

**EFFECTS REPORT FOR
THE US 41 INTERSECTION IMPROVEMENT PROJECT
DES NO. 2000186
DHPA NO. 27933
EVANSVILLE, VANDERBURGH COUNTY, INDIANA**

1) Project Location

The Indiana Department of Transportation (INDOT), with funding from the Federal Highway Administration (FHWA), proposes to proceed with a project involving the construction of a Reduced Conflict Intersection (RCI) at US 41 and Washington Avenue (Des. No. 2000186), approximately 1.00 mile south of the intersection of US 41 and SR 66/SR 62 (Lloyd Expressway) in the city of Evansville, Vanderburgh County, Indiana.

The proposed undertaking is located at the intersection of US 41 and Washington Avenue in the city of Evansville, Vanderburgh County, Indiana. It is within Knight Township, as shown on the Evansville, South, Indiana, USGS Topographic Quadrangle, in Sections 28 & 33, Township 6 South, Range 10 West (see Appendix A [maps]).

Given the nature of the proposed project, the APE was determined to include the proposed project area and a buffer zone based on topography and vegetation surrounding the proposed intersection improvement location. Visual impacts were the driving factor in the creation of the APE, as the project area is visible from surrounding commercial buildings and residences. The APE takes into account the potential direct and indirect effects of the proposed project within the immediate contextual setting, which is comprised of a heavily urbanized area with commercial and residential lots with scattered trees.

2) Project Description

The need for the project is based on the existing crash rates and safety concerns with this intersection. Based on the abbreviated engineer report for the project completed by HNTB, Corporation (HNTB) in January 2020, between 2016 and 2018, there were 15 Fatal and/or Incapacitating Crashes, 15 Non-Incapacitating and Possibly Injury Crash and 85 Property Damage Only Crashes making this intersection a "high crash" location. The purpose of this project is to improve the safety of the intersection.

Currently, the intersection of US 41 and Washington Avenue consists of two northbound and two southbound US 41 lanes with a left-turn lane for each direction separated by a grassy median. Washington Avenue consists of one westbound and one eastbound lane with a right-turn lane and a left-turn lane in each direction.

In order to meet the project purpose and need, a reduced conflict intersection will be constructed at the intersection of US 41 and Washington Avenue. The preferred option is the Boulevard Left intersection type. The Boulevard Left intersection relocates left turns from all legs of the main intersection to median U-turns located approximately 700 feet north and south of Washington Avenue. This intersection type retains the east-west connectivity on

Washington Avenue. The Boulevard Left intersection has fewer signal phases than other types of reduced conflict intersections and provides better mobility to eastbound-westbound traffic on Washington Avenue, which is a minor arterial. The project will also include directional signage, maintenance of drainage within the project extents, replacement of traffic signals at the main intersection, and the installation of new traffic signals at the median U-turn locations. There is no permanent or temporary right-of-way (ROW) acquisition anticipated for this project (see Appendix C [plans]).

Any median crossing work will be constructed prior to the closing of the intersection to allow for traffic movements to be open during construction. US 41 will be reduced to one lane in each direction during construction. Pedestrian maintenance of traffic will be required.

The proposed project area for the intersection improvement project, located at the intersection of US 41 and Washington Avenue, begins on US 41 approximately 560 feet (0.12 mile) south of its intersection with Bellemeade Avenue and extends south along US 41 for approximately 2,600 feet (0.5 mile). The project also begins along Washington Avenue approximately 430 feet (0.08 mile) west of its intersection with US 41 and extends approximately 1,010 feet (0.19 mile) east along Washington Avenue (see Appendix A [maps] and Appendix C [plans]).

3) Describe Affected Historic Properties

CRA did not identify any archaeological sites eligible for listing in the National Register of Historic Preservation (NRHP). The SHPO concurred in a letter dated January 7, 2022.

Lincolnshire Historic District (NR-0908) – The Lincolnshire Historic District was listed in the NRHP in 1989 under Criteria A and C for its significance relating to architecture, commerce, and community planning and development. The district contains 97 “Contributing” and 22 “Non-Contributing” resources that are a mix of Tudor and Colonial Revival commercial and residential properties from the early to mid-twentieth century, some of which were designed by Anderson & Veatch, an Evansville design firm (see Appendix A [maps] and Appendix B [photos]).

Akin Park Historic District (IHSSI No. 163-196-430[01-213]) – The Akin Park Historic District is an excellent example of community planning and development in Evansville during the early to mid-twentieth century. The architecturally diverse residences represent the desire to have a fashionable, well-planned residential neighborhood utilizing local architects. Approximately 190 previously surveyed resources range from “contributing” to “outstanding,” and are set within an urban landscape dotted with mature trees. The district also contains approximately 21 non-contributing resources. The resources are a mix of single-family and multifamily dwellings along with Akin Park. The district features an array of architectural styles such as Craftsman Bungalows, Colonial Revival, Contemporary, Tudor Revival, and vernacular (see Appendix A [maps] and Appendix B [photos]).

Historically, a portion of the district was the location of the Tri-State Fairgrounds that closed in 1912. After the fairgrounds closed, the area was developed into the Akin Park

Neighborhood. The area today looks much the same as it did during the early twentieth century, situated within an urban area of Evansville.

The Akin Park Historic District is recommended eligible under Criterion A as it illustrates a planned and meticulously designed neighborhood in Evansville during the early to mid-twentieth century. Evidence of a planned neighborhood include the boulevards and centrally located park of which the residential neighborhood is centered around.

The Akin Park Historic District is also recommended eligible under Criterion C. The district features a cohesive collection of representative structures that embody the distinctive characteristics of early and mid-twentieth century architectural styles with original materials and forms along with locally designed residences.

The Akin Park Historic District's recommended NRHP boundary is as follows: roughly bounded by the north side of Madison Avenue, west side of South Kentucky Avenue, Ravenswood Drive, south side of Covert Avenue, and Gilbert Avenue (see Appendix A [maps]).

4) Describe Effects to Historic Properties

There will be no physical effects to the Lincolnshire Historic District or to the Akin Park Historic District. There is no permanent or temporary ROW acquisition associated with this project.

The traffic signals and signage located at the intersection of Washington Avenue and US 41 will be replaced in-kind with modern materials of approximately the same height. Additionally, a Boulevard Left intersection will result in the construction of two U-turns with traffic signals, eliminating the left-turn lanes for northbound and southbound lanes of US 41, also near, but not within the historic districts (see Appendix A [maps] and Appendix C [plans]). The project is located adjacent to the Lincolnshire Historic District's and Akin Park Historic District's recommended NRHP boundaries.

5) Explain Application of Criteria of Adverse Effect

According to 36 CFR § 800.5(a)(1), an adverse effect is found in the Section 106 process "when an undertaking may alter, directly or indirectly, any of the characteristics of a historic property that qualify the property for inclusion in the NRHP in a manner that would diminish the integrity of the property's location, design, setting, materials, workmanship, feeling, or association. Adverse effects may include reasonably foreseeable effects caused by the undertaking that may occur later in time, be farther removed in distance or be cumulative." Per 36 CFR § 800.5(a)(2), examples of adverse effect include but are not limited to:

- (i) Physical destruction of or damage to all or part of a property;
- (ii) Alteration of a property, including restoration, rehabilitation, repair, maintenance, stabilization, hazardous material remediation, and provision of handicapped access not consistent with the Secretary's Standards for the Treatment of Historic Properties (36 CFR part 68) and applicable guidelines;

- (iii) Removal of the property from its historic location;
- (iv) Change of the character of the property's use or physical features within the property's setting that contribute to its historic significance;
- (v) Introduction of visual, atmospheric, or audible elements that diminish the integrity of the property's significant historic features;
- (vi) Neglect of a property which causes its deterioration, except where such neglect and deterioration are recognized qualities of a property of religious and cultural significance to an Indian tribe or Native Hawaiian organization; and
- (vii) Transfer, lease, or sale of a property out of Federal ownership or control without adequate and legally enforceable restrictions or conditions to ensure long-term preservation of the property's historic significance.

The discussion below considers the effects of the undertaking, using the examples given in 36 CFR 800.5(a)(2), on the historic resources within the area of potential effects (APE) for this undertaking: the Lincolnshire Historic District (NR-0908) and the Akin Park Historic District (IHSSI No. 163-196-430[01-213]).

Lincolnshire Historic District (NR-0908) – According to 36 CFR 800.5(a)(1) the criteria of adverse effect do not apply.

Per 36 CFR 800.5(a)(2)(i), the undertaking will not result in the “Physical destruction of or damage to all or part of the property.” None of the NRHP-listed district resources will be damaged. No permanent or temporary ROW will be acquired from the historic district.

Per 36 CFR 800.5(a)(2)(ii), the undertaking will not cause “Alteration of a property, including restoration, rehabilitation, repair, maintenance, stabilization, hazardous material remediation and provision of handicapped access, that is not consistent with the Secretary's Standards for the Treatment of Historic Properties and/or other applicable guidelines.”

Per 36 CFR 800.5(a)(2)(iii), the undertaking will not result in the “Removal of the property from its historic location.”

Per 36 CFR 800.5(a)(2)(iv), the undertaking will not result in a “Change of the character of the property's uses or of physical features within the property setting that contribute to the historic significance” of the property. The proposed project will not alter the Lincolnshire Historic District's setting as none of the activities associated with the project occur within the district, only adjacent to it. The traffic signals and signage located at the intersection of Washington Avenue and US 41 will be replaced in-kind with modern materials of approximately the same height. Replacement in-kind of modern materials will not affect the district's setting as a form of these utilities has existed at the intersection for over 50 years. Additionally, a Boulevard Left intersection will result in the construction of two U-turns and the addition of traffic signals at each U-turn, eliminating the left-turn lanes for northbound and southbound lanes of US 41, also near, but not within the historic district.

Per 36 CFR 800.5(a)(2)(v), the undertaking will not result in the “Introduction of visual, atmospheric, or audible elements that diminish the integrity of the property's significant

historic features.” The previously mentioned project activities are all located outside of the historic district. Work associated with the alteration of the intersection of Washington Avenue and US 41 will be minimally visible from the historic district, as the work will occur at or near surface grade or will involve the replacement of existing more visible items, such as traffic signals and signage. The traffic signals and signage will be replaced in-kind with modern materials of approximately the same height in approximately the same location. New traffic signals will be placed at each median U-turn and will be suspended from strain poles located within the grassed ROW between the edge of pavement and the ROW fence. However, the work will not alter the district’s ability to convey its significance.

Per 36 CFR 800.5(a)2(vi), the undertaking will not cause the “Neglect of a property which causes its deterioration...” The project will have no effect that will result in neglect.

Per 36 CFR 800.5(a)2(vii), the undertaking will not cause the “Transfer, lease, or sale of property out of Federal ownership or control...” There are no federally owned properties within the proposed project area.

Akin Park Historic District (IHSSI No. 163-196-430[01-213]) – According to 36 CFR 800.5(a)(1) the criteria of adverse effect do not apply.

Per 36 CFR 800.5(a)2(i), the undertaking will not result in the “Physical destruction of or damage to all or part of the property.” None of the resources within the proposed boundary for the Akin Park Historic District will be damaged. No permanent or temporary ROW will be acquired from the proposed historic district boundary.

Per 36 CFR 800.5(a)2(ii), the undertaking will not cause “Alteration of a property, including restoration, rehabilitation, repair, maintenance, stabilization, hazardous material remediation and provision of handicapped access, that is not consistent with the Secretary’s Standards for the Treatment of Historic Properties and/or other applicable guidelines.”

Per 36 CFR 800.5(a)2(iii), the undertaking will not result in the “Removal of the property from its historic location.”

Per 36 CFR 800.5(a)2(iv), the undertaking will not result in a “Change of the character of the property’s uses or of physical features within the property setting that contribute to the historic significance” of the property. The proposed project will not alter the Akin Park Historic District’s setting as none of the activities associated with the project occur within the district. The traffic signals and signage located at the intersection of Washington Avenue and US 41 will be replaced in-kind with modern materials of approximately the same height. Replacement in-kind of modern materials will not affect the district’s setting as a form of these utilities has existed at the intersection for over 50 years. Additionally, a Boulevard Left intersection will result in the construction of two U-turns and the addition of traffic signals at each U-turn, eliminating the left-turn lanes for northbound and southbound lanes of US 41, also near, but not within the historic district.

Per 36 CFR 800.5(a)(2)(v), the undertaking will not result in the “Introduction of visual, atmospheric, or audible elements that diminish the integrity of the property’s significant historic features.” The previously mentioned project activities are all located outside of the historic district. Work associated with the alteration of the intersection of Washington Avenue and US 41 will be minimally visible from the historic district, as the work will occur at or near surface grade or will involve the replacement of existing more visible items, such as traffic signals and signage. The traffic signals and signage will be replaced in-kind with modern materials of approximately the same height in approximately the same location. New traffic signals will be placed at each median U-turn and will be suspended from strain poles located within the grassed ROW between the edge of pavement and the ROW fence. However, the work will not alter the district’s ability to convey its significance.

Per 36 CFR 800.5(a)(2)(vi), the undertaking will not cause the “Neglect of a property which causes its deterioration...” The project will have no effect that will result in neglect.

Per 36 CFR 800.5(a)(2)(vii), the undertaking will not cause the “Transfer, lease, or sale of property out of Federal ownership or control...” There are no federally owned properties within the proposed project area.

Conclusions:

In accordance with Section 106 of the National Historic Preservation Act of 1966, as amended (16 .S.C. 470f), the project historians examined this project and assessed its potential to adversely affect historic properties in the APE. As noted above, historians believe that this project would have no adverse effect on the Lincolnshire Historic District or the Akin Park Historic District. The overall historic integrity of the districts will remain. The Akin Park Historic District will still be able to maintain its association with architecture and community planning and development in Evansville during the early to mid-twentieth century. The Lincolnshire Historic District will maintain its ability to convey its significance relating to architecture, commerce, and community planning and development. Work associated with the project will be located at or near surface grade or will involve the replacement of existing more visible items, such as traffic signals, but will not alter either districts ability to convey their previously mentioned areas of significance. Therefore, it is the professional opinion of the historians that the appropriate overall finding for this undertaking is “No Adverse Effect.”

APPENDIX A: MAPS

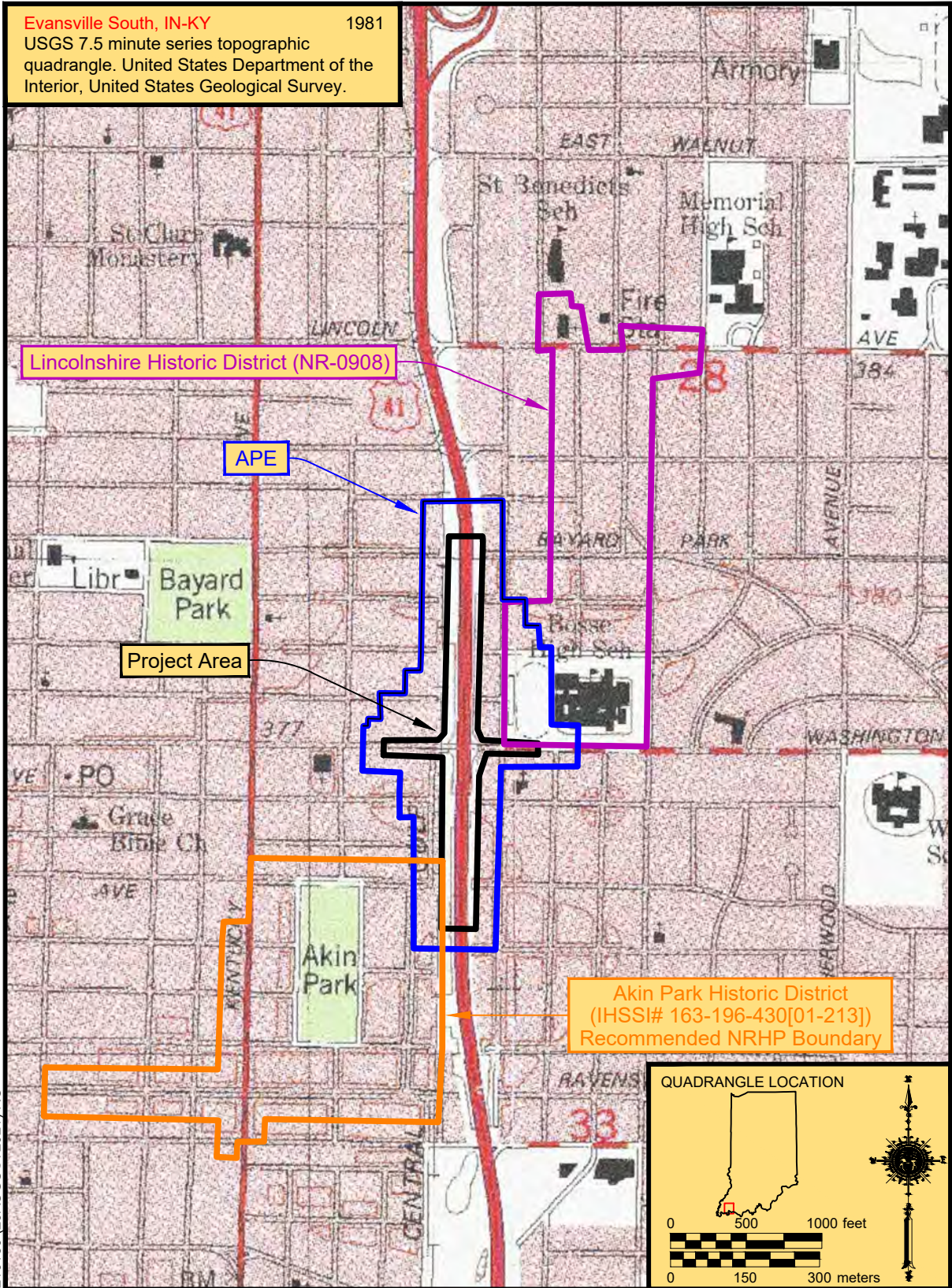


Figure 1. Topographic map showing the location of the proposed project, APE, and survey sites.

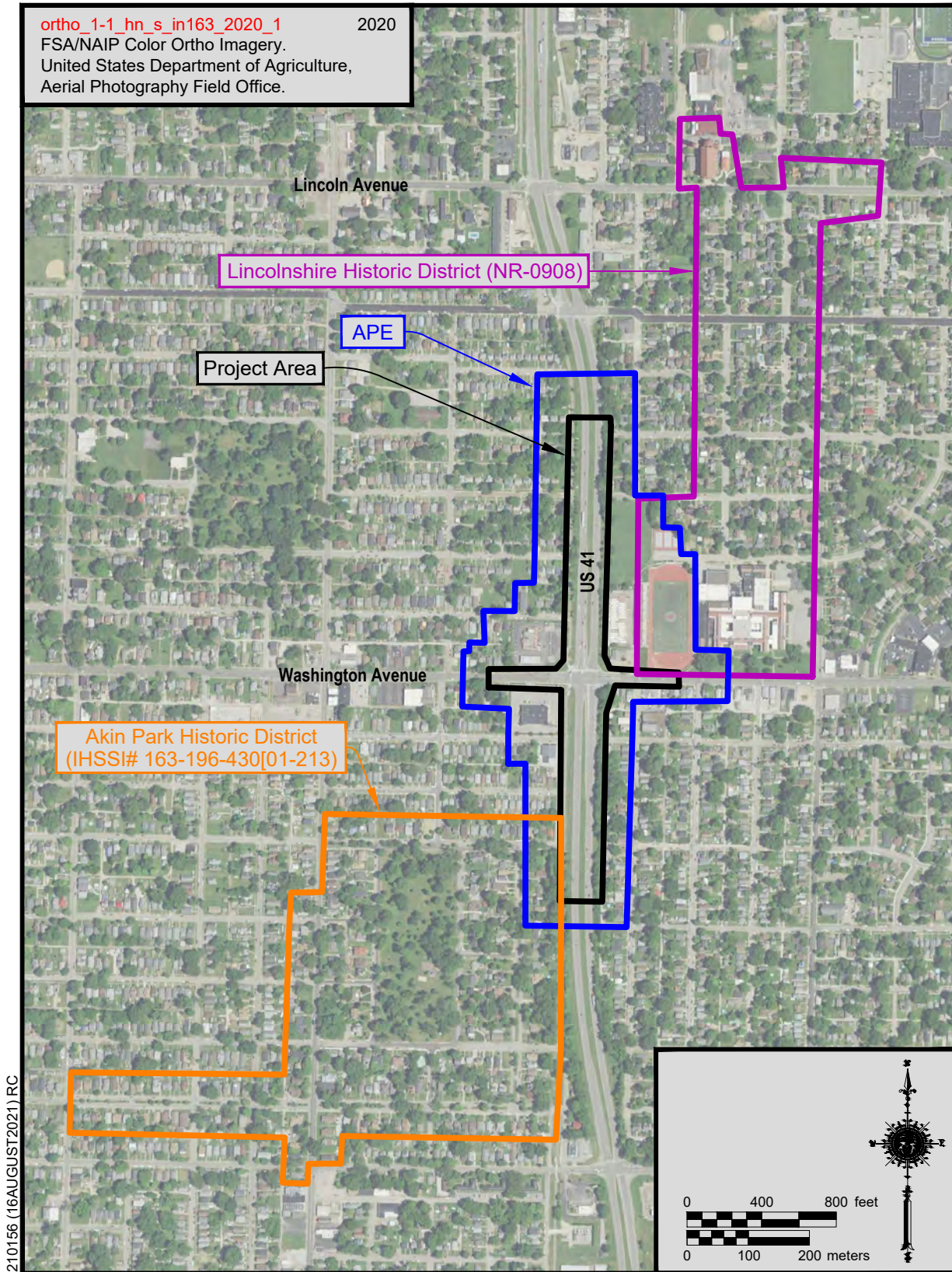


Figure 2. Aerial map showing the location of the proposed project, APE, and survey sites.

APPENDIX B. PHOTOS

ortho_1-1_hn_s_in163_2020_1 2020
FSA/NAIP Color Ortho Imagery.
United States Department of Agriculture,
Aerial Photography Field Office.

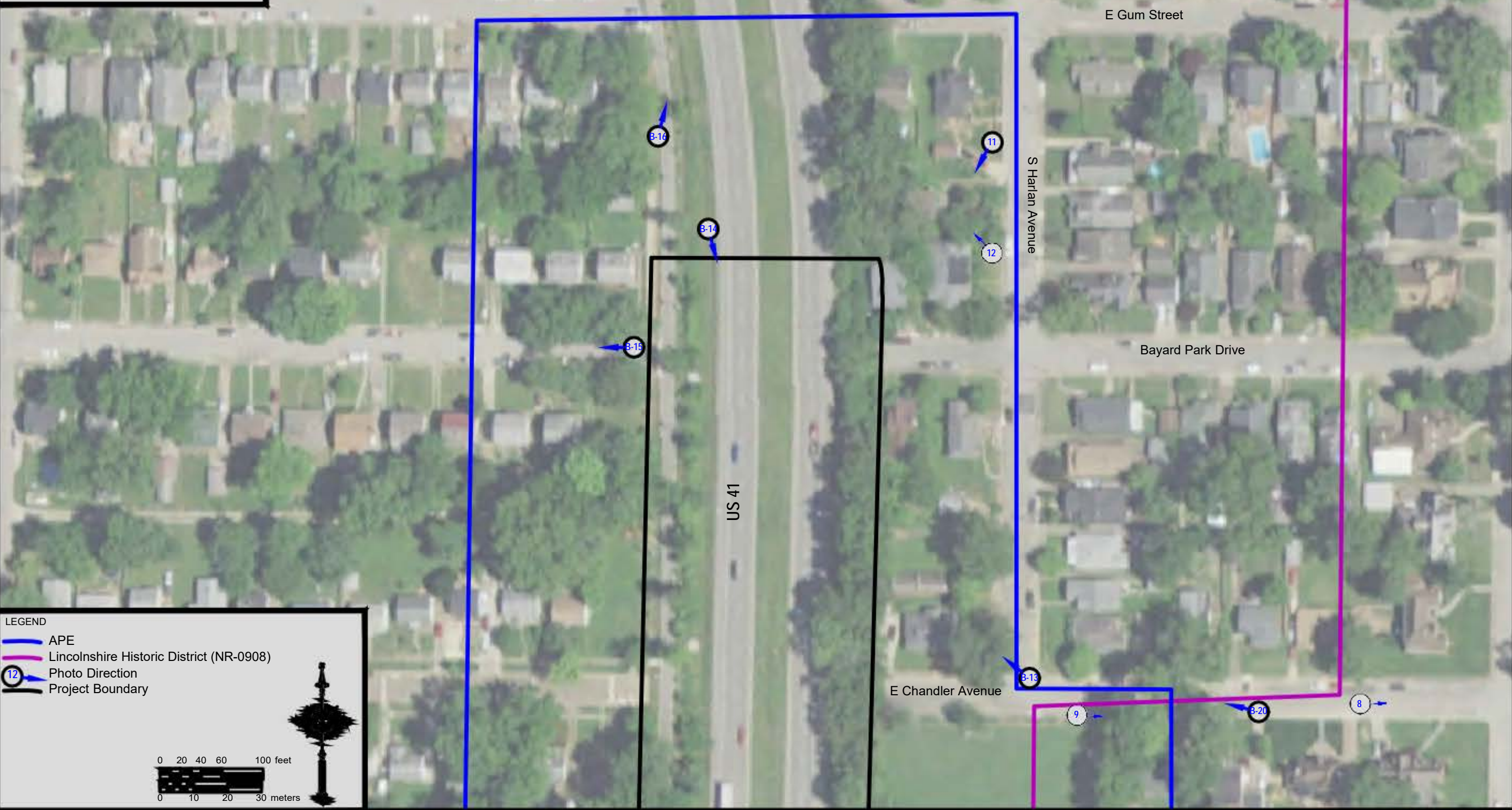
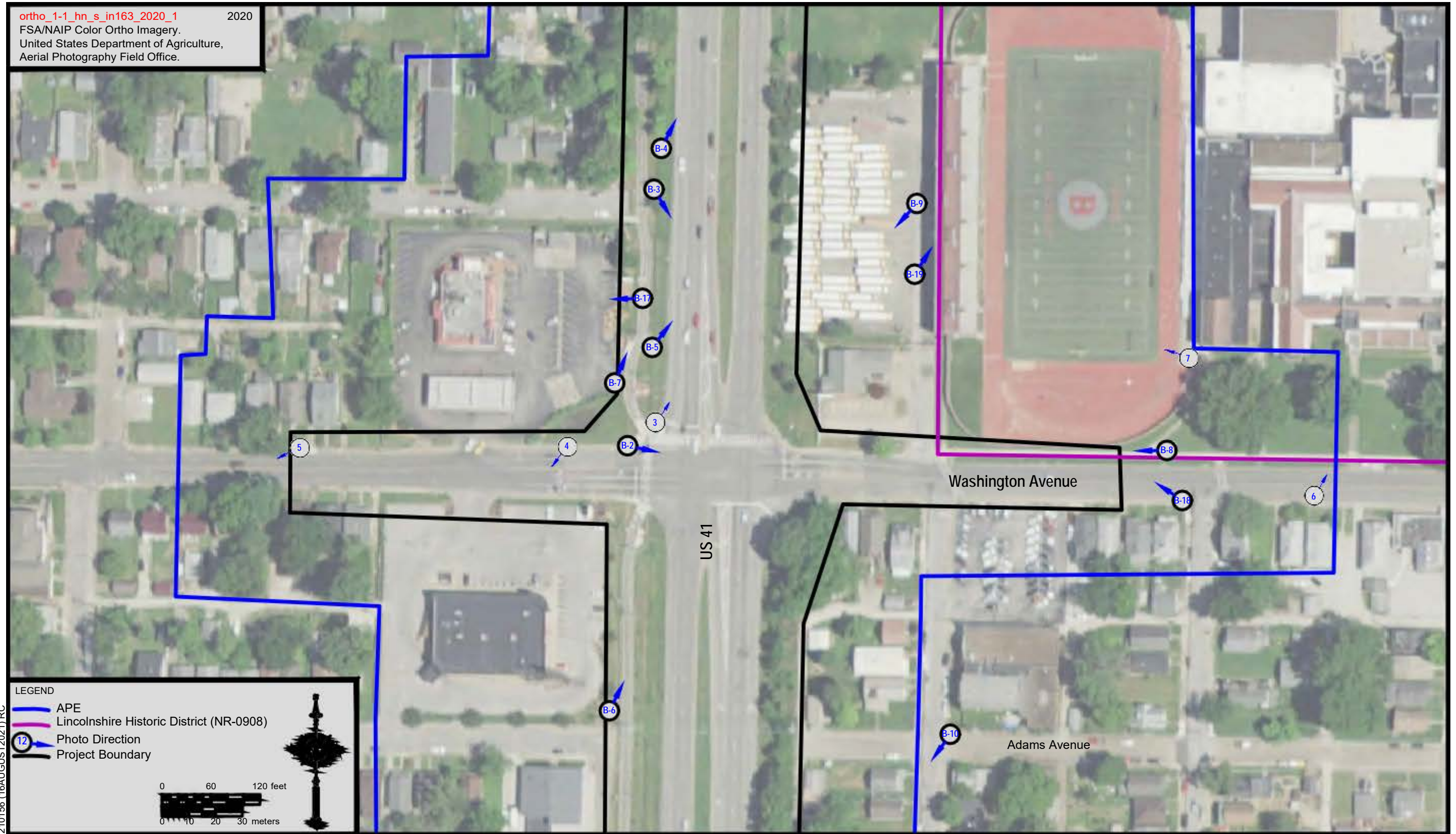


Figure B-1a. Washington Avenue and US 41 intersection improvement project photomap.

ortho_1-1_hn_s_in163_2020_1 2020
FSA/NAIP Color Ortho Imagery.
United States Department of Agriculture,
Aerial Photography Field Office.



1210156 (16AUGUST2021)RC

Figure B-1b. Washington Avenue and US 41 intersection improvement project photomap.

