marion Counties, Indiana (Des. 1601072)

Subject: Updated list of threatened and endangered species that may occur in your proposed project location, and/or may be affected by your proposed project

To Whom It May Concern:

The attached species list identifies any federally threatened, endangered, proposed and candidate species that may occur within the boundary of your proposed project or may be affected by your proposed project. The list also includes designated critical habitat if present within your proposed project area or affected by your project. This list is provided to you as the initial step of the consultation process required under section 7(c) of the Endangered Species Act, also referred to as Section 7 Consultation.

Section 7 of the Endangered Species Act of 1973 requires that actions authorized, funded, or carried out by Federal agencies not jeopardize federally threatened or endangered species or adversely modify designated critical habitat. To fulfill this mandate, Federal agencies (or their designated non-federal representative) must consult with the Service if they determine their project “may affect” listed species or critical habitat.

Under 50 CFR 402.12(e) (the regulations that implement Section 7 of the Endangered Species Act) the accuracy of this species list should be verified after 90 days. This verification can be completed formally or informally. You may verify the list by visiting the ECOS-IPaC website <http://ecos.fws.gov/ipac/> at regular intervals during project planning and implementation and completing the same process you used to receive the attached list. As an alternative, you may contact this Ecological Services Field Office for updates.

Please use the species list provided and visit the U.S. Fish and Wildlife Service's Region 3 Section 7 Technical Assistance website at - <http://www.fws.gov/midwest/endangered/section7/s7process/index.html>. This website contains step-by-step instructions which will help you



determine if your project will have an adverse effect on listed species and will help lead you through the Section 7 process.

For all **wind energy projects** and **projects that include installing towers that use guy wires or are over 200 feet in height**, please contact this field office directly for assistance, even if no federally listed plants, animals or critical habitat are present within your proposed project or may be affected by your proposed project.

Although no longer protected under the Endangered Species Act, be aware that bald eagles are protected under the Bald and Golden Eagle Protection Act (16 U.S.C. 668 *et seq.*) and Migratory Bird Treaty Act (16 U.S.C. 703 *et seq.*), as are golden eagles. Projects affecting these species may require measures to avoid harming eagles or may require a permit. If your project is near an eagle nest or winter roost area, see our Eagle Permits website at <http://www.fws.gov/midwest/midwestbird/EaglePermits/index.html> to help you determine if you can avoid impacting eagles or if a permit may be necessary.

We appreciate your concern for threatened and endangered species. Please include the Consultation Tracking Number in the header of this letter with any request for consultation or correspondence about your project that you submit to our office.

Attachment(s):

- Official Species List

# Official Species List

This list is provided pursuant to Section 7 of the Endangered Species Act, and fulfills the requirement for Federal agencies to "request of the Secretary of the Interior information whether any species which is listed or proposed to be listed may be present in the area of a proposed action".

This species list is provided by:

**Indiana Ecological Services Field Office**

620 South Walker Street

Bloomington, IN 47403-2121

(812) 334-4261

## Project Summary

Consultation Code: 03E12000-2019-SLI-0357

Event Code: 03E12000-2019-E-03343

Project Name: US 36 Added Travel Lanes in the Town of Avon, Hendricks and Marion Counties, Indiana (Des. 1601072)

Project Type: TRANSPORTATION

Project Description: The FHWA and the INDOT Crawfordsville District propose to proceed with an added travel lane project located on US 36 through the Town of Avon in Hendricks and Marion Counties, Indiana (Des. Number 1601072). The project begins at Shiloh Park Drive, continues east for approximately 1.1 miles and ends approximately 1,500 feet east of Raceway Road. Within the project area, US 36 consists of two 12-foot wide travel lanes, one 16-foot wide two-way-left-turn-lane and up to a maximum 11-foot wide paved shoulder for east and westbound directions. A discontinuous right-turn lane is present within portions of the project area. The project will involve milling, resurfacing and widening of the existing roadway. The proposed cross section will consist of one 11-foot wide travel lane and two continuous 12-foot wide travel lanes with a discontinuous 11-foot wide right-turn-lane bordered by curb and gutter with a 2-foot offset for east and westbound directions. A 11-foot wide left-turn-lane will be provided for at each signalized intersection for east and westbound directions. An at-grade pedestrian crossing, and median pedestrian refuge will be installed to connect the existing trail located north of US 36, along the west side of Ronald Reagan Parkway, to the proposed trail located south of US 36, along the west. The existing horizontal alignment of US 36 will remain unchanged. The profile grade will match the existing profile grade. Where needed, existing driveway/approach pipe culverts drainage will be replaced, some structures will have a larger diameter. Curb turnouts with ditches and driveway/approach pipe culverts are proposed to handle drainage between Shiloh Park Drive and Raceway Road.

Approximately 0.25 acre of permanent and 0.75 acre of temporary right-of-way is anticipated. Construction is anticipated to begin in 2021 and last for nine months. There is suitable habitat near the project area; however, no tree clearing is required. Per the Red Flag Investigation, dated November 30, 2018, a review of the USFWS Database by INDOT Crawfordsville District did not indicate the presence of endangered bat species in or within 0.5 mile of the project area. The December 24, 2015 INDOT Bridge Inspection Report for CV 036-032-64.80 contains no

information about whether bats are present or absent in the culvert. Per a site investigation on February 12, 2019, seven structures were inspected for evidence of bats and no evidence of bats was observed. Temporary lighting is likely to be used during construction, and permanent lighting is not required.

Project Location:

Approximate location of the project can be viewed in Google Maps: <https://www.google.com/maps/place/39.76376927107428N86.33274596857518W>



Counties: Hendricks, IN | Marion, IN



## Endangered Species Act Species

There is a total of 2 threatened, endangered, or candidate species on this species list.

Species on this list should be considered in an effects analysis for your project and could include species that exist in another geographic area. For example, certain fish may appear on the species list because a project could affect downstream species. Note that 1 of these species should be considered only under certain conditions.

IPaC does not display listed species or critical habitats under the sole jurisdiction of NOAA Fisheries<sup>1</sup>, as USFWS does not have the authority to speak on behalf of NOAA and the Department of Commerce.

See the "Critical habitats" section below for those critical habitats that lie wholly or partially within your project area under this office's jurisdiction. Please contact the designated FWS office if you have questions.

- 
1. [NOAA Fisheries](#), also known as the National Marine Fisheries Service (NMFS), is an office of the National Oceanic and Atmospheric Administration within the Department of Commerce.

## Mammals

NAME	STATUS
Indiana Bat <i>Myotis sodalis</i> There is <b>final</b> critical habitat for this species. Your location is outside the critical habitat. Species profile: <a href="https://ecos.fws.gov/ecp/species/5949">https://ecos.fws.gov/ecp/species/5949</a> Species survey guidelines: <a href="https://ecos.fws.gov/ipac/guideline/survey/population/1/office/31440.pdf">https://ecos.fws.gov/ipac/guideline/survey/population/1/office/31440.pdf</a>	Endangered
Northern Long-eared Bat <i>Myotis septentrionalis</i> No critical habitat has been designated for this species. This species only needs to be considered under the following conditions: <ul style="list-style-type: none"><li>▪ Incidental take of the NLEB is not prohibited here. Federal agencies may consult using the 4(d) rule streamlined process. Transportation projects may consult using the programmatic process. See <a href="http://www.fws.gov/midwest/endangered/mammals/nleb/index.html">www.fws.gov/midwest/endangered/mammals/nleb/index.html</a></li></ul> Species profile: <a href="https://ecos.fws.gov/ecp/species/9045">https://ecos.fws.gov/ecp/species/9045</a> Species survey guidelines: <a href="https://ecos.fws.gov/ipac/guideline/survey/population/10043/office/31440.pdf">https://ecos.fws.gov/ipac/guideline/survey/population/10043/office/31440.pdf</a>	Threatened

## Critical habitats

THERE ARE NO CRITICAL HABITATS WITHIN YOUR PROJECT AREA UNDER THIS OFFICE'S JURISDICTION.



# United States Department of the Interior



## FISH AND WILDLIFE SERVICE

Indiana Ecological Services Field Office

620 South Walker Street

Bloomington, IN 47403-2121

Phone: (812) 334-4261 Fax: (812) 334-4273

<http://www.fws.gov/midwest/Endangered/section7/s7process/step1.html>

In Reply Refer To:

March 15, 2019

Consultation Code: 03E12000-2019-I-0357

Event Code: 03E12000-2019-E-03345

Project Name: US 36 Added Travel Lanes in the Town of Avon, Hendricks and Marion Counties, Indiana (Des. 1601072)

Subject: Concurrence verification letter for the 'US 36 Added Travel Lanes in the Town of Avon, Hendricks and Marion Counties, Indiana (Des. 1601072)' project under the revised February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion for Transportation Projects within the Range of the Indiana Bat and Northern Long-eared Bat.

To whom it may concern:

The U.S. Fish and Wildlife Service (Service) has received your request dated to verify that the **US 36 Added Travel Lanes in the Town of Avon, Hendricks and Marion Counties, Indiana (Des. 1601072)** (Proposed Action) may rely on the concurrence provided in the February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion for Transportation Projects within the Range of the Indiana Bat and Northern Long-eared Bat (PBO) to satisfy requirements under Section 7(a)(2) of the Endangered Species Act of 1973 (ESA) (87 Stat. 884, as amended; 16 U.S.C 1531 *et seq.*).

Based on the information you provided (Project Description shown below), you have determined that the Proposed Action is within the scope and adheres to the criteria of the PBO, including the adoption of applicable avoidance and minimization measures, may affect, but is not likely to adversely affect (NLAA) the endangered Indiana bat (*Myotis sodalis*) and/or the threatened Northern long-eared bat (*Myotis septentrionalis*).

The Service has 14 calendar days to notify the lead Federal action agency or designated non-federal representative if we determine that the Proposed Action does not meet the criteria for a NLAA determination under the PBO. If we do not notify the lead Federal action agency or designated non-federal representative within that timeframe, you may proceed with the Proposed Action under the terms of the NLAA concurrence provided in the PBO. This verification period allows Service Field Offices to apply local knowledge to implementation of the PBO, as we may

identify a small subset of actions having impacts that were unanticipated. In such instances, Service Field Offices may request additional information that is necessary to verify inclusion of the proposed action under the PBO.

**For Proposed Actions that include bridge/structure removal, replacement, and/or maintenance activities:** If your initial bridge/structure assessments failed to detect Indiana bats, but you later detect bats during construction, please submit the Post Assessment Discovery of Bats at Bridge/Structure Form (User Guide Appendix E) to this Service Office. In these instances, potential incidental take of Indiana bats may be exempted provided that the take is reported to the Service.

If the Proposed Action is modified, or new information reveals that it may affect the Indiana bat and/or Northern long-eared bat in a manner or to an extent not considered in the PBO, further review to conclude the requirements of ESA Section 7(a)(2) may be required. If the Proposed Action may affect any other federally-listed or proposed species, and/or any designated critical habitat, additional consultation is required. If the proposed action has the potential to take bald or golden eagles, additional coordination with the Service under the Bald and Golden Eagle Protection Act may also be required. In either of these circumstances, please contact this Service Office.

## Project Description

The following project name and description was collected in IPaC as part of the endangered species review process.

### Name

US 36 Added Travel Lanes in the Town of Avon, Hendricks and Marion Counties, Indiana (Des. 1601072)

### Description

The FHWA and the INDOT Crawfordsville District propose to proceed with an added travel lane project located on US 36 through the Town of Avon in Hendricks and Marion Counties, Indiana (Des. Number 1601072). The project begins at Shiloh Park Drive, continues east for approximately 1.1 miles and ends approximately 1,500 feet east of Raceway Road. Within the project area, US 36 consists of two 12-foot wide travel lanes, one 16-foot wide two-way-left-turn-lane and up to a maximum 11-foot wide paved shoulder for east and westbound directions. A discontinuous right-turn lane is present within portions of the project area. The project will involve milling, resurfacing and widening of the existing roadway. The proposed cross section will consist of one 11-foot wide travel lane and two continuous 12-foot wide travel lanes with a discontinuous 11-foot wide right-turn-lane bordered by curb and gutter with a 2-foot offset for east and westbound directions. A 11-foot wide left-turn-lane will be provided for at each signalized intersection for east and westbound directions. An at-grade pedestrian crossing, and median pedestrian refuge will be installed to connect the existing trail located north of US 36, along the west side of Ronald Reagan Parkway, to the proposed trail located south of US 36, along the west. The existing horizontal alignment of US 36 will remain unchanged. The profile grade will match the existing profile grade. Where needed, existing driveway/approach pipe culverts drainage will be replaced, some structures will have a larger diameter. Curb turnouts with ditches and driveway/approach pipe culverts are proposed to handle drainage between Shiloh Park Drive and Raceway Road.

Approximately 0.25 acre of permanent and 0.75 acre of temporary right-of-way is anticipated. Construction is anticipated to begin in 2021 and last for nine months. There is suitable habitat near the project area; however, no tree clearing is required. Per the Red Flag Investigation, dated November 30, 2018, a review of the USFWS Database by INDOT Crawfordsville District did not indicate the presence of endangered bat species in or within 0.5 mile of the project area. The December 24, 2015 INDOT Bridge Inspection Report for CV 036-032-64.80 contains no information about whether bats are present or absent in the culvert. Per a site investigation on February 12, 2019, seven structures were inspected for evidence of bats and no evidence of bats was observed. Temporary lighting is likely to be used during construction, and permanent lighting is not required.



# Determination Key Result

Based on your answers provided, this project(s) may affect, but is not likely to adversely affect the endangered Indiana bat and/or the threatened Northern long-eared bat. Therefore, consultation with the U.S. Fish and Wildlife Service pursuant to Section 7(a)(2) of the Endangered Species Act of 1973 (ESA) (87 Stat. 884, as amended 16 U.S.C. 1531 *et seq.*) is required. However, also based on your answers provided, this project may rely on the concurrence provided in the revised February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion for Transportation Projects within the Range of the Indiana Bat and Northern Long-eared Bat.

## Qualification Interview

1. Is the project within the range of the Indiana bat<sup>[1]</sup>?

[1] See [Indiana bat species profile](#)

**Automatically answered**

*Yes*

2. Is the project within the range of the Northern long-eared bat<sup>[1]</sup>?

[1] See [Northern long-eared bat species profile](#)

**Automatically answered**

*Yes*

3. Which Federal Agency is the lead for the action?

*A) Federal Highway Administration (FHWA)*

4. Are *all* project activities limited to non-construction<sup>[1]</sup> activities only? (examples of non-construction activities include: bridge/abandoned structure assessments, surveys, planning and technical studies, property inspections, and property sales)

[1] Construction refers to activities involving ground disturbance, percussive noise, and/or lighting.

*No*

5. Does the project include *any* activities that are **greater than 300** feet from existing road/rail surfaces<sup>[1]</sup>?

[1] Road surface is defined as the actively used [e.g. motorized vehicles] driving surface and shoulders [may be pavement, gravel, etc.] and rail surface is defined as the edge of the actively used rail ballast.

*No*

6. Does the project include *any* activities **within** 0.5 miles of a known Indiana bat and/or NLEB hibernaculum<sup>[1]</sup>?

[1] For the purpose of this consultation, a hibernaculum is a site, most often a cave or mine, where bats hibernate during the winter (see suitable habitat), but could also include bridges and structures if bats are found to be hibernating there during the winter.

*No*

7. Is the project located **within** a karst area?

*No*

8. Is there *any* suitable<sup>[1]</sup> summer habitat for Indiana Bat or NLEB **within** the project action area<sup>[2]</sup>? (includes any trees suitable for maternity, roosting, foraging, or travelling habitat)

[1] See the Service's [summer survey guidance](#) for our current definitions of suitable habitat.

[2] The action area is defined as all areas to be affected directly or indirectly by the Federal action and not merely the immediate area involved in the action (50 CFR Section 402.02). Further clarification is provided by the [national consultation FAQs](#).

*Yes*

9. Will the project remove *any* suitable summer habitat<sup>[1]</sup> and/or remove/trim any existing trees **within** suitable summer habitat?

[1] See the Service's [summer survey guidance](#) for our current definitions of suitable habitat.

*No*

10. Does the project include activities **within documented Indiana bat habitat**<sup>[1][2]</sup>?

[1] Documented roosting or foraging habitat – for the purposes of this consultation, we are considering documented habitat as that where Indiana bats and/or NLEB have actually been captured and tracked using (1) radio telemetry to roosts; (2) radio telemetry biangulation/triangulation to estimate foraging areas; or (3) foraging areas with repeated use documented using acoustics. Documented roosting habitat is also considered as suitable summer habitat within 0.25 miles of documented roosts.)

[2] For the purposes of this key, we are considering documented corridors as that where Indiana bats and/or NLEB have actually been captured and tracked to using (1) radio telemetry; or (2) treed corridors located directly between documented roosting and foraging habitat.

*No*

11. Does the project include activities **within documented NLEB habitat**<sup>[1][2]</sup>?

[1] Documented roosting or foraging habitat – for the purposes of this consultation, we are considering documented habitat as that where Indiana bats and/or NLEB have actually been captured and tracked using (1) radio telemetry to roosts; (2) radio telemetry biangulation/triangulation to estimate foraging areas; or (3) foraging areas with repeated use documented using acoustics. Documented roosting habitat is also considered as suitable summer habitat within 0.25 miles of documented roosts.)

[2] For the purposes of this key, we are considering documented corridors as that where Indiana bats and/or NLEB have actually been captured and tracked to using (1) radio telemetry; or (2) treed corridors located directly between documented roosting and foraging habitat.

*No*

12. Does the project include maintenance of the surrounding landscape at existing facilities (e.g., rest areas, stormwater detention basins)?

*No*

13. Does the project include wetland or stream protection activities associated with compensatory wetland mitigation?

*No*

14. Does the project include slash pile burning?

*No*

15. Does the project include *any* bridge removal, replacement, and/or maintenance activities (e.g., any bridge repair, retrofit, maintenance, and/or rehabilitation work)?

*Yes*

16. Is there *any* suitable habitat<sup>[1]</sup> for Indiana bat or NLEB **within** 1,000 feet of the bridge? (includes any trees suitable for maternity, roosting, foraging, or travelling habitat)

[1] See the Service's current [summer survey guidance](#) for our current definitions of suitable habitat.

*Yes*

17. Has a bridge assessment<sup>[1]</sup> been conducted **within** the last 24 months<sup>[2]</sup> to determine if the bridge is being used by bats?

[1] See [User Guide Appendix D](#) for bridge/structure assessment guidance

[2] Assessments must be completed no more than 2 years prior to conducting any work below the deck surface on all bridges that meet the physical characteristics described in the Programmatic Consultation, regardless of whether assessments have been conducted in the past. Due to the transitory nature of bat use, a negative result in one year does not guarantee that bats will not use that bridge/structure in subsequent years.

*Yes*

#### **SUBMITTED DOCUMENTS**

- *Structure Assessment Form Table Photos for all Structures.pdf* <https://ecos.fws.gov/ipac/project/RDDXJHHXFZFDRC4KBVRGKDXAJY/projectDocuments/15800054>

18. Did the bridge assessment detect *any* signs of Indiana bats and/or NLEBs roosting in/under the bridge (bats, guano, etc.)<sup>[1]</sup>?

[1] If bridge assessment detects signs of *any* species of bats, coordination with the local FWS office is needed to identify potential threatened or endangered bat species. Additional studies may be undertaken to try to identify which bat species may be utilizing the bridge prior to allowing *any* work to proceed.

Note: There is a small chance bridge assessments for bat occupancy do not detect bats. Should a small number of bats be observed roosting on a bridge just prior to or during construction, such that take is likely to occur or does occur in the form of harassment, injury or death, the PBO requires the action agency to report the take. Report all unanticipated take within 2 working days of the incident to the USFWS. Construction activities may continue without delay provided the take is reported to the USFWS and is limited to 5 bats per project.

*No*

19. Will the bridge removal, replacement, and/or maintenance activities include installing new or replacing existing **permanent** lighting?

*No*

20. Does the project include the removal, replacement, and/or maintenance of *any* structure other than a bridge? (e.g., rest areas, offices, sheds, outbuildings, barns, parking garages, etc.)

*No*

21. Will the project involve the use of **temporary** lighting *during* the active season?

*Yes*

22. Is there *any* suitable habitat **within** 1,000 feet of the location(s) where **temporary** lighting will be used?

*Yes*

23. Will the project install new or replace existing **permanent** lighting?

*No*

24. Does the project include percussives or other activities (**not including tree removal/trimming or bridge/structure work**) that will increase noise levels above existing traffic/background levels?

*No*

25. Are *all* project activities that are **not associated with** habitat removal, tree removal/trimming, bridge and/or structure activities, temporary or permanent lighting, or use of percussives, limited to actions that DO NOT cause any additional stressors to the bat species?

Examples: lining roadways, unlighted signage , rail road crossing signals, signal lighting, and minor road repair such as asphalt fill of potholes, etc.

*Yes*

26. Will the project raise the road profile **above the tree canopy**?

*No*

27. Are the project activities that are not associated with habitat removal, tree removal/trimming, bridge and/or structure activities, temporary or permanent lighting, or use of percussives consistent with a No Effect determination in this key?

**Automatically answered**

*Yes, other project activities are limited to actions that DO NOT cause any additional stressors to the bat species as described in the BA/BO*

28. Is the bridge removal, replacement, or maintenance activities portion of this project consistent with a No Effect determination in this key?

**Automatically answered**

*Yes, because the bridge has been assessed using the criteria documented in the BA and no signs of bats were detected*



29. **General AMM 1**

Will the project ensure *all* operators, employees, and contractors working in areas of known or presumed bat habitat are aware of *all* FHWA/FRA/FTA (Transportation Agencies) environmental commitments, including all applicable Avoidance and Minimization Measures?

*Yes*

30. **Lighting AMM 1**

Will *all* **temporary** lighting be directed away from suitable habitat during the active season?

*Yes*

## Project Questionnaire

1. Have you made a No Effect determination for *all* other species indicated on the FWS IPaC generated species list?

*Yes*

2. Have you made a May Affect determination for *any* other species on the FWS IPaC generated species list?

*No*

3. Please describe the proposed bridge work:

*The preferred alternative will involve milling, resurfacing and widening of the existing roadway. The proposed cross section will consist of one 11-foot wide travel lane and two continuous 12-foot wide travel lanes with a discontinuous 11-foot wide right-turn-lane bordered by curb and gutter with a 2-foot offset for east and westbound directions. A 11-foot wide left-turn-lane will be provided for at each signalized intersection for east and westbound directions. An at-grade pedestrian crossing, and median pedestrian refuge will be installed to connect the existing trail located north of US 36, along the west side of Ronald Reagan Parkway, to the proposed trail located south of US 36, along the west*

*The existing horizontal alignment of US 36 will remain unchanged. The profile grade will match the existing profile grade. Where needed, existing driveway/approach pipe culverts drainage will be replaced, some structures will have a larger diameter. Curb turnouts with ditches and driveway/approach pipe culverts are proposed to handle drainage between Shiloh Park Drive and Raceway Road.*

4. Please state the timing of all proposed bridge work:

*Construction is anticipated to begin in 2021 - no bridge work within the project area;  
ONLY maintenance to existing pipes and culverts within the project area as listed within  
the attached Assessment Form Table*

5. Please enter the date of the bridge assessment:

*May 25, 2018 and February 12, 2019*

## **Avoidance And Minimization Measures (AMMs)**

This determination key result includes the commitment to implement the following Avoidance and Minimization Measures (AMMs):

### **GENERAL AMM 1**

Ensure all operators, employees, and contractors working in areas of known or presumed bat habitat are aware of all FHWA/FRA/FTA (Transportation Agencies) environmental commitments, including all applicable AMMs.

### **LIGHTING AMM 1**

Direct temporary lighting away from suitable habitat during the active season.

## **Determination Key Description: FHWA, FRA, FTA Programmatic Consultation For Transportation Projects Affecting NLEB Or Indiana Bat**

This key was last updated in IPaC on March 16, 2018. Keys are subject to periodic revision.

This decision key is intended for projects/activities funded or authorized by the Federal Highway Administration (FHWA), Federal Railroad Administration (FRA), and/or Federal Transit Administration (FTA), which require consultation with the U.S. Fish and Wildlife Service (Service) under Section 7 of the Endangered Species Act (ESA) for the endangered **Indiana bat** (*Myotis sodalis*) and the threatened **Northern long-eared bat** (NLEB) (*Myotis septentrionalis*).

This decision key should only be used to verify project applicability with the Service's [February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion for Transportation Projects](#). The programmatic biological opinion covers limited transportation activities that may affect either bat species, and addresses situations that are both likely and not likely to adversely affect either bat species. This decision key will assist in identifying the effect of a specific project/activity and applicability of the programmatic consultation. The programmatic biological opinion is not intended to cover all types of transportation actions. Activities outside the scope of the programmatic biological opinion, or that may affect ESA-listed species other than the Indiana bat or NLEB, or any designated critical habitat, may require additional ESA Section 7 consultation.

June 27, 2019

«Agency\_1»

«Agency\_2»

«Address\_1»

«Address\_2»«

City», «State» «Zip» **Example Re-coordination Letter**

Re: Agencies Re-coordination Letter  
Des. Numbers 1601072, 1901655, and 1901657  
US 36 Added Travel Lane Project  
Avon, Hendricks and Marion Counties, Indiana

Dear «Position»,

The Federal Highway Administration (FHWA) and the Indiana Department of Transportation (INDOT) Crawfordsville District propose to proceed with an added travel lane project located on US 36 through the Town of Avon in Hendricks and Marion Counties, Indiana (Des. Number 1601072, 1901655, and 1901657). The FHWA is providing funds and is designated as the lead Federal agency.

Coordination regarding this project was initiated with your agency by RQAW on January 28, 2019; however, slight changes in the project design and construction limits have occurred since that time. This letter is part of the re-coordination phase of the environmental review process. Please also note that two additional Des. Numbers (1901655 and 1901657) were added to this project since coordination was originally initiated. Des. Number 1901655 involves installing new traffic signals within the project area and Des. Number 1901657 involves lengthening a pipe that carries Avon Creek under US 36 (Structure CV-036-032-64.80).

The project begins at Shiloh Park Drive, continues east for approximately 1.1 miles and ends approximately 1,500 feet east of Raceway Road. The project is in Washington Township (Hendricks County), Wayne Township (Marion County), Clermont U.S. Geological Survey (USGS) Quadrangle, Township 15 North, Range 2 East, and Sections 4, 5, 8 and 9. The project area is in a developed area surrounded by residential and commercial properties.

**Proposed Conditions as Described in the Previous Coordination Letter (January 28, 2019):** The preferred alternative will involve milling, resurfacing and widening of the existing roadway. The proposed cross section will consist of one 11-foot wide travel lane and two continuous 12-foot wide travel lanes with a discontinuous 11-foot wide right-turn-lane bordered by curb and gutter with a 2-foot offset for east and westbound directions. A 11-foot wide left-turn-lane will be provided at each signalized intersection for east and westbound directions. An at-grade pedestrian crossing, and median pedestrian refuge will be installed to connect the existing trail located north of US 36, along the west side of Ronald Reagan Parkway, to the proposed trail located south of US 36, along the west side of Ronald Reagan Parkway (Des. Number 1601121). Des. Number 1601121 will be constructed prior to this added travel lanes project (Des. Number 1601072). Des. Number 1601121 will terminate just south of the added travel lanes project; the added travel lanes project will fill in this gap to provide connectivity of the trail along both sides of US 36.

The existing horizontal alignment of US 36 will remain unchanged. The profile grade will match the existing profile grade. Where needed, existing driveway/approach pipe culverts drainage will be replaced, some structures will have a larger diameter. Curb turnouts with ditches and driveway/approach pipe culverts are proposed to handle drainage between Shiloh Park Drive and Raceway Road. Between Raceway Road and the end of the project, an enclosed drainage system is proposed to match the existing drainage system; some inlets may be moved farther from the road in this section. Work to the CMP east of the US 36/Ronald Reagan Parkway Intersection will involve extending the length of the pipe approximately six feet north of US 36 and approximately 15 feet south of US 36. Work to

Structure CV 036-032-64.80 will involve extending the length of the pipe approximately 10 feet north of US 36 and approximately 22 feet south of US 36. Work to the structure that conveys Avon Creek under Shiloh Crossing Drive may involve work to the southern wingwalls.

Construction will be phased, and two lanes of traffic will be maintained in each direction. Access to all properties will be maintained during construction. Most of the construction is anticipated to occur within the existing right-of-way. Approximately 0.25 acre of permanent and 0.75 acre of temporary right-of-way is anticipated to be required. The maximum depth of excavation is anticipated to be approximately eight feet below ground surface. Construction is anticipated to begin in 2021 and last for nine months.

**Environmental Resources as Described in the Previous Coordination Letter (January 28, 2019):** To identify potential environmental concerns within the project vicinity, a Red Flag Investigation was performed for a 0.5-mile radius of the project area by RQAW. The Red Flag Investigation noted:

- Rainbow Acres Church of God is adjacent to the project area.
- Speedway Public Airport is located within 3.8 miles (20,000 feet) of the project area.
- Two trail segments transect the project area on the west side of Ronald Reagan Parkway. One trail segment abuts the north project area along the east side of Ronald Reagan Parkway. All trail segments are part of the Ronald Reagan Parkway Corridor.
- Avon Creek is within the project area and is listed as impaired for *Escherichia coli* (E. coli).
- The project area is within an Urbanized Area Boundary.
- Hazardous material concerns are mapped within and/or adjacent to the project area. Two gas stations are adjacent to the project area. A home furnishings store is adjacent to the project area and is mapped as a National Pollutant Discharge Elimination System (NPDES) facility. A car detailing store is adjacent to the project area and is mapped as a state cleanup site.

RQAW performed site visits on May 25, 2018 and August 30, 2018 to identify any ecological resources present. Avon Creek and two unnamed tributaries (UNTs) of Avon Creek were observed within the project area. Six wetlands were also delineated within the project area. Tree clearing will not be needed.

The project qualifies for the application of the U.S. Fish and Wildlife Service (USFWS) range-wide programmatic informal consultation for the Indiana bat and northern long-eared bat. Project information was provided to the USFWS separately. RQAW investigated the Area of Potential Effect (APE) for archaeological and historic resources for compliance with Section 106 and the project will result in a finding of *No Adverse Effect*.

**Proposed Project Changes:** Changes to the proposed project design and scope of work are discussed below. Unless specifically mentioned, the remaining project scope of work will remain the same as originally proposed. The construction limits have been extended approximately 405 feet east of the eastern project area. This change is needed to replace and extend an existing 30-inch diameter CMP located within the expanded construction limits. The existing CMP transports a roadside ditch located just west of the UNT to Shiloh Creek on the north side of US 36. Work to the structure includes replacing it with a 36-inch diameter CMP and extending it 44 feet to the northeast to redirect the flow of the roadside ditch into the UNT of Shiloh Creek. Please see attached map showing the modified project area.

**Environmental Resources:** The expanded construction limits will result in impacts to one additional stream, an UNT to Shiloh Creek, for extension of the existing CMP and installing riprap. This will impact up to approximately 20 linear feet (0.0009 acre) of UNT of Shiloh Creek. This will not result in additional permanent or temporary right-of-way.

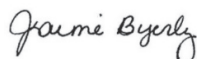
A supplemental review of the Geographic Information System (GIS) layers used in the previously completed Red Flag Investigation was conducted by RQAW on June 10, 2019 to identify additional environmental resources near the project area. There are no additional environmental resources mapped within the vicinity of the project.



Regarding Section 106, the expanded construction limits are within the APE for archaeological and above-ground resources. No additional coordination is necessary.

If you have any questions regarding this matter, please contact Jaime Byerly of the Environmental Department at RQAW, at 317.588.1798 or at [jbyerly@rqaw.com](mailto:jbyerly@rqaw.com), or the INDOT Project Manager, Richard Gilyeat, at 765.361.5684, or at [rgilyeat@indot.in.gov](mailto:rgilyeat@indot.in.gov). Thank you in advance.

Sincerely,



Jaime Byerly  
NEPA Specialist  
RQAW | Environmental Department

Attachment: Aerial map showing updated construction limits

Removed to avoid duplication. See graphic in Appendix A of the EA document.

Cc:

- Indiana Department of Natural Resources (IDNR) Division of Fish and Wildlife (electronic coordination)
- Indiana Department of Environmental Management (IDEM) (electronic coordination)
- U.S. Army Corps of Engineers, Louisville District (electronic coordination)
- Hendricks County Surveyor
- Marion County Surveyor (electronic coordination)



# Indiana Department of Environmental Management

*We Protect Hoosiers and Our Environment.*

100 North Senate Avenue - Indianapolis, IN 46204  
(800) 451-6027 - (317) 232-8603 - [www.idem.IN.gov](http://www.idem.IN.gov)

RQAW  
Lisa Casler  
8770 North Street  
Suite 110  
Fishers , IN 46038  
Date

RQAW  
Jaime Byerly  
8770 North Street  
Suite 110  
Fishers , IN 46038

To Engineers and Consultants Proposing Roadway Construction Projects:

RE: Early coordination was initiated with IDEM on January 28, 2019. Since that time, construction limits were expanded east and will result in impacts to one additional stream, an UNT to Shiloh Creek. The impacts are due to the extension of the existing CMP and installing riprap within the stream channel. This will impact up to approximately 20 linear feet (0.0009 acre) of UNT of Shiloh Creek. This information is for your project records.

This letter from the Indiana Department of Environmental Management (IDEM) serves as a standardized response to enquiries inviting IDEM comments on roadway construction, reconstruction, or other improvement projects within existing roadway corridors when the proposed scope of the project is beneath the threshold requiring a formal National Environmental Policy Act-mandated Environmental Assessment or Environmental Impact Statement. As the letter attempts to address all roadway-related environmental topics of potential concern, it is possible that not every topic addressed in the letter will be applicable to your particular roadway project.

For additional information on specific roadway-related topics of interest, please visit the appropriate Web pages cited below, many of which provide contact information for persons within the various program areas who can answer questions not fully addressed in this letter. Also please be mindful that some environmental requirements may be subject to change and so each person intending to include a copy of this letter in their project documentation packet is advised to download the most recently revised version of the letter; found at: <http://www.in.gov/idem/5283.htm> (<http://www.in.gov/idem/5283.htm>).

To ensure that all environmentally-related issues are adequately addressed, IDEM recommends that you read this letter in its entirety, and consider each of the following issues as you move forward with the planning of your proposed roadway construction, reconstruction, or improvement project:

# WATER AND BIOTIC QUALITY

1. Section 404 of the Clean Water Act requires that you obtain a permit from the U.S. Army Corps of Engineers (USACE) before discharging dredged or fill materials into any wetlands or other waters, such as rivers, lakes, streams, and ditches. Other activities regulated include the relocation, channelization, widening, or other such alteration of a stream, and the mechanical clearing (use of heavy construction equipment) of wetlands. Thus, as a project owner or sponsor, it is your responsibility to ensure that no wetlands are disturbed without the proper permit. Although you may initially refer to the U.S. Fish and Wildlife Service National Wetland Inventory maps as a means of identifying potential areas of concern, please be mindful that those maps do not depict jurisdictional wetlands regulated by the USACE or the Department of Environmental Management. A valid jurisdictional wetlands determination can only be made by the USACE, using the 1987 Wetland Delineation Manual.

USACE recommends that you have a consultant check to determine whether your project will abut, or lie within, a wetland area. To view a list of consultants that have requested to be included on a list posted by the USACE on their Web site, see USACE Permits and Public Notices (<http://www.lrl.usace.army.mil/orf/default.asp>) (<http://www.lrl.usace.army.mil/orf/default.asp>) and then click on "Information" from the menu on the right-hand side of that page. Their "Consultant List" is the fourth entry down on the "Information" page. Please note that the USACE posts all consultants that request to appear on the list, and that inclusion of any particular consultant on the list does not represent an endorsement of that consultant by the USACE, or by IDEM.

Much of northern Indiana (Newton, Lake, Porter, LaPorte, St. Joseph, Elkhart, LaGrange, Steuben, and Dekalb counties; large portions of Jasper, Starke, Marshall, Noble, Allen, and Adams counties; and lesser portions of Benton, White, Pulaski, Kosciusko, and Wells counties) is served by the USACE District Office in Detroit (313-226-6812). The central and southern portions of the state (large portions of Benton, White, Pulaski, Kosciusko, and Wells counties; smaller portions of Jasper, Starke, Marshall, Noble, Allen, and Adams counties; and all other Indiana counties located in north-central, central, and southern Indiana ) are served by the USACE Louisville District Office (502-315-6733).

Additional information on contacting these U.S. Army Corps of Engineers (USACE) District Offices, government agencies with jurisdiction over wetlands, and other water quality issues, can be found at <http://www.in.gov/idem/4396.htm> (<http://www.in.gov/idem/4396.htm>). IDEM recommends that impacts to wetlands and other water resources be avoided to the fullest extent.

2. In the event a Section 404 wetlands permit is required from the USACE, you also must obtain a Section 401 Water Quality Certification from the IDEM Office of Water Quality Wetlands Program. To learn more about the Wetlands Program, visit: <http://www.in.gov/idem/4384.htm> (<http://www.in.gov/idem/4384.htm>).
3. If the USACE determines that a wetland or other water body is isolated and not subject to Clean Water Act regulation, it is still regulated by the state of Indiana . A State Isolated Wetland permit from IDEM's Office of Water Quality (OWQ) is required for any activity that results in the

discharge of dredged or fill materials into isolated wetlands. To learn more about isolated wetlands, contact the OWQ Wetlands Program at 317-233-8488.

4. If your project will involve over a 0.5 acre of wetland impact, stream relocation, or other large-scale alterations to water bodies such as the creation of a dam or a water diversion, you should seek additional input from the OWQ Wetlands Program staff. Consult the Web at: <http://www.in.gov/idem/4384.htm> (<http://www.in.gov/idem/4384.htm>) for the appropriate staff contact to further discuss your project.
5. Work within the one-hundred year floodway of a given water body is regulated by the Department of Natural Resources, Division of Water. The Division issues permits for activities regulated under the follow statutes:
  - IC 14-26-2 Lakes Preservation Act 312 IAC 11
  - IC 14-26-5 Lowering of Ten Acre Lakes Act No related code
  - IC 14-28-1 Flood Control Act 310 IAC 6-1
  - IC 14-29-1 Navigable Waterways Act 312 IAC 6
  - IC 14-29-3 Sand and Gravel Permits Act 312 IAC 6
  - IC 14-29-4 Construction of Channels Act No related code

For information on these Indiana (statutory) Code and Indiana Administrative Code citations, see the DNR Web site at: <http://www.in.gov/dnr/water/9451.htm> (<http://www.in.gov/dnr/water/9451.htm>) . Contact the DNR Division of Water at 317-232-4160 for further information.

The physical disturbance of the stream and riparian vegetation, especially large trees overhanging any affected water bodies should be limited to only that which is absolutely necessary to complete the project. The shade provided by the large overhanging trees helps maintain proper stream temperatures and dissolved oxygen for aquatic life.

6. For projects involving construction activity (which includes clearing, grading, excavation and other land disturbing activities) that result in the disturbance of one (1), or more, acres of total land area, contact the Office of Water Quality – Watershed Planning Branch (317/233-1864) regarding the need for of a Rule 5 Storm Water Runoff Permit. Visit the following Web page
  - <http://www.in.gov/idem/4902.htm> (<http://www.in.gov/idem/4902.htm>)

To obtain, and operate under, a Rule 5 permit you will first need to develop a Construction Plan (<http://www.in.gov/idem/4917.htm#constreq> (<http://www.in.gov/idem/4917.htm#constreq>)), and as described in 327 IAC 15-5-6.5 (<http://www.in.gov/legislative/iac/T03270/A00150> [PDF] (<http://www.in.gov/legislative/iac/T03270/A00150.PDF>), pages 16 through 19). Before you may apply for a Rule 5 Permit, or begin construction, you must submit your Construction Plan to your county Soil and Water Conservation District (SWCD) (<http://www.in.gov/isda/soil/contacts/map.html> (<http://www.in.gov/isda/soil/contacts/map.html>)).

Upon receipt of the construction plan, personnel of the SWCD or the Indiana Department of Environmental Management will review the plan to determine if it meets the requirements of 327 IAC 15-5. Plans that are deemed deficient will require re-submittal. If the plan is sufficient you will be notified and instructed to submit the verification to IDEM as part of the Rule 5 Notice of Intent

(NOI) submittal. Once construction begins, staff of the SWCD or Indiana Department of Environmental Management will perform inspections of activities at the site for compliance with the regulation.

Please be mindful that approximately 149 Municipal Separate Storm Sewer System (MS4) areas are now being established by various local governmental entities throughout the state as part of the implementation of Phase II federal storm water requirements. All of these MS4 areas will eventually take responsibility for Construction Plan review, inspection, and enforcement. As these MS4 areas obtain program approval from IDEM, they will be added to a list of MS4 areas posted on the IDEM Website at: <http://www.in.gov/idem/4900.htm> (<http://www.in.gov/idem/4900.htm>).

If your project is located in an IDEM-approved MS4 area, please contact the local MS4 program about meeting their storm water requirements. Once the MS4 approves the plan, the NOI can be submitted to IDEM.

Regardless of the size of your project, or which agency you work with to meet storm water requirements, IDEM recommends that appropriate structures and techniques be utilized both during the construction phase, and after completion of the project, to minimize the impacts associated with storm water runoff. The use of appropriate planning and site development and appropriate storm water quality measures are recommended to prevent soil from leaving the construction site during active land disturbance and for post construction water quality concerns. Information and assistance regarding storm water related to construction activities are available from the Soil and Water Conservation District (SWCD) offices in each county or from IDEM.

7. For projects involving impacts to fish and botanical resources, contact the Department of Natural Resources - Division of Fish and Wildlife (317/232-4080) for addition project input.
8. For projects involving water main construction, water main extensions, and new public water supplies, contact the Office of Water Quality - Drinking Water Branch (317-308-3299) regarding the need for permits.
9. For projects involving effluent discharges to waters of the State of Indiana , contact the Office of Water Quality - Permits Branch (317-233-0468) regarding the need for a National Pollutant Discharge Elimination System (NPDES) permit.
10. For projects involving the construction of wastewater facilities and sewer lines, contact the Office of Water Quality - Permits Branch (317-232-8675) regarding the need for permits.

## **AIR QUALITY**

The above-noted project should be designed to minimize any impact on ambient air quality in, or near, the project area. The project must comply with all federal and state air pollution regulations.

Consideration should be given to the following:

1. Regarding open burning, and disposing of organic debris generated by land clearing activities; some types of open burning are allowed (<http://www.in.gov/idem/4148.htm> (<http://www.in.gov/idem/4148.htm>)) under specific conditions. You also can seek an open burning variance from IDEM.



However, IDEM generally recommends that you take vegetative wastes to a registered yard waste composting facility or that the waste be chipped or shredded with composting on site (you must register with IDEM if more than 2,000 pounds is to be composted; contact 317/232-0066). The finished compost can then be used as a mulch or soil amendment. You also may bury any vegetative wastes (such as leaves, twigs, branches, limbs, tree trunks and stumps) onsite, although burying large quantities of such material can lead to subsidence problems, later on.

Reasonable precautions must be taken to minimize fugitive dust emissions from construction and demolition activities. For example, wetting the area with water, constructing wind barriers, or treating dusty areas with chemical stabilizers (such as calcium chloride or several other commercial products). Dirt tracked onto paved roads from unpaved areas should be minimized.

Additionally, if construction or demolition is conducted in a wooded area where blackbirds have roosted or abandoned buildings or building sections in which pigeons or bats have roosted for 3-5 years precautionary measures should be taken to avoid an outbreak of histoplasmosis. This disease is caused by the fungus *Histoplasma capsulatum*, which stems from bird or bat droppings that have accumulated in one area for 3-5 years. The spores from this fungus become airborne when the area is disturbed and can cause infections over an entire community downwind of the site. The area should be wetted down prior to cleanup or demolition of the project site. For more detailed information on histoplasmosis prevention and control, please contact the Acute Disease Control Division of the Indiana State Department of Health at (317) 233-7272.

2. The U.S. EPA and the Surgeon General recommend that people not have long-term exposure to radon at levels above 4 pCi/L. (For a county-by-county map of predicted radon levels in Indiana, visit: <http://www.in.gov/idem/4145.htm> (<http://www.in.gov/idem/4145.htm>.)

The U.S. EPA further recommends that all homes (and apartments within three stories of ground level) be tested for radon. If in-home radon levels are determined to be 4 pCi/L, or higher, EPA recommends a follow-up test. If the second test confirms that radon levels are 4 pCi/L, or higher, EPA recommends the installation of radon-reduction measures. (For a list of qualified radon testers and radon mitigation (or reduction) specialists visit:

[http://www.in.gov/isdh/regsvcs/radhealth/pdfs/radon\\_testers\\_mitigators\\_list.pdf](http://www.in.gov/isdh/regsvcs/radhealth/pdfs/radon_testers_mitigators_list.pdf) ([http://www.in.gov/isdh/regsvcs/radhealth/pdfs/radon\\_testers\\_mitigators\\_list.pdf](http://www.in.gov/isdh/regsvcs/radhealth/pdfs/radon_testers_mitigators_list.pdf).) It also is recommended that radon reduction measures be built into all new homes, particularly in areas like Indiana that have moderate to high predicted radon levels.

To learn more about radon, radon risks, and ways to reduce exposure visit:

<http://www.in.gov/isdh/regsvcs/radhealth/radon.htm> (<http://www.in.gov/isdh/regsvcs/radhealth/radon.htm>), <http://www.in.gov/idem/4145.htm> (<http://www.in.gov/idem/4145.htm>), or <http://www.epa.gov/radon/index.html> (<http://www.epa.gov/radon/index.html>).

3. With respect to asbestos removal: all facilities slated for renovation or demolition (except residential buildings that have (4) four or fewer dwelling units and which will not be used for commercial purposes) must be inspected by an Indiana-licensed asbestos inspector prior to the commencement of any renovation or demolition activities. If regulated asbestos-containing

material (RACM) that may become airborne is found, any subsequent demolition, renovation, or asbestos removal activities must be performed in accordance with the proper notification and emission control requirements.

If no asbestos is found where a renovation activity will occur, or if the renovation involves removal of less than 260 linear feet of RACM off of pipes, less than 160 square feet of RACM off of other facility components, or less than 35 cubic feet of RACM off of all facility components, the owner or operator of the project does not need to notify IDEM before beginning the renovation activity.

For questions on asbestos demolition and renovation activities, you can also call IDEM's Lead/Asbestos section at 1-888-574-8150.

However, in all cases where a demolition activity will occur (even if no asbestos is found), the owner or operator must still notify IDEM 10 working days prior to the demolition, using the form found at <http://www.in.gov/icpr/webfile/formsdiv/44593.pdf> (<http://www.in.gov/icpr/webfile/formsdiv/44593.pdf>).

Anyone submitting a renovation/demolition notification form will be billed a notification fee based upon the amount of friable asbestos containing material to be removed or demolished. Projects that involve the removal of more than 2,600 linear feet of friable asbestos containing materials on pipes, or 1,600 square feet or 400 cubic feet of friable asbestos containing material on other facility components, will be billed a fee of \$150 per project; projects below these amounts will be billed a fee of \$50 per project. All notification remitters will be billed on a quarterly basis.

For more information about IDEM policy regarding asbestos removal and disposal, visit: <http://www.in.gov/idem/4983.htm> (<http://www.in.gov/idem/4983.htm>).

4. With respect to lead-based paint removal: IDEM encourages all efforts to minimize human exposure to lead-based paint chips and dust. IDEM is particularly concerned that young children exposed to lead can suffer from learning disabilities. Although lead-based paint abatement efforts are not mandatory, any abatement that is conducted within housing built before January 1, 1978, or a child-occupied facility is required to comply with all lead-based paint work practice standards, licensing and notification requirements. For more information about lead-based paint removal visit: <http://www.in.gov/isdh/19131.htm> (<http://www.in.gov/isdh/19131.htm>).
5. Ensure that asphalt paving plants are permitted and operate properly. The use of cutback asphalt, or asphalt emulsion containing more than seven percent (7%) oil distillate, is prohibited during the months April through October. See 326 IAC 8-5-2, Asphalt Paving Rule (<http://www.ai.org/legislative/iac/T03260/A00080.PDF> (<http://www.ai.org/legislative/iac/T03260/A00080.PDF>)).
6. If your project involves the construction of a new source of air emissions or the modification of an existing source of air emissions or air pollution control equipment, it will need to be reviewed by the IDEM Office of Air Quality (OAQ). A registration or permit may be required under 326 IAC 2 (View at: [www.ai.org/legislative/iac/t03260/a00020.pdf](http://www.ai.org/legislative/iac/t03260/a00020.pdf) (<http://www.ai.org/legislative/iac/t03260/a00020.pdf>)). New sources that use or emit hazardous

air pollutants may be subject to Section 112 of the Clean Air Act and corresponding state air regulations governing hazardous air pollutants.

7. For more information on air permits visit: <http://www.in.gov/idem/4223.htm> (<http://www.in.gov/idem/4223.htm>), or to initiate the IDEM air permitting process, please contact the Office of Air Quality Permit Reviewer of the Day at (317) 233-0178 or OAMPROD at [atdem.state.in.us](mailto:atdem.state.in.us).

## LAND QUALITY

In order to maintain compliance with all applicable laws regarding contamination and/or proper waste disposal, IDEM recommends that:

1. If the site is found to contain any areas used to dispose of solid or hazardous waste, you need to contact the Office of Land Quality (OLQ) at 317-308-3103.
2. All solid wastes generated by the project, or removed from the project site, need to be taken to a properly permitted solid waste processing or disposal facility. For more information, visit <http://www.in.gov/idem/4998.htm> (<http://www.in.gov/idem/4998.htm>).
3. If any contaminated soils are discovered during this project, they may be subject to disposal as hazardous waste. Please contact the OLQ at 317-308-3103 to obtain information on proper disposal procedures.
4. If PCBs are found at this site, please contact the Industrial Waste Section of OLQ at 317-308-3103 for information regarding management of any PCB wastes from this site.
5. If there are any asbestos disposal issues related to this site, please contact the Industrial Waste Section of OLQ at 317-308-3103 for information regarding the management of asbestos wastes (Asbestos removal is addressed above, under Air Quality).
6. If the project involves the installation or removal of an underground storage tank, or involves contamination from an underground storage tank, you must contact the IDEM Underground Storage Tank program at 317/308-3039. See: <http://www.in.gov/idem/4999.htm> (<http://www.in.gov/idem/4999.htm>).

## FINAL REMARKS

Should you need to obtain any environmental permits in association with this proposed project, please be mindful that IC 13-15-8 requires that you notify all adjoining property owners and/or occupants within ten days your submittal of each permit application. However, if you are seeking multiple permits, you can still meet the notification requirement with a single notice if all required permit applications are submitted with the same ten day period.

Should the scope of the proposed project be expanded to the extent that a National Environmental Policy Act Environmental Assessment (EA) or Environmental Impact Statement (EIS) is required, IDEM will actively participate in any early interagency coordination review of the project.

Meanwhile, please note that this letter does not constitute a permit, license, endorsement or any other form of approval on the part of the Indiana Department of Environmental Management regarding any project for which a copy of this letter is used. Also note that it is the responsibility of the project engineer or consultant using this letter to ensure that the most current draft of this document, which is located at <http://www.in.gov/idem/5284.htm> (<http://www.in.gov/idem/5284.htm>), is used.

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## Signature(s) of the Applicant

I acknowledge that the following proposed roadway project will be financed in part, or in whole, by public monies.

## Project Description

Early coordination was initiated with IDEM on January 28, 2019. Since that time, construction limits were expanded east and will result in impacts to one additional stream, an UNT to Shiloh Creek. The impacts are due to the extension of the existing CMP and installing riprap within the stream channel. This will impact up to approximately 20 linear feet (0.0009 acre) of UNT of Shiloh Creek. This information is for your project records.

With my signature, I do hereby affirm that I have read the letter from the Indiana Department of Environment that appears directly above. In addition, I understand that in order to complete that project in which I am interested, with a minimum of impact to the environment, I must consider all the issues addressed in the aforementioned letter, and further, that I must obtain any required permits.

Date: July 3, 2019

Signature of the INDOT  
Project Engineer or Other Responsible Agent *Lisa A. Casler*

Lisa Casler

Date: June 27, 2019

Signature of the  
For Hire Consultant Jaime Byerly

Jaime Byerly

**Environmental Assessment**

# **Appendix C**

**Cultural Resources**



**US 36 Added Travel Lanes**  
Hendricks & Marion Counties, Indiana  
Des. No.: 1601072  
DHPA No.: 23088




3/15/2019

**Section 106, 800.11(e) Documentation**

Prepared for:

The Federal Highway Administration  
and Crawfordsville District, Indiana  
Department of Transportation

Prepared by:

  
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**FEDERAL HIGHWAY ADMINISTRATION'S  
SECTION 4(F) COMPLIANCE REQUIREMENTS (for historic properties) AND  
SECTION 106 FINDINGS AND DETERMINATIONS  
AREA OF POTENTIAL EFFECTS  
ELIGIBILITY DETERMINATIONS  
EFFECT FINDING  
US 36 Added Travel Lanes  
Hendricks & Marion County, Indiana  
Des. No.: 1601072  
DHPA No.: 23088**

**AREA OF POTENTIAL EFFECTS  
(Pursuant to 36 CFR Section 800.1(a)(1))**

The Area of Potential Effects (APE) for this project is an irregular polygon that generally encompasses those areas adjacent to the proposed work where ground-disturbing activity may occur, areas within a viewshed of the proposed work, and areas where other non-visual direct or indirect effects such as audible effects may occur. Most of the APE extends approximately 700 feet wide and it expands to approximately 900 feet wide where open viewsheds occur. Please see Appendix A for maps of the APE.

**ELIGIBILITY DETERMINATIONS  
(Pursuant to 36 CFR 800.4(c)(2))**

As a result of identification and evaluation efforts for this project, two properties are identified as listed in, eligible for, or potentially eligible for the National Register of Historic Places (NRHP) within the APE for this undertaking.