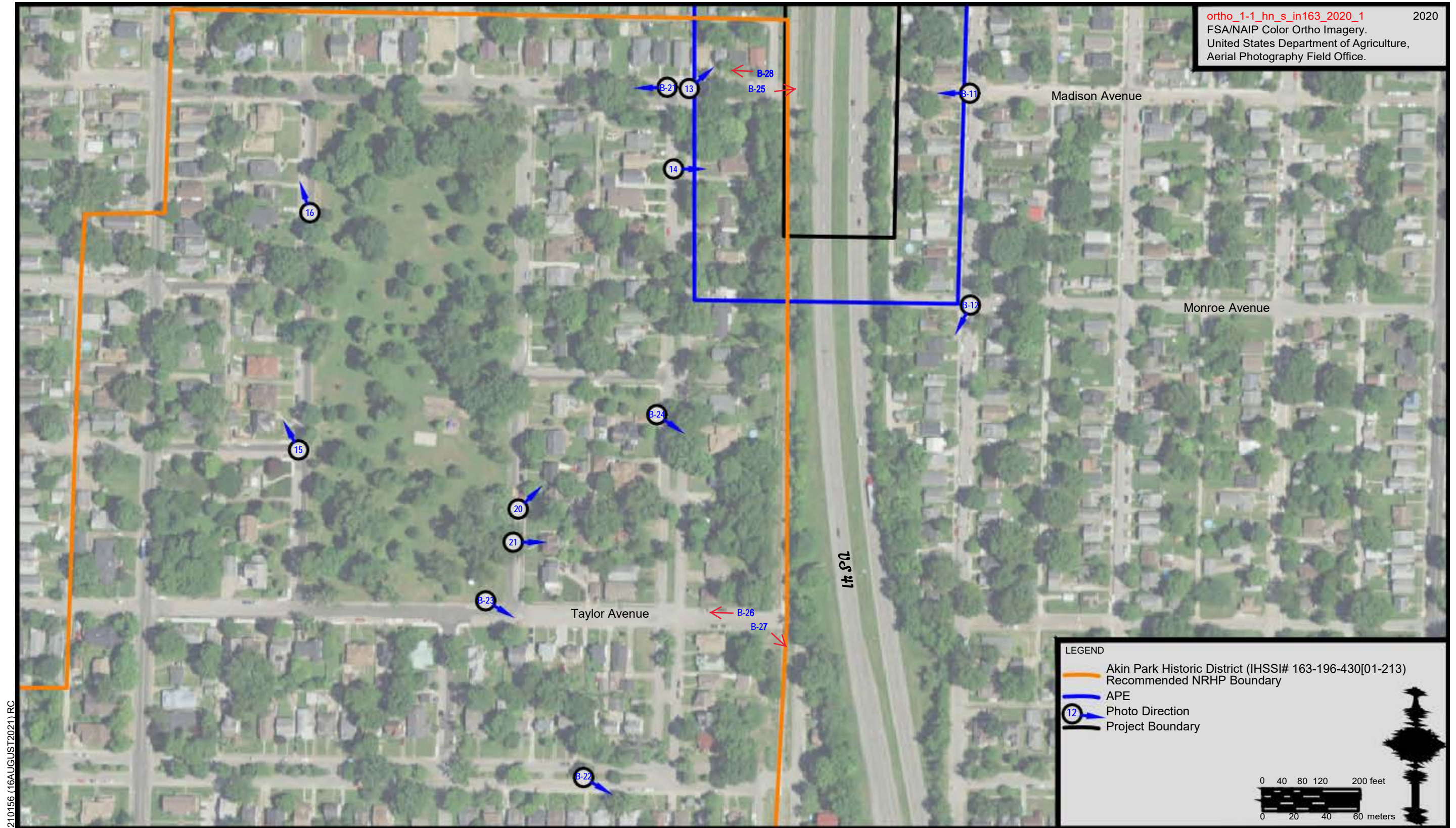


Figure B-1c. Washington Avenue and US 41 intersection improvement project photomap.

ortho_1-1_hn_s_in163_2020_1 2020
FSA/NAIP Color Ortho Imagery.
United States Department of Agriculture,
Aerial Photography Field Office.

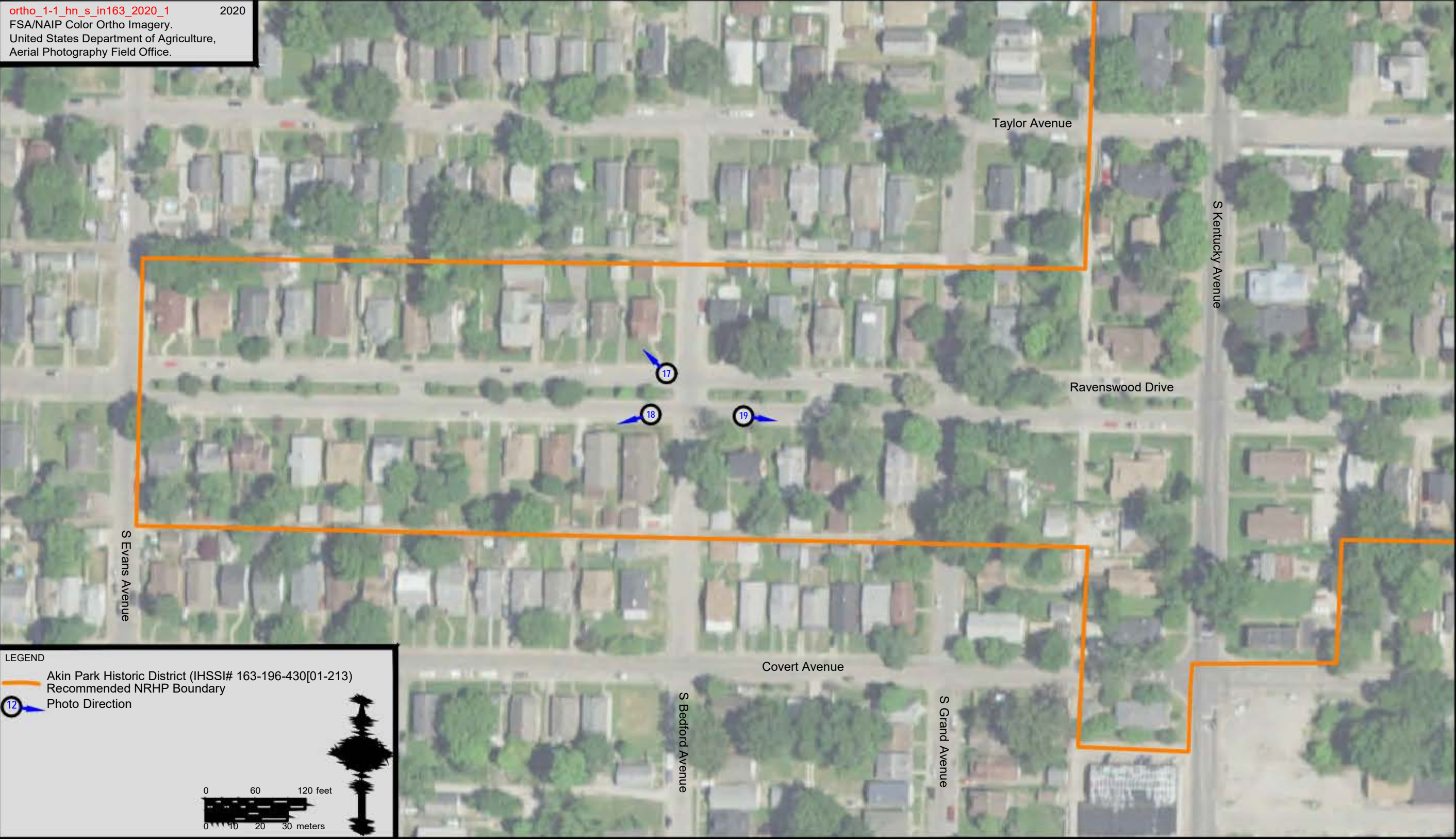


Figure B-1d. Washington Avenue and US 41 intersection improvement project photomap.



Figure 1. Overview of project area at the intersection of Washington Avenue and US 41, looking northeast



Figure 2. Benjamin Bosse High School (IHSSI No. 163-196-41045), "Outstanding," located within the Lincolnshire Historic District (NR-0908), façade (south) elevation, facing northeast.



Figure 3. Overview of the interior of Enlow Field (IHSSI No. 163-196-41044), "Outstanding," located within the Lincolnshire Historic District (NR-0908), facing northwest.



Figure 4. Overview of the Akin Park Historic District at the intersection of Jackson Street and Park Drive, facing northwest.



Figure 5. Overview of the Akin Park Historic District along Park Drive, facing northwest.



Figure 6. Overview of the Akin Park Historic District at the intersection of South Bedford Avenue and Ravenswood Drive, facing northwest.



Figure 7. Overview of the Akin Park Historic District at the intersection of South Bedford Avenue and Ravenswood Drive, facing southwest.



Figure 8. Overview of the Akin Park Historic District at the intersection of South Bedford Avenue and Ravenswood Drive, facing southeast, showing the boulevard.



Figure 9. Façade (west) and south elevation of the Irma Schmidt House (IHSSI No. 163-196-430[168]; "Notable," designed by Alfred Neucks, facing northeast, outside of the APE.



Figure 10. Façade (west) elevation of the Dr. C. Gardner Johnson House (IHSSI No. 163-196-430[166]; "Notable"), designed by Walter K. Stuart, facing east, outside of the APE.



Figure 11. Overview of project area at the intersection of US 41 and Washington Avenue, facing southeast.



Figure 12. Overview of APE along the west side of US 41, facing southeast.



Figure 13. View of project area toward the Lincolnshire Historic District (NR-0908) with Enlow Field (IHSSI No. 163- 196-41044), "Outstanding," in the foreground, facing northeast.



Figure 14. Overview of APE along US 41, facing northeast.



Figure 15. Overview of APE along Washington Avenue, facing west.



Figure 16. Overview of project area from the Enlow Field parking lot, facing southwest.



Figure 17. Overview of the project area along US 41, facing southeast near the Akin Park Historic District.



Figure 18. Exterior of Enlow Field (IHSSI No. 163-196-41044), part of the Lincolnshire Historic District (NR-0908) along Washington Avenue, facing northwest.



Figure 19. Exterior of Enlow Field (IHSSI No. 163-196-41044), part of the Lincolnshire Historic District (NR-0908) from the parking lot, facing northeast.



Figure 20. Overview of the Akin Park Historic District (IHSSI No. 163-196-430[01-213]) at the intersection of Akin Drive and Madison Avenue, facing west.



Figure 21. Overview of the Akin Park Historic District (IHSSI No. 163-196-430[01-213]) along Ravenswood Drive, facing southeast.



Figure 22. Overview of Akin Park Historic District (IHSSI No. 163-196-430[01-213]) along Taylor Avenue, facing southeast.



Figure B-2. Overview of project area at the intersection of US 41 and Washington Avenue, facing southeast.



Figure B-3. Overview of APE along the west side of US 41, facing southeast.



Figure B-4. Overview of APE along the west side of US 41, facing northeast.



Figure B-5. View of project area toward the Lincolnshire Historic District (NR-0908) with Enlow Field (IHSSI No. 163-196-41044), "Outstanding," in the foreground, facing northeast.



Figure B-6. Overview of APE along US 41, facing northeast.



Figure B-7. Overview of project area along west side of US 41, facing northeast.



Figure B-8. Overview of APE along Washington Avenue, facing west.



Figure B-9. Overview of project area from the Enlow Field parking lot, facing southwest.



Figure B-10. Overview of the APE at the intersection of Adams Avenue and South Harlan Avenue, facing southwest.



Figure B-11. Overview of the project area from the intersection of South Harlan Avenue and Madison Avenue, facing west.



Figure B-12. Overview of resources located just outside of the APE at the intersection of South Harlan Avenue and Monroe Avenue, facing southwest.



Figure B-13. Overview of the APE at the intersection of South Harlan Avenue and East Chandler Avenue, facing northwest.



Figure B-14. Overview of the project area along US 41, facing southeast.



Figure B-15. Overview of the APE along Bayard Park Drive, facing west.



Figure B-16. Overview of the APE along US 41, facing northeast.



Figure B-17. Example of a property constructed during the early twenty-first century at the intersection of US 41 and Washington Avenue, facing northwest.



Figure B-18. Exterior of Enlow Field (IHSSI No. 163-196-41044), part of the Lincolnshire Historic District (NR-0908) along Washington Avenue, facing northwest.



Figure B-19. Exterior of Enlow Field (IHSSI No. 163-196-41044), part of the Lincolnshire Historic District (NR-0908) from the parking lot, facing northeast.



Figure B-20. Overview of a portion of the Lincolnshire Historic District (NR-0908) located within and just outside of the APE along East Powell Avenue, facing northwest.



Figure B-21. Overview of the Akin Park Historic District (IHSSI No. 163-196-430[01-213]) at the intersection of Akin Drive and Madison Avenue, facing west.



Figure B-22. Overview of the Akin Park Historic District (IHSSI No. 163-196-430[01-213]) along Ravenswood Drive, facing southeast.



Figure B-23. Overview of Akin Park Historic District (IHSSI No. 163-196-430[01-213]) along Taylor Avenue, facing southeast.



Figure B-24. Overview of Akin Park Historic District (IHSSI No. 163-196-430[01-213]) along Akin Drive, facing southeast.



Figure B-25. Overview of project area at the eastern end of Madison Avenue near the Akin Park Historic District looking toward the project area along US 41, facing east.



Figure B-26. Overview of the Akin Park Historic District on Taylor Avenue near the eastern edge of the project area along US 41, facing west.



Figure B-27. Overview of the project area from **Taylor Avenue** near the Akin Park Historic District, facing southeast.

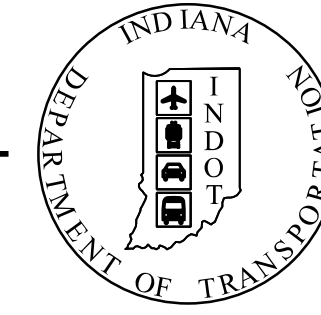


Figure B-28. Overview of the Akin Park Historic District from the eastern end of **Madison Avenue** near the project area along US 41, facing southwest.

APPENDIX C: PLANS

PROJECT	DESIGNATION
2000186	2000186
CONTRACT	
B-42876	

INDIANA DEPARTMENT OF TRANSPORTATION



TRAFFIC DATA	U.S. 41	WASHINGTON AVE.
A.A.D.T. (2023)	30,200 V.P.D.	12,900 V.P.D.
A.A.D.T. (2044)	37,100 V.P.D.	13,100 V.P.D.
D.H.V. (2044)	2,970 V.P.H.	1,180 V.P.H.
DIRECTIONAL DISTRIBUTION	59.00%	57.00%
TRUCKS	11.00% OF AADT	2.00% OF AADT
	1.00% OF DHV	1.00% OF DHV

DESIGN DATA	U.S. 41	WASHINGTON AVE.
DESIGN SPEED	40 MPH	30 MPH
PROJECT DESIGN CRITERIA	3R NON-FREEWAY	3R NON-FREEWAY
FUNCTIONAL CLASSIFICATION	PRINCIPAL ARTERIAL	MINOR ARTERIAL
RURAL/URBAN	URBAN	URBAN
TERRAIN LEVEL	LEVEL	LEVEL
ACCESS CONTROL	PARTIAL	PARTIAL

ROAD PLANS

US 41 AND WASHINGTON AVENUE INTERSECTION IMPROVEMENT

ROUTE: U.S. 41 FROM: RP 2+82 TO: RP 2+89

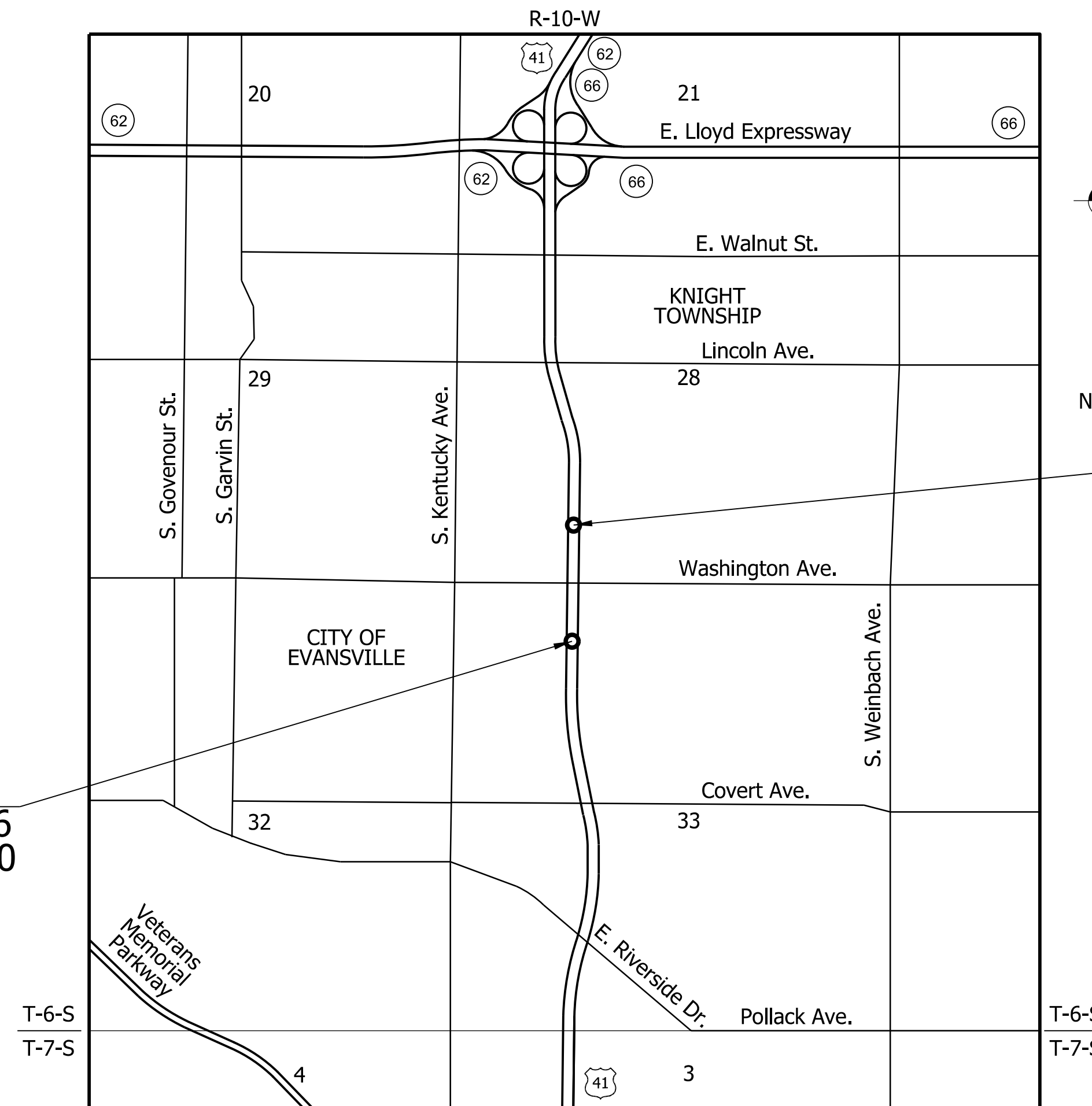
PROJECT NO. 2000186 P.E.
2000186 CONST.

KIN PROJECT INFORMATION		
DESIGNATION	PROJECT DESCRIPTION	LEAD DES
1902709	PEDESTRIAN BRIDGE OVER US 41	
2000186	US 41 AND WASHINGTON AVENUE INTERSECTION IMPROVEMENT	

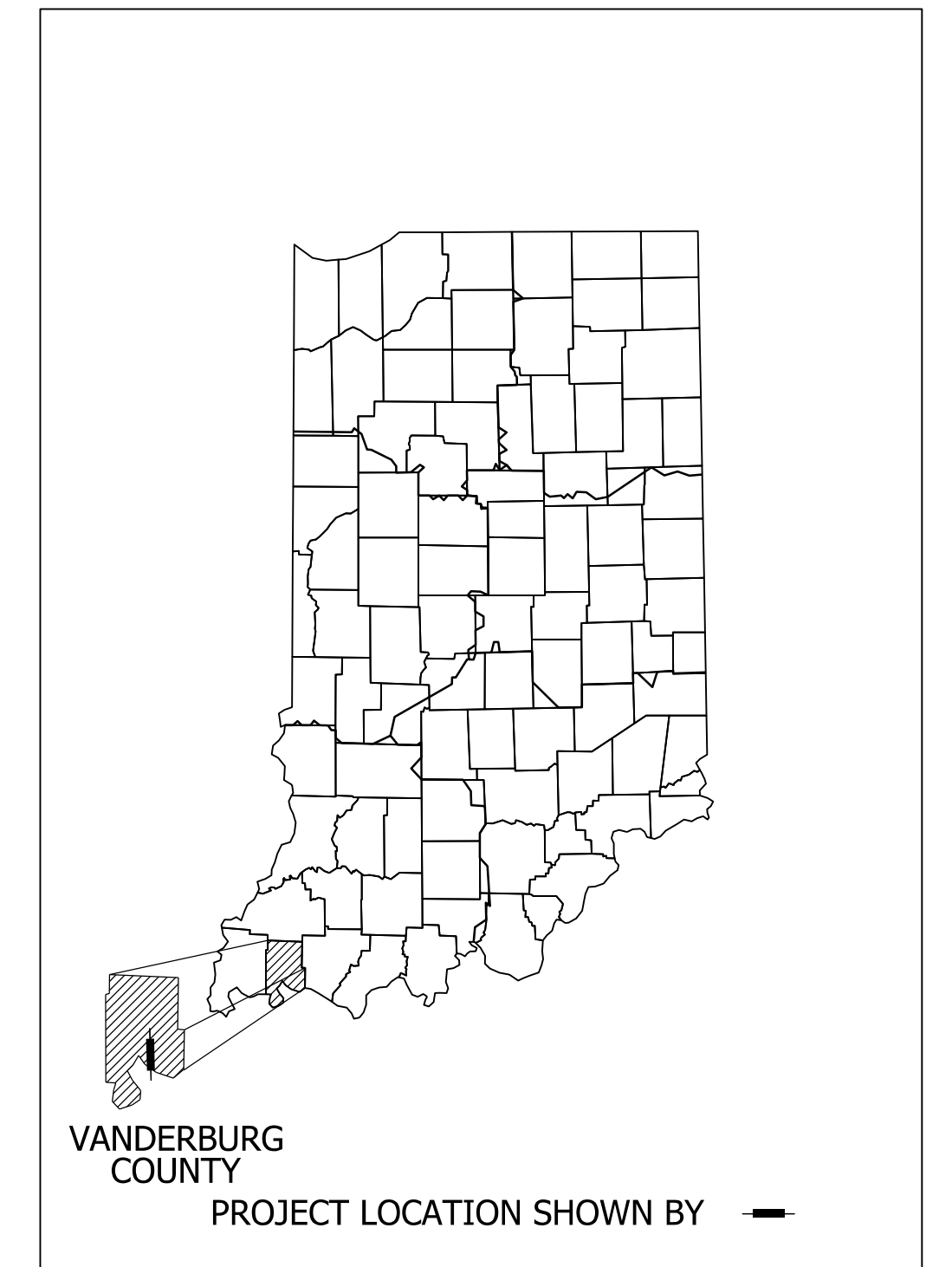
PROJECT DESCRIPTION
INTERSECTION IMPROVEMENTS AND ROADWAY MODIFICATIONS ON U.S. 41 AND WASHINGTON AVENUE APPROXIMATELY 1.00 MILES SOUTH OF THE U.S. 41/E. LLOYD EXPRESSWAY IN SECTION 28 & 33, T-6-S, R-10-W, CITY OF EVANSVILLE, KNIGHT TOWNSHIP, VANDERBURGH COUNTY, INDIANA.

STAGE 2 REVIEW PLANS
APRIL 1, 2022

BEGIN PROJECT
PROJECT NO. 2000186
P.O.C. STA. 273+00.00
LINE "B"



END PROJECT
PROJECT NO. 2000186
P.O.T. STA. 293+95.00
LINE "B"



LATITUDE: 37°57'46"N LONGITUDE: 87°32'34"W

GROSS LENGTH: 0.397 MI.
NET LENGTH: 0.397 MI.
MAX. GRADE: 0.73%

HYDROLOGIC UNIT CODE: 05140202010030

INDIANA DEPARTMENT OF TRANSPORTATION
STANDARD SPECIFICATIONS DATED 2022
TO BE USED WITH THESE PLANS

HNTB
THE HNTB COMPANIES
INFRASTRUCTURE SOLUTIONS
111 MONUMENT CIRCLE
SUITE 1200
INDIANAPOLIS, IN 46204-5178

DRAFT
NOT FOR CONSTRUCTION

PLANS PREPARED BY: HNTB INDIANA, INC. (317) 636-4682 PHONE NUMBER
CERTIFIED BY: _____ DATE
APPROVED FOR LETTING: _____ DATE
INDIANA DEPARTMENT OF TRANSPORTATION

DESIGNATION	
2000186	
SURVEY BOOK	SHEETS
ELECTRONIC	1 of 45
CONTRACT	PROJECT
B-42876	2000186

UTILITIES

COMMUNICATIONS:

AT&T - DISTRIBUTION & TRANSMISSION
240 N. MERIDIAN ST., ROOM 1791
INDIANAPOLIS, IN 46204
MATT SPINDLER
(317) 265-3050
MS4822@ATT.COM

CABLE TV:

WIDE OPEN WEST (WOW)
6600 HANK AVENUE
EVANSVILLE, IN 47715
JAMES FAMBROUGH
(812) 437-0395
JAMES.FAMBROUGH@WOWINC.COM

GAS:

VECTREN
16000 ALLISONVILLE ROAD
NOBLESVILLE, IN 46061
(812) 491-4765
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SPECTRUM
1900 N. FARES AVE.
EVANSVILLE, IN. 47711
JOHN WADE
(812) 253-2170
JOHN.WADE@CHARTER.COM

WATER & SEWER:

EVANSVILLE WATER & SEWER UTILITY
1931 ALLENS LANE
EVANSVILLE, IN 47720
(812) 421-2120
WATER - RYAN MAYER; RJMAYER@EWSU.COM
JAOLYN SCHMITT; JSCHMITT@EWSU.COM

NOTE TO REVIEWER

**UTILITY CONTACTS ARE PRELIMINARY AND
WILL BE FINALIZED FOR FUTURE SUBMITTALS.**

CENTERPOINT ENERGY - ELECTRIC DISTRIBUTION
1 N. MAIN ST.
EVANSVILLE, IN 47702
(812) 491-4714
HEATHER.GRAVINO@CENTERPOINTENERGY.COM

ONPOINT CONSTRUCTION MANAGEMENT
1728 CHURCHMAN AVE.
INDIANAPOLIS, IN 47452
DALE SEALS
(765) 561-7047
DALE.DEALS@OCMGROUPS.COM

RVP FIBER COMPANY LLC
201 IONA AVE. SW
GRAND RAPIDS, MI 49503
RYAN MIEDEMA
(616) 223-7144
RJMIEDEMA@USSIGNAL.COM



GENERAL NOTES

	The final cross sections of the grading contract will be the original cross sections of the paving contract. However, partial or complete cross sections shall be taken if necessary to determine the actual excavation quantities.
	The paper relocation lines will be cross sectioned by the engineer before construction.
	Existing asphalt pavement located outside the construction limits shall be removed as directed.
	Existing pccp sidewalks and curbs shall be removed from project right of way limits unless otherwise noted and shall be paid for under clearing right of way.
XX	All earth shoulders, median areas, cut and fill slopes shall be plain or mulched seeded except where sodding is specified.
XX	All existing storm drainage pipes, inlets, and manholes shall remain unless otherwise noted.
	All limited access right of way is to be fenced with black vinyl coated chain link type fence (b.v.c.l.t.f.) unless otherwise noted.
	All existing right of way fence shall be removed unless otherwise noted.
XX	All disturbed areas shall be seeded with seed mixture "R" unless otherwise noted.
XX	All slopes are to be repaired with a minimum amount of grading so as not to disturb existing vegetation more than necessary.
	All pipes that are to be removed which connect to existing sewers that are to remain in operation shall be sealed water tight.

XX Denotes General Notes Required.

SHEET INDEX

SHEET NO.	DESIGNATION
1	TITLE SHEET
2	INDEX AND GENERAL NOTES
3-4	ROADWAY TYPICAL CROSS SECTIONS
5-18	MAINTENANCE OF TRAFFIC
19-22	ROADWAY PLAN LINE "B"
23	ROADWAY PLAN LINE "C"
24-27	CONSTRUCTION DETAILS
28-30	INTERSECTION DETAILS
31-36	PAVEMENT MARKING DETAILS
37-38	SIGNING PLAN
39-41	TRAFFIC SIGNALS
42	APPROACH TABLE
43	STRUCTURE DATA TABLE
44-56	ROADWAY CROSS SECTIONS LINE "R"

NOTE TO REVIEWER
CROSS SECTIONS NOT INCLUDED FOR LINE "C"
DUE TO THE ONLY WORK ALONG THIS
ALIGNMENT BEING SIGNING AND
PAVEMENT MARKINGS.

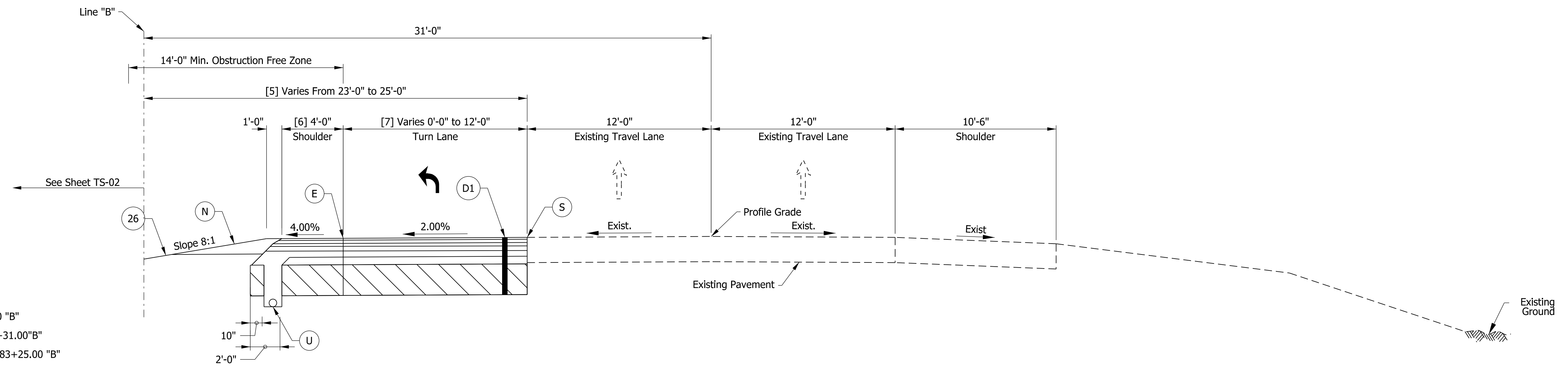
REVISIONS

SHEET NO.	DATE	REVISED

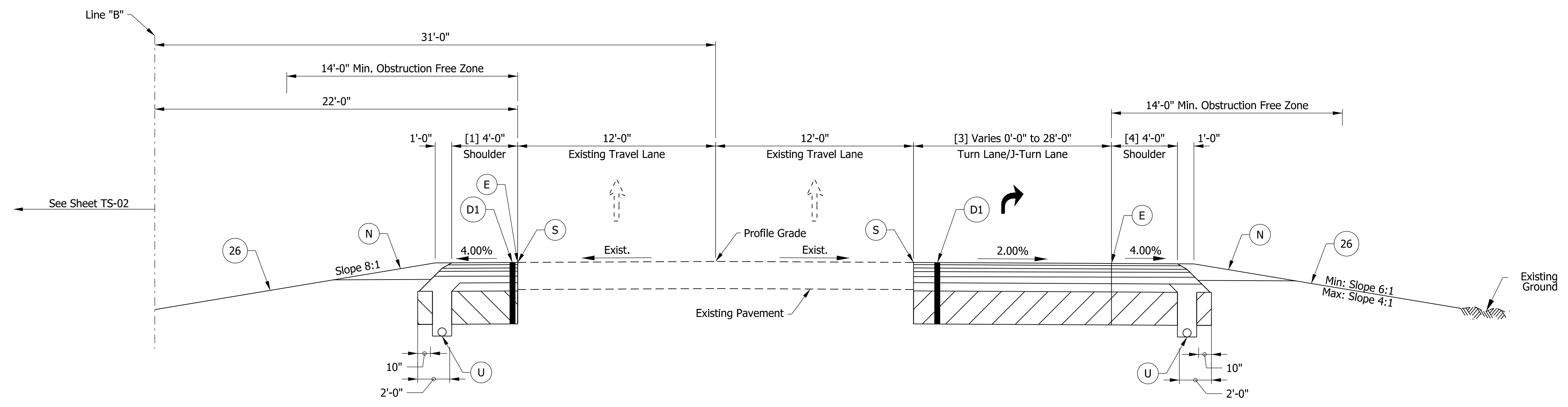
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NOT FOR CONSTRUCTION

RECOMMENDED FOR APPROVAL _____ DESIGN ENGINEER _____ DATE _____	INDIANA DEPARTMENT OF TRANSPORTATION		HORIZONTAL SCALE 1"=1'-0"	BRIDGE FILE N/A
DESIGNED: _____ NSX _____ DRAWN: _____ CPH _____	INDEX AND GENERAL NOTES		VERTICAL SCALE N/A	DESIGNATION 2000186
CHECKED: _____ TNE _____ CHECKED: _____ NSX _____			SURVEY BOOK ELECTRONIC	SHEETS IN-01 2 of 45
			CONTRACT B-42876	PROJECT 2000186

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TYPICAL SECTION U.S. 41 NORTHBOUND
STA. 286+85.00 "B" TO STA. 293+30.00 "B"



TYPICAL SECTION U.S. 41 NORTHBOUND
STA. 273+00.00 "B" TO STA. 283+25.00 "B"

Notes

- [1] Inside Shoulder is 4'-0" at Sta. 273+00.00 "B" to Sta. 283+25.00 "B"
Omit Inside Shoulder at Turn Lane From Sta. 273+94.32 "B" to Sta. 274+33.97 "B"
- [3] Right Turn Lane is 0'-0" From Sta. 273+00.00 "B" to Sta. 273+48.00 "B"
Right Turn Lane Varies From 0'-0" at Sta. 273+48.00 "B" to 28'-0" at Sta. 273+98.00 "B"
Right Turn Lane is 28'-0" From Sta. 273+98.00 "B" to Sta. 274+73.00 "B"
Right Turn Lane Varies From 28'-0" at Sta. 274+73.00 "B" to Sta. 12'-0" at Sta. 275+31.00 "B"
Right Turn Lane is 12'-0" From Sta. 275+31.00 "B" to 279+30.00 "B"
Omit Right Turn Lane at Existing Right Turn Lane From Sta. 279+30.00 "B" to Sta. 283+25.00 "B"
- [4] Outside Shoulder is 11'-0" From Sta. 273+00.00 "B" to Sta. 273+48.00 "B"
Outside Shoulder Varies From 11'-0" at Sta. 273+48.00 "B" to 4'-0" at Sta. 273+59.46 "B"
Outside Shoulder is 4'-0" From Sta. 273+59.46 "B" to Sta. 279+30.00 "B"
Omit Outside Shoulder at Existing Outside Shoulder From Sta. 279+30.00 "B" to Sta. 283+25.00 "B"
- [5] Inside Edge of Pavement Varies From 23'-0" at Sta. 283+85.00 "B" to 25'-0" at Sta. 290+00.00 "B"
Inside Edge of Pavement is 25'-0" at Sta. 290+00.00 "B" to Sta. 293+30.00 "B"
- [6] Inside Shoulder is 4'-0" at Sta. 283+85.00 "B" to Sta. 293+30.00 "B"
Omit Inside Shoulder at Turn Lane From Sta. 292+38.00 "B" to Sta. 292+78.66 "B"
- [7] Left Turn Lane is 0'-0" From Sta. 283+86.00 "B" to Sta. 284+35.00 "B"
Left Turn Lane Varies From 0'-0" at Sta. 284+35.00 "B" to 12'-0" at Sta. 285+35.00 "B"
Left Turn Lane is 12'-0" From Sta. 285+35.00 "B" to Sta. 292+52.24 "B"
Omit Left Turn Lane From Sta. 292+55.00 "B" to Sta. 293+30.00 "B"

Legend

- D1 165#/Syd. QC/OA-HMA, 4, 70, Surface, 9.5 mm on
275#/Syd. QC/OA-HMA, 4, 70, Intermediate, 19.0 mm on
880#/Syd. QC/OA-HMA, 4, 64, Base, 25.0 mm on
300#/Syd. QC/OA-HMA, 4, 76, Intermediate OG, 19.0 mm on
6 in. of Compacted Aggregate No. 53 on
Subgrade Treatment Type IC
- E Milled HMA Corrugations, Conventional (See Note 1)
- N Compacted Aggregate, No. 53
- S Saw Cut (No Direct Pay)
- U 6 In. Underdrain
(See Std. Dwg. No. 718-UNDR-01)
- 26 Seed Mixture, U
- ** Proposed Cross Slope to Match
Existing Slope of Adjacent Lane.
- *** Cross Slope Varies. See Spot Elevation
Details for Additional Information.

Notes

- 1. Milled HMA Corrugations, Conventional shall be installed per Std. Dwg. E 606-SHCG-02.

Note to Reviewer: Safety edge detail to be provided in a future submittal.

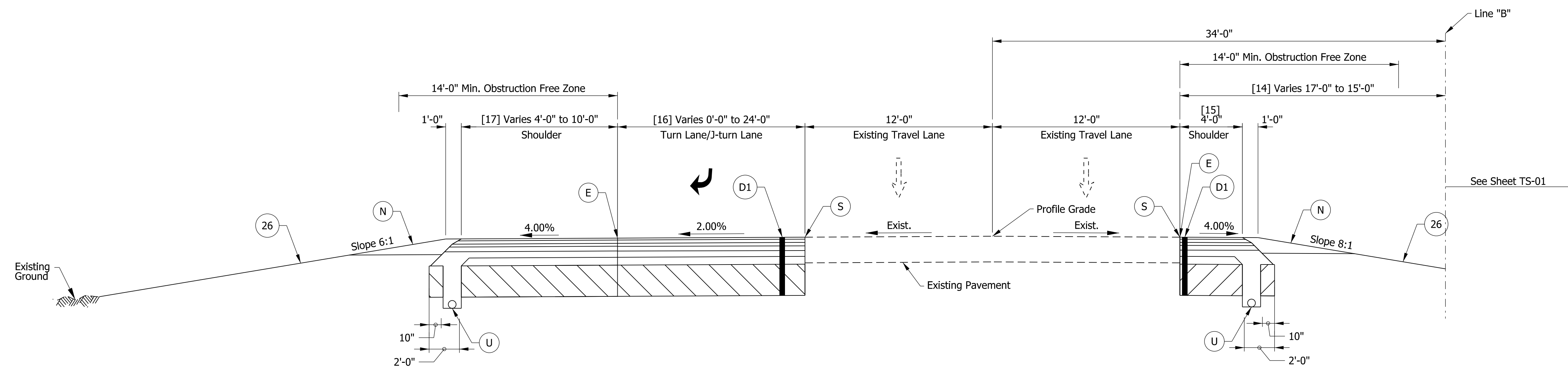
Note to Reviewer: Pavement assumed to be asphalt. Final pavement design to be determined in a future submittal.

DRAFT
NOT FOR CONSTRUCTION

RECOMMENDED FOR APPROVAL _____	DESIGN ENGINEER _____	DATE _____
DESIGNED: _____ NSX _____	DRAWN: _____ LMC _____	
CHECKED: _____ TNE _____	CHECKED: _____ NSX _____	

INDIANA DEPARTMENT OF TRANSPORTATION		
ROADWAY TYPICAL CROSS SECTIONS		

HORIZONTAL SCALE	BRIDGE FILE
1/4"=1'-0"	N/A
VERTICAL SCALE	DESIGNATION
1"=5'	2000186
SURVEY BOOK	SHEETS TS-01
ELECTRONIC	3 of 45
CONTRACT	PROJECT
B-42876	2000186



TYPICAL SECTION U.S. 41 SOUTHBOUND
STA. 283+85.00 "B" TO STA. 293+95.00 "B"

Notes

[11] Inside Edge of Pavement is 19'-0" From Sta. 273+58.00 "B" to Sta. 277+50.00 "B"
Inside Edge of Pavement Varies From 19'-0" at Sta. 277+50.00 "B" to 18'-0" at Sta. 280+00.00 "B"
Inside Edge of Pavement is 18'-0" at Sta. 280+00.00 "B" to Sta. 283+25.00 "B"

[12] Omit Left Turn Lane From Sta. 273+94.00 "B" to Sta. 274+33.00 "B"
Left Turn Lane is 12'-0" From Sta. 274+33.00 "B" to Sta. 281+75.00 "B"
Left Turn Lane Varies From 12'-0" at Sta. 281+75.00 to 0'-0" at Sta. 282+75.00 "B"
Left Turn Lane is 0'-0" From Sta. 282+75.00 "B" to Sta. 283+25.00 "B"

[13] Omit Inside Shoulder at Left Turn Lane From Sta. 274+09.34 "B" to Sta. 274+50.00 "B"

[14] Inside Edge of Pavement is 16'-0" at Sta. 283+85.00 "B" to Sta. 293+95.00 "B"

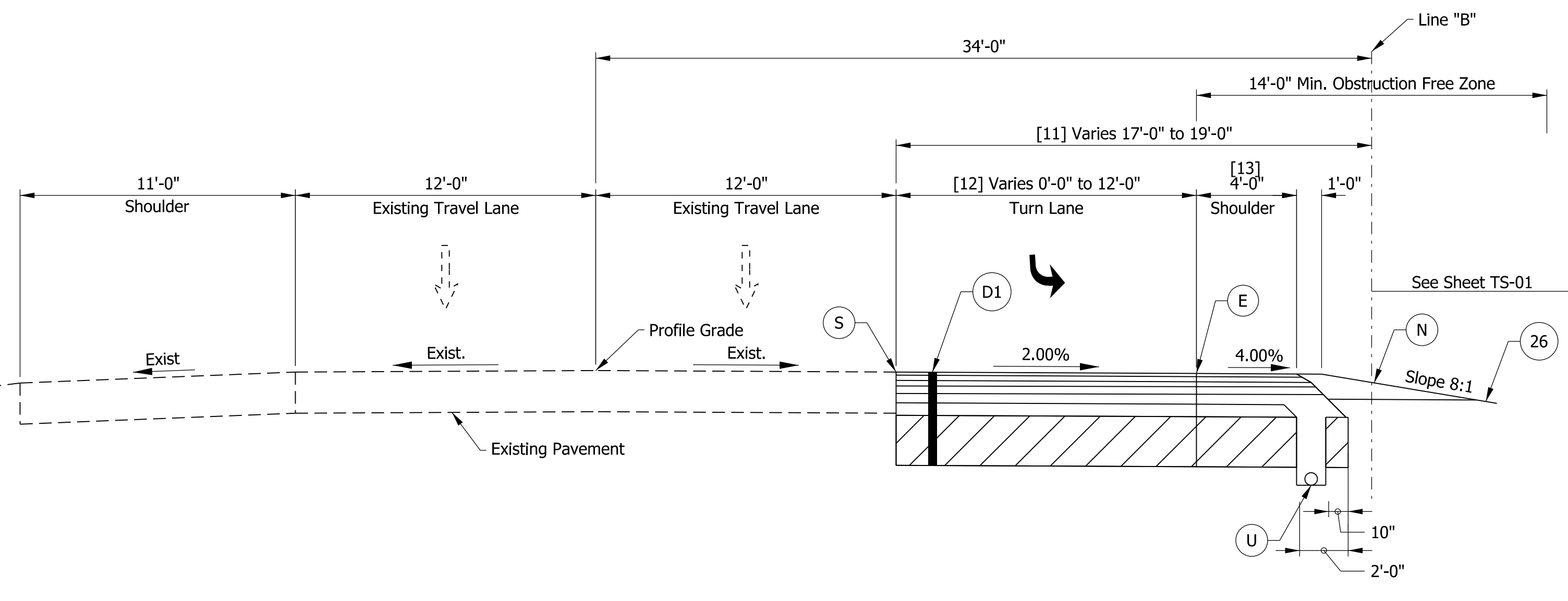
[15] Inside Shoulder is 4'-0" at Sta. 283+85.00 "B" to Sta. 293+30.00 "B"
Omit Inside Shoulder at Turn Lane From Sta. 292+53.72 "B" to Sta. 292+94.00 "B"

[16] Omit Right Turn Lane at Existing Right Turn Lane From Sta. 283+86.00 "B" to Sta. 289+33.00 "B"
Right Turn Lane is 12'-0" From Sta. 289+33.00 "B" to Sta. 291+25.00 "B"
Right Turn Lane Varies From 12'-0" at Sta. 291+25.00 "B" to 24'-0" at Sta. 292+15.00 "B"
Right Turn Lane is 24'-0" From Sta. 292+15.00 "B" to Sta. 292+90.00 "B"
Right Turn Lane Varies From 24'-0" at Sta. 292+90.00 "B" to 0'-0" at Sta. 293+40.00 "B"
Right Turn Lane is 0'-0" From Sta. 293+40.00 "B" to Sta. 293+95.00 "B"

[17] Omit Outside Shoulder at Existing Outside Shoulder From Sta. 283+86.00 "B" to Sta. 289+33.00 "B"
Outside Shoulder is 4'-0" From Sta. 289+33.00 "B" to Sta. 293+13.67 "B"
Outside Shoulder Varies From 4'-0" at Sta. 293+28.41 "B" to 10'-0" at Sta. 293+40.00 "B"
Outside Shoulder is 10'-0" From Sta. 293+40.00 "B" to Sta. 293+95.00 "B"

Legend

- (D1) 165#/Syd. QC/OA-HMA, 4, 70, Surface, 9.5 mm on 275#/Syd. QC/OA-HMA, 4, 70, Intermediate, 19.0 mm on 880#/Syd. QC/OA-HMA, 4, 64, Base, 25.0 mm on 300#/Syd. QC/OA-HMA, 4, 76, Intermediate OG, 19.0 mm on 6 in. of Compacted Aggregate No. 53 on Subgrade Treatment Type IC
- (E) Milled HMA Corrugations, Conventional (See Note 1)
- (N) Compacted Aggregate, No. 53
- (S) Saw Cut (No Direct Pay)
- (U) 6 In. Underdrain (See Std. Dwg. No. 718-UNDR-01)
- (26) Seed Mixture, U
- ** Proposed Cross Slope to Match Existing Slope of Adjacent Lane.
- *** Cross Slope Varies. See Spot Elevation Details for Additional Information.



TYPICAL SECTION U.S. 41 SOUTHBOUND
STA. 273+58.00 "B" TO STA. 283+25.00 "B"

Note to Reviewer: Spot elevation details to be provided in a future submittal.

Note to Reviewer: Safety edge detail to be provided in a future submittal.

Notes

1. Milled HMA Corrugations, Conventional shall be installed per Std. Dwg. E 606-SHCG-02.

Note to Reviewer: Pavement assumed to be asphalt. Final pavement design to be determined in a future submittal.

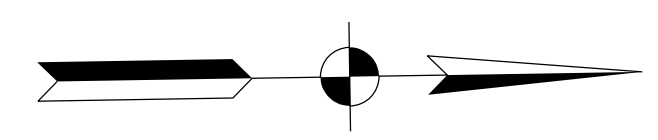
DRAFT
NOT FOR CONSTRUCTION

RECOMMENDED FOR APPROVAL _____	DESIGN ENGINEER _____	DATE _____
DESIGNED: _____ NSX _____	DRAWN: _____ LMC _____	
CHECKED: _____ TNE _____	CHECKED: _____ NSX _____	

INDIANA DEPARTMENT OF TRANSPORTATION	
ROADWAY TYPICAL CROSS SECTIONS	

HORIZONTAL SCALE	BRIDGE FILE
1/4"=1'-0"	N/A
VERTICAL SCALE	DESIGNATION
1"=5'	2000186
SURVEY BOOK	SHEETS TS-02
ELECTRONIC	4 of 45
CONTRACT	PROJECT
B-42876	2000186

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270+00

275+00

280+00

285+00

290+00

295+00

INDEX OF OWNERS

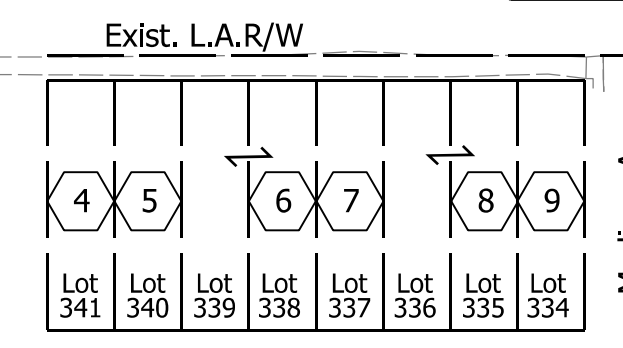
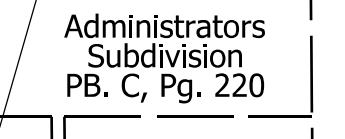
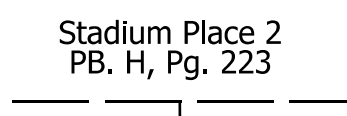
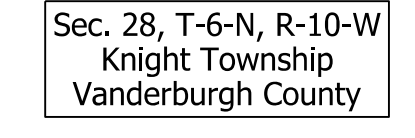
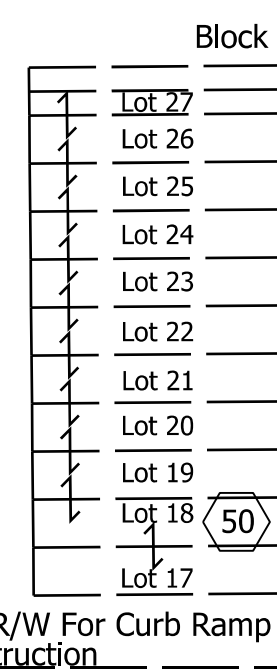
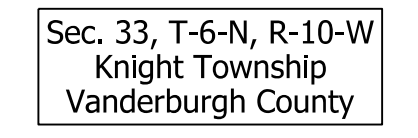
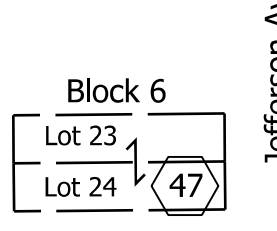
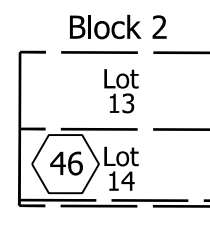
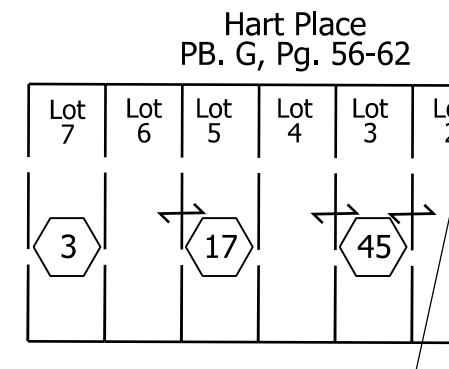
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- 17 SEWARD, ELIZABETH
- 45 LEWIS, MARVIN E. & TRACY Y.
- 46 JOHNSON, JOHN & COOPER, FARESTINE
- 47 HAWKINS, LAWANDA J.
- 48 RHODES, JOYCE
- 49 WHEELER, STANLEY
- 50 WARREN WASHINGTON AVENUE PROPERTY LLC

INDEX OF OWNERS

- 36 SYSTEM CAPITAL REAL PROPERTY CORPORATION 13-0681
- 37 FLINT CREEK PROPERTIES LLC GOEBEL COMMERCIAL REALTY
- 38 MCGLOTHEN, KELLY
- 39 WADDELL, EVIE C.
- 40 OCHOA, MIGUEL & NERI, NICOLASA H/W
- 41 M A VALLEY INC.
- 42 SOUTHERN INDIANA GAS & ELECTRIC CO
- 43 CHAVOUS, STEPHANIE

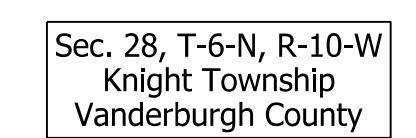
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LINE "B"
N: 176331.32
E: 789440.14

END PROJECT
PROJECT NO. 1800226
P.O.T. STA. 293+95.00
LINE "B"
N: 178426.20
E: 789457.38



INDEX OF OWNERS

- 4 TENNYSON, THOMAS K.
- 5 GIBSON, RATHEN
- 6 REISINGER, RONALD E. & MARY LOU
- 7 REISINGER, RONALD E. & MARY LOU
- 8 REISINGER, RONALD E. & MARY LOU
- 9 REISINGER, RONALD E. & MARY LOU
- 10 JONES, BRITTANY HOLLOWAY & KILEY HILTON W/H
- 11 HADFIELD HOLDINGS LLC
- 12 SHREE LAXME LLC
- 13 BROCKMAN RANDY J.
- 14 LOWE, W WALT & D LYNN TRUSTEES T/C
- 15 SHARYGIN, ANDREI
- 16 WELCOME MANAGEMENT LLC
- 18 EFFINGER, ANGELA G.
- 19 YODER, WALTER & DEBORAH T/E
- 20 BARRON, THOMAS J. & MARY E.
- 21 PADDOCK, DANNY RAY SR.
- 22 ALEXANDER, JANE E.
- 23 MCDANIEL, KATHY L.
- 24 AMERSON, ELBERT D.
- 25 BONNIE, GENE PATRICK JR. & SHERWOOD, RACHEL
- 26 RUFF, TAMMY
- 27 THOMAS, GORDON S. & BRADFORD, DIXIE K.
- 28 HUSK, KIP N. & SANDRA L. H/W
- 29 MOORE, ROBERT C.
- 2 GRACE OF CHRIST INC.
- 1 IRWIN, LEATRICE & ROBINSON, RAPINA



INDEX OF OWNERS

- 30 SPIRIT MASTER FUNDING VI LLC C/O SPIRIT REALTY CAPITAL
- 31 EVANSVILLE VANDERBURGH SCHOOL CORPORATION
- 32 GLOVER, KATHERINE F.
- 33 SCHLECHTE, KATHLEEN M.
- 34 HODNETT, JAMES C.
- 35 SANDEFUR, MICHAEL & APRIL H/W

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RECOMMENDED FOR APPROVAL _____	DESIGN ENGINEER _____	DATE _____
DESIGNED: _____ NSX _____	DRAWN: _____ CPH _____	
CHECKED: _____ TNE _____	CHECKED: _____ NSX _____	

INDIANA
DEPARTMENT OF TRANSPORTATION

PLAT NO. 1

HORIZONTAL SCALE	BRIDGE FILE
1"=100'	N/A
VERTICAL SCALE	DESIGNATION
N/A	2000186
SURVEY BOOK	SHEETS PLT-01
ELECTRONIC	5 of 45
CONTRACT	PROJECT
B-42876	2000186

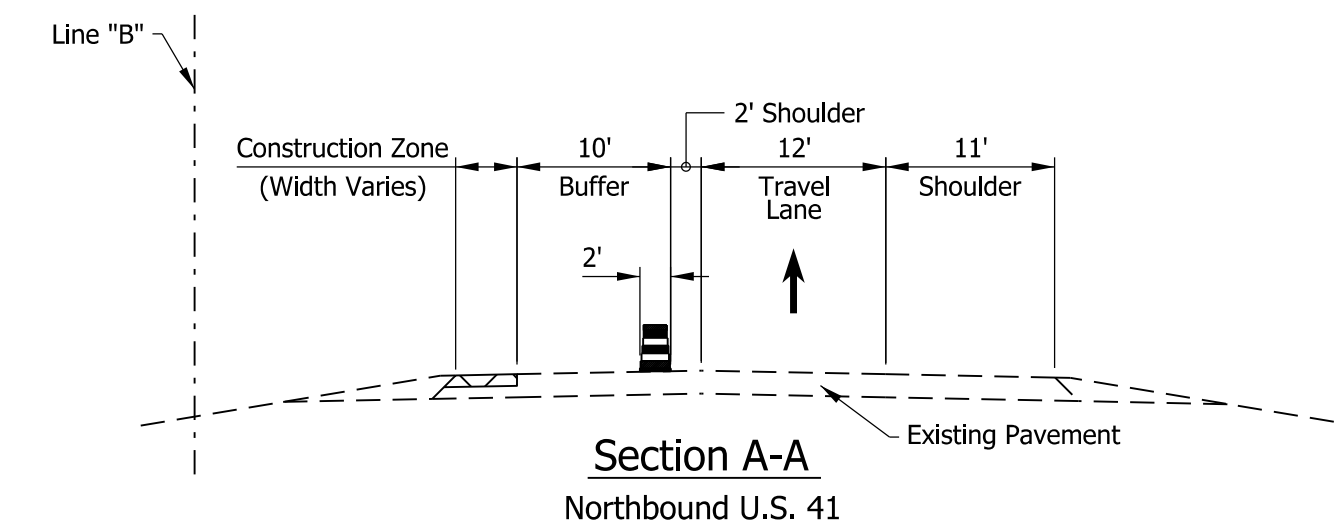
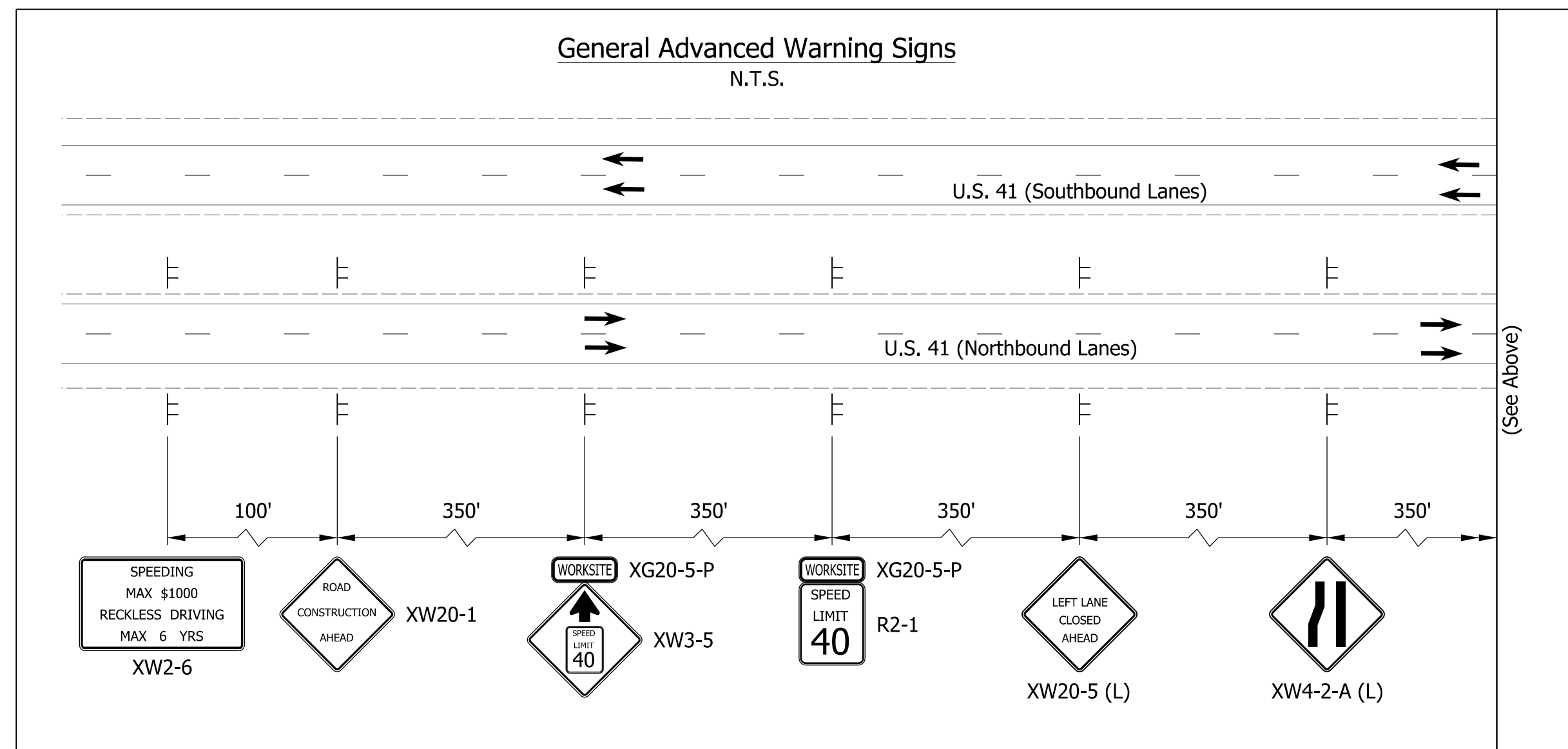
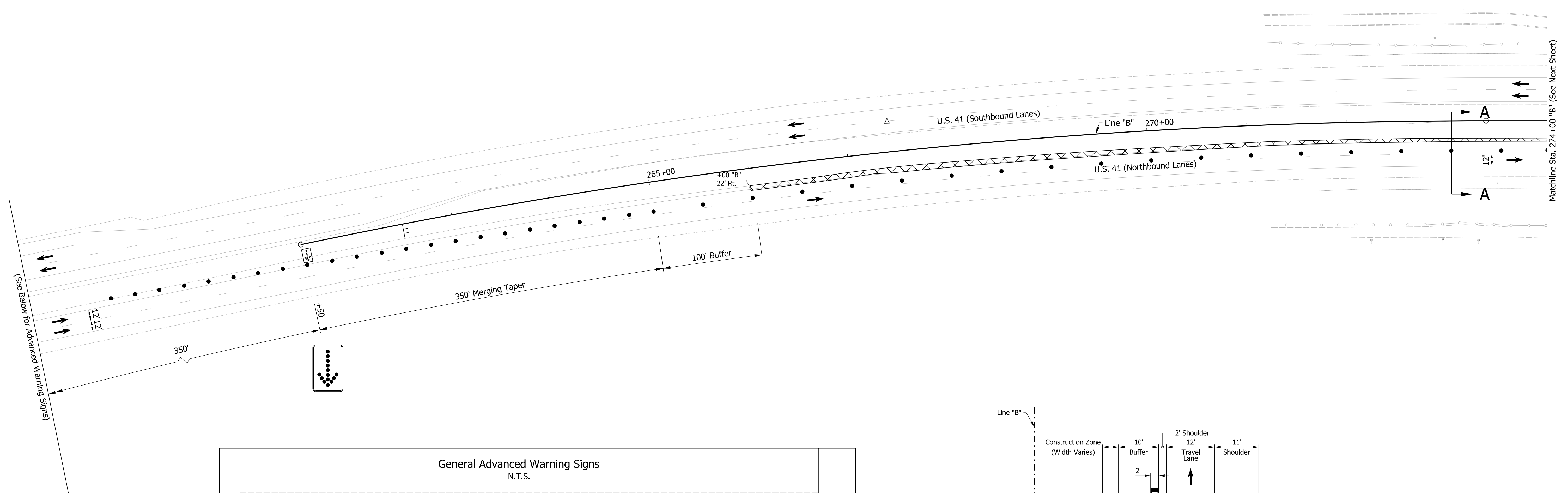
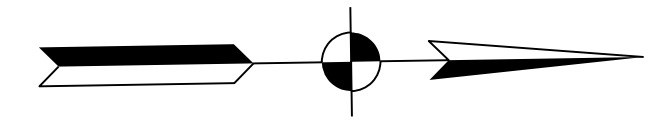
CONSTRUCTION SEQUENCING

PHASE 1:

1. Install Advanced Warning Signs.
2. Place temporary Traffic Drums adjacent to US 41 workzones.
3. Construct the Northbound and Southbound Shoulder Strengthening along US 41.