**Charles McLain House (Indiana Historic Sites and Structures Inventory (IHSSI) # 063-117-45024)**

The Charles McLain House was rated "Notable" in the *Hendricks County Interim Report* (1989). The Charles McLain House is located at 10664 East US 36 in Avon, Hendricks County, Indiana and is a two-and-one-half story Free Classic Queen Ann style house. The Charles McLain House is eligible for the NRHP under Criterion C because it is a good example (although with alterations) of a circa 1904 Free Classic Queen Ann style house with original architectural features in Washington Township, Hendricks County.

**David Faucett House (IHSSI # 097-117-56005)**

The David Faucett House was rated "Notable" in the *Wayne Township, Marion County Interim Report* (1993). The David Faucett House is located at 9055 Rockville Road (US 36) in Marion County, Indiana and is two-story I-house. The David Faucett House is eligible for the NRHP under Criterion A for its association with the early development of Wayne Township, Marion County and Washington Township, Hendricks County; and under Criterion C because it is a good example (although with alterations) of a circa 1846 I-house with Greek Revival architectural features in Wayne Township, Marion County.

## **EFFECT FINDING**

Charles McLain House – No Adverse Effect

David Faucett House – No Adverse Effect

The Indiana Department of Transportation (INDOT), acting on the Federal Highway Administration (FHWA)'s behalf, has determined a "No Adverse Effect" finding is appropriate for this undertaking. INDOT respectfully requests the Indiana State Historic Preservation Officer provide written concurrence with the Section 106 determination of effect.

### **SECTION 4(F) COMPLIANCE REQUIREMENTS (for historic properties)**

**Charles McLain House (IHSSI # 063-117-45024)** – This undertaking will temporarily occupy land from the Charles McLain House, a Section 4(f) historic property. INDOT, acting on FHWA's behalf has determined the appropriate Section 106 finding is "No Adverse Effect". FHWA believes that the temporary occupancy will not constitute a Section 4(f) use because all of the conditions listed in 23 CFR 774.13(d) are satisfied:

1. Duration must be temporary, i.e., less than the time needed for construction of the project, and there should be no change in ownership of the land;
2. Scope of the work must be minor, i.e., both the nature and the magnitude of the changes to the Section 4(f) property are minimal;
3. There are no anticipated permanent adverse physical impacts, nor will there be interference with the protected activities, features, or attributes of the property, on either a temporary or permanent basis;
4. The land being used must be fully restored, i.e., the property must be returned to a condition which is at least as good as that which existed prior to the project; and
5. There must be documented agreement of the official(s) with jurisdiction over the Section 4(f) resource regarding the above conditions.

The fulfillment of conditions 1-4 are detailed in Section 4 of the attached documentation, "Describe the Undertaking's Effects on Historic Properties." With regard to condition 5, FHWA respectfully requests the Indiana State Historic Preservation Officer provide written concurrence that they are in agreement that the above criteria have been met and that the impacts to the Charles McLain House constitute a temporary occupancy.

**David Faucett House (IHSSI # 097-117-56005)** – This undertaking will not convert property from the David Faucett House, a Section 4(f) historic property, to a transportation use; INDOT has determined the appropriate Section 106 finding is "No Adverse Effect"; therefore no Section 4(f) evaluation is required for the David Faucett House.

*Anuradha V. Kumar*

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Anuradha V. Kumar, for FHWA

Manager

INDOT Cultural Resources

03/18/2019

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Approved Date

**FEDERAL HIGHWAY ADMINISTRATION  
DOCUMENTATION OF SECTION 106 FINDING OF  
NO ADVERSE EFFECT  
SUBMITTED TO THE STATE HISTORIC PRESERVATION OFFICER  
PURSUANT TO 36 CFR SECTION 800.5(c)**

**US 36 Added Travel Lanes  
Hendricks & Marion County, Indiana  
Des. No.: 1601072  
DHPA No.: 23088**

**1. DESCRIPTION OF THE UNDERTAKING**

The Indiana Department of Transportation (INDOT) with funding from the Federal Highway Administration (FHWA) intend to proceed with the US 36 Added Travel Lanes project in Avon, Hendricks County and Indianapolis, Marion County, Indiana. Section 106 of the National Historic Preservation Act requires Federal agencies to take into account the effects of their undertakings on historic properties. The federal involvement in the project is funding received from the FHWA.

The proposed undertaking is on US 36 (Rockville Road) beginning at Shiloh Park Drive in Avon, Hendricks County and continuing east to approximately 0.28 mile (1,500 feet) east of Raceway Road in Indianapolis, Marion County, Indiana. It is within Washington Township, Hendricks County, and Wayne Township, Marion County, Clermont USGS Topographic Quadrangle, in Sections 4, 5, 8 and 9, Township 15 North, Range 2 East. Adjacent land use consists of residential and commercial. Please see project graphics in Appendices A, B, and F.

The need for this project is due to the inadequate level of service motorists experience when traveling through the corridor. The primary purpose this project is to reduce delays on US 36 with a focus on the heavily congested portion between Ronald Reagan Parkway and Raceway Road.

The preferred alternative will involve work to the existing roadway to provide added travel lanes in both directions on US 36. The current typical cross section consists of two 12-foot wide travel lanes, a 16-foot wide two-way-left-turn-lane, and up to an 11-foot shoulder in each direction. A discontinuous 12-foot wide right-turn lane is present within portions of the project area. The proposed typical cross section in each direction will consist of three travel lanes (one 11-foot wide and two 12-foot wide) bordered by curb and gutter. An 11-foot wide left turn lane will be provided in both directions at signalized intersections and right turn lanes will be reconstructed. At the current design phase, the anticipated right-of-way is expected not to exceed approximately 0.3 acre of permanent and approximate 0.7 acre of temporary right-of-way. The project is anticipated to be let in 2021. Traffic is anticipated to be maintained through the area of construction with two lanes being maintained in each direction. No signed detours are anticipated. Access to residences and businesses along US 36 will be continuously maintained during construction. Please see maps of the project area in Appendix A and preliminary plans in Appendix F.

Per 36 CFR 800.9(a), the Area of Potential Effects (APE) is “the geographic area or areas within which an undertaking may directly or indirectly cause alterations in the character or use of historic properties. The APE is influenced by the scale and nature of an undertaking. . .” The APE encompasses the entire project



site, including all locations where the project may result in disturbance of the ground; all locations from which ground-disturbing elements of the project may be visible or audible; all locations where ground-disturbing activity may result in changes in traffic patterns, land use, or public access; and all areas where there may be direct or indirect effects due to the ground-disturbing elements of the project. The APE for this project is an irregular polygon. The APE boundaries were established using guidance in the INDOT Cultural Resources Manual. It narrows where views to the project are limited by structures and dense vegetation. The APE follows US 36 through the eastern portion of Avon in Washington Township in eastern Hendricks County to the western portion of Indianapolis in Wayne Township in western Marion County. The APE is along US 36 from 0.21 mile west of Shiloh Park Drive to 0.36 mile east of South Raceway Road. Most of the APE extends approximately 700 feet wide and it expands to approximately 900 feet wide where open viewsheds occur. Please see maps indicating the APE in Appendix A.

## **2. EFFORTS TO IDENTIFY HISTORIC PROPERTIES**

The National Register of Historic Places (NRHP) and the Indiana Register of Historic Sites and Structures (State Register) were checked using the State Historic Architectural and Archaeological Research Database (SHAARD) and the Indiana Historic Buildings, Bridges, and Cemeteries Map (IHBBC). No resources are listed in the NRHP or State Register within the APE. The Indiana Historic Sites and Structures Inventory (IHSSI) *Hendricks County Interim Report* (1989) and *Wayne Township, Marion County Interim Report* (1993) data was examined and identified four previously surveyed properties within the APE.

The cemetery registry was also consulted using SHAARD and SHAARD GIS. No cemeteries were identified within the APE. The *Indiana Historic Bridge Inventory Volume 2: Listing of Historic and Non-Historic Bridges* (February 2009) by Mead and Hunt was also reviewed. No bridges eligible for listing in the NRHP were identified within the APE.

Early coordination was initiated on October 1, 2018. The agencies/individuals/tribes listed below were sent an email with an early coordination letter inviting them to become Section 106 consulting parties. All consulting parties were invited to view the early coordination letter on IN SCOPE (INDOT's online portal for public viewing Section 106 documents at <https://erms.indot.in.gov/Section106Documents/>). In addition, a hard copy of the early coordination letter was mailed to the Indiana State Historic Preservation Office (SHPO). The organizations identified in bold responded and agreed to be consulting parties.

- 1. Indiana State Historic Preservation Officer (automatic consulting party)**
- 2. Indiana Landmarks, Central Regional Office**
3. Hendricks County Historical Society/Hendricks County Museum
4. Hendricks County Heritage Alliance
5. Hendricks County Historian
6. Marion County Historian
7. Wayne Township Historical Society
8. Indianapolis Metropolitan Planning Organization
9. Avon Town Manager
10. Hendricks County Commissioner's Office

11. Hendricks County Engineer
12. Hendricks County Highway Office Manager
13. Hendricks County Plan Commission Chair
14. Indianapolis Mayor
15. City of Indianapolis Department of Public Works Director
16. Marion County Commissioner's Office
- 17. Delaware Nation of Oklahoma**
18. Eastern Shawnee Tribe of Oklahoma
19. Forest County Potawatomi Community
- 20. Miami Tribe of Oklahoma**
21. Peoria Tribe of Indians of Oklahoma
22. Pokagon Band of Potawatomi Indians

Indiana Landmarks, Central Regional Office accepted to be a consulting party in a letter dated October 30, 2018. The Delaware Nation of Oklahoma accepted to be a consulting party and concurred with the proposed plan at that time in an email dated November 6, 2018. The Miami Tribe of Oklahoma accepted to be a consulting party and offered no objection to the project at that time in a letter dated November 7, 2018. In a letter dated November 1, 2018, the Indiana SHPO recommended no other consulting parties. Please see responses in Appendix D.

A Historic Property Report (HPR by Moscato, December 11, 2018) was completed for this project. On August 24, 2018, the professional staff at RQAW Corporation performed a site inspection of the APE. The Qualified Professional architectural historian walked and drove through the project area within the APE and photographed all resources within the APE that will be 50 years of age or older at the proposed letting date (2021). As a result of this field survey and associated documentary research, the architectural historian recommended two properties as eligible for listing in the NRHP: the Charles McLain House (IHSSI # 063-117-45024) and the David Faucett House (IHSSI # 097-117-56005). Please see Appendix E for an excerpt from the HPR.

An archaeological report (Phase Ia Archaeological Reconnaissance by Martin, October 9, 2018) was completed for this project. The archaeological reconnaissance identified no archaeological resources in the project area and recommended that the project be allowed to proceed. Please see Appendix E for an excerpt from the archaeological report.

On December 11, 2018, a hard copy of a coordination letter, the HPR, and archaeological report were mailed to the Indiana SHPO while other consulting parties were informed that the coordination letter, HPR, and archaeology report (tribes only) could be viewed electronically by accessing IN SCOPE. INDOT's Section 106 guidance recommends that owners of recommended historic resources be invited to become consulting parties. Therefore, the coordination letter and HPR were mailed to those property owners listed below on December 11, 2018 (the owner identified in bold responded and agreed to be a consulting party).

1. Rockville Marketplace LLC, Historic Property Owner for David Faucett House, 9055 Rockville Rd.
- 2. Andrew Vinson, Historic Property Owner for Charles McLain House, 10664 E. US 36**

In a phone call on January 8, 2019, Mr. Andrew Vinson, Charles McLain House property owner, contacted INDOT to inquire about the coordination letter and HPR that he received. The conversation is noted in an email from Shaun Miller that day. Mr. Vinson was added to the list of consulting parties. In a letter dated January 11, 2019, Indiana Landmarks concurred with the recommendations in the HPR including: the APE, that the Charles McLain House and David Faucett House are eligible for the NRHP, and their proposed historic resource boundaries. Please see correspondence in Appendix D.

In a letter dated January 11, 2019, the Indiana SHPO concurred with the archaeology report stating, “we have not identified any currently known archaeological resources listed in or eligible for inclusion in the NRHP within the proposed project area.” Please see the letter in Appendix D.

In a letter dated January 11, 2019, the Indiana SHPO staff concurred with the recommendations in the HPR stating, “we agree that the only above-ground historic properties within the APE identified in the HPR are the Charles McLain House (Indiana Historic Sites and Structures Inventory [IHSSI] No. 063-117-45024) at 10664 East US 36 in Avon and the David Faucett House (HSSI No. 097-117-56005) at 9055 Rockville Road (US 36) in Indianapolis.” Please see the letter in Appendix D. In regard to the possibility of additional historic resources, the Indiana SHPO staff inquired on the confirmation or not of “any elements of the former right-of-way of the Terre Haute, Indianapolis & Eastern Traction Company remain visible on the north side of the existing US 36 pavement within the APE.”

In response to the Indiana SHPO staff’s inquiry an Effects Letter (by Boot, February 11, 2019) provided research documentation and explained that it is believed that any remnants from the Terre haute, Indianapolis, & Eastern Traction Company line were demolished during the replacement and widening of US 36 during the 1970s. Please see the Effects Letter in Appendix D for additional information.

No other consulting parties provided comments/questions with regard to the identification of historic properties. Please see Appendix D for correspondence.

## **DESCRIBE AFFECTED HISTORIC PROPERTIES**

### **A. Charles McLain House (IHSSI # 063-117-45024) 10664 E. US 36.**

The Charles McLain House is located on the north side of US 36 and east of N. Ronald Reagan Parkway in the Avon, Indiana. The Charles McLain House is a good example of a Free Classic Queen Ann style house, circa 1904. The house possesses a fair amount of integrity with the original overall plan, some detailing, and stylistic features. However, some aluminum siding, roof replacement, and replacement of the one-over-one windows has taken place. The period of significance for the Charles McLain House is from circa 1904, when the original house and carriage house were constructed. The Charles McLain House is recommended eligible under Criterion C because it is a good example (although with alterations) of a circa 1904 Free Classic Queen Ann style house with original architectural features in Washington Township, Hendricks County. It is not the work of a master but embodies distinctive characteristics of this type.

### **B. David Faucett House (IHSSI # 097-117-56005) 9055 Rockville Rd. (US 36).**

The David Faucett House is located on the south side of Rockville Rd. (US 36) and east of N. Raceway Rd. in Wayne Township, Marion County, Indiana. The David Faucett House is a good example of a frame I-house, circa 1846. The house possesses a good amount of integrity with the original massing, façade fenestration, and wood windows. The period of significance for the David Faucett House is from circa 1846, when the original house was constructed. The David Faucett House is recommended eligible under Criterion A for its association with the early development of Wayne township, Marion County and Washington township, Hendricks County. The David Faucett house may very well be one of the last, if not the last mid-19th century farmhouse in Wayne Township. Also, the David Faucett House is recommended eligible under Criterion C because it is a good example (although with alterations) of a circa 1846 I-house with Greek Revival architectural features in Wayne Township, Marion County. It is not the work of a master but embodies distinctive characteristics of this type.

## **3. DESCRIBE THE UNDERTAKING'S EFFECTS ON HISTORIC PROPERTIES**

The two above-mentioned properties eligible for listing in the NRHP within the APE are located along US 36 (Rockville Road). The undertaking's effects on historic properties is primarily visual within an already extensively built-up US highway corridor and suburban landscape. The visual effects are likely to include:

- Pavement replacement on the same alignment with maintaining 12-foot wide travel lanes. The pavement may be milled and overlaid in some locations, depending on existing conditions or associated excavation needs.
- Pavement widening with the addition of a travel through lane in each direction.
- Installation of a curb and gutter along the roadway edge.

- Converting the two-way center turn lane to a dedicated left turn lane with small curb. The left turn lanes will be in each direction at signaled intersections, but all other intersections will be accessible only via right-in right-out.
- Reconstruction and lengthening of the existing right turn lanes.

Please see Appendix F for the Stage 1 Plans enclosed with the NRHP-eligible resources highlighted.

**A. Charles McLain House (IHSSI # 063-117-45024) 10664 E. US 36.**

Project impacts adjacent to the historic resource boundary will include roadway widening and reconstruction, driveway reconstruction, and limited sidewalk reconstruction to tie the existing sidewalk into the new driveway. No permanent right-of-way is anticipated from the Charles McLain House; however, approximately 0.02 acre of temporary right-of-way is needed for driveway and adjacent sidewalk reconstruction. The existing roadway, driveway, parking lot, and sidewalk are modern concrete construction. The sidewalk, parking lot, and driveway were installed circa 2013.<sup>1</sup> The historic resource boundary goes to the back (north edge) of the existing sidewalk. The anticipated construction limits will not extend into the historic resource boundary. (please see the Photo Effects Diagram with the Effects Letter in Appendix D and the Stage 1 Plans, sheets 9 and 17 in Appendix F).

The proposed work in this area will widen the US 36 pavement by approximately 17 feet to add a travel lane, recreate and extend the existing right turn lane, and install a curb and gutter. The added travel lane near the Charles McLain House is anticipated to provide only a 10% increase in capacity from 2021 to 2041. The property's setting has been drastically altered by development over recent decades from the expansion of US 36 and the property's own conversion to commercial use (i.e. parking lot and sidewalk construction). Additionally, note that the Charles McLain House significance is limited to architecture (Criterion C), so the resource's integrity is less sensitive to setting changes than resources that derive significance from Criterion A.

The project's minimal introduction of visual and audible elements to the property's setting will not constitute a significant alteration to historic features that contribute to the Charles McLain House historic significance because of the already transformed, suburban landscape. Although permanent work is proposed near the Charles McLain House property, it is anticipated that there will be no adverse effect to the house or carriage house because no contributing historic features will be impacted directly. Therefore, the alterations adjacent to the historic resource boundary and its setting are not anticipated to reduce the significance or impact any of the characteristics that qualify the Charles McLain House for the NRHP in a manner that would diminish the integrity of the property's location, design, setting, materials, workmanship, feeling, or association. As a result, the project is anticipated to have "No Adverse Effect" to the Charles McLain House.

<sup>1</sup> Google, *Google Earth* (Google), accessed January 18, 2019, <https://www.google.com/earth/>.



**B. David Faucett House (IHSSI # 097-117-56005) 9055 Rockville Rd. (US 36).**

Project impacts near the historic resource boundary of the David Faucett House will be strictly visual and limited to the existing right-of-way. Neither temporary nor permanent right-of-way will be required from the David Faucett House. In terms of the David Faucett House setting, the east end of new construction will occur approximately 99 feet northwest of the David Faucett House historic property boundary; while the east end of the incidental construction consisting of pavement milling to transition the new construction to the existing pavement will occur approximately 35 feet northwest of the David Faucett House boundary. No work will occur within the historic boundary nor immediately adjacent to the frontage of it (please see the Stage 1 Plans, sheets 12 and 21 in Appendix F).

The David Faucett House's setting has been drastically altered by development over recent decades from the expansion of US 36. The project's minimal introduction of visual and audible elements to the property's setting will not constitute a significant alteration from this already transformed, suburban landscape. Thus, the alterations near the historic resource boundary and neighboring visible changes (as described above) will have no direct impact on the David Faucett House other than a viewshed change approximately 99 feet away for pavement replacement and the addition of a curb and gutter. This minor viewshed alteration is not anticipated to reduce the significance or impact any of the characteristics that qualify the David Faucett House for the NRHP in a manner that would diminish the integrity of the property's location, design, setting, materials, workmanship, feeling, or association. Therefore, the project is anticipated to have "No Adverse Effect" to the David Faucett House.

**4. EXPLAIN APPLICATION OF CRITERIA OF ADVERSE EFFECT – INCLUDE CONDITIONS OR FUTURE ACTIONS TO AVOID, MINIMIZE OR MITIGATE ADVERSE EFFECTS**

According to 36 CFR 800.5(a)(1) "An adverse effect is found when an undertaking may alter, directly or indirectly, any of the characteristics of a historic property that qualify the property for inclusion in the National Register in a manner that would diminish the integrity of the property's location, design, setting, materials, workmanship, feeling or association."

**A. Charles McLain House (IHSSI # 063-117-45024) 10664 E. US 36.**

According to 36 CFR 800.5(a)(1) the criteria of adverse effect do not apply.

Per 36 CFR 800.5(a)(2)(i), There will not be physical damage to any part of the property that contributes to its historic integrity. Physical impacts will be limited to 0.02 acre of previously altered (circa 2013) modern concrete driveway and sidewalk and will not alter the characteristics of the property that qualify it for inclusion in the National Register in a manner that diminishes the property's integrity.

Per 36 CFR 800.5(a)2(ii): the "Alteration of a property, including restoration, rehabilitation, repair, maintenance, stabilization, hazardous material remediation and provision of handicapped access,

that is not consistent with the Secretary's Standards for the Treatment of Historic Properties and/or other applicable guidelines" will not occur. The only alterations to the property will occur to the modern concrete driveway and sidewalk which will be replaced in-kind. The project will have no direct impact on the Charles McLain House or any historic features on the property.

Per 36 CFR 800.5(a)(2)(iii), the "Removal of the property from its historic location" will not occur. The project will not remove any portion of the Charles McLain House from its historic location.

Per 36 CFR 800.5(a)(2)(iv), due to the pavement widening, construction of a curb and gutter, and driveway and sidewalk replacement, a change of the character of the property's use or of physical features within the property's setting will occur; however, the changes will not alter the property's setting in a way that diminishes the historic features that contribute to its historic significance. The Charles McLain House is significant under Criterion C for its original architectural features and Free Classic Queen Ann style. The property's setting in this area along US 36 has been drastically altered by development over recent decades from the expansion of US 36 and the property's own conversion to commercial use. Overall, the minor alterations as part of the US 36 pavement widening will not have a significant negative impact on the Charles McLain House's setting and will not affect the property's ability to convey historic significance. Thus, the impacts do not rise to the level of being an adverse effect.

Per 36 CFR 800.5(a)(2)(v), the "Introduction of visual, atmospheric or audible elements that diminish the integrity of the property's significant historic features" will not occur. Although adding a travel lane, widening and replacing the pavement, and adding a curb and gutter will introduce visual elements and provide increased capacity for vehicular noise, the additions will not diminish the integrity of the property's significant historic features. The added travel lane near the Charles McLain House is anticipated to provide only a 10% increase in capacity from 2021 to 2041 (please see the traffic data on sheet one of the preliminary plans in Appendix F). Therefore, in the area of the Charles McLain House, the marginal visual setting, atmospheric and audible changes will not rise to the level of being an adverse because they will not diminish the resource's integrity or ability to portray historic significance.

Per 36 CFR 800.5(a)(2)(vi), the "Neglect of a property which causes its deterioration..." will not occur as a result of the project. The undertaking will not cause the deterioration of any aspect of the Charles McLain House.

Per 36 CFR 800.5(a)(2)(vii), the "Transfer, lease, or sale of property out of Federal ownership or control..." will not occur. Ownership of the resource will not change as a result of this project.

**B. David Faucett House (IHSSI # 097-117-56005) 9055 Rockville Rd. (US 36).**

According to 36 CFR 800.5(a)(1) the criteria of adverse effect do not apply.

Per 36 CFR 800.5(a)(2)(i), the "Physical destruction of or damage to all or part of the property" will not occur. The undertaking will not directly impact the David Faucett House. No right-of-way will be required from the property and no portion of the historic resource boundary is within the construction limits.

Per 36 CFR 800.5(a)(2)(ii), Per 36 CFR 800.5(a)2(ii): the “Alteration of a property, including restoration, rehabilitation, repair, maintenance, stabilization, hazardous material remediation and provision of handicapped access, that is not consistent with the Secretary’s Standards for the Treatment of Historic Properties and/or other applicable guidelines” will not occur. The project will have no direct impact on the David Faucett House.

Per 36 CFR 800.5(a)(2)(iii), the “Removal of the property from its historic location” will not occur. The project will not remove any portion of the David Faucett House from its historic location.

Per 36 CFR 800.5(a)(2)(iv), a “Change of the character of the property’s use or of physical features within the property’s setting that contribute to its historic significance” will not occur. The project will not cause a change of character to any of the David Faucett’s features. Although pavement and a curb and gutter will be added to US 36 (Rockville Road) up to approximately 99 feet from the David Faucett House historic boundary and pavement milled and overlaid up to approximately 35 feet from the historic boundary will occur; no historic features of the property will be disturbed. Therefore, the project will have no significant impact on the physical features of this property’s setting. Overall, the minor alterations as part of the US 36 pavement improvements will not have a significant negative impact on the David Faucett House’s setting and will not affect the property’s ability to convey historic significance. Thus, the impacts do not rise the level of being an adverse effect.

Per 36 CFR 800.5(a)(2)(v), the “Introduction of visual, atmospheric or audible elements that diminish the integrity of the property’s significant historic features” will not occur. Although widening and replacing the pavement and adding a curb and gutter northwest of the David Faucett House will introduce visual elements, the additions will not diminish the integrity of the property’s significant historic features. The vehicular capacity in the vicinity of the David Faucett House is not anticipated to increase because an added travel lane is not proposed in this location (i.e. the two travel lanes and a left turn lane will be reconstructed and tie into the existing pavement.) Therefore, in the area of the David Faucett House, the marginal visual setting, atmospheric and audible changes will not rise to the level of being an adverse because they will not diminish the resource’s integrity or ability to portray historic significance.

Per 36 CFR 800.5(a)(2)(vi), the “Neglect of a property which causes its deterioration...” will not occur as a result of the project. The undertaking will not cause the deterioration of any aspect of the David Faucett House.

Per 36 CFR 800.5(a)(2)(vii), the “Transfer, lease, or sale of property out of Federal ownership or control...” will not occur. Ownership of the resource will not change as a result of this project.

## **5. SUMMARY OF CONSULTING PARTIES AND PUBLIC VIEWS**

On January 8, 2019, Mr. Andrew Vinson spoke with Shaun Miller of INDOT regarding the project and Section 106/NEPA. On January 15, 2019, Kyle Boot (RQAW Architectural Historian) followed up with Mr. Vinson who inquired about the proposed work near the Charles McLain property. The minor potential

impacts were explained to Mr. Vinson who provided no response regarding potential effects to the historic characteristics of the Charles McLain property. Please see correspondence in Appendix D.

In a letter dated January 11, 2019, the Indiana SHPO staff concurred with the HPR's recommendations of the Charles McLain House and David Faucett House as eligible for listing in the NRHP; they also concurred with the archaeology report. In the same letter, the Indiana SHPO staff requested additional information regarding the proposed roadway design and right-of-way requirements. The letter requested "a fairly detailed plan of the widened roadway where it will pass adjacent to the Charles McClain House and adjacent to the David Faucett House... [and] where the permanent right-of-way, in particular, would be acquired in the vicinity of the historic properties, in order to enable evaluation of effects" to historic properties (see this correspondence in Appendix D).

On February 11, 2019, a hard copy of the Effects Letter was mailed to the Indiana SHPO while other consulting parties were informed that the Effects Letter could be viewed electronically by accessing IN SCOPE. The Effects Letter described the proposed alterations and anticipated right-of-way in the vicinity of the historic properties. It furthermore provided preliminary plans (with historic properties highlighted) demonstrating the widened roadway and anticipated right-of-way near historic properties. It should be noted that the Miami Tribe of Oklahoma was mistakenly omitted from the list in the effects letter that identified agencies/individuals/tribes who had agreed to serve as consulting parties for this project. However, the Miami Tribe of Oklahoma was included in the email distribution list for the effects letter. Please see the Effects Letter in Appendix D (note that some enclosures have been removed to avoid duplication.)

On February 19, 2019, the Indiana SHPO staff responded to the Effects Letter indicating appreciation for the information in it. In regard to the historic David Faucett House, the SHPO staff commented, "Given the existing, previously widened US 36 highway in front of that historic house, it does not seem likely that the currently proposed modifications, most of which end to the west of the property, would have an indirect adverse effect on the David Faucett House." In regard to the Charles McLain House, note that the SHPO staff identified a discrepancy in the parcel boundary/property line. The written descriptions and exhibits inconsistently described its location. Some identified it along the front edge of the existing sidewalk while others identified it along the back edge. However, the SHPO staff commented, "we are content to treat the back edge of the sidewalk as the historic property boundary, which is the usual way of delineating the boundary of an individual historic property in an urban area." The SHPO staff further stated, "We anticipate that the widening will make a noticeable change to the setting of the house. As the [effects] letter also indicates, however, the house is considered to be eligible for inclusion in the NRHP only for its architecture, under NRHP Criterion C. Furthermore, given the already wide and busy US 36 in front of the house, we do not think that the integrity of any characteristic of the house that makes it significant will be demonstrably diminished." Finally, in regard to the consulting party's review period of the Effects Letter, the Indiana SHPO staff commented, "If a consulting party asks for additional time to comment, up to the usual 30 days, we ask that such a request be honored." Please see the SHPO's letter in Appendix D.

On March 15, 2019, Indiana Landmarks responded to the Effects Letter. The letter stated that, “we do not anticipate that the undertaking will result in adverse effects to the Charles McLain House (IHSSI# 063-117-45024) (eligible under Criterion C) or the David Faucett House (IHSSI# 097-117-56005), provided there are not changes to the footprint of the construction area for the project.” They also ask, “that INDOT’s specific reasons for requesting a 15-day comment period be added to the public record for this project.” Please see Appendix D for a copy of the correspondence. In response for the record, the INDOT project manager requested an expedited review to arrive at a Section 106 finding sooner to advance the overall project schedule. It is anticipated that procedures related to project planning and other services after the Section 106 and environmental phases may take longer than usual. It should be noted that 36 CFR 800 does not specify a comment period length for interim Section 106 documents, such as effects letters. As required by 36 CFR 800, consulting parties will be given 30 days to review and comment on the Section 106 finding and documentation for this project.

No additional comments/questions were received during Section 106 consultation. Please see Appendix D for all Section 106 correspondence.

A public notice of the FHWA finding of “No Adverse Effect” will be published in the *Indianapolis Star*. Additionally, the finding and supporting documentation will be posted on IN SCOPE. A 30-day comment period will be given, and this document will be updated to reflect any comments received.

## **APPENDICES**

- APPENDIX A: Project Area Maps
- APPENDIX B: General Photographs
- APPENDIX C: Consulting Parties List
- APPENDIX D: Consulting Parties Correspondence
- APPENDIX E: Historic Property Report and Archaeology Summaries
- APPENDIX F: Preliminary Plans

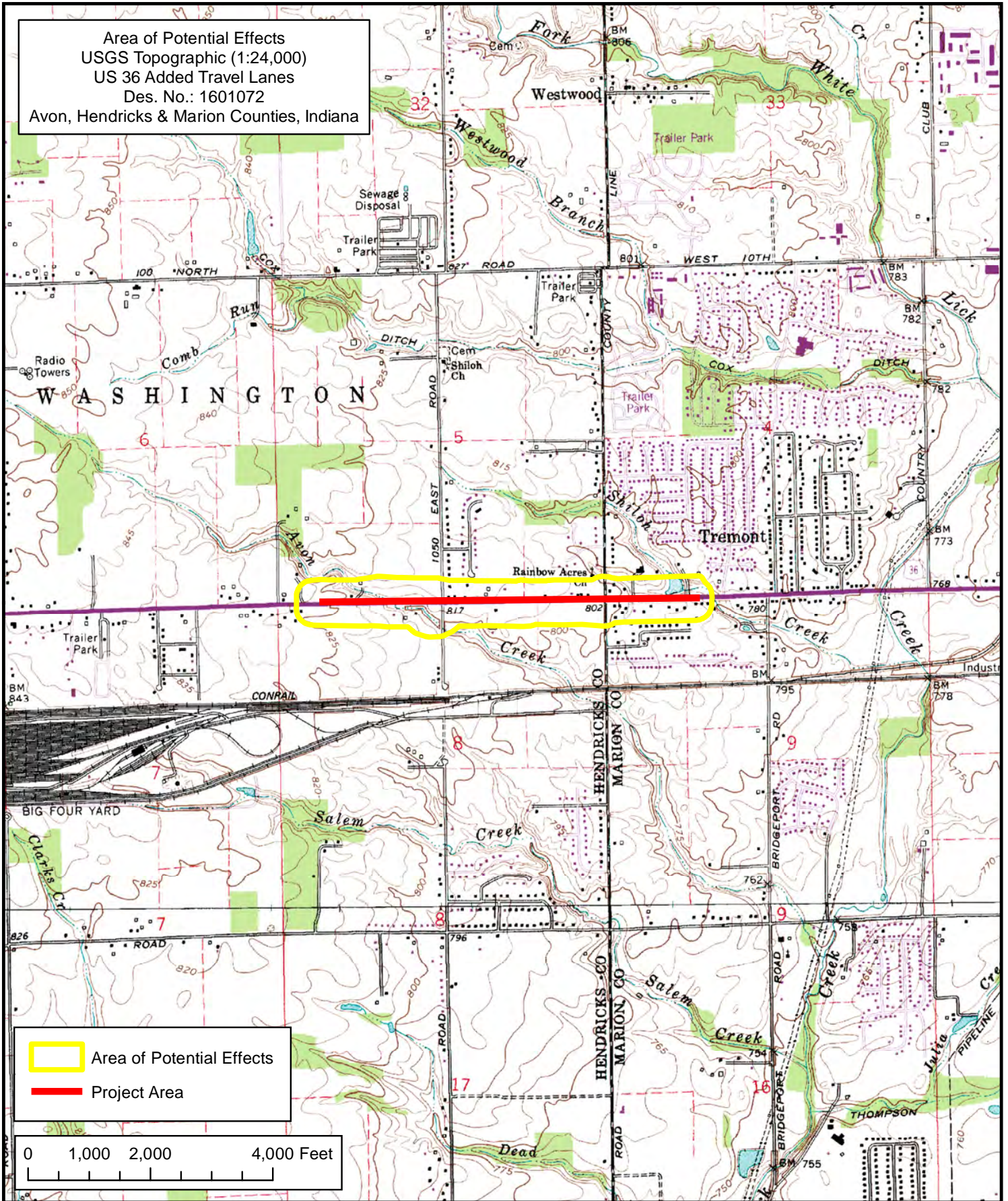




Appendix A:

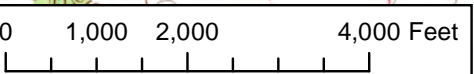
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# Project Area Maps

Area of Potential Effects  
 USGS Topographic (1:24,000)  
 US 36 Added Travel Lanes  
 Des. No.: 1601072  
 Avon, Hendricks & Marion Counties, Indiana



 Area of Potential Effects  
 Project Area



8770 North Street, Suite 110  
 Fishers, IN 46038

This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.

Topographic Quadrangle obtained from USGS (Clermont Quadrangle)

Map Datum: NAD 83  
 Map Projection: UTM Zone 16 North





Area of Potential Effects  
 2017 Aerial Photograph  
 US 36 Added Travel Lanes  
 Des. No.: 1601072  
 Avon, Hendricks & Marion Counties, Indiana



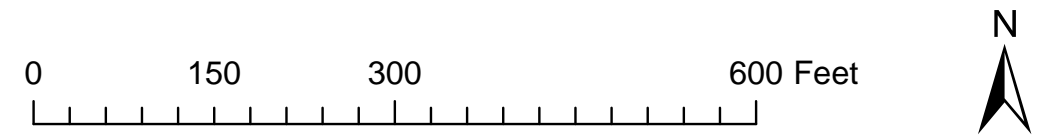
- Proposed Permanent Right-of-Way
- Proposed Temporary Right-of-Way
- Construction Limits
- Proposed Pavement
- Historic Property Boundaries
- Area of Potential Effects

Source: Esri, DigitalGlobe, GeoEye, Earthstar Geographics, CNES/Airbus DS, USDA, USGS, AeroGRID, IGN, and the GIS User Community

**RQAW**  
 ENVIRONMENTAL  
 8770 North Street, Suite 110  
 Fishers, IN 46038

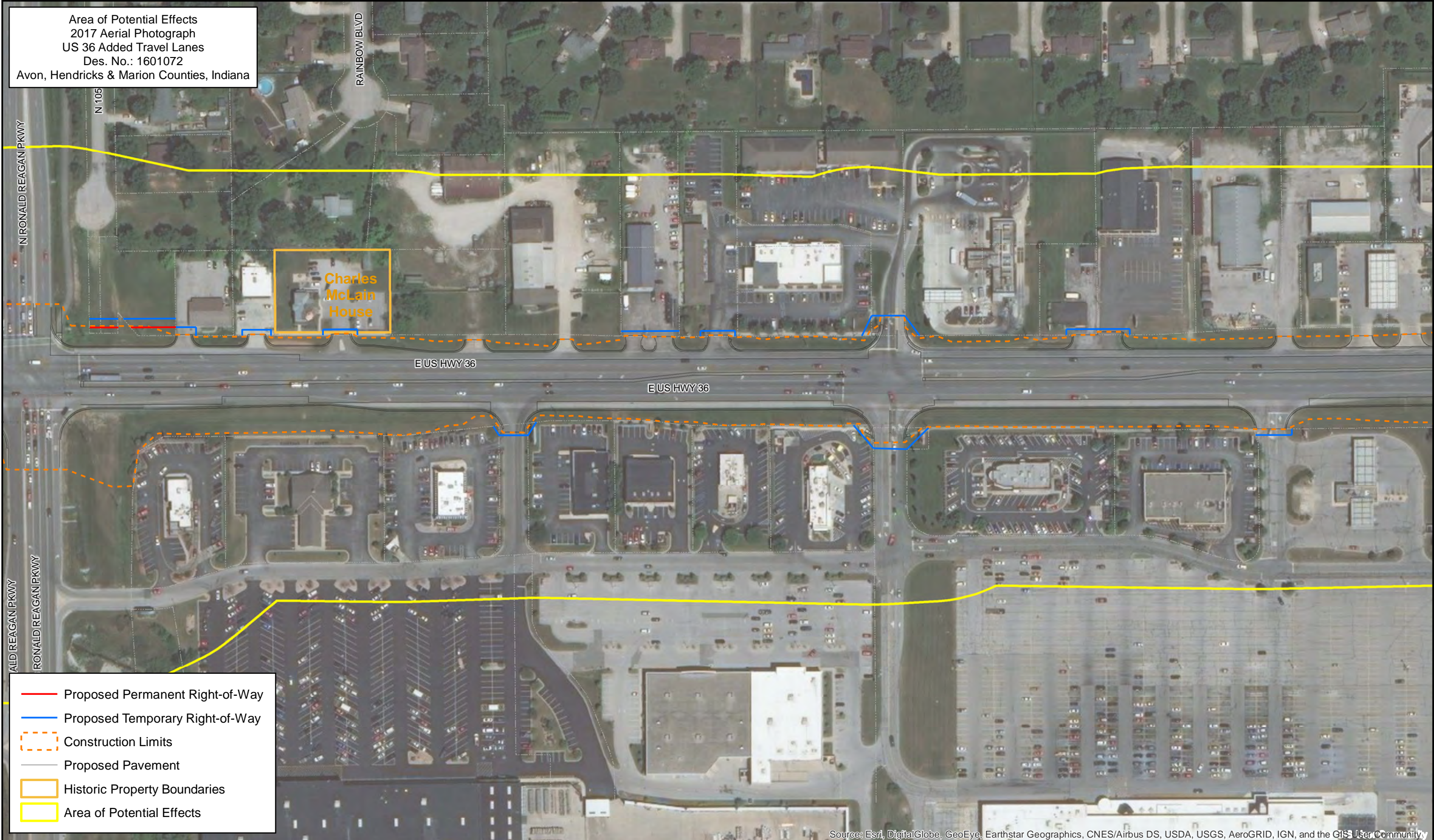
This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.

Map Datum: NAD 83  
 Map Projection: UTM Zone 16 North





Area of Potential Effects  
 2017 Aerial Photograph  
 US 36 Added Travel Lanes  
 Des. No.: 1601072  
 Avon, Hendricks & Marion Counties, Indiana

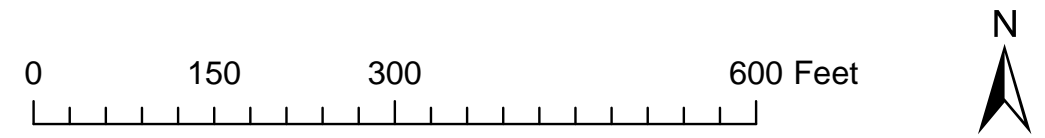


- Proposed Permanent Right-of-Way
- Proposed Temporary Right-of-Way
- Construction Limits
- Proposed Pavement
- Historic Property Boundaries
- Area of Potential Effects

Source: Esri, DigitalGlobe, GeoEye, Earthstar Geographics, CNES/Airbus DS, USDA, USGS, AeroGRID, IGN, and the GIS User Community

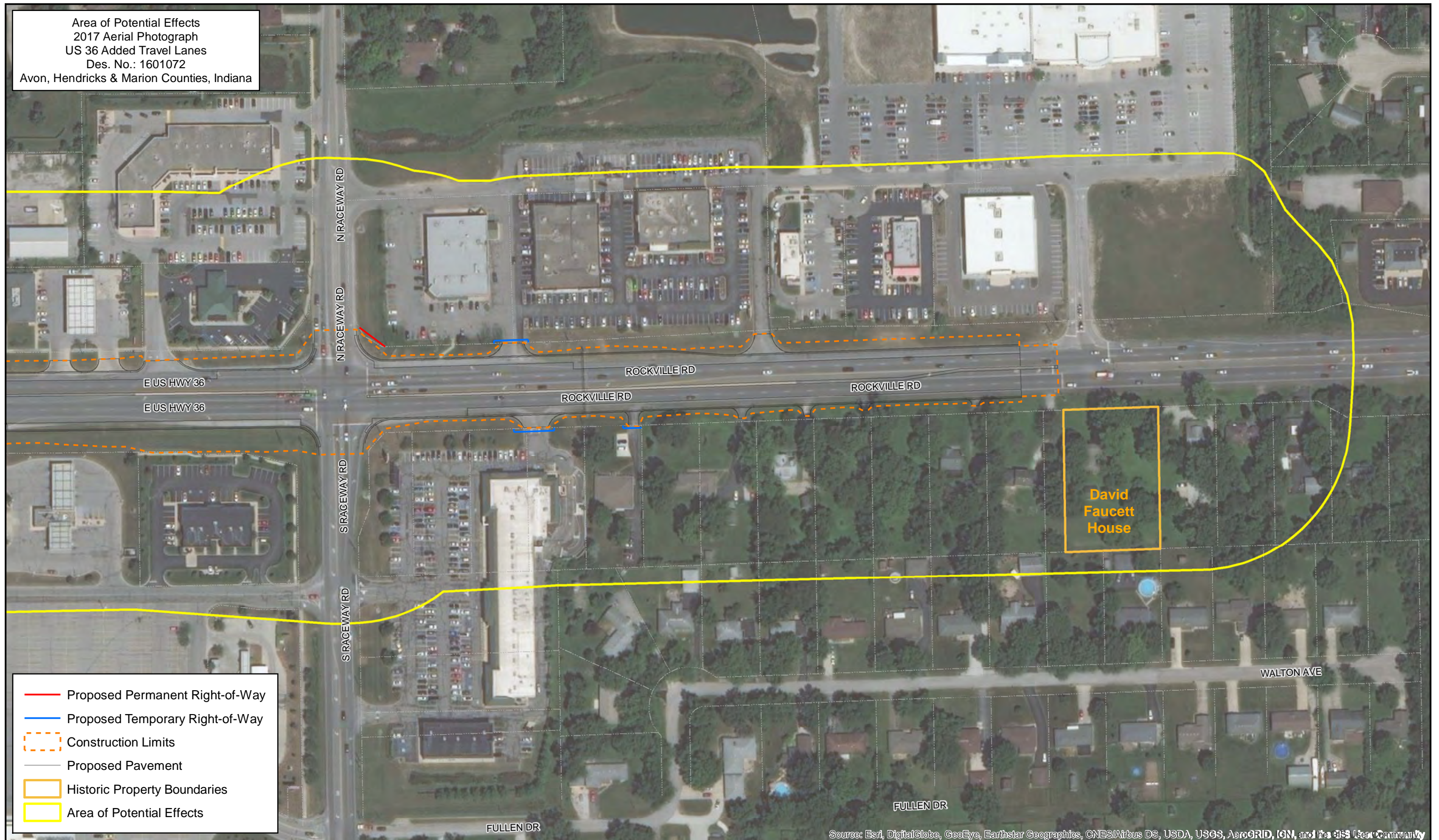
**RQAW**  
 ENVIRONMENTAL  
 8770 North Street, Suite 110  
 Fishers, IN 46038

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Area of Potential Effects  
 2017 Aerial Photograph  
 US 36 Added Travel Lanes  
 Des. No.: 1601072  
 Avon, Hendricks & Marion Counties, Indiana

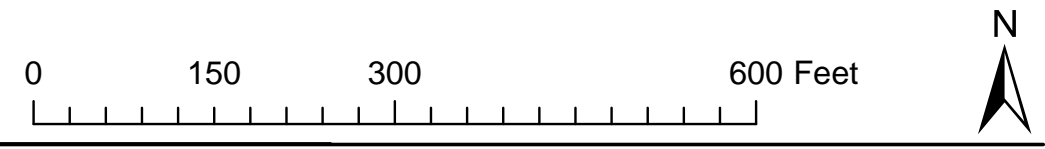


- Proposed Permanent Right-of-Way
- Proposed Temporary Right-of-Way
- Construction Limits
- Proposed Pavement
- Historic Property Boundaries
- Area of Potential Effects

Source: Esri, DigitalGlobe, GeoEye, Earthstar Geographics, CNES/Airbus DS, USDA, USGS, AeroGRID, IGN, and the GIS User Community

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 Map Datum: NAD 83  
 Map Projection: UTM Zone 16 North





## Appendix B:

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# General Photographs

Photographs omitted to avoid duplication. See photographs in Appendix B of this EA document.

## Appendix C:

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# Consulting Parties List

LIST OF INDIVIDUALS/AGENCIES/ORGANIZATIONS  
 INVITED TO BE SECTION 106 CONSULTING PARTIES

Position	Agency 1	Agency 2	Address 1	Address 2	City	State	Zip	Email	Notes
Manager of Cultural Resources Section	Indiana Department of Transportation	Office of Environmental Services	100 N. Senate Ave.	Room N642	Indianapolis	IN	46204		
State Historic Preservation Officer	Division of Historic Preservation & Archaeology	Indiana Department of Natural Resources	402 W. Washington St.	Room W274	Indianapolis	IN	46204		
Sam Burgess	Indiana Landmarks	Central Regional Office	1201 Central Avenue		Indianapolis	IN	46202-3204	<a href="mailto:sburgess@indianalandmarks.org">sburgess@indianalandmarks.org</a>	
Gail Tharp	Hendricks County Historical Society/Hendricks County Museum		170 South Washington Street		Danville	IN	46122	<a href="mailto:museum@co.hendricks.in.us">museum@co.hendricks.in.us</a>	317-718-6158
Jan Davis	Hendricks County Heritage Alliance		49985 S. SR 39		Clayton	IN	46118	<a href="mailto:hheritagealliance@gmail.com">hheritagealliance@gmail.com</a>	765-318-6655
John O. Parsons	Hendricks County Historian		5676 S. 200 E.		Clayton	IN	46118	<a href="mailto:jparsons@co.hendricks.in.us">jparsons@co.hendricks.in.us</a>	317-539-6680
Steven Barnett	Marion County Historian		62 N. Layman Ave.		Indianapolis	IN	46219	<a href="mailto:srbarnett44@yahoo.com">srbarnett44@yahoo.com</a>	317-847-8099
Beuford Hall	Wayne Township Historical Society		1155 S. High School Rd.		Indianapolis	IN	46241		317-299-7857
Anna Gremling	Indianapolis Metropolitan Planning Organization		200 E. Washington St. Suite 1922, City/County Bldg.		Indianapolis	IN	46204	<a href="mailto:anna.gremling@indy.gov">anna.gremling@indy.gov</a>	317-327-5487
Tom Klein	Avon Town Manager		6570 E. U.S. 36		Avon	IN	461523	<a href="mailto:tklein@avongov.org">tklein@avongov.org</a>	(317) 272-0948
Matthew Whetstone	Hendricks County Commissioner		355 South Washington Street	Suite 204	Danville	IN	46122	<a href="mailto:mwhetstone@co.hendricks.in.us">mwhetstone@co.hendricks.in.us</a>	(317) 745-9221
Phyllis A Palmer	Hendricks County Commissioner		355 South Washington Street	Suite 204	Danville	IN	46122	<a href="mailto:ppalmer@co.hendricks.in.us">ppalmer@co.hendricks.in.us</a>	(317) 745-9221
Robert Gentry	Hendricks County Commissioner		355 South Washington Street	Suite 204	Danville	IN	46122	<a href="mailto:rgentry@co.hendricks.in.us">rgentry@co.hendricks.in.us</a>	(317) 745-9221
John E. Ayers	Hendricks County Engineer		355 South Washington Street	Suite 209	Danville	IN	46122	<a href="mailto:jayers@co.hendricks.in.us">jayers@co.hendricks.in.us</a>	(317) 745-9235
Cathy Burkert, Manager	Hendricks County Highway Office		930 E. Main Street		Danville	IN	46122	<a href="mailto:cburkert@co.hendricks.in.us">cburkert@co.hendricks.in.us</a>	(317) 745-9227
Don I. Reitz	Hendricks County Plan Commission Chair		355 South Washington Street	Suite 212	Danville	IN	46122		(317) 745-9346
Joe Hogsett	Indianapolis Mayor		200 E. Washington St. City/County Bldg.	Suite 2501	Indianapolis	IN	46204		
Lori Miser, Director	City of Indianapolis	Department of Public Works	200 E. Washington St. City/County Bldg.	Suite 2460	Indianapolis	IN	46204	<a href="mailto:lori.miser@indy.gov">lori.miser@indy.gov</a>	317-327-4000
Joseph O'Connor	Marion County Commissioner		200 E. Washington St. City/County Bldg.	Suite 801	Indianapolis	IN	46204		(317) 327-4907
Julie L. Voorhies	Marion County Commissioner		200 E. Washington St. City/County Bldg.	Suite 801	Indianapolis	IN	46204		317-327-3001
Claudia O. Fuentes	Marion County Commissioner		200 E. Washington St. City/County Bldg.	Suite 801	Indianapolis	IN	46204		(317) 327-4040
Kim Pendorf	Delaware Nation of Oklahoma		31064 State Highway 281	PO Box 825	Anadarko	OK	73005	<a href="mailto:kpendrod@delawarenation.com">kpendrod@delawarenation.com</a>	405-247-2448 ext. 1403

US 36 Added Travel Lanes  
 Avon, Hendricks County, and Indianapolis, Marion County Indiana  
 DES Number: 1601072

**LIST OF INDIVIDUALS/AGENCIES/ORGANIZATIONS  
 INVITED TO BE SECTION 106 CONSULTING PARTIES**

Position	Agency 1	Agency 2	Address 1	Address 2	City	State	Zip	Email	Notes
	Eastern Shawnee Tribe of Oklahoma								
	Forest County Potawatomi Community								
<b>Diane Hunter</b>	<b>Miami Tribe of Oklahoma</b>		<b>P.O. Box 1326</b>		<b>Miami</b>	<b>OK</b>	<b>74355</b>	<a href="mailto:dunter@miamination.com">dunter@miamination.com</a>	<b>918-541-1300</b>
	Peoria Tribe of Indians of Oklahoma								
	Pokagon Band of Potawatomi Indians								
Property Owner	Rockville Marketplace LLC		586 S. SR 135	Suite G	Greenwood	IN	46142-144		
<b>Andrew Vinson</b>	<b>Andrew Vinson</b>		<b>PO Box 30173</b>		<b>Indianapolis</b>	<b>IN</b>	<b>46230</b>	<a href="mailto:vinsonproperties@gmail.com">vinsonproperties@gmail.com</a>	<b>317-833-3790</b>

Note: Federal Highway Administration (FHWA), INDOT Cultural Resources Office (INDOT-CRO) and the State Historic Preservation Officer (SHPO) are automatically consulting parties. Consulting parties that responded are in **bold** and highlighted.

Appendix D:

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# Consulting Parties Correspondence





# INDIANA DEPARTMENT OF TRANSPORTATION

100 North Senate Avenue  
Room N642  
Indianapolis, Indiana 46204

PHONE: (317) 234-5168

**Eric Holcomb, Governor**  
**Joe McGuinness, Commissioner**

September 26, 2018

This letter was sent to the listed parties.

RE: DES Number 1601072  
US 36 Added Travel Lanes Project  
Avon, Hendricks County & Indianapolis, Marion County, Indiana

Dear Consulting Party,

The Indiana Department of Transportation (INDOT) with funding from the Federal Highway Administration (FHWA) proposes to proceed with the US 36 Added Travel Lanes project, Des. No. 1601072. RQAW is under contract with INDOT to advance the environmental documentation for the referenced project.

This letter is part of the early coordination phase of the environmental review process requesting comments associated with this project. We are requesting comments from your area of expertise regarding any possible environmental effects associated with this project. Please use the above Des. Number and project description in your reply and your comments will be incorporated into the formal environmental study.

The proposed undertaking is on US 36 (Rockville Road) beginning at Shiloh Park Drive in Avon, Hendricks County, Indiana and continuing east to approximately 0.28 mile (1,500 feet) east of Raceway Road in Indianapolis, Marion County, Indiana. It is within Washington Township, Hendricks County, and Wayne Township, Marion County, Clermont USGS Topographic Quadrangle, in Sections 4, 5, 8 and 9, Township 15 North, Range 2 East.

The proposed added travel lane project will involve milling, resurfacing, and widening of the existing roadway to provide three through lanes in both the east and west bound directions on US 36. The typical cross section in each direction will consist of two 11-foot wide travel lanes, and a 12-foot wide right travel lane bordered by curb and gutter with a 2-foot off set. A left turn lane will be provided in both the eastbound and westbound directions at each signalized intersection. The length of the left turn lane will be provided as per the traffic operations analysis. Right turn lanes will be perpetuated and provided as per the traffic operations and analysis.

The need for this project is due to the existing inadequate level of service motorists experience when traveling through the corridor. The primary purpose of the added travel lane is to reduce delays of US 36 with a focus on the heavily congested portion between Ronald Reagan Parkway and Raceway Road. An additional desirable outcome of this project would be to reduce the crash rate through the corridor.