Construction Design Speed: 40 MPH
 Construction Clear Zone: 13'-0"

Maintenance of Traffic Quantity Summary - Phase 1		
Item	Amount	Unit
Construction Sign A	20	EACH
Construction Sign B	6	EACH
Temporary Worksite Speed Limit Sign Assembly	8	EACH
Flashig Arrow Sign	14	DAY



Notes:

1. Shoulder Strengthening to be completed by Single Lane Closure per SDD E 801-TCLC-11
2. Contractor shall maintain a 1' Minimum Shoulder at all times
3. All Sign Spacing shall be 350' Minimum unless otherwise noted
3. Traffic Drum Spacing along Tapers shall be at 25'
4. Traffic Drum Spacing along Tangents shall be at 50'
5. All temporary Pavement Markings shall be Removable
6. Existing Transverse Rumble Strips to remain in place
7. No changes to the Traffic Signal at US 41 and Washington Ave. Intersection

LEGEND

- | | | | |
|---------------------------|--------------------------------------|---|--|
| Construction Zone | Energy Absorbing Terminal CZ, Type 2 | Temporary Pavement Markings, 6 In., Solid, White | Temporary Transverse Pavement Markings, 24 In., Solid, Stop Line |
| Temporary Pavement | Construction Sign | Temporary Pavement Markings, 6 In., Solid, Yellow | Temporary Transverse Pavement Markings, 24 In., Solid, White |
| Direction of Traffic | Traffic Drum | Temporary Pavement Markings, 6 In., Broken, White | Temporary Pavement Message Marking, Lane Indication Arrow |
| Temporary Traffic Barrier | | | |

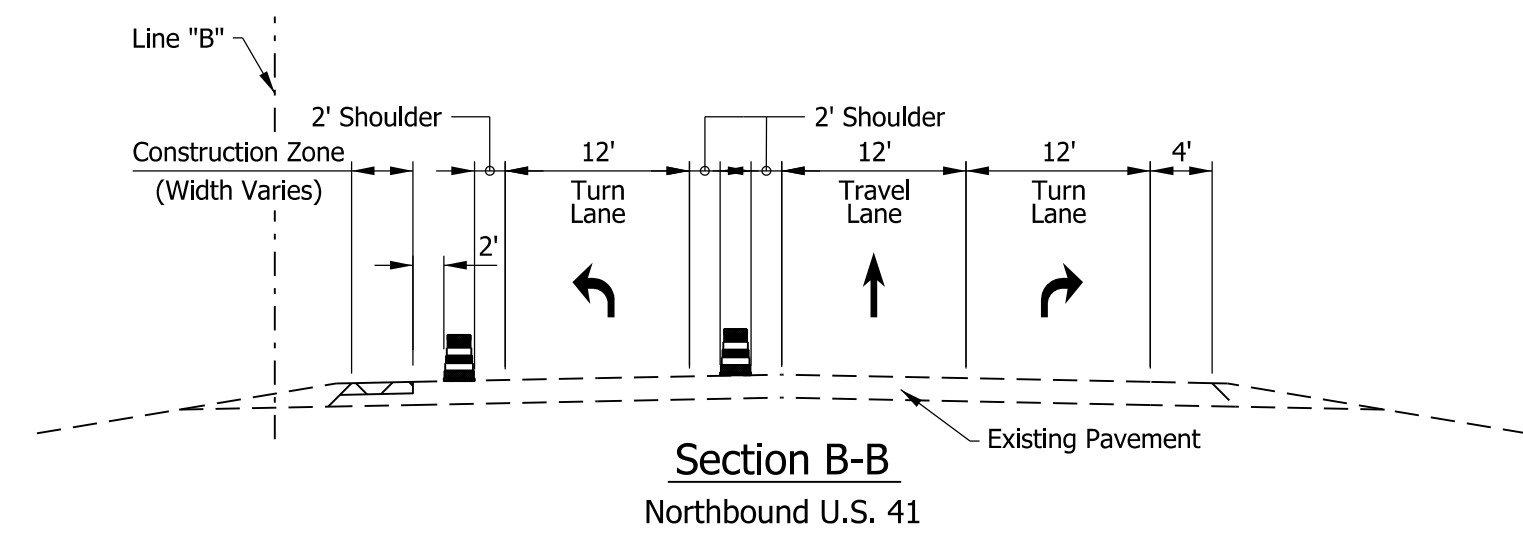
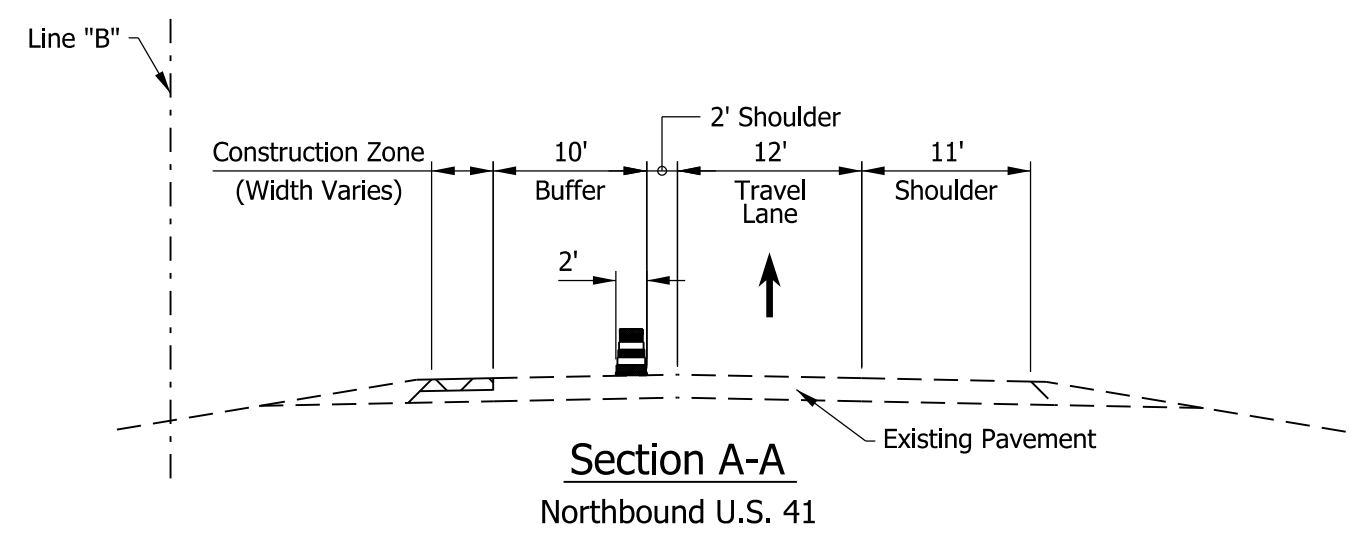
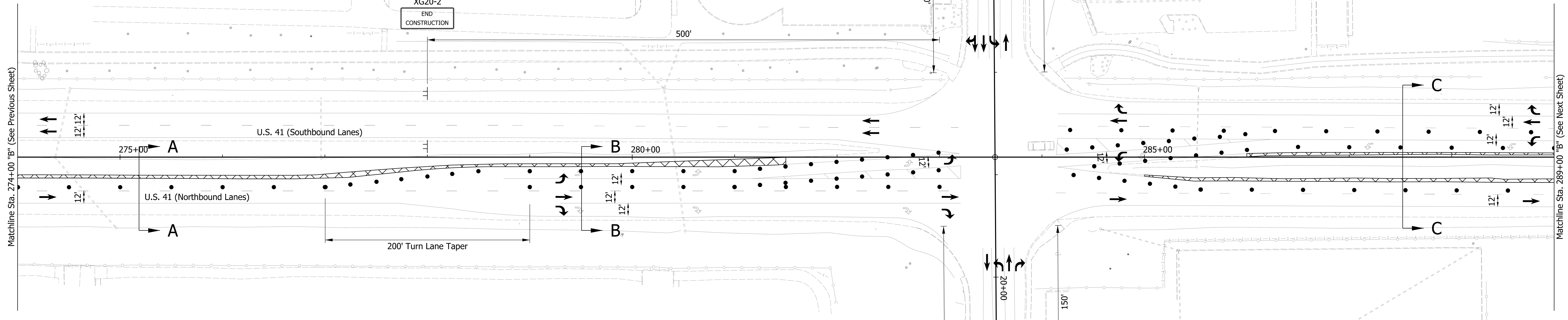
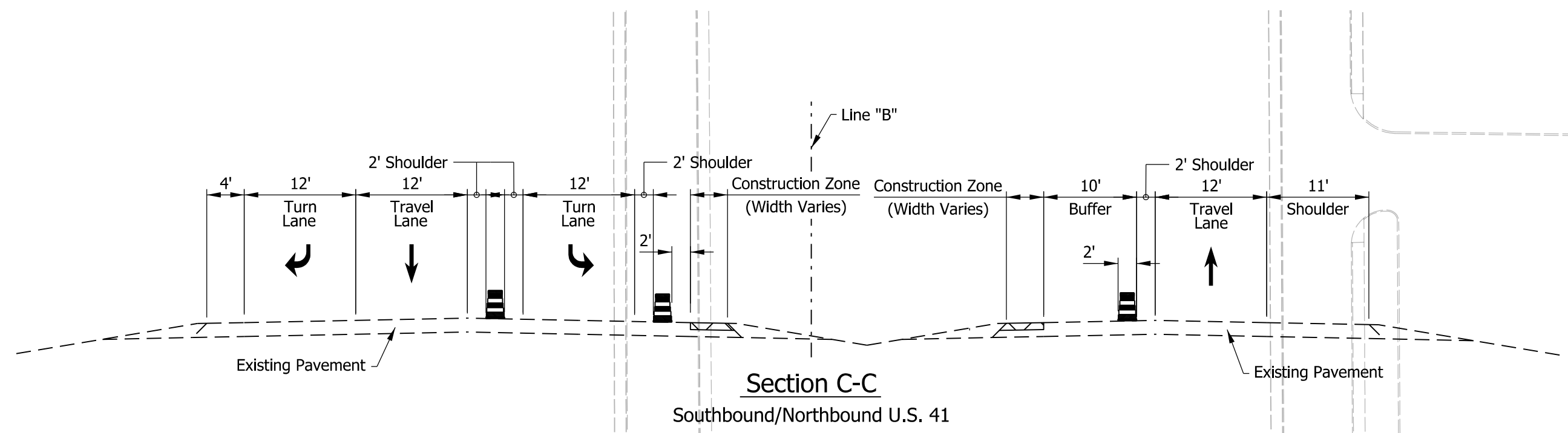
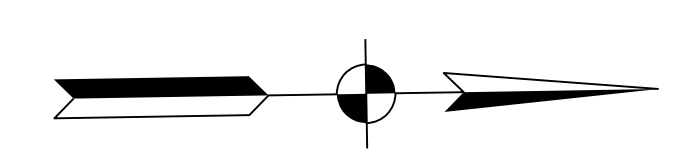
DRAFT
NOT FOR CONSTRUCTION

RECOMMENDED FOR APPROVAL _____	DESIGN ENGINEER _____	DATE _____
DESIGNED: _____ NSX _____	DRAWN: _____ HDW _____	
CHECKED: _____ TNE _____	CHECKED: _____ NSX _____	

INDIANA
DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC
PHASE 1

HORIZONTAL SCALE	BRIDGE FILE
1"=50'	N/A
VERTICAL SCALE	DESIGNATION
N/A	2000186
SURVEY BOOK	SHEETS MOT-01
ELECTRONIC	6 of 54
CONTRACT	PROJECT
B-42876	2000186



XW2-6
SPEEDING
MAX \$1000
RECKLESS DRIVING
MAX 6 YRS

XW20-1
ROAD
CONSTRUCTION
AHEAD

END
CONSTRUCTION
XG20-2

XG20-2
END
CONSTRUCTION

XW2-6
SPEEDING
MAX \$1000
RECKLESS DRIVING
MAX 6 YRS

END
CONSTRUCTION
XG20-2

Notes:

- Shoulder Strengthening to be completed by Single Lane Closure per SDD E 801-TCLC-11
- Contractor shall maintain a 1' Minimum Shoulder at all times
- All Sign Spacing shall be 350' Minimum unless otherwise noted
- Traffic Drum Spacing along Tapers shall be at 25'
- Traffic Drum Spacing along Tangents shall be at 50'
- All temporary Pavement Markings shall be Removable
- Existing Transverse Rumble Strips to remain in place
- No changes to the Traffic Signal at US 41 and Washington Ave. Intersection

LEGEND

- | | | | | | | | |
|--|---------------------------|--|--------------------------------------|--|---|--|--|
| | Construction Zone | | Energy Absorbing Terminal CZ, Type 2 | | 1 Temporary Pavement Markings, 6 In., Solid, White | | 5 Temporary Transverse Pavement Markings, 24 In., Solid, Stop Line |
| | Temporary Pavement | | Construction Sign | | 2 Temporary Pavement Markings, 6 In., Solid, Yellow | | 6 Temporary Transverse Pavement Markings, 24 In., Solid, White |
| | Direction of Traffic | | Traffic Drum | | 3 Temporary Pavement Markings, 6 In., Broken, White | | 7 Temporary Pavement Message Marking, Lane Indication Arrow |
| | Temporary Traffic Barrier | | | | | | |

DRAFT
NOT FOR CONSTRUCTION

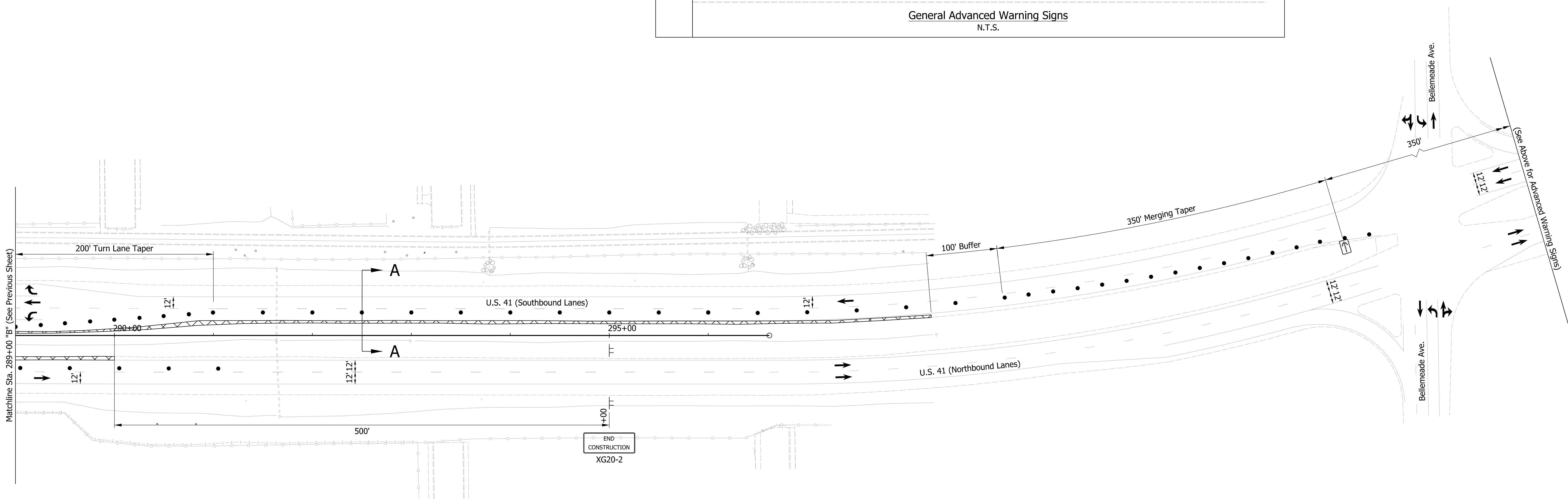
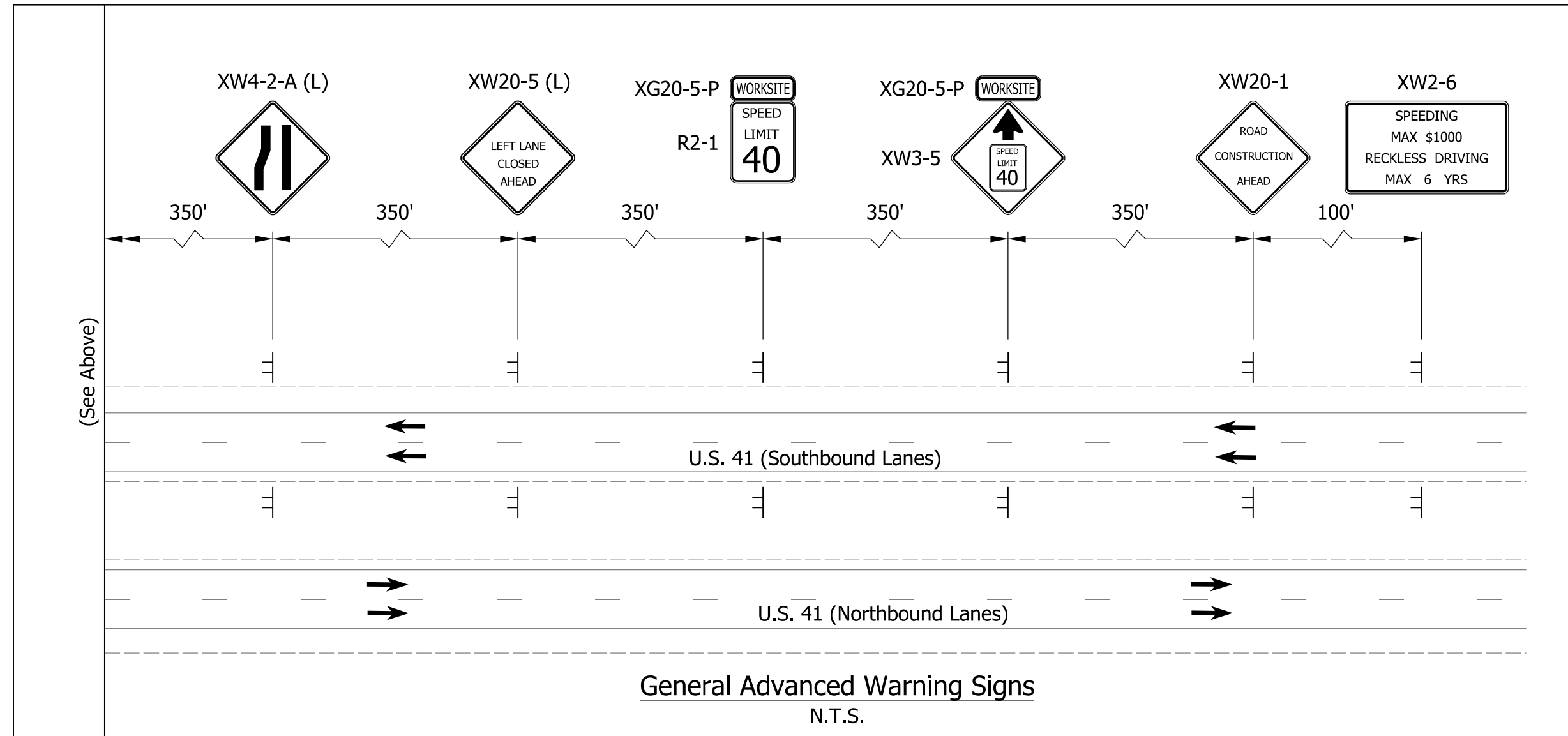
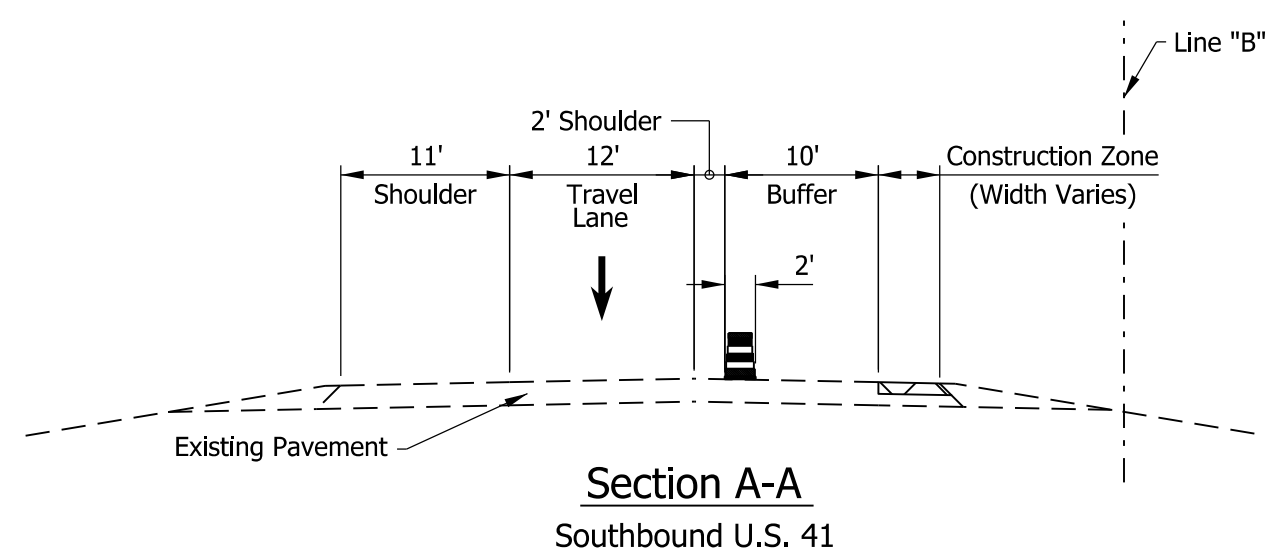
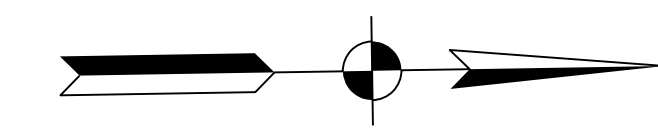
RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: NSX	DRAWN: HDW	
CHECKED: TNE	CHECKED: NSX	

INDIANA
DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC
PHASE 1

HORIZONTAL SCALE	BRIDGE FILE
1"=50'	N/A
VERTICAL SCALE	DESIGNATION
N/A	2000186
SURVEY BOOK	SHEETS MOT-02
ELECTRONIC	7 of 54
CONTRACT	PROJECT
B-42876	2000186

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- Notes:**
- Shoulder Strengthening to be completed by Single Lane Closure per SDD E 801-TCLC-11
 - Contractor shall maintain a 1' Minimum Shoulder at all times
 - All Sign Spacing shall be 350' Minimum unless otherwise noted
 - Traffic Drum Spacing along Tapers shall be at 25'
 - Traffic Drum Spacing along Tangents shall be at 50'
 - All temporary Pavement Markings shall be Removable
 - Existing Transverse Rumble Strips to remain in place
 - No changes to the Traffic Signal at US 41 and Washington Ave. Intersection

LEGEND	
	Construction Zone
	Temporary Pavement
	Direction of Traffic
	Temporary Traffic Barrier
	Energy Absorbing Terminal CZ, Type 2
	Construction Sign
	Traffic Drum
	1 Temporary Pavement Markings, 6 In., Solid, White
	2 Temporary Pavement Markings, 6 In., Solid, Yellow
	3 Temporary Pavement Markings, 6 In., Broken, White
	5 Temporary Transverse Pavement Markings, 24 In., Solid, Stop Line
	6 Temporary Transverse Pavement Markings, 24 In., Solid, White
	7 Temporary Pavement Message Marking, Lane Indication Arrow

DRAFT
NOT FOR CONSTRUCTION

RECOMMENDED FOR APPROVAL _____	DESIGN ENGINEER _____	DATE _____
DESIGNED: _____ NSX _____	DRAWN: _____ HDW _____	
CHECKED: _____ TNE _____	CHECKED: _____ NSX _____	

INDIANA
DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC
PHASE 1

HORIZONTAL SCALE	BRIDGE FILE
1"=50'	N/A
VERTICAL SCALE	DESIGNATION
N/A	2000186
SURVEY BOOK	SHEETS MOT-03
ELECTRONIC	8 of 54
CONTRACT	PROJECT
B-42876	2000186

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CONSTRUCTION SEQUENCING

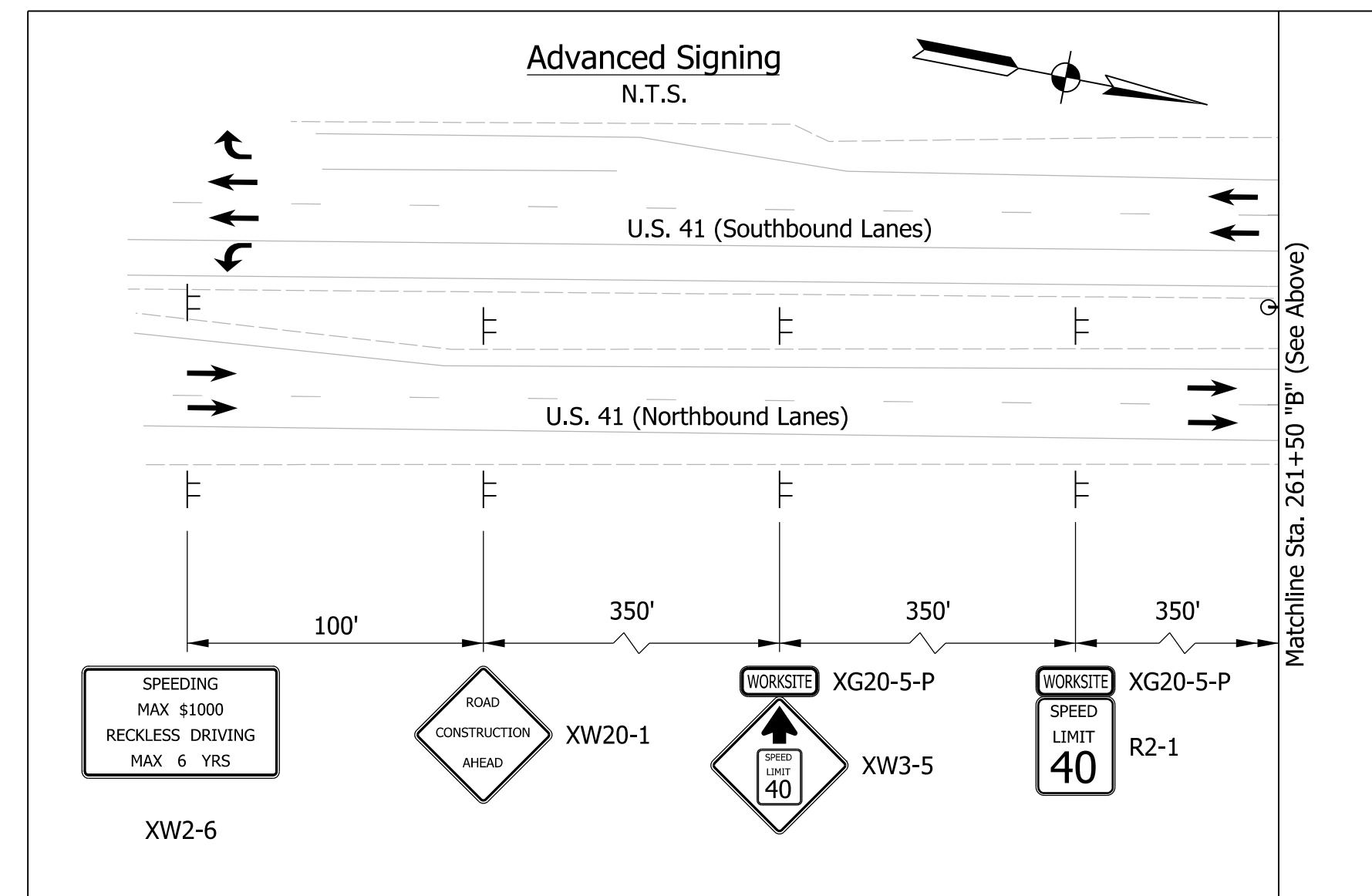
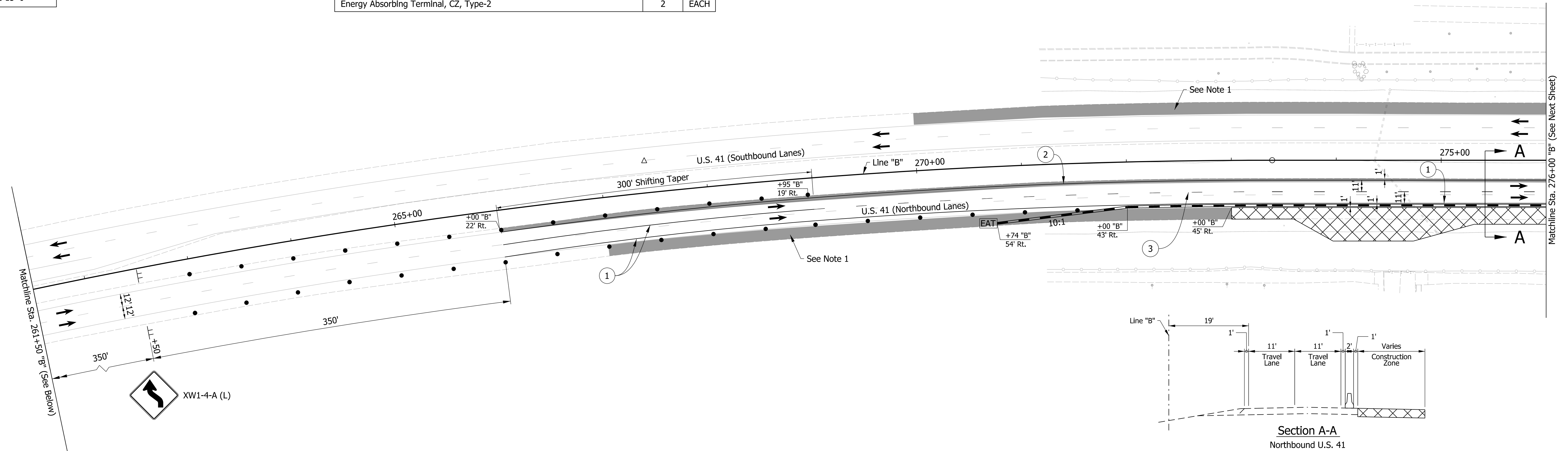
PHASE 2:

1. Install Advanced Warning Signs.
2. Remove existing Pavement Markings along US 41.
3. Install temporary Pavement Markings and Traffic Drums.
4. Place temporary Traffic Barrier adjacent to US 41 workzones.
5. Construct the Northbound and Southbound along US 41 Outside Shoulder Loops.