The existing horizontal alignment of US 36 will remain unchanged. The profile grade will match the existing profile grade. Drainage through the corridor will be addressed to minimize impacts and costs. Curb turnouts

with ditches and drive culverts are proposed to handle drainage between Shiloh Park Drive and Raceway Road. Between Raceway Road and the end of the project, an enclosed drainage system will be proposed to match the existing drainage system.

The right-of-way needed for this project is unknown at this time. However, minor amounts of permanent and temporary right-of-way are anticipated for construction activities within strips along existing right-of-way, corner cuts at intersections, and driveway reconstruction. The project is anticipated to be let in 2021. The proposed maintenance of traffic for the construction of the project utilizes phased construction. A work zone speed limit of 35 miles per hour (mph) will be utilized through the construction zone for the duration of the project. The lane configuration during construction will consist of a 12-foot outside lane, an 11-foot inside lane, and an 11-foot left turn lane at each intersection for all phases. Access to residences and businesses along US 36 will be continuously maintained during construction.

Section 106 of the National Historic Preservation Act requires federal agencies to take into account the effects of their undertakings on historic and archaeological properties. In accordance with 36 CFR 800.2 (c), you are hereby requested to be a consulting party to participate in the Section 106 process. Entities that have been invited to participate in the Section 106 consultation process for this project are identified in the attached list. Per 36 CFR 800.3(f), we hereby request that the Indiana State Historic Preservation Officer (SHPO) notify this office if the SHPO staff is aware of any other parties that may be entitled to be consulting parties or should be contacted as potential consulting parties for the project.

The Section 106 process involves efforts to identify historic properties potentially affected by the undertaking, assess its effects and seek ways to avoid, minimize or mitigate any adverse effects on historic properties. For more information regarding the protection of historic resources, please see the Advisory Council on Historic Preservation's guide: Protecting Historic Properties: A Citizen's Guide to Section 106 Review available online at <http://www.achp.gov/citizensguide.pdf>.

The Area of Potential Effects (APE) is the area in which the proposed project may cause alterations in the character or use of historic resources. At this time, no cultural resource investigations have occurred; however, the results of cultural resource identification and evaluation efforts, both above-ground and archaeological, will be forthcoming. Consulting parties will receive notification when these reports are completed.

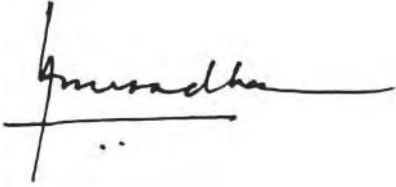
Please review the information and comment within thirty (30) calendar days of receipt. If you indicate that you do not desire to be a consulting party, or if you do not respond, you will not be included on the list of consulting parties for this project. If we do not receive your response in the time allotted, the project will proceed consistent with the proposed design and you will not receive further information about the project unless the design changes.

For questions concerning specific project details, you may contact Kyle Boot of RQAW at 317-588-1762 or [kboot@rqaw.com](mailto:kboot@rqaw.com). All future responses regarding the proposed project should be forwarded to RQAW at the following address:

Kyle Boot  
Architectural Historian  
RQAW  
8770 North Street, Suite 110  
Fishers, IN 46038  
[kboot@rqaw.com](mailto:kboot@rqaw.com).

Tribal contacts may contact Shaun Miller at [smiller@indot.in.gov](mailto:smiller@indot.in.gov) or 317-233-6795 or Michelle Allen at FHWA at [michelle.allen@dot.gov](mailto:michelle.allen@dot.gov) or 317-226-7344.

Sincerely,



Anuradha V. Kumar, Manager  
Cultural Resources Office  
Environmental Services

Enclosures: General Location Map  
USGS Topographic Map

Enclosures removed to  
avoid duplicates.

Distribution List:

- Chad Slider, for Indiana State Historic Preservation Officer (SHPO)
- Sam Burgess, Indiana Landmarks, Central Regional Office
- Nancy Johnson, Hendricks County Historical Society/Hendricks County Museum
- Jan Davis, Hendricks County Heritage Alliance
- John O. Parsons, Hendricks County Historian
- Steve Barnett, Marion County Historian
- Beuford Hall, Wayne Township Historical Society
- Anne Gremling, Indianapolis Metropolitan Planning Organization
- Tom Klein, MPA, Avon Town Manager
- Matthew Whetstone, Hendricks County Commissioner
- Phyllis A Palmer, Hendricks County Commissioner
- Robert Gentry, Hendricks County Commissioner
- John E. Ayers, P.E., Hendricks County Engineer
- Cathy Burkert, Hendricks County Highway Office Manager
- Don I. Reitz, AICP, Hendricks County Plan Commission Chair
- Joe Hogsett, Indianapolis Mayor
- Lori Miser, City of Indianapolis Department of Public Works Director
- Joseph O'Connor, Marion County Commissioner
- Julie L. Voorhies, Marion County Commissioner
- Claudia O. Fuentes, Marion County Commissioner
- Delaware Nation of Oklahoma
- Eastern Shawnee Tribe of Oklahoma
- Forest County Potawatomi Community
- Miami Tribe of Oklahoma
- Peoria Tribe of Indians of Oklahoma
- Pokagon Band of Potawatomi Indians

## Haylee Moscato

---

**From:** Kyle J. Boot  
**Sent:** Monday, October 1, 2018 9:33 AM  
**To:** 'sburgess@indianalandmarks.org'; museum@co.hendricks.in.us; hheritagealliance@gmail.com; jparsons@co.hendricks.in.us; srbarnett44@yahoo.com; anna.gremling@indy.gov; tklein@avongov.org; mwhetstone@co.hendricks.in.us; ppalmer@co.hendricks.in.us; bgentry@co.hendricks.in.us; jayers@co.hendricks.in.us; cburkert@co.hendricks.in.us; dreitz@co.hendricks.in.us; lori.miser@indy.gov; jvoorhies@indygov.org  
**Cc:** 'Kumar, Anuradha (akumar@indot.IN.gov)'; Branigin, Susan; Shaun Miller (smiller@indot.IN.gov); Ross, Anthony; 'Coon, Matthew (mcoon@indot.IN.gov)'; Joseph Dabkowski; Lisa Casler; Haylee Moscato; 'Richard Gilyeat (RGilyeat@indot.IN.gov)'; 'Asfahan Khan (akhan@indot.IN.gov)'; Krzton-Presson, Amy M; Diefenbaugh, Cedric A  
**Subject:** FHWA Project: Des. No. 1601072; US 36 Added Travel Lanes, Hendricks and Marion Counties, Indiana  
**Attachments:** US36ATLAvon\_Des1601072\_ECL\_2018-09-28.pdf

**Des. No.: 1601072**

**Project Description: US 36 Added Travel Lanes**

**Location: Avon, Hendrick County & Indianapolis, Marion County, Indiana**

The Indiana Department of Transportation, with funding from the Federal Highway Administration, proposes to proceed with the US 36 Added Travel Lanes project, Des. No. 1601072.

Section 106 of the National Historic Preservation Act requires federal agencies to take into account the effects of their undertakings on historic properties. The following agencies/individuals are being invited to become consulting parties:

Chad Slider, for Indiana State Historic Preservation Officer (SHPO)  
Sam Burgess, Indiana Landmarks, Central Regional Office  
Nancy Johnson, Hendricks County Historical Society/Hendricks County Museum  
Jan Davis, Hendricks County Heritage Alliance  
John O. Parsons, Hendricks County Historian  
Steve Barnett, Marion County Historian  
Beuford Hall, Wayne Township Historical Society  
Anne Gremling, Indianapolis Metropolitan Planning Organization  
Tom Klein, Avon Town Manager  
Matthew Whetstone, Hendricks County Commissioner  
Phyllis A Palmer, Hendricks County Commissioner  
Robert Gentry, Hendricks County Commissioner  
John E. Ayers, P.E., Hendricks County Engineer  
Cathy Burkert, Hendricks County Highway Office Manager  
Don I. Reitz, AICP, Hendricks County Plan Commission Chair  
Joe Hogsett, Indianapolis Mayor  
Lori Miser, City of Indianapolis Department of Public Works Director  
Joseph O'Connor, Marion County Commissioner  
Julie L. Voorhies, Marion County Commissioner  
Claudia O. Fuentes, Marion County Commissioner  
Delaware Nation of Oklahoma  
Eastern Shawnee Tribe of Oklahoma  
Forest County Potawatomi Community  
Miami Tribe of Oklahoma

Peoria Tribe of Indians of Oklahoma  
Pokagon Band of Potawatomi Indians

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Please review the letter located in IN SCOPE at <http://erms.indot.in.gov/Section106Documents/> (the Des. No. is the most efficient search term, once in IN SCOPE), and respond with your comments on any historic resource impacts incurred as a result of this project so that an environmental report can be completed. We also welcome your related opinions and other input to be considered in the preparation of the environmental document. If a hard copy of the materials is needed, please respond to this email with your request within seven (7) days.

Consulting parties have thirty (30) calendar days from receipt of this information to review and provide comment. If we do not receive a response from an invited consulting party in the time allotted, the project will proceed consistent with the proposed design. **Therefore, if we do not receive a response within thirty (30) days, your agency or organization will not receive any further information on the project unless the scope of work changes.**

Tribal contacts may contact Shaun Miller at [smiller@indot.in.gov](mailto:smiller@indot.in.gov) or 317-233-6795 or Michelle Allen at FHWA at [michelle.allen@dot.gov](mailto:michelle.allen@dot.gov) or 317-226-7344.

Thank you in advance for your input,  
Kyle Boot



**Kyle Boot | Architectural Historian**

8770 North St., Ste. 110

Fishers, IN 46038

O: 317.588.1798

C: 317.410.0845

[www.rqaw.com](http://www.rqaw.com)





## Haylee Moscato

---

**From:** Ross, Anthony <ARoss3@indot.IN.gov>  
**Sent:** Monday, October 1, 2018 10:11 AM  
**To:** dhunter@miamination.com; Matthew Bussler; thpo@estoo.net; lpappenfort@peoriatribe.com; Allison.Daniels@fcpotawatomi-nsn.gov; kpenrod@delawarenation.com  
**Cc:** Kyle J. Boot; Haylee Moscato; Miller, Shaun (INDOT); Kumar, Anuradha; Branigin, Susan; Allen, Michelle (FHWA)  
**Subject:** FHWA Project: Des. No. 1601072; US 36 Added Travel Lanes, Hendricks and Marion Counties, Indiana  
**Attachments:** US36ATLAvon\_Des1601072\_ECL\_2018-09-28.pdf

**Des. No.: 1601072**

**Project Description: US 36 Added Travel Lanes**

**Location: Avon, Hendricks County & Indianapolis, Marion County, Indiana**

The Indiana Department of Transportation, with funding from the Federal Highway Administration, proposes to proceed with the US 36 Added Travel Lanes project, Des. No. 1601072.

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John E. Ayers, P.E., Hendricks County Engineer  
Cathy Burkert, Hendricks County Highway Office Manager  
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Joe Hogsett, Indianapolis Mayor  
Lori Miser, City of Indianapolis Department of Public Works Director  
Joseph O'Connor, Marion County Commissioner  
Julie L. Voorhies, Marion County Commissioner  
Claudia O. Fuentes, Marion County Commissioner  
Delaware Nation of Oklahoma  
Eastern Shawnee Tribe of Oklahoma  
Forest County Potawatomi Community  
Miami Tribe of Oklahoma  
Peoria Tribe of Indians of Oklahoma  
Pokagon Band of Potawatomi Indians

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Tribal contacts may contact Shaun Miller at [smiller@indot.in.gov](mailto:smiller@indot.in.gov) or 317-233-6795 or Michelle Allen at FHWA at [michelle.allen@dot.gov](mailto:michelle.allen@dot.gov) or 317-226-7344.

Thank you in advance for your input,

**Anthony Ross, Ph.D.**

*Historian*

*Cultural Resources Office*

*Environmental Services*

100 N. Senate Ave., Room N642-ES

Indianapolis, IN 46204

**Office:** (317) 234-0142

**Email:** [aross3@indot.in.gov](mailto:aross3@indot.in.gov)



**\*\*Updated Historic Property Report (HPR) guidelines can be found [here](#)**



October 30, 2018

Kyle J. Boot  
Architectural Historian  
RQAW Environmental  
8770 North St., Ste. 110  
Fishers, IN 46038

Re: Des. No. 1601072, U.S. 36 Added Travel Lanes Project, Avon, Hendricks County and Indianapolis, Marion County, Indiana

Dear Mr. Boot:

Thank you for the opportunity to comment on the above undertaking. Indiana Landmarks agrees to serve as a consulting party for the project. We will look forward to reviewing the results of future cultural resource investigations within the area of potential effects.

Sincerely,

Sam Burgess  
Community Preservation Specialist



Division of Historic Preservation & Archaeology · 402 W. Washington Street, W274 · Indianapolis, IN 46204-2739  
Phone 317-232-1646 · Fax 317-232-0693 · dhpa@dnr.IN.gov · www.IN.gov/dnr/historic



November 1, 2018

Kyle Boot  
Architectural Historian  
RQAW  
8770 North Street, Suite 110  
Fishers, Indiana 46038

Federal Agency: Indiana Department of Transportation (“INDOT”),  
on behalf of Federal Highway Administration, Indiana Division (“FHWA”)

Re: Early coordination letter for the US 36 Added Travel Lanes Project, from Shiloh Park Drive  
in the Town of Avon, Hendricks County to approximately 0.28 mile east of Raceway Road  
in the City of Indianapolis, Marion County, Indiana (Des. No. 1601072; DHPA No. 23088)

Dear Mr. Boot:

Pursuant to Section 106 of the National Historic Preservation Act of 1966, as amended (54 U.S.C. § 306108), 36 C.F.R. Part 800, and the “Programmatic Agreement (PA) Among the Federal Highway Administration, the Indiana Department of Transportation, the Advisory Council on Historic Preservation and the Indiana State Historic Preservation Officer Regarding the Implementation of the Federal Aid Highway Program In the State of Indiana,” the staff of the Indiana State Historic Preservation Officer (“Indiana SHPO” or “INDNR-DHPA”) has reviewed INDOT’s September 26, 2018, early coordination letter, which we received on October 9 under your October 1 review request submittal form.

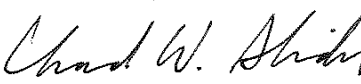
We have no additional recommendations for parties to invite to participate in the Section consultation on this undertaking.

We look forward to reviewing the reports on above-ground and archaeological cultural resources that INDOT indicated will be forthcoming, once they have been prepared in accordance with INDOT’s *Cultural Resources Manual*.

The archaeological reviewer for this project on the Indiana SHPO staff is Wade T. Tharp, and the structures reviewer is John Carr. However, if you have questions about our comments or about a procedural issue, please contact initially an INDOT Cultural Resources staff member who is assigned to this project.

In all future correspondence regarding this project (Des. No. 1601072), please refer to DHPA No. 23088.

Very truly yours,

  
Christopher A. Smith  
Deputy Director  
Indiana Department of Natural Resources

CAS:JLC:jlc

Kyle Boot  
November 1, 2018  
Page 2

emc: Anuradha Kumar, INDOT  
Shaun Miller, INDOT  
Susan Branigin, INDOT  
Shirley Clark, INDOT  
Kyle Boot, RQAW  
Wade T. Tharp, INDNR-DHPA  
John Carr, INDNR-DHPA



## Haylee Moscato

---

**From:** Kimberly Penrod <kpenrod@delawarenation.com>  
**Sent:** Tuesday, November 6, 2018 12:37 PM  
**To:** Ross, Anthony  
**Subject:** RE: FHWA Project: Des. No. 1601072; US 36 Added Travel Lanes, Hendricks and Marion Counties, Indiana

\*\*\*\* This is an EXTERNAL email. Exercise caution. DO NOT open attachments or click links from unknown senders or unexpected email. \*\*\*\*

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Anthony,  
The protection of our tribal cultural resources and tribal trust resources will take all of us working together. We look forward to working with you and your agency.  
With the information you have submitted we can concur at present with this proposed plan.

As with any new project, we never know what may come to light until work begins. The Delaware Nation asks that you keep us up to date on the progress of this project and if any discoveries arise please contact us immediately.

Our department is trying to go as paper free as possible. If it is at all feasible for your office to send email correspondence we would greatly appreciate.

If you need anything additional from me please do not hesitate to contact me.

*Respectfully,*

*Kim Penrod  
Delaware Nation  
Director, Historic Preservation  
31064 State Highway 281  
PO Box 825  
Anadarko, OK 73005  
(405)-247-2448 Ext. 1403 Office  
(405)-924-9485 Cell  
[kpenrod@delawarenation.com](mailto:kpenrod@delawarenation.com)*

*Unless someone like you cares a whole awful lot, nothing is going to get better. It's not. ~Dr. Seuss*

CONFIDENTIALITY NOTE:

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---

**From:** Ross, Anthony <ARoss3@indot.IN.gov>

**Sent:** Monday, October 1, 2018 9:11 AM

**To:** dhunter@miamination.com; Matthew Bussler <Matthew.Bussler@pokagonband-nsn.gov>; thpo@estoo.net; lpappenfort@peoriatribe.com; Allison.Daniels@fcpotawatomi-nsn.gov; Kimberly Penrod <kpenrod@delawarenation.com>

**Cc:** Kyle J. Boot <KBoot@RQAW.com>; Haylee Moscato <hmoscato@rqaw.com>; Miller, Shaun (INDOT) <smiller@indot.IN.gov>; Kumar, Anuradha <akumar@indot.IN.gov>; Branigin, Susan <SBranigin@indot.IN.gov>; Allen, Michelle (FHWA) <michelle.allen@dot.gov>

**Subject:** FHWA Project: Des. No. 1601072; US 36 Added Travel Lanes, Hendricks and Marion Counties, Indiana

**Des. No.: 1601072**

**Project Description: US 36 Added Travel Lanes**

**Location: Avon, Hendricks County & Indianapolis, Marion County, Indiana**

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Tribal contacts may contact Shaun Miller at [smiller@indot.in.gov](mailto:smiller@indot.in.gov) or 317-233-6795 or Michelle Allen at FHWA at [michelle.allen@dot.gov](mailto:michelle.allen@dot.gov) or 317-226-7344.

Thank you in advance for your input,

**Anthony Ross, Ph.D.**

*Historian*

*Cultural Resources Office*

*Environmental Services*

100 N. Senate Ave., Room N642-ES

Indianapolis, IN 46204

Office: (317) 234-0142

Email: [aross3@indot.in.gov](mailto:aross3@indot.in.gov)



**\*\*Updated Historic Property Report (HPR) guidelines can be found [here](#)**

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## Miami Tribe of Oklahoma

3410 P St. NW, Miami, OK 74354 • P.O. Box 1326, Miami, OK 74355  
Ph: (918) 541-1300 • Fax: (918) 542-7260  
[www.miamination.com](http://www.miamination.com)



November 7, 2018

Shaun Miller  
Archaeological Team Lead  
Cultural Resources Office  
Indiana DOT  
575 North Pennsylvania Street  
Indianapolis, IN 46204

Re: Des. No. 1601072 US 36 Added Travel Lanes, Hendricks and Marion Counties, Indiana –  
Comments of the Miami Tribe of Oklahoma

Dear Mr. Miller:

Aya, kikwehsitoole – I show you respect. My name is Diane Hunter, and I am the Tribal Historic Preservation Officer for the Federally Recognized Miami Tribe of Oklahoma. In this capacity, I am the Miami Tribe's point of contact for all Section 106 issues.

The Miami Tribe offers no objection to the above-mentioned project at this time, as we are not currently aware of existing documentation directly linking a specific Miami cultural or historic site to the project site. However, as this site is within the aboriginal homelands of the Miami Tribe, if any human remains or Native American cultural items falling under the Native American Graves Protection and Repatriation Act (NAGPRA) or archaeological evidence is discovered during any phase of this project, the Miami Tribe requests immediate consultation with the entity of jurisdiction for the location of discovery. In such a case, please contact me at 918-541-8966 or by email at [dhunter@miamination.com](mailto:dhunter@miamination.com) to initiate consultation.

The Miami Tribe accepts the invitation to serve as a consulting party to the proposed project. In my capacity as Tribal Historic Preservation Officer I am the point of contact for consultation.

Respectfully,

Diane Hunter  
Tribal Historic Preservation Officer



# INDIANA DEPARTMENT OF TRANSPORTATION

100 North Senate Avenue  
Room N642  
Indianapolis, Indiana 46204

PHONE: (317) 234-5168

**Eric Holcomb, Governor**  
**Joe McGuinness, Commissioner**

December 11, 2018

This letter was sent to the listed parties.

RE: US 36 Added Travel Lanes  
Des. No.: 1601072, DHPA No.: 23088  
Hendricks and Marion Counties, Indiana

Dear Consulting Party (see attached list),

The Indiana Department of Transportation (INDOT), with funding from the Federal Highway Administration, proposes to proceed with US 36 Added Travel Lanes in Hendricks & Marion Counties, Indiana (Des. No.: 1601072, DHPA No. 23088). RQAW is under contract with INDOT to advance the environmental documentation for the referenced project. The proposed undertaking is on US 36 (Rockville Road) beginning at Shiloh Park Drive in Avon, Hendricks County, Indiana and continuing east to approximately 0.28 mile (1,500 feet) east of Raceway Road in Indianapolis, Marion County, Indiana. It is within Washington Township, Hendricks County, and Wayne Township, Marion County, Clermont USGS Topographic Quadrangle, in Section 4, 5, 8 and 9, Township 15 North, Range 2 East. Land use in the project area is a mix of residential and commercial properties along US 36.

Section 106 of the National Historic Preservation Act requires federal agencies to take into account the effects of their undertakings on historic and archaeological properties. In accordance with 36 CFR 800.2 (c), an early coordination letter dated September 26, 2018 invited potential consulting parties to participate in the Section 106 process for this project. This letter is being sent to the individuals and organizations that accepted the invitation to become consulting parties. In addition, INDOT's Section 106 guidance recommends that owners of recommended historic resources be invited to become consulting parties. Therefore, this letter and enclosures are being mailed to those property owners. Owners of recommended historic resources are invited to view the early coordination letter on INSCOPE and to become a consulting party. If you indicate that you do not desire to be a consulting party, or if you do not respond, you will not be included on the list of consulting parties for this project.

The Section 106 process involves efforts to identify historic properties potentially affected by the undertaking, assess its effects and seek ways to avoid, minimize or mitigate any adverse effects on historic properties. For more information regarding the protection of historic resources, please see the Advisory Council on Historic Preservation's guide: Protecting Historic Properties: A Citizen's Guide to Section 106 Review available online at <http://www.achp.gov/citizensguide.pdf>.



The Area of Potential Effects (APE) is the area in which the proposed project may cause alterations in the character or use of historic resources. The APE contains no resources listed in the National Register of Historic Places (NRHP). A historian who meets the Secretary of the Interior's Professional Qualification Standards identified and evaluated above-ground resources within the APE for potential eligibility for the NRHP. As a result of the historic property identification and evaluation efforts, the Charles McLain House (IHSSI # 063-117-45024) at 10664 US 36 and the David Faucett House (IHSSI # 097-117-56005) at 9055 Rockville Road (US 36) are recommended as eligible for listing in the NRHP.

With regards to archaeological resources, an archaeologist who meets the Secretary of the Interior's Professional Qualification Standards, identified no sites within the project area. The Phase Ia archaeological reconnaissance has located no archaeological sites within the project area and it is recommended that the project be allowed to proceed as planned. As a result of these efforts, no further archaeology work is recommended within the survey area.

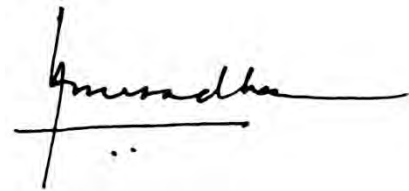
The Historic Property Report and Archaeology Report (Tribes only) are available for review in IN SCOPE at <http://erms.indot.in.gov/Section106Documents/> (the Des. No. is the most efficient search term, once in IN SCOPE). You are invited to review the HPR and Archaeological Report and respond with comments on any historic resource impacts incurred as a result of this project so that an environmental report can be completed. We also welcome your related opinions and other input to be considered in the preparation of the environmental document. If you prefer a hard copy of this material, please respond to this email with your request within seven (7) days.

For questions concerning specific project details, you may contact Kyle Boot of RQAW at 317-588-1762 or [kboot@rqaw.com](mailto:kboot@rqaw.com). All future responses regarding the proposed project should be forwarded to RQAW at the following address:

Kyle Boot  
Architectural Historian  
RQAW  
8770 North Street, Suite 110  
Fishers, IN 46038  
[kboot@rqaw.com](mailto:kboot@rqaw.com).

Tribal contacts may contact Shaun Miller at [smiller@indot.in.gov](mailto:smiller@indot.in.gov) or 317-233-6795 or Michelle Allen at FHWA at [michelle.allen@dot.gov](mailto:michelle.allen@dot.gov) or 317-226-7344.

Sincerely,

A handwritten signature in black ink, appearing to read 'Anuradha', written over a horizontal line. There are two small dots below the line.

Anuradha V. Kumar, Manager  
Cultural Resources Office  
Environmental Services

Enclosures: Historic Property Report  
Archeological Report

Distribution List: Chad Slider, for Indiana State Historic Preservation Officer (SHPO)  
Sam Burgess, Indiana Landmarks, Central Regional Office  
Property Owner of 9055 Rockville Rd.  
Property Owner of 10664 E. US 36.

## Haylee Moscato

---

**From:** Haylee Moscato  
**Sent:** Tuesday, December 11, 2018 3:00 PM  
**To:** 'sburgess@indianalandmarks.org'  
**Cc:** Kyle J. Boot; 'Ross, Anthony'; Coon, Matthew; Branigin, Susan; Miller, Shaun (INDOT); 'amartin@crai-ky.com'; Kumar, Anuradha; Gilyeat, Richard; Khan, Asfahan; Joseph Dabkowski; Lisa Casler  
**Subject:** FHWA Project: Des. No. 1601072; US 36 Added Travel Lanes, Hendricks and Marion Counties, Indiana  
**Attachments:** US36ATL\_Des1601072\_HPR\_CL\_2018-12-11.pdf

**Des. No.: 1601072**

**Project Description: US 36 Added Travel Lanes**

**Location: Avon, Hendrick County & Indianapolis, Marion County, Indiana**

The Indiana Department of Transportation, with funding from the Federal Highway Administration, proposes to proceed with the US 36 Added Travel Lanes project through Avon, Indiana, Des. No.: 1601072. The Section 106 Early Coordination Letter for this project was originally distributed on October 1, 2018.

As part of Section 106 of the National Historic Preservation Act, a Historic Property Report, Archaeology Report, and Coordination Letter have been prepared and are ready for review and comment by consulting parties.

Please review this documentation located in IN SCOPE at <http://erms.indot.in.gov/Section106Documents/> (the Des. No. is the most efficient search term, once in IN SCOPE), and respond with any comments that you may have. If a hard copy of the materials is needed, please respond to this email with your request within seven (7) days.

Consulting parties have thirty (30) calendar days from receipt of this information to review and provide comment. Tribal contacts may contact Shaun Miller at [smiller@indot.in.gov](mailto:smiller@indot.in.gov) or 317-233-6795 or Michelle Allen at FHWA at [michelle.allen@dot.gov](mailto:michelle.allen@dot.gov) or 317-226-7344.

Thank you in advance for your input,



**Haylee Moscato | Environmental Intern**

8770 North St., Ste. 110

Fishers, IN 46038

O: 317.588.1798

[www.rqaw.com](http://www.rqaw.com)



## Haylee Moscato

---

**From:** Ross, Anthony <ARoss3@indot.IN.gov>  
**Sent:** Tuesday, December 11, 2018 3:33 PM  
**To:** dhunter@miamination.com; kpenrod@delawarenation.com  
**Cc:** Kyle J. Boot; Haylee Moscato; Miller, Shaun (INDOT); Kumar, Anuradha; Branigin, Susan; Allen, Michelle (FHWA); Tharp, Wade; Carr, John  
**Subject:** RE: FHWA Project: Des. No. 1601072; US 36 Added Travel Lanes, Hendricks and Marion Counties, Indiana  
**Attachments:** US36ATL\_Des1601072\_HPR\_CL\_2018-12-11.pdf

**Des. No.: 1601072**

**Project Description: US 36 Added Travel Lanes**

**Location: Avon, Hendricks County & Indianapolis, Marion County, Indiana**

The Indiana Department of Transportation, with funding from the Federal Highway Administration, proposes to proceed with the US 36 Added Travel Lanes project through Avon, Indiana, Des. No.: 1601072. The Section 106 Early Coordination Letter for this project was originally distributed on October 1, 2018.

As part of Section 106 of the National Historic Preservation Act, a Historic Property Report, Archaeology Report, and Coordination Letter have been prepared and are ready for review and comment by consulting parties.

Please review this documentation located in IN SCOPE at <http://erms.indot.in.gov/Section106Documents/> (the Des. No. is the most efficient search term, once in IN SCOPE), and respond with any comments that you may have. If a hard copy of the materials is needed, please respond to this email with your request within seven (7) days.

Consulting parties have thirty (30) calendar days from receipt of this information to review and provide comment. Tribal contacts may contact Shaun Miller at [smiller@indot.in.gov](mailto:smiller@indot.in.gov) or 317-233-6795 or Michelle Allen at FHWA at [michelle.allen@dot.gov](mailto:michelle.allen@dot.gov) or 317-226-7344.

Thank you in advance for your input,

**Anthony Ross, Ph.D.**

*Historian*

*Cultural Resources Office*

*Environmental Services*

100 N. Senate Ave., Room N642-ES

Indianapolis, IN 46204

**Office:** (317) 234-0142

**Email:** [aross3@indot.in.gov](mailto:aross3@indot.in.gov)



**\*\*Updated Historic Property Report (HPR) guidelines can be found [here](#)**

---

**From:** Ross, Anthony

**Sent:** Monday, October 01, 2018 10:11 AM

**To:** 'dhunter@miamination.com' <dhunter@miamination.com>; 'Matthew Bussler' <Matthew.Bussler@pokagonband-nsn.gov>; 'thpo@estoo.net' <thpo@estoo.net>; lpappenfort@peoriatribe.com; 'Allison.Daniels@fcpotawatomi-nsn.gov' <Allison.Daniels@fcpotawatomi-nsn.gov>; 'kpenrod@delawarenation.com' <kpenrod@delawarenation.com>  
**Cc:** Kyle J. Boot <KBoot@RQAW.com>; 'Haylee Moscato' <hmoscato@rqaw.com>; Miller, Shaun (INDOT) <smiller@indot.IN.gov>; Kumar, Anuradha <akumar@indot.IN.gov>; Branigin, Susan (SBranigin@indot.IN.gov) <SBranigin@indot.IN.gov>; 'Allen, Michelle (FHWA)' <michelle.allen@dot.gov>  
**Subject:** FHWA Project: Des. No. 1601072; US 36 Added Travel Lanes, Hendricks and Marion Counties, Indiana

**Des. No.: 1601072**

**Project Description: US 36 Added Travel Lanes**

**Location: Avon, Hendricks County & Indianapolis, Marion County, Indiana**

The Indiana Department of Transportation, with funding from the Federal Highway Administration, proposes to proceed with the US 36 Added Travel Lanes project, Des. No. 1601072.

Section 106 of the National Historic Preservation Act requires federal agencies to take into account the effects of their undertakings on historic properties. The following agencies/individuals are being invited to become consulting parties:

Chad Slider, for Indiana State Historic Preservation Officer (SHPO)  
Sam Burgess, Indiana Landmarks, Central Regional Office  
Nancy Johnson, Hendricks County Historical Society/Hendricks County Museum  
Jan Davis, Hendricks County Heritage Alliance  
John O. Parsons, Hendricks County Historian  
Steve Barnett, Marion County Historian  
Beuford Hall, Wayne Township Historical Society  
Anne Gremling, Indianapolis Metropolitan Planning Organization  
Tom Klein, Avon Town Manager  
Matthew Whetstone, Hendricks County Commissioner  
Phyllis A Palmer, Hendricks County Commissioner  
Robert Gentry, Hendricks County Commissioner  
John E. Ayers, P.E., Hendricks County Engineer  
Cathy Burkert, Hendricks County Highway Office Manager  
Don I. Reitz, AICP, Hendricks County Plan Commission Chair  
Joe Hogsett, Indianapolis Mayor  
Lori Miser, City of Indianapolis Department of Public Works Director  
Joseph O'Connor, Marion County Commissioner  
Julie L. Voorhies, Marion County Commissioner  
Claudia O. Fuentes, Marion County Commissioner  
Delaware Nation of Oklahoma  
Eastern Shawnee Tribe of Oklahoma  
Forest County Potawatomi Community  
Miami Tribe of Oklahoma  
Peoria Tribe of Indians of Oklahoma  
Pokagon Band of Potawatomi Indians

This letter is part of the early coordination phase of the environmental review process requesting comments associated with this project. We are requesting comments from your area of expertise regarding any possible environmental effects associated with this project. Please use the above Des. Number and project description in your reply and your comments will be incorporated into the formal environmental study.

Please review the letter located in IN SCOPE at <http://erms.indot.in.gov/Section106Documents/> (the Des. No. is the most efficient search term, once in IN SCOPE), and respond with your comments on any historic resource impacts incurred as a



result of this project so that an environmental report can be completed. We also welcome your related opinions and other input to be considered in the preparation of the environmental document. If a hard copy of the materials is needed, please respond to this email with your request within seven (7) days.

Consulting parties have thirty (30) calendar days from receipt of this information to review and provide comment. If we do not receive a response from an invited consulting party in the time allotted, the project will proceed consistent with the proposed design. **Therefore, if we do not receive a response within thirty (30) days, your agency or organization will not receive any further information on the project unless the scope of work changes.**

Tribal contacts may contact Shaun Miller at [smiller@indot.in.gov](mailto:smiller@indot.in.gov) or 317-233-6795 or Michelle Allen at FHWA at [michelle.allen@dot.gov](mailto:michelle.allen@dot.gov) or 317-226-7344.

Thank you in advance for your input,

**Anthony Ross, Ph.D.**

*Historian*

*Cultural Resources Office*

*Environmental Services*

100 N. Senate Ave., Room N642-ES

Indianapolis, IN 46204

**Office:** (317) 234-0142

**Email:** [aross3@indot.in.gov](mailto:aross3@indot.in.gov)



**\*\*Updated Historic Property Report (HPR) guidelines can be found [here](#)**

## Haylee Moscato

---

**From:** Coon, Matthew <mcoon@indot.IN.gov>  
**Sent:** Thursday, December 13, 2018 8:22 AM  
**To:** 'kpenrod@delawarenation.com'; 'thpo@estoo.net'; 'Allison Daniels'; 'dhunter@miamination.com'; 'lpappenfort@peoriatribe.com'; 'Matthew.Bussler@pokagonband-nsn.gov'  
**Cc:** 'Allen, Michelle (FHWA)'; Miller, Shaun (INDOT); Haylee Moscato  
**Subject:** FHWA Project: Des. No. 1601072; US 36 Added Travel Lanes, Hendricks and Marion Counties, Indiana

**Des. No.: 1601072**

**Project Description: US 36 Added Travel Lanes**

**Location: Avon, Hendricks County & Indianapolis, Marion County, Indiana**

The Indiana Department of Transportation, with funding from the Federal Highway Administration (FHWA), proposes to proceed with the US 36 Added Travel Lanes project through Avon, Indiana, Des. No.: 1601072. The Section 106 Early Coordination Letter for this project was originally distributed on October 1, 2018.

As part of Section 106 of the National Historic Preservation Act, a Historic Property Report, Archaeology Report, and Coordination Letter have been prepared and are ready for review and comment by consulting parties.

Please review this documentation located in IN SCOPE at <http://erms.indot.in.gov/Section106Documents/> (the Des. No. is the most efficient search term, once in IN SCOPE), and respond with any comments that you may have. If a hard copy of the materials is needed, please respond to this email with your request within seven (7) days.

Consulting parties have thirty (30) calendar days from receipt of this information to review and provide comment. Tribal contacts may contact Shaun Miller at [smiller@indot.in.gov](mailto:smiller@indot.in.gov) or 317-233-6795 or Michelle Allen at FHWA at [michelle.allen@dot.gov](mailto:michelle.allen@dot.gov) or 317-226-7344.

Thank you in advance for your input,

**Matt Coon**

*Archaeologist, Cultural Resources Office  
INDOT Environmental Services  
100 N. Senate Avenue, Room N642  
Indianapolis, IN 46204  
Phone: 317.233.2083*



January 11, 2019

Kyle J. Boot  
Architectural Historian  
RQAW Environmental  
8770 North St., Ste. 110  
Fishers, IN 46038

Re: Des. No. 1601072, U.S. 36 Added Travel Lanes Project, Avon, Hendricks County and Indianapolis, Marion County, Indiana

Dear Mr. Boot:

Thank you for the continued opportunity to comment on the above undertaking. We concur with the assessment that there are two resources eligible for the National Register of Historic Places (NRHP) within the Area of Potential Effects (APE), namely, the Charles McLain House (IHSSI# 063-117-45024) (eligible under Criterion C) and the David Faucett House (IHSSI# 097-117-56005). We further concur with the proposal to assign NRHP-district boundaries commensurate with the platted parcel boundaries for those two resources.

We will look forward to remaining involved in the Section 106 process for this project.

Sincerely,

Sam Burgess  
Community Preservation Specialist



Indiana Department  
of Natural Resources

Eric Holcomb, Governor  
Cameron F. Clark, Director

Division of Historic Preservation & Archaeology · 402 W. Washington Street, W274 · Indianapolis, IN 46204-2739  
Phone 317-232-1646 · Fax 317-232-0693 · [dhp@dnr.IN.gov](mailto:dhp@dnr.IN.gov) · [www.IN.gov/dnr/historic](http://www.IN.gov/dnr/historic)



January 11, 2019

Kyle Boot  
Architectural Historian  
RQAW  
8770 North Street, Suite 110  
Fishers, Indiana 46038

Federal Agency: Indiana Department of Transportation (“INDOT”),  
on behalf of Federal Highway Administration, Indiana Division (“FHWA”)

Re: Indiana archaeological short report (Martin, 10/09/2018) and historic property report (Moscato, 12/11/2018), for the US 36 Added Travel Lanes, from Shiloh Park Drive in the Town of Avon, Washington Township, Hendricks County to approximately 0.28 mile east of Raceway Road in the City of Indianapolis, Wayne Township, Marion County, Indiana (Des. No. 1601072; DHPA No. 23088)

Dear Mr. Boot:

Pursuant to Section 106 of the National Historic Preservation Act of 1966, as amended (54 U.S.C. § 306108), 36 C.F.R. Part 800, and the “Programmatic Agreement (PA) Among the Federal Highway Administration, the Indiana Department of Transportation, the Advisory Council on Historic Preservation and the Indiana State Historic Preservation Officer Regarding the Implementation of the Federal Aid Highway Program In the State of Indiana,” the staff of the Indiana State Historic Preservation Officer (“Indiana SHPO” or “INDNR-DHPA”) has reviewed INDOT’s December 11, 2018, letter enclosing the aforementioned reports, which we received on December 14, 2018, for this project.

The area of potential effects (“APE”) proposed in the historic property report (“HPR”; Moscato, 12/11/2018) appears to be of adequate size to encompass the geographic area in which this project could cause direct or indirect effects.

For the purposes of the Section 106 review of this undertaking and based on the information provided in the HPR, we agree that the only above-ground historic properties within the APE identified in the HPR are the Charles McClain House (Indiana Historic Sites and Structures Inventory [IHSSI] No. 063-117-45024) at 10664 East US 36 in Avon and the David Faucett House (IHSSI No. 097-117-56005) at 9055 Rockville Road (US 36) in Indianapolis. We further agree that both appear to be eligible for inclusion in the National Register of Historic Places (“NRHP”) under Criterion C and that the David Faucett House also appears eligible under Criterion A, although we must qualify our opinion because restrictions on INDOT and its consultants regarding access to the privately-owned properties greatly limit our ability to evaluate the interior integrity of those properties. For the purposes of this review, we also agree with the proposed historic property boundaries of the Charles McClain House and the David Faucett House.

Can you confirm whether or not any elements of the former right-of-way of the Terre Haute, Indianapolis & Eastern Traction Company remain visible on the north side of the existing US 36 pavement within the APE? Figures 9 through 12 in the HPR depict the interurban line running parallel to and immediately north of the US 36 roadway at the time. It seems doubtful that rails or ties are extant, but we are wondering whether some stretches of the grade or other structures such as culverts might still be apparent.

The HPR indicates that the length of the left turn lanes that will be added and of the right turn lanes that will be perpetuated on US 36 by this project (apparently in addition to the new travel lanes) will be determined by the traffic operations analysis. We are

*The DNR mission: Protect, enhance, preserve and wisely use natural, cultural and recreational resources for the benefit of Indiana's citizens through professional leadership, management and education.*

[www.DNR.IN.gov](http://www.DNR.IN.gov)  
An Equal Opportunity Employer

Kyle Boot  
January 11, 2019  
Page 2

not told when that determination will be made. We urge that INDOT provide a fairly detailed plan of the widened roadway where it will pass adjacent to the Charles McClain House and adjacent to the David Faucett House by the time this Section 106 review reaches the adverse effects assessment step of the process. Otherwise, it will be difficult, if not impossible, for the other consulting parties and our office to draw our own conclusions about the project's effects on historic above-ground properties.

INDOT's September 26, 2018, early coordination letter had indicated that some temporary and permanent right-of-way likely will be required. It will be important, also, to show where the permanent right-of-way, in particular, would be acquired in the vicinity of the historic properties, in order to enable evaluation of effects.

In regard to archaeological resources, based on the submitted information and documentation available to the staff of the INDNR-DHPA, we have not identified any currently known archaeological resources listed in or eligible for inclusion in the NRHP within the proposed project area. However, this identification is subject to the project activities remaining within areas disturbed by previous construction of a recent and non-historical nature. If archaeological deposits are encountered from the post-contact period, they will be evaluated regarding their eligibility for the NRHP in consultation with the staff of INDNR-DHPA. Please contact our office if such deposits are encountered. The archaeological recording must be done in accordance with the Secretary of the Interior's "Standards and Guidelines for Archaeology and Historic Preservation" (48 F.R. 44716) and a report of the archaeological documentation must be submitted to our office for review and comment.

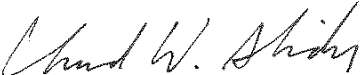
If any prehistoric or historic archaeological artifacts or human remains are uncovered during construction, demolition, or earthmoving activities, state law (Indiana Code 14-21-1-27 and -29) requires that the discovery be reported to the Indiana Department of Natural Resources, Division of Historic Preservation and Archaeology ("IDNR-DHPA") within two (2) business days. In that event, please call (317) 232-1646. Be advised that adherence to Indiana Code 14-21-1-27 and -29 does not obviate the need to adhere to applicable federal statutes and regulations, including but not limited to 36 C.F.R. Part 800.

We would appreciate receiving, along with or prior to the next regular correspondence on this project, e-mail addresses, if available, of the owners of the identified historic properties within the APE.

The archaeological reviewer for this project on the Indiana SHPO staff is Wade T. Tharp, and the structures reviewer is John Carr. However, if you have questions about our comments or about a procedural issue, please contact initially an INDOT Cultural Resources staff member who is assigned to this project.

In all future correspondence regarding the US 36 Added Travel Lanes in Washington Township of Hendricks County and Wayne Township of Marion County (Des. No. 1601072), please continue to refer to DHPA No. 23088.

Very truly yours,



Beth K. McCord  
Deputy State Historic Preservation Officer

BKM:JLC:WTT:wtt

cc: Property Owner, 10664 East US 36, Avon  
Property Owner, 9055 Rockville Road, Indianapolis

emc: Robert Dirks, P.E., FHWA  
Anuradha Kumar, INDOT  
Shaun Miller, INDOT  
Susan Branigin, INDOT  
Shirley Clark, INDOT  
Kyle Boot, RQAW  
Haylee Moscato, RQAW  
Andrew Martin, RPA, Cultural Resource Analysts, Inc.  
Sam Burgess, Indiana Landmarks  
Wade T. Tharp, INDNR-DHPA  
John Carr, INDNR-DHPA



## Kyle J. Boot

---

**From:** Miller, Shaun (INDOT) <smiller@indot.IN.gov>  
**Sent:** Tuesday, January 8, 2019 1:38 PM  
**To:** Kyle J. Boot; Ross, Anthony; Haylee Moscato  
**Subject:** 1601072 US 36 ATL in Avon

Hello,

I just talked to Mr. Andrew Vinson who owns the McCain House. He was curious about the letter and booklet he received. I talked to him a little bit about the project and Section 106/NEPA and I think he will ask to be included as a consulting party. He was very interested and nice to talk to. I asked that he reply to Kyle if wished to be a CP. Just letting you all know.

Thank you,

Shaun Miller  
Archaeological Team Lead  
INDOT, Cultural Resources Office  
smiller@indot.in.gov  
(317) 233-6795

January 15, 2019

## P H O N E L O G

Project: Des. Number 1601072  
US 36 Added Travel Lanes Project  
Avon, Hendricks and Marion County, Indiana

Participants: Mr. Andrew Vinson, Property Owner of the McClain house at 10664 E. US 36  
Kyle Boot, Architectural Historian, RQAW

- Recapped conversation between Mr. Vinson and Shaun Miller on 1/8/2019.
- Mr. Vinson inquired about the proposed work in front of his property and Mr. Boot explained the currently proposed design:
  - Existing 2-way center left turn lane will be replaced with a small curb/median and left turn lane will be established for westbound traffic only. This will prevent right turn lanes out of Mr. Vinson's property and only allow for right-in and right-out.
  - Currently there are two through travel lanes and a right turn lane to Ronald Reagan Parkway begins just west of Mr. Vinson's property. The proposed work will add a through travel lane and the right turn lane will be extended and begin taper in front of Mr. Vinson's property. This will push the pavement closer to Mr. Vinson's property and have a curb and gutter edge.
- Mr. Vinson inquired if his property would incur impacts and Mr. Boot explained:
  - No permanent right-of-way would be needed for the project.
  - The driveway will be replaced between the existing joint and the new roadway.
  - A few feet of sidewalk on either side of the driveway would likely need to be replaced. No other work will occur to the sidewalk, parking lot, or any other features.
  - The driveway and sidewalk reconstruction will likely require a small amount of temporary right-of-way for construction activities.
- Mr. Vinson inquired when construction would occur. Mr. Boot responded that according to the HPR the proposed letting is in 2021.
- Mr. Vinson provided his email address to future correspondence.

Kyle Boot  
RQAW Corporation



# INDIANA DEPARTMENT OF TRANSPORTATION

100 North Senate Avenue  
Room N642  
Indianapolis, Indiana 46204

PHONE: (317) 234-5168

**Eric Holcomb, Governor**  
**Joe McGuinness, Commissioner**

February 11, 2019

This letter was sent to the listed parties.

RE: Potential Effects  
DES Number: 1601072  
DHPA Number: 23088  
US 36 Added Travel Lanes Project from  
Shiloh Park Drive in Avon, Hendricks County to  
Raceway Road in Indianapolis, Marion County, Indiana

Dear Consulting Party (see attached list),

The Indiana Department of Transportation (INDOT) with funding from the Federal Highway Administration (FHWA) proposes to proceed with the US 36 Added Travel Lanes project, Des. No.: 1601072, DHPA No.: 23088 in Avon, Hendricks County and Indianapolis, Marion County, Indiana. In response to the Indiana State Historic Preservation Officer (SHPO) staff letter dated January 11, 2019, and consulting party correspondence (enclosed), this letter provides a professional evaluation of the project's anticipated effect on historic resources as part of the Section 106 process under 36 C.F.R. § 800.5.

The project is on US 36 (Rockville Road) beginning at Shiloh Park Drive in Avon, Hendricks County and continuing east to approximately 0.28 mile (1,500 feet) east of Raceway Road in Indianapolis, Marion County, Indiana. It is within Washington Township, Hendricks County, and Wayne Township, Marion County, Clermont USGS Topographic Quadrangle, in Sections 4, 5, 8 and 9, Township 15 North, Range 2 East. Please see enclosed project graphics.

Please note that a hard copy of this letter and enclosures are being submitted to the Indiana SHPO. In an effort to conserve resources, a copy of this letter and enclosures are posted to the Indiana Department of Transportation's online portal for Section 106 documents, also known as IN SCOPE (<http://erms.indot.in.gov/Section106Documents/>), for your review and comment. Once you have accessed the portal, you are able to search for the desired information by Des. No. Should you require a hard copy of this letter and enclosures, please contact this office and one will be mailed to you. Please note that all Section 106 documents for this project can be viewed at IN SCOPE.

## **Proposed Project**

The preferred alternative will involve roadwork to the existing roadway to provide additional travel lanes in both directions on US 36. The current typical cross section consists of two 12-foot wide travel lanes, a 16-foot wide two-way-left-turn-lane, and up to an 11-foot shoulder in each direction. A discontinuous 12-foot wide right-turn lane is present within portions of the project area. The proposed typical cross section in each direction will consist of three travel lanes (one 11-foot wide and two 12-foot wide) bordered by curb and gutter. An 11-foot wide left turn lane will be provided in both directions at signalized intersections and right turn lanes will be reconstructed. The length of turn lanes will vary and are determined by traffic analysis. At the current design phase, the anticipated right-of-way acquisition is expected not to exceed approximately 0.3 acre of permanent and approximately 0.7 acre of temporary right-of-way. The project is anticipated to be let in 2021. Traffic is anticipated to be maintained through the area of construction with two lanes being maintained in each direction. No signed detours are anticipated. Access to residences and businesses along US 36 will be continuously maintained during construction.

Per 36 CFR 800.9(a), the area of potential effect (APE) is the “the geographic area or areas within which an undertaking may directly or indirectly cause alterations in the character or use of historic properties. The APE is influenced by the scale and nature of an undertaking.” The APE for this project is an irregular polygon that generally encompasses those areas adjacent to the proposed work where ground-disturbing activity may occur, areas within a viewshed of the proposed work, and areas where other non-visual direct or indirect effects such as audible effects may occur. The Indiana SHPO staff concurred with the APE in a letter dated January 11, 2019, stating, “[it] appears to be of appropriate size for a project of this nature, in which direct or indirect effects could occur.” Please see the enclosed Proposed Work on Aerial Photographs Maps showing the APE.

## **Efforts to Identify Historic Properties and Consulting Party Correspondence**

Early coordination was initiated on October 1, 2018. The agencies/individuals/tribes listed below responded and agreed to be consulting parties.

1. Indiana State Historic Preservation Officer (automatic consulting party)
2. Indiana Landmarks, Central Regional Office
3. Delaware Nation of Oklahoma.

A historic property report (HPR) (Moscato, December 11, 2018) was completed for this project. The HPR recommended two properties as eligible for listing in the National Register of Historic Places (“NRHP”):

- Charles McLain House (Indiana Historic Sites and Structures Inventory [“IHSSI”] # 063-117-45024),
- David Faucett House (IHSSI # 097-117-56005).

An archaeological report (Phase Ia Archaeological Reconnaissance by Martin, October 9, 2018) was completed for this project. The archaeological reconnaissance identified no archaeological resources in the project area and recommended that the project be allowed to proceed.

On December 11, 2018, the HPR, archaeological report, and a coordination letter were mailed to the Indiana SHPO while other consulting parties were informed that the coordination letter, HPR, and archaeology report (tribes only) could be viewed electronically by accessing IN SCOPE. INDOT’s Section 106 guidance recommends that owners of historic resources be invited to become consulting parties. Therefore, the coordination letter and HPR were mailed to those property owners listed below on December 11, 2018 (the owner identified in bold responded and agreed to be a consulting party).

1. Rockville Marketplace LLC, Historic Property Owner
2. **Andrew Vinson, Historic Property Owner**

In a letter dated January 11, 2019 (enclosed), the Indiana SHPO staff concurred with the recommendations made in the archaeology report and the HPR. However, in regard to the possibility of additional historic resources, the Indiana SHPO staff asked whether “any elements of the former right-of-way of the Terre Haute, Indianapolis & Eastern Traction Company remain visible on the north side of the existing US 36 pavement within the APE.”

In response to the Indiana SHPO staff’s inquiry: the investigation and fieldwork for the HPR and the archeology report did not identify rails, ties, structures, stretches of grade, or other remnants or features from the Terre Haute, Indianapolis, & Eastern Traction Company. It is believed that any remnants from the Terre Haute, Indianapolis, & Eastern Traction Company were demolished during the replacement and widening of US 36 during the 1970s. According to Kingsbury, the line was abandoned in 1930.<sup>1</sup> An aerial photograph from 1937 contains a marking that may indicate remnants of this line along the northside of a then narrow Rockville Road (Figure 1).<sup>2</sup> During the 1970s, the roadway was removed and replaced with a wider concrete road. The alignment of the new road shifted to the north and probably encroached upon the right-of-way of the former Terre Haute, Indianapolis, & Eastern Traction Company line. Plans from 1971 depict a “Private Drive (Gravel & Cinders)” which is very likely the former railroad bed (please see the 1971 Plan Sheets enclosed). The plans also depict the previous Rockville Road and proposed wider multi-lane roadway. The proposed roadway’s south edge is approximately at the same location of the previous roadway, while the north edge of the proposed roadway is approximately 40 feet north of the previous road edge (please see the 1971 Plan Sheets enclosed). Therefore, it is believed that any remnants from the Terre Haute, Indianapolis, & Eastern Traction Company line were demolished during the replacement and widening of US 36 during the 1970s.

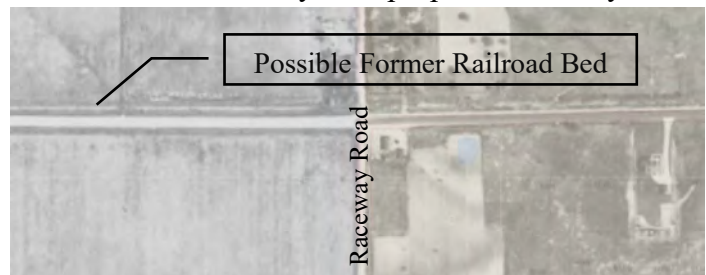


Figure 1: 1937 Aerial Photograph, MapIndy

The Indiana SHPO staff also inquired about the inclusion of a fairly detailed plan of the widened roadway where it will pass adjacent to the Charles McLain House and the David Faucett House. Additionally, they indicated the importance of showing where right-of-way would be acquired in the vicinity of the historic properties in order to evaluate effects. In response, the description of the proposed alterations and anticipated right-of-way in the vicinity of the historic properties and the Preliminary Stage 1 Plans demonstrate the plan for the widened roadway and anticipated right-of-way near historic resources. Please find the descriptions below and the plans (with historic properties highlighted) enclosed.