Construction Design Speed: 40 MPH

Construction Clear Zone: 13'-0"

Maintenance of Traffic Quantity Summary - Phase 2		
Item	Amount	Unit
Construction Sign A	18	EACH
Construction Sign B	4	EACH
Temporary Worksite Speed Limit Sign Assembly	8	EACH
Temporary Pavement Marking, Removable, 6 In.	10,518	LFT
Temporary Transverse Pavement Marking, Removable, Lane Indication Arrow	4	EACH
Temporary Transverse Pavement Marking, Removable, 24 In.	100	LFT
Temporary Traffic Barrier, Type 2	1,614	LFT
Energy Absorbing Terminal, CZ, Type-2	2	EACH



Notes:

1. Shoulder Strengthening to be completed by Single Lane Closure per SDD E 801-TCLC-11
2. Screened Signs were placed in Previous Construction Phase
3. Contractor shall maintain a 1' Minimum Shoulder at all times
4. All Sign Spacing shall be 350' Minimum unless otherwise noted
5. Traffic Drum Spacing along Tapers shall be at 25'
6. Traffic Drum Spacing along Tangents shall be at 50'
7. All temporary Pavement Markings shall be Removable
8. Existing Transverse Rumble Strips to remain in place
9. No changes to the Traffic Signal at US 41 and Washington Ave. Intersection

LEGEND

- | | | | |
|---------------------------|--------------------------------------|---|--|
| Construction Zone | Energy Absorbing Terminal CZ, Type 2 | Temporary Pavement Markings, 6 In., Solid, White | Temporary Transverse Pavement Markings, 24 In., Solid, Stop Line |
| Temporary Pavement | Construction Sign | Temporary Pavement Markings, 6 In., Solid, Yellow | Temporary Transverse Pavement Markings, 24 In., Solid, White |
| Direction of Traffic | Traffic Drum | Temporary Pavement Markings, 6 In., Broken, White | Temporary Pavement Message Marking, Lane Indication Arrow |
| Temporary Traffic Barrier | | | |

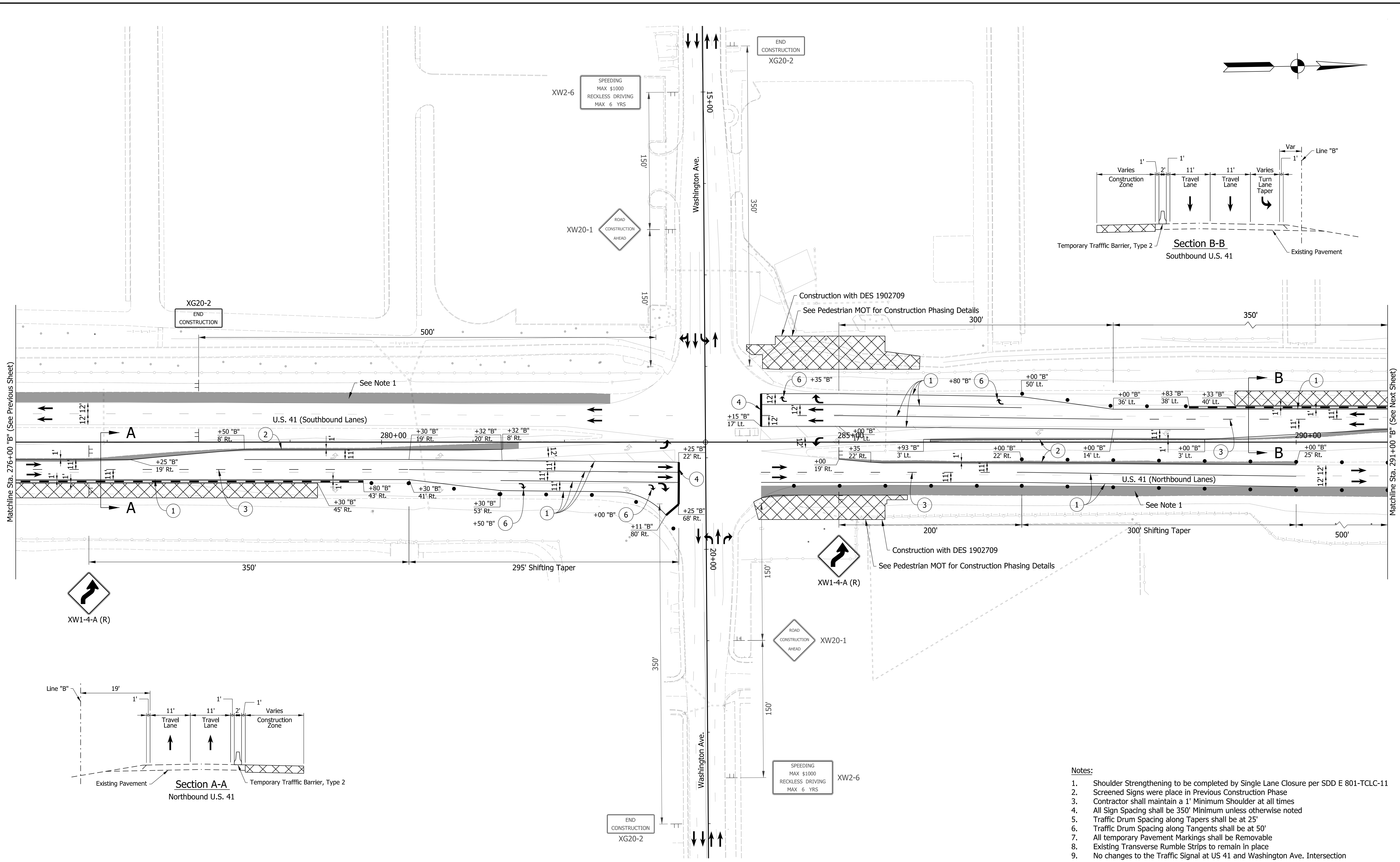
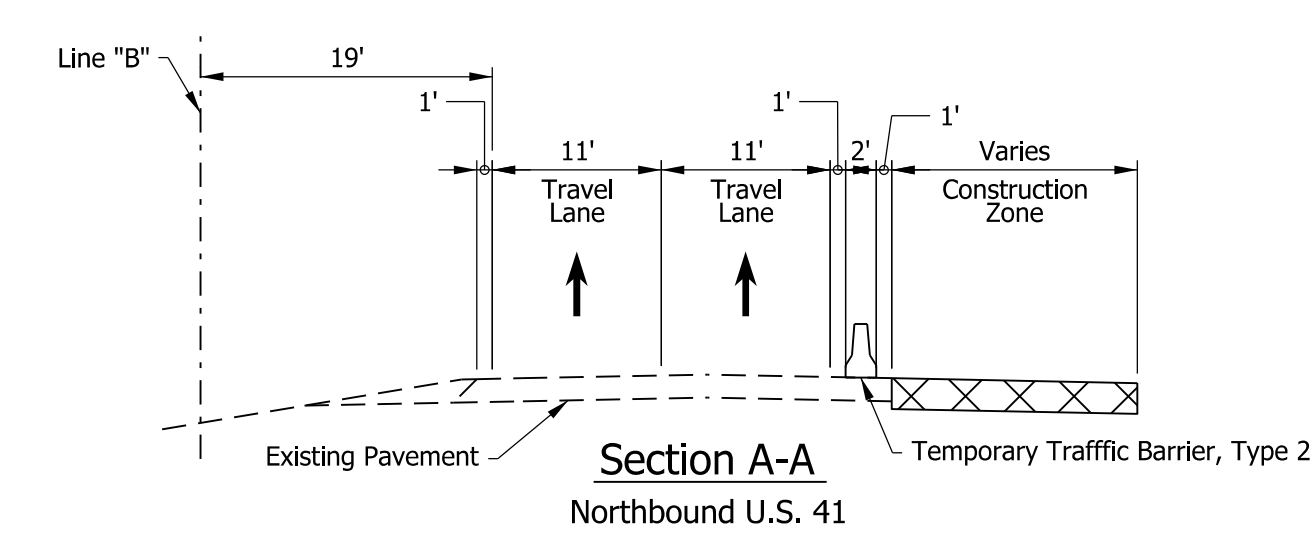
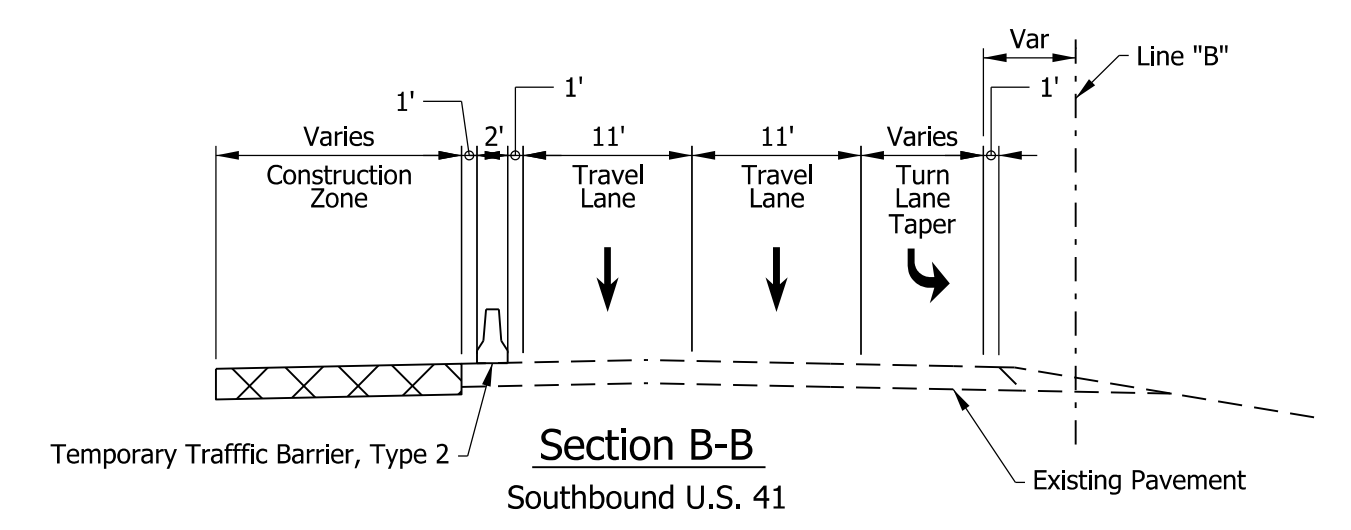
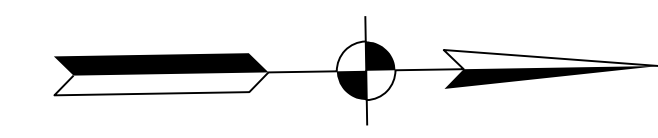
DRAFT
NOT FOR CONSTRUCTION

RECOMMENDED FOR APPROVAL _____	DESIGN ENGINEER _____	DATE _____
DESIGNED: _____ NSX _____	DRAWN: _____ HDW _____	
CHECKED: _____ TNE _____	CHECKED: _____ NSX _____	

INDIANA
DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC
PHASE 2

HORIZONTAL SCALE	BRIDGE FILE
1"=50'	N/A
VERTICAL SCALE	DESIGNATION
N/A	2000186
SURVEY BOOK	SHEETS MOT-04
ELECTRONIC	9 of 54
CONTRACT	PROJECT
B-42876	2000186



- Notes:**
- Shoulder Strengthening to be completed by Single Lane Closure per SDD E 801-TCLC-11
 - Screened Signs were placed in Previous Construction Phase
 - Contractor shall maintain a 1' Minimum Shoulder at all times
 - All Sign Spacing shall be 350' Minimum unless otherwise noted
 - Traffic Drum Spacing along Tapers shall be at 25'
 - Traffic Drum Spacing along Tangents shall be at 50'
 - All temporary Pavement Markings shall be Removable
 - Existing Transverse Rumble Strips to remain in place
 - No changes to the Traffic Signal at US 41 and Washington Ave. Intersection

LEGEND	
	Construction Zone
	Temporary Pavement
	Direction of Traffic
	Temporary Traffic Barrier
	Energy Absorbing Terminal CZ, Type 2
	Construction Sign
	Traffic Drum
	Temporary Pavement Markings, 6 In., Solid, White
	Temporary Pavement Markings, 6 In., Solid, Yellow
	Temporary Pavement Markings, 6 In., Broken, White
	Temporary Transverse Pavement Markings, 24 In., Solid, Stop Line
	Temporary Transverse Pavement Markings, 12 In., Solid, Yellow
	Temporary Pavement Message Marking, Lane Indication Arrow

DRAFT
NOT FOR CONSTRUCTION

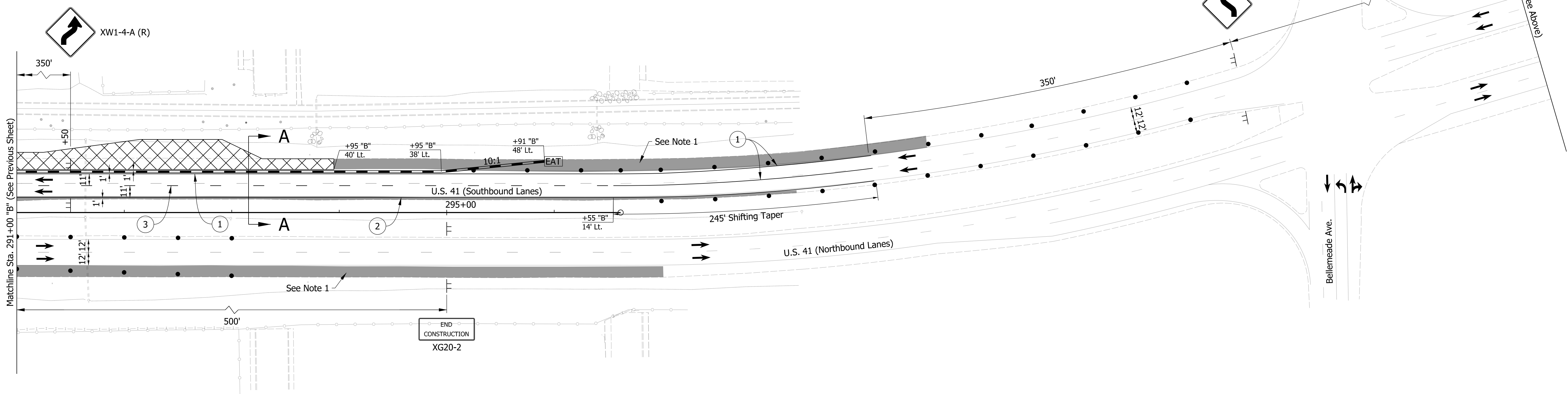
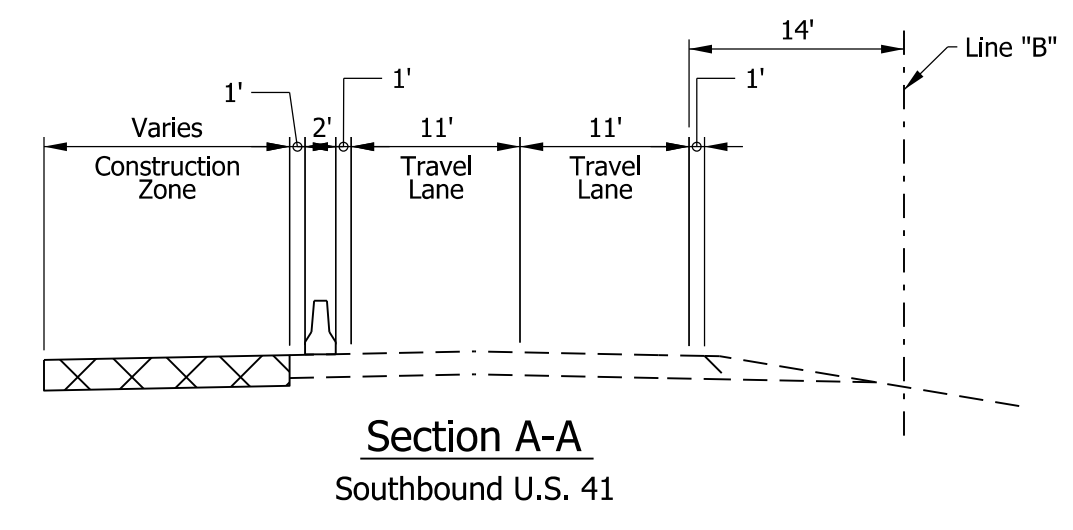
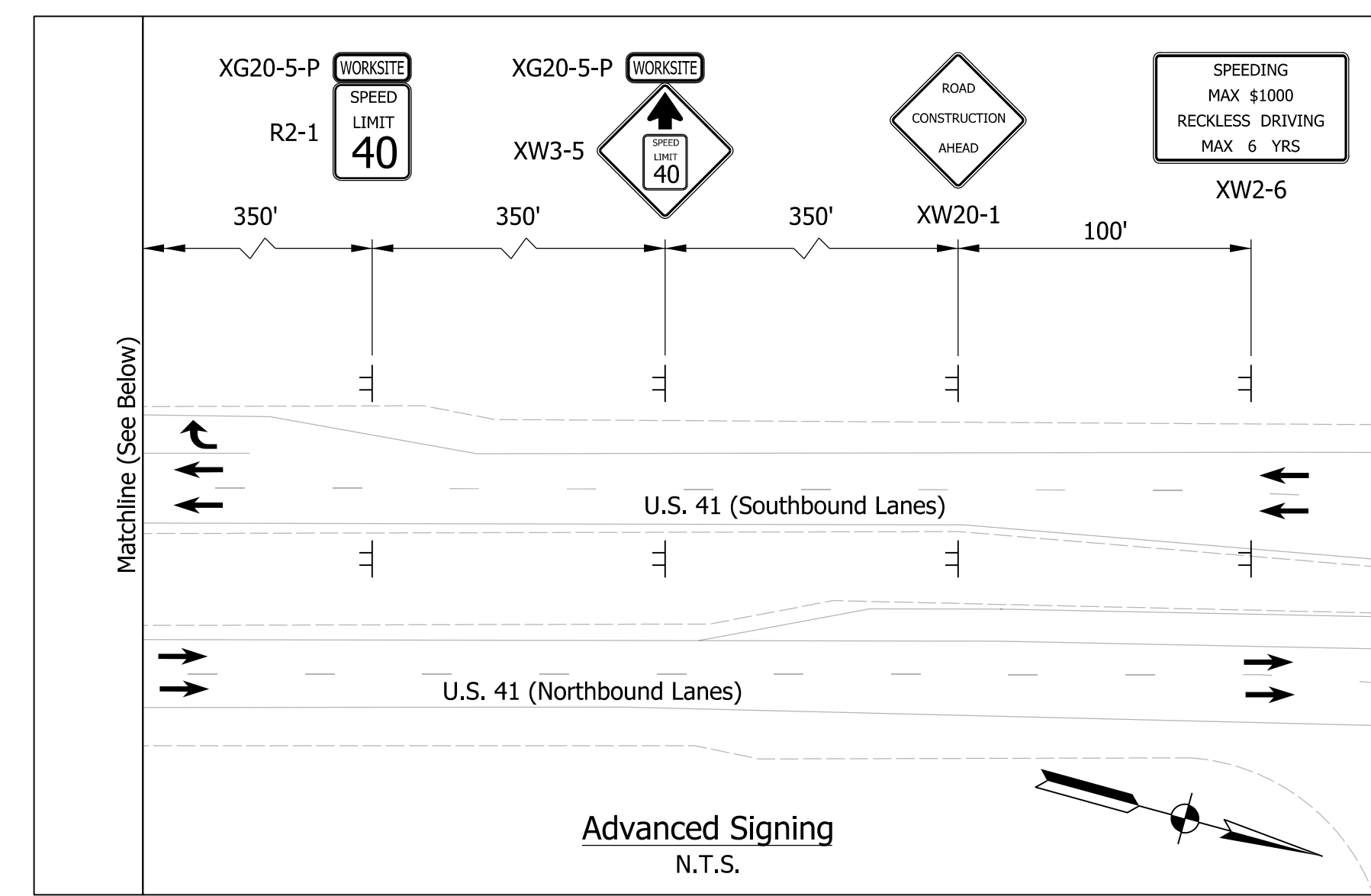
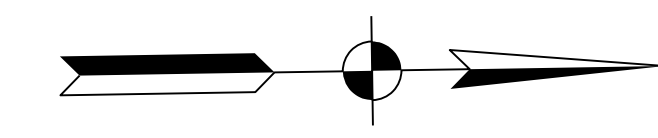
RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: NSX	DRAWN: HDW	
CHECKED: TNE	CHECKED: NSX	

INDIANA
DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC
PHASE 2

HORIZONTAL SCALE	BRIDGE FILE
1"=50'	N/A
VERTICAL SCALE	DESIGNATION
N/A	2000186
SURVEY BOOK	SHEETS
ELECTRONIC	10 of 54
CONTRACT	PROJECT
B-42876	2000186

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- Notes:**
- Shoulder Strengthening to be completed by Single Lane Closure per SDD E 801-TCLC-11
 - Screened Signs were placed in Previous Construction Phase
 - Contractor shall maintain a 1' Minimum Shoulder at all times
 - All Sign Spacing shall be 350' Minimum unless otherwise noted
 - Traffic Drum Spacing along Tapers shall be at 25'
 - Traffic Drum Spacing along Tangents shall be at 50'
 - All temporary Pavement Markings shall be Removable
 - Existing Transverse Rumble Strips to remain in place
 - No changes to the Traffic Signal at US 41 and Washington Ave. Intersection

LEGEND	
	Construction Zone
	Temporary Pavement
	Direction of Traffic
	Temporary Traffic Barrier
	Energy Absorbing Terminal CZ, Type 2
	Construction Sign
	Traffic Drum
	Temporary Pavement Markings, 6 In., Solid, White
	Temporary Pavement Markings, 6 In., Solid, Yellow
	Temporary Pavement Markings, 6 In., Broken, White
	Temporary Transverse Pavement Markings, 24 In., Solid, Stop Line
	Temporary Transverse Pavement Markings, 24 In., Solid, White
	Temporary Pavement Message Marking, Lane Indication Arrow

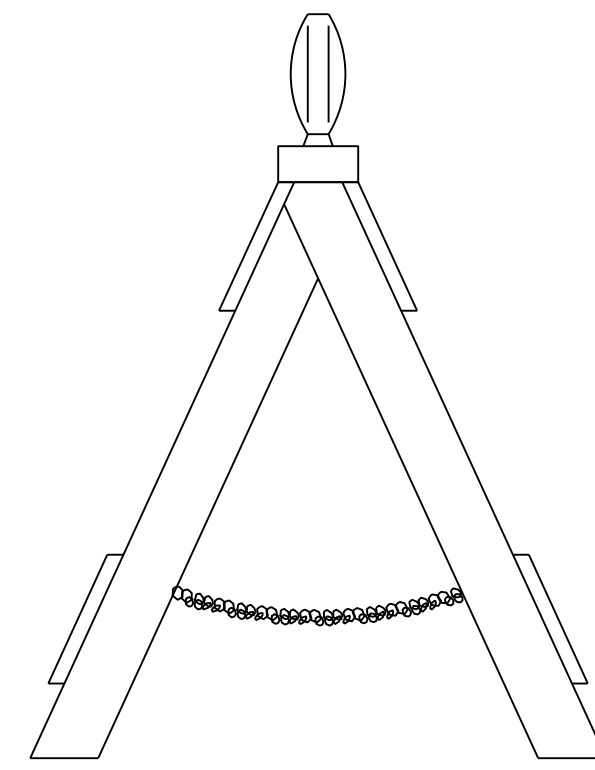
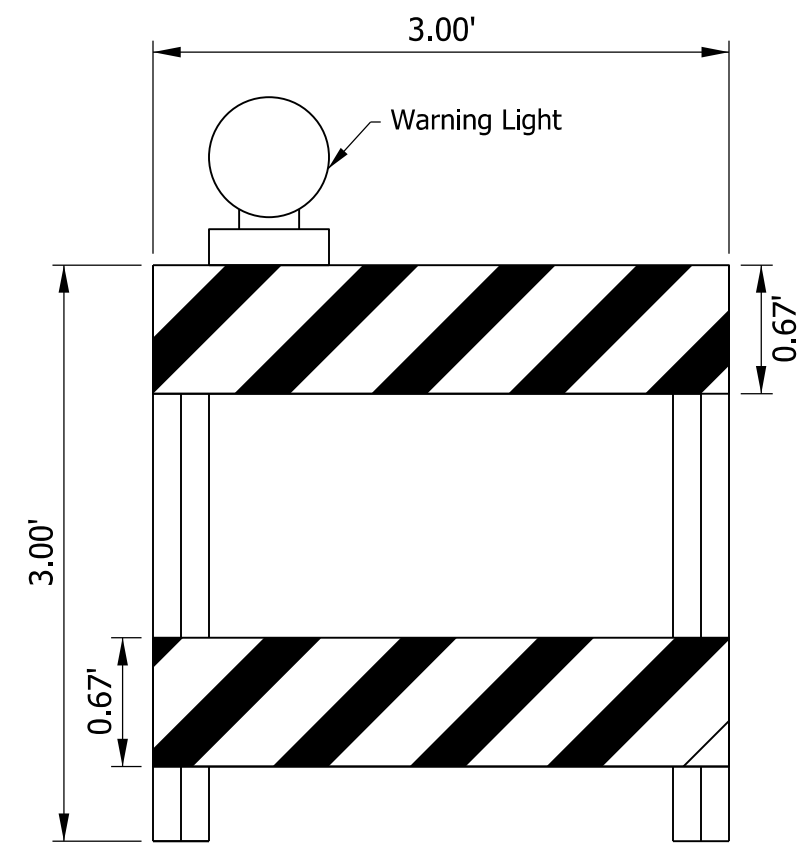
DRAFT
NOT FOR CONSTRUCTION

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: NSX	DRAWN: HDW	
CHECKED: TNE	CHECKED: NSX	

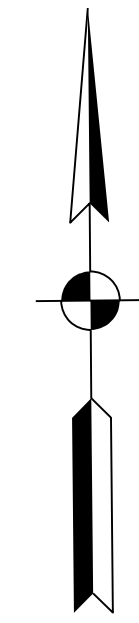
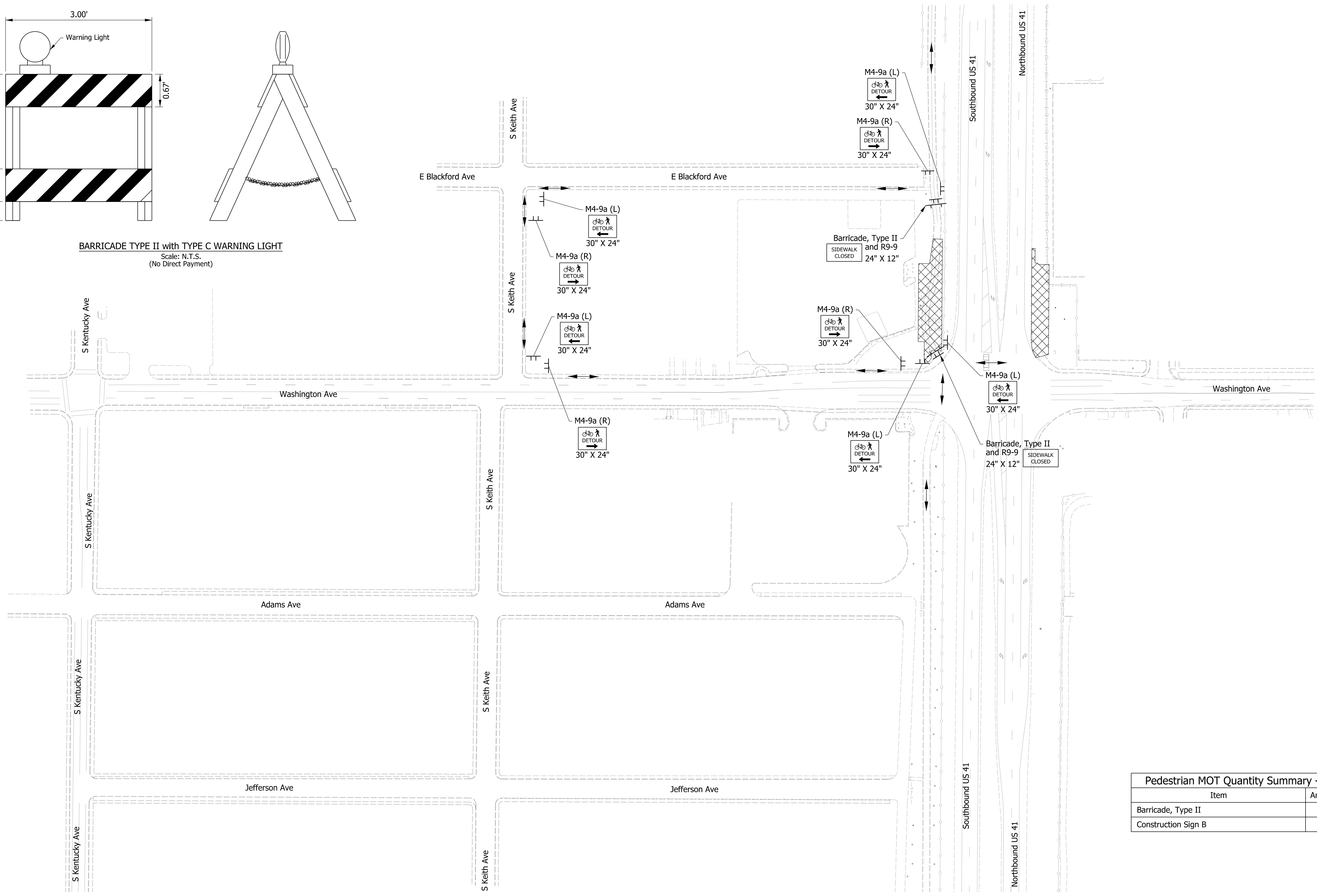
INDIANA DEPARTMENT OF TRANSPORTATION
MAINTENANCE OF TRAFFIC PHASE 2