<sup>1</sup> Robert C. Kingsbury and John M. Hollingsworth, *An Atlas of Indiana*, Occasional Publication, no. 5 (Bloomington, IN: Dept. of Geography, Indiana University, 1970), 78.

<sup>2</sup> City of Indianapolis, *MapIndy*, Web Map (City of Indianapolis, January 18, 2019), <http://maps.indy.gov/MapIndy/>.



In a letter dated January 11, 2019, Indiana Landmarks concurred with the recommendations made in the HPR. Please see this letter enclosed.

In a phone call on January 8, 2019, Mr. Andrew Vinson spoke with Shaun Miller of INDOT regarding the project, as well as the Section 106 and the National Environmental Policy Act processes. On January 15, 2019, Kyle Boot (RQAW Architectural Historian) followed up with Mr. Vinson who inquired about the proposed work near the Charles McLain property. The minor potential impacts were explained to Mr. Vinson who provided no response regarding potential effects to the historic characteristics of the Charles McLain property. Mr. Vinson was added to the list of consulting parties. Please see the phone log of this conversation enclosed.

### **Historic Properties and Potential Effects**

The following two properties eligible for listing in the NRHP within the APE are located along US 36 (Rockville Road). The undertaking's effects on historic properties is primarily visual within an already extensively built-up US highway corridor, suburban landscape. The visual effects are likely to include:

- Pavement replacement on the same alignment within the existing 12-foot wide travel lanes. The pavement may be milled and overlaid in some locations, depending on existing conditions or associated excavation needs.
- Pavement widening with the addition of a travel lane in each direction.
- Installation of a curb and gutter along the roadway edge.
- Conversion of the two-way center turn lane to a dedicated left turn lane with small curb. The left turn lanes will be installed for each direction at signaled intersections, but all other intersections will be accessible only via right-in and right-out.
- Reconstruction and lengthening of the existing right turn lanes.

Please see the Stage 1 Plans enclosed with the following NRHP-eligible resources highlighted.

#### **Charles McLain House (IHSSI # 063-117-45024) 10664 E. US 36**

The Charles McLain House is recommended eligible under Criterion C because it is a good example (although with alterations) of a circa 1904 Free Classic Queen Ann style house with original architectural features in Washington Township, Hendricks County. Project impacts adjacent to the historic resource boundary will include roadway widening and reconstruction, driveway reconstruction, and limited sidewalk reconstruction to tie the existing sidewalk into the new driveway. No permanent right-of-way is anticipated from the Charles McLain House; however, approximately 0.02 acre of temporary right-of-way is needed for driveway and adjacent sidewalk reconstruction. The existing roadway, driveway, parking lot, and sidewalk are modern concrete construction. The sidewalk, parking lot, and driveway were installed circa 2013.<sup>3</sup> The historic resource boundary goes to the back (north edge) of the existing sidewalk. The anticipated construction limits will not extend into the historic resource boundary (please see the Photo Effects Diagram and the Stage 1 Plans, sheets 9 and 17 enclosed).

In terms of the Charles McLain House setting, the proposed work in this area will widen the US 36 pavement by approximately 17 feet to add a travel lane, recreate and extend the existing right turn lane, and install a curb and gutter. The added travel lane near the Charles McLain House is anticipated to provide only a 10% increase

<sup>3</sup> Google, *Google Earth* (Google), accessed January 18, 2019, <https://www.google.com/earth/>.

in capacity from 2021 to 2041 (please see the traffic data on sheet one of the enclosed Stage 1 Plans). The property's setting has been drastically altered by development over recent decades from the expansion of US 36 and the property's own conversion to commercial use (i.e. parking lot and sidewalk construction). Additionally, note that the Charles McLain House significance is limited to architecture (Criterion C), so the resource's integrity is less sensitive to setting changes than resources that derive significance from Criterion A.

The project's minimal introduction of visual and audible elements to the property's setting will not constitute a significant alteration to historic features that contribute to the Charles McLain House historic significance because of the already transformed, suburban landscape. Although permanent work is proposed near the Charles McLain House property, it is anticipated that there will be no adverse effect to the house or carriage house because no contributing historic features will be impacted directly. Therefore, the alterations adjacent to the historic resource boundary and its setting are not anticipated to reduce the significance or impact any of the characteristics that qualify the Charles McLain House for the NRHP in a manner that would diminish the integrity of the property's location, design, setting, materials, workmanship, feeling, or association. As a result, the project is anticipated to have "No Adverse Effect" to the Charles McLain House.

### **David Faucett House (IHSSI # 097-117-56005) 9055 Rockville Rd. (US 36)**

The David Faucett House is recommended eligible under Criterion A for its association with the early development of Wayne township, Marion County and Washington township, Hendricks County. The David Faucett house may very well be one of the last, if not the last mid-19th century farmhouse in Wayne Township. Also, the David Faucett House is recommended eligible under Criterion C because it is a good example (although with alterations) of a circa 1846 I-house with Greek Revival architectural features in Wayne Township, Marion County.

Project impacts near the historic resource boundary of the David Faucett House will be strictly visual and limited to the existing right-of-way. Neither temporary nor permanent right-of-way will be required from the David Faucett House. In terms of the David Faucett House setting, the east end of new construction will occur approximately 99 feet northwest of the David Faucett House historic property boundary; while the east end of the incidental construction consisting of pavement milling to transition the new construction to the existing pavement will occur approximately 35 feet northwest of the David Faucett House boundary. No work will occur within the historic boundary nor immediately adjacent to the frontage of it (please see the Stage 1 Plans, sheets 12 and 21 enclosed).

The David Faucett House's setting has been drastically altered by development over recent decades from the expansion of US 36. The project's minimal introduction of visual and audible elements to the property's setting will not constitute a significant alteration from this already transformed, suburban landscape. Thus, the alterations near the historic resource boundary and neighboring visible changes (as described above) will have no direct impact on the David Faucett House other than a viewshed change approximately 99 feet away for pavement replacement and the addition of a curb and gutter. This minor viewshed alteration is not anticipated to reduce the significance or impact any of the characteristics that qualify the David Faucett House for the NRHP in a manner that would diminish the integrity of the property's location, design, setting, materials, workmanship, feeling, or association. Therefore, the project is anticipated to have "No Adverse Effect" to the David Faucett House.

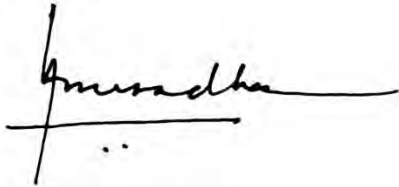
Please review the information in this letter and comment within fifteen (15) calendar days of receipt. For questions concerning specific project details, you may contact Kyle Boot of RQAW at 317-588-1762 or [kboot@rqaw.com](mailto:kboot@rqaw.com). All future responses regarding the proposed project should be forwarded to RQAW at the following address:

Kyle Boot  
Architectural Historian  
RQAW  
8770 North Street, Suite 110  
Fishers, IN 46038  
[kboot@rqaw.com](mailto:kboot@rqaw.com)

Tribal contacts may contact Shaun Miller at [smiller@indot.in.gov](mailto:smiller@indot.in.gov) or 317-233-6795 or Michelle Allen at FHWA at [michelle.allen@dot.gov](mailto:michelle.allen@dot.gov) or 317-226-7344.

Upon review and comment, within fifteen (15) calendar days, from consulting parties on the information provided in this letter and previous correspondence, the INDOT, on behalf of FHWA, will finish assessing potential impacts to historic resources and issue a Section 106 finding.

Sincerely,

A handwritten signature in black ink, appearing to read "Anuradha", with a horizontal line underneath and a vertical line extending downwards from the left side.

Anuradha V. Kumar, Manager  
Cultural Resources Office  
Environmental Services

Enclosures:  
Consulting Party Correspondence  
Project Graphics

Distribution List:  
Indiana State Historic Preservation Officer (SHPO)  
Anthony Ross, INDOT-CRO  
Michelle Allen, FHWA  
Indiana Landmarks, Central Regional Office  
Mr. Andrew Vinson, Historic Property Owner  
Delaware Nation of Oklahoma  
Miami Tribe of Oklahoma  
Kyle Boot, RQAW Corporation  
Haylee Moscato, RQAW Corporation

Enclosure:  
Consulting Party Correspondence

Enclosures removed  
from Effects Letter to  
avoid duplication.

- SHPO letter, January 11, 2019
- Indiana Landmarks letter, January 11, 2019
- Mr. Andrew Vison phone log, January 15, 2019

## Enclosure: Project Graphics

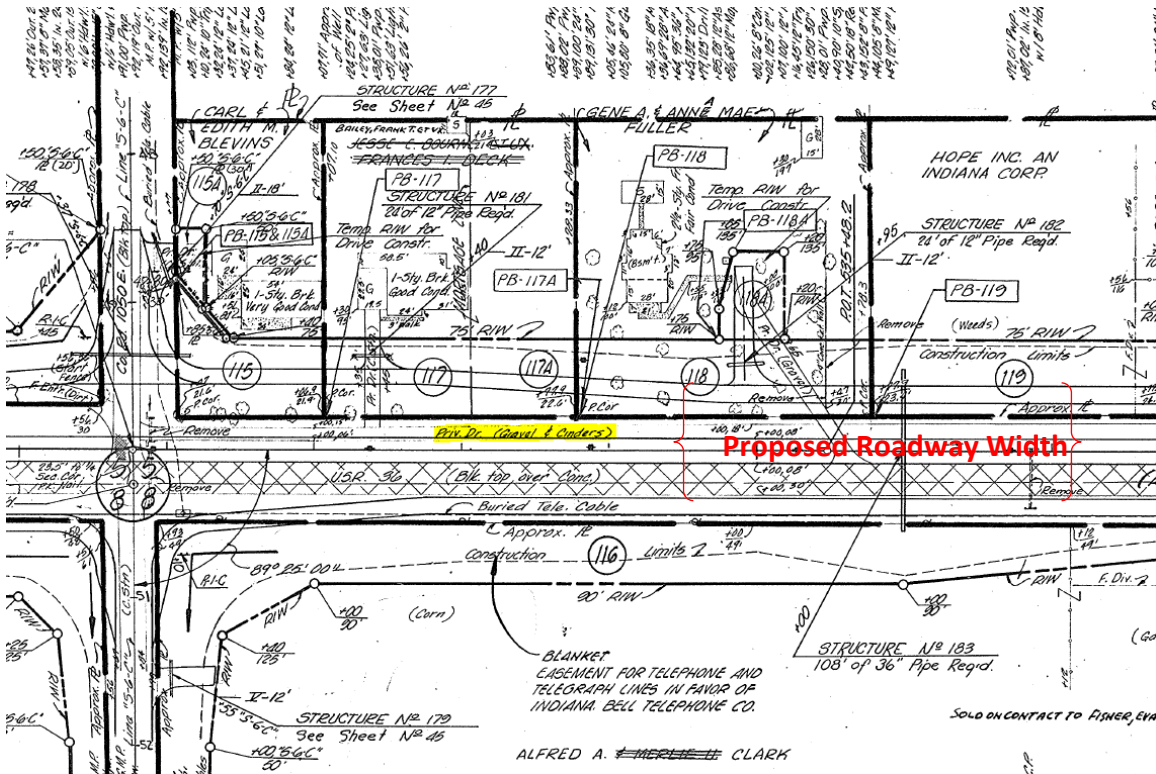
Enclosures removed  
from Effects Letter to  
avoid duplication.  
Please see the 800.11  
Appendix A.

- 1971 Plan Sheets
- Proposed Work on Aerial Photograph Maps
- Photo Effects Diagram at Charles McLain House
- Preliminary Stage 1 Plans, select pages with NRHP-eligible resources highlighted

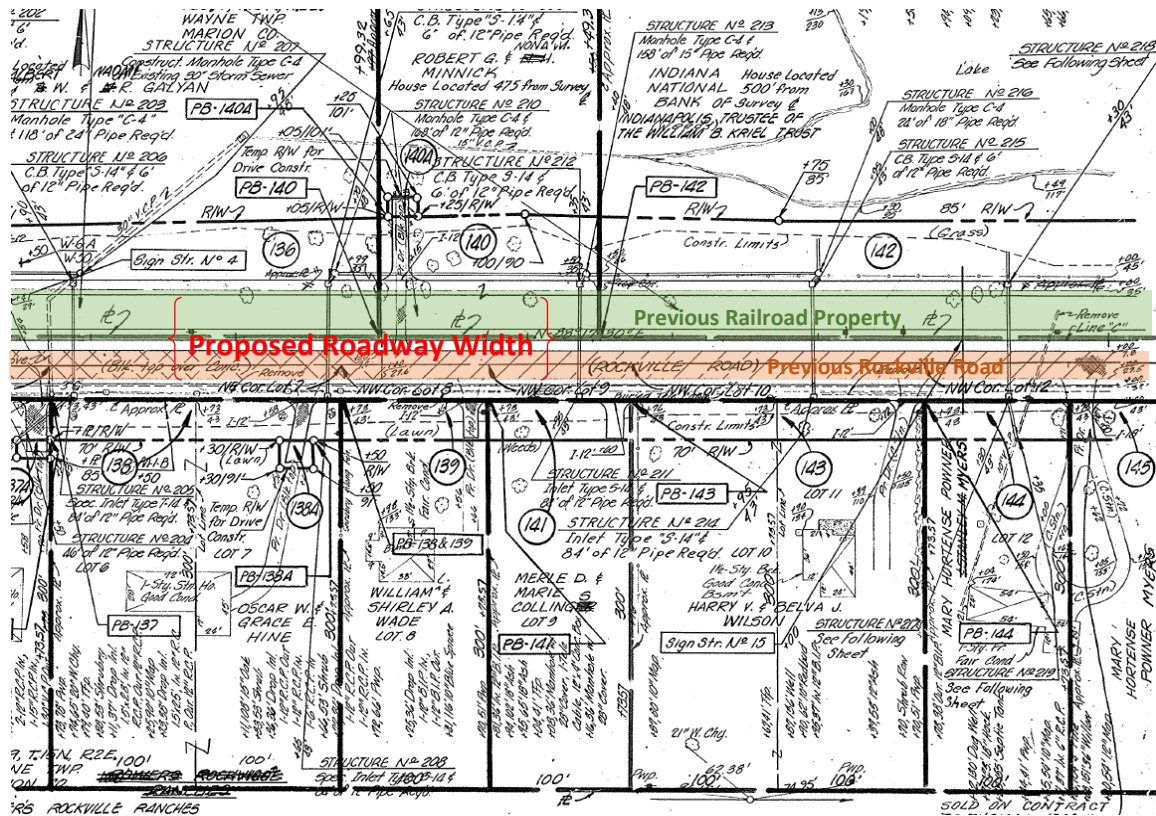
Enclosures removed  
from Effects Letter to  
avoid duplication.  
Please see the 800.11  
Appendix F.



1971 Plan Sheets



Plan and Profile of Proposed State Highway ST-F Project No. 86, Sheet 27, 1971

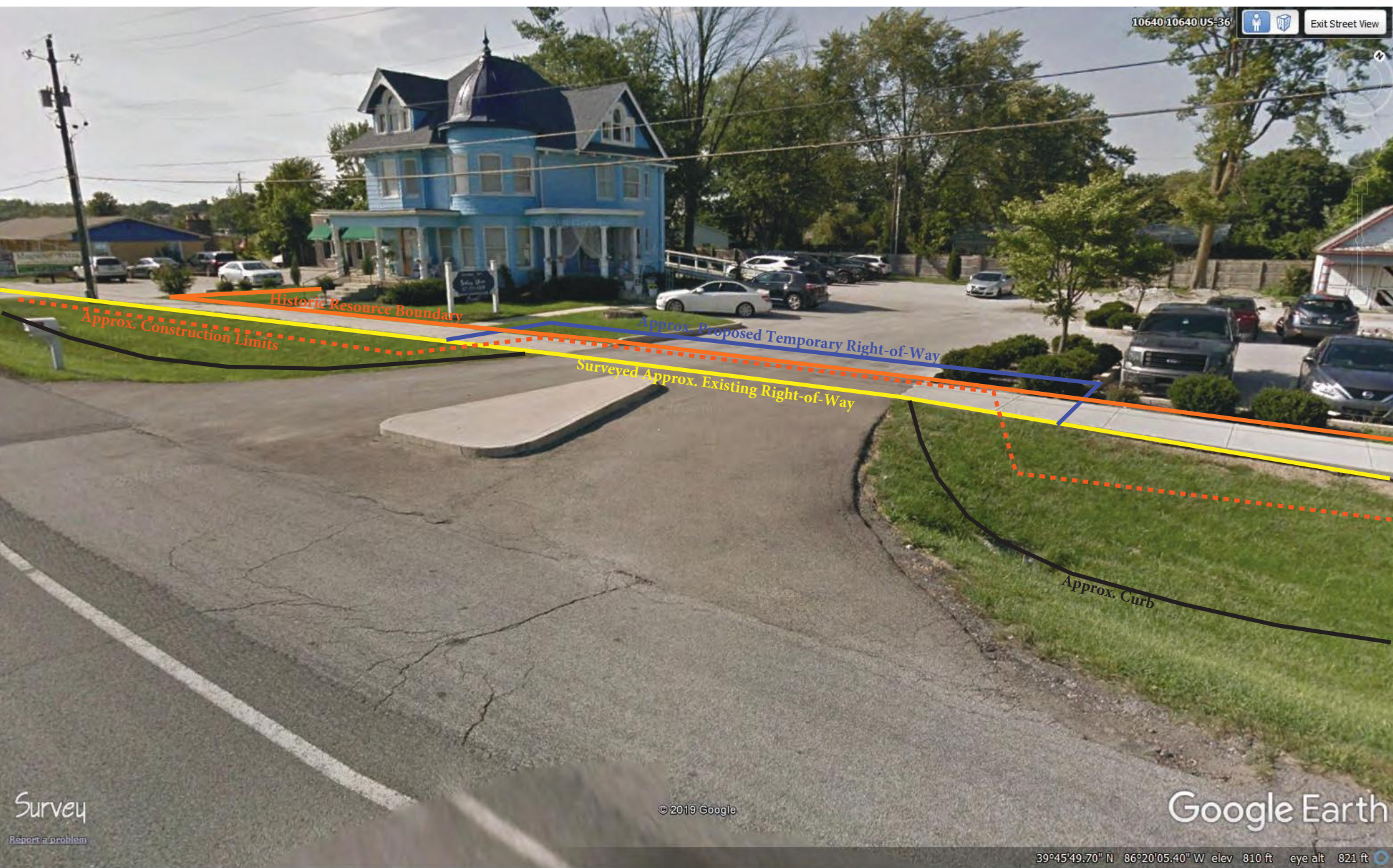


Plan and Profile of Proposed State Highway ST-F Project No. 86, Sheet 29, 1971



10640 10640 US-36

Exit Street View



Survey  
Report a problem

© 2019 Google

Google Earth

39°45'49.70" N 86°20'05.40" W elev 810 ft eye alt 821 ft

## Kyle J. Boot

---

**From:** Haylee Moscato  
**Sent:** Wednesday, February 13, 2019 3:41 PM  
**To:** vinsonproperties@gmail.com; sburgess@indianalandmarks.org  
**Cc:** Kyle J. Boot; Ross, Anthony; Miller, Shaun (INDOT); Branigin, Susan; Kumar, Anuradha; Joseph Dabkowski; Dylan Sievers; Lisa Casler; Aaron Lawson; Coon, Matthew; Gilyeat, Richard  
**Subject:** FHWA Project: Des. No. 1601072; US 36 Added Travel Lanes, Hendricks and Marion Counties, Indiana  
**Attachments:** US36ATL\_Des1601072\_EffectsLtr\_2019-02-11.pdf

### Des. No.: 1601072

### Project Description: US 36 Added Travel Lanes

### Location: Avon, Hendricks and Marion Counties, Indiana

The Indiana Department of Transportation, with funding from the Federal Highway Administration, proposes to proceed with US 36 added travel lanes project through Avon, Indiana, Des. No.: 1601072. This correspondence is part of the coordination phase of the environmental review process. In response to the Indiana State Historic Preservation Officer (SHPO) staff letter dated January 11, 2019, and consulting party correspondence, this correspondence provides a professional evaluation of the project's anticipated effect on historic resources as part of the Section 106 process under 36 C.F.R. § 800.5.

Please review this documentation located in IN SCOPE at <http://erms.indot.in.gov/Section106Documents/> (the Des. No. is the most efficient search term, once in IN SCOPE), and respond with any comments that you may have. If a hard copy of the materials is needed, please respond to this email with your request within seven (7) days.

Consulting parties have fifteen (15) calendar days from receipt of this information to review and provide comment. Tribal contacts may contact Shaun Miller at [smiller@indot.in.gov](mailto:smiller@indot.in.gov) or 317-233-6795 or Michelle Allen at FHWA at [michelle.allen@dot.gov](mailto:michelle.allen@dot.gov) or 317-226-7344.

Thank you in advance for your input,



Haylee Moscato | Architectural Historian

8770 North St., Ste. 110

Fishers, IN 46038

O: 317.588.1798

[www.rqaw.com](http://www.rqaw.com)





## Kyle J. Boot

---

**From:** Ross, Anthony <ARoss3@indot.IN.gov>  
**Sent:** Wednesday, February 13, 2019 3:54 PM  
**To:** dhunter@miamination.com; kpenrod@delawarenation.com; Tharp, Wade; Carr, John  
**Cc:** Kumar, Anuradha; Branigin, Susan; Miller, Shaun (INDOT); Allen, Michelle (FHWA); Haylee Moscato; Kyle J. Boot; McMullen, Kenneth B; Slider, Chad (DNR); Dhpacommentsfromcro, Dnr  
**Subject:** FHWA Project: Des. No. 1601072; US 36 Added Travel Lanes, Hendricks and Marion Counties, Indiana

**Des. No.: 1601072**

**Project Description: US 36 Added Travel Lanes**

**Location: Avon, Hendricks and Marion Counties, Indiana**

The Indiana Department of Transportation, with funding from the Federal Highway Administration, proposes to proceed with US 36 added travel lanes project through Avon, Indiana, Des. No.: 1601072. This correspondence is part of the coordination phase of the environmental review process. The Section 106 Early Coordination Letter for this project was originally distributed on December 11, 2018.

In response to the Indiana State Historic Preservation Officer (SHPO) staff letter dated January 11, 2019, and consulting party correspondence, this correspondence provides a professional evaluation of the project's anticipated effect on historic resources as part of the Section 106 process under 36 C.F.R. § 800.5.

Please review this documentation located in IN SCOPE at <http://erms.indot.in.gov/Section106Documents/> (the Des. No. is the most efficient search term, once in IN SCOPE), and respond with any comments that you may have. If a hard copy of the materials is needed, please respond to this email with your request within seven (7) days.

Consulting parties have fifteen (15) calendar days from receipt of this information to review and provide comment. Tribal contacts may contact Shaun Miller at [smiller@indot.in.gov](mailto:smiller@indot.in.gov) or 317-233-6795 or Michelle Allen at FHWA at [michelle.allen@dot.gov](mailto:michelle.allen@dot.gov) or 317-226-7344.

Thank you in advance for your input,

**Anthony Ross, Ph.D.**

*Historian*

*Cultural Resources Office*

*Environmental Services*

100 N. Senate Ave., Room N642-ES

Indianapolis, IN 46204

**Office:** (317) 234-0142

**Email:** [aross3@indot.in.gov](mailto:aross3@indot.in.gov)



**\*\*Updated Historic Property Report (HPR) guidelines can be found [here](#)**



Division of Historic Preservation & Archaeology · 402 W. Washington Street, W274 · Indianapolis, IN 46204-2739  
Phone 317-232-1646 · Fax 317-232-0693 · dhpa@dnr.IN.gov · www.IN.gov/dnr/historic



February 19, 2019

Kyle Boot  
Architectural Historian  
RQAW  
8770 North Street, Suite 110  
Fishers, Indiana 46038

Federal Agency: Indiana Department of Transportation (“INDOT”),  
on behalf of Federal Highway Administration, Indiana Division (“FHWA”)

Re: Detailed project information and effects assessment for the US 36 Added Travel Lanes, from Shiloh Park Drive in the Town of Avon, Washington Township, Hendricks County, to approximately 0.28 mile east of Raceway Road in the City of Indianapolis, Wayne Township, Marion County, Indiana (Des. No. 1601072; DHPA No. 23088)

Dear Mr. Boot:

Pursuant to Section 106 of the National Historic Preservation Act of 1966, as amended (54 U.S.C. § 306108), 36 C.F.R. Part 800, and the “Programmatic Agreement (PA) Among the Federal Highway Administration, the Indiana Department of Transportation, the Advisory Council on Historic Preservation and the Indiana State Historic Preservation Officer Regarding the Implementation of the Federal Aid Highway Program In the State of Indiana,” the staff of the Indiana State Historic Preservation Officer (“Indiana SHPO” or “INDNR-DHPA”) has reviewed INDOT’s February 11, 2019, letter and enclosures, which we received in paper form on February 15, 2019.

We appreciate having received the additional information about the likely destruction in the 1970s, during an earlier widening of US 36, of whatever had remained of the Terre Haute, Indianapolis & Eastern Traction Company roadbed within the current project area. We also appreciate the detailed information and plans depicting how US 36 will be modified by this project in the vicinity of the Charles McLain House at 10664 East US 36 in Avon and the David Faucett House at 9055 Rockville Road (US 36) in Indianapolis.

It appears that the proposed construction, for the most part, will come no closer to the David Faucett House than do the existing US 36 roadway improvements. Given the existing, previously widened US 36 highway in front of that historic house, it does not seem likely that the currently proposed modifications, most of which end to the west of the property, would have an indirect adverse effect on the David Faucett House.

We note that on the plans and the marked street view photograph, the previously agreed upon historic property boundary of the historic Charles McClain House is shown as the parcel boundary, which ends at the back (house side) edge of the existing sidewalk. However, on some of those exhibits, the apparent, existing right-of-way and property line is shown at the front edge (highway side) of the sidewalk. We are not sure what accounts for this apparent discrepancy, but we are content to treat the back edge of the sidewalk as the historic property boundary, which is the usual way of delineating the boundary of an individual historic property in an urban area.



According to INDOT's February 11 letter, the proposed improvements to US 36 in front of the Charles McClain House will widen the pavement by about 17 feet, which, judging by the visual exhibits, will bring it to within several feet of the sidewalk. We anticipate that the widening will make a noticeable change to the setting of the house. As the letter also indicates, however, the house is considered to be eligible for inclusion in the National Register of Historic Places ("NRHP") only for its architecture, under NRHP Criterion C. Furthermore, given the already wide and busy US 36 in front of the house, we do not think that the integrity of any characteristic of the house that makes it significant will be demonstrably diminished.

As we said in our January 11, 2019, letter in regard to archaeological resources, based on the submitted information and documentation available to the staff of the INDNR-DHPA, we have not identified any currently known archaeological resources listed in or eligible for inclusion in the NRHP within the proposed project area. However, this identification is subject to the project activities remaining within areas disturbed by previous construction of a recent and non-historical nature. If archaeological deposits are encountered from the post-contact period, they will be evaluated regarding their eligibility for the NRHP in consultation with the staff of INDNR-DHPA. Please contact our office if such deposits are encountered. The archaeological recording must be done in accordance with the Secretary of the Interior's "Standards and Guidelines for Archaeology and Historic Preservation" (48 F.R. 44716) and a report of the archaeological documentation must be submitted to our office for review and comment.

If any prehistoric or historic archaeological artifacts or human remains are uncovered during construction, demolition, or earthmoving activities, state law (Indiana Code 14-21-1-27 and -29) requires that the discovery be reported to INDNR-DHPA within two (2) business days. In that event, please call (317) 232-1646. Be advised that adherence to Indiana Code 14-21-1-27 and -29 does not obviate the need to adhere to applicable federal statutes and regulations, including but not limited to 36 C.F.R. Part 800.

INDOT has requested, without explanation, that all consulting parties and the Indiana SHPO comment within 15 calendar days of receipt of INDOT's February 11 letter. Typically, consulting parties are allowed 30 days to comment on project submissions. If a consulting party asks for additional time to comment, up to the usual 30 days, we ask that such a request be honored.

The archaeological reviewer for this project on the Indiana SHPO staff is Wade T. Tharp, and the structures reviewer is John Carr. However, if you have questions about our comments or about a procedural issue, please contact initially an INDOT Cultural Resources staff member who is assigned to this project.

In all future correspondence regarding the US 36 Added Travel Lanes in Washington Township of Hendricks County and Wayne Township of Marion County (Des. No. 1601072), please continue to refer to DHPA No. 23088.

Very truly yours,



Beth K. McCord  
Deputy State Historic Preservation Officer

BKM:JLC:jlc

cc: Property Owner, David Faucett House, 9055 Rockville Road, Indianapolis

emc: Robert Dirks, P.E., FHWA  
Anuradha Kumar, INDOT  
Shaun Miller, INDOT  
Susan Branigin, INDOT  
Shirley Clark, INDOT  
Kyle Boot, RQAW  
Haylee Moscato, RQAW  
Andrew Martin, RPA, Cultural Resource Analysts, Inc.

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Kyle Boot  
February 19, 2019  
Page 3

Andrew Vinson, owner of McClain House  
Brice Obermeyer, Delaware Nation of Oklahoma  
Diane Hunter, Miami Tribe of Oklahoma  
Sam Burgess, Indiana Landmarks  
Wade T. Tharp, INDNR-DHPA  
John Carr, INDNR-DHPA

**From:** [Ross, Anthony](#)  
**To:** "[sburgess@indianalandmarks.org](mailto:sburgess@indianalandmarks.org)"; [vinsonproperties@gmail.com](mailto:vinsonproperties@gmail.com); "[dhunter@miamination.com](mailto:dhunter@miamination.com)"; "[kpenrod@delawarenation.com](mailto:kpenrod@delawarenation.com)"  
**Cc:** [Haylee Moscato](#); [Joseph Dabkowski](#); [Aaron Lawson](#); [Branigin, Susan \(SBranigin@indot.IN.gov\)](#); [Gilyeat, Richard](#); [Kumar, Anuradha](#); [Carr, John](#); "[Allen, Michelle \(FHWA\)](#)"; "[Kyle J. Boot](#)"  
**Subject:** RE: FHWA Project: Des. No. 1601072; US 36 Added Travel Lanes, Hendricks and Marion Counties, Indiana  
**Date:** Friday, March 15, 2019 9:55:00 AM  
**Attachments:**   
  
  
  
  
  
  
  
  


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**Des. No.: 1601072**  
**Project Description: US 36 Added Travel Lanes**  
**Location: Avon, Hendricks and Marion Counties, Indiana**

Dear Consulting Parties,

Yesterday (3/14/2019) Indiana Landmarks sent an email regarding this project (see below). In response to that email, we are welcoming comments regarding our 2/13/2019 effects letter until the end of today (which is 30 days from the letter's distribution on 2/13/2019).

We would like to emphasize that the 2/13/2019 effects letter did not constitute a Section 106 finding of effect. We plan to sign the finding on Monday. We will then distribute the finding and 800.11 documentation to consulting parties for a 30-day comment period.

Please contact us with any questions or concerns. Tribal contacts may contact Shaun Miller at [smiller@indot.in.gov](mailto:smiller@indot.in.gov) or 317-233-6795 or Michelle Allen at FHWA at [michelle.allen@dot.gov](mailto:michelle.allen@dot.gov) or 317-226-7344.

Thank you,

**Anthony Ross, Ph.D.**

*Historian*

*Cultural Resources Office*

*Environmental Services*

100 N. Senate Ave., Room N642-ES

Indianapolis, IN 46204

**Office:** (317) 234-0142

**Email:** [aross3@indot.in.gov](mailto:aross3@indot.in.gov)



**\*\*Updated Historic Property Report (HPR) guidelines can be found [here](#)**

---

**From:** Kyle J. Boot [mailto:KBoot@RQAW.com]  
**Sent:** Thursday, March 14, 2019 5:16 PM  
**To:** 'sburgess@indianalandmarks.org' <sburgess@indianalandmarks.org>; Ross, Anthony <ARoss3@indot.IN.gov>  
**Cc:** Haylee Moscato <hmoscato@rqaw.com>; Joseph Dabkowski <jdabkowski@RQAW.com>; Aaron Lawson <alawson@rqaw.com>  
**Subject:** FW: FHWA Project: Des. No. 1601072; US 36 Added Travel Lanes, Hendricks and Marion Counties, Indiana

**\*\*\*\* This is an EXTERNAL email. Exercise caution. DO NOT open attachments or click links from unknown senders or unexpected email. \*\*\*\***

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Hi Sam,

The comment period to the DHPA and consulting parties for the Effects Letter (note that this is not the 800.11 Effects Finding) was advanced to 15 days per the INDOT PM's request. That request was made based on a number of factors. The DHPA responded to the effects letter on 2/19/19. Their response indicated that you should have received it via emc. I've attached it to this email just in case you did not receive it. The DHPA recommended that if a consulting party asked for additional time to comment, up to the usual 30 days, that the request be honored. If Indiana Landmarks is requesting additional time, note that 30 days from 2/13/2019 (when the Effects Letter was distributed to consulting parties) is Friday 3/15/19.

Anthony,

I have not received INDOT's signature (on behalf of FHWA) for the 800.11 Effects Finding document. It was resubmitted to INDOT-CRO on 3/12/19. Would it be appropriate to include Indiana Landmark's comments on the Effects Letter if received on Friday 3/15/19? I could update the 800.11 documentation and resubmit to you by EOD Friday or first thing Monday morning, depending on when comment is received. Can you please advise on how to proceed?

Thank you both,  
Kyle

**Kyle Boot | Architectural Historian**

**O: 317.588.1798**

**C: 317.410.0845**

[www.rqaw.com](http://www.rqaw.com)

---

**From:** Haylee Moscato  
**Sent:** Thursday, March 14, 2019 3:10 PM  
**To:** Kyle J. Boot <[KBoot@RQAW.com](mailto:KBoot@RQAW.com)>  
**Subject:** FW: FHWA Project: Des. No. 1601072; US 36 Added Travel Lanes, Hendricks and Marion Counties, Indiana

**Haylee Moscato | Architectural Historian**

8770 North St., Ste. 110  
Fishers, IN 46038

---

**From:** Sam Burgess <[sburgess@indianalandmarks.org](mailto:sburgess@indianalandmarks.org)>  
**Sent:** Thursday, March 14, 2019 11:24 AM  
**To:** Haylee Moscato <[hmoscato@rqaw.com](mailto:hmoscato@rqaw.com)>  
**Subject:** RE: FHWA Project: Des. No. 1601072; US 36 Added Travel Lanes, Hendricks and Marion Counties, Indiana

Haylee:

Indiana Landmarks would like to provide comments on the Finding of Effects document regarding Des. No. 1601072, but I just noticed that the comment period was set at 15 days. Generally when we receive reports concerning Finding of Effects, we are given 30 days to comment. Can you explain why the comment period was 15 days in this case?

Thanks,

Sam Burgess

.....  
Community Preservation Specialist  
.....

**Indiana Landmarks**  
Central Regional Office  
1201 Central Avenue  
Indianapolis, IN 46202  
Ph. 317-639-4534, 800-450-4534  
Fax: 317-639-6734  
[www.indianalandmarks.org](http://www.indianalandmarks.org)

**Indiana Landmarks revitalizes communities, reconnects us to our heritage, and saves meaningful places.**

---

**From:** Haylee Moscato <[hmoscato@rqaw.com](mailto:hmoscato@rqaw.com)>  
**Sent:** Wednesday, February 13, 2019 3:41 PM  
**To:** [vinsonproperties@gmail.com](mailto:vinsonproperties@gmail.com); Sam Burgess <[sburgess@indianalandmarks.org](mailto:sburgess@indianalandmarks.org)>  
**Cc:** Kyle J. Boot <[KBoot@RQAW.com](mailto:KBoot@RQAW.com)>; Ross, Anthony <[ARoss3@indot.IN.gov](mailto:ARoss3@indot.IN.gov)>; Miller, Shaun (INDOT) <[smiller@indot.IN.gov](mailto:smiller@indot.IN.gov)>; Branigin, Susan <[S Branigin@indot.IN.gov](mailto:S Branigin@indot.IN.gov)>; Kumar, Anuradha <[akumar@indot.IN.gov](mailto:akumar@indot.IN.gov)>; Joseph Dabkowski <[jdabkowski@RQAW.com](mailto:jdabkowski@RQAW.com)>; Dylan Sievers <[dsievers@RQAW.com](mailto:dsievers@RQAW.com)>; Lisa Casler <[lcasler@rqaw.com](mailto:lcasler@rqaw.com)>; Aaron Lawson <[alawson@rqaw.com](mailto:alawson@rqaw.com)>; Coon, Matthew <[mcoon@indot.IN.gov](mailto:mcoon@indot.IN.gov)>; Gilyeat, Richard <[RGilyeat@indot.IN.gov](mailto:RGilyeat@indot.IN.gov)>  
**Subject:** FHWA Project: Des. No. 1601072; US 36 Added Travel Lanes, Hendricks and Marion Counties, Indiana

**Des. No.: 1601072**  
**Project Description: US 36 Added Travel Lanes**





March 15, 2019

Kyle J. Boot  
Architectural Historian  
RQAW Environmental  
8770 North St., Ste. 110  
Fishers, IN 46038

Re: Des. No. 1601072, U.S. 36 Added Travel Lanes Project, Avon, Hendricks County and Indianapolis, Marion County, Indiana

Dear Mr. Boot:

Thank you for the continued opportunity to comment on Des. No. 1601072. Based on the information and analysis presented in INDOT's letter of February 11, 2019, we do not anticipate that the undertaking will result in adverse effects to the Charles McLain House (IHSSI# 063-117-45024) (eligible under Criterion C) or the David Faucett House (IHSSI# 097-117-56005), provided there are no changes to the footprint of the construction area for the project.

We appreciate the SHPO's request that any consulting party wishing to comment on the February 11, 2019 letter be permitted to do so within 30 days of receipt rather than the 15 days requested by INDOT. We also ask that INDOT's specific reasons for requesting a 15-day comment period be added to the public record for this project.

We will look forward to receiving the official finding and 800.11 documentation.

Sincerely,

Sam Burgess  
Community Preservation Specialist

Appendix E:

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# Historic Property Report & Archaeology Report Summaries

# US 36 Added Travel Lanes

Hendricks & Marion Counties, Indiana

Des. No.: 1601072

Indiana DHPA No.: 23088




12/11/2018

## Historic Property Report

Prepared for:

The Federal Highway Administration  
and Crawfordsville District, Indiana  
Department of Transportation

Prepared by:

  
Haylee N. Moscato  
hmoscato@RQAW.com

**RQAW**

ENVIRONMENTAL

8770 North Street, Suite 110  
Fishers, IN 46038  
Phone: (317) 588-1798  
www.rqaw.com

## VII. CONCLUSIONS

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The APE contains no properties listed in the National Register.

As a result of the identification and evaluation efforts for this project, two properties are recommended eligible for listing in the National Register:

- Charles McLain House at 10664 E. US 36 and
- David Faucett House at 9055 Rockville Road/US 36.



# INDIANA ARCHAEOLOGICAL SHORT REPORT

State Form 54566 (1-11)

## INDIANA DEPARTMENT OF NATURAL RESOURCES DIVISION OF HISTORIC PRESERVATION AND ARCHAEOLOGY

402 West Washington Street, Room W274  
Indianapolis, Indiana 46204-2739  
Telephone Number: (317) 232-1646  
Fax Number: (317) 232-0693  
E-mail: dhpa@dnr.IN.gov

Where applicable, the use of this form is recommended but not required by the Division of Historic Preservation and Archaeology.

Author: Andrew V. Martin, RPA 61710

Date (month, day, year): October 9, 2018

Project Title: A Phase Ia Archaeological Reconnaissance Survey for the US 36 Added Travel Lanes Project in Hendricks and Marion Counties, Indiana (CRA Contract Publication #18-434)

### PROJECT OVERVIEW

Project Description:

Added travel lanes are proposed along US 36 in Avon, Hendricks and Marion Counties, Indiana (Figures 1 and 2). The project will begin at Shiloh Park Drive and continue east of Raceway Road, totaling approximately 1.22 mi (1.96 km). The proposed improvements will involve milling, resurfacing, and widening of the existing roadway to provide three through lanes in both the east and west bound directions on US 36. A left turn lane will also be provided in both the eastbound and westbound directions at each signalized intersection. The existing alignment of US 36 will remain unchanged. Curb turnouts with ditches and drive culverts are proposed to handle drainage between Shiloh Park Drive and Raceway Road. Between Raceway Road and the end of the project, an enclosed drainage system will be proposed to match the existing drainage system. The entire project area covers approximately 13.35 ha. (33.0 acres). Although, excluding existing pavement, the survey area covers approximately 8.0 ha (20.0 acres) of greenspace along either side of US 36.

INDOT Designation Number/ Contract Number: 1601072 Project Number: CRA #I18R012

DHPA Number: Approved DHPA Plan Number: N/A

Prepared For: RQAW, Inc.

Contact Person: Kyle Boot

Address: 8770 North St.

City: Fishers State: IN ZIP Code: 46038

Telephone Number: (317) 815-7232 Email Address: kboot@rqaw.com

Principal Investigator: Andrew V. Martin, RPA

Signature:

Company/Institution: Cultural Resource Analysts, Inc.

Address: 201 NW 4th Street, Suite 204

City: Evansville State: IN ZIP Code: 47708

Telephone Number: (812) 253-3009 Email Address: amartin@crai-ky.com



## Results

- Archaeological records check has determined that the project area does not have the potential to contain archaeological resources.
- Archaeological records check has determined that the project area has the potential to contain archaeological resources.
- Phase Ia reconnaissance has located no archaeological resources in the project area.
- Phase Ia reconnaissance has identified landforms conducive to buried archaeological deposits.

Actual Area Surveyed hectares:  acres:

Comments: 

Oakfield probes conducted in the southeastern portion of the project area had mottled brown (10YR 4/3) and dark brown (10YR 3/3) silty clay loam with fine gravel inclusions beginning at the ground surface and extended to at least 25 cm below the ground surface. This profile appears to represent either fill or rocky B horizon sediment common in the Crosby soil series mapped for the area.
---

## Recommendation

- The archaeological records check has determined that the project area has the potential to contain archaeological resources and a Phase Ia archaeological reconnaissance is recommended.
- The archaeological records check has determined that the project area does not have the potential to contain archaeological resources and no further work is recommended before the project is allowed to proceed.
- The Phase Ia archaeological reconnaissance has located no archaeological sites within the project area and it is recommended that the project be allowed to proceed as planned.

## Appendix F:

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# Preliminary Plans

Preliminary plans omitted to avoid duplication. See plans in Appendix B of this EA document.

The Indianapolis Star  
130 South Meridian Street  
Indianapolis, IN 46225  
Marion County, Indiana

Federal Id: 06-1032273

RQAW CORPORATION

Account #:INI-25326  
Order #:0003446511  
# of Affidavits:1  
Total Amount of Claim:\$88.43

RQAW CORPORATION  
ATTN KYLE BOOT  
8770 NORTH ST STE 110  
FISHERS, IN 46038

### PUBLISHER'S AFFIDAVIT

STATE OF WISCONSIN,  
County Of Brown } SS:

Personally appeared before me, a notary public in and for said county and state, the undersigned

I, being duly sworn, say that I am a clerk for THE INDIANAPOLIS NEWSPAPERS a DAILY STAR newspaper of general circulation printed and published in the English language in the city of INDIANAPOLIS in state and county of Marion, and that the printed matter attached hereto is a true copy, which was duly published in said paper for 1 times., the dates of publication being as follows:

The insertion being on the 03/21/2019  
Newspaper has a website and this public notice was posted in the same day as it was published in the newspaper.

Pursuant to the provisions and penalties of Ch. 155, Acts 1953,

I hereby certify that the foregoing account is just and correct, that the amount claimed is legally due, after allowing all just credits, and that no part of the same has been paid.

Kazoua Yang

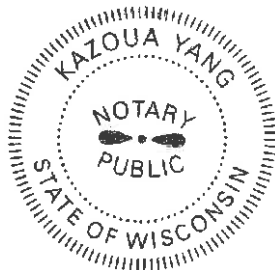
Date: 3-21, 20 19 Title: Clerk

Subscribed and sworn to before me this 21 day of March, 2019

Kazoua Yang  
Notary Public

Notary Expires:

11/9/22



\_\_\_\_\_  
(Governmental Unit)  
\_\_\_\_\_  
County, Indiana

To: **INDIANAPOLIS STAR**

**Indianapolis, IN**

**PUBLISHER'S CLAIM**

Acct #: INI-25326  
Ad #: 0003446511

74 lines, 2 columns wide equals 148 equivalent lines at \$0.60 per line @ 1 days,	\$88.43
Website Publication	<u>\$0</u>
Charge for proof(s) of publication	<u>\$0.00</u>

DATA FOR COMPUTING COST

Width of single column 9.5 cms  
Number of insertions 1  
Size of type 7 point

TOTAL AMOUNT OF CLAIM \$88.43

Claim No. \_\_\_\_\_ Warrant No. \_\_\_\_\_

IN FAVOR OF  
**The Indianapolis Star**  
Indianapolis, IN  
Marion County  
130 S. Meridian St. Indianapolis, IN 46225

I have examined the within claim and hereby certify as follows:

That it is in proper form.

That it is duly authenticated as required by law.

That it is based upon statutory authority.

That it is apparently (correct)  
(incorrect)

\$ \_\_\_\_\_  
On Account of Appropriation For

FED. ID  
#06-1032273

Allowed \_\_\_\_\_, 20\_\_\_\_

In the sum of \$ \_\_\_\_\_

I certify that the within claim is true and correct; that the services there-in itemized and for which charge is made were ordered by me and were necessary to the public business.

**Public Notice  
Des. No. 1601072**

The Indiana Department of Transportation (INDOT) is planning to undertake an added travel lanes project, funded in part by the Federal Highway Administration (FHWA). The project is located on US36 from Shiloh Park Drive in Avon, Hendricks County and continue east to approximately 0.28 mile (1,500 feet) east of Raceway Road in Indianapolis, Marion County.

Under the preferred alternative, the proposed project would involve roadwork to the existing roadway to provide added travel lanes in both directions on US36. The typical cross-section in each direction will consist of three travel lanes bordered by a curb and gutter. A left turn lane will be provided in both directions at each signalized intersection and right turn lanes will be reconstructed. The length of turn lanes will vary and are determined by traffic analysis. At the current design phase, the anticipated right-of-way is expected not to exceed approximately 0.3 acre of permanent and approximate 0.7 acre of temporary right-of-way. The project is anticipated to be let in 2021. Traffic is anticipated to be maintained through the area of construction with two lanes being maintained in each direction. No signed detours are anticipated. Access to residences and businesses along US36 will be continuously maintained during construction.

Properties listed in or eligible for the National Register of Historic Places (NRHP) located within the Area of Potential Effects (APE) include the Charles McLain House and the David Faucett House. The proposed action impacts properties listed in or eligible for the NRHP. The INDOT, on behalf of the FHWA, has issued a "No Adverse Effect" finding for the project because the project will not diminish the integrity of the characteristics that qualify the historic properties within the APE for inclusion in the NRHP. In accordance with the National Historic Preservation Act, the views of the public are being sought regarding the effect of the proposed project on the historic elements as per 36 CFR 800.2(d), 800.3(e) and 800.6(a)(4). Pursuant to 36 CFR 800.4(d)(2), the documentation specified in 36 CFR 800.11(e) is available for inspection in RQAW's office. Additionally, this documentation can be viewed electronically by accessing INDOT's Section 106 document posting website IN SCOPE at <http://erms.indot.in.gov/Section106Documents>. This documentation serves as the basis for the "No Adverse Effect" finding. The views of the public on this effect finding are being sought. Please reply with any comments to Haylee Moscato of RQAW, 8770 North St., Ste. 110, Fishers, IN 46038, 317-588-1762 or [hmoscato@rqaw.com](mailto:hmoscato@rqaw.com) no later than April 20, 2019.

In accordance with the "Americans with Disabilities Act", if you have a disability for which INDOT needs to provide accessibility to the document(s) such as interpreters or readers, please contact Rickie Clark at 317-232-6601 or [rclark@indot.in.gov](mailto:rclark@indot.in.gov).

(S - 3/21/19 - 3446511)

hspaxlp





Division of Historic Preservation & Archaeology · 402 W. Washington Street, W274 · Indianapolis, IN 46204-2739  
Phone 317-232-1646 · Fax 317-232-0693 · dhpa@dnr.IN.gov · www.IN.gov/dnr/historic



April 12, 2019

Kyle Boot  
Architectural Historian  
RQAW  
8770 North Street, Suite 110  
Fishers, Indiana 46038

Federal Agency: Indiana Department of Transportation (“INDOT”),  
on behalf of Federal Highway Administration, Indiana Division (“FHWA”)

Re: INDOT’s Finding of No Adverse Effect, with supporting documentation, for the US 36  
Added Travel Lanes, from Shiloh Park Drive in the Town of Avon, Washington Township,  
Hendricks County, to approximately 1,500 feet east of Raceway Road in the City of  
Indianapolis, Wayne Township, Marion County, Indiana (Des. No. 1601072; DHPA No.  
23088)

Dear Mr. Boot:

Pursuant to Section 106 of the National Historic Preservation Act of 1966, as amended (54 U.S.C. § 306108), 36 C.F.R. Part 800, and the “Programmatic Agreement (PA) Among the Federal Highway Administration, the Indiana Department of Transportation, the Advisory Council on Historic Preservation and the Indiana State Historic Preservation Officer Regarding the Implementation of the Federal Aid Highway Program In the State of Indiana,” the staff of the Indiana State Historic Preservation Officer (“Indiana SHPO” or “INDNR-DHPA”) has reviewed RQAW’s March 19, 2019, review request submittal form, with INDOT’s finding and documentation enclosed, all of which we received on March 21.

As we have said in previous letters on this project, based on the submitted information and documentation available to the staff of the INDNR-DHPA, we have not identified any currently known archaeological resources listed in or eligible for inclusion in the National Register of Historic Places (“NRHP”) within the proposed project area. However, this identification is subject to the project activities remaining within areas disturbed by previous construction of a recent and non-historical nature. If archaeological deposits are encountered from the post-contact period, they will be evaluated regarding their eligibility for the NRHP in consultation with the staff of INDNR-DHPA. Please contact our office if such deposits are encountered. The archaeological recording must be done in accordance with the Secretary of the Interior’s “Standards and Guidelines for Archaeology and Historic Preservation” (48 F.R. 44716) and a report of the archaeological documentation must be submitted to our office for review and comment.

If any prehistoric or historic archaeological artifacts or human remains are uncovered during construction, demolition, or earthmoving activities, state law (Indiana Code 14-21-1-27 and -29) requires that the discovery be reported to INDNR-DHPA within two (2) business days. In that event, please call (317) 232-1646. Be advised that adherence to Indiana Code 14-21-1-27 and -29 does not obviate the need to adhere to applicable federal statutes and regulations, including but not limited to 36 C.F.R. Part 800.

*The DNR mission: Protect, enhance, preserve and wisely use natural, cultural and recreational resources for the benefit of Indiana’s citizens through professional leadership, management and education.*

**www.DNR.IN.gov**  
An Equal Opportunity Employer

Kyle Boot  
April 12, 2019  
Page 2

We agree with INDOT's determination that the NRHP-eligible Charles McClain House (Indiana Historic Sites and Structures Inventory ["IHSSI"] No. 063-117-45024) and the NRHP-eligible David Faucett House (IHSSI No. 097-117-56005) are the only historic properties that have been identified within this project's area of potential effects. We further agree that neither the Charles McClain House nor the David Faucett House will be adversely affected by this project.

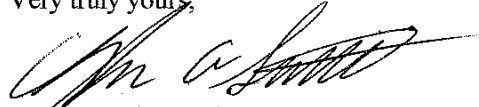
Accordingly, we concur with INDOT's March 18, 2019, Section 106 finding, on behalf of FHWA, of No Adverse Effect for this federal undertaking as a whole.

INDOT's finding indicates that temporary right-of-way will be acquired from the Charles McClain House property, and the plans included in the documentation indicate that the purpose is to reconstruct the driveway. For the purposes of Section 4(f) of the U.S. Department of Transportation Act of 1966, we agree that the conditions listed in 23 C.F.R. § 774.13(d) are satisfied and that the involvement in the project of land of the Charles McClain House will constitute a temporary occupancy.

The archaeological reviewer for this project on the Indiana SHPO staff is Wade T. Tharp, and the structures reviewer is John Carr. However, if you have questions about our comments or about a procedural issue, please contact initially an INDOT Cultural Resources staff member who is assigned to this project.

If there is any future correspondence regarding the US 36 Added Travel Lanes in Washington Township of Hendricks County and Wayne Township of Marion County (Des. No. 1601072), please continue to refer to DHPA No. 23088.

Very truly yours,



Christopher A. Smith  
Deputy Director  
Indiana Department of Natural Resources

CAS:JLC:jlc

cc: Property Owner, David Faucett House

emc: Robert Dirks, P.E., FHWA  
Anuradhá Kumar, INDOT  
Shaun Miller, INDOT  
Susan Branigin, INDOT  
Anthony Ross, Ph.D., INDOT  
Shirley Clark, INDOT  
Kyle Boot, RQAW  
Haylee Moscato, RQAW  
Andrew Martin, RPA, Cultural Resource Analysts, Inc.  
Andrew Vinson, owner of Charles McClain House  
Brice Obermeyer, Delaware Nation of Oklahoma  
Diane Hunter, Miami Tribe of Oklahoma  
Sam Burgess, Indiana Landmarks  
Wade T. Tharp, INDNR-DHPA  
John Carr, INDNR-DHPA