HORIZONTAL SCALE	BRIDGE FILE
1"=50'	N/A
VERTICAL SCALE	DESIGNATION
N/A	2000186
SURVEY BOOK	SHEETS MOT-06
ELECTRONIC	11 of 54
CONTRACT	PROJECT
B-42876	2000186

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BARRICADE TYPE II with TYPE C WARNING LIGHT
Scale: N.T.S.
(No Direct Payment)



Item	Amount	Unit
Barricade, Type II	2	EACH
Construction Sign B	11	EACH

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	Construction Sign, B		Direction of Pedestrian Travel
	Barricade, Type II with Type C Warning Light		Construction Area
	Drum		

DRAFT
 NOT FOR CONSTRUCTION

RECOMMENDED FOR APPROVAL _____	DESIGN ENGINEER _____	DATE _____
DESIGNED: _____ HDW	DRAWN: _____ HDW	
CHECKED: _____ TNE	CHECKED: _____ AJP	

INDIANA DEPARTMENT OF TRANSPORTATION
PEDESTRIAN MAINTENANCE OF TRAFFIC PHASE 2

HORIZONTAL SCALE	BRIDGE FILE
CUSTOM 75	N/A
VERTICAL SCALE	DESIGNATION
N/A	2000186
SURVEY BOOK	SHEETS MOT-07
ELECTRONIC	12 of 56
CONTRACT	PROJECT
B-42876	2000186

CONSTRUCTION SEQUENCING

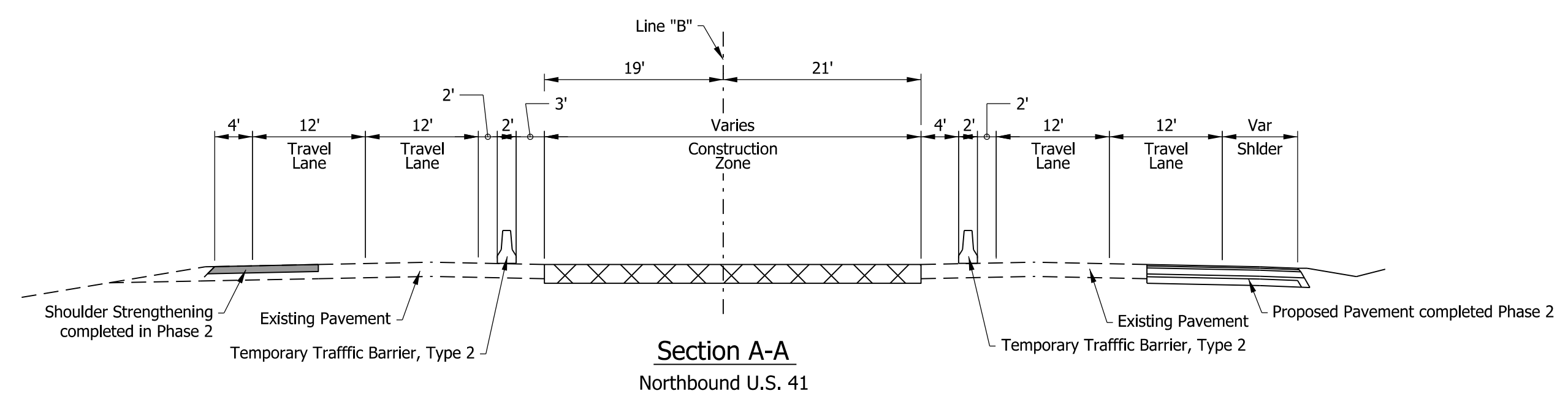
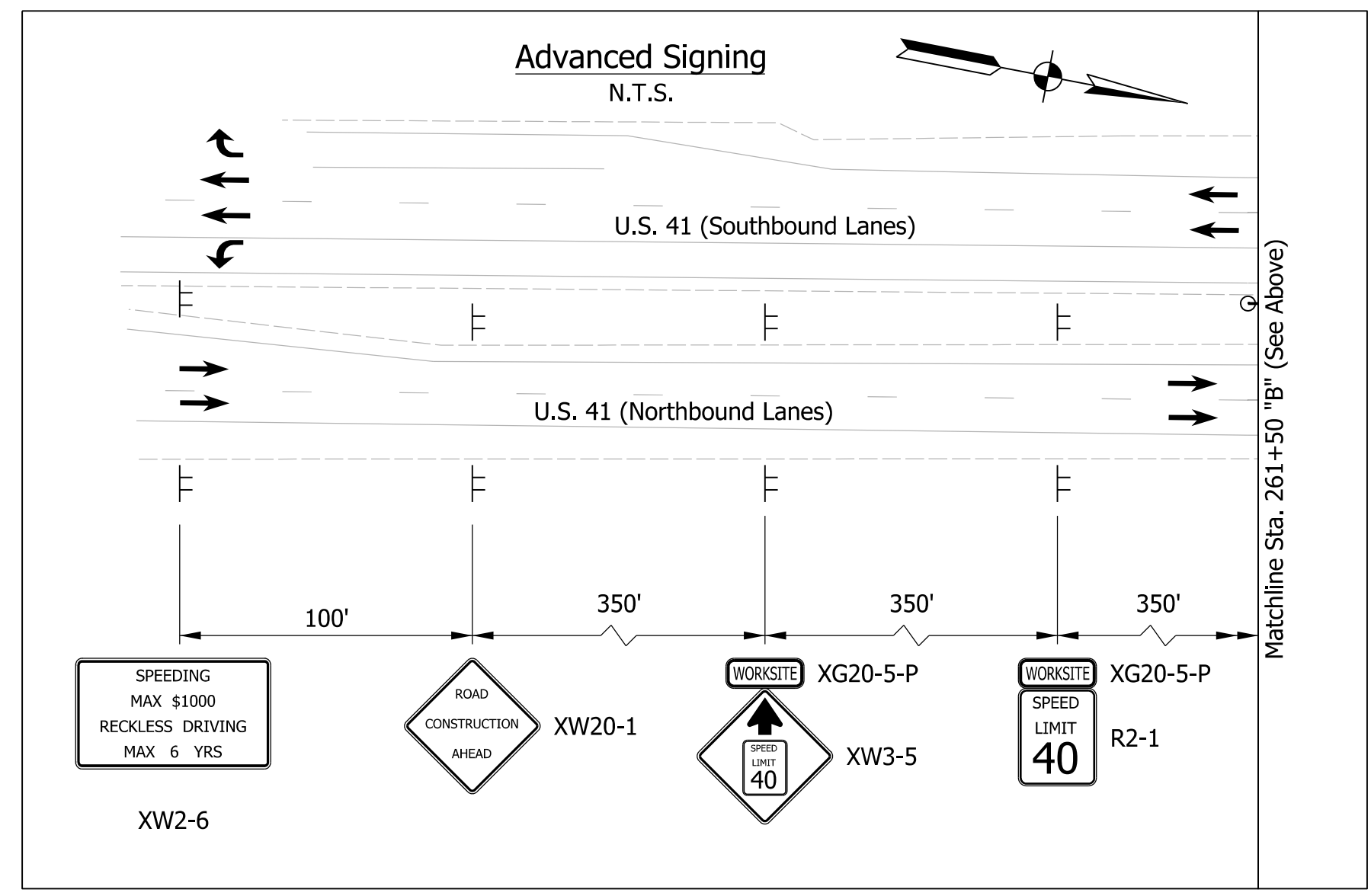
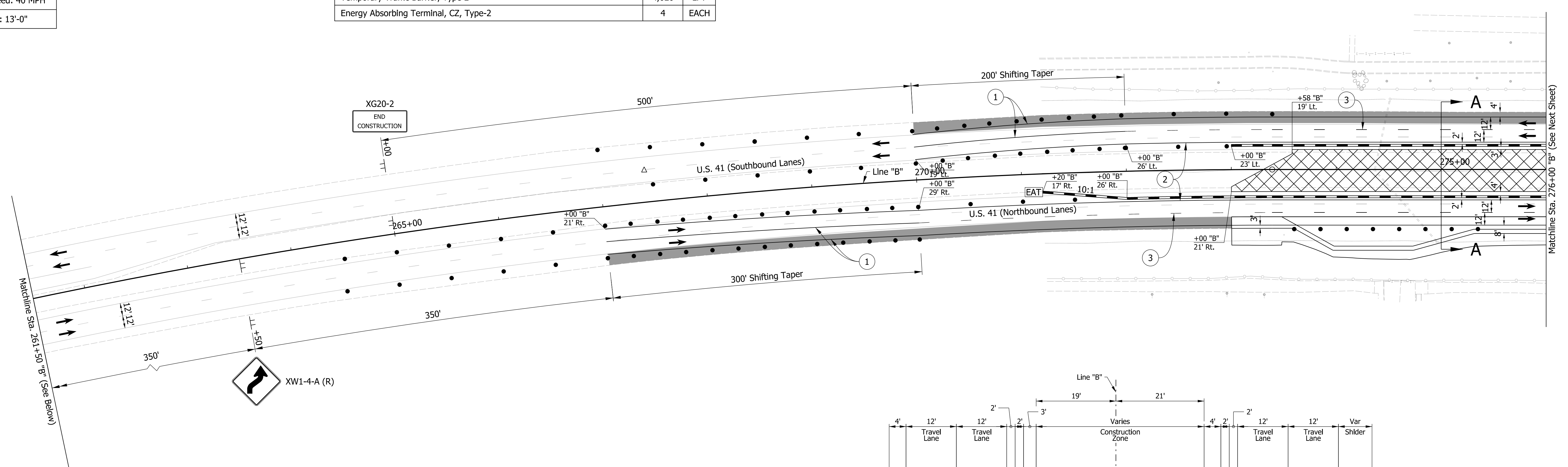
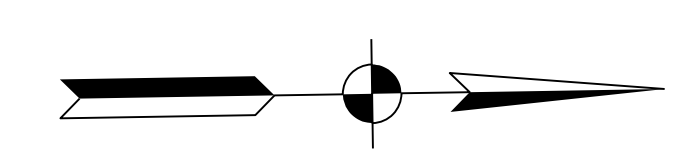
PHASE 3A:

1. Install Advanced Warning Signs.
2. Remove existing Pavement Markings and temporary Pavement Markings from Phase 2 along US 41.
3. Install temporary Pavement Markings and Traffic Drums.
4. Place temporary Traffic Barrier adjacent to US 41 workzones.
5. Construct the Northbound and Southbound Median U-Turn Lanes and Median Widening along US 41.

Construction Design Speed: 40 MPH

Construction Clear Zone: 13'-0"

Maintenance of Traffic Quantity Summary - Phase 3A		
Item	Amount	Unit
Construction Sign A	18	EACH
Construction Sign B	6	EACH
Temporary Worksite Speed Limit Sign Assembly	8	EACH
Temporary Pavement Marking, Removable, 6 In.	15,285	LFT
Temporary Transverse Pavement Marking, Removable, Lane Indication Arrow	2	EACH
Temporary Transverse Pavement Marking, Removable, 24 In.	113	LFT
Temporary Traffic Barrier, Type 2	4,020	LFT
Energy Absorbing Terminal, CZ, Type-2	4	EACH



- Notes:**
1. Contractor shall maintain a 2' Minimum Shoulder at all times
 2. All Sign Spacing shall be 350' Minimum unless otherwise noted
 3. Traffic Drum Spacing along Tapers shall be at 25'
 4. Traffic Drum Spacing along Tangents shall be at 50'
 5. All temporary Pavement Markings shall be Removable
 6. Existing Transverse Rumble Strips to remain in place
 7. No changes to the Traffic Signal at US 41 and Washington Ave. Intersection

LEGEND

- | | | | |
|---------------------------|--------------------------------------|---|--|
| Construction Zone | Energy Absorbing Terminal CZ, Type 2 | Temporary Pavement Markings, 6 In., Solid, White | Temporary Transverse Pavement Markings, 24 In., Solid, Stop Line |
| Temporary Pavement | Construction Sign | Temporary Pavement Markings, 6 In., Solid, Yellow | Temporary Transverse Pavement Markings, 24 In., Solid, White |
| Direction of Traffic | Traffic Drum | Temporary Pavement Markings, 6 In., Broken, White | Temporary Pavement Message Marking, Lane Indication Arrow |
| Temporary Traffic Barrier | | | |

DRAFT
NOT FOR CONSTRUCTION

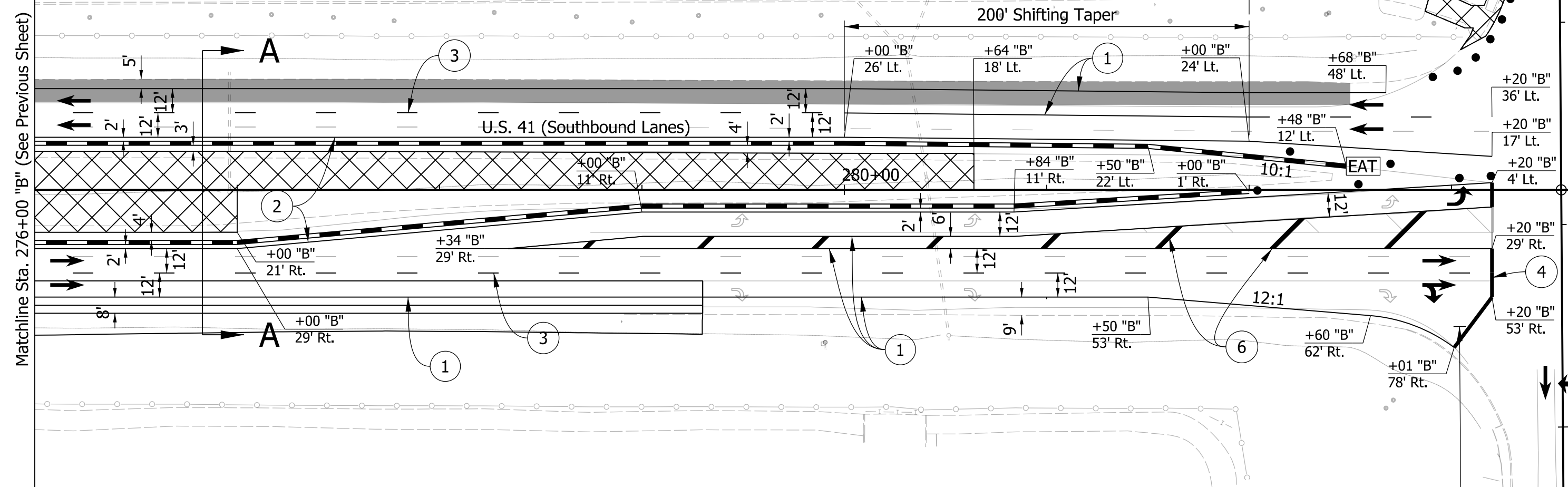
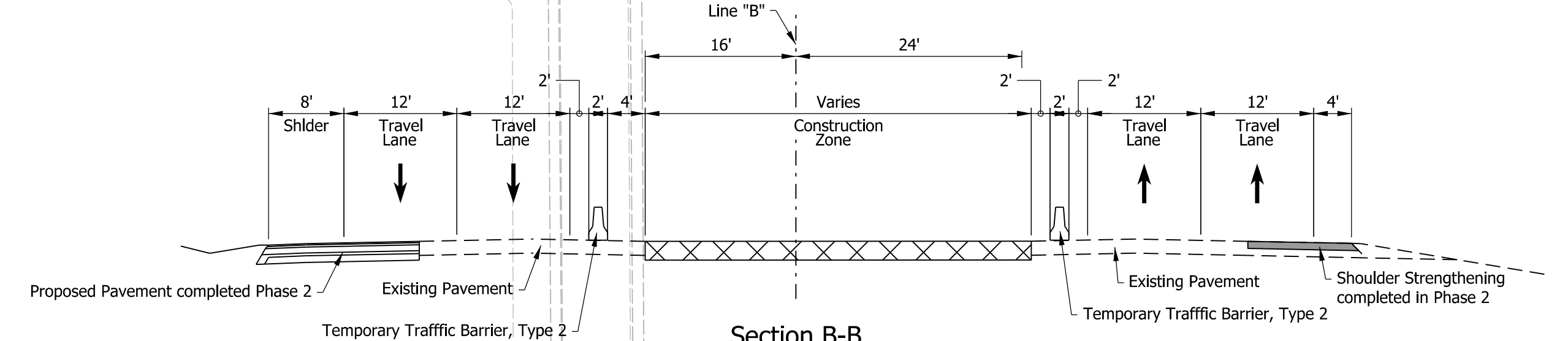
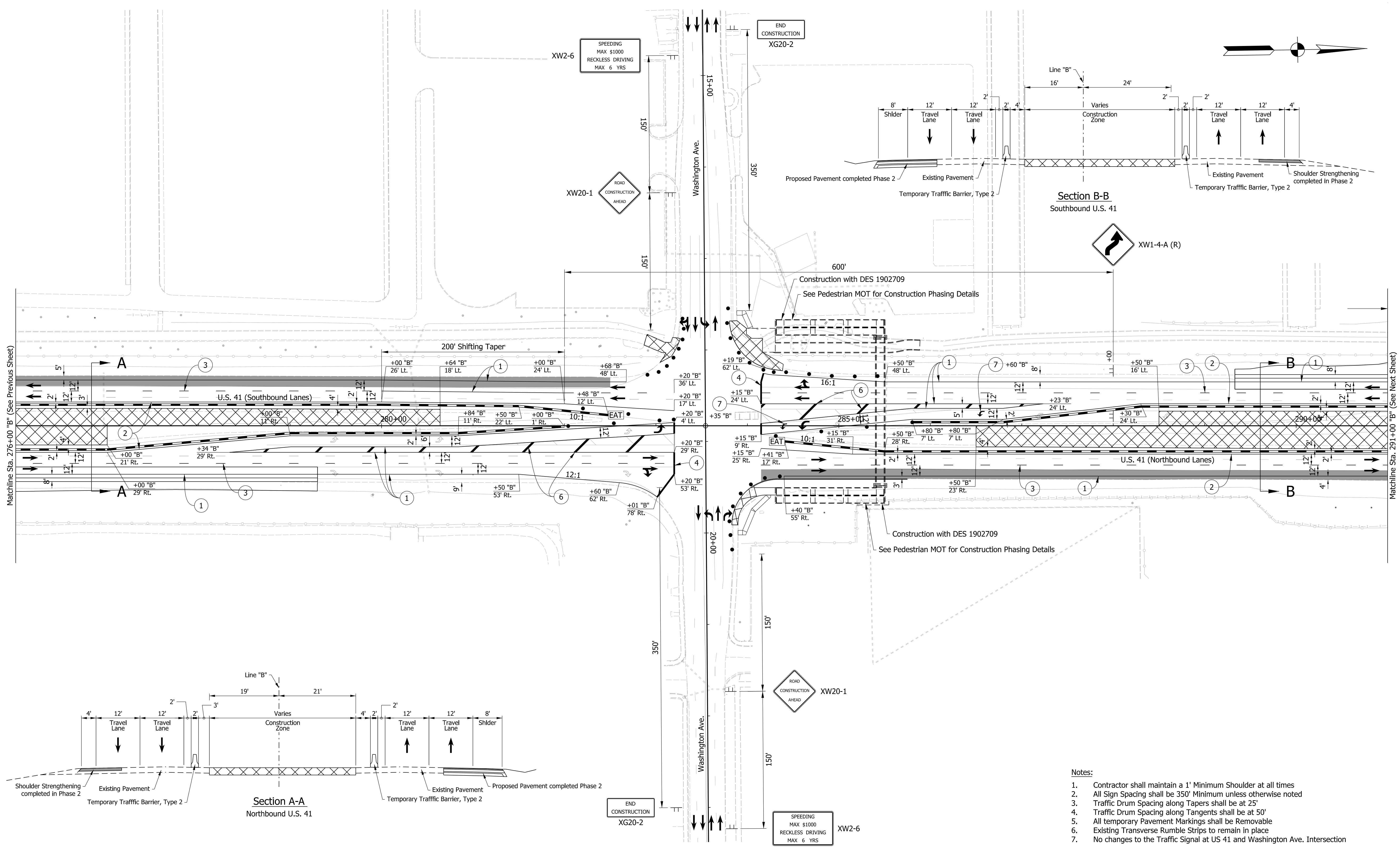
RECOMMENDED FOR APPROVAL _____	DESIGN ENGINEER _____	DATE _____
DESIGNED: _____ NSX _____	DRAWN: _____ HDW _____	
CHECKED: _____ TNE _____	CHECKED: _____ NSX _____	

INDIANA DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC PHASE 3A

HORIZONTAL SCALE	BRIDGE FILE
1"=50'	N/A
VERTICAL SCALE	DESIGNATION
N/A	2000186
SURVEY BOOK	SHEETS MOT-07
ELECTRONIC	12 of 54
CONTRACT	PROJECT
B-42876	2000186

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- Notes:**
- Contractor shall maintain a 1' Minimum Shoulder at all times
 - All Sign Spacing shall be 350' Minimum unless otherwise noted
 - Traffic Drum Spacing along Tapers shall be at 25'
 - Traffic Drum Spacing along Tangents shall be at 50'
 - All temporary Pavement Markings shall be Removable
 - Existing Transverse Rumble Strips to remain in place
 - No changes to the Traffic Signal at US 41 and Washington Ave. Intersection

LEGEND			
	Construction Zone		Energy Absorbing Terminal CZ, Type 2
	Temporary Pavement		Construction Sign
	Direction of Traffic		Traffic Drum
	Temporary Traffic Barrier		Temporary Pavement Markings, 6 In., Solid, White
			Temporary Pavement Markings, 6 In., Solid, Yellow
			Temporary Pavement Markings, 6 In., Broken, White
			Temporary Transverse Pavement Markings, 24 In., Solid, Stop Line
			Temporary Transverse Pavement Markings, 24 In., Solid, White
			Temporary Pavement Message Marking, Lane Indication Arrow

DRAFT
NOT FOR CONSTRUCTION

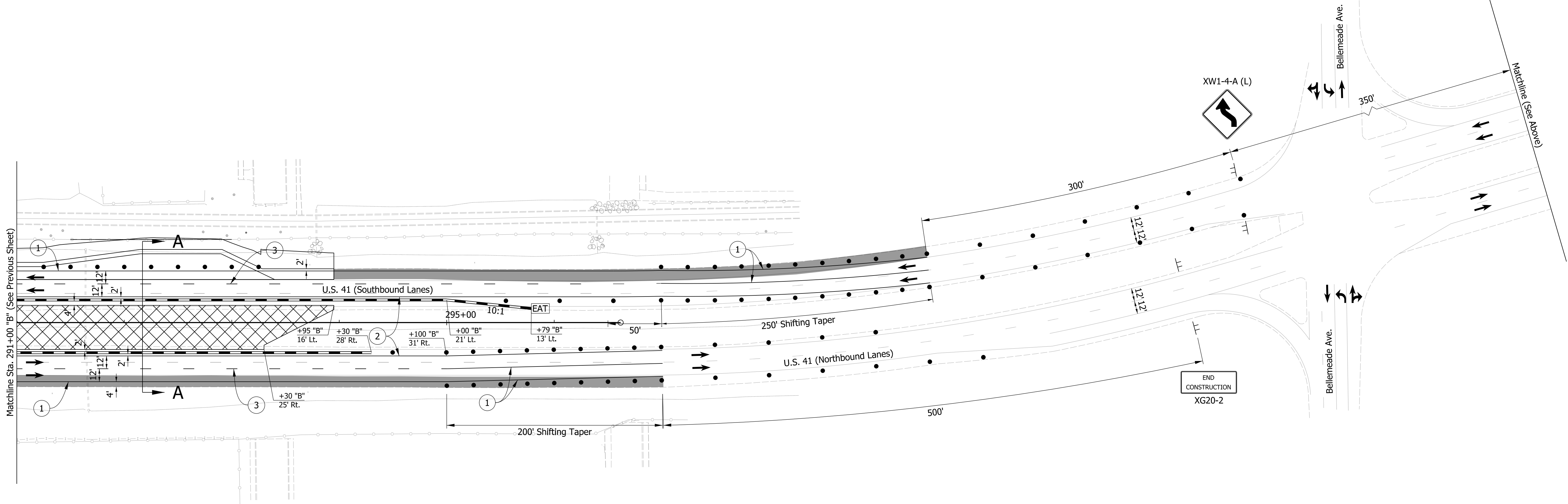
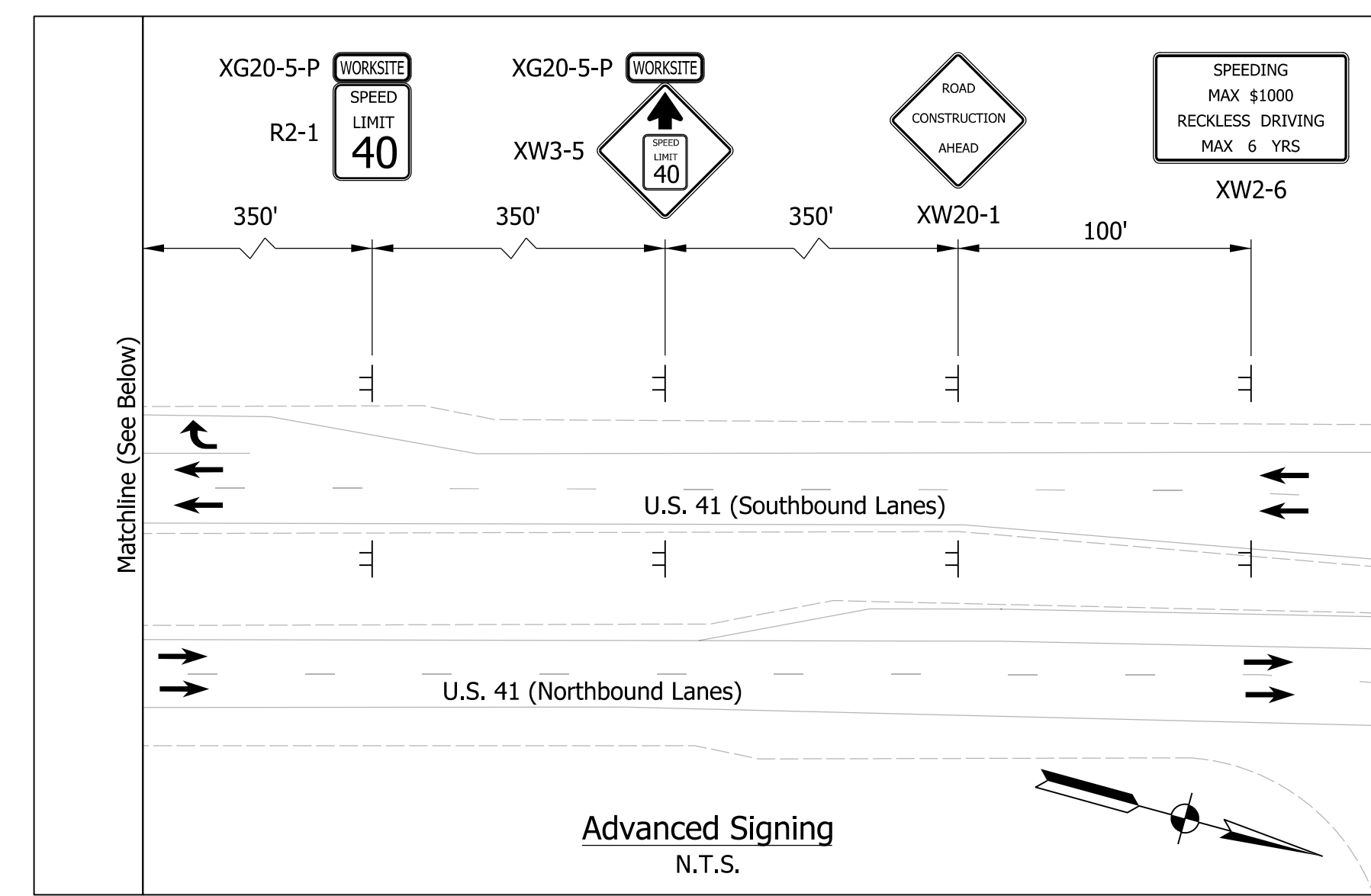
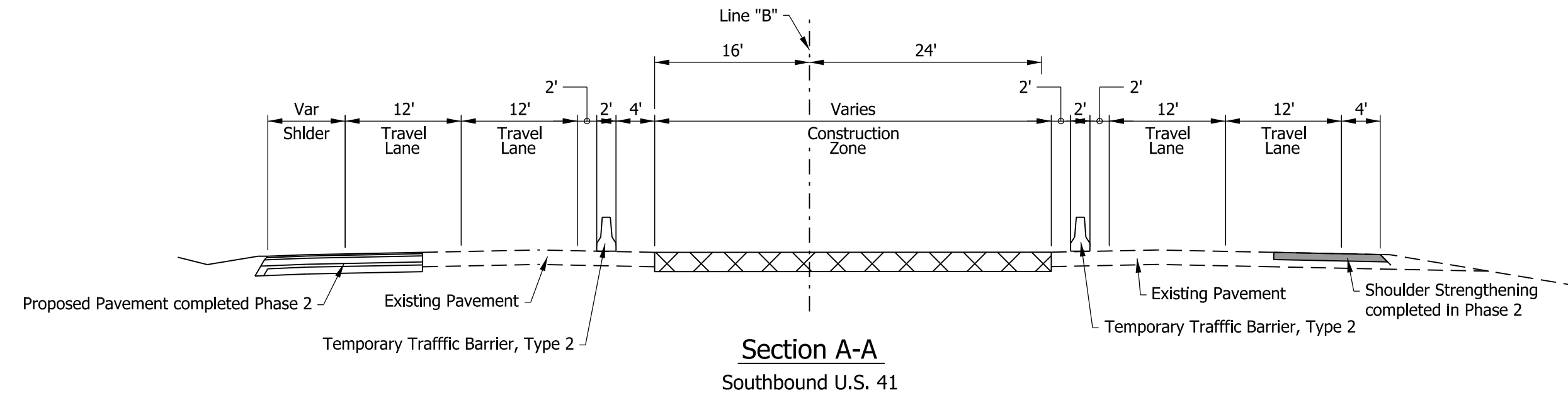
RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: NSX	DRAWN: HDW	
CHECKED: TNE	CHECKED: NSX	

INDIANA
DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC
PHASE 3A

HORIZONTAL SCALE	BRIDGE FILE
1"=50'	N/A
VERTICAL SCALE	DESIGNATION
N/A	2000186
SURVEY BOOK	SHEETS MOT-08
ELECTRONIC	13 of 54
CONTRACT	PROJECT
B-42876	2000186

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- Notes:**
- Shoulder Strengthening to be completed by Single Lane Closure per SDD E 801-TCLC-11
 - Contractor shall maintain a 1' Minimum Shoulder at all times
 - All Sign Spacing shall be 350' Minimum unless otherwise noted
 - Traffic Drum Spacing along Tapers shall be at 25'
 - Traffic Drum Spacing along Tangents shall be at 50'
 - All temporary Pavement Markings shall be Removable
 - Existing Transverse Rumble Strips to remain in place
 - No changes to the Traffic Signal at US 41 and Washington Ave. Intersection

LEGEND	
	Construction Zone
	Temporary Pavement
	Direction of Traffic
	Temporary Traffic Barrier
	Energy Absorbing Terminal CZ, Type 2
	Construction Sign
	Traffic Drum
	1 Temporary Pavement Markings, 6 In., Solid, White
	2 Temporary Pavement Markings, 6 In., Solid, Yellow
	3 Temporary Pavement Markings, 6 In., Broken, White
	5 Temporary Transverse Pavement Markings, 24 In., Solid, Stop Line
	6 Temporary Transverse Pavement Markings, 24 In., Solid, White
	7 Temporary Pavement Message Marking, Lane Indication Arrow

DRAFT
NOT FOR CONSTRUCTION

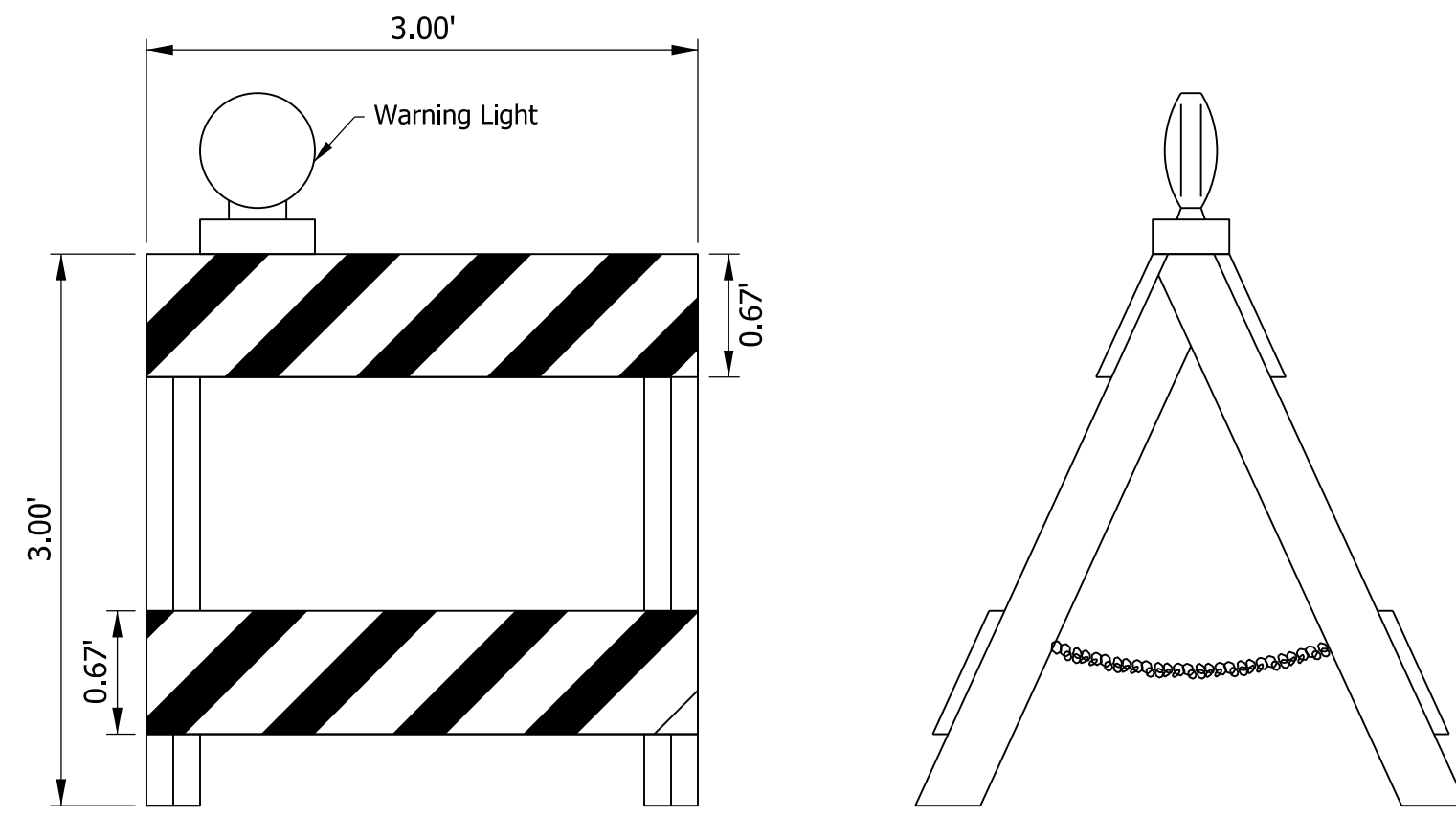
RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: NSX	DRAWN: HDW	
CHECKED: TNE	CHECKED: NSX	

INDIANA
DEPARTMENT OF TRANSPORTATION

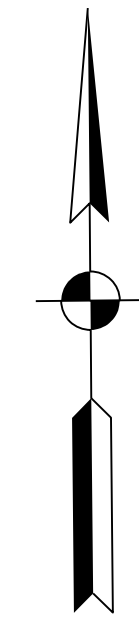
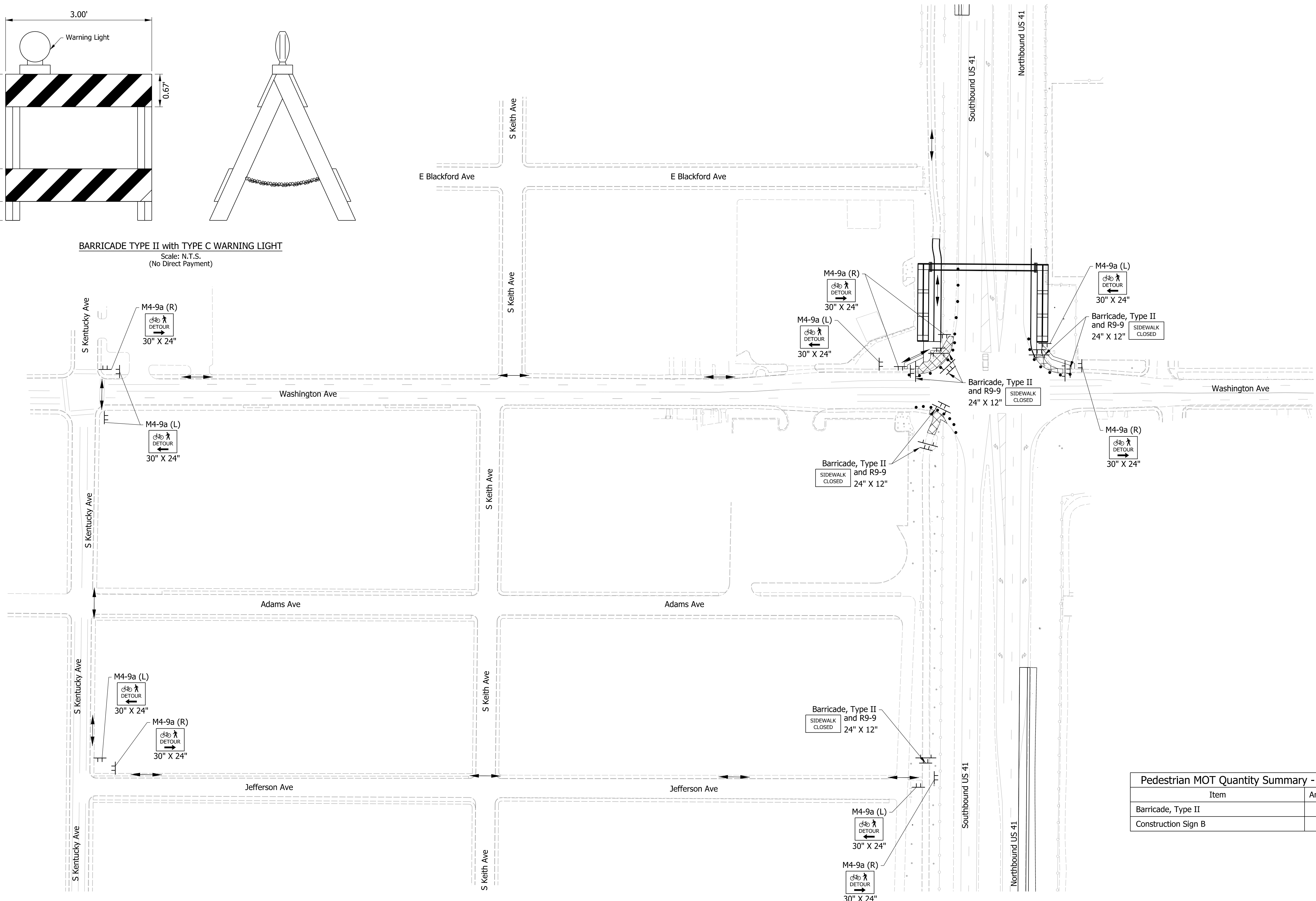
MAINTENANCE OF TRAFFIC
PHASE 3A

HORIZONTAL SCALE	BRIDGE FILE
1"=50'	N/A
VERTICAL SCALE	DESIGNATION
N/A	2000186
SURVEY BOOK	SHEETS MOT-09
ELECTRONIC	14 of 54
CONTRACT	PROJECT
B-42876	2000186

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BARRICADE TYPE II with TYPE C WARNING LIGHT
 Scale: N.T.S.
 (No Direct Payment)



Pedestrian MOT Quantity Summary - Phase 3A		
Item	Amount	Unit
Barricade, Type II	8	EACH
Construction Sign B	20	EACH

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LEGEND	
	Construction Sign, B
	Barricade, Type II with Type C Warning Light
	Drum
	Direction of Pedestrian Travel
	Construction Area

DRAFT
 NOT FOR CONSTRUCTION

RECOMMENDED FOR APPROVAL _____	DESIGN ENGINEER _____	DATE _____
DESIGNED: _____ HDW	DRAWN: _____ HDW	
CHECKED: _____ TNE	CHECKED: _____ AJP	

INDIANA DEPARTMENT OF TRANSPORTATION
PEDESTRIAN MAINTENANCE OF TRAFFIC PHASE 3A

HORIZONTAL SCALE	BRIDGE FILE
CUSTOM 75	N/A
VERTICAL SCALE	DESIGNATION
N/A	2000186
SURVEY BOOK	SHEETS MOT-11
ELECTRONIC	16 of 56
CONTRACT	PROJECT
B-42876	2000186

CONSTRUCTION SEQUENCING

PHASE 3B:

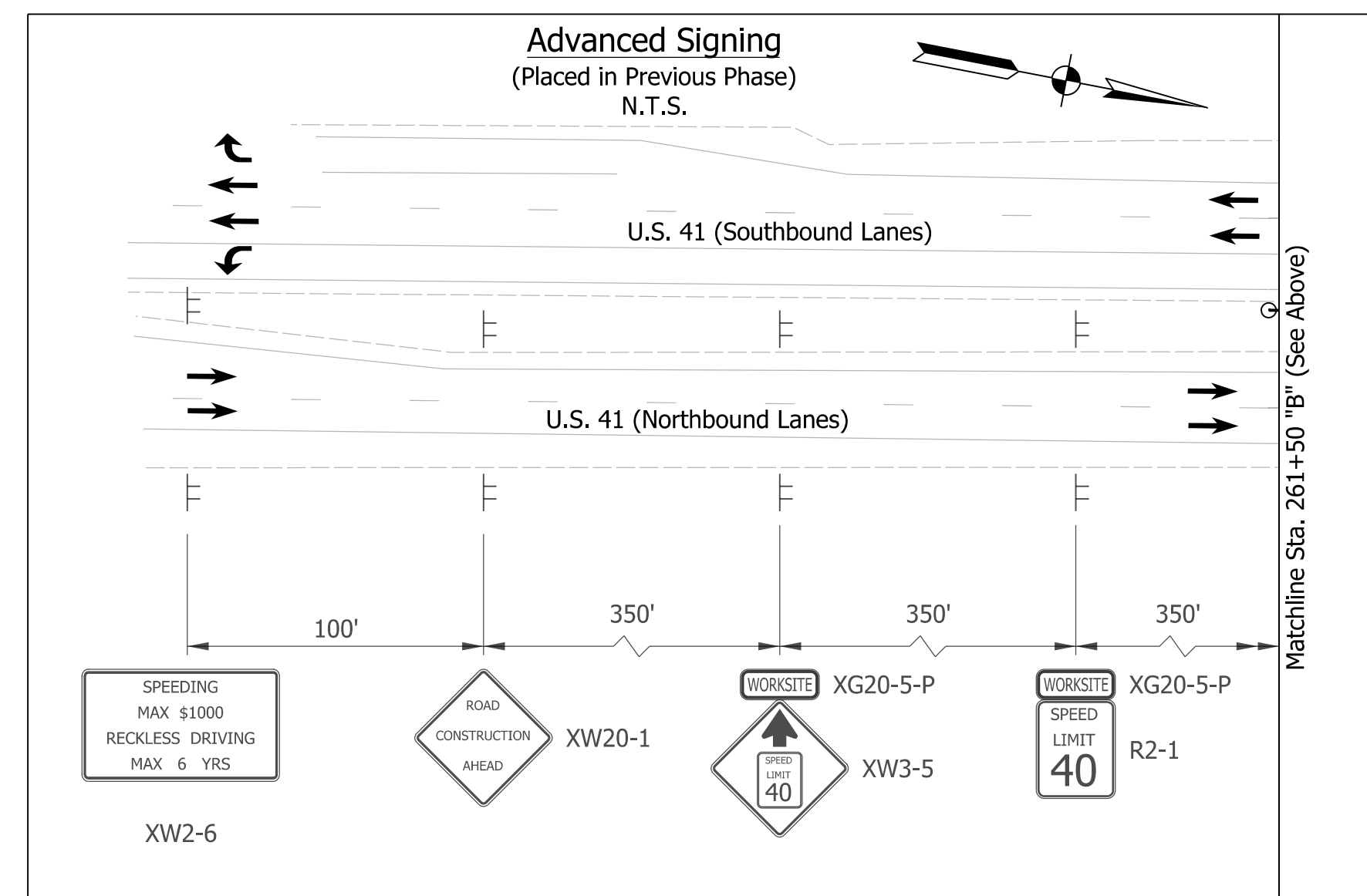
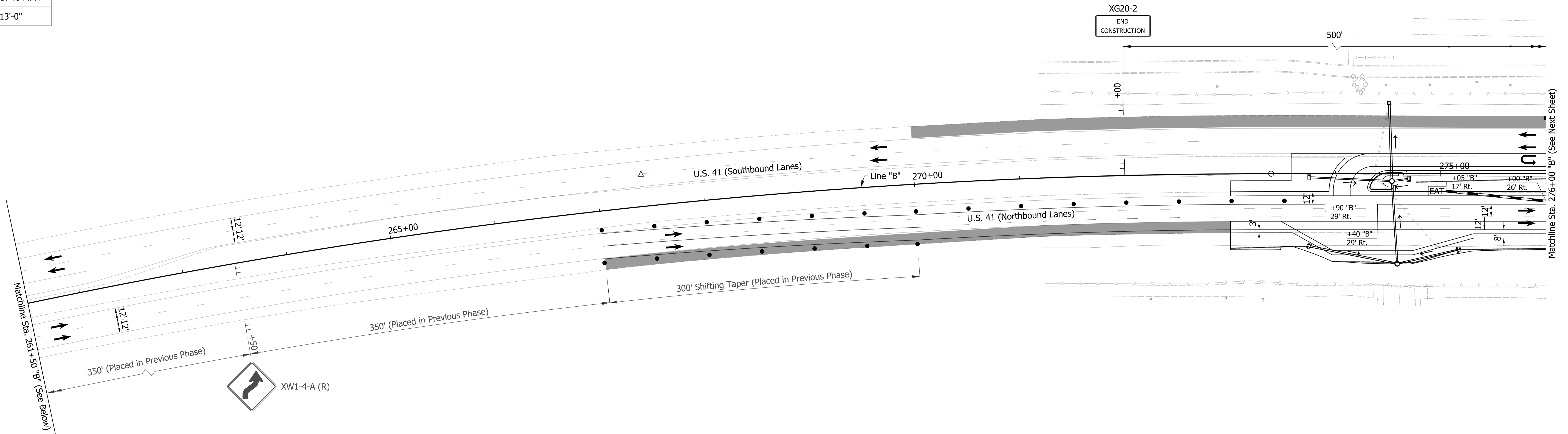
1. Remove any conflicting temporary Pavement Markings from Phase 3A along US 41.
2. Install temporary Pavement Markings and Traffic Drums.
3. Install Permanent Pavement Markings on and adjacent to Completed U-Turn Lanes along US 41.
4. Place temporary Traffic Barrier adjacent to US 41 workzones.
5. Finish constructing the Northbound and Southbound Median U-Turn Lanes and Median Widening along US 41 and Remove Existing Left Turn Lanes and Intersection Pavement.

Construction Design Speed: 40 MPH

Construction Clear Zone: 13'-0"

Maintenance of Traffic Quantity Summary - Phase 3B

Item	Amount	Unit
Construction Sign A	4	EACH
Construction Sign B	4	EACH
Temporary Pavement Marking, Removable, 6 In.	2,877	LFT
Temporary Traffic Barrier, Type 2	2,743	LFT
Energy Absorbing Terminal, CZ, Type-2	2	EACH



Notes:

1. Screened Pavement Markings and Signs were placed in Construction Phase 3A
2. Contractor shall maintain a 2' Minimum Shoulder at all times
3. All Sign Spacing shall be 350' Minimum unless otherwise noted
4. Traffic Drum Spacing along Tapers shall be at 25'
5. Traffic Drum Spacing along Tangents shall be at 50'
6. All temporary Pavement Markings shall be Removable
7. Existing Transverse Rumble Strips to remain in place
8. No changes to the Traffic Signal at US 41 and Washington Ave. Intersection

LEGEND

- | | | | |
|---------------------------|--------------------------------------|---|--|
| Construction Zone | Energy Absorbing Terminal CZ, Type 2 | Temporary Pavement Markings, 6 In., Solid, White | Temporary Transverse Pavement Markings, 24 In., Solid, Stop Line |
| Temporary Pavement | Construction Sign | Temporary Pavement Markings, 6 In., Solid, Yellow | Temporary Transverse Pavement Markings, 24 In., Solid, White |
| Direction of Traffic | Traffic Drum | Temporary Pavement Markings, 6 In., Broken, White | Temporary Pavement Message Marking, Lane Indication Arrow |
| Temporary Traffic Barrier | | | |

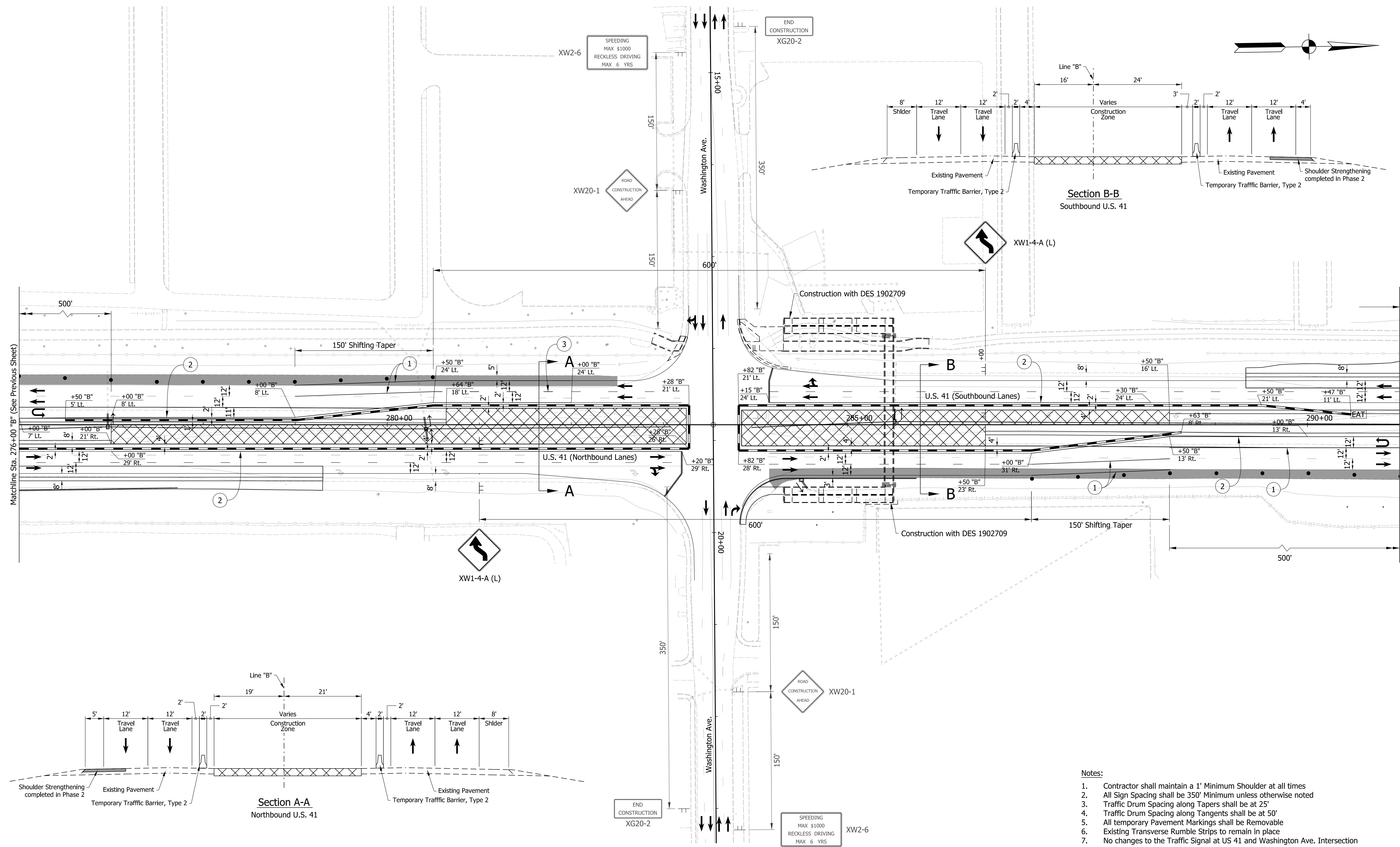
DRAFT
NOT FOR CONSTRUCTION

RECOMMENDED FOR APPROVAL _____	DESIGN ENGINEER _____	DATE _____
DESIGNED: _____ NSX _____	DRAWN: _____ HDW _____	
CHECKED: _____ TNE _____	CHECKED: _____ NSX _____	

INDIANA
DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC
PHASE 3B

HORIZONTAL SCALE	BRIDGE FILE
1"=50'	N/A
VERTICAL SCALE	DESIGNATION
N/A	2000186
SURVEY BOOK	SHEETS MOT-10
ELECTRONIC	15 of 54
CONTRACT	PROJECT
B-42876	2000186



Construction Zone	Energy Absorbing Terminal CZ, Type 2	1 Temporary Pavement Markings, 6 In., Solid, White	5 Temporary Transverse Pavement Markings, 24 In., Solid, Stop Line
Temporary Pavement	Construction Sign	2 Temporary Pavement Markings, 6 In., Solid, Yellow	6 Temporary Transverse Pavement Markings, 24 In., Solid, White
Direction of Traffic	Traffic Drum	3 Temporary Pavement Markings, 6 In., Broken, White	7 Temporary Pavement Message Marking, Lane Indication Arrow
Temporary Traffic Barrier			

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NOT FOR CONSTRUCTION

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: NSX	DRAWN: HDW	
CHECKED: TNE	CHECKED: NSX	

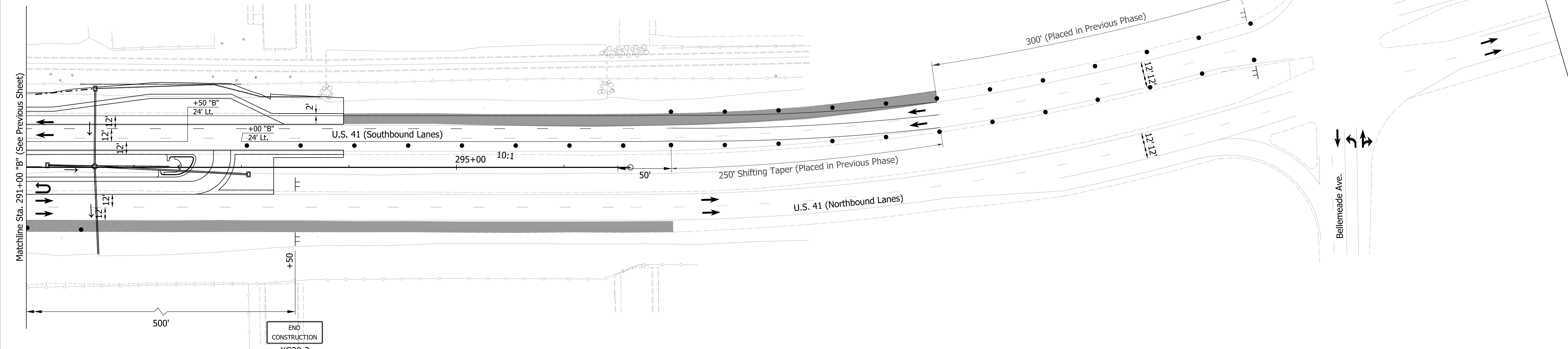
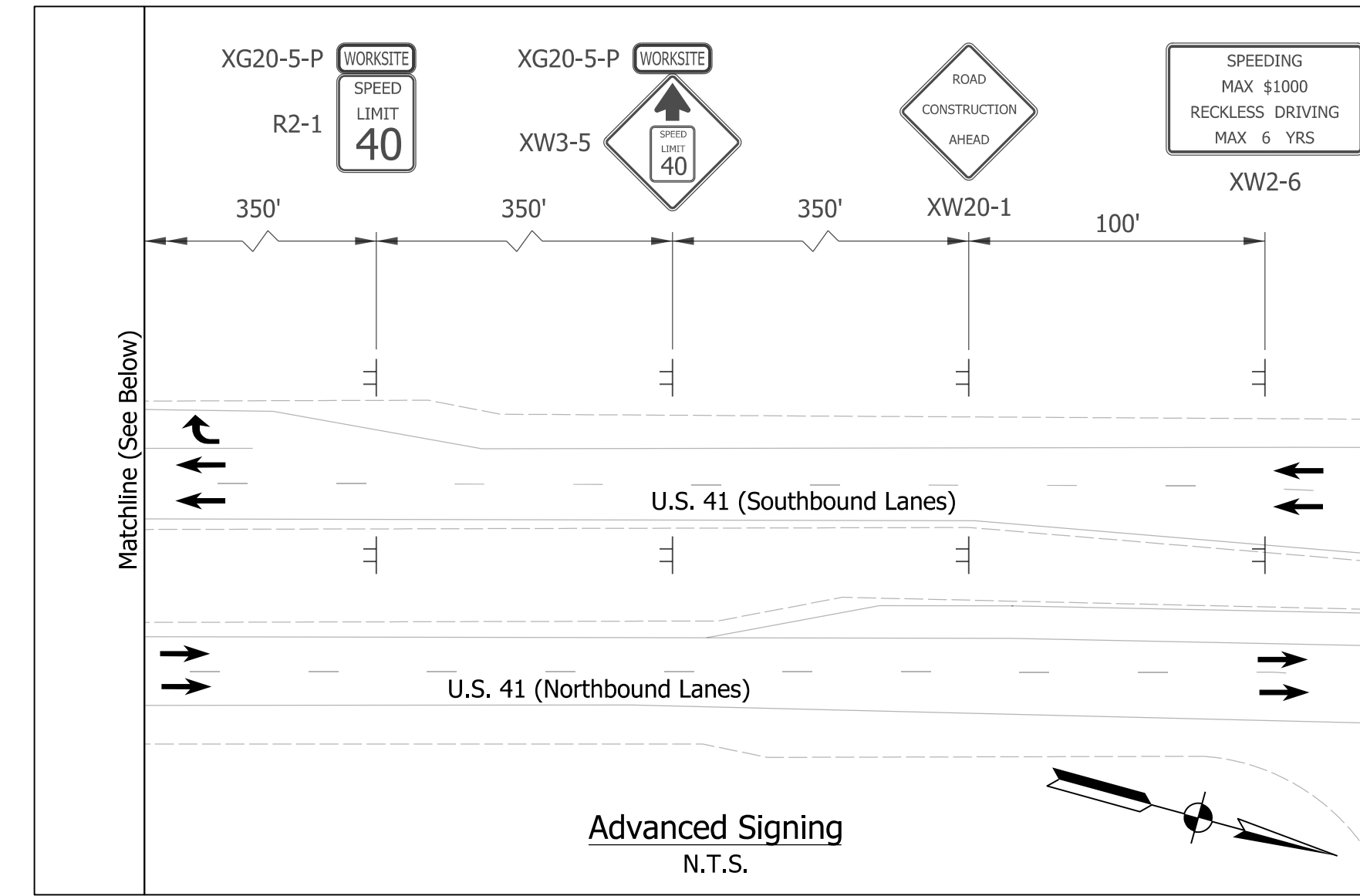
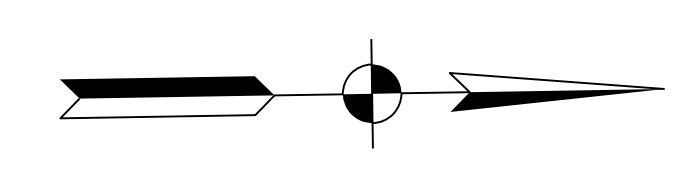
INDIANA
DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC
PHASE 3B

HORIZONTAL SCALE	BRIDGE FILE
1"=50'	N/A
VERTICAL SCALE	DESIGNATION
N/A	2000186
SURVEY BOOK	SHEETS
ELECTRONIC	16 of 54
CONTRACT	PROJECT
B-42876	2000186

- Notes:**
- Contractor shall maintain a 1' Minimum Shoulder at all times
 - All Sign Spacing shall be 350' Minimum unless otherwise noted
 - Traffic Drum Spacing along Tapers shall be at 25'
 - Traffic Drum Spacing along Tangents shall be at 50'
 - All temporary Pavement Markings shall be Removable
 - Existing Transverse Rumble Strips to remain in place
 - No changes to the Traffic Signal at US 41 and Washington Ave. Intersection

hvilhelm
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 model:Sheet2
 file:c:\additdb\pv\hvilhelm\greatlakes\0286823179067-rd-s-mat02b.dgn



Matchline Sta. 291+00 "B" (See Previous Sheet)

Matchline (See Above)

LEGEND	
	Construction Zone
	Temporary Pavement
	Direction of Traffic
	Temporary Traffic Barrier
	Energy Absorbing Terminal CZ, Type 2
	Construction Sign
	Traffic Drum
	1 Temporary Pavement Markings, 6 In., Solid, White
	2 Temporary Pavement Markings, 6 In., Solid, Yellow
	3 Temporary Pavement Markings, 6 In., Broken, White
	5 Temporary Transverse Pavement Markings, 24 In., Solid, Stop Line
	6 Temporary Transverse Pavement Markings, 24 In., Solid, White
	7 Temporary Pavement Message Marking, Lane Indication Arrow

DRAFT
NOT FOR CONSTRUCTION

RECOMMENDED FOR APPROVAL _____	DESIGN ENGINEER _____	DATE _____
DESIGNED: _____ NSX _____	DRAWN: _____ HDW _____	
CHECKED: _____ TNE _____	CHECKED: _____ NSX _____	

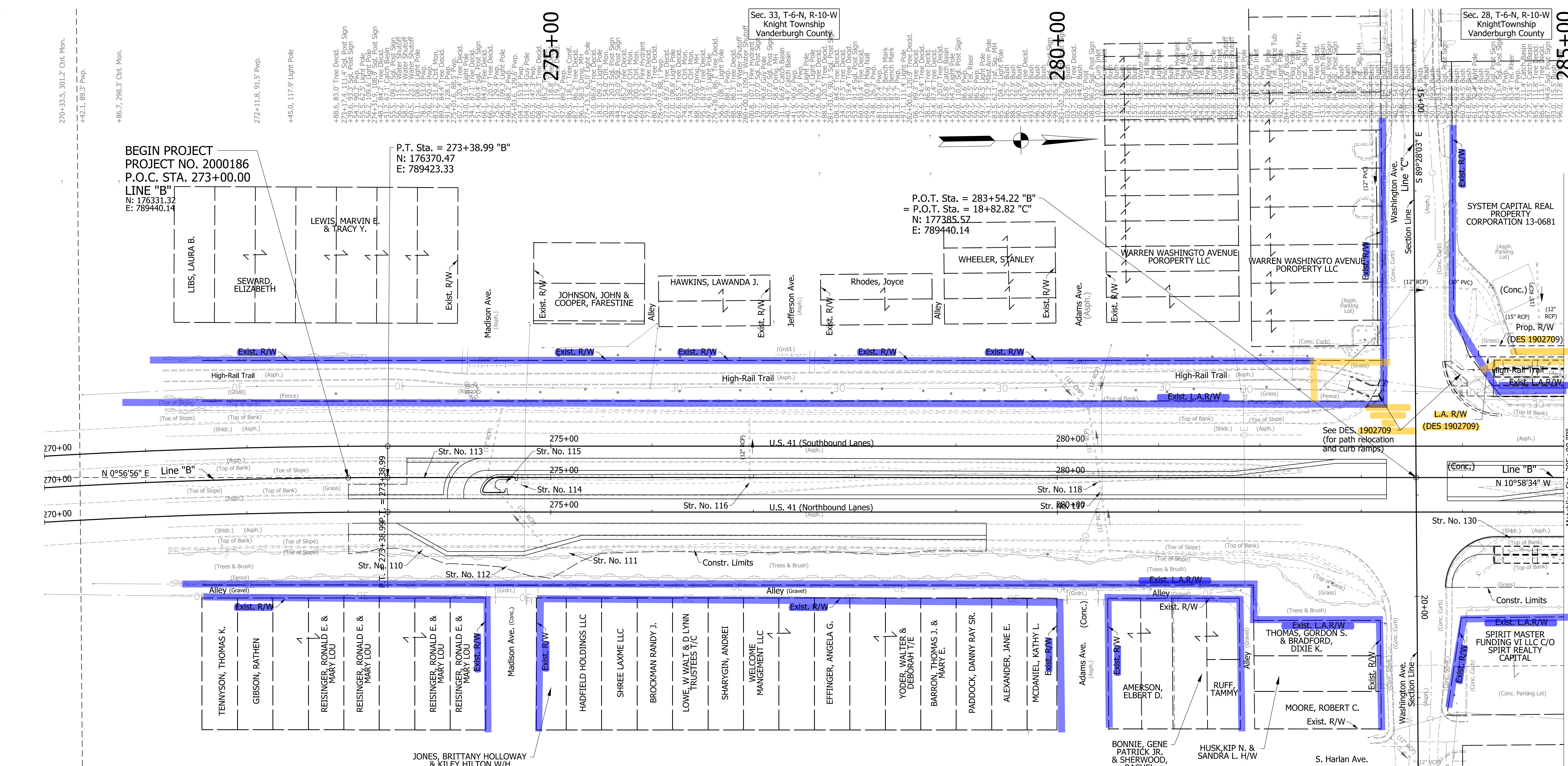
INDIANA
DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC
PHASE 3B

HORIZONTAL SCALE	BRIDGE FILE
1"=50'	N/A
VERTICAL SCALE	DESIGNATION
N/A	2000186
SURVEY BOOK	SHEETS MOT-12
ELECTRONIC	17 of 54
CONTRACT	PROJECT
B-42876	2000186

- Notes:
1. Screened Pavement Markings and Signs were placed in Construction Phase 3A
 2. Contractor shall maintain a 2' Minimum Shoulder at all times
 3. All Sign Spacing shall be 350' Minimum unless otherwise noted
 4. Traffic Drum Spacing along Tapers shall be at 25'
 5. Traffic Drum Spacing along Tangents shall be at 50'
 6. All temporary Pavement Markings shall be Removable
 7. Existing Transverse Rumble Strips to remain in place
 8. No changes to the Traffic Signal at US 41 and Washington Ave. Intersection

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 3/25/2022 8:53:09 am
 model-Sheet3
 file:c:\adidib\p\wilhelm\greatlakes\0286823\79067-rd-s-mot03b.dgn



BEGIN PROJECT
PROJECT NO. 2000186
P.O.C. STA. 273+00.00
LINE "B"
N: 176331.32
E: 789440.14

P.T. Sta. = 273+38.99 "B"
N: 176370.47
E: 789423.33

P.O.T. Sta. = 283+54.22 "B"
= P.O.T. Sta. = 18+82.82 "C"
N: 177385.57
E: 789440.14

Curve Data
P.I. Sta. 267+44.90 "B"
(Off the Sheet)
N: 175772.14
E: 789413.42
Delta = 11°55'30" Rt.
R = 5729.58 Ft.
T = 598.41 Ft.
L = 1192.50 Ft.
M = 31.17 Ft.
SE = N.A.

P.C. Sta. 261+46.49 "B"
(Off the Sheet)
N: 175184.68
E: 789527.35

Sec. 33, T-6-N, R-10-W
Knight Township
Vanderburgh County

Sec. 28, T-6-N, R-10-W
Knight Township
Vanderburgh County

DRAFT
NOT FOR CONSTRUCTION

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: NSX	DRAWN: CPH	
CHECKED: TNE	CHECKED: NSX	

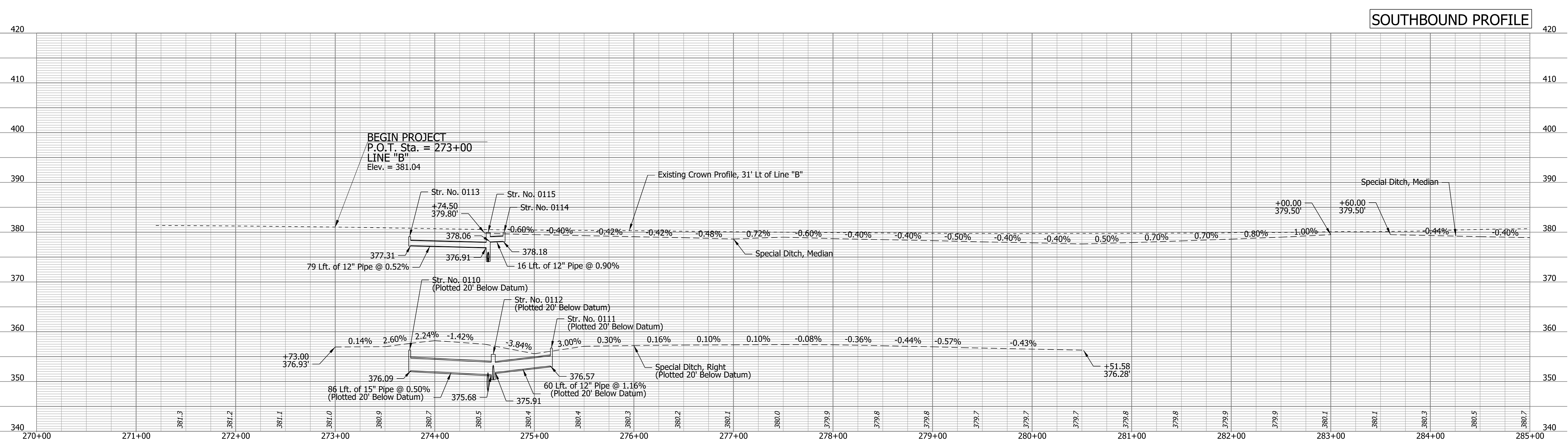
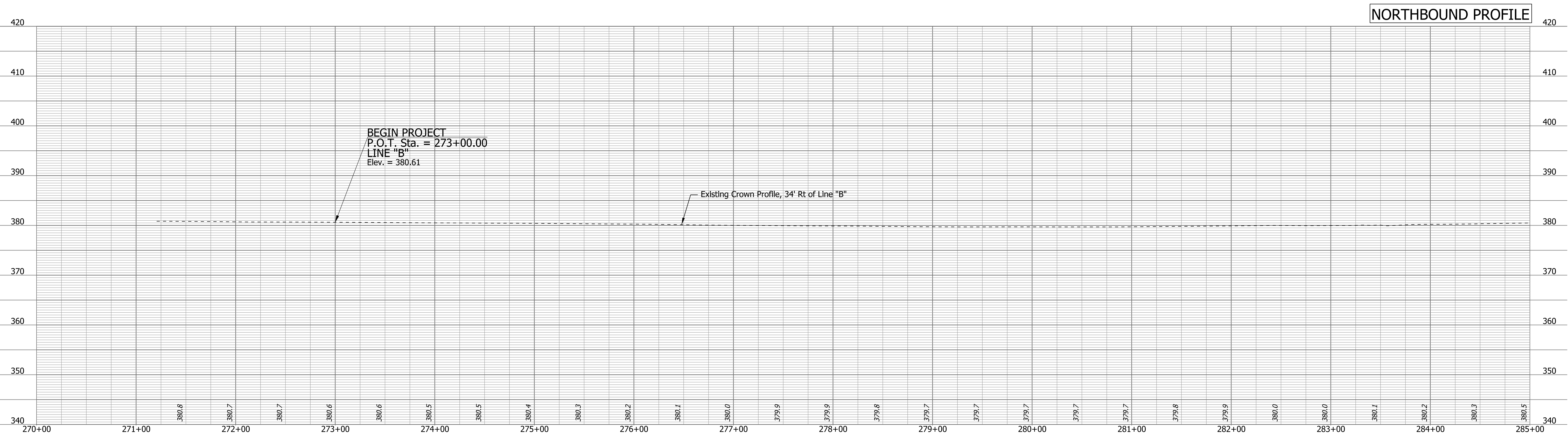
INDIANA DEPARTMENT OF TRANSPORTATION

**ROADWAY PLAN
US 41 - LINE "B"
STA. 270+00 TO STA. 285+00**

HORIZONTAL SCALE	BRIDGE FILE
1"=50'	N/A
VERTICAL SCALE	DESIGNATION
1"=10'	2000186
SURVEY BOOK	SHEETS
ELECTRONIC	12 of 45
CONTRACT	PROJECT
B-42876	2000186

apacad
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 model-sheet1
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Blue - Existing right-of-way
Orange - Right of way to be acquired for Pedestrian Bridge Des. No. 1902709



Note:
The existing profiles, at the crown, are provided for information only. No revisions required to the profile.

DRAFT
NOT FOR CONSTRUCTION

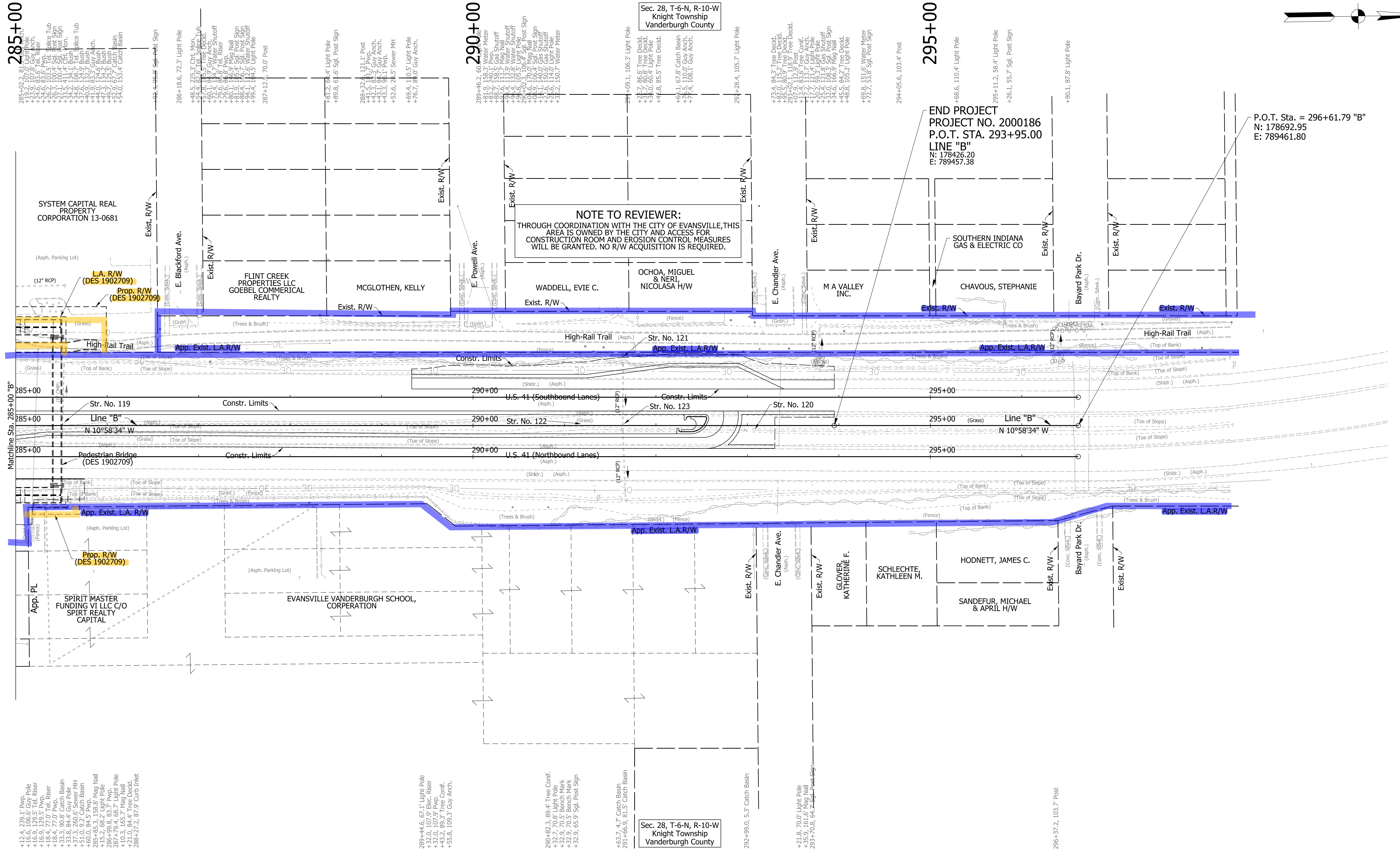
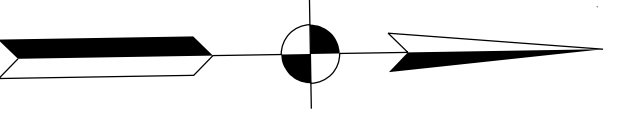
RECOMMENDED FOR APPROVAL _____	DESIGN ENGINEER _____	DATE _____
DESIGNED: _____ NSX _____	DRAWN: _____ CPH _____	
CHECKED: _____ TNE _____	CHECKED: _____ NSX _____	

INDIANA
DEPARTMENT OF TRANSPORTATION

ROADWAY PROFILE
US 41 - LINE "B"
STA. 270+00 TO STA. 285+00

HORIZONTAL SCALE 1"=50'	BRIDGE FILE N/A	
VERTICAL SCALE 1"=10'	DESIGNATION 2000186	
SURVEY BOOK ELECTRONIC	13	PLN-02 of 45
CONTRACT B-42876	PROJECT 2000186	

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 model-sheet3
 file: c:\cad\itb\pw\lgarvin\lgarvin\p067-rt-s-pln01.dgn



285+00

290+00

295+00

- +12.4, 239.1 Pwp.
- +16.6, 106.6' Guy Pole
- +16.9, 129.5' Pwp.
- +18.4, 77.0' Tel. Riser
- +18.4, 77.0' Pwp.
- +33.8, 84.4' Catch Basin
- +37.3, 260.6' Sewer MH
- +51.0, 8.2' Catch Basin
- +60.0, 84.5' Pwp. Man Nail
- +68.7, 79.4' Pwp.
- +103.3, 165.7' Man Nail
- +288.2, 72.2, 87.9' Curb Inlet

- 289+44.6, 67.1' Light Pole
- +32.0, 107.9' Elec. Riser
- +32.0, 107.9' Pwp.
- +55.8, 109.3' Guy Anch.

- 290+82.3, 89.4' Tree Confir.
- +32.9, 70.5' Bench Mark
- +32.9, 70.5' Bench Mark
- +32.9, 65.9' Sgl. Post Sign

- +63.7, 4.7' Catch Basin
- 291+66.5, 81.5' Catch Basin

- 292+99.0, 5.3' Catch Basin

- +21.8, 70.0' Light Pole
- 293+70.8, 64.7' Sgl. Post Sign

- 296+37.2, 103.7' Post

DRAFT
NOT FOR CONSTRUCTION

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: NSX	DRAWN: CPH	
CHECKED: TNE	CHECKED: NSX	

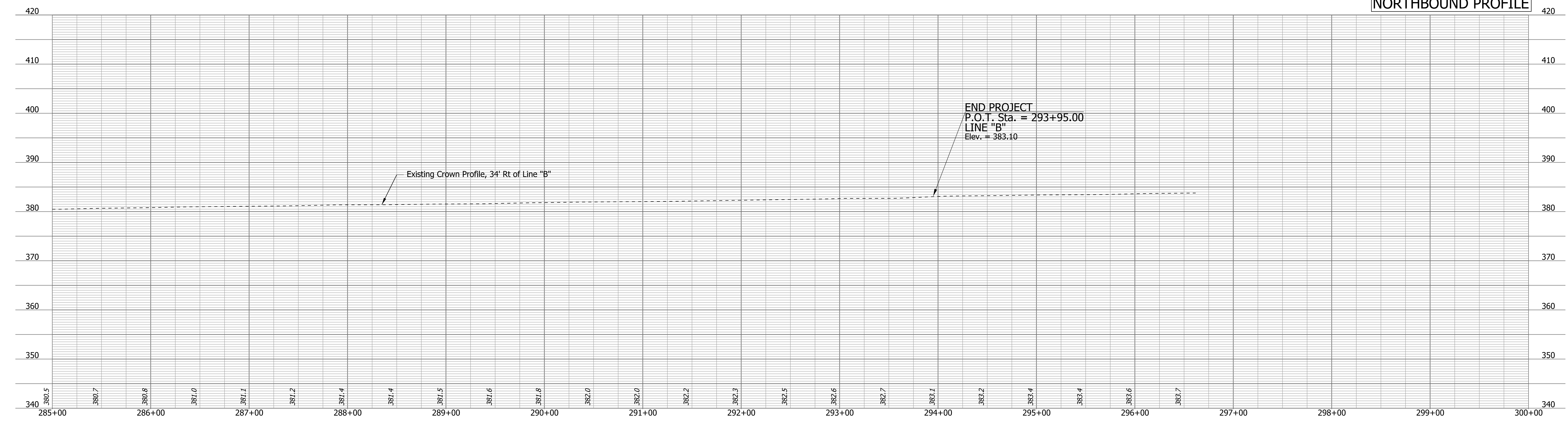
INDIANA DEPARTMENT OF TRANSPORTATION
ROADWAY PLAN
US 41 - LINE "B"
STA. 285+00 TO STA. 296+61.79

HORIZONTAL SCALE	BRIDGE FILE	
1"=50'	N/A	
VERTICAL SCALE	DESIGNATION	
1"=10'	2000186	
SURVEY BOOK	SHEETS	PLN-03
ELECTRONIC	14	of 45
CONTRACT	PROJECT	
8-42876	2000186	

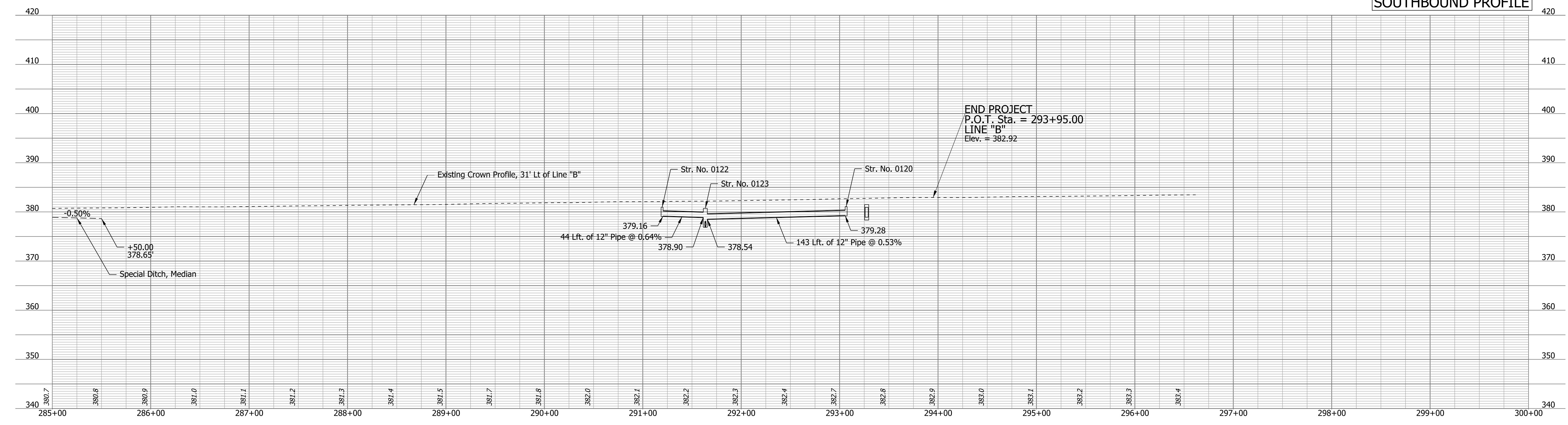
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Blue - Existing right-of-way
Orange - Right of way to be acquired for Pedestrian Bridge Des. No. 1902709

NORTHBOUND PROFILE



SOUTHBOUND PROFILE



Note:
The existing profiles, at the crown, are provided for information only. No revisions required to the profile.

DRAFT
NOT FOR CONSTRUCTION

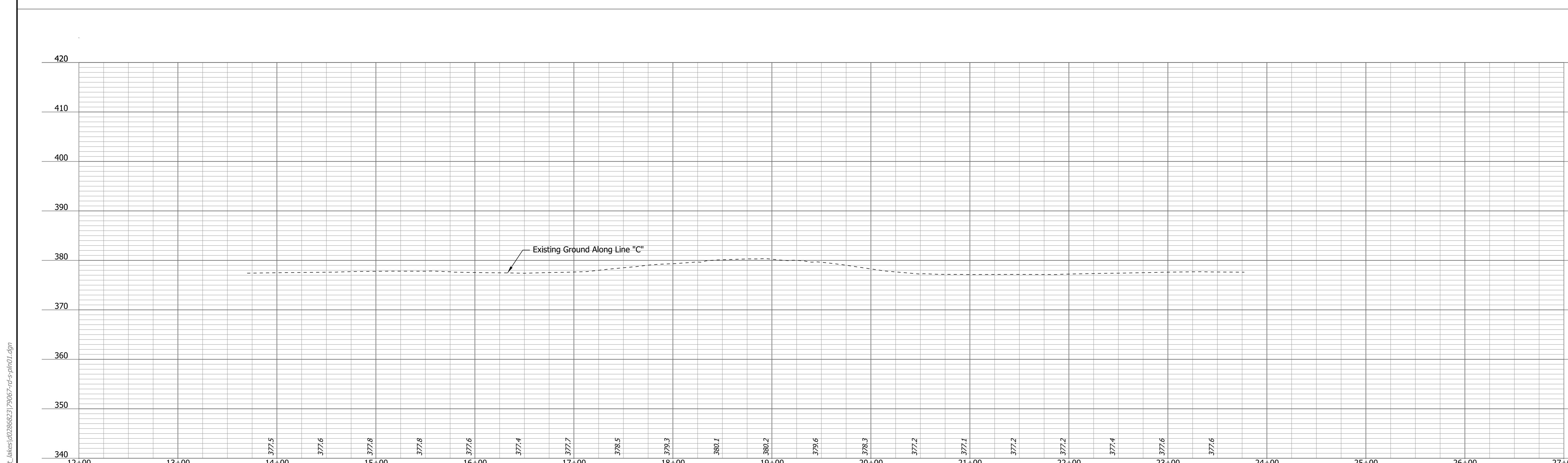
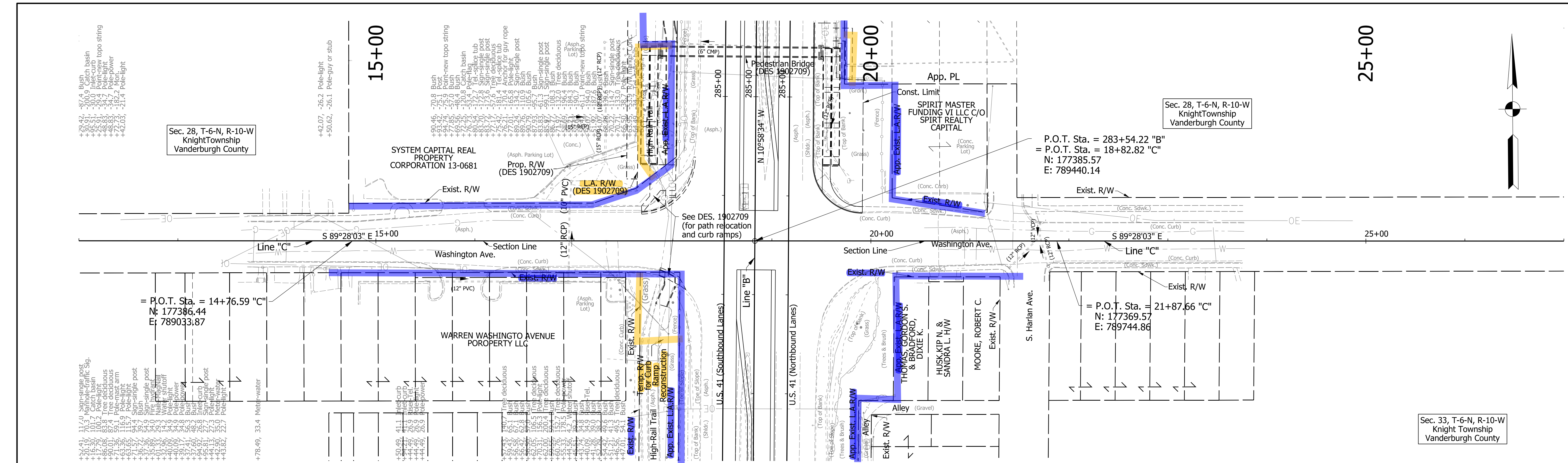
RECOMMENDED FOR APPROVAL _____	DESIGN ENGINEER _____	DATE _____
DESIGNED: _____ NSX _____	DRAWN: _____ CPH _____	
CHECKED: _____ TNE _____	CHECKED: _____ NSX _____	

INDIANA
DEPARTMENT OF TRANSPORTATION

ROADWAY PROFILE
US 41 - LINE "B"
STA. 285+00 TO STA. 296+61.79

HORIZONTAL SCALE	BRIDGE FILE	
1"=50'	N/A	
VERTICAL SCALE	DESIGNATION	
1"=10'	2000186	
SURVEY BOOK	SHEETS	PLN-04
ELECTRONIC	15 of	45
CONTRACT	PROJECT	
B-42876	2000186	

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 model-Sheet4
 file:c:\additb\pvt\lgarvin\pvgreat_bikes\0286623\9067-rd-s-pln01.dgn



<p>apxacad</p> <p>3/29/2022 12:31:25 pm</p> <p>model:Sheet5</p> <p>file:c:\cad\lib\pwr\lgarvin\lwgreat_bkes\028662379067-rd-s-pln01.dgn</p>	<p>Note:</p> <p>The existing profiles, at the crown, are provided for information only. No revisions required to the profile.</p>	<p>RECOMMENDED FOR APPROVAL _____</p> <p>DESIGN ENGINEER _____ DATE _____</p>		<p>INDIANA DEPARTMENT OF TRANSPORTATION</p> <p>ROADWAY PLAN AND PROFILE WASHINGTON AVE - LINE "C" STA. 12+00 TO STA. 27+00</p>		<p>SCALE 1"=50'</p> <p>VERTICAL SCALE 1"=10'</p>		<p>BRIDGE FILE N/A</p> <p>DESIGNATION 2000186</p>	
		<p>DESIGNED: NSX DRAWN: CPH</p> <p>CHECKED: TNE CHECKED: NSX</p>				<p>SURVEY BOOK N/A</p> <p>CONTRACT B-42876</p>		<p>SHEETS 16 of 45</p> <p>PROJECT 2000186</p>	

Blue - Existing right-of-way
 Orange - Right of way to be acquired for Pedestrian Bridge Des. No. 1902709